

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
342	G-RS-4	LAKE	24	1
FED. ROAD DIST. NO.		ILLINOIS	CONTRACT NO. 60C32	

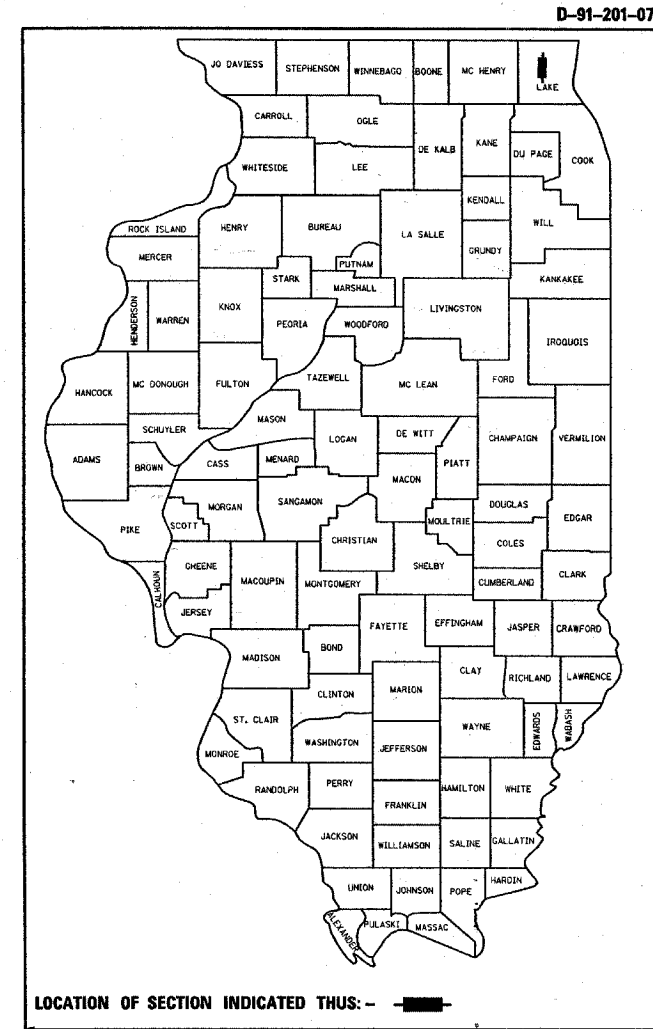
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**

PROPOSED HIGHWAY PLANS

**F.A.P. ROUTE 342: ILL 120 (E BELVIDERE RD)
W. OF US 45 TO ATKINSON RD. (E. OF ILL 83)
SECTION G-RS-4
RESURFACING (MAINTENANCE)
LAKE COUNTY
C-91-201-07**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT LOCATED
IN THE VILLAGE OF GRAYSLAKE

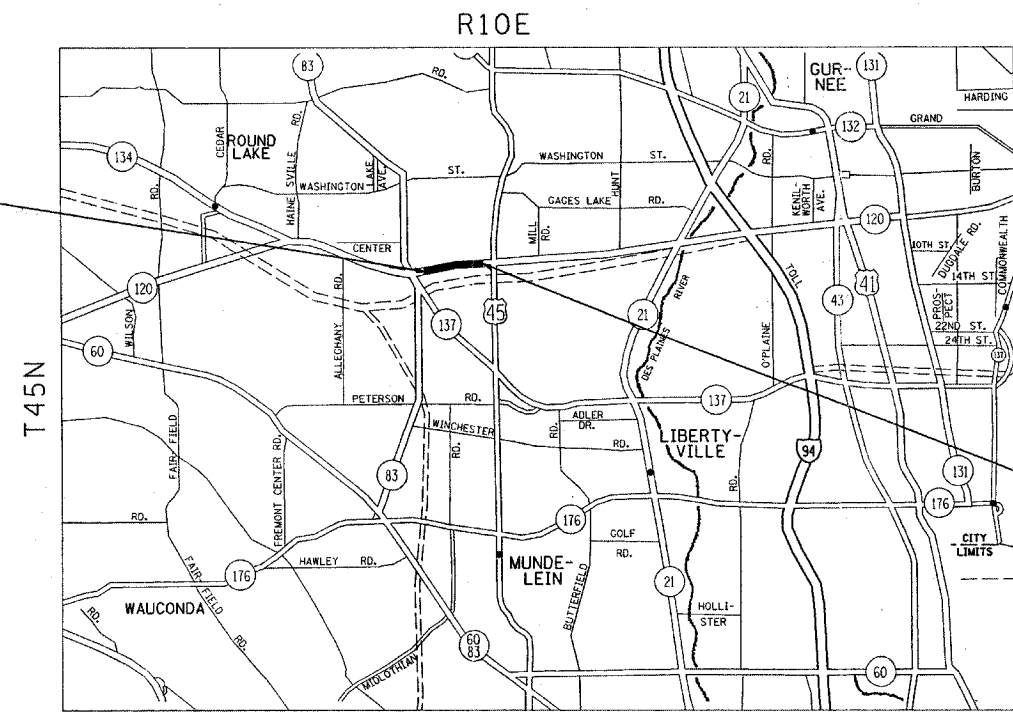


LOCATION OF SECTION INDICATED THUS: — ■ —



TRAFFIC DATA
2005 ADT = 19,800
POSTED SPEED = 35 MPH

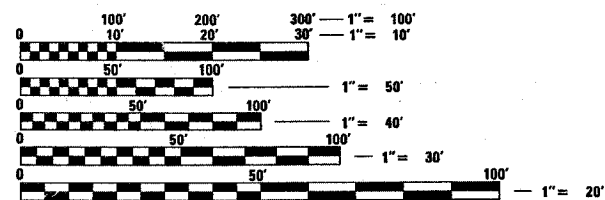
IMPROVEMENT BEGINS:
STATION 33+95



IMPROVEMENT ENDS:
STATION 84+47

LAKE VILLA AND AVON TOWNSHIPS

GROSS & NET LENGTH OF IMPROVEMENT = 5052 FEET (0.95 MILE)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER - FRITZ GUILLAUME
PROJECT MANAGER - KEN ENG (847) 705-4247

CONTRACT NO. 60C32

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**

SUBMITTED April 3, 20 08

Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 9, 20 08
Eric E. Harn
ENGINEER OF DESIGN AND ENVIRONMENT

May 9, 20 08
Christina M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

LIST OF STATE STANDARDS

GENERAL NOTES

<u>SHEET NO.</u>	<u>DESCRIPTION</u>
1	COVER SHEET
2	INDEX OF SHEETS, STANDARDS, AND GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5	TYPICAL SECTIONS PLAN
6-8	ROADWAY & PAVEMENT MARKINGS PLANS
9	DETECTOR LOOP REPLACEMENT PLANS
10-13	PAVEMENT REPLACEMENT PLANS
14	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER
15	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
16	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
17	BUTT JOINT AND HMA TAPER DETAILS
18	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
19	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
20	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
21	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
22	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
23	ARTERIAL ROAD INFORMATION SIGN
24	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

<u>STANDARD NO.</u>	<u>DESCRIPTION</u>
280001-04	TEMPORARY EROSION CONTROL SYSTEMS
442201-03	CLASS C AND D PATCHES
606001-03	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-02	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-02	LANE CLOSURE 2L, 2W MOVING OPERATIONS-DAY ONLY
701501-04	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-05	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901	TRAFFIC CONTROL DEVICES
704001-04	TEMPORARY CONCRETE BARRIER

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (45 KM/H) OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (45 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE RESIDENT ENGINEER SHALL CONTACT MS DEBBIE HANLON AREA TRAFFIC FIELD ENGINEER AT (847) 438-2300 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKING.

THE RESIDENT ENGINEER SHALL VERIFY ALL EXISTING PAVEMENT MARKINGS BEFORE MILLING

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKING ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847)705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

CONTRACT # 60C32

SUMMARY OF QUANTITIES			URBAN 100% STATE	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000	1000			
20201006	GRADING AND SHAPING SHOULDERS	UNIT	101	101				
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	557	557				△
20800150	TRENCH BACKFILL	CU YD	37		37			
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	60		60			
25200110	SODDING, SALT TOLERANT	SO YD	60		60			
31101400	SUB-BASE GRANULAR MATERIAL, TYPE B 6"	SO YD	360		360			
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	20	20				
40600300	AGGREGATE (PRIME COAT)	TON	100	100				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10	10				
40600995	CONSTRUCTING TEST STRIP	EACH	1	1				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	33	33				
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SO YD	3	3				
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	425	425				△
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	4	4				△
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2044	2040	4			△
40701921	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 12"	SO YD	310		310			
44000100	PAVEMENT REMOVAL	SO YD	310		310			
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	24190	24190				
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	180		180			
44002236	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 9"	SO YD	817	817				△
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SO YD	300	300				
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SO YD	25	25				
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SO YD	375	375				
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	912	912				

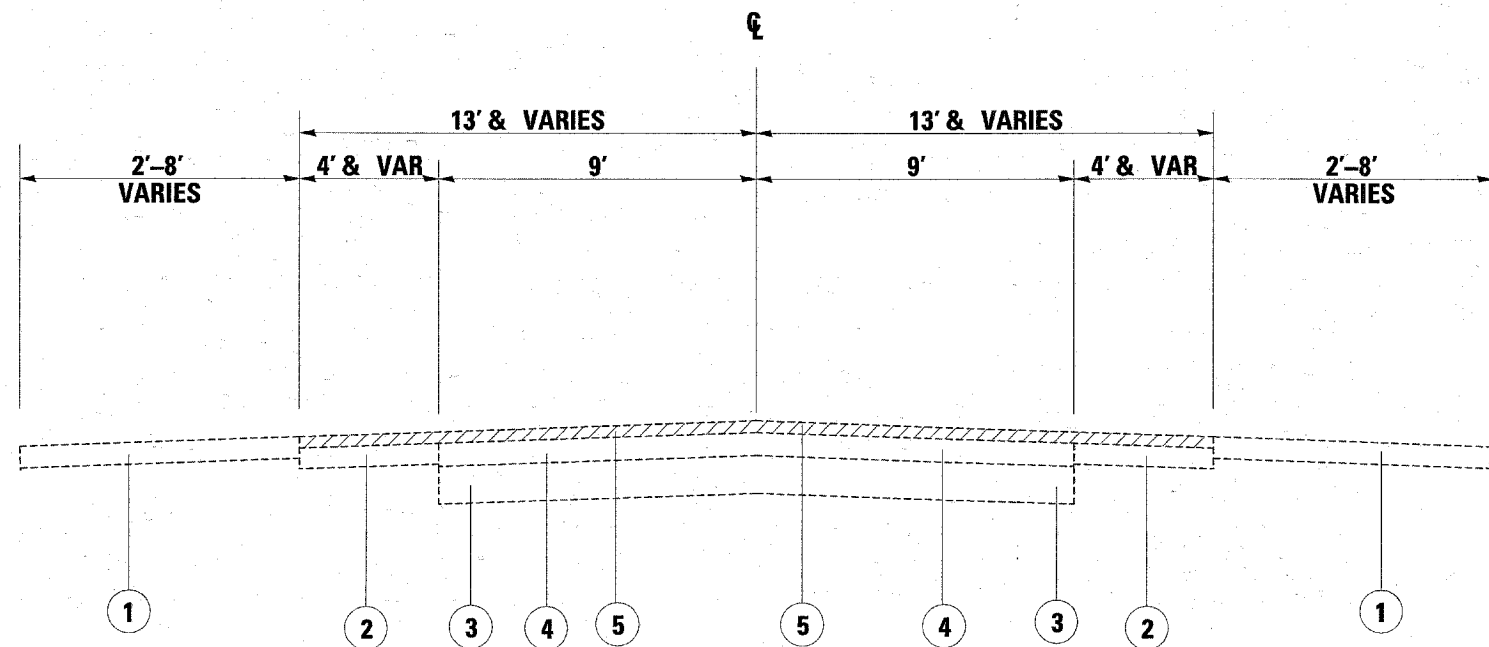
SUMMARY OF QUANTITIES			URBAN 100% STATE	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000	1000			
50300300	PROTECTIVE COAT	SO YD	40		40			
50800105	REINFORCEMENT BARS	POUND	55		55			△
54248510	CONCRETE COLLAR	CU YD	0.5		0.5			△
55021600	STORM SEWERS, TYPE 2, REINFORCED CONCRETE CULVERT, STORM DRAIN, AND SEWER PIPE, CLASS III 12"	FOOT	6		6			
55022200	STORM SEWERS, TYPE 2, REINFORCED CONCRETE CULVERT, STORM DRAIN, AND SEWER PIPE, CLASS III 30"	FOOT	26		26			
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	7	7				
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	180		180			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4	4				△
67100100	MOBILIZATION	L SUM	1	1				
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	0.5	0.5			△
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	14480	14240	240			
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	568.2	508.2	60			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	30100	26100	4000			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2940	2540	400			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	2800	2500	300			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	150	150				
70400100	TEMPORARY CONCRETE BARRIER	FOOT	388		388			
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	508.2	508.2				
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	26100	26100				
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2540	2540				
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	2500	2500				

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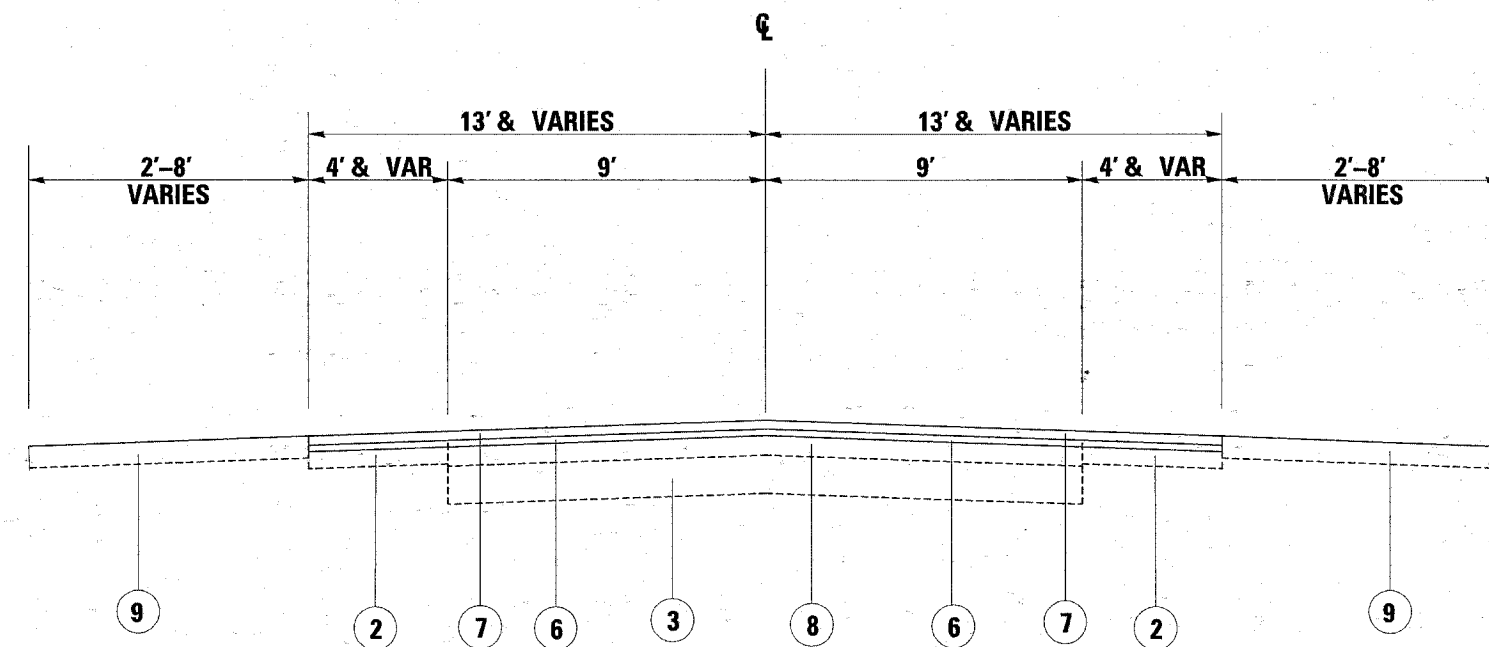
△ Rev. 6-3-08
* SPECIALTY ITEMS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES



EXISTING TYPICAL SECTION
IL ROUTE 120



PROPOSED TYPICAL SECTION
IL ROUTE 120

LEGEND

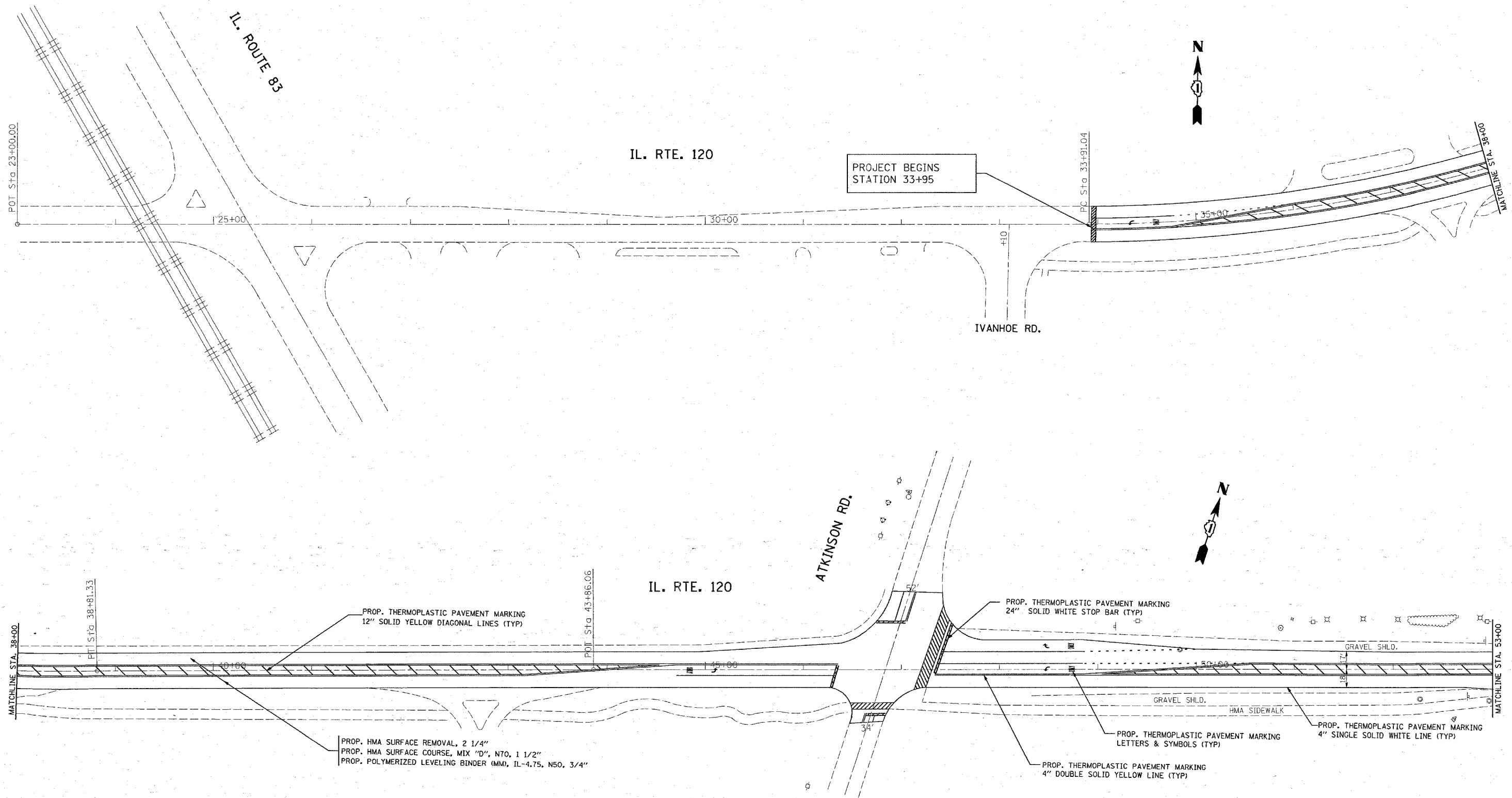
- ① EXISTING AGGREGATE SHOULDER
- ② EXISTING HOT-MIX ASPHALT BASE COURSE WIDENING (± 9")
- ③ EXISTING PCC PAVEMENT (9"-7"-9")
- ④ EXISTING HOT-MIX ASPHALT SURFACE (± 8 3/4")
- ⑤ PROP. HOT-MIX ASPHALT SURFACE REMOVAL 2 1/4"
- ⑥ PROP. POLY. LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, 3/4"
- ⑦ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑧ EXISTING HOT-MIX ASPHALT SURFACE OVERLAY
- ⑨ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

	MIXTURE TYPE	AC TYPE	AIR VOIDS
RESURFACING	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	PG 64-22	4% @ 70 GYR
	POLYMERIZED LEVELING BINDER, (MM), IL-4.75, N50	SBS/SBR PG 76-28/-22	4% @ 50 GYR
PATCHES	CLASS D PATCHES, (HMA BINDER IL-19 mm)	PG 64-22*	4% @ 70 GYR
	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES, (HMA BINDER IL-19 mm)	PG 64-22*	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.

*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22



FILE NAME =
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 PLOT DATE = 4/17/2008

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

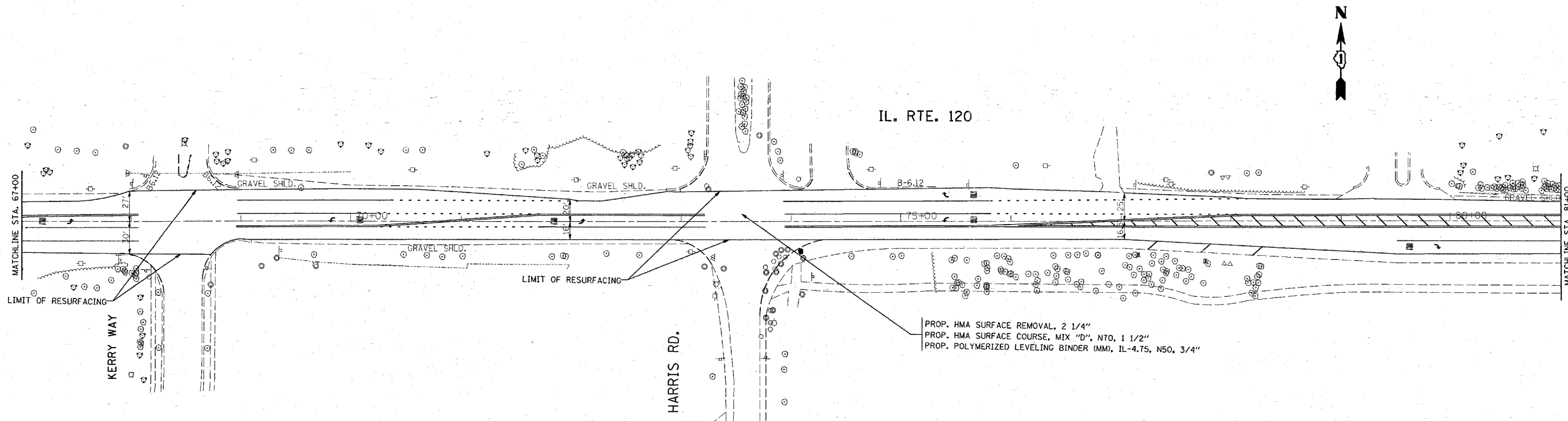
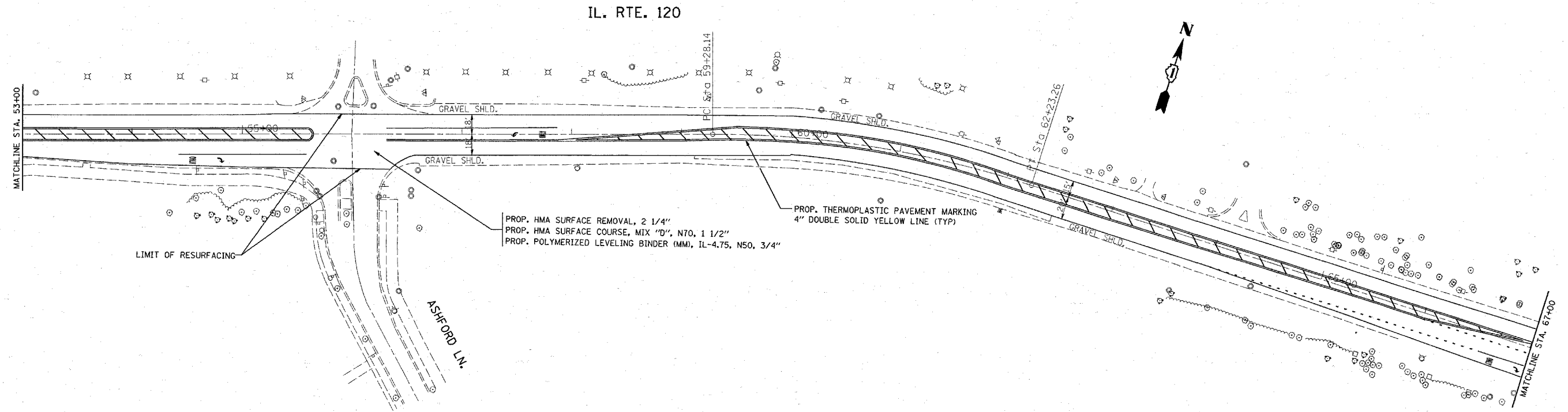
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL. ROUTE 120 (ATKINSON RD. - W/O U.S. 45)
 ROADWAY PLAN**

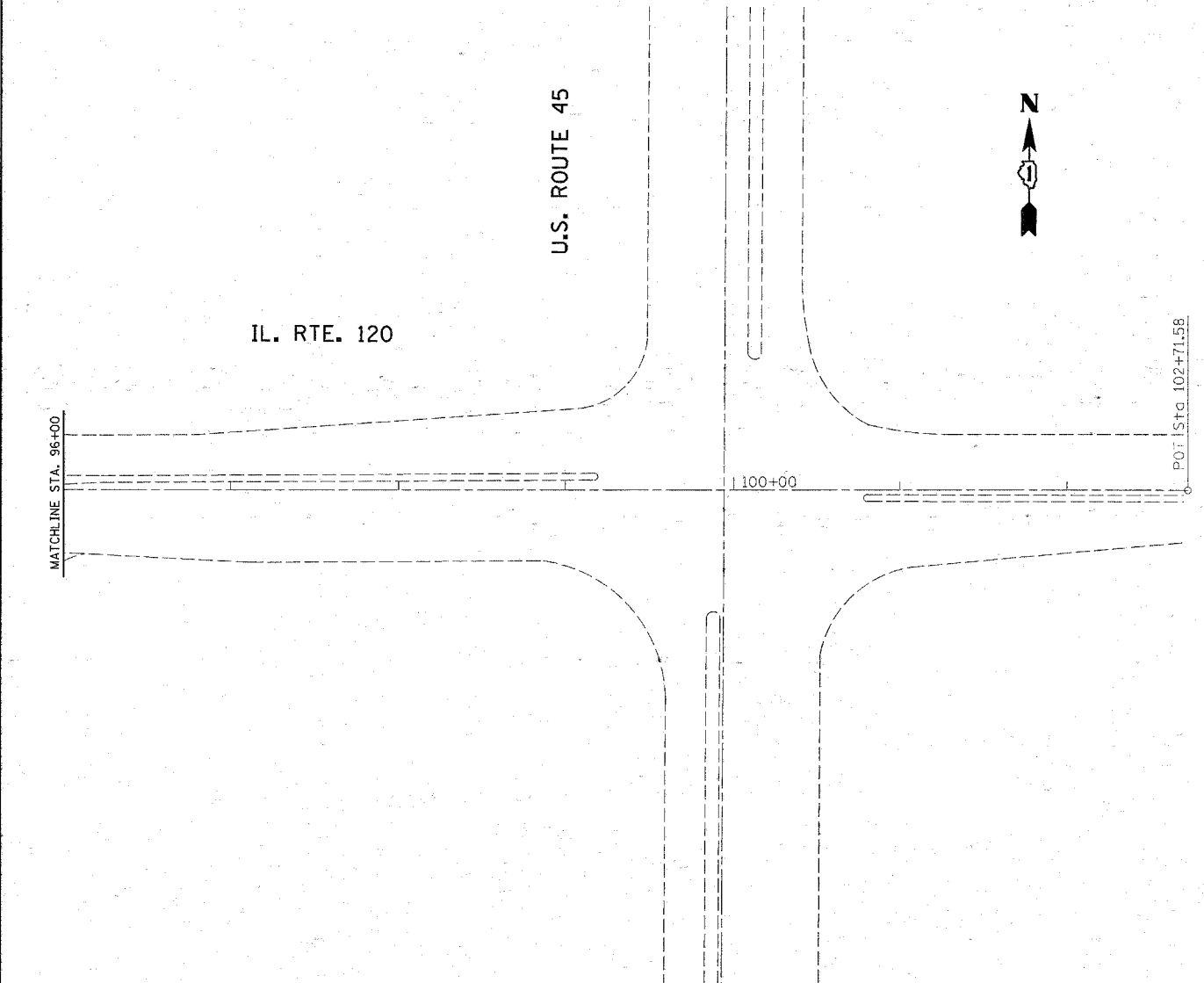
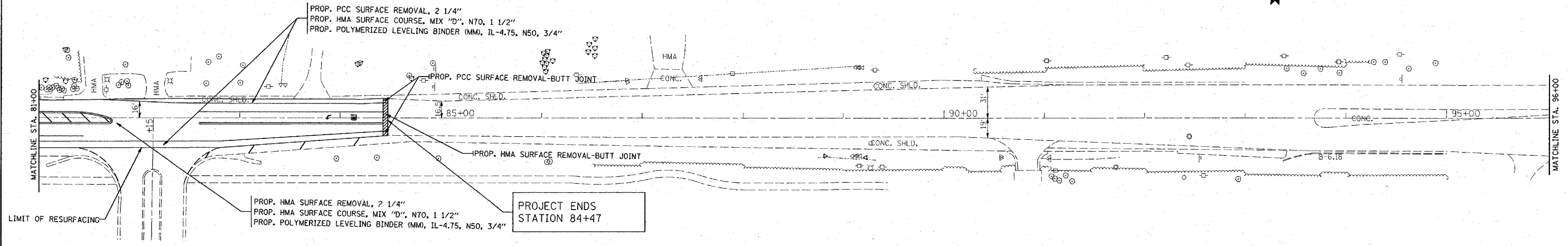
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
342	G-RS-4	LAKE	24	6
CONTRACT NO. 60C32				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



