

# CONSTRUCTION PLANS

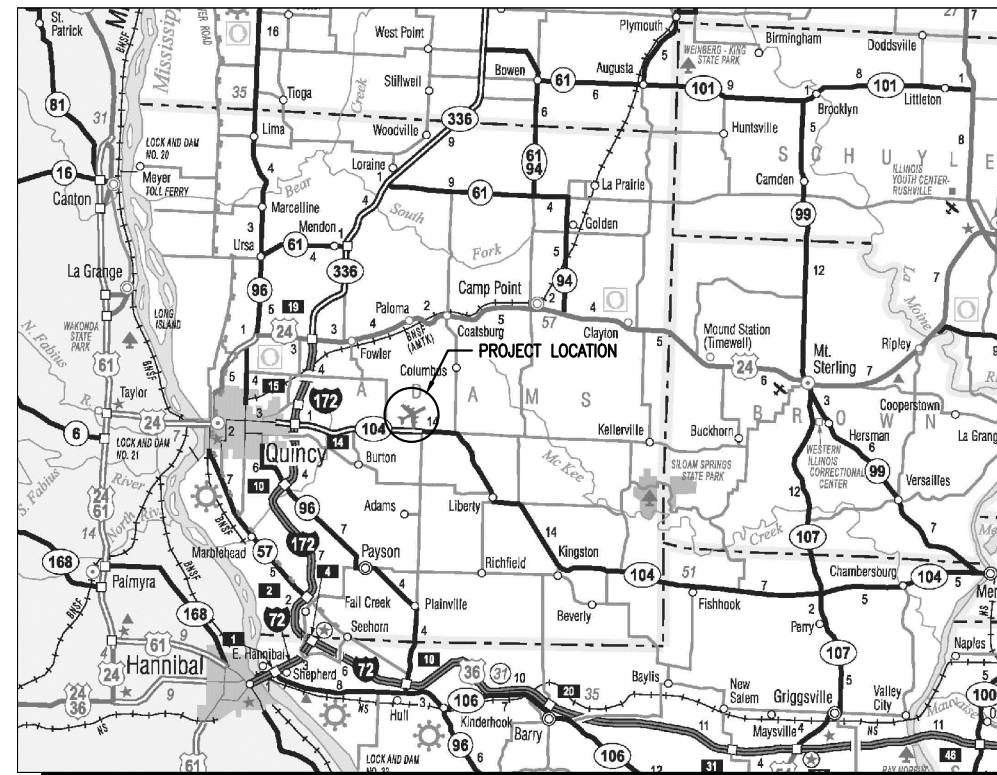
# REALIGN TAXIWAY D

**CITY OF QUINCY  
QUINCY REGIONAL AIRPORT-BALDWIN FIELD (UIN)  
QUINCY, ADAMS COUNTY, ILLINOIS**

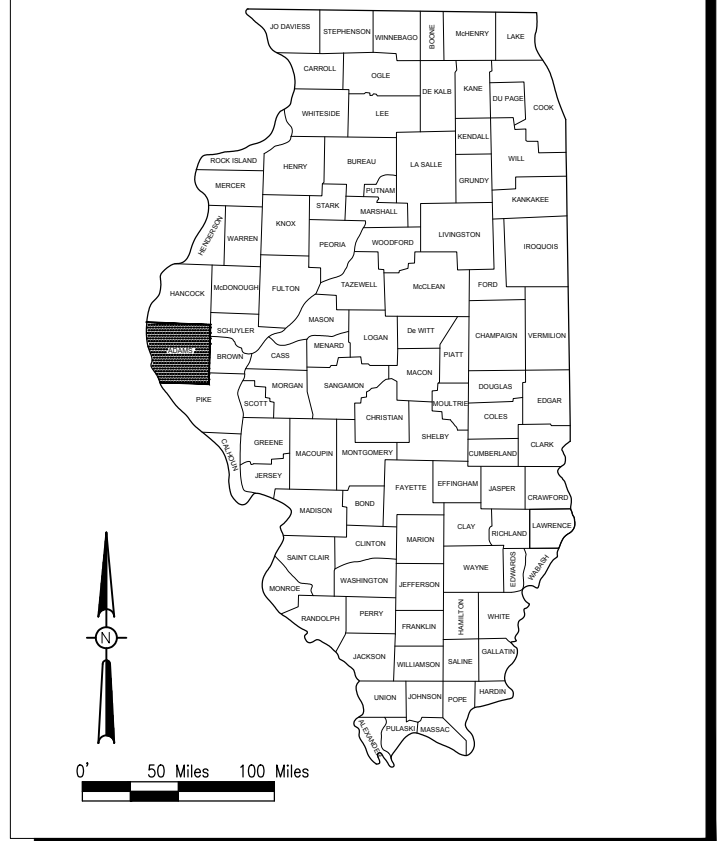
**SBG PROJECT NO. 3-17-SBGP-105  
IDA PROJECT NO. UIN-4332**

DUE TO THE ANTICIPATED TIMING OF CONTRACT AWARD AND POTENTIAL FOR INCLEMENT WEATHER, IT IS NOT EXPECTED THAT CONSTRUCTION WILL BEGIN BEFORE MAY 2016.

VICINITY MAP



LOCATION MAP



NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

No.	Issue/Description	Sheets Changed	Date	By

COVERING ELECTRICAL DESIGN

*Kevin N. Lightfoot*  
Kevin N. Lightfoot, P.E.  
Electrical Engineer

EXP. 11/30/2015

6/9/2015 Date

**HANSON**  
HANSON PROFESSIONAL SERVICES INC.  
1525 South Sixth Street  
Springfield, Illinois 62703-2886  
Telephone: 217.788.2450  
Fax: 217.788.2503

*Barry S. Stolz*  
Barry S. Stolz, P.E.  
Civil Engineer

EXP. 11/30/15

6/9/15 Date

**QUINCY Regional Airport**  
CITY OF QUINCY  
Quincy Regional Airport - Baldwin Field  
1645 Highway 104  
Quincy, Illinois 62305  
Telephone: 217.885.3285

*Jarred Hester*  
Jarred Hester  
Airport Manager

6-9-15 Date



**UTILITY NOTE**

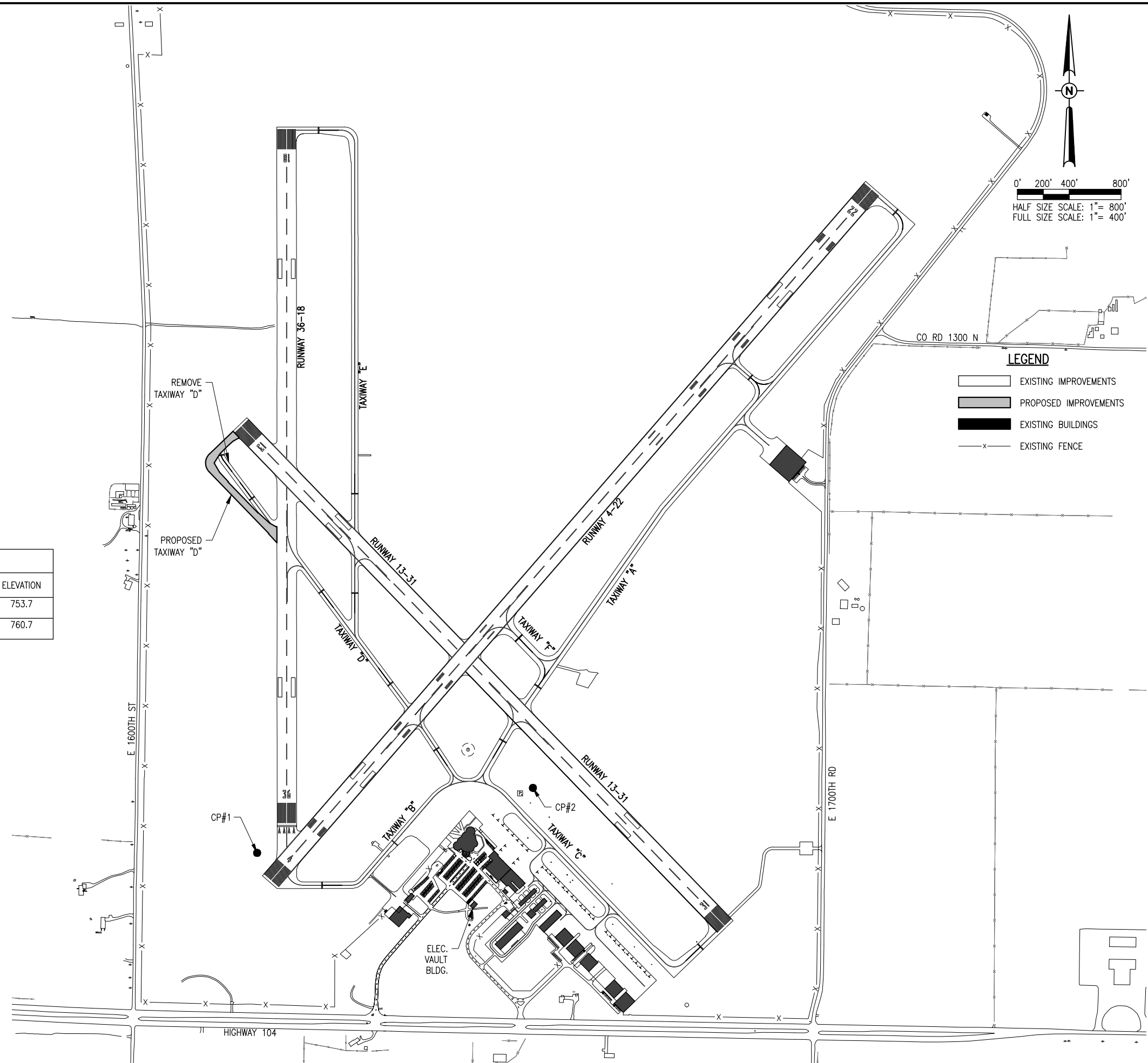
THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. **CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123.** CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

**J.U.L.I.E. INFORMATION**

COUNTY \_\_\_\_\_ ADAMS  
 CITY \_\_\_\_\_ QUINCY  
 TOWNSHIP \_\_\_\_\_ GLIMER  
 SECTION NO. \_\_\_\_\_ 34  
 ADDRESS \_\_\_\_\_ QUINCY REGIONAL AIRPORT - BALDWIN FIELD  
 1645 HIGHWAY 104  
 QUINCY, ILLINOIS 62305  
 AIRPORT OFFICE NO. \_\_\_\_\_ 217-885-3285

CONTROL POINT DATA

CP#	DESCRIPTION	NORTHING	EASTING	ELEVATION
CP#1	UIN B2	1192474.391	2006921.841	753.7
CP#2	UIN C	1192998.558	2009012.623	760.7



0' 200' 400' 800'  
 HALF SIZE SCALE: 1"= 800'  
 FULL SIZE SCALE: 1"= 400'

**LEGEND**

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- EXISTING FENCE



Engineering | Planning | Allied Services  
 Offices Nationwide  
 www.hanson-inc.com

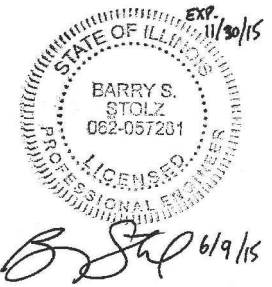
Hanson Professional Services Inc.  
 1525 S. 6th Street  
 Springfield, IL 62568  
 phone: 217-788-2450  
 fax: 217-788-2503

Illinois Licensed  
 Professional Service Corporation  
 #184-001084

Quincy Regional Airport -  
 Baldwin Field



1645 Highway 104  
 Quincy, IL 62305



**REALIGN TAXIWAY D**

IDA No: UIN-4332

SBG No: 3-17-SBGP-105

QI061

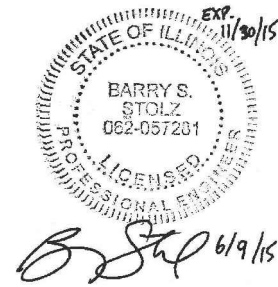

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 9, 2015  
 PROJECT NO: 13A0080D  
 CAD FILE: G-003-SOW.DWG  
 DESIGN BY: JRH 06/09/2014  
 DRAWN BY: JRH 06/09/2014  
 REVIEWED BY:

SHEET TITLE

**PROPOSED SCOPE OF WORK**





**REALIGN TAXIWAY D**

IDA No: UIN-4332

SBG No: 3-17-SBGP-105

QI061


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 9, 2015

PROJECT NO: 13A0080D

CAD FILE: C-004-SFY.DWG

DESIGN BY: JRH 06/09/2014

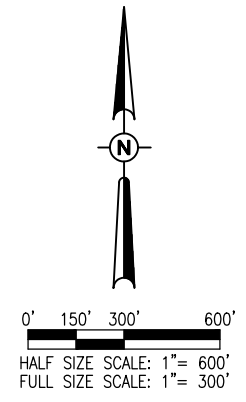
DRAWN BY: JRH 06/09/2014

REVIEWED BY: JDW 11/12/2014

SHEET TITLE

**CONSTRUCTION  
SAFETY AND  
PHASING PLAN-  
WORK AREA 1**

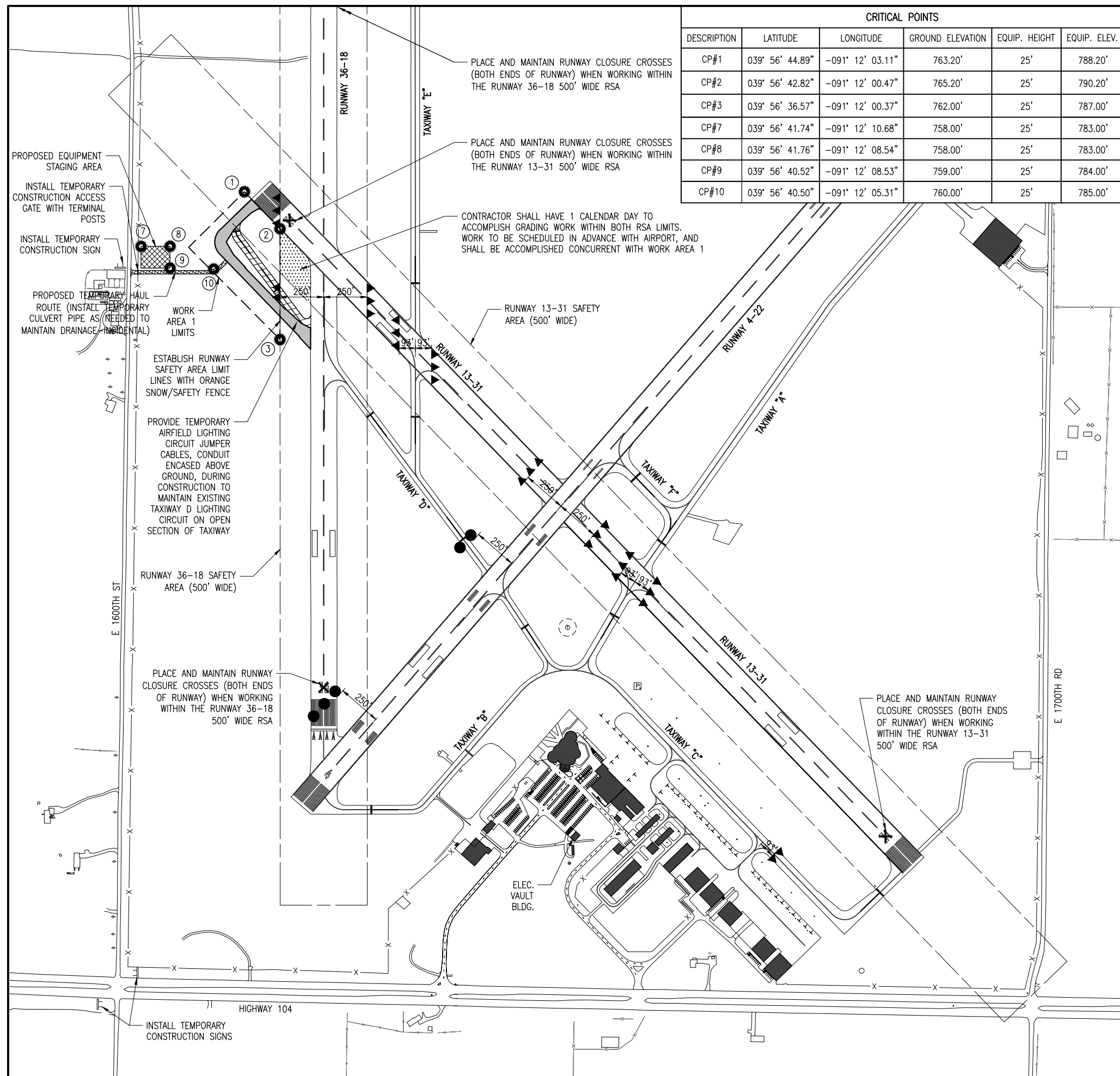
CRITICAL POINTS					
DESCRIPTION	LATITUDE	LONGITUDE	GROUND ELEVATION	EQUIP. HEIGHT	EQUIP. ELEV.
CP#1	039° 56' 44.89"	-091° 12' 03.11"	763.20'	25'	788.20'
CP#2	039° 56' 42.82"	-091° 12' 00.47"	765.20'	25'	790.20'
CP#3	039° 56' 36.57"	-091° 12' 00.37"	762.00'	25'	787.00'
CP#7	039° 56' 41.74"	-091° 12' 10.68"	758.00'	25'	783.00'
CP#8	039° 56' 41.76"	-091° 12' 08.54"	758.00'	25'	783.00'
CP#9	039° 56' 40.52"	-091° 12' 08.53"	759.00'	25'	784.00'
CP#10	039° 56' 40.50"	-091° 12' 05.31"	760.00'	25'	785.00'



- LEGEND**
- EXISTING IMPROVEMENTS
  - EXISTING BUILDINGS
  - PROPOSED TAXIWAY PAVEMENT
  - EXISTING TAXIWAY PAVEMENT TO BE REMOVED
  - PROPOSED CONTRACTOR STAGING AREA AND HAUL ROAD
  - EXISTING FENCE
  - RUNWAY SAFETY AREA (500' WIDE)
  - CONSTRUCTION TRAFFIC
  - PLACE AND MAINTAIN BARRICADES WHEN RUNWAY 13-31 IS CLOSED
  - PLACE AND MAINTAIN BARRICADES WHEN RUNWAY 36-18 IS CLOSED
  - TEMPORARY CONSTRUCTION SIGN
  - CRITICAL POINT

**WORK AREA NOTES**

- THE WORK ITEMS TO BE COMPLETED IN THIS AREA INCLUDE REMOVAL OF THE EXISTING TAXIWAY PAVEMENT AND ASSOCIATED LIGHTING AND DRAINAGE, CONSTRUCTION OF THE NEW TAXIWAY PAVEMENT AND ASSOCIATED LIGHTING AND DRAINAGE, INCLUDING PAVING, MARKING, AND EROSION CONTROL.
- THE CONTRACTOR MAY NOT WORK IN WORK AREAS 1 AND 2 SIMULTANEOUSLY, WITH THE EXCEPTION OF THE DESIGNATED AREA AS SHOWN ON THE WORK AREA 1 SAFETY AND PHASING PLAN. WORK WITHIN THIS DESIGNATED AREA SHALL BE LIMITED TO 1 CALENDAR DAY.
- CLOSURE CROSSES AND BARRICADES SHALL BE IN PLACE PRIOR TO BEGINNING CONSTRUCTION.
- AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE CLOSURES.
- AT THE COMPLETION OF ALL WORK AREA CONSTRUCTION, THE HAUL ROADS, CULVERT, AND GATE ARE TO BE REMOVED. THE FENCELINE, HAUL ROAD AREA AND CONSTRUCTION EQUIPMENT PARKING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS PER THE SPECIFICATIONS.
- THE COSTS FOR PROVISION, PLACEMENT, MAINTENANCE AND REMOVAL OF BARRICADES, CLOSURE CROSSES AND TEMPORARY JUMPER CABLES/CONDUITS ON THE AIRFIELD AND ALL ASSOCIATED INCIDENTALS SHALL BE PAID FOR UNDER ITEM AR150520 MOBILIZATION.
- THE COSTS FOR CONSTRUCTION, MAINTENANCE AND REMOVAL OF TEMPORARY ACCESS GATE, TEMPORARY HAUL ROUTE AND EQUIPMENT STAGING AREA, TEMPORARY SIGNAGE AND ALL ASSOCIATED INCIDENTALS SHALL BE PAID FOR UNDER ITEM AR150540 HAUL ROUTE.



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*B. Stolz* 6/9/15

**REALIGN TAXIWAY D**

IDA No: UIN-4332

SBG No: 3-17-SBGP-105

QI061


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 9, 2015

PROJECT NO: 13A0080D

CAD FILE: C-004-SFY.DWG

DESIGN BY: JRH 06/09/2014

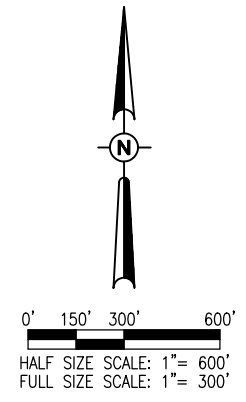
DRAWN BY: JRH 06/09/2014

REVIEWED BY: JDW 11/12/2014

SHEET TITLE

**CONSTRUCTION  
SAFETY AND  
PHASING PLAN-  
WORK AREA 2**

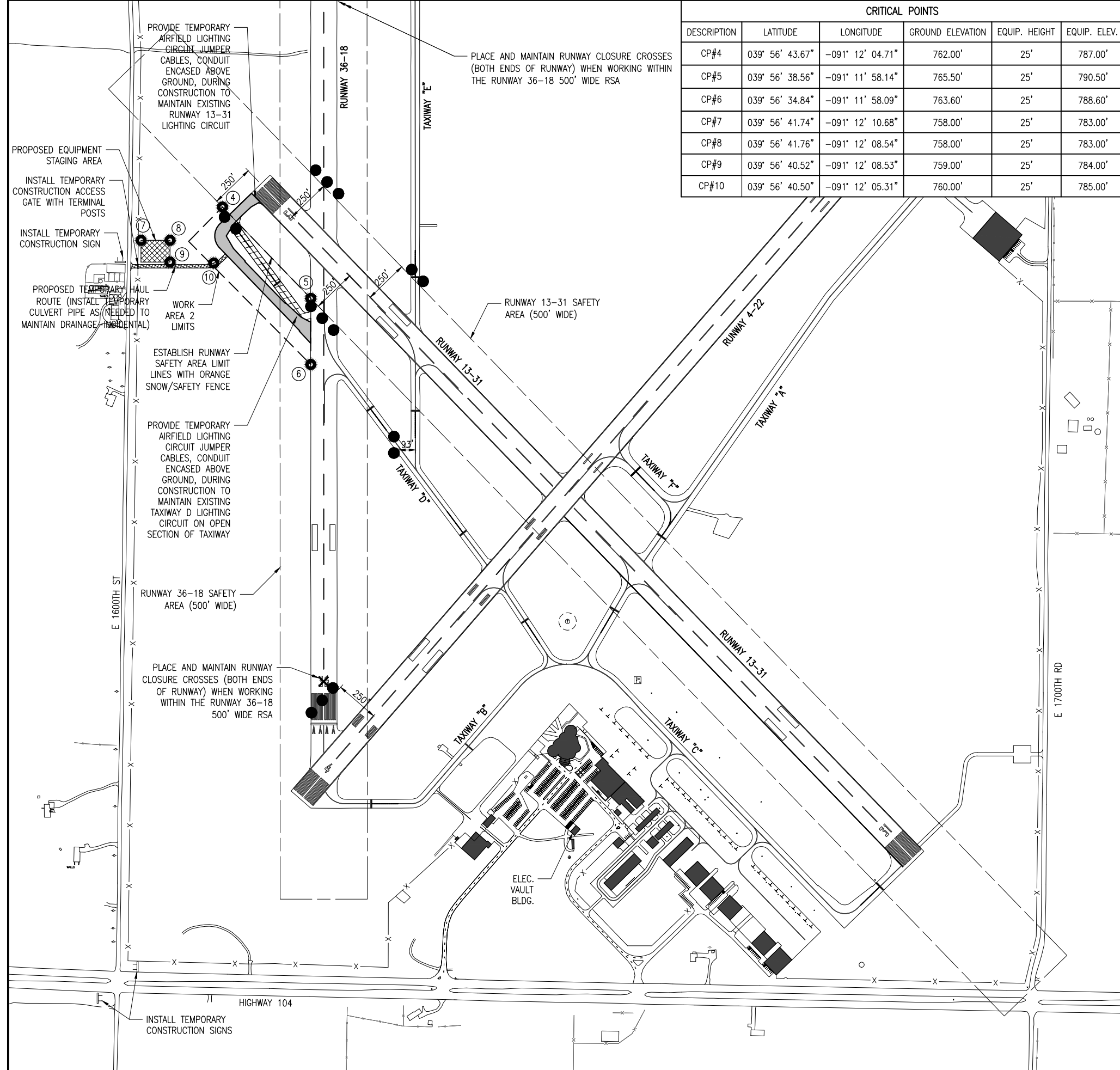
CRITICAL POINTS					
DESCRIPTION	LATITUDE	LONGITUDE	GROUND ELEVATION	EQUIP. HEIGHT	EQUIP. ELEV.
CP#4	039° 56' 43.67"	-091° 12' 04.71"	762.00'	25'	787.00'
CP#5	039° 56' 38.56"	-091° 11' 58.14"	765.50'	25'	790.50'
CP#6	039° 56' 34.84"	-091° 11' 58.09"	763.60'	25'	788.60'
CP#7	039° 56' 41.74"	-091° 12' 10.68"	758.00'	25'	783.00'
CP#8	039° 56' 41.76"	-091° 12' 08.54"	758.00'	25'	783.00'
CP#9	039° 56' 40.52"	-091° 12' 08.53"	759.00'	25'	784.00'
CP#10	039° 56' 40.50"	-091° 12' 05.31"	760.00'	25'	785.00'



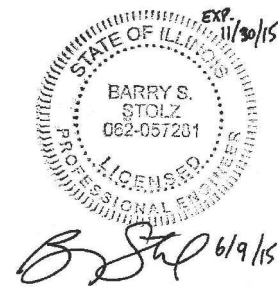
- LEGEND**
- EXISTING IMPROVEMENTS
  - EXISTING BUILDINGS
  - PROPOSED TAXIWAY PAVEMENT
  - EXISTING TAXIWAY PAVEMENT TO BE REMOVED
  - PROPOSED CONTRACTOR STAGING AREA AND HAUL ROAD
  - EXISTING FENCE
  - RUNWAY SAFETY AREA (500' WIDE)
  - CONSTRUCTION TRAFFIC
  - PLACE AND MAINTAIN BARRICADES WHEN RUNWAY 13-31 IS CLOSED
  - PLACE AND MAINTAIN BARRICADES WHEN RUNWAY 36-18 IS CLOSED
  - TEMPORARY CONSTRUCTION SIGN
  - CRITICAL POINT

**WORK AREA NOTES**

- THE WORK ITEMS TO BE COMPLETED IN THIS AREA INCLUDE REMOVAL OF THE EXISTING TAXIWAY PAVEMENT AND ASSOCIATED LIGHTING AND DRAINAGE, CONSTRUCTION OF THE NEW TAXIWAY PAVEMENT AND ASSOCIATED LIGHTING AND DRAINAGE, INCLUDING PAVING, MARKING, AND EROSION CONTROL.
- THE CONTRACTOR MAY NOT WORK IN WORK AREAS 1 AND 2 SIMULTANEOUSLY, WITH THE EXCEPTION OF THE DESIGNATED AREA AS SHOWN ON THE WORK AREA 1 SAFETY AND PHASING PLAN. WORK WITHIN THIS DESIGNATED AREA SHALL BE LIMITED TO 1 CALENDAR DAY.
- CLOSURE CROSSES AND BARRICADES SHALL BE IN PLACE PRIOR TO BEGINNING CONSTRUCTION.
- AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE CLOSURES.
- AT THE COMPLETION OF ALL WORK AREA CONSTRUCTION, THE HAUL ROADS, CULVERT, AND GATE ARE TO BE REMOVED. THE FENCELINE, HAUL ROAD AREA AND CONSTRUCTION EQUIPMENT PARKING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS PER THE SPECIFICATIONS.
- THE COSTS FOR PROVISION, PLACEMENT, MAINTENANCE AND REMOVAL OF BARRICADES, CLOSURE CROSSES AND TEMPORARY JUMPER CABLES/CONDUITS ON THE AIRFIELD AND ALL ASSOCIATED INCIDENTALS SHALL BE PAID FOR UNDER ITEM AR150520 MOBILIZATION.
- THE COSTS FOR CONSTRUCTION, MAINTENANCE AND REMOVAL OF TEMPORARY ACCESS GATE, TEMPORARY HAUL ROUTE AND EQUIPMENT STAGING AREA, TEMPORARY SIGNAGE AND ALL ASSOCIATED INCIDENTALS SHALL BE PAID FOR UNDER ITEM AR150540 HAUL ROUTE.



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**REALIGN TAXIWAY D**

IDA No: UIN-4332

SBG No: 3-17-SBGP-105

Q1061

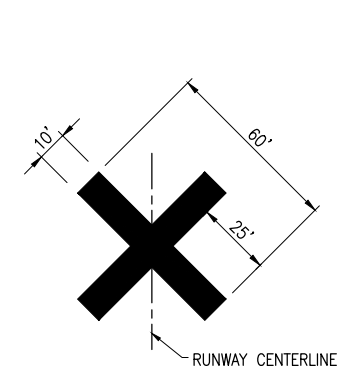
NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 9, 2015  
PROJECT NO: 13A0080D  
CAD FILE: G-501-SFY.DWG  
DESIGN BY: JRH 06/10/2014  
DRAWN BY: JRH 06/10/2014  
REVIEWED BY: JDW 11/12/2014  
SHEET TITLE

**CONSTRUCTION SAFETY AND PHASING DETAILS AND NOTES**

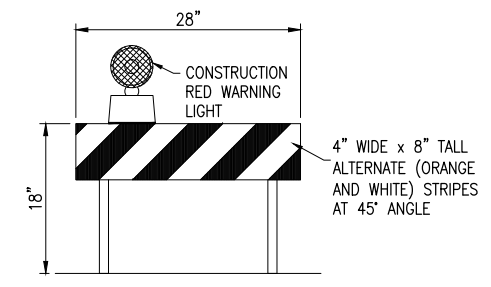
**SAFETY NOTES**

- FOLLOWING ARE THE CONSTRUCTION SAFETY PROCEDURES THAT THE CONTRACTOR SHALL FOLLOW THROUGHOUT THIS PROJECT. ADDITIONAL REQUIREMENTS ARE SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET AND THIS SHEET.
- ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER AT THE PRECONSTRUCTION CONFERENCE, OR DURING THE COURSE OF THE CONTRACT.
- THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND PHASING AREAS PRESENTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
- NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE AIRPORT MANAGER RESERVES THE RIGHT TO SUSPEND OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT.
- CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED AT THE STAGING AREA SHOWN ON THE PLAN VIEW, EXCEPT AS OTHERWISE PROVIDED FOR AT THE PRECONSTRUCTION CONFERENCE.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION.
- NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 250' OF ANY ACTIVE RUNWAY, WITHIN 93' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA, OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE RESIDENT ENGINEER) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY.
- CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- NO OPEN TRENCHES WITHIN 250' OF AN ACTIVE RUNWAY CENTERLINE OR WITHIN 93' OF ANY AIRPORT OPERATIONS AREA WILL BE PERMITTED UNLESS PROPERLY MARKED. OTHER TRENCHES SHALL BE MAINTAINED SAFE, I.E., BARRICADED OR COVERED WITH STEEL PLATES IN ALL OTHER AREAS.
- OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
- NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT. HOWEVER OTHER EQUIPMENT TALLER THAN 25' MAY BE PERMITTED WITH THE APPROVAL OF THE AIRPORT MANAGER AND AIRSPACE APPROVAL BY THE FAA.
- NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER NO FLARE POTS ARE ALLOWED ON THE PROJECT.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEP, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS. STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT ENGINEER.
- CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED LIGHTS DURING PERIODS OF LOW VISIBILITY AS REQUIRED.
- THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE CHANGE.
- CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER AT NO ADDITIONAL COST.
- CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER.
- CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE.
- THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER AS NECESSARY TO CONTROL DUST.
- NO CONSTRUCTION VEHICLES SHALL BE DRIVEN ACROSS ANY ACTIVE RUNWAY, INCLUDING TURF RUNWAYS. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL NOT BE PERMITTED WITHIN 250' OF ANY ACTIVE RUNWAY CENTERLINE OR WITHIN 93' OF ANY OTHER ACTIVE AIRPORT TAXIWAY OR APRON. HOWEVER, CONSTRUCTION MAY BE PERMITTED IN THESE AREAS IF THE CONTRACTOR HAS GAINED APPROVAL FROM THE AIRPORT MANAGER AT LEAST 7 DAYS IN ADVANCE OF THE SCHEDULED CONSTRUCTION PERIOD AND THE OPERATIONAL AREA IS CLOSED TO TRAFFIC AND PROPER NOTAMS ARE ISSUED BY THE AIRPORT MANAGER TO THE APPROPRIATE FLIGHT SERVICE STATION.
- UNLESS SPECIFIED OTHERWISE, COST FOR THE ABOVE IS TO BE CONSIDERED INCIDENTAL TO THE PROJECT. SEPARATE PAYMENT SHALL NOT BE MADE.



- TEMPORARY CLOSURE CROSS MARKINGS SHALL BE "AVIATION YELLOW."
- TEMPORARY CLOSURE CROSS MARKINGS SHALL BE CONSTRUCTED OF PLYWOOD, SNOW FENCE OR APPROVED FABRIC AND SHALL BE SECURED TO PAVEMENT BY SANDBAGS OR OTHER APPROVED METHOD.
- COST FOR PROVIDING, PLACING, MAINTAINING, AND REMOVING CLOSURE CROSSES SHALL BE INCLUDED IN THE COST OF THE OTHER CONTRACT ITEMS.

