

KANKAKEE VALLEY AIRPORT AUTHORITY

KANKAKEE, ILLINOIS

INDEX TO SHEETS

1. COVER SHEET
2. SITE PLAN AND PROJECT CONTROL PLAN
3. SEQUENCE OF CONSTRUCTION - SHEET 1
4. SEQUENCE OF CONSTRUCTION - SHEET 2
5. SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS - SHEET 1
6. SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS - SHEET 2
7. STORMWATER POLLUTION PREVENTION PLAN
8. STORMWATER POLLUTION PREVENTION PLAN NOTES AND DETAILS - SHEET 1
9. STORMWATER POLLUTION PREVENTION PLAN NOTES AND DETAILS - SHEET 2
10. TYPICAL SECTIONS AND PAVEMENT REPAIR DETAILS
11. EXISTING CONDITIONS AND REMOVALS - SHEET 1
12. EXISTING CONDITIONS AND REMOVALS - SHEET 2
13. RUNWAY 16/34 PLAN AND PROFILE - SHEET 1
14. RUNWAY 16/34 PLAN AND PROFILE - SHEET 2
15. RUNWAY 16/34 PLAN AND PROFILE - SHEET 3
16. RUNWAY 16/34 PLAN AND PROFILE - SHEET 4
17. BITUMINOUS PAVING AND MILLING TABLE - RUNWAY 16-34
18. INTERSECTION GRADING PLAN
19. PAVEMENT MARKING - SHEET 1
20. PAVEMENT MARKING - SHEET 2
21. GROOVING PLAN

CONSTRUCTION PLANS FOR GREATER KANKAKEE AIRPORT REHABILITATE RUNWAY 16/34

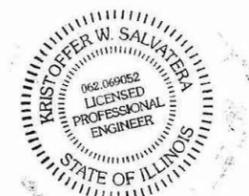
SUMMARY OF QUANTITIES				
REHABILITATE RUNWAY 16/34				
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR150520	MOBILIZATION	LS	1	
AR152410	UNCLASSIFIED EXCAVATION	CY	250	
AR152480	SHOULDER ADJUSTMENT	SY	5100	
AR152540	SOIL STABILIZATION FABRIC	SY	350	
AR156520	INLET PROTECTION	EACH	2	
AR201663	SAND MIX CRACK REPAIR	LF	1800	
AR208515	POROUS GRANULAR EMBANKMENT	CY	120	
AR209612	CRUSHED AGG. BASE COURSE - 12"	SY	350	
AR401610	BITUMINOUS SURFACE COURSE	TON	5023	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1	
AR401640	BITUMINOUS PAVEMENT GROOVING	SY	25800	
AR401650	BITUMINOUS PAVEMENT MILLING	SY	40125	
AR401915	REM & REP BIT PAVEMENT - TYPE A	SY	335	
AR401916	REM & REP BIT PAVEMENT - TYPE B	SY	225	
AR603510	BITUMINOUS TACK COAT	GAL	6020	
AR620520	PAVEMENT MARKING-WATERBORNE	SF	29691	
AR620525	PAVEMENT MARKING-BLACK BORDER	SF	9751	
AR705944	ADJUST UNDERDRAIN CLEANOUT	EACH	4	
AR901510	SEEDING	ACRE	1.1	
AR908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	1.1	

811 Know what's below.
 Call before you dig.

J.U.L.I.E.
 JOINT UTILITY LOCATING
 INFORMATION FOR EXCAVATORS
 www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.



KWS

ILLINOIS PROJECT: IKK-4809
 S.B.G. PROJECT: 3-17-SBGP-139/TBD

JUNE 5, 2020

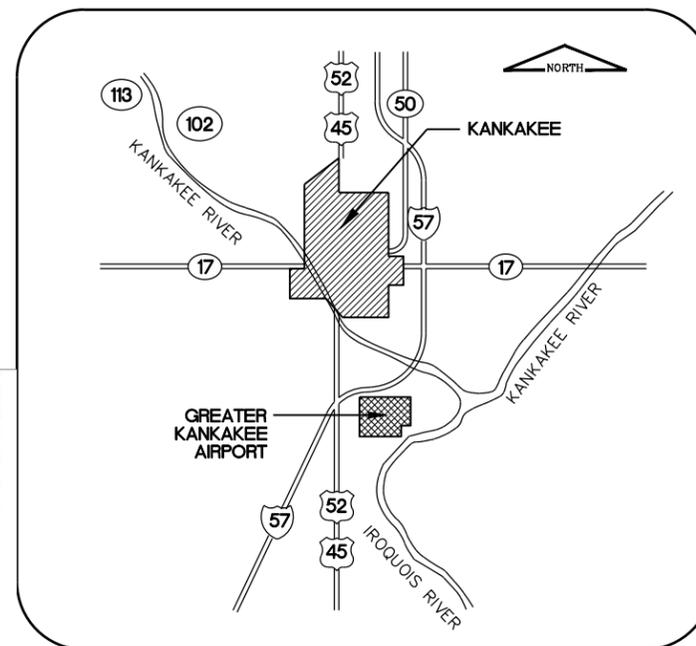
© Copyright CMT, Inc. 190075-02-00

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

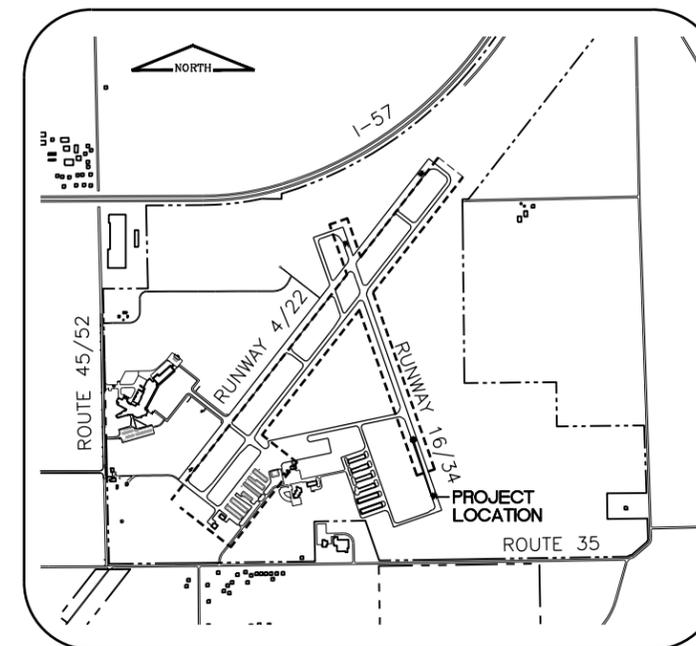
SUBMITTED BY Kristoffer W. Salvatera, P.E.
 DATE 6/5/2020

KANKAKEE VALLEY AIRPORT AUTHORITY

APPROVED Jeff Benoit
 JEFF BENOIT - AIRPORT MANAGER
 DATE 6-3-20 20__

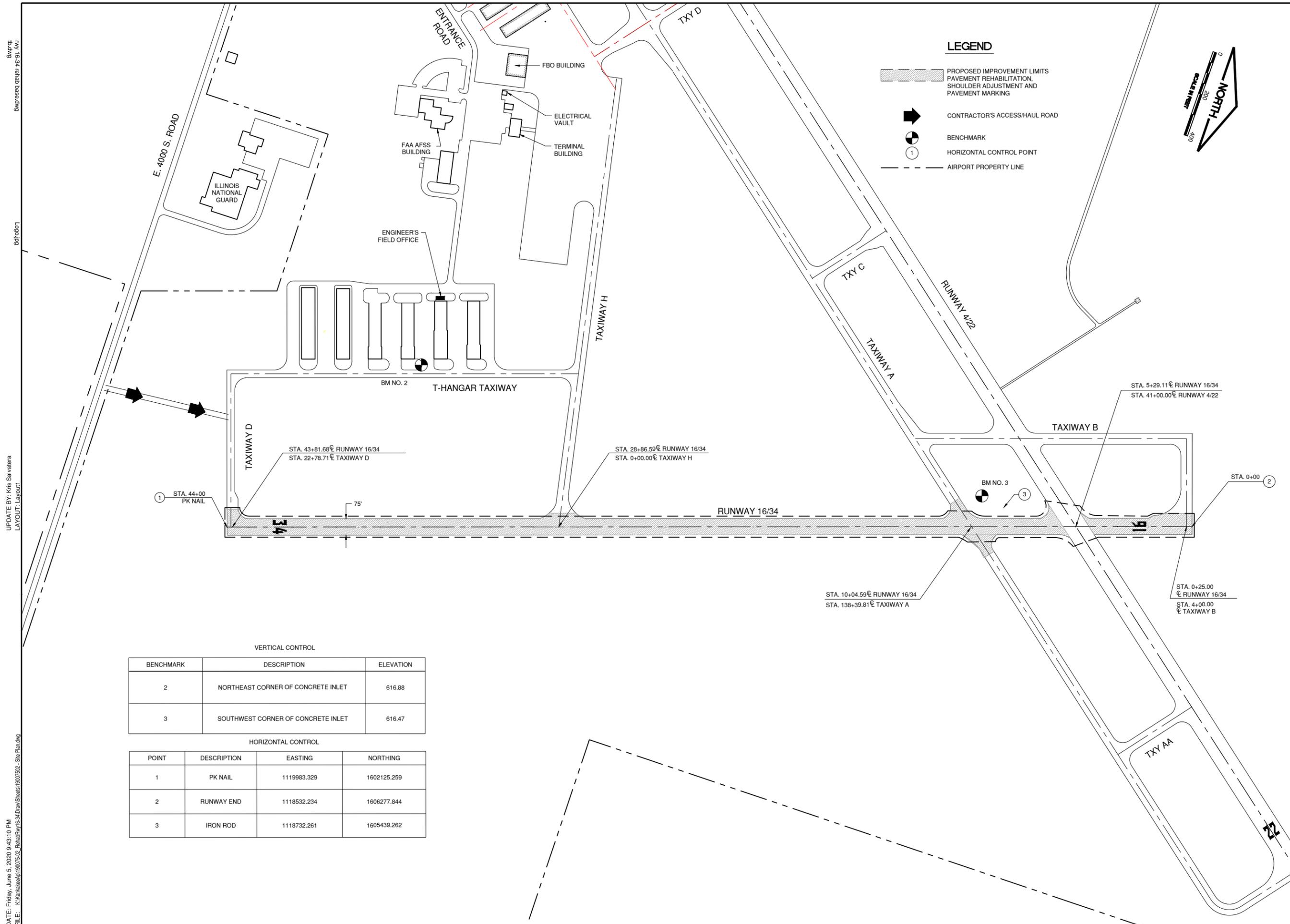


LOCATION MAP



SITE PLAN

RUNWAY 4/22
DESIGN AIRCRAFT APPROACH CATEGORY D
RUNWAY 16/34
DESIGN AIRCRAFT APPROACH CATEGORY B
TAXIWAY A, AA, B, C, D, E, H
TAXIWAY DESIGN GROUP II
KANKAKEE VALLEY AIRPORT AUTHORITY GREATER KANKAKEE AIRPORT
SECTION: 21 COUNTY: KANKAKEE RANGE: R 12 E TOWNSHIP: T 30 N
UNICOM RADIO FREQUENCY - 123.0



IL CONTRACT: **KA050**
 IL LETTING ITEM: **06A**
 IL PROJECT: **IKK-4809**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE RUNWAY 16/34**

SITE PLAN AND PROJECT CONTROL PLAN

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

KANKAKEE VALLEY
KVA
 AIRPORT AUTHORITY

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	6/5/2020
JOB No:	19007502.00

FINAL

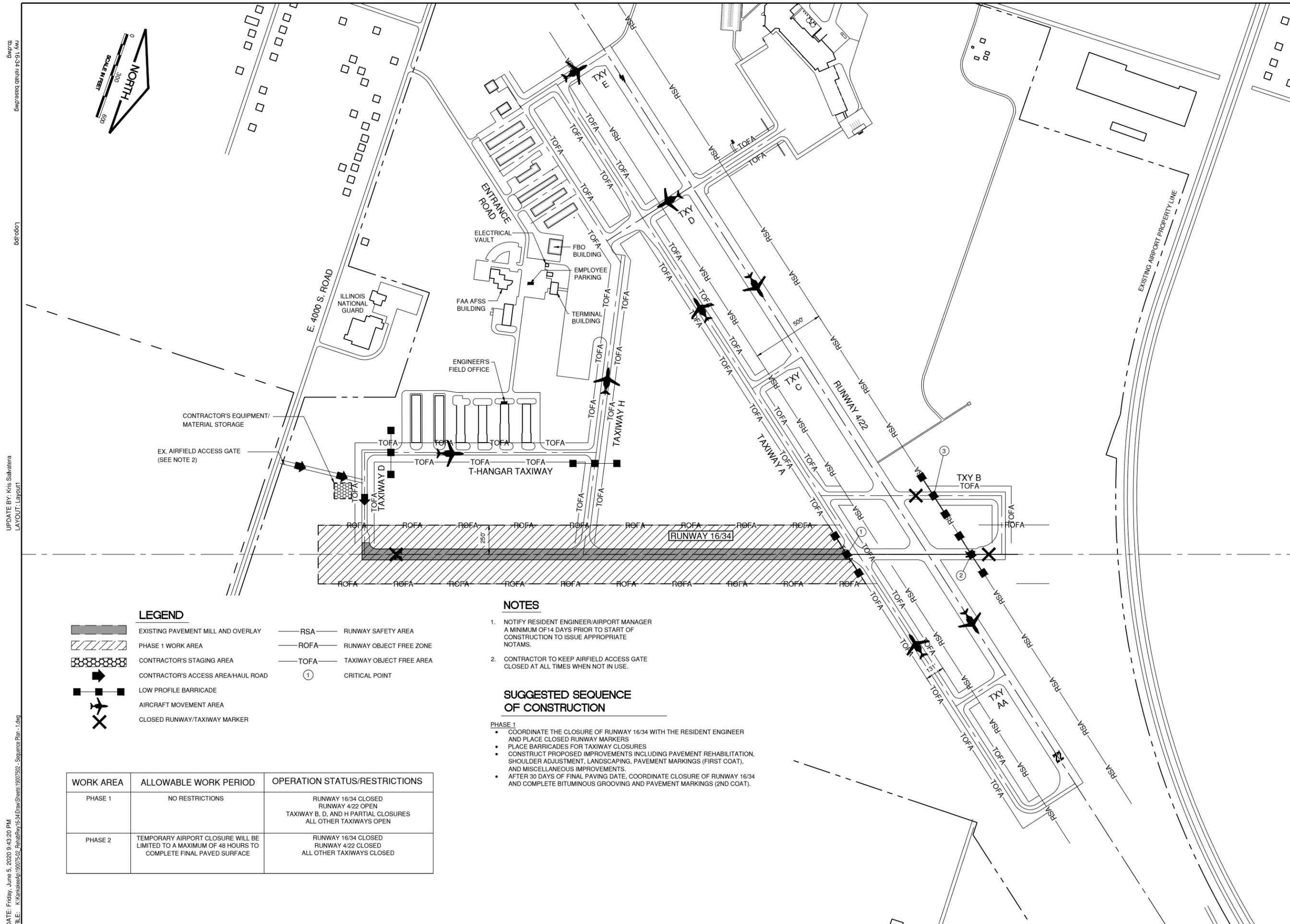
VERTICAL CONTROL

BENCHMARK	DESCRIPTION	ELEVATION
2	NORTHEAST CORNER OF CONCRETE INLET	616.88
3	SOUTHWEST CORNER OF CONCRETE INLET	616.47

HORIZONTAL CONTROL

POINT	DESCRIPTION	EASTING	NORTHING
1	PK NAIL	1119983.329	1602125.259
2	RUNWAY END	1118532.234	1606277.844
3	IRON ROD	1118732.261	1605439.262

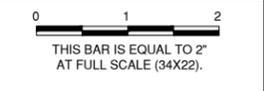
DATE: Friday, June 5, 2020 9:43:10 PM
 FILE: K:\kankakee\19007502 - Rehabilitation\16-34\Draw\Sheet\19007502 - Site Plan.dwg
 UPDATE BY: Kris Salvatera
 LAYOUT: Layout1
 8dfc6c7
 8mrc7d
 8mrc7d
 8mrc7d



IL CONTRACT: **KA050**
 IL LETTING ITEM: **06A**
 IL PROJECT: **IKK-4809**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE RUNWAY 16/34**

**SEQUENCE OF CONSTRUCTION PLAN
 SHEET 1**

LEGEND

- | | | | | |
|--|------------------------------------|--|------|--------------------------|
| | EXISTING PAVEMENT MILL AND OVERLAY | | RSA | RUNWAY SAFETY AREA |
| | PHASE 1 WORK AREA | | ROFA | RUNWAY OBJECT FREE ZONE |
| | CONTRACTOR'S STAGING AREA | | TOFA | TAXIWAY OBJECT FREE AREA |
| | CONTRACTOR'S ACCESS AREA/HAUL ROAD | | ① | CRITICAL POINT |
| | LOW PROFILE BARRICADE | | | |
| | AIRCRAFT MOVEMENT AREA | | | |
| | CLOSED RUNWAY/TAXIWAY MARKER | | | |

NOTES

1. NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER A MINIMUM OF 14 DAYS PRIOR TO START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
2. CONTRACTOR TO KEEP AIRFIELD ACCESS GATE CLOSED AT ALL TIMES WHEN NOT IN USE.

SUGGESTED SEQUENCE OF CONSTRUCTION

- PHASE 1**
- COORDINATE THE CLOSURE OF RUNWAY 16/34 WITH THE RESIDENT ENGINEER AND PLACE CLOSED RUNWAY MARKERS
 - PLACE BARRICADES FOR TAXIWAY CLOSURES
 - CONSTRUCT PROPOSED IMPROVEMENTS INCLUDING PAVEMENT REHABILITATION, SHOULDER ADJUSTMENT, LANDSCAPING, PAVEMENT MARKINGS (FIRST COAT), AND MISCELLANEOUS IMPROVEMENTS.
 - AFTER 30 DAYS OF FINAL PAVING DATE, COORDINATE CLOSURE OF RUNWAY 16/34 AND COMPLETE BITUMINOUS GROOVING AND PAVEMENT MARKINGS (2ND COAT).

WORK AREA	ALLOWABLE WORK PERIOD	OPERATION STATUS/RESTRICTIONS
PHASE 1	NO RESTRICTIONS	RUNWAY 16/34 CLOSED RUNWAY 4/22 OPEN TAXIWAY B, D, AND H PARTIAL CLOSURES ALL OTHER TAXIWAYS OPEN
PHASE 2	TEMPORARY AIRPORT CLOSURE WILL BE LIMITED TO A MAXIMUM OF 48 HOURS TO COMPLETE FINAL PAVED SURFACE	RUNWAY 16/34 CLOSED RUNWAY 4/22 CLOSED ALL OTHER TAXIWAYS CLOSED

© Copyright CMT, Inc.