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	PLOT DATE = 4/17/2009	DATE -	REVISED -					CONTRACT NO. 60C32							

IL. RTE. 120



USER NAME = guillaumejp
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 4/17/2008

DESIGNED -
DRAWN -
CHECKED -
DATE -

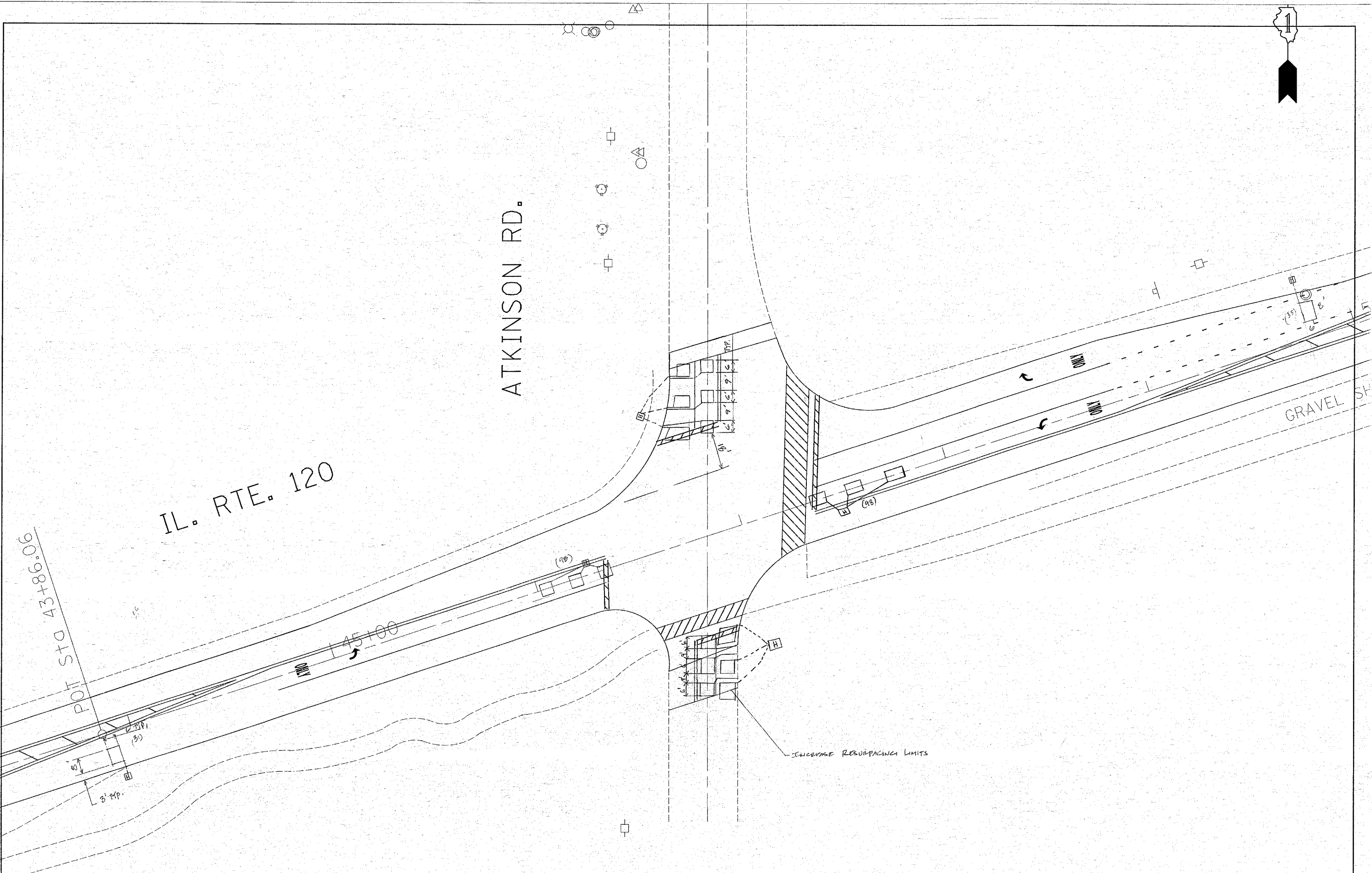
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 120 (ATKINSON RD. - W/O U.S. 45)
ROADWAY PLAN
SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
342	G-RS-4	LAKE	24	8
CONTRACT NO. 60C32			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

2/11/08
4/6/08



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	PLOT SCALE = 1/8" = 1' IN.	CHECKED -	REVISED -				CONTRACT NO. 60C32				
	PLOT DATE = 3/31/2008	DATE -	REVISED -				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

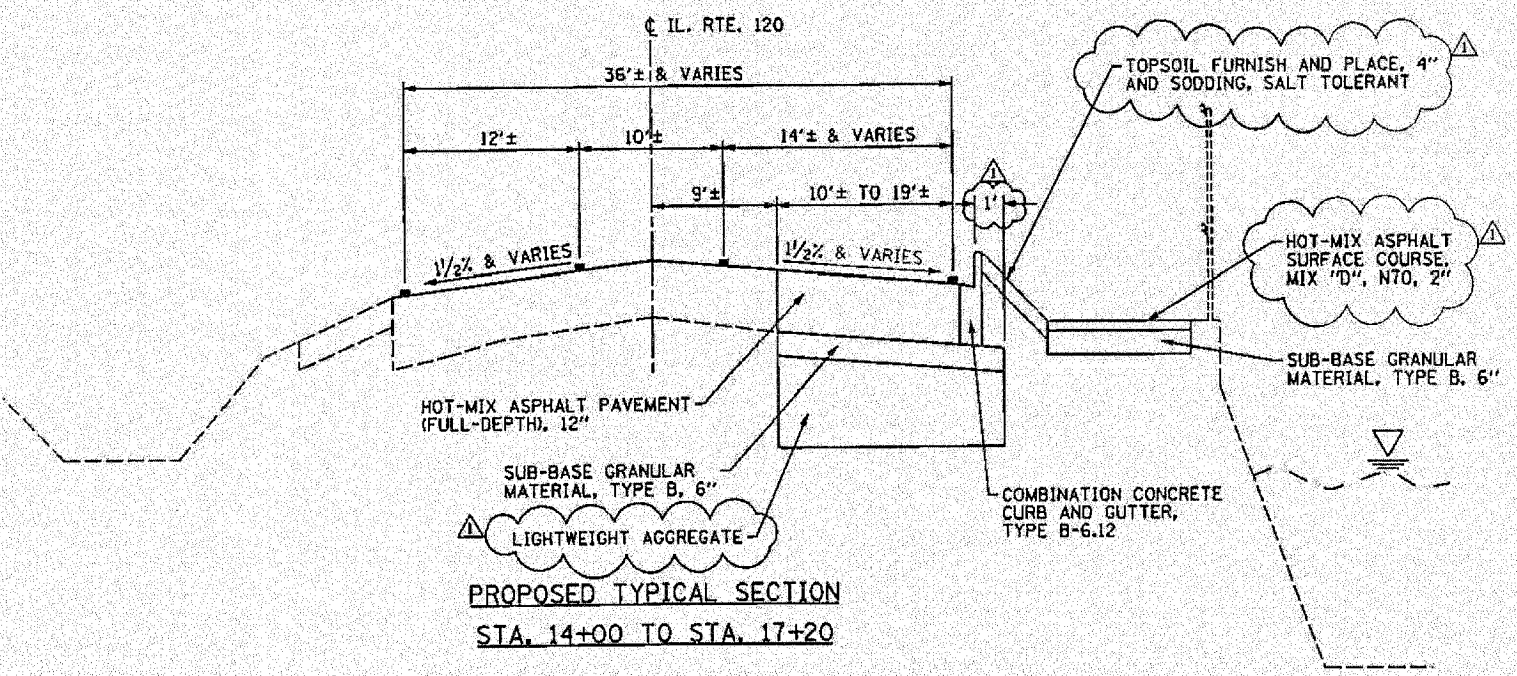
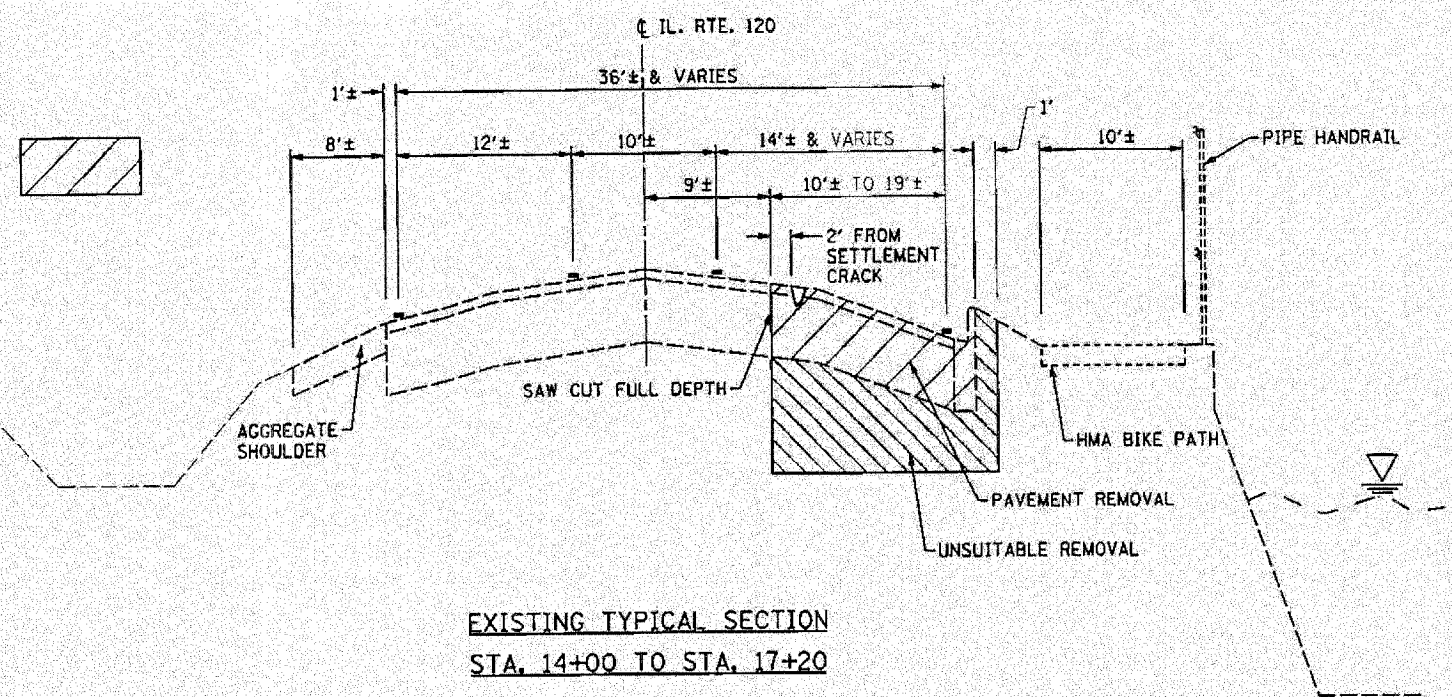
PAVEMENT REPLACEMENT - BILL OF MATERIALS		
ITEM	UNIT	TOTAL QUANTITIES
REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU. YD.	557
TOPSOIL FURNISH AND PLACE, 4"	SQ. YD.	60
SODDING, SALT TOLERANT	SQ. YD.	60
PERIMETER EROSION BARRIER	FOOT	208
SUB-BASE GRANULAR MATERIAL, TYPE B, 6"	SQ. YD.	360
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4
HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 12"	SQ. YD.	310
PAVEMENT REMOVAL	SQ. YD.	310
HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ. YD.	34
COMBINATION CURB AND GUTTER REMOVAL	FOOT	180
PROTECTIVE COAT	SQ. YD.	40
REINFORCEMENT BARS	POUND	55
PRECAST REINFORCED CONCRETE FLARED END SECTIONS, 30"	EACH	1
GRATING FOR CONCRETE FLARED END SECTION, 30"	EACH	1
CONCRETE COLLAR	CU. YD.	0.5
STORM SEWERS, TYPE 2, REINFORCED CONCRETE CULVERT, STORM DRAIN, AND SEWER PIPE, CLASS III, 12"	FOOT	6
STORM SEWERS, TYPE 2, REINFORCED CONCRETE CULVERT, STORM DRAIN, AND SEWER PIPE, CLASS III, 30"	FOOT	32
STORM SEWERS TO BE CLEANED	FOOT	40
STORM SEWER REMOVAL, 30"	FOOT	40
MANHOLES TO BE ADJUSTED	EACH	1
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	180
TRAFFIC CONTROL AND PROTECTION, STANDARD T01501	L. SUM	1
SHORT-TERM PAVEMENT MARKING	FOOT	240
TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ. FT.	60
TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	4,000
TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	400
TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	300
TEMPORARY CONCRETE BARRIER	FOOT	388
PAVEMENT MARKING REMOVAL	SQ. FT.	300
LIGHTWEIGHT AGGREGATE	CU. YD.	521
REMOVING AND RE-ERECTING EXISTING RAILING	FOOT	20
REMOVE AND REPLACE STONE RIPRAP	CU. YD.	24
PERMANENT STEEL SHEET PILING	SQ. FT.	2,520
IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 2	EACH	2

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AC TYPE	AIR VOIDS
FULL-DEPTH PAVEMENT		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm); 2"	PG 64-22	4% @ 70 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50; 10"	PG 64-22*	4% @ 50 GYR.
HOT-MIX ASPHALT BIKEWAY		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm); 2"	PG 64-22	4% @ 70 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS / SQ. YD. / IN.

* WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.

