

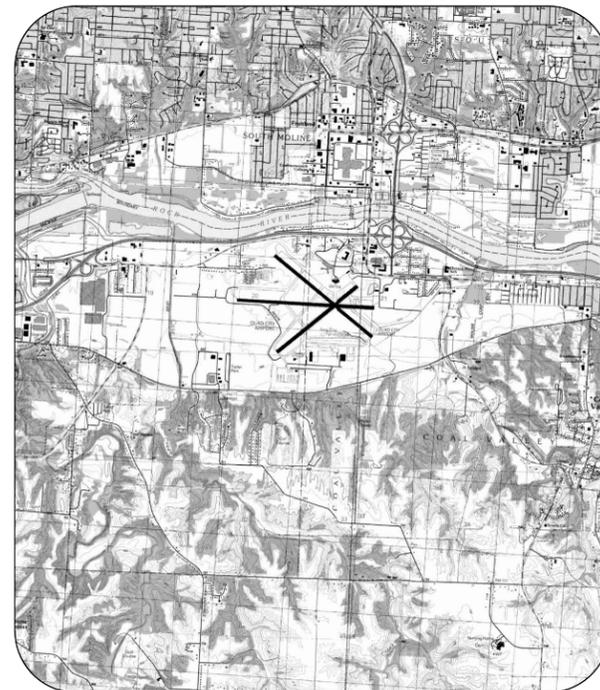
CONSTRUCTION PLANS FOR QUAD CITY INTERNATIONAL AIRPORT

METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
MOLINE, ILLINOIS

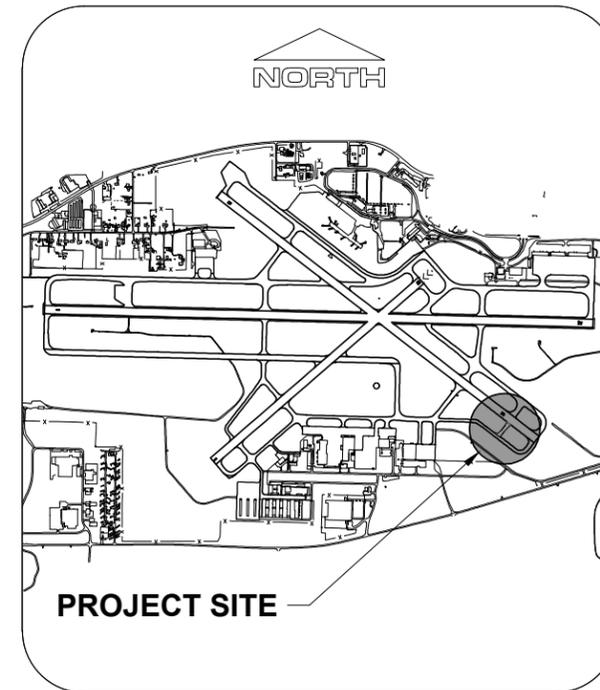
IL. PROJ. NO. MLI-4482
AIP PROJ. NO. 3-17-0068-XX

CONSTRUCT TAXIWAY K CONNECTOR (K12)

JUNE 26, 2015



LOCATION MAP



SITE PLAN

TAXIWAY K12 GEOMETRIC DESIGN CRITERIA

AIRCRAFT DESIGN GROUP III
DESIGN APPROACH CATEGORY C & D
TAXIWAY DESIGN GROUP 3



THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

CALL J.U.L.I.E.
BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: 17 NORTH
RANGE: 1 WEST
SECTION: 20
COUNTY: ROCK ISLAND
CIVIL TOWNSHIP: UNINCORPORATED
ROCK ISLAND COUNTY

MAXIMUM EQUIPMENT HEIGHT - 25'
UNICOM FREQUENCY - 122.95

METROPOLITAN AIRPORT AUTHORITY
OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS

APPROVED: *Bruce Carter*
DATE: *April 13, 2015*



CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS

SUBMITTED BY: *Se M*
SEAN M. SMITH, P.E.
DATE: *17 April 2015*

CMT JOB NUMBER: 14014-07-00

INDEX TO SHEETS

Sheet Number	Sheet Title
01	COVER
02	INDEX TO SHEETS AND SUMMARY OF QUANTITIES
03	SITE PLAN
04	PROJECT CONTROL PLAN
05	SEQUENCE OF CONSTRUCTION
06	SEQUENCE OF CONSTRUCTION GENERAL NOTES
07	SEQUENCE OF CONSTRUCTION DETAILS
08	STORM WATER POLLUTION PREVENTION PLAN
09	STORMWATER POLLUTION PREVENTION PLAN NOTES AND DETAILS SHEET 1
10	STORMWATER POLLUTION PREVENTION PLAN NOTES AND DETAILS SHEET 2
11	TYPICAL SECTIONS
12	EXISTING CONDITIONS AND PROPOSED REMOVALS
13	PLAN AND PROFILE
14	GRADING AND DRAINAGE PLAN
15	DRAINAGE DETAILS
16	PAVEMENT JOINTING PLAN
17	PAVEMENT JOINT ELEVATION PLAN
18	PAVEMENT JOINTING DETAILS
19	ELECTRICAL AND PAVEMENT MARKING PLAN
20	ELECTRICAL DETAILS SHEET 1
21	ELECTRICAL DETAILS SHEET 2
22	ELECTRICAL DETAILS SHEET 3
23	PAVEMENT MARKING DETAILS
24	INDEX TO CROSS SECTIONS AND EARTHWORK SUMMARY
25	CROSS SECTIONS SHEET 1
26	CROSS SECTIONS SHEET 2
27	SOIL BORING LOCATIONS

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR108158	1/C #8 5KV UG CABLE IN UD	LF	3,500	
AR109210	VAULT MODIFICATIONS	LS	1	
AR110312	2" STEEL DUCT, JACKED	LF	175	
AR110504	4-WAY CONCRETE ENCASED DUCT	LF	75	
AR125415	MITL-BASE MOUNTED	EACH	24	
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	2	
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EACH	4	
AR125470	MODIFY EXISTING SIGN PANEL	EACH	1	
AR125565	SPLICE CAN	EACH	1	
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	8	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	2	
AR125915	RELOCATE RGL	PAIR	1	
AR150520	MOBILIZATION	LS	1	
AR152410	UNCLASSIFIED EXCAVATION	CY	3,093	
AR1524540	SOIL STABILIZATION FABRIC	SY	2,750	
AR156520	INLET PROTECTION	EACH	4	
AR208515	POROUS GRANULAR EMBANKMENT	CY	150	
AR209610	CRUSHED AGG. BASE COURSE - 10"	SY	2,750	
AR501509	9" PCC PAVEMENT	SY	2,575	
AR501900	REMOVE PCC PAVEMENT	SY	37	
AR605510	JOINT SEALING FILLER	LF	3,920	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	7,195	
AR620900	PAVEMENT MARKING REMOVAL	SF	3,560	
AR701006	6" PVC STORM SEWER	LF	75	
AR701512	12" RCP, CLASS IV	LF	415	
AR705506	6" PERFORATED UNDERDRAIN	LF	815	
AR705635	UNDERDRAIN COLLECTION STRUCTURE	EACH	2	
AR705900	REMOVE UNDERDRAIN	LF	380	
AR751540	MANHOLE 4'	EACH	2	
AR800022	2-1/C #4 XLP-USE, 600V AND 1/C #8 GND IN 1-1/4" UD	LF	1,000	
AR800038	REMOVE BASE MOUNTED LIGHT (FIXTURE ONLY)	EACH	25	
AR800116	PAVEMENT MARKING - PREFORMED THERMOPLASTIC	SF	2,660	
AR901510	SEEDING	ACRE	1.00	
AR904510	SODDING	SY	535	
AR908510	MULCHING	ACRE	1.00	

QU020

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS

CONSTRUCT TAXIWAY K CONNECTOR (K12)
 INDEX TO SHEETS AND SUMMARY OF
 QUANTITIES

© Copyright CMT, Inc.



CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

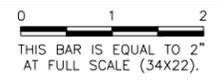


DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00

IL PROJ. NO. MLI-4482
 AIP PROJ. NO. 3-17-0068-XX

QU020

REVISIONS		
NUMBER	BY	DATE



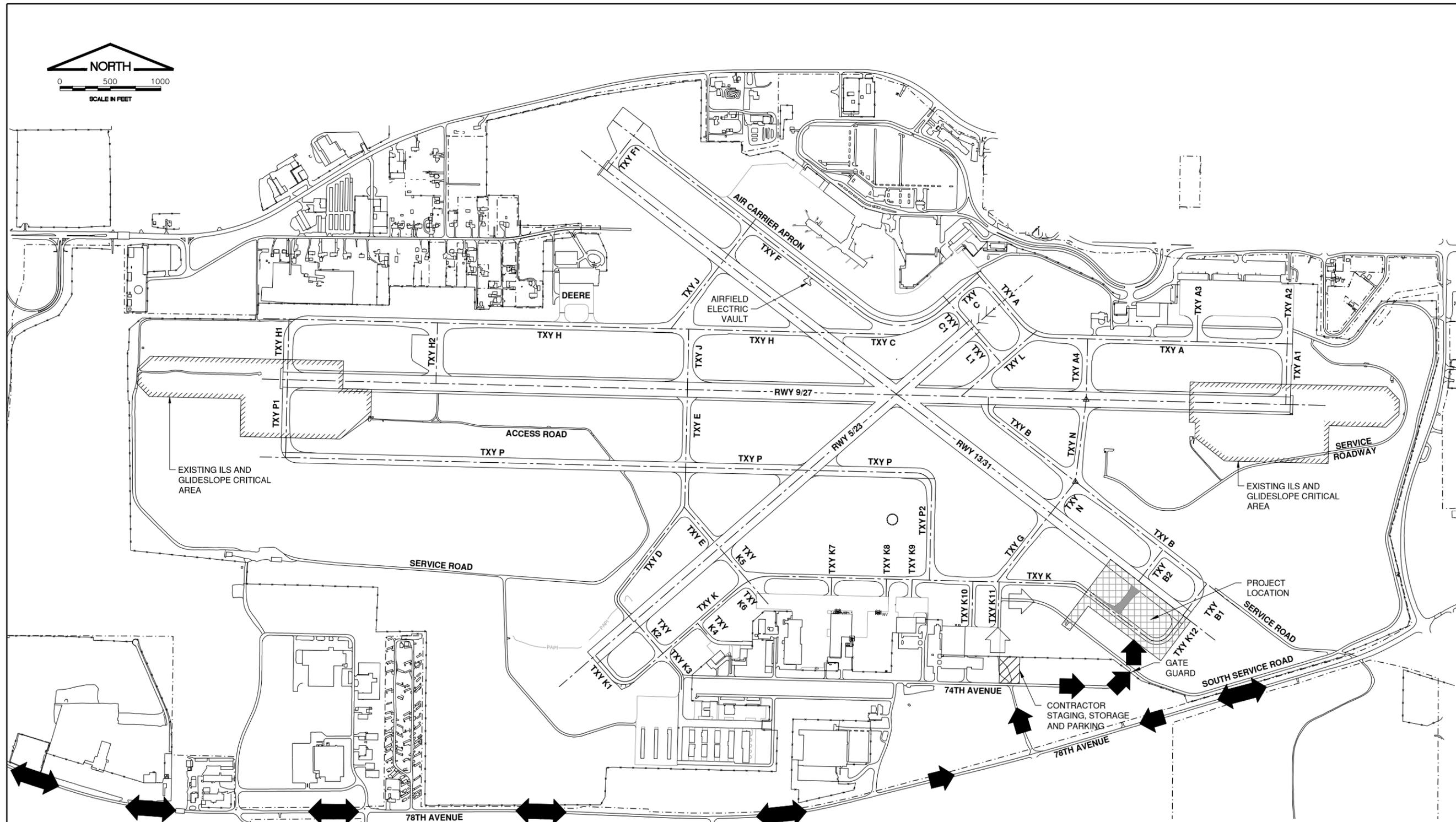
**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 SITE PLAN**

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL PROJ. NO. MLI-4482 AIP PROJ. NO. 3-17-0068-XX	
SHEET 03 OF 27 SHEETS	



LEGEND

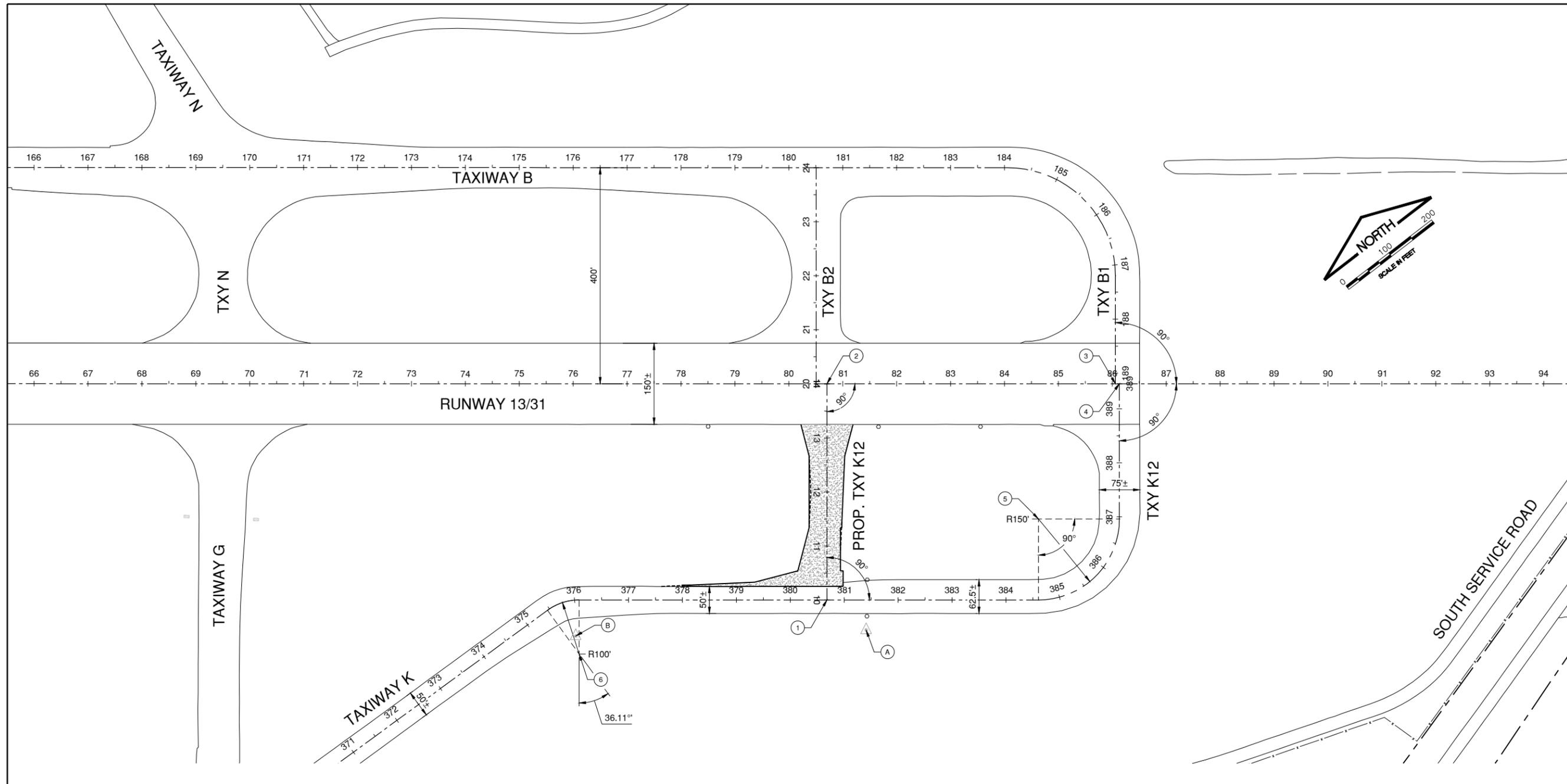
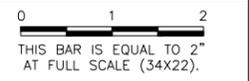
	CONSTRUCTION WORK AREA
	NEW TAXIWAY K12
	CONTRACTOR STAGING, STORAGE AND PARKING
	PREFERRED CONTRACTOR ACCESS ROUTE
	ALTERNATE CONTRACTOR ACCESS ROUTE
	EXISTING FENCELINE
	EXISTING AIRPORT PROPERTY LINE

- NOTES**
1. CONTRACTOR STAGING, STORAGE AND PARKING AREA SHALL BE LOCATED OUTSIDE OF ALL RUNWAY SAFETY AREAS AND OBJECT FREE AREAS.
 2. CONTRACTOR SHALL BE REQUIRED TO SECURE HIS ACCESS GATES DURING WORKING AND NON-WORKING HOURS.
 3. DAMAGE TO AIRPORT SERVICE ROADS AND AIRFIELD PAVEMENTS SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE TO THE SATISFACTION OF THE AIRPORT AND THE ENGINEER.
 4. CONTRACTOR TO NOTIFY COUNTY, TOWNSHIP AND CITY TRANSPORTATION DEPARTMENTS AS REQUIRED FOR OFF AIRFIELD ACCESS ROUTES.
 5. TAXIWAY NAMES SHOWN IN THESE DOCUMENTS REPRESENT FUTURE TAXIWAY NAMING PER THE AIRPORT'S UPDATED SIGNAGE PLAN. SIGNAGE UPDATES ARE SCHEDULED TO BE COMPLETED BY FALL 2015.

K:\Moineap\14014-07_TaxiwayConnector\Drawn\Sheets

QU020

REVISIONS		
NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 PROJECT CONTROL PLAN**

HORIZONTAL CONTROL POINTS				
POINT	€ STATION 1	€ STATION 2	NORTHING	EASTING
1	TAXIWAY K12 STA. 10+00.00	TAXIWAY K STA. 380+68.24	1,739,702.71	2,206,062.68
2	TAXIWAY K12 STA. 14+00.26	RUNWAY 13/31 STA. 80+70.56	1,740,019.85	2,206,306.87
3	RUNWAY 13/31 STA. 86+05.50	TAXIWAY B STA. 189+19.66	1,739,693.40	2,206,730.66
4	RUNWAY 13/31 STA. 86+13.00	TAXIWAY K STA. 389+46.45	1,739,688.82	2,206,736.60
5	TAXIWAY K STA. 384+60.60	TAXIWAY K STA. 386+96.21	1,739,582.12	2,206,465.06
6	TAXIWAY K STA. 375+45.57	TAXIWAY K STA. 376+08.60	1,739,903.96	2,205,637.52

VERTICAL CONTROL POINTS					
POINT	€ STATION 1	€ STATION 2	NORTHING	EASTING	ELEVATION
A - IRON PIN	TAXIWAY K STA. 381+42.98, 55.77' RT.	TAXIWAY K12 STA. 9+44.23, 72.57' RT.	1,739,614.24	2,206,086.15	574.85
B - IRON PIN	TAXIWAY K STA. 375+89.79, 66.05' RT.	TAXIWAY K12 STA. 9+33.35, 465.99' LT.	1,739,934.26	2,205,652.84	572.29

LEGEND	
	CONTROL POINT - IRON PIN
	NEW PCC PAVEMENT

- NOTES**
- THE CONTRACTOR SHALL VERIFY THE HORIZONTAL AND VERTICAL CONTROL POINTS PRIOR TO BEGINNING CONSTRUCTION.
 - DISCREPANCIES IN THE CONTROL POINTS SHALL BE BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER AS SOON AS THEY ARE DISCOVERED. THE CONTRACTOR SHALL NOT PROCEED WITH ANY WORK THAT COULD BE IMPACTED BY CONTROL POINT DISCREPANCIES UNTIL RESOLUTION IS COMPLETE.

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL. PROJ. NO. MLI-4482 AIP PROJ. NO. 3-17-0068-XX	
SHEET 04 OF 27 SHEETS	

K:\Moline\14014-07_TaxiwayConnector\Draw\Sheets

CRITICAL POINTS

POINT NO.	LATITUDE	LONGITUDE	ELEV.*
A	41° 26' 27.60"	90° 30' 01.16"	604.00
B	41° 26' 30.20"	90° 30' 01.08"	604.00
C	41° 26' 37.51"	90° 30' 01.22"	601.00
D	41° 26' 43.35"	90° 29' 55.49"	600.00
E	41° 26' 46.47"	90° 29' 52.28"	594.00
F	41° 26' 35.60"	90° 29' 47.42"	601.20
G	41° 26' 32.35"	90° 29' 41.77"	603.30

* ELEVATION INCLUDES 25' ANTICIPATED MAXIMUM EQUIPMENT HEIGHT

PHASING NOTES (ALL PHASES)

- ANY TIME THE HAUL ROUTE USES OR CROSSES EXISTING AIRPORT PAVEMENT, THE PAVEMENT SHALL BE SWEEPED AND CLEANED AS REQUIRED TO THE SATISFACTION OF THE AIRPORT AND/OR RESIDENT ENGINEER.
- DAMAGE TO AIRPORT SERVICE ROADS, CONTRACTOR STAGING AREA AND AIRFIELD PAVEMENTS SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- ALL CLOSED TAXIWAY SECTIONS SHALL HAVE THE TAXIWAY EDGE LIGHTS AND TAXIWAY GUIDANCE SIGNS REMOVED FROM SERVICE. IF DEACTIVATING THE CIRCUIT IMPACTS AN OPEN TAXIWAY SECTION, THE EDGE LIGHTS AND GUIDANCE SIGNS MAY BE COVERED IN A MANNER THAT PREVENTS VISIBLE LIGHT. ADDITIONALLY, A TEMPORARY JUMPER MAY BE INSTALLED TO REMOVE THE LIGHTS FROM THE ACTIVE CIRCUIT. METHOD OF DEACTIVATION SHALL BE APPROVED BY THE RESIDENT ENGINEER. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- CONTRACTOR SHALL INSTALL TAXIWAY CLOSURE MARKERS AND REMOVE TAXIWAY LEAD-IN LINES AS SHOWN OR AS DIRECTED BY THE RESIDENT ENGINEER. TO MINIMIZE IMPACT TO THE AIRPORT THIS WORK SHALL BE EXPEDITED AND COORDINATED TO BE COMPLETED WITHIN THE SAME CALENDAR DAY. SEE SEQUENCE OF CONSTRUCTION DETAILS SHEET FOR TAXIWAY CLOSURE MARKER DETAILS AND NOTES.
- CONTRACTOR SHALL INSTALL AND MAINTAIN RUNWAY CLOSURE MARKERS AT BOTH ENDS OF THE RUNWAY WHEN THE RUNWAY IS CLOSED. SEE SEQUENCE OF CONSTRUCTION DETAILS SHEET FOR RUNWAY CLOSURE MARKER DETAILS AND NOTES.

- CONSTRUCTION ACTIVITIES WILL REQUIRE THE CLOSURE OF VARIOUS AIRFIELD PAVEMENTS THROUGHOUT THE DURATION OF PROJECT. ALL PAVEMENT CLOSURES SHALL BE COORDINATED WITH THE AIRPORT AND/OR RESIDENT ENGINEER A MINIMUM OF 72 HOURS PRIOR TO CLOSURE. SEE PHASING SUMMARY TABLE LOCATED ON SEQUENCE OF CONSTRUCTION DETAILS SHEET FOR DETAILED PAVEMENT CLOSURE INFORMATION.
- THE LOCATION OF THE BARRICADES SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. SHOULD THE PHASING REQUIRE THE BARRICADES TO BE REPOSITIONED THE WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

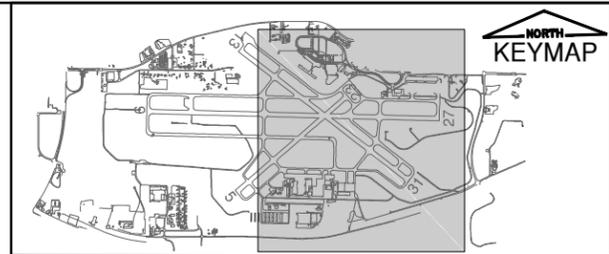
- ADJUST SHOULDERS, TOPSOIL, SEED AND MULCH
- PLACE PAVEMENT MARKINGS, INCLUDING TAXIWAY CLOSURE MARKERS.
- CLEAN PAVEMENTS, REMOVE EROSION CONTROL MEASURES WITHIN THE RUNWAY SAFETY AREA, REMOVE RUNWAY CLOSURE MARKERS, REMOVE BARRICADES WITHIN SAFETY AREA OF RUNWAY AND REOPEN RUNWAY 13/31.

PHASE B: SUGGESTED SEQUENCE OF CONSTRUCTION

- MARK RUNWAY SAFETY AREA (R.S.A) WITH LATH AND RIBBON AND PLACE BARRICADES AS SHOWN OR DIRECTED BY THE ENGINEER.
- EXCAVATE TO SUBGRADE AND PLACE PGE AND AGGREGATE BASE COURSE.
- CONSTRUCT PCC PAVEMENT AND DRAINAGE IMPROVEMENTS.
- INSTALL NEW ELECTRICAL.
- ADJUST SHOULDERS, TOPSOIL, SEED AND MULCH.
- PLACE PAVEMENT MARKINGS
- CLEAN PAVEMENTS, REMOVE EROSION CONTROL MEASURES, REMOVE TAXIWAY CLOSURE MARKERS, REMOVE BARRICADES, REMOVE HAUL ROAD AND REOPEN ALL TAXIWAYS.

PHASE A: SUGGESTED SEQUENCE OF CONSTRUCTION

- PLACE RUNWAY CLOSED MARKERS ON BOTH ENDS OF RUNWAY 13/31 AND PLACE BARRICADES AS SHOWN OR AS DIRECTED BY THE ENGINEER.
- REMOVE TAXIWAY LEAD-IN MARKINGS.
- INSTALL EROSION CONTROL MEASURES AS REQUIRED.
- EXCAVATE TO SUBGRADE AND PLACE PGE AND AGGREGATE BASE COURSE.
- CONSTRUCT PCC PAVEMENT AND DRAINAGE IMPROVEMENTS.
- INSTALL NEW ELECTRICAL.



