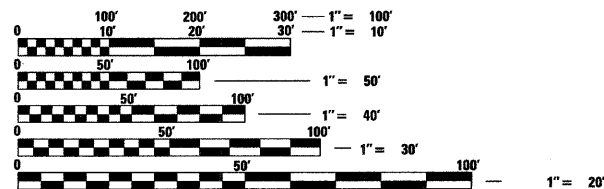


INDEX OF SHEETS

- 1 COVER SHEET
- 2 GENERAL NOTES AND COMMITMENTS
- 3 SUMMARY OF QUANTITIES
- 4-9 TYPICAL SECTIONS
- 10-11 SCHEDULES
- 12 ALIGNMENT AND TIES
- 13-16 PLAN AND PROFILE SHEETS
- 17-20 SUGGESTED STAGING SHEETS
- 21-25 STORM WATER POLLUTION PREVENTION PLAN SHEETS
- 26-28 DRAINAGE PLAN SHEETS
- 29-30 INTERSECTION DETAIL SHEETS
- 31-34 PAVEMENT MARKING PLANS & DETAILS
- 35-43 TRAFFIC SIGNALS SHEETS
- 44-46 SIGNING SHEETS
- 47 DETAILS
- 48 CULVERT SECTION FOURNE LANE
- 49 CULVERT SECTION EASTPORT PLAZA DRIVE
- 50-54 STORM SEWER SECTIONS HORSESHOE LAKE ROAD
- 55-64 CROSS SECTION HORSESHOE LAKE ROAD

HIGHWAY STANDARDS:

- 006001-05 STANDARDS SYMBOLS, ABBREVIATIONS AND PATTERNS
- 280001-04 TEMPORARY EROSION CONTROL SYSTEMS
- 442201-03 CLASS C AND D PATCHES
- 482006-03 HMA SHOULDER ADJACENT TO RIGID PAVEMENT
- 542301-01 PRECAST REINFORCED CONCRETE FLARED END SECTION
- 542401 METAL END SECTION FOR PIPE CULVERTS
- 542406 METAL END SECTIONS FOR PIPE ARCHES
- 601001-02 SUBSURFACE DRAINS
- 601101 CONCRETE HEADWALL FOR PIPE DRAIN
- 602301-01 INLET, TYPE A
- 604061-01 FRAME AND GRATE, TYPE 12
- 606001-03 CONCRETE CURB, TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- 606301-03 PC CONCRETE ISLANDS AND MEDIANS
- 665001-01 WOVEN WIRE FENCE
- 701001-01 OFF ROAD OPERATIONS, 2L, 2W, MORE THAN 4.5 m (15') AWAY > 45 MPH
- 701006-02 OFF ROAD OPERATIONS, 2L, 2W, 4.5 m (15') TO 600 mm (24") FROM PAVEMENT EDGE > 45 MPH
- 701011-01 OFF ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY > 45MPH
- 701201-02 LANE CLOSURE, 2L, 2W, - DAY ONLY FOR SPEEDS > 45 MPH
- 701301-02 LANE CLOSURE, 2L, 2W - SHORT TIME OPERATIONS
- 701306-01 LANE CLOSURE, 2L, 2W - SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS > 45 MPH
- 701311-02 LANE CLOSURE, 2L, 2W - MOVING OPERATIONS DAY ONLY
- 701326-02 LANE CLOSURE, 2L, 2W - PAVEMENT WIDENING FOR SPEEDS > 45 MPH
- 701336-04 LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES FOR SPEEDS > 45 MPH
- 701701-05 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701901 TRAFFIC CONTROL DEVICES
- 720001 SIGN PANEL MOUNTING DETAILS
- 720006-01 SIGN PANEL ERECTION DETAILS
- 720011 METAL POSTS FOR SIGNS, MARKERS, & DELINEATORS
- 720021-01 SIGN PANELS EXTRUDED ALUMINUM TYPE
- 780001-01 TYPICAL PAVEMENT MARKINGS
- 781001-02 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
- 805001 ELECTRIC SERVICE INSTALLATION DETAILS
- 814001-01 HANDHOLES
- 814006-01 DOUBLE HANDHOLES
- 857001-06 STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
- 873001-01 TRAFFIC SIGNAL GROUNDING & BONDING
- 877001-03 STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
- 878001-06 CONCRETE FOUNDATION DETAILS
- 880006 TRAFFIC SIGNAL MOUNTING DETAILS
- 886001 DETECTOR LOOP INSTALLATIONS
- 886006 TYPICAL LAYOUT FOR DETECTION LOOPS



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER – PATTI LEBEAU
PROJECT MANAGER – CHERYL KEPLAR
CONTRACT NO. 76B22

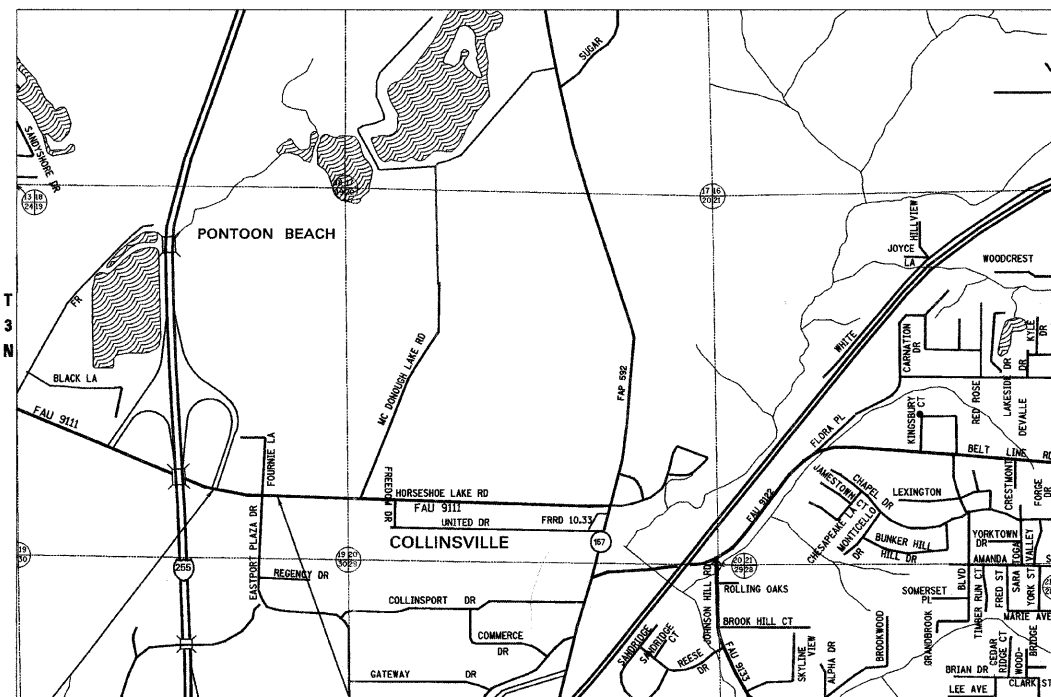
DESIGN DESIGNATION
(TO BE PROVIDED BY IDOT)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

PROPOSED
HIGHWAY PLANS

FAU ROUTE 9111
(HORSESHOE LAKE RD)
SECTION 73-15TS
PROJECT: ACHSIP-ACM-9111(001)
MADISON COUNTY
C-98-101-07

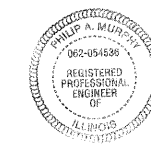


BEGIN SECTION 73-15TS STA. 225 + 06.5
END SECTION 73-15TS STA. 242 + 85 R 8 W

LOCATION MAP



SECTION GROSS LENGTH = 1,778.5' = 0.337 MILES
SECTION NET LENGTH = 1,778.5' = 0.337 MILES



EXPIRES 11-30-2009

Philip Murphy
PHILIP A. MURPHY, P.E.

DATE: 3/18/08

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9111	73-15TS	MADISON	64	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 76B22		

64+4=68

D-98-097-07



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED March 18, 2008

Mary C. Jumei
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 9, 2008
Eric E. Haran
ENGINEER OF DESIGN AND ENVIRONMENT

May 9, 2008
Christie M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS



QATES ASSOCIATES
Consulting Engineers

GENERAL NOTES

COMMITMENTS

- 1 UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND RECORDS. THEIR LOCATIONS MUST BE CONSIDERED TO BE APPROXIMATE ONLY. IT IS POSSIBLE THERE MAY BE OTHERS, THE EXISTENCE OF WHICH IS NOT PRESENTLY KNOWN OR SHOWN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THEIR EXISTENCE AND EXACT LOCATION AND TO AVOID DAMAGE THERETO.
- 2 ANY FACILITIES OR APPURTENANCES WHICH ARE THE PROPERTY OF ANY PUBLIC UTILITY LOCATED WITHIN THE LIMITS OF CONSTRUCTION SHALL BE RELOCATED OR ADJUSTED BY THEIR RESPECTIVE OWNERS. THE CONTRACTOR SHALL NOTIFY AND COOPERATE WITH THE OWNERS OF ANY SUCH FACILITY IN THEIR REMOVAL AND REARRANGEMENT OPERATIONS IN ORDER THAT THESE OPERATIONS AND THE CONSTRUCTION OF THIS PROJECT MAY PROGRESS IN A REASONABLE MANNER. ALL ROADSIDE OBJECTS (UTILITY POLES, FIRE HYDRANTS, SIGNS, ETC.) SHALL BE RELOCATED TO PROVIDE A MINIMUM OF 2 FEET CLEARANCE, MEASURED FROM THE FACE OF CURB TO THE NEAR EDGE OF THE OBJECT.
- 3 ILLINOIS STATE LAW REQUIRES A 48 - HOUR NOTICE TO BE GIVEN TO UTILITIES BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J.U.L.I.E. OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:

UTILITY COMPANY	TYPE
*AMERENIP	GAS & ELECTRIC
*AT&T CORPORATION	COMMUNICATIONS
*CHARTER COMMUNICATIONS, INC.	CABLE TV
*CITY OF COLLINSVILLE	WATER & SANITARY SEWER
*AT&T ILLINOIS	COMMUNICATIONS
*SOUTHWESTERN ELECTRIC COOPERATIVE, INC.	ELECTRIC

*MEMBERS OF JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS (J.U.L.I.E.), THE J.U.L.I.E. SYSTEM PHONE NUMBER IS 1-800-892-0123. NON-J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.
- 4 PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL CONTACT THE LOCAL POSTMASTER FOR APPROVAL OF THE TEMPORARY MAILBOX LOCATIONS.
- 5 THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO ORDERING MATERIALS AND COMMENCING CONSTRUCTION.
- 6 THE CONTRACTOR SHALL STAGE ALL WORK IN SUCH A WAY AS TO MAINTAIN INGRESS AND EGRESS TO ALL ADJUTING PROPERTIES AT ALL TIMES DURING CONSTRUCTION
- 7 WHEN THE MILLING OPERATION COMMENCES, "ROUGH GROOVED SURFACE" (W9-1106-(0)-(48) SIGNS SHALL BE PLACED BY THE CONTRACTOR AT EACH END OF THE SECTION, AND EACH INTERSECTING SIDE ROAD. THE COST OF THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICES BID FOR THE VARIOUS ITEMS OF WORK INVOLVED.
- 8 THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURES ARE PLACED.
- 9 STORM SEWER INVERTS SHOWN ON THE PLANS HAVE BEEN CALCULATED TO THE CENTER OF THE STRUCTURE. THE STORM SEWER SLOPES SHOWN ON THE PLANS IS THE PERCENT GRADE FROM CENTER TO CENTER OF STRUCTURE. THE LENGTH OF STORM SEWERS SHOWN ON THE PLANS IS THE DISTANCE FROM CENTER TO CENTER OF STRUCTURE. STORM SEWER SHALL BE MEASURED AND PAID FOR AS SPECIFIED IN ARTICLE 550.09 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION." FLARED END SECTIONS ARE LOCATED BY STATION, OFFSET AND FLOWLINE ELEVATION AT THE FLARED END OF THE FLARED END SECTION.
- 10 THE CONTRACTOR SHALL INCLUDE THE COST OF MAKING CONNECTIONS TO EXISTING DRAINAGE STRUCTURES IN THE CONTRACT UNIT PRICE FOR THE STORM SEWER ITEMS INVOLVED.
- 11 ALL DRAINAGE STRUCTURES CONSTRUCTED, ADJUSTED OR RECONSTRUCTED UNDER THE CONTRACT, SHALL BE CLEANED OF ANY ACCUMULATION OF SILT, DEBRIS OR FOREIGN MATTER AT THE END OF EACH WORKING DAY AND AT THE TIME OF FINAL INSPECTION. THE COST OF THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICES BID FOR THE VARIOUS DRAINAGE STRUCTURE ITEMS INCLUDED IN THE CONTRACT.
- 12 THE CONTRACTOR SHALL APPLY TEMPORARY PAVEMENT MARKINGS TO THE MILLED, PRIMED AND EACH HOT-MIX ASPHALT SURFACE LAYER. A QUANTITY FOR TEMPORARY PAVEMENT MARKING EQUAL TO THE AMOUNT OF PERMANENT PAVMENT MARKING TIMES THE NUMBER OF REQUIRED APPLICATIONS HAS BEEN INCLUDED IN THE PLANS.
- 13 THE TRAFFIC SIGNAL PLANS WERE DESIGNED AND PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION
- 14 REMOVAL OF EXISTING AGGREGATE SHOULDERS SHALL BE INCLUDED WITH EARTH EXCAVATION.

- 15 THE COST OF THE COARSE AGGREGATE THAT IS REQUIRED UNDER THE CONCRETE MEDIAN SURFACE SHALL BE INCLUDED IN THE COST OF THE CONCRETE MEDIAN SURFACE, 4".
- 16 SUGGESTED STAGING PLANS HAVE BEEN INCLUDED WITH THIS CONTRACT. SHOULD THE CONTRACTOR WANT TO MODIFY THE STAGING, A PROPOSAL SHALL BE SUBMITTED AND APPROVED BY THE ENGINEER PRIOR TO MODIFICATIONS.
- 17 THE GEOTECHNICAL ENGINEER SHALL BE NOTIFIED IN ADVANCE WHEN THE SUBGRADE IS EXPECTED TO BE COMPLETED. ARTICLE 301.04 OF THE STANDARD SPECIFICATIONS STATES THAT THE SUBGRADE SHALL HAVE A MINIMUM IMMEDIATE BEARING VALUE (IBV) OF 8. THIS WILL BE DETERMINED ACCORDING TO THE ILLINOIS TEST PROCEDURE 501 OR 502.
- 18 MIXTURE REQUIREMENTS - SUPERPAVE PROJECT

ROUTE	FAU 9111	HORSESHOE LAKE ROAD
SECTION	73-15TS	
COUNTY	MADISON	
CONTRACT	76B22	

DESCRIPTION:	WIDENING AND RESURFACING ON HORSESHOE LAKE ROAD AND INTERSECTION IMPROVEMENTS ON EASTPORT PLAZA DRIVE
--------------	---

ADT (CONSTRUCTION YR.)	24000
MU'S:	2
SU'S:	2
20 YR. ESAL'S:	3.26

MIXTURE USE	SURFACE	LEVEL BINDER	BINDER/PATCHING	BASE COURSE
ACPG	SBS PG 76-22	PG 64-22	PG 64-22	PG 64-22
RAP % (MAX)	10%	10%	10%	10%
DESIGN AIR VOIDS	4.0% @ Ndes=90	4.0% @ Ndes=90	4.0% @ Ndes=90	4.0% @ Ndes=90
MIX COMPOSITION			IL-19.0	IL-19.0
(GRADATION MIXTURE)				
FRICITION AGG	MIXTURE "D"	MIXTURE "C"	MIXTURE "B"	MIXTURE "B"

MIXTURE USE	SHOULDERS	TOP LIFT SHOULDERS		
ACPG	PG 58-22	PG 58-22		
RAP % (MAX)	30%	30%		
DESIGN AIR VOIDS	2.0% @ Ndes=30	**2.0% @ Ndes=30		
MIX COMPOSITION				
(GRADATION MIXTURE)				
FRICITION AGG	BAM	BAM		

** TOP LIFT SHOULDERS - DESIGN THIS MIX AT 2.0% VOIDS AND ADD ASPHALT TO REDUCE VOIDS TO 1.5%

PLAN QUANTITIES FOR HOT-MIX ASPHALT SURFACE COURSE ITEMS ARE CALCULATED USING A UNIT WEIGHT OF 112 LBS/SQ YD/IN (59.8 KG/SQ M/25 MM THICKNESS).

19. ADVANCE WARNING SIGNS SHALL BE PLACED ON THE TRAIL TO WARN TRAIL USERS OF THE CONSTRUCTION AHEAD. THE TRAIL SHALL NOT BE CLOSED EXCEPT WHEN PAVING AND REMOVING SHOULDERS ON EASTPORT PLAZA DRIVE WHERE THE TRAIL INTERSECTS. THE COST FOR THESE SIGNS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF OTHER TRAFFIC CONTROL ITEMS.



Rev. 6-6-08

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	URBAN TOTAL	ACHSIP	ACM	ACHSIP	ACHSIP				
				1000-1A	1000-1A	Y031-1F	Y031-1F				
				ROADWAY	ROADWAY	TRAFFIC SIGNALS	TRAFFIC SIGNALS				
				RT TURN LANE		I-255	PLAZA DR.				
				HSIP	STP	RAMPS 3 & 4	FOURNIE LN.				
				90% FEDERAL 10% STATE	80% FEDERAL 20% STATE	90% FEDERAL 10% STATE	57 Collinsville 90% FEDERAL 5% STATE				
20200100	EARTH EXCAVATION	CU YD	1190	1052	138						
20400800	FURNISHED EXCAVATION	CU YD	588	588							
20800150	TRENCH BACKFILL	CU YD	105	105							
25000210	SEEDING, CLASS 2A	ACRE	1.2	0.6	0.6						
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	108	54	54						
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	108	54	54						
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	108	54	54						
25100115	MULCH, METHOD 2	ACRE	1.2	0.6	0.6						
25100630	EROSION CONTROL BLANKET	SQ YD	1566	1566							
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	120	60	60						
28000300	TEMPORARY DITCH CHECKS	EACH	5	5							
28000400	PERIMETER EROSION BARRIER	FOOT	3710	3710							
28000500	INLET AND PIPE PROTECTION	EACH	22	22							
28100103	STONE RIPRAP, CLASS A2	SQ YD	40	40							
28200200	FILTER FABRIC	SQ YD	40	40							
30200650	PROCESSING MODIFIED SOIL 12"	SQ YD	5131	4446	685						
30201500	LIME	TON	99	86	13						
35501330	HOT-MIX ASPHALT BASE COURSE, 11 1/2"	SQ YD	3923	3238	685						
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	3.3	3.1	0.2						
40600300	AGGREGATE (PRIME COAT)	TON	16	15	1						
40600645	LEVELING BINDER (MACHINE METHOD), N90	TON	1324	1209	115						
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	196	196							
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	53	53							
40600990	TEMPORARY RAMP	SQ YD	246	246							
40603545	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90	TON	876	818	58						
44000100	PAVEMENT REMOVAL	SQ YD	393	393							
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	61	61							
44003100	MEDIAN REMOVAL	SQ FT	5840	5840							
44004250	PAVED SHOULDER REMOVAL	SQ YD	3459	3459							
44200132	PAVEMENT PATCHING, TYPE II, 11 INCH	SQ YD	22	22							
48100700	AGGREGATE SHOULDERS, TYPE A 8"	SQ YD	33	33							
48101200	AGGREGATE SHOULDERS, TYPE B	TON	160	160							
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	2236	2236							
50105220	PIPE CULVERT REMOVAL	FOOT	87	87							

40603545

Rev. 5-27-08

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	ACHSIP	ACM	ACHSIP	ACHSIP				
				I000-1A	I000-1A	Y031-1F	Y031-1F	TRAFFIC SIGNALS	TRAFFIC SIGNALS		
				ROADWAY	ROADWAY						
					RT TURN LANE	I-255	PLAZA DR.				
				HSIP	STP	RAMPS 3 & 4	FOURNE LN.				
				90% FEDERAL 10% STATE	80% FEDERAL 20% STATE	90% FEDERAL 10% STATE	5% Collinsville 90% FEDERAL 5% STATE				
54001001	BOX CULVERT END SECTION, CULVERT NO. 1	EACH	1	1							
54010302	PRECAST CONCRETE BOX CULVERT 3' X 2'	FOOT	5	5							
54205923	PIPE CULVERTS, TYPE 1, CORRUGATED STEEL, EQUIVALENT ROUND-SIZE 48"	FOOT	33	33							
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	2	2							
54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	2	2							
54215163	STEEL END SECTIONS, EQUIVALENT ROUND-SIZE 48"	EACH	2	2							
54248510	CONCRETE COLLAR	CU YD	0.3	0.3							
542A0229	PIPE CULVERTS, CLASS A, TYPE 1 24"	FOOT	80	80							
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	415	415							
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	97	97							
59300100	CONTROLLED LOW-STRENGTH MATERIAL	CU YD	28	28							
60100060	CONCRETE HEADWALL FOR PIPE DRAINS	EACH	12	12							
60107600	PIPE UNDERDRAINS 4"	FOOT	3205	3205							
60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	144	144							
60236900	INLETS, TYPE A, TYPE 12 FRAME AND GRATE	EACH	17	17							
60500060	REMOVING INLETS	EACH	3	3							
60604400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18	FOOT	121	121							
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	180	180							
60609200	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12	FOOT	195	195							
60610400	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24	FOOT	164	164							
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	1211	1211							
60622800	CONCRETE MEDIAN, TYPE SM-6.12	SQ FT	3168	3168							
60625900	P. C. C. RAMPED MEDIAN TERMINAL	EACH	3	3							
66500105	WOVEN WIRE FENCE, 4'	FOOT	621		621						
66502300	WOVEN WIRE FENCE REMOVAL	FOOT	650		650						
67100100	MOBILIZATION	L SUM	1	1							
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1							
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1							
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1							
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1							

ILLINOIS DEPARTMENT OF TRANSPORTATION SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	ACHSEP	ACM	ACHSEP	ACHSEP				
				1000-1A	1000-1A	Y031-1F	Y031-1F	TRAFFIC SIGNALS	TRAFFIC SIGNALS		
				ROADWAY	ROADWAY						
				HSIP	RT TURN LANE	1-255	EASTPORT				
					STP	RAMPS 3 & 4	PLAZA DR.				
							FOURNIE LN.				
				90% FEDERAL 10% STATE	80% FEDERAL 20% STATE	90% FEDERAL 10% STATE	2% Collinsville 90% FEDERAL 5% STATE				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1							
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	60	40	20						
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	749	631	118						
70300210	TEMPORARY PAVEMENT MARKING, LETTERS AND SYMBOLS	SQ FT	156	109	47						
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	11054	10465	589						
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	311	197	114						
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	633	571	62						
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	177	159	18						
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	5002	4586	416						
72000100	SIGN PANEL - TYPE 1	SQ FT	29	8	6		15				
72000200	SIGN PANEL - TYPE 2	SQ FT	49				49				
72400330	REMOVE SIGN PANEL - TYPE 3	SQ FT	344	200	144						
72400730	RELOCATE SIGN PANEL - TYPE 3	SQ FT	344	200	144						
72700100	STRUCTURAL STEEL SIGN SUPPORT - BREAKAWAY	POUND	3692	2448	1244						
73000100	WOOD SIGN SUPPORT	FOOT	54	36	18						
73400100	CONCRETE FOUNDATIONS	CU YD	7.4	4.9	2.5						
78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	156	109	47						
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	11054	10465	589						
78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	311	197	114						
78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	390	320	62						
78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	177	159	18						
78001150	PAINT PAVEMENT MARKING - LINE 12"	FOOT	243	243							
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	103	87	16						
78200300	PRISMATIC CURB REFLECTOR	EACH	52	52							
80300100	LOCATING UNDERGROUND CABLE	FOOT	80			40	40				
80500100	SERVICE INSTALLATION, TYPE A	EACH	1				1				
81012300	CONDUIT IN TRENCH, 1" DIA., PVC	FOOT	213			65	148				
81012500	CONDUIT IN TRENCH, 1 1/2" DIA., PVC	FOOT	590				590				
81012600	CONDUIT IN TRENCH, 2" DIA., PVC	FOOT	109				109				
81012700	CONDUIT IN TRENCH, 2 1/2" DIA., PVC	FOOT	55				55				
81012800	CONDUIT IN TRENCH, 3" DIA., PVC	FOOT	63			16	47				
81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	198				198				

*SPECIALTY ITEMS

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES

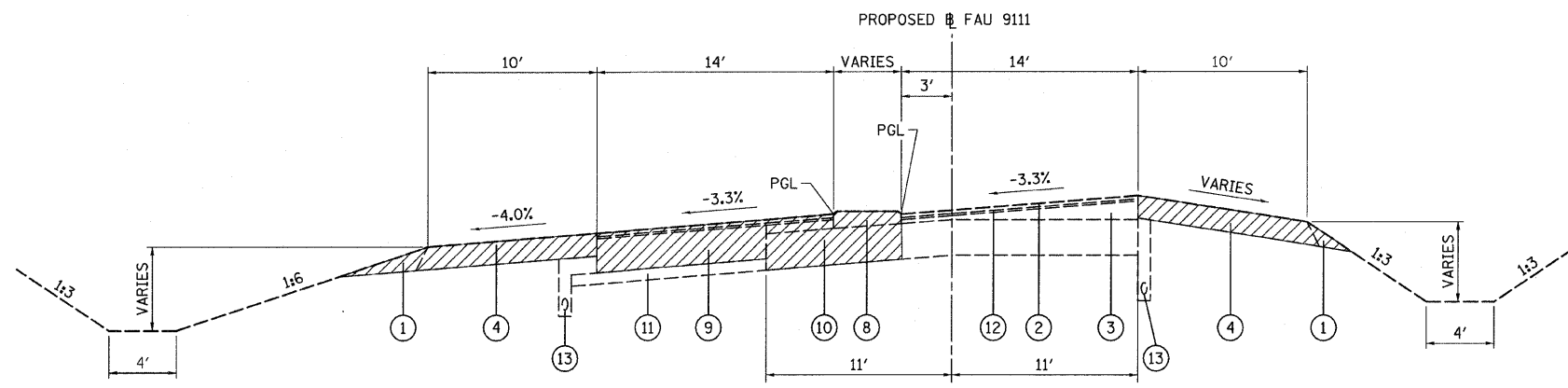
CODE NO	ITEM	UNIT	URBAN TOTAL	ACHSTP	ACM	ACHSTP	ACHSTP				
				1000-1A	1000-1A	Y031-1F	Y031-1F				
			QUANTITIES	ROADWAY	ROADWAY	TRAFFIC SIGNALS	TRAFFIC SIGNALS				
					RT TURN LANE	I-255	PLAZA DR.				
				HSIP	STP	RAMPS 3 & 4	FOURNIE LN.				
				90% FEDERAL 10% STATE	80% FEDERAL 20% STATE	90% FEDERAL 10% STATE	5% Collinsville 90% FEDERAL 5% STATE				
81018600	CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL	FOOT	56			56					
81400700	HANDHOLE, PORTLAND CEMENT CONCRETE	EACH	10			2	8				
81400720	DOUBLE HANDHOLE, PORTLAND CEMENT CONCRETE	EACH	1				1				
81603035	UNIT DUCT, 600V, 2-1C NO. 6, 1/C NO. 6 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE	FOOT	1222			703	519				
81702110	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 10	FOOT	250				250				
81702130	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6	FOOT	1480				1480				
81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	1710			768	942				
81900205	TRENCH AND BACKFILL FOR ELECTRICAL WORK (SPECIAL)	FOOT	24			4	20				
82103900	LUMINAIRE, SODIUM VAPOR, MULTI-MOUNT, 250 WATT	EACH	2				2				
83027500	LIGHT POLE, ALUMINUM, TRANSFORMER BASE, 50 FT. M.H., TENON MOUNT - TWIN	EACH	1				1				
83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	21			14	7				
84200500	REMOVAL OF EXISTING LIGHTING UNIT, SALVAGE	EACH	1				1				
84200700	LIGHTING FOUNDATION REMOVAL	EACH	3			2	1				
84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	2			2					
85700205	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1				1				
86400100	TRANSCEIVER - FIBER OPTIC	EACH	1				1				
87100105	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, 4F	FOOT	506			272	234				
87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1211				1211				
87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1257				1257				
87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1991			458	1533				
87700230	STEEL MAST ARM ASSEMBLY AND POLE, 38 FT.	EACH	1				1				
87700250	STEEL MAST ARM ASSEMBLY AND POLE, 42 FT.	EACH	1				1				
87700270	STEEL MAST ARM ASSEMBLY AND POLE, 46 FT.	EACH	1				1				
87700310	STEEL MAST ARM ASSEMBLY AND POLE, 54 FT.	EACH	1				1				
87800200	CONCRETE FOUNDATION, TYPE D	FOOT	3				3				
87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	64				64				

* SPECIALTY ITEMS

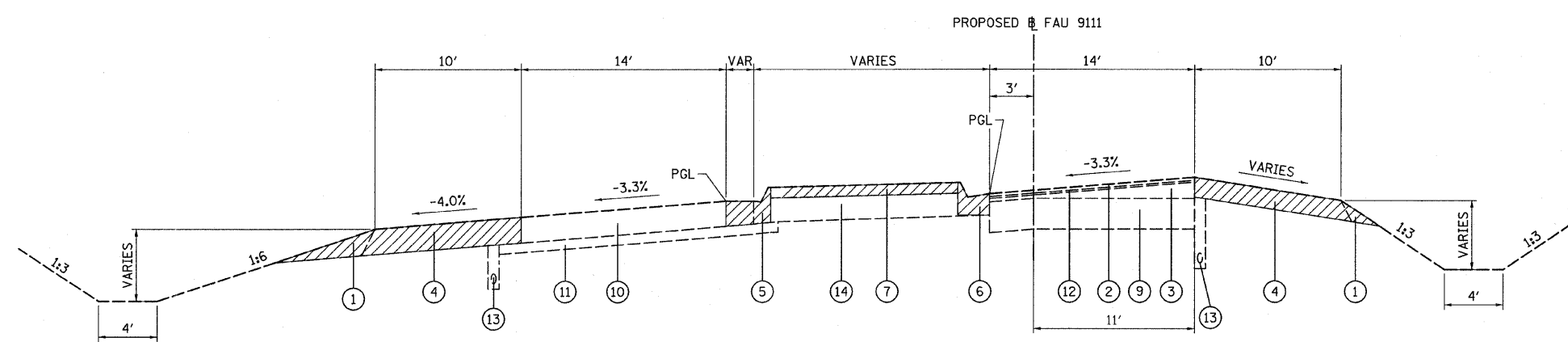
ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	ACHETP	ACM	ACHETP	ACHETP				
				1000-1A	1000-1A	Y031-1F	Y031-1F				
				ROADWAY	ROADWAY	TRAFFIC SIGNALS	TRAFFIC SIGNALS				
				HSIP	RT TURN LANE	I-255	PLAZA DR.				
				90% FEDERAL 10% STATE	80% FEDERAL 20% STATE	90% FEDERAL 10% STATE	90% FEDERAL 10% STATE				
88040070	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	4				4				
88040090	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	4				4				
88040110	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 4-SECTION, BRACKET MOUNTED	EACH	2				2				
88040120	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 4-SECTION, MAST ARM MOUNTED	EACH	2				2				
88040150	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2				2				
* 88040160	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	2				2				
88200100	TRAFFIC SIGNAL BACKPLATE	EACH	8				8				
88500100	INDUCTIVE LOOP DETECTOR	EACH	15			1	14				
88600100	DETECTOR LOOP, TYPE I	FOOT	2280			427	1853				
89502210	MODIFY EXISTING CONTROLLER CABINET	EACH	1			1					
89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	60			60					
89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	660			660					
89502380	REMOVE EXISTING HANDHOLE	EACH	1			1					
X0323221	PLUG AND ABANDON EXISTING PIPE	CU YD	4	4							
* X7800100	PAINT PAVEMENT MARKING - RAISED MEDIAN	SQ FT	79	79							
X7800200	PAINT PAVEMENT MARKING CURB	FOOT	132	132							
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	0.5	0.5						
Z0040530	PIPE UNDERDRAIN REMOVAL	FOOT	2771	2271	500						
Z0050900	REMOVE CONCRETE FOUNDATION	EACH	8	6	2						
© Z0076600	TRAINEES	HOURL	500	500							

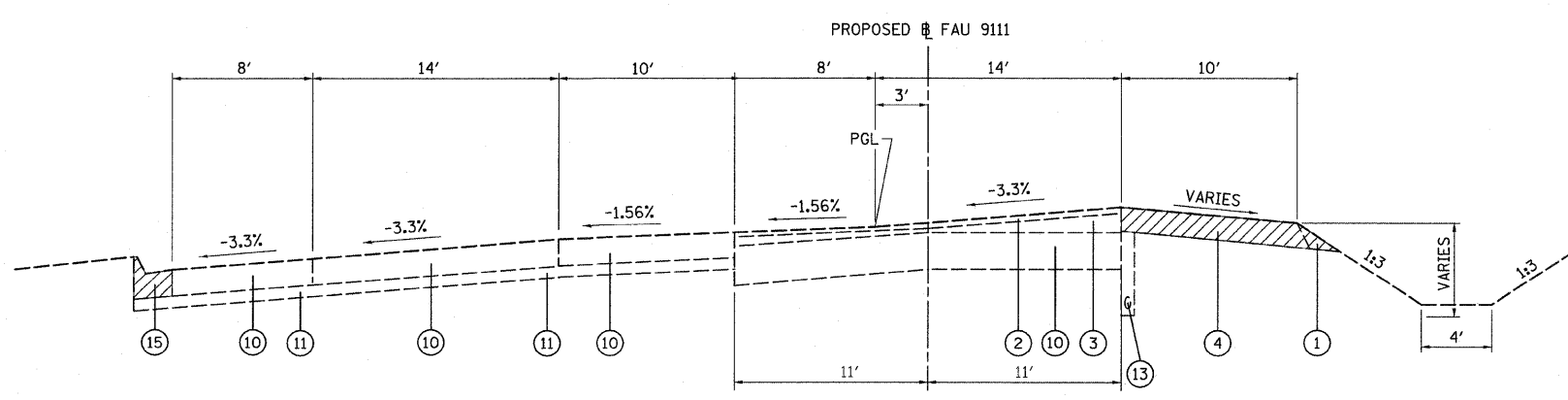
© Y080
 * SPECIALTY ITEMS



EXISTING TYPICAL SECTION
STA. 229+53.12 TO STA. 231+44.26
(NOT TO SCALE)



EXISTING TYPICAL SECTION
STA. 225+32 TO STA. 229+53.12
(NOT TO SCALE)

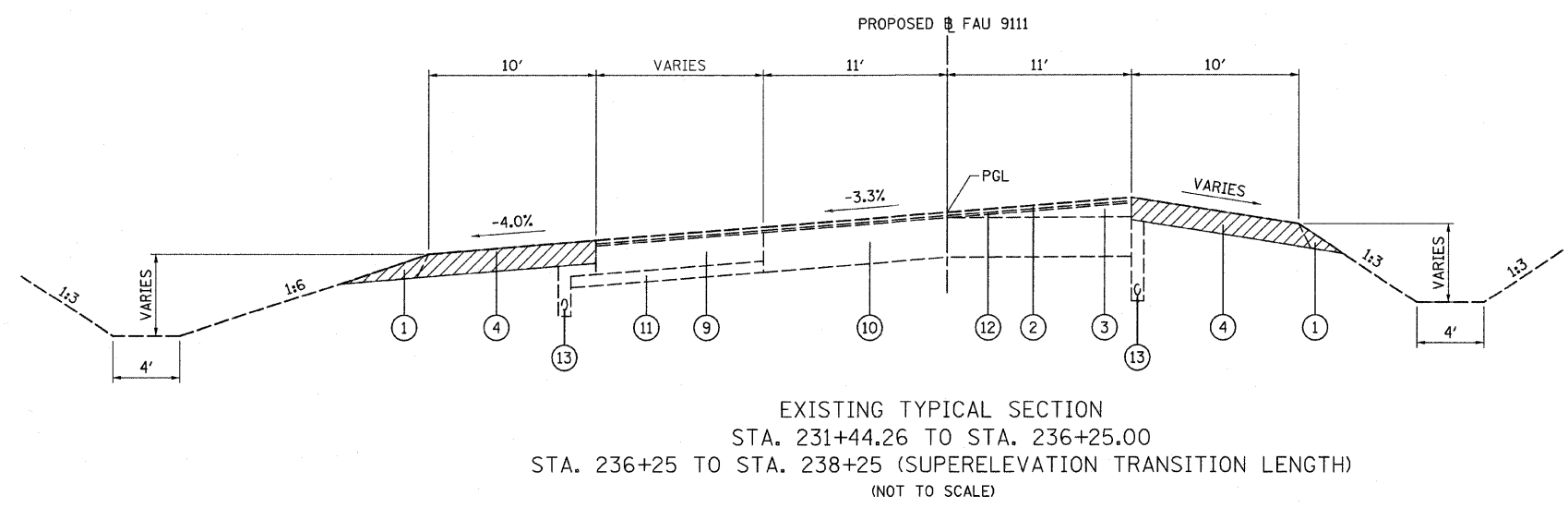
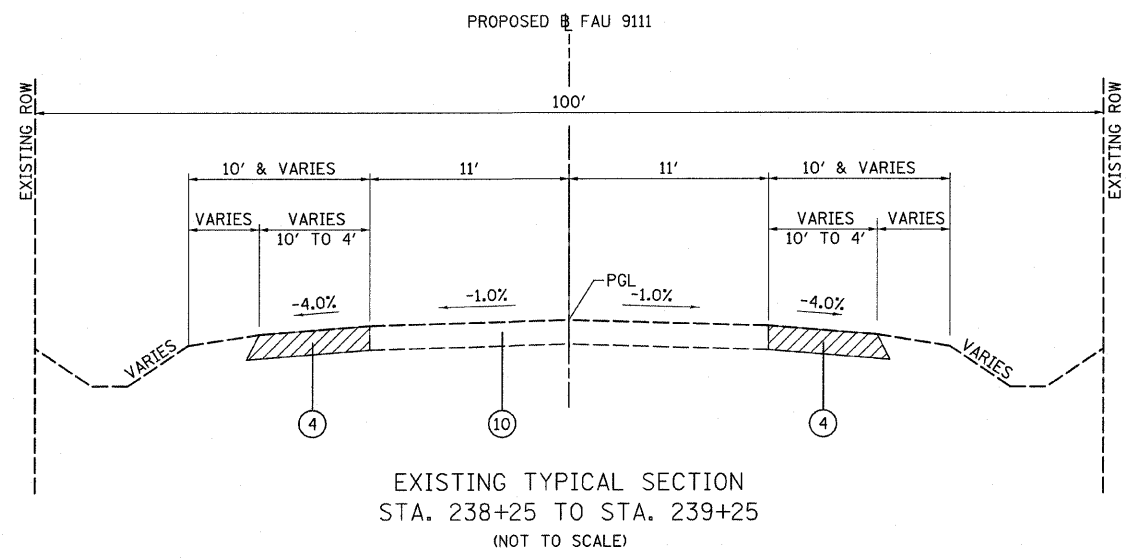
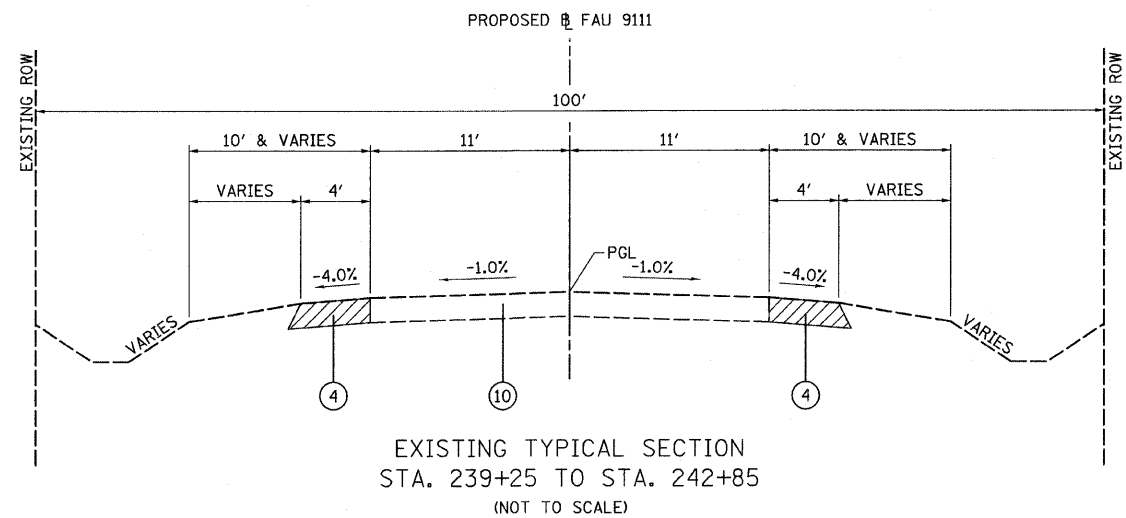


EXISTING TYPICAL SECTION
STA. 225+06.5 TO STA. 225+32
(NOT TO SCALE)

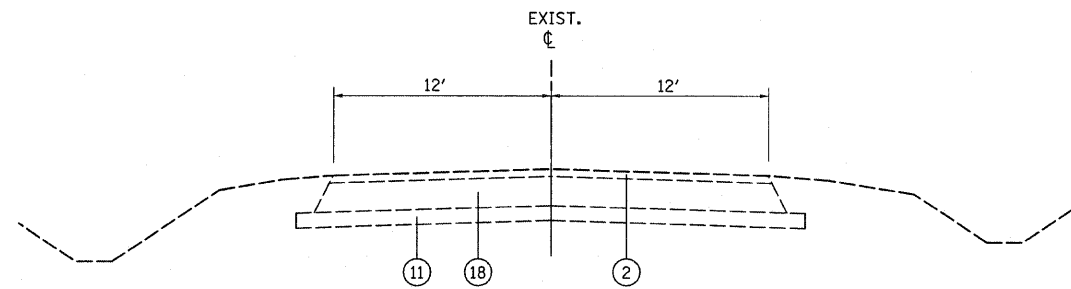
- LEGEND**
- ① EXISTING AGGREGATE SHOULDER, TYPE B
 - ② EXISTING BITUMINOUS CONCRETE SURFACE COURSE, MIXTURE D, CLASS I, 1 1/4"
 - ③ EXISTING LEVELING BINDER (MACHINE METHOD)
 - ④ EXISTING BITUMINOUS SHOULDER 8"
 - ⑤ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.06
 - ⑥ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.18
 - ⑦ EXISTING CONCRETE MEDIAN SURFACE, 4"
 - ⑧ EXISTING CONCRETE MEDIAN, TYPE C4 (MODIFIED)
 - ⑨ EXISTING PORTLAND CEMENT CONCRETE BASE COURSE 10"
 - ⑩ EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, 8"
 - ⑪ EXISTING STABILIZED SUB-BASE 4"
 - ⑫ EXISTING BITUMINOUS CONCRETE BINDER COURSE 3/4"
 - ⑬ EXISTING 4" PIPE UNDERDRAINS
 - ⑭ EXISTING SAND BACKFILL
 - ⑮ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.24
 - ⑯ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B6.24
 - ⑰ EXISTING SUBBASE GRANULAR MATERIAL 5"
 - ⑱ EXISTING HOT-MIX ASPHALT BASE COURSE, 8"
 - ⑲ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, 2"
 - ⑳ PROPOSED LEVELING BINDER (MACHINE METHOD), N90, VARIABLE DEPTH (3/4" MIN.)
 - ㉑ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90, 1 1/2"
 - ㉒ PROPOSED HOT-MIX ASPHALT BASE COURSE, 1 1/2"
 - ㉓ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
 - ㉔ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12
 - ㉕ PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
 - ㉖ PROPOSED CONCRETE MEDIAN, TYPE SM-6.12
 - ㉗ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
 - ㉘ PROPOSED 4" PIPE UNDERDRAINS
 - ㉙ PROPOSED AGGREGATE SHOULDERS, TYPE B
 - ㉚ PROPOSED AGGREGATE SHOULDERS, TYPE A, 8"
 - ㉛ PROPOSED LIME MODIFIED SOIL, 12"
 - ㉜ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
 - ㉝ PROPOSED AGGREGATE (PRIME COAT)
 - ㉞ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
 - ▨ ITEMS TO BE REMOVED

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING TYPICAL SECTIONS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
HP\25004\Technical Production\Civil\N07	7\Microsofton\typical02.dgn	DRAWN -	REVISED -					9111	73-15TS	MADISON	64	4
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -		SCALE: NONE			FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
	PLOT DATE = 3/14/2008	DATE -	REVISED -		SHEET NO. 1 OF 6 SHEETS			CONTRACT NO. 76B22				

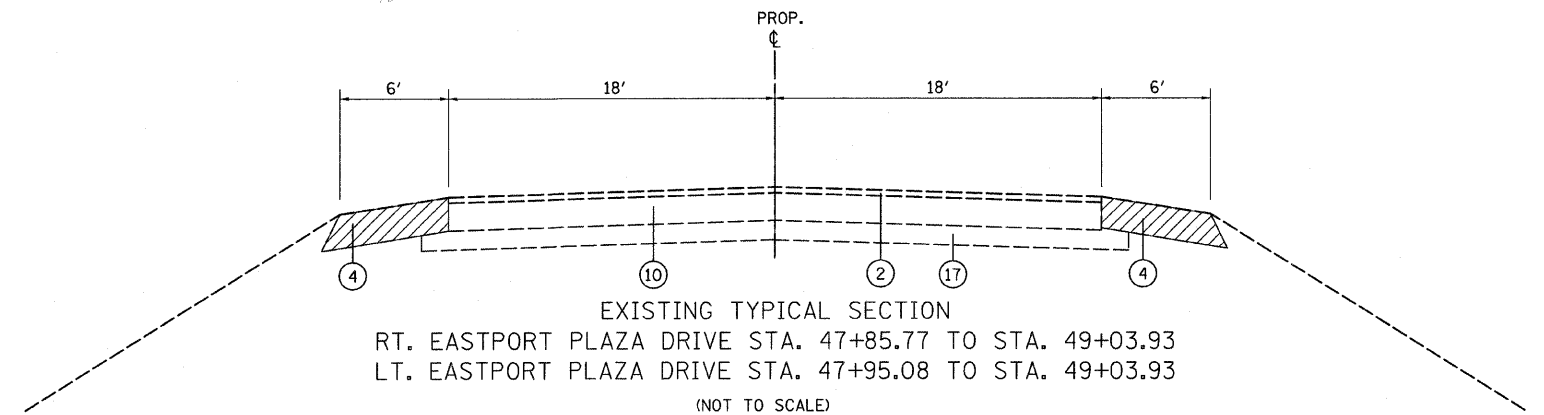
- LEGEND**
- ① EXISTING AGGREGATE SHOULDER, TYPE B
 - ② EXISTING BITUMINOUS CONCRETE SURFACE COURSE, MIXTURE D, CLASS I, 1 1/4"
 - ③ EXISTING LEVELING BINDER (MACHINE METHOD)
 - ④ EXISTING BITUMINOUS SHOULDER 8"
 - ⑤ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.06
 - ⑥ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.18
 - ⑦ EXISTING CONCRETE MEDIAN SURFACE, 4"
 - ⑧ EXISTING CONCRETE MEDIAN, TYPE C4 (MODIFIED)
 - ⑨ EXISTING PORTLAND CEMENT CONCRETE BASE COURSE 10"
 - ⑩ EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, 8"
 - ⑪ EXISTING STABILIZED SUB-BASE 4"
 - ⑫ EXISTING BITUMINOUS CONCRETE BINDER COURSE 3/4"
 - ⑬ EXISTING 4" PIPE UNDERDRAINS
 - ⑭ EXISTING SAND BACKFILL
 - ⑮ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.24
 - ⑯ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B6.24
 - ⑰ EXISTING SUBBASE GRANULAR MATERIAL 5"
 - ⑱ EXISTING HOT-MIX ASPHALT BASE COURSE, 8"
 - ⑲ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, 2"
 - ⑳ PROPOSED LEVELING BINDER (MACHINE METHOD), N90, VARIABLE DEPTH (3/4" MIN.)
 - ㉑ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90, 1 1/2"
 - ㉒ PROPOSED HOT-MIX ASPHALT BASE COURSE, 1 1/2"
 - ㉓ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
 - ㉔ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12
 - ㉕ PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
 - ㉖ PROPOSED CONCRETE MEDIAN, TYPE SM-6.12
 - ㉗ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
 - ㉘ PROPOSED 4" PIPE UNDERDRAINS
 - ㉙ PROPOSED AGGREGATE SHOULDERS, TYPE B
 - ㉚ PROPOSED AGGREGATE SHOULDERS, TYPE A, 8"
 - ㉛ PROPOSED LIME MODIFIED SOIL, 12"
 - ㉜ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
 - ㉝ PROPOSED AGGREGATE (PRIME COAT)
 - ㉞ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
 - ▨ ITEMS TO BE REMOVED



FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING TYPICAL SECTIONS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
H:\P\25004\Technical Production\Civil\W07	7\Microstation\typical02.dgn	DRAWN -	REVISED -			9111	73-15TS	MADISON	64	5	
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 76B22					
	PLOT DATE = 3/14/2008	DATE -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					



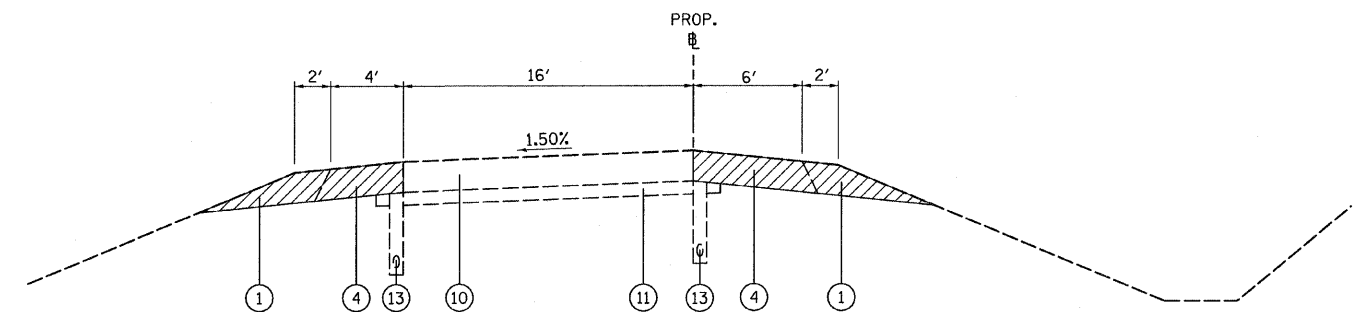
EXISTING TYPICAL SECTION
 LT. FOURNIE LANE STA. 10+53.63 TO STA. 11+03.33
 RT. FOURNIE LANE STA. 10+22.50 TO STA. 11+03.33
 (NOT TO SCALE)



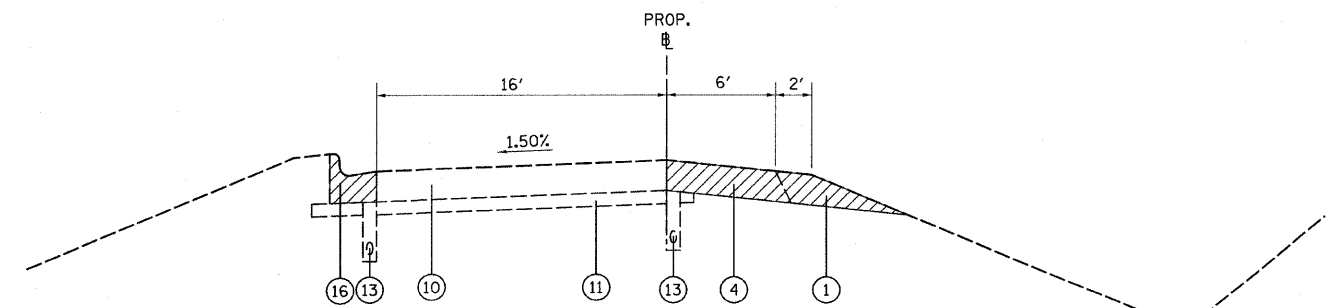
EXISTING TYPICAL SECTION
 RT. EASTPORT PLAZA DRIVE STA. 47+85.77 TO STA. 49+03.93
 LT. EASTPORT PLAZA DRIVE STA. 47+95.08 TO STA. 49+03.93
 (NOT TO SCALE)