LEGEND
REMOVAL ITEMS



PAVEMENT REPLACEMENT GENERAL NOTES

- LOCATIONS OF EXISTING UTILITIES SHOWN ON THE PLANS ARE FROM BEST AVAILABLE RECORD INFORMATION AND MUST BE FIELD VERIFIED BY THE CONTRACTOR.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
- THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS WHICH WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE VILLAGE OF GRAYSLAKE.
- ALL PAVEMENT MARKINGS REMOVED DURING CONSTRUCTION SHALL BE REPLACED WITH TEMP. PAVEMENT MARKINGS.
- FIELD VERIFICATION SHALL BE INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
- THE COST OF DEWATERING SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

FILE NAME = 1824C-ah1-101001.dgn
PLOT DATE = 5/28/2008

CHRISTIAN EDGE & ASSOCIATES, INC.
ENGINEERS-PLANNERS-SURVEYORS
171 WEST WACKER DRIVE
CHICAGO, ILLINOIS 60606
PHONE: (312) 572-2073 FAX: (312) 372-5274

DESIGNED - S.J.P.
DRAWN - A.C.S.
CHECKED - E.J.M.
DATE - APRIL 9, 2008

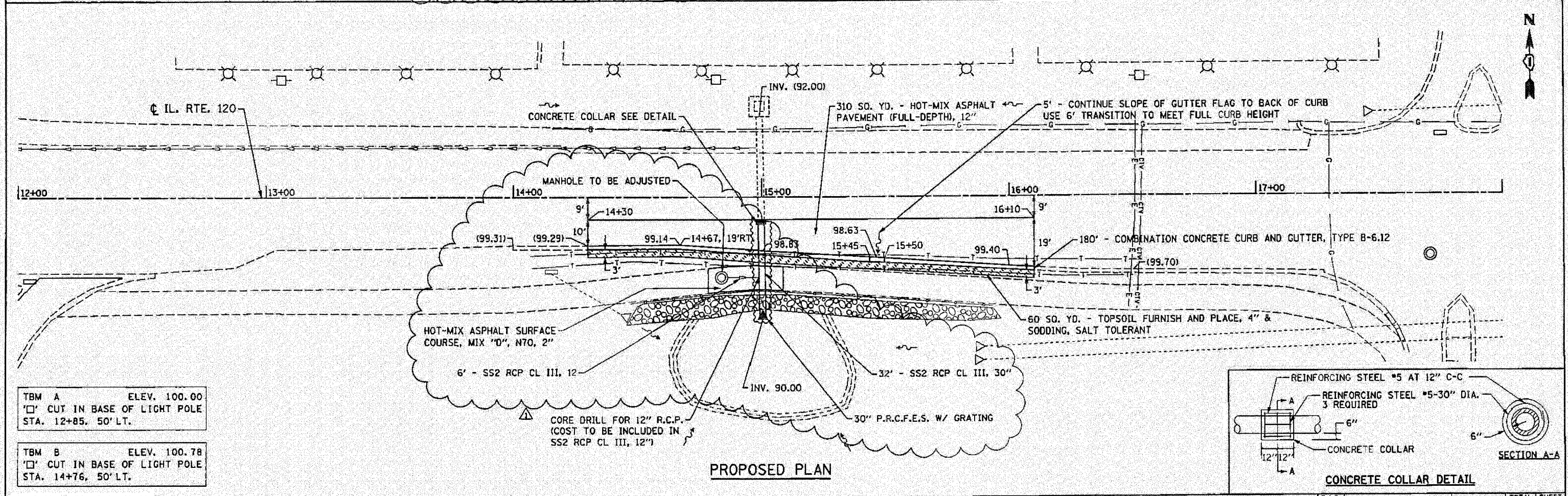
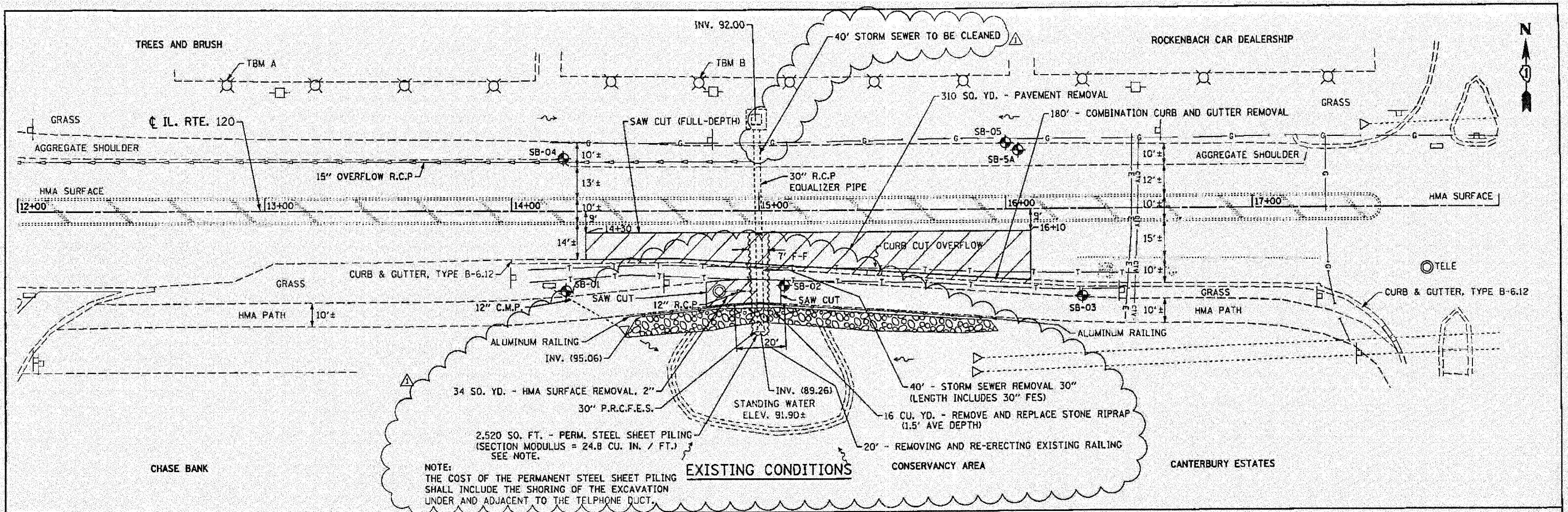
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT REPLACEMENT - GENERAL NOTES, TYPICAL SECTION AND DETAILS IL RTE 120
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. 14+00 TO STA. 17+20

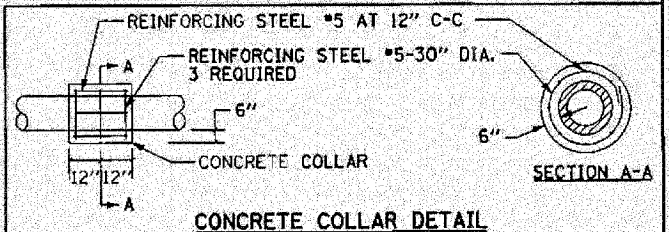
F.A.P. RTE. 342	SECTION G-RS-4	COUNTY LAKE	TOTAL SHEETS 24	SHEET NO. 10
CONTRACT NO. 60C32			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

(6-2-08)
KE



TBM A ELEV. 100.00
 6" CUT IN BASE OF LIGHT POLE
 STA. 12+85, 50' LT.

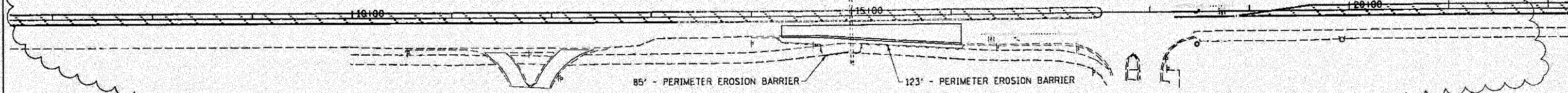
TBM B ELEV. 100.78
 6" CUT IN BASE OF LIGHT POLE
 STA. 14+76, 50' LT.



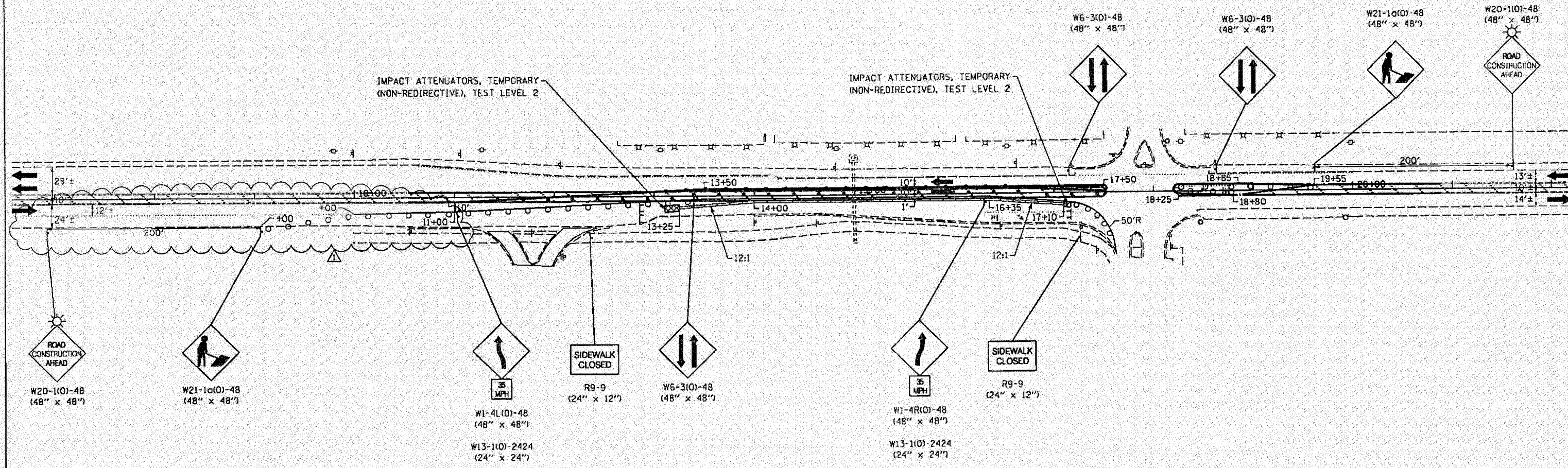
FILE NAME = 1824C-dwt-plrpf.dgn PLOT DATE = 5/28/2008	CHRISTIAN-ROSE & ASSOCIATES, INC. ENGINEERS-PLANNERS-SURVEYORS 211 WEST WACKER DRIVE CHICAGO, ILLINOIS 60606 PHONE: (312)372-2023 FAX: (312)372-5274	DESIGNED - S.J.P. DRAWN - A.C.S. CHECKED - E.J.M. DATE - APRIL 9, 2008	REVISED - Δ ADD. 1 - 05-28-2008 REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT REPLACEMENT - EXISTING CONDITIONS AND PROPOSED PLAN IL. RTE. 120 SCALE: 1" = 20' SHEET NO. 1 OF 1 SHEETS STA. 12+00 TO STA. 18+00	F.A.P. RTE. 342 SECTION G-RS-4 COUNTY LAKE TOTAL SHEETS 24 SHEET NO. 11 CONTRACT NO. 60C32 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT
--	---	---	---	---	--	---

EROSION CONTROL NOTE:

ALL PUMP DISCHARGES SHALL BE FILTERED THROUGH A PORTABLE SEDIMENT TANK BEFORE OUTLETTING.



EROSION CONTROL PLAN



MAINTENANCE OF TRAFFIC

LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- PAVEMENT MARKING REMOVAL
- TEMPORARY CONCRETE BARRIER
- TYPE III BARRICADES WITH FLASHING LIGHTS
- SIGN
- FLEXIBLE DELINEATORS @ 20' C-C
- BARRICADES OR DRUMS WITH BI-DIRECTIONAL STEADY BURNING LIGHTS @ 20' C-C

MAINTENANCE OF TRAFFIC NOTES:
 THE MAINTENANCE OF TRAFFIC FOR THE PAVEMENT REPLACEMENT WILL BE PAID FOR AS "TRAFFIC CONTROL AND PROTECTION STANDARD 701501."

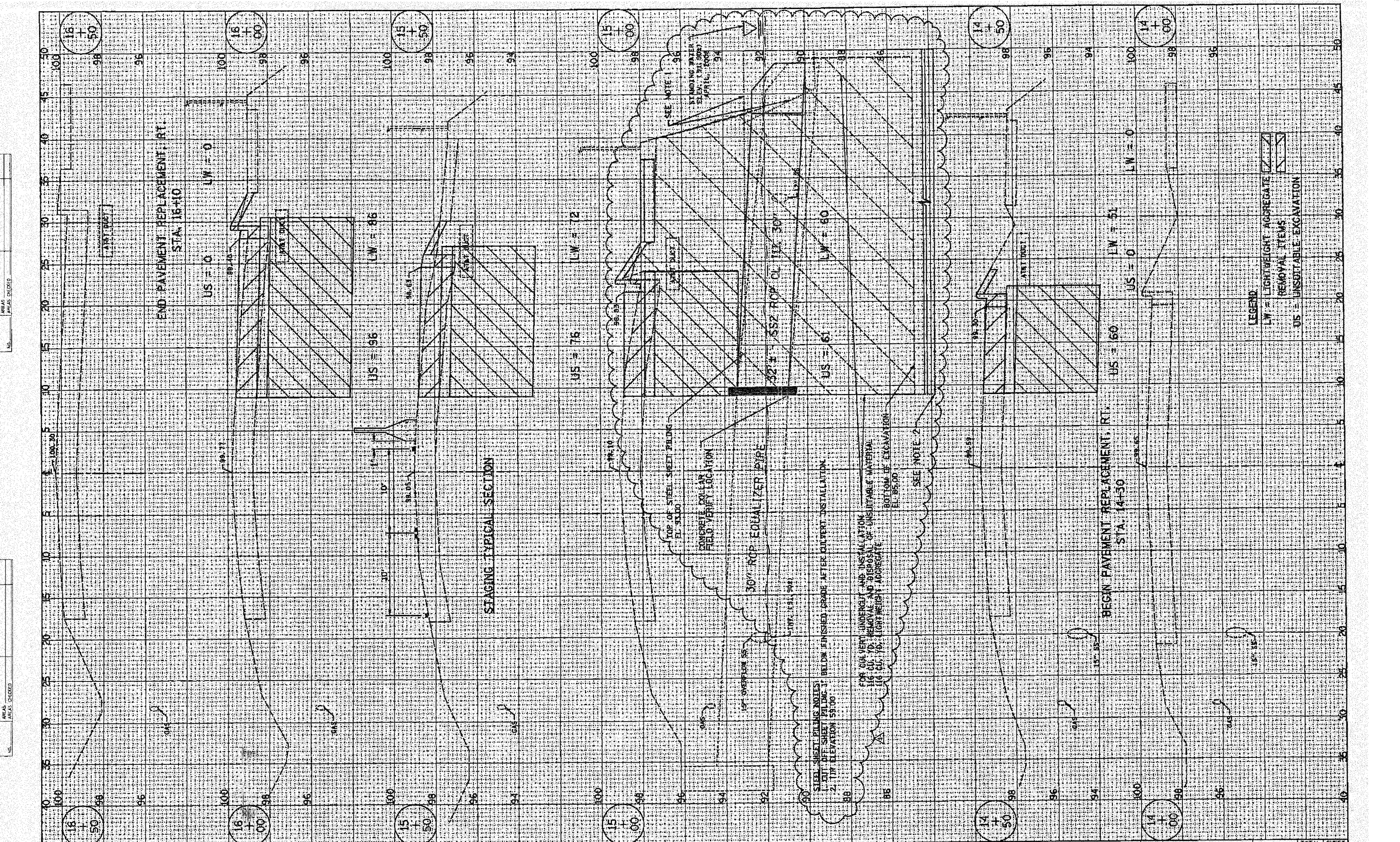
FILE NAME - 1824C-110-staging.dgn	DESIGNED - S.J.P.	REVISED - ADD. 1 05-28-2008
PLOT DATE - 5/29/2008	DRAWN - A.C.S.	REVISED -
(6/2/08) KE	CHECKED - E.J.M.	REVISED -
	DATE - APRIL 9, 2008	REVISED -