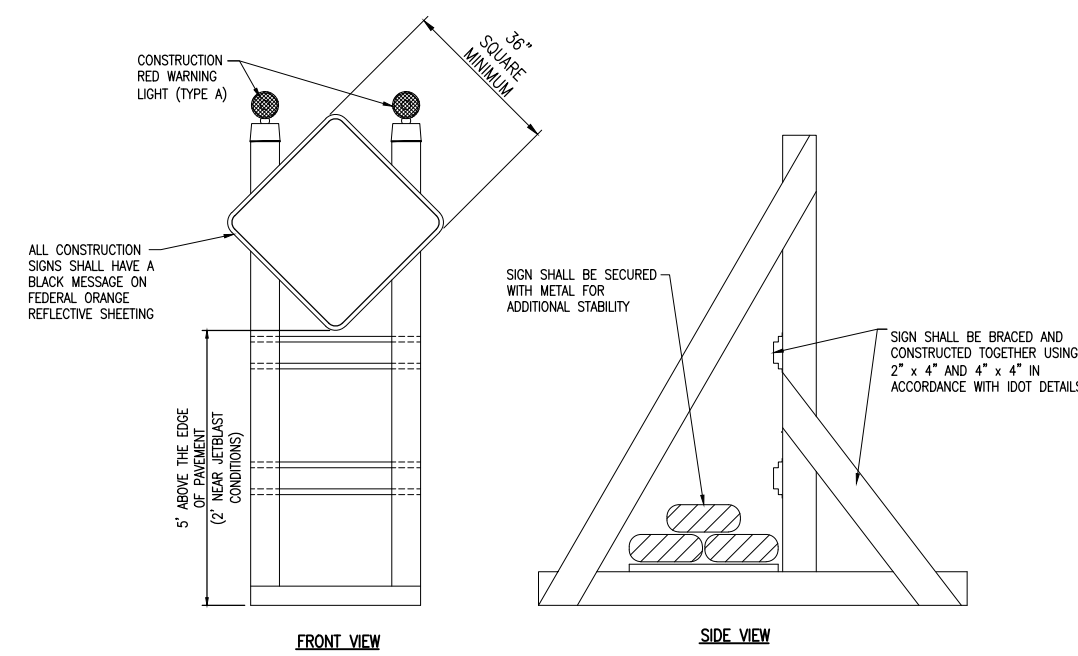
**CLOSURE CROSS MARKER DETAIL**  
NOT TO SCALE



**MODIFIED TYPE II BARRICADE**  
NOT TO SCALE

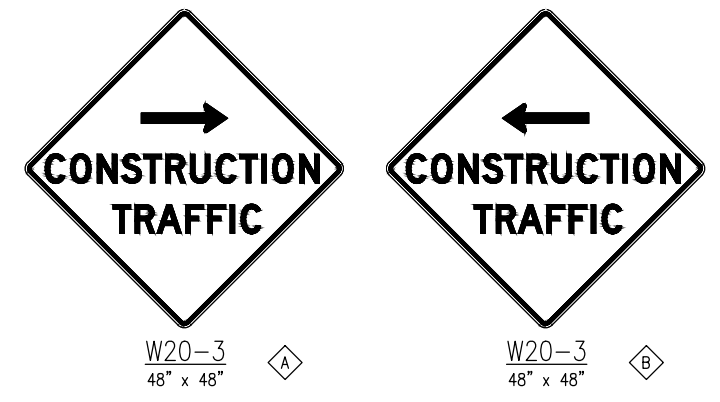
**BARRICADE NOTES**

- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- MODIFIED TYPE II BARRICADES SHALL BE SPACED END TO END THE WIDTH OF THE PAVEMENT IN 10' INCREMENTS. BARRICADES ARE TO BE SET BACK 93' FROM THE ACTIVE TAXIWAY CENTERLINE OR AS SHOWN ON THE PLANS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- THE ONLY COLOR COMBINATION ON TYPE II BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- COST FOR PROVIDING, PLACING, MAINTAINING, AND REMOVING BARRICADES SHALL BE INCLUDED IN THE COST OF THE OTHER CONTRACT ITEMS.



**SIGNAGE NOTES**

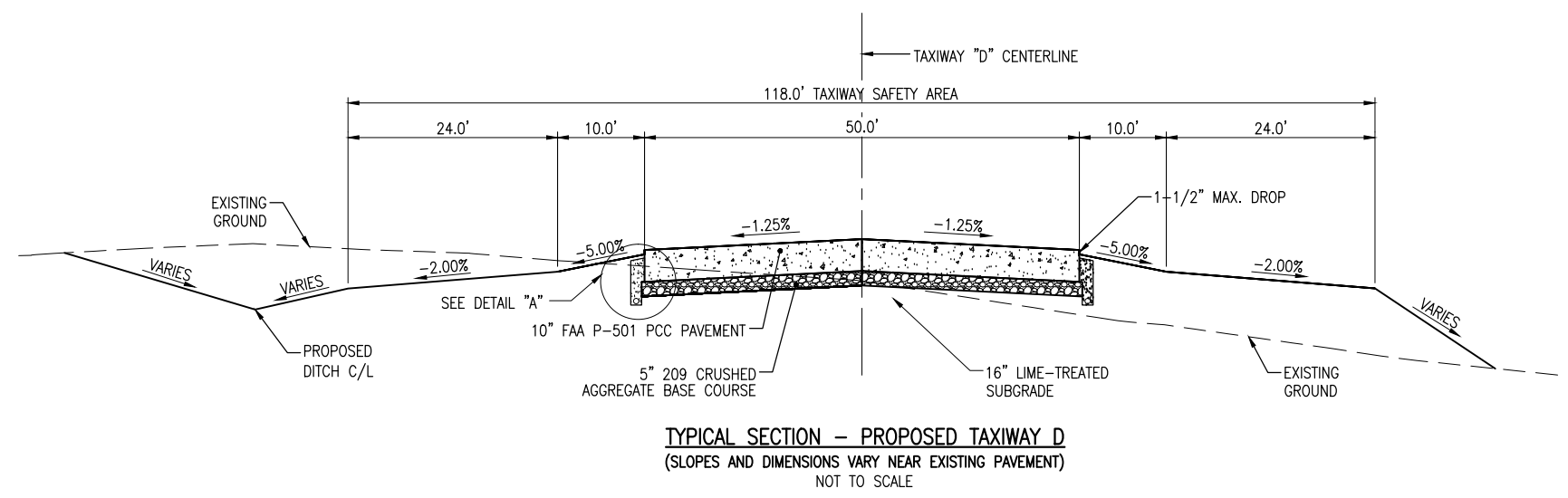
- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- UNLESS OTHERWISE SPECIFIED, CONSTRUCTION SIGNS SHALL BE MOUNTED ON PORTABLE OR NON-PORTABLE SUPPORTS. A PORTABLE SUPPORT IS DEFINED AS A TYPICAL SIGN STANDARD AS SHOWN ON THIS SHEET, OR A SMALL LIGHT WEIGHT TRAILER. A NON-PORTABLE SUPPORT IS DEFINED AS DRIVEN METAL OR WOOD POST. ALL SIGNS, REGARDLESS OF THE TYPE OF SUPPORTS USED, SHALL BE MOUNTED SUCH THAT THE MESSAGE ON THE SIGN IS LEVEL IN THE HORIZONTAL PLANE AFTER PLACEMENT. THE COST OF CONSTRUCTION WARNING LIGHTS SHALL BE INCLUDED IN THE COST OF THE CONSTRUCTION SIGNS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY ARE TO BE USED IN A LOW INTENSITY FLASHING MODE (TYPE A).
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- COST FOR PROVIDING, PLACING, MAINTAINING, AND REMOVING SIGNS SHALL BE INCLUDED IN ITEM AR150540 HAUL ROUTE.



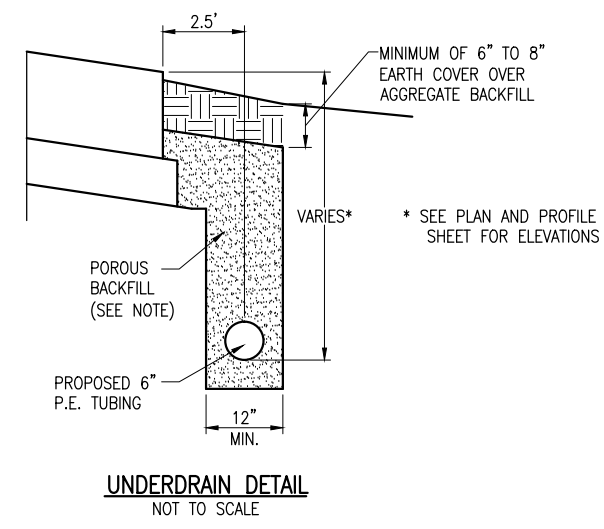
**CONSTRUCTION SIGNS**  
NOT TO SCALE



*B. Stolz* 6/9/15

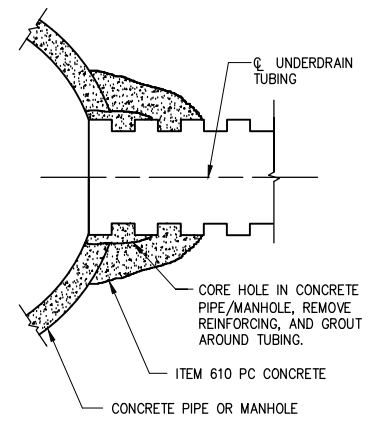


**TYPICAL SECTION - PROPOSED TAXIWAY D**  
(SLOPES AND DIMENSIONS VARY NEAR EXISTING PAVEMENT)  
NOT TO SCALE



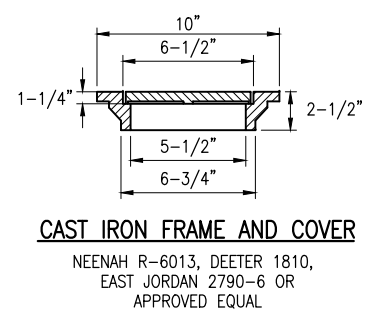
**UNDERDRAIN DETAIL**  
NOT TO SCALE

NOTE:  
POROUS BACKFILL SHALL CONFORM TO THE REQUIREMENTS FOR IDOT CA-14 OR IDOT CA-16 AND WILL BE CONSIDERED INCIDENTAL TO AR705506 6" PERF. UNDERDRAIN AND NO ADDITIONAL COMPENSATION ALLOWED. CONTRACTOR SHALL PLACE AND CONSOLIDATE THE POROUS BACKFILL TO THE SATISFACTION OF THE RESIDENT ENGINEER.



**STORM SEWER CONCRETE COLLAR AND GROUT CONNECTION**  
NOT TO SCALE

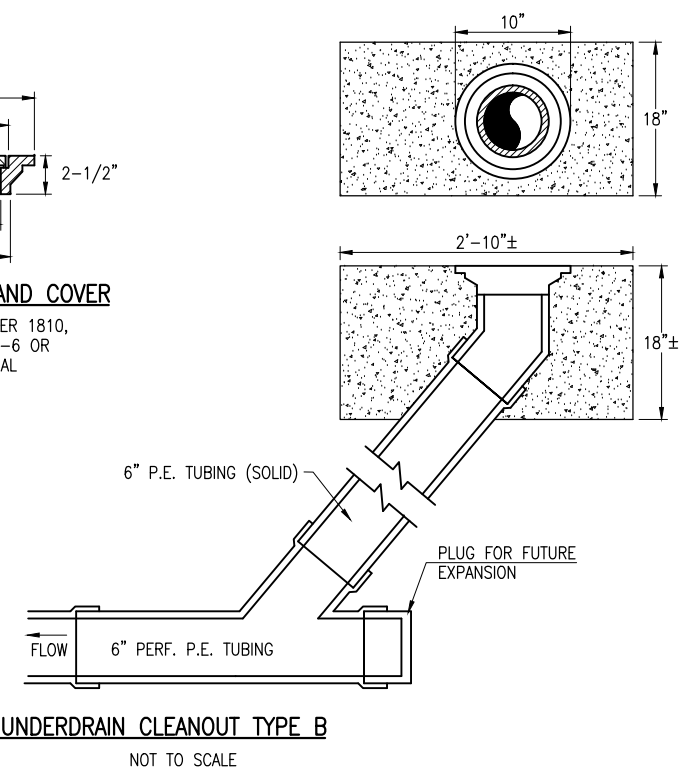
NOTE: COST FOR CONCRETE COLLAR SHALL BE INCIDENTAL TO UNDERDRAIN ITEMS.



**CAST IRON FRAME AND COVER**  
NEENAH R-6013, DEETER 1810,  
EAST JORDAN 2790-6 OR  
APPROVED EQUAL

**CLEANOUT NOTES**

1. DIAMETER OF PIPE AS SPECIFIED.
2. TOP OF CLEANOUTS SHALL BE 2" ABOVE FINISH GROUND LINE AT LOCATION SHOWN ON PLANS.
3. 1/2" CHAMFER TO BE USED ON ALL EXPOSED EDGES OF CLEANOUTS.
4. THE CONCRETE SHALL BE STRUCTURAL PORTLAND CEMENT CONCRETE (NON-REINFORCED) IN ACCORDANCE WITH ITEM 610.



**UNDERDRAIN CLEANOUT TYPE B**  
NOT TO SCALE

REALIGN TAXIWAY D  
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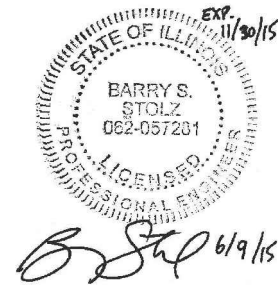
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ISSUE: JUNE 9, 2015  
PROJECT NO: 13A0080D  
CAD FILE: C-301-TYP.DWG  
DESIGN BY: JRH 06/10/2014  
DRAWN BY: JRH 06/10/2014  
REVIEWED BY: JDW 11/12/2014

SHEET TITLE

TYPICAL SECTIONS





**REALIGN TAXIWAY D**

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		DES	DWN	REV

ISSUE: JUNE 9, 2015

PROJECT NO: 13A0080D

CAD FILE: C-121-CON.DWG

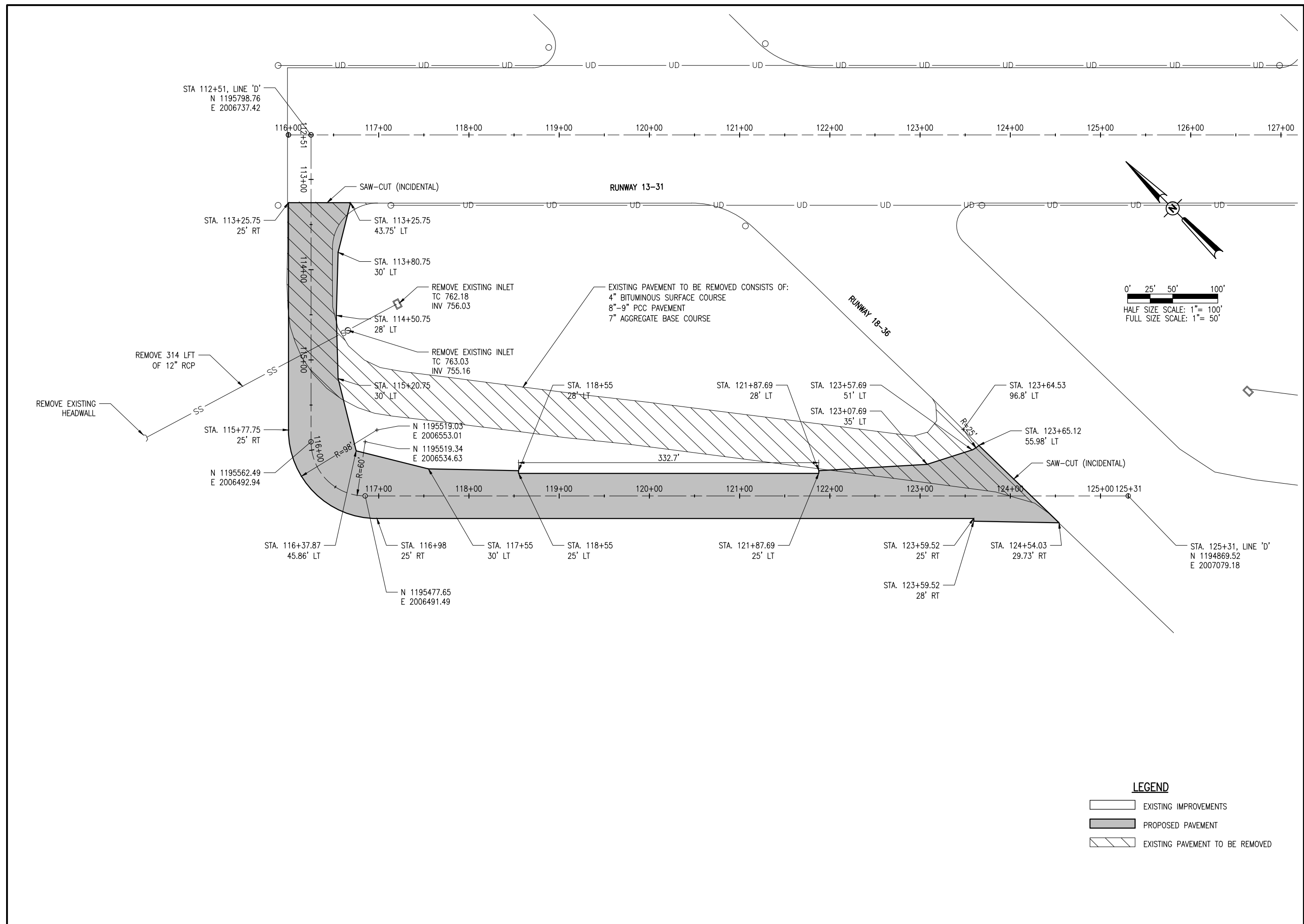
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DRAWN BY: JRH 06/09/2014

REVIEWED BY: JDW 11/12/2014

SHEET TITLE

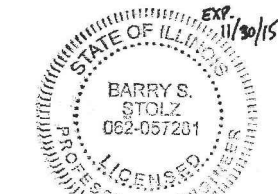
**PROPOSED  
CONSTRUCTION  
PLAN**



**LEGEND**

- EXISTING IMPROVEMENTS
- PROPOSED PAVEMENT
- EXISTING PAVEMENT TO BE REMOVED

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**REALIGN TAXIWAY D**

IDA No: UIN-4332

SBG No: 3-17-SBGP-105

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ISSUE: JUNE 9, 2015

PROJECT NO: 13A0080D

CAD FILE: C-701-PNP.DWG

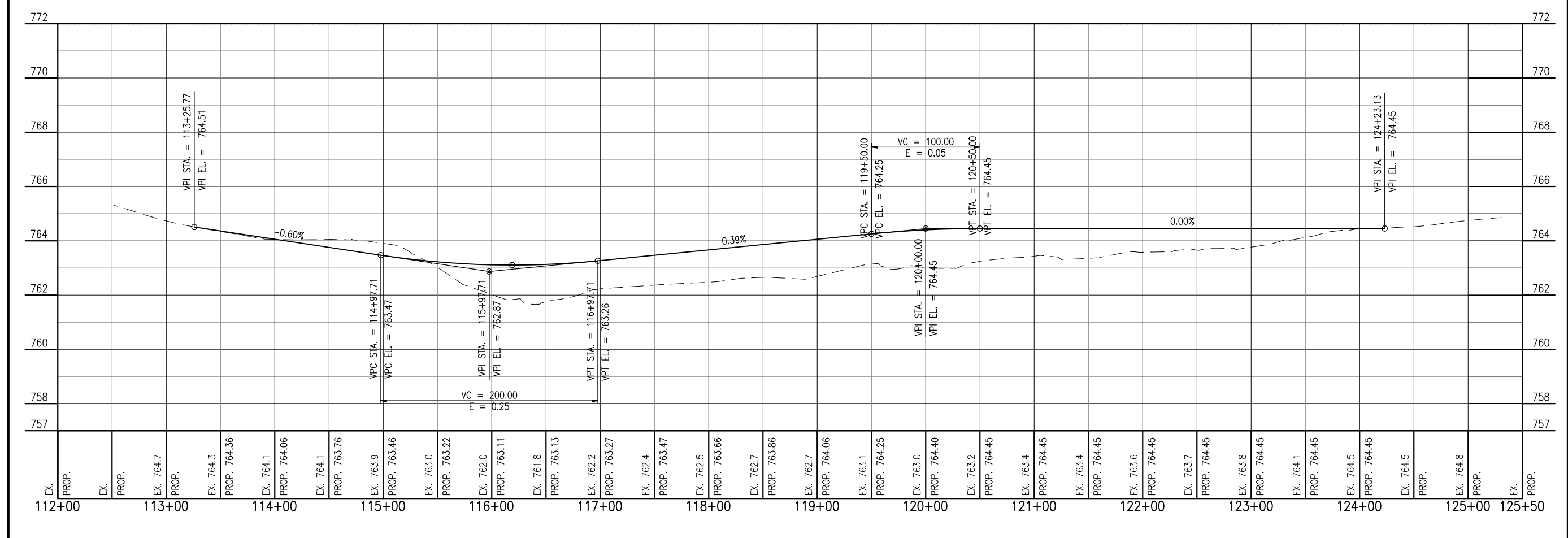
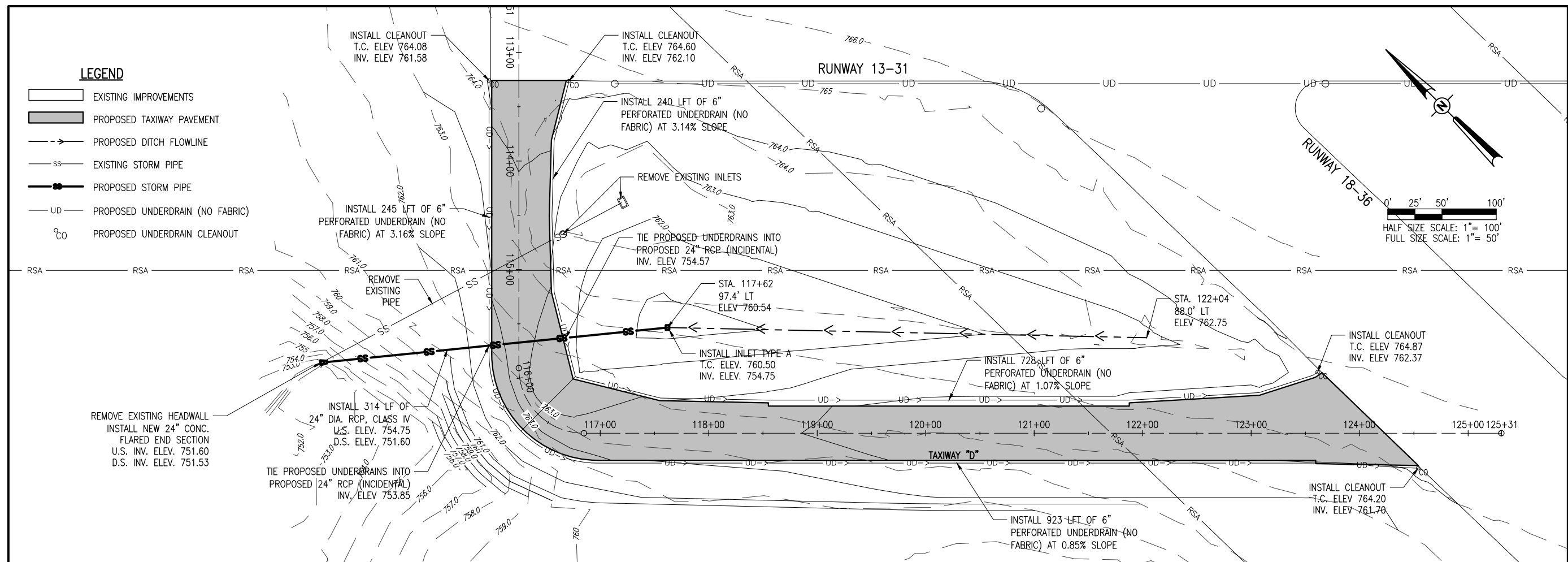
DESIGN BY: JRH 06/09/2014

DRAWN BY: JRH 06/09/2014

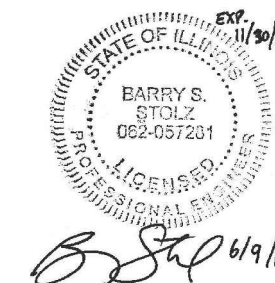
REVIEWED BY: JDW 11/12/2014

SHEET TITLE

**PROPOSED PLAN AND PROFILE**



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REALIGN TAXIWAY D

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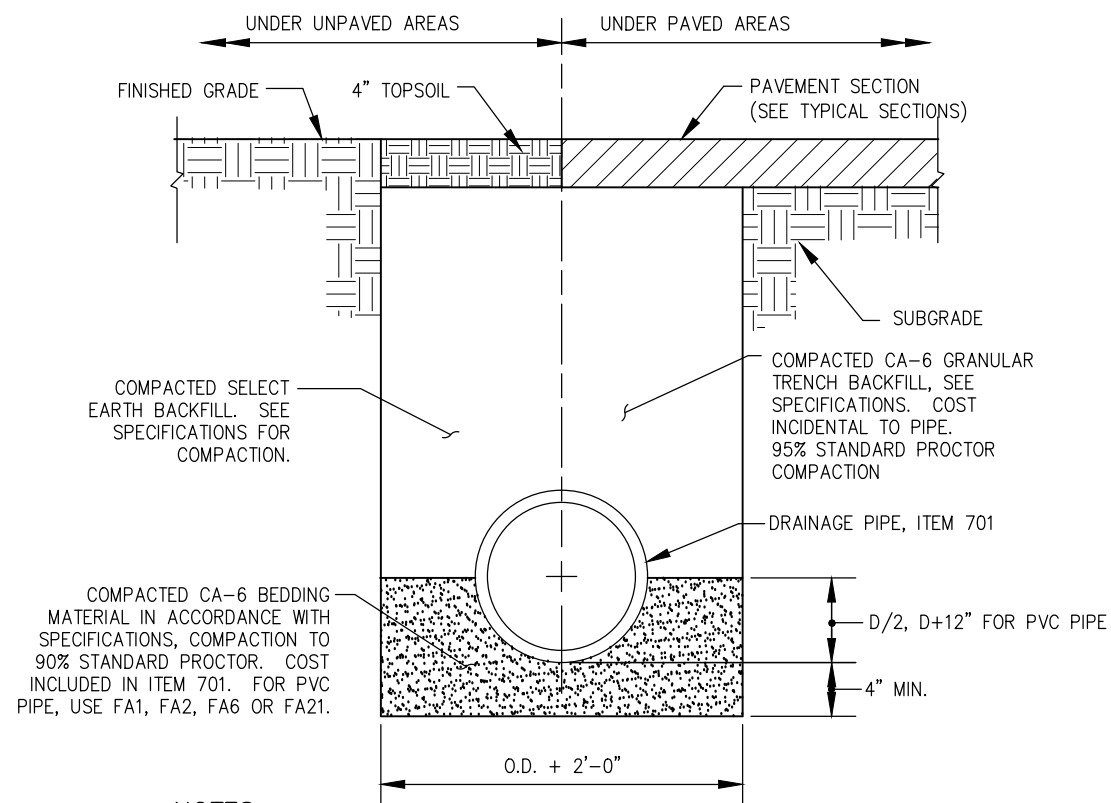
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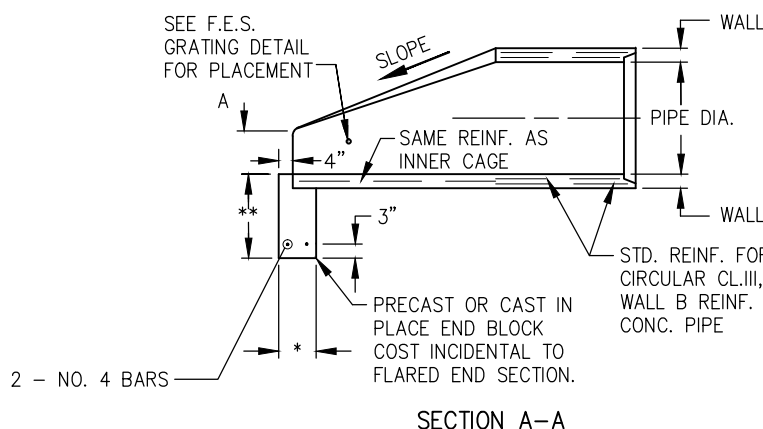
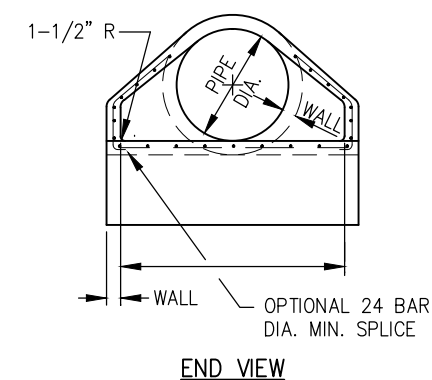
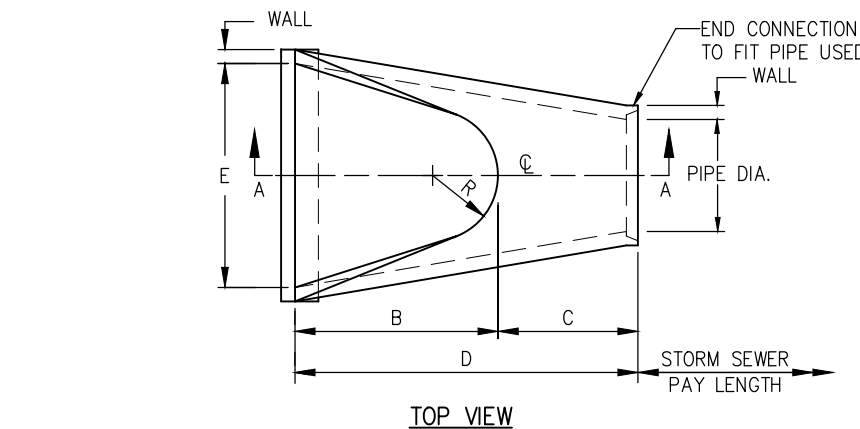
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 DESIGN BY: JRH 06/10/2014  
 DRAWN BY: JRH 06/10/2014  
 REVIEWED BY: JDW 11/12/2014

SHEET TITLE

DRAINAGE DETAILS


**NOTES**

- UNSATURATED MATERIAL ENCOUNTERED DURING PLACEMENT OF BEDDING SHALL BE REMOVED AND REPLACED.
- WITHIN 3 FEET OF PAVED AREA, GRANULAR BACKFILL IS TO BE USED INSTEAD OF EARTH BACKFILL.
- AT CONTRACTOR'S OPTION IDOT CONTROLLED LOW STRENGTH MATERIAL WITH A HIGH EARLY STRENGTH, "FLASH FILL", MAY BE USED INSTEAD OF GRANULAR TRENCH BACKFILL UNDER PAVEMENTS AT NO ADDITIONAL COST TO THE CONTRACT.

**PIPE TRENCH**  
NOT TO SCALE


- \* 8" - 36" DIA. PIPE OR LESS
- 10" - GREATER THAN 36" DIA. PIPE
- \*\* 18" - 36" DIA. PIPE OR LESS
- 24" - GREATER THAN 36" DIA. PIPE

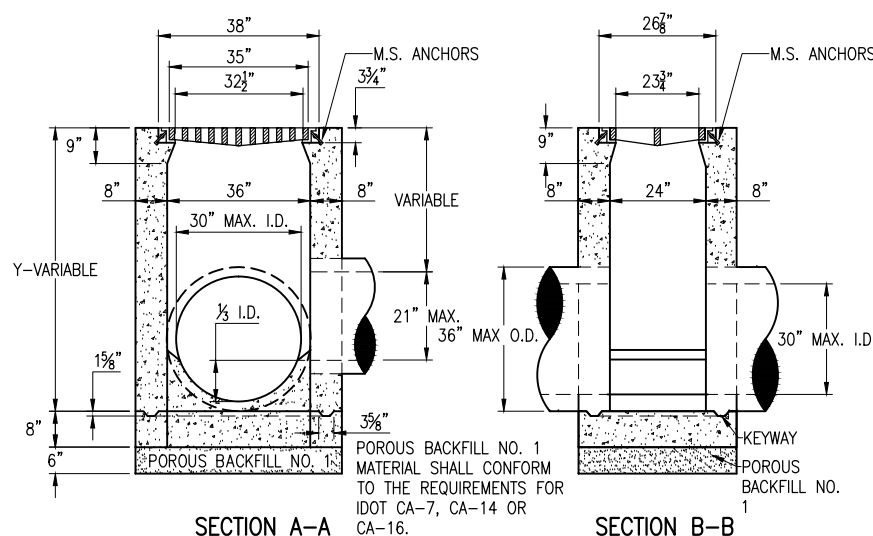
PIPE DIA.	WALL	A	B	C	D	E	R	SLOPE
24"	3"	9.5"	3'-7.5"	30"	6'-1.5"	4'-0"	*	1:2.5

\* RADIUS AS FURNISHED BY MANUFACTURER

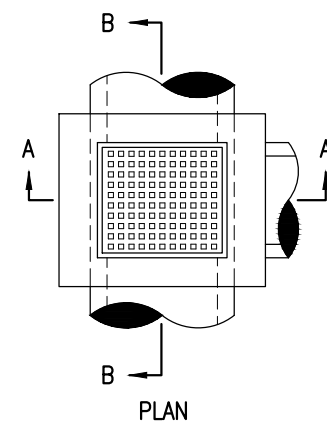
**PRECAST CONCRETE FLARED END SECTION**  
(IDOT STANDARD 542301-MODIFIED)  
NOT TO SCALE

**TYPE A INLET NOTES**

- INLET TO BE CONSTRUCTED OF STRUCTURAL P.C. CONCRETE. THE CONTRACT UNIT PRICE PER INLET SHALL INCLUDE THE FRAME, GRATE AND STEPS IN PLACE AND COMPLETE PER UNIT.
- 1/2" CHAMFER TO BE USED ON ALL EXPOSED CORNERS OF INLETS. BARS TO BE INSTALLED 2" FROM FACE OF WALLS.
- THE FRAME AND GRATE SHALL BE NEENAH R-3475-A, DEETER 2425-E.
- INLET STEPS SHALL BE NEENAH R-1980-1. 12" TO 15" C.C. STEPS TO BE INSTALLED WHEN Y IS GREATER THAN 5". THE COST OF THE STEPS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR EACH INLET.
- THE PROPOSED TYPE A INLET SHALL BE PAID FOR UNDER: AR751411 INLET - TYPE A - - - PER EACH.