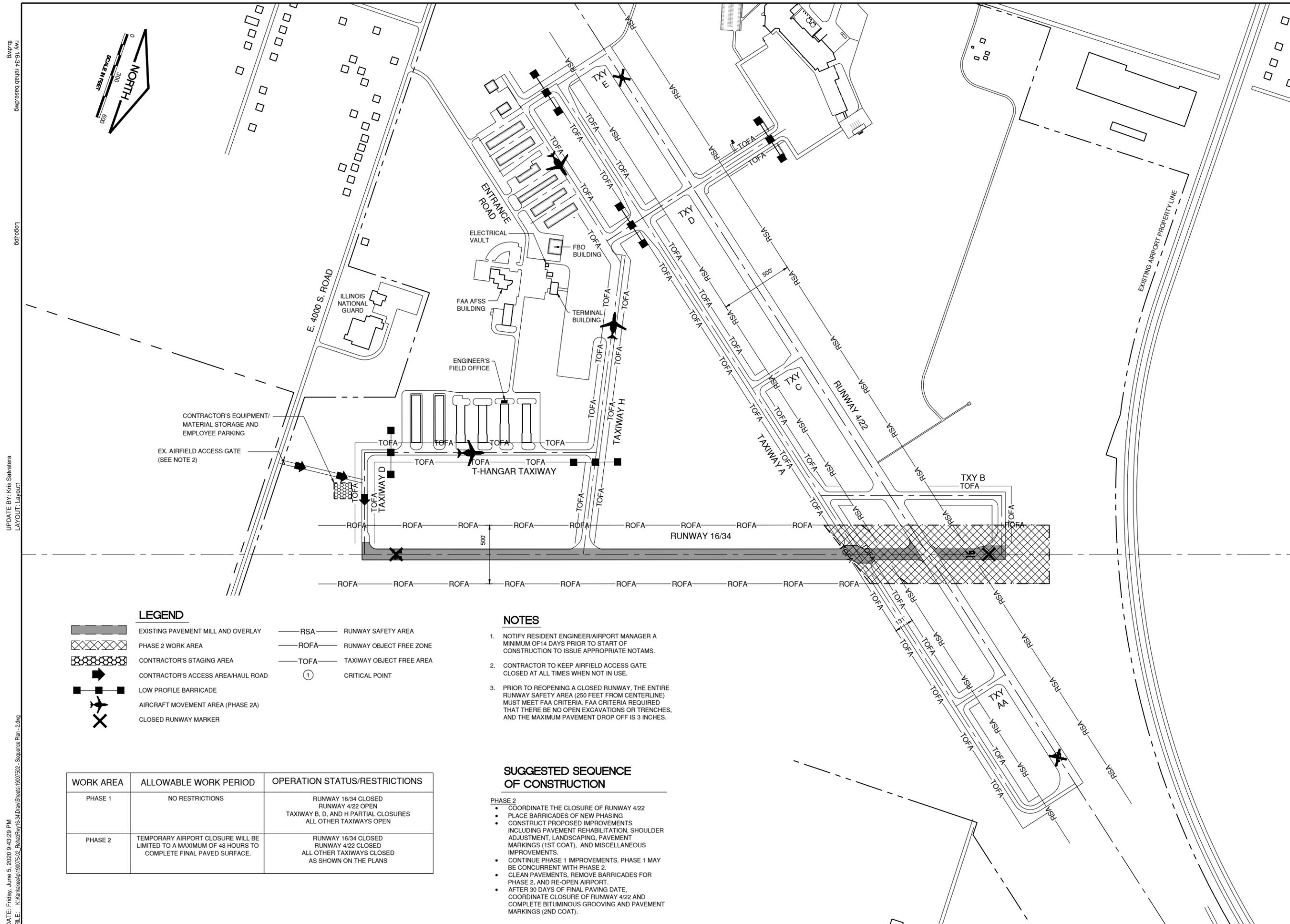
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	---
DATE:	6/5/2020
JOB No:	19007502.00

FINAL

SHEET 3 OF 21 SHEETS

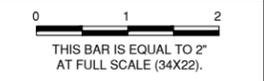
DATE: Friday, June 5, 2020 9:42:20 PM
 FILE: K:\Kankakee\19007502_Plan\16-34\Draw\Sheet\19007502 - Sequence Plan - Long
 UPDATE BY: Kris Salwaters
 LAYOUT: Layout1
 6dfc8071
 6mfc101
 6mfc888 qeub-12-31-14U



IL CONTRACT: **KA050**
 IL LETTING ITEM: **06A**
 IL PROJECT: **IKK-4809**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE RUNWAY 16/34**

**SEQUENCE OF CONSTRUCTION PLAN
 SHEET 2**

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	---
DATE:	6/5/2020
JOB No:	19007502.00

FINAL

SHEET 4 OF 21 SHEETS

LEGEND

- | | | | | |
|--|------------------------------------|--|------|--------------------------|
| | EXISTING PAVEMENT MILL AND OVERLAY | | RSA | RUNWAY SAFETY AREA |
| | PHASE 2 WORK AREA | | ROFA | RUNWAY OBJECT FREE ZONE |
| | CONTRACTOR'S STAGING AREA | | TOFA | TAXIWAY OBJECT FREE AREA |
| | CONTRACTOR'S ACCESS AREA/HAUL ROAD | | ① | CRITICAL POINT |
| | LOW PROFILE BARRICADE | | | |
| | AIRCRAFT MOVEMENT AREA (PHASE 2A) | | | |
| | CLOSED RUNWAY MARKER | | | |

NOTES

1. NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER A MINIMUM OF 14 DAYS PRIOR TO START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
2. CONTRACTOR TO KEEP AIRFIELD ACCESS GATE CLOSED AT ALL TIMES WHEN NOT IN USE.
3. PRIOR TO REOPENING A CLOSED RUNWAY, THE ENTIRE RUNWAY SAFETY AREA (250 FEET FROM CENTERLINE) MUST MEET FAA CRITERIA. FAA CRITERIA REQUIRED THAT THERE BE NO OPEN EXCAVATIONS OR TRENCHES, AND THE MAXIMUM PAVEMENT DROP OFF IS 3 INCHES.

SUGGESTED SEQUENCE OF CONSTRUCTION

- PHASE 2**
- COORDINATE THE CLOSURE OF RUNWAY 4/22
 - PLACE BARRICADES OF NEW PHASING
 - CONSTRUCT PROPOSED IMPROVEMENTS INCLUDING PAVEMENT REHABILITATION, SHOULDER ADJUSTMENT, LANDSCAPING, PAVEMENT MARKINGS (1ST COAT), AND MISCELLANEOUS IMPROVEMENTS.
 - CONTINUE PHASE 1 IMPROVEMENTS. PHASE 1 MAY BE CONCURRENT WITH PHASE 2.
 - CLEAN PAVEMENTS, REMOVE BARRICADES FOR PHASE 2, AND RE-OPEN AIRPORT.
 - AFTER 30 DAYS OF FINAL PAVING DATE, COORDINATE CLOSURE OF RUNWAY 4/22 AND COMPLETE BITUMINOUS GROOVING AND PAVEMENT MARKINGS (2ND COAT).

WORK AREA	ALLOWABLE WORK PERIOD	OPERATION STATUS/RESTRICTIONS
PHASE 1	NO RESTRICTIONS	RUNWAY 16/34 CLOSED RUNWAY 4/22 OPEN TAXIWAY B, D, AND H PARTIAL CLOSURES ALL OTHER TAXIWAYS OPEN
PHASE 2	TEMPORARY AIRPORT CLOSURE WILL BE LIMITED TO A MAXIMUM OF 48 HOURS TO COMPLETE FINAL PAVED SURFACE.	RUNWAY 16/34 CLOSED RUNWAY 4/22 CLOSED ALL OTHER TAXIWAYS CLOSED AS SHOWN ON THE PLANS

DATE: Friday, June 5, 2020 9:42:29 PM
 FILE: K:\Kankakee\19007502_Plan\Draw\Sheet\19007502 - Sequence Plan - 2.dwg
 UPDATE BY: Kris Salwaters
 LAYOUT: Layout1
 6/5/2020
 6/5/2020

DATE: Friday, June 5, 2020 9:43:38 PM
FILE: K:\candela\p13107532_Plan\Draw\Sheet\15071532 - Sequence Notes - 1.dwg
UPDATE BY: Kris Sallaterra
LAYOUT: Layout1
Bef:0601
Emp:0601
Emp:0601

GENERAL NOTES

1. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
2. ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2G (LATEST EDITION) OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
3. CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
4. THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
5. ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
6. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER.
7. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
8. THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
9. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
11. PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES WITH TWO ORANGE FLAGS (20" x 20") ON EACH BARRICADE SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A STEADY BURN OR FLASHING RED LIGHT. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL SUPPLY AND USE AS DIRECTED BY THE AIRPORT, REFLECTIVE LOW PROFILE TYPE BARRICADES. ALL BARRICADES SHALL BE PLACED OUTSIDE OF ACTIVE SAFETY AREAS.
12. THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER FOURTEEN (14) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
14. CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
15. IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
16. DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
17. THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN ASPHALT/STONE TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A DUMP POSITION.
18. IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
19. THE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
20. APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
21. MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.
22. LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO NEXT EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.

23. COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE RESIDENT ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
24. THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
25. DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.
26. CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS / TAXILANES AND THE RUNWAYS AIRCRAFT OPERATIONS AREA.
27. CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 IMAGINARY SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
28. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
29. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAVE BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
30. ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER. ANY DEFECIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.

CONTRACTOR CROSSING RUNWAY/TAXIWAY/TAXILANE/APRON AIR OPERATIONS AREA (A.O.A.)

1. ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH AIR TRAFFIC SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
2. ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
3. WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE MEN AND EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY'S AIRCRAFT OPERATIONS AREA (AOA) AND TAXIWAY/TAXILANE OBJECT FREE AREA (TOFA)

RUNWAYS:

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (14) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING DAY THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED. AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES. IF NECESSARY STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE RSA IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT. REFERENCE TABLE ON PREVIOUS SHEET FOR SAFETY AREA WIDTHS.

TAXIWAYS / TAXILANES:

ANY WORK WITHIN TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) WILL REQUIRE A TAXIWAY / TAXILANE CLOSURE. WORK WITHIN THE TOFA SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. REFERENCE TABLE ON PREVIOUS SHEET FOR OBJECT FREE AREA WIDTHS. NO DROP-OFFS OR OPEN EXCAVATIONS WILL BE ALLOWED WITHIN THE TAXIWAY / TAXILANE SAFETY AREAS OF OPEN TAXIWAYS / TAXILANES.

PHASING NOTES (ALL PHASES)

1. THE INTENT OF THE PHASING PLANS IS TO MINIMIZE THE IMPACT OF CONSTRUCTION ON THE OPERATION OF THE AIRPORT. THE CONTRACTOR SHALL CONSTRUCT THE PROJECT IN CONSECUTIVE PHASES AS OUTLINED IN THE PLANS UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT EXECUTIVE DIRECTOR.
2. PRIOR TO REOPENING A CLOSED RUNWAY, THE ENTIRE (RSA) RUNWAY SAFETY AREA (250 FEET FROM CENTERLINE AND INCLUDING BEYOND THE END OF THE RUNWAY WITHIN AIRPORT PROPERTY) AND (TOFA) TAXIWAY OBJECT FREE AREA MUST MEET FAA CRITERIA. FAA CRITERIA REQUIRES THAT THERE BE NO OPEN EXCAVATIONS OR TRENCHES IN THE SAFETY AREA(S), THE MAXIMUM PAVEMENT DROP OFF BE 3 INCHES, AND ALL GRADES IN ANY DIRECTION BE LESS THAN 5 PERCENT. STEEL PLATES, TEMPORARY WEDGING OF BASE COURSE AND BITUMINOUS CONCRETE MAY BE REQUIRED TO MEET CRITERIA. ALL NECESSARY TEMPORARY MEASURES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
3. THE CONTRACTOR SHALL SUBMIT A CONSTRUCTION SCHEDULE. STRICT ADHERENCE TO THE APPROVED SCHEDULE WILL BE ENFORCED TO AVOID CONFLICTS WITH OTHER CONSTRUCTION ACTIVITIES ON THE AIRPORT AND THE ADVERSE EFFECTS THEY COULD HAVE ON AIRPORT OPERATIONS.
4. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT STAFF TO SCHEDULE THE RUNWAY/TAXIWAY CLOSURES. ITEMS SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT DEPENDABILITY AND MANPOWER AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THIS CRITICAL CLOSURE. THE AIRPORT EXECUTIVE DIRECTOR AND THE CONTRACTOR SHALL MUTUALLY AGREE ON THE EXACT DATES AND TIMES OF THE CLOSURE(S).
5. CONTRACTOR MUST MAINTAIN ACCESS TO ALL ACTIVE AND OPEN AREAS AT ALL TIMES. CONTRACTOR SHALL RELOCATE EQUIPMENT AT NO ADDITIONAL COST TO CONTRACT TO ALLOW AIRCRAFT TO PASS. CONTRACTOR SHALL COORDINATE CONSTRUCTION OPERATIONS AT ALL ACTIVE AND OPEN AREAS TO PROVIDE MINIMAL DISRUPTIONS TO AIRCRAFT MOVEMENT IN THAT AREA.
6. FAA AND AIRPORT ACCESS ROAD(S) SHALL NOT BE USED AS A HAUL ROAD BY THE CONTRACTOR WITHOUT PRIOR APPROVAL.
7. TO THE EXTENT POSSIBLE THE CONTRACTOR SHALL HAVE ALL EMPLOYEE PARKING OUTSIDE OF AIRPORT FENCE OR AS INDICATED AT THE LOCATION SHOWN.
8. THE AIRPORT RESERVES THE RIGHT TO MODIFY THE SEQUENCE OF CONSTRUCTION INCLUDING BUT NOT LIMITED TO PHASING, WORK AREAS, BARRICADE PLACEMENT, ACCESS AND HAUL ROUTES, AND CONTRACTOR MOVEMENTS AT ANY TIME DURING THE PROJECT WITH FAA, IDA AND ATCT APPROVAL.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS.

POINT	NEAREST ACTIVE RUNWAY	LATITUDE	LONGITUDE	ELEVATION
1	RUNWAY 4/22	41° 04' 23.44"	87°50' 42.07"	625
2	RUNWAY 4/22	41°04' 31.38"	87°50' 45.67"	627
3	RUNWAY 4/22	41° 04' 25.83"	87°50' 43.15"	626

DESIGN AIRCRAFT APPROACH CATEGORY: B AND D

DESIGN AIRPORT GROUP: II
RUNWAY 4/22 SAFETY AREA WIDTH: 250'
RUNWAY 16/34 SAFETY AREA WIDTH: 250'
TAXIWAY CENTERLINE TO OBJECT SEPARATION: 65.5'
TAXILANE CENTERLINE TO OBJECT SEPARATION: 57.5'

MAXIMUM ANTICIPATED HEIGHT OF EQUIPMENT - 25'

IL CONTRACT: **KA050**

IL LETTING ITEM: **06A**

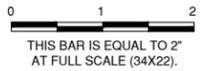
IL PROJECT: **IKK-4809**

S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS

NUMBER	BY	DATE



GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS
REHABILITATE RUNWAY 16/34

SEQUENCE OF CONSTRUCTION
GENERAL NOTES AND DETAILS - 1

© Copyright CMT, Inc.

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613



DESIGN BY: KWS

DRAWN BY: JRO

CHECKED BY: DKP

APPROVED BY: ---

DATE: 5/8/2020

JOB No: 19007502.00

FINAL

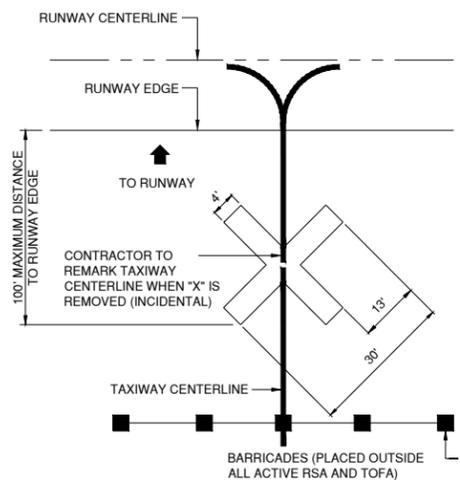
SHEET 5 OF 21 SHEETS

6/10/2021
6/10/2021 9:43:48 AM

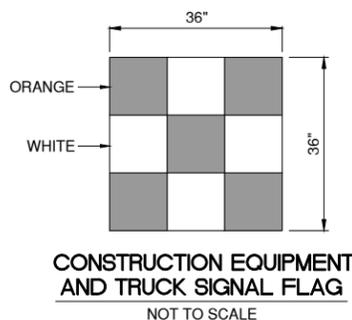
6/10/2021

UPDATE BY: Kris Salwaters
LAYOUT: Layout

DATE: Friday, June 5, 2020 9:43:48 PM
FILE: K:\candela\p19107532_Plan\Draw\Sheet\19107532 - Sequence Notes - 2.dwg



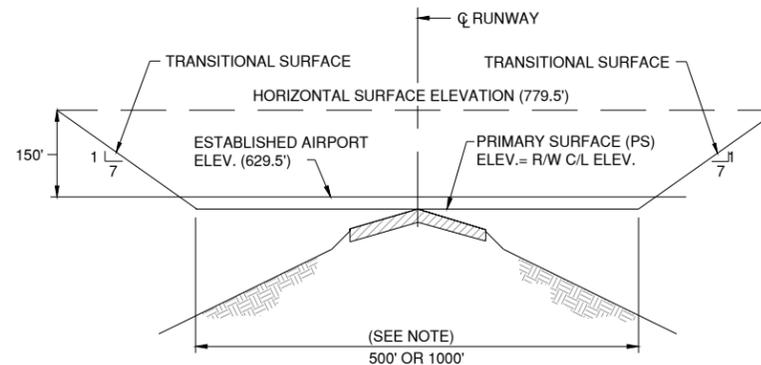
CLOSED TAXIWAY MARKER DETAIL
NOT TO SCALE



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
NOT TO SCALE

AIRFIELD LIGHTS AND SIGNS NOTES

- CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
- CONTRACTOR SHALL TURN OFF RUNWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES. CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY. THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.