CHRISTIAN EDGE & ASSOCIATES, INC.
 ENGINEERS-PLANNERS-SURVEYORS
 211 WEST WALKER DRIVE
 CHICAGO, ILLINOIS 60604
 PHONE: (312)372-2023 FAX: (312)372-5274

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT REPLACEMENT - MAINTENANCE OF TRAFFIC AND EROSION CONTROL PLAN IL RTE. 120
 SCALE: 1" = 50' SHEET NO. 1 OF 1 SHEETS STA. 7+00 TO STA. 22+00

F.A.P. RTE. 342	SECTION G-RS-4	COUNTY LAKE	TOTAL SHEETS 24	SHEET NO. 12
CONTRACT NO. 60C32				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



END PAVEMENT REPLACEMENT, RT.
STA. 16+10

BEGIN PAVEMENT REPLACEMENT, RT.
STA. 14+50

STAGING TYPICAL SECTION

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT REPLACEMENT - CROSS SECTIONS
IL RTE. 120

FILE NAME = 1824C-ah-t-waah-L.dgn
PLOT DATE = 5/21/2008

CR
CHRISTIAN BOGE & ASSOCIATES, INC.
ENGINEERS-PLANNERS-SURVEYORS
711 WEST WALCKER DRIVE
CHICAGO, ILLINOIS 60606
PHONE: (312) 372-2023 FAX: (312) 372-5274

DESIGNED - S.J.P.	REVISED - ADD. 1 05-28-2008
DRAWN - A.C.S.	REVISED -
CHECKED - E.J.M.	REVISED -
DATE - APRIL 9, 2008	REVISED -

SCALE: 1" = 5' SHEET NO. 1 OF 1 SHEETS STA. 14+00 TO STA. 16+50

F.A.P. RTE. 342	SECTION 6-RS-4	COUNTY LAKE	TOTAL SHEETS 24	SHEET NO. 13
CONTRACT NO. 60C32				
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

(6/2/08) KE

LEGEND
LW = LIGHTWEIGHT AGGREGATE
REMOVAL ITEMS
US = UNSUITABLE EXCAVATION

STEEL SHEET PILING NOTES:
1. CUT OFF SHEET PILING 3' BELOW FINISHED GRADE AFTER CURBENT INSTALLATION
2. TIP ELEVATION 58'00"

FOR USE VERT. UNDERCUT AND INSTALLATION
1. 60 YD. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
2. 16 CU. YD. LIGHTWEIGHT AGGREGATE
BOTTOM OF EXCAVATION
EL. 95.00

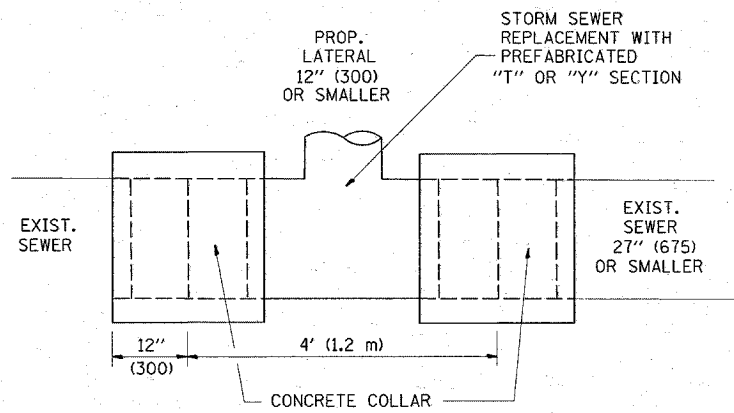
TOP OF STEEL SHEET PILING
EL. 91.00
CONCRETE DOLAR FIELD VERIFY LOCATION

30" ROP EQUALIZER PIPE
LIT. (31.50)

SEE NOTE 1

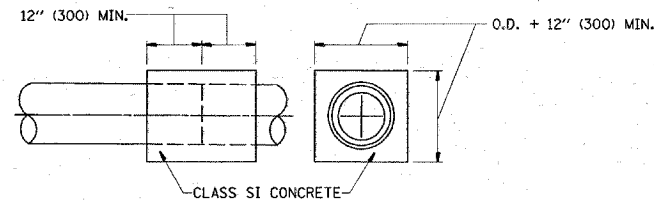
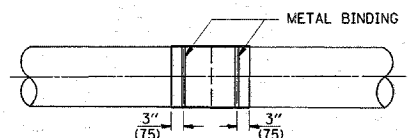
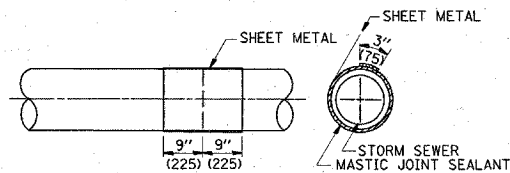
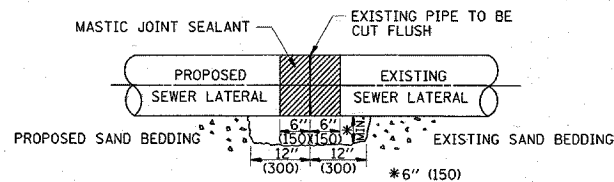
SEE NOTE 2

STANDING WATER
EL. 91.50 (1.50)
APPLIC. ROCK



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

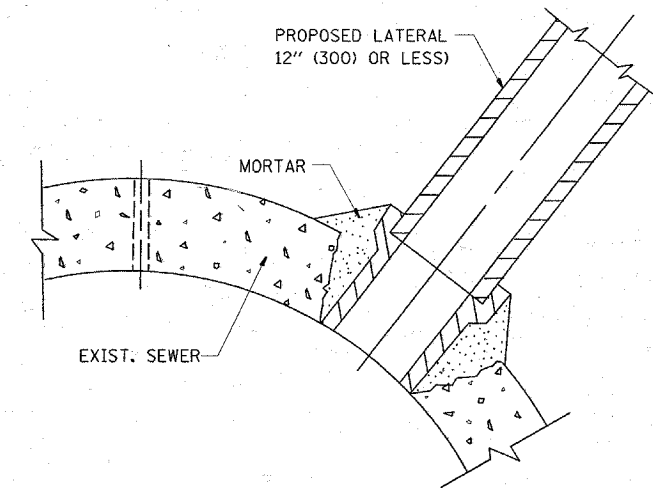


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

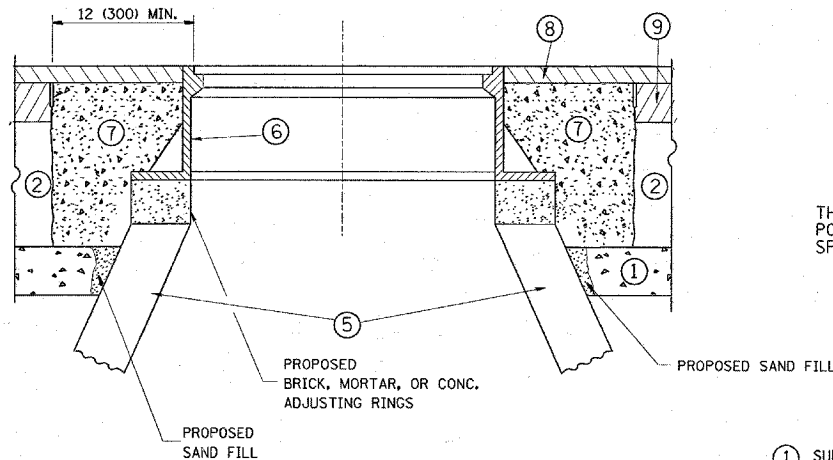
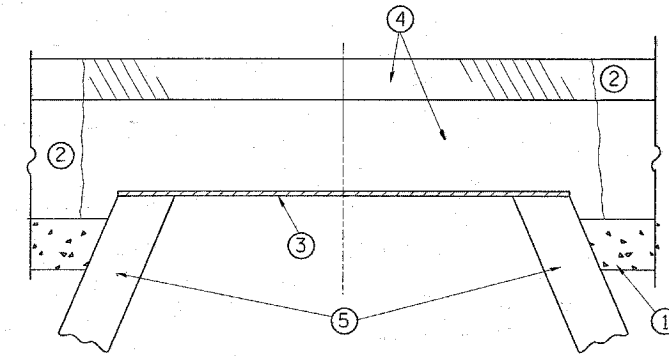
REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = M:\diststd\22x34\bd07.dgn	USER NAME = guillaumejp	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER		F.A.P. RTE. 342	SECTION G-RS-4	COUNTY Lake	TOTAL SHEETS 24	SHEET NO. 14
	PLOT SCALE = 50.000 ' / IN.	DRAWN -	REVISED - R. SHAH 09-09-94		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. 60C32		
	PLOT DATE = 4/17/2008	CHECKED -	REVISED - R. SHAH 10-25-94		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						
		DATE - 07-25-90	REVISED - R. SHAH 06-12-96								



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

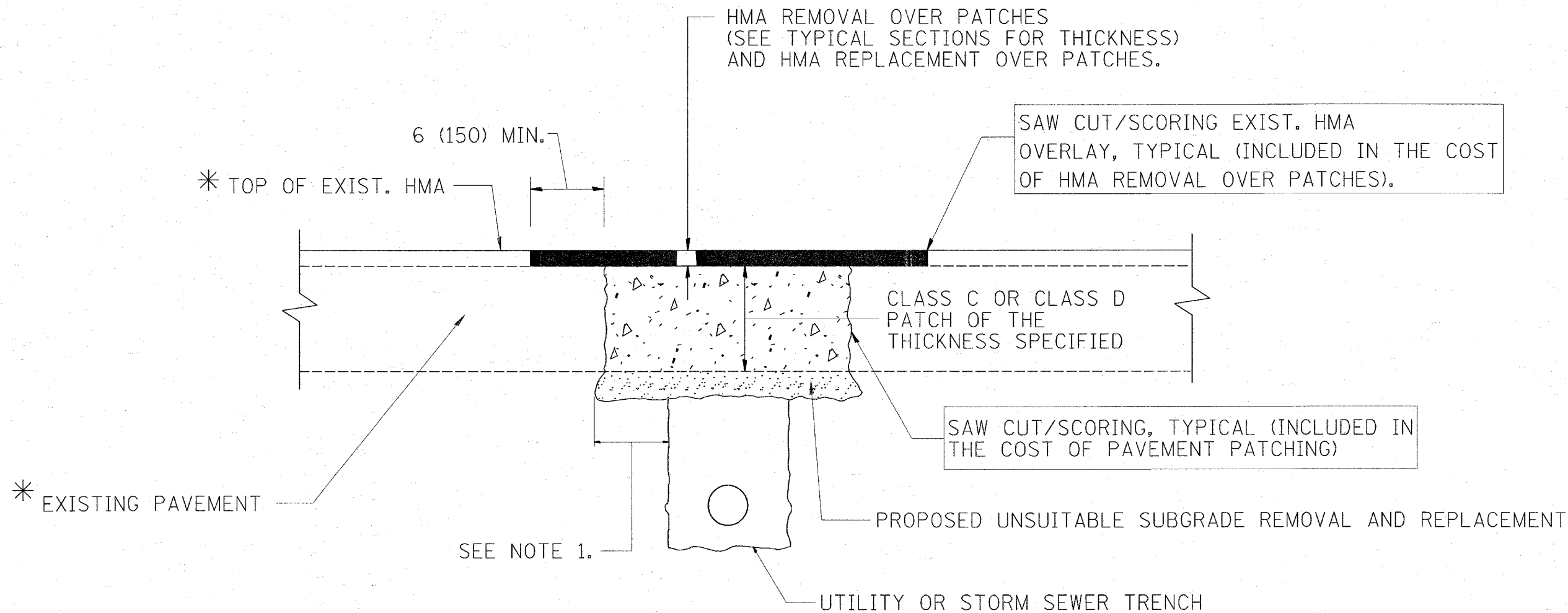
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\diststd\22x34\bd08.dgn	USER NAME = guillaumejp	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. WIEDEMAN 05-14-04
	PLOT DATE = 4/2/2008	DATE - 10-25-94	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING		F.A.P. RTE. 342	SECTION G-R5-4	COUNTY LAKE	TOTAL SHEETS 24	SHEET NO. 15
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		CONTRACT NO. 60C32	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

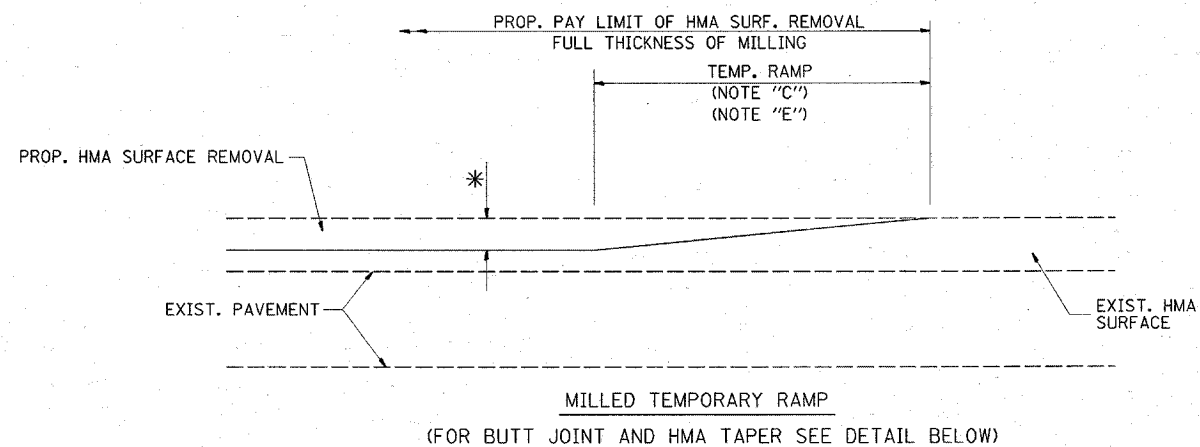
1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