**PROPOSED TYPE A INLET**  
NOT TO SCALE

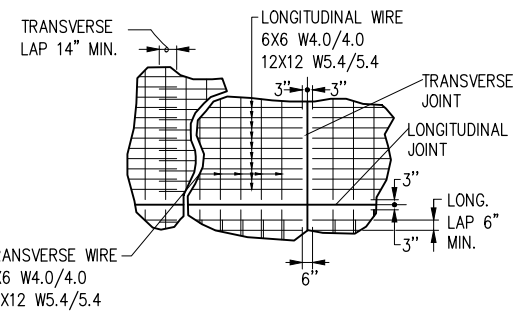
TYPE	QUANTITY PER INLET	DIMENSIONS		SIZE	APPROX. WT. OF BARS IN INLET
		A	B		
A	2	3'-4"	2'-4"	#5	16.7


**REINFORCING STEEL BAR TYPE**

D INSIDE DIA. OF PIPE	CU. YARDS TO SUBTRACT FOR 8" WALL	INVERT FORM	BOTTOM SIDES AND TOP	
			Y	TOTAL CU. YDS.
12"	.036	.120	3'	1.30
15"	.051	.152	4'	1.62
18"	.071	.134	5'	1.93
21"	.095	.140	6'	2.40
24"	.121	.142	7'	2.55
30"	.175	.146	8'	2.86
			9'	3.18
			10'	3.49
			11'	3.80
			12'	4.11
			13'	4.42
			14'	4.74

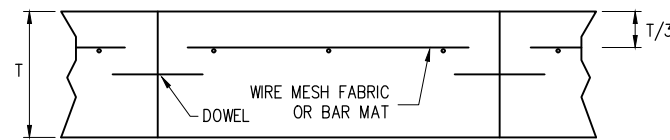






**REINFORCEMENT SHEET WIRE FABRIC DETAIL**  
NOT TO SCALE

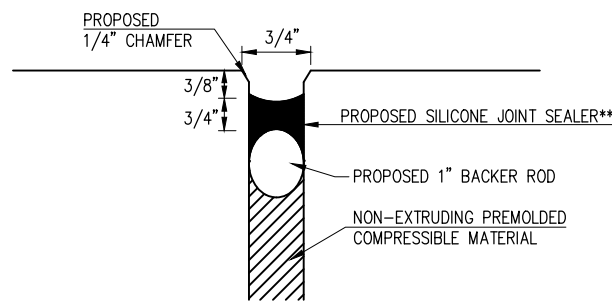
NOTE:  
DIFFERENT WIRE MESH SIZES/CONFIGURATIONS OR SIZES ARE PERMITTED  
IF THE MINIMUM CROSS SECTIONAL AREA EQUALS 0.5 OR GREATER.



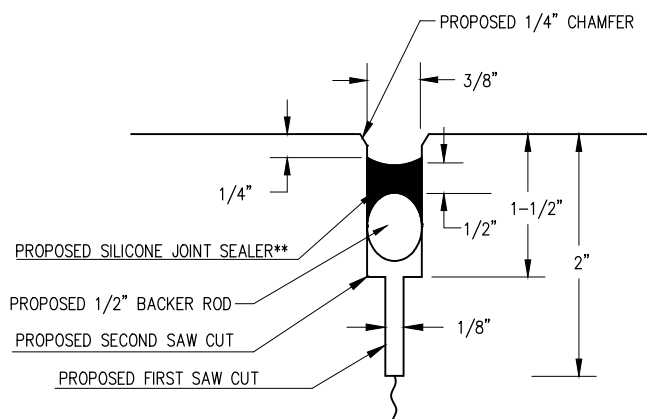
**PAVEMENT REINFORCING DETAIL**  
NOT TO SCALE

**REINFORCING NOTES:**

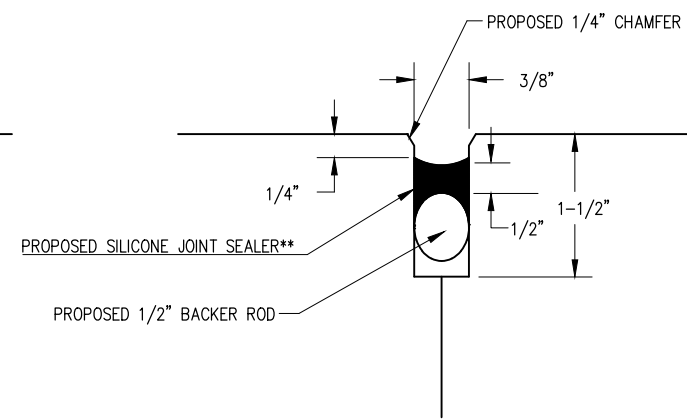
1. END LAPS SHALL BE A MINIMUM OF 12", BUT NOT LESS THAN 30 TIMES THE DIAMETER OF THE LONGITUDINAL WIRE OR BAR.
2. SIDE LAPS SHALL BE A MINIMUM OF 6", BUT NOT LESS THAN 20 TIMES THE DIAMETER OF TRANSVERSE WIRE OR BAR.
3. END AND SIDE CLEARANCES SHALL BE A MAXIMUM OF 6" AND A MINIMUM OF 2".
4. LONGITUDINAL MEMBERS SHALL BE SPACED NOT LESS THAN 4" NOR MORE THAN 12" APART.
5. TRANSVERSE MEMBERS SHALL BE SPACED NOT LESS THAN 4" NOR MORE THAN 24" APART.
6. REINFORCING SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS, ITEM AR800510, SECTION 501-2.6.
7. PAVEMENT REINFORCING SHALL BE INCLUDED IN THE COST OF THE PCC PAVEMENT.



**DETAIL 1**  
NOT TO SCALE



**DETAIL 2**  
NOT TO SCALE



**DETAIL 3**  
NOT TO SCALE

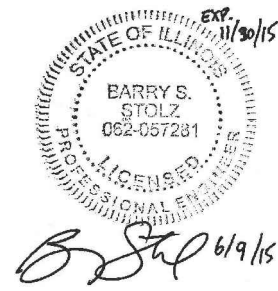
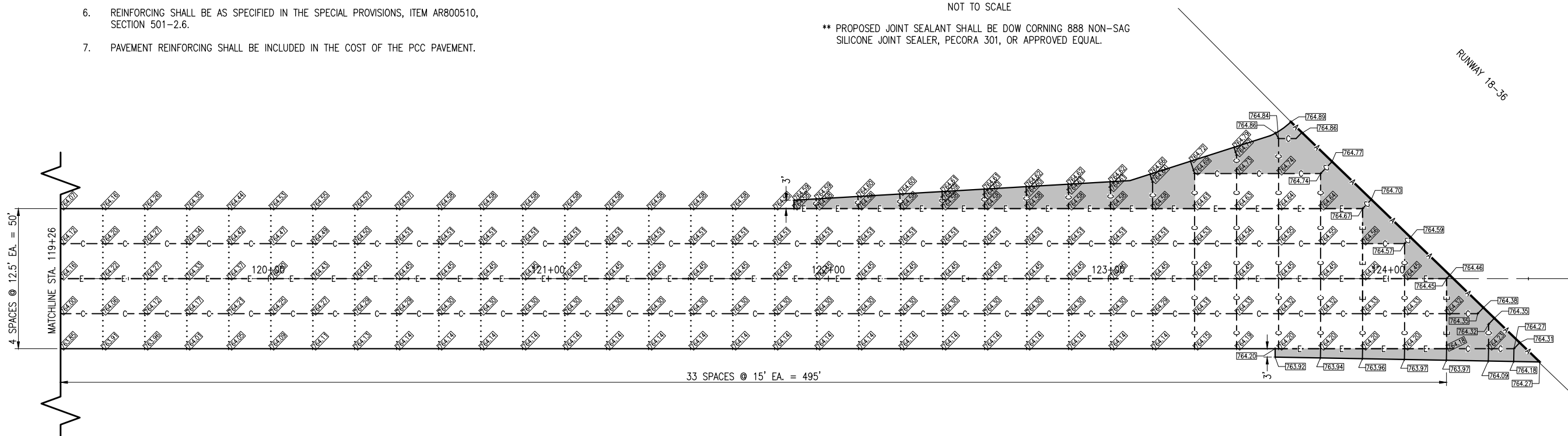
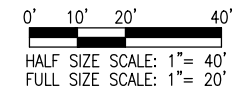
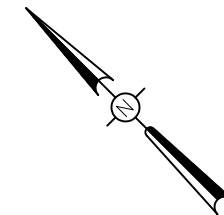
**JOINT SEALING DETAILS**  
NOT TO SCALE

\*\* PROPOSED JOINT SEALANT SHALL BE DOW CORNING 888 NON-SAG SILICONE JOINT SEALER, PECORA 301, OR APPROVED EQUAL.

**LEGEND**

- PROPOSED PAVEMENT
- CONSTRUCTION/CONTRACTION JOINT (TYPE C,E)
- CONTRACTION JOINT (TYPE D)
- ISOLATION JOINT (TYPE A)
- PROPOSED PAVEMENT SPOT ELEVATION
- REINFORCED PANEL (QUANTITY INCLUDED IN PCC PAY ITEM)

NOTE:  
CONTRACTOR MAY PROPOSE ALTERNATIVE PAVING/JOINTING PLAN  
FOR ENGINEER'S REVIEW AND APPROVAL FOLLOWING AWARD.



**REALIGN TAXIWAY D**  
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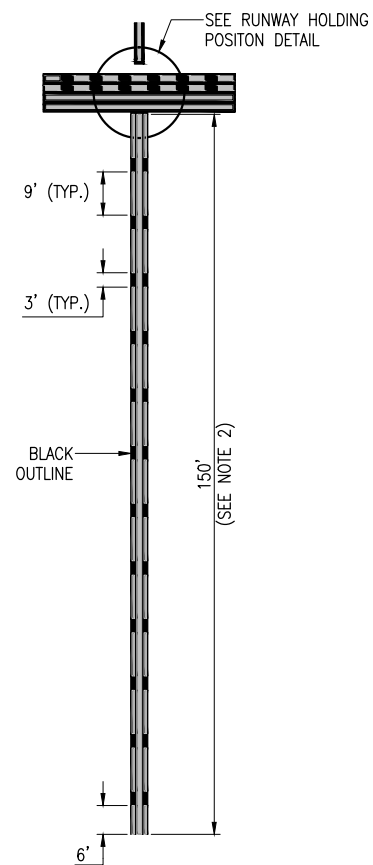
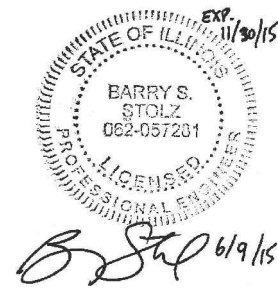
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DESIGN BY: JRH 07/24/2014  
DRAWN BY: JRH 07/24/2014  
REVIEWED BY: JDW 11/12/2014

SHEET TITLE  
**PROPOSED JOINTING PLAN SHEET 2**

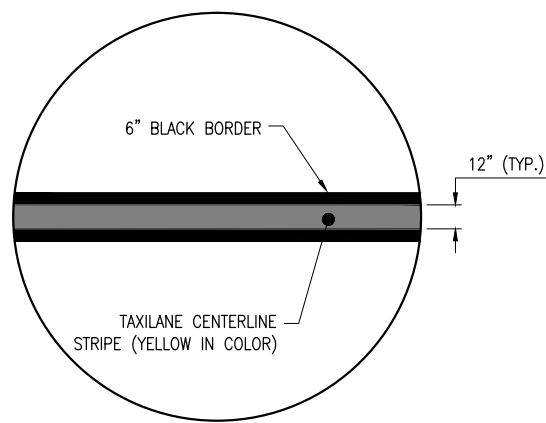




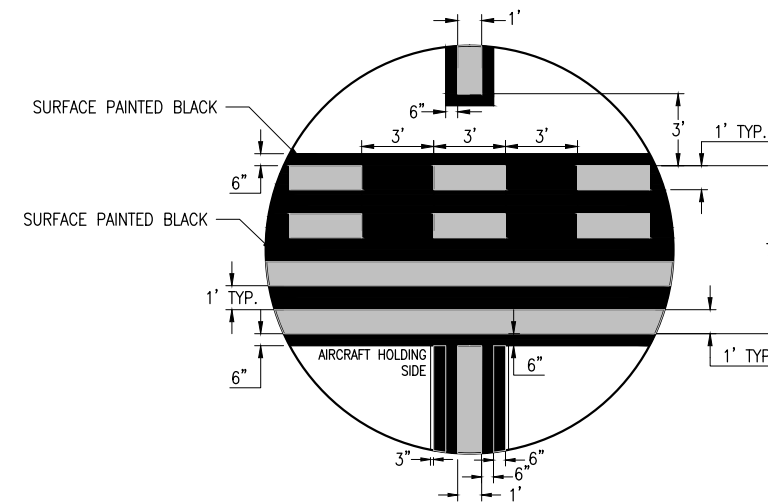


**ENHANCED TAXIWAY CENTERLINE MARKING NOTES:**

1. TAXIWAY CENTERLINE MARKINGS SHALL BE YELLOW IN COLOR AND OUTLINED IN BLACK.
2. TAXIWAY CENTERLINE MARKINGS SHALL BE ENHANCED FOR 150-FT PRIOR TO A RUNWAY HOLDING POSITION MARKING, UNLESS NOTED OTHERWISE. FOR A CURVED TAXIWAY CENTERLINE, THIS DISTANCE SHALL BE MEASURED ALONG THE CENTERLINE BEING ENHANCED TO A DISTANCE OF 150-FT.
3. WHERE TWO TAXIWAY CENTERLINES CONVERGE AT OR BEFORE THE RUNWAY HOLDING POSITION MARKING, PARTIAL INNER DASHED LINES LESS THAN 5 FEET AT THE POINT OF CONVERGENCE MAY BE OMITTED.
4. DASHES ON EITHER SIDE OF THE TAXIWAY CENTERLINE MUST BE ALIGNED, STARTING AND STOPPING WITH THE DASHES ON THE OPPOSITE SIDE OF THE CENTERLINE. TO ACCOMPLISH THIS FOR CURVED TAXIWAY CENTERLINES, THE MEASUREMENTS FOR THE DASHES AND GAPS SHALL BE MADE AT THE CENTERLINE AND EXTENDED PERPENDICULAR FROM THE CENTERLINE TO OBTAIN THE LOCATIONS OF THE DASHES.
5. ENHANCED TAXIWAY CENTERLINE MARKINGS SHALL BE IN ACCORDANCE WITH THE CURRENT EDITION OF FAA AC 150/5340-1, STANDARDS FOR AIRPORT MARKING.

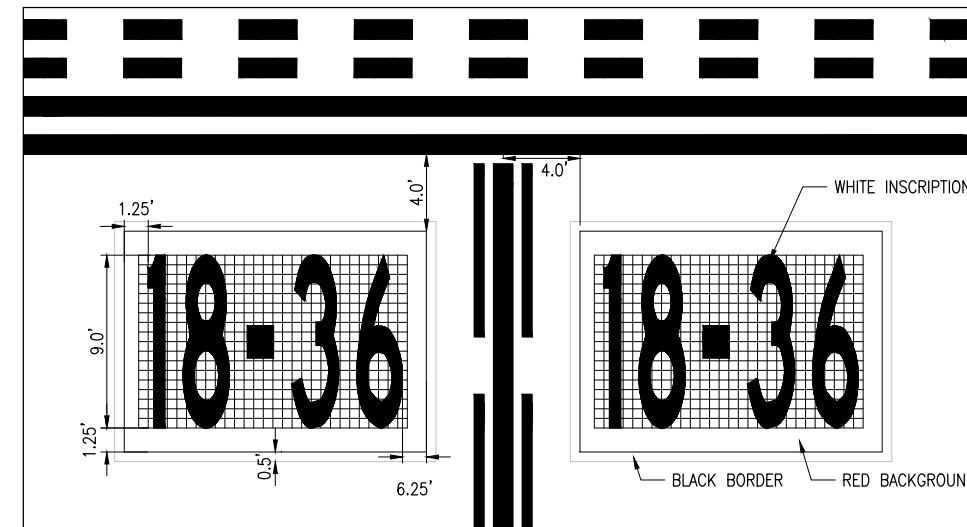
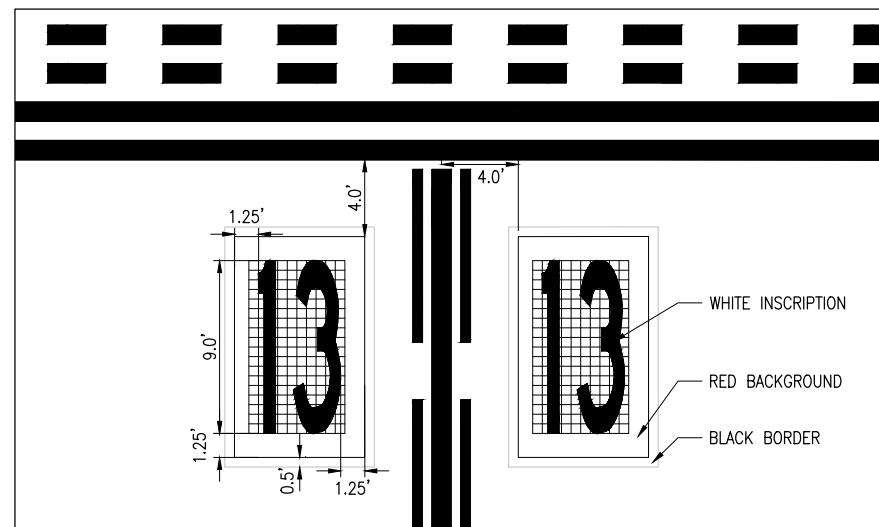


**TAXIWAY CENTERLINE DETAIL**  
NOT TO SCALE



**RUNWAY HOLDING POSITION DETAIL**  
"NOT TO SCALE"

**ENHANCED TAXIWAY CENTERLINE MARKING DETAIL**  
NOT TO SCALE



**SURFACE PAINTED HOLDING POSITION SIGN DETAILS**

TAXIWAY "D"  
SCALE: 1" = 5'

NOTE: GRID SHOWN FOR PROPER SIZING OF INSCRIPTIONS - NOT TO BE PAINTED. GRID SPACING IS 0.5 FEET.

REALIGN TAXIWAY D

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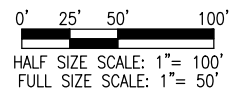
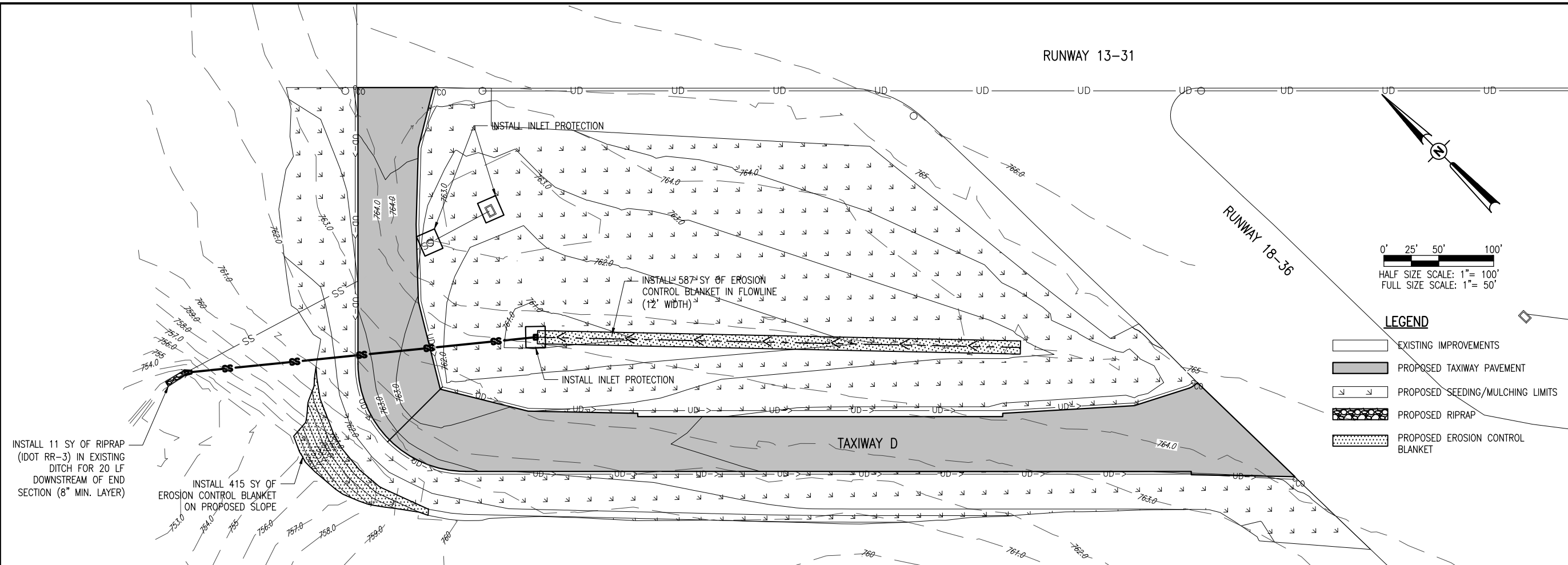
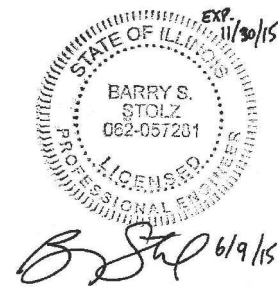
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DRAWN BY: JRH 06/09/2014  
REVIEWED BY: JDW 11/12/2014

SHEET TITLE

PAVEMENT MARKING  
DETAILS

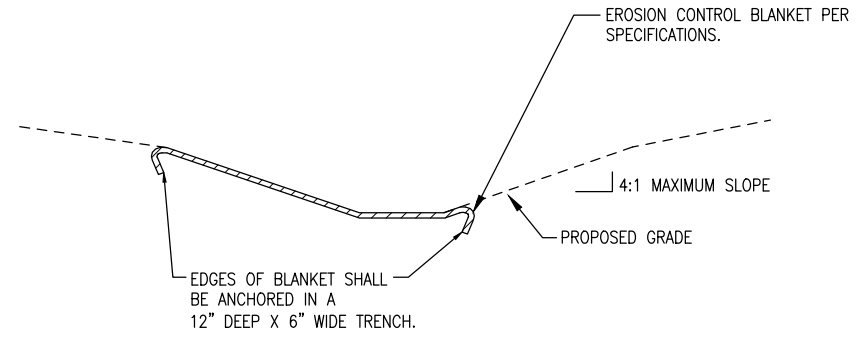


**LEGEND**

- EXISTING IMPROVEMENTS
- PROPOSED TAXIWAY PAVEMENT
- PROPOSED SEEDING/MULCHING LIMITS
- PROPOSED RIPRAP
- PROPOSED EROSION CONTROL BLANKET

**STORM WATER POLLUTION PREVENTION NOTES**

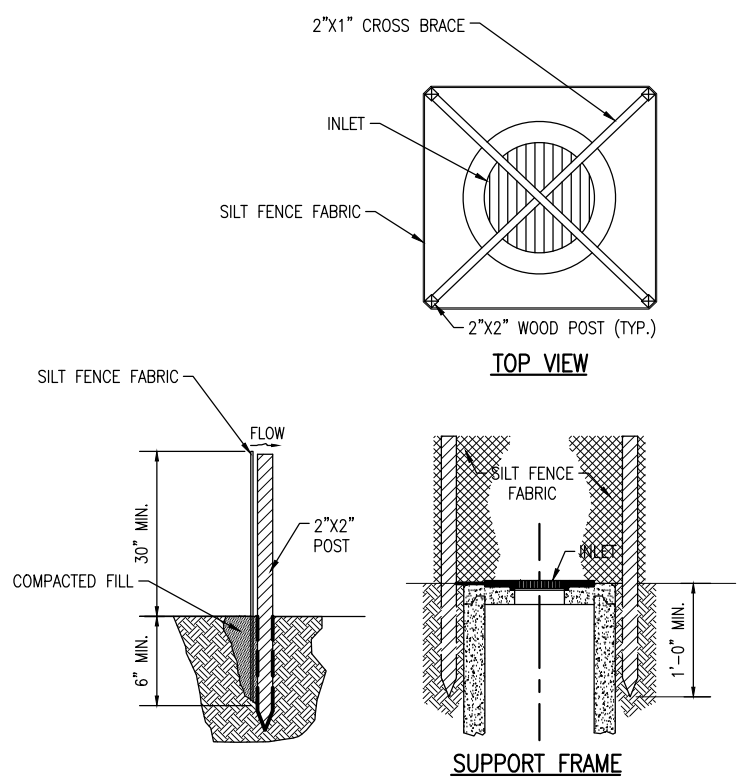
1. THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE. PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.
2. THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHERE EVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES.
3. POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EACH ITEM.
4. THE CONTRACTOR IS TO MAINTAIN AND ADJUST, REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.



NOTE: ALL EROSION CONTROL BLANKETS SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS BASED ON THE PROPOSED TYPE AND USE.

**TYPICAL EROSION CONTROL BLANKET**

NO SCALE



**FABRIC DROP INLET PROTECTION**

NOT TO SCALE

**REALIGN TAXIWAY D**

IDA No: UIN-4332

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DESIGN BY: JRH 06/09/2014  
DRAWN BY: JRH 06/09/2014  
REVIEWED BY: JDW 11/12/2014

SHEET TITLE

**PROPOSED STORMWATER POLLUTION PREVENTION PLAN**



*Kevin N. Lightfoot* 6/9/2015

**REALIGN TAXIWAY D**

IDA No: UIN-4332

SBG No: 3-17-SBGP-105

QI061


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

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PROJECT NO: 13A0080D

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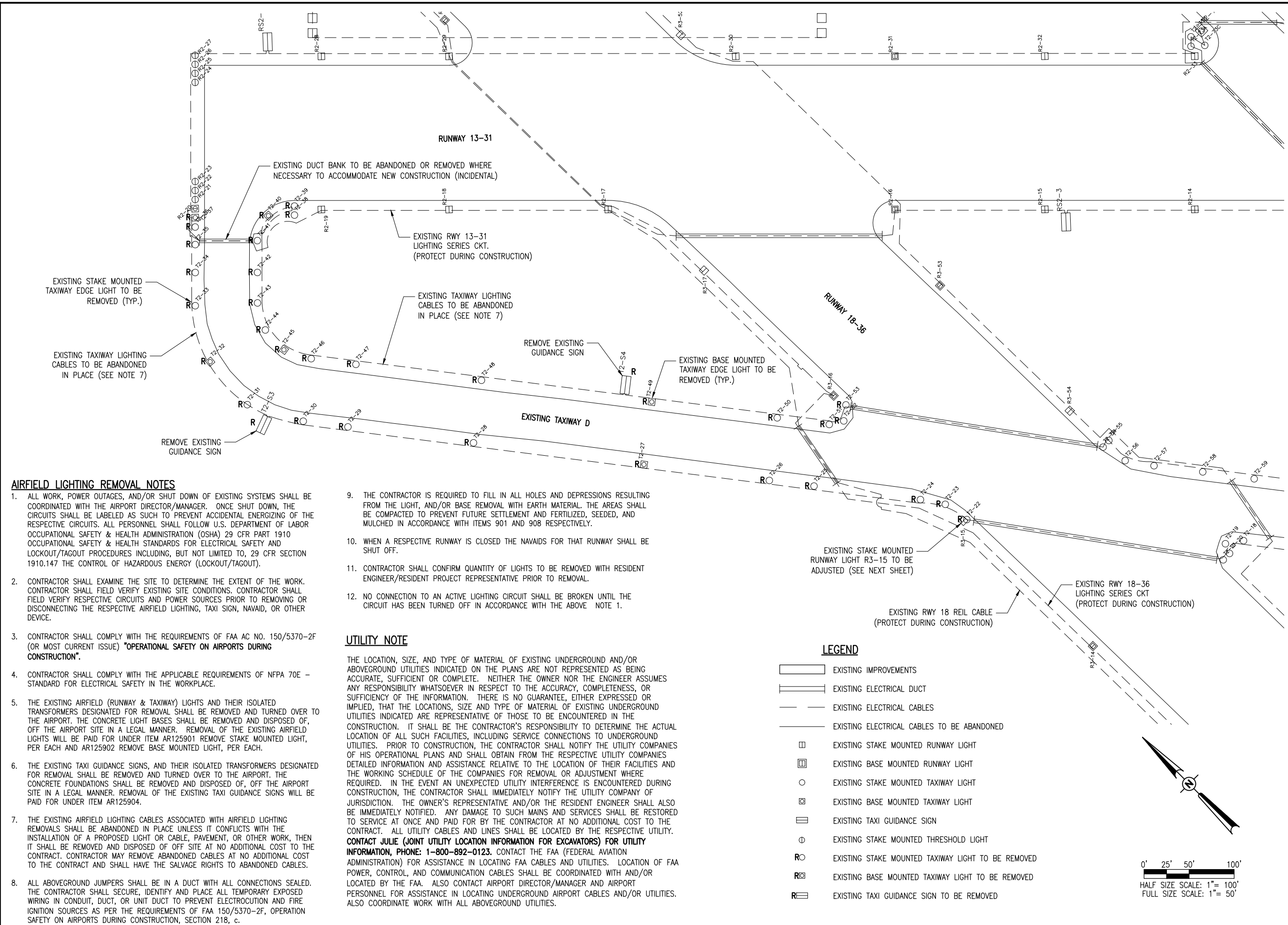
DESIGN BY: JRH 03/03/2014

DRAWN BY: MLH 10/30/2014

REVIEWED BY: KNL 10/30/2014

SHEET TITLE

**EXISTING ELECTRICAL PLAN**



**AIRFIELD LIGHTING REMOVAL NOTES**

- ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT DIRECTOR/MANAGER. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- CONTRACTOR SHALL EXAMINE THE SITE TO DETERMINE THE EXTENT OF THE WORK. CONTRACTOR SHALL FIELD VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING OR DISCONNECTING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, OR OTHER DEVICE.
- CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF FAA AC NO. 150/5370-2F (OR MOST CURRENT ISSUE) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- CONTRACTOR SHALL COMPLY WITH THE APPLICABLE REQUIREMENTS OF NFPA 70E - STANDARD FOR ELECTRICAL SAFETY IN THE WORKPLACE.
- THE EXISTING AIRFIELD (RUNWAY & TAXIWAY) LIGHTS AND THEIR ISOLATED TRANSFORMERS DESIGNATED FOR REMOVAL SHALL BE REMOVED AND TURNED OVER TO THE AIRPORT. THE CONCRETE LIGHT BASES SHALL BE REMOVED AND DISPOSED OF, OFF THE AIRPORT SITE IN A LEGAL MANNER. REMOVAL OF THE EXISTING AIRFIELD LIGHTS WILL BE PAID FOR UNDER ITEM AR125901 REMOVE STAKE MOUNTED LIGHT, PER EACH AND AR125902 REMOVE BASE MOUNTED LIGHT, PER EACH.
- THE EXISTING TAXI GUIDANCE SIGNS, AND THEIR ISOLATED TRANSFORMERS DESIGNATED FOR REMOVAL SHALL BE REMOVED AND TURNED OVER TO THE AIRPORT. THE CONCRETE FOUNDATIONS SHALL BE REMOVED AND DISPOSED OF, OFF THE AIRPORT SITE IN A LEGAL MANNER. REMOVAL OF THE EXISTING TAXI GUIDANCE SIGNS WILL BE PAID FOR UNDER ITEM AR125904.
- THE EXISTING AIRFIELD LIGHTING CABLES ASSOCIATED WITH AIRFIELD LIGHTING REMOVALS SHALL BE ABANDONED IN PLACE UNLESS IT CONFLICTS WITH THE INSTALLATION OF A PROPOSED LIGHT OR CABLE, PAVEMENT, OR OTHER WORK, THEN IT SHALL BE REMOVED AND DISPOSED OF OFF SITE AT NO ADDITIONAL COST TO THE CONTRACT. CONTRACTOR MAY REMOVE ABANDONED CABLES AT NO ADDITIONAL COST TO THE CONTRACT AND SHALL HAVE THE SALVAGE RIGHTS TO ABANDONED CABLES.
- ALL ABOVEGROUND JUMPERS SHALL BE IN A DUCT WITH ALL CONNECTIONS SEALED. THE CONTRACTOR SHALL SECURE, IDENTIFY AND PLACE ALL TEMPORARY EXPOSED WIRING IN CONDUIT, DUCT, OR UNIT DUCT TO PREVENT ELECTROCUTION AND FIRE. IGNITION SOURCES AS PER THE REQUIREMENTS OF FAA 150/5370-2F, OPERATION SAFETY ON AIRPORTS DURING CONSTRUCTION, SECTION 218, c.

- THE CONTRACTOR IS REQUIRED TO FILL IN ALL HOLES AND DEPRESSIONS RESULTING FROM THE LIGHT, AND/OR BASE REMOVAL WITH EARTH MATERIAL. THE AREAS SHALL BE COMPACTED TO PREVENT FUTURE SETTLEMENT AND FERTILIZED, SEED, AND MULCHED IN ACCORDANCE WITH ITEMS 901 AND 908 RESPECTIVELY.
- WHEN A RESPECTIVE RUNWAY IS CLOSED THE NAVAIDS FOR THAT RUNWAY SHALL BE SHUT OFF.
- CONTRACTOR SHALL CONFIRM QUANTITY OF LIGHTS TO BE REMOVED WITH RESIDENT ENGINEER/RESIDENT PROJECT REPRESENTATIVE PRIOR TO REMOVAL.
- NO CONNECTION TO AN ACTIVE LIGHTING CIRCUIT SHALL BE BROKEN UNTIL THE CIRCUIT HAS BEEN TURNED OFF IN ACCORDANCE WITH THE ABOVE NOTE 1.