FILE: 05 SEQUENCE 1.dwg
 UPDATE BY: Sean Smith
 PLOT DATE: 6/26/2015 3:48 PM

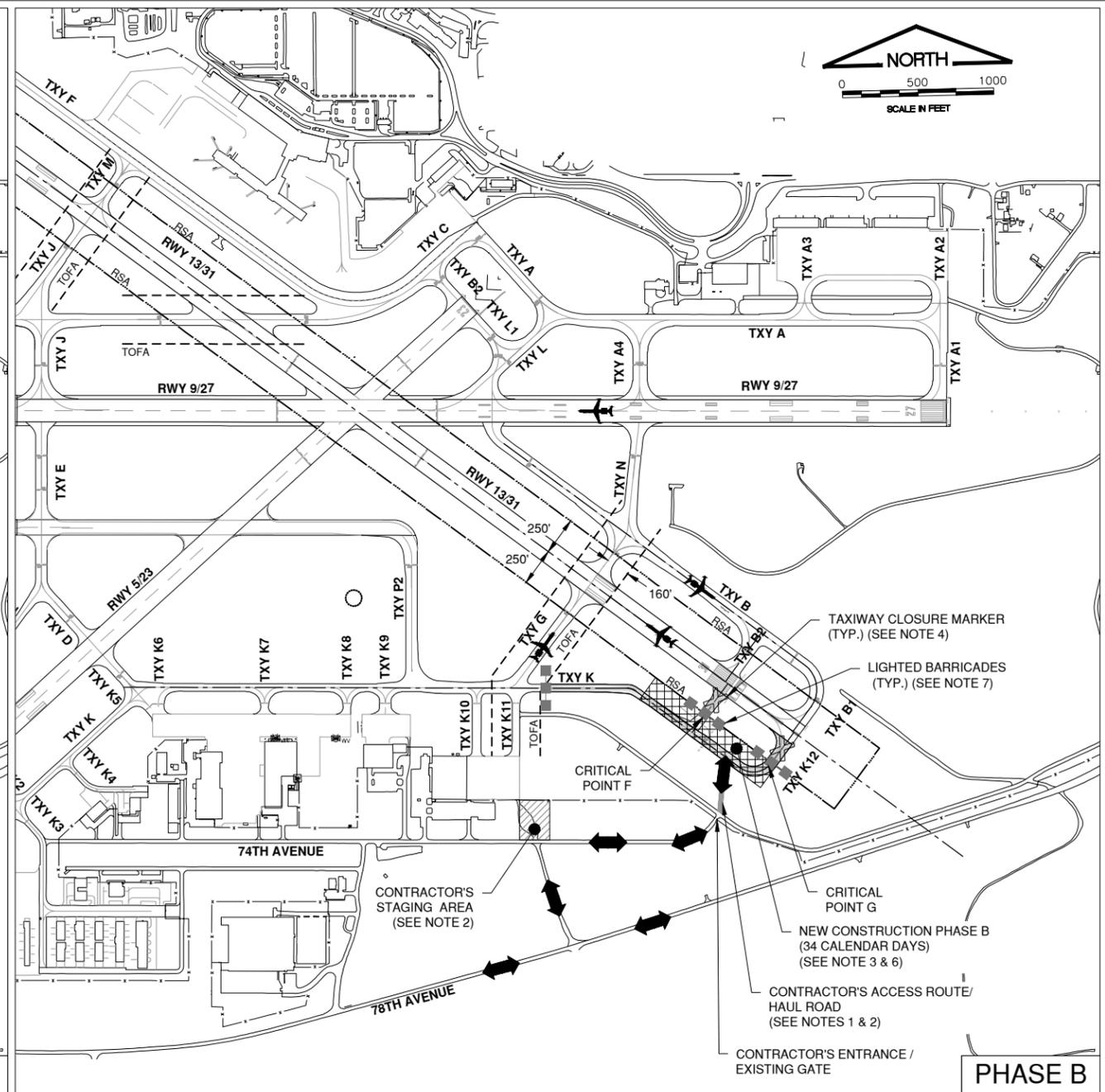
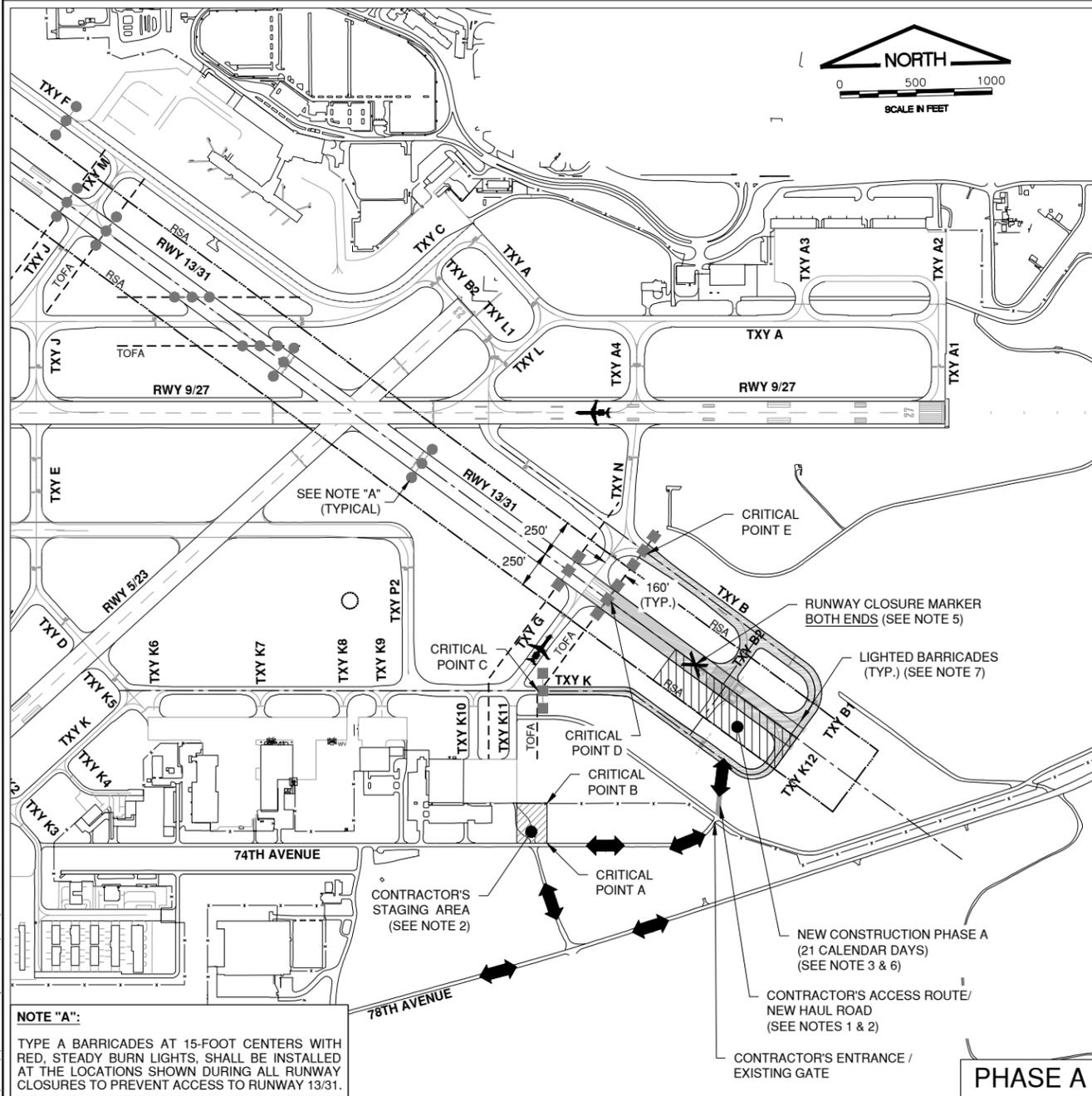
Taxiway K Base
 MLI Alignments

QU020

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



NOTE "A":
 TYPE A BARRICADES AT 15-FOOT CENTERS WITH RED, STEADY BURN LIGHTS, SHALL BE INSTALLED AT THE LOCATIONS SHOWN DURING ALL RUNWAY CLOSURES TO PREVENT ACCESS TO RUNWAY 13/31.

METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS

CONSTRUCT TAXIWAY K CONNECTOR (K12)
 SEQUENCE OF CONSTRUCTION

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL PROJ. NO.	MLI-4482
AIP PROJ. NO.	3-17-0068-XX
SHEET	05 OF 27 SHEETS

GENERAL

1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING SAFETY REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL SUBMIT THE SPCD FOR APPROVAL.
6. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
7. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE RESIDENT ENGINEER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT.
8. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
9. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ONSITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE AIRPORT. PAYMENT FOR DUST CONTROL SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
11. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.
4. THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT DIRECTOR OF AVIATION OR HIS/HER DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD CIRCUITS IMPACTED BY CONSTRUCTION ACTIVITY.
5. CONTRACTOR SHALL PLAN HIS/HER WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORT AND SPECIAL PROVISIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT:

- INSTALL RWY 9 PAPI AND AIRFIELD SIGNAGE UPGRADES
- REHABILITATE TAXIWAY H2

2. PHASING

1. TOTAL CONTRACT TIME SHALL BE 55 CALENDAR DAYS.
2. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.
4. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE EXISTING GATE SHOWN. THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLES ACCESS AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO CITY OF MOLINE AND IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.
3. CERTAIN CONTRACTOR EMPLOYEES SHALL OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
4. ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATION AREA (AOA) SHALL ALSO ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT WITHOUT AN ESCORT. ALL COST ASSOCIATED WITH THE DRIVERS TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
5. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE.
6. THE CONTRACTOR'S STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN AND CONSTRUCTION PHASING PLAN.
7. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
8. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
9. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
10. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
11. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS, STORAGE AREAS AND/OR STAGING AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
12. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
13. THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
5. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

AIRCRAFT OPERATOR
BRUCE CARTER - DIRECTOR OF AVIATION (309) 757-1732
BRYAN JOHNSON - ASSISTANT DIRECTOR OF AVIATION (309) 757-1754

ENGINEER
CMT - RESIDENT ENGINEER (217) 787-8050

10. INSPECTION REQUIREMENTS

1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. SEE SECTION 70-17 OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.
3. SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATE.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

12. PENALTIES

1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.
2. THE GATE SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE AIRPORT DIRECTOR OF AVIATION. SHOULD THE CONTRACTOR'S OPERATIONS REQUIRE THE GATE TO REMAIN OPEN TO PROVIDE ACCESS TO HAULING OPERATIONS, A COMPETENT GATE GUARD SHALL BE REQUIRED TO CONTROL ACCESS TO THE AIRFIELD. A \$1,000 FINE SHALL BE ASSESSED FOR ANY OCCURRENCE OF AN UNSECURE GATE THAT IS THE CONTRACTOR'S RESPONSIBILITY. AN UNSECURED GATE SHALL BE DEFINED AS ANY GATE THAT IS NOT WITHIN THE SIGHT AND PHYSICAL CONTROL OF THE CONTRACTOR'S GUARD. IN THE EVENT THAT THE GATE MAY NOT BE SECURED, THE CONTRACTOR WILL BE CHARGED FOR AIRPORT PERSONNEL TO REMAIN AT THE GATE UNTIL SECURED.

12. PENALTIES (CONT.)

3. THE CONTRACTOR SHALL RESTRICT ALL CONSTRUCTION ACTIVITIES TO THE CONSTRUCTION AREA DETAILED IN THE CONSTRUCTION SAFETY AND PHASING PLAN. ANY UNAUTHORIZED MOVEMENTS, PEDESTRIAN OR VEHICULAR, BEYOND THE CONSTRUCTION LIMITS SHOWN SHALL BE CONSIDERED AN AIRFIELD INCURSION. AIRFIELD INCURSIONS, AT THE DISCRETION OF THE AIRPORT DIRECTOR OF AVIATION, MAY BE FINED \$10,000.00 PER INCIDENT. INCURSION FINES WILL BE ASSESSED IMMEDIATELY AND TAKEN FROM MONIES DUE THE CONTRACTOR ON THE NEXT CONSTRUCTION PAYMENT.

13. SPECIAL CONDITIONS

1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

1. RUNWAY OR TAXIWAY CLOSURES ARE AS DETAILED IN THE CONSTRUCTION SAFETY AND PHASING PLAN FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2F.

15. HAZARD MARKING AND LIGHTING

1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL PLACE ALL BARRICADES AND CONSTRUCTION SETBACK LINES ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, REPOSITION, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
5. ACCESS TO ACTIVE RUNWAY AND TAXIWAY PAVEMENTS (TOWER CONTROLLED AREAS) SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON TYPE II BARRICADES (2 EACH, RIGHT AND LEFT). IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "TOWER CONTROL AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE".

16. PROTECTION

1. CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS AND THE RUNWAY SAFETY AREA (RSA) OF ACTIVE RUNWAYS.

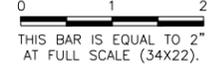
17. OTHER LIMITATIONS ON CONSTRUCTION

1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. THE CONTRACTOR MAY BE REQUIRED TO SUBMIT A REVISED PROGRESS SCHEDULE TO ACCOMMODATE AIRPORT EVENTS (I.E. AIRSHOW). SHOULD A REVISED SCHEDULE BE REQUIRED, THE REVISION SHALL BE COMPLETED AT NO ADDITIONAL COST TO THE CONTRACT.
3. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS.
4. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.
5. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.
6. BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

QU020

REVISIONS

NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

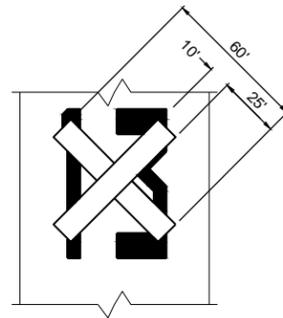
**CONSTRUCT TAXIWAY K CONNECTOR (K12)
SEQUENCE OF CONSTRUCTION GENERAL
NOTES**

© Copyright CMT, Inc.
CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL PROJ. NO.	MLI-4482
AIP PROJ. NO.	3-17-0068-XX
SHEET	06 OF 27 SHEETS

LIGHTED RUNWAY CLOSURE MARKERS NOTES

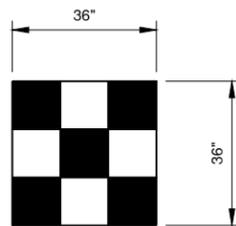
1. THE AIRPORT SHALL FURNISH TWO PORTABLE, LIGHTED RUNWAY CLOSURE MARKERS FOR THE DURATION OF THE PROJECT. IF LIGHTED RUNWAY CLOSURE MARKERS ARE NOT AVAILABLE, THE CONTRACTOR SHALL REFER TO THE "CLOSED RUNWAY MARKER DETAIL" ON THIS SHEET.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRANSPORTING, INSTALLING, MAINTAINING, REFUELING, REPOSITIONING AND REMOVING THE LIGHTED RUNWAY CLOSURE MARKERS AS SHOWN IN THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
3. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS. WHEN CONSTRUCTION OPERATIONS CONFLICT, THE CLOSURE MARKERS SHALL BE MOVED TO AN ALTERNATE LOCATION AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER.
4. IT WILL BE NECESSARY TO CLOSE RUNWAY 13/31 TO AIR TRAFFIC FOR THE DURATION OF PHASE A CONSTRUCTION. THE CONTRACTOR SHALL MARK RUNWAY 13/31 CLOSED BY PLACING LIGHTED RUNWAY CLOSURE MARKER AT THE LOCATION DETAILED IN THE PLANS OR AS DIRECTED BY THE ENGINEER AND AIRPORT. THE LIGHTED RUNWAY CLOSURE MARKERS ARE REQUIRED TO BE IN OPERATION ANYTIME THE RUNWAY IS CLOSED.
5. UPON COMPLETION OF THE PROJECT, THE MARKERS SHALL BE RETURNED TO THE AIRPORT IN GOOD CONDITION.
6. ALL COST ASSOCIATED WITH THE LIGHTED RUNWAY CLOSURE MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



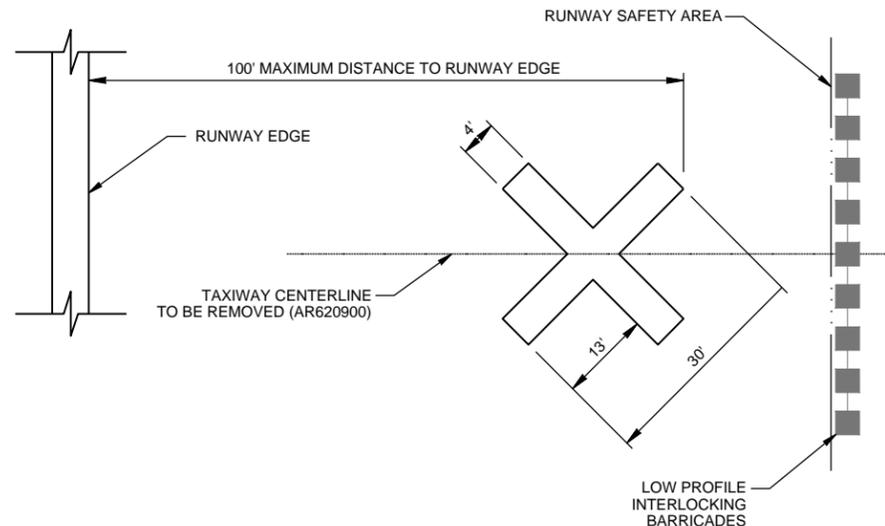
CLOSED RUNWAY MARKER DETAIL
 ON PAVEMENT - NO SCALE

RUNWAY CLOSURE MARKERS NOTES

1. THE CONTRACTOR SHALL USE THE LIGHTED RUNWAY CLOSURE MARKERS PROVIDED BY THE AIRPORT. IF THE LIGHTED RUNWAY CLOSURE MARKERS ARE NOT AVAILABLE, THE CONTRACTOR WILL BE REQUIRED TO INSTALL RUNWAY CLOSURE MARKERS AS DETAILED ABOVE.
2. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
3. MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER AND THE AIRPORT.
4. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
5. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
6. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
7. IT WILL BE NECESSARY TO CLOSE RUNWAY 13/31 TO AIR TRAFFIC FOR THE DURATION OF PHASE A CONSTRUCTION. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET OR AS DIRECTED BY THE ENGINEER AND AIRPORT. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.



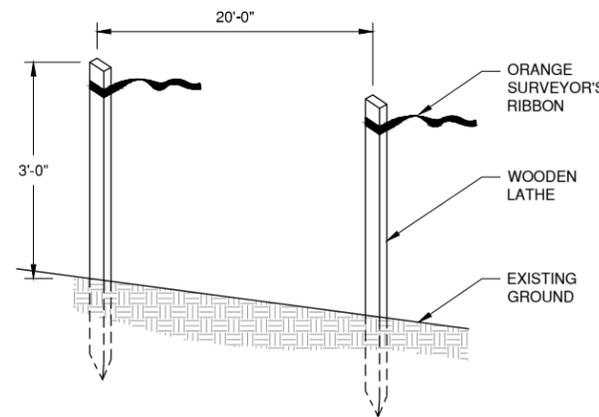
CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG



TYPICAL TAXIWAY CLOSURE MARKER DETAIL
 NOT TO SCALE

TAXIWAY CLOSURE MARKER NOTES

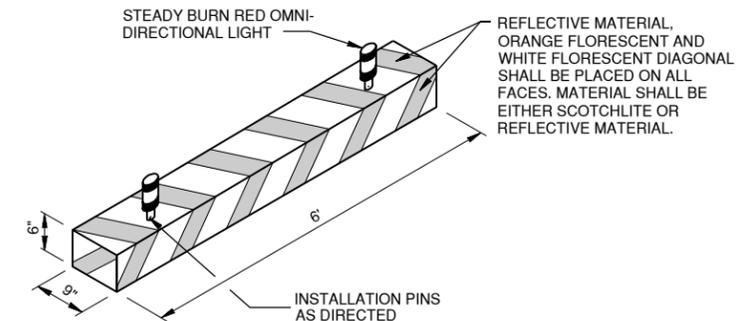
1. THE TAXIWAY CLOSURE MARKER CAN BE PAINTED WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING, OR CAN BE ANOTHER MATERIAL THAT DOES NOT VIOLATE THE OFA CRITERIA AND IS APPROVED BY THE ENGINEER AND THE AIRPORT.
2. THE TAXIWAY CLOSURE MARKER SHALL BE YELLOW AND ADEQUATELY SECURED TO WITHSTAND JET BLAST OF 100 MPH.
3. THE MARKER SHALL BE PLACED OVER THE TAXIWAY CENTERLINE.
4. THE TAXIWAY LEAD-IN LINES AND CENTERLINE WITHIN THE RUNWAY SAFETY AREA (R.S.A.) SHALL BE REMOVED. THE REMOVAL OF THESE MARKINGS WILL BE PAID FOR PER PAY ITEM "AR620900 PAVEMENT MARKING REMOVAL."
5. THE INSTALLATION AND REMOVAL OF THE TAXIWAY CLOSURE MARKERS SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.



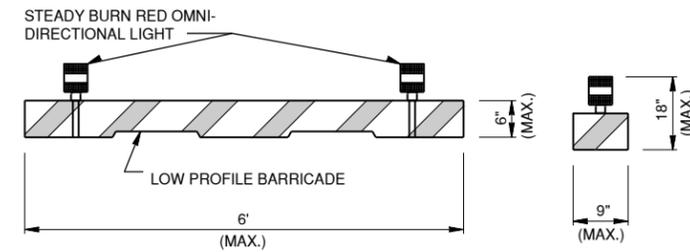
CONSTRUCTION SETBACK LINE DETAIL
 NOT TO SCALE

CONSTRUCTION SETBACK NOTES

1. CONTRACTOR SHALL MARKER THE RUNWAY SAFETY AREA PER THE CONSTRUCTION SETBACK DETAIL AS DIRECTED BY THE RESIDENT ENGINEER.
2. ALL COST ASSOCIATED WITH THE CONSTRUCTION SETBACK LINE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



ISOMETRIC



FRONT ELEVATION

SIDE ELEVATION

INTERLOCKING LOW PROFILE BARRICADE DETAILS
 NOT TO SCALE

INTERLOCKING LOW PROFILE BARRICADE NOTES

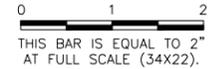
1. LOW PROFILE BARRICADES SHALL BE PLACED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THE BARRICADES SHALL BE INTERLOCKED WITH NO GAPS BETWEEN BARRICADES.
2. BARRICADES SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
2. THE BARRICADE LINE SHALL EXTEND ONE BARRICADE PAST THE EDGE OF PAVEMENT INTO THE TURF.
4. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR AS LOW POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND.
6. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

PHASING SUMMARY

PHASE / CALENDAR DAYS	WORK AREA	AIRPORT OPERATIONAL RESTRICTIONS
PHASE A / 21 CALENDAR DAYS	WORK INSIDE RUNWAY 13/31 SAFETY AREA AND TAXIWAY K SAFETY AREA.	<ul style="list-style-type: none"> • RUNWAY 13/31 CLOSED. • TAXIWAY B SOUTH OF TAXIWAY N CLOSED. • TAXIWAY CONNECTOR B1 CLOSED. • TAXIWAY CONNECTOR B2 CLOSED. • TAXIWAY K EAST OF TAXIWAY CONNECTOR K11 CLOSED. • TAXIWAY CONNECTOR K12 CLOSED.
PHASE B / 34 CALENDAR DAYS	WORK OUTSIDE RUNWAY 13/31 SAFETY AREA AND WORK WITHIN TAXIWAY K SAFETY AREA.	<ul style="list-style-type: none"> • TAXIWAY K EAST OF TAXIWAY CONNECTOR K11 CLOSED. • TAXIWAY CONNECTOR K12 CLOSED.
55 CALENDAR DAYS	AT THE CONTRACTOR'S OPTION, PHASE B SHALL BE ALLOWED TO BE CONCURRENT WITH PHASE A, PROVIDED RUNWAY 13/31 IS CLOSED FOR NO MORE THAN 21 CALENDAR DAYS.	

QU020

REVISIONS		
NUMBER	BY	DATE



METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS

CONSTRUCT TAXIWAY K CONNECTOR (K12)
SEQUENCE OF CONSTRUCTION DETAILS

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	TAS
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL PROJ. NO. MLI-4482	
AIP PROJ. NO. 3-17-0068-XX	
SHEET 07 OF 27 SHEETS	

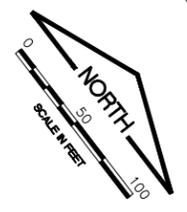
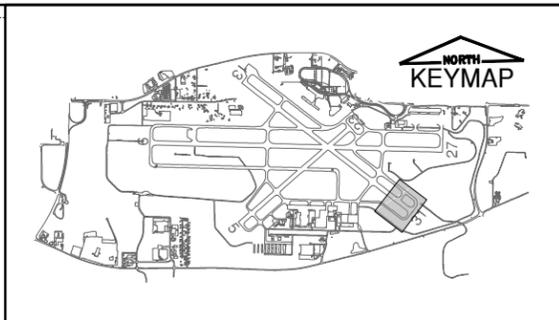
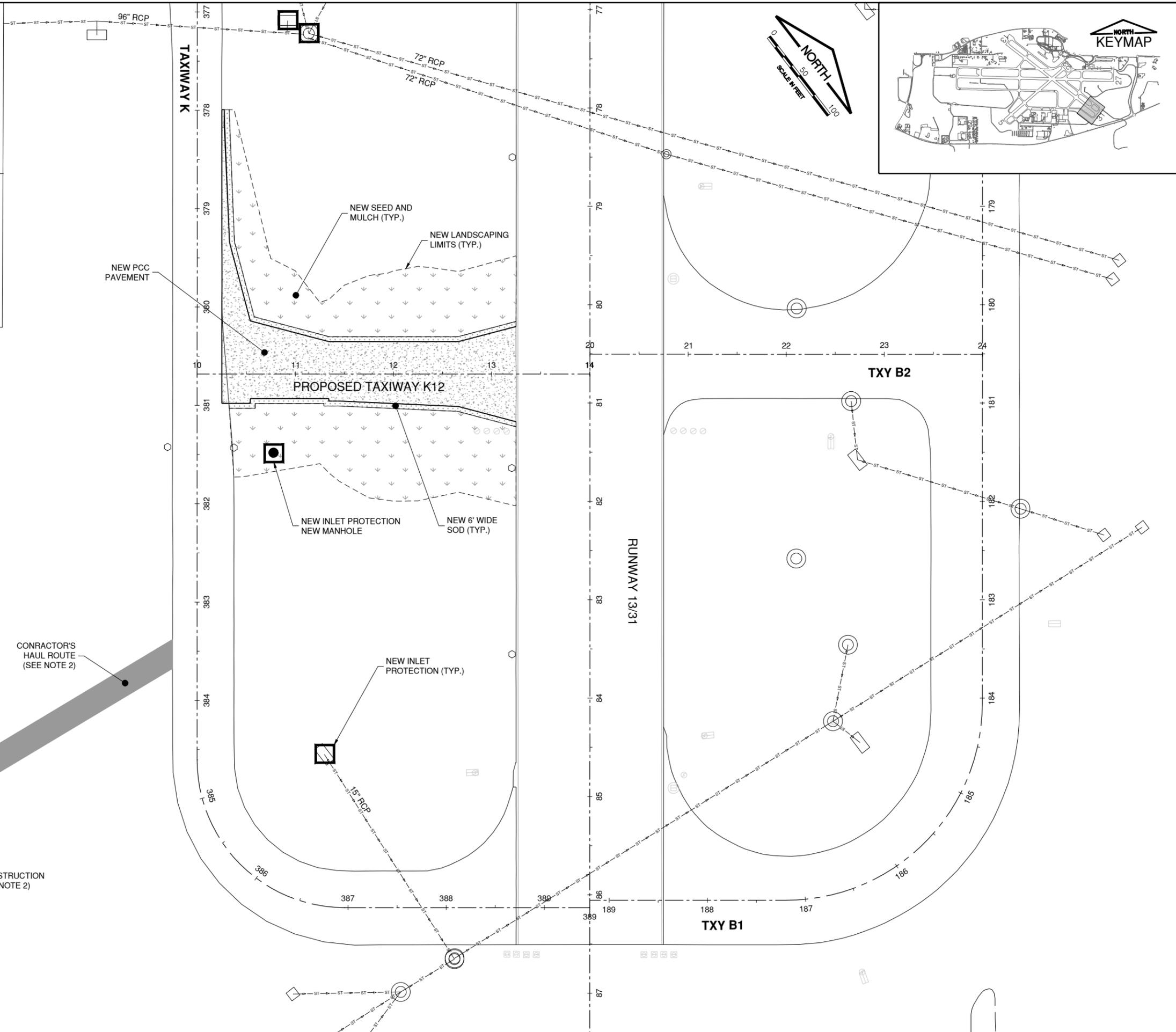
K:\Moineap\14014-07_TaxiwayConnector\Draw\Sheets

LEGEND

- NEW PCC PAVEMENT
- NEW SEED AND MULCH
- NEW SODDING
- NEW GRADING AND LANDSCAPING LIMITS
- INLET PROTECTION
- NEW MANHOLE
- EXISTING MANHOLE / INLET
- EXISTING STORM SEWER

NOTES

- TEMPORARY CONCRETE WASHOUT SHALL BE INSTALLED AT THE CONTRACTOR'S STAGING AREA OR AS DIRECTED BY THE ENGINEER. ALL COST ASSOCIATED WITH THE CONCRETE WASHOUT SHALL BE INCIDENTAL TO THE CONTRACT.
- ALL COST ASSOCIATED WITH THE CONTRACTOR'S HAUL ROUTE AND STABILIZED CONSTRUCTION ENTRANCE SHALL BE INCIDENTAL TO THE CONTRACT.