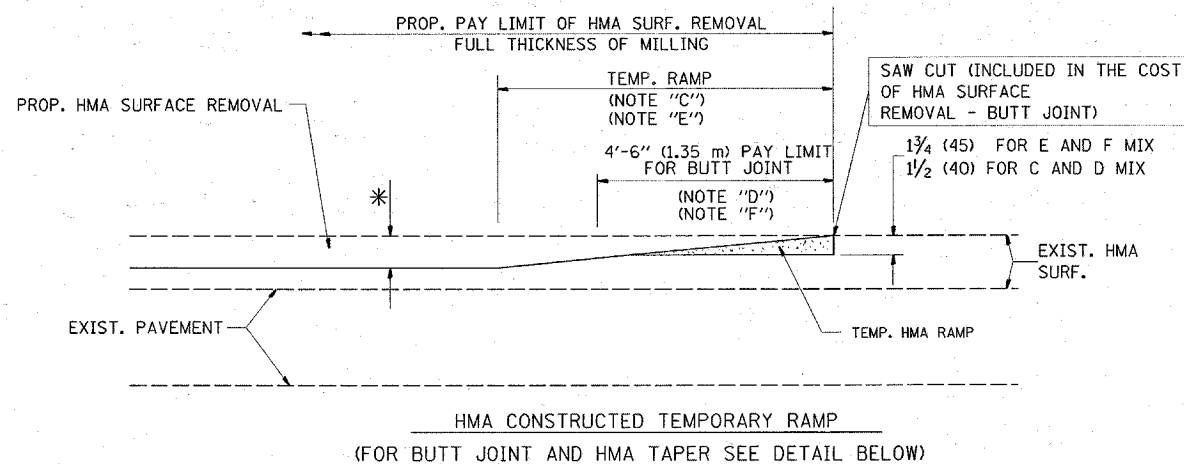
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = w:\distatd\22x34\bd22.dgn	USER NAME = guillaumsfp	DESIGNED - R. SHAH	REVISED - A. ABBAS 01-20-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.P. RTE. 342	SECTION G-RS-4	COUNTY LAKE	TOTAL SHEETS 24	SHEET NO. 16
PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 01-01-07	BD400-04 (BD-22)			CONTRACT NO. 60C32				
PLOT DATE = 4/2/2008	DATE - 10-25-94	REVISED - R. BORO 09-04-07	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS				STA.	TO STA.			

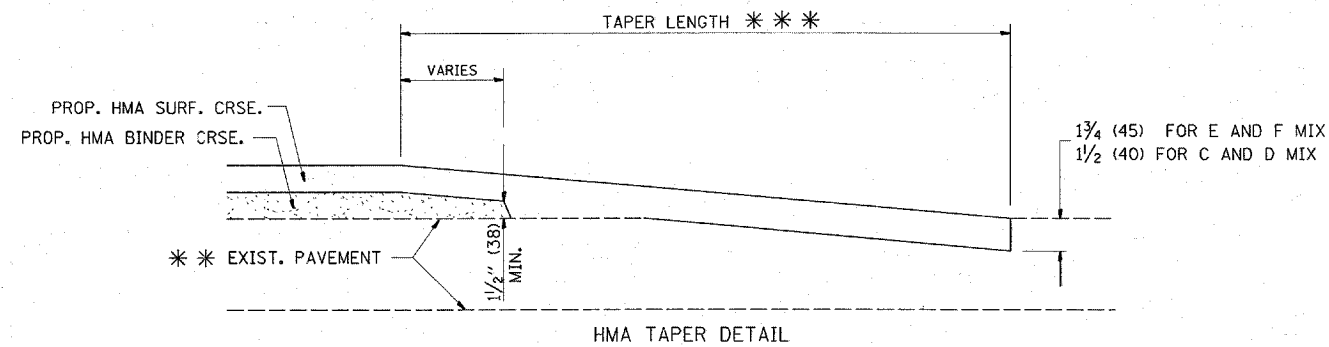
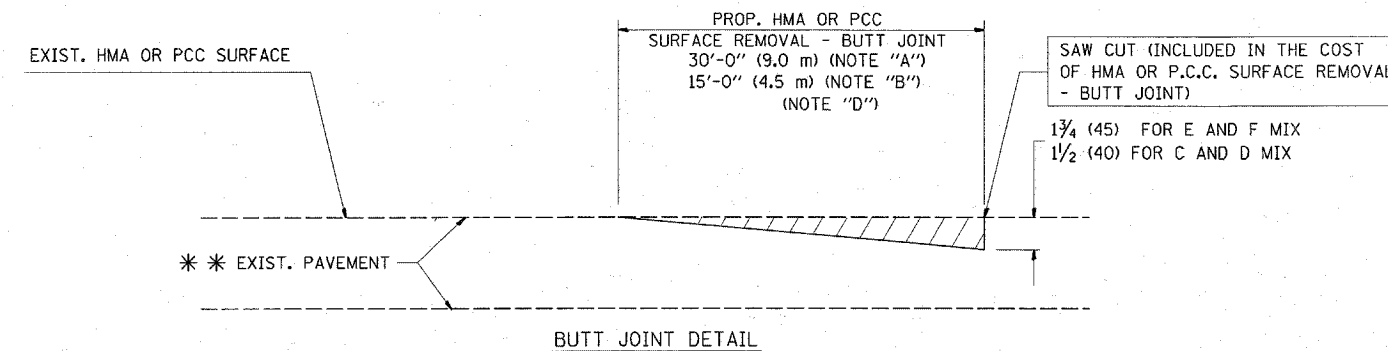


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

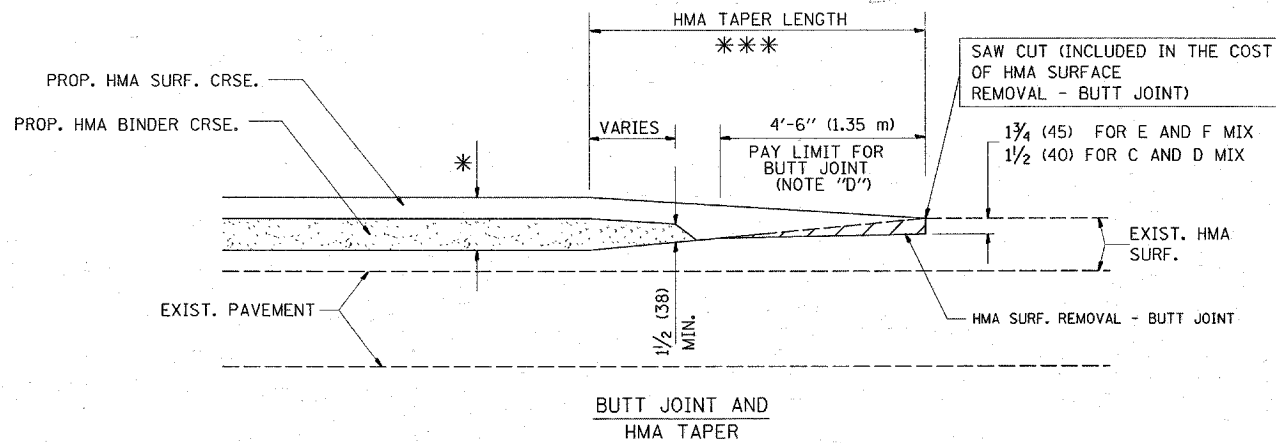
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

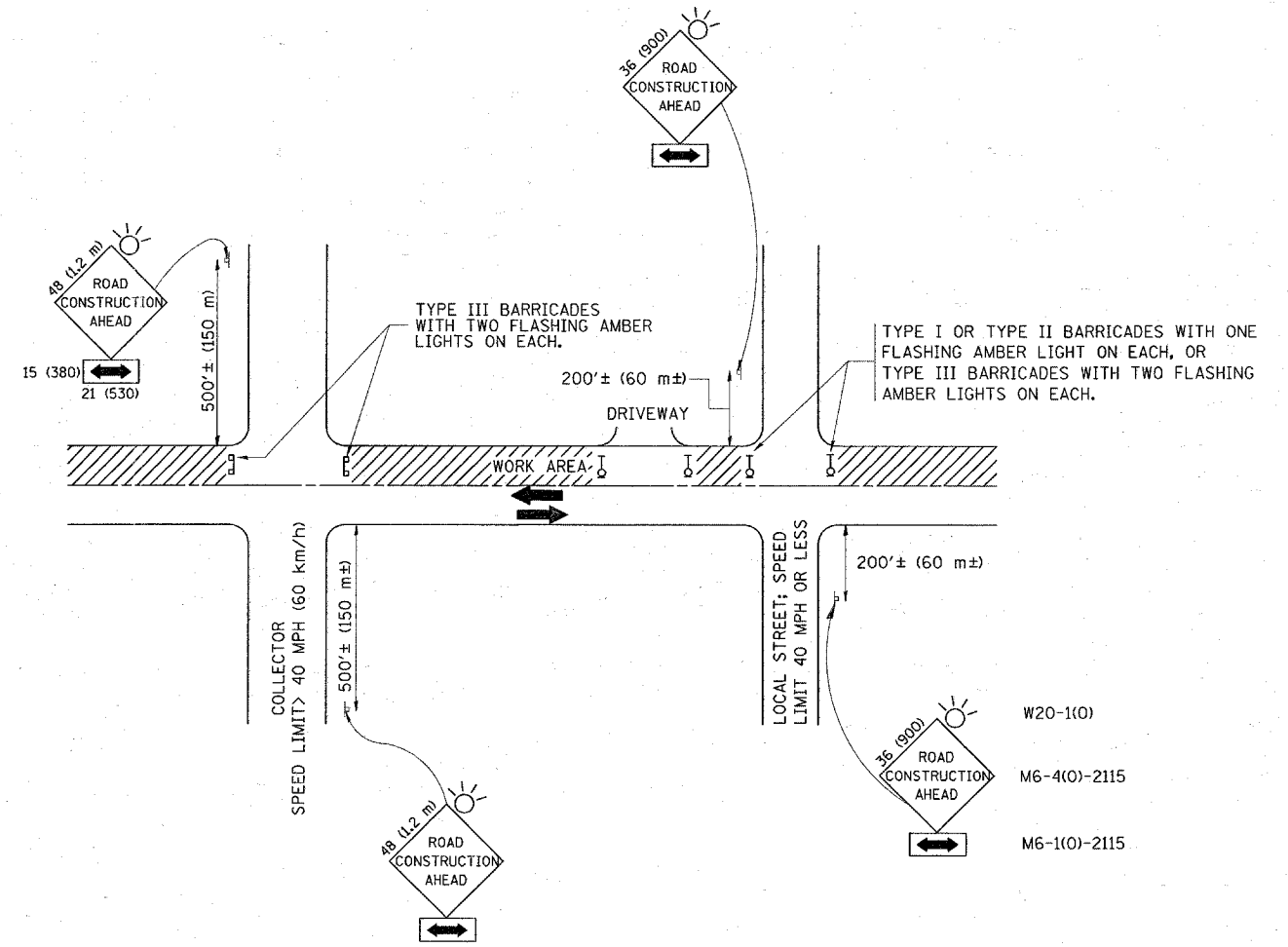
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 4/2/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
342	G-85-4	LAKE	24	17
BD400-05 BD32		CONTRACT NO. 6032		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

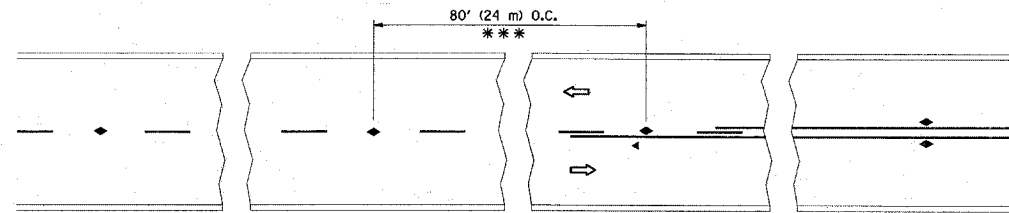
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = guillaumejp	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
W:\data\td\22x34\td10.dgn		DRAWN -	REVISED - A. HOUSEH 03-06-96
		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

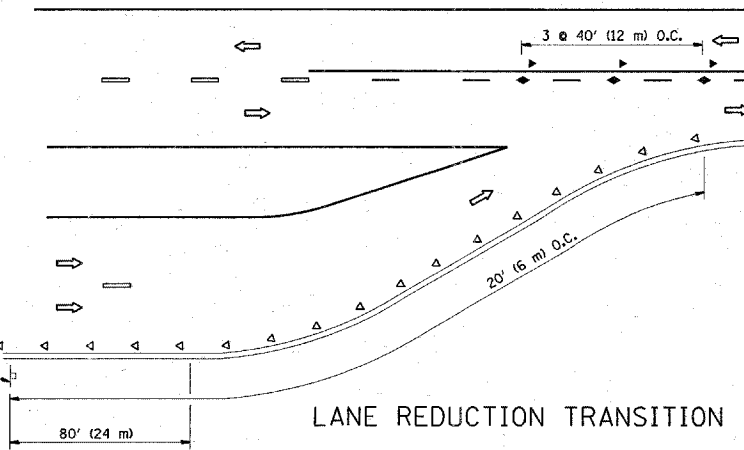
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	G-RS-7	LAKE	24	18
TC-10			CONTRACT NO. 60C32	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

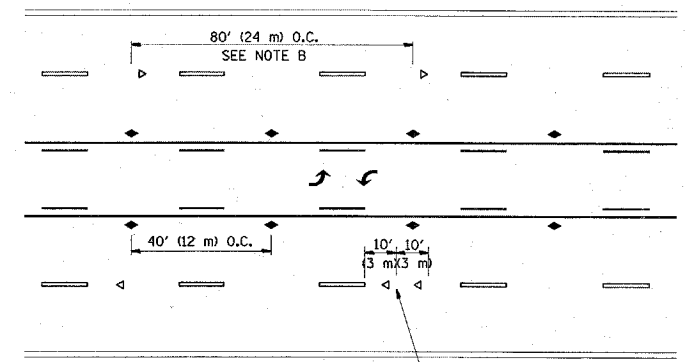


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

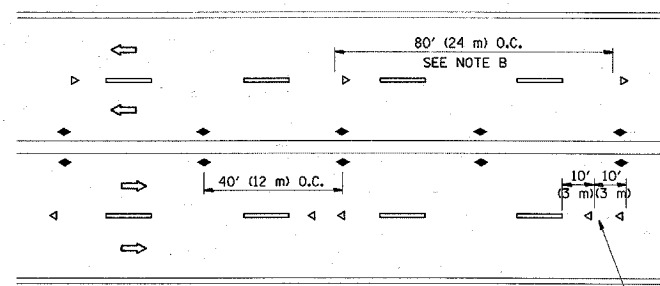
TWO-LANE/TWO-WAY



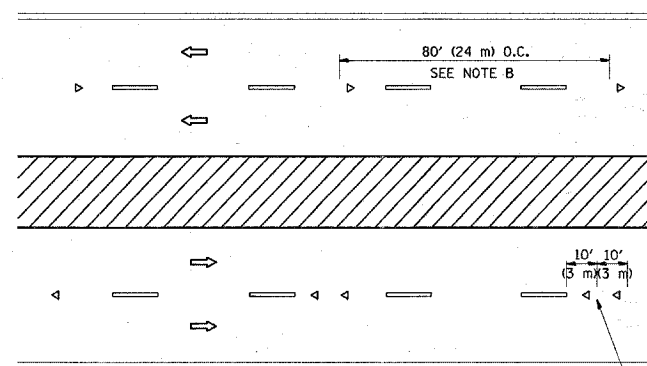
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