LEGEND

- ① EXISTING AGGREGATE SHOULDER, TYPE B
- ② EXISTING BITUMINOUS CONCRETE SURFACE COURSE, MIXTURE D, CLASS I, 1/4"
- ③ EXISTING LEVELING BINDER (MACHINE METHOD)
- ④ EXISTING BITUMINOUS SHOULDER 8"
- ⑤ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.06
- ⑥ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.18
- ⑦ EXISTING CONCRETE MEDIAN SURFACE, 4"
- ⑧ EXISTING CONCRETE MEDIAN, TYPE C4 (MODIFIED)
- ⑨ EXISTING PORTLAND CEMENT CONCRETE BASE COURSE 10"
- ⑩ EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, 8"
- ⑪ EXISTING STABILIZED SUB-BASE 4"
- ⑫ EXISTING BITUMINOUS CONCRETE BINDER COURSE 3/4"
- ⑬ EXISTING 4" PIPE UNDERDRAINS
- ⑭ EXISTING SAND BACKFILL
- ⑮ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.24
- ⑯ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B6.24
- ⑰ EXISTING SUBBASE GRANULAR MATERIAL 5"
- ⑱ EXISTING HOT-MIX ASPHALT BASE COURSE, 8"
- ⑲ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, 2"
- ⑳ PROPOSED LEVELING BINDER (MACHINE METHOD), N90, VARIABLE DEPTH (3/4" MIN.)
- ㉑ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90, 1/2"
- ㉒ PROPOSED HOT-MIX ASPHALT BASE COURSE, 1 1/2"
- ㉓ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
- ㉔ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12
- ㉕ PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- ㉖ PROPOSED CONCRETE MEDIAN, TYPE SM-6.12
- ㉗ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- ㉘ PROPOSED 4" PIPE UNDERDRAINS
- ㉙ PROPOSED AGGREGATE SHOULDERS, TYPE B
- ㉚ PROPOSED AGGREGATE SHOULDERS, TYPE A, 8"
- ㉛ PROPOSED LIME MODIFIED SOIL, 12"
- ㉜ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
- ㉝ PROPOSED AGGREGATE (PRIME COAT)
- ㉞ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- ▨ ITEMS TO BE REMOVED



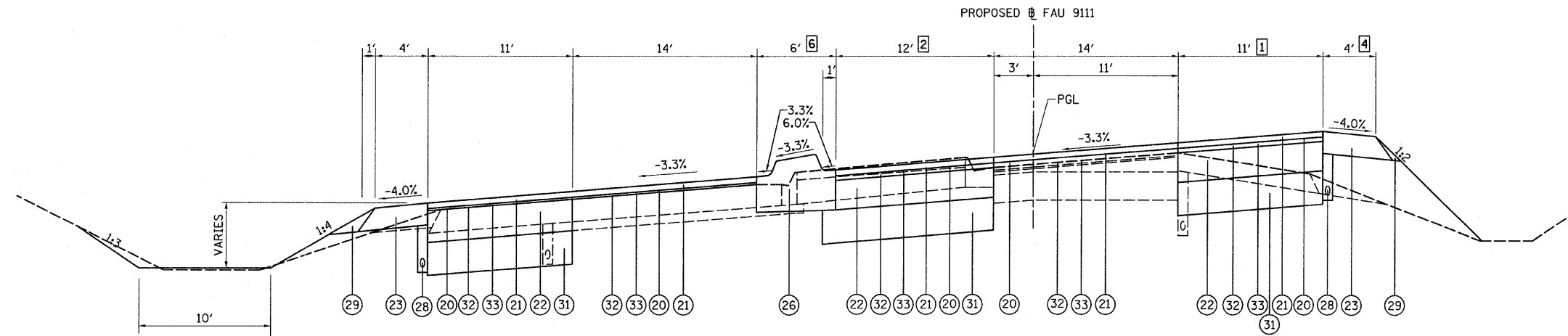
EXISTING TYPICAL SECTION
 RAMP 4 - LT. STA. 0+78.19 TO STA. 1+77.86
 RAMP 4 - RT. STA. 1+20.20 TO STA. 1+77.86
 (NOT TO SCALE)



EXISTING TYPICAL SECTION
 RAMP 4 - LT. STA. 0+39.91 TO STA. 0+78.19
 RAMP 4 - RT. STA. 0+48+51 TO STA. 1+20.20
 (NOT TO SCALE)

FILE NAME =	USER NAME = #USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING TYPICAL SECTIONS			F.A.U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
H:\P\25204\Technical Production\Civil\W07	7\Microstation\typical02.dgn	DRAWN -	REVISED -					9111	73-15TS	MADISON	64	6
	PLOT SCALE = 28.0000' / IN.	CHECKED -	REVISED -		SCALE: NONE SHEET NO. 3 OF 6 SHEETS STA. TO STA.			CONTRACT NO. 76B22				
	PLOT DATE = 3/14/2008	DATE -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							

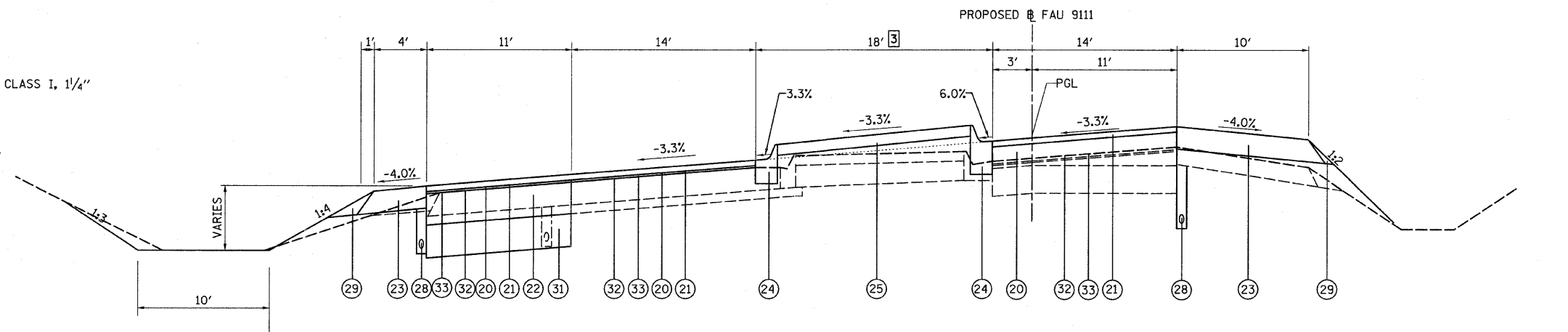
- 1 VARIES 0' TO 11'
FROM STA. 225+85.77 TO STA. 226+35.77
- 2 VARIES 0' TO 12'
FROM STA. 225+85.77 TO STA. 226+35.77
- 3 VARIES 18' TO 6'
FROM STA. 225+85.77 TO STA. 226+35.77
- 4 VARIES 10' TO 4'
FROM STA. 225+85.77 TO STA. 226+12.95
- 5 10' FROM STA. 237+25 TO STA. 238+33.99
VARIES 10' - 4' FROM STA. 238+33.99 TO STA. 239+59.32
4' FROM STA. 239+59.32 TO STA. 242+85
- 6 VARIES 6' TO 0'
FROM STA. 229+51.94 TO STA. 231+73.01
- 7 VARIES 59.48" TO 0'
FROM STA. 10+22.50 TO STA. 10+86.70



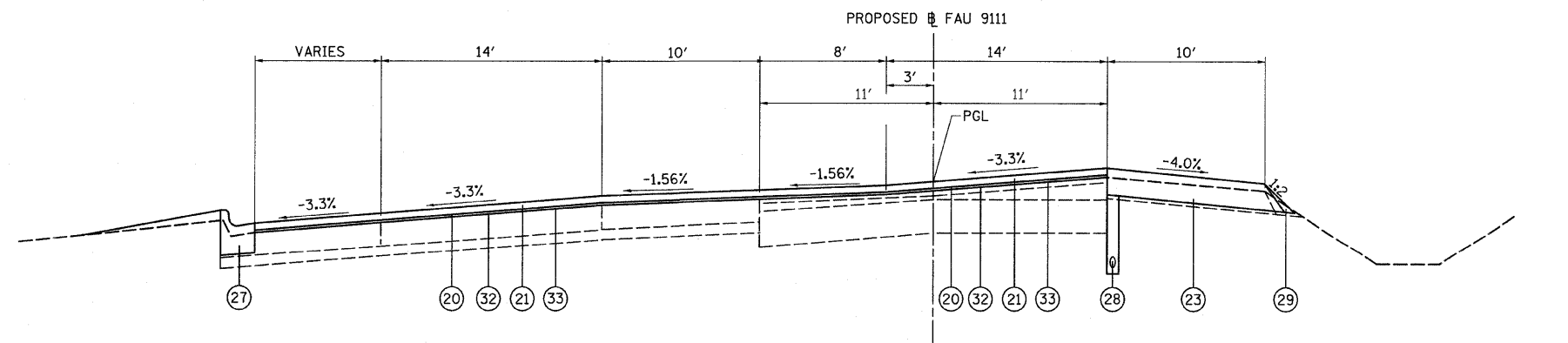
PROPOSED TYPICAL SECTION
STA. 226+35.77 TO STA. 229+53.12
(NOT TO SCALE)

LEGEND

- 1 EXISTING AGGREGATE SHOULDER, TYPE B
- 2 EXISTING BITUMINOUS CONCRETE SURFACE COURSE, MIXTURE D, CLASS I, 1 1/4"
- 3 EXISTING LEVELING BINDER (MACHINE METHOD)
- 4 EXISTING BITUMINOUS SHOULDER 8"
- 5 EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.06
- 6 EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.18
- 7 EXISTING CONCRETE MEDIAN SURFACE, 4"
- 8 EXISTING CONCRETE MEDIAN, TYPE C4 (MODIFIED)
- 9 EXISTING PORTLAND CEMENT CONCRETE BASE COURSE 10"
- 10 EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, 8"
- 11 EXISTING STABILIZED SUB-BASE 4"
- 12 EXISTING BITUMINOUS CONCRETE BINDER COURSE 3/4"
- 13 EXISTING 4" PIPE UNDERDRAINS
- 14 EXISTING SAND BACKFILL
- 15 EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.24
- 16 EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B6.24
- 17 EXISTING SUBBASE GRANULAR MATERIAL 5"
- 18 EXISTING HOT-MIX ASPHALT BASE COURSE, 8"
- 19 EXISTING BITUMINOUS CONCRETE SURFACE COURSE, 2"
- 20 PROPOSED LEVELING BINDER (MACHINE METHOD), N90, VARIABLE DEPTH (3/4" MIN.)
- 21 PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90, 1 1/2"
- 22 PROPOSED HOT-MIX ASPHALT BASE COURSE, 11 1/2"
- 23 PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
- 24 PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12
- 25 PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- 26 PROPOSED CONCRETE MEDIAN, TYPE SM-6.12
- 27 PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- 28 PROPOSED 4" PIPE UNDERDRAINS
- 29 PROPOSED AGGREGATE SHOULDERS, TYPE B
- 30 PROPOSED AGGREGATE SHOULDERS, TYPE A, 8"
- 31 PROPOSED LIME MODIFIED SOIL, 12"
- 32 PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
- 33 PROPOSED AGGREGATE (PRIME COAT)
- 34 PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- ITEMS TO BE REMOVED



PROPOSED TYPICAL SECTION
STA. 225+32 TO STA. 226+35.77
(NOT TO SCALE)



PROPOSED TYPICAL SECTION
STA. 225+06.5 TO STA. 225+32
(NOT TO SCALE)

FILE NAME = H:\P\25004\Technical Production\Civil\W0	USER NAME = #USER#	DESIGNED -	REVISED -
7\Microstation\typical\oe2.dgn		DRAWN -	REVISED -
PLOT SCALE = 20.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = 3/14/2008		DATE -	REVISED -

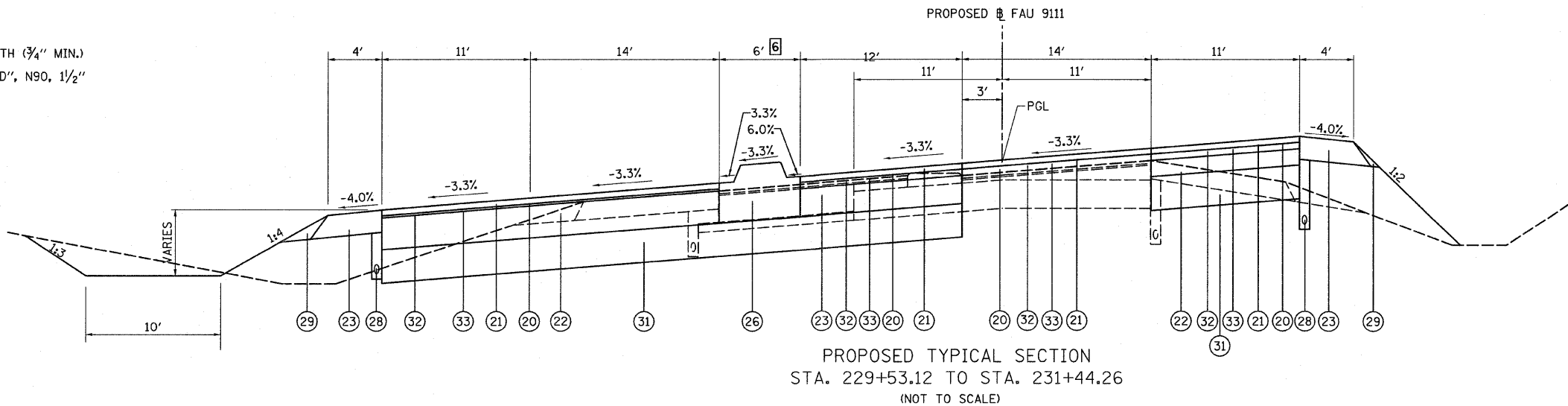
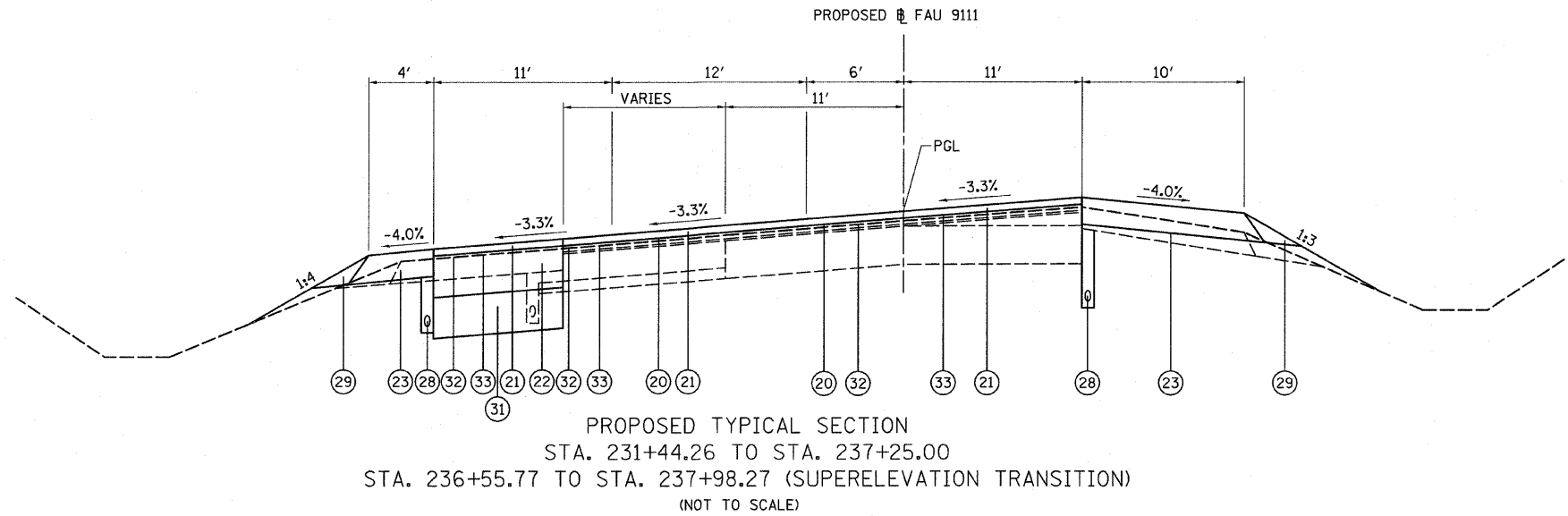
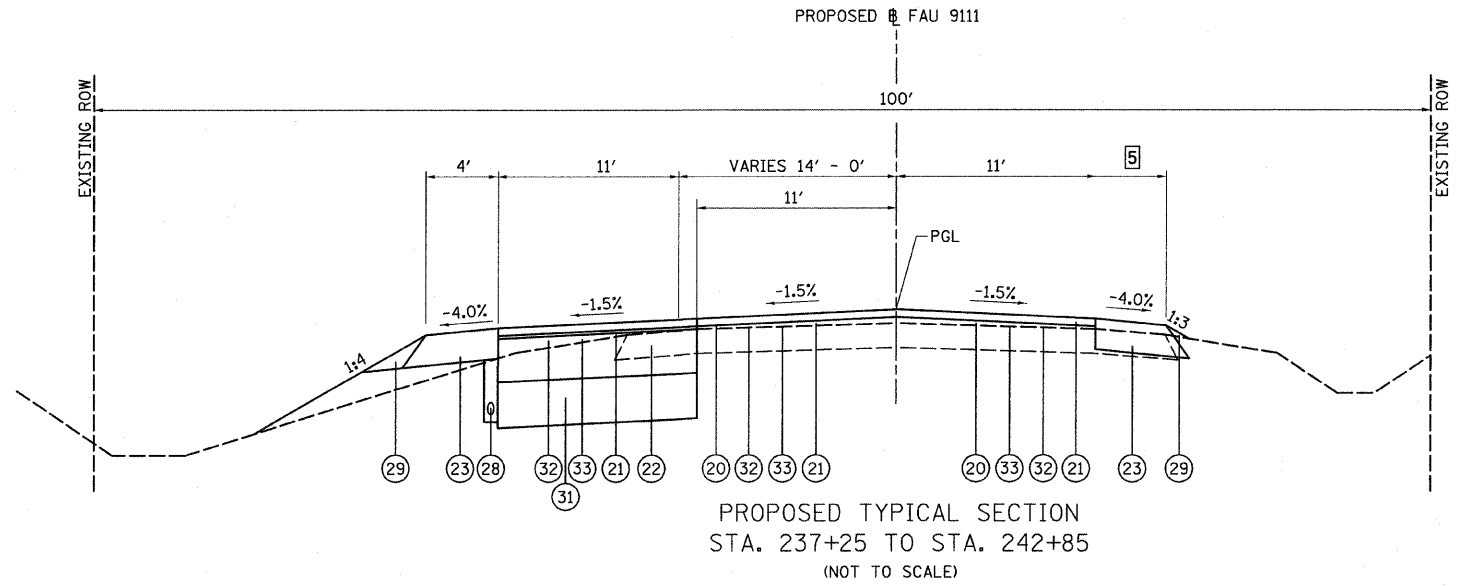
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED TYPICAL SECTIONS

SCALE: NONE SHEET NO. 4 OF 6 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9111	73-15TS	MADISON	64	7
CONTRACT NO. 76B22				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

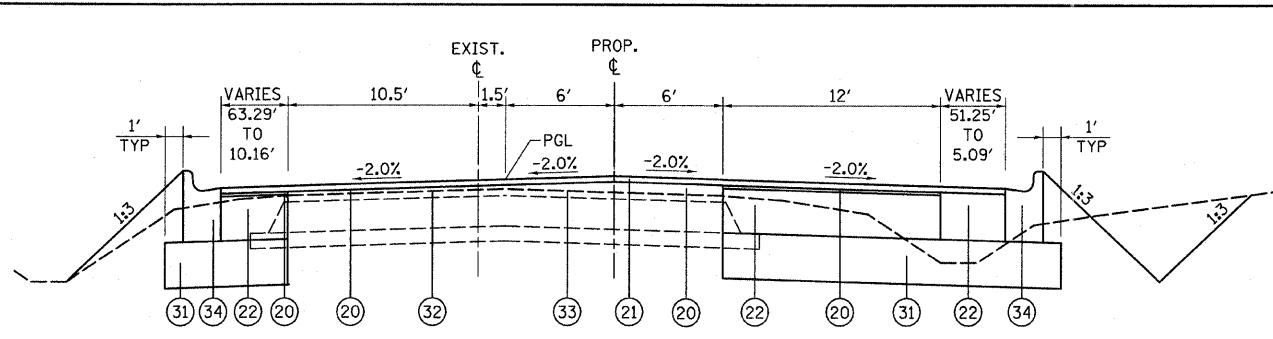
- 1 VARIES 0' TO 11'
FROM STA. 225+85.77 TO STA. 226+35.77
- 2 VARIES 0' TO 12'
FROM STA. 225+85.77 TO STA. 226+35.77
- 3 VARIES 18' TO 6'
FROM STA. 225+85.77 TO STA. 226+35.77
- 4 VARIES 10' TO 4'
FROM STA. 225+85.77 TO STA. 226+12.95
- 5 10' FROM STA. 237+25 TO STA. 238+33.99
VARIES 10' - 4' FROM STA. 238+33.99 TO STA. 239+59.32
4' FROM STA. 239+59.32 TO STA. 242+85
- 6 VARIES 6' TO 0'
FROM STA. 229+51.94 TO STA. 231+73.01
- 7 VARIES 59.48" TO 0"
FROM STA. 10+22.50 TO STA. 10+86.70



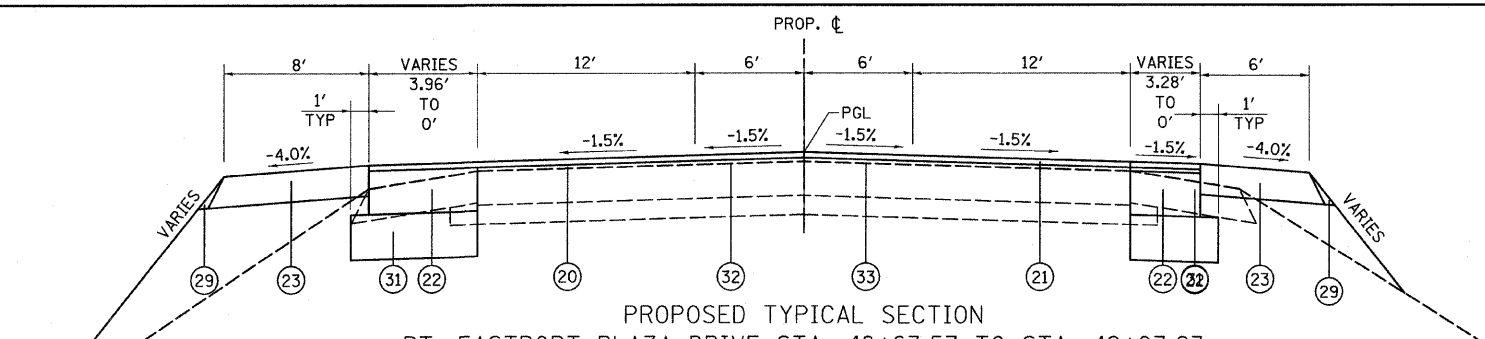
LEGEND

- 1 EXISTING AGGREGATE SHOULDER, TYPE B
- 2 EXISTING BITUMINOUS CONCRETE SURFACE COURSE, MIXTURE D, CLASS I, 1 1/4"
- 3 EXISTING LEVELING BINDER (MACHINE METHOD)
- 4 EXISTING BITUMINOUS SHOULDER 8"
- 5 EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.06
- 6 EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.18
- 7 EXISTING CONCRETE MEDIAN SURFACE, 4"
- 8 EXISTING CONCRETE MEDIAN, TYPE C4 (MODIFIED)
- 9 EXISTING PORTLAND CEMENT CONCRETE BASE COURSE 10"
- 10 EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, 8"
- 11 EXISTING STABILIZED SUB-BASE 4"
- 12 EXISTING BITUMINOUS CONCRETE BINDER COURSE 3/4"
- 13 EXISTING 4" PIPE UNDERDRAINS
- 14 EXISTING SAND BACKFILL
- 15 EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.24
- 16 EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B6.24
- 17 EXISTING SUBBASE GRANULAR MATERIAL 5"
- 18 EXISTING HOT-MIX ASPHALT BASE COURSE, 8"
- 19 EXISTING BITUMINOUS CONCRETE SURFACE COURSE, 2"
- 20 PROPOSED LEVELING BINDER (MACHINE METHOD), N90, VARIABLE DEPTH (3/4" MIN.)
- 21 PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90, 1 1/2"
- 22 PROPOSED HOT-MIX ASPHALT BASE COURSE, 1 1/2"
- 23 PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
- 24 PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12
- 25 PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- 26 PROPOSED CONCRETE MEDIAN, TYPE SM-6.12
- 27 PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- 28 PROPOSED 4" PIPE UNDERDRAINS
- 29 PROPOSED AGGREGATE SHOULDERS, TYPE B
- 30 PROPOSED AGGREGATE SHOULDERS, TYPE A, 8"
- 31 PROPOSED LIME MODIFIED SOIL, 12"
- 32 PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
- 33 PROPOSED AGGREGATE (PRIME COAT)
- 34 PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- ▨ ITEMS TO BE REMOVED

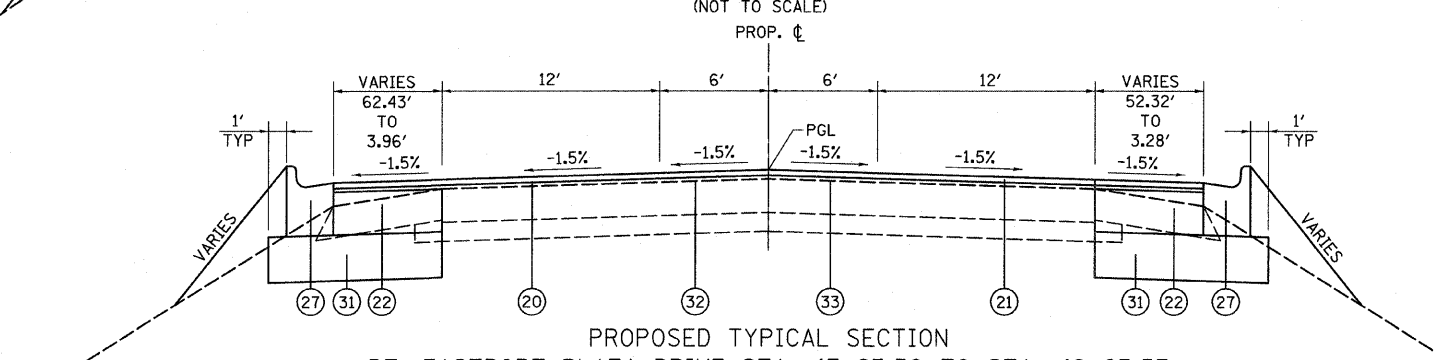
FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		PROPOSED TYPICAL SECTIONS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
H:\P\25004\Technical Production\Civil\W07	7\Microstation\typical02.dgn	DRAWN -	REVISED -						9111	73-15TS	MADISON	64	8
	PLOT SCALE = 20,000' / IN.	CHECKED -	REVISED -						CONTRACT NO. 76B22			ILLINOIS FED. AID PROJECT	
	PLOT DATE = 3/14/2008	DATE -	REVISED -						SCALE: NONE		SHEET NO. 5 OF 6 SHEETS		STA. TO STA.



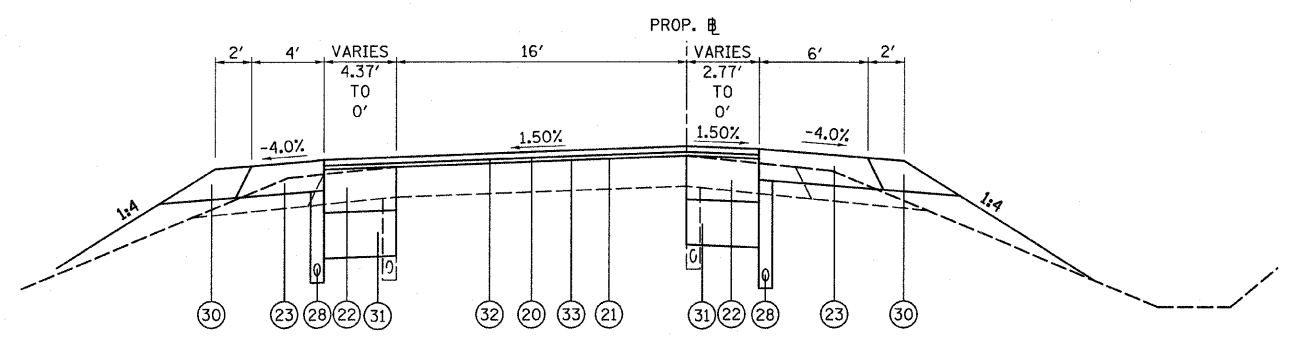
PROPOSED TYPICAL SECTION
 LT. FOURNIE LANE STA. 10+53.45 TO STA. 10+72.89
 RT. FOURNIE LANE STA. 10+22.05 TO STA. 10+72.89
 (NOT TO SCALE)



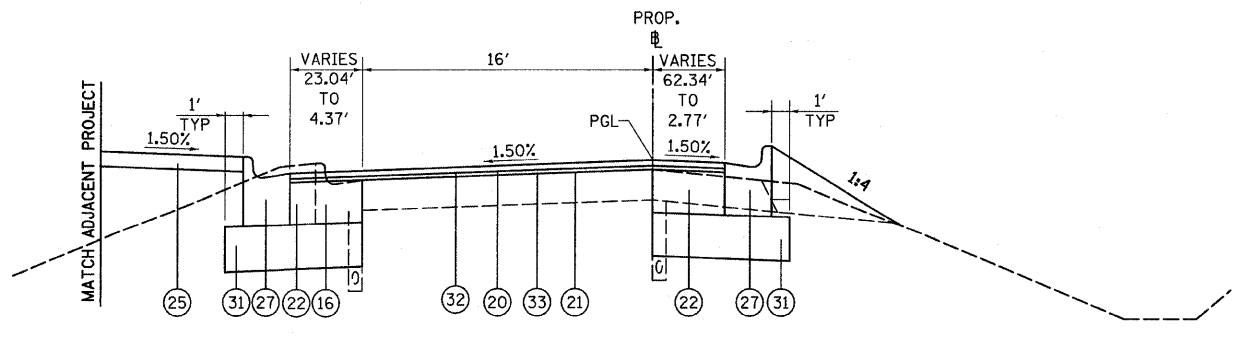
PROPOSED TYPICAL SECTION
 RT. EASTPORT PLAZA DRIVE STA. 48+63.57 TO STA. 49+03.93
 LT. EASTPORT PLAZA DRIVE STA. 48+32.27 TO STA. 49+03.93
 (NOT TO SCALE)



PROPOSED TYPICAL SECTION
 RT. EASTPORT PLAZA DRIVE STA. 47+83.50 TO STA. 48+63.57
 LT. EASTPORT PLAZA DRIVE STA. 47+94.41 TO STA. 48+32.27
 (NOT TO SCALE)



PROPOSED TYPICAL SECTION
 RAMP 4 - LT. STA. 0+78.19 TO STA. 1+77.86
 RAMP 4 - RT. STA. 1+20.20 TO STA. 1+77.86
 (NOT TO SCALE)



PROPOSED TYPICAL SECTION
 RAMP 4 - LT. STA. 0+39.91 TO STA. 0+78.19
 RAMP 4 - RT. STA. 0+48+51 TO STA. 1+20.20
 (NOT TO SCALE)

- LEGEND**
- ① EXISTING AGGREGATE SHOULDER, TYPE B
 - ② EXISTING BITUMINOUS CONCRETE SURFACE COURSE, MIXTURE D, CLASS I, 1 1/4"
 - ③ EXISTING LEVELING BINDER (MACHINE METHOD)
 - ④ EXISTING BITUMINOUS SHOULDER 8"
 - ⑤ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.06
 - ⑥ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.18
 - ⑦ EXISTING CONCRETE MEDIAN SURFACE, 4"
 - ⑧ EXISTING CONCRETE MEDIAN, TYPE C4 (MODIFIED)
 - ⑨ EXISTING PORTLAND CEMENT CONCRETE BASE COURSE 10"
 - ⑩ EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, 8"
 - ⑪ EXISTING STABILIZED SUB-BASE 4"
 - ⑫ EXISTING BITUMINOUS CONCRETE BINDER COURSE 3/4"
 - ⑬ EXISTING 4" PIPE UNDERDRAINS
 - ⑭ EXISTING SAND BACKFILL
 - ⑮ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M6.24
 - ⑯ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B6.24
 - ⑰ EXISTING SUBBASE GRANULAR MATERIAL 5"
 - ⑱ EXISTING HOT-MIX ASPHALT BASE COURSE, 8"
 - ⑲ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, 2"
 - ⑳ PROPOSED LEVELING BINDER (MACHINE METHOD), N90, VARIABLE DEPTH (3/4" MIN.)
 - ㉑ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90, 1 1/2"
 - ㉒ PROPOSED HOT-MIX ASPHALT BASE COURSE, 1 1/2"
 - ㉓ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
 - ㉔ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12
 - ㉕ PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
 - ㉖ PROPOSED CONCRETE MEDIAN, TYPE SM-6.12
 - ㉗ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
 - ㉘ PROPOSED 4" PIPE UNDERDRAINS
 - ㉙ PROPOSED AGGREGATE SHOULDERS, TYPE B
 - ㉚ PROPOSED AGGREGATE SHOULDERS, TYPE A, 8"
 - ㉛ PROPOSED LIME MODIFIED SOIL, 12"
 - ㉜ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
 - ㉝ PROPOSED AGGREGATE (PRIME COAT)
 - ㉞ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
 - ㉟ ITEMS TO BE REMOVED

- ① VARIES 0' TO 11'
FROM STA. 225+85.77 TO STA. 226+35.77
- ② VARIES 0' TO 12'
FROM STA. 225+85.77 TO STA. 226+35.77
- ③ VARIES 18' TO 6'
FROM STA. 225+85.77 TO STA. 226+35.77
- ④ VARIES 10' TO 4'
FROM STA. 225+85.77 TO STA. 226+12.95
- ⑤ 10' FROM STA. 237+25 TO STA. 238+33.99
VARIES 10' - 4' FROM STA. 238+33.99 TO STA. 239+59.32
4' FROM STA. 239+59.32 TO STA. 242+85
- ⑥ VARIES 6' TO 0'
FROM STA. 229+51.94 TO STA. 231+73.01
- ⑦ VARIES 59.48" TO 0'
FROM STA. 10+22.50 TO STA. 10+86.70

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICAL SECTIONS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
H:\P\26004\Technical Production\Civil\W07\Microstation\typical\oe2.dgn		DRAWN -	REVISED -		SCALE: NONE	SHEET NO. 6 OF 6 SHEETS	STA.	TO STA.	9111	73-15TS	MADISON	64	9
PLOT SCALE = 20,0000' / IN.		CHECKED -	REVISED -					CONTRACT NO. 76B22					
PLOT DATE = 3/14/2008		DATE -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT								

PAVEMENT SCHEDULE

STATION	STATION	PROCESS MOD SOIL 12' (SQ YD)	LIME (NOTE 1) (TON)	HMA BASE CSE 11 1/2 (SQ YD)	LEV BIND MM N90 (TON)	HMA SURF REM BUTT JT (SQ YD)	PCC SURF REM BUTT JT (SQ YD)	TEMPORARY RAMPS (SQ YD)	P HMA SC "D" N90 (TON)	BIT MATLS PR CT (NOTE 2) (GAL)	AGG PR CT (NOTE 3) (TON)	PAVT PATCH T2 11 (SQ YD)	AGGREGATE SHLDS A 8 (SQ YD)	AGGREGATE SHLDS B (TON)	HMA SHOULDERS 8" (SQ YD)		
WB LANES																	
225+06.50	225+85.77				19				10	9	0.2						
225+85.77	226+35.77	36	0.7	23	12				12	11	0.2					249	
226+12.06	231+72.32											11					
226+35.00	226+45.00																
226+35.77	229+53.12	734	14.1	558	43				74	66	1.3				17.6		
228+35.00	228+45.00											11					
229+53.12	231+44.26	602	11.6	496	59				44	40	0.8				10.6		
231+44.26	233+26.96	568	10.9	471	189				85	76	1.5				10.2		
233+18.99	242+85															429	
233+26.96	237+25.00	884	17.0	663	149				105	94	1.9				22.1		
237+25.00	242+85.00	747	14.4	436	57	37			94	84	1.7				31.1		
EB LANES																	
225+06.50	225+85.77				42				8	7	0.1				1.4	88	
225+85.77	226+12.95														0.5	21	
226+12.95	226+35.77	17	0.3	14	57				8	7	0.1				0.4	10	
226+35.77	229+53.12	418	8.0	383	421				65	58	1.2				5.4		
229+53.12	231+44.26	250	4.8	229	102				39	35	0.7				3.3		
226+35.77	231+65.73															236	
231+44.26	233+26.96	272	5.2	243	73				94	84	1.7				4.9		
233+18.99	242+85															41	
233+26.96	237+25.00	23	0.4	17	41				42	38	0.8				19.2		
237+25.00	242+85.00				36	37			57	51	1.0				27.0		
233+83.59	242+85.00															1002	
MEDIAN																	
225+06.50	225+85.77	9	0.2	6					9	8	0.2						
225+85.77	226+35.77																
226+35.77	229+53.12	40	0.8	26					4	4	0.1						
229+53.12	231+44.26	257	4.9	169					44	40	0.8						
226+35.77	231+65.73	152	2.9	99					27	24	0.5						
231+44.26	233+26.96								4	4	0.1						
EASTPORT PLAZA DRIVE																	
48+73.93	49+03.93							122								0.6	83
RAMP 4																	
LT 0+37.75	LT 1+77.86	62	1.2	46	14		53	53	30	26	0.5		22				44
RT 0+37.75	RT 0+48.51	60	1.2	44	10				21	19	0.4						33
TOTAL		5131	99	3923	1324	196	53	249	876	785	16	22	33	160			2236

NOTES: 1. LIME QUANTITY IS CALCULATED USING AN ASSUMED APPLICATION RATE OF 38.5 LB/SQ YD. THE ACTUAL APPLICATION RATE WILL BE DETERMINED BASED ON SOIL TESTS PROVIDED DURING CONSTRUCTION.
 2. BITUMINOUS MATERIALS PRIME COAT IS CALCULATED AT AN APPLICATION RATE OF 0.075 GAL/SQ YD
 3. AGGREGATE PRIME COAT IS CALCULATED AT AN APPLICATION RATE OF 3 LB/SQ YD.

REMOVAL SCHEDULE

STATION	STATION	PAVEMENT REM (SQ YD)	COMB CURB GUTTER REM (FOOT)	MEDIAN REMOVAL (SQ FT)	PAVED SHLD REMOVAL (SQ YD)	PIPE CULVERT REMOV (FOOT)	REMOV CONC FOUND (EACH)	PIPE UNDERDRAIN REMOV (FOOT)
RAMP 4								
LT 0+39.65	LT 0+91.72		61					
LT 0+91.72	LT 1+77.86				36			
RT 0+49.84	RT 1+77.86				103			
FAU RTE. 9111 (HORSESHOE LAKE ROAD)								
RT 225+06.5	RT 228+00							292
LT 1+55 (RAMP 4)	LT 228+00							331
RT 225+06.50	RT 231+83				757			
LT 226+02	LT 231+71				703			
64.0' LT 226+20							2	
LT 228+00	LT 233+00							500
RT 228+00	RT 233+00							500
LT 225+33	LT 229+52			4591				
LT 228+68	LT 231+44			1249				
LT 227+00	LT 231+74	393						
50.2' LT 229+26.36								
LT 231+71	LT 242+85				882		1	
LT 232+55	LT 238+25							587
38.93' LT 235+16.25								2
37.05' RT 230+60.94								2
32.43' RT 231+99.66								1
RT 232+70	RT 238+25							561
RT 233+18	RT 242+85				766			
EASTPORT PLAZA DRIVE								
RT 47+75	RT 49+04				110			
LT 47+87	LT 49+04				102			
FOURNIE LANE								
RT 10+45	LT 10+58					87		
TOTAL		393	61	5840	3459	87	8	2771

CURB & GUTTER SCHEDULE

STATION	STATION	COMB CC&G TB6.18 (FOOT)	COMB CC&G TB6.24 (FOOT)	COMB CC&G TM6.12 (FOOT)	COMB CC&G TM6.24 (FOOT)	PCC RAMP MED TERM (EACH)	CONC MEDIAN SURF 4 (SQ FT)	CON MED TSM6.12 (SQ FT)
RAMP 4								
LT 0+37.75	LT 0+45.79				54	1	257	
LT 0+45.79	LT 0+90.65				110			
RT 0+48.51	RT 1+29.07							
HORSESHOE LAKE ROAD								
LT 225+38.66	LT 226+35.77			195		1	954	
LT 226+35.77	LT 231+66.99					1		3168
EASTPORT PLAZA DRIVE								
RT 47+83.50	RT 48+63.57		106					
LT 47+94.41	LT 48+32.27		74					
FOURNIE LANE								
LT 10+54.20	RT 10+73.64	49						
RT 10+22.80	RT 10+73.64	72						
TOTAL		121	180	195	164	3	1211	3168

SEEDING SCHEDULE

STATION	STATION	SEEDING CL 2A SPL (ACRE)	NITROGEN FERT NUTR (POUND)	PHOSPHORUS FERT NUTR (POUND)	POTASSIUM FERT NUTR (POUND)	MULCH METHOD 2 (ACRE)
HORSESHOE LAKE ROAD						
LT 225+06.5	LT 232+21	0.6	54	54	54	0.6
LT 232+45	LT 242+85	0.2	18	18	18	0.2
RT 225+06.5	RT 232+35	0.2	18	18	18	0.2
RT 232+80	RT 242+85	0.2	18	18	18	0.2
TOTAL		1.2	108	108	108	1.2

WOVEN WIRE FENCE SCHEDULE

STATION	STATION	WOV W FENCE 4 (FOOT)	WOV W FENCE REMOV (FOOT)
HORSESHOE LAKE ROAD			
LT 1+32 (RAMP 4)	LT 231+92		650
LT 225+82	LT 232+17	621	
TOTAL		621	650

EARTHWORK SCHEDULE

STATION	STATION	EARTH EXCAVATION (CU YD)	EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (NOTE 1) (CU YD)	EMBANKMENT (NOTE 2) (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (NOTE 3) (CU YD)
HORSESHOE LAKE ROAD					
225+08	242+85	1188	891	1145	-254
EASTPORT PLAZA DRIVE					
47+83	49+03			316	-316
FOURNIE LANE					
10+54	10+74	2		18	-18
TOTAL		1190	891	1479	588

EARTHWORK NOTES:
 1. ESTIMATED SHRINKAGE FACTOR = 25%.
 2. APPROXIMATE EMBANKMENT QUANTITY IS SHOWN FOR INFORMATION ONLY.
 3. APPROXIMATE EARTHWORK BALANCE IS SHOWN FOR INFORMATION ONLY.

PROPOSED SIGNING

PROP. STA	RT/LT	OFFSET EOP TO EDGE OF SIGN	OFFSET CL TO CENTER OF SIGN SEE NOTE	TYPE	LEGEND	HIDE COL. SIZE (SQ FT)	SIGN PANEL (SQ FT)	POST LENGTH (STD 729001)				POST	
								SIGN	CLEAR	DROP	GRND	WOOD LIN FT	NO. OF POSTS
228+25	RT	18'		R3-1100-24	RIGHT TURN LANE	4.0	4.0	2	7	8	5	22	1
228+25	LT		18'	R3-1100-24	LEFT TURN LANE	4.0	4.0	2.0	7	0	5	14	1
230+50	LT	18'		R3-7R-30	RIGHT LANE MUST TURN RIGHT	6.3	6.3	2.5	7	3	5	18	1
TOTAL							14.3					54	

NOTE: FOR SIGNS IN THE MEDIAN THE DISTANCE IS FROM CENTERLINE TO POST

PAVEMENT MARKING SCHEDULE

STATION	STATION	DESCRIPTION	SHORT-TERM PAVT MKNG (FOOT)	TEMP PVT MK LN 4" (FOOT)	TEMP PVT MK LN 8" (FOOT)	TEMP PVT MK LN 12" (FOOT)	TEMP PVT MK LN 24" (FOOT)	TEMP PAVT MK L&S (SQ FT)	WORK ZONE PAVT MK REM (SQ FT)	THPL PVT MK LTR & SYM (SQ FT)	THPL PVT MK LN 4" (FOOT)	THPL PVT MK LN 8" (FOOT)	THPL PVT MK LN 12" (FOOT)	THPL PVT MK LN 24" (FOOT)	RAISED REFL PAVT MKR (NOTE 2) (EACH)	PRISMATIC CURB REFL (EACH)	PT PVT MK- RAISED MED (SQ FT)	PAINT PVT MARK CURB (FOOT)
RAMP 4																		
LT 0+40	LT 0+46	Med Ramp Nose															27	35
LT 0+46	LT 0+78	CC&G												144				
LT 0+40	LT 1+78	LT EOP		144										144				
RT 0+49	RT 1+78	RT EOP		162										162				
RT 0+51	RT 0+63	Stop Bar					18		36					18				
FAU RTE. 9111 (HORSESHOE LAKE ROAD)																		
LT 225+48	LT 225+81	Island			114	62			138				114	62				
LT 225+32	LT 225+38	Med Ramp Nose															28	
LT 225+38	LT 225+86	EB Med Curb													3			49
LT 225+38	LT 231+67	WB CC&G		629					208		629				17			
LT 230+95	LT 231+67	WB Med Curb															24	48
LT 231+67	LT 231+73	Med Ramp Nose																
LT 226+12	LT 231+72	WB EOP									185							
LT 225+82	LT 231+72	WB Ln Line	118	589							194							
LT 225+57	LT 225+57	WB Stop Bar					14		28					14				
LT 228+55	LT 228+63	WB Arrows						15.6	16	15.6								
LT 228+85	LT 228+93	WB Arrow						15.6	16	15.6								
LT 225+38	LT 231+67	EB CC&G		629					208		629					32		
LT 225+86	LT 228+26	EB Lt. Turn Ln		62					20		62							
LT 228+26	LT 232+11	EB Lt. Turn Ln	77	385					127		385				11			
LT 228+97	LT 229+05	EB Arrow						15.6	16	15.6								
LT 231+12	LT 231+20	EB Arrow						15.6	16	15.6								
LT 231+15	LT 231+23	WB Arrow						15.6	16	15.6								
RT 231+67	RT 231+67	EB Stop Bar					12		24					12				
RT 225+07	RT 231+66	EB EOP		659					217					659				
RT 225+86	RT 228+25	EB Rt. Turn Ln		62					20		62							
RT 228+25	RT 231+98	EB Rt. Turn Ln	75	373					123		373				10			
RT 232+11	LT 232+11	EB Stop Bar					14		28					14				
RT 228+97	RT 229+05	EB Arrow						15.6	16	15.6								
RT 231+12	RT 231+20	EB Arrows						15.6	16	15.6								
RT 231+98	RT 232+28	Island		109	39				112			109	39					
FOURNIE LANE																		
LT 10+54	LT 10+73	LT EOP		52					17		52							
LT 10+65	LT 10+72	Stop Bar					24		48					24				
RT 10+22	RT 10+73	RT EOP		99					33		99							
EASTPORT PLAZA DRIVE																		
RT 47+83	RT 48+64	RT EOP		110					36		110							
RT 47+90	RT 48+02	Stop Bar					18		36					18				
RT 48+18	RT 48+65	Dbl Ln Line	5	48					16		48				3			
RT 48+50	LT 48+58	Lt Turn Arrow						15.6	16	15.6								
RT 48+64	LT 48+81	Bike Path			243				243									
LT 47+93	LT 47+93	Stop Bar					12		24					12				
LT 47+93	LT 48+66	Lane Line	15	73					24		73							
LT 47+92	LT 48+67	LT EOP		166					55		166							
LT 47+93	LT 48+07	Stop Bar					18		36					18				
LT 48+18	LT 48+18	Stop Bar					12		24					12				
RT 48+78	RT 49+04	RT EOP		26					9		26							
RT 48+79	RT 49+04	Dbl Ln Line	3	25					8		25				3			
LT 48+80	LT 49+04	Lane Line	5	24					8		24							
LT 48+81	LT 49+04	LT EOP		24					8		24							
FAU RTE. 9111 (HORSESHOE LAKE ROAD)																		
RT 232+64	RT 232+91	Island			88	34			93			88	34					
RT 233+84	RT 242+85	RT EOP		901					297		901							
LT 232+95	LT 242+85	Median	396	3960		255			1562		3960		255		52			
LT 233+04	LT 233+04	Stop Bar					12		24					12				
LT 232+71	LT 232+78	WB Stop Bar					23		46					23				
LT 232+71	LT 235+45	WB Ln Line	55	274					90		274				8			
LT 233+50	LT 233+58	WB Arrows						15.6	16	15.6								
LT 234+90	LT 234+98	WB Arrow						15.6	16	15.6								
LT 235+45	LT 237+25	WB Ln Line		47					16		47							
LT 233+14	LT 242+85	LT EOP		971					320		971							
TOTAL			749	11054	311	633	177	156	5002	156	11054	311	177	103	52	79	132	

PAVEMENT MARKING NOTES:

- RAISED REFLECTIVE PAVEMENT MARKER QUANTITY INCLUDES THE FOLLOWING TYPES:

64 EACH	EACH	ONE-WAY AMBER MARKERS
45 EACH	EACH	ONE-WAY CRYSTAL MARKERS
EACH	EACH	TWO-WAY AMBER MARKERS
109 EACH	EACH	TOTAL