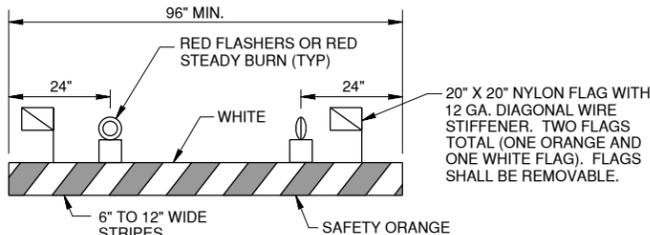


TYPICAL SECTION F.A.R. PART 77 IMAGINARY SURFACES
NO SCALE

NOTE:
IMAGINARY SURFACE REQUIREMENTS FOR EXISTING ACTIVE RUNWAYS (R/W) ARE SIMILAR EXCEPT PRIMARY SURFACE (PS) DIMENSIONS VARY



PLAN VIEW

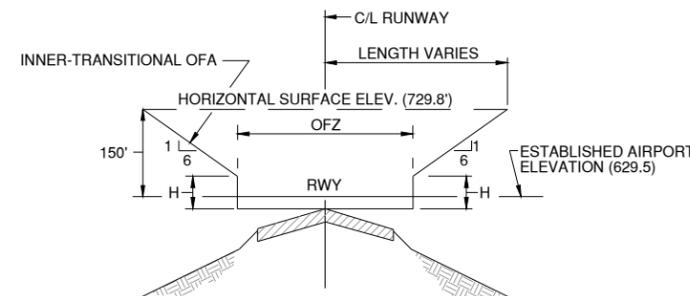


SIDE VIEW

AIRSIDE LOW PROFILE LIGHTED BARRICADE
NOT TO SCALE

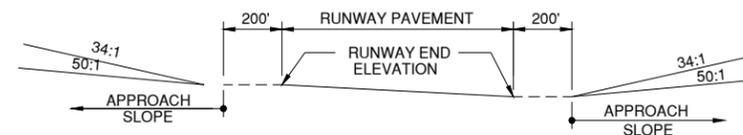
BARRICADE NOTES

- FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
- FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
- BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
- BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
- BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
- PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
- ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



TYPICAL SECTION OBSTACLE FREE ZONE (OFZ)
NO SCALE

RUNWAY	TYPE OF RUNWAY	H (FEET)
16-34	NON-PRECISION	150
4-22	PRECISION	150

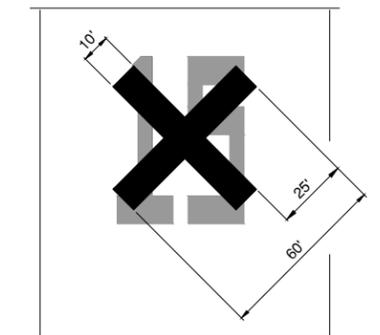


TYPICAL PROFILE F.A.R. PART 77 IMAGINARY SURFACES
NO SCALE

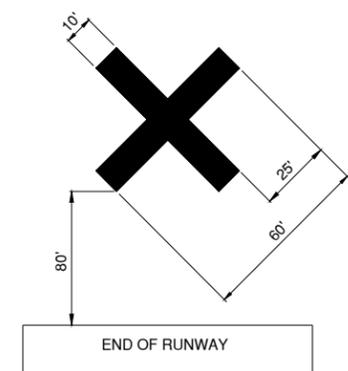
RUNWAY END	ELEVATION	APPROACH SLOPE
16	621	34:1
34	617	34:1
4	624	50:1
22	630	34:1

CLOSED TAXIWAY MARKER DETAIL NOTES

- CLOSED TAXIWAY MARKERS SHALL BE PAINTED YELLOW WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OBJECT FREE AREA CRITERIA AND RUNWAY SAFETY AREA CRITERIA PER ADVISORY CIRCULAR 150/5300-13 (LATEST EDITION) AND ARE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT.
- CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
- COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- PLACE MARKERS OVER TAXIWAY CENTERLINE.
- MARKERS SHALL BE ADEQUATELY SECURED TO PREVENT MOVEMENT BY PROPELLER WASH, JET BLAST OR OTHER WIND CURRENTS.
- MARKERS ARE ONLY REQUIRED FOR CLOSURES EXCEEDING 72 HOURS.
- AS THE CONSTRUCTION OF THE PORTION OF THE TAXIWAY WITHIN THE RUNWAY SAFETY AREA, PROGRESSES, SHOULD THE NEW TAXIWAY BE CLOSED FOR MORE THAN 72 HOURS, THE CONTRACTOR SHALL INSTALL A CLOSED TAXIWAY MARKER AS SHOWN IN THE DETAIL. THE CLOSED TAXIWAY MARKER SHALL NOT BE REMOVED UNLESS THE TAXIWAY WILL BE OPENED AND REMAIN OPEN WITHIN 72 HOURS OF MARKER REMOVAL.



ON PAVEMENT TEMPORARY CLOSED RUNWAY MARKER DETAIL
NOT TO SCALE



OFF PAVEMENT TEMPORARY CLOSED RUNWAY MARKER DETAIL
NOT TO SCALE

CLOSED RUNWAY MARKER DETAIL NOTES

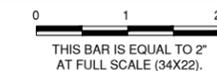
- CLOSED RUNWAY MARKERS SHALL BE YELLOW.
- MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
- CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
- MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
- COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.

IL CONTRACT: **KA050**
IL LETTING ITEM: **06A**
IL PROJECT: **IKK-4809**
S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS

NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS
REHABILITATE RUNWAY 16/34**

**SEQUENCE OF CONSTRUCTION
GENERAL NOTES AND DETAILS - 2**

© Copyright CMT, Inc.

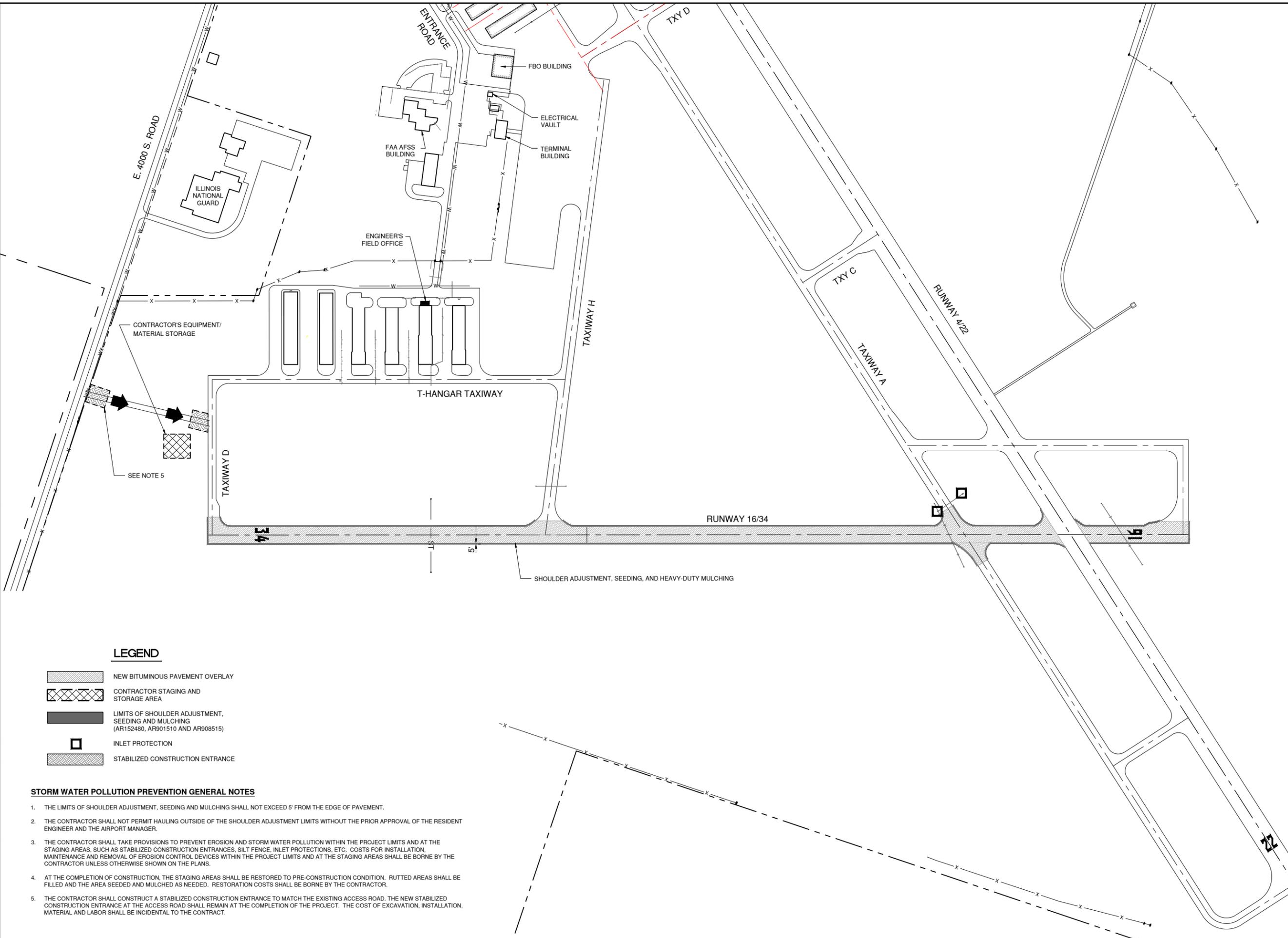
CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613



DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	---
DATE:	6/5/2020
JOB No:	19007502.00

FINAL

DATE: Friday, June 5, 2020 9:43:59 PM
 FILE: K:\Kendall\1907502_Plan\Draw\Sheet\1907502 - Shop\Plan.dwg
 UPDATE BY: Kris Salamera
 LAYOUT: Layout1
 6dfc807
 6dfc807



LEGEND

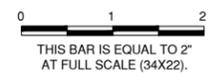
-  NEW BITUMINOUS PAVEMENT OVERLAY
-  CONTRACTOR STAGING AND STORAGE AREA
-  LIMITS OF SHOULDER ADJUSTMENT, SEEDING AND MULCHING (AR152480, AR901510 AND AR908515)
-  INLET PROTECTION
-  STABILIZED CONSTRUCTION ENTRANCE

STORM WATER POLLUTION PREVENTION GENERAL NOTES

1. THE LIMITS OF SHOULDER ADJUSTMENT, SEEDING AND MULCHING SHALL NOT EXCEED 5' FROM THE EDGE OF PAVEMENT.
2. THE CONTRACTOR SHALL NOT PERMIT HAULING OUTSIDE OF THE SHOULDER ADJUSTMENT LIMITS WITHOUT THE PRIOR APPROVAL OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
3. THE CONTRACTOR SHALL TAKE PROVISIONS TO PREVENT EROSION AND STORM WATER POLLUTION WITHIN THE PROJECT LIMITS AND AT THE STAGING AREAS, SUCH AS STABILIZED CONSTRUCTION ENTRANCES, SILT FENCE, INLET PROTECTIONS, ETC. COSTS FOR INSTALLATION, MAINTENANCE AND REMOVAL OF EROSION CONTROL DEVICES WITHIN THE PROJECT LIMITS AND AT THE STAGING AREAS SHALL BE BORNE BY THE CONTRACTOR UNLESS OTHERWISE SHOWN ON THE PLANS.
4. AT THE COMPLETION OF CONSTRUCTION, THE STAGING AREAS SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITION. RUTTED AREAS SHALL BE FILLED AND THE AREA SEEDED AND MULCHED AS NEEDED. RESTORATION COSTS SHALL BE BORNE BY THE CONTRACTOR.
5. THE CONTRACTOR SHALL CONSTRUCT A STABILIZED CONSTRUCTION ENTRANCE TO MATCH THE EXISTING ACCESS ROAD. THE NEW STABILIZED CONSTRUCTION ENTRANCE AT THE ACCESS ROAD SHALL REMAIN AT THE COMPLETION OF THE PROJECT. THE COST OF EXCAVATION, INSTALLATION, MATERIAL AND LABOR SHALL BE INCIDENTAL TO THE CONTRACT.

IL. CONTRACT: **KA050**
 IL. LETTING ITEM: **06A**
 IL. PROJECT: **IKK-4809**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE RUNWAY 16/34**

STORMWATER POLLUTION PREVENTION PLAN

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	---
DATE:	6/5/2020
JOB No:	19007502.00

FINAL

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE AT THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION:

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATION OF AN EXISTING BITUMINOUS PAVEMENT AT THE GREATER KANKAKEE AIRPORT. THE PROJECT INCLUDES TURF SHOULDER ADJUSTMENT, VARIOUS PAVEMENT ITEMS, PAVEMENT MARKING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

DESCRIPTION OF CONSTRUCTION ACTIVITY:

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL.

VARIABLE DEPTH BITUMINOUS PAVEMENT MILLING AND PAVING.

TURF SHOULDER ADJUSTMENT, SEEDING AND MULCHING.

INSTALLATION OF NEW PAVEMENT MARKING.

REMOVAL AND DISPOSAL OF TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES.

AREA OF CONSTRUCTION SITE:

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 5.6 ACRES OF WHICH 1.1 ACRES WILL BE DISTURBED BY GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE KANKAKEE RIVER THROUGH A STORM SEWER SYSTEM.

EROSION AND SEDIMENT CONTROL:

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, SOD, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND ALL OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.

DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN SEVEN (7) DAYS.

THE DOWN STREAM SIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIER.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS WITHIN THE STAGING AREA. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT PERIODICALLY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2" OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE RESIDENT ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT THE EROSION AND SEDIMENT CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE CONTRACT.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT, AND ANY OTHER POTENTIAL HAZARDOUS MATERIALS THAT MAY EXIST ON SITE.

NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

COST OF MAINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCIDENTAL TO THE CONTRACT.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RE-SEEDED AND/OR SODDED.

MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

DOCUMENTATION:

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF INTENT (NOI)" PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL POST A SIGN OR OTHER NOTICE NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE. IF THIS IS NOT POSSIBLE, THEN IT MAY BE PERMITTED TO POST THIS NOTICE IN A LOCAL PUBLIC BUILDING. THE SIGN OR NOTICE MUST CONTAIN THE FOLLOWING:

1. A COPY OF THE COMPLETED NOTICE OF INTENT (NOI) AS SUBMITTED TO THE IEPA
2. THE LOCATION OF THE SWPPP AND NAME AND 24/7 TELEPHONE NUMBER OF THE CONTACT PERSON.

THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN AND UPDATE AN "AS-BUILT" SET OF STORM WATER POLLUTION PREVENTION PLANS IN THE PROJECT FILES. THE SWPPP SHALL BE UPDATED WITHIN 7-DAYS OF ANY MODIFICATIONS TO THE PLANS. THE SWPPP AND ALL REVISIONS SHALL BE RETAINED FOR THREE YEARS AFTER FINAL STABILIZATION OF THE SITE, WHICH SHALL BE DEFINED AS VEGETATION COVER OF AT LEAST 70% OF HISTORIC CONDITIONS.

A STORM WATER POLLUTION PREVENTION PLAN EROSION CONTROL INSPECTION REPORT (FORM BC 2259) SHALL BE COMPLETED WITH INSPECTION FREQUENCIES AS OUTLINED HEREIN. SWPPP REPORTS SHALL BE RETAINED FOR THREE YEARS AFTER THE DATE OF FINAL STABILIZATION AS DEFINED HEREIN.

IF ANY VIOLATION OF THE PROVISIONS OF THE PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION COVERED IN THIS PLAN, THE ENGINEER AND/OR CONTRACTOR SHALL COMPLETE AND FILE AN "INCIDENT OF NONCOMPLIANCE (ION)" REPORT FOR THE IDENTIFIED VIOLATION. THE FORMS SHALL BE AS PROVIDED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, AND SHALL INCLUDE SPECIFIC INFORMATION ON THE INCIDENT THAT CAUSED NONCOMPLIANCE, ACTIONS THAT WERE TAKEN TO CORRECT THE NONCOMPLIANCE AND TO PREVENT ITS REOCCURRENCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NONCOMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G. OF THE GENERAL PERMIT.

AFTER PROJECT FINAL ACCEPTANCE, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF TERMINATION (NOT)" FORM PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY. FORMS FOR THE IEPA SHALL BE MAILED TO THE FOLLOWING ADDRESS:

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
DIVISION OF WATER POLLUTION CONTROL, MAIL CODE #15
ATTN: PERMIT SECTION
1021 NORTH GRAND AVENUE EAST
P.O. BOX 19276
SPRINGFIELD, ILLINOIS 62794-9276

NPDES PERMIT #	_____
DATE ISSUED	_____
DATE EXPIRED	_____

GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL:

1. ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
2. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, COOK COUNTY, CHICAGO EXECUTIVE AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

CONTRACTOR CERTIFICATION STATEMENT	
THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.	
PROJECT INFORMATION:	
ROUTE: <u>GREATER KANKAKEE AIRPORT</u>	MARKED: <u>REHABILITATE RUNWAY 16/34</u>
SECTION: <u>16, 20, & 21</u>	PROJECT NUMBER: <u>IKK-4809</u>
COUNTY: <u>KANKAKEE</u>	CONTRACT NUMBER: <u>3-17-SBGP-TBD</u>
I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.	
SIGNATURE: _____	DATE: _____
PRINTED NAME: _____	TITLE: _____
NAME OF FIRM: _____	
STREET ADDRESS: _____	
CITY, STATE, ZIP: _____	
PHONE NUMBER: _____	
THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.	

RECORD OF SITE DISTURBANCE AND STABILIZATION	
MAJOR GRADING ACTIVITIES:	BEGINNING DATE: _____
LOCATION: _____	COMPLETION DATE: _____
MAJOR GRADING ACTIVITIES:	BEGINNING DATE: _____
LOCATION: _____	COMPLETION DATE: _____
SITE STABILIZATION:	BEGINNING DATE: _____
LOCATION: _____	COMPLETION DATE: _____
SITE STABILIZATION:	BEGINNING DATE: _____
LOCATION: _____	COMPLETION DATE: _____
CONSTRUCTION CEASED:	BEGINNING DATE: _____
EXPLANATION: _____	COMPLETION DATE: _____
THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AS CONSTRUCTION PROGRESSES IN ACCORDANCE WITH THE NPDES GENERAL PERMIT FOR STORMWATER DISCHARGES. THIS INFORMATION MAY ALSO BE NOTED DIRECTLY ON THE SWPPP SITE MAP.	

IL. CONTRACT: **KA050**

IL. LETTING ITEM: **06A**

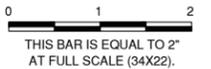
IL. PROJECT: **IKK-4809**

S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # _____ BOOK # _____

REVISIONS

NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS
REHABILITATE RUNWAY 16/34**

**STORMWATER POLLUTION PREVENTION PLAN
NOTES AND DETAILS - SHEET 1**

© Copyright CMT, Inc.