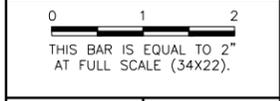


FILE: 08 SWPPP.dwg
 UPDATE BY: Sean Smith
 PLOT DATE: 6/26/2015 3:48 PM

Taxiway K Base
 PROP--GEOMETRY
 MLI Alignments

QU020

REVISIONS		
NUMBER	BY	DATE



METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS

CONSTRUCT TAXIWAY K CONNECTOR (K12)
STORM WATER POLLUTION PREVENTION PLAN

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL. PROJ. NO. MLI-4482 AIP PROJ. NO. 3-17-0068-XX	
SHEET 08 OF 27 SHEETS	

K:\Maineap\14014-07_TxyKConnector\Draw\Sheets

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTION OF TAXIWAY K CONNECTOR AT THE QUAD CITY INTERNATIONAL AIRPORT. THE PROJECT INCLUDES GRADING, EXCAVATION, FILL, TOPSOIL PLACEMENT, PAVEMENT CONSTRUCTION, ELECTRICAL, LANDSCAPING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

1. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS PERIMETER SILT FENCE AND INLET PROTECTION.
2. EXCAVATION AND EMBANKMENT WILL BE COMPLETED WITHIN THE PROJECT LIMITS.
3. PAVEMENT CONSTRUCTION.
4. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
5. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCHING.
6. REMOVAL OF TEMPORARY EROSION CONTROL / PROTECTION FACILITIES.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 1.56 ACRES OF WHICH 1.56 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE ROCK RIVER VIA OVERLAND FLOW AND THROUGH A STORM SEWER SYSTEM.

CONTROLS - EROSION CONTROLS AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION

1. THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
2. AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER.
3. THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
- B. CONSTRUCT DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.
- C. BUILD NECESSARY EMBANKMENT AT CULVERT/STORM SEWER LOCATIONS AND THEN EXCAVATE AND PLACE PIPE.
- D. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.

4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

5. THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2 INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD.

6. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

7. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

ALLOWABLE NON-STORM WATER DISCHARGES:

1. DISCHARGES FROM FIRE-FIGHTING ACTIVITIES;
2. FIRE HYDRANT FLUSHINGS;
3. WATER USED TO CONTROL DUST;
4. POTABLE WATER INCLUDING UNCONTAMINATED WATER LINE FLUSHINGS;
5. UNCONTAMINATED GROUND WATER OR SPRING WATER;
6. UNCONTAMINATED EXCAVATION DEWATERING;
7. LANDSCAPE IRRIGATION;

RUNOFF COEFFICIENT AFTER CONSTRUCTION = 0.90

MAINTENANCE:

WHEN REQUESTED BY THE CONTRACTOR, THE RESIDENT ENGINEER WILL PROVIDE GENERAL MAINTENANCE GUIDES TO THE CONTRACTOR FOR THE PRACTICES ASSOCIATED WITH THIS PROJECT. THE FOLLOWING ADDITIONAL PROCEDURES WILL BE USED TO MAINTAIN, IN GOOD AND EFFECTIVE OPERATING CONDITIONS, THE VEGETATION, EROSION AND SEDIMENT CONTROL MEASURES AND OTHER PROTECTIVE MEASURES IDENTIFIED IN THIS PLAN. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ATTAIN MAINTENANCE GUIDELINES FOR ANY MANUFACTURED BMPs WHICH ARE TO BE INSTALLED AND MAINTAINED PER MANUFACTURE'S SPECIFICATIONS.

1. SEEDING - WHERE TEMPORARY CESSATION OF THE EARTH DISTURBING ACTIVITIES OCCURS, STABILIZATION OF DISTURBED AREAS MUST BE INITIATED WITHIN 1 WORKING DAY IF WORK WILL NOT RESUME FOR A PERIOD EXCEEDING 14 DAYS.
2. PERIMETER EROSION BARRIER - SEDIMENT WILL BE REMOVED IF THE INTEGRITY OF THE FENCING IS IN JEOPARDY AND ANY FENCE KNOCKED DOWN WILL BE REPAIRED IMMEDIATELY.
3. DITCH CHECK - SEDIMENT WILL BE REMOVED IF THE INTEGRITY OF THE DITCH CHECK IS IN JEOPARDY. ANY DITCH CHECKS WHICH FAILS WILL BE REPAIRED OR REPLACED IMMEDIATELY.
4. INLET PROTECTION/FILTERS - SEDIMENT WILL BE REMOVED IF THE INTEGRITY OF THE INLET PROTECTION IS IN JEOPARDY. ANY INLET PROTECTION FILTERS WHICH FAILS WILL BE REPLACED IMMEDIATELY.

INSPECTIONS:

QUALIFIED PERSONNEL SHALL INSPECT DISTURBED AREAS OF THE CONSTRUCTION SITE WHICH HAVE NOT YET BEEN FINALLY STABILIZED, STRUCTURAL CONTROL MEASURES, AND LOCATIONS WHERE VEHICLES AND EQUIPMENT ENTER AND EXIT THE SITE USING IDOT STORM WATER POLLUTION PREVENTION PLAN EROSION CONTROL INSPECTION REPORT (BC 2259). SUCH INSPECTIONS SHALL BE CONDUCTED AT LEAST ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN TWENTY-FOUR (24) HOURS OF THE END OF A STORM OR BY THE END OF THE FOLLOWING BUSINESS OR WORK DAY THAT IS 0.5 INCH OR GREATER OR EQUIVALENT SNOWFALL.

INSPECTIONS MAY BE REDUCED TO ONCE PER MONTH WHEN CONSTRUCTION ACTIVITIES HAVE CEASED DUE TO FROZEN CONDITIONS. WEEKLY INSPECTIONS WILL RECOMMENCE WHEN CONSTRUCTION ACTIVITIES ARE CONDUCTED, OR IF THERE IS 0.5" OR GREATER RAIN EVENT, OR A DISCHARGE DUE TO SNOWMELT OCCURS. IF ANY VIOLATION OF THE PROVISIONS OF THIS PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION WORK COVERED BY THIS PLAN, THE RESIDENT ENGINEER SHALL NOTIFY THE APPROPRIATE IEPA FIELD OPERATIONS SECTION OFFICE BY EMAIL AT: EPA.SWNONCOMP@ILLINOIS.GOV, TELEPHONE OR FAX WITHIN TWENTY-FOUR (24) HOURS OF THE INCIDENT. THE RESIDENT ENGINEER SHALL THEN COMPLETE AND SUBMIT AN "INCIDENCE OF NON-COMPLIANCE (ION) REPORT FOR THE IDENTIFIED VIOLATION WITHIN FIVE (5) DAYS OF THE INCIDENT. THE RESIDENT ENGINEER SHALL USE FORMS PROVIDED BY IEPA AND SHALL INCLUDE SPECIFIC INFORMATION ON THE CAUSE OF NONCOMPLIANCE, ACTIONS WHICH WERE TAKEN TO PREVENT ANY FURTHER CAUSES OF NONCOMPLIANCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NON-COMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G OF THE PERMIT ILR10. THE INCIDENT OF NON-COMPLIANCE SHALL BE MAILED TO THE FOLLOWING ADDRESS:

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
DIVISION OF WATER POLLUTION CONTROL
ATTN: COMPLIANCE ASSURANCE SECTION
1021 NORTH GRAND EAST
POST OFFICE BOX 19276
SPRINGFIELD, ILLINOIS 62794-9276
ADDITIONAL INSPECTIONS REQUIRED:

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDDED AND ESTABLISHED. ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDDED.

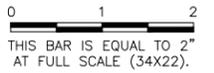
MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

QU020

REVISIONS

NUMBER	BY	DATE

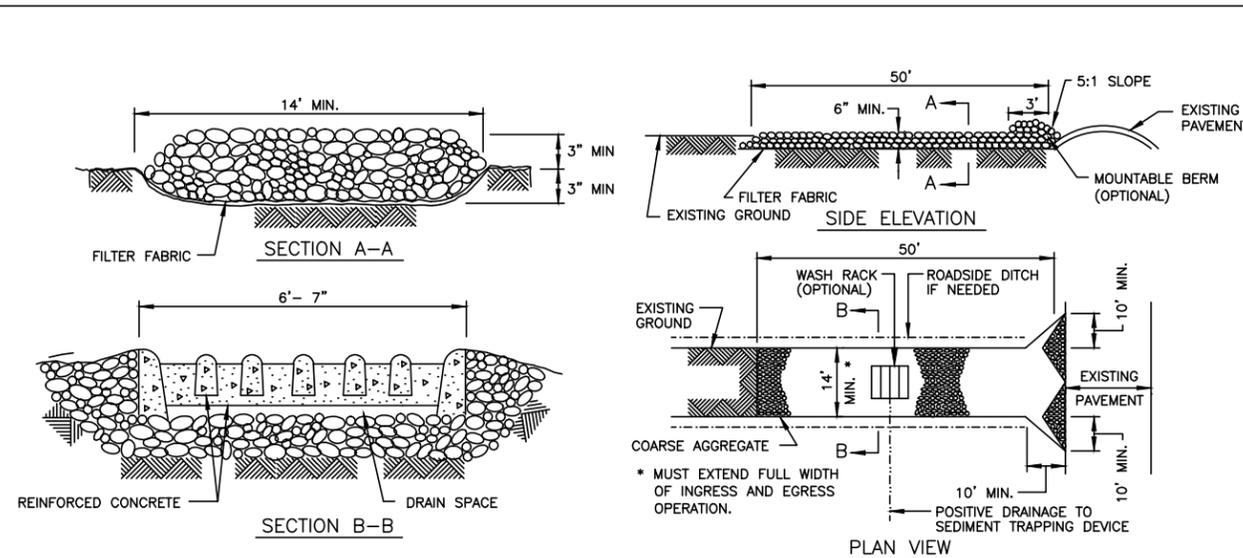


**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

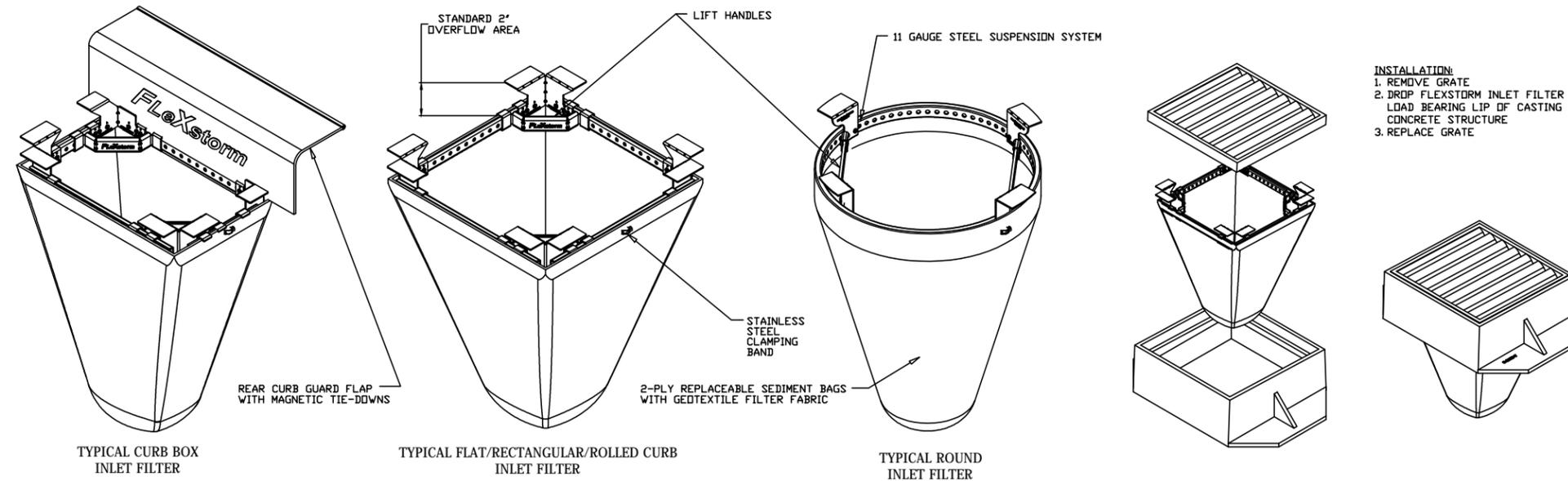
**CONSTRUCT TAXIWAY K CONNECTOR (K12)
STORMWATER POLLUTION PREVENTION PLAN
NOTES AND DETAILS SHEET 1**

STABILIZED CONSTRUCTION ENTRANCE NOTES

1. FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED UNDER SECTION 1080.03, OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED JANUARY 1, 2007.
2. ROCK OR RECLAIMED CONCRETE SHALL MEET ONE OF THE FOLLOWING IDOT COARSE AGGREGATE GRADATION, CA-1, CA-2, CA-3 OR CA-4. COMPACTION SHALL BE TO THE SATISFACTION OF THE ENGINEER.
3. ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHALL BE CONSTRUCTED ACCORDING TO MANUFACTURERS SPECIFICATIONS AND SHALL BE INCIDENTAL TO THE CONTRACT.
4. MINIMUM WIDTH IS 14' FOR ONE-WAY TRAFFIC AND 20' FOR TWO WAY TRAFFIC. TWO-WAY TRAFFIC WIDTHS SHALL BE INCREASED A MINIMUM OF 4' FOR TRAILER TRAFFIC. DEPENDING ON THE TYPE OF VEHICLE OR EQUIPMENT, SPEED, LOADS, CLIMATIC AND OTHER CONDITIONS UNDER WHICH VEHICLES AND EQUIPMENT OPERATE AN INCREASE IN THE MINIMUM WIDTHS MAY BE REQUIRED.
5. ROADWAY SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE.
6. IF WASH RACK ARE USED THEY SHALL BE INSTALLED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS.
7. THE STABILIZED CONSTRUCTION ENTRANCE SHALL BE INCIDENTAL TO THE CONTRACT.



STABILIZED CONSTRUCTION ENTRANCE
FROM NRCS STANDARD DRAWING NO. IL-630



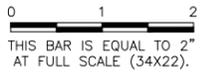
INSTALLATION:
 1. REMOVE GRATE
 2. DROP FLEXSTORM INLET FILTER INTO LOAD BEARING LIP OF CASTING OR CONCRETE STRUCTURE
 3. REPLACE GRATE

IPP Flexstorm Inlet Filter Specifications		
Material Property	Test Method	Value (min ave)
> Inner Filter Bag Specs (2 ft³ min vol)		
		Non-Woven Woven Mono
Grab Tensile	ASTM D 4632	100 lbs 200 lbs
Puncture Strength	ASTM D 4833	65 lbs 90 lbs
Trapezoidal Tear	ASTM D 4533	45 lbs 75 lbs
UV Resistance	ASTM D 4355	70% at 500 hrs 90%
App Open Size (AOS)	ASTM D 4751	70 sieve (.212 mm) 40 sieve (.425 mm)
Permittivity	ASTM D 4491	2.0 /sec 2.1/sec
Water Flow Rate	ASTM D 4491	145 gpm/sqft 145 gpm/sqft
> Polyester Outer Reinforcement Bag Specifications		
Weight	ASTM D 3776	4.55 oz/sqyd +/- 15%
Thickness	ASTM D 1777	.040 +/- .005
> Frame Construction		
A36 Structural Steel; 11 Gauge; Zinc Plated	ASTM A 576	Tensile Strength > 58,000 psi; Yield Strength > 36,000 psi

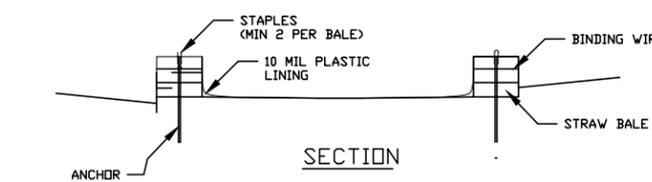
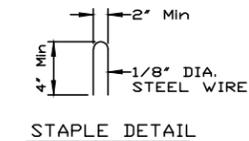
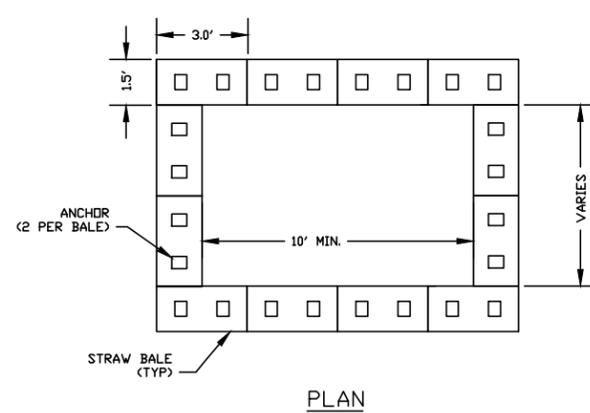
QU020

REVISIONS

NUMBER	BY	DATE

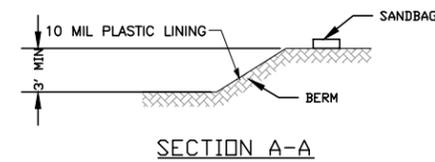
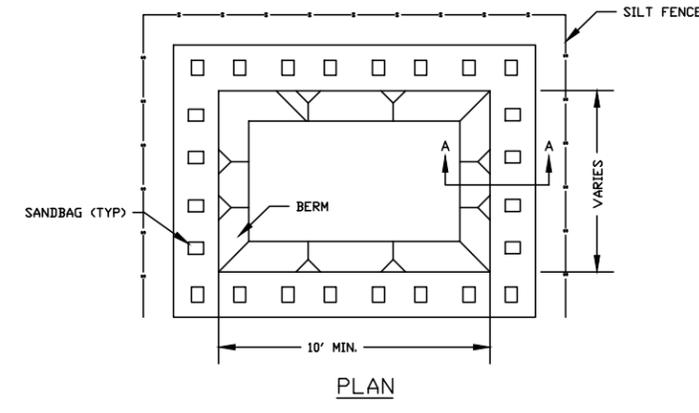


INLET PROTECTION / SEDIMENT TRAP
 NOT TO SCALE
 STORM SEWER INLET PROTECTION SHALL BE FLEXSTORM INLET FILTERS AS DETAILED HEREIN OR APPROVED EQUAL



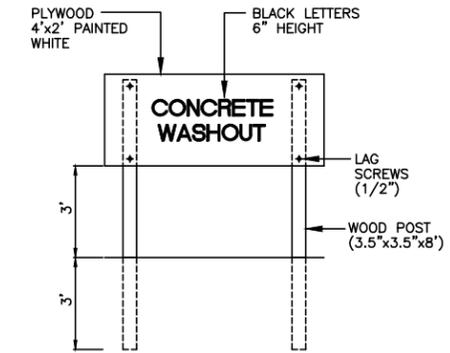
ABOVE GROUND TEMPORARY WASHOUT

- NOTES:**
- CONTRACTOR SHALL DETERMINE LOCATION AND SIZE OF WASHOUT.
 - WASHOUT SIZE AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
 - A CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 20 FEET OF THE TEMPORARY CONCRETE WASHOUT FACILITY. AT A MINIMUM, THE SIGN SHALL READ "CONCRETE WASHOUT" IN 6" TALL LETTERS.
 - INSPECTION SHALL OCCUR ONCE PER WEEK AND DAILY DURING CONCRETE OPERATIONS. REPAIR/REPLACEMENT OF THE FACILITY SHALL BE MADE SUCH THAT CONCRETE WASTE IS CONTAINED.
 - MEDIA SHALL BE REMOVED AND DISPOSED OF AT A LEGAL OFF-SITE LOCATION WHEN THE FACILITY HAS REACHED 50% CAPACITY.
 - UPON COMPLETION OF CONCRETE OPERATIONS, THE CONCRETE WASHOUT AND ALL MATERIALS CONTAINED WITHIN SHALL BE DISPOSED OF AT A LEGAL OFF-SITE LOCATION.



BELOW GROUND TEMPORARY WASHOUT

- NOTES:**
- CONTRACTOR SHALL DETERMINE LOCATION AND SIZE OF WASHOUT.
 - WASHOUT SIZE AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
 - SANDBAGS SHALL BE INSTALLED TO ANCHOR THE LINING. THE NUMBER OF SANDBAGS SHALL BE DETERMINED BY THE CONTRACTOR. THE CONTRACTOR SHALL ADD SANDBAGS SO AS TO MAINTAIN ANCHORING OF THE LINING.
 - A CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 20 FEET OF THE TEMPORARY CONCRETE WASHOUT FACILITY. AT A MINIMUM, THE SIGN SHALL READ "CONCRETE WASHOUT" IN 6" TALL LETTERS.
 - THE TEMPORARY WASHOUT FACILITY SHALL BE SURROUNDED BY SILT FENCE ON ALL SIDES.
 - INSPECTION SHALL OCCUR ONCE PER WEEK AND DAILY DURING CONCRETE OPERATIONS. REPAIR/REPLACEMENT OF THE FACILITY SHALL BE MADE SUCH THAT CONCRETE WASTE IS CONTAINED.
 - MEDIA SHALL BE REMOVED AND DISPOSED OF AT A LEGAL OFF-SITE LOCATION WHEN THE FACILITY HAS REACHED 50% CAPACITY.
 - UPON COMPLETION OF CONCRETE OPERATIONS, THE CONCRETE WASHOUT AND ALL MATERIALS CONTAINED WITHIN SHALL BE DISPOSED OF AT A LEGAL OFF-SITE LOCATION.



CONCRETE WASHOUT SIGN DETAIL (OR EQUIVALENT)

CONCRETE WASHOUT
 NOT TO SCALE

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 STORMWATER POLLUTION PREVENTION PLAN
 NOTES AND DETAILS SHEET 2**

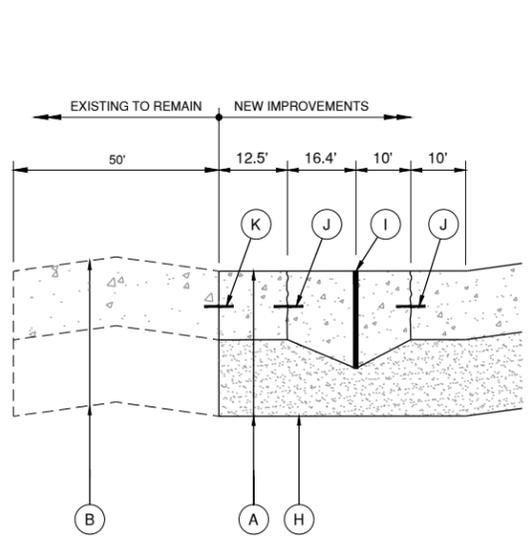
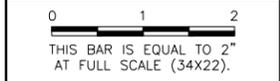
© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL PROJ. NO.	MLI-4482
AIP PROJ. NO.	3-17-0068-XX
SHEET	10 OF 27 SHEETS

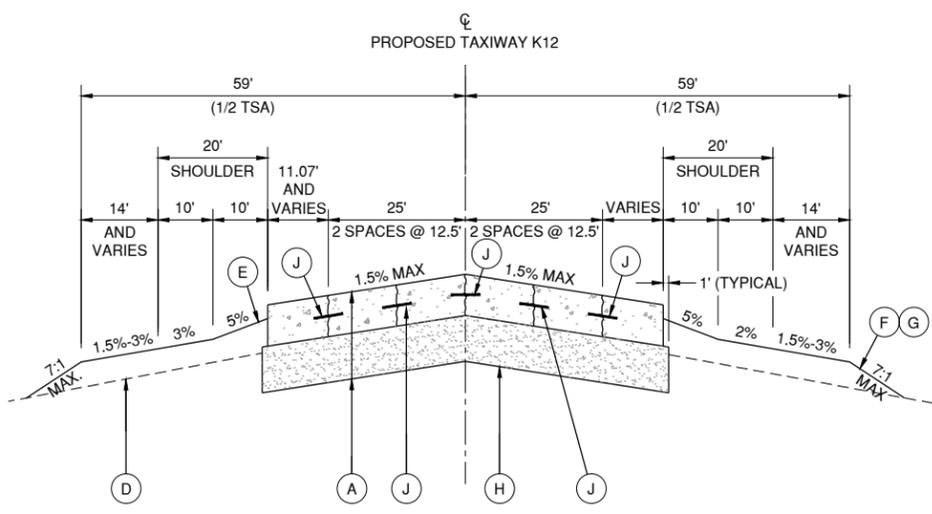
K:\Moline\14014-07_TaxiwayConnector\Draw\Sheets

QU020

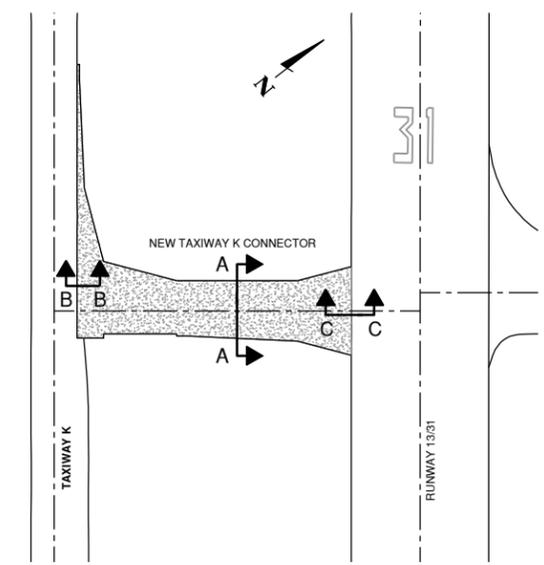
REVISIONS		
NUMBER	BY	DATE



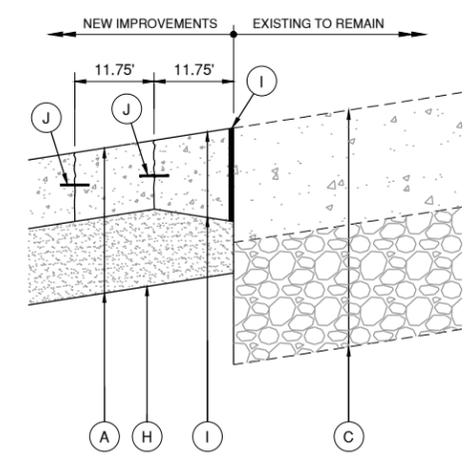
**TAXIWAY K CONNECTOR AND
 TAXIWAY K INTERSECTION
 SECTION B-B**
 NOT TO SCALE



**TAXIWAY K CONNECTOR
 SECTION A-A**
 NOT TO SCALE



KEY MAP



**TAXIWAY K CONNECTOR AND
 RUNWAY 13/31 INTERSECTION
 SECTION C-C**
 NOT TO SCALE

LEGEND

- (A) NEW 9" PCC PAVEMENT (501)
NEW 10" CRUSHED AGGREGATE BASE COURSE (209)
- (B) EXISTING 9" PCC
EXISTING 10" AGGREGATE BASE COURSE
- (C) EXISTING 15" PCC
EXISTING 16" CRUSHED AGGREGATE BASE COURSE
- (D) EXISTING GROUNDLINE
- (E) NEW 6' WIDE SODDING (904)
- (F) NEW GROUNDLINE
- (G) NEW SEEDING AND MULCHING (908)
- (H) NEW SOIL STABILIZATION FABRIC
- (I) NEW ISOLATION JOINT W/THICKENED EDGE PCC PAVEMENT
- (J) NEW DOWEL BAR
- (K) NEW DOWEL BAR IN EXISTING PAVEMENT

