

NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE MAINTAINED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR REPLACEMENT AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12" (300mm). HEADS SHALL BE MAINTAINED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES AND RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. ALL THE EXISTING TEMPORARY TRAFFIC SIGNAL EQUIPMENT SHALL BE RETURNED TO THE FOLLOWING AGENCY AFTER THE INSTALLATION OF PERMANENT SIGNALS. (NAME OF AGENCY.....)
8. RIGHT TURN OVERLAP ARROW SECTIONS SHALL BE BAGGED UNTIL RIGHT TURN LANE IS OPEN TO TRAFFIC.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS TEMPORARY SIGNAL SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

NOTE: THE EXISTING CONDUITS, LOOP DETECTORS AND MAGNETIC DETECTORS SHALL BE ABANDONED.

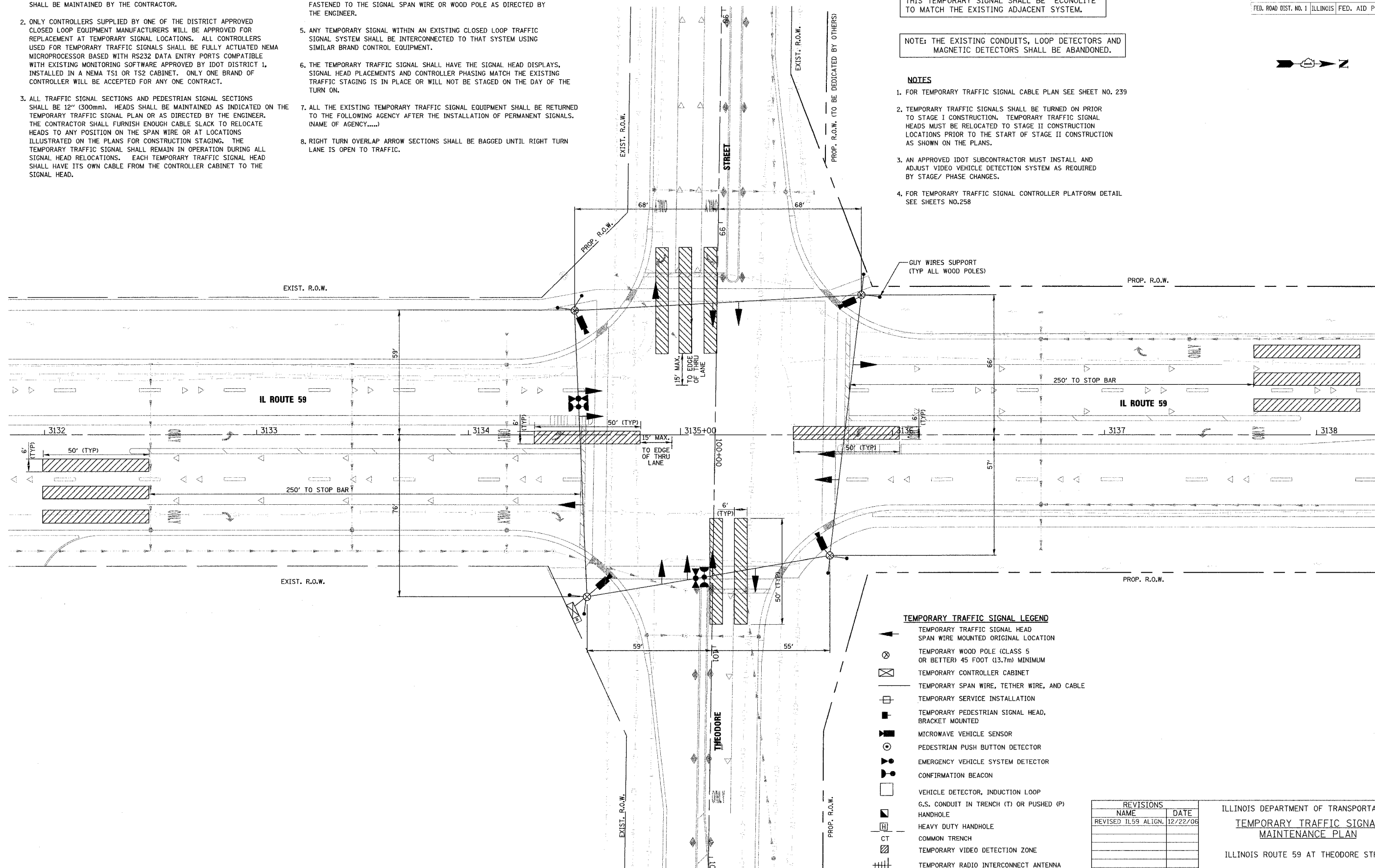
NOTES

1. FOR TEMPORARY TRAFFIC SIGNAL CABLE PLAN SEE SHEET NO. 239
2. TEMPORARY TRAFFIC SIGNALS SHALL BE TURNED ON PRIOR TO STAGE I CONSTRUCTION. TEMPORARY TRAFFIC SIGNAL HEADS MUST BE RELOCATED TO STAGE II CONSTRUCTION LOCATIONS PRIOR TO THE START OF STAGE II CONSTRUCTION AS SHOWN ON THE PLANS.
3. AN APPROVED IDOT SUBCONTRACTOR MUST INSTALL AND ADJUST VIDEO VEHICLE DETECTION SYSTEM AS REQUIRED BY STAGE/ PHASE CHANGES.
4. FOR TEMPORARY TRAFFIC SIGNAL CONTROLLER PLATFORM DETAIL SEE SHEETS NO.258



PLAN	DATE
REVISION	BY
PLotted	
NOTE BOOK	
NO.	

PROFILE	DATE
REVISION	BY
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NOTE BOOK	
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TEMPORARY TRAFFIC SIGNAL LEGEND

- ▲ TEMPORARY TRAFFIC SIGNAL HEAD
- TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- ⊠ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- TEMPORARY SERVICE INSTALLATION
- TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- ⊞ MICROWAVE VEHICLE SENSOR
- ⊙ PEDESTRIAN PUSH BUTTON DETECTOR
- ⊖ EMERGENCY VEHICLE SYSTEM DETECTOR
- ⊕ CONFIRMATION BEACON
- VEHICLE DETECTOR, INDUCTION LOOP
- ⊞ G.S. CONDUIT IN TRENCH (T) OR PUSHED (P) HANDHOLE
- ⊞ HEAVY DUTY HANDHOLE
- CT COMMON TRENCH
- ⊞ TEMPORARY VIDEO DETECTION ZONE
- ⊞ TEMPORARY RADIO INTERCONNECT ANTENNA
- ⊞ UNINTERRUPTED POWER SUPPLY (UPS)

REVISIONS	
NAME	DATE
REVISED IL59 ALIGN.	12/22/06

ILLINOIS DEPARTMENT OF TRANSPORTATION
**TEMPORARY TRAFFIC SIGNAL
 MAINTENANCE PLAN**
 ILLINOIS ROUTE 59 AT THEODORE STREET

SCALE 1"=20'
 DATE 3/18/08

DRAWN BY BAR
 DESIGNED BY FA
 CHECKED BY KMM