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

KANKAKEE VALLEY
KVA
AIRPORT AUTHORITY

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	----
DATE:	6/5/2020
JOB No:	19007502.00

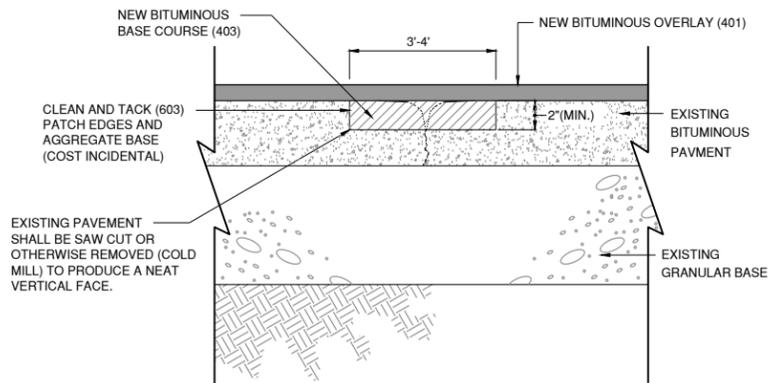
FINAL

6/10/20
6/10/2020 9:44:28 PM

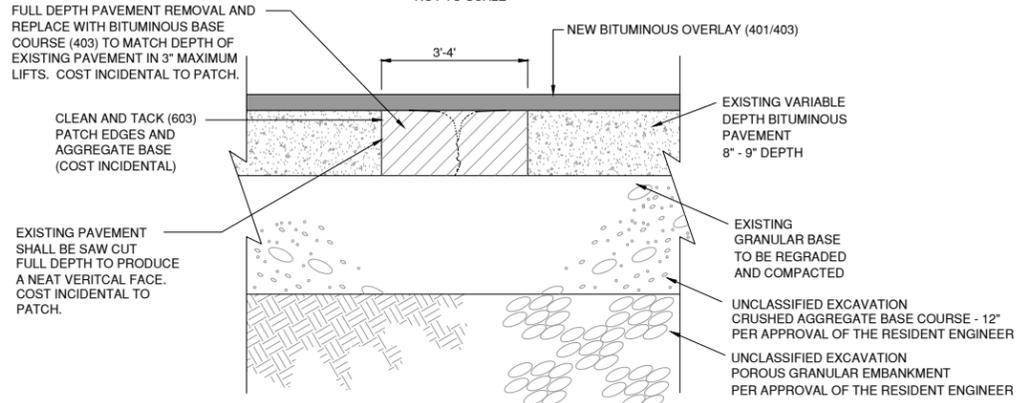
6/10/2020

UPDATE BY: Kris Salwaters
LAYOUT: Layout

DATE: Friday, June 5, 2020 9:44:28 PM
FILE: K:\cadd\p1\1907502 - Rehab Runway 16-34\Draw Sheets\1907502 - Typical Sections and Details.dwg



REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A
NOT TO SCALE



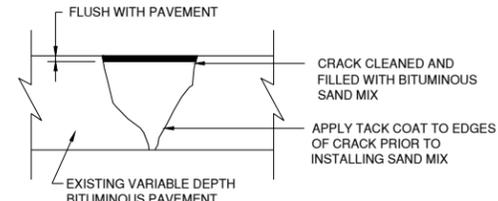
REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B
NOT TO SCALE

PAVEMENT REMOVAL AND REPLACEMENT NOTES

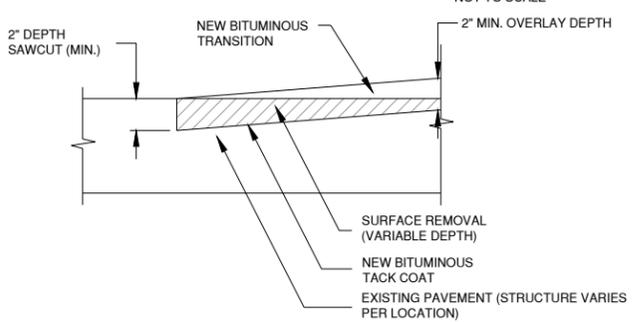
1. DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS, AIRPORT PERSONNEL, AND PAVEMENT CORES. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
2. PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. EXISTING MEDIUM TO HIGH SEVERITY TRANSVERSE CRACKS ARE SPACED AT VARIABLE INTERVALS. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.
3. THIS WORK SHALL BE DETERMINED IN THE FIELD WITH THE AIRPORT/ENGINEER AFTER INSPECTION OF THE MILLED SURFACE.
4. AS FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION, UNDERCUTS MAY BE NECESSARY. FURNISHING AND PLACING OF POROUS GRANULAR EMBANKMENT AND/OR REMOVAL AND REPLACEMENT OF AGGREGATE BASE COURSE SHALL BE DONE AT SECTIONS WHERE THE REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B IS UTILIZED. THIS SHALL BE DONE TO REPAIR SOFT SUBGRADE AS DETERMINED BY THE RESIDENT ENGINEER BASED ON RESULTS OF THE GEOTECHNICAL TESTING OR PROOF ROLL. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES.

CRACK REPAIR NOTES:

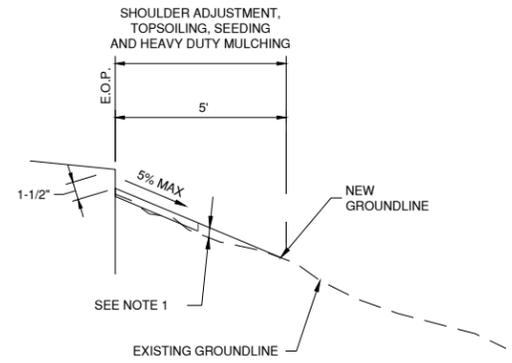
1. CRACKS TO BE FILLED WITH SAND MIX SHALL BE DESIGNATED BY THE RESIDENT ENGINEER.
2. SAND MIX CRACK REPAIR SHALL BE USED ON ALL CRACKS GREATER THAN 1" WIDE AS DESIGNATED BY THE RESIDENT ENGINEER.
3. CONTRACTOR SHALL ROUTE, CRACK PLOW AND REMOVE LOOSE FILLER, LOOSE AGGREGATE AND LAITANCE WITH COMPRESSED AIR AS SPECIFIED PRIOR TO SEALING.



HMA SAND MIX CRACK REPAIR DETAIL
NOT TO SCALE



BUTT JOINT DETAIL
NOT TO SCALE
-INCIDENTAL TO BITUMINOUS PAVEMENT MILLING



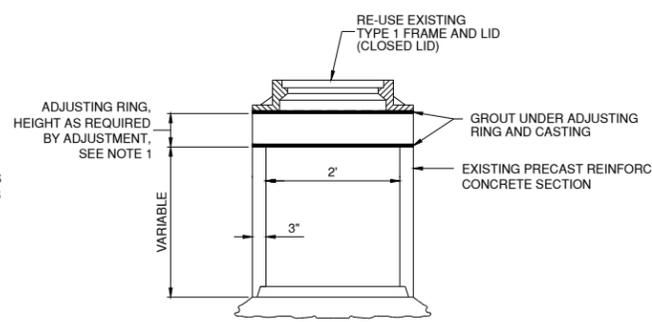
TURF SHOULDER DETAIL
NOT TO SCALE

NOTES:

1. TOPSOIL REQUIRED FOR FILL SHALL BE INCLUDED IN THE COSTS FOR SHOULDER ADJUSTMENT (AR152480)
2. LIMITS OF TURF SHOULDER SHALL BE ADJUSTED IN THE FIELD AS REQUIRED.

LEGEND

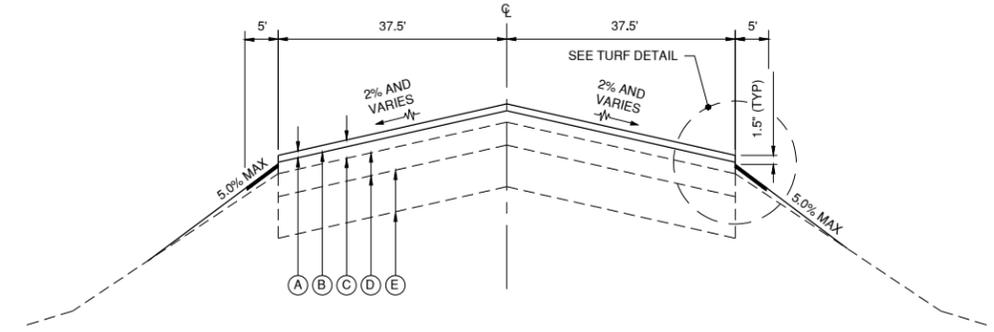
- (A) NEW 2" AND VARIES BITUMINOUS PAVEMENT MILLING
NEW 2" AND VARIES BITUMINOUS SURFACE COURSE
- (B) NEW TACK COAT
- (C) EXISTING BITUMINOUS PAVEMENT (8" - 9")
- (D) EXISTING 4" CRUSHED AGGREGATE BASE
- (E) EXISTING 6" AGGREGATE SUBBASE
- (F) EXISTING 12" WATERBOUND MACDAM
- (G) NEW SHOULDER ADJUSTMENT



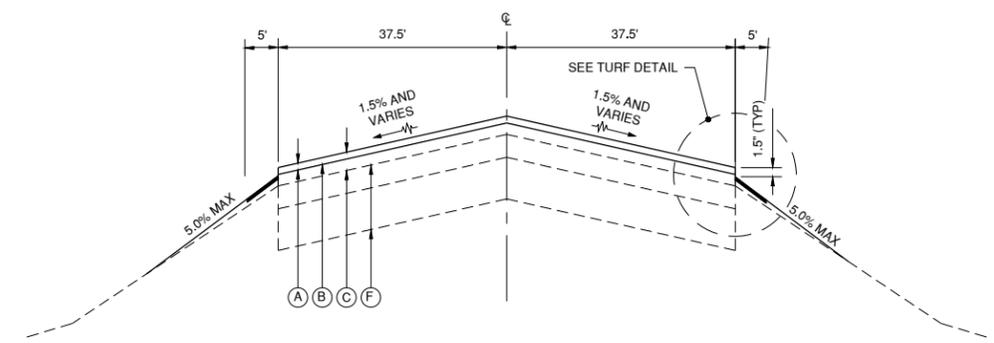
ADJUST UNDERDRAIN COLLECTION STRUCTURE
NOT TO SCALE

STRUCTURE ADJUSTMENT NOTES:

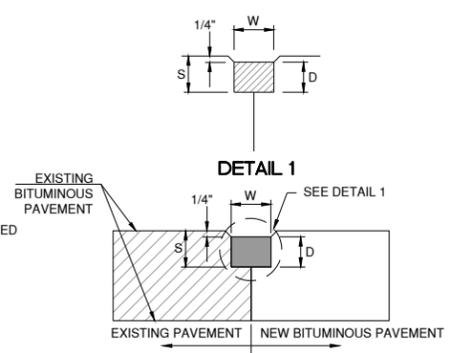
1. CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD CHECKING EXISTING MANHOLE CONFIGURATIONS AND SIZES FOR THE NECESSARY ADJUSTMENT.
2. THE MAXIMUM AMOUNT OF ADJUSTING RINGS ALLOWED SHALL BE THREE. STRUCTURES WITH THREE ADJUSTING RINGS ALREADY IN PLACE SHALL HAVE THE EXISTING RINGS REMOVED AND LARGER RINGS INSTALLED TO COMPLETE THE ADJUSTMENT. COSTS OF REMOVING AND DISPOSING OF EXISTING RINGS SHALL BE INCLUDED IN THE UNIT PRICE FOR THE ADJUSTMENT.
3. THE MAXIMUM HEIGHT OF ANY SINGLE ADJUSTING RING(S) SHALL BE 8 INCHES, INCLUDING EXISTING RINGS.
4. ADJUSTING RINGS SHALL BE PRE-CAST REINFORCED CONCRETE. ALL ADJUSTING RINGS AND METAL FRAME SHALL BE MORTARED INTO PLACE WITHOUT THE USE OF SHIMS OF ANY TYPE.
5. CONTRACTOR SHALL TAKE CARE TO PREVENT DIRT AND DEBRIS FROM ENTERING THE STRUCTURE. CONTRACTOR SHALL REMOVE ANY DIRT AND DEBRIS FOUND IN THE STRUCTURE PRIOR TO OR AFTER THE ADJUSTMENT, COSTS INCLUDED IN THE UNIT PRICE FOR THE ADJUSTMENT.
6. TOPSOIL PLACED AROUND STRUCTURES IN TURF SHALL BE THOROUGHLY COMPACTED TO PREVENT SETTLING.
7. AT THE CONTRACTOR'S OPTION AND AT NO ADDITIONAL COST TO THE CONTRACT, HEAVY DUTY IRON ADJUSTING RINGS MAY BE USED IN LIEU OF PRECAST ADJUSTING RINGS. ONLY ONE EXTENSION RING SHALL BE ALLOWED PER STRUCTURE.
8. EXISTING CASTINGS DAMAGED BY THE CONTRACTOR DURING REMOVAL OPERATIONS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE CONTRACT.



TYPICAL SECTION RUNWAY 16/34
STA. 0+00 TO STA. 32+00



TYPICAL SECTION RUNWAY 16/34
STA. 32+00 TO STA. 44+00



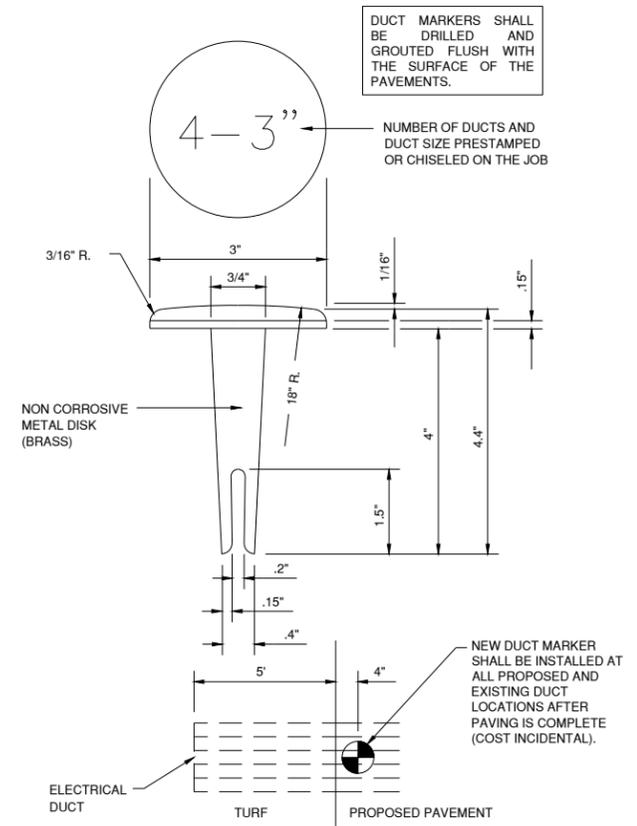
BITUMINOUS / BITUMINOUS INTERFACE JOINT SEALING
NOT TO SCALE

NOTES

1. THE JOINT BETWEEN NEW AND EXISTING BITUMINOUS PAVEMENT SHALL BE SAWED AND SEALED (PROJECT LIMITS) AND SHALL BE INCLUDED IN THE COST OF HMA BITUMINOUS SURFACE.

	DETAIL 1
W=WIDTH OF SEALANT RESERVOIR (IN.)	1/2
D=DEPTH OF SEALANT RESERVOIR (IN.)	1/2
B=BACKER ROD DIAMETER (IN.)	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	3/4

NOTE: JOINT SEALING SHALL BE PER SECTION 605 OF SPECIAL PROVISIONS.

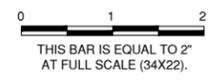


DUCT MARKER DETAIL
NOT TO SCALE

IL CONTRACT: **KA050**
IL LETTING ITEM: **06A**
IL PROJECT: **IKK-4809**
S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS
REHABILITATE RUNWAY 16/34**

**TYPICAL SECTIONS AND
PAVEMENT REPAIR DETAILS**

© Copyright CMT, Inc.

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

KANKAKEE VALLEY AIRPORT AUTHORITY

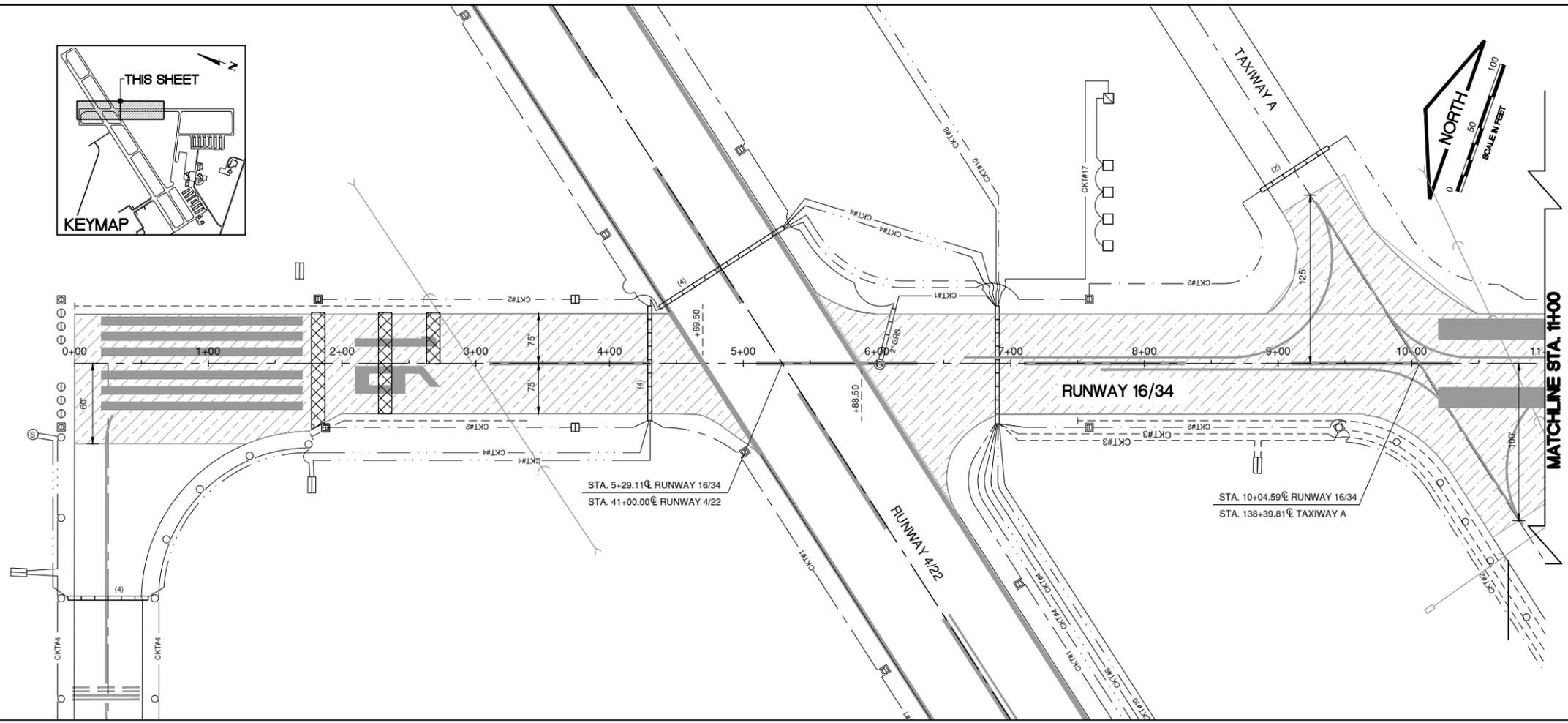
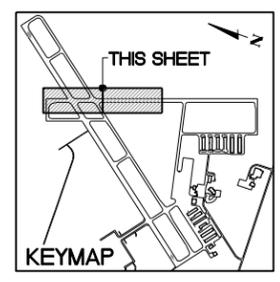
DESIGN BY: KWS
DRAWN BY: JRO
CHECKED BY: DKP
APPROVED BY: ---
DATE: 6/5/2020
JOB No: 19007502.00

FINAL

DATE: Friday, June 5, 2020 9:44:43 PM
 FILE: K:\kandalegi\1907502_Plan\Draw\Sheet\1907502 - Econd1.dwg
 UPDATE BY: Kris Salvatera
 LAYOUT: Layout1
 Express queries: PC-91.kmu
 BDF: BDF1

NOTES

1. PRIOR TO PAVING OPERATIONS, THE FINAL MILLED SURFACE SHALL BE BROOMED CLEAN AND THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL INSPECT THE FINAL SURFACE. ALL LOOSE PIECES SHALL BE REMOVED. ALL CRACKS FOUND TO BE WIDER THAN 1" SHALL BE REPAIRED UNDER ITEM AR201663, SAND MIX CRACK REPAIR.
2. THE DETERMINATION AND LIMITS OF REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A AND TYPE B SHALL BE COORDINATED WITH THE RESIDENT ENGINEER AFTER INSPECTION OF THE MILLED SURFACE.
3. EXISTING DUCT MARKERS SHALL BE SURVEYED BY THE CONTRACTOR BEFORE SURFACE IS MILLED. DUCT MARKERS SHALL BE REPLACED AT SAME LOCATIONS SURVEYED (COST INCIDENTAL TO THE CONTRACT).
4. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
5. DRAINAGE COLLECTION STRUCTURES TO BE ADJUSTED SHALL BE DETERMINED IN THE FIELD AND THE NEW RIM ELEVATION SHALL MATCH THE NEW GRADING ELEVATIONS.
6. REFER TO SHEET "BITUMINOUS MILLING DEPTH TABLE - RUNWAY 16/34" FOR LOCATIONS WHERE BITUMINOUS MILLING MAY NOT BE REQUIRED FOR SURFACE LEVELING AREAS.
7. APPROXIMATELY 8-9 LOADS OF BITUMINOUS PAVEMENT MILLING SHALL BE RELOCATED TO A STOCKPILE TO BE DETERMINED BY THE AIRPORT MANAGER. THE REMAINING BITUMINOUS MILLINGS SHALL BE HAULED OFF SITE. THE COST OF RELOCATING THE PAVEMENT MILLINGS SHALL BE CONSIDERED INCIDENTAL TO PAY ITEM AR401650 - BITUMINOUS PAVEMENT MILLING.
8. CONTRACTOR HAS THE OPTION TO USE BITUMINOUS PAVEMENT MILLINGS FOR THE STABILIZED CONSTRUCTION ENTRANCE FROM THE ACCESS ROAD AT NO EXTRA COST TO THE CONTRACT. PLACEMENT AND COMPACTION OF BITUMINOUS MILLINGS SHALL BE TO THE SATISFACTION OF THE RESIDENT ENGINEER.