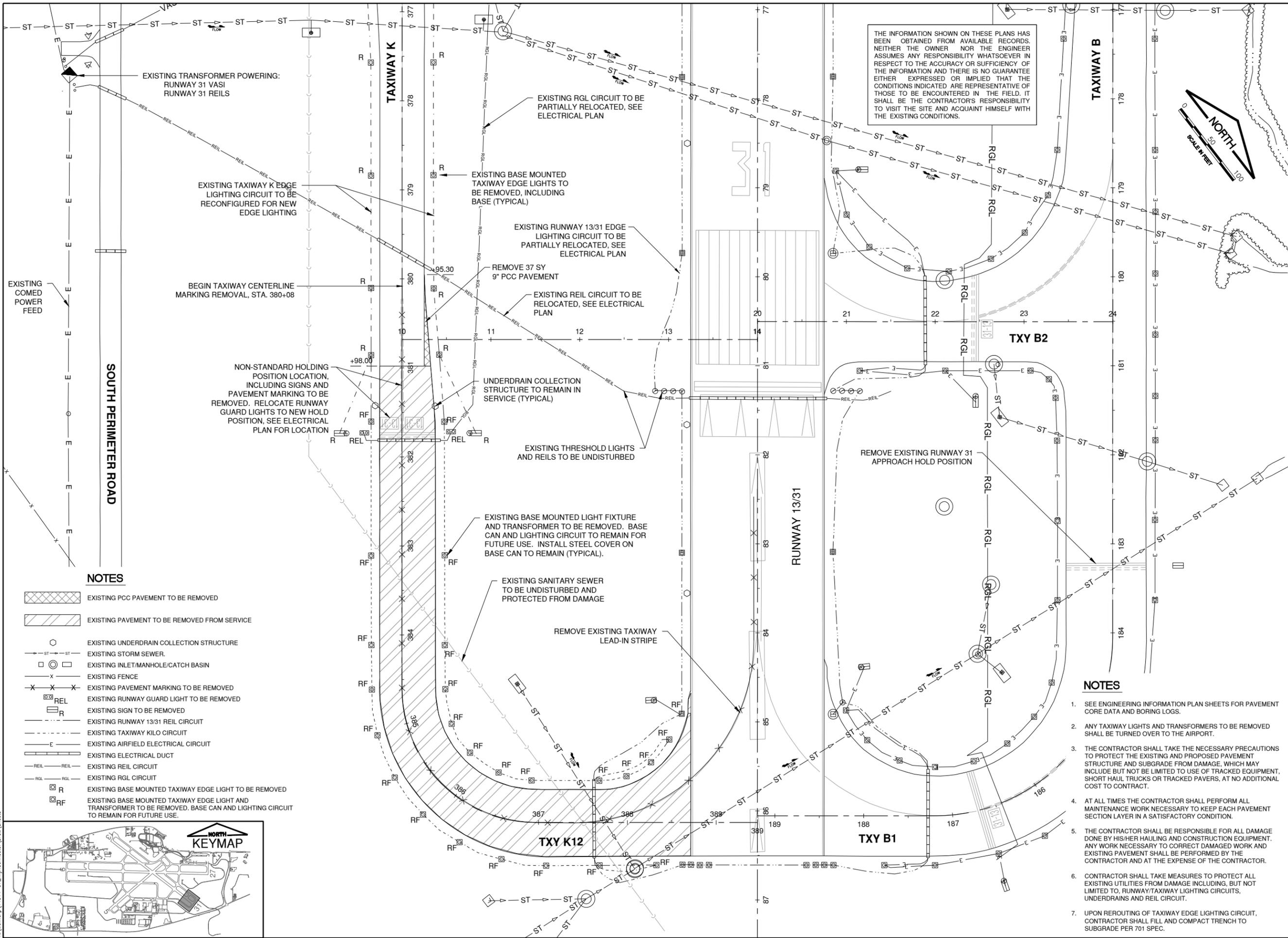
**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 TYPICAL SECTIONS**

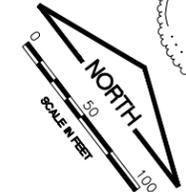
© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL PROJ. NO. MLI-4482 AIP PROJ. NO. 3-17-0068-XX	
SHEET	11 OF 27 SHEETS



THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



FILE: 12 EX COND.dwg
 UPDATE BY: Sean Smith
 PLOT DATE: 6/26/2015 3:50 PM

Taxiway K Base
 MLI Alignments
 PROP-GEOMETRY
 PROP-LIGHT-MARK

QU020

REVISIONS		
NUMBER	BY	DATE

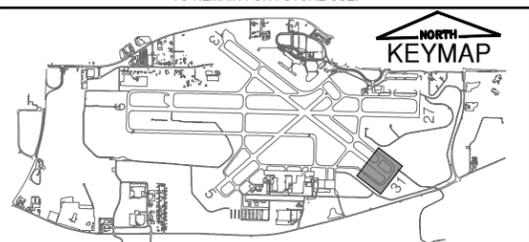
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 EXISTING CONDITIONS AND PROPOSED
 REMOVALS**

- NOTES**
- EXISTING PCC PAVEMENT TO BE REMOVED
 - EXISTING PAVEMENT TO BE REMOVED FROM SERVICE
 - EXISTING UNDERDRAIN COLLECTION STRUCTURE
 - EXISTING STORM SEWER
 - EXISTING INLET/MANHOLE/CATCH BASIN
 - EXISTING FENCE
 - EXISTING PAVEMENT MARKING TO BE REMOVED
 - EXISTING RUNWAY GUARD LIGHT TO BE REMOVED
 - EXISTING SIGN TO BE REMOVED
 - EXISTING RUNWAY 13/31 REIL CIRCUIT
 - EXISTING TAXIWAY KILO CIRCUIT
 - EXISTING AIRFIELD ELECTRICAL CIRCUIT
 - EXISTING ELECTRICAL DUCT
 - EXISTING REIL CIRCUIT
 - EXISTING RGL CIRCUIT
 - EXISTING BASE MOUNTED TAXIWAY EDGE LIGHT TO BE REMOVED
 - EXISTING BASE MOUNTED TAXIWAY EDGE LIGHT AND TRANSFORMER TO BE REMOVED. BASE CAN AND LIGHTING CIRCUIT TO REMAIN FOR FUTURE USE.

- NOTES**
1. SEE ENGINEERING INFORMATION PLAN SHEETS FOR PAVEMENT CORE DATA AND BORING LOGS.
 2. ANY TAXIWAY LIGHTS AND TRANSFORMERS TO BE REMOVED SHALL BE TURNED OVER TO THE AIRPORT.
 3. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE EXISTING AND PROPOSED PAVEMENT STRUCTURE AND SUBGRADE FROM DAMAGE, WHICH MAY INCLUDE BUT NOT BE LIMITED TO USE OF TRACKED EQUIPMENT, SHORT HAUL TRUCKS OR TRACKED PAVERS, AT NO ADDITIONAL COST TO CONTRACT.
 4. AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
 5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS/HER HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
 6. CONTRACTOR SHALL TAKE MEASURES TO PROTECT ALL EXISTING UTILITIES FROM DAMAGE INCLUDING, BUT NOT LIMITED TO, RUNWAY/TAXIWAY LIGHTING CIRCUITS, UNDERDRAINS AND REIL CIRCUIT.
 7. UPON REROUTING OF TAXIWAY EDGE LIGHTING CIRCUIT, CONTRACTOR SHALL FILL AND COMPACT TRENCH TO SUBGRADE PER 701 SPEC.



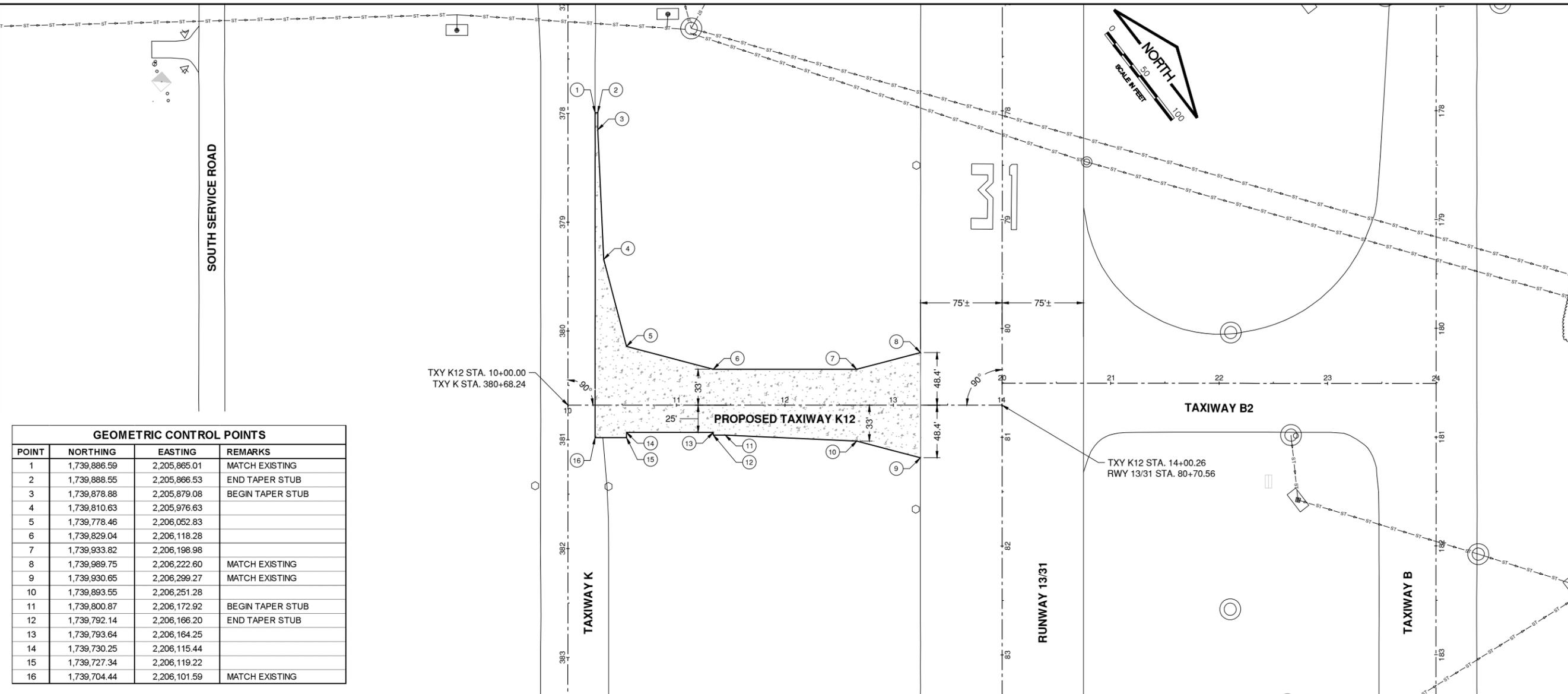
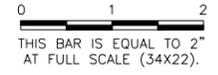
© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL. PROJ. NO. MLI-4482	
AIP PROJ. NO. 3-17-0068-XX	
SHEET 12 OF 27 SHEETS	

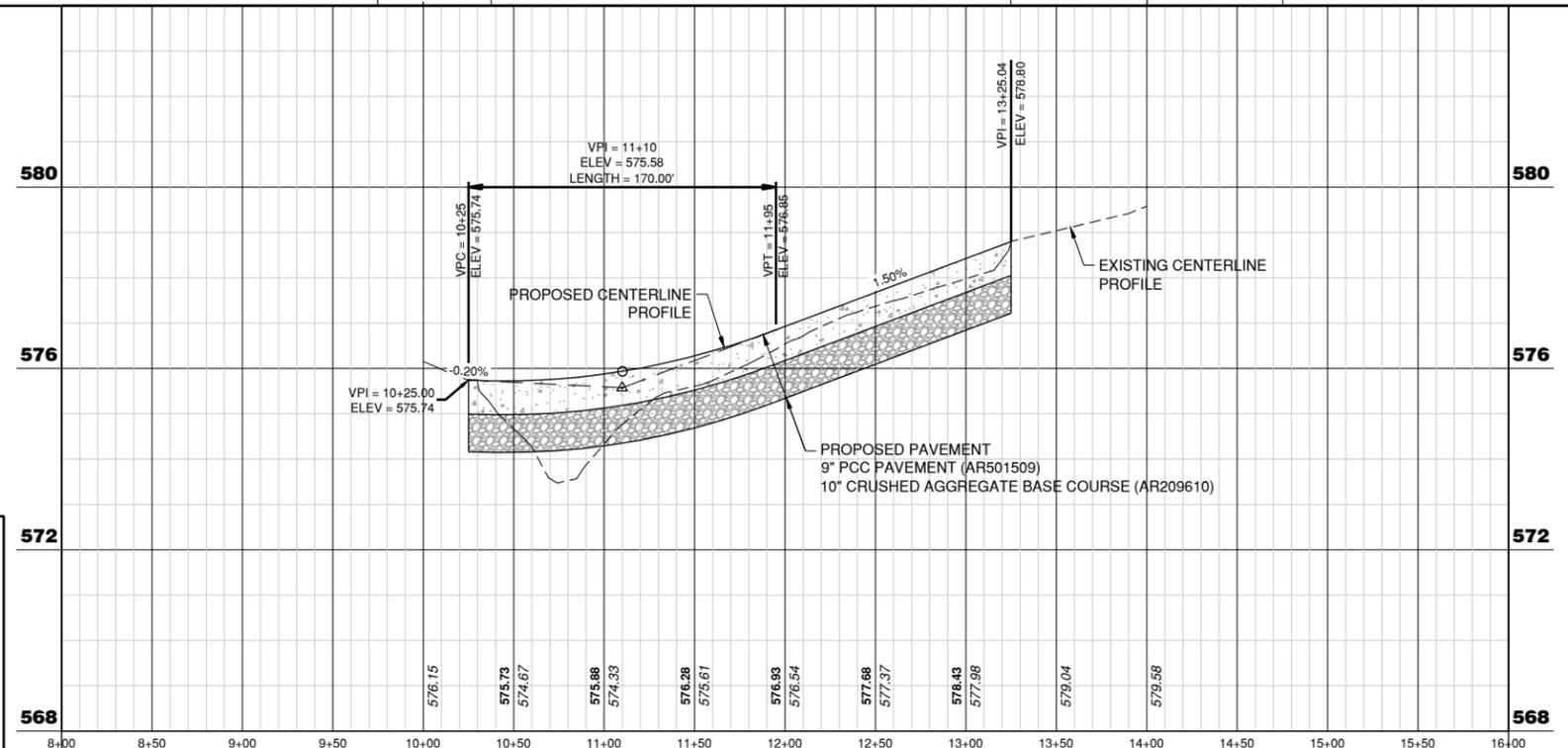
QU020

REVISIONS		
NUMBER	BY	DATE



GEOMETRIC CONTROL POINTS			
POINT	NORTHING	EASTING	REMARKS
1	1,739,886.59	2,205,865.01	MATCH EXISTING
2	1,739,888.55	2,205,866.53	END TAPER STUB
3	1,739,878.88	2,205,879.08	BEGIN TAPER STUB
4	1,739,810.63	2,205,976.63	
5	1,739,778.46	2,206,052.83	
6	1,739,829.04	2,206,118.28	
7	1,739,933.82	2,206,198.98	
8	1,739,989.75	2,206,222.60	MATCH EXISTING
9	1,739,930.65	2,206,299.27	MATCH EXISTING
10	1,739,893.55	2,206,251.28	
11	1,739,800.87	2,206,172.92	BEGIN TAPER STUB
12	1,739,792.14	2,206,166.20	END TAPER STUB
13	1,739,793.64	2,206,164.25	
14	1,739,730.25	2,206,115.44	
15	1,739,727.34	2,206,119.22	
16	1,739,704.44	2,206,101.59	MATCH EXISTING

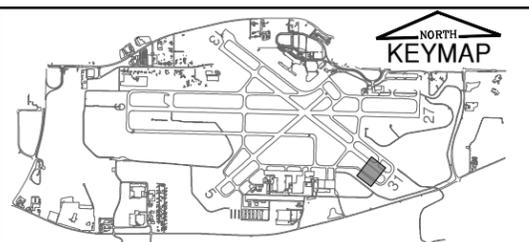
METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS
CONSTRUCT TAXIWAY K CONNECTOR (K12)
PLAN AND PROFILE



LEGEND

	NEW 9" PCC PAVEMENT
	NEW 10" CRUSHED AGG. BASE

- NOTES**
- REFER TO PAVEMENT JOINTING PLAN FOR ADDITIONAL DIMENSIONS.
 - POINT 1 SHALL MATCH EXISTING EXISTING TAXIWAY K JOINT.
 - POINT 16 SHALL MATCH EXISTING TAXIWAY K JOINT CORNER.
 - SEE GRADING AND DRAINAGE PLAN FOR UNDERDRAIN LAYOUT.



© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

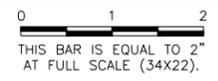
DESIGN BY:	SMS
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL PROJ. NO.	MLI-4482
AIP PROJ. NO.	3-17-0068-XX
SHEET	13 OF 27 SHEETS

K:\Moineap\14014-07_TaxiwayConnector\Draw\Sheets

Taxiway K Base
 PROP--GEOMETRY
 MLI Alignments
 Contour Plan

QU020

REVISIONS		
NUMBER	BY	DATE



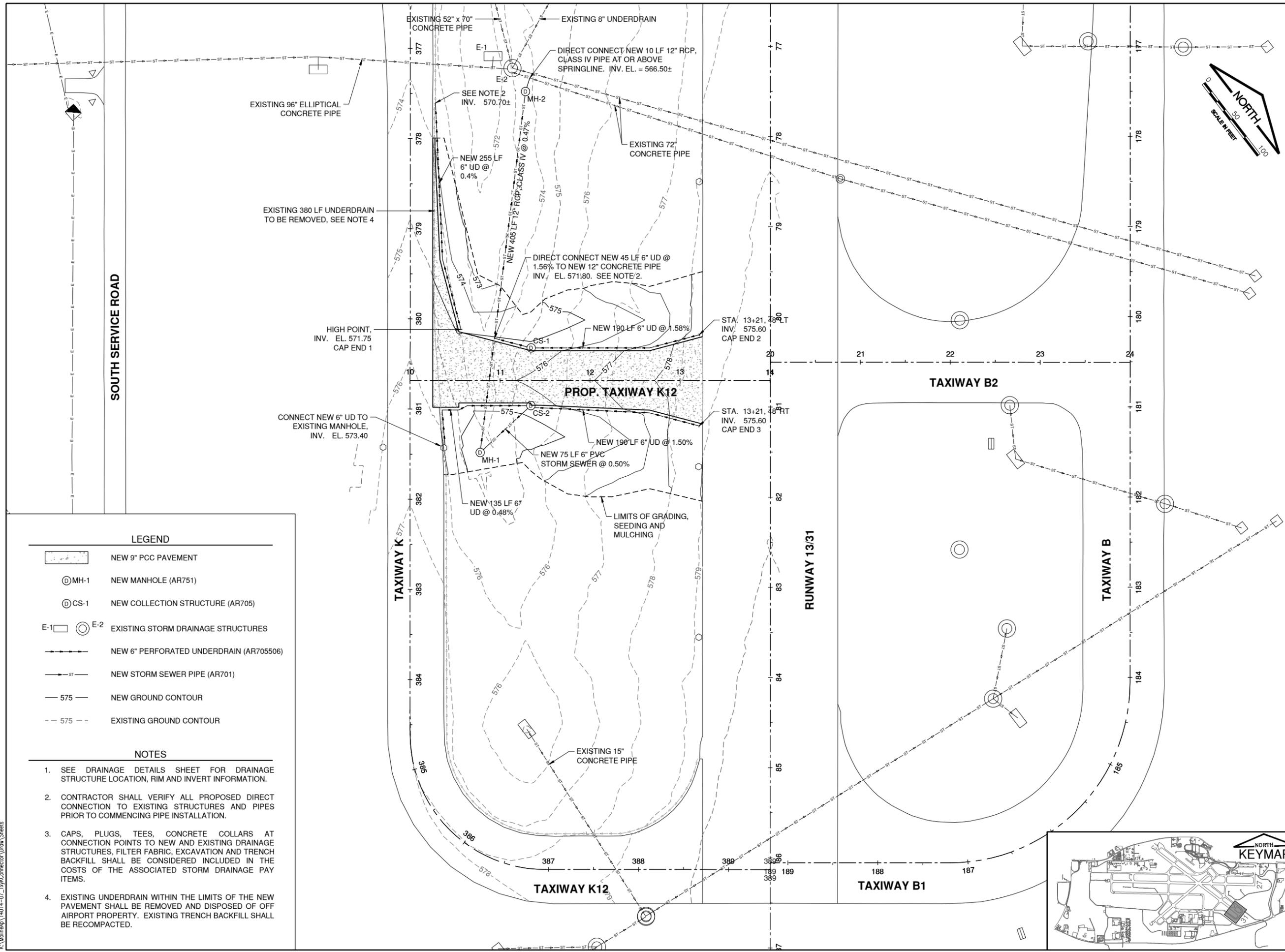
**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 GRADING AND DRAINAGE PLAN**

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

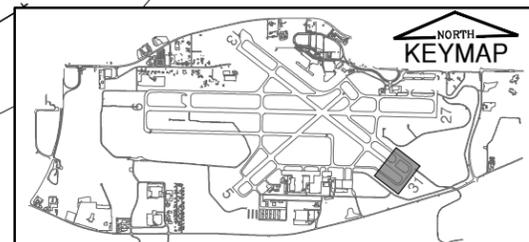
DESIGN BY:	SMS
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL PROJ. NO. MLI-4482 AIP PROJ. NO. 3-17-0068-XX	
SHEET 14 OF 27 SHEETS	



LEGEND

NEW 9" PCC PAVEMENT
MH-1 NEW MANHOLE (AR751)
CS-1 NEW COLLECTION STRUCTURE (AR705)
E-1 E-2 EXISTING STORM DRAINAGE STRUCTURES
NEW 6" PERFORATED UNDERDRAIN (AR705506)
NEW STORM SEWER PIPE (AR701)
575 NEW GROUND CONTOUR
575 EXISTING GROUND CONTOUR

- NOTES**
- SEE DRAINAGE DETAILS SHEET FOR DRAINAGE STRUCTURE LOCATION, RIM AND INVERT INFORMATION.
 - CONTRACTOR SHALL VERIFY ALL PROPOSED DIRECT CONNECTION TO EXISTING STRUCTURES AND PIPES PRIOR TO COMMENCING PIPE INSTALLATION.
 - CAPS, PLUGS, TEES, CONCRETE COLLARS AT CONNECTION POINTS TO NEW AND EXISTING DRAINAGE STRUCTURES, FILTER FABRIC, EXCAVATION AND TRENCH BACKFILL SHALL BE CONSIDERED INCLUDED IN THE COSTS OF THE ASSOCIATED STORM DRAINAGE PAY ITEMS.
 - EXISTING UNDERDRAIN WITHIN THE LIMITS OF THE NEW PAVEMENT SHALL BE REMOVED AND DISPOSED OF OFF AIRPORT PROPERTY. EXISTING TRENCH BACKFILL SHALL BE RECOMPACTED.



K:\Maineap\14014-07_TaxiwayConnector\Draw\Sheets

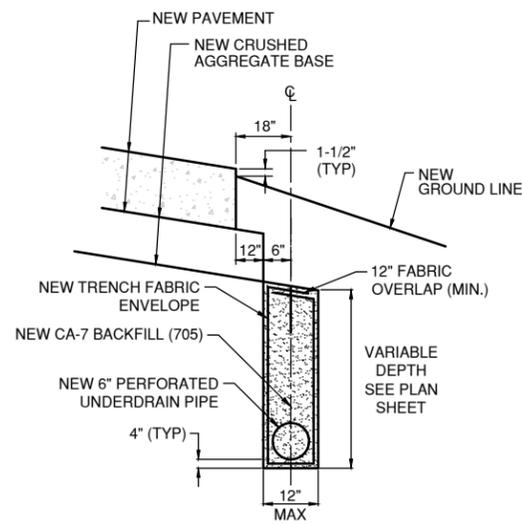
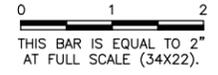
STORM SEWER/UNDERDRAIN NOTES

1. CONTRACTOR SHALL FIELD VERIFY EXISTING STORM SEWER OR UNDERDRAIN INVERTS BEFORE INSTALLING PROPOSED PIPE, CONNECTIONS AND ORDERING MATERIALS.
2. ALL UNDERDRAIN CONNECTIONS, CORING INTO STRUCTURES, CAPS, TEES, BENDS, STORM SEWER ETC. SHALL BE CONSIDERED INCLUDED IN THE COST OF THE UNDERDRAIN.
3. UNDERDRAIN SLOPES FOLLOW EDGE OF PAVEMENT SLOPE UNLESS OTHERWISE NOTED.
4. INSTALL PROPOSED ELECTRICAL DUCTS AND CONDUITS TO BE CLEAR OF UNDERDRAIN, COSTS INCLUDED.
5. UNDERDRAIN CONFLICTS WITH EXISTING CONDITIONS SHALL BE RESOLVED AND COST SHALL BE INCIDENTAL TO UNDERDRAIN.
6. CORING OF DRAINAGE STRUCTURE AND REMOVAL OF EXISTING STORM SEWER MANHOLE/INLET CONCRETE BENCHES TO FACILITATE CONNECTIONS OF PROPOSED STORM SEWER AND UNDERDRAIN PIPE SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PIPE.
7. ALL FRAMES, LIDS AND GRATES SHALL BE AIRPORT CONSTRUCTION CASTINGS.