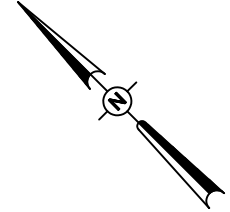
**UTILITY NOTE**

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY.

**CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123.** CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

**LEGEND**

- EXISTING IMPROVEMENTS
- EXISTING ELECTRICAL DUCT
- EXISTING ELECTRICAL CABLES
- EXISTING ELECTRICAL CABLES TO BE ABANDONED
- EXISTING STAKE MOUNTED RUNWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING TAXI GUIDANCE SIGN
- EXISTING STAKE MOUNTED THRESHOLD LIGHT
- EXISTING STAKE MOUNTED TAXIWAY LIGHT TO BE REMOVED
- EXISTING BASE MOUNTED TAXIWAY LIGHT TO BE REMOVED
- EXISTING TAXI GUIDANCE SIGN TO BE REMOVED



0' 25' 50' 100'  
HALF SIZE SCALE: 1" = 100'  
FULL SIZE SCALE: 1" = 50'





*Kevin N. Lightfoot*  
6/9/2015

**REALIGN TAXIWAY D**

IDA No: UIN-4332

SBG No: 3-17-SBGP-105

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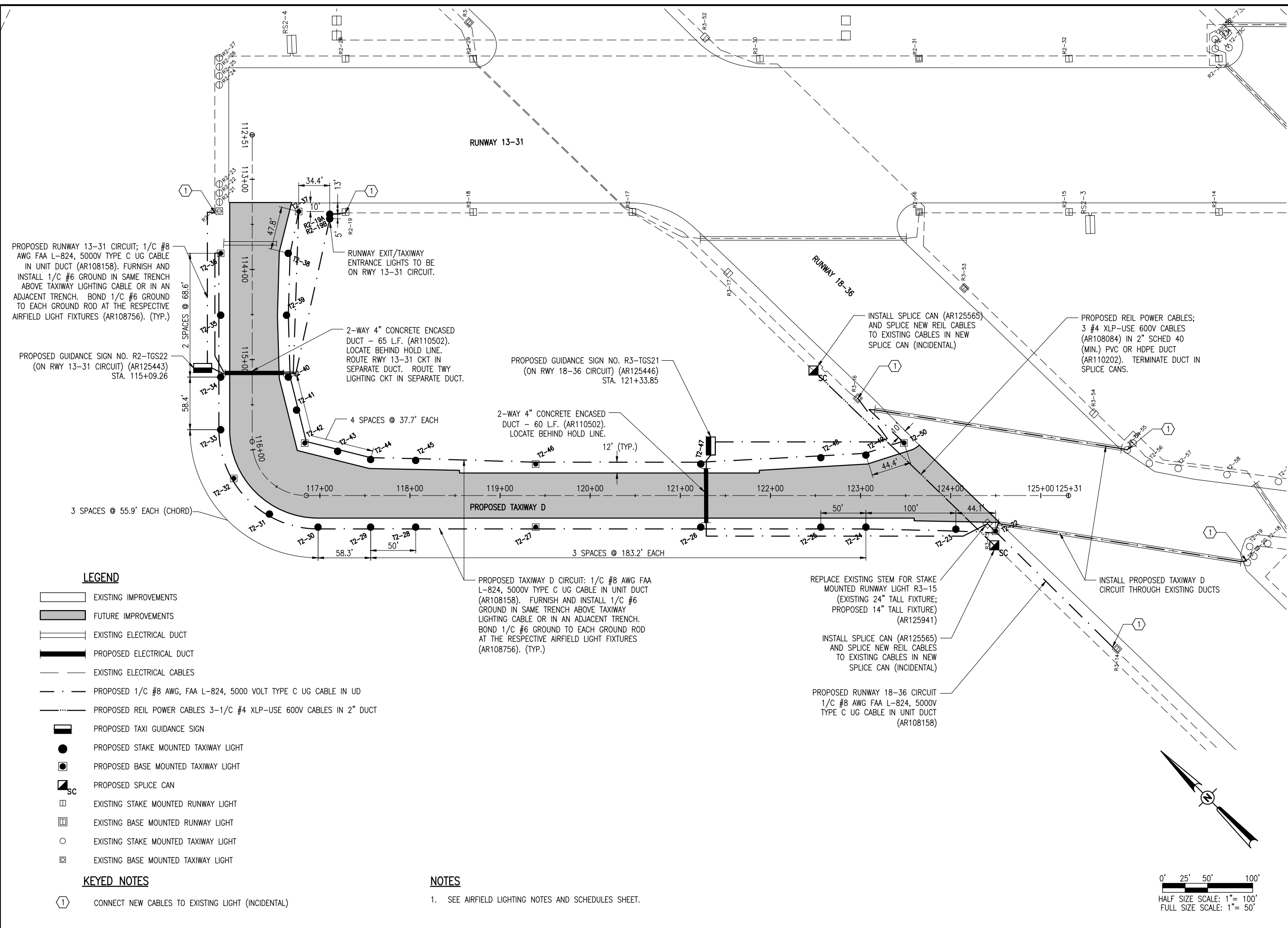
DESIGN BY: JRH 06/11/2014

DRAWN BY: JRH 06/11/2014

REVIEWED BY: KNL 10/30/2014

SHEET TITLE

**PROPOSED ELECTRICAL PLAN**



PROPOSED RUNWAY 13-31 CIRCUIT; 1/C #8 AWG FAA L-824, 5000V TYPE C UG CABLE IN UNIT DUCT (AR108158). FURNISH AND INSTALL 1/C #6 GROUND IN SAME TRENCH ABOVE TAXIWAY LIGHTING CABLE OR IN AN ADJACENT TRENCH. BOND 1/C #6 GROUND TO EACH GROUND ROD AT THE RESPECTIVE AIRFIELD LIGHT FIXTURES (AR108756). (TYP.)

PROPOSED GUIDANCE SIGN NO. R2-TGS22 (ON RWY 13-31 CIRCUIT) (AR125443) STA. 115+09.26

RUNWAY EXIT/TAXIWAY ENTRANCE LIGHTS TO BE ON RWY 13-31 CIRCUIT.

2-WAY 4" CONCRETE ENCASED DUCT - 65 L.F. (AR110502). LOCATE BEHIND HOLD LINE. ROUTE RWY 13-31 CKT IN SEPARATE DUCT. ROUTE TWY LIGHTING CKT IN SEPARATE DUCT.

PROPOSED GUIDANCE SIGN NO. R3-TGS21 (ON RWY 18-36 CIRCUIT) (AR125446) STA. 121+33.85

2-WAY 4" CONCRETE ENCASED DUCT - 60 L.F. (AR110502). LOCATE BEHIND HOLD LINE.

PROPOSED TAXIWAY D CIRCUIT; 1/C #8 AWG FAA L-824, 5000V TYPE C UG CABLE IN UNIT DUCT (AR108158). FURNISH AND INSTALL 1/C #6 GROUND IN SAME TRENCH ABOVE TAXIWAY LIGHTING CABLE OR IN AN ADJACENT TRENCH. BOND 1/C #6 GROUND TO EACH GROUND ROD AT THE RESPECTIVE AIRFIELD LIGHT FIXTURES (AR108756). (TYP.)

INSTALL SPLICE CAN (AR125565) AND SPLICE NEW REIL CABLES TO EXISTING CABLES IN NEW SPLICE CAN (INCIDENTAL)

PROPOSED REIL POWER CABLES; 3 #4 XLP-USE 600V CABLES (AR108084) IN 2" SCHED 40 (MIN.) PVC OR HDPE DUCT (AR110202). TERMINATE DUCT IN SPLICE CANS.

REPLACE EXISTING STEM FOR STAKE MOUNTED RUNWAY LIGHT R3-15 (EXISTING 24" TALL FIXTURE; PROPOSED 14" TALL FIXTURE) (AR125941)

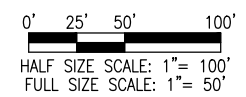
INSTALL SPLICE CAN (AR125565) AND SPLICE NEW REIL CABLES TO EXISTING CABLES IN NEW SPLICE CAN (INCIDENTAL)

PROPOSED RUNWAY 18-36 CIRCUIT 1/C #8 AWG FAA L-824, 5000V TYPE C UG CABLE IN UNIT DUCT (AR108158)

- LEGEND**
- EXISTING IMPROVEMENTS
  - FUTURE IMPROVEMENTS
  - EXISTING ELECTRICAL DUCT
  - PROPOSED ELECTRICAL DUCT
  - EXISTING ELECTRICAL CABLES
  - PROPOSED 1/C #8 AWG, FAA L-824, 5000 VOLT TYPE C UG CABLE IN UD
  - PROPOSED REIL POWER CABLES 3-1/C #4 XLP-USE 600V CABLES IN 2" DUCT
  - PROPOSED TAXI GUIDANCE SIGN
  - PROPOSED STAKE MOUNTED TAXIWAY LIGHT
  - PROPOSED BASE MOUNTED TAXIWAY LIGHT
  - PROPOSED SPLICE CAN
  - EXISTING STAKE MOUNTED RUNWAY LIGHT
  - EXISTING BASE MOUNTED RUNWAY LIGHT
  - EXISTING STAKE MOUNTED TAXIWAY LIGHT
  - EXISTING BASE MOUNTED TAXIWAY LIGHT

- KEYED NOTES**
- CONNECT NEW CABLES TO EXISTING LIGHT (INCIDENTAL)

- NOTES**
1. SEE AIRFIELD LIGHTING NOTES AND SCHEDULES SHEET.



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**AIRFIELD LIGHTING NOTES**

- ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT DIRECTOR/MANAGER. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- CONTRACTOR SHALL FIELD VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING OR DISCONNECTING THE RESPECTIVE AIRFIELD LIGHTING, NAVAID, OR OTHER DEVICE.
- PROPOSED AIRFIELD AND TAXIWAY LIGHTS SHALL BE PLACED 10' (FT.) FROM THE PAVEMENT EDGE UNLESS SHOWN OTHERWISE ON THESE CONSTRUCTION DRAWINGS. RUNWAY EXIT/TAXIWAY ENTRANCE LIGHTS SHALL BE PLACED AT 13 FT AND 18 FT FROM RUNWAY PAVEMENT EDGE AS DETAILED HEREIN. PROPOSED TAXI GUIDANCE SIGNS SHALL BE LOCATED SUCH THAT THE CLOSEST SIDE OF THE SIGN IS 15' FROM THE PAVEMENT EDGE, UNLESS SHOWN OTHERWISE.
- PROPOSED AIRFIELD LIGHTS, TAXIWAY LIGHTS, GUIDANCE SIGNS, OTHER AIRFIELD LIGHTING, SPLICE CANS, HANDHOLES, MANHOLES, ELECTRICAL DUCTS, AND CABLE SHALL BE INSTALLED AT THE LOCATIONS SHOWN AND IN COMPLIANCE WITH THE SPECIFICATIONS, SPECIAL PROVISIONS, RESPECTIVE DETAILS, AND MANUFACTURER'S RECOMMENDATIONS.
- PROPOSED CABLE FOR RUNWAY AND TAXIWAY LIGHTING SHALL BE INSTALLED APPROXIMATELY 12' FROM THE PAVEMENT EDGE. CABLES SHALL BE PLACED A MINIMUM OF 18" BELOW FINISHED GRADE.
- THE PROPOSED RUNWAY AND TAXIWAY LIGHTING CABLE SHALL BE 1/C, #8 AWG, FAA L-824, 5000 VOLT, TYPE C UNDERGROUND CABLE IN UNIT DUCT.
- IN AREAS WHERE THERE IS A CONGESTION OF CABLES OR WHERE THE PROPOSED CABLE CROSSES AN EXISTING CABLE, THE CONTRACTOR IS REQUIRED TO HAND DIG THE TRENCH NECESSARY FOR THE PROPOSED CABLE. AT OTHER LOCATIONS, THE PROPOSED CABLE MAY BE TRENCHED OR PLOWED INTO PLACE. HAND DIGGING, TRENCHING AND/OR PLOWING WILL BE CONSIDERED INCIDENTAL TO THE PROPOSED CABLES AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ALL PROPOSED TAXIWAY LIGHTS WILL BE FITTED WITH 360° BLUE LENSES.
- ALL PROPOSED RUNWAY, THRESHOLD, TAXIWAY LIGHTS, AND TAXI GUIDANCE SIGNS SHALL BE TAGGED BY THE CONTRACTOR IN ACCORDANCE WITH THE LIGHT NUMBERS SHOWN ON THESE CONSTRUCTION DRAWINGS.
- SEE "TAXI GUIDANCE SIGN SCHEDULE" FOR INFO ON SIGN LEGENDS.
- RUNWAY EXIT/TAXIWAY ENTRANCE LIGHTS (DUAL TAXIWAY LIGHTS TO DEFINE THE THROAT OR ENTRANCE INTO THE INTERSECTING TAXIING ROUTE) SHALL BE CONNECTED TO THE RESPECTIVE RUNWAY SERIES CIRCUIT TO BE ILLUMINATED WHEN THE RUNWAY EDGE LIGHTS ARE ON TO COMPLY WITH FAA AC150/5340-30H PART 2.1.4b(4).
- HOLDING POSITION SIGNS FOR RUNWAYS SHALL BE CONNECTED TO THE RESPECTIVE RUNWAY SERIES CIRCUIT TO BE ILLUMINATED WHEN THE ASSOCIATED RUNWAY LIGHTS ARE ILLUMINATED TO COMPLY WITH FAA AC150/5340-18F, CHAPTER 1, PART 15 "SIGN OPERATION".
- THE CONTRACTOR SHALL SECURE, IDENTIFY AND PLACE ALL TEMPORARY EXPOSED WIRING IN CONDUIT, DUCT OR UNIT DUCT TO PREVENT ELECTROCUTION AND FIRE IGNITION SOURCES AS PER THE REQUIREMENTS OF FAA AC 150/5370-2F, PART 218, PARAGRAPH C. ALL LABOR, MATERIALS, AND TIME NECESSARY TO COMPLY WITH THIS REQUIREMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- HOMERUN CABLES FOR A RESPECTIVE CIRCUIT THAT ARE INSTALLED IN CONDUIT OR DUCT SHALL BE RUN TOGETHER IN THE SAME RACEWAY OR DUCT.
- EXISTING AIRFIELD LIGHTING CABLES (SCHEDULED FOR REPLACEMENT) IN AREAS OF NEW WORK SHALL BE DISCONNECTED & REMOVED WHERE IN CONFLICT WITH NEW CONSTRUCTION. IN OTHER AREAS CABLES MAY BE ABANDONED IN PLACE.
- THE CONTRACTOR IS REQUIRED TO FILL IN ALL HOLES AND DEPRESSIONS RESULTING FROM THE NEW WORK, WITH EARTH MATERIAL. THE AREAS SHALL BE COMPACTED TO PREVENT FUTURE SETTLEMENT AND FERTILIZED, SEEDED, AND MULCHED IN ACCORDANCE WITH ITEMS 901 AND 908 RESPECTIVELY.
- IN THE EVENT THAT OTHER CONSTRUCTION PROJECTS ARE IN PROGRESS AT THE AIRPORT AT THE SAME TIME AS THIS PROJECT, THE CONTRACTOR WILL BE REQUIRED TO COOPERATE WITH ALL OTHER CONTRACTORS AND THE AIRPORT MANAGER IN THE COORDINATION OF THE WORK.
- NO CONNECTION TO AN ACTIVE LIGHTING CIRCUIT WILL BE BROKEN UNTIL THE CIRCUIT HAS BEEN TURNED OFF IN ACCORDANCE WITH NOTE 1.

TAXI GUIDANCE SIGN SCHEDULE			
SIGN NUMBERS	LOCATION	SIDE A	SIDE B
R2-TGS22	TAXIWAY D INTERSECTION WITH RUNWAY 13 AT HOLD LINE	D 13	D BLANK
R3-TGS21	TAXIWAY D INTERSECTION WITH RUNWAY 18-36 AT HOLD LINE	D 18-36	13 ↑

**TAXI GUIDANCE SIGN SCHEDULE**

- D** TYPE L-858L LOCATION SIGN – YELLOW LEGEND AND BORDER ON A BLACK BACKGROUND
- 18-36** TYPE L-858R MANDATORY INSTRUCTION SIGN – BLACK OUTLINE ON OUTSIDE EDGE OF WHITE LEGEND ON A RED BACKGROUND
- RAMP ↑** TYPE L-858Y DIRECTION, DESTINATION, AND BOUNDARY SIGN – BLACK LEGEND ON A YELLOW BACKGROUND
- BLANK** BLANK – BLACK BACKGROUND

**TAXI GUIDANCE SIGN NOTES**

- THE PROPOSED TAXI GUIDANCE SIGNS SHALL CONFORM TO ADVISORY CIRCULAR 150/5345-44J (OR LATEST ISSUE IN FORCE) AND BE FAA-APPROVED FOR TYPE L-858Y DIRECTION, DESTINATION, AND BOUNDARY SIGNS (BLACK LEGEND ON YELLOW BACKGROUND); TYPE L-858R MANDATORY INSTRUCTION SIGN (BLACK OUTLINE ON OUTSIDE EDGE OF WHITE LEGEND ON RED BACKGROUND); AND/OR TYPE L-858L LOCATION SIGN (YELLOW LEGEND AND BORDER ON BLACK BACKGROUND).
- THE SIGNS SHALL BE SIZE 1, 18-IN. SIGN FACE WITH A 12-IN. LEGEND; STYLE 2, POWERED FROM A 4.8 TO 6.6 AMP SERIES LIGHTING CIRCUIT; CLASS 2, FOR OPERATION FROM -40 DEGREES F TO 131 DEGREES F; MODE 2, TO WITHSTAND WIND LOADS OF 200 M.P.H., BASE-MOUNTED, DOUBLE-SIDED, AS SPECIFIED ON THE PLANS.
- THE EXISTING GUIDANCE SIGNS NOTED FOR REMOVAL ARE LUMACURVE BRAND.
- THE PROPOSED TAXI GUIDANCE SIGNS SHALL BE LOCATED SUCH THAT THE CLOSEST SIDE OF THE SIGN IS 15' FROM THE PAVEMENT EDGE OR RESPECTIVE RUNWAY SURFACE EDGE.
- ALL PROPOSED TAXI GUIDANCE SIGNS SHALL BE TAGGED BY THE CONTRACTOR IN ACCORDANCE WITH THE SIGN NUMBERS SHOWN ON THESE CONSTRUCTION DRAWINGS.
- THE PROPOSED LIGHTED TAXI GUIDANCE SIGNS WILL BE PAID FOR UNDER THE FOLLOWING ITEMS:  
AR125443 TAXI GUIDANCE SIGN, 3 CHARACTER \_\_\_\_\_ PER EACH.  
AR125446 TAXI GUIDANCE SIGN, 6 CHARACTER \_\_\_\_\_ PER EACH.

LIGHT LENS SCHEDULE			
LIGHT NUMBERS	LENS	ORIENTATION	FIXTURE TYPE
T2-22 TO T2-50	BLUE	---	L-861T
R2-19A TO R2-19B	BLUE	---	L-861T

**TAXIWAY LIGHT FIXTURE NOTES**

- THE PROPOSED TAXIWAY LIGHT FIXTURES SHALL CONFORM TO ADVISORY CIRCULAR 150/5345-46D (OR LATEST ISSUE IN FORCE) AND BE FAA APPROVED FOR TYPE L-861T.



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Quincy, IL 62305



*Kevin N. Lightfoot* 6/9/2015

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REVIEWED BY: KNL 11/03/2014

SHEET TITLE

**AIRFIELD LIGHTING NOTES AND SCHEDULES**



*Kevin N. Lightfoot*  
6/30/2015

REALIGN TAXIWAY D

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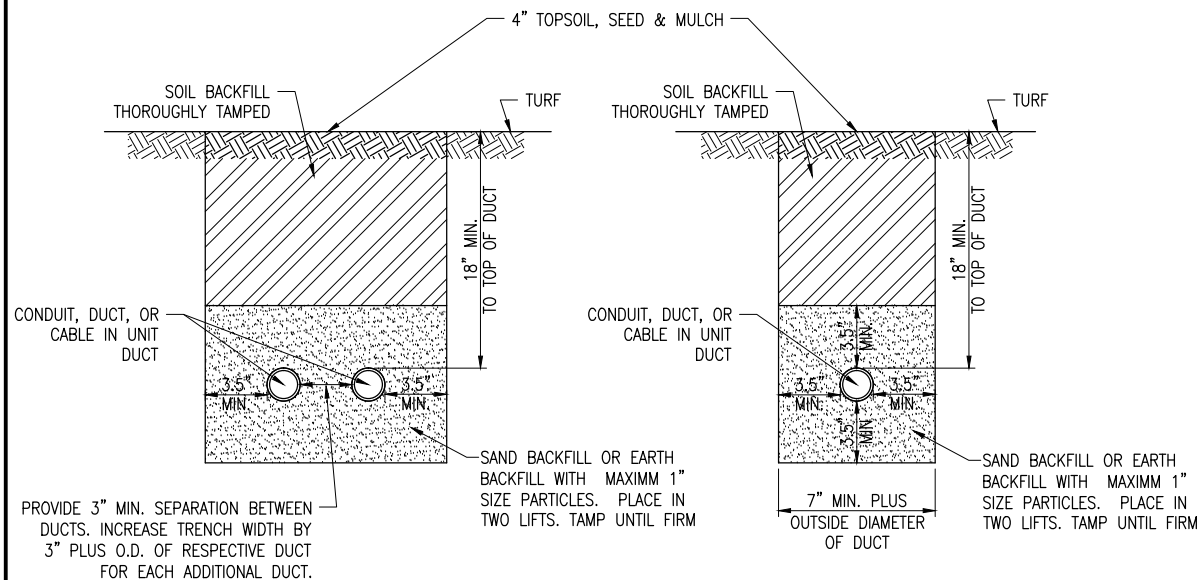
DESIGN BY: KNL 10/28/2014

DRAWN BY: MLH 11/03/2014

REVIEWED BY: KNL 11/05/2014

SHEET TITLE

ELECTRICAL DETAILS  
SHEET 1

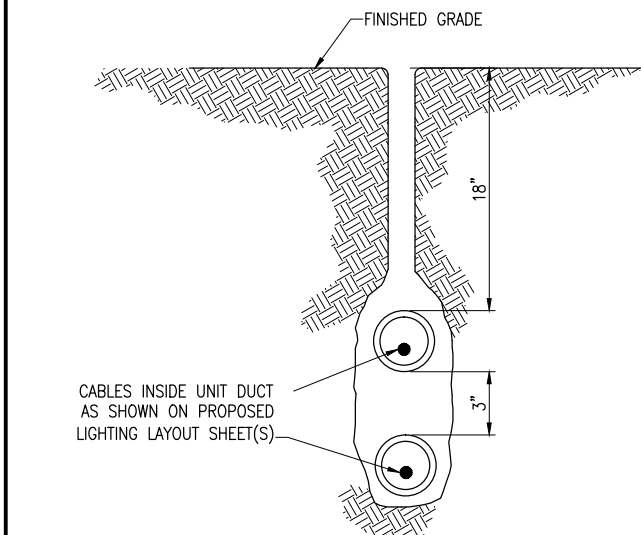


**CONDUIT IN TRENCH – NON-PAVEMENT AREAS**

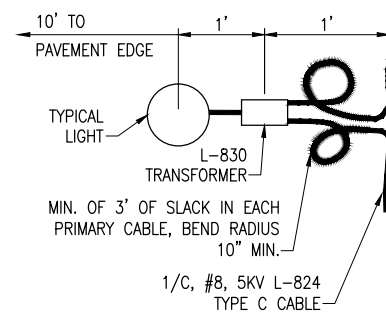
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**NOTES:**

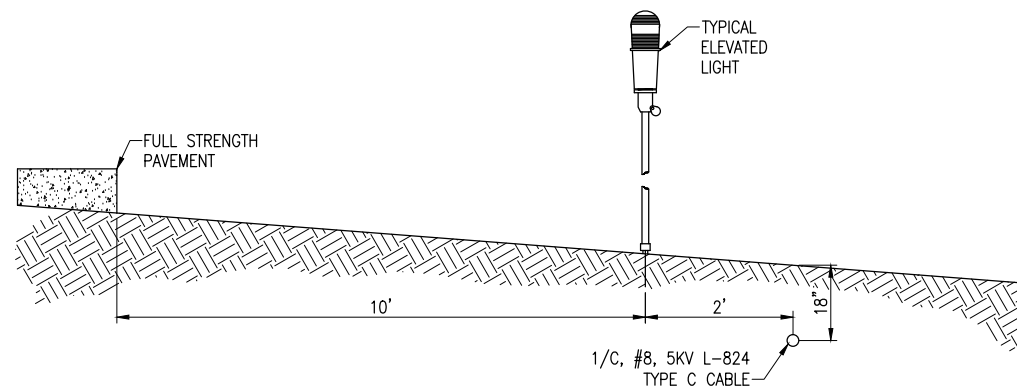
- DIMENSIONS FOR COVERAGE AND SEPARATION BETWEEN DUCTS ARE MINIMUM.
- TRENCHES WITH MORE THAN TWO DUCTS OR CABLE IN UNIT DUCTS SHALL BE INCREASED 3" IN WIDTH PLUS DIAMETER OF RESPECTIVE DUCT FOR EACH ADDITIONAL CONDUIT, DUCT, OR CABLE IN UNIT DUCT; IF SPECIFIED ON PLANS TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS. MINIMUM COVER REQUIREMENTS FOR CABLES AND DUCTS AT AIRPORT RUNWAYS AND ADJACENT AREAS WHERE TRESPASSING IS PROHIBITED IS 18 INCHES PER NEC 300.5 AND 300.50. COVER IS DEFINED AS THE SHORTEST DISTANCE IN INCHES MEASURED BETWEEN A POINT ON THE TOP SURFACE OF ANY DIRECT-BURIED CONDUCTOR, CABLE, CONDUIT, OR OTHER RACEWAY AND THE TOP SURFACE OF FINISHED GRADE, CONCRETE OR SIMILAR COVER.
- HIGH VOLTAGE AND LOW VOLTAGE CIRCUITS SHALL NOT BE INSTALLED IN THE SAME RACEWAY, CONDUIT, DUCT, HANDHOLE, OR MANHOLE.
- DUCT INTERFACE TO HANDHOLES, MANHOLES, SPLICE CANS, OR OTHER JUNCTION STRUCTURES WILL BE CONSIDERED INCIDENTAL TO THE RESPECTIVE CABLE IN UNIT DUCT PAY ITEM OR RESPECTIVE DUCT PAY ITEM.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH.



**PLOWED CABLE**  
(NOT TO SCALE)



**PLAN VIEW**

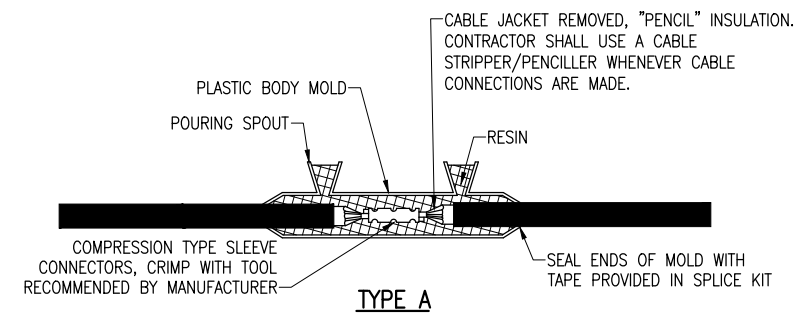


**PROFILE VIEW**

**LIGHT AND CABLE INSTALLATION DETAIL**

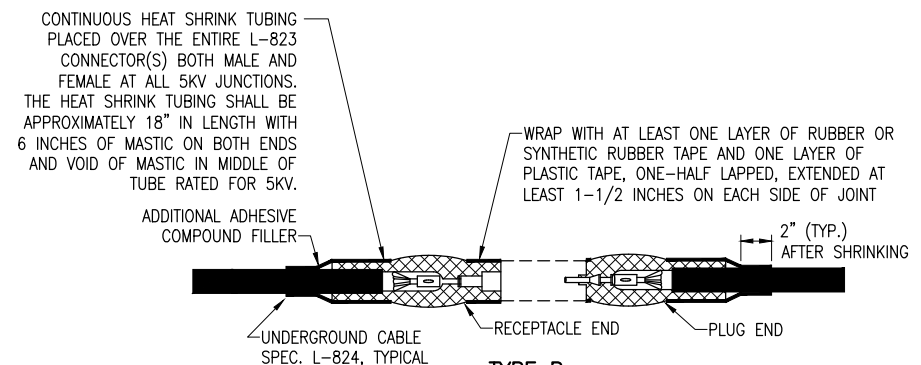
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NOTES:  
SEE PROPOSED LIGHTING LAYOUT SHEET FOR LIGHT LOCATIONS.



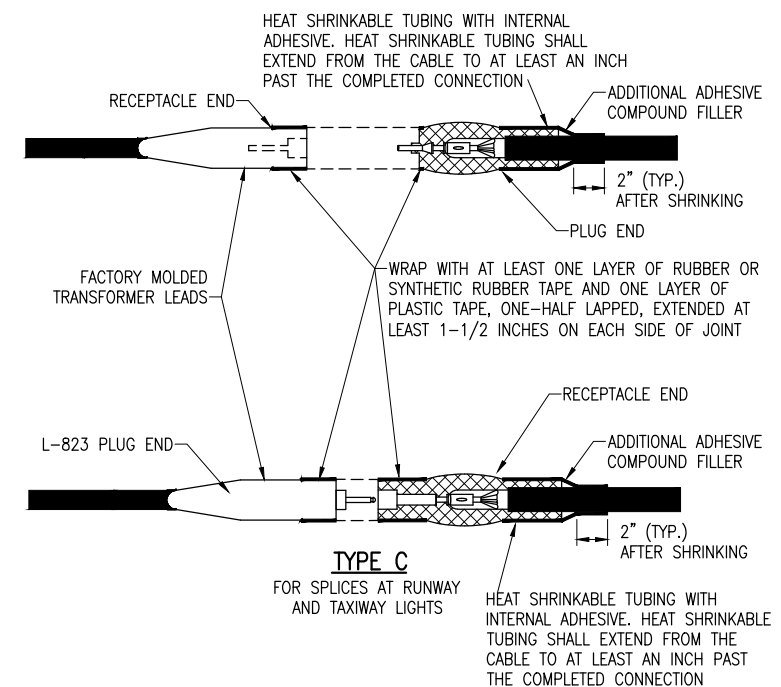
**TYPE A**

FOR SPLICES IN LOW VOLTAGE CABLE (600V) HOMERUNS FOR EXTENSIONS TO EXISTING LOW VOLTAGE CABLES ONLY



**TYPE B**

FOR SPLICES AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT AND FOR SPLICES IN HOMERUNS TO EXISTING CABLES



**TYPE C**

FOR SPLICES AT RUNWAY AND TAXIWAY LIGHTS

**CABLE SPLICES**

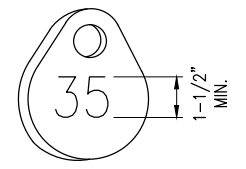
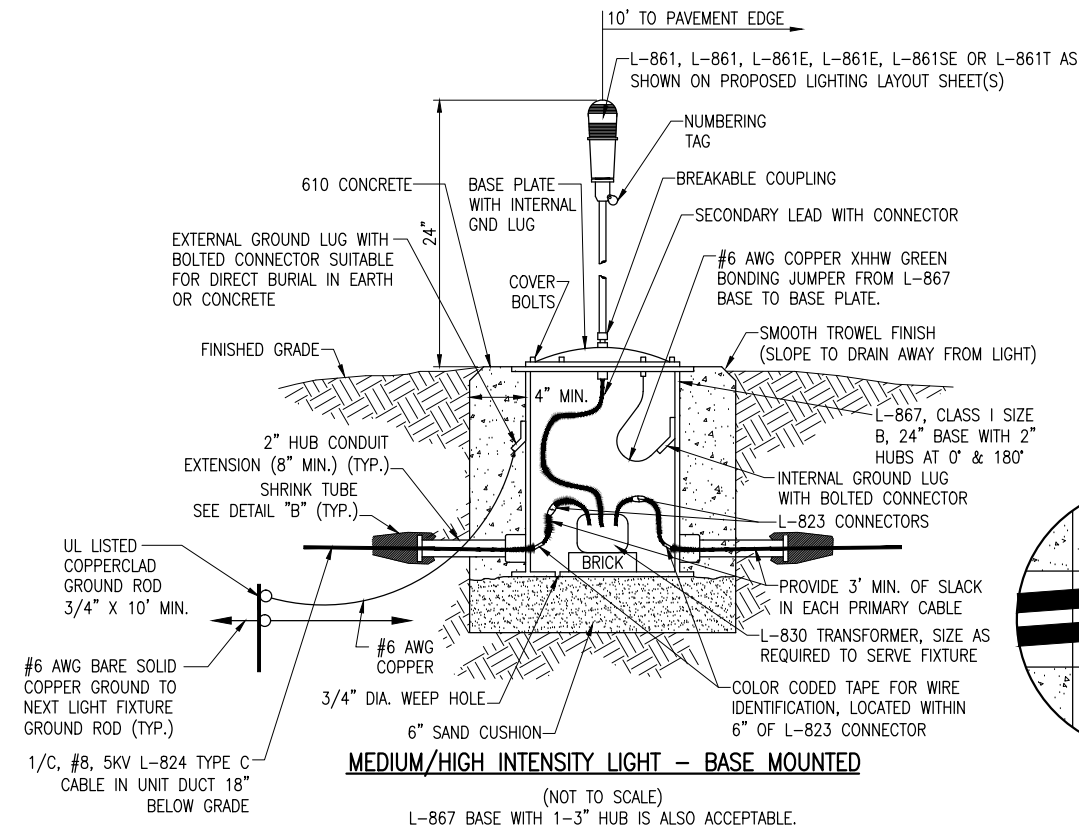
(NOT TO SCALE)

NOTES:  
SEE PROPOSED LIGHTING LAYOUT SHEET(S) FOR SPLICE TYPE.  
INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.