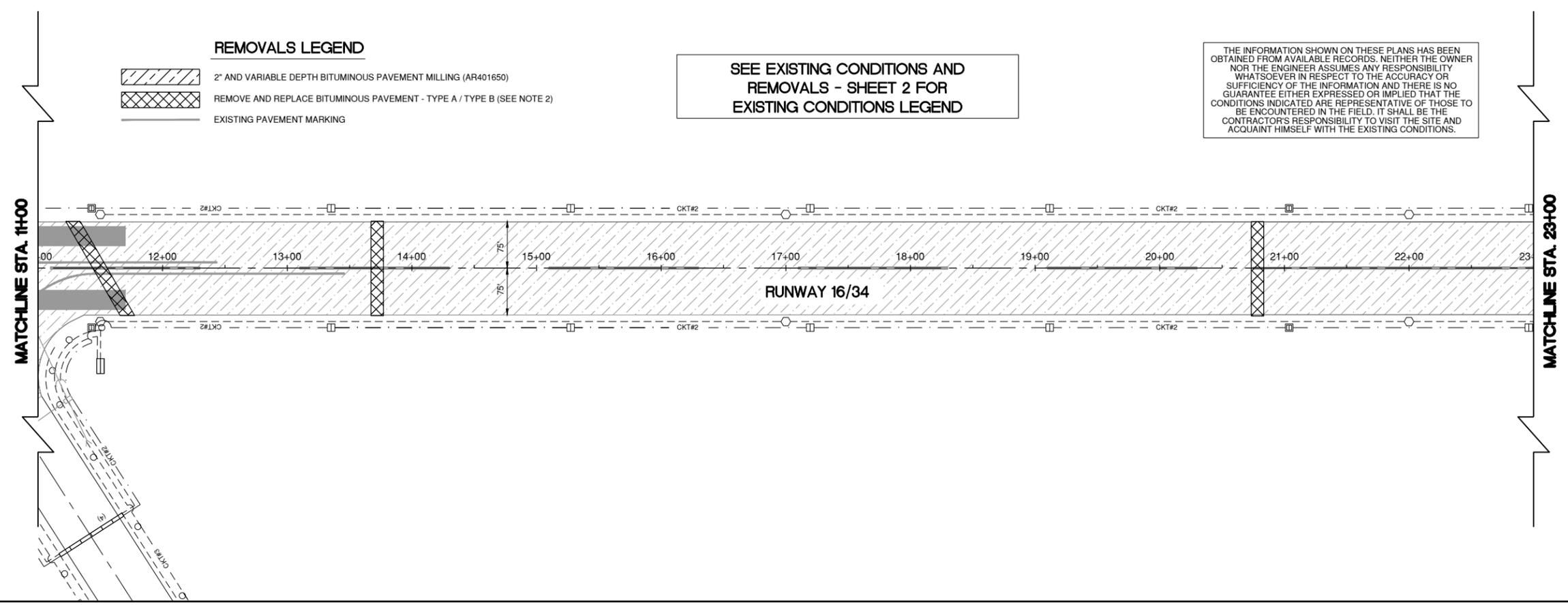


IL CONTRACT: **KA050**
 IL LETTING ITEM: **06A**
 IL PROJECT: **IKK-4809**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



REMOVALS LEGEND

	2" AND VARIABLE DEPTH BITUMINOUS PAVEMENT MILLING (AR401650)
	REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A / TYPE B (SEE NOTE 2)
	EXISTING PAVEMENT MARKING

SEE EXISTING CONDITIONS AND REMOVALS - SHEET 2 FOR EXISTING CONDITIONS LEGEND

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS
REHABILITATE RUNWAY 16/34
EXISTING CONDITIONS AND REMOVALS - SHEET 1

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

KANKAKEE VALLEY

 AIRPORT AUTHORITY

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	KWS
APPROVED BY:	---
DATE:	6/5/2020
JOB No:	19007502.00

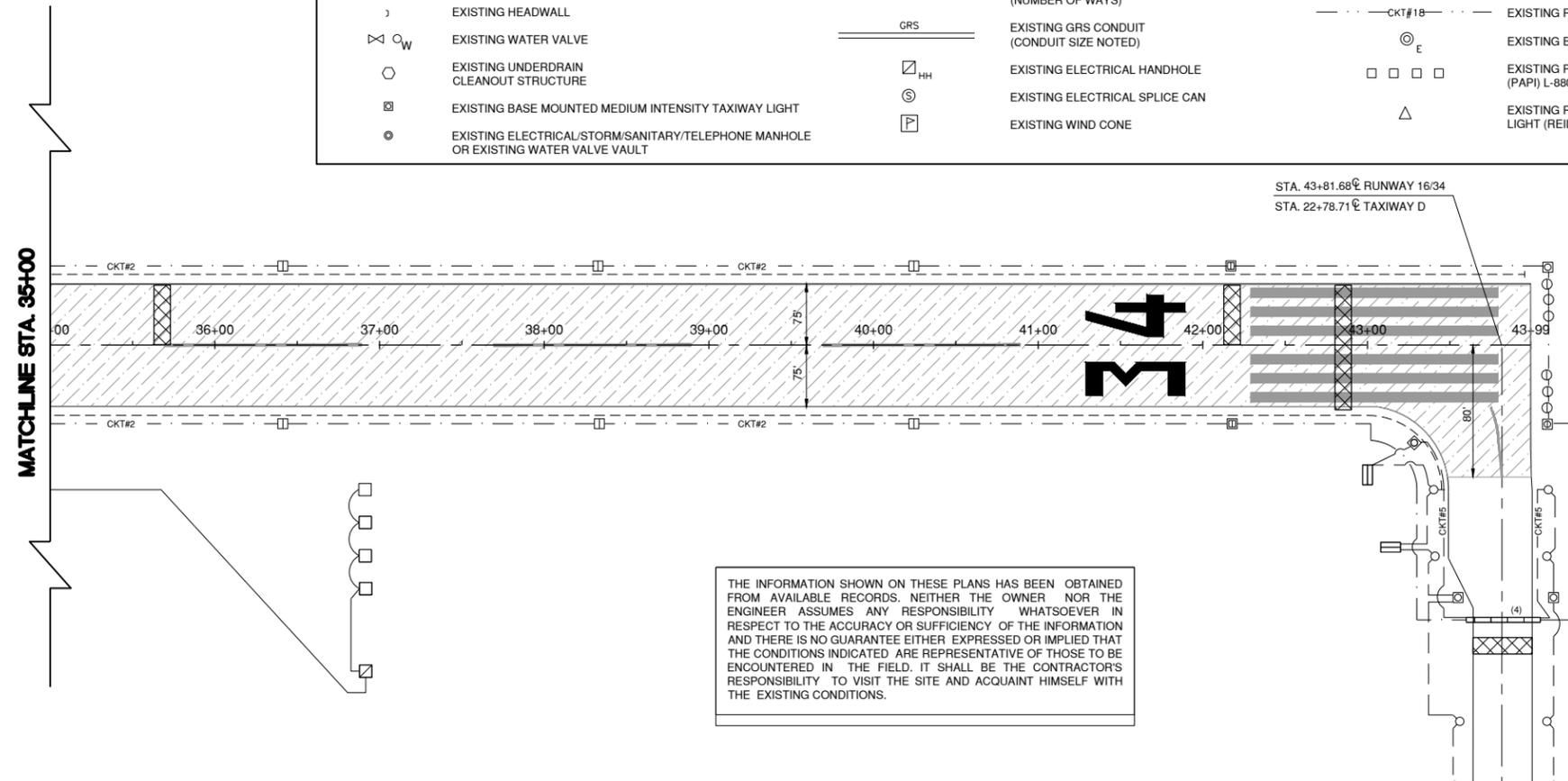
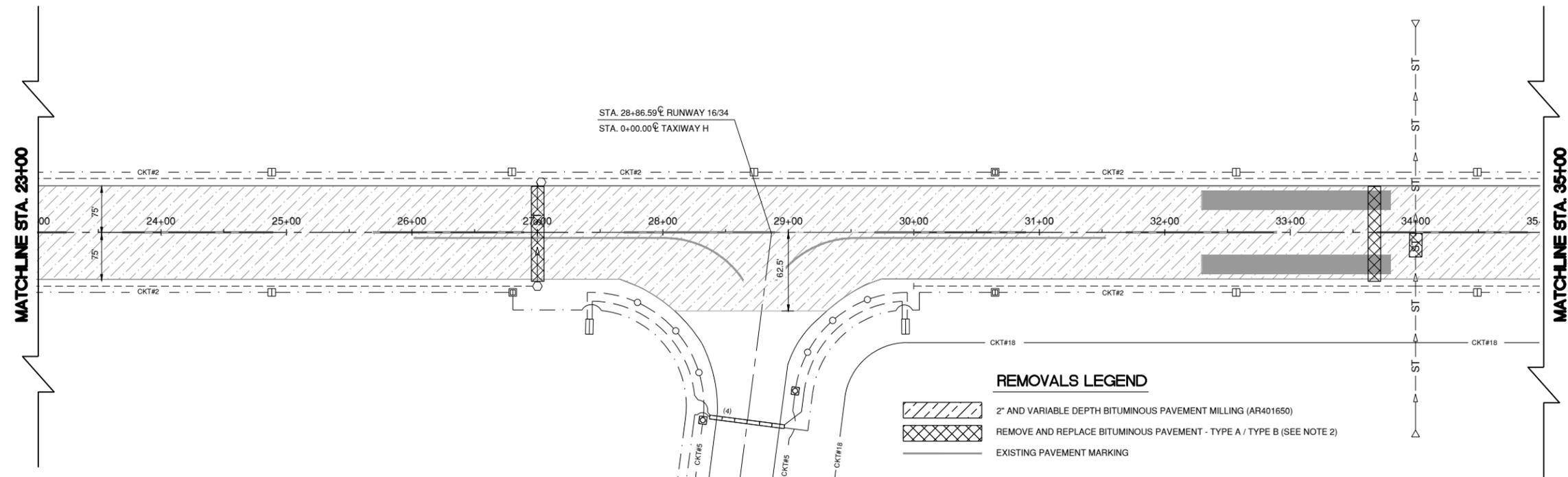
FINAL

6/16/2020 9:44:57 PM
K:\Projects\19007502 - Rehabilitation of Runway 16/34 - Ecorse\Drawings

6/16/2020

UPDATE BY: Kris Salathera
LAYOUT: Layout

DATE: Friday, June 5, 2020 9:44:57 PM
FILE: K:\Projects\19007502 - Rehabilitation of Runway 16/34 - Ecorse\Drawings



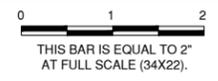
THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

EXISTING CONDITIONS LEGEND			
	EXISTING CONDUIT/DUCT BANK		EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
	EXISTING RUNWAY/TAXIWAY CIRCUIT		EXISTING STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
	EXISTING STORM SEWER		EXISTING STAKE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT
	EXISTING UNDERDRAIN		EXISTING BASE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT
	EXISTING WATERMAIN		EXISTING STAKE MOUNTED THRESHOLD LIGHT
	EXISTING SANITARY SEWER		EXISTING BASE MOUNTED THRESHOLD LIGHT
	EXISTING STORM INLET		EXISTING CONCRETE ENCASED DUCT BANK (NUMBER OF WAYS)
	EXISTING SLOPE BOX		EXISTING GRS CONDUIT (CONDUIT SIZE NOTED)
	EXISTING FLARED END SECTION		EXISTING ELECTRICAL HANDHOLE
	EXISTING HEADWALL		EXISTING ELECTRICAL SPLICE CAN
	EXISTING WATER VALVE		EXISTING WIND CONE
	EXISTING UNDERDRAIN CLEANOUT STRUCTURE		EXISTING AIRFIELD GUIDANCE SIGN
	EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT		EXISTING RUNWAY 4/22 CIRCUIT
	EXISTING ELECTRICAL/STORM/SANITARY/TELEPHONE MANHOLE OR EXISTING WATER VALVE VAULT		EXISTING RUNWAY 16/34 CIRCUIT
			EXISTING TAXIWAY A, AA, C, D, E CIRCUIT
			EXISTING TAXIWAY B/BB CIRCUIT
			EXISTING TAXIWAY H CIRCUIT
			EXISTING WINDCONE/RUNWAY 22 PAPI
			EXISTING RUNWAY 22 REIL
			EXISTING RUNWAY 16 PAPI
			EXISTING RUNWAY 34 PAPI
			EXISTING ELECTRICAL MANHOLE
			EXISTING PRECISION APPROACH PATH INDICATORS (PAPI) L-880 SYSTEM
			EXISTING RUNWAY END IDENTIFIER LIGHT (REIL)

REMOVALS LEGEND	
	2' AND VARIABLE DEPTH BITUMINOUS PAVEMENT MILLING (AR401650)
	REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A / TYPE B (SEE NOTE 2)
	EXISTING PAVEMENT MARKING

IL CONTRACT: KA050
 IL LETTING ITEM: 06A
 IL PROJECT: IKK-4809
 S.B.G. PROJECT: 3-17-SBGP-TBD

REVISIONS		
NUMBER	BY	DATE



GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE RUNWAY 16/34

EXISTING CONDITIONS AND
 REMOVALS - SHEET 2

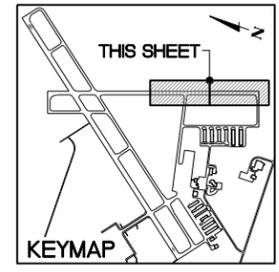
© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

KANKAKEE VALLEY
KVA
 AIRPORT AUTHORITY

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	KWS
APPROVED BY:	---
DATE:	6/5/2020
JOB No:	19007502.00