QU020

REVISIONS

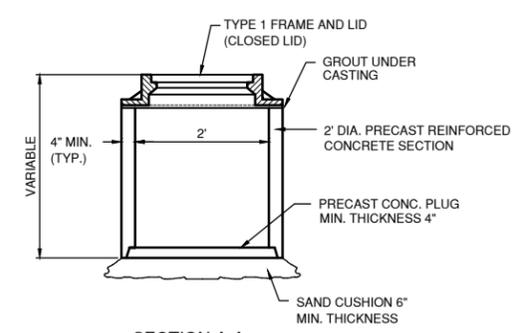
NUMBER	BY	DATE



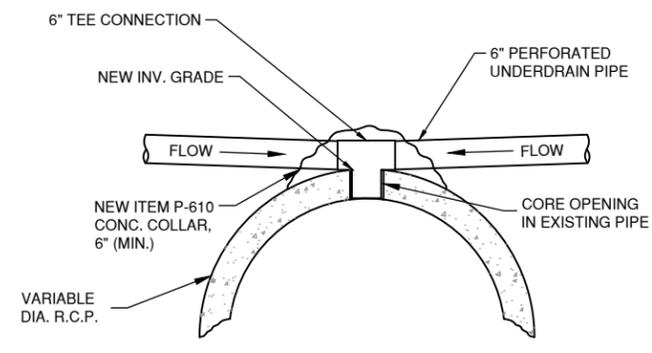
UNDERDRAIN DETAIL - EDGE OF P.C.C. PAVEMENT AREAS
 NOT TO SCALE

NOTES

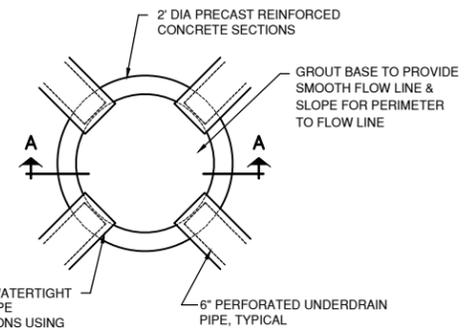
1. THE 6" UNDERDRAIN SHALL BE INSTALLED AFTER THE SUBGRADE IS COMPACTED.
2. THE SPOILS FROM THE 6" UNDERDRAIN CONSTRUCTION SHALL BE REMOVED DAILY FROM THE SURFACE OF THE CRUSHED AGGREGATE BASE.
3. TRENCH FABRIC ENVELOPE AND CA-7 BACKFILL SHALL BE CONSIDERED INCIDENTAL TO THE UNDERDRAIN.



SECTION A-A



DIRECT CONNECTION DETAIL
 NOT TO SCALE

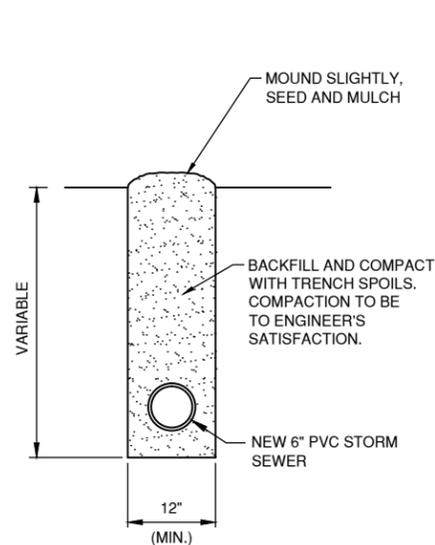


PLAN VIEW

PROVIDE WATERTIGHT SEAL AT PIPE CONNECTIONS USING WATER SEALING GROUT

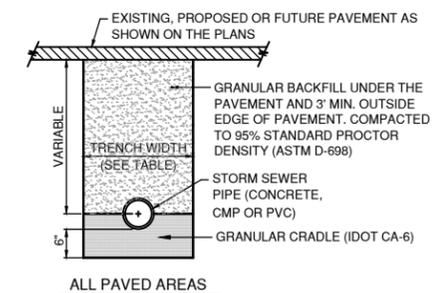
UNDERDRAIN COLLECTION STRUCTURE DETAIL
 NOT TO SCALE

DRAINAGE STRUCTURE DATA TABLE				
STRUCTURE NUMBER	LOCATION	RIM	INVERT	NOTES
CS-1	TXY K12 STA. 11+34, 35' LT.	575.50	INV. (IN) 572.60 INV. (OUT) 572.50	6" UNDERDRAIN
CS-2	TXY K12 STA. 11+34, 30' RT.	575.62	INV. (IN) 572.75 INV. (OUT) 572.65	6" UNDERDRAIN
MH-1	TXY K12 STA. 10+78, 80' RT	574.30	NW INV. (12") 572.00 N INV. (6") 572.25	4' DIA. MANHOLE
MH-2	TXY K12 STA. 11+28, 320' LT.	573.60	SE INV. (12") 570.00 NE INV. (12") 566.57	4' DIA. MANHOLE
DC-1	TXY K12 STA. 11+32, 335' LT.		566.50	FIELD VERIFY LOCATION
E-1	TXY K12 STA. 10+92, 347' LT.	571.20±	UNKNOWN	EXISTING STRUCTURE
E-2	TXY K12 STA. 11+13, 346' LT.	573.20±	563.05±	EXISTING STRUCTURE

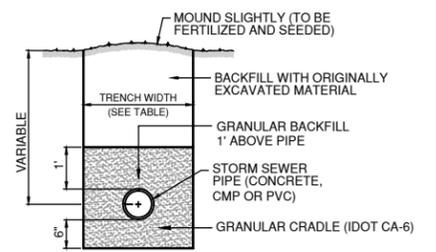


UNDERDRAIN OUTLET PIPE DETAIL OUTSIDE PAVEMENT LIMITS
 NOT TO SCALE

INSIDE DIAMETER OF STORM SEWER (INCHES)	MAXIMUM TRENCH WIDTH	MAXIMUM PAVEMENT REMOVAL WIDTH
6	3'-7"	5'-7"
8	3'-9"	5'-9"
12	4'-2"	6'-2"
15	4'-6"	6'-6"
18	4'-9"	6'-9"
21	5'-0"	7'-0"
24	5'-4"	7'-4"
27	5'-7"	7'-7"
30	5'-11"	7'-11"
36	6'-6"	8'-6"
42	7'-1"	9'-1"
48	7'-8"	9'-8"
54	8'-3"	10'-3"
60	8'-10"	10'-10"
66	9'-5"	11'-5"
72	10'-0"	12'-0"
78	10'-7"	12'-7"
84	11'-2"	13'-2"
90	11'-9"	13'-9"
96	12'-4"	14'-4"
102	12'-11"	14'-11"
108	13'-6"	15'-6"



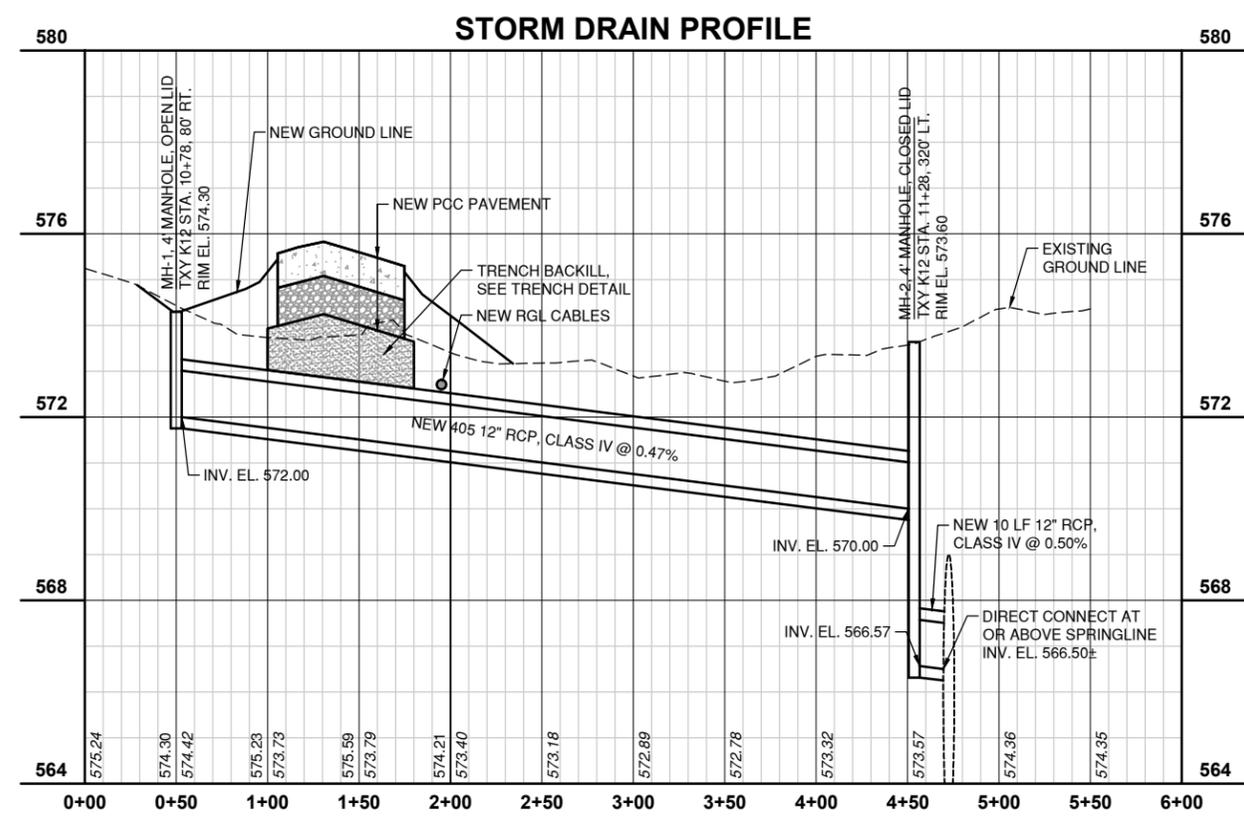
ALL PAVED AREAS



NON-PAVED AREAS

TRENCH DETAILS

NOT TO SCALE



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 DRAINAGE DETAILS**

© Copyright CMT, Inc.

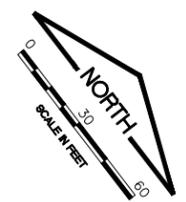
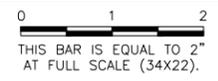
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL PROJ. NO.	MLI-4482
AIP PROJ. NO.	3-17-0068-XX
SHEET	15 OF 27 SHEETS

K:\Maineap\14014-07_TaxiwayConnector\Drawn\Sheets

QU020

REVISIONS		
NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 PAVEMENT JOINTING PLAN**

© Copyright CMT, Inc.



CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

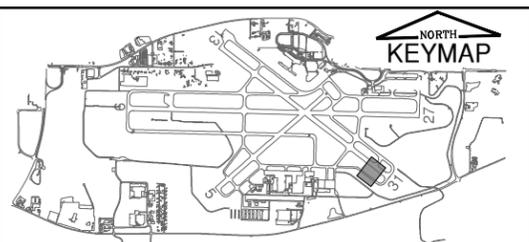
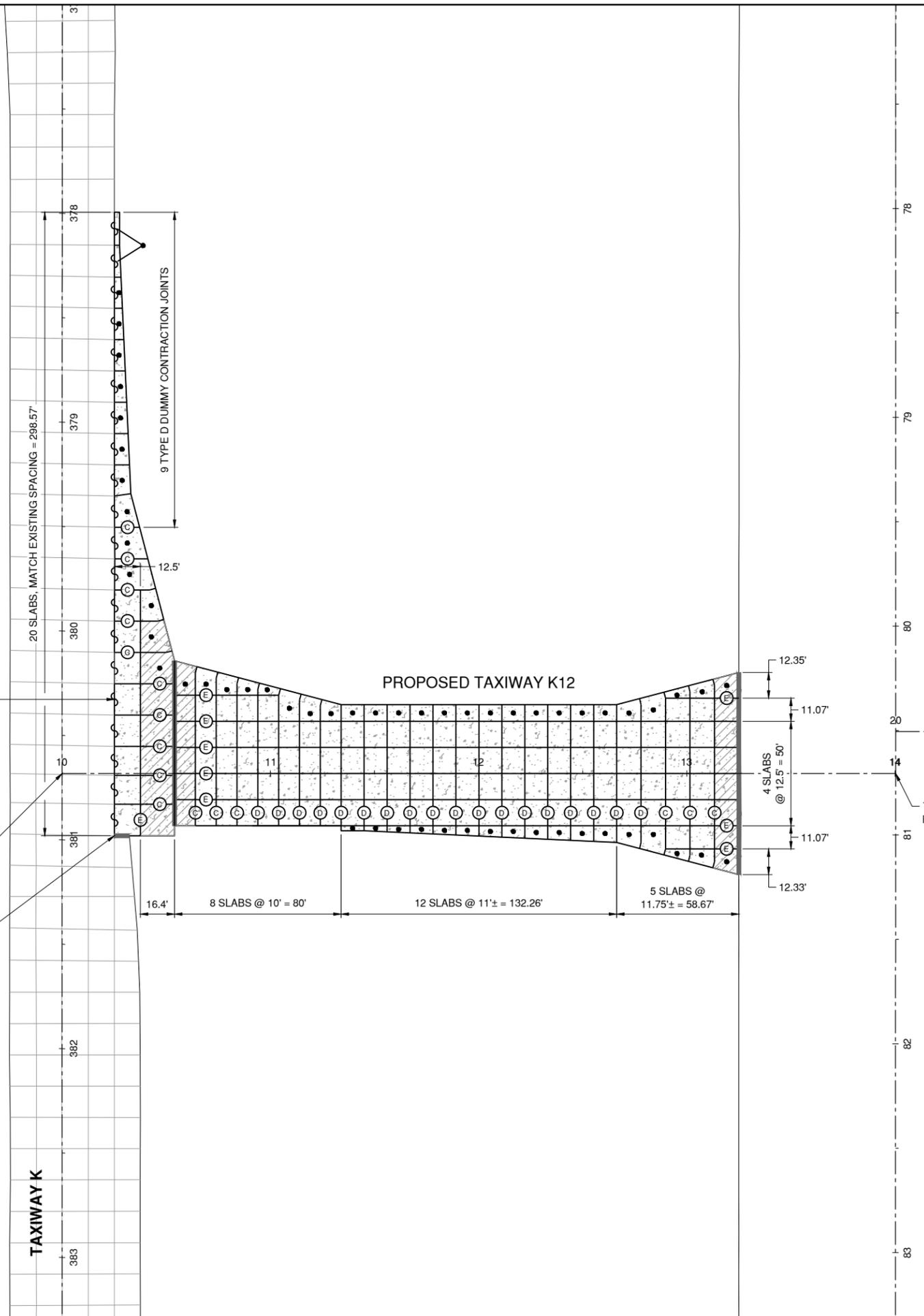


DESIGN BY:	SMS
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
I.L. PROJ. NO. MLI-4482 AIP PROJ. NO. 3-17-0068-XX	
SHEET 16 OF 27 SHEETS	

LEGEND

-  NEW PCC PAVEMENT
-  TYPE A ISOLATION JOINT
-  NEW THICKENED EDGE PCC
-  TYPE C DOWELED CONTRACTION JOINT
-  TYPE D DUMMY CONTRACTION JOINT
-  TYPE E DOWELED CONSTRUCTION JOINT
-  TYPE E DOWELED CONSTRUCTION JOINT IN EXISTING PAVEMENT
-  REINFORCED PANEL

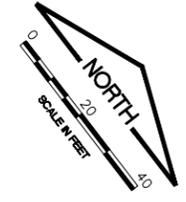
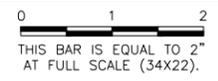
- NOTES**
- INITIAL SAWCUT SHALL BE MADE AS SOON AS POSSIBLE. SAWING SHALL BE STOPPED IF SPALLING OF THE SAWCUT IS NOTED AND RESUMED AFTER ADDITIONAL CURE TIME.
 - DRY CUTTING OF INITIAL CUT AND OF THE FINAL JOINT RESERVOIR SHALL NOT BE ALLOWED. ONLY WET SAWING METHODS SHALL BE EMPLOYED ON THIS PROJECT.
 - DEPENDING ON THE CONTRACTOR'S PAVING PLAN, TYPE C DOWELED CONTRACTION AND TYPE E DOWELED CONSTRUCTION JOINTS ARE INTERCHANGEABLE. CONTRACTOR SHALL PROVIDE THE RESIDENT ENGINEER HIS PAVING PLAN FOR APPROVAL PRIOR TO BEGINNING WORK.
 - CONTRACTOR SHALL INSTALL DOWEL BARS INTO THE EXISTING CONCRETE PAVEMENT, COST INCLUDED IN NEW 9" PCC PAVEMENT (AR501509).
 - JOINT BETWEEN NEW AND EXISTING PCC PAVEMENT SHALL BE SAWED AND SEALED.
 - ONLY SILICONE JOINT SEALANTS SHALL BE USED ON THIS PROJECT. SILICONE JOINT SEALANT SHALL NOT BE SELF-LEVELING.



K:\Moline\14014-07_TaxiwayConnector\Drawn\Sheets

QU020

REVISIONS		
NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 PAVEMENT JOINT ELEVATION PLAN**

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY:	SMS
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL. PROJ. NO. MLI-4482 AIP PROJ. NO. 3-17-0068-XX	
SHEET 17 OF 27 SHEETS	

LEGEND

- NEW 9" PCC PAVEMENT
- PROPOSED ELEVATION
EXISTING ELEVATION

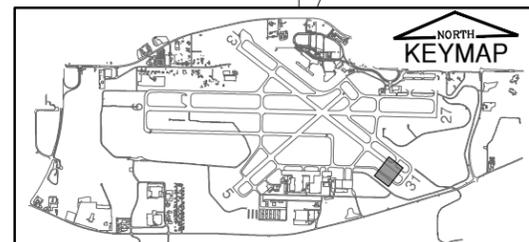
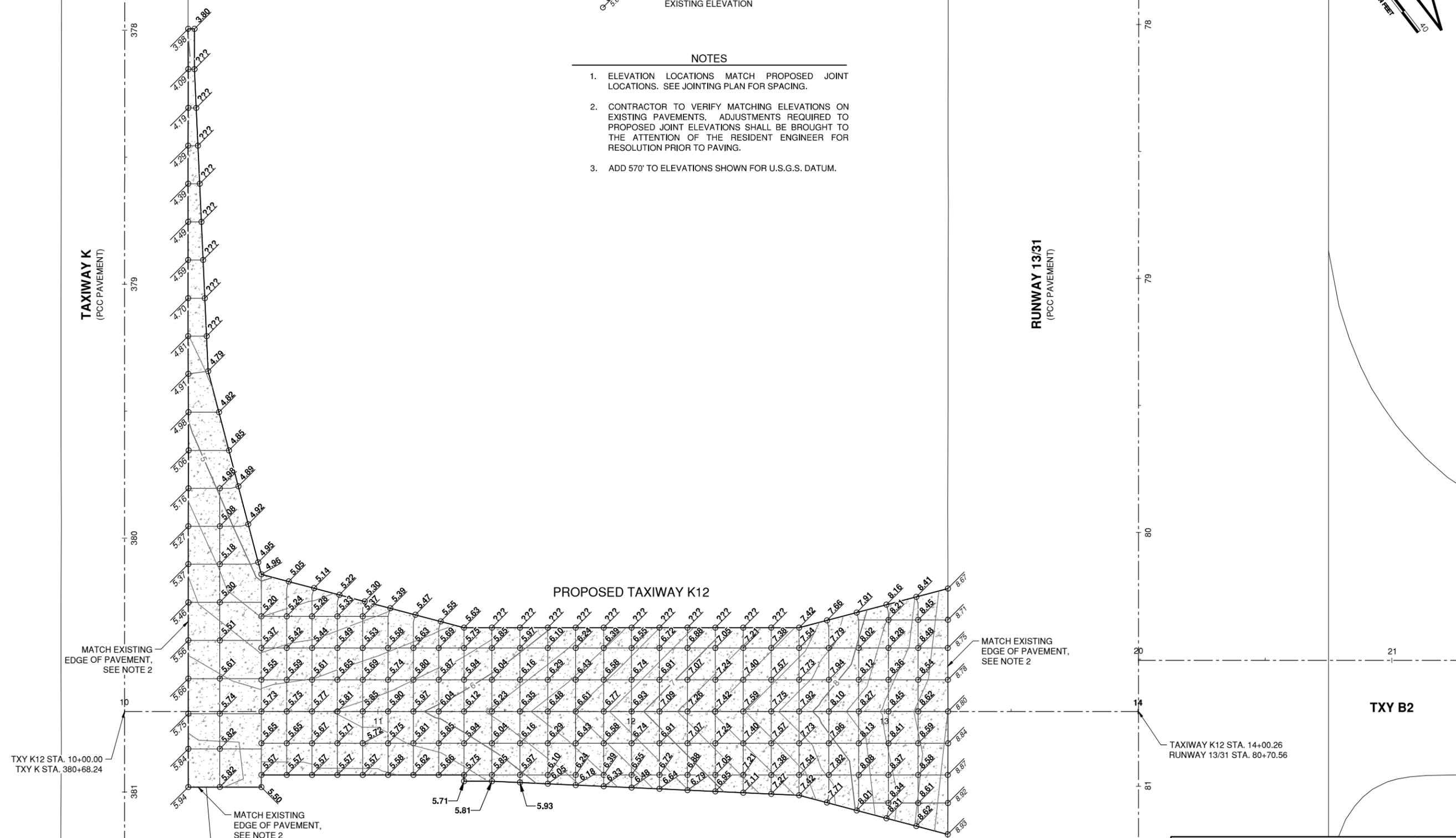
NOTES

1. ELEVATION LOCATIONS MATCH PROPOSED JOINT LOCATIONS. SEE JOINTING PLAN FOR SPACING.
2. CONTRACTOR TO VERIFY MATCHING ELEVATIONS ON EXISTING PAVEMENTS. ADJUSTMENTS REQUIRED TO PROPOSED JOINT ELEVATIONS SHALL BE BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER FOR RESOLUTION PRIOR TO PAVING.
3. ADD 570' TO ELEVATIONS SHOWN FOR U.S.G.S. DATUM.

**TAXIWAY K
 (PCC PAVEMENT)**

**RUNWAY 13/31
 (PCC PAVEMENT)**

PROPOSED TAXIWAY K12



K:\Maineap\14014-07_TxyKConnector\Draw\Sheets

JOINT NOTES

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION. ALTERNATE METHODS OF PLACEMENT OF DOWEL BARS MAY BE PROPOSED BY THE CONTRACTOR, TO BE APPROVED BY THE ENGINEER. TRANSVERSE DOWEL BAR IMPLANTING WILL NOT BE ALLOWED.
- ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR PLACED BY OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A706, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS THAT ARE TO BE BENT OR RE-STRAIGHTEND DURING CONSTRUCTION. TIE BARS DESIGNATED AS GRADE 40 IN ASTM A706 CAN BE USED FOR CONSTRUCTION REQUIRING BENT BARS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
- COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCLUDED IN ITEM AR60510, JOINT SEALING FILLER.
- SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
- EPOXY-COATED DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.
- CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS PER THE DETAIL ON THIS SHEET AT NO ADDITIONAL COST.

QU020

REVISIONS

NUMBER	BY	DATE

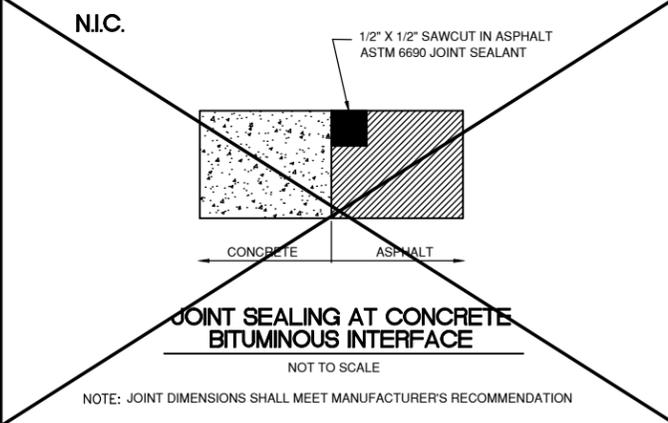
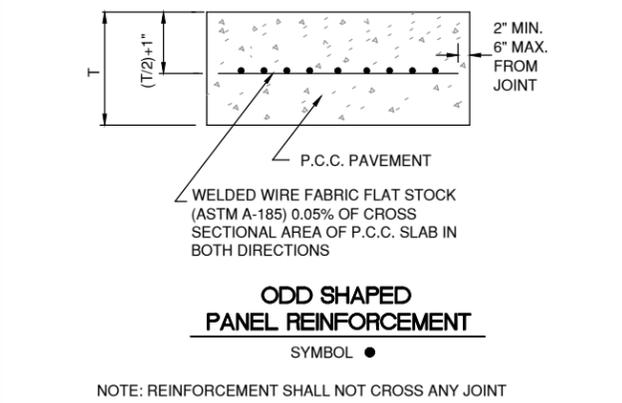
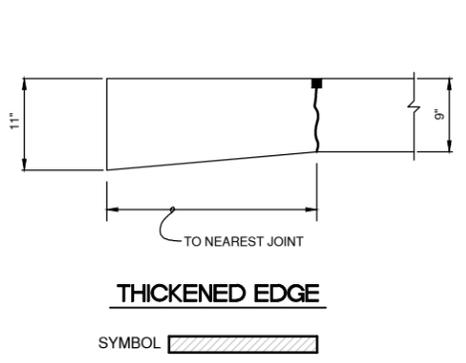
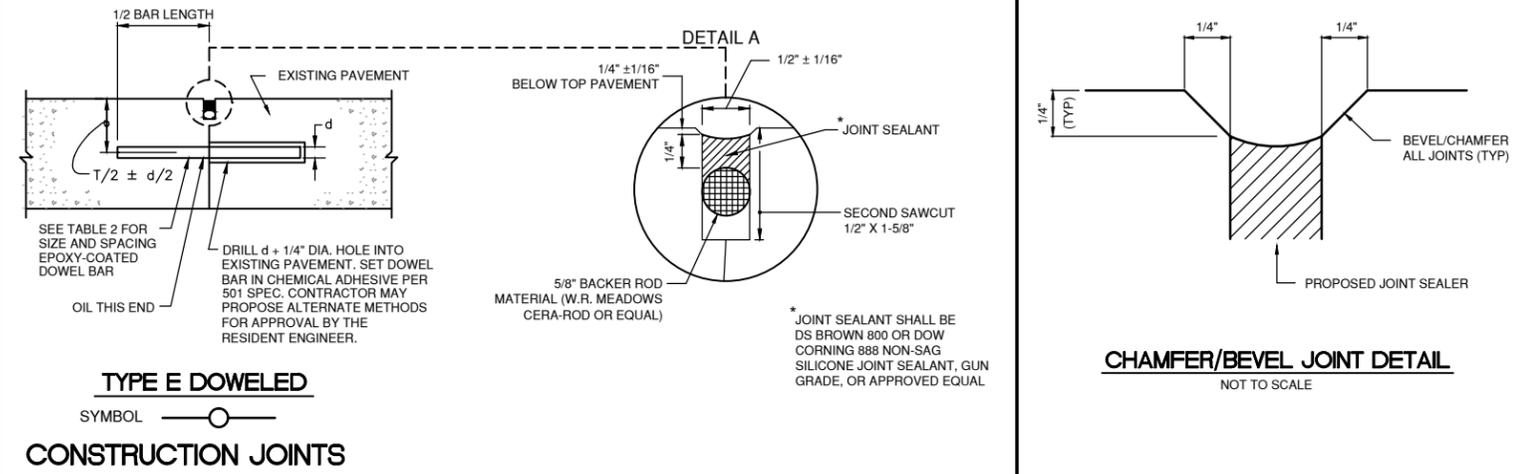
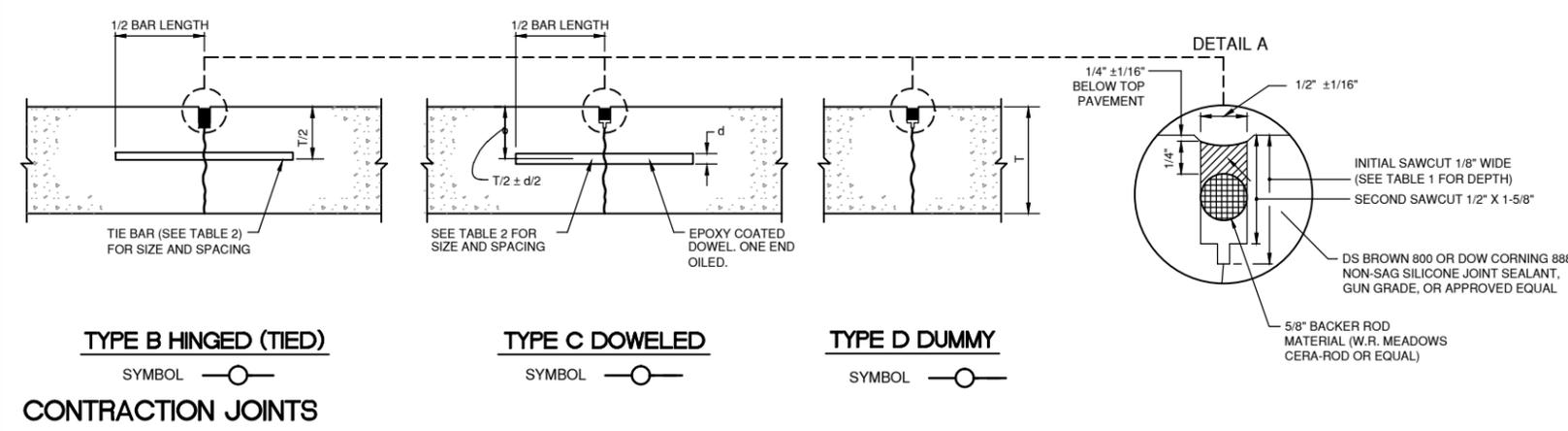
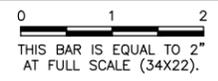
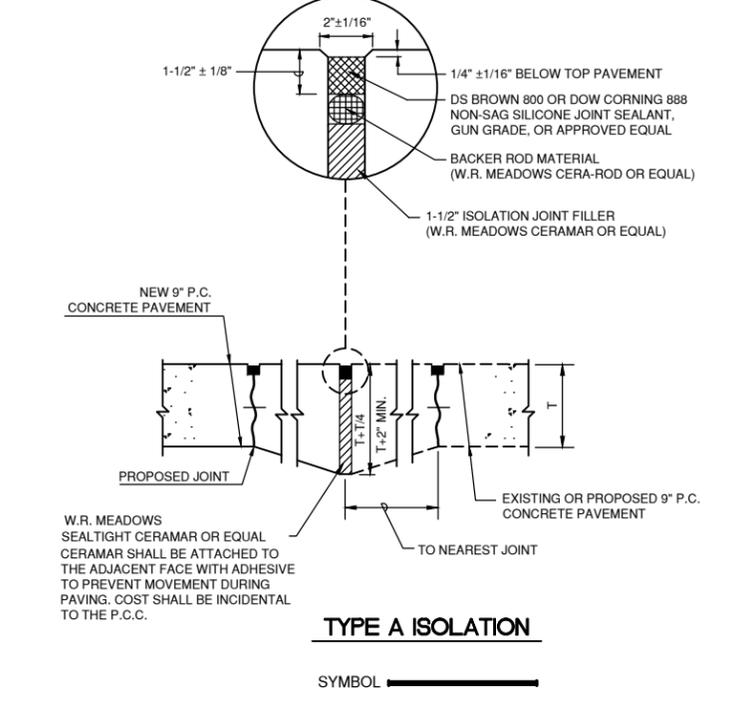
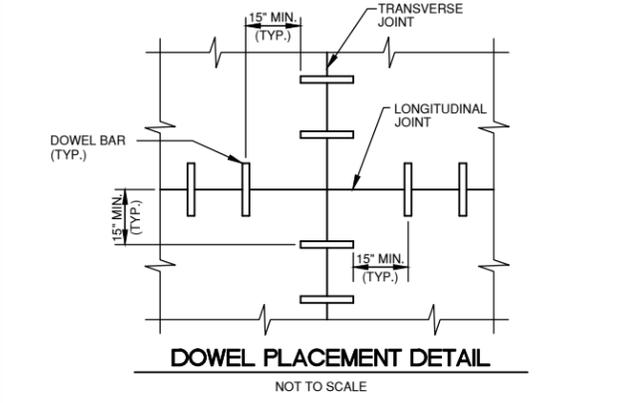