ONE-WAY AMBER MARKERS
ONE-WAY CRYSTAL MARKERS
TWO-WAY AMBER MARKERS
TOTAL

390

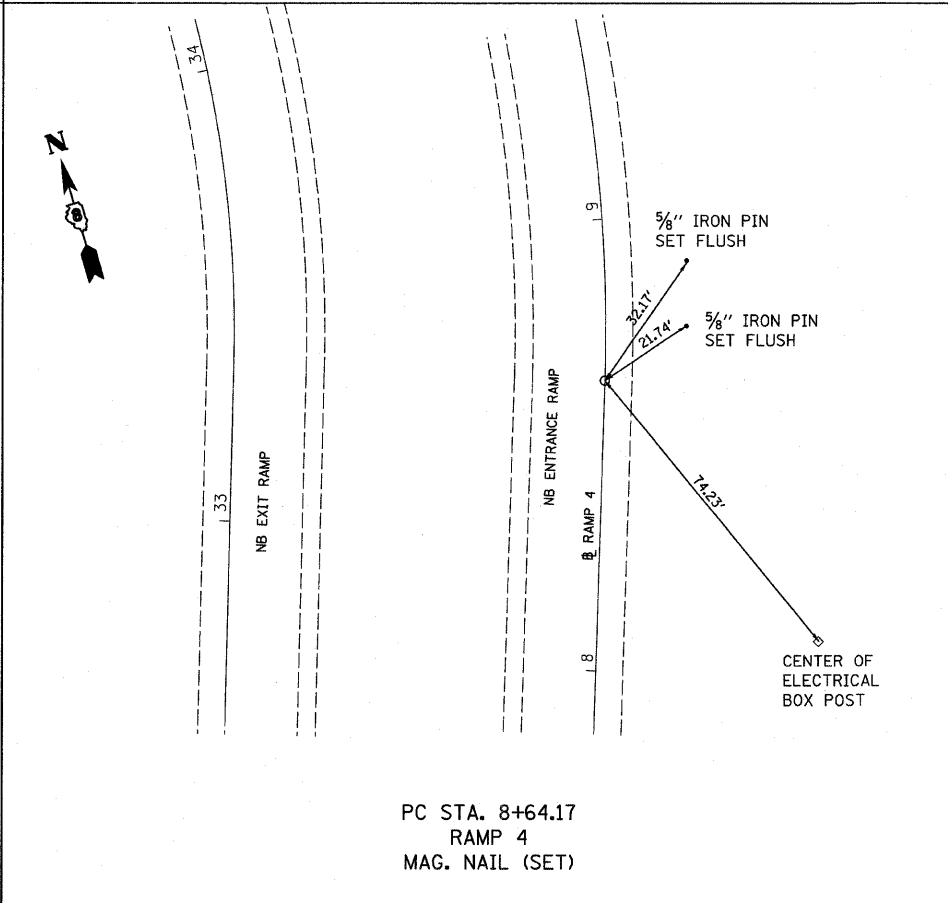
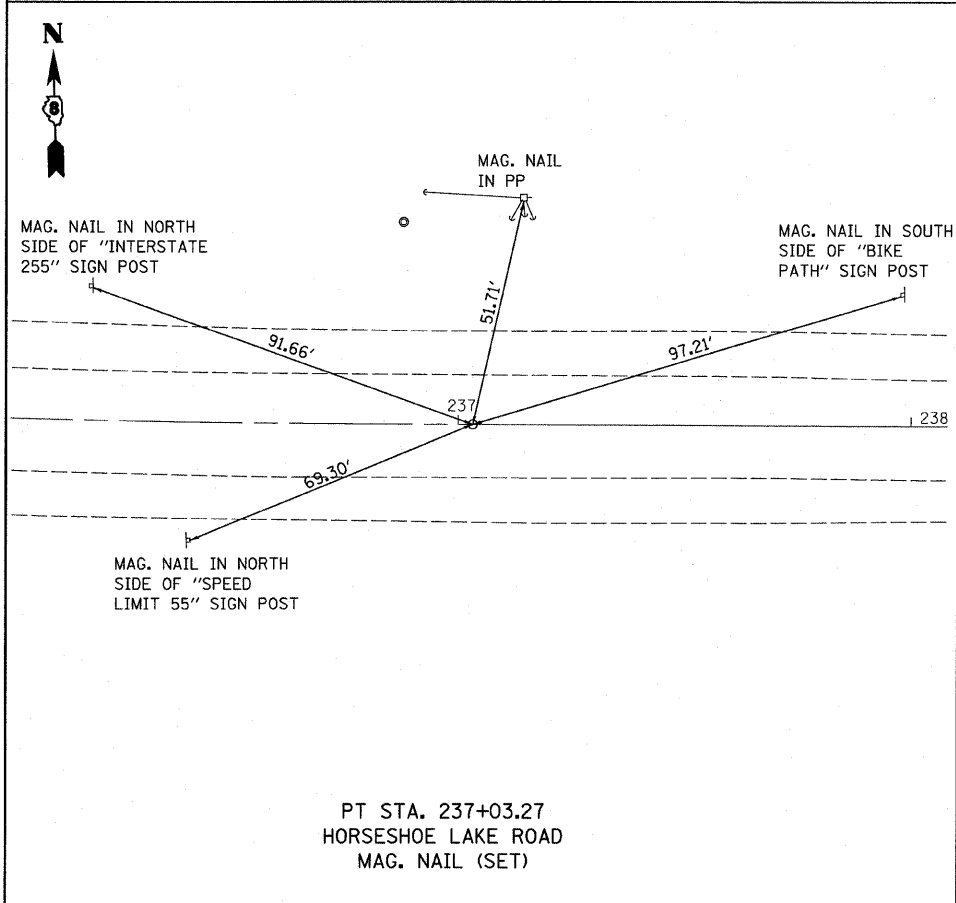
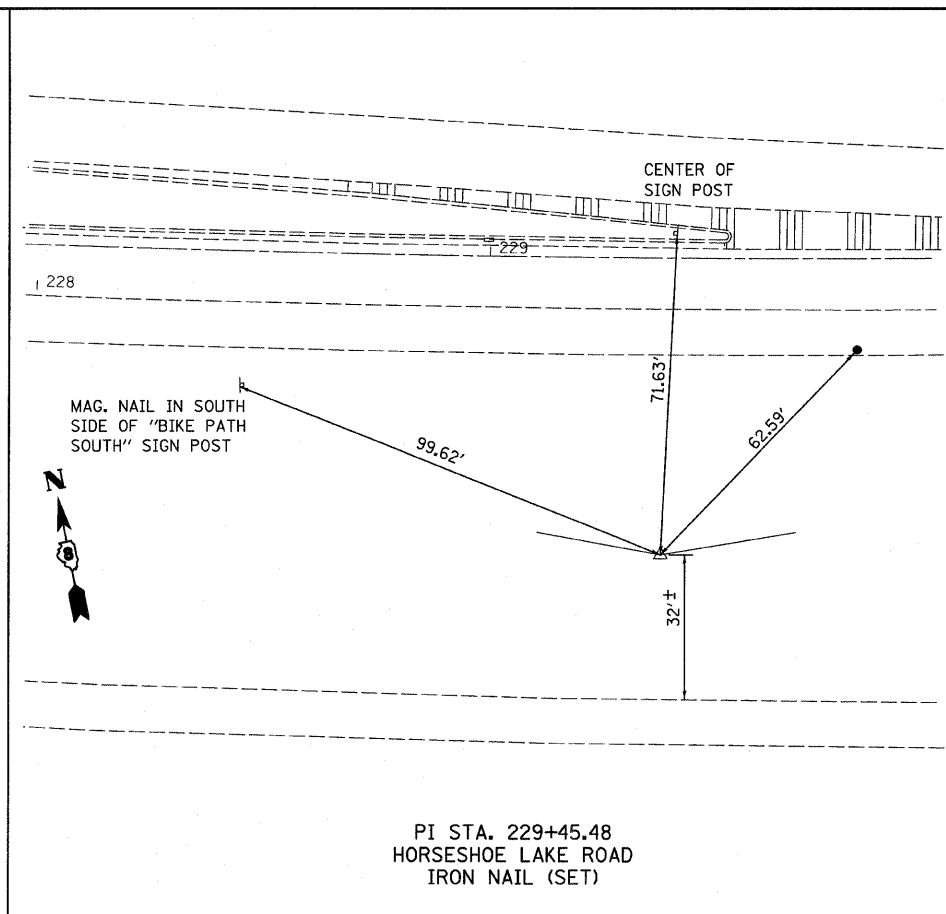
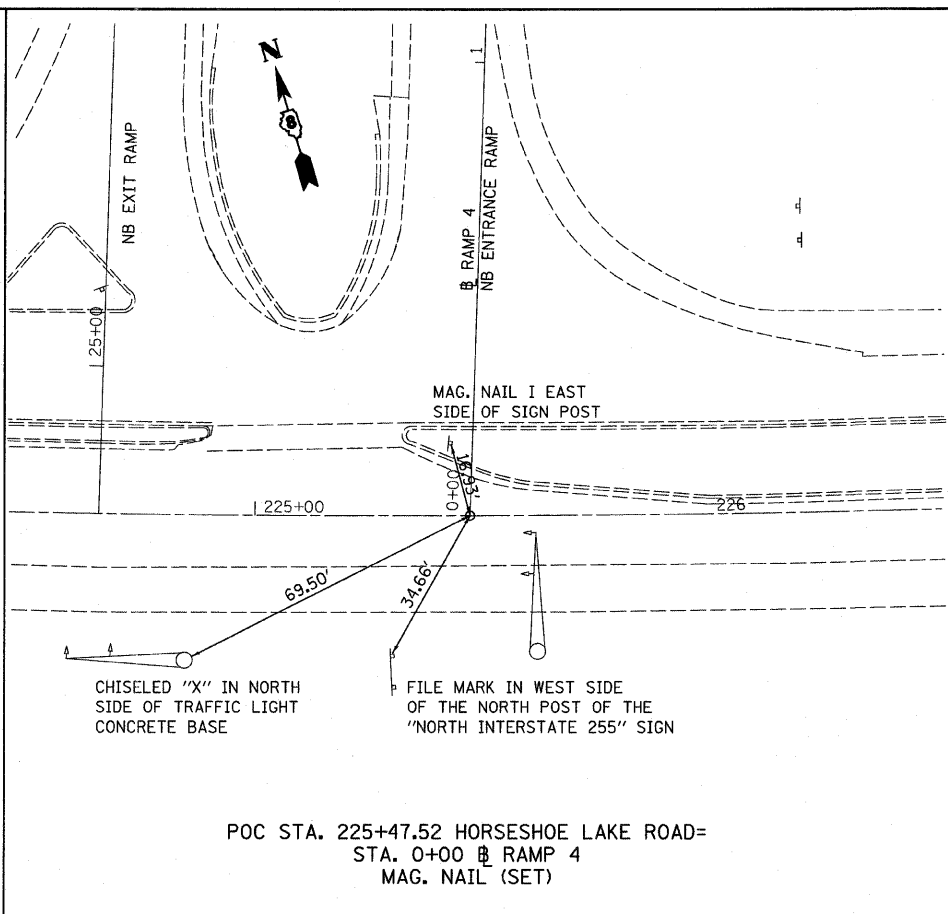
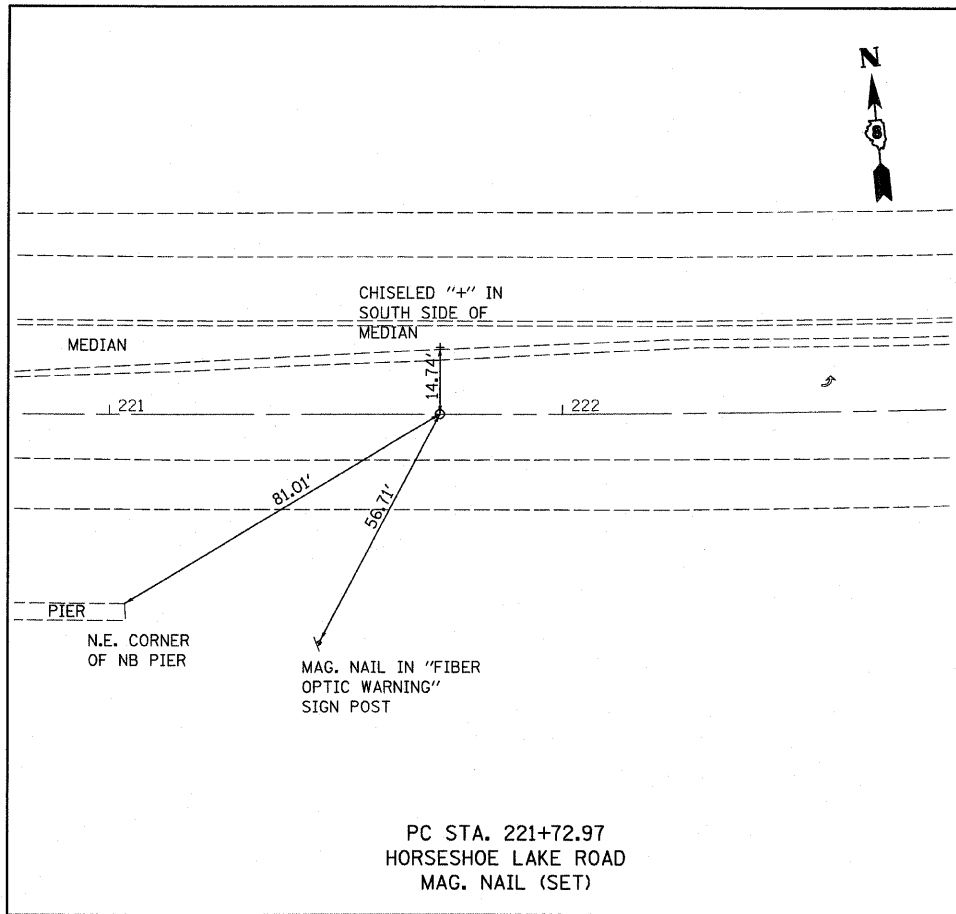
*TO BE PAID FOR AS
PAINT PAVEMENT MARKING - LINE 12"
(70001150)

EROSION CONTROL SCHEDULE

STATION	STATION	EROSION CONTROL BLANKET (SQ YD)	TEMP EROS CONTR SEED (POUND)	TEMPORARY DITCH CHECKS (EACH)	PERIMETER EROS BAR (FOOT)	INLET & PIPE PROTECTION (EACH)
RAMP 4						
RT 1+56						1
LT 0+73	LT 1+78				105	
HORSESHOE LAKE ROAD						
LT 226+40						1
LT 226+50			1			
RT 226+50			1			
LT 226+65						1
LT 226+90						1
LT 227+15						1
LT 227+40						1
LT 227+65						1
LT 227+90						1
LT 228+15						1
LT 228+40						1
LT 228+65						1
LT 228+90						1
LT 229+15						1
LT 229+40						1
LT 229+65						1
LT 229+90						1
LT 230+00						1
LT 230+80						1
LT 231+50				1		
LT 232+92						1
RT 233+02						1
LT 233+50				1		
RT 238+00				1		
RT 239+66						1
LT 239+66						1
RT 1+78 (RAMP 4)	LT 232+17				741	
LT 225+06.5	LT 232+45					
RT 225+06.5	RT 232+12	1566	60			
RT 225+06.5	RT 232+80		20			
RT 225+38	RT 231+87				657	
LT 232+45	LT 242+85		20			
LT 232+71	LT 242+90				1031	
RT 232+80	RT 242+85		20			
RT 233+01	RT 242+90				993	
EASTPORT PLAZA DRIVE						
RT 48+03	RT 48+63					68
LT 48+09	LT 48+67					65
RT 48+81	RT 49+09					28
LT 48+85	LT 49+07					22
TOTALS		1566	120	5	3710	22

PIPE UNDERDRAIN SCHEDULE

STATION	STATION	CONC HDWL FOR P DRAIN (EACH)	PIPE UNDERDRAINS 4 (FOOT)	PIPE UNDERDRAINS 4 SP (FOOT)
FAU RTE. 9111 (HORSESHOE LAKE ROAD)				
LT 226+14	LT 228+90	1	276	20
LT 228+90	LT 231+70	1	280	20
LT 233+20	LT 236+57	1	336	11
LT 236+60	LT 239+80	1	300	9
LT 239+72	LT 242+85	1	313	9
RT 225+06.5	RT 228+38	1	333	10
RT 228+42	RT 231+64	1	322	14
RT 233+32	RT 236+60	1	333	23
RT 236+64	RT 239+80	1	296	4
RT 239+72	RT 242+85	1	313	4
Ramp 4				
LT 0				



POINT NO.	STATION	NORTHING	EASTING
HORSESHOE LAKE ROAD			
8042	P.O.T. STA. 191+30.40	738088.4688	2332765.3169
	P.C. STA. 221+72.97	736995.7837	2335604.9087
8022	P.O.C. STA. 225+47.52	736875.8709	2335959.6257
	P.I. STA. 229+45.48	736718.3506	2336325.8820
	P.T. STA. 237+03.27	736695.5840	2337098.0565
8043	P.O.T. STA. 243+30.00	736677.1136	2337724.5147
EASTPORT PLAZA DRIVE			
31	P.O.T. STA. 47+70	736732.2470	2336641.2470
30	P.O.T. STA. 50+00.80	736501.5760	2336633.5070
FOURNIE LANE			
10	P.O.T. STA. 10+00	736732.3055	2336640.8010
	P.C. STA. 15+45.42	737277.4656	2336657.5718
	PI STA. 16+13.52	737345.5373	2336659.6659
	PT STA. 16+81.42	737412.6966	2336670.9698
11	P.O.T. STA. 17+11.82	737442.6839	2336676.0171
RAMP 4			
8022	P.O.T. STA. 0+00	736875.8709	2335959.6257
	PC STA. 8+64.17	737696.9122	2336229.2131

FILE NAME =	USER NAME = #USER*	DESIGNED -	REVISED -
H:\P\25004\Technical Production\Civil\W0	\Microstation\alittleos2.dgn	DRAWN -	REVISED -
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 3/14/2008	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

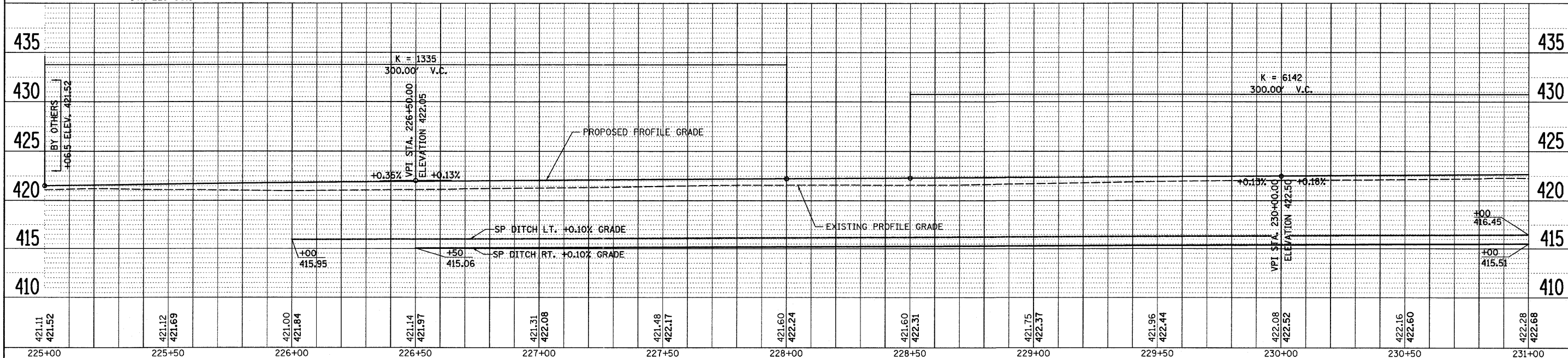
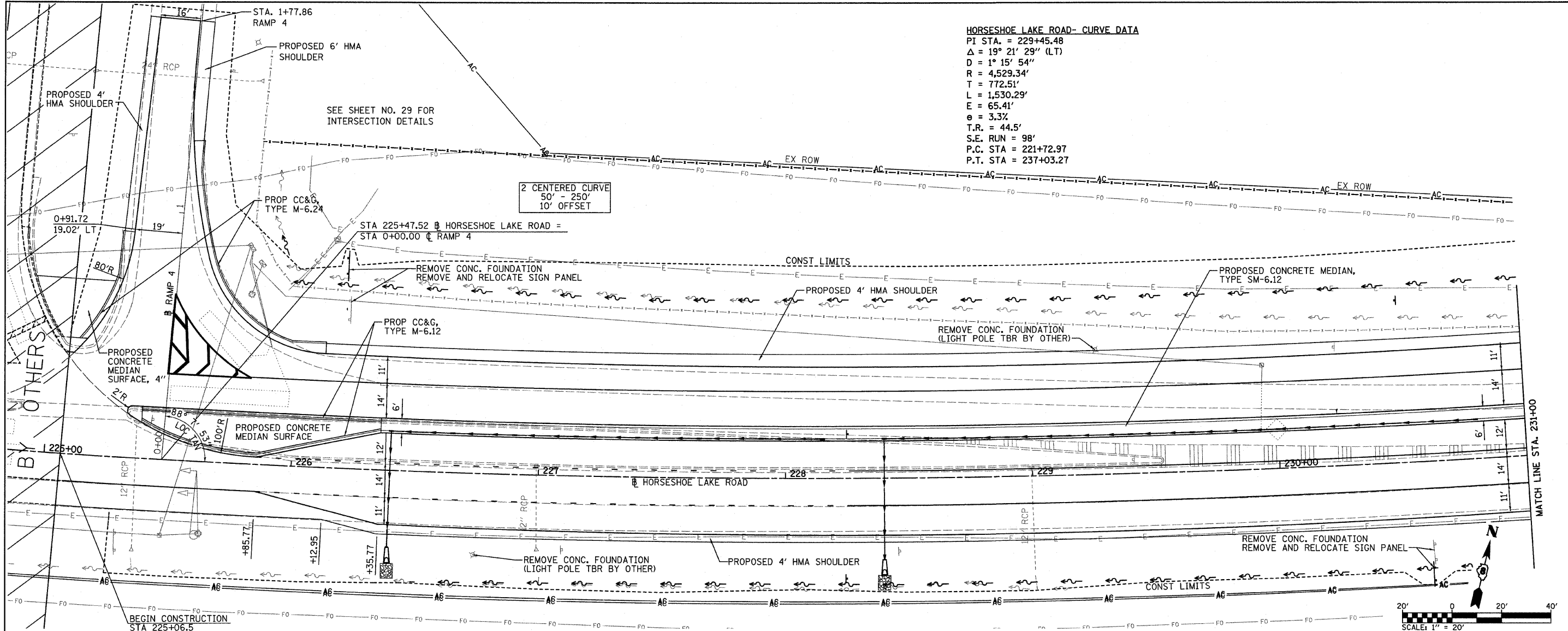
ALIGNMENT TIE SHEET

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9111	73-15TS	MADISON	12	
CONTRACT NO.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

HORSESHOE LAKE ROAD- CURVE DATA

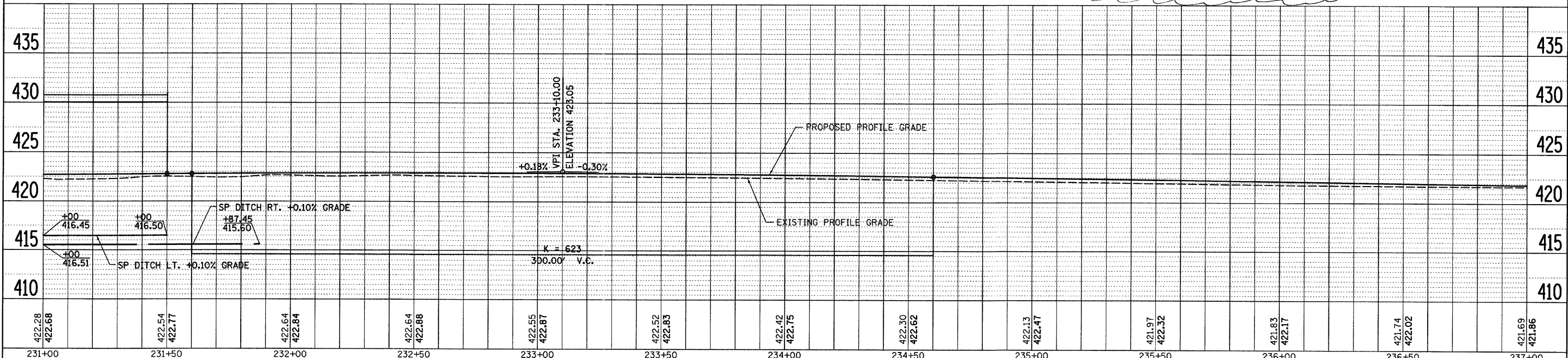
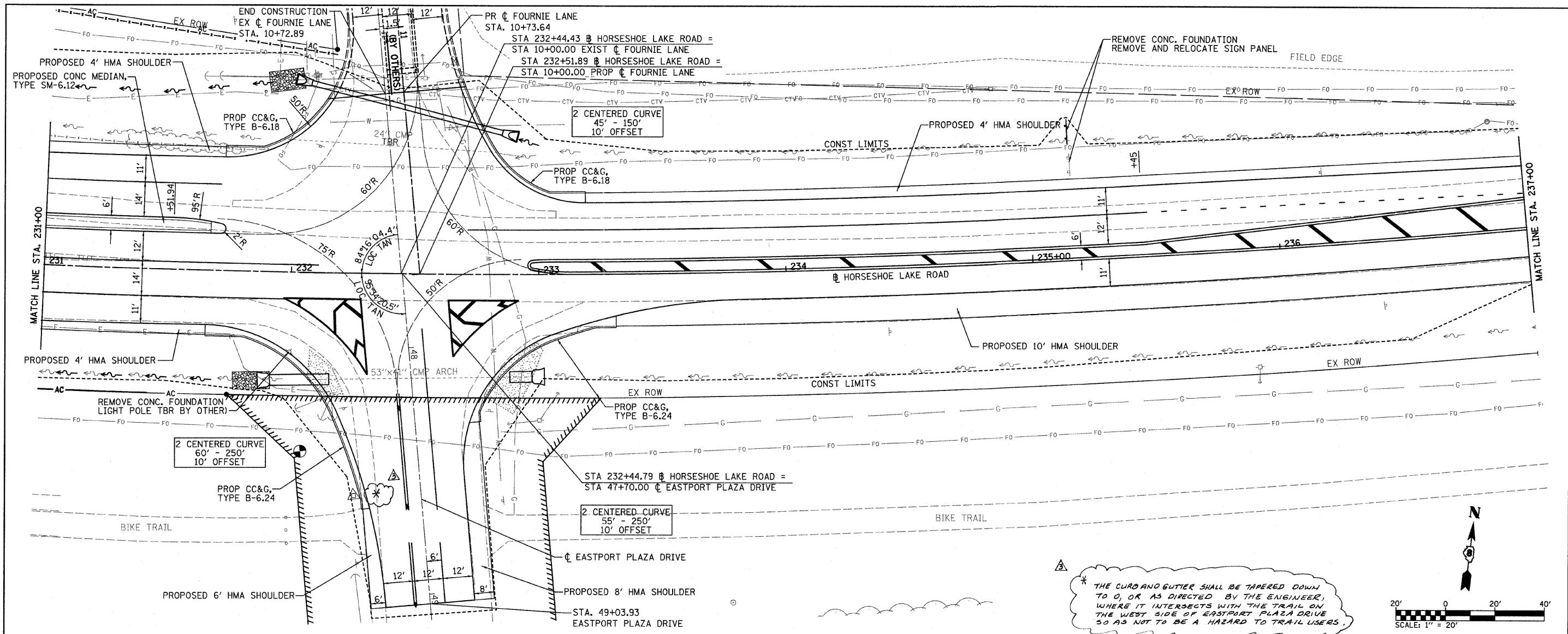
PI STA. = 229+45.48
 $\Delta = 19^\circ 21' 29''$ (LT)
 $D = 1^\circ 15' 54''$
 $R = 4,529.34'$
 $T = 772.51'$
 $L = 1,530.29'$
 $E = 65.41'$
 $e = 3.3\%$
 $T.R. = 44.5'$
 $S.E. RUN = 98'$
 P.C. STA = 221+72.97
 P.T. STA = 237+03.27



PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF DAYS CHECKED	
	NO.	
	CAD FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF DAYS CHECKED	
	NO.	
	STRUCTURE NOTATIONS CHECKED	

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HORSESHOE LAKE ROAD PLAN & PROFILE SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
H:\P\25084\Technical Production\Civil\W017	Microstation\PInoe2b.dgn	DRAWN -	REVISED -			9111	73-15TS	MADISON	64	13	
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 76B22					
	PLOT DATE = 3/14/2008	DATE -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					



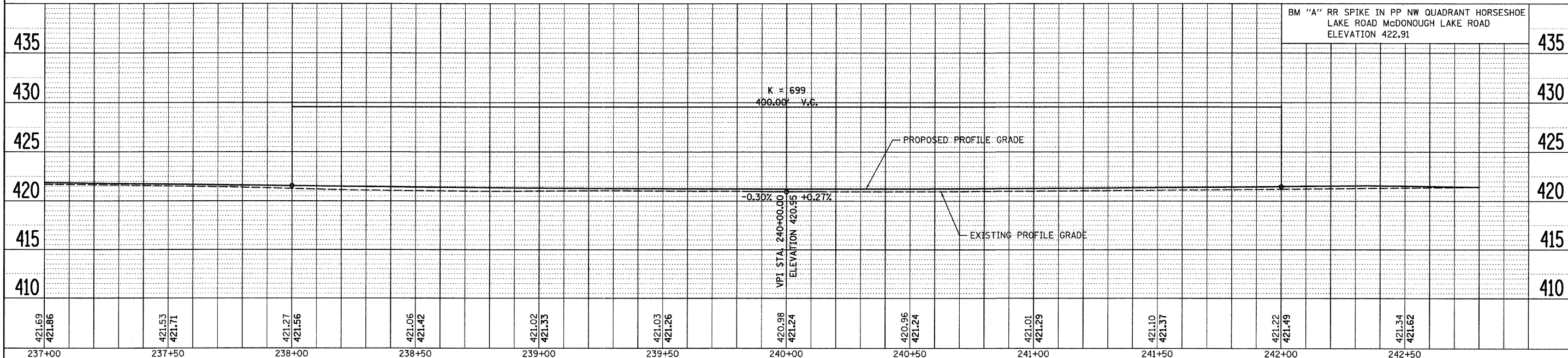
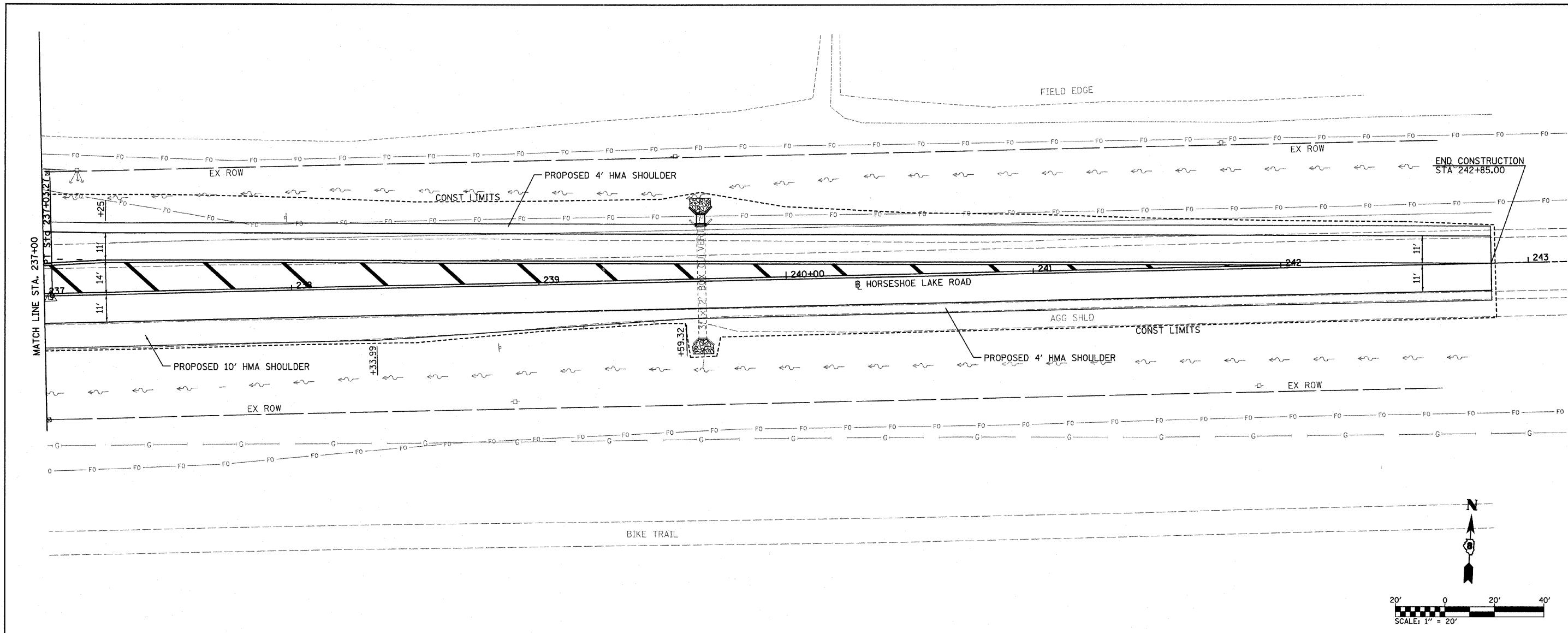
FILE NAME = H:\P\25004\Technical Production\Civil\W017	USER NAME = #USER# Microstation\Plnos2b.dgn	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		HORSESHOE LAKE ROAD PLAN & PROFILE SHEETS Rev. 6-6-08		F.A.U. RTE. 9111	SECTION 73-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 14
PLOT SCALE = 20.0000' / IN.	PLOT DATE = 3/14/2008	DRAWN -	REVISED -					CONTRACT NO. 76B22				
		CHECKED -	REVISED -					SCALE: 1" = 20' SHEET NO. 2 OF 3 SHEETS STA. 231+00.00 TO STA. 237+00.00				
		DATE -	REVISED -					FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
NO.	RT. OF WAY CHECKED	
	CADD FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
	STRUCTURE NOTATIONS CHRG	

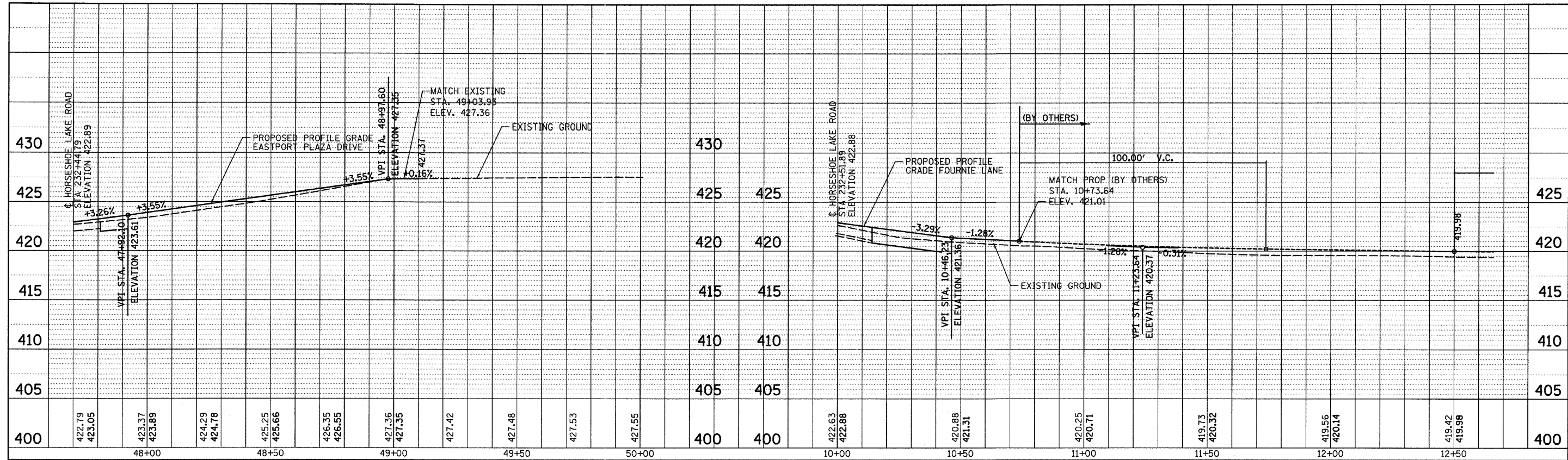
PLAN	SURVEYED	DATE
	PLOTTED	BY
	CHECKED	
	DATE	
	NO. OF DAYS CHECKED	
	CAD FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	CHECKED	
	DATE	
	NO. OF DAYS CHECKED	
	STRUCTURE NOTATION CHFD	



FILE NAME = H:\P\25004\Technical Production\Civil\W017	USER NAME = #USER# Microstation\Plinoe2b.dgn	DESIGNED -	REVISD -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HORSESHOE LAKE ROAD PLAN & PROFILE SHEETS	F.A.U. RTE. 9111	SECTION 73-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 15	
PLOT SCALE = 20.0000' / IN.	PLOT DATE = 3/14/2008	DRAWN -	REVISD -			SCALE: 1" = 20'		SHEET NO. 3 OF 3 SHEETS		STA. 237+00.00 TO STA. 243+00.00	
		CHECKED -	REVISD -			CONTRACT NO. 76B22					
		DATE -	REVISD -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	NOTED		
	NO. _____		
	NO. _____		
	NO. _____		

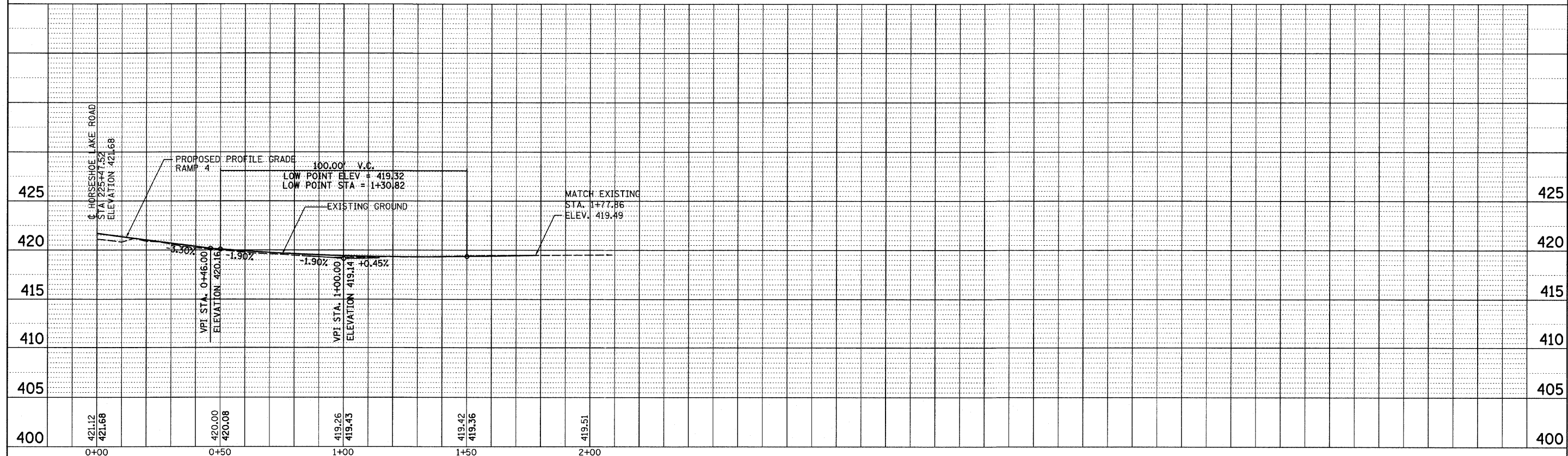


EASTPORT PLAZA DRIVE PROFILES

FOURNIE LANE PROFILES

RAMP 4 PROFILES

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	NOTED		
	NO. _____		
	NO. _____		
	NO. _____		



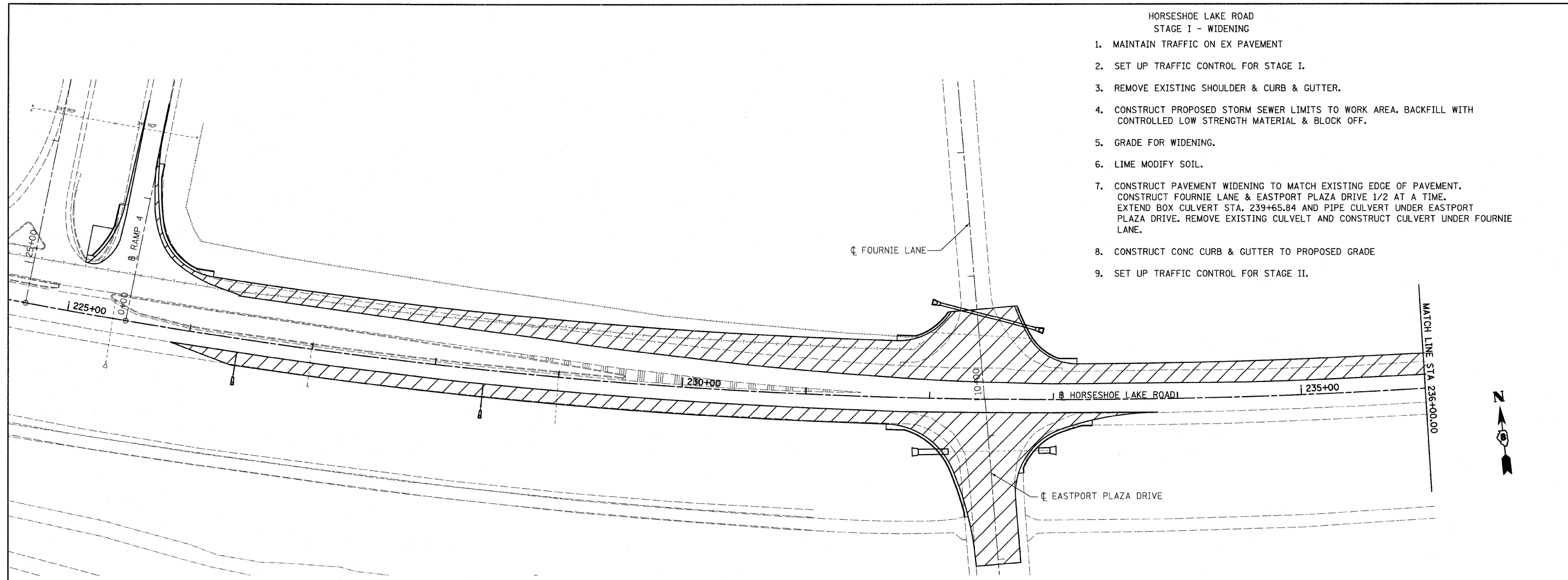
FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -
H:\P\25004\Technical Production\Civil\W017	Microstation\ar_prof001.dgn	DRAWN -	REVISED -
	PLT SCALE = 20.0000' / IN.	CHECKED -	REVISED -
	PLT DATE = 3/14/2008	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

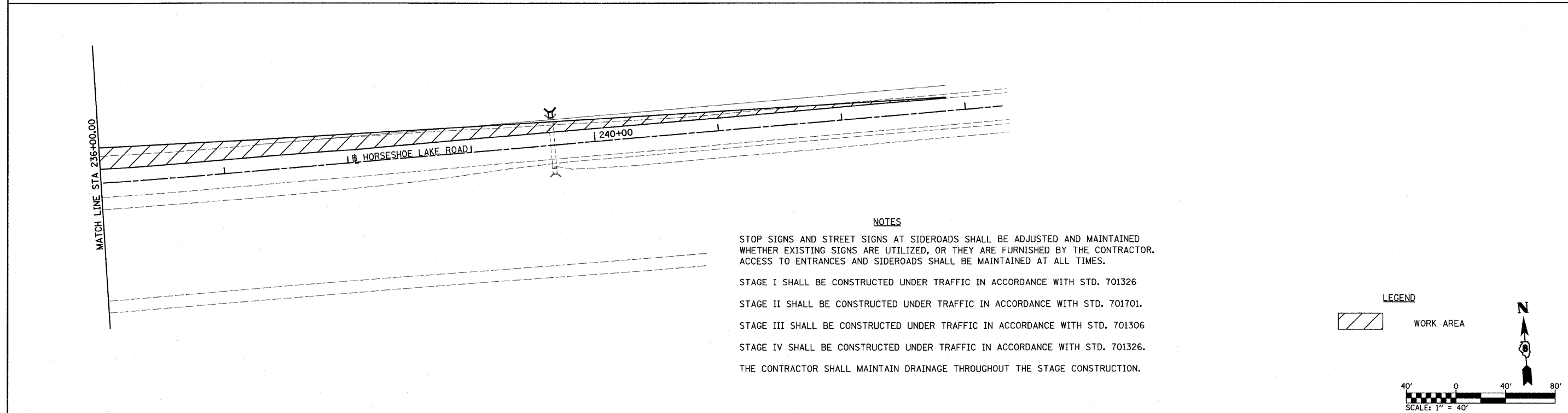
EASTPORT PLAZA DRIVE, FOURNIE LANE AND RAMP 4 PROFILE SHEET

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9111	73-15TS	MADISON	64	16
CONTRACT NO. 76B22			ILLINOIS FED. AID PROJECT	



- HORSESHOE LAKE ROAD
STAGE I - WIDENING
1. MAINTAIN TRAFFIC ON EX PAVEMENT
 2. SET UP TRAFFIC CONTROL FOR STAGE I.
 3. REMOVE EXISTING SHOULDER & CURB & GUTTER.
 4. CONSTRUCT PROPOSED STORM SEWER LIMITS TO WORK AREA. BACKFILL WITH CONTROLLED LOW STRENGTH MATERIAL & BLOCK OFF.
 5. GRADE FOR WIDENING.
 6. LIME MODIFY SOIL.
 7. CONSTRUCT PAVEMENT WIDENING TO MATCH EXISTING EDGE OF PAVEMENT. CONSTRUCT FOURNIE LANE & EASTPORT PLAZA DRIVE 1/2 AT A TIME. EXTEND BOX CULVERT STA. 239+65.84 AND PIPE CULVERT UNDER EASTPORT PLAZA DRIVE. REMOVE EXISTING CULVERT AND CONSTRUCT CULVERT UNDER FOURNIE LANE.
 8. CONSTRUCT CONC CURB & GUTTER TO PROPOSED GRADE
 9. SET UP TRAFFIC CONTROL FOR STAGE II.



NOTES

STOP SIGNS AND STREET SIGNS AT SIDEROADS SHALL BE ADJUSTED AND MAINTAINED WHETHER EXISTING SIGNS ARE UTILIZED, OR THEY ARE FURNISHED BY THE CONTRACTOR. ACCESS TO ENTRANCES AND SIDEROADS SHALL BE MAINTAINED AT ALL TIMES.

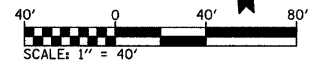
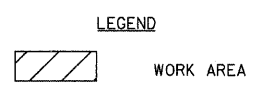
STAGE I SHALL BE CONSTRUCTED UNDER TRAFFIC IN ACCORDANCE WITH STD. 701326

STAGE II SHALL BE CONSTRUCTED UNDER TRAFFIC IN ACCORDANCE WITH STD. 701701.

STAGE III SHALL BE CONSTRUCTED UNDER TRAFFIC IN ACCORDANCE WITH STD. 701306

STAGE IV SHALL BE CONSTRUCTED UNDER TRAFFIC IN ACCORDANCE WITH STD. 701326.

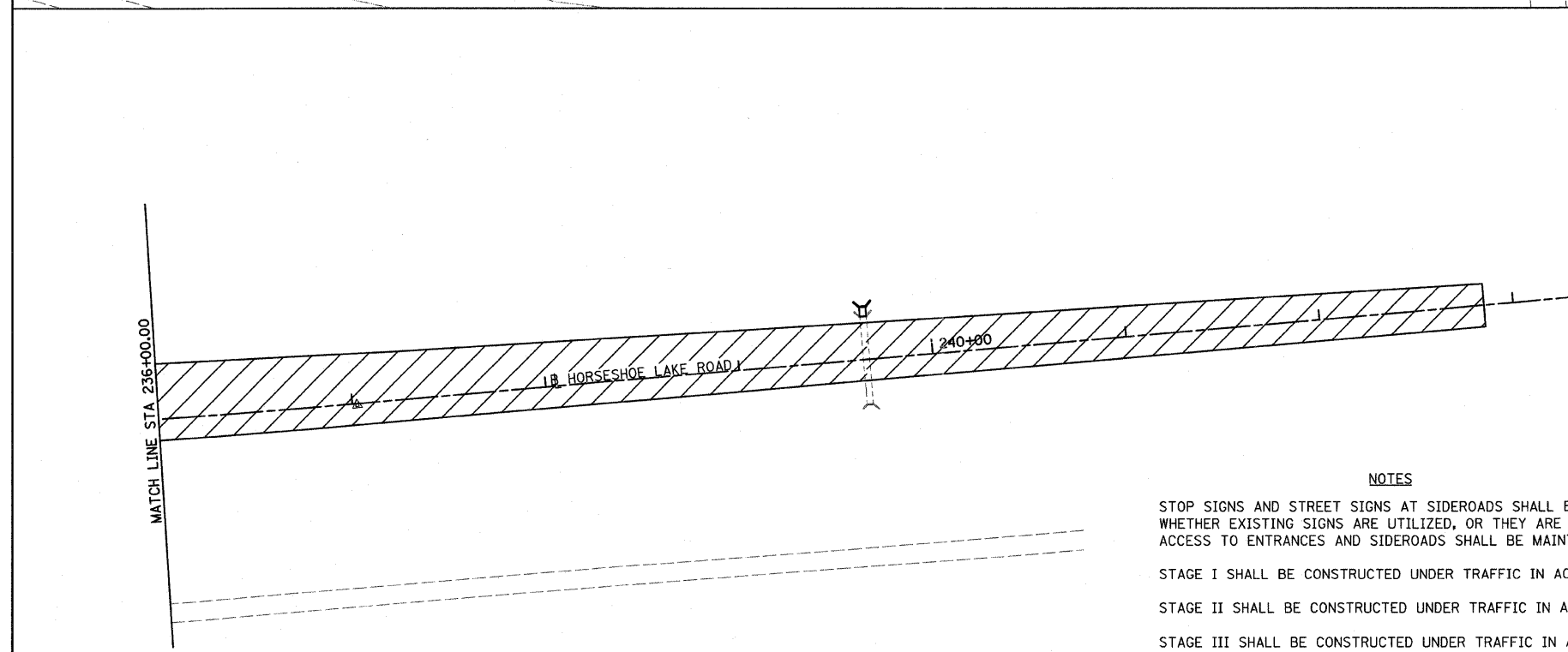
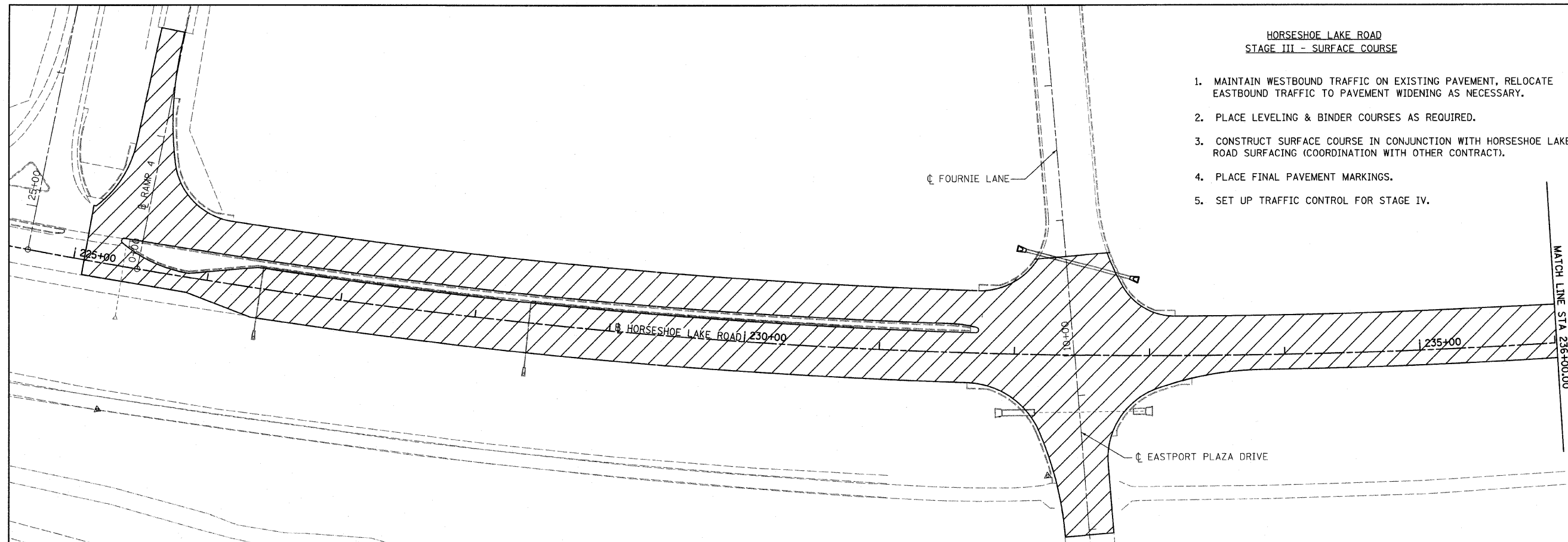
THE CONTRACTOR SHALL MAINTAIN DRAINAGE THROUGHOUT THE STAGE CONSTRUCTION.



FILE NAME = H:\P\25024\Technical Production\Civil\W07	USER NAME = @USER@ 7\Microstation\stg001.dgn	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED STAGING STAGE I HIGHWAY CONSTRUCTION			F.A.U. RTE. 9111	SECTION 73-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 17	
PLOT SCALE = 40,0000 ' / IN.					SCALE: 1" = 40'	SHEET NO. 1 OF 4 SHEETS	STA.	TO STA.	CONTRACT NO. 76B22				
PLOT DATE = 3/14/2008					DATE -	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							
					REVISOR -								

HORSESHOE LAKE ROAD
STAGE III - SURFACE COURSE

1. MAINTAIN WESTBOUND TRAFFIC ON EXISTING PAVEMENT, RELOCATE EASTBOUND TRAFFIC TO PAVEMENT WIDENING AS NECESSARY.
2. PLACE LEVELING & BINDER COURSES AS REQUIRED.
3. CONSTRUCT SURFACE COURSE IN CONJUNCTION WITH HORSESHOE LAKE ROAD SURFACING (COORDINATION WITH OTHER CONTRACT).
4. PLACE FINAL PAVEMENT MARKINGS.
5. SET UP TRAFFIC CONTROL FOR STAGE IV.



NOTES

STOP SIGNS AND STREET SIGNS AT SIDEROADS SHALL BE ADJUSTED AND MAINTAINED WHETHER EXISTING SIGNS ARE UTILIZED, OR THEY ARE FURNISHED BY THE CONTRACTOR. ACCESS TO ENTRANCES AND SIDEROADS SHALL BE MAINTAINED AT ALL TIMES.

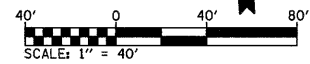
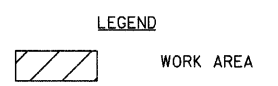
STAGE I SHALL BE CONSTRUCTED UNDER TRAFFIC IN ACCORDANCE WITH STD. 701326

STAGE II SHALL BE CONSTRUCTED UNDER TRAFFIC IN ACCORDANCE WITH STD. 701701.

STAGE III SHALL BE CONSTRUCTED UNDER TRAFFIC IN ACCORDANCE WITH STD. 701306

STAGE IV SHALL BE CONSTRUCTED UNDER TRAFFIC IN ACCORDANCE WITH STD. 701326

THE CONTRACTOR SHALL MAINTAIN DRAINAGE THROUGHOUT THE STAGE CONSTRUCTION.