FINAL



BMP: 19/10/20-20-50.0061
 BMP: 19/10/20-20-50.0061

6dfc867

UPDATE BY: Kris Schwabera
 LAYOUT: Layout

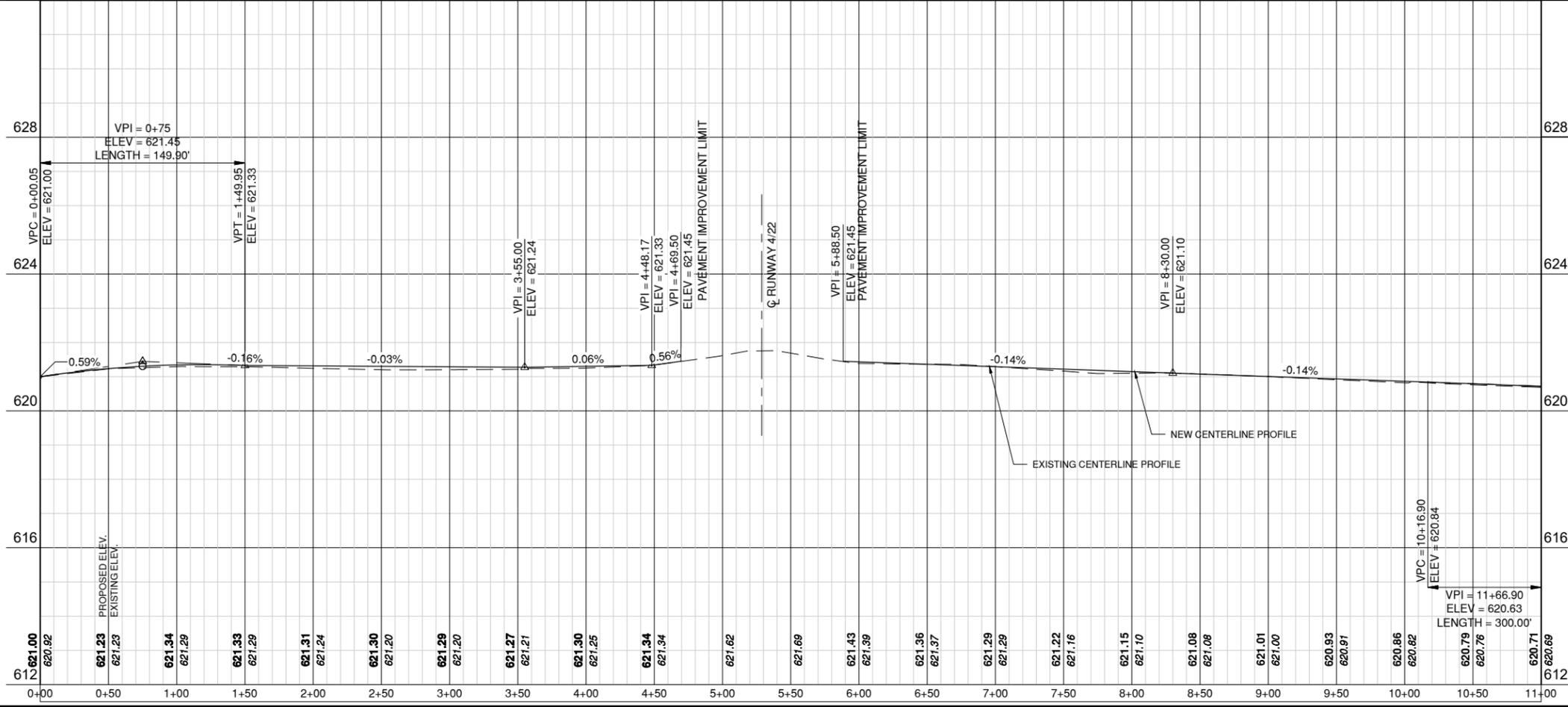
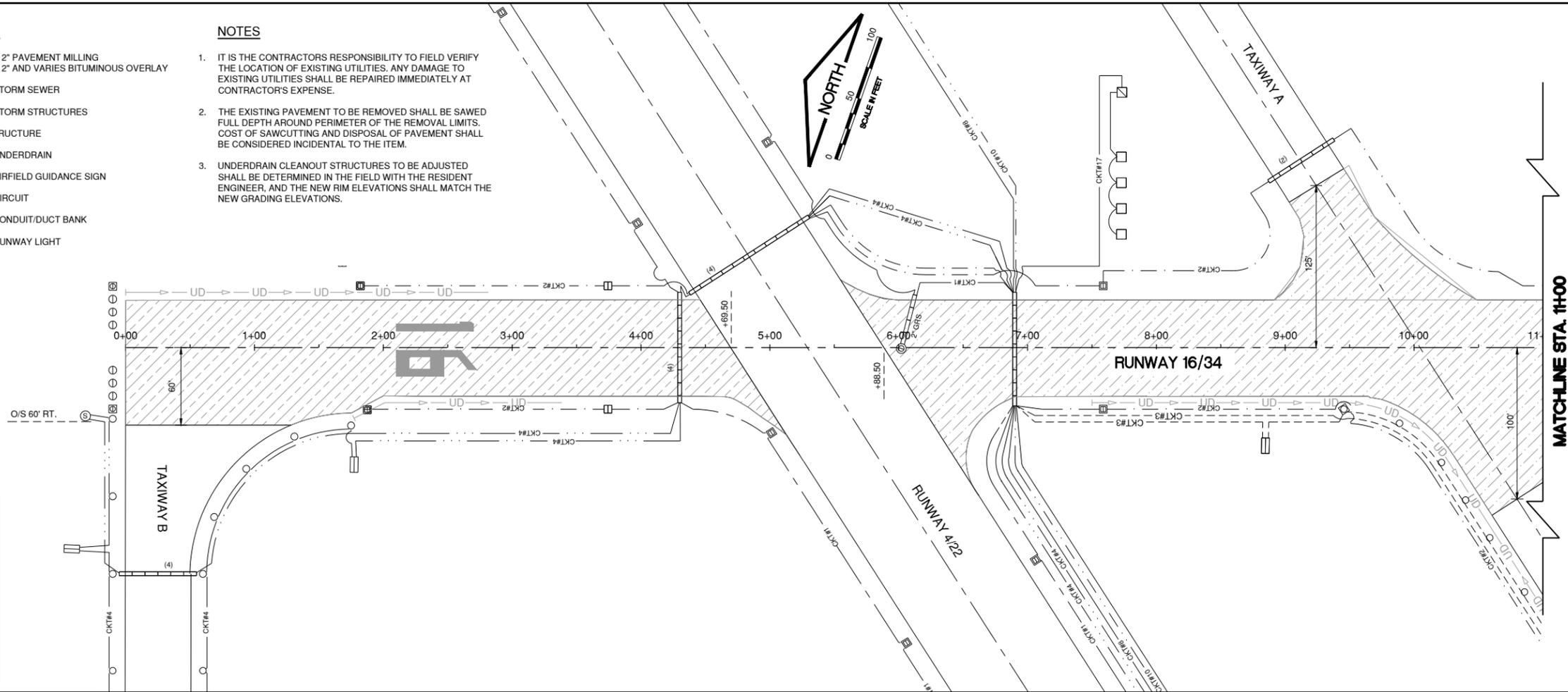
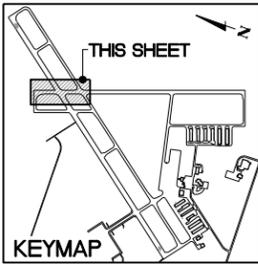
DATE: Friday, June 5, 2020 9:45:16 PM
 FILE: K:\Kankakee\19072020-Paradise\16-34-Draw\Sheet\19072020-Paradise.dwg

LEGEND

- PROPOSED 2" PAVEMENT MILLING
PROPOSED 2" AND VARIES BITUMINOUS OVERLAY
- EXISTING STORM SEWER
- EXISTING STORM STRUCTURES
- ADJUST STRUCTURE
- EXISTING UNDERDRAIN
- EXISTING AIRFIELD GUIDANCE SIGN
- EXISTING CIRCUIT
- EXISTING CONDUIT/DUCT BANK
- EXISTING RUNWAY LIGHT

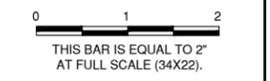
NOTES

1. IT IS THE CONTRACTORS RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT CONTRACTOR'S EXPENSE.
2. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
3. UNDERDRAIN CLEANOUT STRUCTURES TO BE ADJUSTED SHALL BE DETERMINED IN THE FIELD WITH THE RESIDENT ENGINEER, AND THE NEW RIM ELEVATIONS SHALL MATCH THE NEW GRADING ELEVATIONS.



IL CONTRACT: **KA050**
 IL LETTING ITEM: **06A**
 IL PROJECT: **IKK-4809**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE RUNWAY 16/34**

PLAN AND PROFILE - SHEET 1

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

KANKAKEE VALLEY AIRPORT AUTHORITY

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	6/5/2020
JOB No:	19007502.00

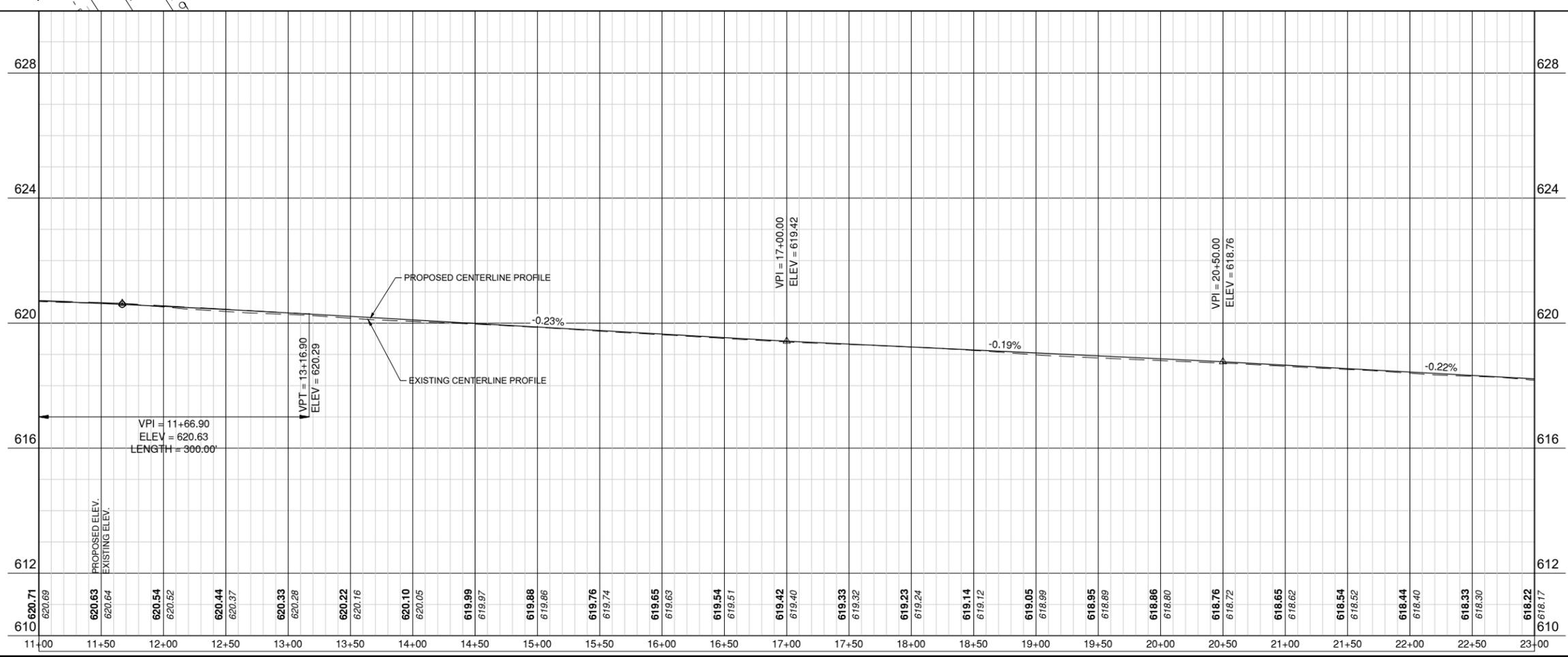
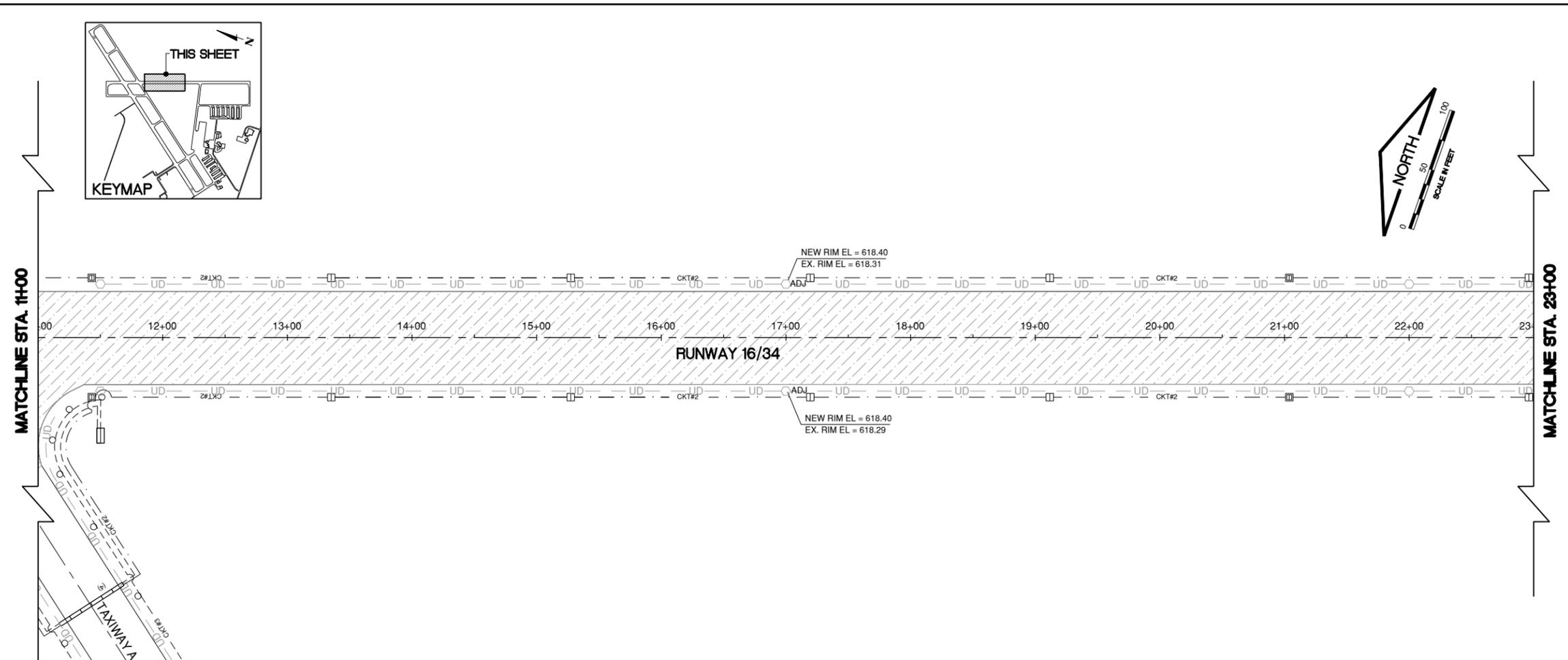
FINAL

SHEET 13 OF 21 SHEETS

BMP: 11/10/20-20-52.0061
BMP: 11/10/20-20-52.0061

UPDATE BY: Kris Salvatera
LAYOUT: Layout

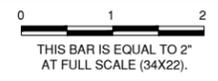
DATE: Friday, June 5, 2020 9:45:44 PM
FILE: K:\kandakoo\1907502_Plan\16-34\Draw\Sheet\1907502_Plan.dwg



IL CONTRACT: **KA050**
IL LETTING ITEM: **06A**
IL PROJECT: **IKK-4809**
S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS
REHABILITATE RUNWAY 16/34**

PLAN AND PROFILE - SHEET 2

© Copyright CMT, Inc.

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	6/5/2020
JOB No:	19007502.00

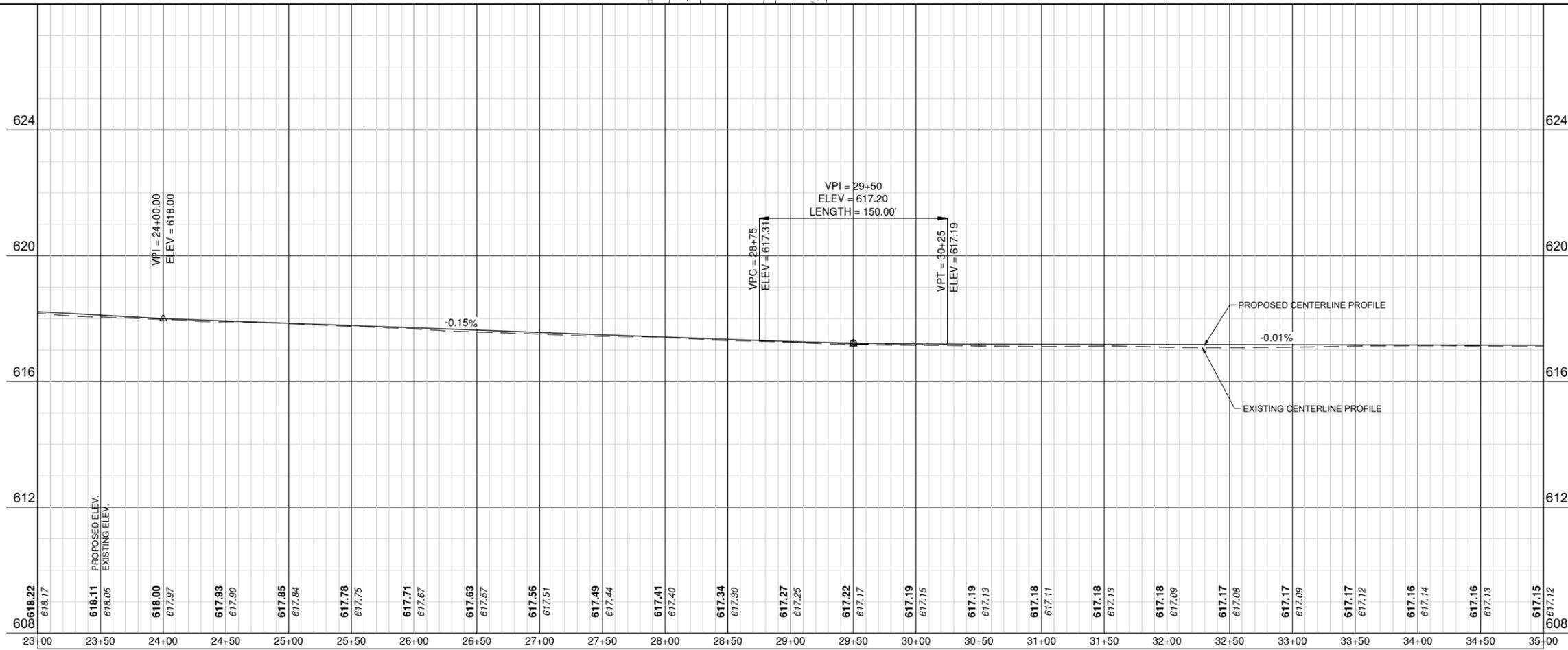
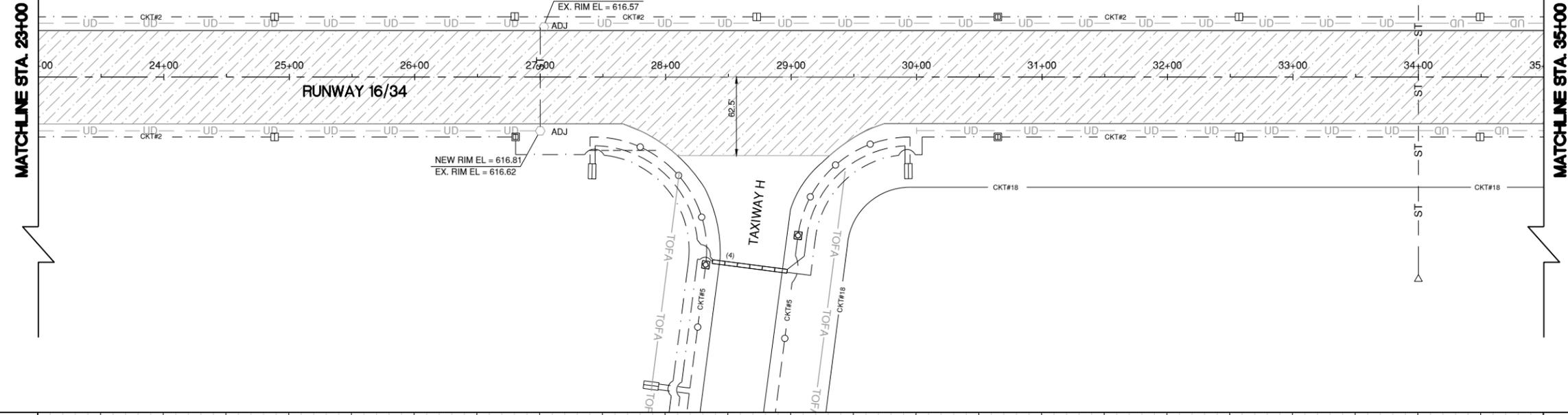
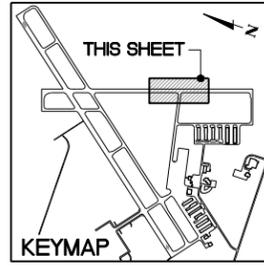
FINAL