TABLE 1

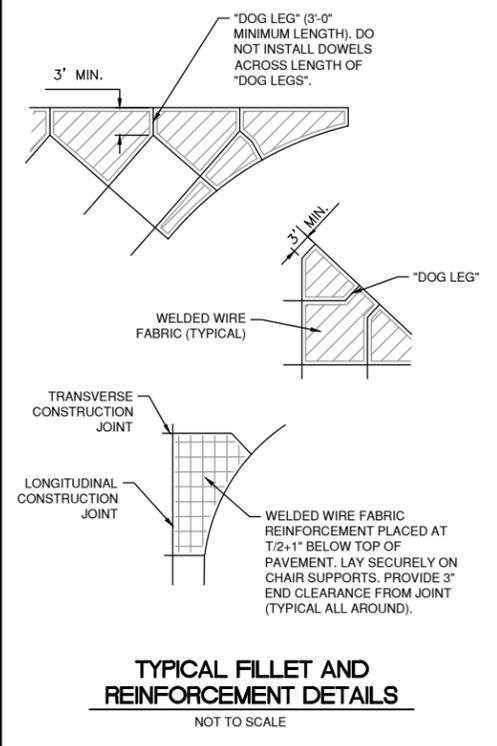
PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T, INCHES T=(T/3) ± 1/4"
9	4

TABLE 2

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA. (d)	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
9	1"	19"	12"	#5	30"	30"



ISOLATION JOINTS



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 PAVEMENT JOINTING DETAILS**

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

QUAD CITY INTERNATIONAL AIRPORT

DESIGN BY: CMT
 DRAWN BY: CMT
 CHECKED BY: SMS
 APPROVED BY: MND
 DATE: JUNE 26, 2015
 JOB No: 14014-07-00

IL PROJ. NO. MLI-4482
 AIP PROJ. NO. 3-17-0068-XX

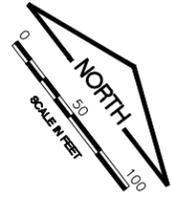
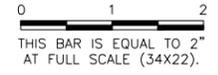
SHEET 18 OF 27 SHEETS

K:\Moline\14014-07_TaxiwayConnector\Drawn\Sheets

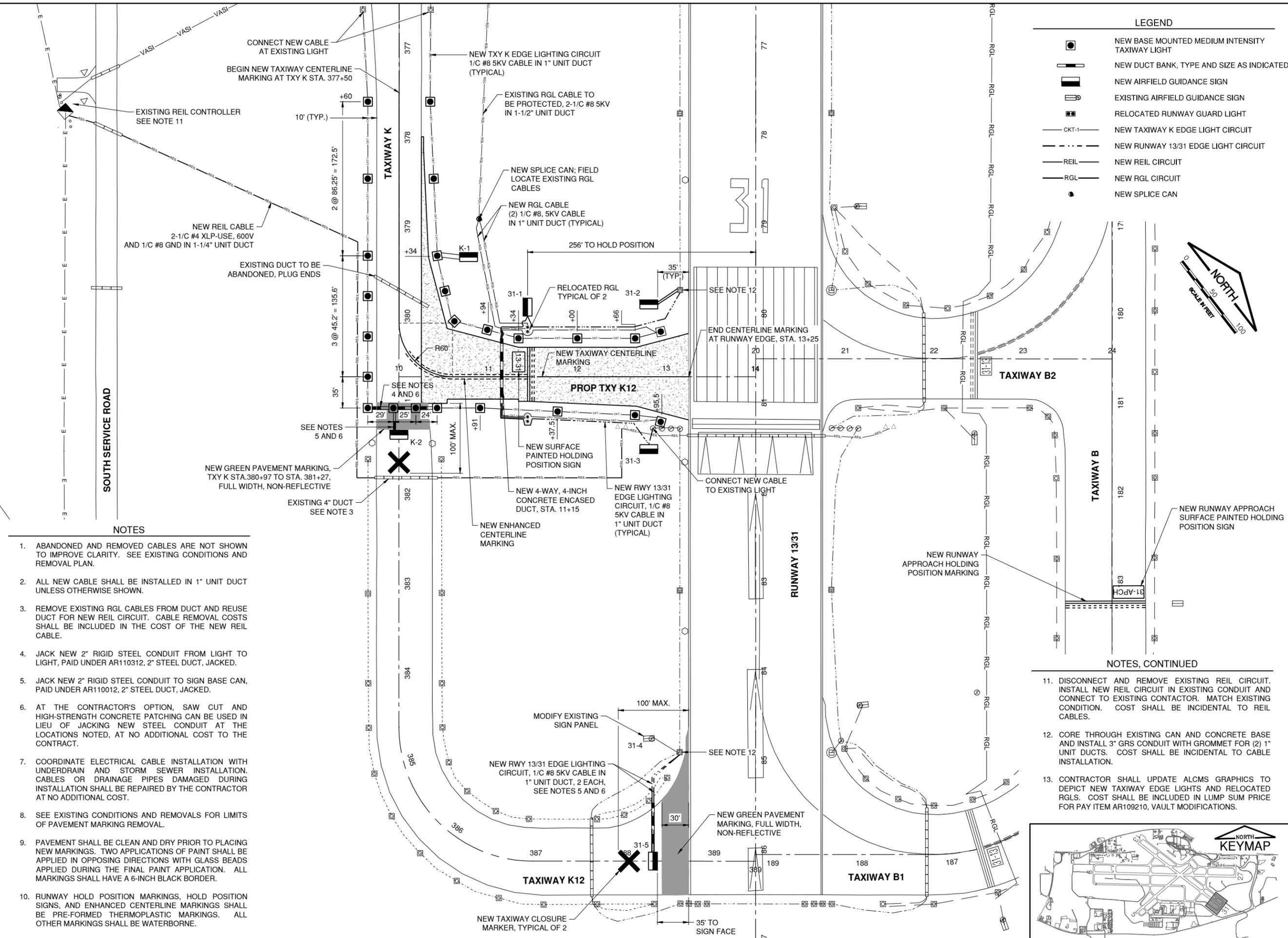
Taxiway K Base
 PROP-GEOMETRY
 MLI Alignments
 PROP-LIGHT-MARK

QU020

REVISIONS		
NUMBER	BY	DATE



LEGEND	
	NEW BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
	NEW DUCT BANK, TYPE AND SIZE AS INDICATED
	NEW AIRFIELD GUIDANCE SIGN
	EXISTING AIRFIELD GUIDANCE SIGN
	RELOCATED RUNWAY GUARD LIGHT
	NEW TAXIWAY K EDGE LIGHT CIRCUIT
	NEW RUNWAY 13/31 EDGE LIGHT CIRCUIT
	NEW REIL CIRCUIT
	NEW RGL CIRCUIT
	NEW SPLICE CAN

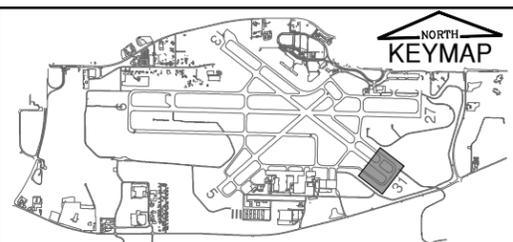


NOTES

- ABANDONED AND REMOVED CABLES ARE NOT SHOWN TO IMPROVE CLARITY. SEE EXISTING CONDITIONS AND REMOVAL PLAN.
- ALL NEW CABLE SHALL BE INSTALLED IN 1" UNIT DUCT UNLESS OTHERWISE SHOWN.
- REMOVE EXISTING RGL CABLES FROM DUCT AND REUSE DUCT FOR NEW REIL CIRCUIT. CABLE REMOVAL COSTS SHALL BE INCLUDED IN THE COST OF THE NEW REIL CABLE.
- JACK NEW 2" RIGID STEEL CONDUIT FROM LIGHT TO LIGHT, PAID UNDER AR110312, 2" STEEL DUCT, JACKED.
- JACK NEW 2" RIGID STEEL CONDUIT TO SIGN BASE CAN, PAID UNDER AR110012, 2" STEEL DUCT, JACKED.
- AT THE CONTRACTOR'S OPTION, SAW CUT AND HIGH-STRENGTH CONCRETE PATCHING CAN BE USED IN LIEU OF JACKING NEW STEEL CONDUIT AT THE LOCATIONS NOTED, AT NO ADDITIONAL COST TO THE CONTRACT.
- COORDINATE ELECTRICAL CABLE INSTALLATION WITH UNDERDRAIN AND STORM SEWER INSTALLATION. CABLES OR DRAINAGE PIPES DAMAGED DURING INSTALLATION SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST.
- SEE EXISTING CONDITIONS AND REMOVALS FOR LIMITS OF PAVEMENT MARKING REMOVAL.
- PAVEMENT SHALL BE CLEAN AND DRY PRIOR TO PLACING NEW MARKINGS. TWO APPLICATIONS OF PAINT SHALL BE APPLIED IN OPPOSING DIRECTIONS WITH GLASS BEADS APPLIED DURING THE FINAL PAINT APPLICATION. ALL MARKINGS SHALL HAVE A 6-INCH BLACK BORDER.
- RUNWAY HOLD POSITION MARKINGS, HOLD POSITION SIGNS, AND ENHANCED CENTERLINE MARKINGS SHALL BE PRE-FORMED THERMOPLASTIC MARKINGS. ALL OTHER MARKINGS SHALL BE WATERBORNE.

NOTES, CONTINUED

- DISCONNECT AND REMOVE EXISTING REIL CIRCUIT. INSTALL NEW REIL CIRCUIT IN EXISTING CONDUIT AND CONNECT TO EXISTING CONTACTOR. MATCH EXISTING CONDITION. COST SHALL BE INCIDENTAL TO REIL CABLES.
- CORE THROUGH EXISTING CAN AND CONCRETE BASE AND INSTALL 3" GRS CONDUIT WITH GROMMET FOR (2) 1" UNIT DUCTS. COST SHALL BE INCIDENTAL TO CABLE INSTALLATION.
- CONTRACTOR SHALL UPDATE ALCMS GRAPHICS TO DEPICT NEW TAXIWAY EDGE LIGHTS AND RELOCATED RGLS. COST SHALL BE INCLUDED IN LUMP SUM PRICE FOR PAY ITEM AR109210, VAULT MODIFICATIONS.



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 ELECTRICAL AND PAVEMENT MARKING PLAN**

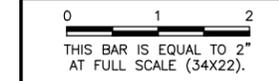
© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL PROJ. NO. MLI-4482 AIP PROJ. NO. 3-17-0068-XX	
SHEET 19 OF 27 SHEETS	

K:\Maineap\14014-07_TaxiwayConnector\Drawn\Sheets

QU020

REVISIONS		
NUMBER	BY	DATE

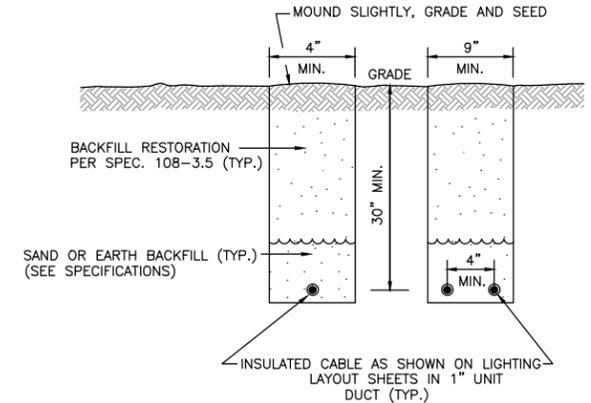


**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 ELECTRICAL DETAILS SHEET 1**

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

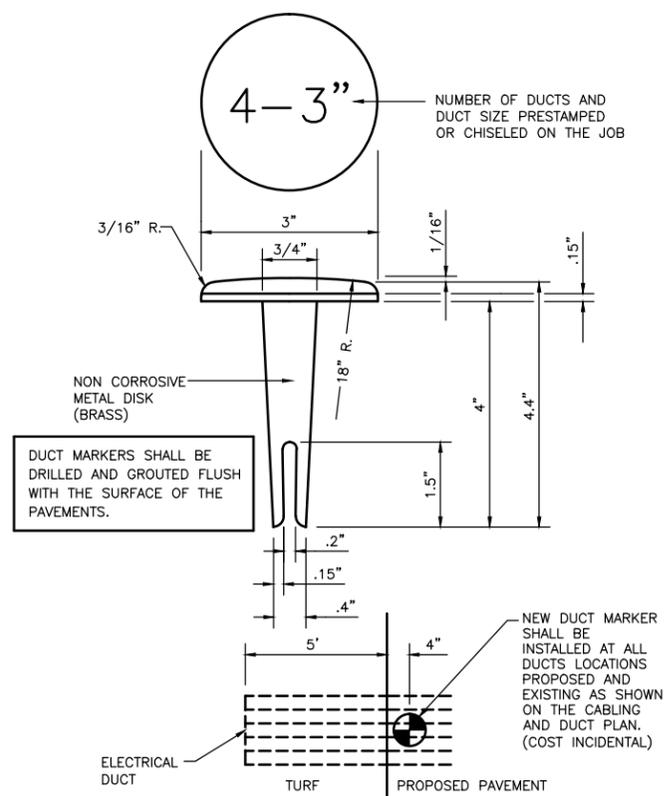
DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL PROJ. NO. MLI-4482 AIP PROJ. NO. 3-17-0068-XX	
SHEET	20 OF 27 SHEETS



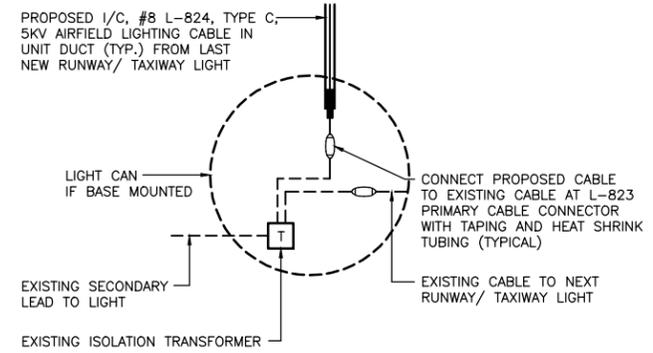
TURF AREA CABLE TRENCH DETAIL
 NOT TO SCALE

NOTES

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION, COST INCLUDED IN THE CABLE.



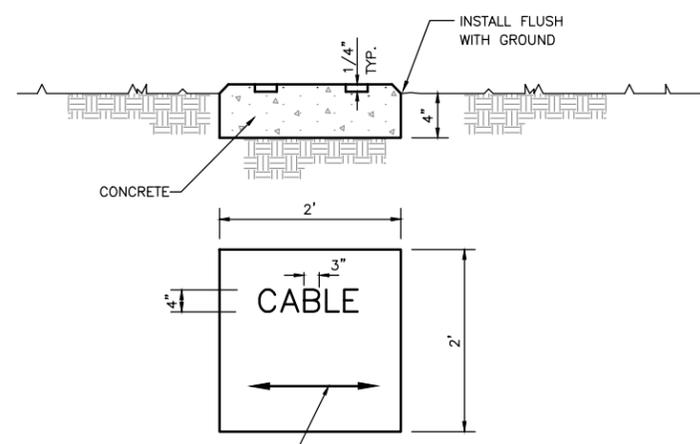
DUCT MARKER DETAIL
 NOT TO SCALE



RUNWAY/TAXIWAY LIGHTING CIRCUIT CONNECTION DETAIL
 NOT TO SCALE

GENERAL NOTES:

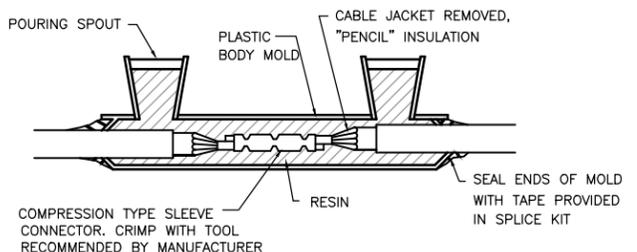
- THE CONCRETE BASE FOR BASE MTD. LIGHTS AND SIGNS SHALL BE TROWEL FINISHED WITH A 45° BEVELED EDGE. SLOPE TO DRAIN (610).
- TRANSFORMER HOLDER SHALL BE ANY COMMERCIALY AVAILABLE BRICK.
- BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
- ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/C, No. 8, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
- ALL SIGNS, LIGHTS, CABLES AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT DIRECTOR, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE.
- CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.
- ALL RUNWAY/TAXIWAY EDGE LIGHTS SHALL HAVE 2" DIA. COLUMN AND FRANGIBLE COUPLINGS, UNLESS NOTED OTHERWISE.
- INSTALL SAFETY GROUND TO EXISTING SIGNS AND L-867 CANS. ATTACH GROUND LUG TO EXISTING CAN AND INSTALL GROUND ROD AS SHOWN ON PLANS.



TURF CABLE MARKER DETAIL
 NOT TO SCALE

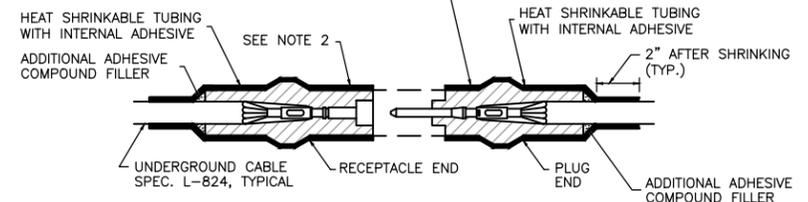
NOTES:

- CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE CABLE RUN.
- ITEM 610 CONCRETE SHALL BE USED.
- ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
- THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- 0.049 CU. YD. CONCRETE PER MARKER.



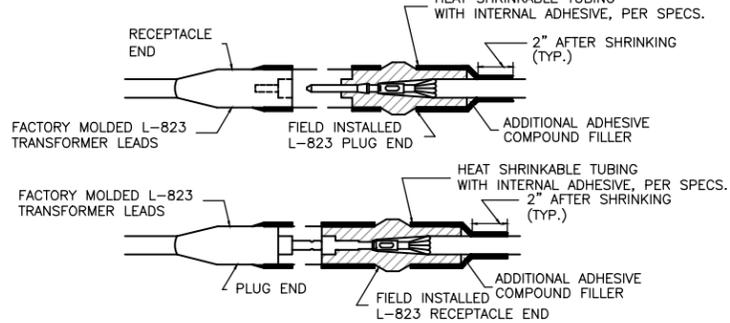
TYPE A - CABLE SPLICE

FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY
 N.T.S.



TYPE B - CABLE SPLICE

FOR SPLICES FOR USE AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT
 N.T.S.



TYPE C AND D - CABLE SPLICE

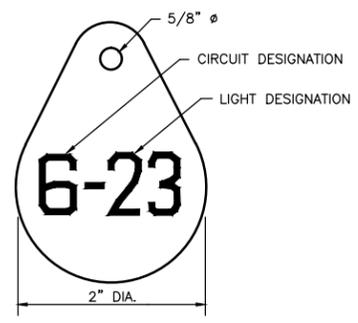
FOR SPLICES AT RUNWAY/TAXIWAY LIGHTS AND SIGNS
 N.T.S.

NOTES

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.
- THE CONTRACTOR MAY ELECT TO USE AN FAA APPROVED PRIMARY CONNECTOR KIT OR COMPLETE KIT IN LIEU OF HEAT SHRINK KIT AT NO ADDITIONAL COST.

NOTES:

- INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
- NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIWAY AND RUNWAY LIGHTS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (NEW OR RELOCATED LIGHTS) SHALL BE RETAGGED.
- COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

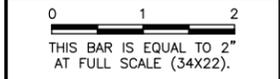


LIGHT IDENTIFICATION DETAIL
 NOT TO SCALE

K:\Maineap\14014-07_TaxiwayConnector\Drawn\Sheets

QU020

REVISIONS		
NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

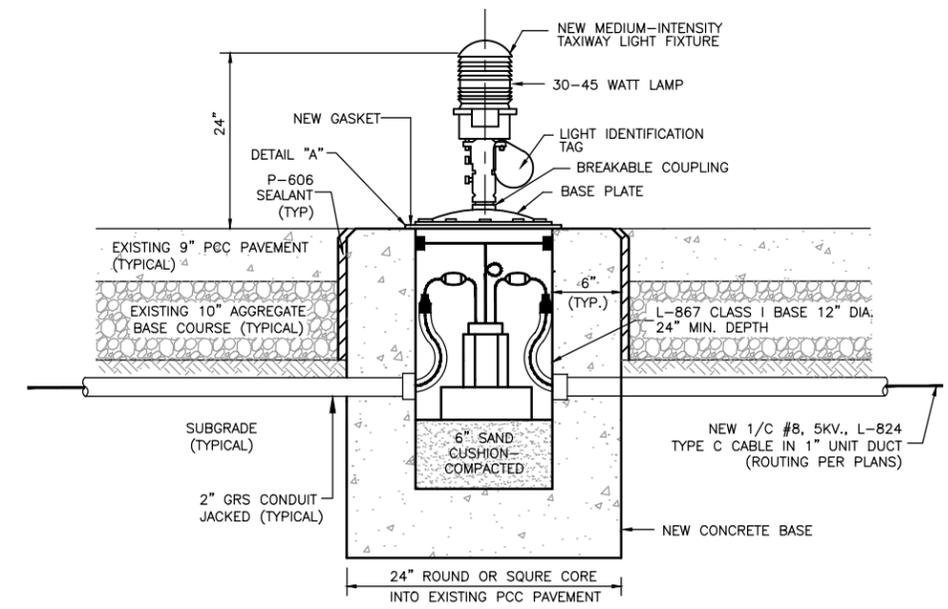
**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 ELECTRICAL DETAILS SHEET 2**

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00

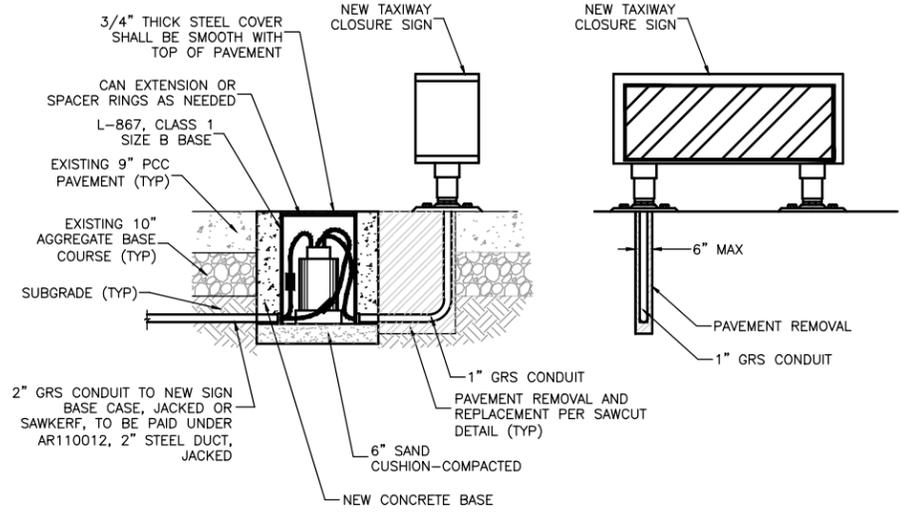
IL PROJ. NO. MLI-4482
 AIP PROJ. NO. 3-17-0068-XX



**NEW BASE MOUNTED TAXIWAY LIGHT
 CORED INTO EXISTING PCC PAVEMENT**
 NOT TO SCALE

INSTALLATION IN EXISTING PCC PAVEMENT NOTES

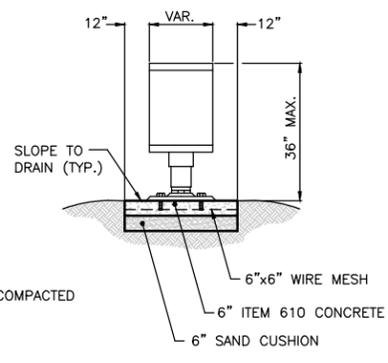
- CORE THROUGH EXISTING PAVEMENT. EXCAVATE TO PROPER DEPTH TO ALLOW 6" CONCRETE ENCASEMENT UNDER NEW BASES AND 6" UNDER NEW CONDUIT. CLEAN CUT EDGES AND COMPACT BOTTOM OF EXCAVATION.
- USE MANUFACTURERS SETTING JIG (OR OTHER DEVICE APPROVED BY THE ENGINEER FOR PROPERLY ALIGNING NEW L-868 BASES. SECURE SETTING JIG TO PREVENT MOVEMENT DURING CONCRETE ENCASEMENT. ALL CONDUITS TO BE SUPPORTED DURING CONSTRUCTION AS REQUIRED.
- ALL LIGHT BASES SHALL BE PROPERLY POSITIONED AND ALIGNED AND CONDUIT CONNECTING THE BASES PROPERLY SECURED IN PLACE BEFORE POURING CONCRETE. ENSURE PROPER ALIGNMENT AFTER CONCRETE ENCASEMENT OF NEW BASE BEFORE CONCRETE SETS. TIGHT CONNECTIONS MUST BE ASSURED TO PREVENT CONCRETE FROM ENTERING BASE OR CONDUIT.
- AFTER FIXTURE INSTALLATION, FILL THE ANNULAR SPACE BETWEEN THE FIXTURE BASE AND SURROUNDING PAVEMENT WITH P-606 SEALANT. ANNULAR SPACE SHALL BE NO MORE THAN 3/4" WIDE.
- INSTALL BASE AND RINGS SO THAT OUTER EDGE OF BASE PLATE WILL BE AT THE SAME ELEVATION AS THE EXISTING PAVEMENT SURFACE TO (+) 0 (-) 1/16" TOLERANCE. THE TOTAL THICKNESS OF THE SPACER/FLANGE RINGS SHALL BE NO LARGER THAN 3/4" MAX. NO MORE THAN 3 RINGS SHALL BE USED.
- ALL BOLTS SHALL BE TREATED WITH APPROVED ANTI-SEIZING COMPOUND.