FILE NAME = H:\P\25004\Technical Production\Civil\W07

USER NAME = @USER@
7\Microstation\stg003.dgn
PLOT SCALE = 40.0000' / IN.
PLOT DATE = 3/14/2008

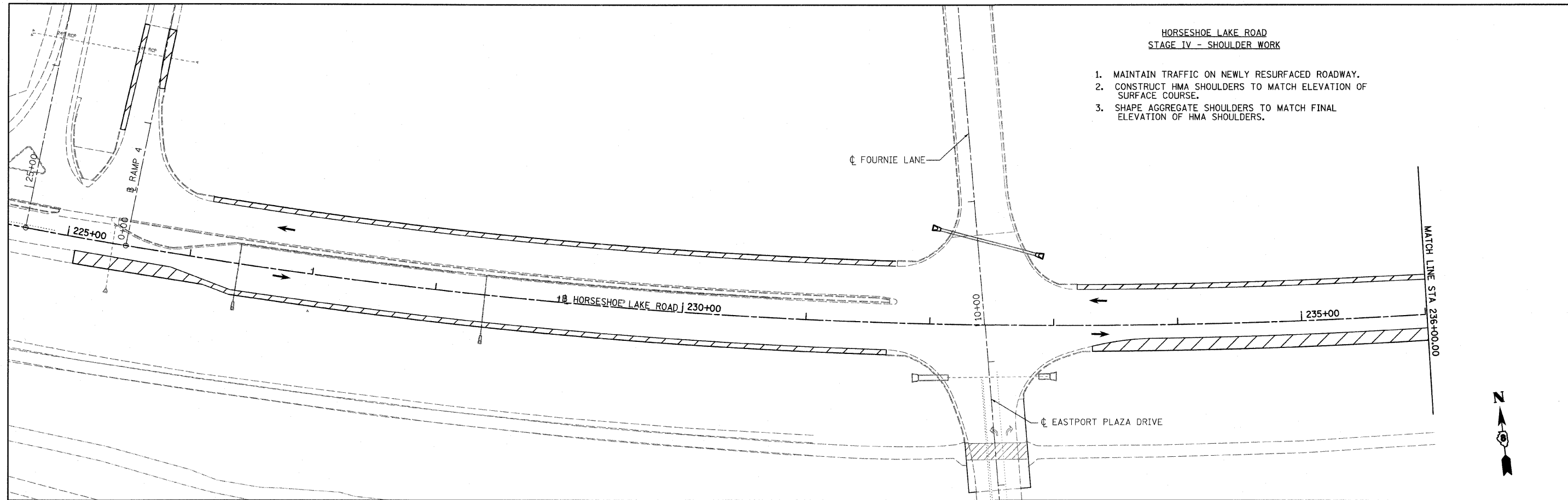
DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGING
STAGE III HIGHWAY CONSTRUCTION

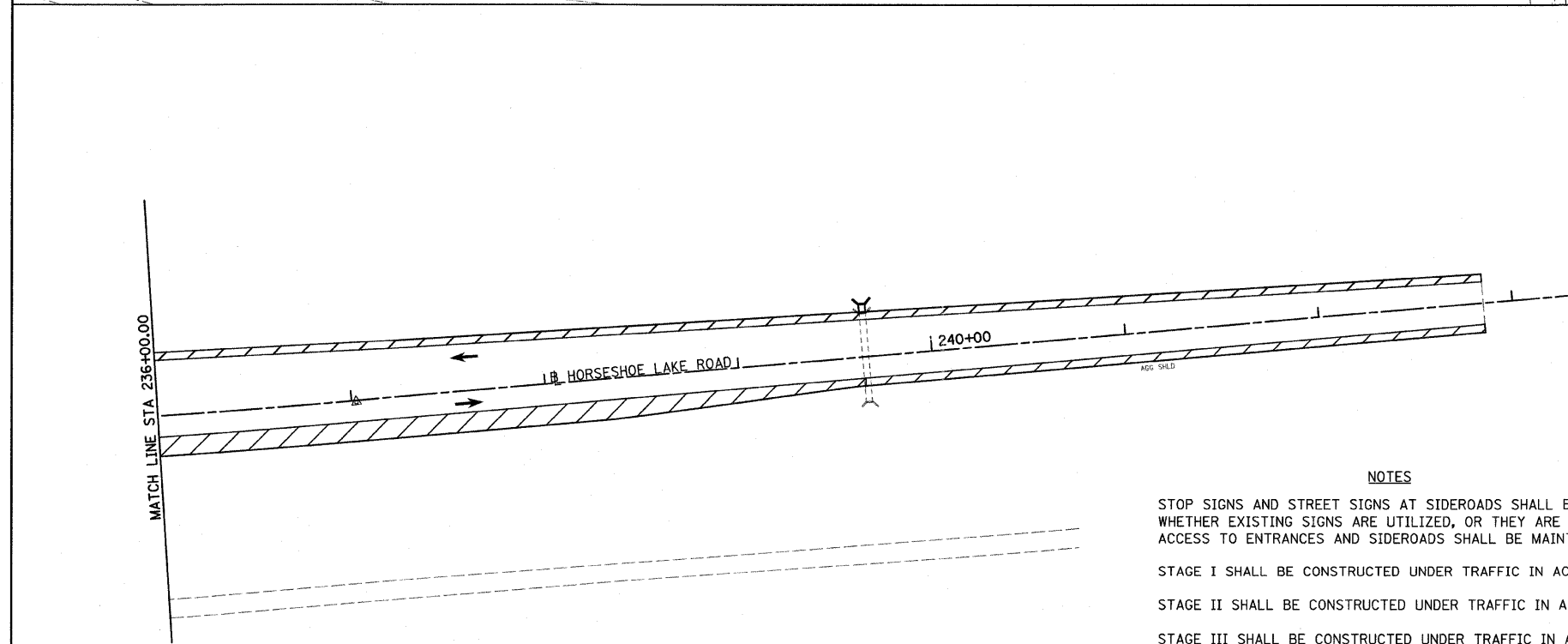
SCALE: 1" = 40' SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9111	73-15TS	MADISON	64	19
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76B22	



**HORSESHOE LAKE ROAD
STAGE IV - SHOULDER WORK**

1. MAINTAIN TRAFFIC ON NEWLY RESURFACED ROADWAY.
2. CONSTRUCT HMA SHOULDERS TO MATCH ELEVATION OF SURFACE COURSE.
3. SHAPE AGGREGATE SHOULDERS TO MATCH FINAL ELEVATION OF HMA SHOULDERS.



NOTES

STOP SIGNS AND STREET SIGNS AT SIDEROADS SHALL BE ADJUSTED AND MAINTAINED WHETHER EXISTING SIGNS ARE UTILIZED, OR THEY ARE FURNISHED BY THE CONTRACTOR. ACCESS TO ENTRANCES AND SIDEROADS SHALL BE MAINTAINED AT ALL TIMES.

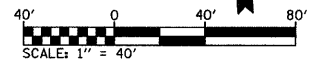
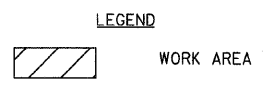
STAGE I SHALL BE CONSTRUCTED UNDER TRAFFIC IN ACCORDANCE WITH STD. 701326

STAGE II SHALL BE CONSTRUCTED UNDER TRAFFIC IN ACCORDANCE WITH STD. 701701.

STAGE III SHALL BE CONSTRUCTED UNDER TRAFFIC IN ACCORDANCE WITH STD. 701306

STAGE IV SHALL BE CONSTRUCTED UNDER TRAFFIC IN ACCORDANCE WITH STD. 701326

THE CONTRACTOR SHALL MAINTAIN DRAINAGE THROUGHOUT THE STAGE CONSTRUCTION.



FILE NAME = H:\P\25004\Technical Production\Civil\W0	USER NAME = #USER# \\Microstation\stg004.dgn	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED STAGING STAGE IV HIGHWAY CONSTRUCTION			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
	PLOT SCALE = 40.0000' / IN.	DRAWN -	REVISED -		SCALE: 1" = 40'	SHEET NO. 4	OF 4 SHEETS	STA.	TO STA.	9111	73-15TS	MADISON	64	20
	PLOT DATE = 3/14/2008	CHECKED -	REVISED -							CONTRACT NO. 76B22				
		DATE -	REVISED -							ILLINOIS FED. AID PROJECT				

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY ON MAY 30, 2003 FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES. THIS PLAN HAS ALSO BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF NPDES PERMIT NUMBER ILR40 FOR DISCHARGES FROM SMALL MUNICIPAL SEPARATE STORM SEWER SYSTEMS IF CHECKED BELOW.

NPDES PERMITS ASSOCIATED WITH THIS PROJECT:

- ILR10
 ILR40 PERMIT NO. 0493

I CERTIFY UNDER PENALTY OF LAW THAT THIS DOCUMENT AND ALL ATTACHMENTS WERE PREPARED UNDER MY DIRECTION OR SUPERVISION IN ACCORDANCE WITH A SYSTEM DESIGNED TO ASSURE THAT QUALIFIED PERSONNEL PROPERLY GATHERED AND EVALUATED THE INFORMATION SUBMITTED. BASED ON MY INQUIRY OF THE PERSON OR PERSONS WHO MANAGE THE SYSTEM, OR THOSE PERSONS DIRECTLY RESPONSIBLE FOR GATHERING THE INFORMATION, THE INFORMATION SUBMITTED IS, TO THE BEST OF MY KNOWLEDGE AND BELIEF, TRUE, ACCURATE AND COMPLETE. I AM AWARE THAT THERE ARE SIGNIFICANT PENALTIES FOR SUBMITTING FALSE INFORMATION, INCLUDING THE POSSIBILITY OF FINE AND IMPRISONMENT FOR KNOWING VIOLATIONS.

 MARY C. LAMIE
 PRINT NAME
 DEPUTY DIRECTOR OF HIGHWAYS
 REGION FIVE ENGINEER
 TITLE
 IL DEPT. OF TRANSPORTATION
 AGENCY

 Mary C. Lamie
 SIGNATURE
 3-20-08
 DATE

I. SITE DESCRIPTION:

A. THE FOLLOWING IS A DESCRIPTION OF THE PROJECT LOCATION:

THE PROJECT CONSISTS OF THE PROPOSED IMPROVEMENTS OF 0.20 MILES OF FAU ROUTE 9111 (HORSESHOE LAKE ROAD), WITH THE INTERSECTION OF EASTPORT PLAZA DRIVE AND FOURNIE LANE.

B. THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

CONSTRUCTION WILL INCLUDE THE WIDENING AND RESURFACING OF FAU ROUTE 9111 (HORSESHOE LAKE ROAD), STORM SEWER AND DRAINAGE STRUCTURES, ASPHALT SHOULDERS, COMBINATION CONCRETE CURB AND GUTTER, TRAFFIC SIGNALS, PAVEMENT MARKING, AND ALL INCIDENTAL AND COLLATERAL WORK NECESSARY TO COMPLETE THE PROJECT AS SHOWN ON THE PLANS.

C. THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

DESCRIPTION OF INTENDED SEQUENCE FOR MAJOR CONSTRUCTION ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE:

STAGE 1: EXTEND THE BOX CULVERT AT STATION 239+65.84 , CONSTRUCT THE CULVERT UNDER FOURNIE LANE AND EXTEND THE PIPE CULVERT UNDER EASTPORT PLAZA DRIVE. CONSTRUCT THE PAVEMENT WIDENING ON THE NORTH AND THE SOUTH SIDE OF HORSESHOE LAKE ROAD. CONSTRUCT FOURNIE LANE AND EASTPORT PLAZA DRIVE

STAGE 2: REMOVE THE MEDIAN AND THE PAVEMENT AS REQUIRED. CONSTRUCT THE INLETS AND THE STORM SEWER. CONSTRUCT THE NEW MEDIAN AND THE PROPOSED BASE COURSE IN THE MEDIAN AREA.

STAGE 3: CONSTRUCT THE LEVELING BINDER AND THE BINDER COURSE. CONSTRUCT THE SURFACE COURSE IN CONJUNCTION WITH HORSESHOE LAKE ROAD SURFACING. (COORDINATION WITH ADJACENT CONTRACT.

STAGE 4: CONSTRUCT PROPOSED SHOULDERS TO MATCH ELEVATION OF SURFACE COURSE.

STAGE 5: CONSTRUCT THE PROPOSED TRAFFIC SIGNALS AND THE PERMANENT PAVEMENT MARKING.

D. THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 3.92 ACRES.

THE TOTAL AREA OF THE SITE THAT IS ESTIMATED WILL BE DISTURBED BY EXCAVATION, GRADING OR OTHER ACTIVITIES IS 2.49 ACRES.

E. THE FOLLOWING IS A WEIGHTED AVERAGE OF THE RUNOFF COEFFICIENT FOR THIS PROJECT AFTER CONSTRUCTION ACTIVITIES ARE COMPLETED: 0.70

F. THE FOLLOWING IS A DESCRIPTION OF THE SOIL TYPES FOUND AT THE PROJECT SITE FOLLOWED BY INFORMATION REGARDING THEIR EROSION:

THREE SOIL TYPES ARE LOCATED WITHIN THE PROJECT AREA OF THE HORSESHOE LAKE, EASTPORT PLAZA DRIVE INTERSECTION, THESE ARE:

ORION SILT LOAM (415) - DEEP, SOMEWHAT POORLY DRAINED, MODERATELY PERMEABLE SOIL ON BOTTOM LAND ALONG THE MAJOR STREAMS AND TRIBUTARIES. THESE SOILS FORMED IN SILTY ALLUVIUM. SLOPES RANGE FROM 0 TO 2 PERCENT. ERODES EASILY.

HAYMOND SILT LOAM (331) - WELL DRAINED, NEARLY LEVEL SOIL IS ON FLOOD PLAINS ALONG THE MAJOR RIVERS AND SMALL STREAMS. IT IS FREQUENTLY FLOODED FOR BRIEF PERIODS FROM MARCH THROUGH MAY. INDIVIDUAL AREAS ARE IRREGULAR IN SHAPE AND ARE 10 TO 200 ACRES IN SIZE. ERODES EASILY.

TICE SILT LOAM (3284) - DEEP, SOMEWHAT POORLY DRAINED, MODERATELY PERMEABLE SOILS ON BOTTOM LANDS. THESE SOILS FORMED IN SILTY ALLUVIUM. SLOPES RANGE FROM 0 TO 2 PERCENT. ERODES EASILY.

G. THERE ARE NO POTENTIALLY EROSION AREAS ASSOCIATED WITH THIS PROJECT:

H. THE FOLLOWING IS A DESCRIPTION OF SOIL DISTURBING ACTIVITIES, THEIR LOCATIONS, AND THEIR EROSION FACTORS (E.G. STEEPNESS OF SLOPES, LENGTH OF SLOPES, ETC):

THE NATURE AND PURPOSE OF LAND DISTURBING ACTIVITIES ON THIS PROJECT IS TO IMPROVE THE INTERSECTION OF FAU ROUTE 9111 (HORSESHOE LAKE ROAD) AND EASTPORT PLAZA DRIVE/FOURNIE LANE. ALL WORK IS EXPECTED TO BE CONTAINED WITHIN THE EXISTING RIGHT OF WAY. ALL SOILS TO BE DISTURBED HAVE EROSION CHARACTERISTICS.

I. SEE THE EROSION CONTROL PLANS AND/OR DRAINAGE PLANS FOR THIS CONTRACT FOR INFORMATION REGARDING DRAINAGE PATTERNS, APPROXIMATE SLOPES ANTICIPATED BEFORE AND AFTER MAJOR GRADING ACTIVITIES, LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE AND CONTROLS TO PREVENT OFF SITE SEDIMENT TRACKING (TO BE ADDED AFTER CONTRACTOR IDENTIFIES LOCATIONS), AREAS OF SOIL DISTURBANCE, THE LOCATION OF MAJOR STRUCTURAL AND NON-STRUCTURAL CONTROLS IDENTIFIED IN THE PLAN, THE LOCATION OF AREAS WHERE STABILIZATION PRACTICES ARE EXPECTED TO OCCUR, SURFACE WATERS (INCLUDING WETLANDS) AND LOCATIONS WHERE STORM WATER IS DISCHARGED TO SURFACE WATER INCLUDING WETLANDS.

J. THE FOLLOWING IS A LIST OF RECEIVING WATER(S) AND THE ULTIMATE RECEIVING WATER(S), AND AERIAL EXTENT OF WETLAND ACREAGE AT THE SITE. THE LOCATION OF THE RECEIVING WATERS CAN BE FOUND ON THE EROSION AND SEDIMENT CONTROL PLANS: CAHOKIA CREEK

K. THE FOLLOWING POLLUTANTS OF CONCERN WILL BE ASSOCIATED WITH THIS CONSTRUCTION PROJECT: (CHECK ALL THAT APPLY)

- | | |
|---|--|
| <input checked="" type="checkbox"/> SOIL SEDIMENT | <input checked="" type="checkbox"/> PETROLEUM (GAS, DIESEL, OIL, KEROSENE, HYDRAULIC OIL/FLUIDS) |
| <input checked="" type="checkbox"/> CONCRETE | <input checked="" type="checkbox"/> ANTIFREEZE / COOLANTS |
| <input checked="" type="checkbox"/> CONCRETE TRUCK WASTE | <input checked="" type="checkbox"/> WASTE WATER FROM CLEANING CONSTRUCTION EQUIPMENT |
| <input checked="" type="checkbox"/> CONCRETE CURING COMPOUNDS | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> SOLID WASTE DEBRIS | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> PAINTS | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> SOLVENTS | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input checked="" type="checkbox"/> FERTILIZERS / PESTICIDES | <input type="checkbox"/> OTHER (SPECIFY)..... |

CONTROLS

II. THIS SECTION OF THE PLAN ADDRESSES THE CONTROLS THAT WILL BE IMPLEMENTED FOR EACH OF THE MAJOR CONSTRUCTION ACTIVITIES DESCRIBED IN I.C. ABOVE AND FOR ALL USE AREAS, BORROW SITES, AND WASTE SITES. FOR EACH MEASURE DISCUSSED, THE CONTRACTOR WILL BE RESPONSIBLE FOR ITS IMPLEMENTATION AS INDICATED. THE CONTRACTOR SHALL PROVIDE TO THE RESIDENT ENGINEER A PLAN FOR THE IMPLEMENTATION OF THE MEASURES INDICATED. THE CONTRACTOR, AND SUBCONTRACTORS, WILL NOTIFY THE RESIDENT ENGINEER OF ANY PROPOSED CHANGES, MAINTENANCE, OR MODIFICATIONS TO KEEP CONSTRUCTION ACTIVITIES COMPLIANT WITH THE PERMIT. EACH SUCH CONTRACTOR HAS SIGNED THE REQUIRED CERTIFICATION ON FORMS WHICH WILL BE PROVIDED AT THE PRE-CONSTRUCTION CONFERENCE, AND ARE A PART OF, THIS PLAN:

A. EROSION AND SEDIMENT CONTROL

1. STABILIZED PRACTICES: PROVIDED BELOW IS A DESCRIPTION OF INTERIM AND PERMANENT STABILIZATION PRACTICES, INCLUDING SITE SPECIFIC SCHEDULING OF THE IMPLEMENTATION OF THE PRACTICES. SITE PLANS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES MAY INCLUDE BUT ARE NOT LIMITED TO: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, GEOTEXTILES, SODDING, VEGETATIVE BUFFER STRIPS, PROTECTION OF TREES, PRESERVATION OF MATURE VEGETATION, AND OTHER APPROPRIATE MEASURES. EXCEPT AS PROVIDED BELOW IN II(A)(1)(c) AND II(A)(3), STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 14 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED ON ALL DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION WILL NOT OCCUR FOR A PERIOD OF 21 OR MORE CALENDAR DAYS.

a. WHERE THE INITIATION OF STABILIZATION MEASURES BY THE 14TH DAY AFTER CONSTRUCTION ACTIVITY TEMPORARILY OR PERMANENTLY CEASES IS PRECLUDED BY SNOW COVER, STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE THEREAFTER.

THE FOLLOWING STABILIZATION PRACTICES WILL BE USED FOR THIS PROJECT: (CHECK ALL THAT APPLY)

- | | |
|---|--|
| <input checked="" type="checkbox"/> PRESERVATION OF MATURE VEGETATION | <input checked="" type="checkbox"/> EROSION CONTROL BLANKET / MULCHING |
| <input type="checkbox"/> VEGETATED BUFFER STRIPS | <input type="checkbox"/> SODDING |
| <input type="checkbox"/> PROTECTION OF TREES | <input type="checkbox"/> GEOTEXTILES |
| <input checked="" type="checkbox"/> TEMPORARY EROSION CONTROL SEEDING | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> TEMPORARY TURF (SEEDING, CLASS 7) | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input checked="" type="checkbox"/> TEMPORARY MULCHING | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input checked="" type="checkbox"/> PERMANENT SEEDING | <input type="checkbox"/> OTHER (SPECIFY)..... |

DESCRIBE HOW THE STABILIZATION PRACTICES LISTED ABOVE WILL BE UTILIZED:

1. TEMPORARY EROSION CONTROL SEEDING - THIS ITEM WILL BE APPLIED TO ALL BARE AREAS EVERY SEVEN DAYS TO MINIMIZE THE AMOUNT OF EXPOSED SURFACE AREAS.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED IF THEY ARE TO REMAIN UNUSED FOR MORE THAN 14 DAYS.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

BARE AND SPARSELY VEGETATED GROUND IN HIGHLY ERODIBLE AREAS AS DETERMINED BY THE ENGINEER SHALL BE TEMPORARILY SEEDED AT THE BEGINNING OF CONSTRUCTION WHERE NO CONSTRUCTION ACTIVITIES ARE EXPECTED WITHIN 7 DAYS.

2. PERMANENT SEEDING - SEEDING, CLASS 2A WILL BE INSTALLED PER IDOT SPECIFICATIONS.

3. EROSION CONTROL BLANKET/MULCHING - EROSION CONTROL BLANKET WILL BE INSTALLED OVER THE 1:2 SLOPES THAT HAVE BEEN BROUGHT TO FINAL GRADE AND SEEDED TO PROTECT SLOPES FROM EROSION AND ALLOW SEEDS TO GERMINATE. MULCH, METHOD 2 WILL BE APPLIED IN RELATIVELY FLAT AREAS TO PROTECT THE DISTURBED AREAS AND PREVENT FURTHER EROSION.

MULCH AS APPLIED TO TEMPORARY EROSION CONTROL SEEDING SHALL BE BY THE METHOD SPECIFIED IN THE CONTRACT AND AT THE DIRECTION OF THE ENGINEER. MULCH WILL BE PAID SEPARATELY AND SHALL CONFORM TO SECTION 251 OF THE STANDARD SPECIFICATIONS.

PERMANENT STABILIZATION - ALL AREAS DISTURBED BY CONSTRUCTION WILL BE STABILIZED WITH PERMANENT SEEDING IMMEDIATELY FOLLOWING THE FINISHED GRADING. EROSION CONTROL BLANKETS WILL BE INSTALLED OVER FILL SLOPES WHICH HAVE BEEN BROUGHT TO FINAL GRADE AND HAVE BEEN SEEDED TO PROTECT THE SLOPES FROM RILL AND GULLY EROSION AND ALLOW SEED TO GERMINATE PROPERLY. MULCH, METHOD 2 WILL BE USED ON RELATIVELY FLAT AREAS.

2. STRUCTURAL PRACTICES: PROVIDED BELOW IS A DESCRIPTION OF STRUCTURAL PRACTICES THAT WILL BE IMPLEMENTED, TO THE DEGREE ATTAINABLE, TO DIVERT FLOWS FROM EXPOSED SOILS, STORE FLOWS OR OTHERWISE LIMIT RUNOFF AND THE DISCHARGE OF POLLUTANTS FROM EXPOSED AREAS OF THE SITE. SUCH PRACTICES MAY INCLUDE BUT ARE NOT LIMITED TO: PERIMETER EROSION BARRIER, EARTH DIKES, DRAINAGE SWALES, SEDIMENT TRAPS, DITCH CHECKS, SUBSURFACE DRAINS, PIPE SLOPE DRAINS, LEVEL SPREADERS, STORM DRAIN INLET PROTECTION, ROCK OUTLET PROTECTION, REINFORCED SOIL RETAINING SYSTEMS, GABIONS, AND TEMPORARY OR PERMANENT SEDIMENT BASINS. THE INSTALLATION OF THESE DEVICES MAY BE SUBJECT TO SECTION 404 OF THE CLEAN WATER ACT.

THE FOLLOWING STRUCTURAL PRACTICES WILL BE USED FOR THIS PROJECT:(CHECK ALL THAT APPLY)

- | | |
|--|--|
| <input checked="" type="checkbox"/> PERIMETER EROSION BARRIER | <input type="checkbox"/> ROCK OUTLET PROTECTION |
| <input checked="" type="checkbox"/> TEMPORARY DITCH CHECK | <input checked="" type="checkbox"/> RIPRAP |
| <input checked="" type="checkbox"/> STORM DRAIN INLET PROTECTION | <input type="checkbox"/> GABIONS |
| <input type="checkbox"/> SEDIMENT TRAP | <input type="checkbox"/> SLOPE MATTRESS |
| <input type="checkbox"/> TEMPORARY PIPE SLOPE DRAIN | <input type="checkbox"/> RETAINING WALLS |
| <input type="checkbox"/> TEMPORARY SEDIMENT BASIN | <input type="checkbox"/> SLOPE WALLS |
| <input type="checkbox"/> TEMPORARY STREAM CROSSING | <input type="checkbox"/> CONCRETE REVETMENT MATS |
| <input type="checkbox"/> STABILIZED CONSTRUCTION EXITS | <input type="checkbox"/> LEVEL SPREADERS |
| <input type="checkbox"/> TURF REINFORCEMENT MATS | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> PERMANENT CHECK DAMS | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> PERMANENT SEDIMENT BASIN | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> AGGREGATE DITCH | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> PAVED DITCH | <input type="checkbox"/> OTHER (SPECIFY)..... |

DESCRIBE HOW THE STRUCTURAL PRACTICES LISTED ABOVE WILL BE UTILIZED:

1. PERIMETER EROSION BARRIER - SILT FENCES WILL BE PLACED ALONG THE LIMITS OF CONSTRUCTION IN AN EFFORT TO CONTAIN SILT AND RUNOFF FROM LEAVING THE SITE.

CONSTRUCT AT BEGINNING OF CONSTRUCTION. REMOVE AT END OF CONSTRUCTION.

2. STORM DRAIN INLET PROTECTION - INLET AND PIPE PROTECTION WILL BE PROVIDED FOR STORM SEWERS AND CULVERTS. SEDIMENT FILTERS WILL BE PLACED IN ALL INLETS, CATCH BASINS AND MANHOLES DURING CONSTRUCTION AND WILL BE CLEANED ON A REGULAR BASIS.

3. TEMPORARY DITCH CHECKS - DITCH CHECKS WILL BE PLACED IN SWALES WHERE RUNOFF VELOCITY IS HIGH. ALL STRUCTURAL PRACTICES ARE SHOWN IN DETAIL ON THE EROSION CONTROL PLANS.

TEMPORARY DITCH CHECKS SHALL BE LOCATED AT EVERY 1 FT. FALL/RISE IN DITCH GRADE.

TEMPORARY DITCH CHECKS, AGGREGATE USES GRADING NO. 3- REMOVE AT END OF CONSTRUCTION.

STRAW BALES, HAY BALES, PERIMETER EROSION BARRIER AND SILT FENCE WILL NOT BE PERMITTED FOR TEMPORARY OR PERMANENT DITCH CHECKS. DITCH CHECKS SHALL BE COMPOSED OF AGGREGATE (IF SPECIFIED), ENVIROBERM, TRIANGULAR SILT DIKES, GEORIDGE AND ROLLED EXCELSIOR.

4. RIPRAP - STONE RIPRAP WITH FILTER FABRIC WILL BE USED AS PROTECTION AT THE DISCHARGE END OF ALL CULVERT END SECTIONS AND AS INLET/OUTLET PROTECTION TO PREVENT SCOURING AT THE END OF PIPES AND PREVENT DOWNSTREAM EROSION.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, TEMPORARY DITCH CHECKS, INLET AND PIPE PROTECTION, AND PERIMETER EROSION BARRIER SHALL BE INSTALLED AS CALLED OUT IN THIS PLAN AND DIRECTED BY THE ENGINEER.

ALL EROSION CONTROL PRODUCTS FURNISHED SHALL BE SPECIFICALLY RECOMMENDED BY THE MANUFACTURER FOR THE USE SPECIFIED IN THE EROSION CONTROL PLAN. PRIOR TO THE APPROVAL AND USE OF THE PRODUCT, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A NOTARIZED CERTIFICATION BY THE PRODUCER STATING THE INTENDED USE OF THE PRODUCT AND THAT THE PHYSICAL PROPERTIES REQUIRED FOR THIS APPLICATION ARE MET OR EXCEEDED. THE CONTRACTOR SHALL PROVIDE MANUFACTURER INSTALLATION PROCEDURES TO FACILITATE THE ENGINEER IN CONSTRUCTION INSPECTION.

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				HORSESHOE LAKE ROAD STORM WATER POLLUTION PREVENTION PLAN SHEETS				F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
H:\P\25004\Technical Production\Civil\W0	7\Microstation\SWPPOA2.dgn	DRAWN -	REVISED -									9111	73-15TS	MADISON	64	21
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -	SCALE:				SHEET NO. 1 OF 5 SHEETS STA. TO STA.				FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				
	PLOT DATE = 3/14/2008	DATE -	REVISED -									CONTRACT NO. 76B22				

3. STORM WATER MANAGEMENT: PROVIDED BELOW IS A DESCRIPTION OF MEASURES THAT WILL BE INSTALLED DURING THE CONSTRUCTION PROCESS TO CONTROL POLLUTANTS IN STORM WATER DISCHARGES THAT WILL OCCUR AFTER CONSTRUCTION OPERATIONS HAVE BEEN COMPLETED. THE INSTALLATION OF THESE DEVICES MAY BE SUBJECT TO SECTION 404 OF THE CLEAN WATER ACT.

- a. SUCH PRACTICES MAY INCLUDE BUT ARE NOT LIMITED TO: STORM WATER DETENTION STRUCTURES (INCLUDING WET PONDS), STORM WATER RETENTION STRUCTURES, FLOW ATTENUATION BY USE OF OPEN VEGETATED SWALES AND NATURAL DEPRESSIONS, INFILTRATION OF RUNOFF ON SITE, AND SEQUENTIAL SYSTEMS (WHICH COMBINE SEVERAL PRACTICES). THE PRACTICES SELECTED FOR IMPLEMENTATION WERE DETERMINED ON THE BASIS OF THE TECHNICAL GUIDANCE IN SECTION 59-8 (EROSION AND SEDIMENT CONTROL) IN CHAPTER 59 (LANDSCAPE DESIGN AND EROSION CONTROL) OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION BUREAU OF DESIGN AND ENVIRONMENT MANUAL. IF PRACTICES OTHER THAN THOSE DISCUSSED IN SECTION 59-8 ARE SELECTED FOR IMPLEMENTATION OR IF PRACTICES ARE APPLIED TO SITUATIONS DIFFERENT FROM THOSE COVERED IN SECTION 59-8, THE TECHNICAL BASIS FOR SUCH DECISIONS WILL BE EXPLAINED BELOW.
- b. VELOCITY DISSIPATION DEVICES WILL BE PLACED AT DISCHARGE LOCATIONS AND ALONG THE LENGTH OF ANY OUTFALL CHANNEL AS NECESSARY TO PROVIDE A NON-EROSIVE VELOCITY FLOW FROM THE STRUCTURE TO A WATER COURSE SO THAT THE NATURAL PHYSICAL AND BIOLOGICAL CHARACTERISTICS AND FUNCTIONS ARE MAINTAINED AND PROTECTED (E.G. MAINTENANCE OF HYDROLOGIC CONDITIONS SUCH AS THE HYDROPERIOD AND HYDRODYNAMICS PRESENT PRIOR TO THE INITIATION OF CONSTRUCTION ACTIVITIES).

DESCRIPTION OF STORM WATER MANAGEMENT CONTROLS:

THE PHASE I LOCATION DRAINAGE STUDY HAS DETERMINED THAT NO STORM WATER DETENTION IS REQUIRED FOR THE PROPOSED STORM SEWER OUTLETS TO BE CONSTRUCTED FOR THIS PROJECT.

4. OTHER CONTROLS:

- a. VEHICLE ENTRANCES AND EXITS - STABILIZED CONSTRUCTION ENTRANCES AND EXITS MUST BE CONSTRUCTED TO PREVENT TRACKING OF SEDIMENTS ONTO ROADWAYS.

THE CONTRACTOR WILL PROVIDE THE RESIDENT ENGINEER WITH A WRITTEN PLAN IDENTIFYING THE LOCATION OF STABILIZED ENTRANCES AND EXITS AND THE PROCEDURES (SHE WILL USE TO CONSTRUCT AND MAINTAIN THEM.

- b. MATERIAL DELIVERY, STORAGE, AND USE - THE FOLLOWING BMPs SHALL BE IMPLEMENTED TO HELP PREVENT DISCHARGES OF CONSTRUCTION MATERIALS DURING DELIVERY, STORAGE, AND USE:

- ALL PRODUCTS DELIVERED TO THE PROJECT SITE MUST BE PROPERLY LABELED.
- WATER TIGHT SHIPPING CONTAINERS AND/OR SEMI TRAILERS SHALL BE USED TO STORE HAND TOOLS, SMALL PARTS, AND MOST CONSTRUCTION MATERIALS THAT CAN BE CARRIED BY HAND, SUCH AS PAINT CANS, SOLVENTS, AND GREASE.
- A STORAGE/CONTAINMENT FACILITY SHOULD BE CHOSEN FOR LARGER ITEMS SUCH AS DRUMS AND ITEMS SHIPPED OR STORED ON PALLETS. SUCH MATERIAL IS TO BE COVERED BY A TIN ROOF OR LARGE SHEETS OF PLASTIC TO PREVENT PRECIPITATION FROM COMING IN CONTACT WITH THE PRODUCTS BEING STORED.
- LARGE ITEMS SUCH AS LIGHT STANDS, FRAMING MATERIALS AND LUMBER SHALL BE STORED IN THE OPEN IN A GENERAL STORAGE AREA. SUCH MATERIAL SHALL BE ELEVATED WITH WOOD BLOCKS TO MINIMIZE CONTACT WITH STORM WATER RUNOFF.
- SPILL CLEAN-UP MATERIALS, MATERIAL SAFETY DATA SHEETS, AN INVENTORY OF MATERIALS, AND EMERGENCY CONTACT NUMBERS SHALL BE MAINTAINED AND STORED IN ONE DESIGNATED AREA AND EACH CONTRACTOR IS TO INFORM HIS/HER EMPLOYEES AND THE RESIDENT ENGINEER OF THIS LOCATION.

- c. STOCKPILE MANAGEMENT - BMPs SHALL BE IMPLEMENTED TO REDUCE OR ELIMINATE POLLUTION OF STORM WATER FROM STOCKPILES OF SOIL AND PAVING MATERIALS SUCH AS BUT NOT LIMITED TO PORTLAND CEMENT CONCRETE RUBBLE, ASPHALT CONCRETE, ASPHALT CONCRETE RUBBLE, AGGREGATE BASE, AGGREGATE SUB BASE, AND PRE-MIXED AGGREGATE. THE FOLLOWING BMPs MAY BE CONSIDERED:

- PERIMETER EROSION BARRIER
- TEMPORARY SEEDING
- TEMPORARY MULCH
- PLASTIC COVERS
- SOIL BINDERS
- STORM DRAIN INLET PROTECTION

THE CONTRACTOR WILL PROVIDE THE RESIDENT ENGINEER WITH A WRITTEN PLAN OF THE PROCEDURES (SHE WILL USE ON THE PROJECT AND HOW THEY WILL BE MAINTAINED.

- d. WASTE DISPOSAL. NO MATERIALS, INCLUDING BUILDING MATERIALS, SHALL BE DISCHARGED INTO WATERS OF THE STATE, EXCEPT AS AUTHORIZED BY A SECTION 404 PERMIT.
- e. THE PROVISIONS OF THIS PLAN SHALL ENSURE AND DEMONSTRATE COMPLIANCE WITH APPLICABLE STATE AND/OR LOCAL WASTE DISPOSAL, SANITARY SEWER OR SEPTIC SYSTEM REGULATIONS.
- f. THE CONTRACTOR SHALL PROVIDE A WRITTEN AND GRAPHIC PLAN TO THE RESIDENT ENGINEER IDENTIFYING WHERE EACH OF THE ABOVE AREAS WILL BE LOCATED AND HOW THEY ARE TO BE MANAGED.

5. APPROVED STATE OR LOCAL LAWS

THE MANAGEMENT PRACTICES, CONTROLS AND PROVISIONS CONTAINED IN THIS PLAN WILL BE IN ACCORDANCE WITH IDOT SPECIFICATIONS, WHICH ARE AT LEAST AS PROTECTIVE AS THE REQUIREMENTS CONTAINED IN THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY'S ILLINOIS URBAN MANUAL, 1995. PROCEDURES AND REQUIREMENTS SPECIFIED IN APPLICABLE SEDIMENT AND EROSION SITE PLANS OR STORM WATER MANAGEMENT PLANS APPROVED BY LOCAL OFFICIALS SHALL BE DESCRIBED OR INCORPORATED BY REFERENCE IN THE SPACE PROVIDED BELOW. REQUIREMENTS SPECIFIED IN SEDIMENT AND EROSION SITE PLANS, SITE PERMITS, STORM WATER MANAGEMENT SITE PLANS OR SITE PERMITS APPROVED BY LOCAL OFFICIALS THAT ARE APPLICABLE TO PROTECTING SURFACE WATER RESOURCES ARE, UPON SUBMITTAL OF AN NOI, TO BE AUTHORIZED TO DISCHARGE UNDER PERMIT ILR10 INCORPORATED BY REFERENCE AND ARE ENFORCEABLE UNDER THIS PERMIT EVEN IF THEY ARE NOT SPECIFICALLY INCLUDED IN THE PLAN.

DESCRIPTION OF PROCEDURES AND REQUIREMENTS SPECIFIED IN APPLICABLE SEDIMENT AND EROSION SITE PLANS OR STORM WATER MANAGEMENT PLANS APPROVED BY LOCAL OFFICIALS:

ALL MANAGEMENT PRACTICES, CONTROLS, AND OTHER PROVISIONS PROVIDED IN THIS PLAN ARE IN ACCORDANCE WITH "IDOT STANDARD SPECIFICATION FOR ROAD AND BRIDGE CONSTRUCTION AND THE ILLINOIS URBAN MANUAL".

III. MAINTENANCE:

THE FOLLOWING IS A DESCRIPTION OF PROCEDURES THAT WILL BE USED TO MAINTAIN, IN GOOD AND EFFECTIVE OPERATING CONDITIONS, THE VEGETATION, EROSION AND SEDIMENT CONTROL MEASURES AND OTHER PROTECTIVE MEASURES IDENTIFIED IN THIS PLAN.

1. SEEDING - ALL ERODIBLE BARE EARTH WILL BE TEMPORARILY SEEDDED ON A WEEKLY BASIS TO MINIMIZE THE AMOUNT OF ERODIBLE SURFACE WITHIN THE CONTRACT LIMITS.
2. PERIMETER EROSION BARRIER - SEDIMENT WILL BE REMOVED IF THE INTEGRITY OF THE FENCING IS IN JEOPARDY AND ANY FENCING KNOCKED DOWN WILL BE REPAIRED IMMEDIATELY.
3. EROSION CONTROL BLANKET/MULCHING - ANY AREAS THAT FAIL WILL BE REPAIRED IMMEDIATELY.
4. DITCH CHECKS - SEDIMENT WILL BE REMOVED IF THE INTEGRITY OF THE DITCH CHECK IS IN JEOPARDY. ANY DITCH CHECKS WHICH FAIL WILL BE REPAIRED OR REPLACED IMMEDIATELY.

THE RESIDENT ENGINEER WILL PROVIDE MAINTENANCE GUIDES TO THE CONTRACTOR FOR THESE PRACTICES. ALL MAINTENANCE OF EROSION CONTROL SYSTEMS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR UNTIL CONSTRUCTION IS COMPLETE AND ACCEPTED BY IDOT AFTER FINAL INSPECTION. ALL LOCATIONS WHERE VEHICLES ENTER AND EXIT THE CONSTRUCTION SITE AND ALL OTHER AREAS SUBJECT TO EROSION SHOULD ALSO BE INSPECTED PERIODICALLY.

INSPECTION OF THESE AREAS SHALL BE MADE AT LEAST ONCE EVERY SEVEN DAYS AND WITHIN 24 HOURS OF THE END OF EACH 0.5 INCHES OR GREATER RAINFALL, OR AN EQUIVALENT SNOWFALL. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE CONSTRUCTION FIELD ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT EROSION CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR THE TEMPORARY EROSION CONTROL SYSTEM.

IV. INSPECTIONS

QUALIFIED PERSONNEL SHALL INSPECT DISTURBED AREAS OF THE CONSTRUCTION SITE WHICH HAVE NOT YET BEEN FINALLY STABILIZED, STRUCTURAL CONTROL MEASURES, AND LOCATIONS WHERE VEHICLES AND EQUIPMENT ENTER AND EXIT THE SITE. SUCH INSPECTIONS SHALL BE CONDUCTED AT LEAST ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM THAT IS 0.5 INCHES OR GREATER OR EQUIVALENT SNOWFALL.

- A. DISTURBED AREAS, USE AREAS (STORAGE OF MATERIALS, STOCKPILES, MACHINE MAINTENANCE FUELING, ETC.), BORROW SITES, AND WASTE SITES SHALL BE INSPECTED FOR EVIDENCE OF, OR THE POTENTIAL FOR, POLLUTANTS ENTERING THE DRAINAGE SYSTEM. EROSION AND SEDIMENT CONTROL MEASURES IDENTIFIED IN THE PLAN SHALL BE OBSERVED TO ENSURE THAT THEY ARE OPERATING CORRECTLY. DISCHARGE LOCATIONS OR POINTS THAT ARE ACCESSIBLE, SHALL BE INSPECTED TO ASCERTAIN WHETHER EROSION CONTROL MEASURES ARE EFFECTIVE IN PREVENTING SIGNIFICANT IMPACTS TO RECEIVING WATERS. LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFF SITE SEDIMENT TRACKING.
- B. BASED ON THE RESULTS OF THE INSPECTION, THE DESCRIPTION OF POTENTIAL POLLUTANT SOURCES IDENTIFIED IN SECTION I ABOVE AND POLLUTION PREVENTION MEASURES IDENTIFIED IN SECTION II ABOVE SHALL BE REVISED AS APPROPRIATE AS SOON AS PRACTICABLE AFTER SUCH INSPECTION. ANY CHANGES TO THIS PLAN RESULTING FROM THE REQUIRED INSPECTIONS SHALL BE IMPLEMENTED WITHIN 1/2 HOUR TO 1 WEEK BASED ON THE URGENCY OF THE SITUATION. THE RESIDENT ENGINEER WILL NOTIFY THE CONTRACTOR OF THE TIME REQUIRED TO IMPLEMENT SUCH ACTIONS THROUGH THE WEEKLY INSPECTION REPORT.
- C. A REPORT SUMMARIZING THE SCOPE OF THE INSPECTION, NAME(S) AND QUALIFICATIONS OF PERSONNEL MAKING THE INSPECTION, THE DATE(S) OF THE INSPECTION, MAJOR OBSERVATIONS RELATING TO THE IMPLEMENTATION OF THIS STORM WATER POLLUTION PREVENTION PLAN, AND ACTIONS TAKEN IN ACCORDANCE WITH SECTION IV(B) SHALL BE MADE AND RETAINED AS PART OF THE PLAN FOR AT LEAST THREE (3) YEARS AFTER THE DATE OF THE INSPECTION. THE REPORT SHALL BE SIGNED IN ACCORDANCE WITH PART VI. G OF THE GENERAL PERMIT.
- D. IF ANY VIOLATION OF THE PROVISIONS OF THIS PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION WORK COVERED BY THIS PLAN, THE RESIDENT ENGINEER SHALL COMPLETE AND FILE AN "INCIDENCE OF NONCOMPLIANCE" (ION) REPORT FOR THE IDENTIFIED VIOLATION. THE RESIDENT ENGINEER SHALL USE FORMS PROVIDED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY AND SHALL INCLUDE SPECIFIC INFORMATION ON THE CAUSE OF NONCOMPLIANCE, ACTIONS WHICH WERE TAKEN TO PREVENT ANY FURTHER CAUSES OF NONCOMPLIANCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NONCOMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G OF THE GENERAL PERMIT. THE INCIDENCE OF NONCOMPLIANCE SHALL BE MAILED TO THE FOLLOWING ADDRESS:

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
DIVISION OF WATER POLLUTION CONTROL
ATTN: COMPLIANCE ASSURANCE CONTROL
1021 NORTH GRAND EAST
POST OFFICE BOX 19276
SPRINGFIELD, ILLINOIS 62794-9276

V. NON-STORM WATER DISCHARGES:

EXCEPT FOR FLOWS FROM FIRE FIGHTING ACTIVITIES, SOURCES OF NON-STORM WATER THAT IS COMBINED WITH STORM WATER DISCHARGES ASSOCIATED WITH THE INDUSTRIAL ACTIVITY ADDRESSED IN THIS PLAN MUST BE DESCRIBED BELOW. APPROPRIATE POLLUTION PREVENTION MEASURES, AS DESCRIBED BELOW, WILL BE IMPLEMENTED FOR THE NON-STORM WATER COMPONENT(S) OF THE DISCHARGE.

- A. SPILL PREVENTION AND CONTROL - BMPs SHALL BE IMPLEMENTED TO CONTAIN AND CLEAN-UP SPILLS AND PREVENT MATERIAL DISCHARGES TO THE STORM DRAIN SYSTEM. THE CONTRACTOR SHALL PRODUCE A WRITTEN PLAN STATING HOW HIS/HER COMPANY WILL PREVENT, REPORT, AND CLEAN UP SPILLS AND PROVIDE A COPY TO ALL OF HIS/HER EMPLOYEES AND THE RESIDENT ENGINEER. THE CONTRACTOR SHALL NOTIFY ALL OF HIS/HER EMPLOYEES ON THE PROPER PROTOCOL FOR REPORTING SPILLS. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER OF ANY SPILLS IMMEDIATELY.

- B. CONCRETE RESIDUALS AND WASHOUT WASTES - THE FOLLOWING BMPs SHALL BE IMPLEMENTED TO CONTROL RESIDUAL CONCRETE, CONCRETE SEDIMENTS, AND RINSE WATER:

1. TEMPORARY CONCRETE WASHOUT FACILITIES SHALL BE CONSTRUCTED FOR RINSING OUT CONCRETE TRUCKS. SIGNS SHALL BE INSTALLED DIRECTING CONCRETE TRUCK DRIVERS WHERE DESIGNATED WASHOUT FACILITIES ARE LOCATED.
2. THE CONTRACTOR SHALL HAVE THE LOCATION OF TEMPORARY CONCRETE WASHOUT FACILITIES APPROVED BY THE RESIDENT ENGINEER.
3. ALL TEMPORARY CONCRETE WASHOUT FACILITIES ARE TO BE INSPECTED BY THE CONTRACTOR AFTER EACH USE AND ALL SPILLS MUST BE REPORTED TO THE RESIDENT ENGINEER AND CLEANED UP IMMEDIATELY.
4. CONCRETE WASTE SOLIDS/LIQUIDS SHALL BE DISPOSED OF PROPERLY.

- C. LITTER MANAGEMENT - A PROPER NUMBER OF DUMPSTERS SHALL BE PROVIDED ON SITE TO HANDLE DEBRIS AND LITTER ASSOCIATED WITH THE PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING HIS/HER EMPLOYEES PLACE ALL LITTER INCLUDING MARKING PAINT CANS, SODA CANS, FOOD WRAPPERS, WOOD LATHE, MARKING RIBBON, CONSTRUCTION STRING, AND ALL OTHER CONSTRUCTION RELATED LITTER IN THE PROPER DUMPSTERS.

- D. VEHICLE AND EQUIPMENT CLEANING - VEHICLES AND EQUIPMENT ARE TO BE CLEANED IN DESIGNATED AREAS ONLY, PREFERABLY OFF SITE.

- E. VEHICLE AND EQUIPMENT FUELING - A VARIETY OF BMPs CAN BE IMPLEMENTED DURING FUELING OF VEHICLES AND EQUIPMENT TO PREVENT POLLUTION. THE CONTRACTOR SHALL INFORM THE RESIDENT ENGINEER AS TO WHICH BMPs WILL BE USED ON THE PROJECT. THE CONTRACTOR SHALL INFORM THE RESIDENT ENGINEER HOW (SHE WILL BE INFORMING HIS/HER EMPLOYEES OF THESE BMPs (I.E. SIGNS, TRAINING, ETC.). BELOW ARE A FEW EXAMPLES OF THESE BMPs:




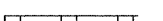

1. CONTAINMENT
2. SPILL PREVENTION AND CONTROL
3. USE OF DRIP PANS AND ABSORBENTS
4. AUTOMATIC SHUT-OFF NOZZLES
5. TOPPING OFF RESTRICTIONS
6. LEAK INSPECTION AND REPAIR

- F. VEHICLE AND EQUIPMENT MAINTENANCE - ON SITE MAINTENANCE MUST BE PERFORMED IN ACCORDANCE WITH ALL ENVIRONMENTAL LAWS SUCH AS PROPER STORAGE AND NO DUMPING OF OLD ENGINE OIL OR OTHER FLUIDS ON SITE.

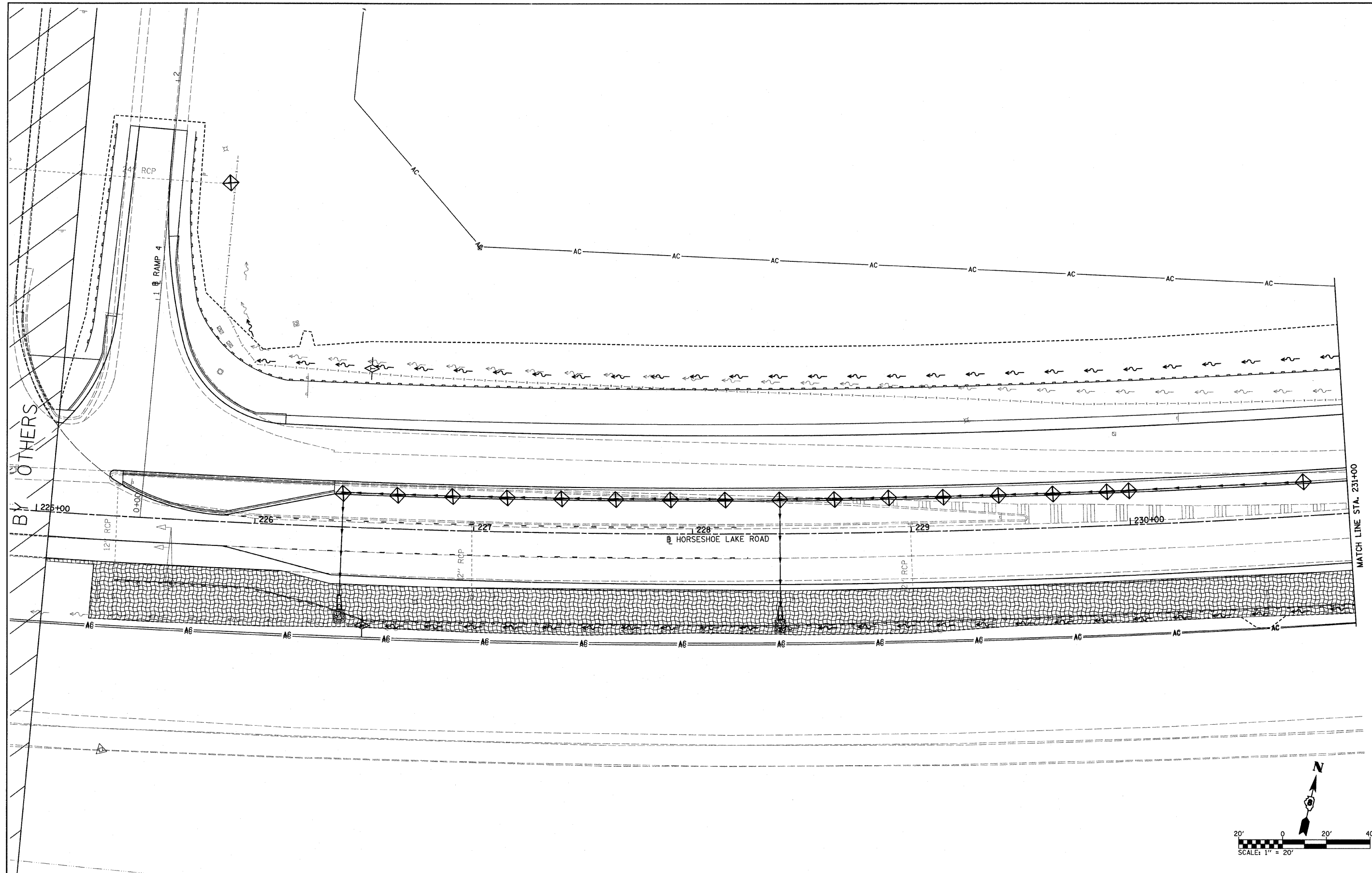
VI. FAILURE TO COMPLY:

FAILURE TO COMPLY WITH ANY PROVISIONS OF THIS STORM WATER POLLUTION PREVENTION PLAN WILL RESULT IN THE IMPLEMENTATION OF AN EROSION AND SEDIMENT CONTROL DEFICIENCY DEDUCTION AGAINST THE CONTRACTOR AND/OR PENALTIES UNDER THE NPDES PERMIT WHICH COULD BE PASSED ONTO THE CONTRACTOR.