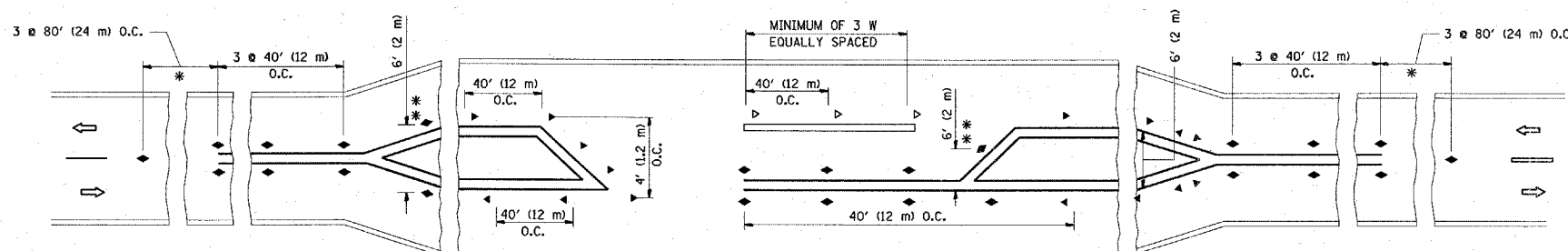
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

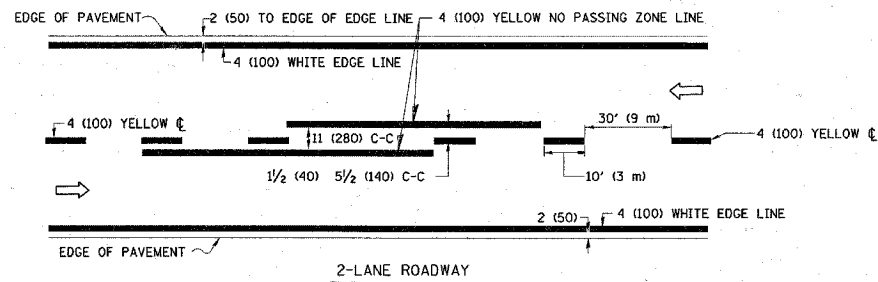
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

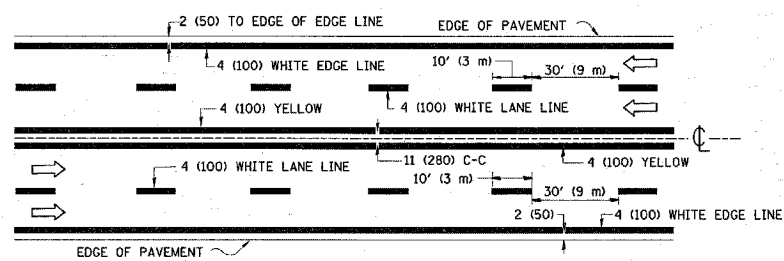
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	PLOT SCALE = 500.000' / IN. PLOT DATE = 4/2/2008	CHECKED - DATE -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

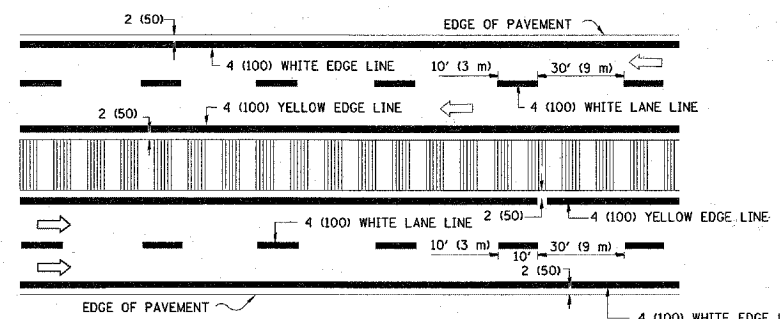
TYPICAL APPLICATIONS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)		342	G-RS-7	LAKE	24	19
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		CONTRACT NO. 60C32		
		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



2-LANE ROADWAY



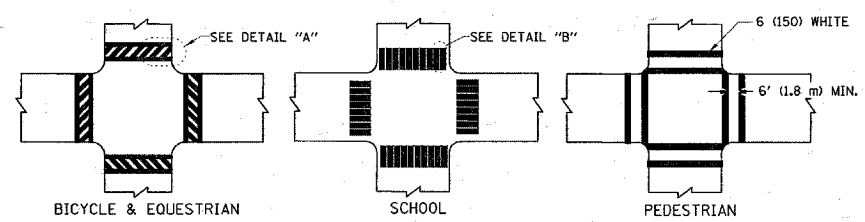
MULTI-LANE UNDIVIDED



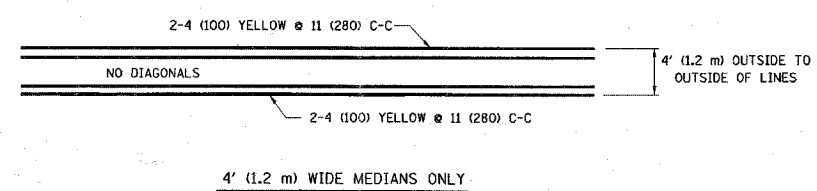
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

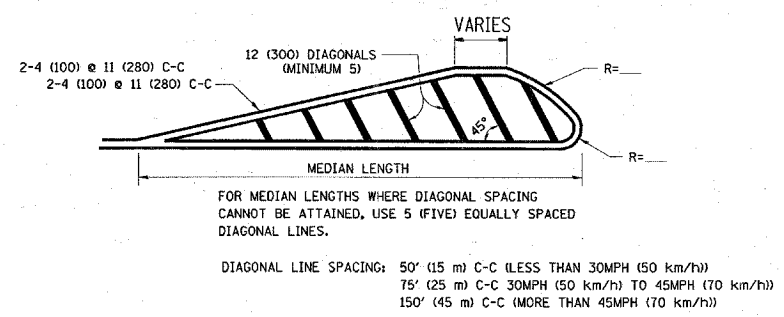
TYPICAL LANE AND EDGE LINE MARKING



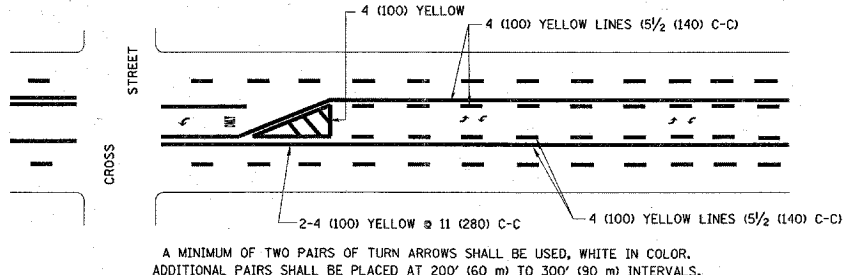
TYPICAL CROSSWALK MARKING



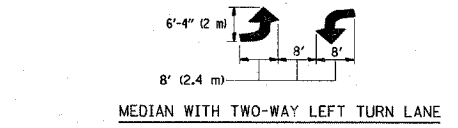
4' (1.2 m) WIDE MEDIANS ONLY



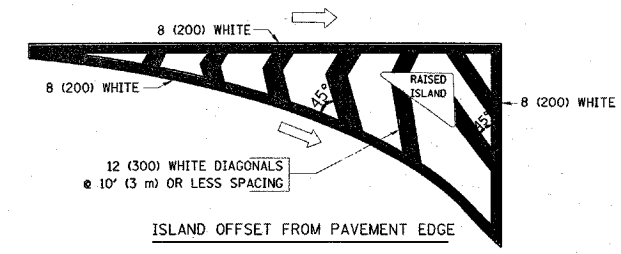
MEDIANS OVER 4' (1.2 m) WIDE



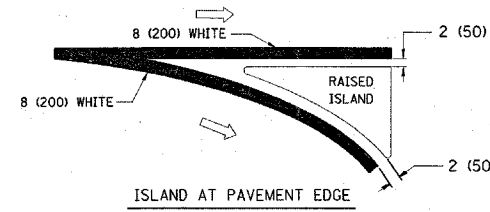
TYPICAL PAINTED MEDIAN MARKING



TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE

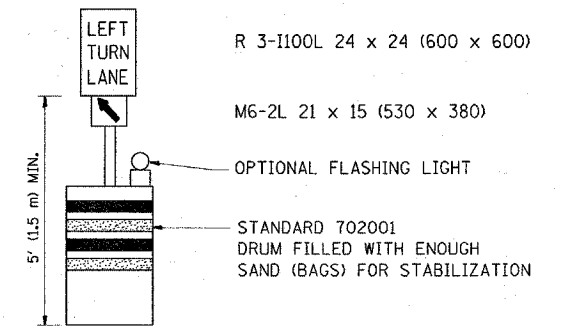
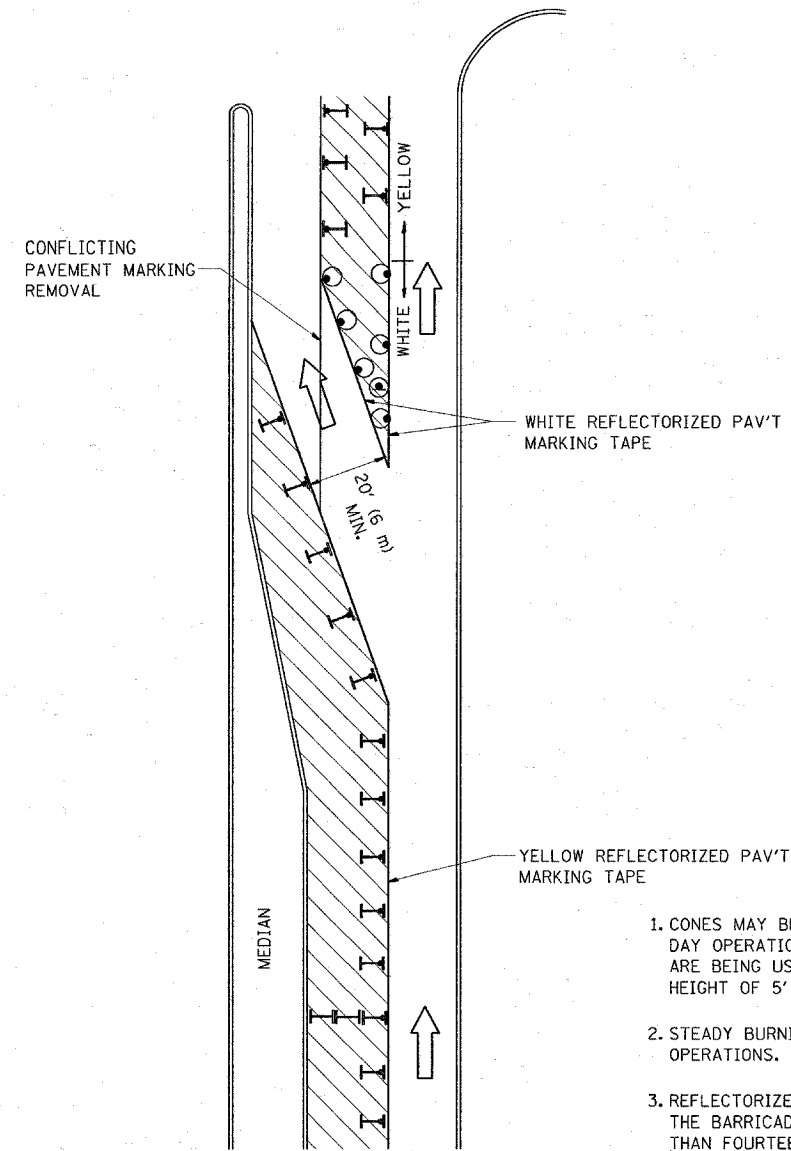


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL))	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

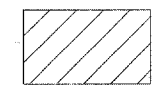





All dimensions are in inches (millimeters) unless otherwise shown.



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

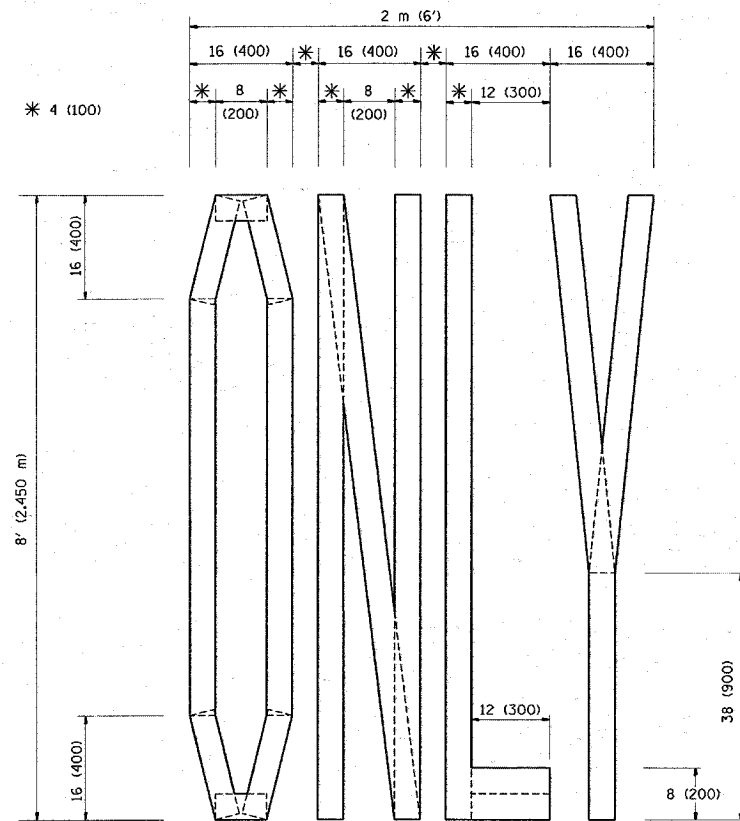
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		CHECKED -	REVISED - A. HOUSEH 10-12-96
		DATE -	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