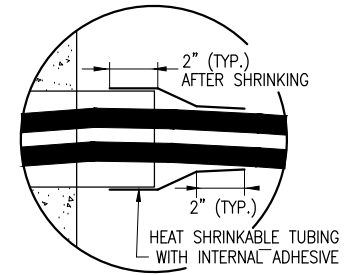


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6/9/2015

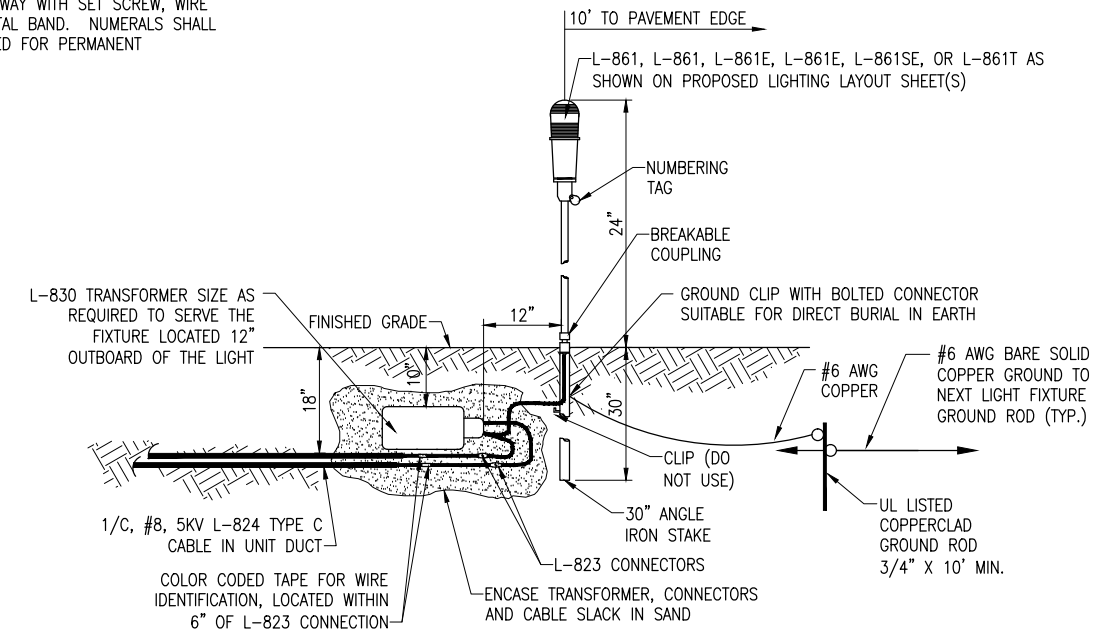


**NOTE:**  
AFFIX NON-CORROSIVE TAG TO FIXTURE FACING RUNWAY WITH SET SCREW, WIRE TIE, OR METAL BAND. NUMERALS SHALL BE ENGRAVED FOR PERMANENT READABILITY.

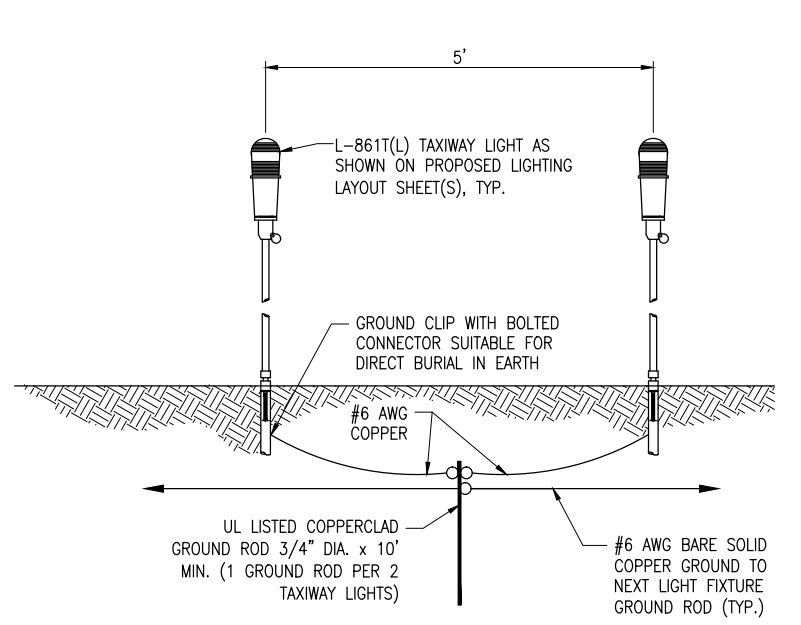
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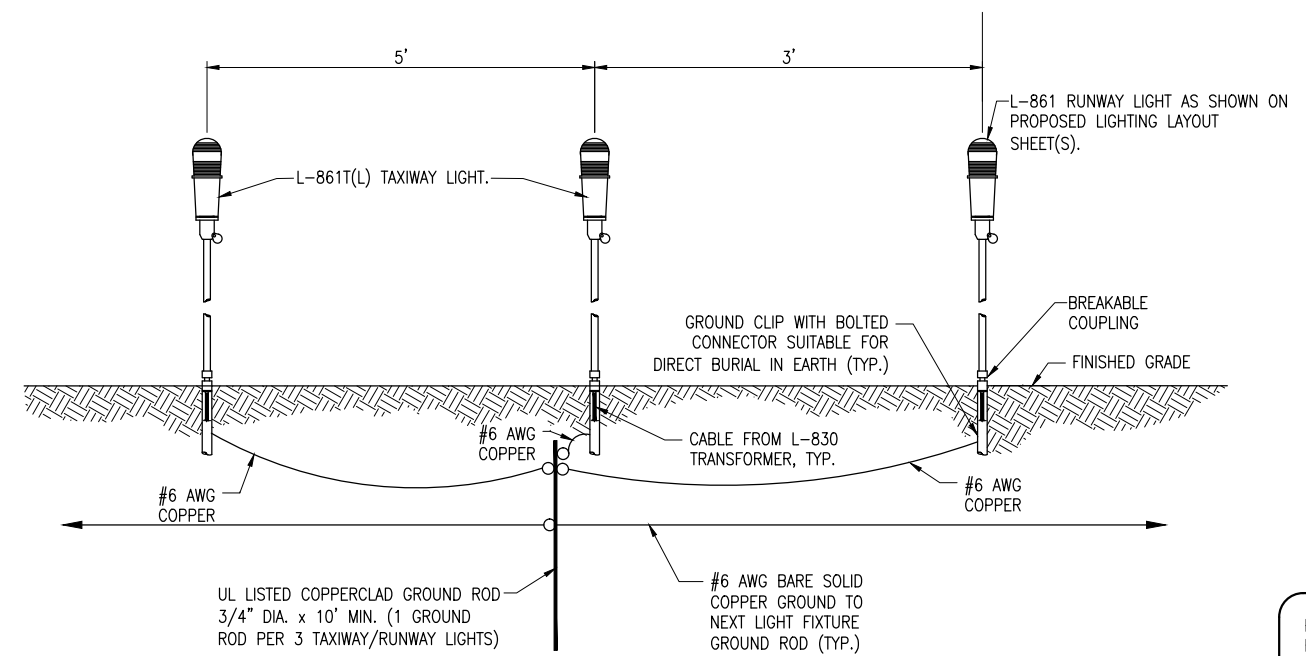
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(NOT TO SCALE)



**MEDIUM INTENSITY LIGHT - STAKE MOUNTED**  
(NOT TO SCALE)



**GROUNDING DETAIL FOR ADJACENT TAXIWAY LIGHTS**  
(NOT TO SCALE)



**GROUNDING DETAIL FOR ADJACENT RUNWAY AND TAXIWAY LIGHTS**  
(NOT TO SCALE)

**NOTES:**

- SEE "ELECTRICAL NOTES SHEETS". SEE "ELECTRICAL NOTES SHEET 2" AND "GROUNDING NOTES" SHEET FOR GROUNDING NOTES FOR AIRFIELD LIGHTING.
- RUNWAY EXIT/TAXIWAY ENTRANCE LIGHTS (DUAL TAXIWAY LIGHTS TO DEFINE THE THROAT OR ENTRANCE INTO THE INTERSECTING TAXING ROUTE) SHALL BE CONNECTED TO THE RESPECTIVE RUNWAY SERIES CIRCUIT TO BE ILLUMINATED WHEN THE RUNWAY EDGE LIGHTS ARE ON TO COMPLY WITH FAA AC 150/5340-30H, PART 2.1.4b(4).

PER FAA AC 150/5340-30H DESIGN AND INSTALLATION DETAILS FOR AIRPORT VISUAL AIDS, A LIGHT BASE GROUND MUST BE INSTALLED AT EACH LIGHT FIXTURE. A LIGHT BASE GROUND SHALL BE INSTALLED AT EACH STAKE MOUNTED LIGHT AND EACH TRANSFORMER BASE/LIGHT CAN ASSOCIATED WITH RUNWAY LIGHTS, TAXIWAY LIGHTS, AND LIGHTED TAXI GUIDANCE SIGNS. THE LIGHT BASE GROUND SHALL BE A #6 AWG BARE COPPER CONDUCTOR CONNECTED TO THE GROUND LUG ON THE RESPECTIVE L-867 TRANSFORMER BASE/LIGHT CAN OR MOUNTING STAKE AND A 3/4-INCH DIAMETER BY 10-FOOT LONG (MINIMUM) UL LISTED COPPER CLAD GROUND ROD.

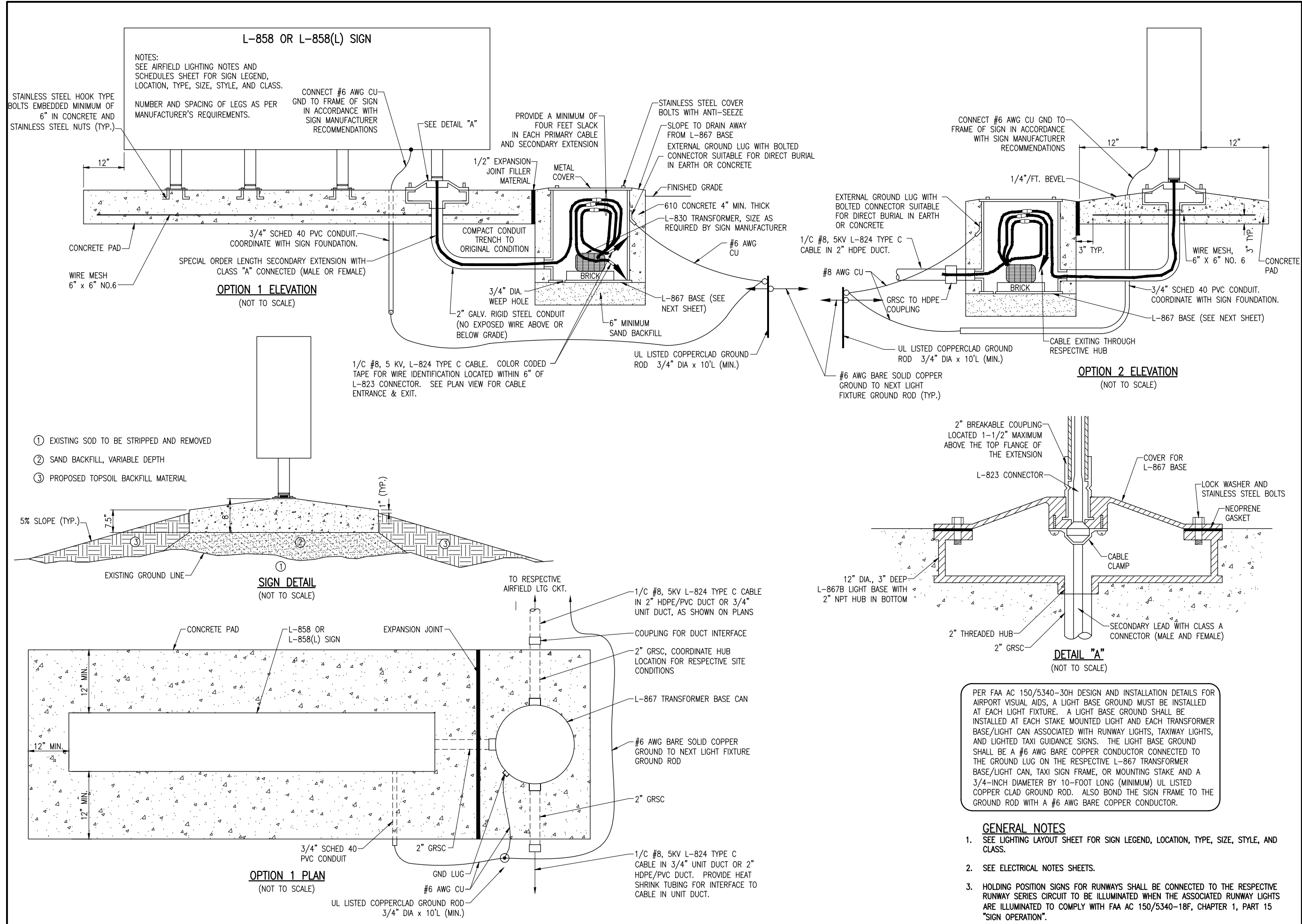
REALIGN TAXIWAY D  
IDA No: UIN-4332  
SBG No: 3-17-SBGP-105  
Q1061

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

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PROJECT NO: 13A0080D  
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DESIGN BY: KNL 11/03/2014  
DRAWN BY: RAD 11/03/2014  
REVIEWED BY: KNL 11/05/2014

SHEET TITLE

ELECTRICAL DETAILS  
SHEET 2



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**REALIGN TAXIWAY D**  
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DRAWN BY: RAD 10/30/2014  
REVIEWED BY: KNL 11/05/2014

SHEET TITLE  
**ELECTRICAL DETAILS SHEET 3**



**REALIGN TAXIWAY D**

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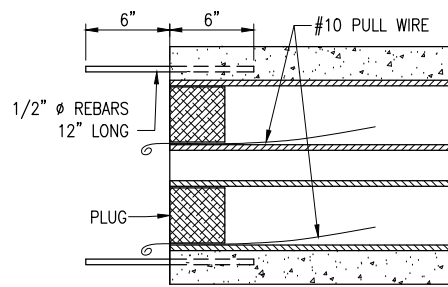
DESIGN BY: KNL 10/29/2014

DRAWN BY: RAD 10/30/2014

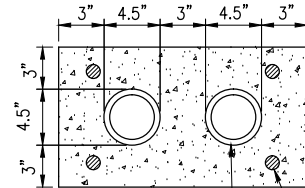
REVIEWED BY: KNL 11/05/2014

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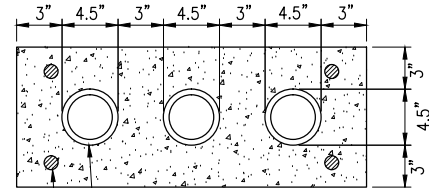
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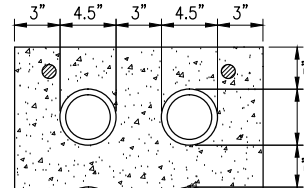
**TYPICAL SECTION**  
(NOT TO SCALE)



**2-DUCT BANK**  
(NOT TO SCALE)



**3-DUCT BANK**  
(NOT TO SCALE)



**4-DUCT BANK**  
(NOT TO SCALE)

1/2" Ø REBAR  
3' LONG IN EACH  
CORNER WHERE  
APPLICABLE (TYP.)

4" I.D. DUCT AS FOR 4"  
DUCT. SIZE OF DUCT SHALL  
BE AS DETAILED HEREIN FOR  
RESPECTIVE APPLICATION

4" I.D. CONDUIT

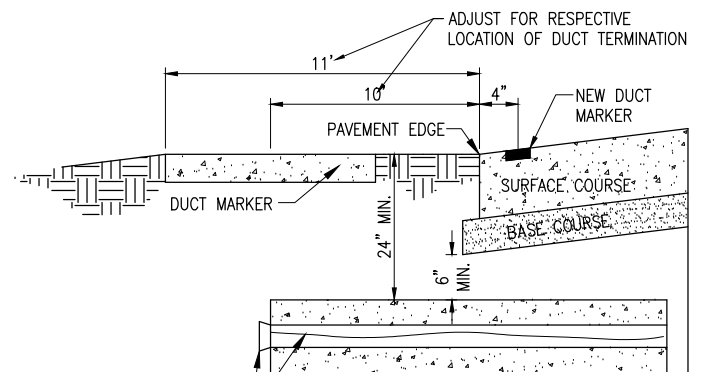
1/2" Ø REBAR  
3' LONG IN EACH  
CORNER WHERE  
APPLICABLE (TYP.)

**DUCT BANK NOTES:**

- DIMENSIONS FOR CONCRETE COVERAGE AND SEPARATION BETWEEN DUCTS ARE MINIMUM.
- INCLUDE DUCT SPACERS AS MANUFACTURED BY UNDERGROUND DEVICES INC., OR APPROVED EQUAL TO MAINTAIN PROPER SEPARATION OF CONDUITS.
- PROVIDE REBAR WHERE APPLICABLE TO ACCOMMODATE INTERFACE OF CONCRETE ENCASED DUCT BANKS TERMINATING IN HANDHOLE. PROVIDE REBAR WHERE APPLICABLE TO EXTEND AN EXISTING CONCRETE ENCASED DUCT BANK. REBAR SHALL CONFORM TO THE REQUIREMENTS OF ASTM A 706, GRADE 60.
- CONDUITS FOR CONCRETE ENCASED DUCT SHALL BE SCHEDULE 40 PVC CONFORMING TO ITEM 110.
- MINIMUM DEPTH OF TOP OF DUCT ENCASEMENT SHALL BE 18" BELOW FINISHED GRADE.
- HIGH VOLTAGE AND LOW VOLTAGE CIRCUITS SHALL NOT BE INSTALLED IN THE SAME RACEWAY, CONDUIT, DUCT, HANDHOLE, OR MANHOLE.
- HOMERUN CABLES FOR A RESPECTIVE CIRCUIT SHALL BE INSTALLED IN THE SAME RACEWAY OR DUCT.
- DUCT INTERFACE TO HANDHOLES OR MANHOLES WILL BE CONSIDERED INCIDENTAL TO THE RESPECTIVE DUCT PAY ITEM.

**CABLE & DUCT MARKER NOTES:**

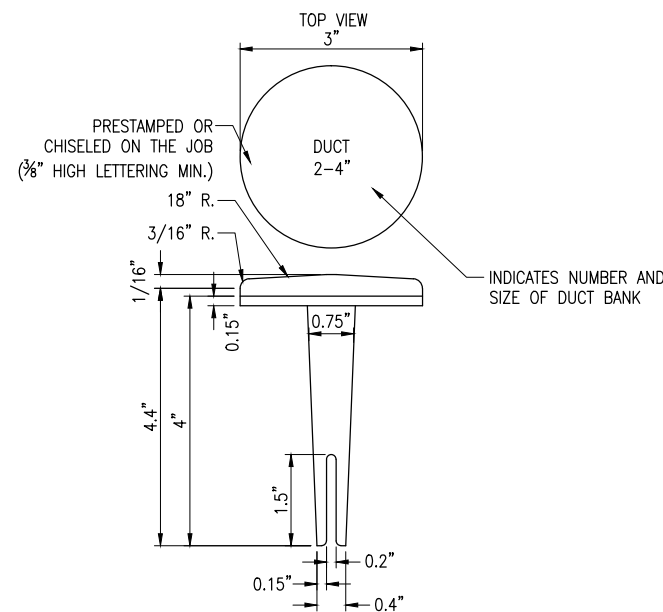
- THE COST OF ALL TURF AND PAVEMENT DUCT MARKERS SHALL BE INCIDENTAL TO THE DUCT. THE COST OF ALL CABLE MARKERS SHALL BE INCIDENTAL TO THE CABLE.
- BITUMINOUS PAVEMENT DUCT MARKER AND CONCRETE DUCT MARKER TO BE PROVIDED AT EACH END OF EACH DUCT AS SHOWN ON THE LOCATION PLAN. FOR CONCRETE PAVEMENT, THE LETTER "D" SHALL BE IMPRESSED IN THE PAVEMENT INSTEAD OF THE MARKER. THE LETTER SHALL BE FORMED AS DESCRIBED IN NOTE 4.
- CABLE MARKERS SHALL BE PLACED AT CHANGES OF DIRECTION AND APPROXIMATELY EVERY 200' ALONG CABLE RUNS.
- CONCRETE CABLE MARKERS AND DUCT MARKERS SHALL HAVE LETTERS 4" HIGH, 3" WIDE WITH WIDTH OF STROKE 1/2" AND 1/4" DEEP. ALL LETTERS, NUMBERS AND ARROWS TO BE IMPRESSED.
  - REDUCE LETTER SIZE TO 3" HIGH, 2" WIDE.
  - INCREASE THE MARKER SIZE TO 30" X 30".
  - PROVIDE ADDITIONAL MARKERS PLACED SIDE BY SIDE.
- EMPLOY THE FOLLOWING METHODS WERE ADDITIONAL SPACE TO FIT LEGEND IS REQUIRED:



**UNDERGROUND ELECTRICAL DUCT**

(NOT TO SCALE)

#10 PULL WIRE COIL A MINIMUM OF 3' AT DUCT ENDS.  
INSTALL APPROVED PLUGS IN END OF DUCTS NOT USED.

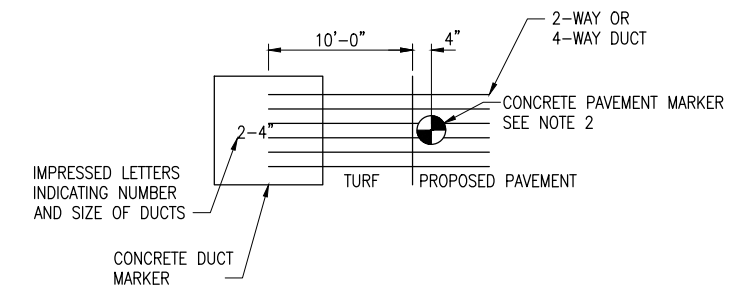


**BITUMINOUS PAVEMENT DUCT MARKERS**

(NOT TO SCALE)

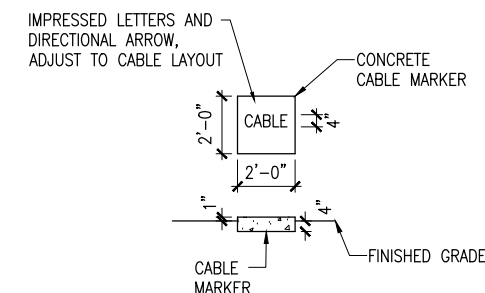
**NOTES:**

- TOP OF MARKER SHALL BE FLUSH WITH FINISHED PAVEMENT SURFACE. MARKER MAY BE INSTALLED IN A DRILLED HOLE AND SECURED WITH EPOXY GLUE.
- BRASS DUCT MARKERS ARE AVAILABLE FROM G&S FOUNDRY & MANUFACTURING CO., INC., 210 KASKASKIA DRIVE, RED BUD, IL 62278, PHONE: (618)-282-4114



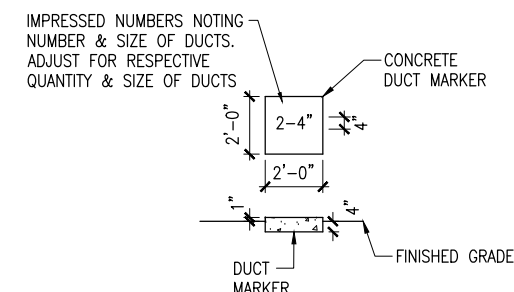
**DUCT MARKER DETAIL**

(NOT TO SCALE)



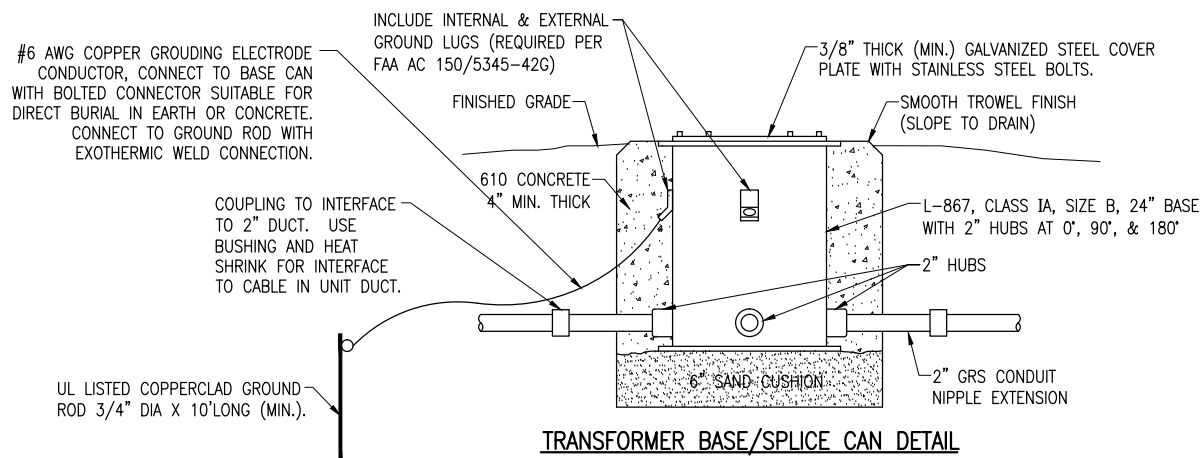
**TURF CABLE MARKERS**

(NOT TO SCALE)



**TURF DUCT MARKERS**

(NOT TO SCALE)



**TRANSFORMER BASE/SPLICE CAN DETAIL**

(NOT TO SCALE)

**NOTE:**

FOR THE PURPOSE OF ENHANCING SAFETY, EACH BASE MUST HAVE INSTALLED, BY THE MANUFACTURER, AN INTERNAL AND EXTERNAL GROUND STRAP THAT IS AVAILABLE FOR THE PURPOSE OF ATTACHING A GROUND LUG THAT IS CONNECTED TO AN EARTH GROUND OR A SAFETY GROUND CONDUCTOR INSTALLED WITH THE RESPECTIVE CIRCUIT. FOR AIRPORT PROJECTS RECEIVING FEDERAL FUNDS THIS REQUIREMENT IS MANDATORY PER FAA AC 150/5345-42G.





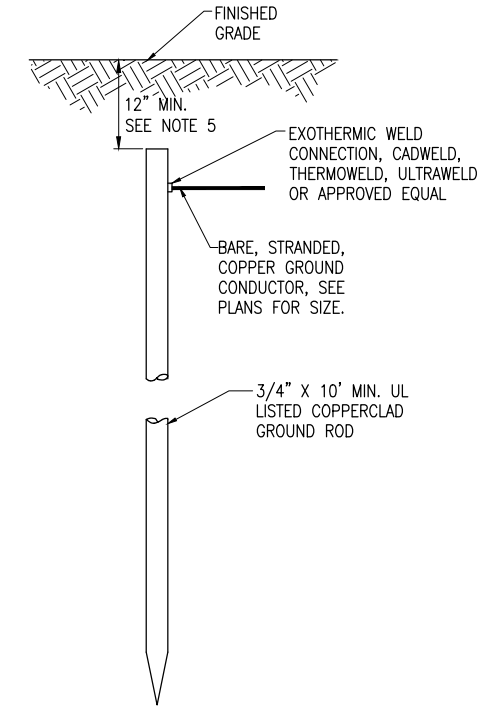




**GROUNDING NOTES**

- THE CONTRACTOR SHALL FURNISH AND INSTALL ALL GROUNDING AS MAY BE NECESSARY OR REQUIRED TO MAKE A COMPLETE GROUNDING SYSTEM AS REQUIRED BY THE LATEST NATIONAL ELECTRICAL CODE (NFPA 70) IN FORCE AND FAA-STD-019e (LIGHTNING AND SURGE PROTECTION, GROUNDING, BONDING, AND SHIELDING REQUIREMENTS FOR FACILITIES AND ELECTRONIC EQUIPMENT). THE RELIABILITY OF THE GROUNDING SYSTEM IS DEPENDENT ON CAREFUL, PROPER INSTALLATION AND CHOICE OF MATERIALS. IMPROPER PREPARATION OF SURFACES TO BE JOINED TO MAKE AN ELECTRICAL PATH, LOOSE JOINTS OR CORROSION CAN INTRODUCE IMPEDANCE THAT WILL SERIOUSLY IMPAIR THE ABILITY OF THE GROUND PATH TO PROTECT PERSONNEL AND EQUIPMENT AND TO ABSORB TRANSIENTS THAT CAN CAUSE NOISE IN COMMUNICATIONS CIRCUITS. THE FOLLOWING FUNCTIONS ARE PARTICULARLY IMPORTANT TO ENSURE A RELIABLE GROUND SYSTEM:
- FURNISH AND INSTALL GROUND RODS AS DETAILED HEREIN. GROUND RODS FOR AIRFIELD LIGHTING (RUNWAY LIGHTING, TAXIWAY LIGHTING, TAXI GUIDANCE SIGNS, AND SPLICE CANS SHALL BE MINIMUM 3/4-IN. DIAMETER BY 10-FT LONG, UL-LISTED COPPER CLAD WITH 10-MIL MINIMUM COPPER COATING. GROUND RODS FOR OTHER APPLICATIONS SHALL BE MINIMUM 3/4-IN. DIAMETER BY 10-FT LONG, UL-LISTED, COPPER CLAD WITH 10-MIL MINIMUM COPPER COATING. GROUND RODS SHALL BE SPACED OR AS DETAILED ON THE RESPECTIVE PLANS, AND IN NO CASE SPACED LESS THAN ONE ROD LENGTH APART. ALL CONNECTIONS TO GROUND RODS AND THE GROUND RING SHALL BE MADE WITH EXOTHERMIC WELD TYPE CONNECTORS, CADWELD BY ERICO PRODUCTS, INC., SOLON, OHIO, (PHONE 1-800-248-9353), THERMOWELD BY CONTINENTAL INDUSTRIES, INC., TULSA, OKLAHOMA (PHONE 918-663-1440) OR ULTRAWELD BY HARGER, GRAYSLAKE, ILLINOIS (PHONE 1-800-842-7437) OR APPROVED EQUAL. EXOTHERMIC WELD CONNECTIONS SHALL BE INSTALLED IN CONFORMANCE WITH THE RESPECTIVE MANUFACTURER'S DIRECTIONS USING MOLDS AS REQUIRED FOR EACH RESPECTIVE APPLICATION. BOLTED CONNECTIONS WILL NOT BE PERMITTED AT GROUND RODS OR AT BURIED GROUNDING ELECTRODE CONDUCTORS.
- CONTRACTOR SHALL TEST EACH MADE ELECTRODE GROUND ROD/GROUND FIELD/GROUND RING WITH AN INSTRUMENT SPECIFICALLY DESIGNED FOR TESTING GROUND FIELD SYSTEMS. IF GROUND RESISTANCE EXCEEDS 25 OHMS, CONTACT THE PROJECT ENGINEER FOR FURTHER DIRECTION. COPIES OF GROUND ROD TEST RESULTS SHALL BE FURNISHED TO THE RESIDENT ENGINEER/RESIDENT PROJECT REPRESENTATIVE.
- ALL PRODUCTS ASSOCIATED WITH THE GROUNDING SYSTEM SHALL BE UL-LISTED AND LABELED.
- ALL BOLTED OR MECHANICAL CONNECTIONS SHALL BE COATED WITH A CORROSION PREVENTATIVE COMPOUND BEFORE JOINING, SANICHEM INC. 'NO-OX-ID 'A-SPECIAL' COMPOUND, BURNDY PENETROX E, OR APPROVED EQUAL.
- METALLIC SURFACES TO BE JOINED SHALL BE PREPARED BY THE REMOVAL OF ALL NON-CONDUCTIVE MATERIAL, PER 2014 NATIONAL ELECTRICAL CODE ARTICLE 250-12. ALL COPPER BUS BARS MUST BE CLEANED PRIOR TO MAKING CONNECTIONS TO REMOVE SURFACE OXIDATION.
- METALLIC RACEWAY FITTINGS SHALL BE MADE UP TIGHT TO PROVIDE A PERMANENT LOW IMPEDANCE PATH FOR ALL CIRCUITS. METAL CONDUIT TERMINATIONS IN ENCLOSURES SHALL BE BONDED TO THE ENCLOSURE WITH UL-LISTED FITTINGS SUITABLE FOR GROUNDING. PROVIDE GROUNDING BUSHINGS WITH BONDING JUMPERS FOR ALL METAL CONDUITS ENTERING SERVICE EQUIPMENT (METER BASE, CT CABINET, MAIN SERVICE BREAKER ENCLOSURE, ETC.). PROVIDE GROUNDING BUSHINGS WITH BONDING JUMPERS FOR ALL METAL CONDUITS ENTERING AN ENCLOSURE THROUGH CONCENTRIC OR ECCENTRIC KNOCKOUTS THAT ARE PUNCHED OR OTHERWISE FORMED SO AS TO IMPAIR THE ELECTRICAL CONNECTION TO GROUND. STANDARD LOCKNUTS OR BUSHINGS SHALL NOT BE THE SOLE MEANS FOR BONDING WHERE A CONDUIT ENTERS AN ENCLOSURE THROUGH A CONCENTRIC OR ECCENTRIC KNOCKOUT
- ALL CONNECTIONS, LOCATED ABOVE GRADE, BETWEEN THE DIFFERENT TYPES OF GROUNDING CONDUCTORS SHALL BE MADE USING UL-LISTED DOUBLE COMPRESSION CRIMP TYPE CONNECTORS OR UL-LISTED BOLTED GROUND CONNECTORS. FOR GROUND CONNECTIONS TO ENCLOSURES, CASES AND FRAMES OF ELECTRICAL EQUIPMENT NOT SUPPLIED WITH GROUND LUGS THE CONTRACTOR SHALL DRILL REQUIRED HOLES FOR MOUNTING A BOLTED GROUND CONNECTOR. ALL BOLTED GROUND CONNECTORS SHALL BE BURNDY, THOMAS AND BETTS, OR EQUAL. TIGHTEN CONNECTIONS TO COMPLY WITH TIGHTENING TORQUES IN UL STANDARD 486A TO ASSURE PERMANENT AND EFFECTIVE GROUNDING.
- ALL METAL EQUIPMENT ENCLOSURES, CONDUITS, CABINETS, BOXES, RECEPTACLES, MOTORS, ETC. SHALL BE BONDED TO THE RESPECTIVE GROUNDING SYSTEM.
- PROVIDE ALL BOXES FOR PROPOSED OUTLETS, SWITCHES, CIRCUIT BREAKERS, ETC. WITH GROUNDING SCREWS. PROVIDE ALL PANELBOARD, SWITCHGEAR, ETC., ENCLOSURES WITH GROUNDING BARS WITH INDIVIDUAL SCREWS, LUGS, CLAMPS, ETC., FOR EACH OF THE GROUNDING CONDUCTORS THAT ENTER THEIR RESPECTIVE ENCLOSURES.
- EACH NEW FEEDER CIRCUIT AND/OR BRANCH CIRCUIT SHALL INCLUDE AN EQUIPMENT GROUND WIRE. METAL RACEWAY OR CONDUIT SHALL NOT MEET THIS REQUIREMENT. THE EQUIPMENT GROUND WIRE FROM EQUIPMENT SHALL NOT BE SMALLER THAN ALLOWED BY 2014 NEC TABLE 250-122 "MINIMUM SIZE CONDUCTORS OR GROUNDING RACEWAY AND EQUIPMENT." WHEN CONDUCTORS ARE ADJUSTED IN SIZE TO COMPENSATE FOR VOLTAGE DROP, EQUIPMENT-GROUNDING CONDUCTORS SHALL BE ADJUSTED PROPORTIONATELY ACCORDING TO CIRCULAR MIL AREA. ALL EQUIPMENT GROUND WIRES SHALL BE COPPER, EITHER BARE OR INSULATED GREEN IN COLOR. WHERE THE EQUIPMENT GROUNDING CONDUCTORS ARE INSULATED, THEY SHALL BE IDENTIFIED BY THE COLOR GREEN, AND SHALL BE THE SAME INSULATION TYPE AS THE PHASE CONDUCTORS.