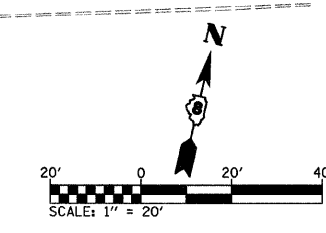
LEGEND

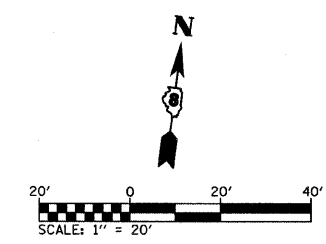
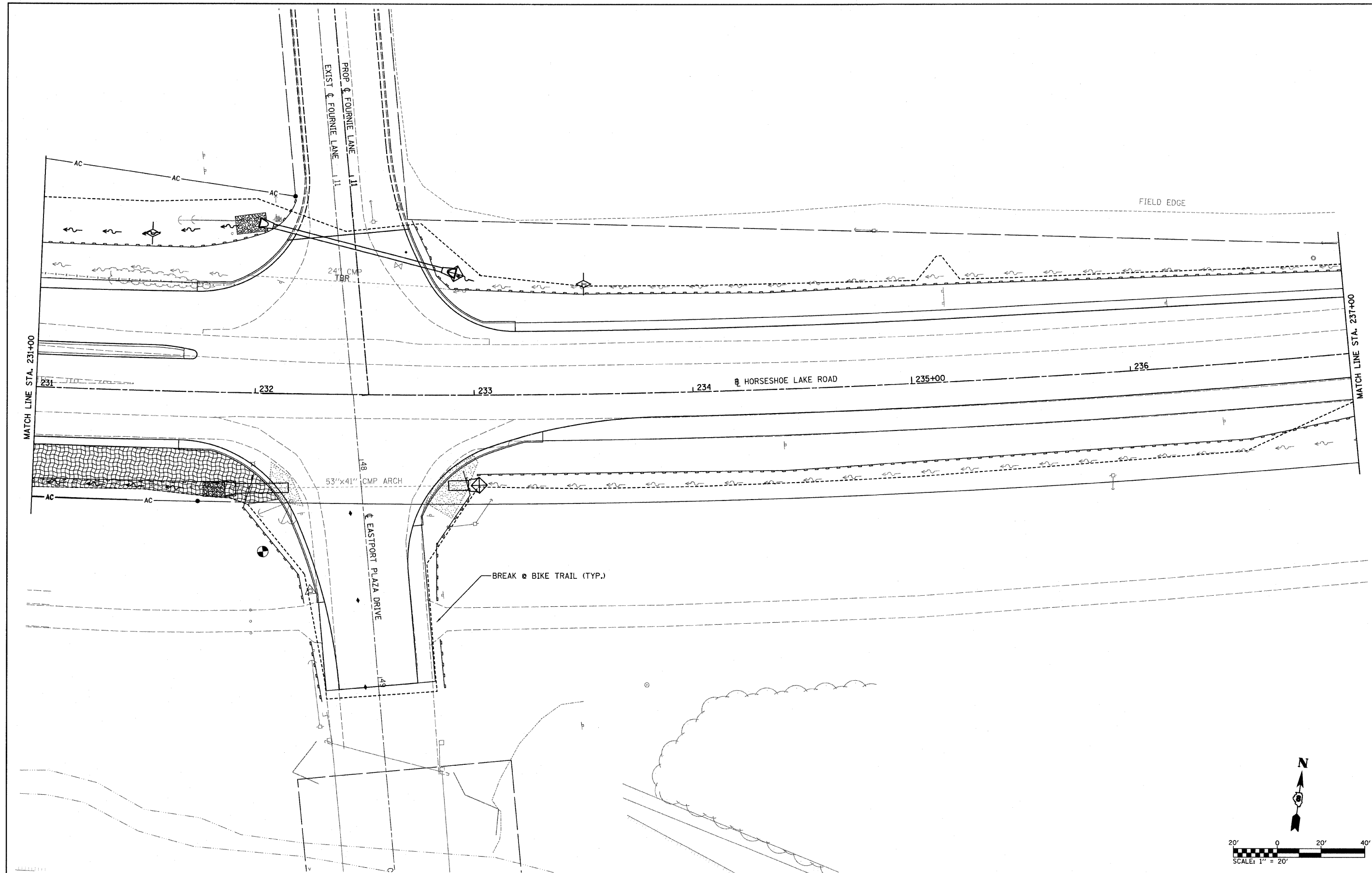
-  TEMPORARY DITCH CHECK- ROLLED EXCELSIOR, SILT WEDGES/PANELS
-  TEMPORARY DITCH CHECK- AGGREGATE
-  EROSION CONTROL BLANKET
-  PERIMETER EROSION BARRIER- SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
-  INLET AND PIPE PROTECTION- STRAW BALES, FILTER FABRIC, AGGREGATES

FILE NAME =	USER NAME = #USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				HORSESHOE LAKE ROAD				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
H:\NP\25804\Technical Production\Civil\W07	7\Microstation\SWPP042.dgn	DRAWN -	REVISED -					STORM WATER POLLUTION PREVENTION PLAN SHEETS				9111	73-15TS	MADISON	64	22
	PLOT SCALE = 20.0000' / 1" IN.	CHECKED -	REVISED -	SCALE:				SHEET NO. 2 OF 5 SHEETS	STA.	TO STA.	CONTRACT NO. 76B22					
	PLOT DATE = 3/14/2008	DATE -	REVISED -	FED. ROAD DIST. NO.				ILLINOIS FED. AID PROJECT								

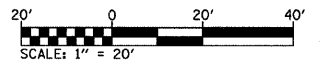
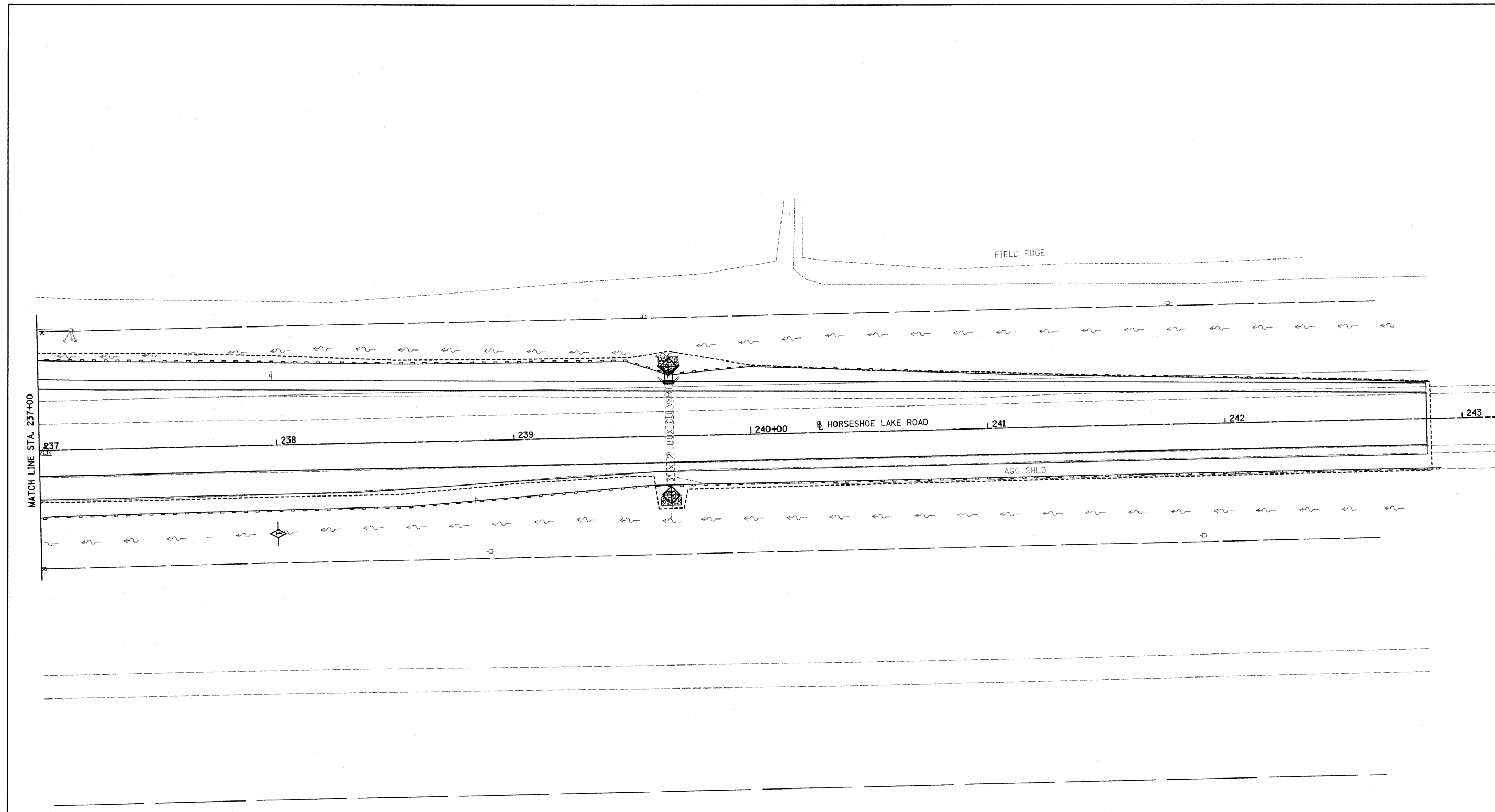


FILE NAME = H:\P\26004\Technical Production\Civil\WO	USER NAME = #USER# N:\Microstation\SWPP0A2.dgn	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HORSESHOE LAKE ROAD STORM WATER POLLUTION PREVENTION PLAN SHEETS			F.A.U. RTE. 9111	SECTION 73-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 23	
PLOT SCALE = 20,0000 ' / IN.	PLOT DATE = 3/14/2008	DRAWN -	REVISED -		SCALE: 1" = 20'	SHEET NO. 3	OF 5	SHEETS	STA. 225+00	TO STA. 231+00	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 76B22
		CHECKED -	REVISED -										
		DATE -	REVISED -										





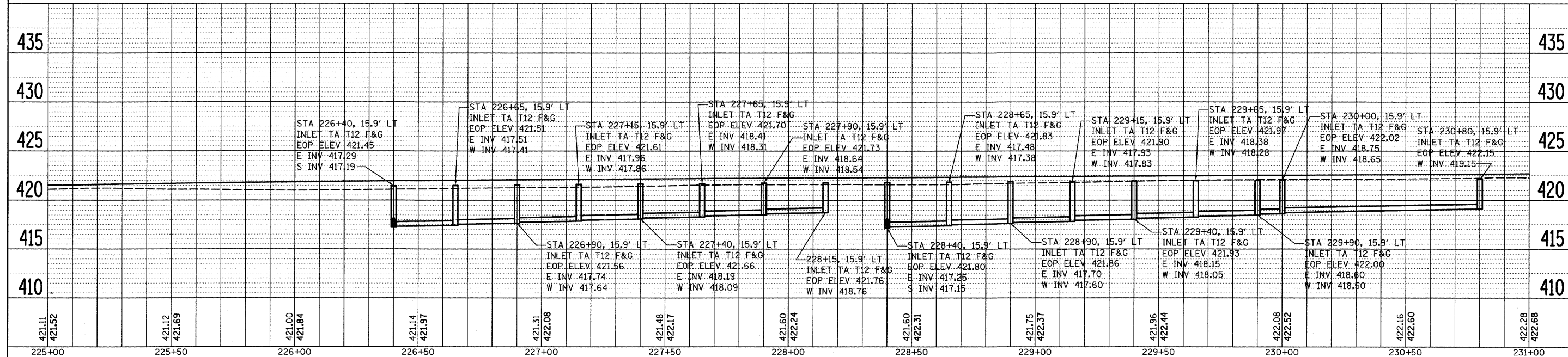
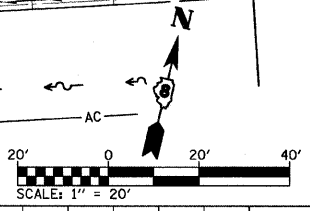
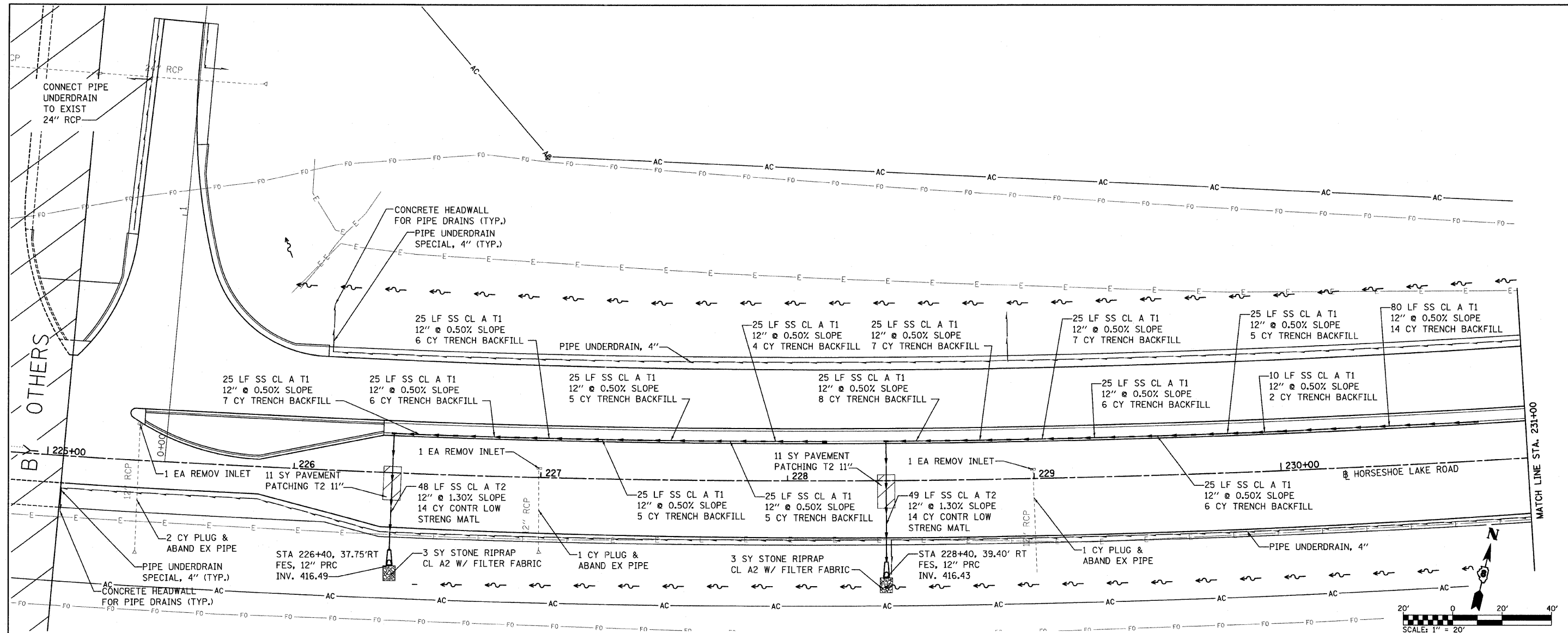
FILE NAME = H:\P\20084\Technical Production\Civil\W0	USER NAME = #USER# Microstation\SWPP0A2.dgn	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HORSESHOE LAKE ROAD STORM WATER POLLUTION PREVENTION PLAN SHEETS			F.A.U. RTE. 9111	SECTION 73-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 24
	PLOT SCALE = 20.0000' / IN.	DRAWN -	REVISED -		SCALE: 1" = 20'	SHEET NO. 4	OF 5 SHEETS	STA. 231+00	TO STA. 237+00	CONTRACT NO. 76B22		
	PLOT DATE = 3/14/2008	CHECKED -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									



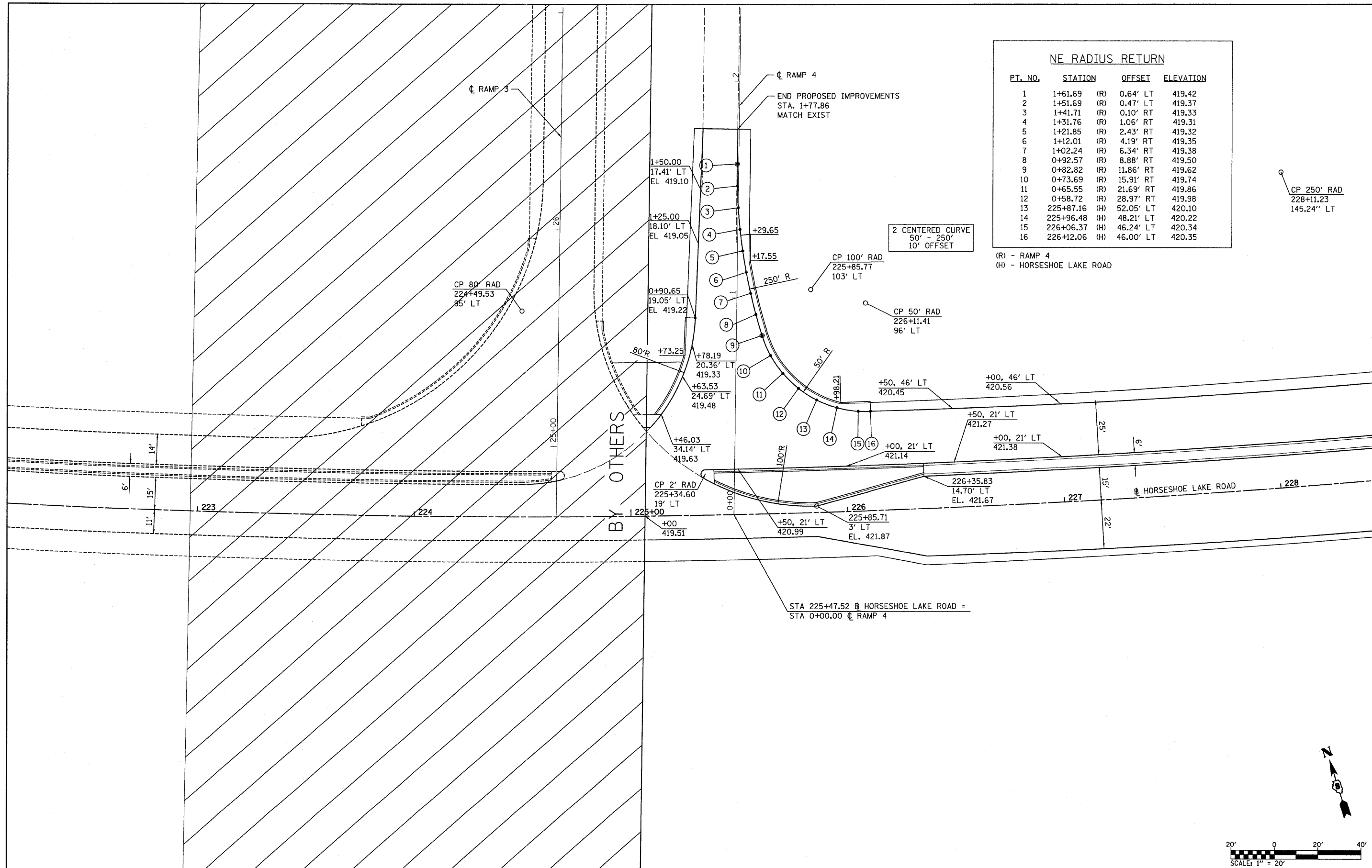
FILE NAME = H:\P\25004\Technical Production\Civil\W07	USER NAME = @USER@	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HORSESHOE LAKE ROAD STORM WATER POLLUTION PREVENTION PLAN SHEETS			F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	7\Microstation\SWPP0A2.dgn	DRAWN -	REVISED -		9111	73-15TS	MADISON	64	25			
PLOT SCALE = 20,0000 ' / IN.	CHECKED -	REVISED -	SCALE: 1" = 20' SHEET NO. 5 OF 5 SHEETS STA. 237+00 TO STA. 242+85				CONTRACT NO. 76B22					
PLOT DATE = 3/14/2008	DATE -	REVISED -	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT									

DATE	BY
REVISIONS	
NO.	DATE
1	
2	
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	
17	
18	
19	
20	
21	
22	
23	
24	
25	
26	
27	
28	
29	
30	
31	
32	
33	
34	
35	
36	
37	
38	
39	
40	
41	
42	
43	
44	
45	
46	
47	
48	
49	
50	
51	
52	
53	
54	
55	
56	
57	
58	
59	
60	
61	
62	
63	
64	
65	
66	
67	
68	
69	
70	
71	
72	
73	
74	
75	
76	
77	
78	
79	
80	
81	
82	
83	
84	
85	
86	
87	
88	
89	
90	
91	
92	
93	
94	
95	
96	
97	
98	
99	
100	

DATE	BY
REVISIONS	
NO.	DATE
1	
2	
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	
17	
18	
19	
20	
21	
22	
23	
24	
25	
26	
27	
28	
29	
30	
31	
32	
33	
34	
35	
36	
37	
38	
39	
40	
41	
42	
43	
44	
45	
46	
47	
48	
49	
50	
51	
52	
53	
54	
55	
56	
57	
58	
59	
60	
61	
62	
63	
64	
65	
66	
67	
68	
69	
70	
71	
72	
73	
74	
75	
76	
77	
78	
79	
80	
81	
82	
83	
84	
85	
86	
87	
88	
89	
90	
91	
92	
93	
94	
95	
96	
97	
98	
99	
100	



FILE NAME = H:\PA\25084\Technical Production\Civil\W017	USER NAME = #USER# Microstation\Drnos2e.dgn	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HORSESHOE LAKE ROAD DRAINAGE PLAN SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 20.0000' / IN.	PLOT DATE = 3/14/2008	DRAWN -	REVISED -			9111	73-15TS	MADISON	64	26	
		CHECKED -	REVISED -			CONTRACT NO. T6B22					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



NE RADIUS RETURN

PT. NO.	STATION	OFFSET	ELEVATION
1	1+61.69 (R)	0.64' LT	419.42
2	1+51.69 (R)	0.47' LT	419.37
3	1+41.71 (R)	0.10' RT	419.33
4	1+31.76 (R)	1.06' RT	419.31
5	1+21.85 (R)	2.43' RT	419.32
6	1+12.01 (R)	4.19' RT	419.35
7	1+02.24 (R)	6.34' RT	419.38
8	0+92.57 (R)	8.88' RT	419.50
9	0+82.82 (R)	11.86' RT	419.62
10	0+73.69 (R)	15.91' RT	419.74
11	0+65.55 (R)	21.69' RT	419.86
12	0+58.72 (R)	28.97' RT	419.98
13	225+87.16 (H)	52.05' LT	420.10
14	225+96.48 (H)	48.21' LT	420.22
15	226+06.37 (H)	46.24' LT	420.34
16	226+12.06 (H)	46.00' LT	420.35

(R) - RAMP 4
(H) - HORSESHOE LAKE ROAD

CP 250' RAD
228+11.23
145.24' LT

2 CENTERED CURVE
50' - 250'
10' OFFSET

STA 225+47.52 @ HORSESHOE LAKE ROAD =
STA 0+00.00 @ RAMP 4

FILE NAME = H:\P\25204\Technical Production\Civil\W0	USER NAME = #USER# 7\Microstation\intdet001.dgn	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INTERSECTION DETAIL HORSESHOE LAKE RD/RAMP 4		F.A.U. RTE. 9111	SECTION 73-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 29	
PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -	REVISED -		SCALE: 1"=20'	SHEET NO. 1 OF 2 SHEETS	STA.	TO STA.	CONTRACT NO. 76B22			
PLOT DATE = 3/14/2008	DATE -	REVISED -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							

NW RADIUS RETURN				
PT. NO.	STATION	OFFSET		ELEVATION
1	11+03.12	(F) 10.49'	LT	420.31
2	10+98.98	(F) 10.66'	LT	420.36
3	10+89.16	(F) 12.48'	LT	420.47
4	10+79.90	(F) 16.21'	LT	420.59
5	10+71.57	(F) 21.71'	LT	420.70
6	10+64.33	(F) 28.94'	LT	420.82
7	10+58.86	(F) 37.22'	LT	420.94
8	10+55.12	(F) 46.48'	LT	421.05
9	231+82.39	(H) 46.99'	LT	421.16
10	231+72.32	(H) 46.00'	LT	421.28

(F) - FOURNIE LANE
(H) - HORSESHOE LAKE ROAD

NE RADIUS RETURN				
PT. NO.	STATION	OFFSET		ELEVATION
1	11+11.62	(F) 25.51'	RT	420.24
2	11+05.62	(F) 25.63'	RT	420.33
3	10+95.65	(F) 26.36'	RT	420.48
4	10+85.75	(F) 27.75'	RT	420.62
5	10+75.96	(F) 29.81'	RT	420.77
6	10+66.34	(F) 32.51'	RT	420.92
7	10+56.91	(F) 35.84'	RT	421.06
8	10+47.73	(F) 39.79'	RT	421.21
9	10+41.55	(F) 43.33'	LT	421.31
10	232+90.92	(H) 38.55'	LT	421.46
11	232+99.50	(H) 33.33'	LT	421.61
12	233+09.01	(H) 30.10'	LT	421.75
13	233+18.99	(H) 29.00'	LT	421.90

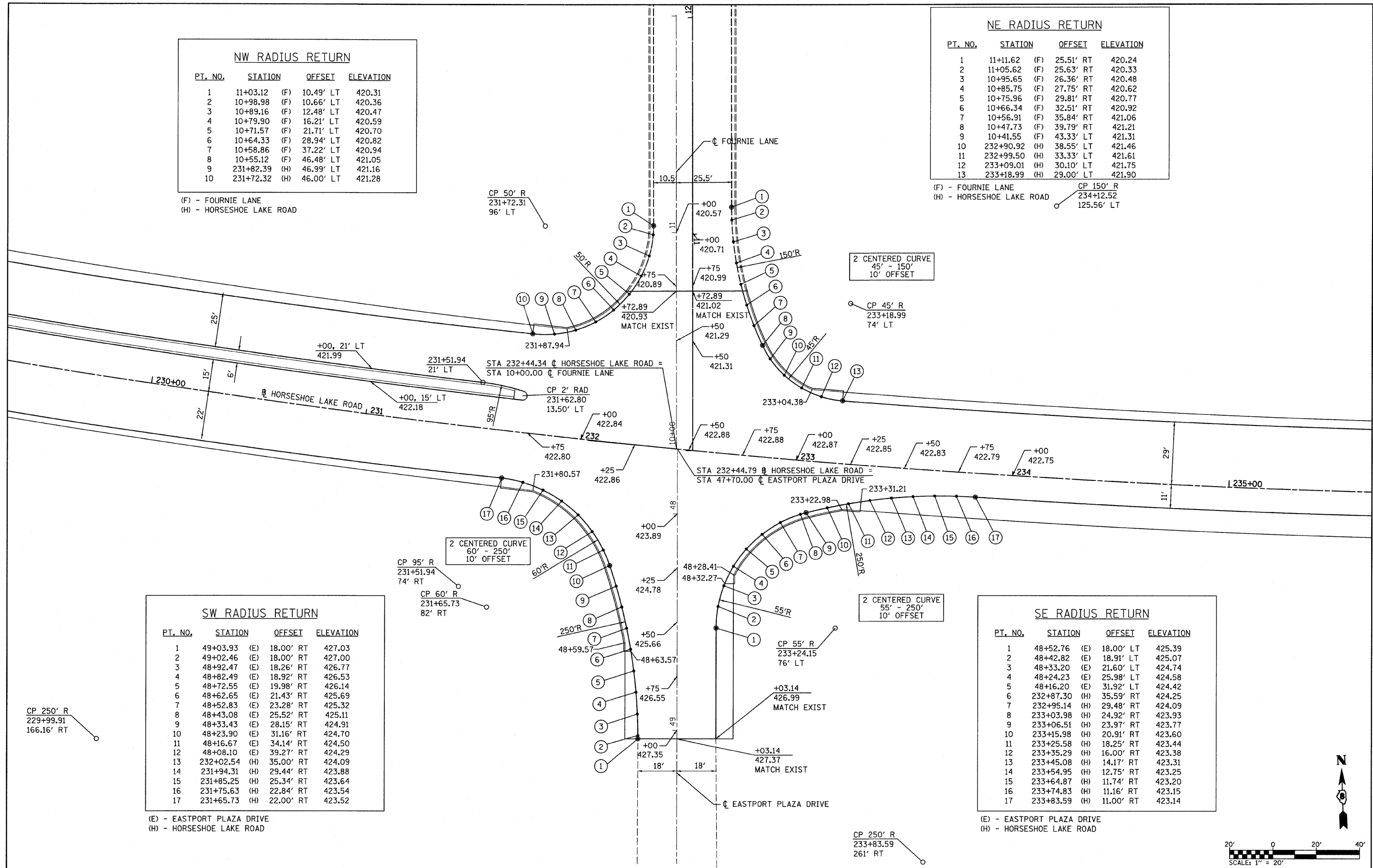
(F) - FOURNIE LANE
(H) - HORSESHOE LAKE ROAD

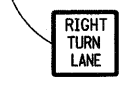
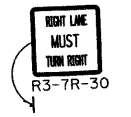
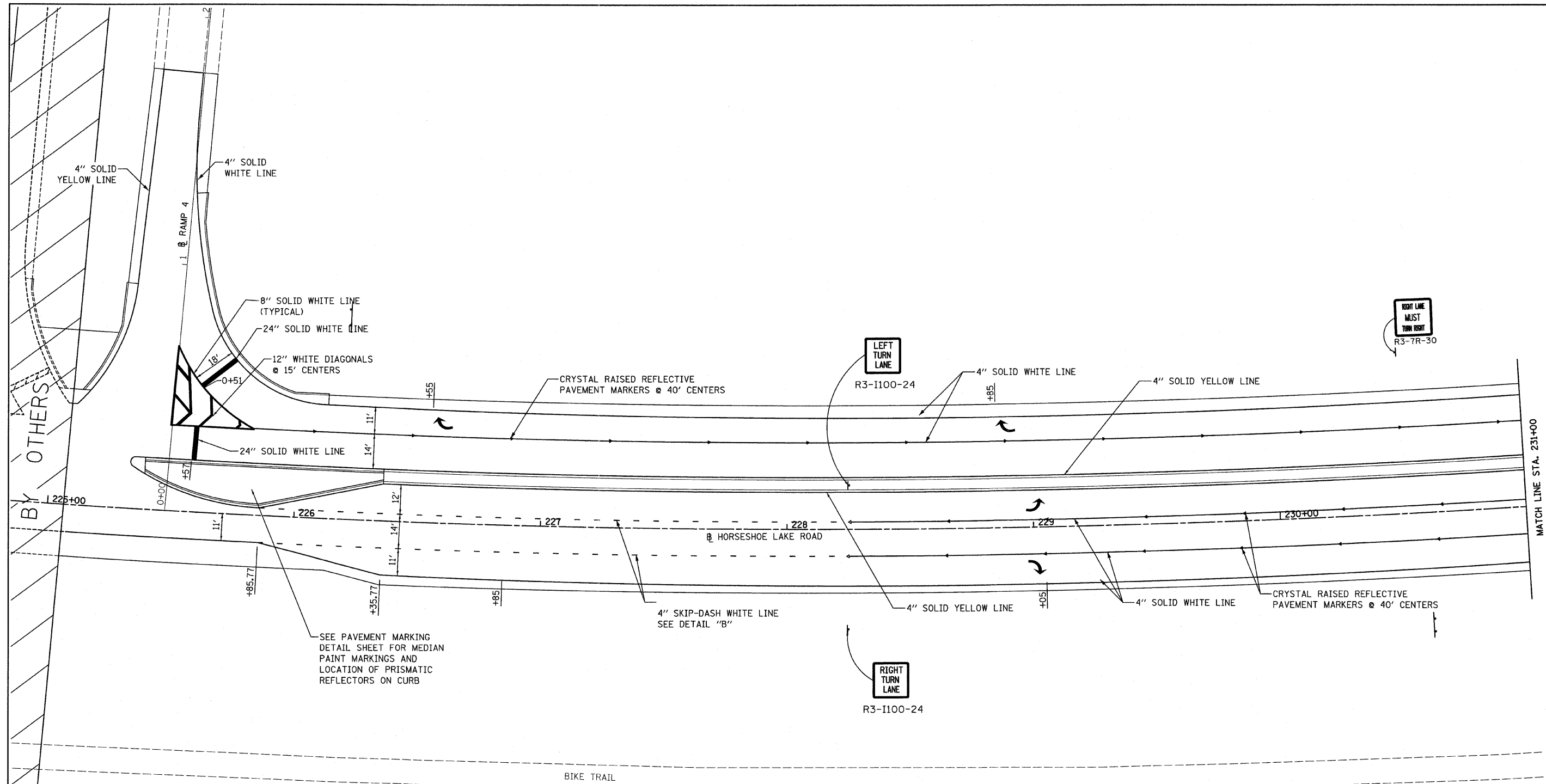
SW RADIUS RETURN				
PT. NO.	STATION	OFFSET		ELEVATION
1	49+03.93	(E) 18.00'	RT	427.03
2	49+02.46	(E) 18.00'	RT	427.00
3	48+92.47	(E) 18.26'	RT	426.77
4	48+82.49	(E) 18.92'	RT	426.53
5	48+72.55	(E) 19.98'	RT	426.14
6	48+62.65	(E) 21.43'	RT	425.69
7	48+52.83	(E) 23.28'	RT	425.32
8	48+43.08	(E) 25.52'	RT	425.11
9	48+33.43	(E) 28.15'	RT	424.91
10	48+23.90	(E) 31.16'	RT	424.70
11	48+16.67	(E) 34.14'	RT	424.50
12	48+08.10	(E) 39.27'	RT	424.29
13	232+02.54	(H) 35.00'	RT	424.09
14	231+94.31	(H) 29.44'	RT	423.88
15	231+85.25	(H) 25.34'	RT	423.64
16	231+75.63	(H) 22.84'	RT	423.54
17	231+65.73	(H) 22.00'	RT	423.52

(E) - EASTPORT PLAZA DRIVE
(H) - HORSESHOE LAKE ROAD

SE RADIUS RETURN				
PT. NO.	STATION	OFFSET		ELEVATION
1	48+52.76	(E) 18.00'	LT	425.39
2	48+42.82	(E) 18.91'	LT	425.07
3	48+33.20	(E) 21.60'	LT	424.74
4	48+24.23	(E) 25.98'	LT	424.58
5	48+16.20	(E) 31.92'	LT	424.42
6	232+87.30	(H) 35.59'	RT	424.25
7	232+95.14	(H) 29.48'	RT	424.09
8	233+03.98	(H) 24.92'	RT	423.93
9	233+06.51	(H) 23.97'	RT	423.77
10	233+15.98	(H) 20.91'	RT	423.60
11	233+25.58	(H) 18.25'	RT	423.44
12	233+35.29	(H) 16.00'	RT	423.38
13	233+45.08	(H) 14.17'	RT	423.31
14	233+54.95	(H) 12.75'	RT	423.25
15	233+64.87	(H) 11.74'	RT	423.20
16	233+74.83	(H) 11.16'	RT	423.15
17	233+83.59	(H) 11.00'	RT	423.14

(E) - EASTPORT PLAZA DRIVE
(H) - HORSESHOE LAKE ROAD

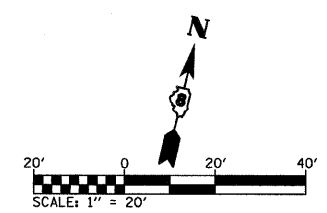




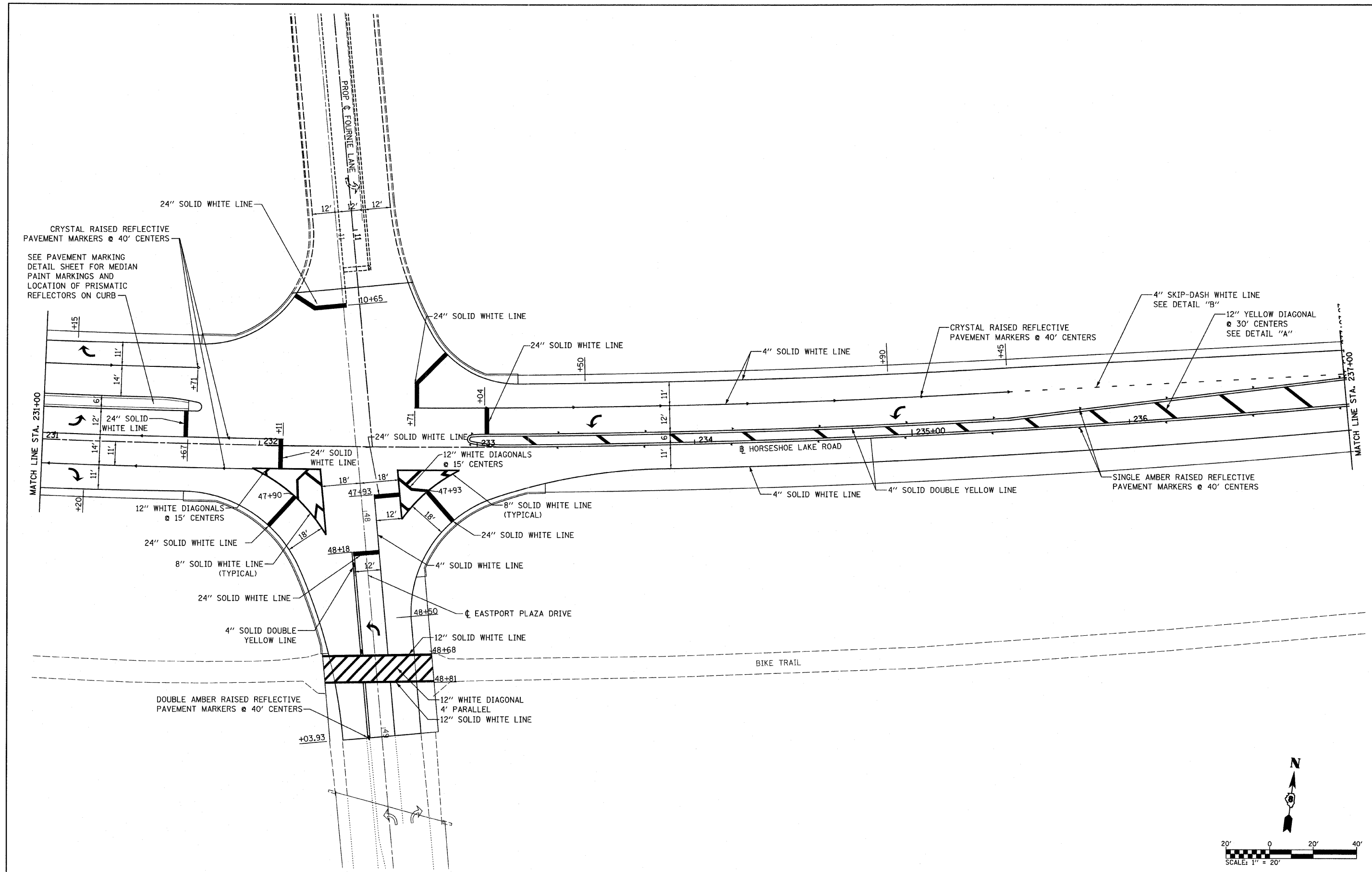
SEE PAVEMENT MARKING
DETAIL SHEET FOR MEDIAN
PAINT MARKINGS AND
LOCATION OF PRISMATIC
REFLECTORS ON CURB

4" SKIP-DASH WHITE LINE
SEE DETAIL "B"

CRYSTAL RAISED REFLECTIVE
PAVEMENT MARKERS @ 40' CENTERS



FILE NAME = H:\P\25004\Technical Production\Civil\W07	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING PLANS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	Microstation\pvmrk0R2.dgn	DRAWN -	REVISED -					9111	73-15TS	MADISON	64	31
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -					CONTRACT NO. 76B22				
	PLOT DATE = 3/14/2008	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				
				SCALE: 1" = 20'		SHEET NO. 1 OF 4 SHEETS		STA. 225+00 TO STA. 231+00				



FILE NAME = H:\P\25004\Technical Production\Civil\W07
 USER NAME = @USER#
 7\Microstation\pvmrk0a2.dgn
 PLOT SCALE = 20.0000' / IN.
 PLOT DATE = 3/14/2008

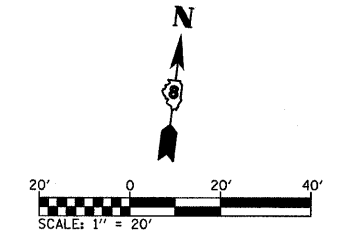
DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

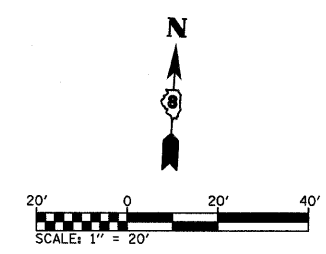
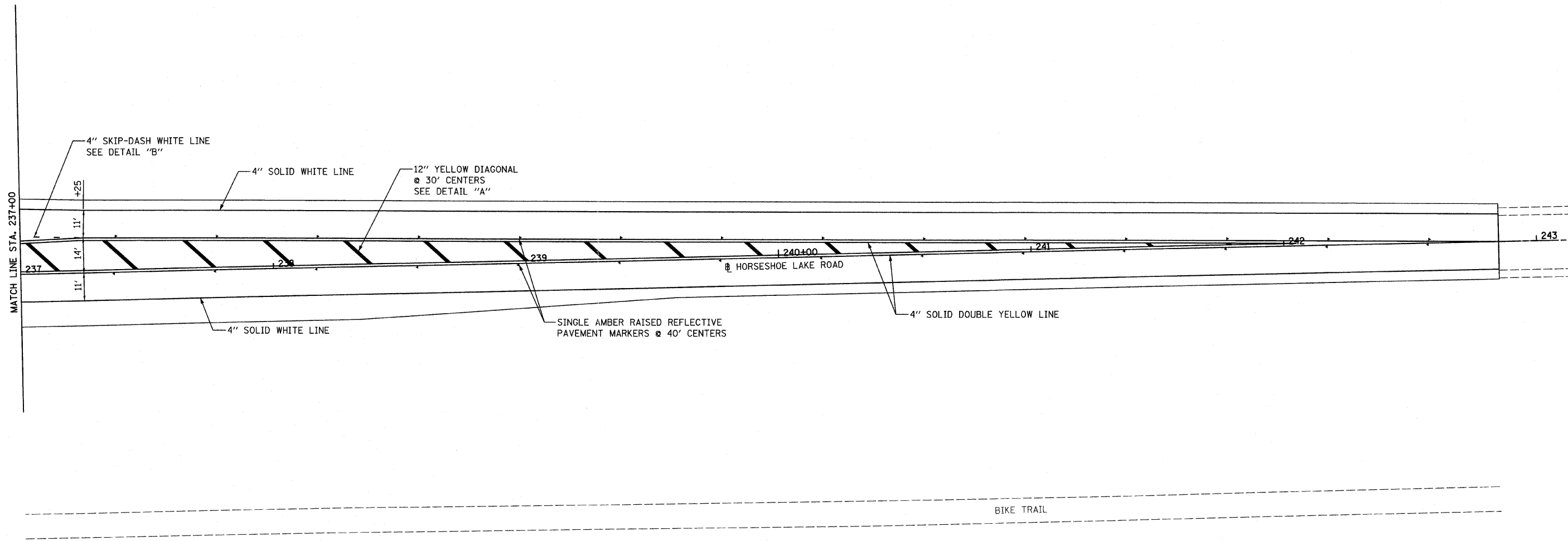
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLANS

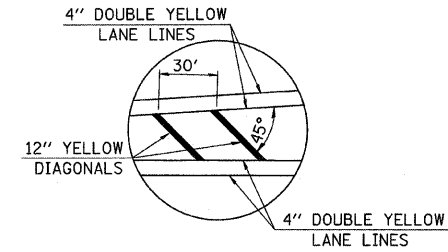
SCALE: 1"=20' SHEET NO. 2 OF 4 SHEETS STA. 231+00 TO STA. 237+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9111	73-1STS	MADISON	64	32
CONTRACT NO. 76B22				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

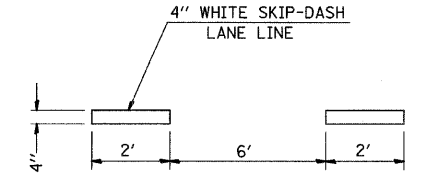




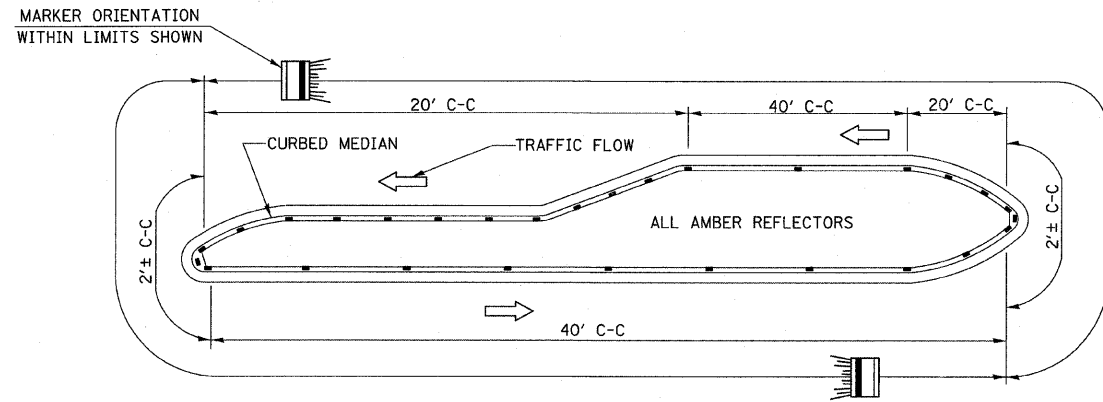
FILE NAME = H:\P\25004\Technical Production\Civil\W0	USER NAME = #USER# 7\Microstation\pvmrk0R2.dgn	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING PLANS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 20.0000' / IN.	DRAWN -	REVISED -					9111	73-15TS	MADISON	64	33
	PLOT DATE = 3/14/2008	CHECKED -	REVISED -		SCALE: 1" = 20'			SHEET NO. 3 OF 4 SHEETS		STA. 237+00 TO STA. 243+00	CONTRACT NO. 76B22	
		DATE -	REVISED -		FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT					



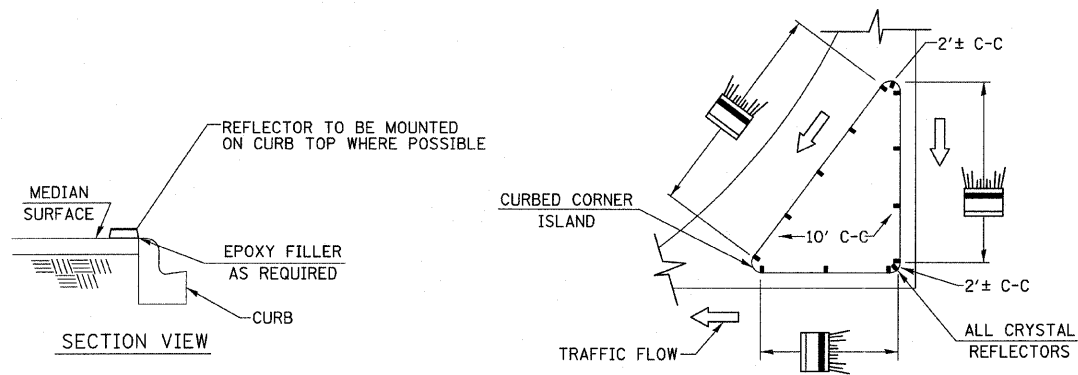
TYPICAL APPLICATION FOR MEDIAN STRIPING
DETAIL 'A'
 NOT TO SCALE



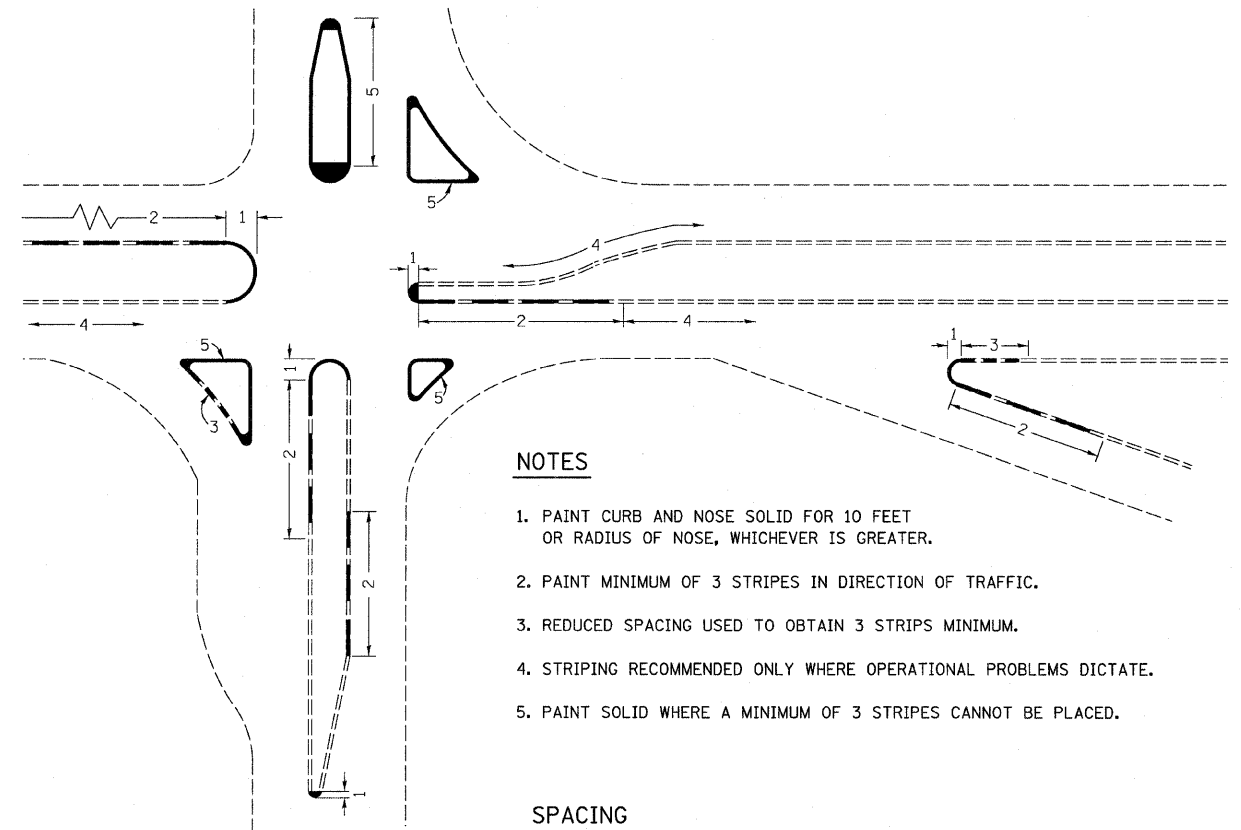
TYPICAL APPLICATION FOR ABBREVIATED WHITE SKIP-DASH LANE LINES
DETAIL 'B'
 NOT TO SCALE



- NOTES**
1. PRISMATIC REFLECTORS SHALL BE MONO-DIRECTIONAL AND POSITIONED SO THAT THE REFLECTIVE FACE IS FACING THE APPROACHING TRAFFIC.
 2. PRISMATIC REFLECTORS SHALL BE SECURED IN PLACE WITH AN EPOXY ADHESIVE.
 3. PRISMATIC REFLECTORS SHALL BE EITHER AMBER OR CRYSTAL IN COLOR.



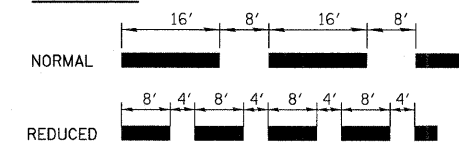
TYPICAL PLACEMENT OF PRISMATIC REFLECTORS ON CURBS
 (NO SCALE)



NOTES

1. PAINT CURB AND NOSE SOLID FOR 10 FEET OR RADIUS OF NOSE, WHICHEVER IS GREATER.
2. PAINT MINIMUM OF 3 STRIPES IN DIRECTION OF TRAFFIC.
3. REDUCED SPACING USED TO OBTAIN 3 STRIPS MINIMUM.
4. STRIPING RECOMMENDED ONLY WHERE OPERATIONAL PROBLEMS DICTATE.
5. PAINT SOLID WHERE A MINIMUM OF 3 STRIPES CANNOT BE PLACED.