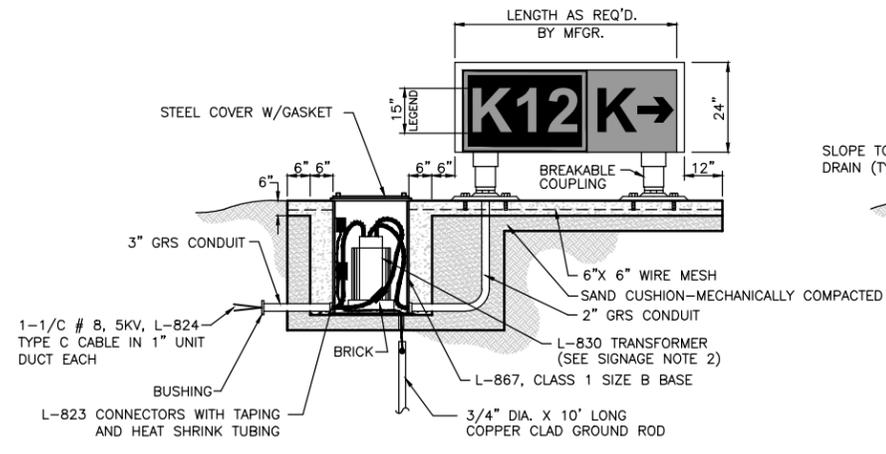
NEW LED AIRFIELD GUIDANCE SIGN ON EXISTING PCC PAVEMENT
 NOT TO SCALE

SIGNAGE NOTES

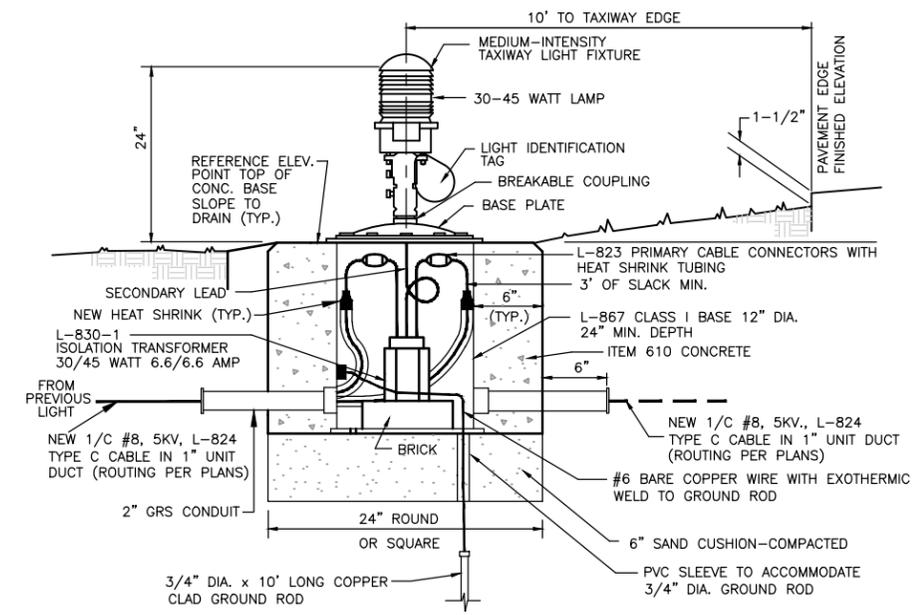
- ALL SIGNS ARE 2-SIDED LED SIGNS, STYLE 2 AND CLASS 2 AS MANUFACTURED BY ADB OR APPROVED EQUAL.
- TRANSFORMER WATTAGE AS RECOMMENDED BY MANUFACTURER FOR LED SIGNS.
- LIGHTED SIGNS SHALL BE BASE MOUNTED ONLY.
- THE NUMBER OF MODULES PER SIGN SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
- THE CONTRACTOR SHALL SUBMIT DETAILED SHOP DRAWINGS, INCLUDING SIGN, COLOR, SIZE AND PROPOSED LEGEND, IN ENOUGH DETAIL TO DETERMINE PROPOSED SPACING AND OTHER INFORMATION REQUIRED BY THE SPECIAL PROVISIONS. THE CONTRACTOR SHALL VERIFY PROPOSED SIGN LOCATIONS AND ORIENTATIONS WITH THE RESIDENT ENGINEER PRIOR TO INSTALLATION.
- NEW SIGNS SHALL BE PROVIDED WITH THE NUMBER OF MODULES REQUIRED TO ACCOMMODATE THE PROPOSED FUTURE LEGEND AS NOTED IN THE SIGNAGE SCHEDULE.
- TYPICAL DISTANCES FOR RUNWAY HOLD SIGNS FROM RUNWAY CENTERLINE ARE 256' FOR RUNWAY 13/31.
- ALL SIGNS CONNECTED TO HIGH INTENSITY RUNWAY CIRCUIT (5-STEP) SHALL BE STYLE 3 (5-STEP).



DETAIL 'A'
 NOT TO SCALE



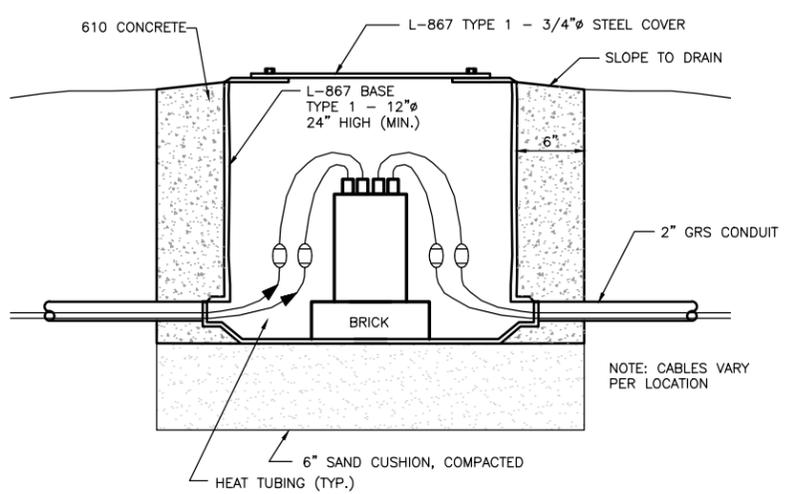
NEW LED AIRFIELD GUIDANCE SIGN L-858
 NOT TO SCALE



NEW BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
 NOT TO SCALE - AR125415

GENERAL NOTES

- TRANSFORMER HOLDER MAY BE ANY COMMERCIALY AVAILABLE BRICK.
- BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
- ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/C, No. 8, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
- ALL LIGHTS, CABLES AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE.
- DUCT MARKERS SHALL BE INSTALLED AT EVERY NEW DUCT AND AT EVERY EXISTING DUCT USED FOR THIS PROJECT.
- AT THE CONTRACTOR'S OPTION, IN LIEU OF TAPE AND HEAT SHRINKABLE TUBING, A SELF-SEALING STYLE L-823 "COMPLETE KITS" OR FAA APPROVED EQUAL.

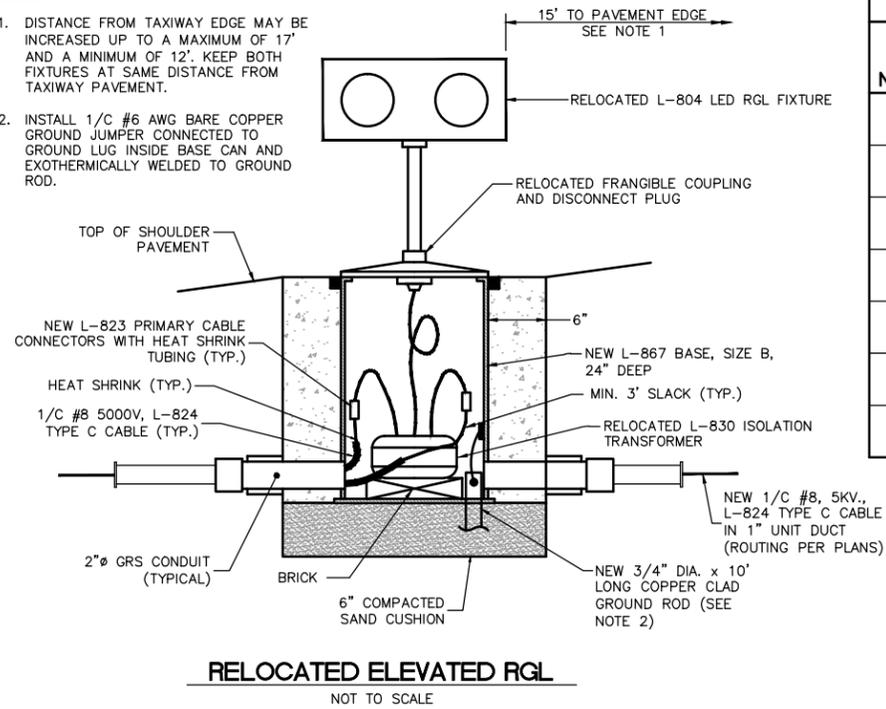


AIRFIELD SPLICE CAN
 NOT TO SCALE

K:\Maine\14014-07_TaxiwayConnector\Drawn\Sheets

NOTES:

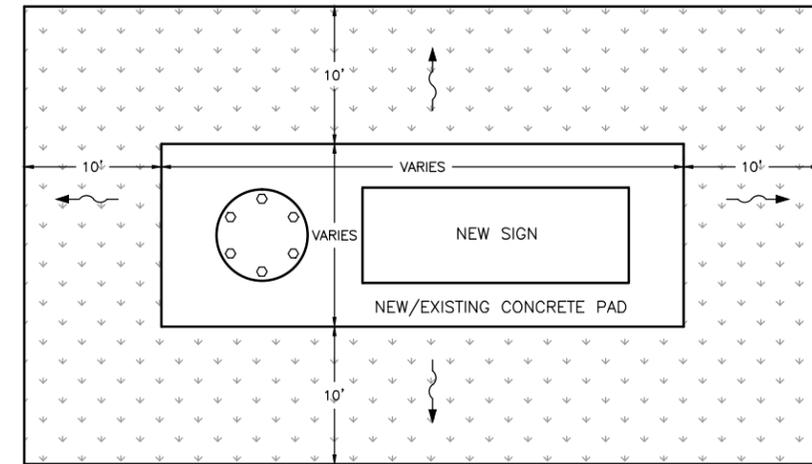
- DISTANCE FROM TAXIWAY EDGE MAY BE INCREASED UP TO A MAXIMUM OF 17' AND A MINIMUM OF 12'. KEEP BOTH FIXTURES AT SAME DISTANCE FROM TAXIWAY PAVEMENT.
- INSTALL 1/C #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN AND EXOTHERMICALLY WELDED TO GROUND ROD.



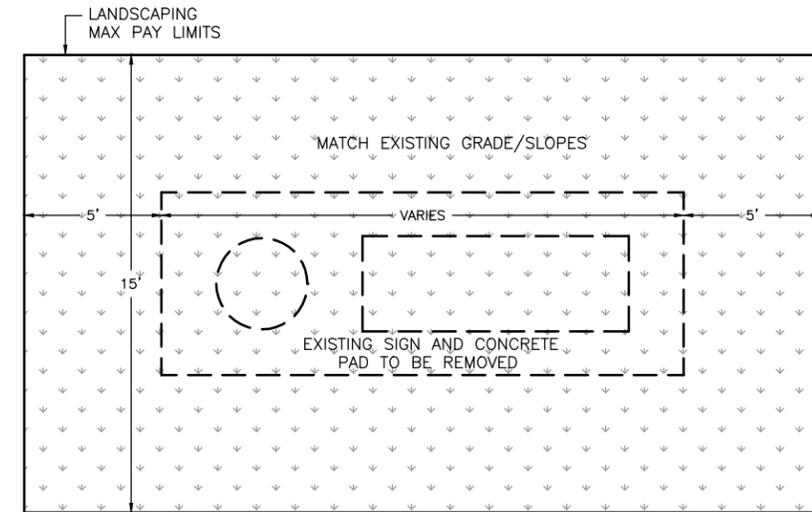
AIRFIELD SIGNAGE SCHEDULE					
SIGN NUMBER	SIDE	EXISTING SIGN	FINAL MESSAGE	SIGN LOCATION	COMMENTS
K-1	SE NW	-		STA. 379+34.20 CL TAXIWAY K12	5 CHARACTER
K-2	SE NW	-		STA. 381+33.24 CL TAXIWAY K12	5 CHARACTER
31-1	NE SW	-		STA. 379+90.19 CL TAXIWAY K12	5 CHARACTER
31-2	SE NW	-		STA. 379+87.19 CL TAXIWAY K12	4 CHARACTER
31-3	NW SE	-		STA. 381+49.19 CL TAXIWAY K12	4 CHARACTER
31-4	NW SE			-	REPLACE EXISTING SIGN PANEL
31-5	SW NE	-		-	5 CHARACTER

SIGN COLOR DESCRIPTION

- BLACK ON YELLOW BACKGROUND
- YELLOW INSCRIPTION ON BLACK BACKGROUND
- WHITE INSCRIPTION WITH BLACK BORDER ON RED BACKGROUND
- BLANK PANEL

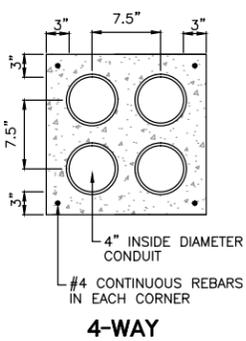


TYPICAL LANDSCAPING FOR AR12544X / AR80013X
 NEW SIGN / NEW SIGN ON EXISTING PAD
 NOT TO SCALE

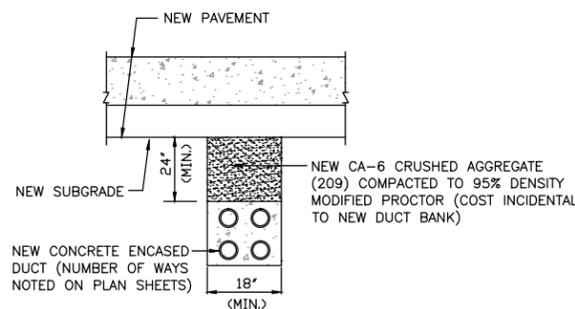


TYPICAL LANDSCAPING FOR AR125904 - REMOVE TAXI GUIDANCE SIGN
 NOT TO SCALE

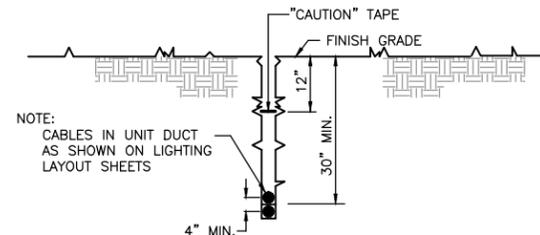
-



CONCRETE ENCASED DUCT BANKS
 NOT TO SCALE



CONCRETE ENCASED DUCT BACKFILL
 NOT TO SCALE

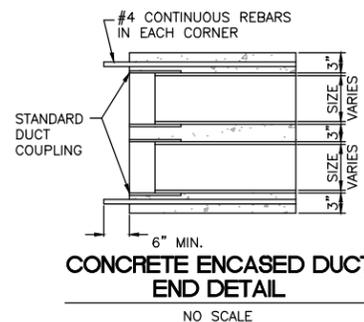


CABLE IN UNIT DUCT - PLOWED
 NOT TO SCALE

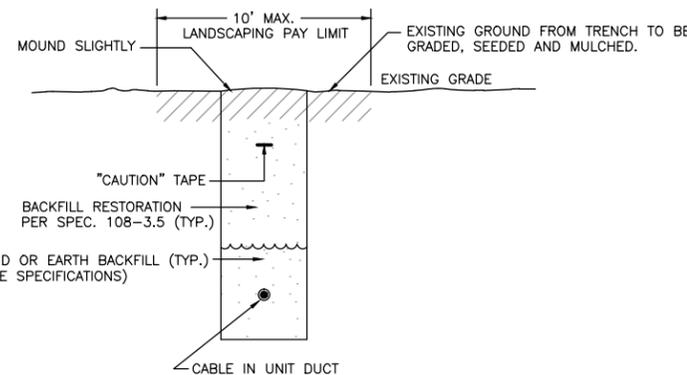
CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.

CONCRETE ENCASED DUCT NOTES:

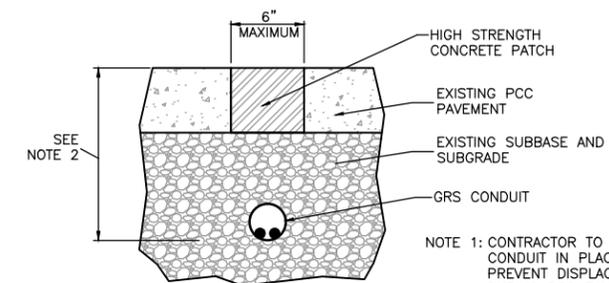
- DIMENSIONS ARE MINIMUM.
- CONCRETE SHALL CONFORM TO ITEM 610.
- ALL CONDUIT SHALL BE SCHEDULE 40 PVC.
- TOP OF CONCRETE ENCASEMENT IN TURF AREAS SHALL NOT BE LESS THAN 24" BELOW FINISHED GRADE.



CONCRETE ENCASED DUCT END DETAIL
 NO SCALE



TYPICAL LANDSCAPING CABLE TRENCH DETAIL
 NOT TO SCALE



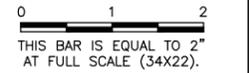
SAWCUT DETAIL
 NOT TO SCALE

- NOTE 1: CONTRACTOR TO SECURE CONDUIT IN PLACE TO PREVENT DISPLACEMENT DURING PCC PAVING
- NOTE 2: DEPTH OF SAWCUT AND PAVEMENT REMOVAL AS NEEDED TO ALIGN WITH CONDUIT HUB

QU020

REVISIONS

NUMBER	BY	DATE



METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS

CONSTRUCT TAXIWAY K CONNECTOR (K12)
ELECTRICAL DETAILS SHEET 3

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

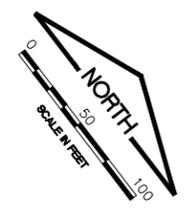
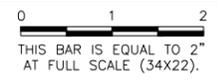


DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00

IL PROJ. NO. MLI-4482
 AIP PROJ. NO. 3-17-0068-XX

QU020

REVISIONS		
NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 INDEX TO CROSS SECTIONS AND
 EARTHWORK SUMMARY**

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



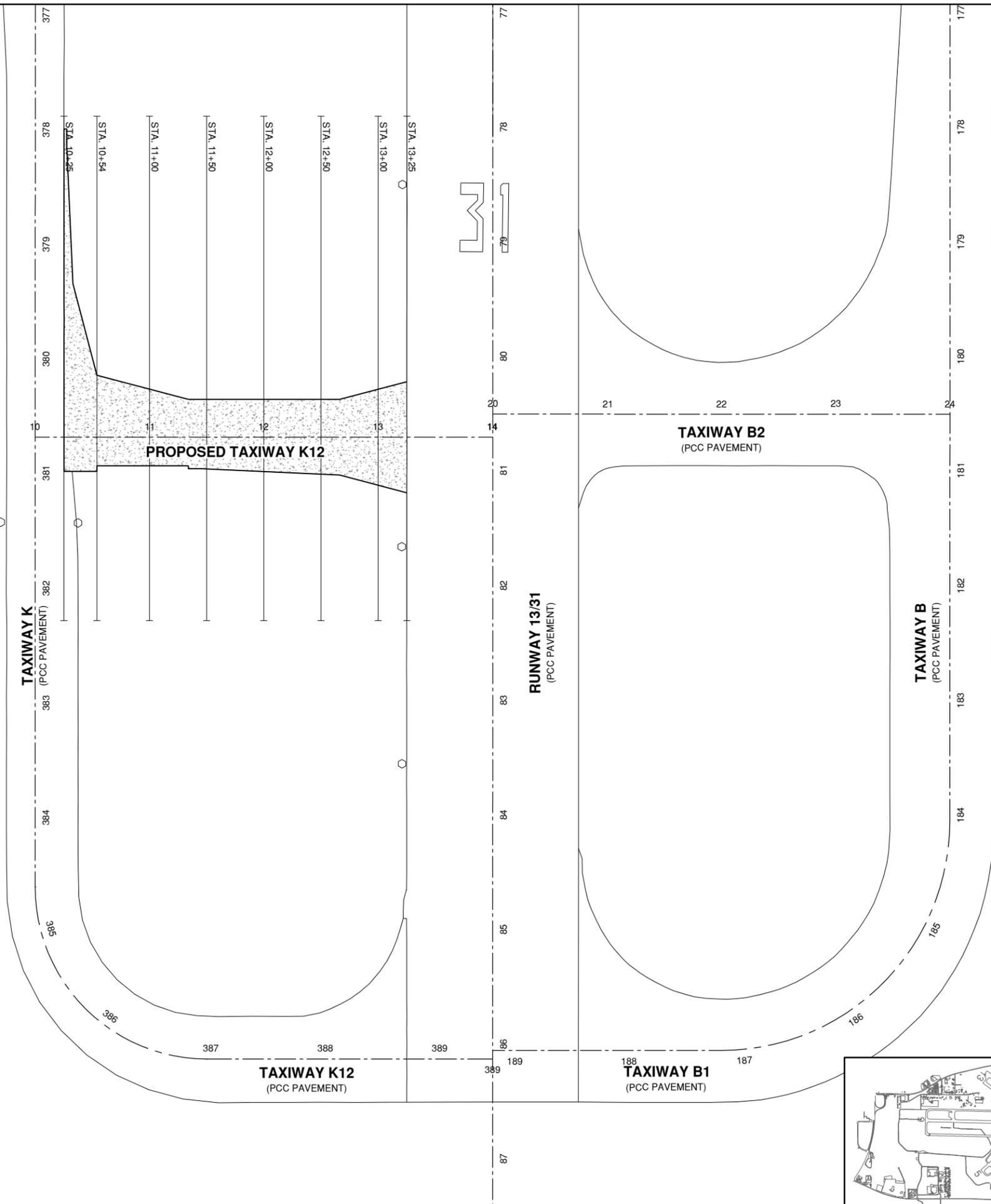
DESIGN BY:	SMS
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL PROJ. NO.	MLI-4482
AIP PROJ. NO.	3-17-0068-XX
SHEET	24 OF 27 SHEETS

LEGEND

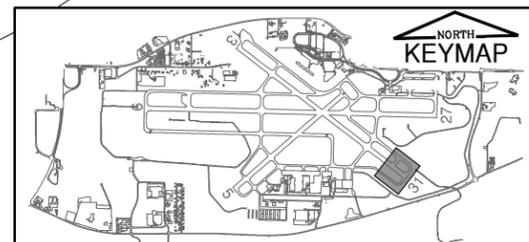


NOTES

- ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON THE MATERIAL IN ITS INITIAL OR FINAL POSITION AS SHOWN IN THE PLANS AND QUANTIFIED BY THE METHOD OF AVERAGE END AREAS.
- AREAS OF UNSUITABLE MATERIAL/UNDERCUT AREAS (PAID AS AR152410) SHALL BE AS DESIGNATED BY THE ENGINEER. THE QUANTITY OF UNSUITABLE MATERIAL/UNDERCUT AREAS SHALL NOT BE USED AS EMBANKMENT FILL MATERIAL UNLESS AUTHORIZED BY THE ENGINEER.
- PAYMENT FOR UNCLASSIFIED EXCAVATION IS THE SUM OF TOPSOIL STRIPPING, UNCLASSIFIED EXCAVATION AND UNDERCUT AREAS AND SHALL BE PAID FOR UNDER ITEM NO. AR152410.
- ALL HAUL ROADS TO BE CONSTRUCTED FOR THE PROJECT WILL NOT BE MEASURED FOR PAYMENT BUT SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- ANY CONTRACTOR HAUL ROADS TO THE SITE SHALL BE RESTORED WITH 4" MINIMUM OF TOPSOIL PLACED. ALL HAUL ROAD RESTORATION SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
- TOPSOIL PLACEMENT AND SHOULDER FILL SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE FOR TOPSOIL STRIPPING (ITEM AR152410). NO SEPARATE PAYMENT WILL BE MADE FOR TOPSOIL PLACEMENT AND SHOULDER FILL.
- ANY EXCESS MATERIAL INCLUDING EMBANKMENT FILL MATERIAL AND TOPSOIL SHALL BE HAULED OFF AND DISPOSED OF BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
- A 15% SHRINKAGE FACTOR WAS USED TO DETERMINE THE REQUIRED FILL IN ITS INITIAL POSITION. THE DIFFERENCE BETWEEN THE REQUIRED FILL AND REQUIRED CUT QUANTITY WAS USED TO DETERMINE THE NUMBER OF CUBIC YARDS OF MATERIAL TO BE DISPOSED OF OFF AIRPORT PROPERTY. NO ADJUSTMENTS IN EARTHWORK QUANTITIES WILL BE ALLOWED FOR VARIATIONS IN ACTUAL SHRINKAGE AND OR EXPANSION FACTORS ENCOUNTERED DURING CONSTRUCTION.
- ANY TESTING AND HANDLING REQUIRED BY THE CONTRACTOR AND/OR CONTRACTOR'S DISPOSAL FACILITY FOR COMPLIANCE WITH CCDD REGULATIONS SHALL BE COMPLETED AT THE CONTRACTOR'S EXPENSE. IF THE CONTRACTOR ENCOUNTERS ANY SOIL FROM THIS SITE/PROJECT THAT IS POTENTIALLY CONTAMINATED, THE ENGINEER AND OWNER SHALL BE NOTIFIED PRIOR TO HAULING THE POTENTIALLY CONTAMINATED SOIL OFF SITE.
- TOPSOIL SHALL NOT BE USED FOR EMBANKMENT UNDER NEW PAVEMENT OR WITHIN THE LIMITS OF THE TAXIWAY SAFETY AREA (TSA).
- CONTRACTOR SHALL ENSURE THAT A 4" THICK LAYER OF TOPSOIL CAN BE PLACED OVER THE ENTIRETY OF THE DISTURBANCE LIMITS. IN SOME CASES, IT MAY BE NECESSARY TO OVEREXCAVATE IN AREAS IN ORDER TO PROVIDE THE REQUIRED THICKNESS OF TOPSOILING. THE QUANTITY FOR AR152410, UNCLASSIFIED EXCAVATION, INCLUDES THE QUANTITY OF OVEREXCAVATION NEEDED.