- ALL EXTERIOR METAL CONDUIT, WHERE NOT ELECTRICALLY CONTINUOUS BECAUSE OF MANHOLES, HANDHOLES, NON-METALLIC JUNCTION BOXES, ETC., SHALL BE BONDED TO ALL OTHER METAL CONDUIT IN THE RESPECTIVE DUCT RUN, AND AT EACH END, WITH A COPPER-BONDING JUMPER SIZED IN CONFORMANCE WITH 2014 NEC 250-102. WHERE METAL CONDUITS TERMINATE IN AN ENCLOSURE (SUCH AS A MOTOR CONTROL CENTER, SWITCHBOARD, ETC) WHERE THERE IS NOT ELECTRICAL CONTINUITY WITH THE CONDUIT AND THE RESPECTIVE ENCLOSURE, PROVIDE A BONDING JUMPER FROM THE RESPECTIVE ENCLOSURE GROUND BUS TO THE CONDUIT SIZED PER 2014 NEC 250-102.
- IT IS THE INTENT OF THIS SPECIFICATION THAT ALL MOTOR FRAMES, PUMP BASES ELECTRICAL EQUIPMENT ENCLOSURES, PANEL HOUSINGS, CONDUITS, BOXES, ETC. HAVE A CONTINUOUS COPPER WIRE GROUND CONNECTION AND SHALL BE POSITIVELY BONDED TO THE RESPECTIVE GROUNDING SYSTEM. CONDUIT CONNECTORS WILL NOT BE CONSIDERED AS ADEQUATE GROUNDING.
- PROVIDE A POSITIVE GROUND BOND FOR ALL OUTLET BOXES, ELECTRICAL EQUIPMENT ENCLOSURES, GROUNDING RECEPTACLES, TOGGLE SWITCHES, ETC. INSTALL A GROUNDING CONDUCTOR IN ALL WIRE AND CABLE RACEWAYS. GROUND CONDUCTOR TO HAVE 600-VOLT INSULATION AND BE IDENTIFIED BY A CONTINUOUS GREEN COLOR COATING. THEY SHALL BE USED SOLELY FOR GROUNDING PURPOSES AND BE ENTIRELY SEPARATE FROM WHITE GROUNDED NEUTRAL CONDUCTOR, EXCEPT AT SUPPLY SIDE OF SERVICE DISCONNECTING MEANS, WHERE GROUNDING AND NEUTRAL SYSTEMS ARE TO BE CONNECTED TO SERVICE GROUND.
- EACH AND ALL GROUNDED CASED AND METAL PARTS ASSOCIATED WITH ELECTRICAL EQUIPMENT SHALL BE TESTED FOR CONTINUITY OF CONNECTION WITH GROUND BUS SYSTEM BY CONTRACTOR IN PRESENCE OF OWNER'S REPRESENTATIVE.
- ALL CONNECTIONS BETWEEN THE DIFFERENT TYPES OF GROUNDING CONDUCTORS ABOVE GRADE SHALL BE MADE USING BOLTED GROUND CONNECTORS. GROUND LUGS SHALL BE PROVIDED IN ALL ENCLOSURES AND WIRING TERMINATION JUNCTION BOXES. EQUIPMENT GROUNDS AND GROUNDING CONDUCTOR SHALL BE CONNECTED TO THESE GROUND LUGS. FOR GROUND CONNECTIONS TO ENCLOSURES, CASES AND FRAMES OF ELECTRICAL EQUIPMENT NOT SUPPLIED WITH GROUND LUGS THE CONTRACTOR SHALL DRILL REQUIRED HOLES FOR MOUNTING A BOLTED GROUND CONNECTOR. ALL BOLTED GROUND CONNECTORS SHALL BE BURNDY, OR APPROVED EQUAL.
- BOND ALL NONCURRENT-CARRYING PARTS OF METAL EQUIPMENT TO GROUND SYSTEM.
- BUILDING STRUCTURAL STEEL SYSTEM SHALL BE BONDED TO ELECTRICAL GROUND SYSTEM.
- INSTALL GROUNDING ELECTRODE CONDUCTORS, LIGHTNING PROTECTION DOWN CONDUCTORS AND SEPARATE GROUND CONDUCTORS IN SCHEDULE 40 OR SCHEDULE 80 PVC CONDUIT OR EXPOSED WHERE ACCEPTABLE TO LOCAL CODES. WHERE GROUNDING ELECTRODE CONDUCTORS, LIGHTNING PROTECTION DOWN CONDUCTORS OR INDIVIDUAL GROUND CONDUCTORS ARE RUN IN PVC CONDUIT, DO NOT COMPLETELY ENCIRCLE CONDUIT WITH FERROUS AND/OR MAGNETIC MATERIALS. USE NON-METALLIC REINFORCED FIBERGLASS STRUT SUPPORT. WHERE METAL CONDUIT CLAMPS ARE INSTALLED, USE NYLON BOLTS, NUTS, WASHERS AND SPACERS TO INTERRUPT A COMPLETE METALLIC PATH FROM ENCIRCLING THE CONDUIT. THIS IS REQUIRED TO AVOID GIRDLING OF GROUND CONDUCTORS. GIRDLING OF A GROUND CONDUCTOR IS THE RESULT OF PLACING THE CONDUCTOR IN A RING OF MAGNETIC MATERIAL. THIS RING COULD BE A METALLIC CONDUIT, U-BOLT OR STRUT SUPPORT PIPE CLAMP, OR OTHER SUPPORT HARDWARE. THE RESULT OF GIRDLING GROUND CONDUCTORS SIGNIFICANTLY INCREASES THE INDUCTIVE IMPEDANCE OF THE GROUND CONDUCTOR. INDUCTIVE AND CAPACITIVE IMPEDANCE IS A TYPE OF RESISTANCE THAT OPPOSES THE FLOW OF ALTERNATING CURRENT. ANY INCREASE IN THE IMPEDANCE OF A GROUND CONDUCTOR REDUCES ITS ABILITY TO EFFECTIVELY MITIGATE RADIO FREQUENCY NOISE IN THE GROUND SYSTEM. THE CONDITION WHERE A GROUND CONDUCTOR IS GIRDLED DURING A LIGHTNING STRIKE RESULTS IN PHENOMENA KNOWN AS SURGE IMPEDANCE LOADING. SURGE IMPEDANCE LOADING IS A RESULT OF VOLTAGE AND CURRENT REACHING 500,000 VOLTS AND 10,000 AMPS FOR A SHORT DURATION. GIRDLING FURTHER INCREASES THE IMPEDANCE AT LIGHTNING FREQUENCIES OF 100 KILOHERTZ TO 100 MEGAHERTZ. AT THESE POWER AND FREQUENCY LEVELS ANY INCREASE IN THE IMPEDANCE OF THE GROUND CONDUCTOR MUST BE CONTROLLED. DURING LIGHTNING DISCHARGE CONDITIONS A LOW INDUCTIVE IMPEDANCE PATH IS MORE IMPORTANT THAN A LOW DC RESISTANCE PATH.
- IF LOCAL CODES DICTATE THAT INDIVIDUAL GROUNDING CONDUCTORS MUST BE RUN IN METAL CONDUIT OR RACEWAY, THEN THE CONDUIT OR RACEWAY MUST BE BONDED AT EACH END OF THE RUN WITH A BONDING JUMPER SIZED EQUAL TO THE INDIVIDUAL GROUNDING CONDUCTOR OR AS REQUIRED BY 2014 NEC 250-102. NOTE THIS DOES NOT APPLY TO AC EQUIPMENT GROUNDING CONDUCTORS RUN WITH AC CIRCUITS.
- WHERE A CONFLICT IS DETERMINED WITH RESPECT TO GROUNDING REQUIREMENTS PER MANUFACTURER INSTALLATION INSTRUCTIONS, NEC, AND/OR THE CONTRACT DOCUMENTS, CONTACT THE RESIDENT ENGINEER OR PROJECT ENGINEER FOR FURTHER DIRECTIONS.
- GROUND RODS SHALL BE MANUFACTURED IN THE UNITED STATES OF AMERICA TO COMPLY WITH THE AIRPORT IMPROVEMENT PROGRAM BUY AMERICAN REQUIREMENTS. STEEL USED TO MANUFACTURE GROUND RODS SHALL BE 100 PERCENT DOMESTIC STEEL.



**10 FT. GROUND ROD**

**NOTES**

- TYPE AND MINIMUM NUMBER OF GROUND RODS SHALL BE AS SPECIFIED ON THE PLAN.
- THE RESISTANCE TO GROUND OF THE GROUNDING SYSTEM SHALL NOT EXCEED 25 OHMS.
- COST OF GROUND RODS IS INCIDENTAL TO THE ASSOCIATED ITEMS REQUIRING GROUNDING UNLESS OTHERWISE SPECIFIED.
- GROUND RODS SHALL BE SPACED AS DETAILED ON THE PLANS AND SHALL NOT BE SPACED LESS THAN ONE ROD LENGTH APART.
- TOP OF GROUND RODS SHALL BE 12" MINIMUM BELOW GRADE UNLESS DETAILED OTHERWISE HEREIN.
- GROUND RODS FOR SPLICE CANS AND AIRFIELD LIGHTING SHALL BE A MINIMUM 3/4-INCH DIAMETER BY 10-FT LONG UL LISTED COPPER CLAD.

**GROUND RODS**  
(NOT TO SCALE)



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Quincy Regional Airport -  
Baldwin Field



1645 Highway 104  
Quincy, IL 62305



*Kevin N. Lightfoot*  
6/9/2015

REALIGN TAXIWAY D

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Q1061

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REVIEWED BY: KNL 11/05/2014

SHEET TITLE

GROUNDING NOTES







*Kevin N. Lightfoot*  
6/9/2015

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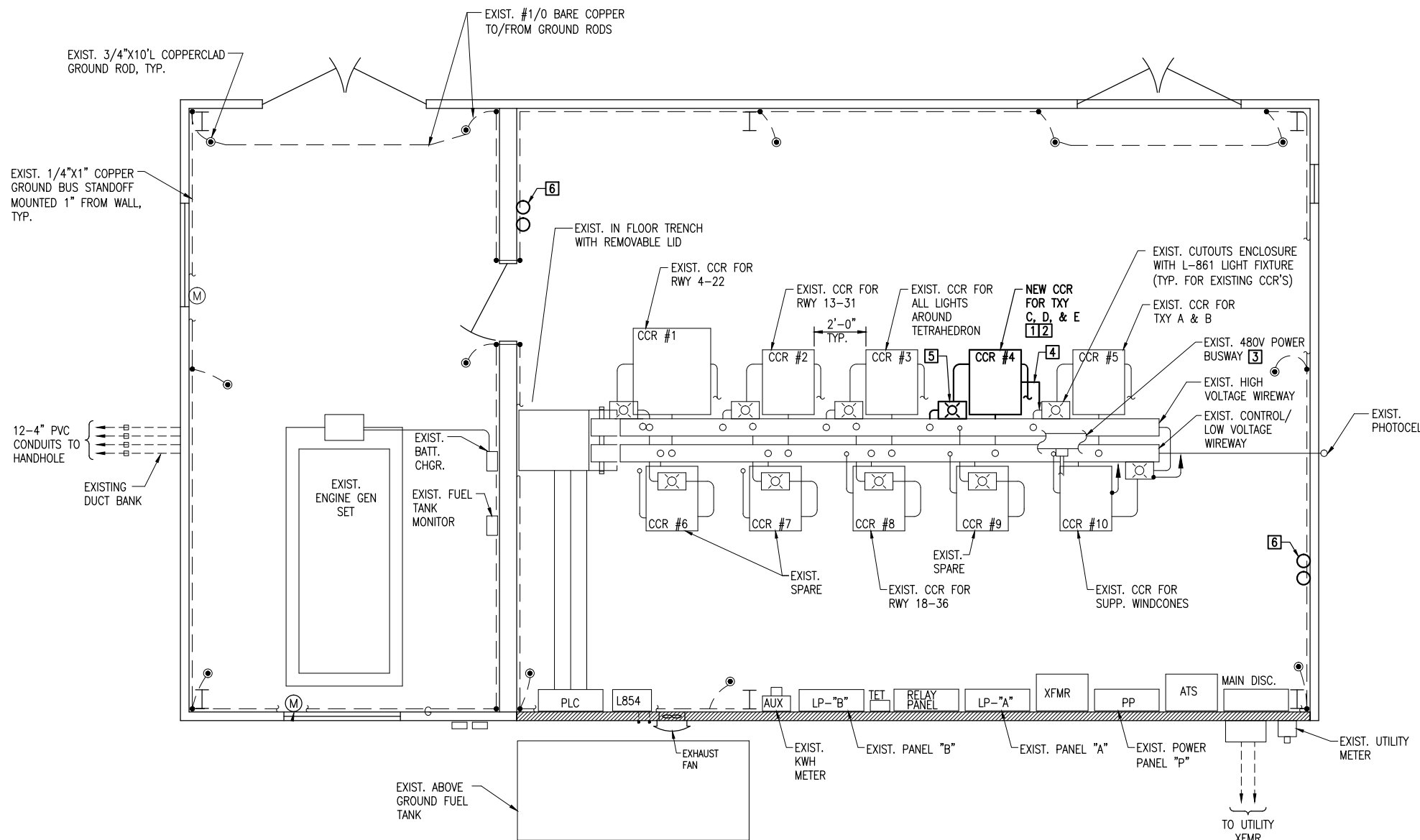
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CAD FILE: E-101-VLT.DWG  
DESIGN BY: KNL 10/29/2014  
DRAWN BY: MLH 10/30/2014  
REVIEWED BY: KNL 10/30/2014

SHEET TITLE

**ELECTRICAL VAULT FLOOR PLAN**

**NOTES:**

- CONTRACTOR SHALL COORDINATE WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS WITH THE AIRPORT MANAGER AND THE RESIDENT ENGINEER/RESIDENT PROJECT REPRESENTATIVE. ANY SHUTDOWN OF EXISTING SYSTEMS SHALL BE SCHEDULED WITH AND APPROVED BY THE AIRPORT MANAGER PRIOR TO SHUTDOWN. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY AND HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- CONTRACTOR SHALL EXAMINE THE SITE TO CONFIRM AND FIELD VERIFY EXISTING SITE CONDITIONS.
- THE RESPECTIVE PERSONNEL PERFORMING AIRFIELD LIGHTING WORK, VAULT WORK, AND/OR TESTS SHALL BE FAMILIAR WITH AND QUALIFIED TO WORK ON 5000 VOLT AIRFIELD LIGHTING SERIES CIRCUITS, CONSTANT CURRENT REGULATORS, AND ASSOCIATED AIRPORT ELECTRICAL VAULT EQUIPMENT.
- CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF FAA AC NO. 150/5370-2F (OR MOST CURRENT ISSUE) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- CONTRACTOR SHALL COMPLY WITH THE APPLICABLE REQUIREMENTS OF NFPA 70E - STANDARD FOR ELECTRICAL SAFETY IN THE WORKPLACE.
- CONTRACTOR SHALL CONDUCT MEGGER TESTS AND RECORD RESULTS FOR EACH EXISTING SERIES CIRCUIT PRIOR TO AIRFIELD LIGHTING MODIFICATIONS AND CABLE WORK. CONTRACTOR SHALL CONDUCT MEGGER TESTS AND RECORD RESULTS FOR EACH EXISTING SERIES CIRCUIT AFTER AIRFIELD LIGHTING MODIFICATIONS, ADDITIONS, AND /OR UPGRADES HAVE BEEN COMPLETED.
- THE RESPECTIVE SERIES CIRCUIT CABLE LOOPS SHALL HAVE THE RESISTANCE TESTED AND RECORDED FOR EACH CIRCUIT AT THE VAULT PRIOR TO AIRFIELD LIGHTING MODIFICATIONS AND CABLE WORK. THE RESPECTIVE SERIES CIRCUIT CABLE LOOPS SHALL HAVE THE RESISTANCE TESTED AND RECORDED FOR EACH CIRCUIT AT THE VAULT AFTER AIRFIELD LIGHTING MODIFICATIONS, ADDITIONS, AND /OR UPGRADES HAVE BEEN COMPLETED.
- EACH CCR SHALL BE TESTED FOR PROPER OPERATION BEFORE ANY AIRFIELD WORK THAT MIGHT AFFECT THE LIGHTING CIRCUITS, REMOVAL WORK, MODIFICATIONS AND/OR ADDITIONS AND AFTER THE AIRFIELD WORK AND ADDITIONS HAVE BEEN COMPLETED. CONTRACTOR SHALL TEST AND RECORD THE INPUT CURRENT AND OUTPUT CURRENT FOR EACH CONSTANT CURRENT REGULATORS IN THE AUTOMATIC AND MANUAL MODES OF OPERATION. CONTRACTOR SHALL REPORT CONCERNS AND/OR DEFICIENCIES TO THE RESIDENT PROJECT REPRESENTATIVE/RESIDENT ENGINEER.
- CONTRACTOR SHALL FURNISH AND INSTALL LEGEND PLATES, WARNING LABELS AND SIGNS FOR THE VAULT AND VAULT EQUIPMENT AS DETAILED HEREIN. SEE "LEGEND PLATE SCHEDULES" SHEET.
- ALL ELECTRICAL EQUIPMENT SHALL BE INSTALLED IN CONFORMANCE WITH NFPA 70 - NATIONAL ELECTRICAL CODE (NEC) MOST CURRENT ISSUE IN FORCE, THE RESPECTIVE EQUIPMENT MANUFACTURE'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCE, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, ETL LISTING, (OR OTHER THRID PARTY LISTING) AND/OR THE MANUFACTURE'S WARRANTY OF A DEVICE WILL NOT BE PERMITTED.
- MAINTAIN SEPARATION OF HIGH VOLTAGE WIRING FROM LOW VOLTAGE WIRING TO COMPLY WITH NEC 300.3(C)(2). HIGH VOLTAGE AND LOW VOLTAGE WIRING SHALL NOT BE INSTALLED IN THE SAME RACEWAY, CONDUIT, WIREWAY, PULL BOX, SPLICE CAN, HANDHOLE, OR MANHOLE.
- MAINTAIN SEPARATION OF HIGH VOLTAGE AND LOW VOLTAGE CIRCUITS. LOW VOLTAGE WIRING SHALL ENTER THE RESPECTIVE CCR AT THE LOW VOLTAGE SECTION. HIGH VOLTAGE WIRING SHALL ENTER THE RESPECTIVE CCR AT THE HIGH VOLTAGE SECTION.



**ELECTRICAL VAULT FLOOR PLAN**  
SCALE 3/8"=1'-0"  
0 2 4 FEET

**KEYED NOTES:**

- EXISTING 10 KW CCR FOR TAXIWAYS C, D, AND E SHALL BE REMOVED AND REPLACED WITH A NEW 15 KW CCR. EXISTING CCR SHALL BE TURNED OVER TO THE AIRPORT.
- SEE "PROPOSED VAULT ADDITIONS ONE LINE DIAGRAM" FOR LOW VOLTAGE INPUT POWER WIRING REQUIREMENTS TO CCR (CONSTANT CURRENT REGULATOR). SEE "PROPOSED HIGH VOLTAGE WIRING SCHEMATIC FOR TAXIWAYS C, D & E" FOR CCR OUTPUT WIRING REQUIREMENTS. RECONNECT EXISTING CONTROL WIRING TO NEW CCR.
- REPLACE EXISTING 480 VAC CIRCUIT BREAKER FOR TAXIWAY C, D, AND E CCR WITH A NEW CIRCUIT BREAKER THAT IS COMPATIBLE WITH THE EXISTING 480 VAC BUSWAY SYSTEM AND PROPERLY SIZED FOR THE RESPECTIVE CCR. CIRCUIT BREAKER SIZED IS ESTIMATED TO BE 50 AMP, 2-POLE OR 60 AMP, 2-POLE CORRESPONDING TO THE RESPECTIVE CCR MANUFACTURER RECOMMENDATIONS. CIRCUIT BREAKER SHALL HAVE 25,000 AIC (MINIMUM) RATING AT 480 VAC.
- #6 AWG COPPER BONDING CONDUCTOR FROM CCR FRAME TO VAULT GROUND BUS. CONNECT TO CCR WITH MANUFACTURER'S GROUND LUG OR TWO-HOLE TONGUE LONG BARREL COMPRESSION LUG AND 3/8" STAINLESS STEEL BOLTS, NUTS AND WASHERS. CONNECT TO VAULT GROUND BUS WITH TWO-HOLE TONGUE LONG BARREL COMPRESSION LUG AND 3/8" STAINLESS STEEL BOLTS, NUTS AND WASHERS.
- EXISTING CUTOUT ENCLOSURE WITH NEW CUTOUTS. SEE "PROPOSED HIGH VOLTAGE WIRING SCHEMATIC FOR TAXIWAYS C, D & E" FOR CCR OUTPUT WIRING REQUIREMENTS.
- FURNISH AND INSTALL TWO UL RATED, 10 POUND CARBON DIOXIDE FIRE EXTINGUISHERS SUITABLE FOR USE ON CLASS C FIRES AND TWO 10 POUND CLASS 4A:80B:C DRY CHEMICAL ABC FIRE EXTINGUISHERS SUITABLE FOR USE ON CLASS A,B,C FIRES, IN THE VAULT SHELTER. PER NFPA 10 "PORTABLE FIRE EXTINGUISHERS" CLASS C ARE FOR FIRES THAT INVOLVE ENERGIZED ELECTRICAL EQUIPMENT. FIRE EXTINGUISHERS SHALL BE MADE IN THE UNITED STATES OF AMERICA TO COMPLY WITH BUY AMERICAN PREFERENCE REQUIREMENT. FIRE EXTINGUISHER TYPE CO2 SHALL BE AMEREX MODEL 330, ANSUL SENTRY 10 MODEL CD10A-1 OR APPROVED EQUAL. FIRE EXTINGUISHER DRY CHEMICAL TYPE ABC SHALL BE AMEREX MODEL B456, OR APPROVED EQUAL. PROVIDE WALL MOUNTING BRACKET FOR EACH FIRE EXTINGUISHER. CONFIRM MODEL NUMBERS WITH THE RESPECTIVE FIRE EXTINGUISHER MANUFACTURER.

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*Kevin N. Lightfoot*  
6/9/2015

**REALIGN TAXIWAY D**

IDA No: UIN-4332

SBG No: 3-17-SBGP-105

Q1061

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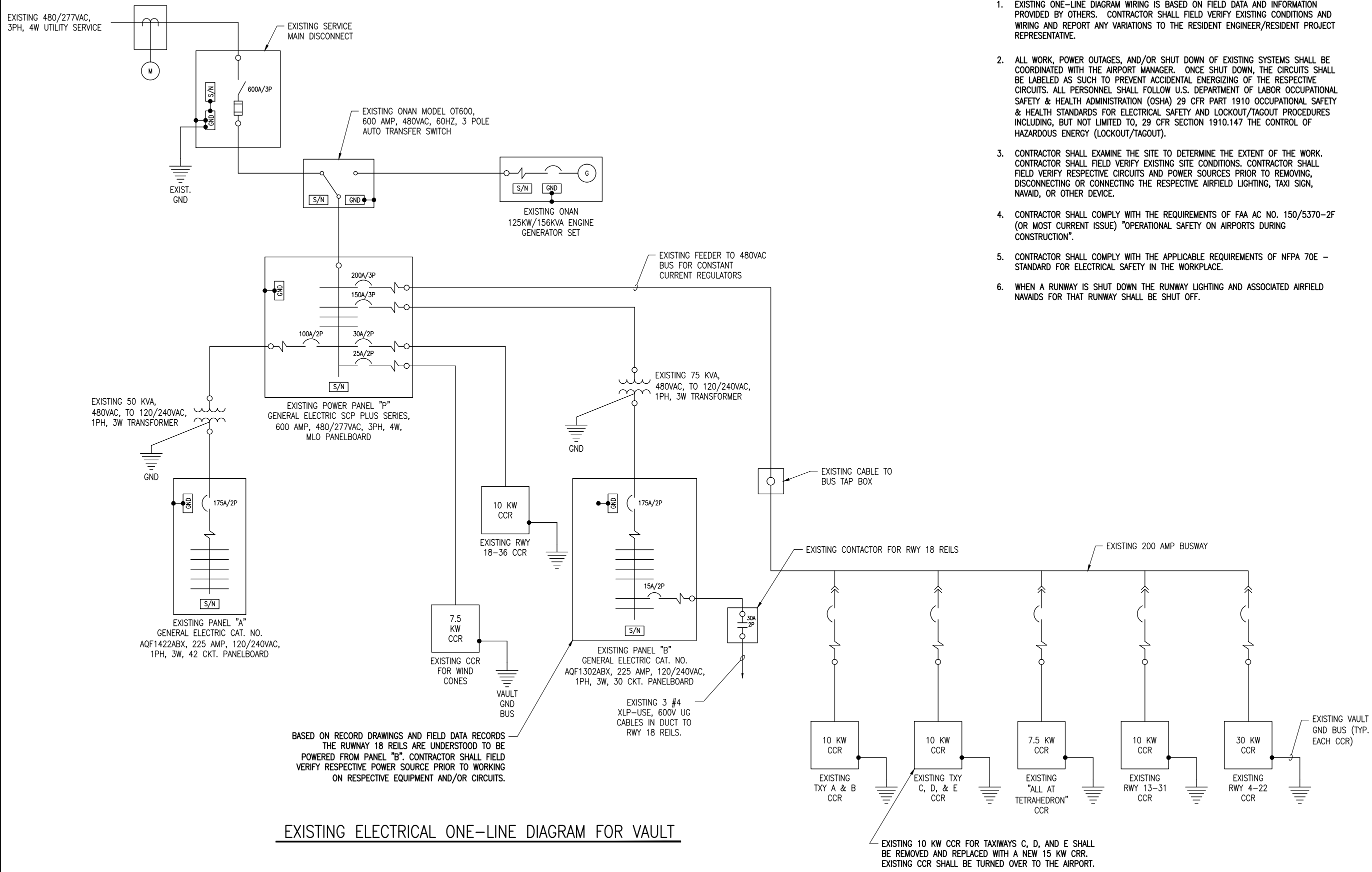
DESIGN BY: KNL 11/03/2014

DRAWN BY: MLH 11/03/2014

REVIEWED BY: KNL 11/05/2014

SHEET TITLE

**EXISTING ONE-LINE  
DIAGRAM FOR VAULT**



**EXISTING ELECTRICAL ONE-LINE DIAGRAM FOR VAULT**

**NOTES:**

- EXISTING ONE-LINE DIAGRAM WIRING IS BASED ON FIELD DATA AND INFORMATION PROVIDED BY OTHERS. CONTRACTOR SHALL FIELD VERIFY EXISTING CONDITIONS AND WIRING AND REPORT ANY VARIATIONS TO THE RESIDENT ENGINEER/RESIDENT PROJECT REPRESENTATIVE.
- ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- CONTRACTOR SHALL EXAMINE THE SITE TO DETERMINE THE EXTENT OF THE WORK. CONTRACTOR SHALL FIELD VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING, DISCONNECTING OR CONNECTING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, OR OTHER DEVICE.
- CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF FAA AC NO. 150/5370-2F (OR MOST CURRENT ISSUE) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- CONTRACTOR SHALL COMPLY WITH THE APPLICABLE REQUIREMENTS OF NFPA 70E - STANDARD FOR ELECTRICAL SAFETY IN THE WORKPLACE.
- WHEN A RUNWAY IS SHUT DOWN THE RUNWAY LIGHTING AND ASSOCIATED AIRFIELD NAVAIDS FOR THAT RUNWAY SHALL BE SHUT OFF.

BASED ON RECORD DRAWINGS AND FIELD DATA RECORDS THE RUNWAY 18 REILS ARE UNDERSTOOD TO BE POWERED FROM PANEL "B". CONTRACTOR SHALL FIELD VERIFY RESPECTIVE POWER SOURCE PRIOR TO WORKING ON RESPECTIVE EQUIPMENT AND/OR CIRCUITS.

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*Kevin N. Lightfoot*  
6/10/2015

**REALIGN TAXIWAY D**

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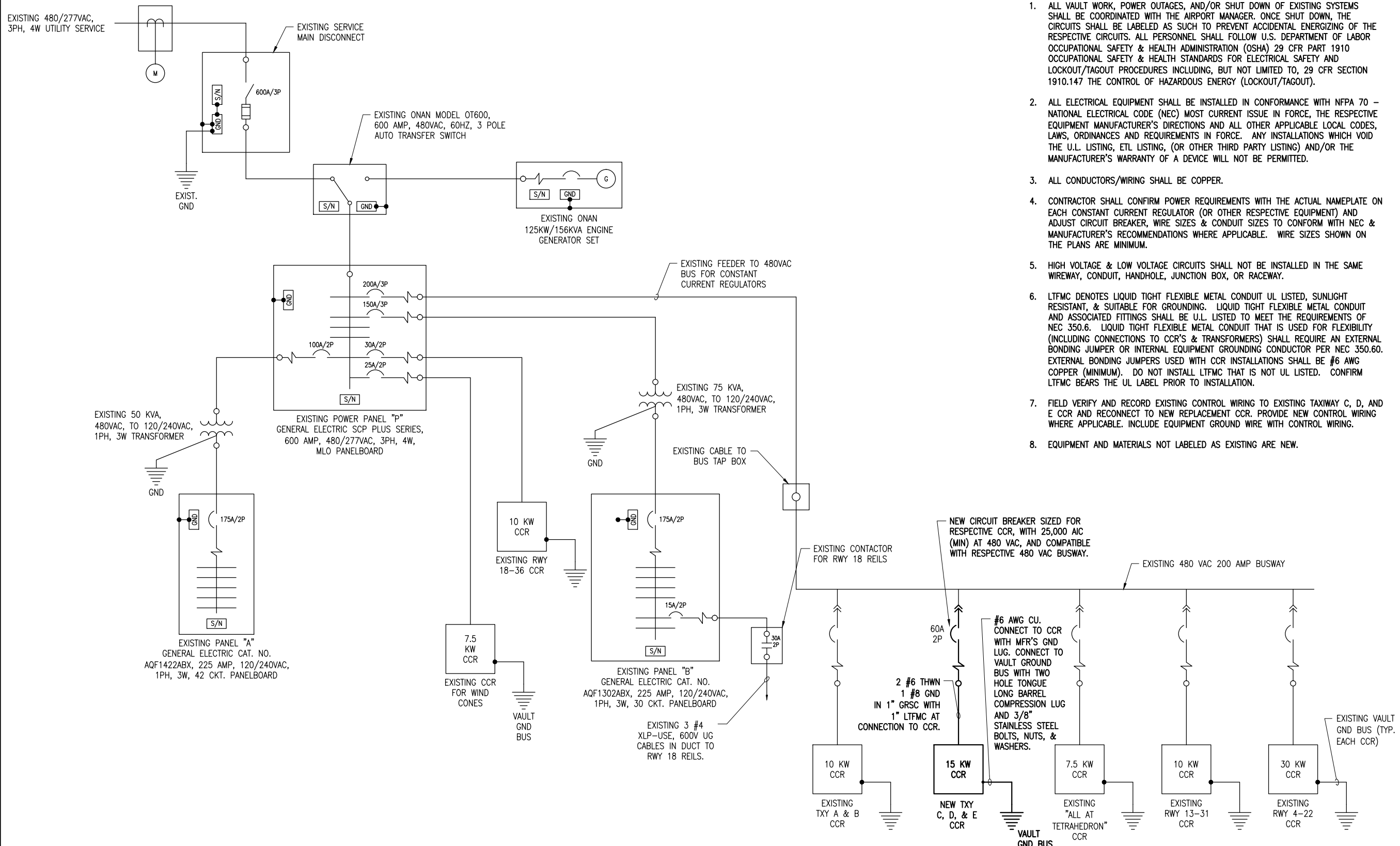
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SHEET TITLE

**PROPOSED VAULT ADDITIONS ONE-LINE DIAGRAM**



**PROPOSED VAULT ADDITIONS ONE-LINE DIAGRAM**

**NOTES:**

1. ALL VAULT WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
2. ALL ELECTRICAL EQUIPMENT SHALL BE INSTALLED IN CONFORMANCE WITH NFPA 70 - NATIONAL ELECTRICAL CODE (NEC) MOST CURRENT ISSUE IN FORCE, THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, ETL LISTING, (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE WILL NOT BE PERMITTED.
3. ALL CONDUCTORS/WIRING SHALL BE COPPER.
4. CONTRACTOR SHALL CONFIRM POWER REQUIREMENTS WITH THE ACTUAL NAMEPLATE ON EACH CONSTANT CURRENT REGULATOR (OR OTHER RESPECTIVE EQUIPMENT) AND ADJUST CIRCUIT BREAKER, WIRE SIZES & CONDUIT SIZES TO CONFORM WITH NEC & MANUFACTURER'S RECOMMENDATIONS WHERE APPLICABLE. WIRE SIZES SHOWN ON THE PLANS ARE MINIMUM.
5. HIGH VOLTAGE & LOW VOLTAGE CIRCUITS SHALL NOT BE INSTALLED IN THE SAME WIREWAY, CONDUIT, HANDHOLE, JUNCTION BOX, OR RACEWAY.
6. LTFMC DENOTES LIQUID TIGHT FLEXIBLE METAL CONDUIT UL LISTED, SUNLIGHT RESISTANT, & SUITABLE FOR GROUNDING. LIQUID TIGHT FLEXIBLE METAL CONDUIT AND ASSOCIATED FITTINGS SHALL BE U.L. LISTED TO MEET THE REQUIREMENTS OF NEC 350.6. LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS USED FOR FLEXIBILITY (INCLUDING CONNECTIONS TO CCR'S & TRANSFORMERS) SHALL REQUIRE AN EXTERNAL BONDING JUMPER OR INTERNAL EQUIPMENT GROUNDING CONDUCTOR PER NEC 350.60. EXTERNAL BONDING JUMPERS USED WITH CCR INSTALLATIONS SHALL BE #6 AWG COPPER (MINIMUM). DO NOT INSTALL LTFMC THAT IS NOT UL LISTED. CONFIRM LTFMC BEARS THE UL LABEL PRIOR TO INSTALLATION.
7. FIELD VERIFY AND RECORD EXISTING CONTROL WIRING TO EXISTING TAXIWAY C, D, AND E CCR AND RECONNECT TO NEW REPLACEMENT CCR. PROVIDE NEW CONTROL WIRING WHERE APPLICABLE. INCLUDE EQUIPMENT GROUND WIRE WITH CONTROL WIRING.
8. EQUIPMENT AND MATERIALS NOT LABELED AS EXISTING ARE NEW.