SPACING



TYPICAL CURB MARKING

FILE NAME = H:\P\25004\Technical Production\Civil\W07	USER NAME = #USER# 7\Microstation\pvmrk0a2.dgn	DESIGNED -	REVISD -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING DETAIL			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISD -	REVISD -					9111	73-15TS	MADISON	64	34
PLOT DATE = 3/14/2008	DATE -	REVISD -	REVISD -		SCALE: NO SCALE SHEET NO. 4 OF 4 SHEETS STA. TO STA.			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		CONTRACT NO. 76B22		

SCHEDULE OF QUANTITIES

SCHEDULE OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE Y031			SCHEDULE OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE Y031		
CODE NO	ITEM	UNIT		I-255 RAMPS 3 & 4	EPRT.PLZ. DR. FOURNIE LN.	-----	CODE NO	ITEM	UNIT		I-255 RAMPS 3 & 4	EPRT.PLZ. DR. FOURNIE LN.	-----
72000100	SIGN PANEL - TYPE 1	SQ FT	15		15		87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1991	458	1533	
72000200	SIGN PANEL - TYPE 2	SQ FT	49		49		87700230	STEEL MAST ARM ASSEMBLY AND POLE, 38 FT.	EACH	1		1	
80300100	LOCATING UNDERGROUND CABLE	FOOT	80	40	40		87700250	STEEL MAST ARM ASSEMBLY AND POLE, 42 FT.	EACH	1		1	
80500100	SERVICE INSTALLATION, TYPE A	EACH	1		1		87700270	STEEL MAST ARM ASSEMBLY AND POLE, 46 FT.	EACH	1		1	
81012300	CONDUIT IN TRENCH, 1" DIA., PVC	FOOT	213	65	148		87700310	STEEL MAST ARM ASSEMBLY AND POLE, 54 FT.	EACH	1		1	
81012500	CONDUIT IN TRENCH, 1 1/2" DIA., PVC	FOOT	590		590		87800200	CONCRETE FOUNDATION, TYPE D	FOOT	3		3	
81012600	CONDUIT IN TRENCH, 2" DIA., PVC	FOOT	109		109		87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	64		64	
81012700	CONDUIT IN TRENCH, 2 1/2" DIA., PVC	FOOT	55		55		88040070	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	4		4	
81012800	CONDUIT IN TRENCH, 3" DIA., PVC	FOOT	63	16	47		88040090	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	4		4	
81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	198		198		88040110	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 4-SECTION, BRACKET MOUNTED	EACH	2		2	
81018600	CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL	FOOT		56			88040120	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 4-SECTION, MAST ARM MOUNTED	EACH	2		2	
81400700	HANDHOLE, PORTLAND CEMENT CONCRETE	EACH	10	2	8		88040150	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2		2	
81400720	DOUBLE HANDHOLE, PORTLAND CEMENT CONCRETE	EACH	1		1		88040160	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	2		2	
81603035	UNIT DUCT, 600V, 2-1C NO. 6, 1/C NO. 6 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE	FOOT	1222	703	519		88200100	TRAFFIC SIGNAL BACKPLATE	EACH	8		8	
81702110	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 10	FOOT	250		250		88500100	INDUCTIVE LOOP DETECTOR	EACH	15	1	14	
81702130	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6	FOOT	1480		1480		88600100	DETECTOR LOOP, TYPE I	FOOT	2280	427	1853	
81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	1710	768	942		89502210	MODIFY EXISTING CONTROLLER CABINET	EACH	1	1		
81900205	TRENCH AND BACKFILL FOR ELECTRICAL WORK (SPECIAL)	FOOT	24	4	20		89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	60	60		
82103900	LUMINAIRE, SODIUM VAPOR, MULTI-MOUNT, 250 WATT	EACH			2		89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	660	660		
83027500	LIGHT POLE, ALUMINUM, TRANSFORMER BASE, 50 FT. M.H., TENON MOUNT - TWIN	EACH	1		1		89502380	REMOVE EXISTING HANDHOLE	EACH	1	1		
83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	21	14	7								
84200500	REMOVAL OF EXISTING LIGHTING UNIT, SALVAGE	EACH	1		1								
84200700	LIGHTING FOUNDATION REMOVAL	EACH	3	2	1								
84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	2	2									
85700205	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1		1								
86400100	TRANSCEIVER - FIBER OPTIC	EACH	1		1								
87100105	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, 4F	FOOT	506	272	234								
87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1211		1211								
87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1257		1257								

LEGEND

GSC	GALVANIZED STEEL CONDUIT
PVCC	POLYVINYL CHLORIDE CONDUIT
RECC	REMOVE ELECTRIC CABLE FROM CONDUIT
RRECC	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT
T & BKFL (SPL)	TRENCH & BACKFILL FOR ELECTRICAL WORK (SPECIAL)
UD-#4	UNIT DUCT, 600V, 2-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE
UD-#6	UNIT DUCT, 600V, 2-1C NO.6, 1/C NO.6 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE
UD-#2	UNIT DUCT, 600V, 2-1C NO.2, 1/C NO.2 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE
	EXISTING SIGNAL POST
	EXISTING TRAFFIC SIGNAL MAST ARM
	EXISTING HANDHOLE
	EXISTING DOUBLE HANDHOLE
	EXISTING DETECTOR LOOP
	EXISTING CONTROLLER
	EXISTING UNIT-DUCT, SIZE SPECIFIED
	EXISTING CONDUIT
	EXISTING LIGHTING CONDUIT
	EXISTING STREET NAME SIGN/TRAFFIC SIGN
	EXISTING SERVICE INSTALLATION
	EXISTING LIGHT POLE/FOUNDATION
	PROPOSED SIGNAL HEAD WITH BACKPLATE, MAST ARM MOUNTED
	PROPOSED HANDHOLE
	PROPOSED DOUBLE HANDHOLE
	PROPOSED DETECTOR LOOP
	PROPOSED CONTROLLER
	PROPOSED CONDUIT: "T" TRENCH, "P" PUSH, SIZE SPECIFIED
	PROPOSED PUSHED LIGHTING CONDUIT, SIZE SPECIFIED
	PROPOSED UNIT-DUCT, SIZE SPECIFIED
	PROPOSED STREET NAME SIGN/TRAFFIC SIGN
	PROPOSED SERVICE INSTALLATION
	PROPOSED SIGNAL POST
	RELOCATED LIGHT POLE W/ PROPOSED FOUNDATION
	CABLE SPLICE (SEE GENERAL NOTES)

TRAFFIC SIGNAL GENERAL NOTES

- ALL VEHICLE AND PEDESTRIAN SIGNAL HEADS SHALL HAVE 12" SECTIONS. MOUNTING HARDWARE SHALL BE UNPAINTED ALUMINUM. ALL BOLTS, SCREWS, NUTS AND WASHERS SHALL BE STAINLESS STEEL. ANTI-SEIZE PASTE COMPOUND SHALL BE USED ON ALL MOUNTING HARDWARE FIELD CONNECTIONS.
- BACKPLATES SHALL BE ABS PLASTIC.
- THE CONTROLLER CABINET SHALL BE UNPAINTED ALUMINUM.
- THE LOCATION OF MAST ARM SUPPORTS SHALL BE APPROVED BY THE ENGINEER BEFORE FOUNDATIONS ARE CONSTRUCTED. MAST ARM POLES SHALL BE LOCATED A MINIMUM OF 10 FEET FROM THE EDGE OF PAVEMENT OR 2 FEET FROM THE EDGE OF SHOULDER, WHICHEVER DISTANCE IS GREATER. IN CURBED SECTIONS, THE MAST ARM POLES SHALL BE LOCATED A MINIMUM OF 10 FEET FROM THE EDGE OF PAVEMENT OR 2 FEET FROM THE EDGE OF SHOULDER, WHICHEVER DISTANCE IS GREATER. IN CURBED SECTIONS, THE MAST ARM POLES SHALL BE LOCATED A MINIMUM OF 5 FEET FROM THE FACE OF THE CURB. THESE DISTANCES ARE TO THE NEAR FACE OF THE MAST ARM POLE.
- ALL TRAFFIC SIGNAL CABLES SHALL BE #14 AWG STRANDED COPPER UNLESS OTHERWISE SPECIFIED. TERMINAL ENDS SHALL HAVE CRIMPED-ON RING TONGUE CONNECTORS.
- THE LOCATION OF ALL DETECTOR LOOPS SHALL BE APPROVED BY THE ENGINEER BEFORE ANY SLOTS ARE SAWS IN THE PAVEMENT.
- DETECTOR LOOP LEAD-IN SPLICES SHALL BE MADE IN A HANDHOLE PER SECTION 873 OF THE STANDARD SPECIFICATIONS. CONDUCTORS SHALL BE SPLICED IN A RIGID MOLD FILLED WITH NON-HARDENING EPOXY FILLER. ROSIN-CORE SOLDER SHALL BE USED.
- CALL CARRY-OVER SHALL FUNCTION ONLY WHEN THE RELATED PHASES ARE IN THE GREEN MODE.
- ALL INDUCTIVE LOOP DETECTORS SUPPLIED FOR THIS PROJECT SHALL HAVE THE CAPACITY OF OPERATING WITH BOTH DELAY AND EXTENSION MODES ACTIVE, IF A TIME SETTING IS PROGRAMMED. THEY SHALL BE RACK MOUNTED.
- ALL HANDHOLES SHALL BE CAST-IN-PLACE PORTLAND CEMENT CONCRETE (PER ARTICLE 814.03(c)). THE CAST IN PLACE LEGEND IN THE COVER SHALL BE "TRAFFIC SIGNALS". SLOPE HANDHOLE COVERS TO MATCH PROPOSED GRADE ELEVATIONS.
- LOCATE UNDERGROUND CABLES PRIOR TO ATTEMPTING TO CONSTRUCT THIS PROJECT.
- ESTIMATED DEPTHS OF THE CONCRETE FOUNDATIONS FOR THE MAST ARM SUPPORT POLES ARE AS FOLLOWS:
-EASTPARK PLAZA DRIVE/FOURNIE LANE
N-E CORNER: 13'-0" DEEP W/ 36" DIA.
N-W CORNER: 13'-0" DEEP W/ 36" DIA.
0.75 TSF MUST BE VERIFIED PRIOR TO CONSTRUCTION FOR:
S-E CORNER: 15'-0" DEEP W/ 36" DIA.
S-W CORNER: 23'-0" DEEP W/ 36" DIA.
- ABANDON UNUSED EXISTING CONDUITS AND CABLES IN PLACE.
- THE CONTRACTOR SHALL FABRICATE, DELIVER AND INSTALL STREET NAME SIGNS AT THE SPECIFIED LOCATIONS. THE SIGNS AND INSTALLATION SHALL CONFORM TO SECTION 720 OF THE STANDARD SPECIFICATIONS AND STANDARDS 720001 AND 720016.
- A 1/4" NYLON PULL ROPE SHALL BE INSTALLED IN ALL CONDUIT RUNS. THE COST OF THIS ROPE SHALL BE INCLUDED IN THE PROPOSED UNIT PRICE OF CABLE INSTALLATION/RE-INSTALLATION ASSOCIATED WITH THAT CONDUIT.

LIGHTING GENERAL NOTES

- DUE TO THE PROPOSED HORSESHOE LAKE ROAD REVISIONS, INSTALL UNIT DUCT AND CONDUIT BETWEEN LIGHT POLES X10 AND D1, AND LIGHT POLES D1 AND X12, AND X10 AND X12, AS SHOWN ON THE PLANS.
- SPLICING OF CONDUCTORS SHALL BE IN POLE BASES, ONLY. SPLICES BELOW GRADES WILL NOT BE PERMITTED.
- BREAKAWAY DEVICES SHALL BE INSTALLED FOR PLUMBING THE POLES. ALL POLES SHALL BE ERECTED PLUMB.
- TO INSURE EVEN DISTRIBUTION OF STRESSES ON BREAKAWAY COUPLINGS, ANCHOR NUTS SHALL NOT BE TIGHTENED UNTIL THE BREAKAWAY COUPLINGS ARE EVENLY AND FIRMLY TIGHTENED AGAINST THE BOTTOM OF THE POLE BASE PLATE.
- UNLESS LOCATED ON STRUCTURES, BEHIND GUARDRAIL, BARRIERS CURBS, OR OTHERWISE PROTECTED FROM COLLISION, LIGHT POLES SHALL HAVE BREAKAWAY DEVICES.
- THE COST OF NUTS AND WASHERS REQUIRED FOR MOUNTING LIGHT POLES ON NEW FOUNDATIONS SHALL BE INCLUDED IN THE UNIT PRICE FOR "RELOCATE EXISTING LIGHTING UNIT".
- A SURGE PROTECTOR SHALL BE INSTALLED IN THE PROPOSED LIGHT POLE, AS SHOWN ON THE PLANS. THE LIGHT POLE UNIT PRICE SHALL INCLUDE THIS ITEM AND WORK.
- THE OFFSET DISTANCE FOR THE LIGHT POLES SHALL BE THE DISTANCE FROM THE EDGE OF PAVEMENT TO THE CENTER OF THE POLE FOUNDATION.
- ALL LIGHT POLE FOUNDATIONS SHALL BE CONCRETE.
- TRENCHED CABLES SHALL RUN IN A STRAIGHT LINE BETWEEN TERMINAL POINTS WHERE FEASIBLE, OR UNLESS SHOWN ON THE PLANS. TO PREVENT EROSION OF THE EMBANKMENTS INVOLVING HIGH FILLS AND STEEP SIDE SLOPES, THE CONTRACTOR SHALL NOT TRENCH DIRECTLY FROM POLE TO POLE. RATHER THE TRENCH SHALL EXTEND FROM THE POLE STRAIGHT DOWN THE SIDE SLOPE, RUN ALONG THE TOE OF THE SLOPE, AND THEN STRAIGHT UP THE SIDE SLOPE TO THE NEXT POLE. THE CONTRACTOR MAY BE DIRECTED BY THE ENGINEER TO USE THIS PROCEDURE AT ADDITIONAL LOCATIONS, IF FIELD CONDITIONS WARRANT SUCH TREATMENT.
- ALL CONDUIT TO BE TRENCHED UNDER PROPOSED PAVEMENT SHALL BE INSTALLED AT A MINIMUM DEPTH OF 42" TO AVOID CONFLICTS WITH UNDERGROUND APPURTENANCES. UNIT DUCT SHALL BE PLACED A MINIMUM DEPTH OF 30" BELOW THE FLOWLINE OF ROADSIDE DRAINAGE DITCHES AND MINIMUM DEPTH OF 24" BELOW THE FINAL ELEVATION OF OTHER LOCATIONS.
- PROPOSED LUMINAIRES SHALL BE 480 VOLTS.
- THE LIGHT POLE LUMINAIRES SHALL BE FACTORY COATED DARK BRONZE TO MATCH THEIR ASSOCIATED POLE.
- LIGHT POLE SHAFTS, TRANSFORMER BASE AND ALL MOUNTING HARDWARE SHALL HAVE A DARK BRONZE POWDER COAT.
- THE REMOVED LIGHT POLE SHALL REMAIN DEPARTMENT PROPERTY.

DETECTOR LOOP REQUIREMENTS AND CALCULATIONS FOR HORSESHOE LAKE ROAD AND I-255 RAMPS 3 AND 4

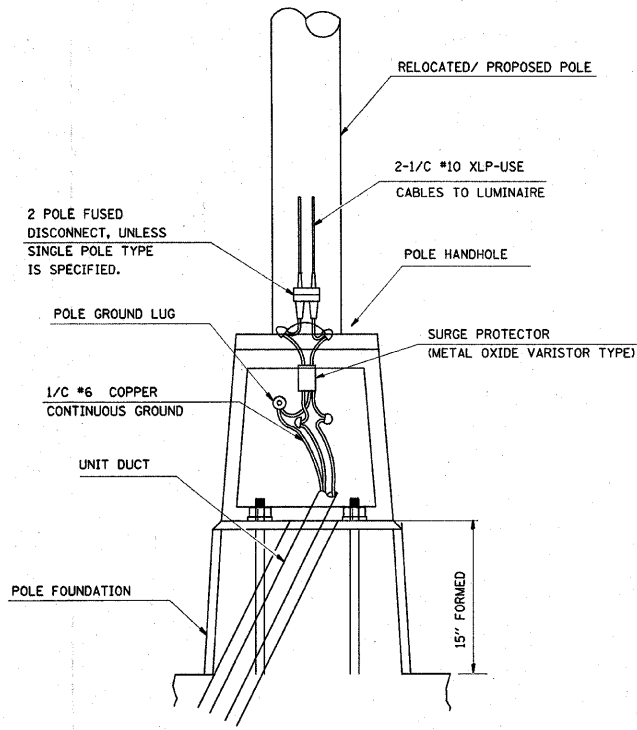
LOOP	PHASE (Ø)	LOOP SIZE(FT)	REQUIRED # OF TURNS	CALCULATED INDUCTANCE MICROHENRIES (µH)	CALCULATED RESISTANCE OHMS (Ω)
1. EB CCO A	6	6 X 6	7	526.3	4.1
2. EB CCO B	6	6 X 6	7	524.2	4.0
3. EB LT CD	1	6 X 50 0	3-6-3	825.6	2.7
4. EB THRU CD	6	6 X 50 0	3-6-3	833.1	2.7
5. WB CCO	2	6 X 6	7	431.8	1.9
6. WB CCO	2	6 X 6	7	429.0	1.9
7. WB THRU CD	2	6 X 50 0	3-6-3	802.5	1.6
8. WB RT CD	2	6 X 50 0	3-6-3	795.0	1.8
9. SB CCO A	6	6 X 6	7	460.8	2.8
10. SB CCO B	6	6 X 6	7	460.8	2.8
11. SB LT CD	6	6 X 50 0	3-6-3	810.5	2.8
12. SB RT CD	6	6 X 50 0	3-6-3	831.5	2.8

THE ABOVE VALUES ARE CALCULATED OF COMBINED LOOP AND LEAD-IN INDUCTANCE AND RESISTANCE. ACTUAL MEASURED VALUES SHOULD BE WITHIN +/- 20% OF THESE VALUES.

DETECTOR LOOP REQUIREMENTS AND CALCULATIONS FOR HORSESHOE LAKE ROAD AND EASTPORT PLAZA DRIVE/FOURNIE LANE

LOOP	PHASE (Ø)	LOOP SIZE(FT)	REQUIRED # OF TURNS	CALCULATED INDUCTANCE MICROHENRIES (µH)	CALCULATED RESISTANCE OHMS (Ω)
1. EB CCO A	6	6 X 6	7	485.9	3.1
2. EB CCO B	6	6 X 6	7	483.1	3.1
3. EB CCO C	6	6 X 6	7	480.0	3.0
4. EB LT CD	1	6 X 50 0	3-6-3	825.6	2.5
5. EB THRU CD	6	6 X 50 0	3-6-3	822.5	2.4
6. EB RT CD	6	6 X 50 0	3-6-3	825.8	2.5
7. NB LT CD	4	6 X 50 0	3-6-3	858.8	3.2
8. NB THRU CD	4	6 X 50 0	3-6-3	855.3	3.2
9. NB RT CD	4	6 X 50 0	3-6-3	854.4	3.1
10. WB CCO	2	6 X 6	7	487.3	3.2
11. WB LT CD	5	6 X 50 0	3-6-3	826.3	2.5
12. WB THRU CD	2	6 X 50 0	3-6-3	823.8	2.5
13. SB THRU CD	2	6 X 50 0	3-6-3	798.0	1.9
14. SB RT CD	2	6 X 50 0	3-6-3	421.8	1.1

THE ABOVE VALUES ARE CALCULATED OF COMBINED LOOP AND LEAD-IN INDUCTANCE AND RESISTANCE. ACTUAL MEASURED VALUES SHOULD BE WITHIN +/- 20% OF THESE VALUES.



POLE BASE MOUNTING AND WIRING

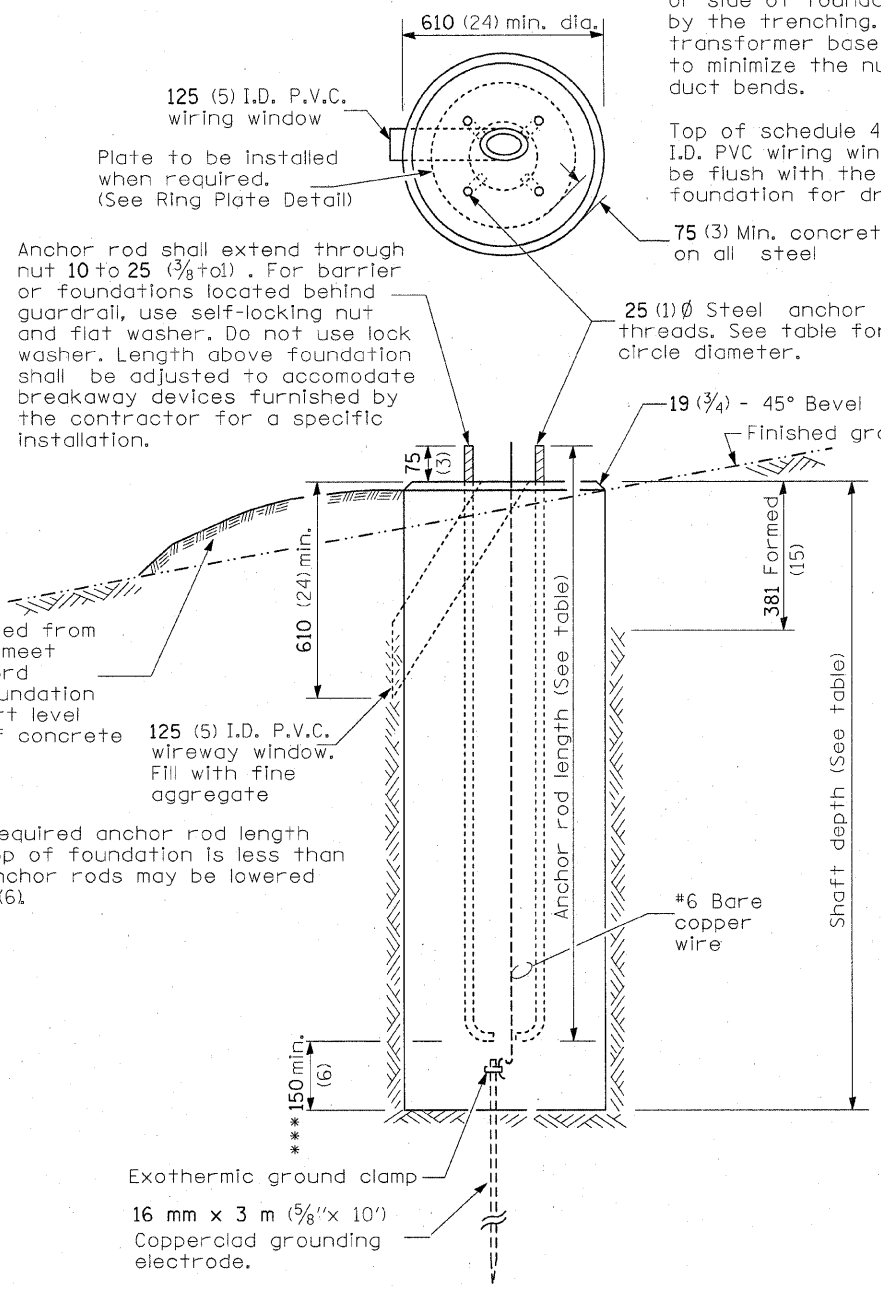
NOTE: A. ON POLES WITH TWO LUMINAIRES, EACH LUMINAIRE SHALL HAVE A SEPARATE FUSED DISCONNECT & ASSOCIATED CABLES TO EACH LUMINAIRE.

CONCRETE FOUNDATION				
LIGHT POLE MOUNTING HEIGHT	BOLT CIRCLE DIAMETER	SHAFT DIAMETER	SHAFT DEPTH	ANCHOR ROD LENGTH*
9.1 m (30')	292 mm (11 1/2")	610mm (24")	1.52 m (5'-0")	1.45 m (4'-9")
9.4 m - 10.7 m (31'-35')	292 mm (11 1/2")	610mm (24")	1.67 m (5'-6")	1.60 m (5'-3")
10.9 m - 12.2 m (36'-40')	381 mm (15")	610mm (24")	1.83 m (6'-0")	1.75 m (5'-9")
12.5 m - 13.7 m (41'-45')	381 mm (15")	610mm (24")	1.98 m (6'-6")	1.90 m (6'-3")
14.0 m - 15.2 m (46'-50')	381 mm (15")	610mm (24")	2.13m (7'-0")	2.00 m (6'-9")

* Length does not include 100 (4)hook
** 220 mm x 2.44 m (8 3/8" x 8'-0") for Twin luminaires

Notes:

All foundations are designed to be located on slopes not exceeding 2:1 where soils have an unconfined compressive strength of at least 1.0 TSF. The contractor shall verify the soil strength during drilling for concrete foundations and notify the engineer if other conditions are encountered.

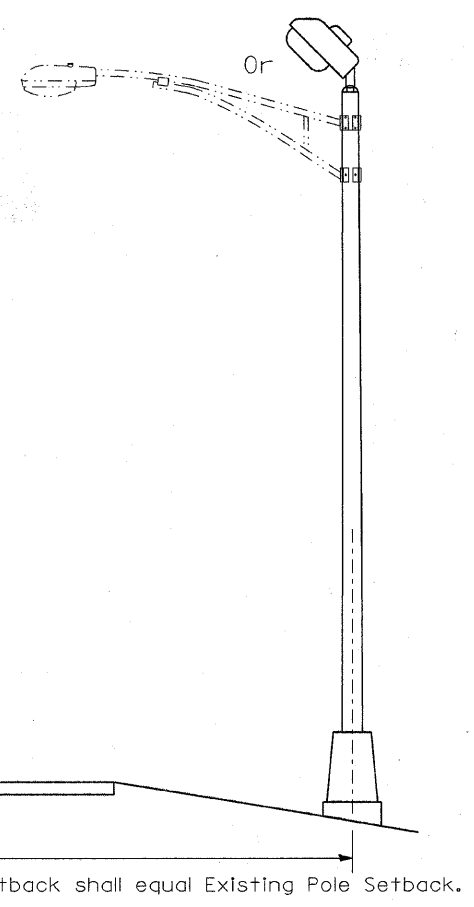


Notes:

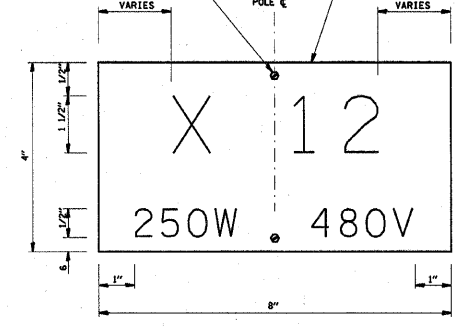
Wireway may be on front, back, or side of foundation as required by the trenching. Place door of transformer base on wireway side to minimize the number of unit duct bends.

Top of schedule 40 PVC 125 (5) I.D. PVC wiring window, shall be flush with the top of foundation for drainage.

25 (1) Ø Steel anchor rod with 230 (9) of threads. See table for the required bolt circle diameter.



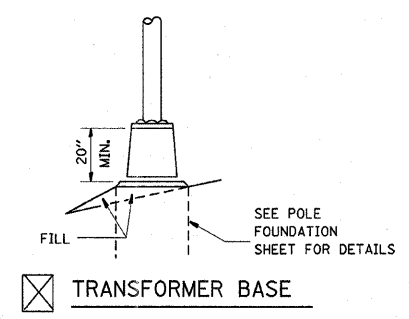
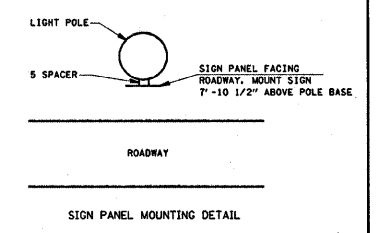
3/4" STAINLESS STEEL SELF-TAPPING SCREWS WITH 13/64" STAINLESS STEEL SPACERS BEHIND PANEL. HOLES FOR MOUNTING SHALL BE DRILLED ON POLE & ONLY (2-SCREWS PER PANEL).
18 GA DARK BRONZED MATTE FINISH ALUMINUM PLATE.



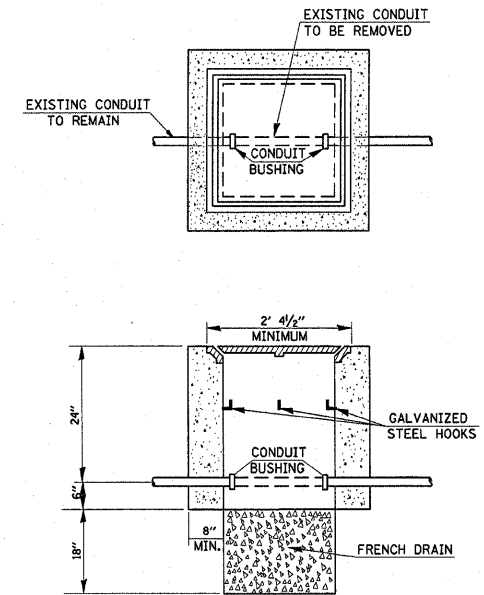
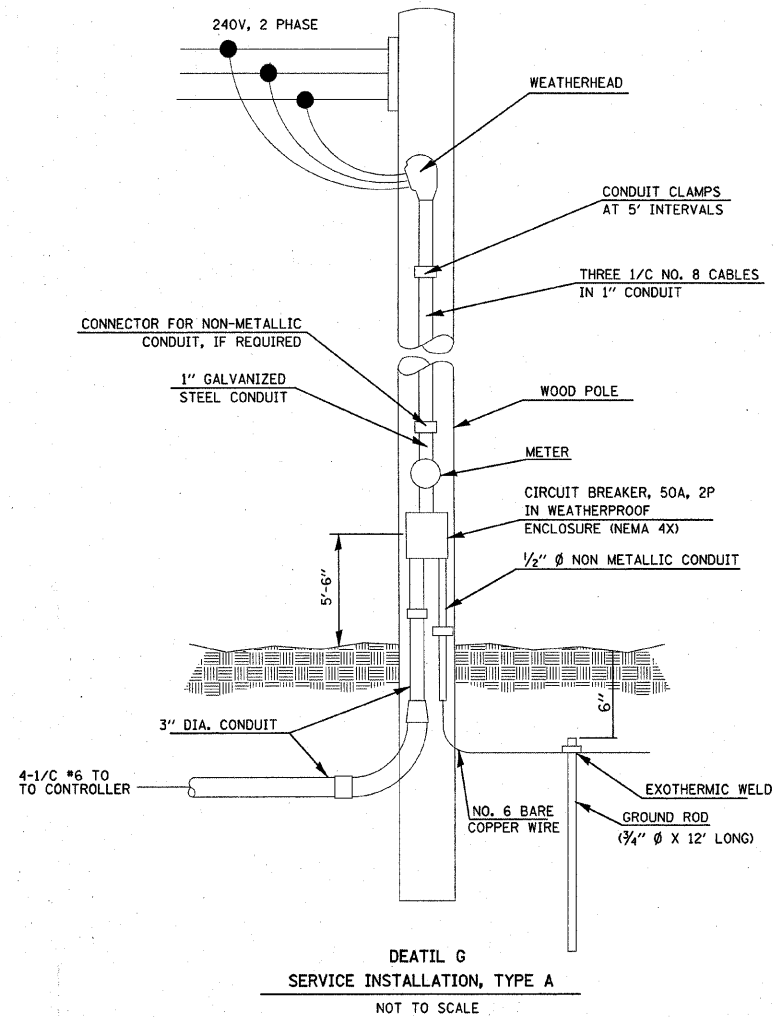
TYPICAL POLE IDENTIFICATION PANEL

SIGN PANEL DETAIL

NOTE: ALL LETTERS AND NUMERALS SHALL BE SERIES C GOTHIC, NON-REFLECTIVE WHITE PREMIUM QUALITY ENAMEL.
SIGN INFORMATION SHALL BE IN ACCORDANCE WITH LIGHT POLE NUMBERING & MOUNTAGES SHOWN IN THE SCHEDULE.
ON BRIDGE PARAPET MOUNTED POLES, MOUNT ONE SIGN PANEL FACING ROADWAY 610 ABOVE POLE BASE.



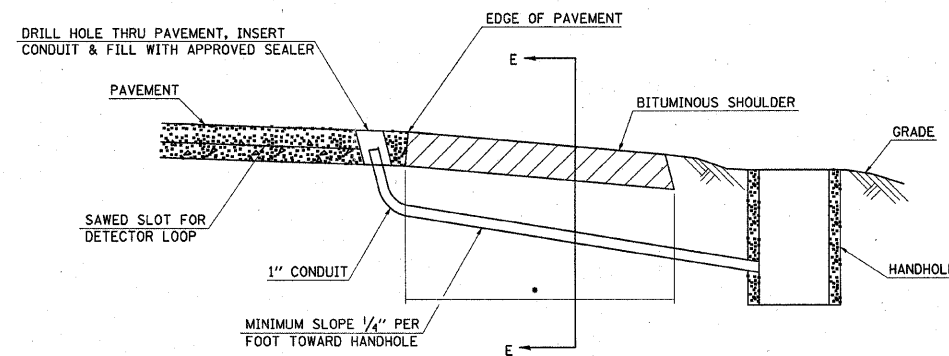
"INSTALL AND ORIENT ARM BRACKET OVER POLE TENON AND FIRMLY HAND TIGHTEN THE TWO SET SCREWS. USE THIRD HOLE IN ARM BRACKET AS A GUIDE TO DRILL A 8.3 (2 3/16) DIAMETER HOLE THROUGH TENON. INSTALL AND TIGHTEN SELF-TAPPING SCREW. TIGHTEN SET SCREWS AN ADDITIONAL (1/4 TO 3/8) TURN WITH HEX KEY (NOT PROVIDED). INSTALL LOCKNUTS ON SET SCREWS IF THREADED PROJECTION ALLOWS."



DETAIL I
HANDHOLE TO INTERCEPT EXISTING CONDUIT *
* NOT A PAY ITEM

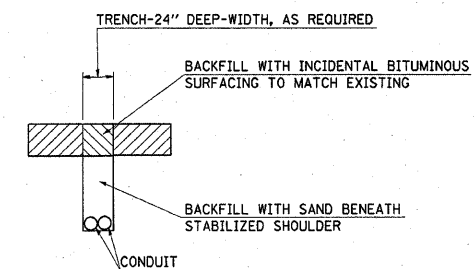
NOTES:

1. REMOVAL OF EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHING SHALL BE INCIDENTAL TO THE HANDHOLE.
2. THE CONTRACTOR SHALL BE PAID TO LOCATE THE CONDUIT UNDER OTHER PROVISIONS OF THIS CONTRACT. SEE LOCATING UNDERGROUND CABLE IN THE STANDARD SPECIFICATIONS.



• LIMITS OF "TRENCH AND BACKFILL FOR ELECTRICAL WORK (SPECIAL)"

DETAIL H
TRENCH AND BACKFILL FOR ELECTRICAL WORK (SPECIAL)
(NO SCALE)



SEC. E-E

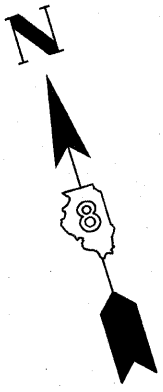
FILE NAME =	USER NAME = keplard	DESIGNED -	REVISD -
et\projects\view\ed09707\electrical\sp1009707.dgn		DRAWN -	REVISD -
		CHECKED -	REVISD -
		DATE -	REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC SIGNAL DETAILS

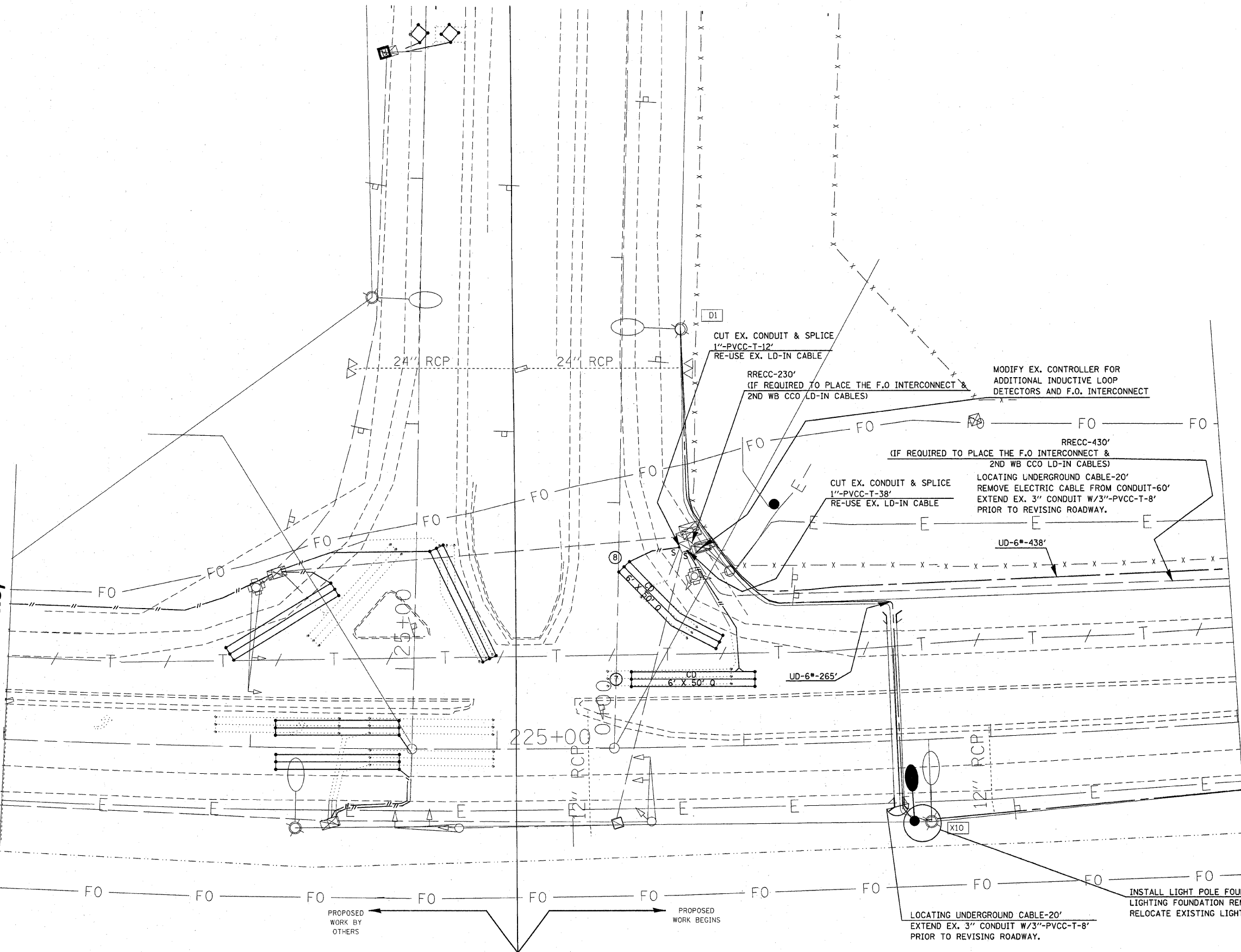
SCALE: _____ SHEET NO. 4 OF 9 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9111	73-15TS	MADISON	64	38
				CONTRACT NO. 76B22
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT				

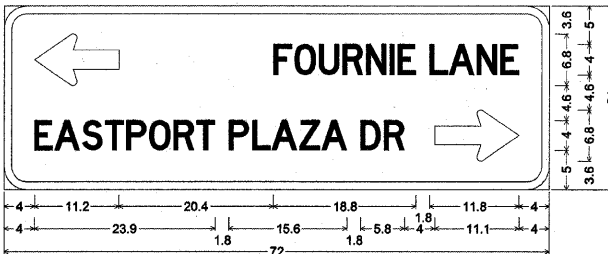
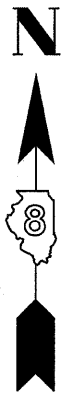


**MATCHLINE STA. 223+00
(PROPOSED WORK BY OTHERS)**

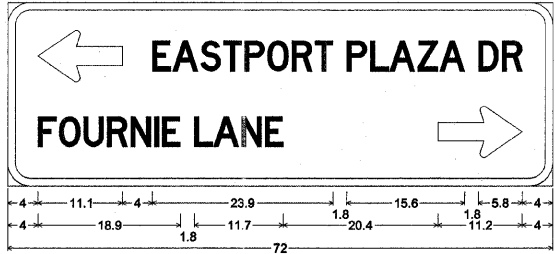
MATCHLINE STA. 228+00



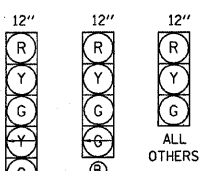
FILE NAME = c:\projects\view\ed09707\electrical\sep09707a.dgn PLOT SCALE = 28.0000' / IN. PLOT DATE = 3/17/2008	USER NAME = keplerc1 DESIGNED - --- DRAWN - --- CHECKED - --- DATE - ---	REVISED - --- REVISED - --- REVISED - --- REVISED - ---	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		I-255 RAMPS 3 & 4 LIGHTING AND TRAFFIC SIGNAL PLAN		F.A.L. RTE. 9111	SECTION 73-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 39	
					SCALE: _____	SHEET NO. 5 OF 9 SHEETS	STA. _____ TO STA. _____	FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT				
									CONTRACT NO. 76B22			



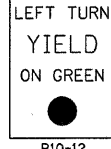
3.0" Radius, 1.0" Border, White on Green;
 Standard Arrow Custom 11.1" X 8.8" 180°; [FOURNIE LANE] D 45% spacing;
 [EASTPORT PLAZA DR] D 45% spacing; Standard Arrow Custom 11.1" X 8.8" 0°;
STREET NAME SIGN
 DETAIL E
 NOT TO SCALE



3.0" Radius, 1.0" Border, White on Green;
 Standard Arrow Custom 11.1" X 8.8" 180°; [EASTPORT PLAZA DR] D 45% spacing;
 [FOURNIE LANE] D 45% spacing; Standard Arrow Custom 11.1" X 8.8" 0°;
STREET NAME SIGN
 DETAIL F
 NOT TO SCALE



PROPOSED TRAFFIC
 SIGNAL FACES



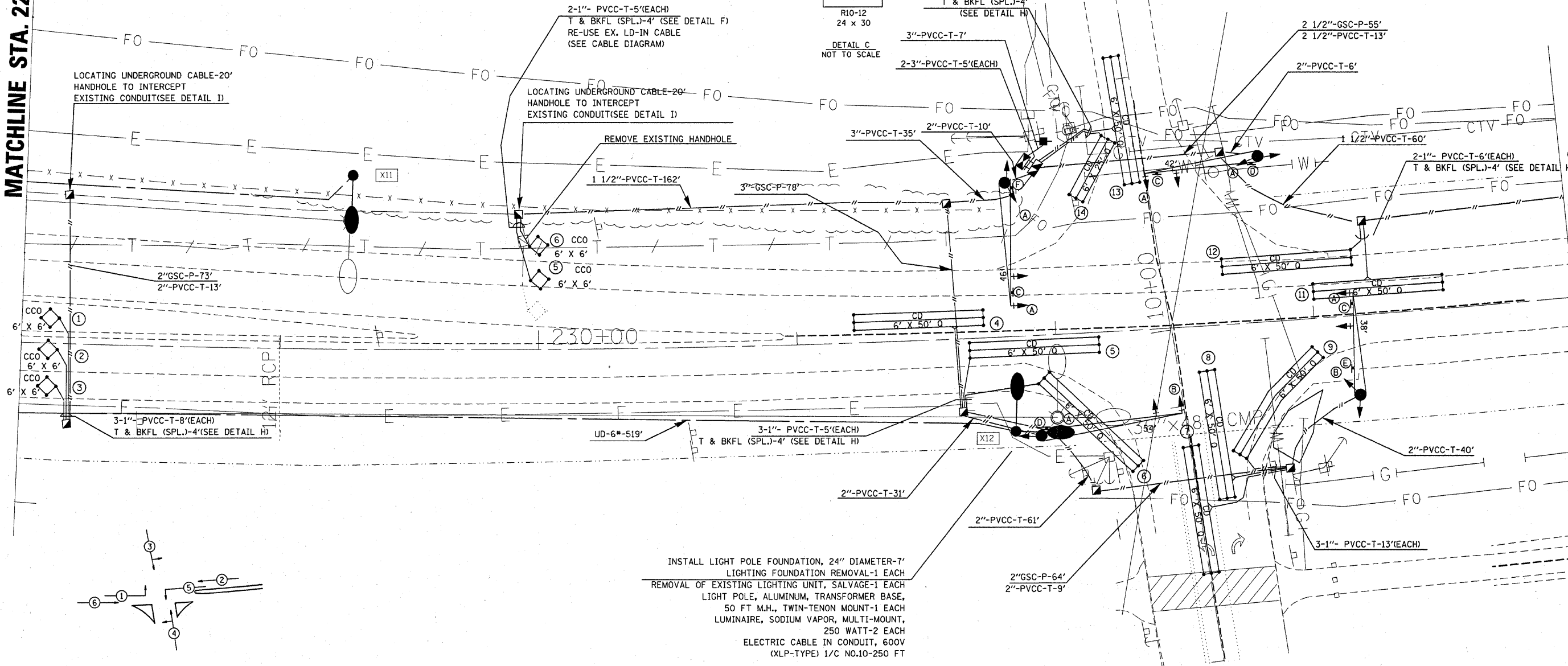
LEFT TURN
 YIELD
 ON GREEN
 R10-12
 24 X 30
 DETAIL C
 NOT TO SCALE



3.0" Radius, 1.0" Border, White on Green;
 [HORSESHOE] D; [LAKE ROAD] D;
STREET NAME SIGN
 DETAIL D
 NOT TO SCALE

MATCHLINE STA. 228 + 00

MATCHLINE STA. 234 + 00



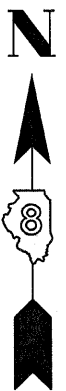
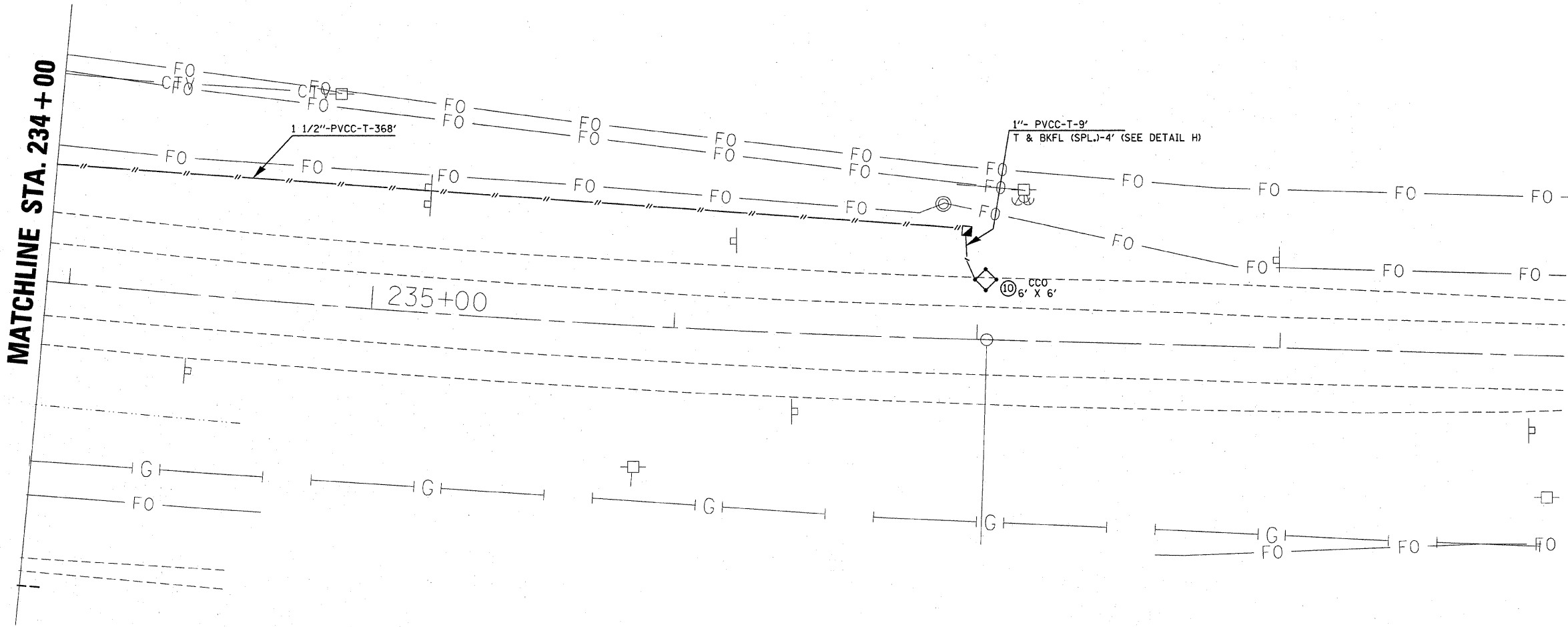
INSTALL LIGHT POLE FOUNDATION, 24" DIAMETER-7'
 LIGHTING FOUNDATION REMOVAL-1 EACH
 REMOVAL OF EXISTING LIGHTING UNIT, SALVAGE-1 EACH
 LIGHT POLE, ALUMINUM, TRANSFORMER BASE,
 50 FT M.H., TWIN-TENON MOUNT-1 EACH
 LUMINAIRE, SODIUM VAPOR, MULTI-MOUNT,
 250 WATT-2 EACH
 ELECTRIC CABLE IN CONDUIT, 600V
 (XLP-TYPE) 1/C NO.10-250 FT

FULL-ACTUATED CONTROLLER TYPE IV CABINET W/ TRANSCEIVER

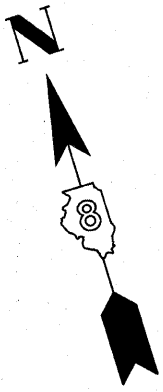
PHASE DESIGNATION DIAGRAM

PHASE	1	2	3	4	5	6	7	8
MOVEMENT	→	←	↔	↔	↔	↔	NOT	NOT
CONCURRENT MOVEMENT PERMITTED	5 OR 6	5 OR 6	NONE	NONE	1 OR 2	1 OR 2	USED	USED

MATCHLINE STA. 234+00

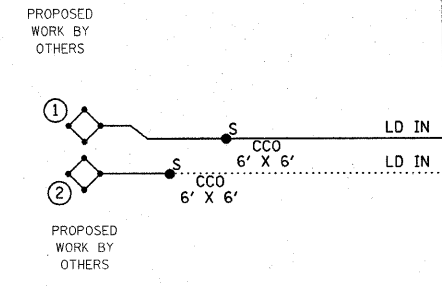
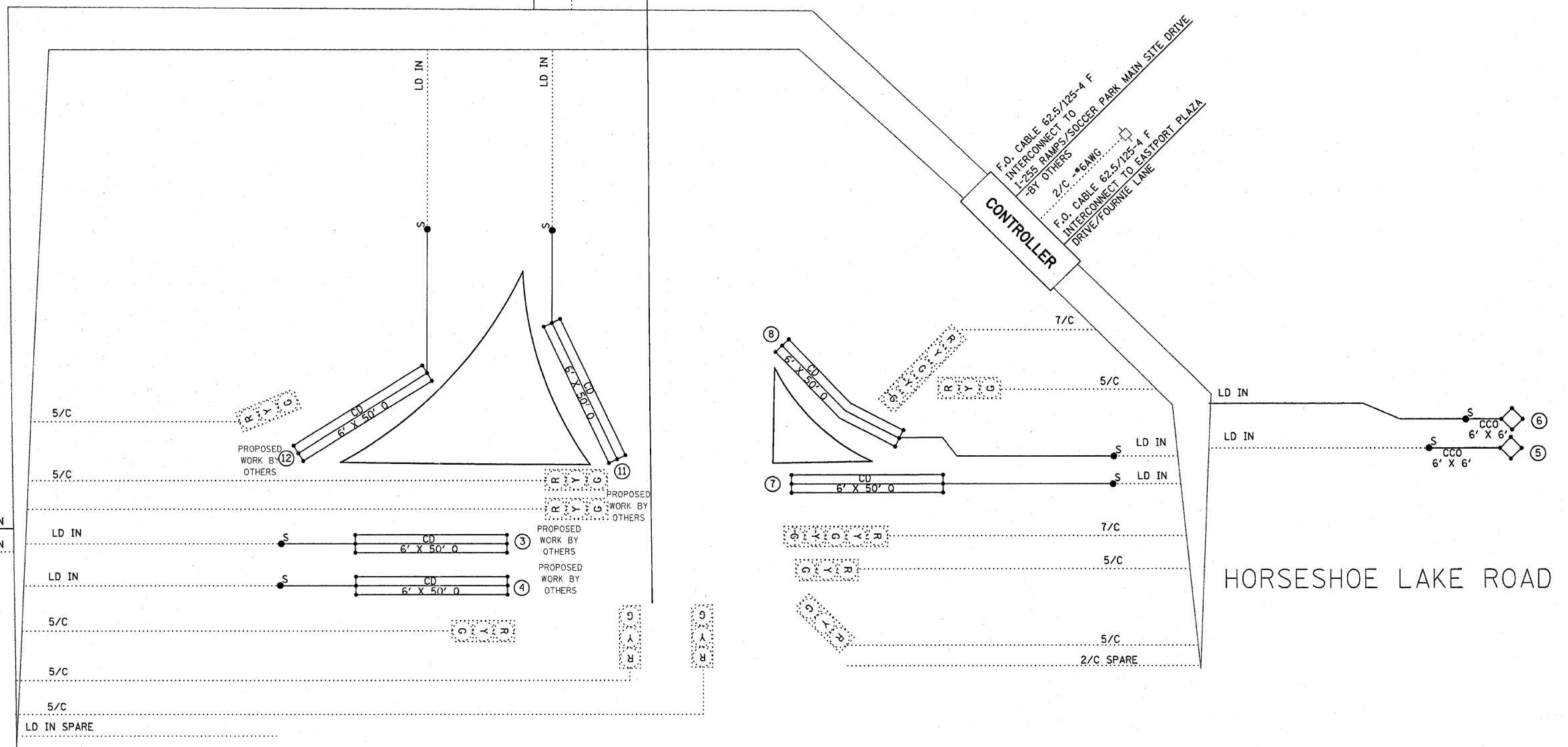
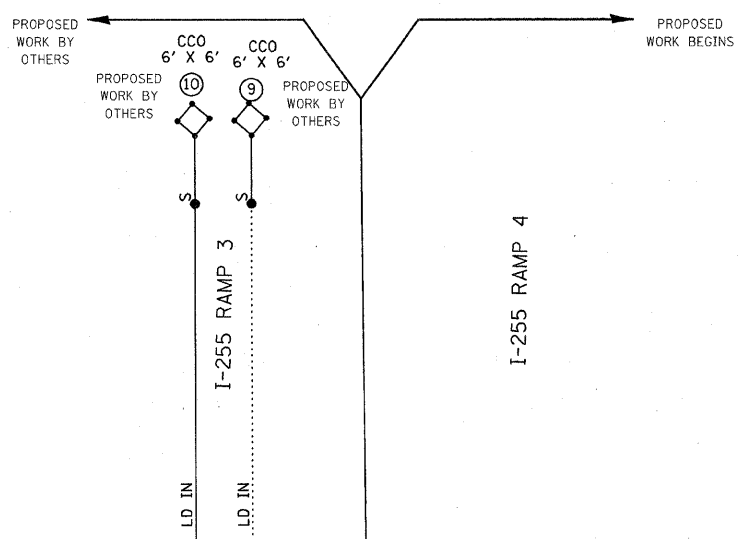