BMP: 11/10/16-20-54-0061
 BMP: 11/10/16-20-54-0061
 BMP: 11/10/16-20-54-0061

8d1c8671

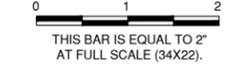
UPDATE BY: Kris Salvatera
 LAYOUT: Layout1

DATE: Friday, June 5, 2020 9:45:06 PM
 FILE: K:\Kankakee\1907502-Rehab\16-34\Draw\Sheets\1907502-16-34.dwg



IL CONTRACT: **KA050**
 IL LETTING ITEM: **06A**
 IL PROJECT: **IKK-4809**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE RUNWAY 16/34**

PLAN AND PROFILE - SHEET 3

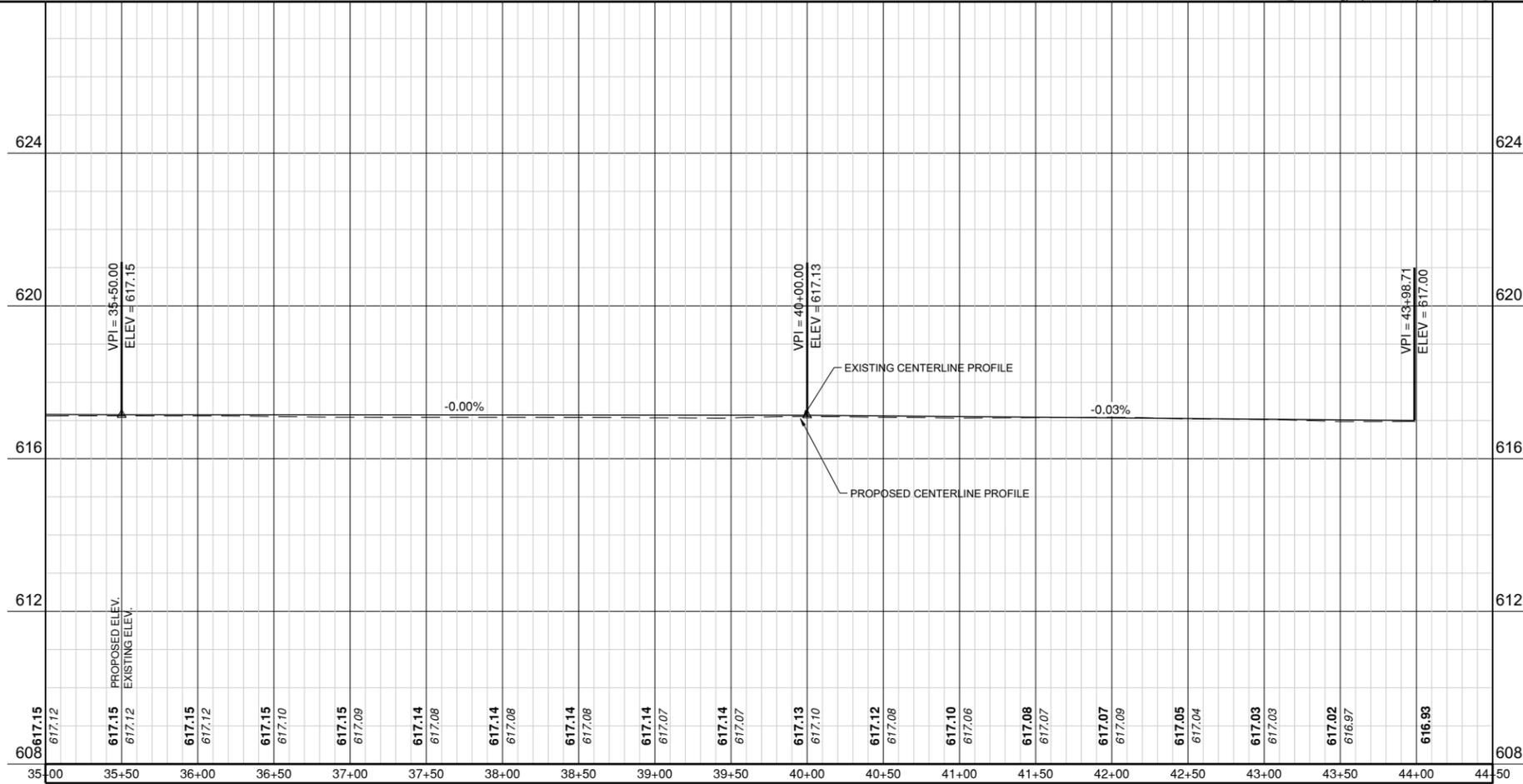
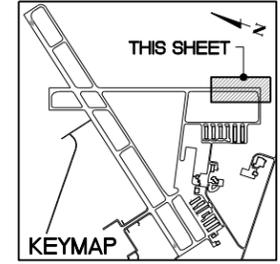
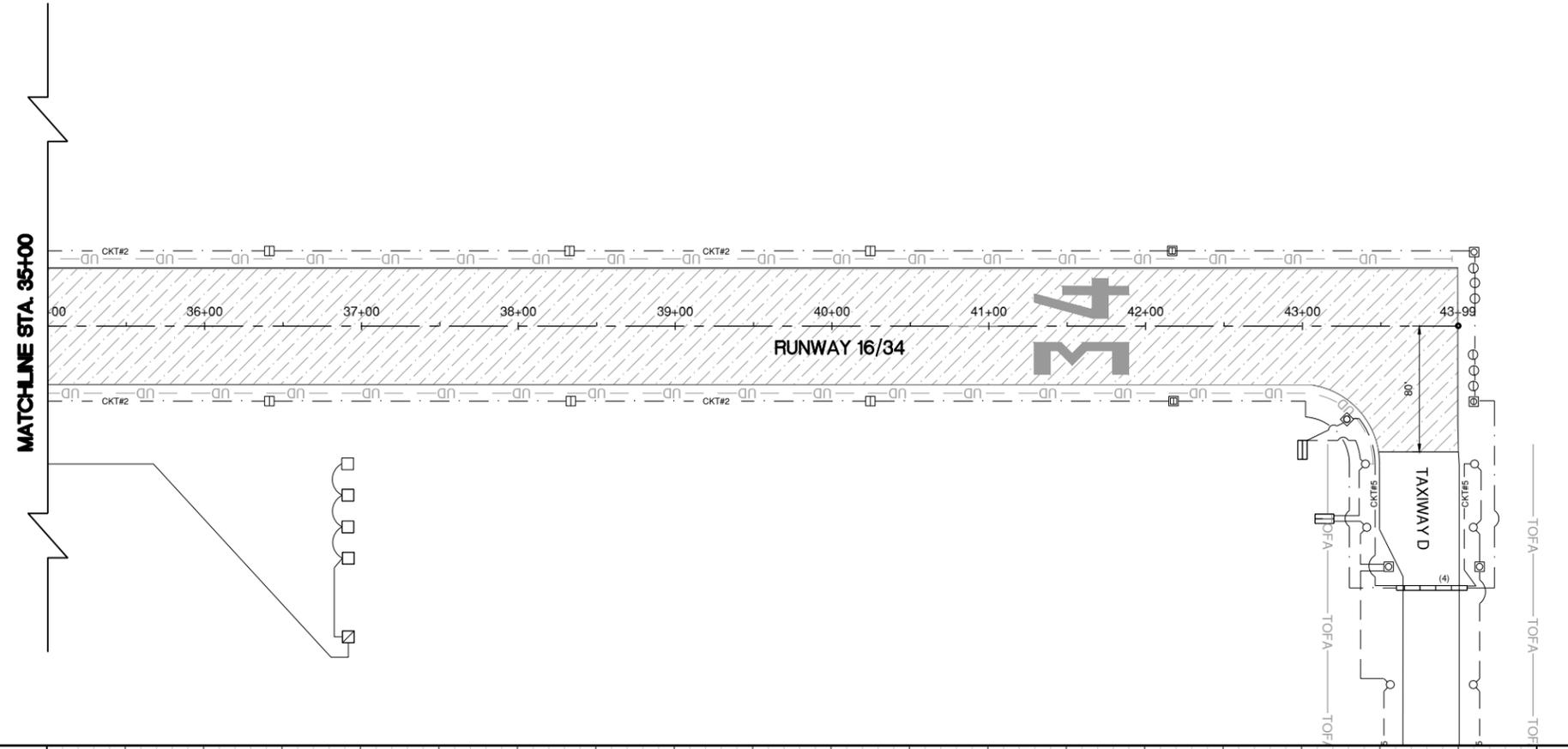
© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

KANKAKEE VALLEY AIRPORT AUTHORITY

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	6/5/2020
JOB No:	19007502.00

FINAL



IL CONTRACT: **KA050**
IL LETTING ITEM: **06A**
IL PROJECT: **IKK-4809**
S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS
REHABILITATE RUNWAY 16/34**

PLAN AND PROFILE - SHEET 4

© Copyright CMT, Inc.



CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613



DESIGN BY: KWS

DRAWN BY: JRO

CHECKED BY: DKP

APPROVED BY: DKP

DATE: 6/5/2020

JOB No: 19007502.00

FINAL

SHEET 16 OF 21 SHEETS

DATE: Friday, June 5, 2020 9:46:33 PM
 FILE: K:\cadd\p1307532_P1307532_R1307532_Bituminous Paving Milling Table.dwg
 UPDATE BY: Kris Salwaters
 LAYOUT: Layout1
 bdfc067
 bdfc067
 bdfc067

BITUMINOUS PAVEMENT MILLING (AR401650) - MILLING DEPTHS															
STATION	37.5' LEFT OF CENTERLINE			18.75' LEFT OF CENTERLINE			CENTERLINE			18.75' RIGHT OF CENTERLINE			37.5' RIGHT OF CENTERLINE		
	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH
0+00.00	620.44	620.43	0.16	620.72	620.75	0.20	621.00	620.94	0.11	620.72	620.74	0.19	620.44	620.33	0.06
0+50.00	620.48	620.54	0.23	620.86	621.00	0.31	621.23	621.23	0.17	620.86	620.98	0.29	620.48	620.52	0.21
1+00.00	620.59	620.59	0.17	620.97	621.02	0.22	621.34	621.29	0.12	620.97	621.07	0.27	620.59	620.61	0.19
1+50.00	620.58	620.62	0.21	620.96	620.98	0.19	621.33	621.29	0.13	620.96	621.09	0.30	620.58	620.57	0.16
2+00.00	620.56	620.61	0.22	620.94	620.96	0.19	621.31	621.24	0.10	620.94	621.00	0.23	620.56	620.46	0.07
2+50.00	620.55	620.60	0.22	620.93	620.91	0.15	621.30	621.20	0.07	620.93	620.97	0.21	620.55	620.47	0.09
3+00.00	620.54	620.50	0.13	620.92	620.89	0.14	621.29	621.20	0.08	620.92	620.98	0.23	620.54	620.47	0.10
3+50.00	620.52	620.46	0.11	620.90	620.96	0.23	621.27	621.21	0.11	620.90	621.00	0.27	620.52	620.58	0.23
4+00.00	620.73	620.73	0.17	621.06	621.06	0.17	621.25	621.25	0.17	621.10	621.10	0.17	620.71	620.71	0.17
6+50.00	620.89	620.89	0.17	621.19	621.19	0.17	621.37	621.37	0.17	621.26	621.26	0.17	621.07	621.07	0.17
7+00.00	620.65	620.65	0.17	621.04	621.04	0.17	621.29	621.29	0.17	621.10	621.10	0.17	620.73	620.73	0.17
7+50.00	620.47	620.43	0.13	620.85	620.90	0.22	621.22	621.16	0.11	620.85	620.92	0.24	620.47	620.38	0.08
8+00.00	620.40	620.33	0.10	620.78	620.77	0.16	621.15	621.10	0.12	620.78	620.73	0.12	620.40	620.33	0.10
8+50.00	620.33	620.30	0.14	620.71	620.73	0.19	621.08	621.08	0.17	620.71	620.75	0.21	620.33	620.33	0.17
9+00.00	620.26	620.25	0.16	620.64	620.65	0.18	621.01	621.00	0.16	620.64	620.68	0.21	620.26	620.29	0.20
9+50.00	620.18	620.11	0.10	620.56	620.55	0.16	620.93	620.91	0.15	620.56	620.61	0.22	620.18	620.16	0.15
10+00.00	620.11	620.12	0.18	620.49	620.47	0.15	620.86	620.82	0.13	620.49	620.46	0.14	620.11	620.09	0.15
10+50.00	620.04	619.94	0.07	620.42	620.41	0.16	620.79	620.76	0.14	620.42	620.45	0.20	620.04	620.04	0.17
11+00.00	619.96	619.91	0.12	620.34	620.35	0.18	620.71	620.69	0.15	620.34	620.37	0.20	619.96	619.96	0.17
11+50.00	619.88	619.80	0.09	620.26	620.29	0.20	620.63	620.64	0.18	620.26	620.32	0.23	619.88	619.76	0.05
12+00.00	619.79	619.70	0.08	620.17	620.15	0.15	620.54	620.52	0.15	620.17	620.23	0.23	619.79	619.77	0.15
12+50.00	619.69	619.59	0.07	620.07	620.02	0.12	620.44	620.37	0.10	620.07	620.10	0.20	619.69	619.63	0.11
13+00.00	619.58	619.48	0.07	619.96	619.91	0.12	620.33	620.28	0.12	619.96	620.01	0.22	619.58	619.56	0.15
13+50.00	619.47	619.38	0.08	619.85	619.78	0.10	620.22	620.16	0.11	619.85	619.87	0.19	619.47	619.46	0.16
14+00.00	619.35	619.26	0.08	619.73	619.69	0.13	620.10	620.05	0.12	619.73	619.74	0.18	619.35	619.33	0.15
14+50.00	619.25	619.13	0.05	619.62	619.61	0.15	620.00	619.97	0.14	619.62	619.62	0.16	619.25	619.21	0.13
15+00.00	619.13	619.06	0.10	619.51	619.52	0.18	619.88	619.86	0.15	619.51	619.53	0.19	619.13	619.14	0.18
15+50.00	619.01	618.95	0.11	619.39	619.38	0.16	619.76	619.74	0.15	619.39	619.43	0.21	619.01	619.04	0.20
16+00.00	618.90	618.85	0.12	619.28	619.27	0.16	619.65	619.63	0.15	619.28	619.30	0.19	618.90	618.92	0.19
16+50.00	618.79	618.70	0.08	619.17	619.15	0.15	619.54	619.51	0.14	619.17	619.17	0.17	618.79	618.76	0.14
17+00.00	618.67	618.61	0.11	619.05	619.04	0.16	619.42	619.40	0.15	619.05	619.05	0.17	618.67	618.59	0.09
17+50.00	618.58	618.55	0.14	618.96	618.96	0.17	619.33	619.32	0.16	618.96	618.97	0.18	618.58	618.53	0.12
18+00.00	618.48	618.42	0.11	618.86	618.87	0.18	619.23	619.24	0.18	618.86	618.90	0.21	618.48	618.44	0.13
18+50.00	618.39	618.33	0.11	618.77	618.75	0.15	619.14	619.12	0.15	618.77	618.76	0.16	618.39	618.29	0.07
19+00.00	618.30	618.21	0.08	618.68	618.62	0.11	619.05	618.99	0.11	618.68	618.64	0.13	618.30	618.16	0.03
19+50.00	618.20	618.13	0.10	618.58	618.52	0.11	618.95	618.89	0.11	618.58	618.54	0.13	618.20	618.09	0.06

NOTES

- MILL DEPTH IS SHOWN IN FEET.
- THE BITUMINOUS PAVEMENT MILLING DEPTHS ARE PROVIDED TO ACHIEVE THE 2" BITUMINOUS OVERLAY DEPTH.
- NEGATIVE BITUMINOUS MILLING DEPTHS REFLECT A REQUIRED BITUMINOUS LEVELING TO ACHIEVE THE 2" BITUMINOUS OVERLAY. NO BITUMINOUS MILLING IS REQUIRED IN THESE LOCATIONS.
- BITUMINOUS LEVELING SHALL BE PLACED AT THE NEGATIVE MILL DEPTH LOCATIONS AS SPECIFIED IN THE TABLE TO ACHIEVE THE APPROPRIATE DEPTH PRIOR TO THE 2" BITUMINOUS OVERLAY. BITUMINOUS LEVELING SHALL BE PAID FOR UNDER AR401610 - BITUMINOUS SURFACE COURSE.
- NO ADDITIONAL PAYMENT FOR MILLING DEPTHS GREATER THAN SPECIFIED IN THE PLANS WILL BE MADE
- N/A ELEVATIONS ARE LOCATIONS OUTSIDE THE RUNWAY PAVEMENT.
- SEE INTERSECTION GRADING PLAN FOR ELEVATIONS OF CROSSING INTERSECTIONS.