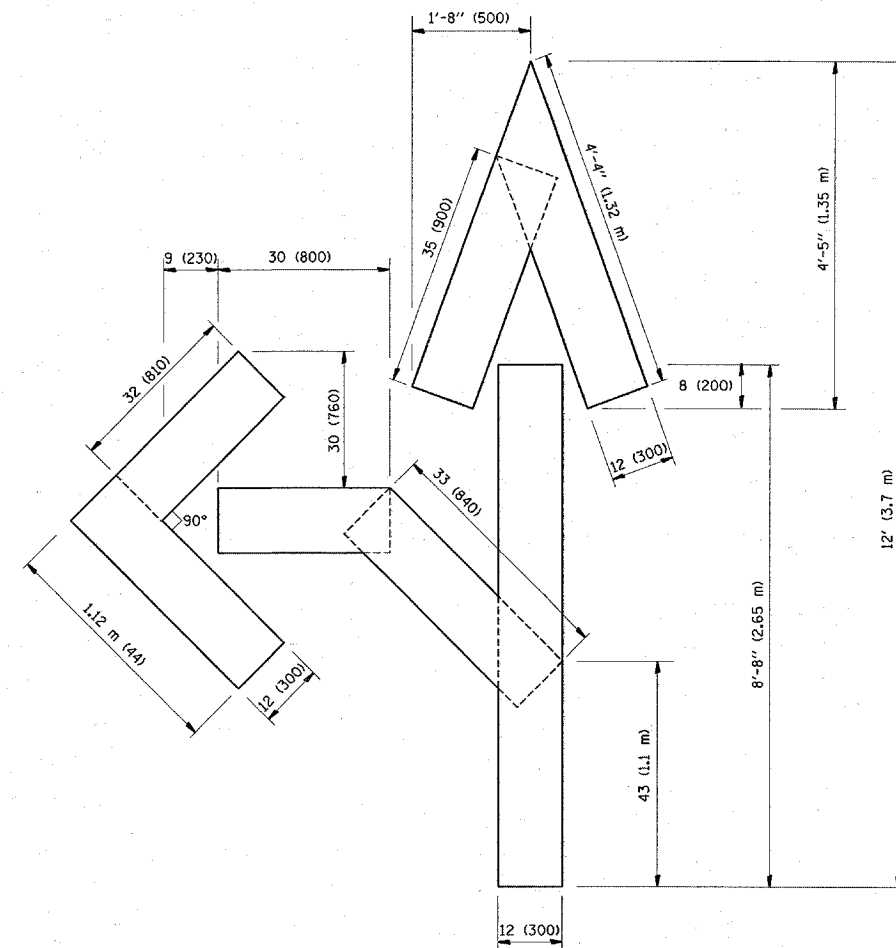
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

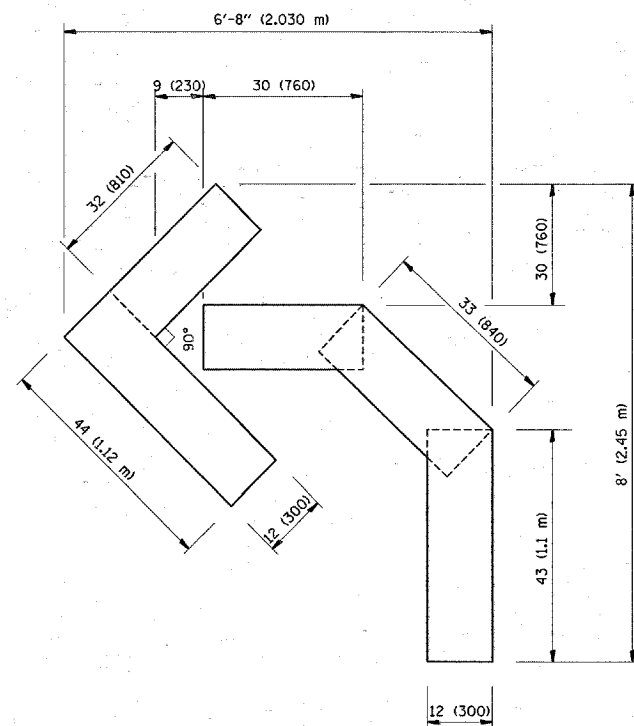
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	G-RS-9	LAKE	24	21
	TC-14		CONTRACT NO. 60C32	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

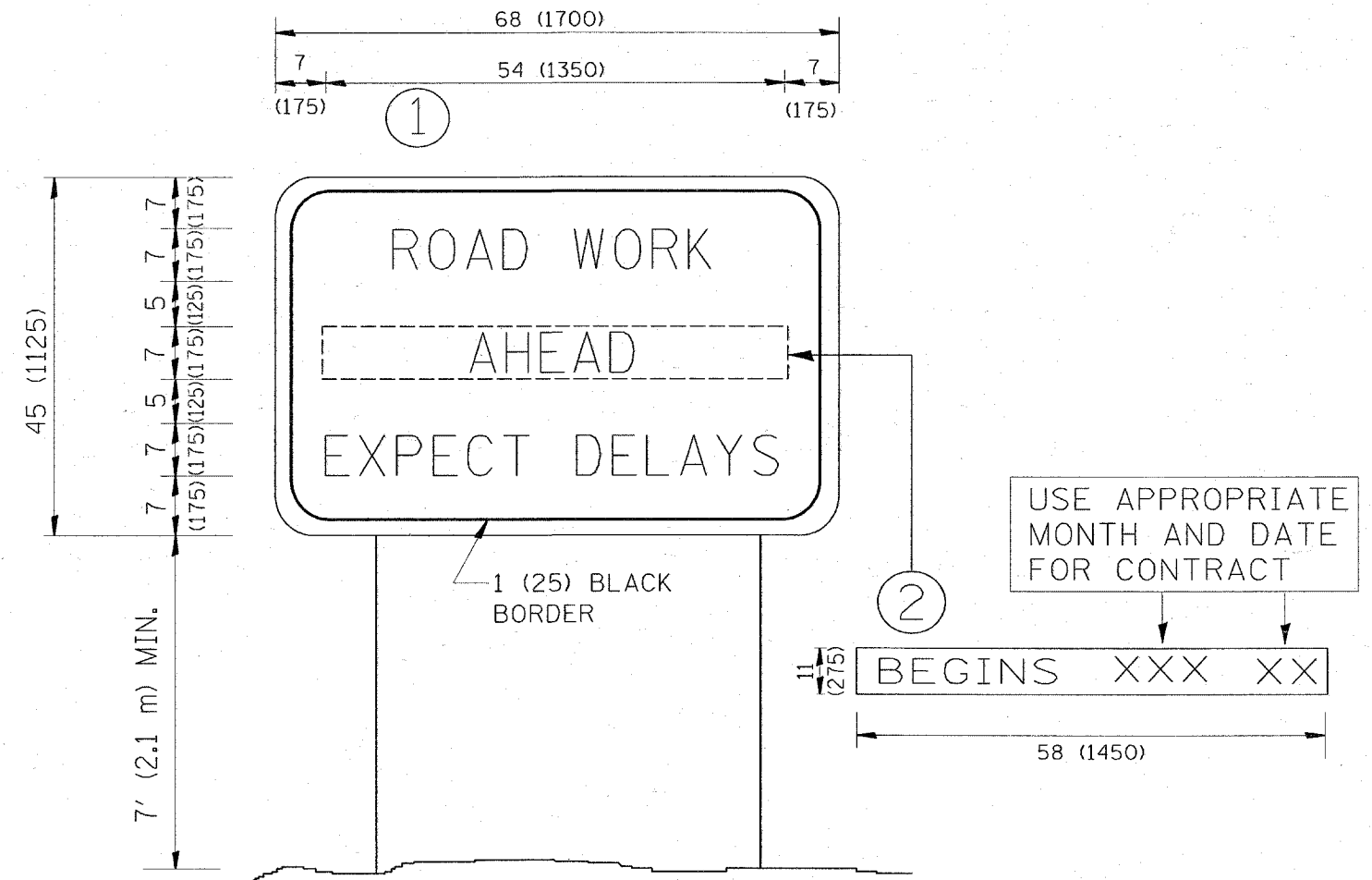
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PLOT SCALE = 50.0000 / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98	
PLOT DATE = 4/2/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	G-RS-4	LAKE	24	22
TC-16			CONTRACT NO. 60132	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\to22.dgn	USER NAME = guillaumejp	DESIGNED -	REVISED - R. MIRS 09-15-97
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

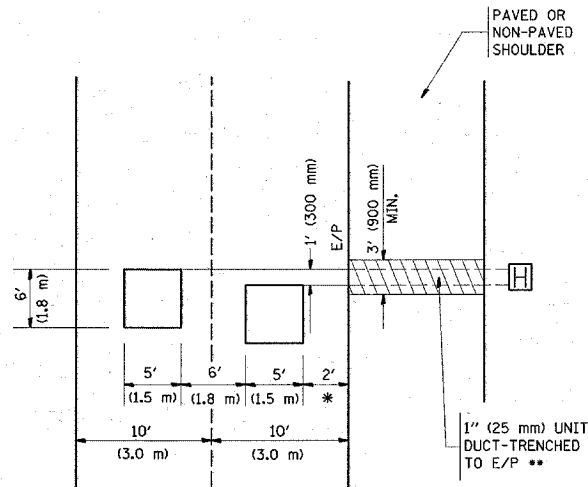
ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.R. RTE. 342	SECTION G-15-4	COUNTY LAKE	TOTAL SHEETS 24	SHEET NO. 23
TC-22		CONTRACT NO. 60032		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

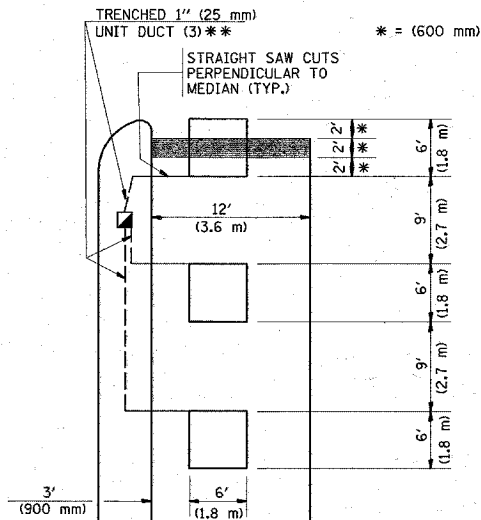


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

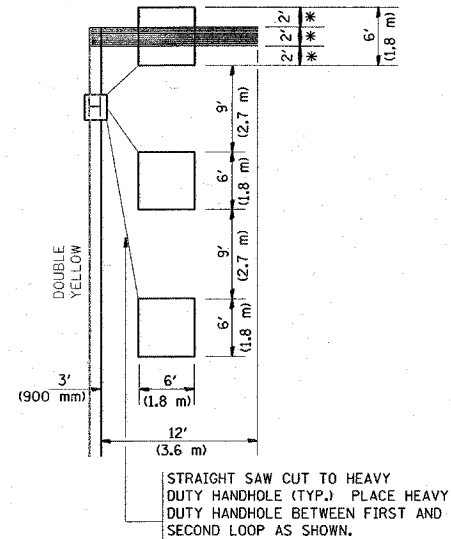


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

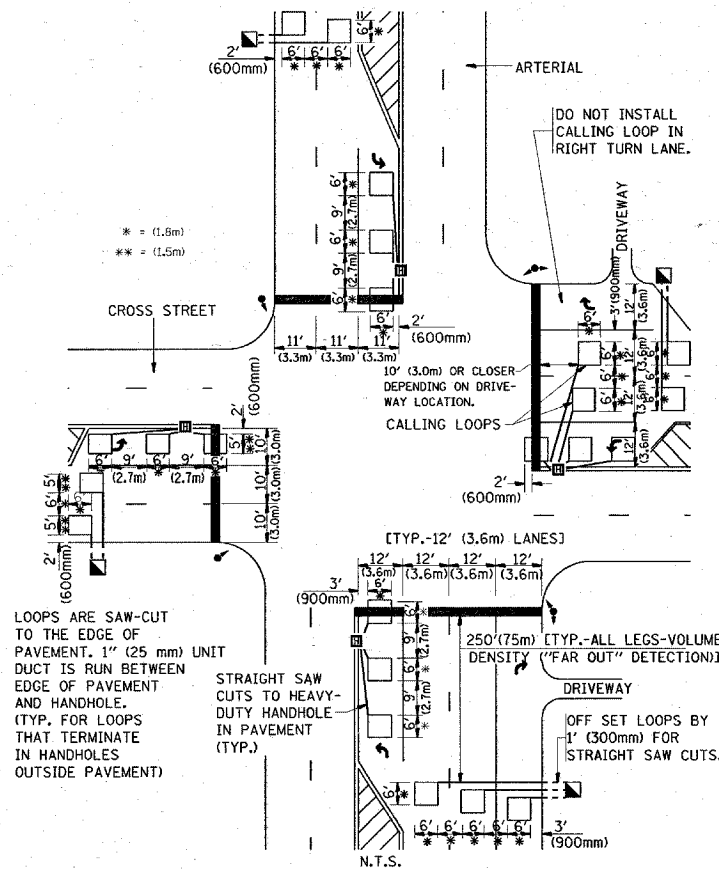
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

* = (600 mm)



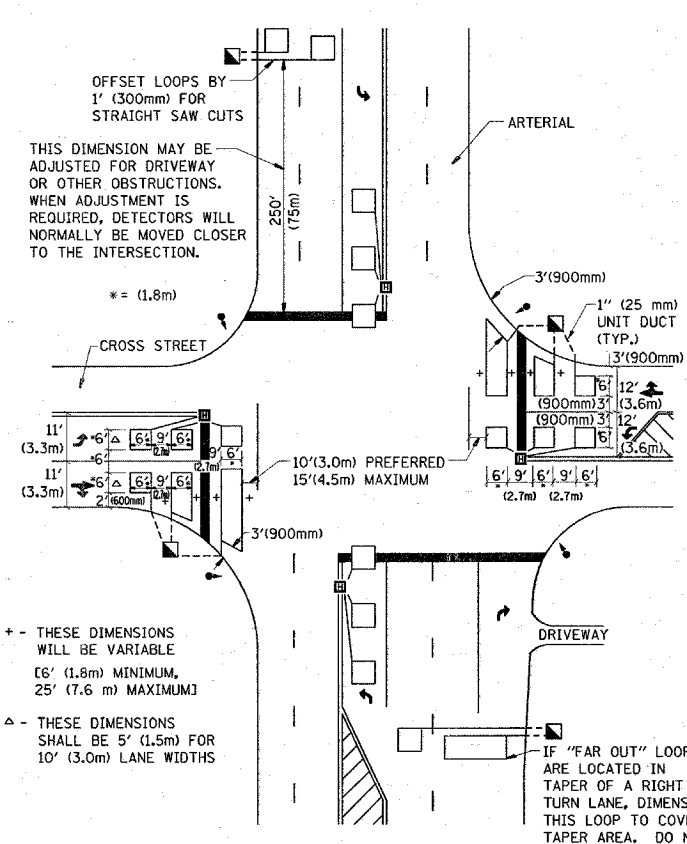
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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PLOT SCALE = 50.0000' / IN.	CHECKED - R.K.F.	DRAWN -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
PLOT DATE = 4/2/2008	DATE	REVISOR	REVISION									