FILE NAME = c:\projects\view\ed09707\electrical\tp109707.dgn	USER NAME = keplerc1	DESIGNED - ---	REVISED - ---	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EASTPORT PLAZA DRIVE /FOURNIE LANE LIGHTING AND TRAFFIC SIGNAL PLAN	F.A.U. RTE. 9111	SECTION 73-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 41
	PLOT SCALE = 20,0000' / IN.	CHECKED - ---	REVISED - ---			SCALE: _____	SHEET NO. 7 OF 9 SHEETS	STA. _____ TO STA. _____	FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT	
PLOT DATE = 3/17/2008	DATE - -----	REVISED - ---	REVISED - ---			CONTRACT NO. 76B22				

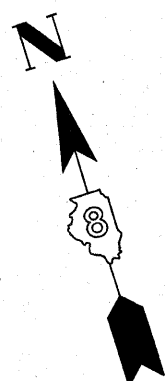
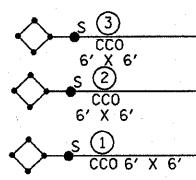
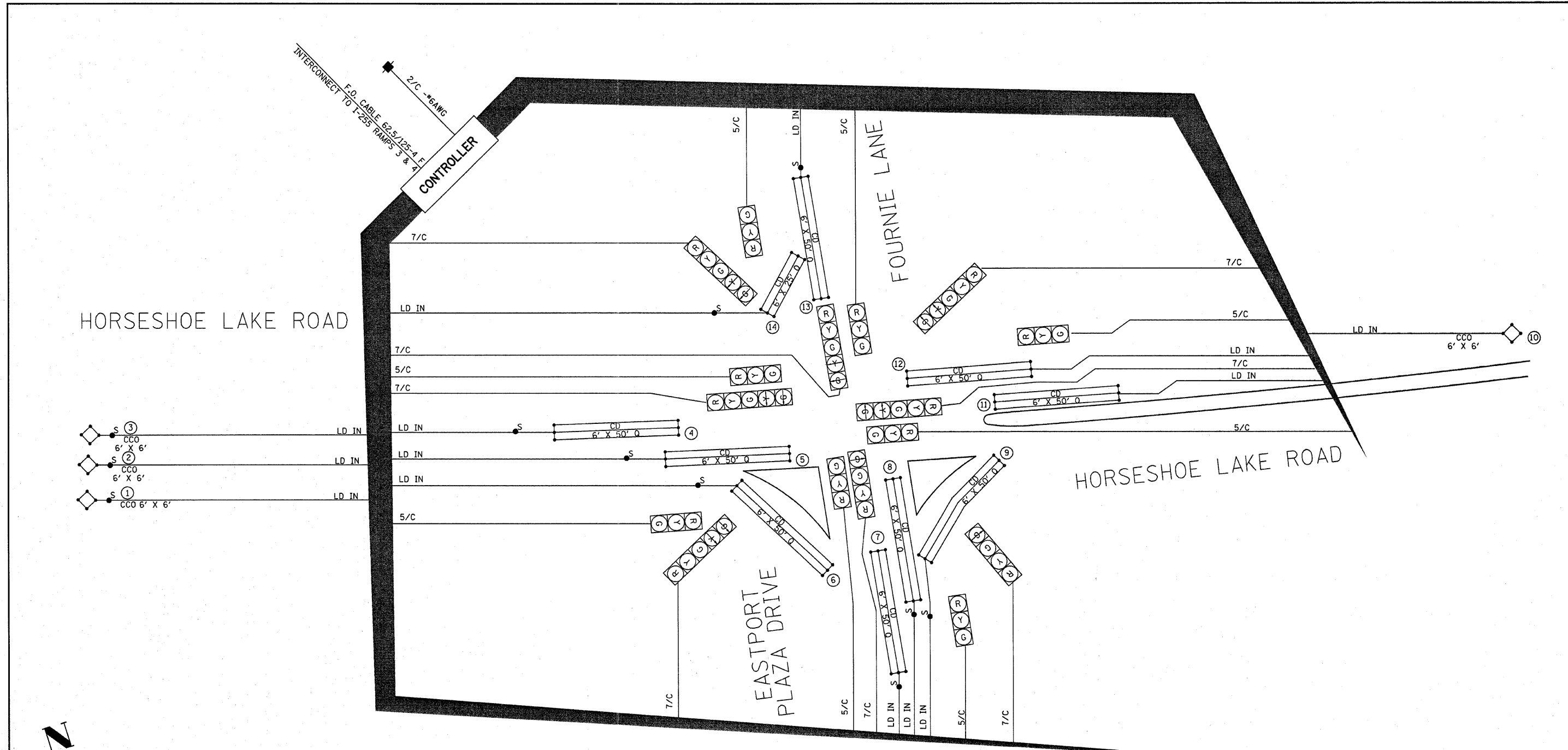


CABLE DIAGRAM LEGEND

- ELECTRIC CABLE IN CONDUIT
- - - EXISTING ELECTRIC CABLE IN CONDUIT
- S CABLE SPLICE (SEE GENERAL NOTES)
- 2/C INDICATES NUMBER OF CONDUCTORS IN CABLE
- LD IN ELECTRIC CABLE LEAD-IN, 1 PAIR
- CD CALL DELAY
- CCO CALL CARRY OVER (SEE GENERAL NOTES)
- SERVICE INSTALLATION
- *6 INDICATES AMERICAN WIRE GAUGE (AWG) SIZE 6 CONDUCTORS



FILE NAME =	USER NAME = keplerc1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-255 RAMPS 3 & 4 CABLE DIAGRAM	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\projects\view\ed09707\electrical\tp109707e.dgn		DRAWN -	REVISED -			9111	73-15TS	MADISON	64	42	
PLOT SCALE = 28.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 76B22					
PLOT DATE = 3/17/2008		DATE -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					
						SCALE: _____	SHEET NO. 8 OF 9 SHEETS	STA. _____ TO STA. _____			



- CABLE DIAGRAM LEGEND**
- ELECTRIC CABLE IN CONDUIT
 - EXISTING ELECTRIC CABLE IN CONDUIT
 - S CABLE SPLICE (SEE GENERAL NOTES)
 - 2/C INDICATES NUMBER OF CONDUCTORS IN CABLE
 - LD IN ELECTRIC CABLE LEAD-IN, 1 PAIR
 - CD CALL DELAY
 - CCO CALL CARRY OVER (SEE GENERAL NOTES)
 - SERVICE INSTALLATION
 - *6 INDICATES AMERICAN WIRE GAUGE (AWG) SIZE 6 CONDUCTORS

FILE NAME =	USER NAME = keple-cel	DESIGNED -	REVISED -
ca:\projects\view\ed09707\electrical\sp109707a.dgn		DRAWN -	REVISED -
PLOT SCALE = 28.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = 3/17/2008		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EASTPORT PLAZA DRIVE /FOURNIE LANE
CABLE DIAGRAM**

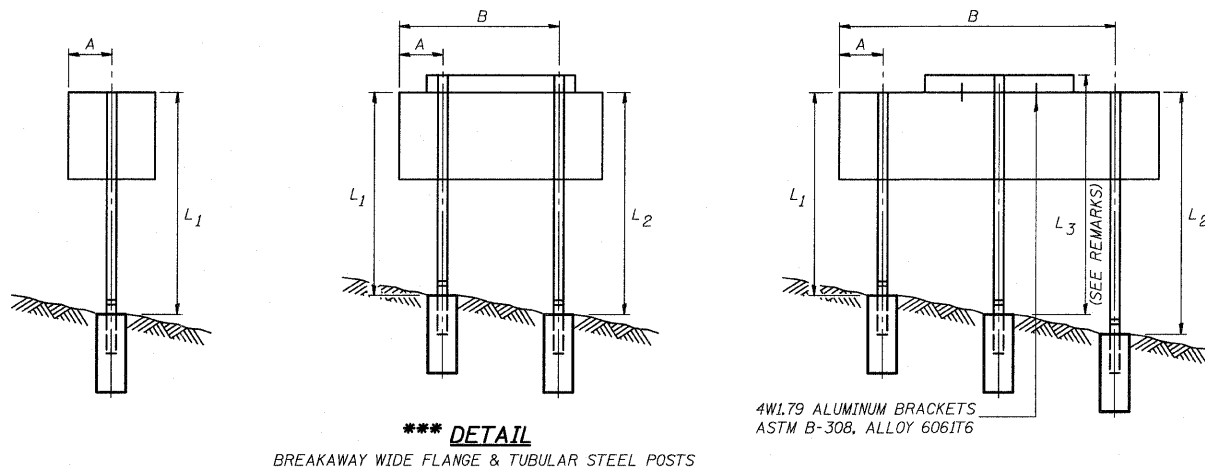
SCALE: _____ SHEET NO. 9 OF 9 SHEETS STA. _____ TO STA. _____

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9111	73-15TS	MADISON	64	43
FED. ROAD DIST. NO. = ILLINOIS FED. AID PROJECT			CONTRACT NO. 76B22	

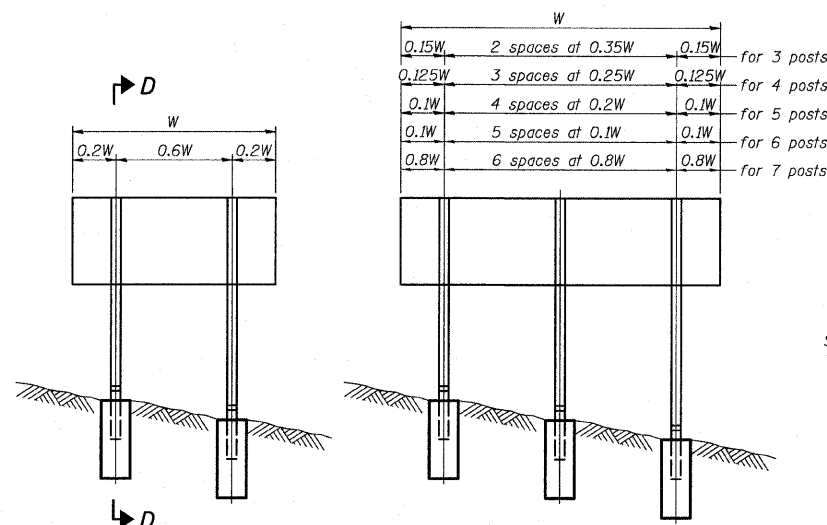
SIGN PANEL SCHEDULE														
LOCATION	PROPOSED OFFSET *	SIGN WIDTH	DEPTH	NUMBER OF POSTS	POST TYPE AND SIZE **	A ***	B ***	L1 ***	L2 ***	REMOV SIGN PANEL T3 (SQ FT)	RELOC SIGN PANEL T3 (SQ FT)	STR STL SIN SUP BA (POUND)	CONC FOUNDATION (CU YD)	COMMENTS
HORSESHOE LAKE ROAD														
LT. STA. 226+20	30.0'	12	12	2	W10x26	2.4	9.6	20'-11"	21'-4"	144	144	1244	2.5	EXISTING SIGN PANEL
RT. STA. 230+60	18.0'	10	10	2	W10x26	2	8	24'-2"	24'-10"	100	100	1419	2.5	EXISTING SIGN PANEL
LT. STA. 235+16	20.0'	10	10	2	W10x22	2	8	20'-5"	20'-9"	100	100	1029	2.4	EXISTING SIGN PANEL
TOTAL										344	344	3692	7.4	

* OFFSET MEASURED FROM EDGE OF PAVEMENT TO NEAR EDGE OF SIGN PANEL

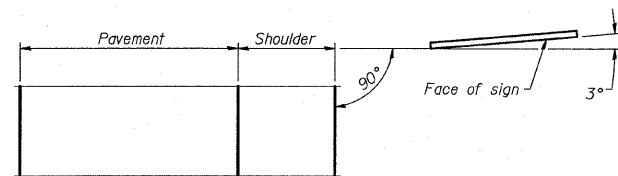
** W: BREAKAWAY WIDE FLANGE
TS: BREAKAWAY TUBULAR STEEL



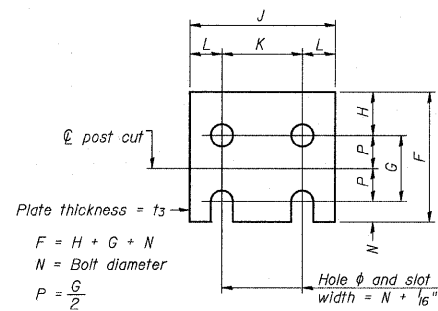
FILE NAME =	USER NAME = #USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BREAK-AWAY WIDE FLANGE STEEL SIGN POST SCHEDULE	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
H:\P\25004\Technical Production\Civil\W07	7\Microstation\signoo2.dgn	DRAWN -	REVISED -			9111	73-15TS	MADISON	64	44	
PLOT SCALE = 10.0000' / IN.	CHECKED -	REVISED -	SCALE:			SHEET NO. 1 OF 3 SHEETS	STA.	TO STA.	CONTRACT NO. 76B22		
PLOT DATE = 3/14/2008	DATE -	REVISED -	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT								



ELEVATION

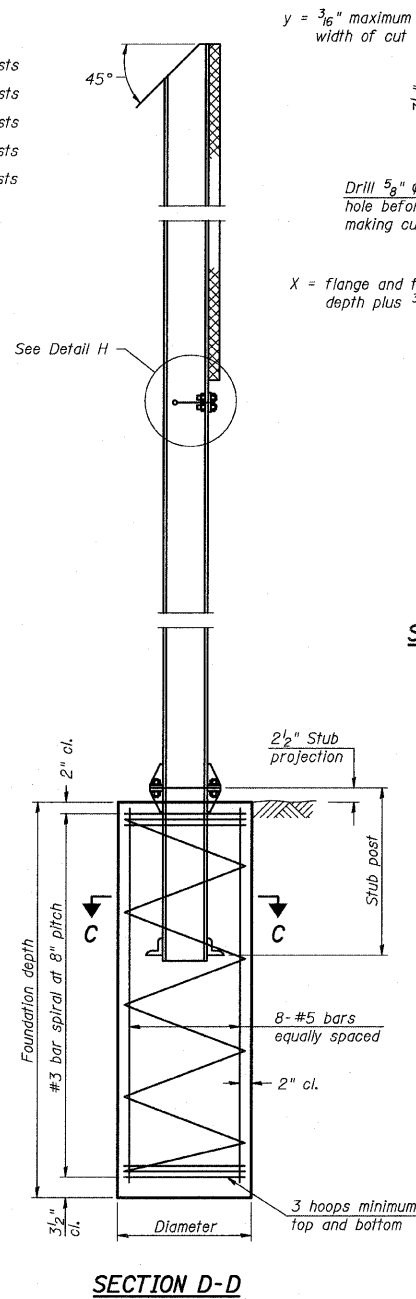


LOCATION SKETCH

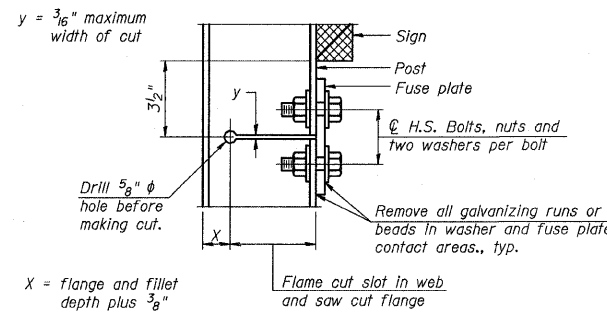


FUSE PLATE DETAIL
(Install with notches down.)

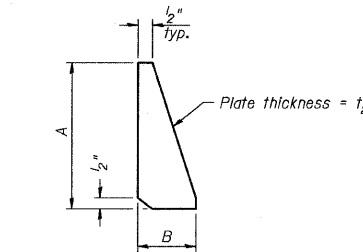
FUSE PLATE DATA			
N = Bolt Diameter	G	H	
1/2"	2"	1 1/8"	
5/8"	2 1/4"	1 1/4"	
3/4"	2 1/2"	1 3/8"	
7/8"	2 3/4"	1 1/2"	
1"	3"	1 5/8"	
1 1/8"	3 1/4"	1 3/4"	
1 1/4"	3 1/2"	1 7/8"	



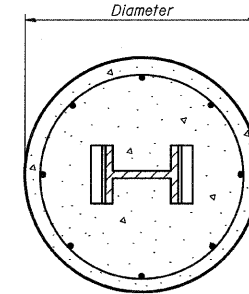
SECTION D-D



DETAIL H



STIFFENER PLATE DETAIL
(See table for dimensions.)



SECTION C-C

GENERAL NOTES

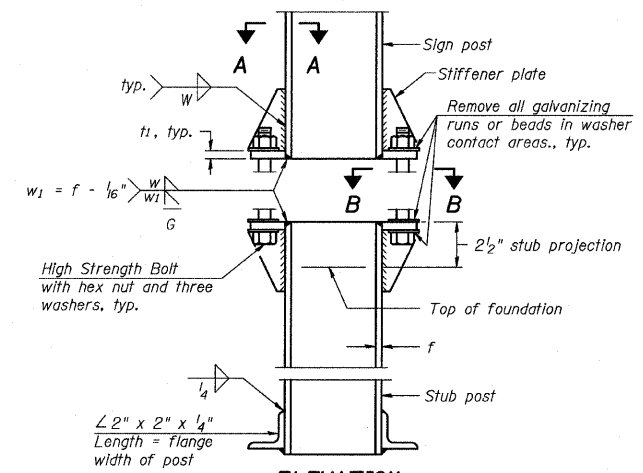
Posts shall be plumbed by using shims with post-to-stub post connection bolts snug tight only. Final tightening of all High Strength Bolts shall be in accordance with Article 727.05 and threads at the junction of the bolt and nut shall be burred or center punched to prevent the nut from loosening.

LOADING: 80 m.p.h. wind with 30% gust factor, normal to sign.

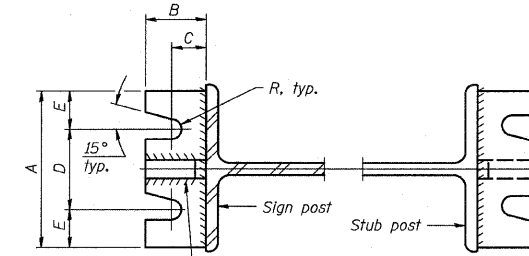
DESIGN STRESSES:
Structural steel - 20,000 p.s.i.
Reinforcing steel - 20,000 p.s.i.
Concrete - 1,400 p.s.i.
Footing soil pressure - 2,000 p.s.f.

After fabrication, the post, fuse plate and upper 6", min. of the stub post shall be hot-dip galvanized in accordance with AASHTO M111. All bolts, nuts and washers shall be hot-dip galvanized in accordance with AASHTO M232.

Work this sheet with Base Sheet BAW-A-2.

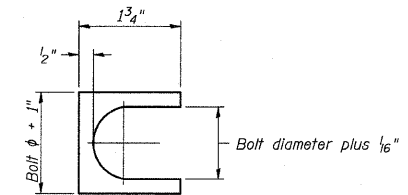


ELEVATION
SIGN POST & STUB POST



SECTION A-A

SECTION B-B



SHIM DETAIL

Furnish two 0.01" thick and two 0.03" thick stainless steel or brass (ASTM B36) shims per post.

NUMBER	REVISION	DATE

BAW-A-1

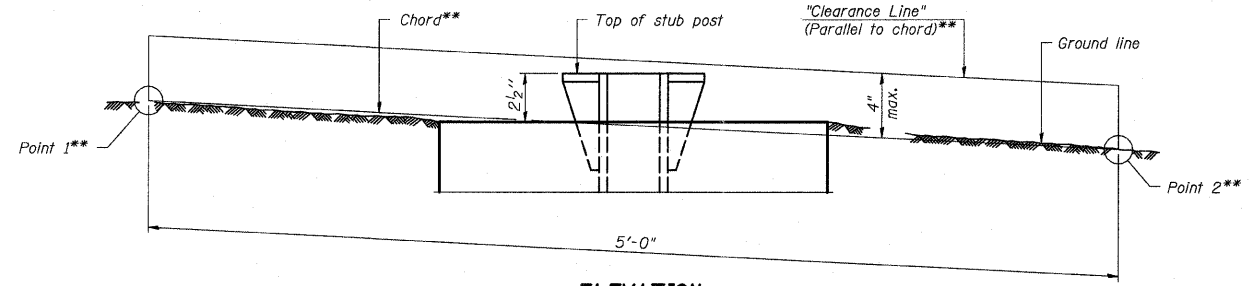
6/01/2007

FILE NAME = H:\VP\25004\Technical Production\Civil\W07\Microstation\signos2.dgn	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BREAK-AWAY WIDE FLANGE STEEL SIGN POST DETAILS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 10,0000' / IN.	CHECKED -	REVISED -	9111			73-15TS	MADISON	64	45	
PLOT DATE = 3/14/2008	DATE -	REVISED -	SCALE: SHEET NO. 2 OF 3 SHEETS STA. TO STA.			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		CONTRACT NO. 76B22		

POST	CONCRETE FOUNDATION TABLE							POST TO STUB POST CONNECTION DATA										FUSE PLATE DATA				
	Foundation			Reinforcement				Stub Post Length	Bolt Size	A	B	C	D	E	t ₁	t ₂	R	W	J	K	L	t ₃
	Diameter	Minimum Depth	Concrete (1) cu. yds.	Vertical Bars Length	Bar Diameter	Spirals Length	lbs. (2)															
W6x9	2'-0"	6'-0"	0.70	5'-9"	1'-8 1/2"	79'-0"	78	2'-3"	5/8" x 3 1/4"	6"	2 1/4"	1 1/4"	3 1/2"	1 1/4"	3/4"	1/2"	11/32"	1 1/4"	4"	2 1/4"	7/8"	1/4"
W6x15	2'-0"	6'-0"	0.70	5'-9"	1'-8 1/2"	79'-0"	78	2'-6"	5/8" x 3 3/4"	6"	2 1/4"	1 1/4"	3 1/2"	1 1/4"	3/4"	1/2"	11/32"	1 1/4"	6"	3 1/2"	1 1/4"	3/8"
W8x18	2'-0"	6'-0"	0.70	5'-9"	1'-8 1/2"	79'-0"	78	2'-6"	3/4" x 3 3/4"	6"	2 1/2"	1 3/8"	3 1/4"	1 3/8"	1"	1/2"	11/32"	5/16"	5 1/4"	2 3/4"	1 1/4"	3/8"
W10x22	2'-6"	6'-6"	1.18	6'-3"	2'-2 1/2"	105'-0"	92	3'-0"	3/4" x 3 3/4"	6"	2 1/2"	1 3/8"	3 1/4"	1 3/8"	1"	1/2"	11/32"	5/16"	5 3/4"	2 3/4"	1 1/2"	1/2"
W10x26	2'-6"	7'-0"	1.27	6'-9"	2'-2 1/2"	112'-0"	98	3'-0"	7/8" x 4"	7"	2 3/4"	1 1/2"	4"	1 1/2"	1"	3/4"	11/32"	3/8"	5 3/4"	2 3/4"	1 1/2"	5/8"
W12x26	2'-6"	7'-9"	1.41	7'-6"	2'-2 1/2"	119'-0"	107	3'-0"	7/8" x 4"	7"	2 3/4"	1 1/2"	4"	1 1/2"	1"	3/4"	11/32"	3/8"	6 1/2"	3 1/2"	1 1/2"	5/8"
W14x30	3'-0"	7'-3"	1.90	7'-0"	2'-8 1/2"	145'-0"	113	3'-0"	7/8" x 4"	7"	2 3/4"	1 1/2"	4"	1 1/2"	1"	3/4"	11/32"	3/8"	6 3/4"	3 1/2"	1 5/8"	1/2"
W14x38	3'-0"	8'-0"	2.09	7'-9"	2'-8 1/2"	153'-0"	122	3'-6"	1" x 4 1/2"	7 1/2"	3"	1 3/4"	4"	1 3/4"	1 1/4"	3/4"	11/32"	3/8"	6 3/4"	3 1/2"	1 5/8"	1/2"
W16x45	3'-0"	8'-6"	2.23	8'-3"	2'-8 1/2"	162'-0"	130	3'-6"	1" x 4 1/2"	7 1/2"	3"	1 3/4"	4"	1 3/4"	1 1/4"	3/4"	11/32"	3/8"	7"	3 1/2"	1 3/4"	1/2"

*Dimensional changes required for varying site conditions shall be approved by the Engineer.

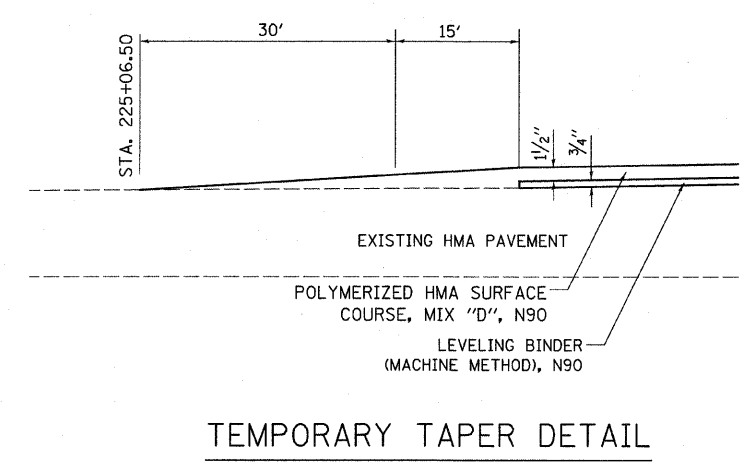
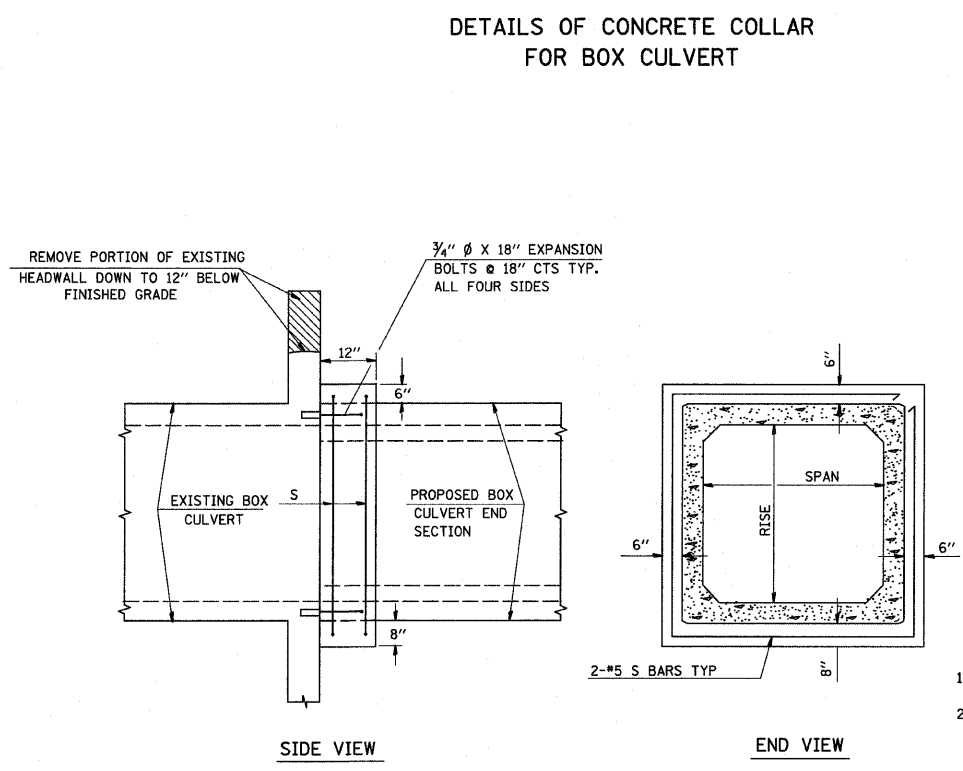
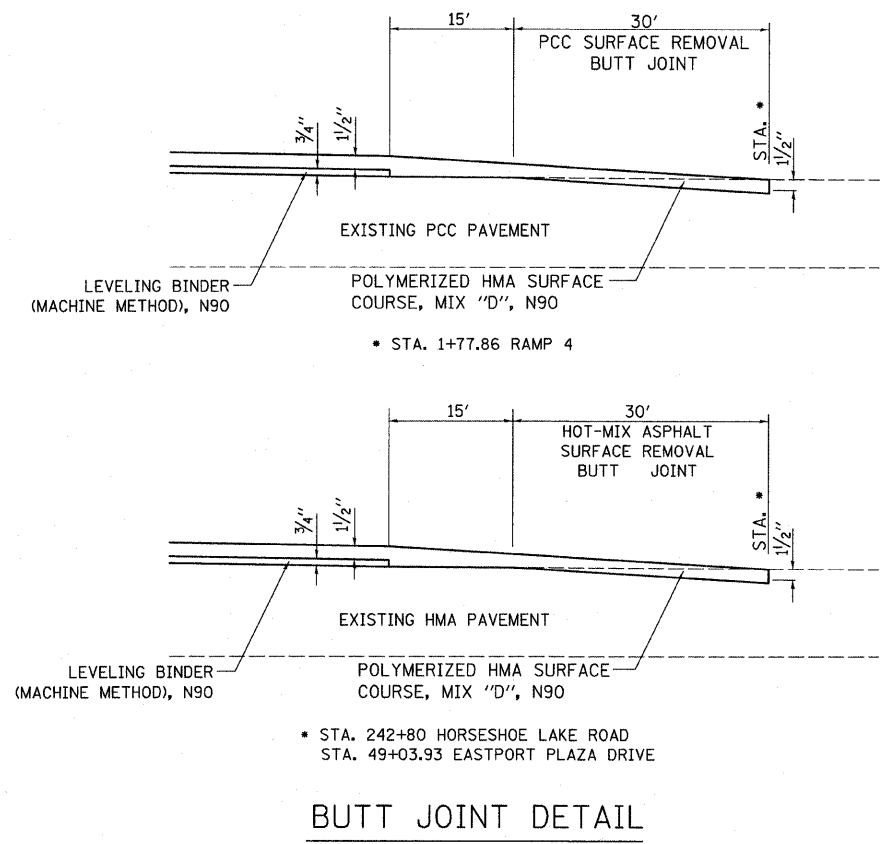
POST	FUSE PLATE BOLT SIZE																				
	Sign Height																				
	4'-0"	5'-0"	6'-0"	7'-0"	8'-0"	9'-0"	10'-0"	11'-0"	12'-0"	13'-0"	14'-0"	15'-0"	16'-0"	17'-0"	18'-0"	19'-0"	20'-0"	21'-0"	22'-0"	23'-0"	24'-0"
W6x9	1/2" x 1 1/2"	1/2" x 1 1/2"	1/2" x 1 1/2"	1/2" x 1 1/2"	---	---	---	---	---	---	---	---	---	---	---	---	---	---	---	---	---
W6x15	1/2" x 1 3/4"	1/2" x 1 3/4"	1/2" x 1 3/4"	5/8" x 2"	5/8" x 2"	3/4" x 2"	3/4" x 2"	3/4" x 2"	3/4" x 2"	---	---	---	---	---	---	---	---	---	---	---	---
W8x18	1/2" x 1 3/4"	1/2" x 1 3/4"	1/2" x 1 3/4"	5/8" x 2"	5/8" x 2"	3/4" x 2"	3/4" x 2"	3/4" x 2"	3/4" x 2"	---	---	---	---	---	---	---	---	---	---	---	---
W10x22	1/2" x 2"	1/2" x 2"	1/2" x 2"	5/8" x 2"	5/8" x 2"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	---	---	---	---	---	---	---	---	---	---	---	---
W10x26	1/2" x 2"	1/2" x 2"	1/2" x 2"	5/8" x 2 1/4"	5/8" x 2 1/4"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	---	---	---	---	---	---	---	---	---	---	---	---
W12x26	1/2" x 2"	1/2" x 2"	1/2" x 2"	5/8" x 2 1/4"	5/8" x 2 1/4"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	---	---	---	---	---	---	---	---	---	---	---	---
W14x30	1/2" x 2"	1/2" x 2"	1/2" x 2"	5/8" x 2 1/4"	5/8" x 2 1/4"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	---	---	---	---	---	---	---	---	---	---	---	---
W14x38	1/2" x 2"	1/2" x 2"	1/2" x 2"	5/8" x 2 1/4"	5/8" x 2 1/4"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	---	---	---	---	---	---	---	---	---	---	---	---
W16x45	---	1/2" x 2"	1/2" x 2"	5/8" x 2 1/4"	5/8" x 2 1/4"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	---	---	---	---	---	---	---	---	---	---	---	---



ELEVATION
GROUND LINE & STUB POST
** For all "Point 1" and "Point 2" locations, "Clearance Line" must be at or above top of stub post.

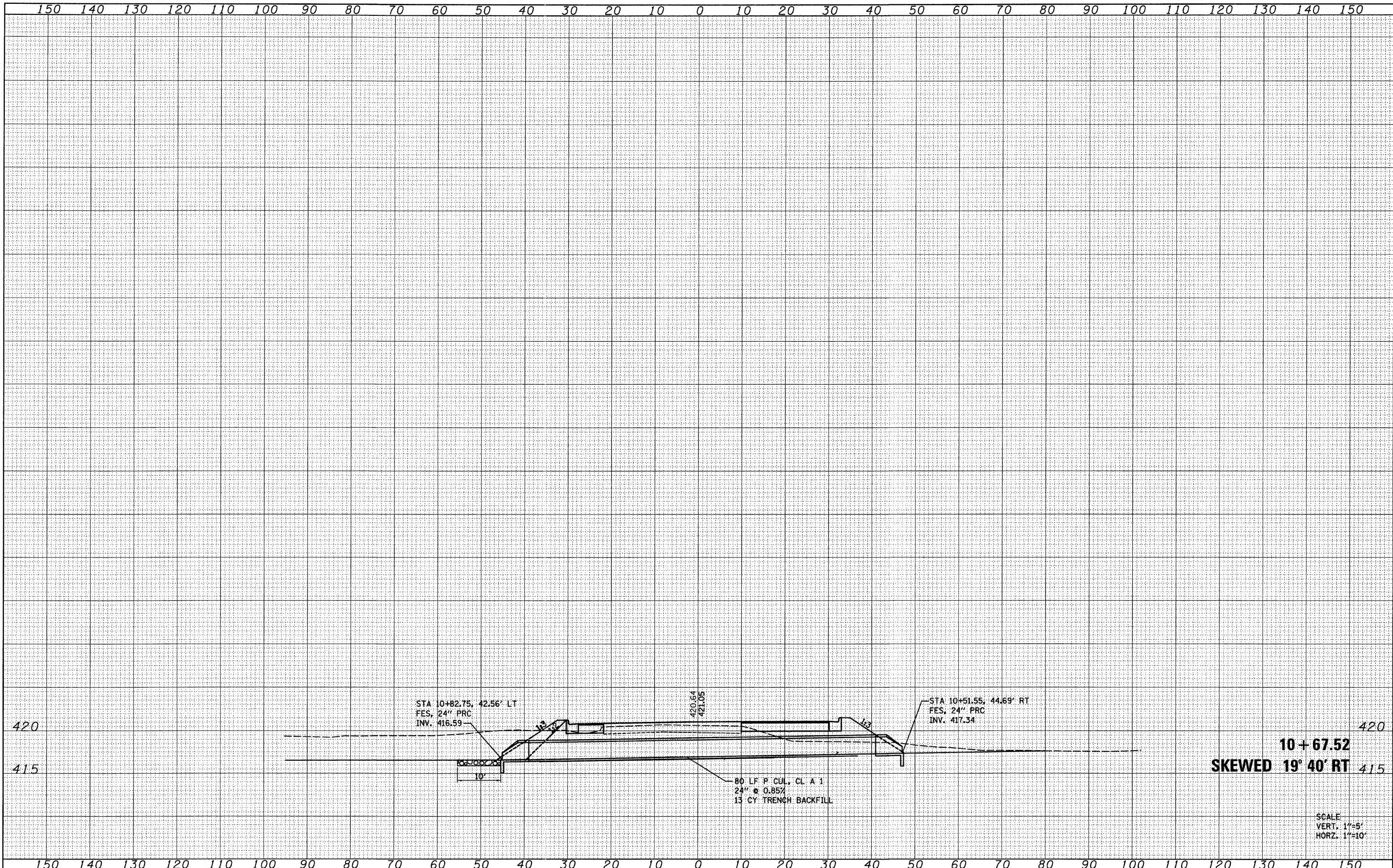
- (1) Quantity includes all concrete necessary for one foundation.
- (2) Includes reinforcement bars and spiral hooping for one foundation.

NUMBER	REVISION	DATE



FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	CHECKED		



10 + 67.52
SKEWED 19° 40' RT

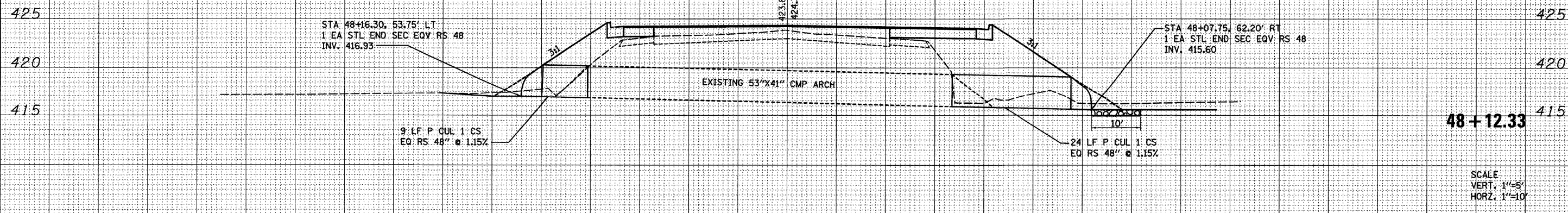
SCALE
 VERT. 1"=5'
 HORZ. 1"=10'

FILE NAME = H:\P\25004\Technical Production\Civil\W017\Microstation\sshtfource2.dgn	USER NAME = \$USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FOURNIE LANE CULVERT SECTION		F.A.J. RTE. 9111	SECTION 75-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 48	
PLOT SCALE = 10.0000' / IN.	PLOT DATE = 3/14/2008	DRAWN -	REVISED -		SCALE:	SHEET NO. 1 OF 1 SHEETS	STA. 10+69.46 TO STA. 11+00.00	CONTRACT NO. 76B22				
		CHECKED -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

FINAL SURVEY	SURVEY	DATE
NOTE BOOK	PLOTTED	BY
NO.	TEMPLATE	
	AREAS CHECKED	
	AREAS CHECKED	

ORIGINAL SURVEY	SURVEY	DATE
NOTE BOOK	PLOTTED	BY
NO.	TEMPLATE	
	AREAS CHECKED	
	AREAS CHECKED	



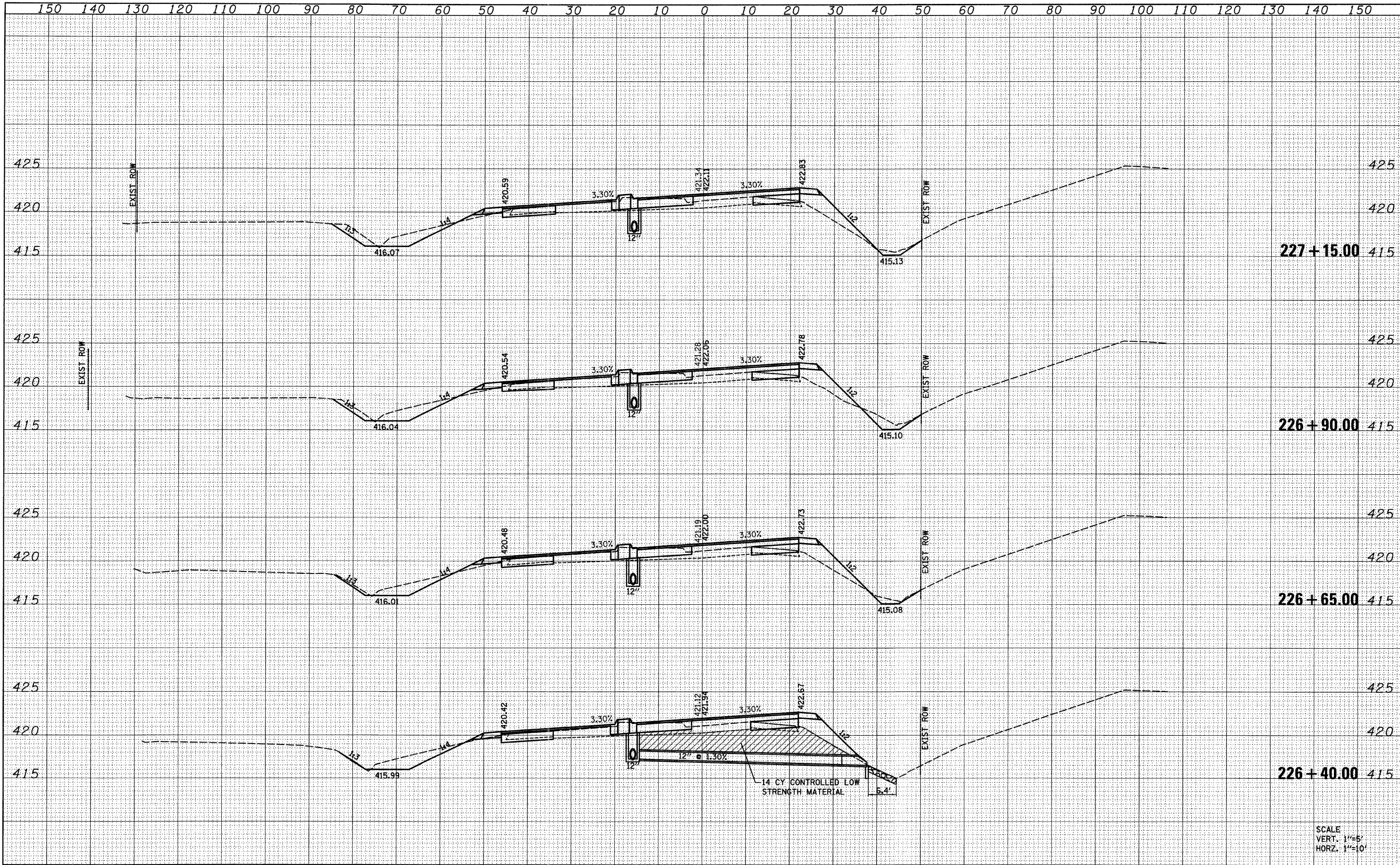
SCALE
VERT. 1"=5'
HORZ. 1"=10'

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

FILE NAME -	USER NAME - #USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EASTPORT PLAZA DRIVE CULVERT SECTION		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
H:\P\25024\Technical Production\Civil\W017\Mapstation\shstpdon2.dgn		DRAWN -	REVISED -		9111	75-15TS	MADISON	64	49		
PLOT SCALE = 10.0000' / IN.		CHECKED -	REVISED -		CONTRACT NO. 76B22						
PLOT DATE = 3/14/2008		DATE -	REVISED -		SCALE: SHEET NO. 1 OF SHEETS		STA. 48+12.33 TO STA. 49+00.00		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		

DATE	BY
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED

DATE	BY
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED



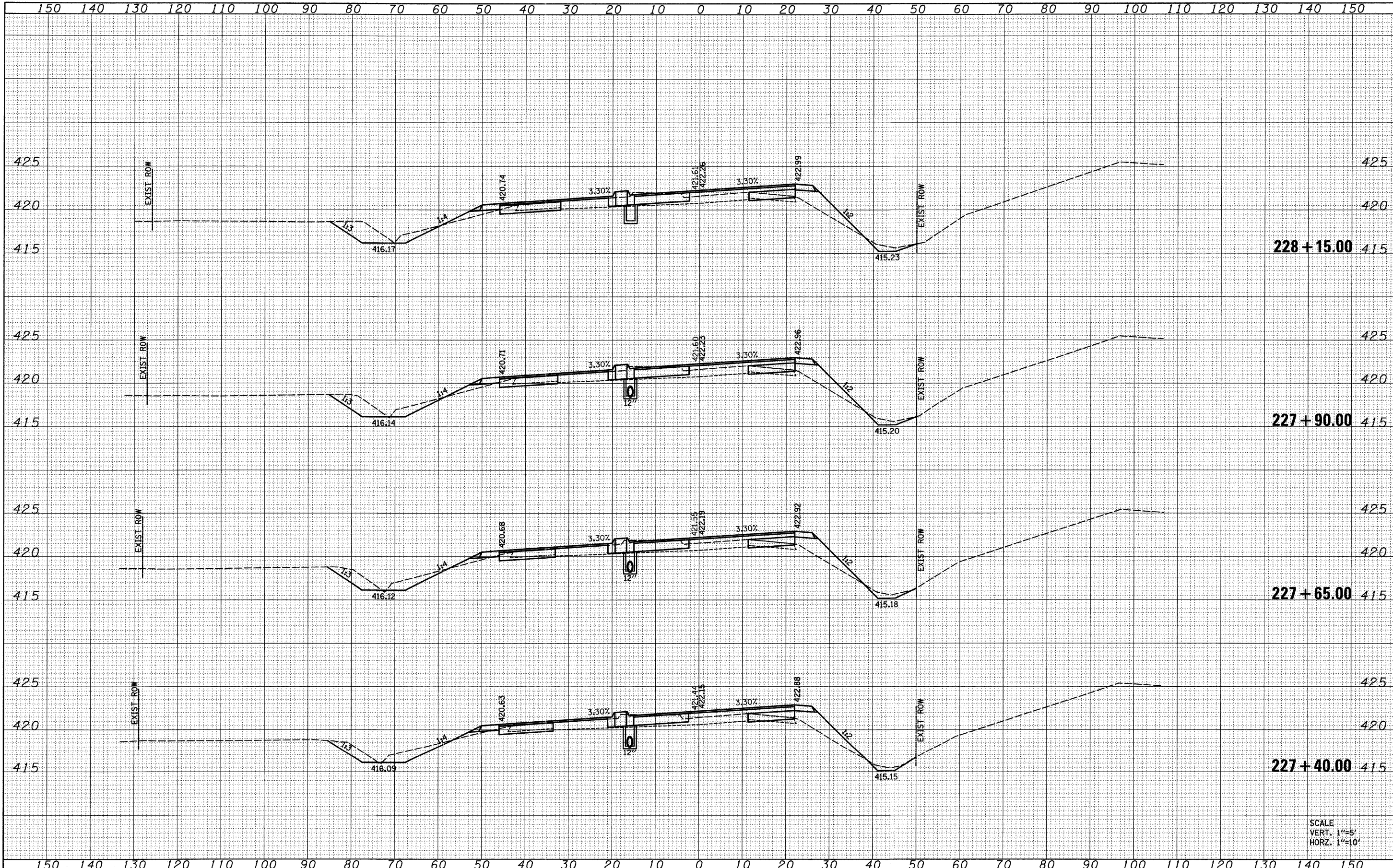
SCALE
VERT. 1"=5'
HORIZ. 1"=10'

FILE NAME =	USER NAME = #USER*	DESIGNED -	REVISED -	<p align="center">STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</p>	<p align="center">HORSESHOE LAKE ROAD STORM SEWER SECTIONS</p>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
H:\P\25004\Technical Production\Civil\W017\Mapstation\sshtml\c10a2.dgn	PLOT SCALE = 10.0000' / IN.	DRAWN -	REVISED -			9111	73-15TS	MADISON	64	50
PLOT DATE = 3/14/2008	DATE -	CHECKED -	REVISED -			CONTRACT NO. 76B22				
		DATE -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

SCALE: SHEET NO. 1 OF 5 SHEETS STA. 226+40.00 TO STA. 227+15.00

DATE	
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	

DATE	
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	

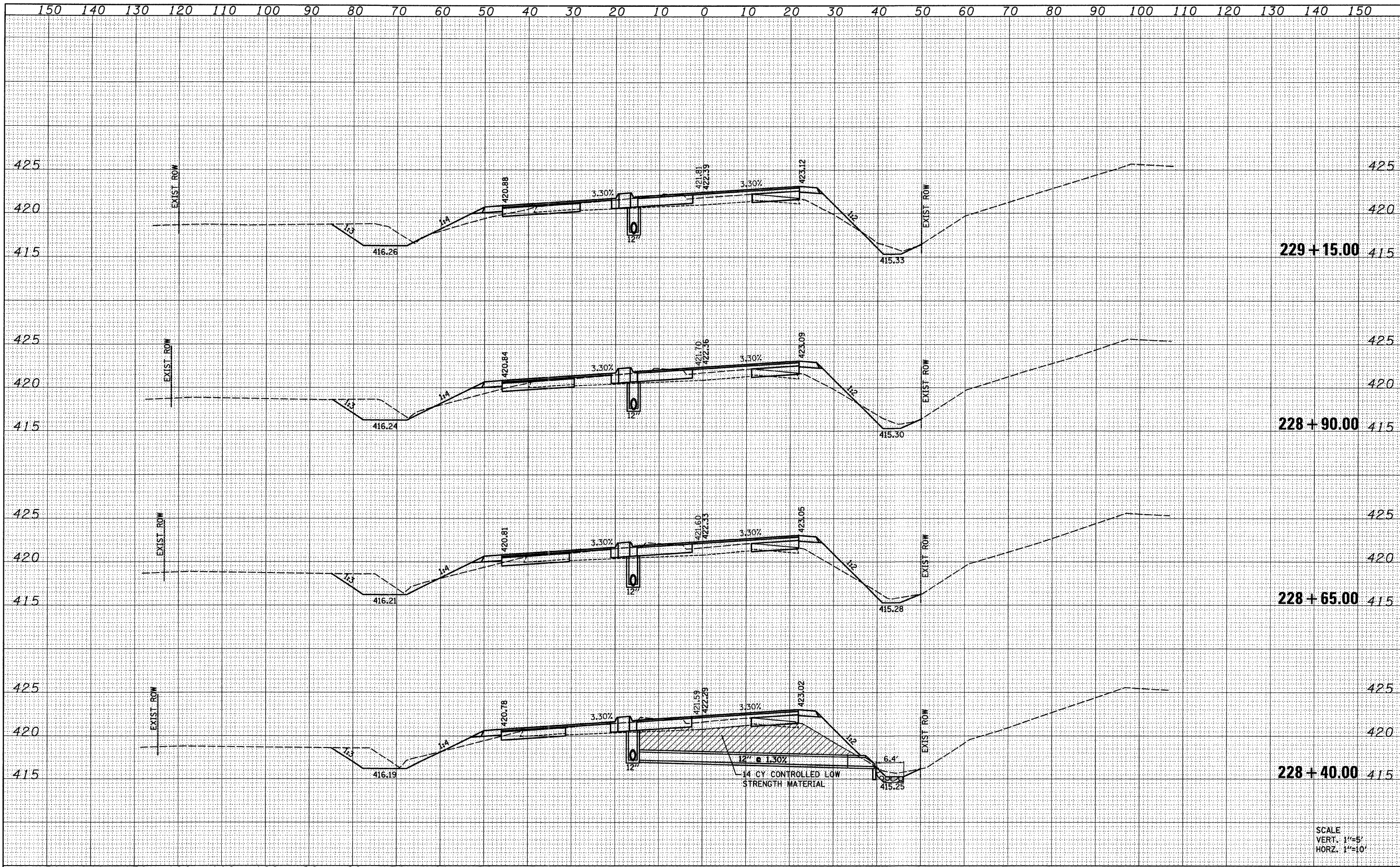


SCALE
VERT. 1"=5'
HORZ. 1"=10'

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HORSESHOE LAKE ROAD STORM SEWER SECTIONS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
H:\P\25004\Technical Production\Civil\W017\Mor...	station\sshtmlouo2.dgn	DRAWN -	REVISED -		9111	73-15TS	MADISON	64	51	CONTRACT NO. 76B22	
	PLOT SCALE = 10.0000' / IN.	CHECKED -	REVISED -		SCALE:	SHEET NO. 2 OF 5 SHEETS	STA. 227+40.00 TO STA. 228+15.00	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		
	PLOT DATE = 3/14/2008	DATE -	REVISED -								