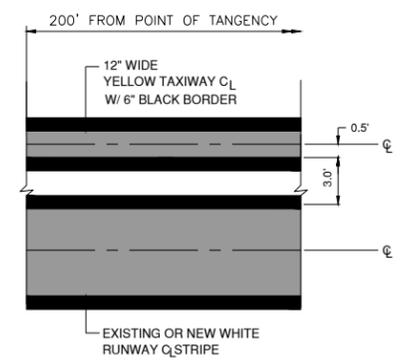
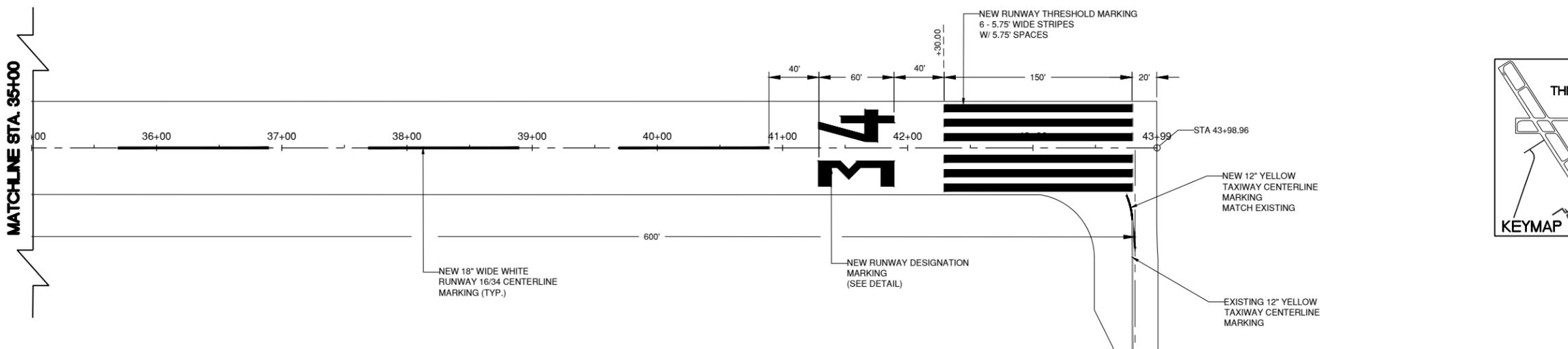
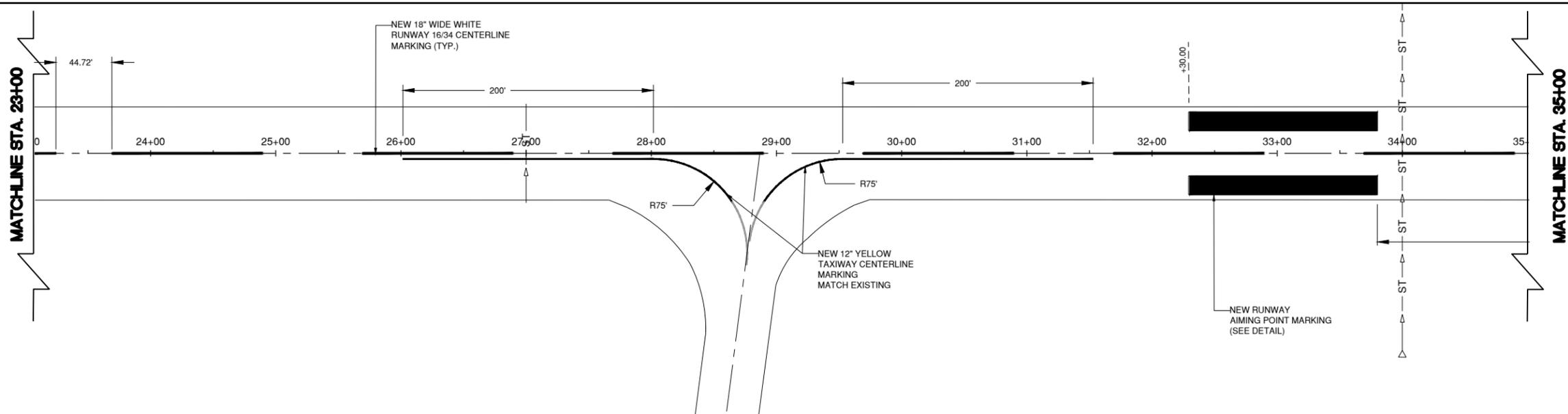
BITUMINOUS PAVEMENT MILLING (AR401650) - MILLING DEPTHS															
STATION	37.5' LEFT OF CENTERLINE			18.75' LEFT OF CENTERLINE			CENTERLINE			18.75' RIGHT OF CENTERLINE			37.5' RIGHT OF CENTERLINE		
	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH
20+00.00	618.11	618.01	0.07	618.49	618.44	0.12	618.86	618.80	0.11	618.49	618.46	0.14	618.11	618.02	0.08
20+50.00	618.01	617.90	0.06	618.39	618.37	0.15	618.76	618.72	0.13	618.39	618.36	0.14	618.01	617.90	0.06
21+00.00	617.90	617.78	0.05	618.28	618.26	0.15	618.65	618.62	0.14	618.28	618.25	0.14	617.90	617.77	0.04
21+50.00	617.79	617.71	0.09	618.17	618.17	0.17	618.54	618.52	0.15	618.17	618.15	0.15	617.79	617.67	0.05
22+00.00	617.69	617.59	0.07	618.07	618.05	0.15	618.44	618.40	0.13	618.07	618.04	0.14	617.69	617.51	-0.01
22+50.00	617.58	617.48	0.07	617.96	617.93	0.14	618.33	618.30	0.14	617.96	617.95	0.16	617.58	617.48	0.07
23+00.00	617.47	617.38	0.08	617.85	617.82	0.14	618.22	618.17	0.12	617.85	617.84	0.16	617.47	617.38	0.08
23+50.00	617.36	617.28	0.09	617.74	617.71	0.14	618.11	618.05	0.11	617.74	617.72	0.15	617.36	617.23	0.04
24+00.00	617.25	617.18	0.10	617.63	617.62	0.16	618.00	617.97	0.14	617.63	617.64	0.18	617.25	617.16	0.08
24+50.00	617.18	617.09	0.08	617.56	617.54	0.15	617.93	617.90	0.14	617.56	617.58	0.19	617.18	617.12	0.11
25+00.00	617.10	617.03	0.10	617.48	617.48	0.17	617.85	617.84	0.16	617.48	617.52	0.21	617.10	617.05	0.12
25+50.00	617.03	616.95	0.09	617.41	617.39	0.15	617.78	617.75	0.14	617.41	617.43	0.19	617.03	617.00	0.14
26+00.00	616.96	616.88	0.09	617.34	617.32	0.15	617.71	617.67	0.13	617.34	617.34	0.17	616.96	616.92	0.13
26+50.00	616.88	616.80	0.09	617.26	617.25	0.16	617.63	617.57	0.11	617.26	617.24	0.15	616.88	616.81	0.10
27+00.00	616.81	616.70	0.06	617.19	617.17	0.15	617.56	617.51	0.12	617.19	617.14	0.12	616.81	616.69	0.05
27+50.00	616.74	616.65	0.08	617.12	617.10	0.15	617.49	617.44	0.12	617.12	617.10	0.15	616.74	616.69	0.12
28+00.00	616.66	616.57	0.08	617.04	617.04	0.17	617.41	617.40	0.16	617.13	617.09	0.13	616.85	616.78	0.10
28+50.00	616.59	616.52	0.10	616.97	616.93	0.13	617.34	617.30	0.13	617.06	617.02	0.13	616.78	616.72	0.11
29+00.00	616.52	616.50	0.15	616.90	616.94	0.21	617.27	617.25	0.15	616.99	616.98	0.16	616.71	616.63	0.09
29+50.00	616.47	616.45	0.15	616.85	616.86	0.18	617.22	617.17	0.12	616.85	616.87	0.19	616.47	616.48	0.18
30+00.00	616.44	616.40	0.13	616.82	616.83	0.18	617.19	617.15	0.13	616.82	616.86	0.21	616.44	616.50	0.23
30+50.00	616.44	616.38	0.11	616.82	616.81	0.16	617.19	617.13	0.11	616.82	616.86	0.21	616.44	616.41	0.14
31+00.00	616.43	616.33	0.07	616.81	616.78	0.14	617.18	617.11	0.10	616.81	616.81	0.17	616.43	616.39	0.13
31+50.00	616.43	616.35	0.09	616.81	616.81	0.17	617.18	617.13	0.12	616.81	616.84	0.20	616.43	616.44	0.18
32+00.00	616.43	616.30	0.04	616.81	616.74	0.10	617.18	617.09	0.08	616.81	616.79	0.15	616.43	616.37	0.11
32+50.00	616.42	616.36	0.11	616.80	616.75	0.12	617.17	617.08	0.08	616.80	616.77	0.14	616.42	616.39	0.14
33+00.00	616.42	616.43	0.18	616.80	616.80	0.17	617.17	617.09	0.09	616.80	616.80	0.17	616.42	616.47	0.22
33+50.00	616.61	616.51	0.07	616.89	616.86	0.14	617.17	617.12	0.12	616.89	616.87	0.15	616.61	616.57	0.13
34+00.00	616.60	616.56	0.13	616.88	616.89	0.18	617.16	617.14	0.15	616.93	61				

6/19/2020 9:47:40 PM

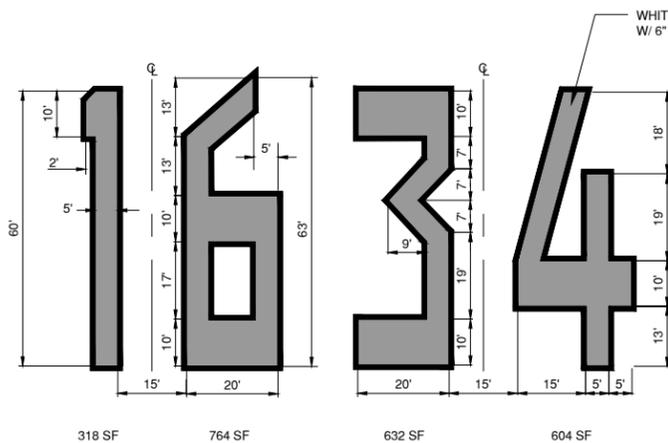
8/1/2021

UPDATE BY: Kris Schallera
LAYOUT: Layout

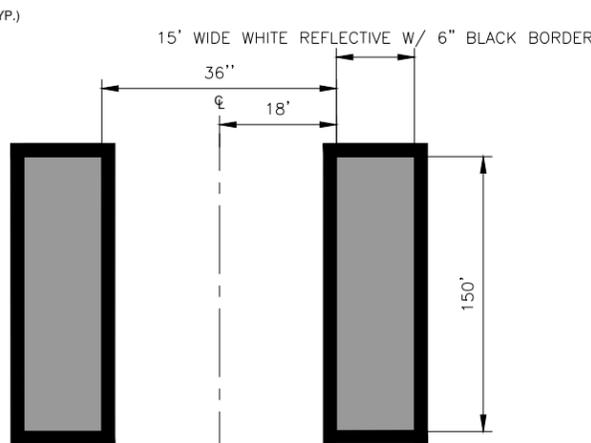
DATE: Friday, June 5, 2020 9:47:40 PM
FILE: K:\aerial\1907202 - Rehab\Draw\Sheet\1907202 - Pavement Marking - 2.dwg



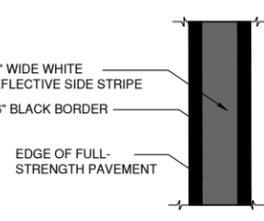
TAXIWAY LEAD-IN TANGENT DETAIL
NOT TO SCALE



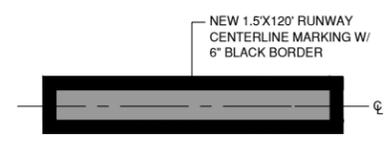
RUNWAY NUMERALS
NOT TO SCALE



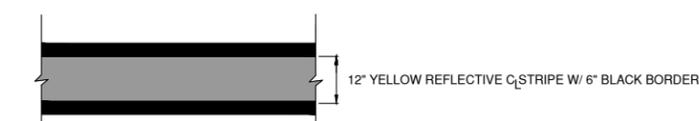
AIMING POINT MARKERS
NOT TO SCALE



RUNWAY EDGE MARKING CONTINUOUS
NOT TO SCALE



RUNWAY CENTERLINE - DASHED
NOT TO SCALE

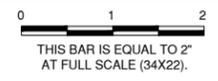


TAXIWAY CENTERLINE DETAIL
NOT TO SCALE

IL CONTRACT: KA050
IL LETTING ITEM: 06A
IL PROJECT: IKK-4809
S.B.G. PROJECT: 3-17-SBGP-TBD

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS
REHABILITATE RUNWAY 16/34

PAVEMENT MARKING - SHEET 2

© Copyright CMT, Inc.

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

KANKAKEE VALLEY
KVA
AIRPORT AUTHORITY

DESIGN BY: KWS
DRAWN BY: JRO
CHECKED BY: KWS
APPROVED BY: ---
DATE: 6/5/2020
JOB No: 19007502.00

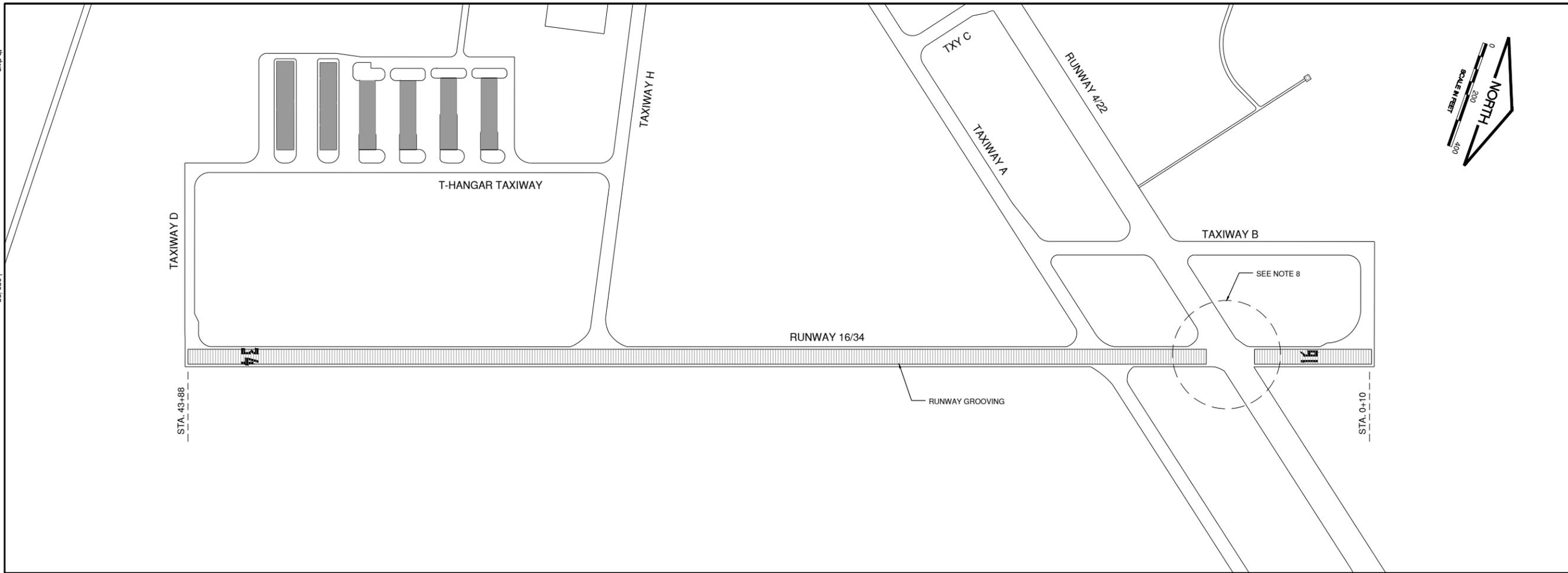
FINAL

6/16/2020 9:47:48 PM

8dfc86c7

UPDATE BY: Kris Sathiera
LAYOUT: Layout

DATE: Friday, June 5, 2020 9:47:48 PM
FILE: K:\Kendall\1907502_Rehab\16-34\Draw\Sheet\grooving_Plan.dwg



IL. CONTRACT: **KA050**
 IL. LETTING ITEM: **06A**
 IL. PROJECT: **IKK-4809**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE RUNWAY 16/34**

GROOVING PLAN

© Copyright CMT, Inc.

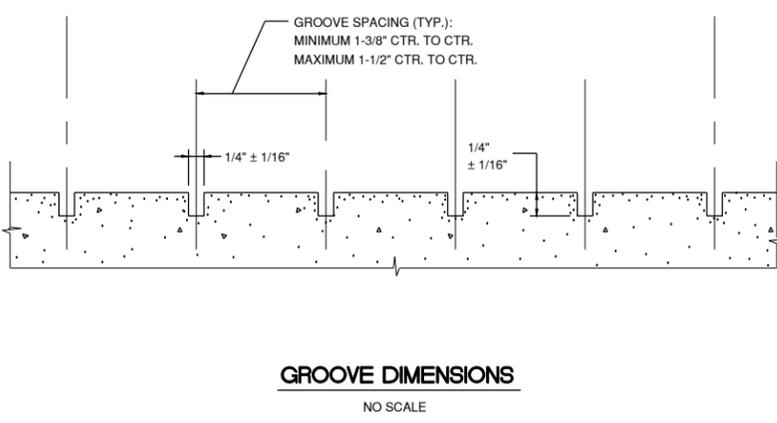
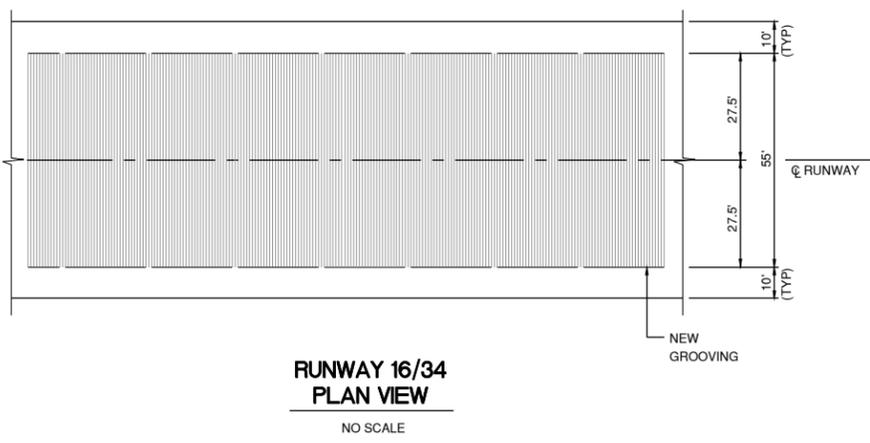
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

KANKAKEE VALLEY
AVAA
 AIRPORT AUTHORITY

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	---
DATE:	6/5/2020
JOB No:	19007502.00

FINAL

SHEET 21 OF 21 SHEETS



GROOVING NOTES

1. AFTER COMPLETION OF HMA PAVING, RUNWAY 16/34 SHALL BE ALLOWED TO CURE A MINIMUM OF 30 DAYS PRIOR TO THE COMMENCEMENT OF GROOVING OPERATIONS.
2. SEE PAVEMENT MARKING PLANS AND NOTES.
3. GROOVING SHALL EXTEND OUTWARD 27.5' FROM CENTERLINE OF RUNWAY 16/34.
4. SUCCESSIVE PASSES OF GROOVING MACHINE SHALL NOT OVERLAP.
5. EXTREME CARE SHALL BE TAKEN WHEN GROOVING NEAR IN-PAVEMENT LIGHT FIXTURES AND SUBSURFACE WIRING. GROOVES SHALL BE SAWED NO CLOSER THAN 6" AND NO MORE THAN 18" TO SUCH FACILITIES.
6. CLEANUP OF WASTE MATERIAL SHALL BE CONTINUOUS AND TIMELY DURING THE GROOVING OPERATION. WASTE MATERIAL SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
7. AT THE CONTRACTOR'S OPTION, SLURRY MAY BE STORED ON SITE AT A LOCATION WITHIN THE CONTRACTOR STAGING AND STORAGE AREA(S). THE CONTRACTOR SHALL BE ALLOWED TO CONSTRUCT A TEMPORARY WASHOUT PIT IN ACCORDANCE WITH STANDARD PRACTICES. A WASHOUT PIT SHALL BE ALLOWED TO BE CONSTRUCTED AT EACH STAGING AREA. THE CONTRACTOR SHALL BE REQUIRED TO REMOVE THE SLURRY FOR PROPER FINAL DISPOSAL OFF AIRPORT PROPERTY WITHIN TWO BUSINESS DAYS TO PREVENT WATERFOWL FROM BEING ATTRACTED TO THE AREA. NO ADDITIONAL COMPENSATION FOR THE EXCAVATION AND RESTORATION OF THE WASHOUT PIT(S) SHALL BE ALLOWED.
8. GROOVING ON RUNWAY 16-34 SHALL STOP 10' FROM EXTENDED EDGE OF RUNWAY 4-22 AND SHALL BE CUT IN A STEP PATTERN. WIDTH OF STEPS SHALL NOT EXCEED 40 INCHES IN WIDTH.