EARTHWORK SUMMARY TABLE (SEE NOTE)	TOTAL QUANTITY	UNIT
UNCLASSIFIED EXCAVATION (NON-TOPSOIL SUBGRADE)(INITIAL POSITION)	649	CY
TOPSOIL STRIPPING - 12" (INITIAL POSITION)	2,444	CY
TOTAL CUT (UNCLASSIFIED EXCAVATION AR152410)	3,093	CY
EMBANKMENT FILL (UNDER PAVEMENT AND WITHIN TSA)(FINAL POSITION)	628	CY
NON-STRUCTURAL FILL (OUTSIDE TSA AND TOPSOILING)(FINAL POSITION)	1,326	CY
TOTAL FILL	1,954	CY
TOTAL EXCESS MATERIAL (TO BE HAULED OFF)	1,954	CY



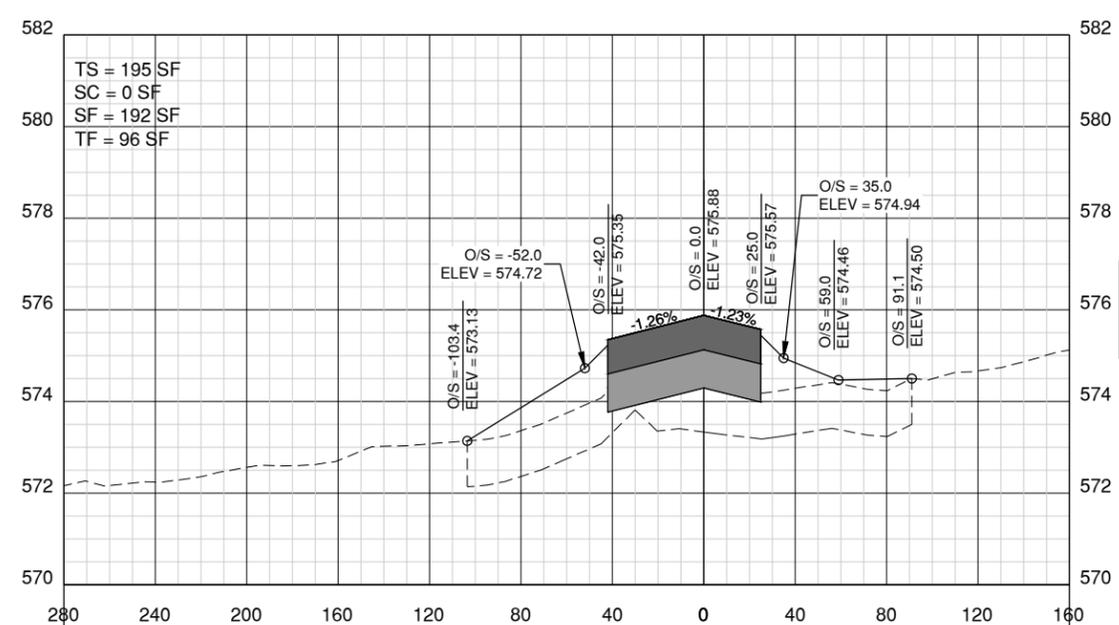
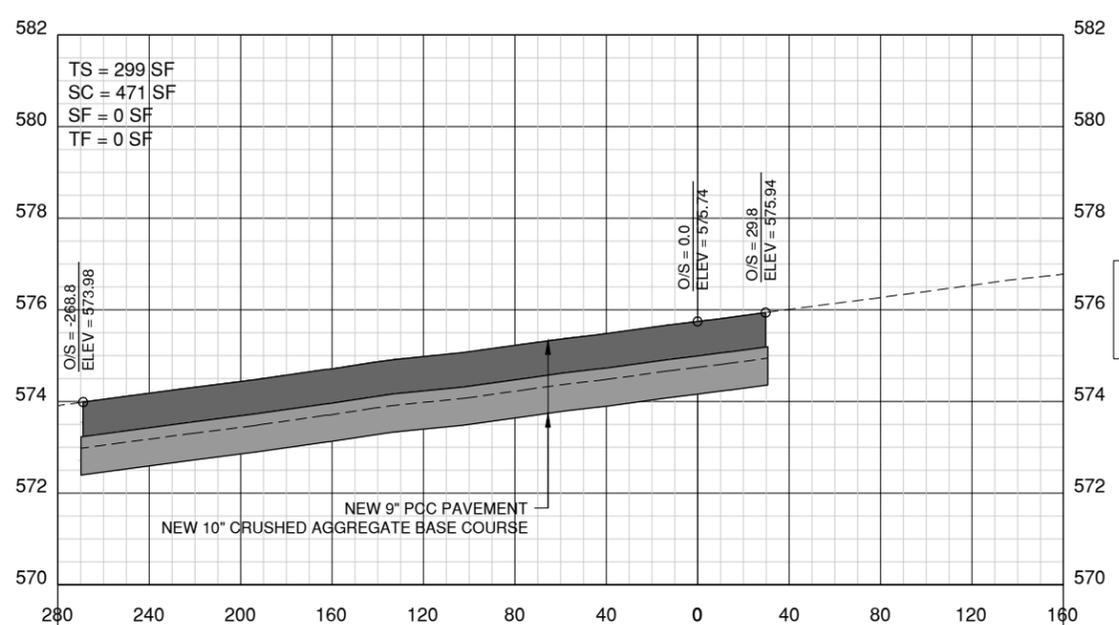
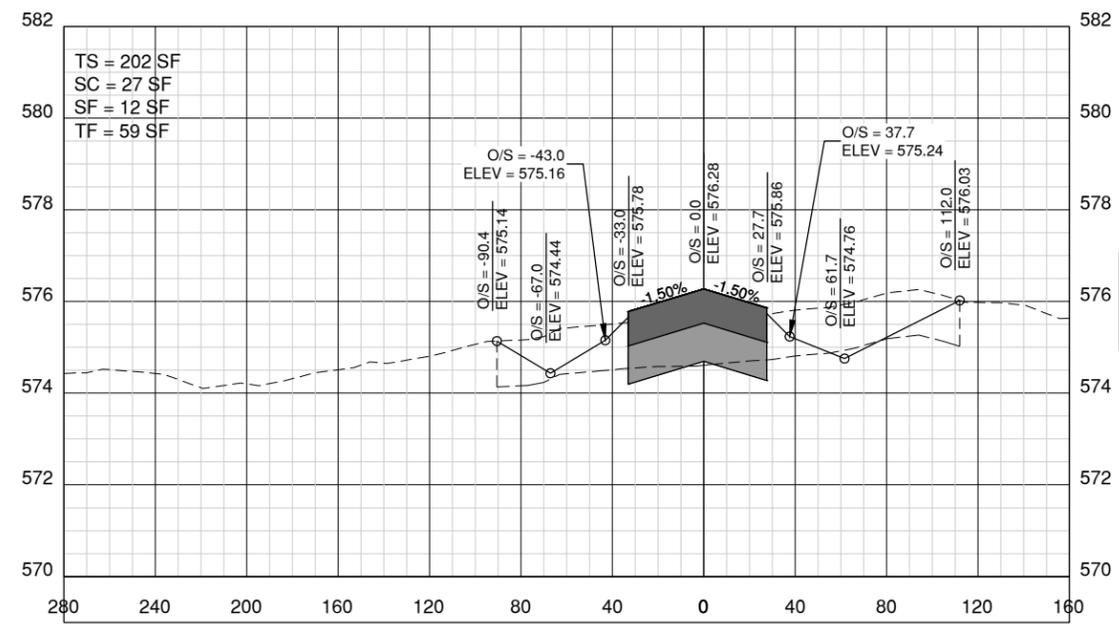
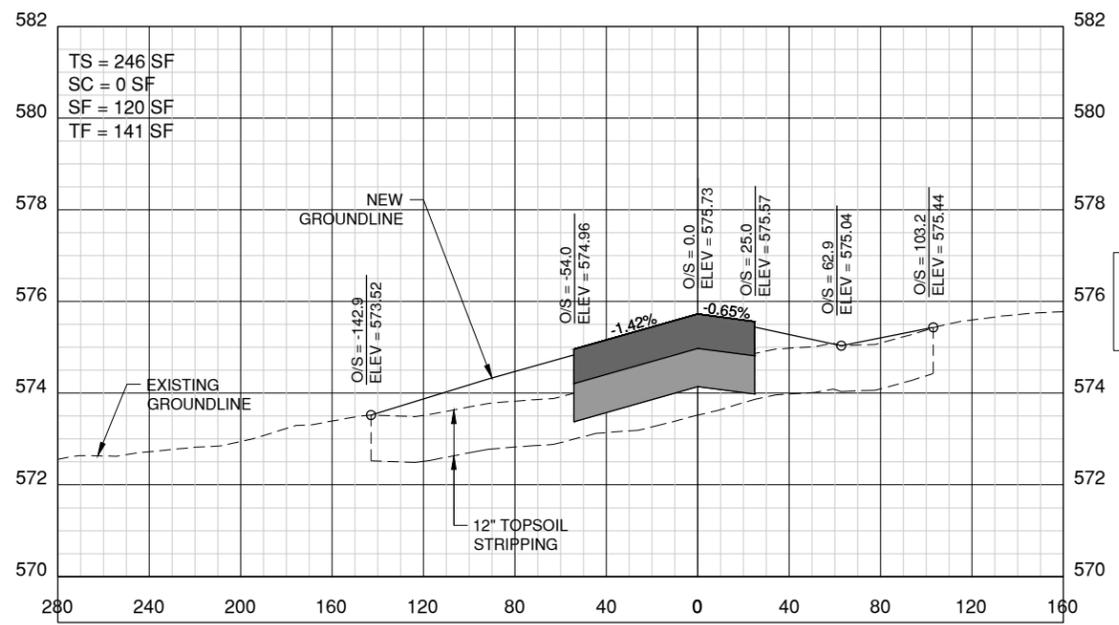
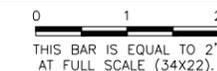
K:\Moline\14014-07_TaxiwayConnector\Draw\Sheets

TS = TOPSOIL STRIPPING
 SC = STRUCTURAL CUT
 SF = STRUCTURAL (EMBANKMENT) FILL
 TF = TOPSOIL FILL

FILE: 25 XSECT 1.dwg
 UPDATE BY: Sean Smith
 PLOT DATE: 6/26/2015 3:55 PM
 14014-07-C-XSEC

QU020

REVISIONS		
NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT TAXIWAY K CONNECTOR (K12)
 CROSS SECTIONS SHEET 1**

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	SMS
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL PROJ. NO.	MLI-4482
AIP PROJ. NO.	3-17-0068-XX
SHEET	25 OF 27 SHEETS

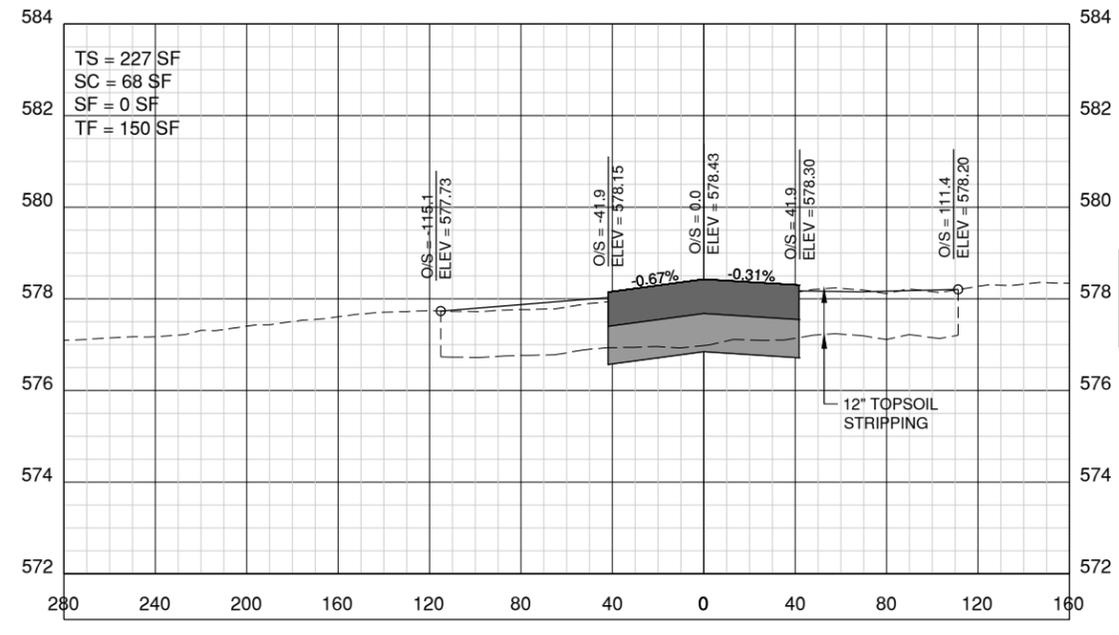
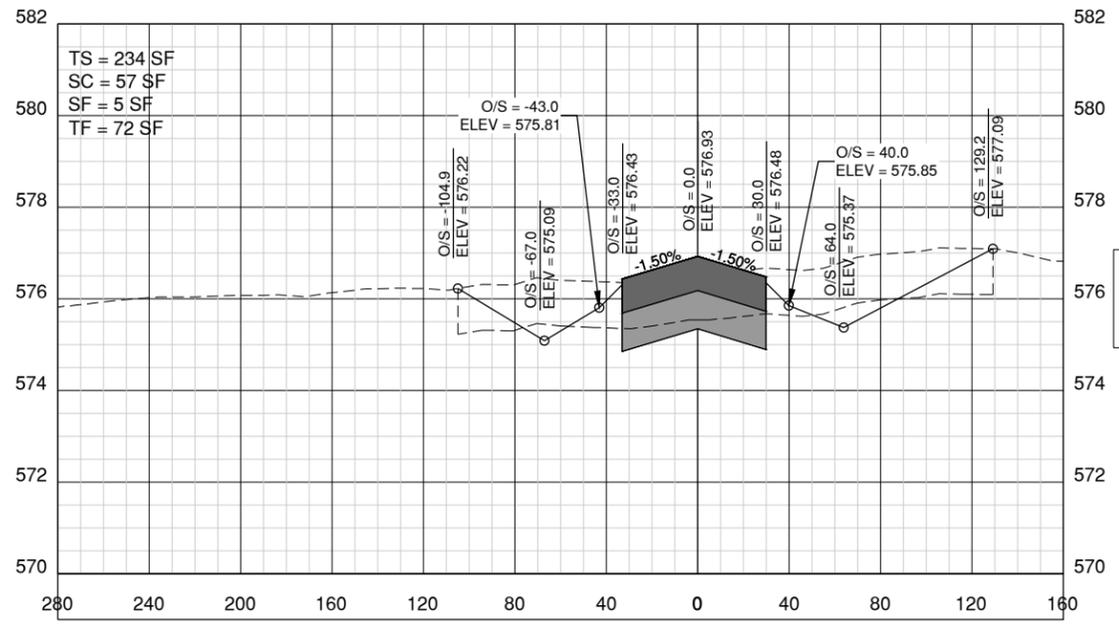
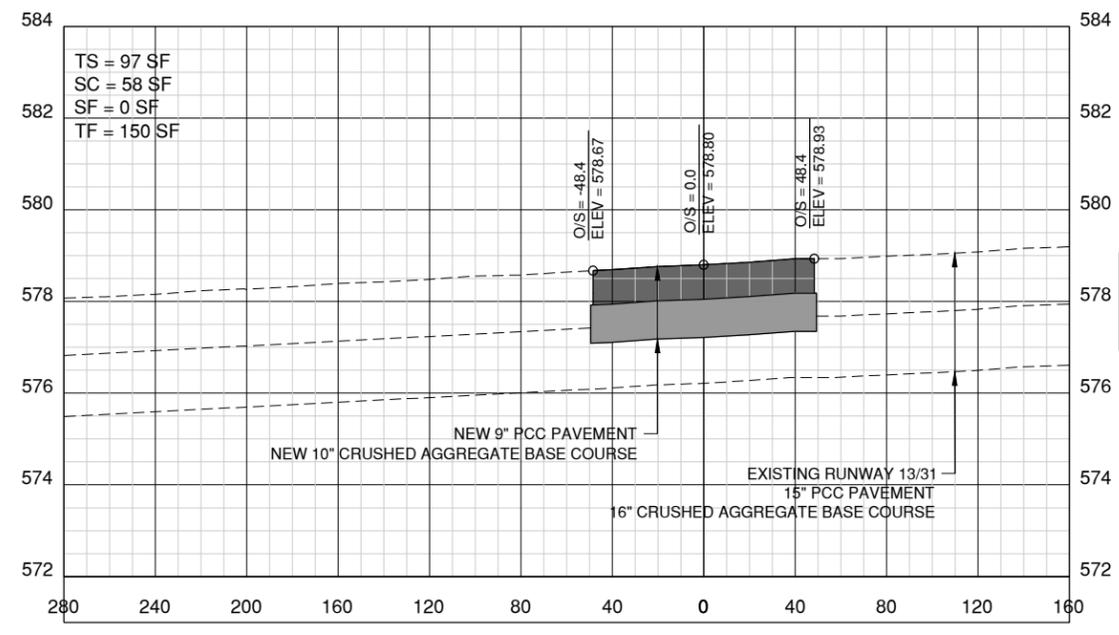
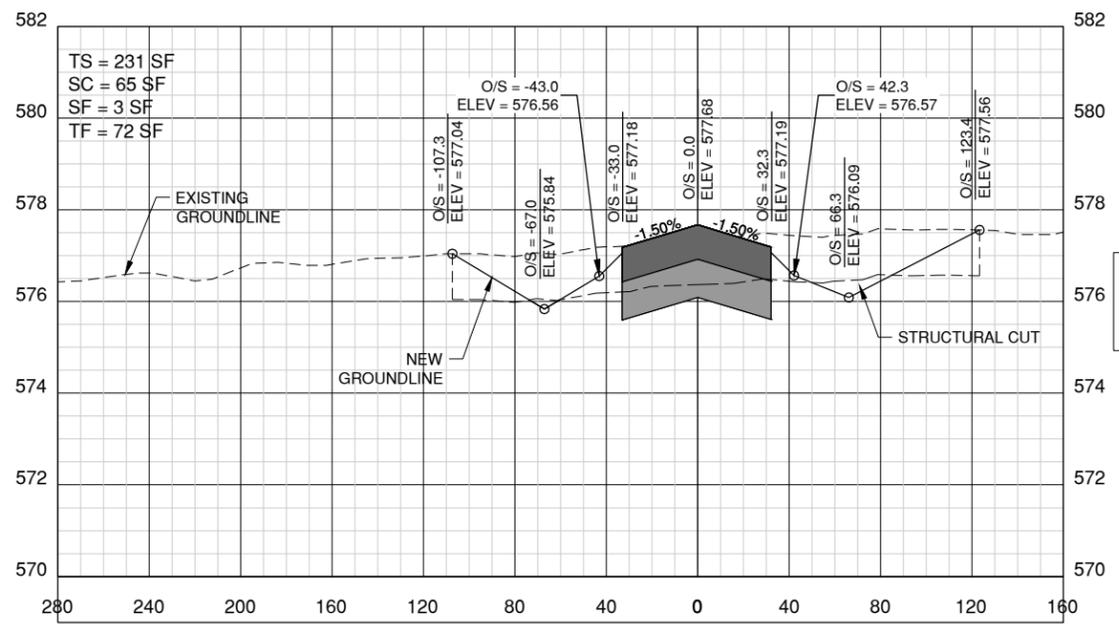
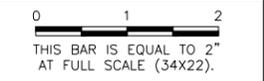
K:\Moline\14014-07_TaxiwayConnector\Draw\Sheets

TS = TOPSOIL STRIPPING
 SC = STRUCTURAL CUT
 SF = STRUCTURAL (EMBANKMENT) FILL
 TF = TOPSOIL FILL

FILE: 26 XSECT 2.dwg
 UPDATE BY: Sean Smith
 PLOT DATE: 6/26/2015 3:55 PM
 14014-07-C-XSEC

QU020

REVISIONS		
NUMBER	BY	DATE



METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS

CONSTRUCT TAXIWAY K CONNECTOR (K12)
 CROSS SECTIONS SHEET 2

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	SMS
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL PROJ. NO. MLI-4482 AIP PROJ. NO. 3-17-0068-XX	
SHEET	26 OF 27 SHEETS

K:\Moline\14014-07_TaxiwayConnector\Draw\Sheets

K:\Moineap\14014-07_TaxiwayConnector\Drawn Sheets



FILE: 27 ENG INFO 1.dwg
 UPDATE BY: Sean Smith
 PLOT DATE: 6/26/2015 3:56 PM

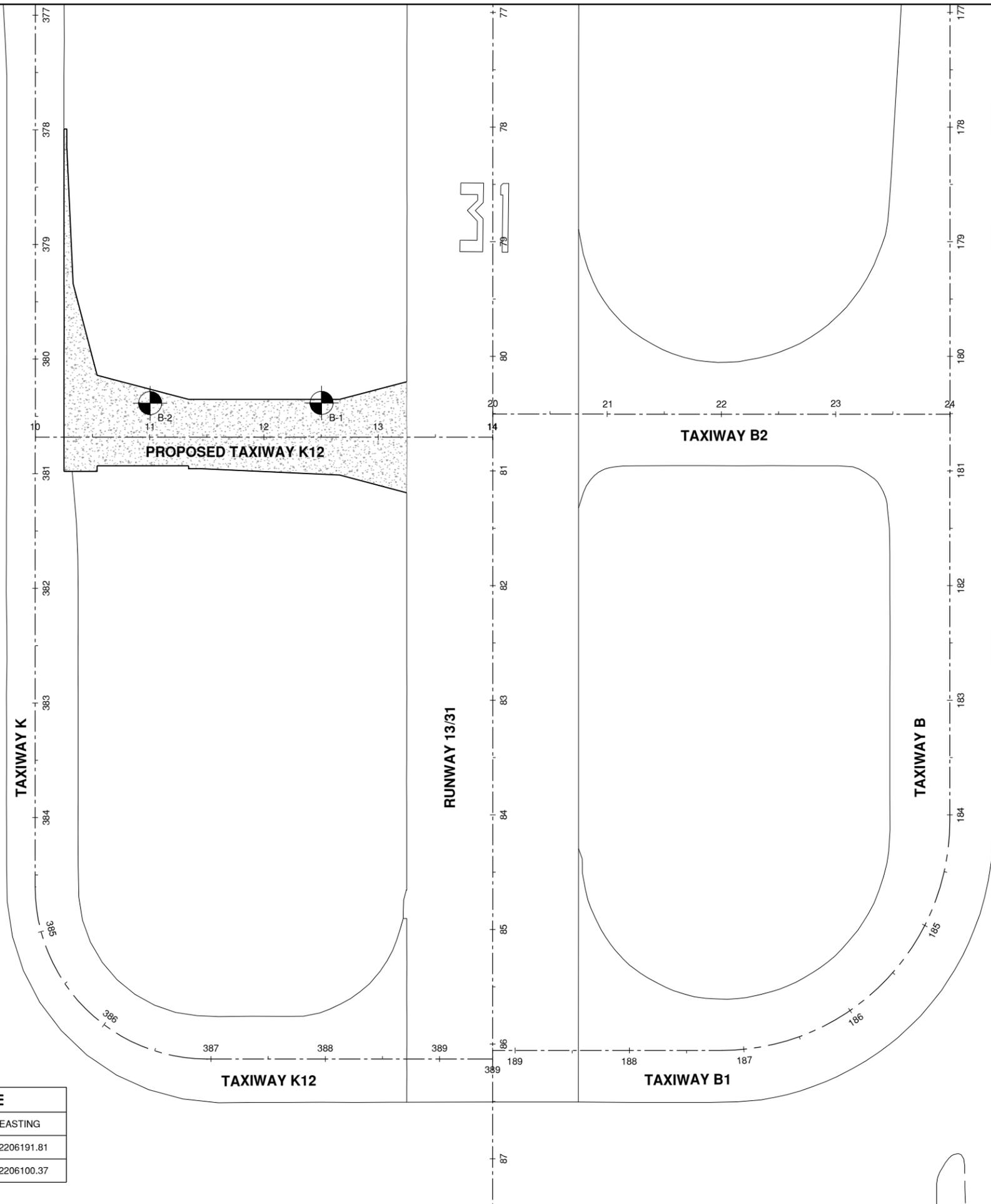
PROP-GEOMETRY
 MLI Alignments
 Taxiway K Base

QU020

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

SOUTH SERVICE ROAD



LEGEND

- NEW PCC PAVEMENT
- BORING LOCATION

NOTES

1. SEE SPECIAL PROVISIONS FOR GEOTECHNICAL ENGINEERING REPORT.

GEOTECHNICAL LAYOUT TABLE

BORING #	STATION/OFFSET	NORTHING	EASTING
B-1	12+50, 30' LT.	1739919.35	2206191.81
B-2	11+00, 30' LT.	1739800.43	2206100.37

METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS

CONSTRUCT TAXIWAY K CONNECTOR (K12)
 SOIL BORING LOCATIONS

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	SMS
APPROVED BY:	MND
DATE:	JUNE 26, 2015
JOB No:	14014-07-00
IL. PROJ. NO. MLI-4482 AIP PROJ. NO. 3-17-0068-XX	
SHEET 27 OF 27 SHEETS	