*Kevin N. Lightfoot*  
11/30/2015

**REALIGN TAXIWAY D**

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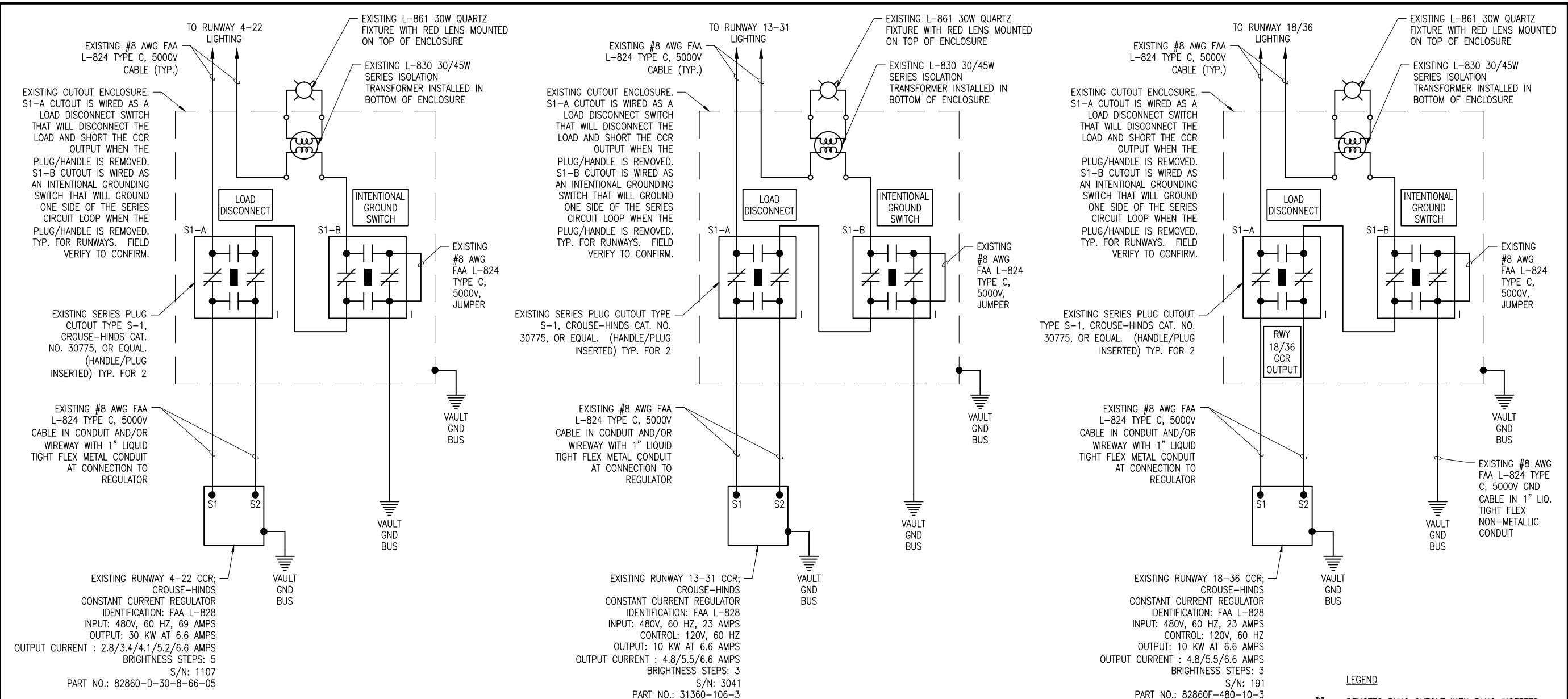
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SHEET TITLE

**EXISTING HIGH VOLTAGE WIRING SCHEMATIC FOR RUNWAYS**



**LEGEND**  
"I" DENOTES PLUG CUTOUT WITH PLUG INSERTED  
"P" DENOTES PLUG CUTOUT WITH PLUG PULLED  
"CCR" DENOTES CONSTANT CURRENT REGULATOR

**EXISTING HIGH VOLTAGE WIRING SCHEMATIC FOR RUNWAYS**

**NOTES:**

- ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER/DIRECTOR AND RESIDENT ENGINEER/RESIDENT PROJECT REPRESENTATIVE. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- CONTRACTOR SHALL EXAMINE THE SITE TO CONFIRM AND FIELD VERIFY EXISTING SITE CONDITIONS.
- THE RESPECTIVE PERSONNEL PERFORMING AIRFIELD LIGHTING WORK, VAULT WORK, AND/OR TESTS SHALL BE FAMILIAR WITH, AND QUALIFIED TO WORK ON, 5000 VOLT AIRFIELD LIGHTING SERIES CIRCUITS, CONSTANT CURRENT REGULATORS, AND ASSOCIATED AIRPORT ELECTRICAL VAULT EQUIPMENT.
- NOTE THE CONDITION OF THE EXISTING CUTOUTS IS UNKNOWN. IT IS POSSIBLE THAT SOME CUTOUTS MIGHT NOT FUNCTION PROPERLY. CONTRACTOR SHALL EXERCISE CAUTION, PRACTICE SAFETY, AND DISCONNECT THE SERIES CIRCUITS FROM THE RESPECTIVE CONSTANT CURRENT REGULATORS, AS APPLICABLE WHEN PERFORMING WORK ON THE AIRFIELD LIGHTING OR WORK THAT MIGHT AFFECT THE AIRFIELD LIGHTING. CONTRACTOR SHALL MAKE NECESSARY ARRANGEMENTS TO DISCONNECT POWER AND LOCKOUT CIRCUITS FOR PROTECTION OF PERSONNEL.
- MEGGER TEST AND RECORD EXISTING SERIES CIRCUITS PRIOR TO CABLE WORK AND AGAIN AFTER AIRFIELD LIGHTING MODIFICATIONS, ADDITIONS, AND/OR UPGRADES HAVE BEEN COMPLETED. ALSO TEST AND RECORD SERIES CIRCUIT LOOP RESISTANCE.
- RUNWAY 4/22 CCR, RUNWAY 13/31 CCR, RUNWAY 18/36 CCR, AND THE ASSOCIATED CUTOUTS ARE EXISTING.
- THE RESPECTIVE CCR'S SHALL BE TESTED FOR PROPER OPERATION BEFORE REMOVAL WORK, MODIFICATIONS, AND/OR ADDITIONS AND AFTER THE NEW CABLES AND LIGHTING SYSTEM MODIFICATIONS AND ADDITIONS HAVE BEEN COMPLETED. CONTRACTOR SHALL TEST AND RECORD THE INPUT CURRENT AND OUTPUT CURRENT FOR EACH CONSTANT CURRENT REGULATOR IN THE AUTOMATIC AND MANUAL MODES OF OPERATIONS. CONTRACTOR SHALL REPORT CONCERNS AND/OR DEFICIENCIES TO THE RESIDENT PROJECT REPRESENTATIVE/RESIDENT ENGINEER.

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*Kevin N. Lightfoot*  
11/30/2015

**REALIGN TAXIWAY D**

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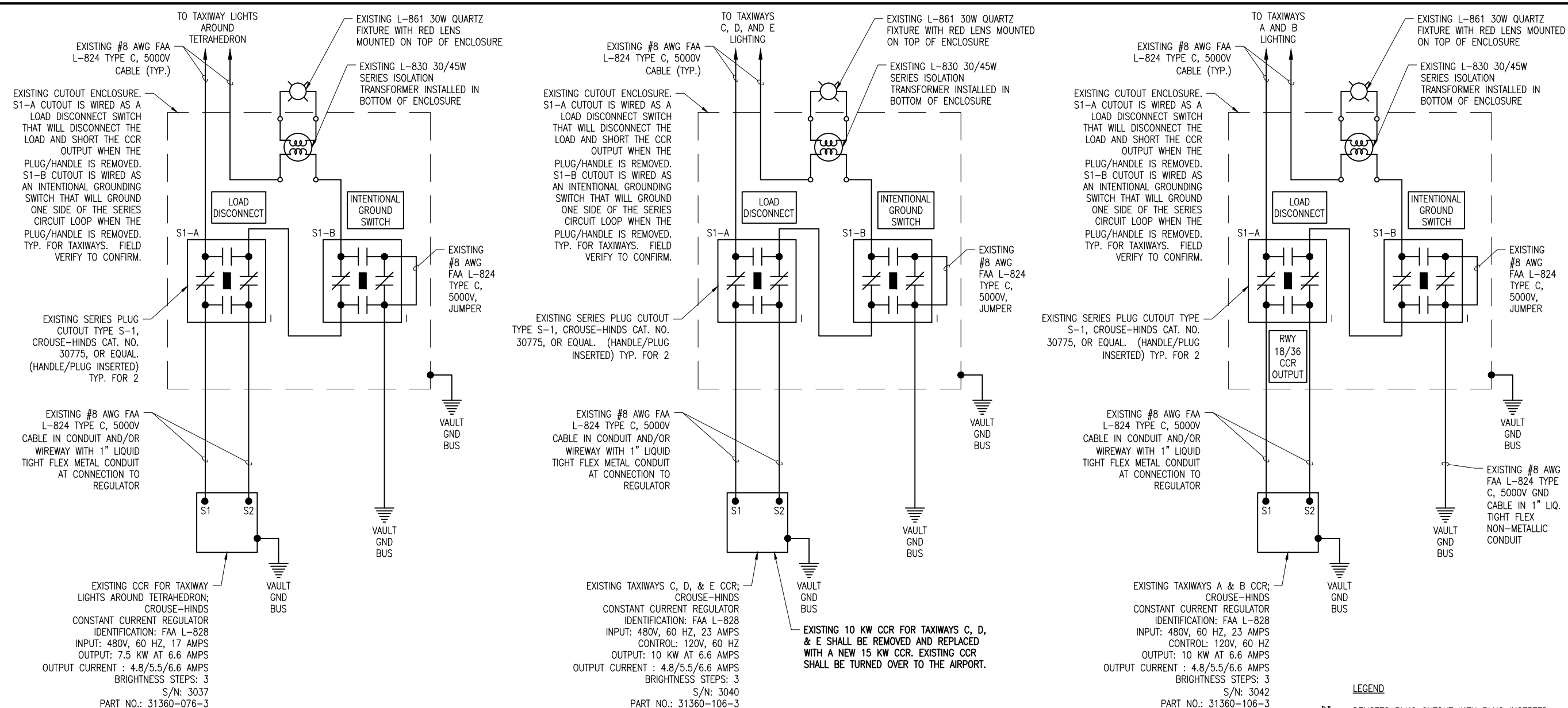
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SHEET TITLE

**EXISTING HIGH VOLTAGE WIRING SCHEMATIC FOR TAXIWAYS**



**EXISTING HIGH VOLTAGE WIRING SCHEMATIC FOR TAXIWAYS**

**NOTES:**

- ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER/DIRECTOR AND RESIDENT ENGINEER/RESIDENT PROJECT REPRESENTATIVE. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- CONTRACTOR SHALL EXAMINE THE SITE TO CONFIRM AND FIELD VERIFY EXISTING SITE CONDITIONS.
- THE RESPECTIVE PERSONNEL PERFORMING AIRFIELD LIGHTING WORK, VAULT WORK, AND/OR TESTS SHALL BE FAMILIAR WITH, AND QUALIFIED TO WORK ON, 5000 VOLT AIRFIELD LIGHTING SERIES CIRCUITS, CONSTANT CURRENT REGULATORS, AND ASSOCIATED AIRPORT ELECTRICAL VAULT EQUIPMENT.
- NOTE THE CONDITION OF THE EXISTING TAXIWAY CIRCUIT CUTOUTS IS UNKNOWN. IT IS POSSIBLE THAT SOME CUTOUTS MIGHT NOT FUNCTION PROPERLY. CONTRACTOR SHALL EXERCISE CAUTION, PRACTICE SAFETY, AND DISCONNECT THE SERIES CIRCUITS FROM THE RESPECTIVE CONSTANT CURRENT REGULATORS, AS APPLICABLE WHEN PERFORMING WORK ON THE AIRFIELD LIGHTING OR WORK THAT MIGHT AFFECT THE AIRFIELD LIGHTING. CONTRACTOR SHALL MAKE NECESSARY ARRANGEMENTS TO DISCONNECT POWER AND LOCKOUT CIRCUITS FOR PROTECTION OF PERSONNEL.
- CONDUCT TESTS FOR AREAS OF WORK WHERE THE RESPECTIVE CIRCUITS MIGHT BE AFFECTED. MEGGER TEST AND RECORD EXISTING SERIES CIRCUITS PRIOR TO CABLE WORK AND AGAIN AFTER AIRFIELD LIGHTING MODIFICATIONS, ADDITIONS, AND/OR UPGRADES HAVE BEEN COMPLETED. ALSO TEST AND RECORD SERIES CIRCUIT LOOP RESISTANCE.
- CCR FOR TAXIWAY LIGHTS AROUND TETRAHEDRON, TAXIWAYS C, D, AND E CCR, TAXIWAYS A AND B CCR, AND THE ASSOCIATED CUTOUTS ARE EXISTING.
- THE RESPECTIVE CCR'S SHALL BE TESTED FOR PROPER OPERATION BEFORE REMOVAL WORK, MODIFICATIONS, AND/OR ADDITIONS AND AFTER THE NEW CABLES AND LIGHTING SYSTEM MODIFICATIONS AND ADDITIONS HAVE BEEN COMPLETED. CONTRACTOR SHALL TEST AND RECORD THE INPUT CURRENT AND OUTPUT CURRENT FOR EACH CONSTANT CURRENT REGULATOR IN THE AUTOMATIC AND MANUAL MODES OF OPERATIONS. CONTRACTOR SHALL REPORT CONCERNS AND/OR DEFICIENCIES TO THE RESIDENT PROJECT REPRESENTATIVE/RESIDENT ENGINEER.

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*Kevin N. Lightfoot*  
6/9/2015

**REALIGN TAXIWAY D**

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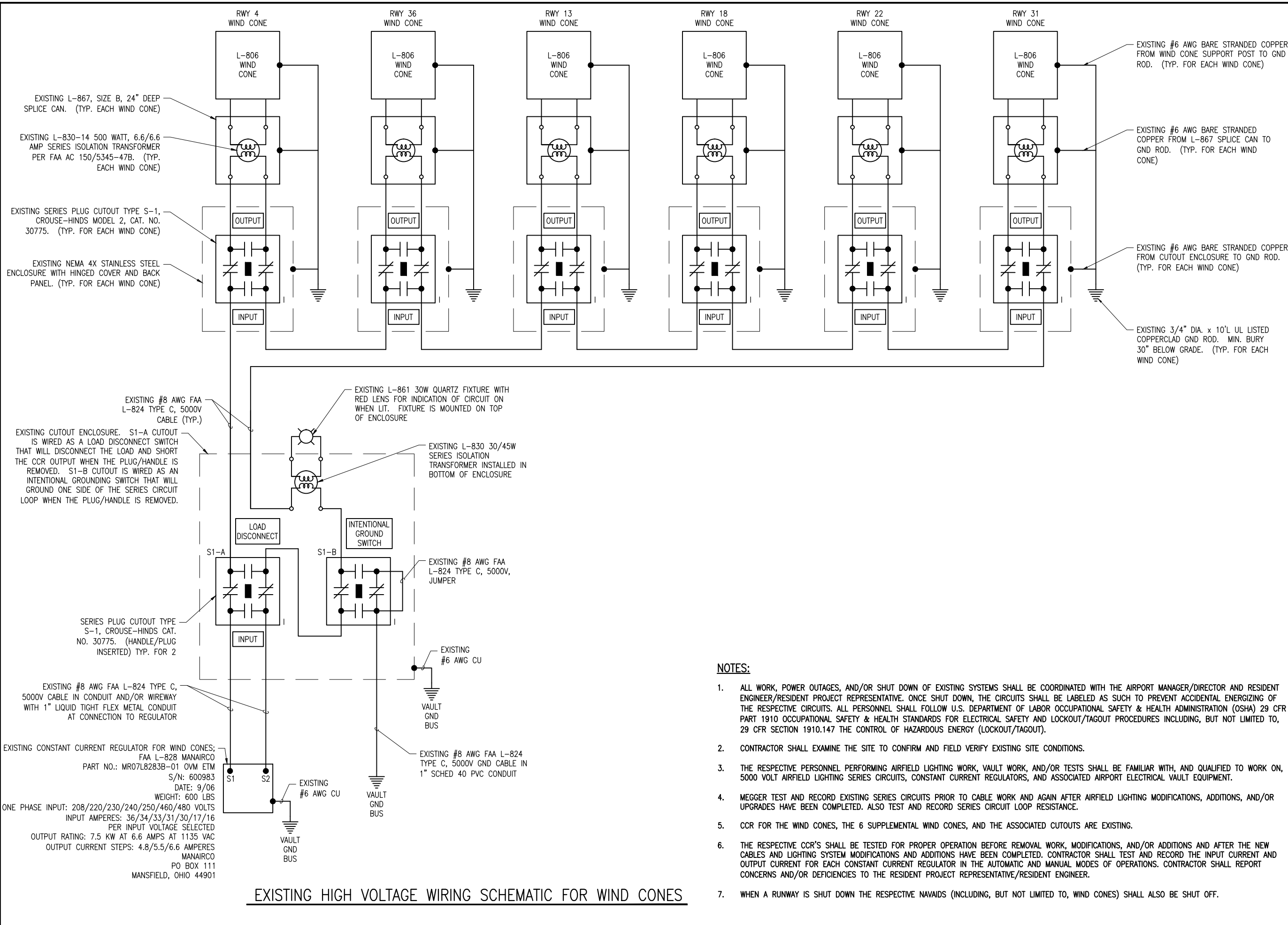
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SHEET TITLE

**EXISTING HIGH VOLTAGE WIRING SCHEMATIC FOR WIND CONES**



**NOTES:**

- ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER/DIRECTOR AND RESIDENT ENGINEER/RESIDENT PROJECT REPRESENTATIVE. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- CONTRACTOR SHALL EXAMINE THE SITE TO CONFIRM AND FIELD VERIFY EXISTING SITE CONDITIONS.
- THE RESPECTIVE PERSONNEL PERFORMING AIRFIELD LIGHTING WORK, VAULT WORK, AND/OR TESTS SHALL BE FAMILIAR WITH, AND QUALIFIED TO WORK ON, 5000 VOLT AIRFIELD LIGHTING SERIES CIRCUITS, CONSTANT CURRENT REGULATORS, AND ASSOCIATED AIRPORT ELECTRICAL VAULT EQUIPMENT.
- MEGGER TEST AND RECORD EXISTING SERIES CIRCUITS PRIOR TO CABLE WORK AND AGAIN AFTER AIRFIELD LIGHTING MODIFICATIONS, ADDITIONS, AND/OR UPGRADES HAVE BEEN COMPLETED. ALSO TEST AND RECORD SERIES CIRCUIT LOOP RESISTANCE.
- CCR FOR THE WIND CONES, THE 6 SUPPLEMENTAL WIND CONES, AND THE ASSOCIATED CUTOUTS ARE EXISTING.
- THE RESPECTIVE CCR'S SHALL BE TESTED FOR PROPER OPERATION BEFORE REMOVAL WORK, MODIFICATIONS, AND/OR ADDITIONS AND AFTER THE NEW CABLES AND LIGHTING SYSTEM MODIFICATIONS AND ADDITIONS HAVE BEEN COMPLETED. CONTRACTOR SHALL TEST AND RECORD THE INPUT CURRENT AND OUTPUT CURRENT FOR EACH CONSTANT CURRENT REGULATOR IN THE AUTOMATIC AND MANUAL MODES OF OPERATIONS. CONTRACTOR SHALL REPORT CONCERNS AND/OR DEFICIENCIES TO THE RESIDENT PROJECT REPRESENTATIVE/RESIDENT ENGINEER.
- WHEN A RUNWAY IS SHUT DOWN THE RESPECTIVE NAVAIDS (INCLUDING, BUT NOT LIMITED TO, WIND CONES) SHALL ALSO BE SHUT OFF.

**EXISTING HIGH VOLTAGE WIRING SCHEMATIC FOR WIND CONES**

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REALIGN TAXIWAY D

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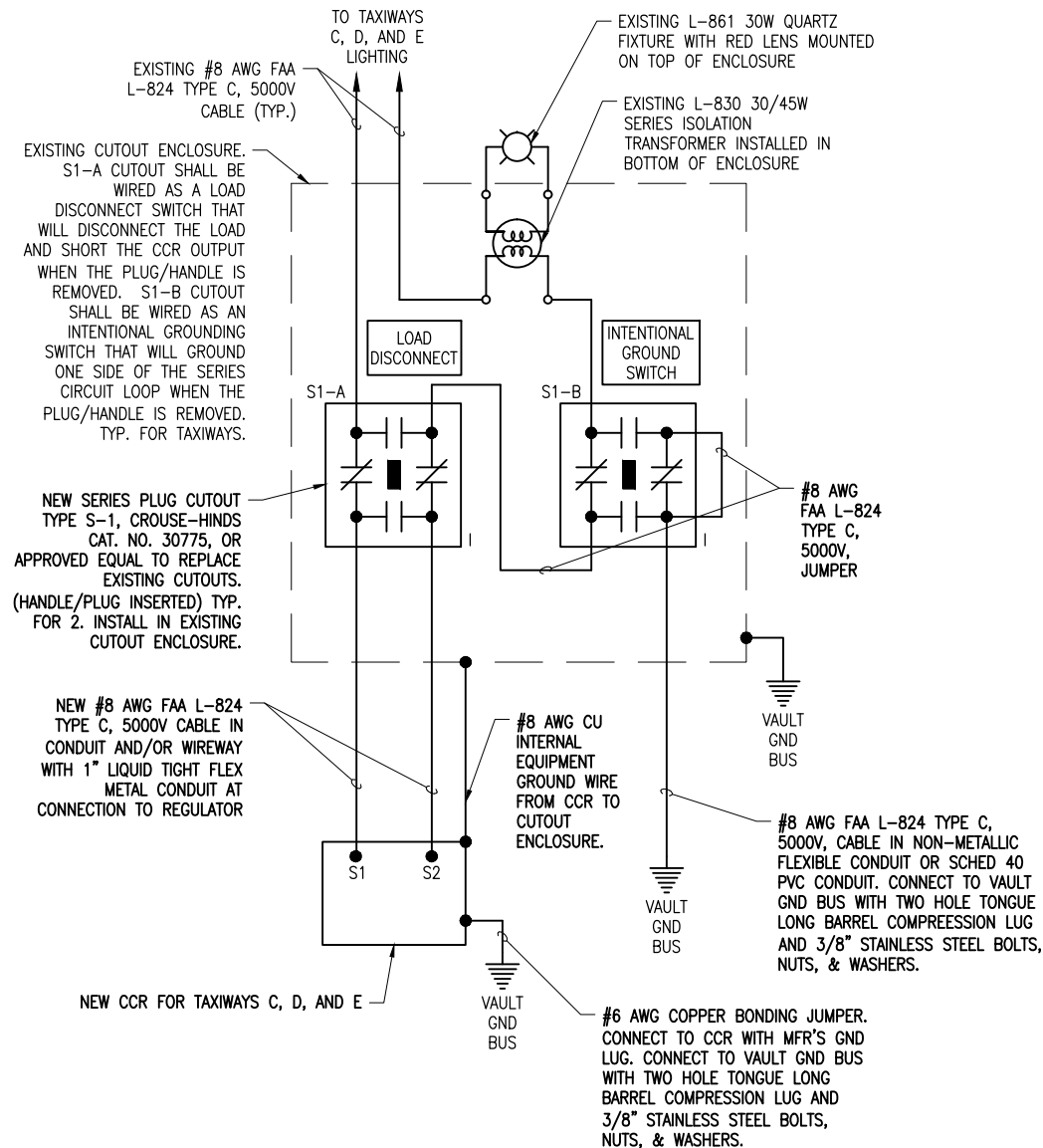
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SHEET TITLE

PROPOSED HIGH VOLTAGE WIRING SCHEMATIC FOR TAXIWAYS C, D, & E



PROPOSED HIGH VOLTAGE WIRING SCHEMATIC FOR TAXIWAYS C, D, & E

**NOTES:**

1. PROVIDE PHENOLIC ENGRAVED LEGEND PLATES FOR EACH CONSTANT CURRENT REGULATOR (EXISTING & NEW) NOTING THE RUNWAY AND/OR TAXIWAY SERVED.
2. EACH PLUG CUTOFF CABINET SHALL BE FURNISHED WITH A PHENOLIC ENGRAVED LEGEND PLATE THAT IDENTIFIES THE RESPECTIVE RUNWAY OR TAXIWAY CIRCUIT OR REGULATOR. INCLUDE AN ADDITIONAL LEGEND PLATE LABELED "CAUTION OPERATE CUTOFFS WITH CCR SHUT OFF".
3. PROVIDE PHENOLIC ENGRAVED LEGEND PLATES FOR THE CUTOFFS TO IDENTIFY THE RESPECTIVE REGULATOR OUTPUT CONNECTION AND THE RESPECTIVE CIRCUIT LOAD CONNECTION.
4. BOND EACH REGULATOR FRAME TO VAULT GROUND BUS WITH A DEDICATED #6 AWG COPPER BONDING JUMPER.
5. PROVIDE ADEQUATE WORKING SPACE IN FRONT OF EACH CUTOFF ENCLOSURE TO MEET NEC CLEARANCE REQUIREMENTS.
6. LIQUID TIGHT FLEXIBLE METAL CONDUIT AND ASSOCIATED FITTINGS SHALL BE U.L. LISTED TO MEET THE REQUIREMENTS OF NEC 350.6, SUITABLE FOR GROUNDING AND SUNLIGHT RESISTANT. LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS USED FOR FLEXIBILITY (INCLUDING CONNECTIONS TO CCR'S & TRANSFORMERS) SHALL REQUIRE AN EXTERNAL BONDING JUMPER OR INTERNAL EQUIPMENT GROUNDING CONDUCTOR PER NEC 350.60. EXTERNAL BONDING JUMPERS USED WITH CCR INSTALLATIONS SHALL BE #6 AWG COPPER (MINIMUM). DO NOT INSTALL LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS NOT UL LISTED. CONFIRM LIQUID TIGHT FLEXIBLE METAL CONDUIT BEARS THE UL LABEL PRIOR TO INSTALLING IT.
7. SERIES PLUG CUTOFFS SHALL BE TYPE S-1, RATED 5000 VOLTS, 20-AMP, AND SHALL COMPLY WITH FAA AC 150/5340-4C, SERIES PLUG CUTOFFS SHALL BE RATED SUITABLE FOR NORMAL OPERATION WITH HANDLE REMOVED OR HANDLE INSERTED. CUTOFFS SHALL DISCONNECT THE INPUT FROM THE OUTPUT, SHORT THE INPUT TERMINALS, AND SHORT THE OUTPUT TERMINALS WHEN THE HANDLE/PLUG IS REMOVED. CUTOFFS SHALL ALSO BE SUITABLE FOR USE AS AN INTENTIONAL GROUND SWITCH. SERIES PLUG CUTOFFS SHALL BE CROUSE-HINDS CAT. NO. 30775, OR APPROVED EQUAL THE RESPECTIVE MANUFACTURER SHALL CERTIFY IN WRITING THAT THEIR CUTOFF IS SUITABLE AND RATED FOR THE RESPECTIVE APPLICATION.
8. HIGH VOLTAGE & LOW VOLTAGE CIRCUITS SHALL NOT BE INSTALLED IN THE SAME WIREWAY, CONDUIT, HANDHOLE, JUNCTION BOX, OR RACEWAY.
9. EQUIPMENT AND MATERIALS NOT LABELED AS EXISTING ARE NEW.







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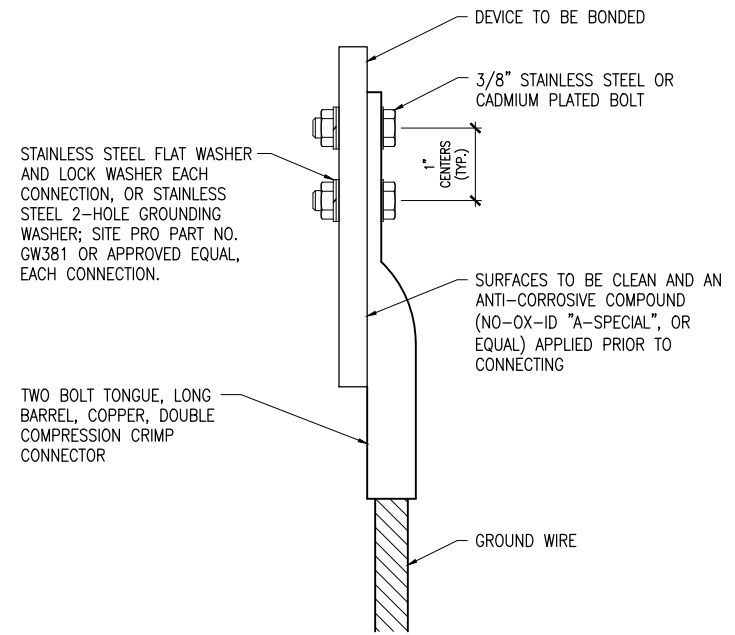
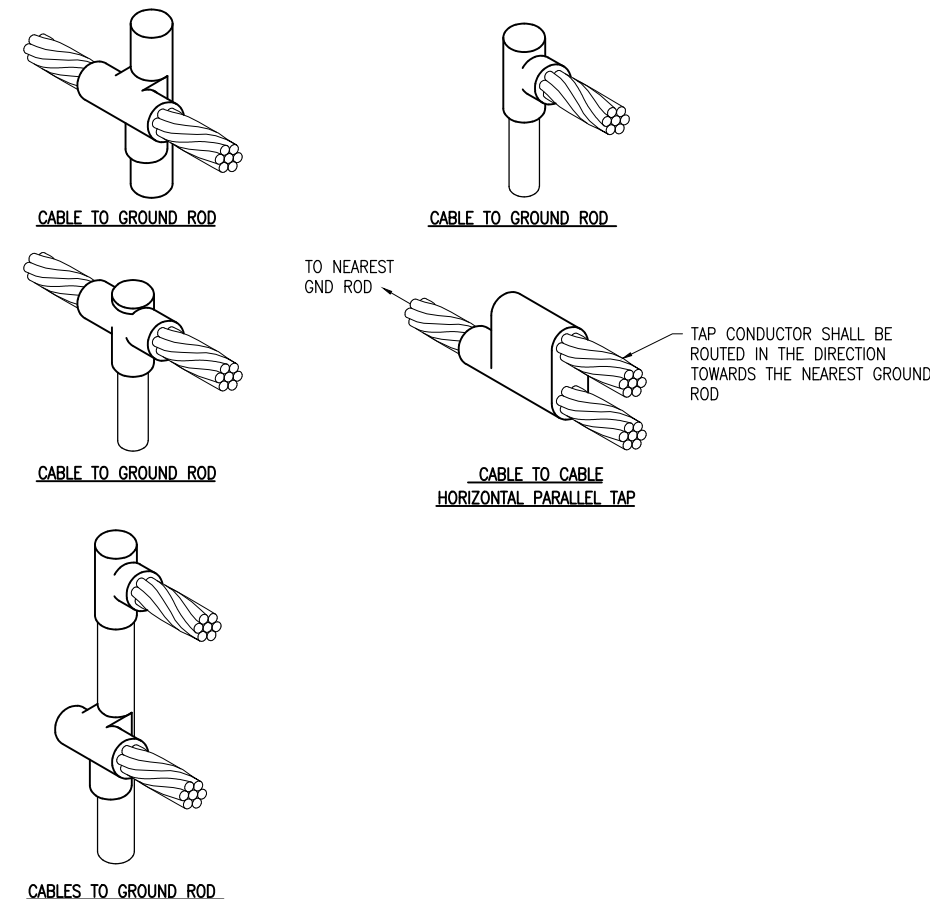
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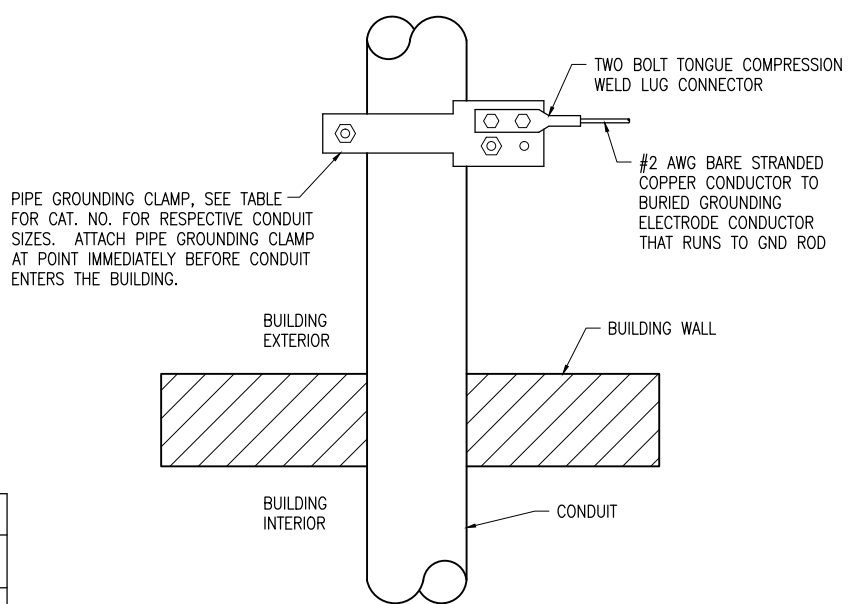
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CAD FILE: E-505-DETL.DWG  
DESIGN BY: KNL 11/04/2014  
DRAWN BY: RAD 11/05/2014  
REVIEWED BY: KNL 11/05/2014

SHEET TITLE

GROUNDING DETAILS



WIRE SIZE	BURNDY CAT. NO.	THOMAS & BETTS CAT. NO.	PENN-UNION CAT. NO.
#8 AWG STRANDED	YA8C-2TC38	256-30695-1157	BBLU-8D-2TC38
#6 AWG SOLID	YA8C-2TC38 OR YGA6C-2TC38E2G1		
#6 AWG STRANDED	YA6C-2TC38	256-30695-1158	BBLU-6D-2TC38
#4 AWG STRANDED	YA4C-2TC38	256-30695-1159	BBLU-4D-2TC38
#2 AWG STRANDED	YA2C-2TC38	256-30695-1160	BBLU-2D-2TC38
#2 AWG SOLID	YA3C-2TC38	256-30695-1160	BBLU-3D-2TC38
#1/0 AWG STRANDED	YA25-2TC38	256-30695-1162	BBLU-1/0D-2TC38
#2/0 AWG STRANDED	YA26-2TC38	256-30695-1116	BBLU-2/0D-2TC38
#3/0 AWG STRANDED	YA27-2TC38	54816BE	BBLU-3/0D-2TC38
#4/0 AWG STRANDED	YA28-2TC38	256-30695-1117	BBLU-4/0D-2TC38



BURNDY CAT. NO.	CONDUIT SIZE
GAR3902TC	1/2" - 1"
GAR3903TC	1 1/4" - 2"
GAR3904TC	2 1/2" - 3 1/2"
GAR3905TC	4" - 5"
GAR3906TC	6"
GAR3907TC	8"

- NOTES**
- EXTERIOR CONDUIT GROUNDING IS REQUIRED FOR THE PHOTOCELL CONDUIT, RADIO ANTENNA CONDUIT, & OTHER CONDUITS EXTENDING TO THE ROOF LEVEL.
  - CONNECTIONS TO BURIED GROUNDING ELECTRODE CONDUCTOR SHALL BE EXOTHERMIC WELD.