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		

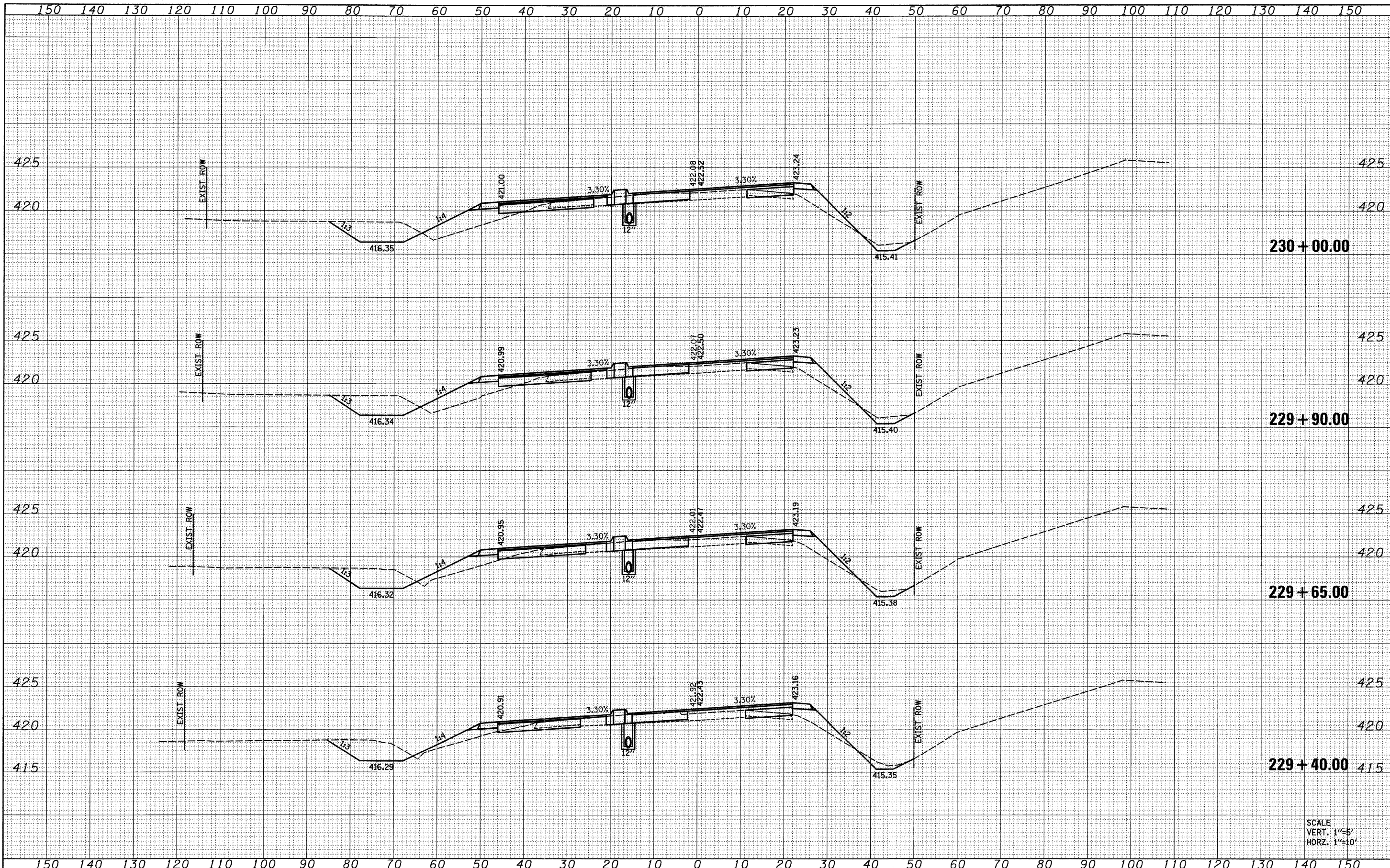


SCALE
VERT. 1"=5'
HORZ. 1"=10'

FILE NAME = H:\P\25884\Technical Production\Civil\W017\Mor...	USER NAME = #USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HORSESHOE LAKE ROAD STORM SEWER SECTIONS	F.A.U. RTE. 9111	SECTION 73-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 52
PLOT SCALE = 10.0000' / IN.	CHECKED -	REVISED -	SCALE: SHEET NO. 3 OF 5 SHEETS STA. 228+40.00 TO STA. 229+15.00			FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
PLOT DATE = 3/14/2008	DATE -	REVISED -								

FINAL SURVEY	DATE
SURVEYED	BY
PLOTTED	
NOTE BOOK	
NO.	
AREAS CHECKED	

ORIGINAL SURVEY	DATE
SURVEYED	BY
PLOTTED	
NOTE BOOK	
NO.	
AREAS CHECKED	



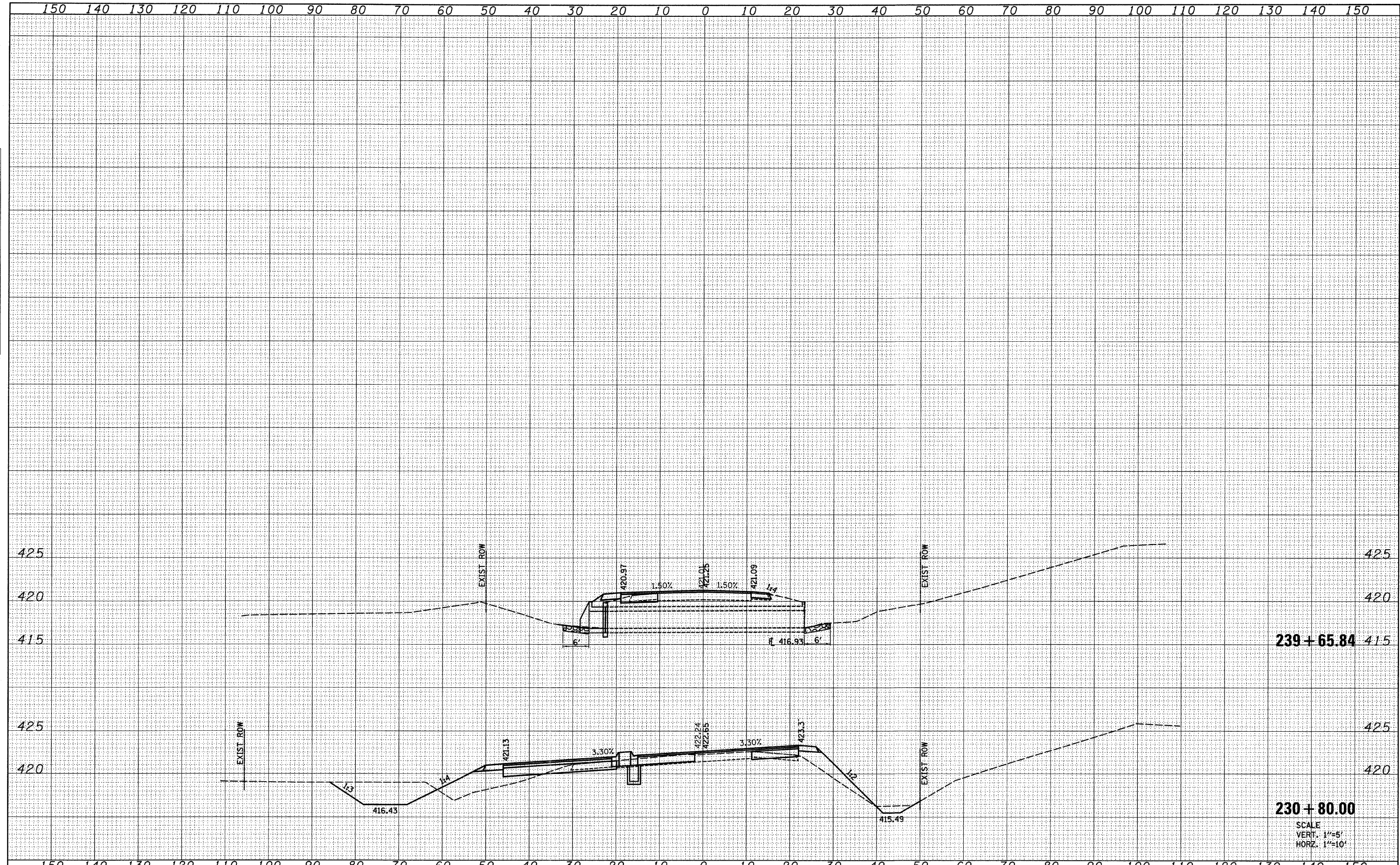
SCALE
VERT. 1"=5'
HORZ. 1"=10'

FILE NAME -	USER NAME - #USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HORSESHOE LAKE ROAD STORM SEWER SECTIONS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
H:\P\25004\Technical Production\Civil\W017\Mc...	station\sshtml\coulaa2.dgn	DRAWN -	REVISED -					9111	73-15TS	MADISON	64	53
PLOT SCALE = 10.0000' / IN.		CHECKED -	REVISED -					CONTRACT NO. 76B22				
PLOT DATE = 3/14/2008		DATE -	REVISED -					FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
				SCALE: SHEET NO. 4 OF 5 SHEETS STA. 229+40.00 TO STA. 230+00.00								

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

FINAL SURVEY NO.	SURVEYED PLOTTED	BY	DATE
NOTE BOOK	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED PLOTTED	BY	DATE
NOTE BOOK	TEMPLATE		
	AREAS CHECKED		



239 + 65.84

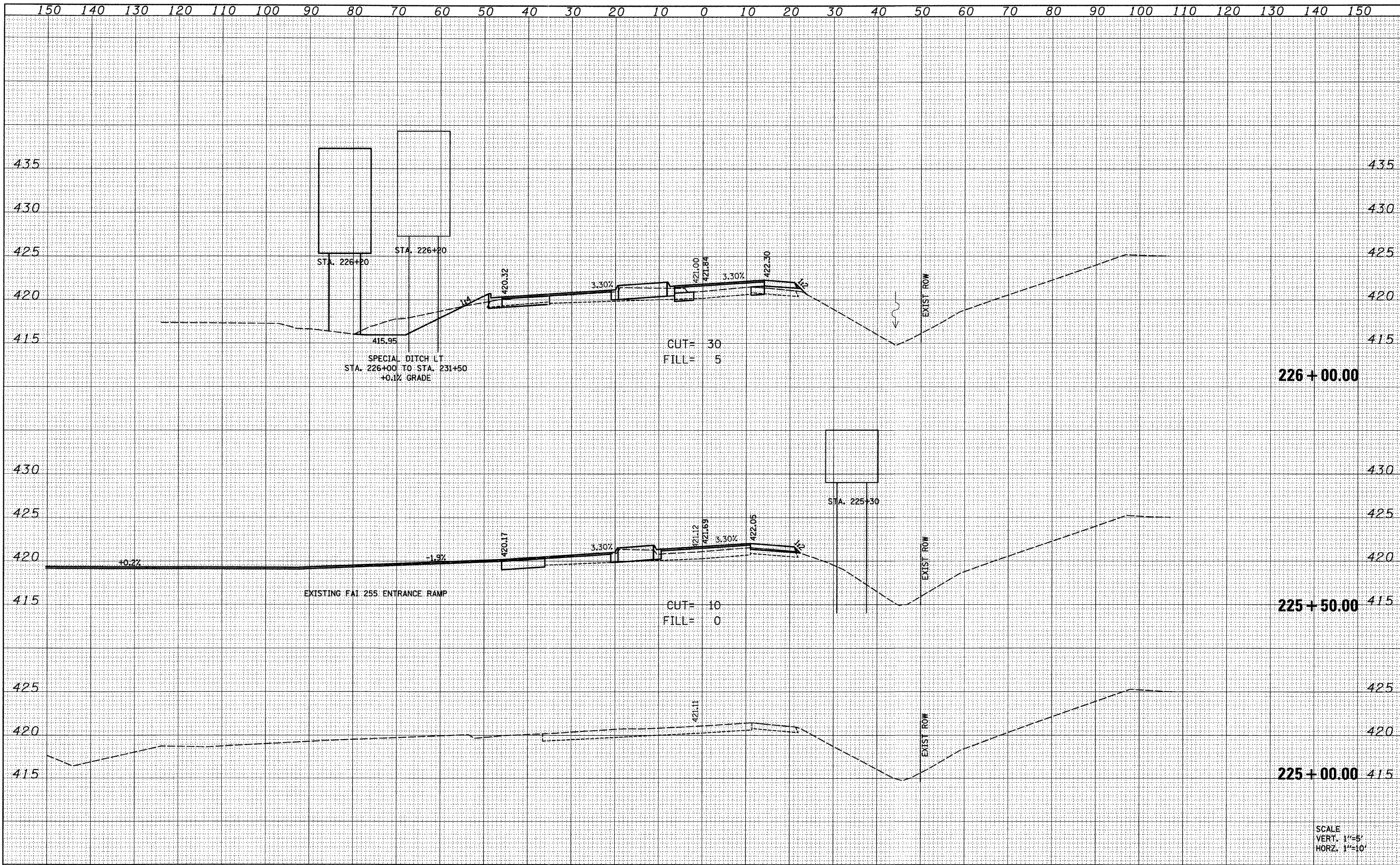
230 + 80.00

SCALE
VERT. 1"=5'
HORZ. 1"=10'

FILE NAME = H:\P\25004\Technical Production\Civil\W017\Mc...	USER NAME = #USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HORSESHOE LAKE ROAD STORM SEWER SECTIONS		F.A.U. RTE. 9111	SECTION 73-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 54
	PLOT SCALE = 10.0000' / IN.	DRAWN -	REVISED -		SCALE:	SHEET NO. 5 OF 5 SHEETS	STA. 239+65.584 TO STA. 239+65.84	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 76B22	
	PLOT DATE = 3/14/2008	CHECKED -	REVISED -								
		DATE -	REVISED -								

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NOTE BOOK	
NO.	

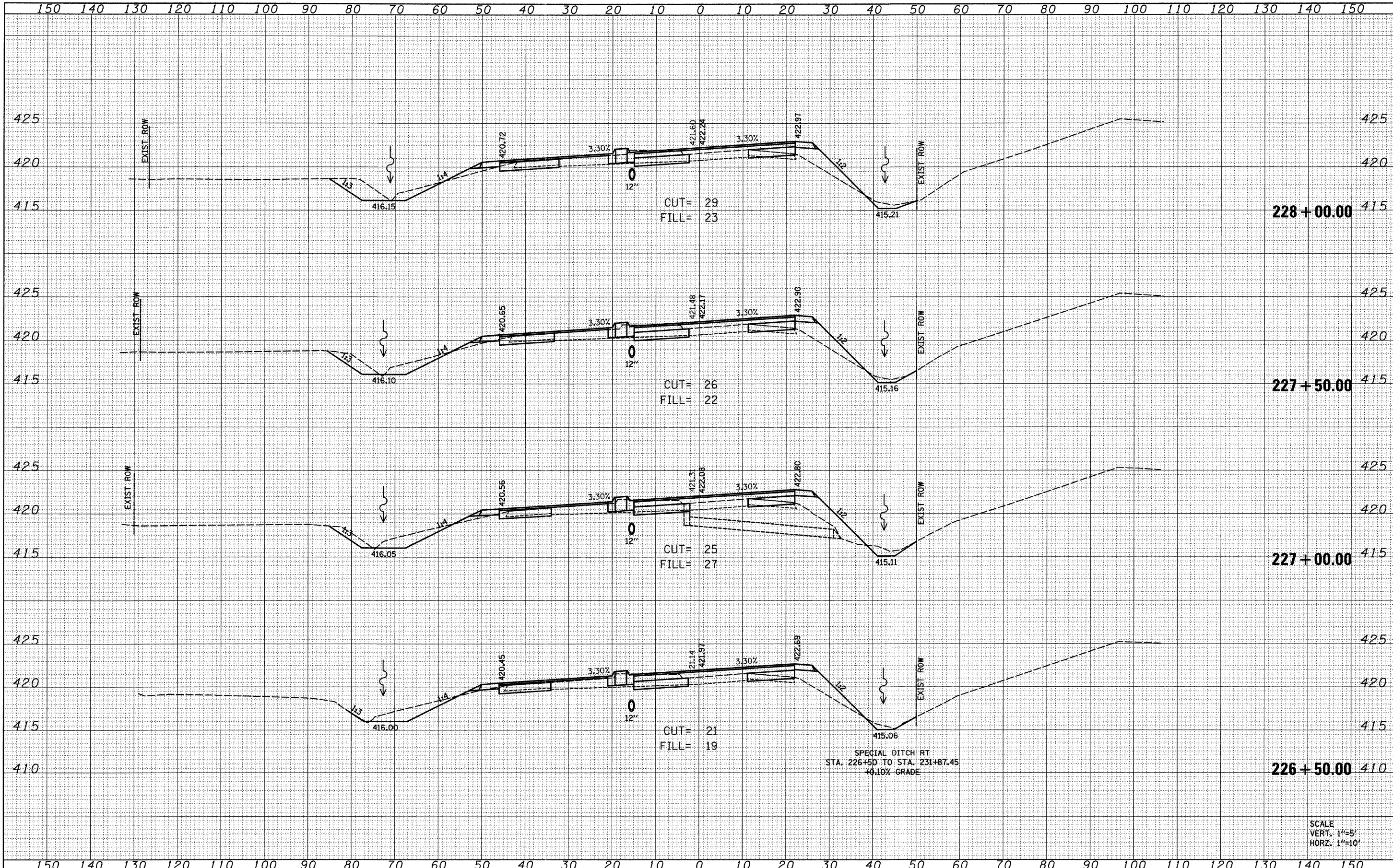


SCALE
VERT. 1"=5'
HORIZ. 1"=10'

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS HORSESHOE LAKE ROAD			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
H:\P\2500\4\Technical Production\Civil\W017\Mc...	station\sheet10a2.dgn	DRAWN -	REVISED -		9111	75-15TS	MADISON	64	55	CONTRACT NO. 76B22		
PLOT SCALE = 10.0000' / IN.		CHECKED -	REVISED -		SCALE:			SHEET NO. 1 OF 10 SHEETS	STA. 225+00.00 TO STA. 226+00.00	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		
PLOT DATE = 3/14/2008		DATE -	REVISED -									

DATE	
BY	
FINAL SURVEY	
NOTED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
NOTED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

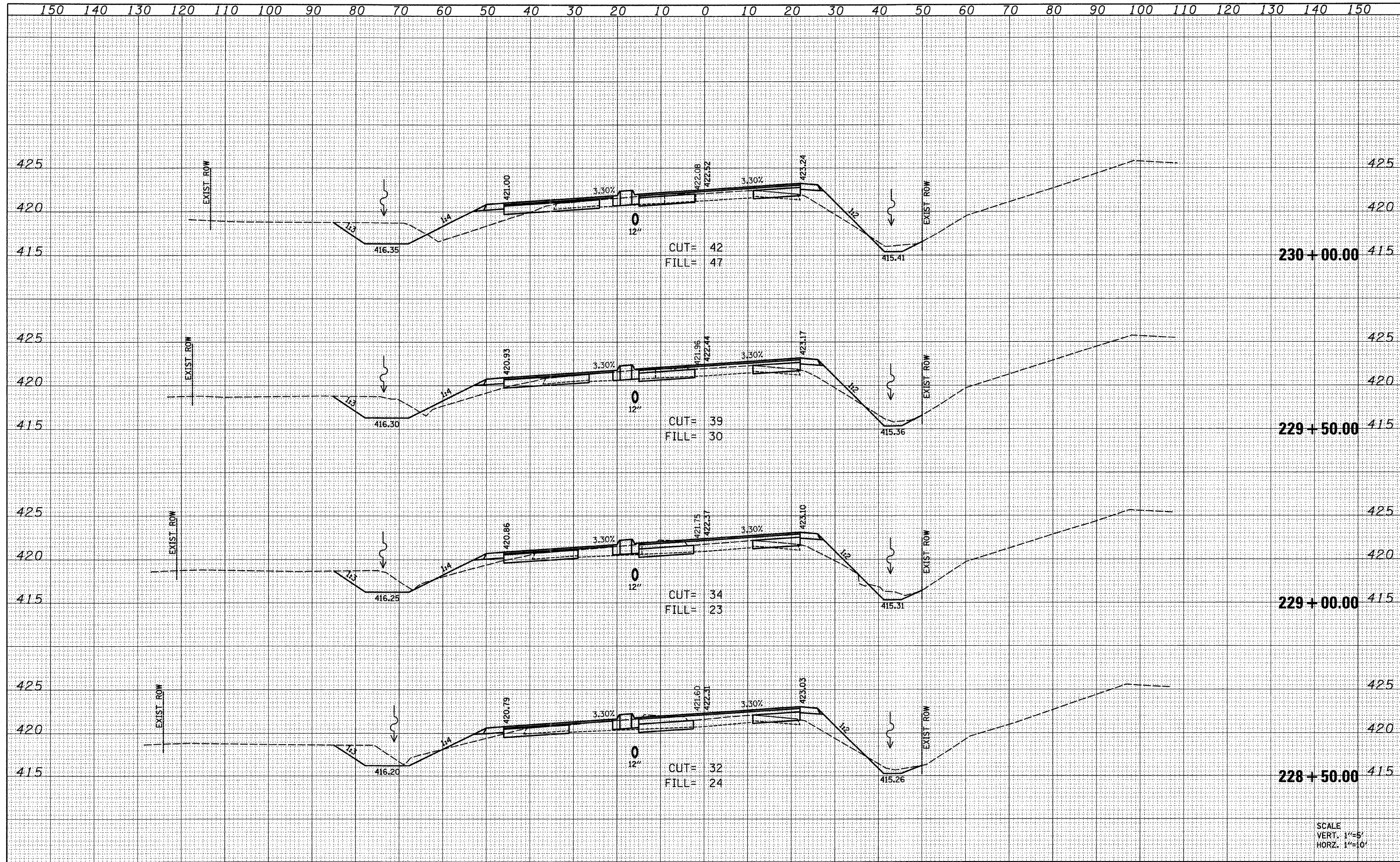


SCALE
 VERT. 1"=5'
 HORZ. 1"=10'

FILE NAME =	USER NAME = #USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS HORSESHOE LAKE ROAD	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
H:\P\25004\Technical Production\Civil\W017\Mapstation\sshtmls2.dgn		DRAWN -	REVISED -			9111	75-15TS	MADISON	64	56
PLOT SCALE = 10.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 76B22				
PLOT DATE = 3/14/2008		DATE -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINISHED	
SURVEY	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

DATE	
BY	
ORIGINAL	
SURVEY	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

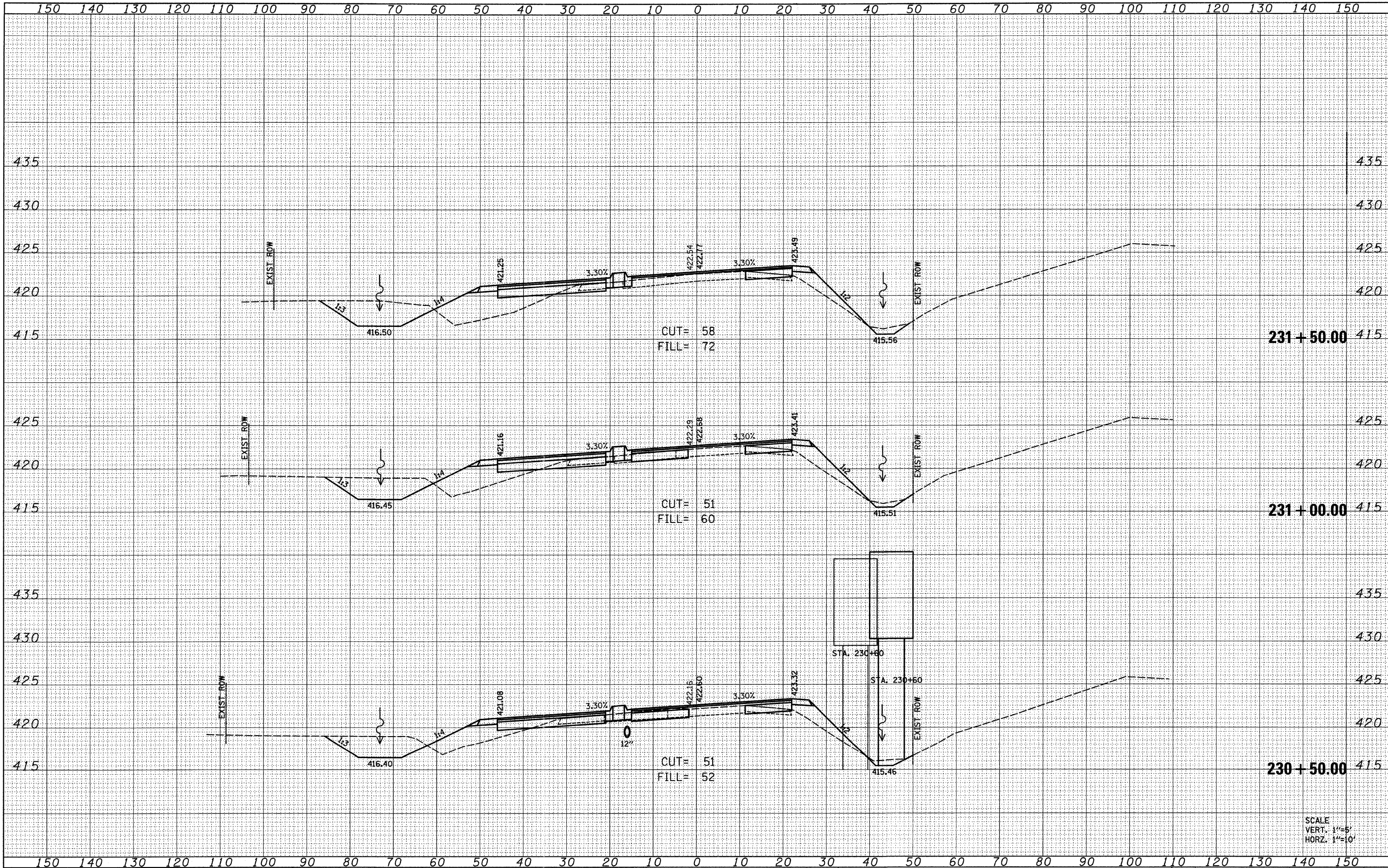


SCALE
VERT. 1"=5'
HORZ. 1"=10'

FILE NAME = H:\P\25004\Technical Production\Civil\W017\Microstation\sshtml0a2.dgn	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS HORSESHOE LAKE ROAD	F.A.U. RTE. 9111	SECTION 75-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 57	
PLOT SCALE = 10.0000' / IN.	PLOT DATE = 3/14/2008	DRAWN -	REVISED -			SCALE:	SHEET NO. 3 OF 10 SHEETS	STA. 228+50.00 TO STA. 230+00.00	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 76B22
		CHECKED -	REVISED -								
		DATE -	REVISED -								

FINAL SURVEY	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	TEMPLATE	
	AREAS	
	CHECKED	

ORIGINAL SURVEY	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	TEMPLATE	
	AREAS	
	CHECKED	

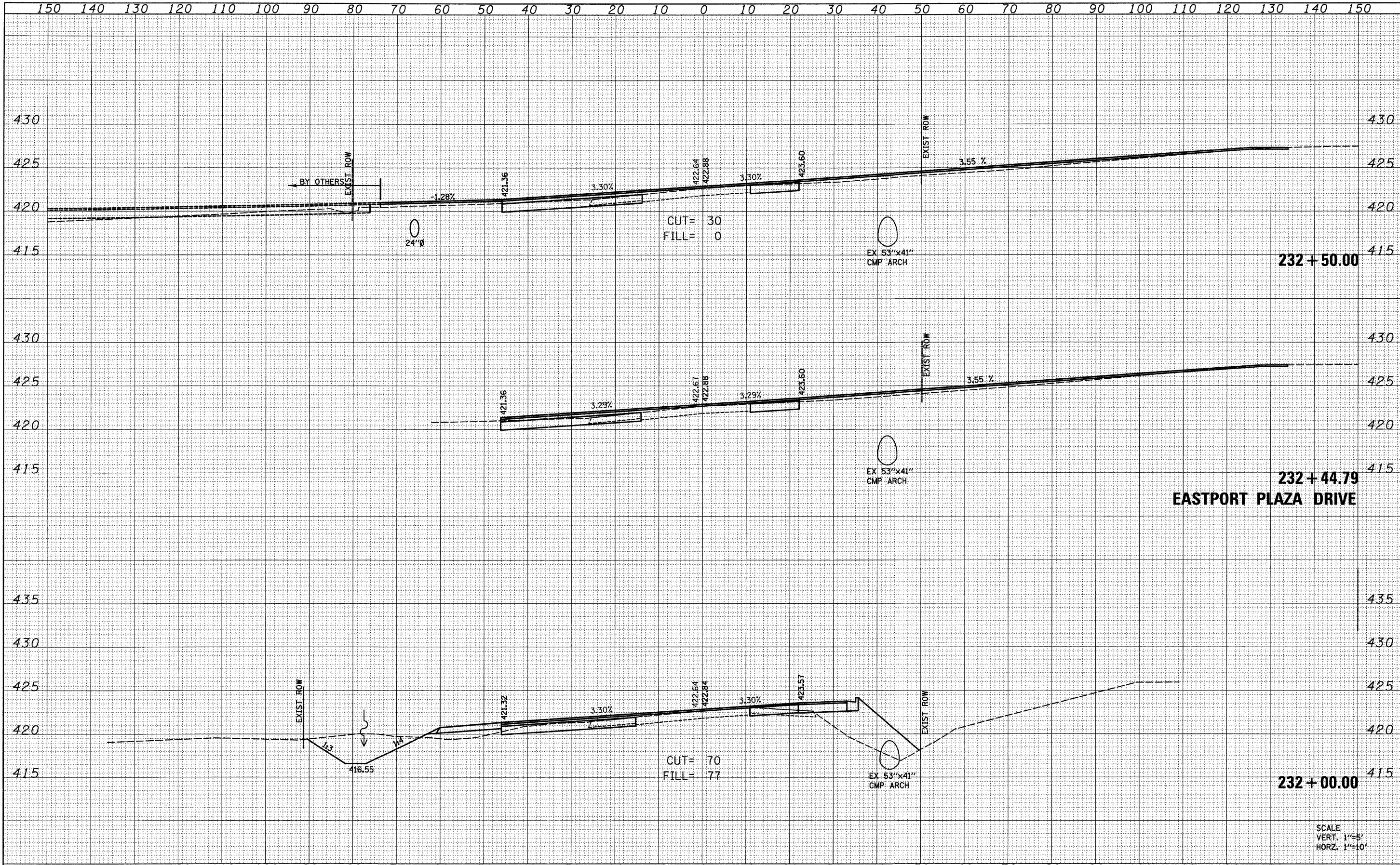


SCALE
VERT. 1"=5'
HORZ. 1"=10'

FILE NAME = H:\P\25004\Technical Production\Civil\W017\Microstation\sshtml0a2.dgn	USER NAME = *USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS HORSESHOE LAKE ROAD			F.A.U. RTE. 9111	SECTION 75-15T5	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 58
PLOT SCALE = 10.0000' / IN.		DRAWN -	REVISED -		SCALE:	SHEET NO. 4 OF 10 SHEETS	STA. 230+50.00 TO STA. 231+50.00	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 76B22		
PLOT DATE = 3/14/2008		CHECKED -	REVISED -									
		DATE -	REVISED -									

FINAL SURVEY	NO.
SURVEYED	BY
PLOTTED	DATE
TEMPLATE	
AREAS	
CHECKED	

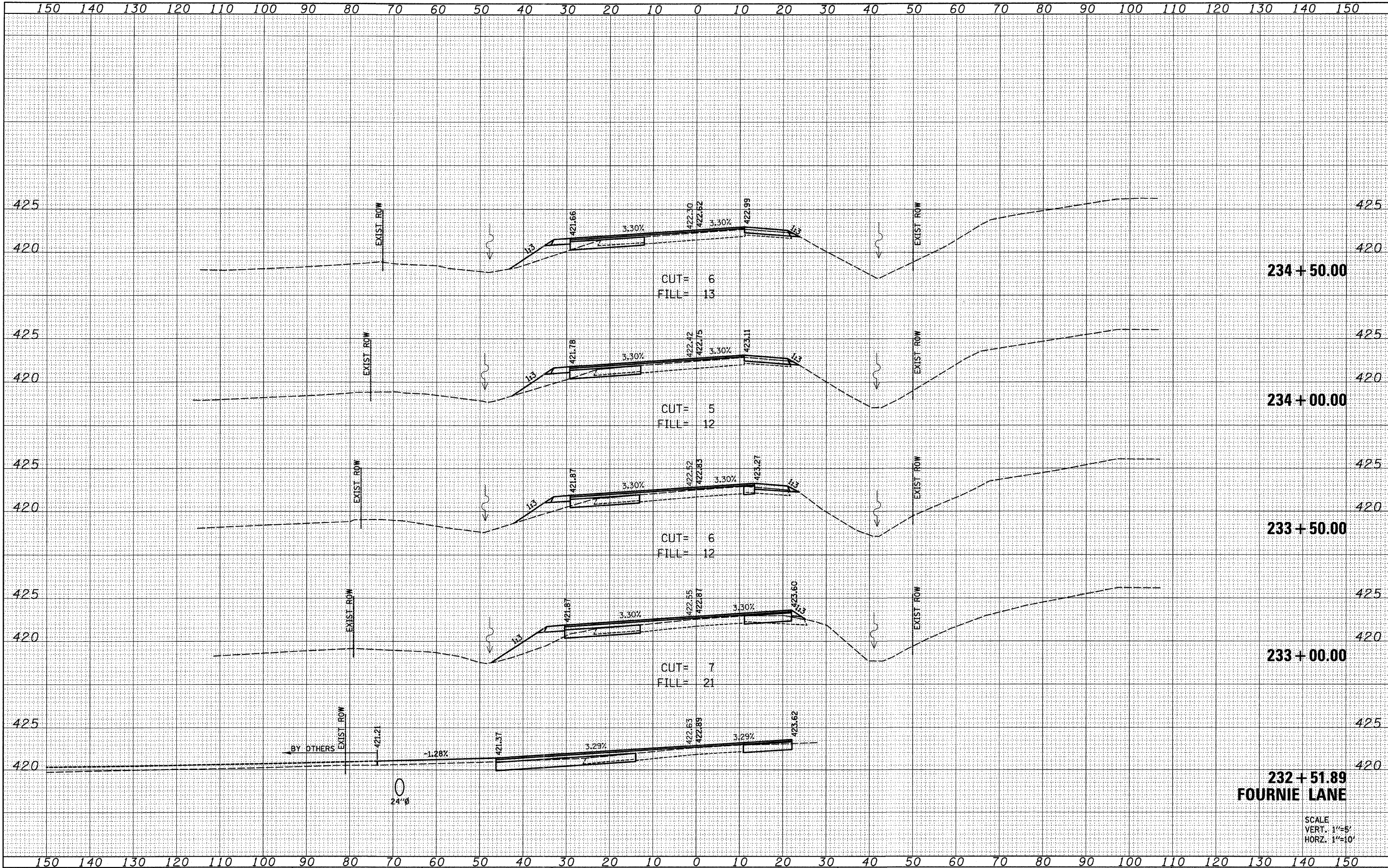
ORIGINAL SURVEY	NO.
SURVEYED	BY
PLOTTED	DATE
TEMPLATE	
AREAS	
CHECKED	



FILE NAME = H:\P\25004\Technical Production\Civil\W017\Microstation\sshtn1oa2.dgn	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS HORSESHOE LAKE ROAD			F.A.U. RTE. 9111	SECTION 75-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 59
PLLOT SCALE = 10.0000' / IN.	PLLOT DATE = 3/14/2008	DRAWN -	REVISED -					SCALE: SHEET NO. 5 OF 10 SHEETS	STA. 232+00.00 TO STA. 232+50.00	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 76B22
		CHECKED -	REVISED -									
		DATE -	REVISED -									

DATE	
BY	
FINISHED	
SURVEY	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

DATE	
BY	
ORIGINAL	
SURVEY	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	



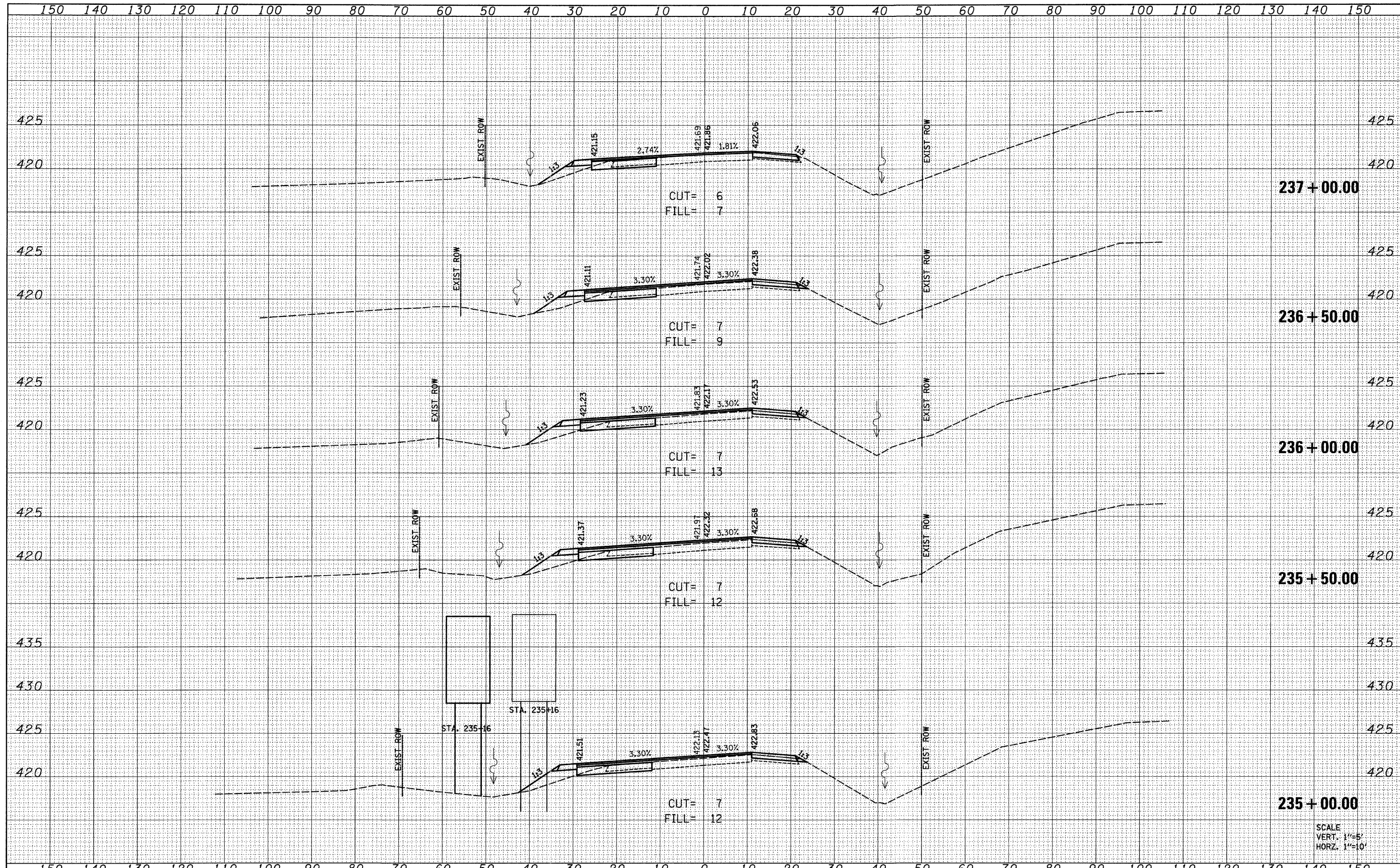
**232 + 51.89
FOURNIE LANE**

SCALE
VERT. 1"=5'
HORZ. 1"=10'

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS HORSESHOE LAKE ROAD			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
H:\P\25004\Technical Production\Civil\W017\Mapstation\sshtml02.dgn		DRAWN -	REVISED -		9111	75-15TS	MADISON	64	60			
PLOT SCALE = 10.0000' / IN.		CHECKED -	REVISED -		CONTRACT NO. 76B22							
PLOT DATE = 3/14/2008		DATE -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							
				SCALE:	SHEET NO. 6 OF 10 SHEETS	STA. 232+51.89 TO STA. 234+50.00						

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

DATE	
BY	
ORIGINAL	
SURVEY	
NOTE	
BOOK	
AREAS	
CHECKED	
NO.	

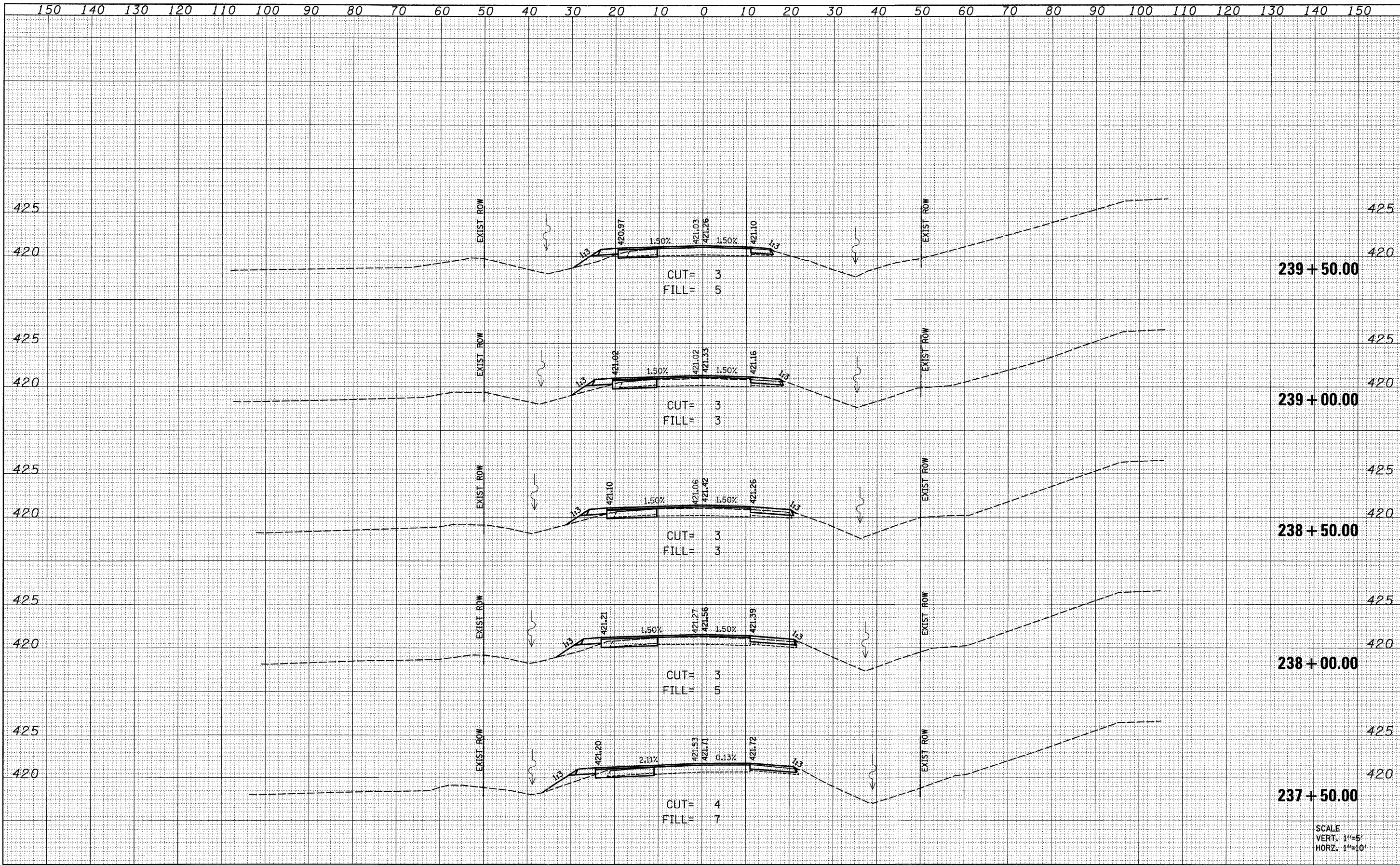


SCALE
VERT. 1"=5'
HORIZ. 1"=10'

FILE NAME = H:\P\25008\Technical Production\Civil\W017\Mapstation\sheet10a2.dgn	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS HORSESHOE LAKE ROAD			F.A.U. RTE. 9111	SECTION 75-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 61
PLOT SCALE = 10.0000' / IN.	PLOT DATE = 3/14/2008	DRAWN -	REVISED -		SCALE:	SHEET NO. 7 OF 10 SHEETS	STA. 235+00.00 TO STA. 237+00.00	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 76822		
		CHECKED -	REVISED -									
		DATE -	REVISED -									

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

DATE	
BY	
ORIGINAL	
SURVEY	
NOTE BOOK	
AREAS	
CHECKED	
NO.	

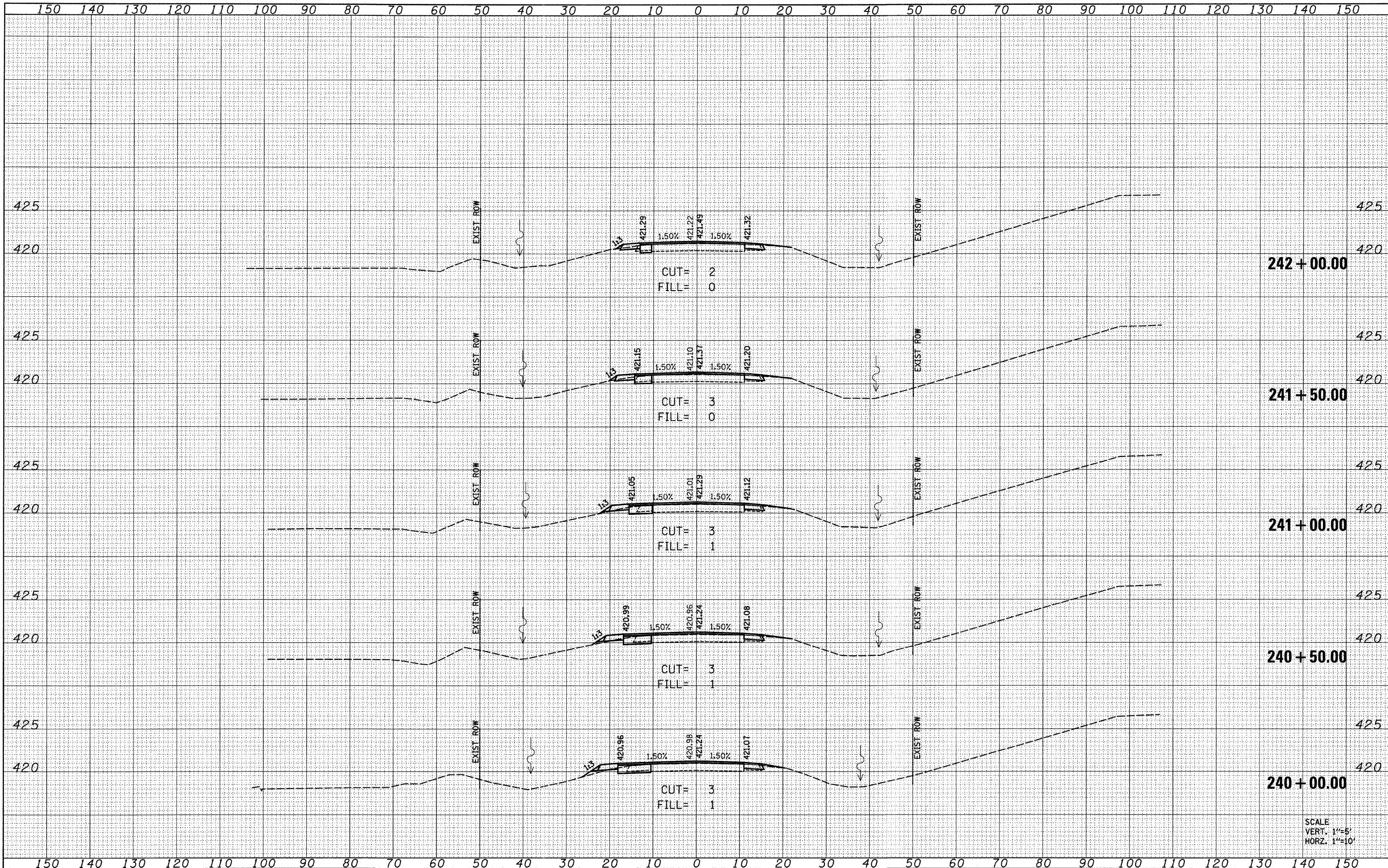


SCALE
VERT. 1"=5'
HORZ. 1"=10'

FILE NAME = H:\V\25004\Technical Production\Civil\W017\Mor...	USER NAME = #USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS HORSESHOE LAKE ROAD			F.A.U. RTE. 9111	SECTION 75-15TS	COUNTY MADISON	TOTAL SHEETS 64	SHEET NO. 62
PLOT SCALE = 10.0000' / IN.	CHECKED -	REVISED -	REVISED -					CONTRACT NO. 76B22				
PLOT DATE = 3/14/2008	DATE -	REVISED -	REVISED -					FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
SCALE: SHEET NO. 8 OF 10 SHEETS STA. 237+50.00 TO STA. 239+50.00												

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

DATE	
BY	
ORIGINAL	
SURVEY	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

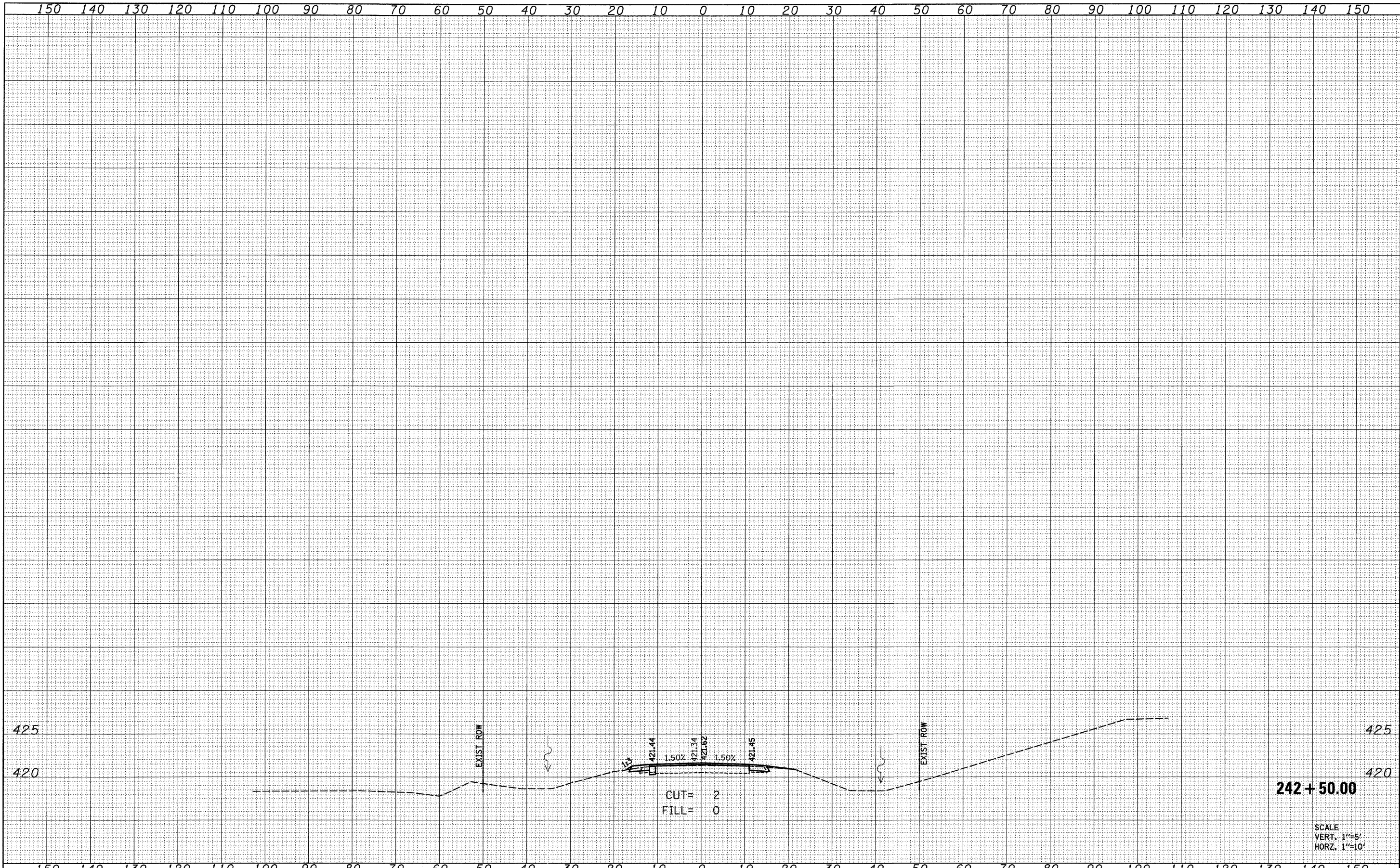


SCALE
VERT. 1"=5'
HORZ. 1"=10'

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS HORSESHOE LAKE ROAD	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
H:\P\25004\Technical Production\Civil\W017\Microstation\xshtml02.dgn		DRAWN -	REVISED -			9111	75-15TS	MADISON	64	63
PLOT SCALE = 10,0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 76B22				
PLOT DATE = 3/14/2008		DATE -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
SCALE:						SHEET NO. 9 OF 10 SHEETS		STA. 240+00.00 TO STA. 242+00.00		

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



242 + 50.00

SCALE
VERT. 1"=5'
HORIZ. 1"=10'

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				CROSS SECTIONS HORSESHOE LAKE ROAD				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
H:\P\25024\Technical Production\Civil\W017\Mcristation\sshtml0a2.dgn		DRAWN -	REVISED -									9111	75-15TS	MADISON	64	64
PLOT SCALE = 10.0000' / IN.		CHECKED -	REVISED -	SCALE:				SHEET NO. 10 OF SHEETS STA. 242+50.00 TO STA. 242+50.00				FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
PLOT DATE = 3/14/2008		DATE -	REVISED -									CONTRACT NO. 76B22				