EXTERIOR CONDUIT GROUNDING DETAIL

- NOTES**
- ALL CONNECTIONS TO GROUND BUS BAR SHALL BE WITH 2 HOLE LONG BARREL COMPRESSION LUGS BOLTED TO THE BUS BAR.
  - GROUND WIRE CONNECTIONS TO EQUIPMENT SHALL BE WITH 2 HOLE TONGUE LONG BARREL COMPRESSION LUGS BOLTED TO THE DEVICE OR WITH THE RESPECTIVE EQUIP MANUFACTURER'S LUG OR TERMINAL WHERE APPLICABLE.
  - GROUNDING ELECTRODE CONDUCTORS, BONDING JUMPERS, & INDIVIDUAL GROUND WIRES SHALL NOT BE INSTALLED IN METAL CONDUIT. WHERE PLASTIC CONDUIT IS USED FOR INDIVIDUAL GROUND WIRES, DO NOT COMPLETELY ENCIRCLE THE CONDUIT WITH FERROUS AND/OR MAGNETIC MATERIALS. WHERE METAL CLAMPS ARE INSTALLED USE NYLON BOLTS, NUTS, WASHERS, & SPACERS TO INTERRUPT A COMPLETE METALLIC APTH FROM ENCIRCLING THE CONDUIT.
  - ALL CONNECTIONS SHALL BE COATED WITH A CORROSION PREVENTATIVE COMPOUND (SANICHEM INC. NO-OX-ID "A-SPECIAL", BURNDY PENETROX E, OR EQUAL) BEFORE JOINING. ALL COPPER BUS BARS SHALL BE CLEANED PRIOR TO MAKING CONNECTIONS TO REMOVE SURFACE OXIDATION. CLEAN SURFACES, OF RESPECTIVE DEVICES TO BE BONDED, TO BARE METAL, PER NEC 250-12.

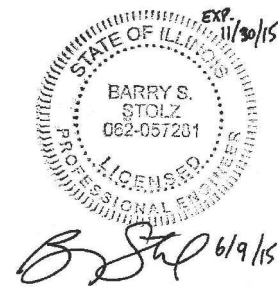
GROUNDING LUG CONNECTION DETAIL

**DETAIL NOTES**

- ALL BELOW GRADE CONNECTIONS TO GROUND RODS & GROUND RING CONDUCTORS SHALL BE EXOTHERMIC WELD TYPE CONNECTIONS. EXOTHERMIC WELDS SHALL BE CADWELD AS MANUFACTURED BY ERICO PRODUCTS, SOLON, OHIO, ULTRAWELD AS MANUFACTURED BY HARGER LIGHTNING PROTECTION & GROUNDING EQUIPMENT, GRAYSLAKE, IL, OR THERMOWELD AS MANUFACTURED BY CONTINENTAL INDUSTRIES, TULSA, OKLAHOMA. VERIFY PROPER SIZES, MOLDS, TYPES, AND REQUIREMENTS FOR THE RESPECTIVE APPLICATION WITH THE MANUFACTURER, AND INSTALL PER THEIR DIRECTIONS.
- FOR APPLICATIONS TO GALVANIZED STEEL OR PAINTED STEEL, REMOVE GALVANIZING AND/OR PAINT & CLEAN THE SURFACE TO EXPOSE BARE STEEL BEFORE MAKING EXOTHERMIC WELD CONNECTION.
- INDIVIDUAL GROUNDING ELECTRODE CONDUCTORS SHALL NOT BE INSTALLED IN METAL CONDUIT. INSTALL GROUNDING ELECTRODE CONDUCTORS IN SCHED 40 PVC CONDUIT AS REQUIRED IN FOUNDATIONS, FOR PROTECTION, WHERE ENTERING ENCLOSURES, ETC. WHERE PLASTIC CONDUIT IS USED FOR INDIVIDUAL GROUND WIRES, DO NOT COMPLETELY ENCIRCLE THE CONDUIT WITH FERROUS AND/OR MAGNETIC MATERIALS. WHERE METAL CLAMPS ARE INSTALLED USE NYLON BOLTS, NUTS, WASHERS, & SPACERS TO INTERRUPT A COMPLETE METALLIC PATH FROM ENCIRCLING THE CONDUIT.

EXOTHERMIC WELD DETAILS

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IDA No: UIN-4332

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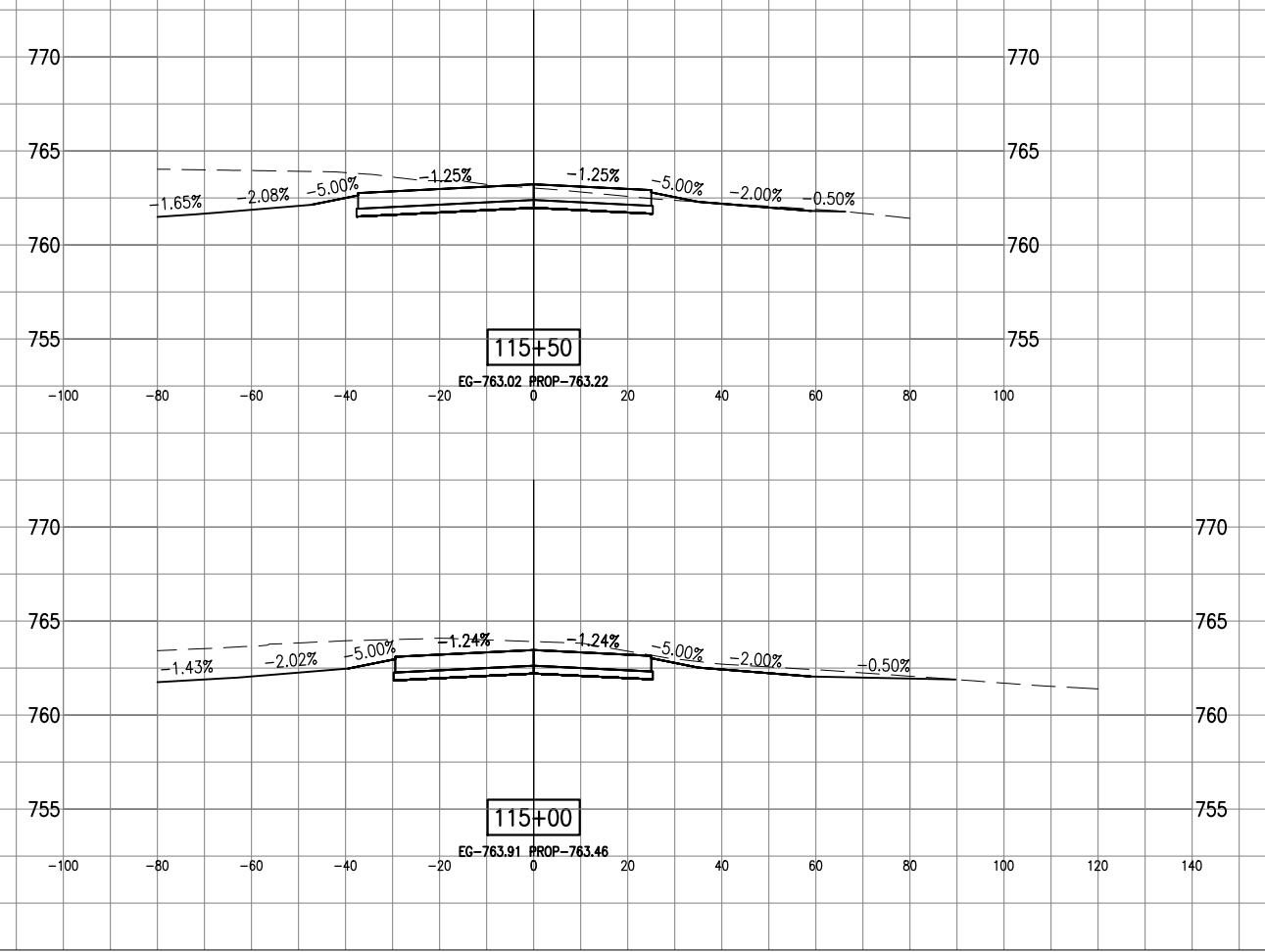
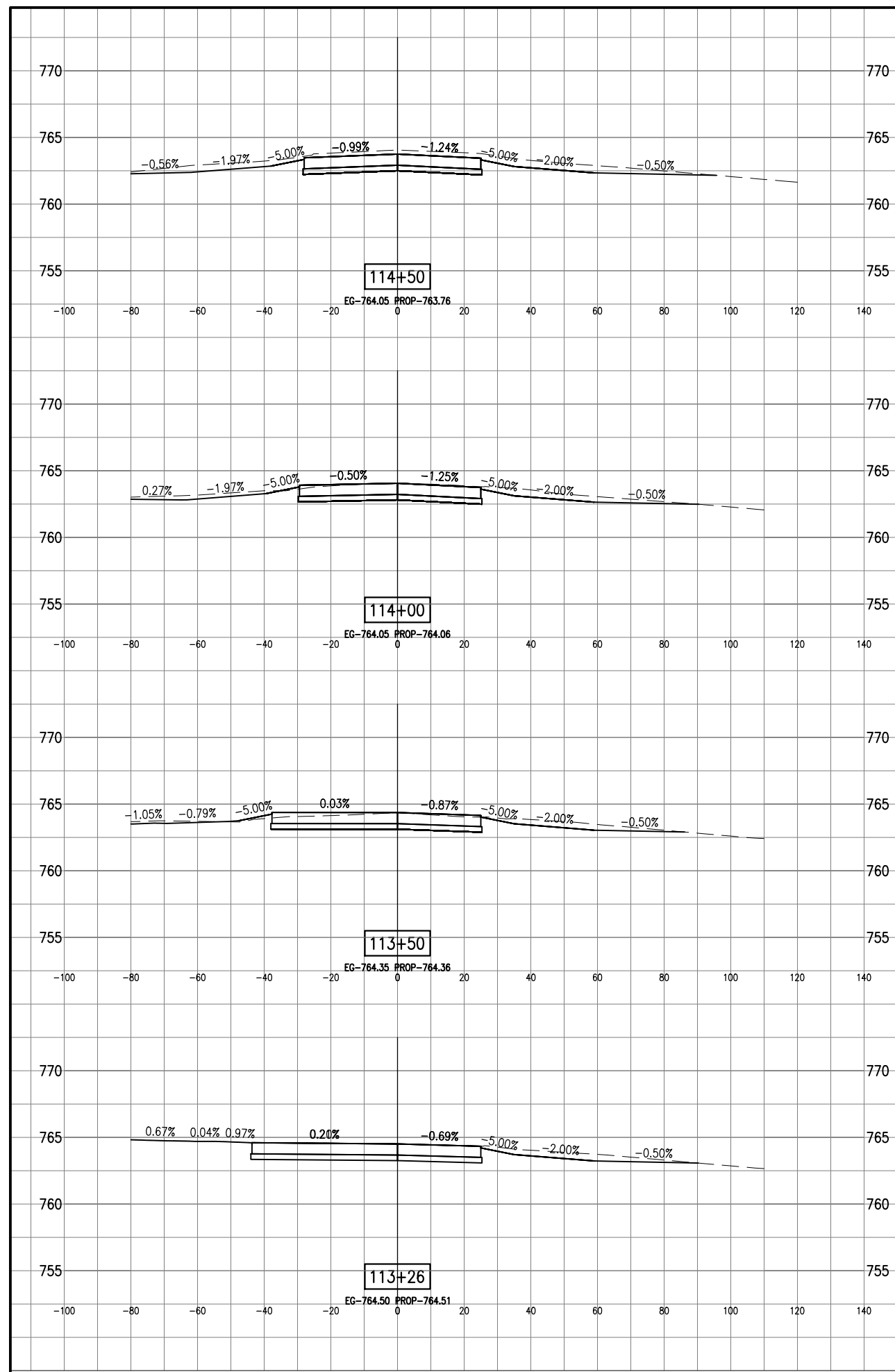
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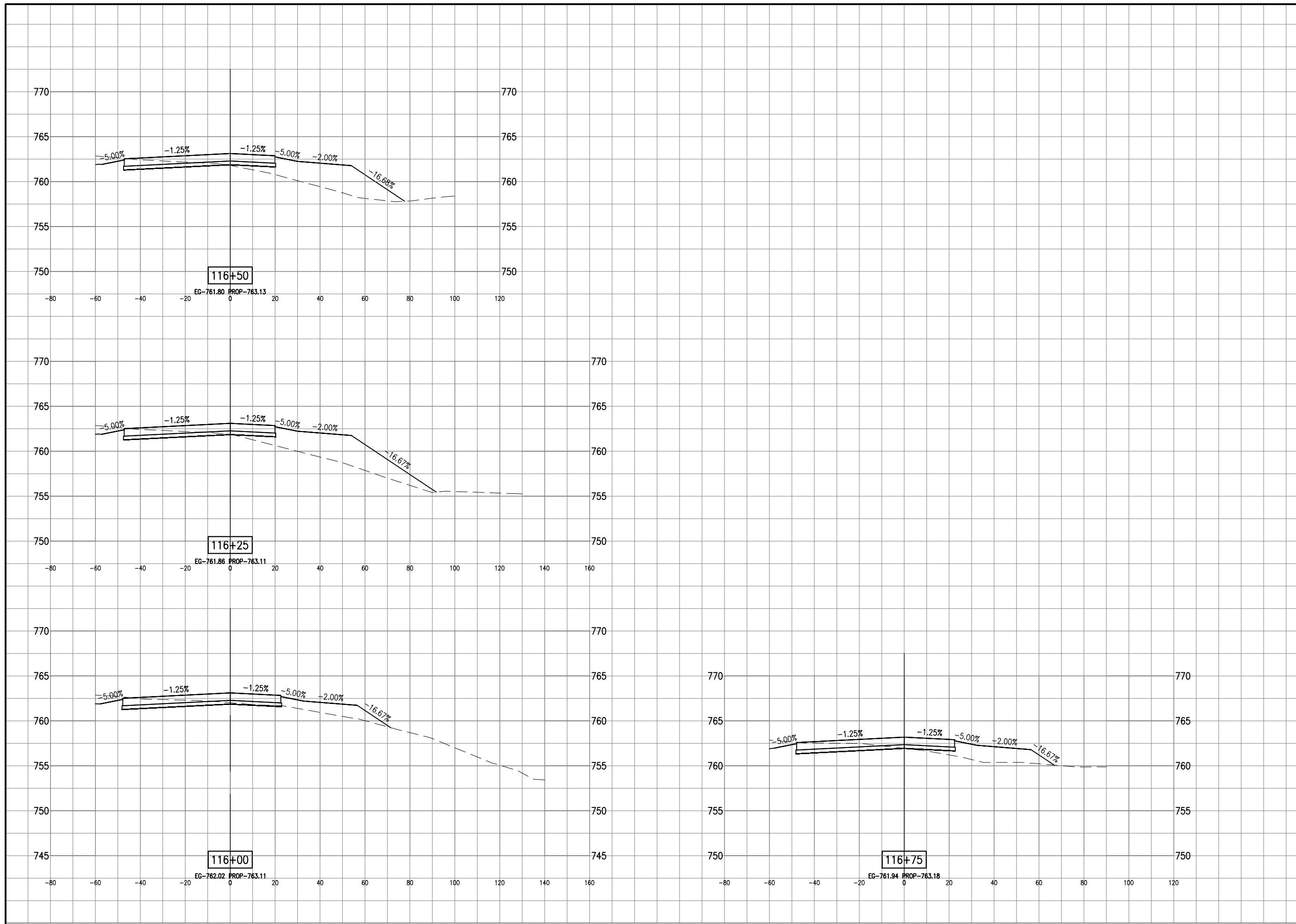
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*B. Stolz* 6/9/15

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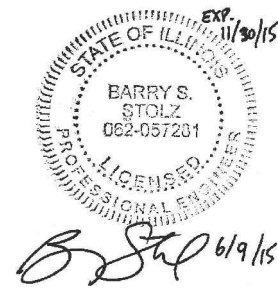

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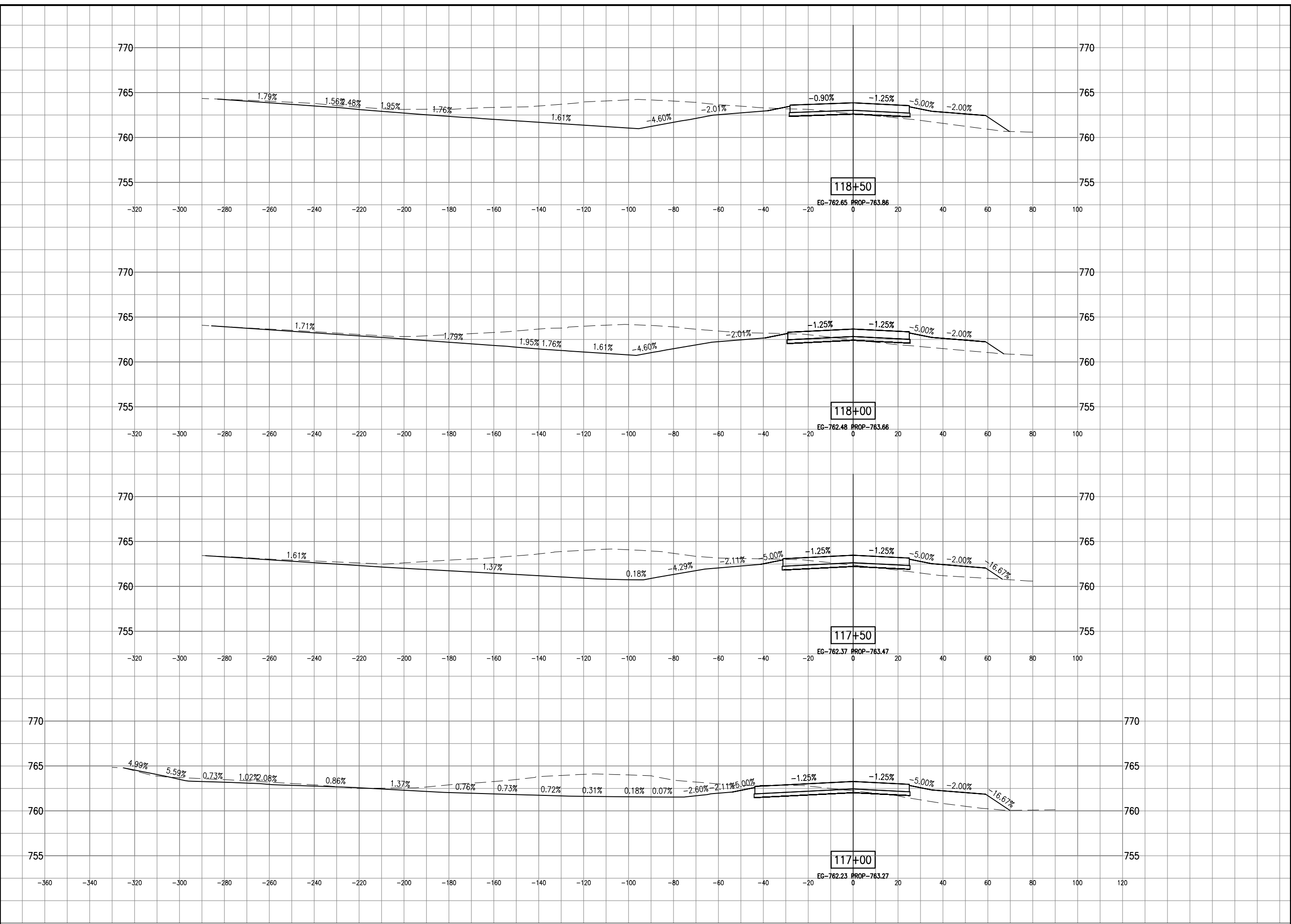
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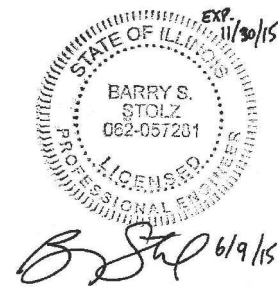
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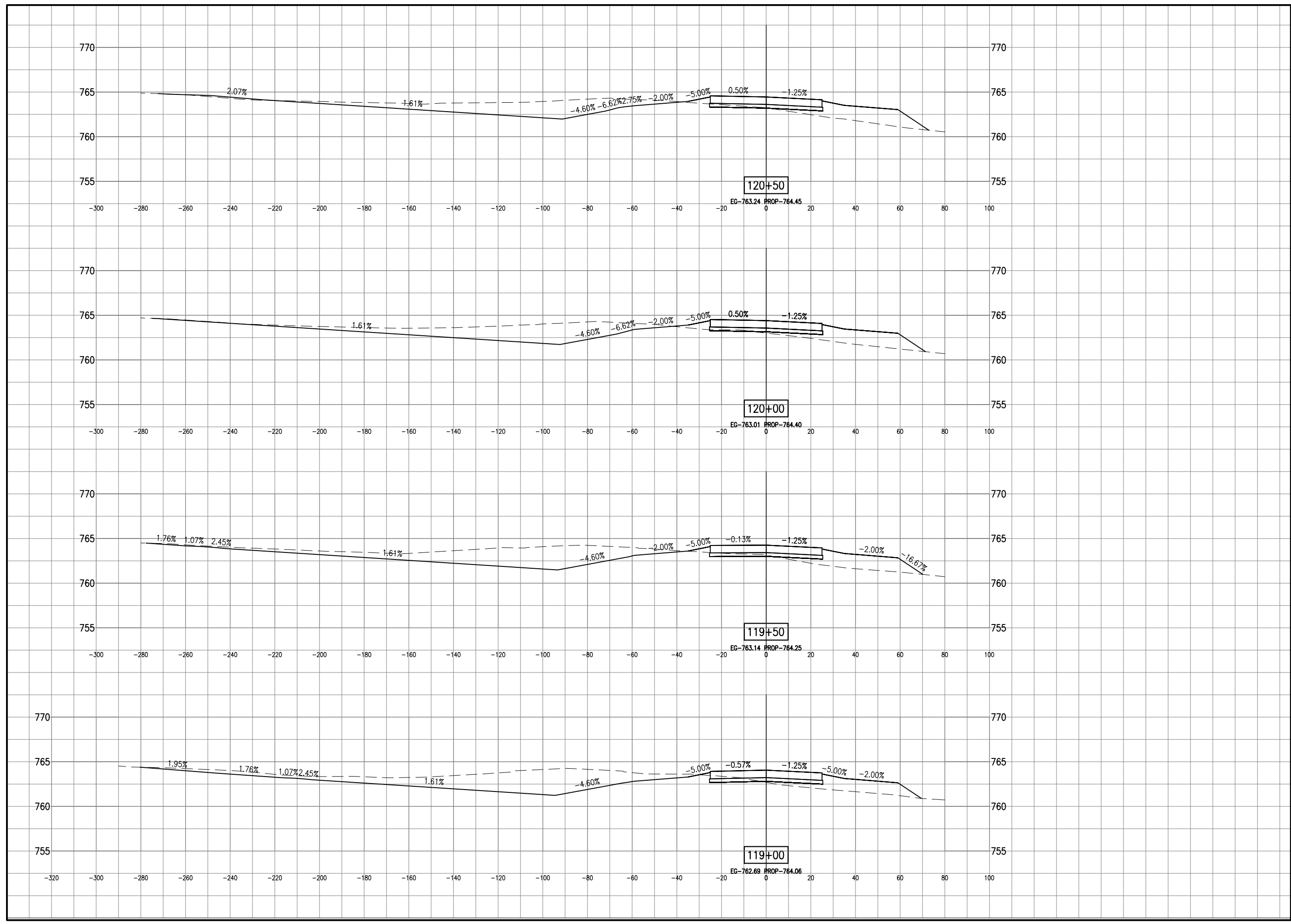
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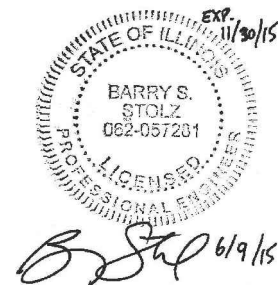
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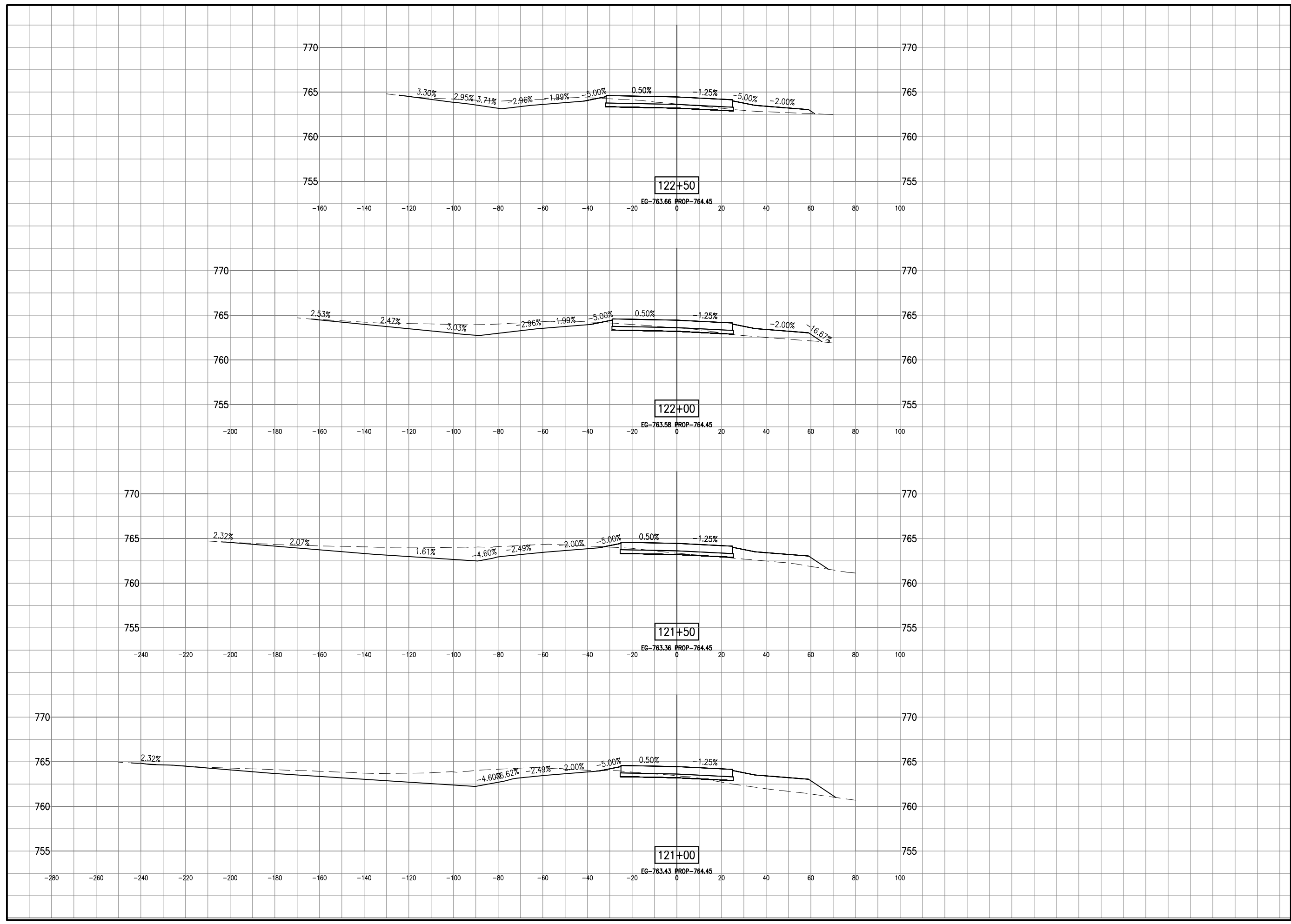
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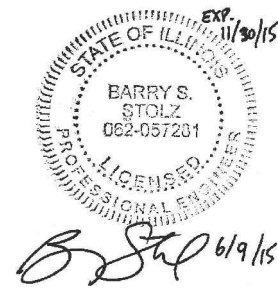
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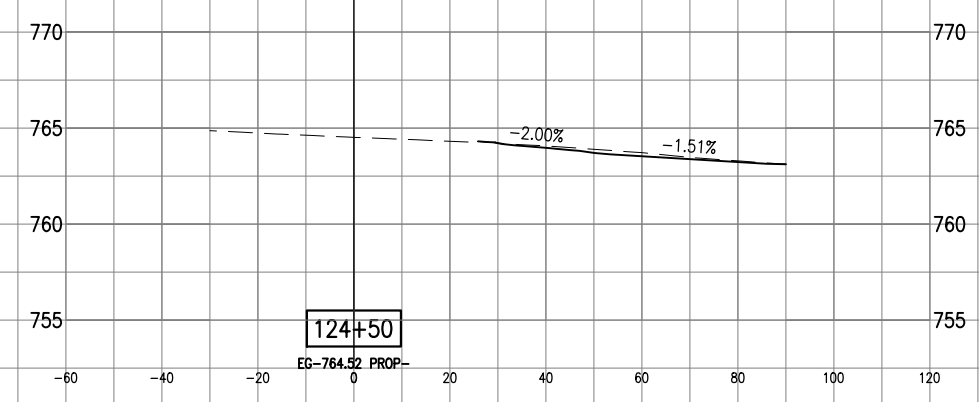
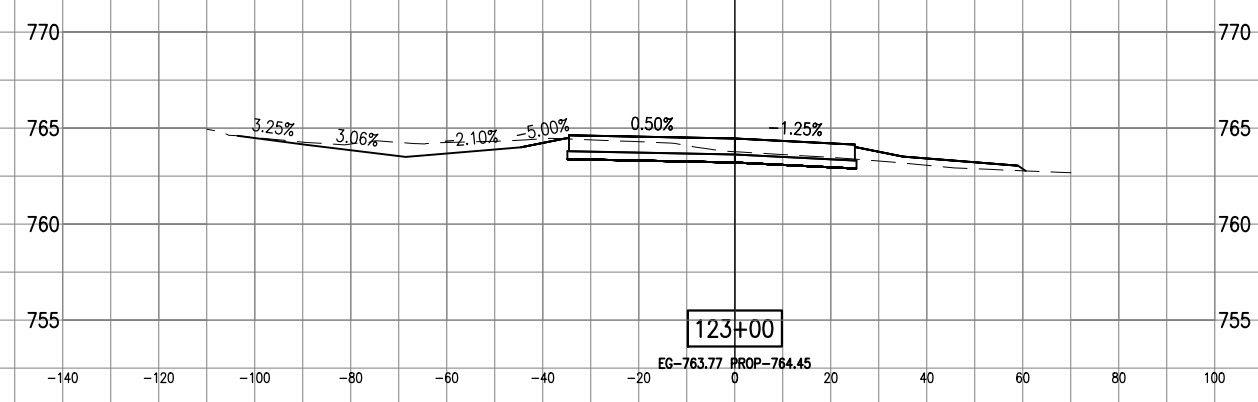
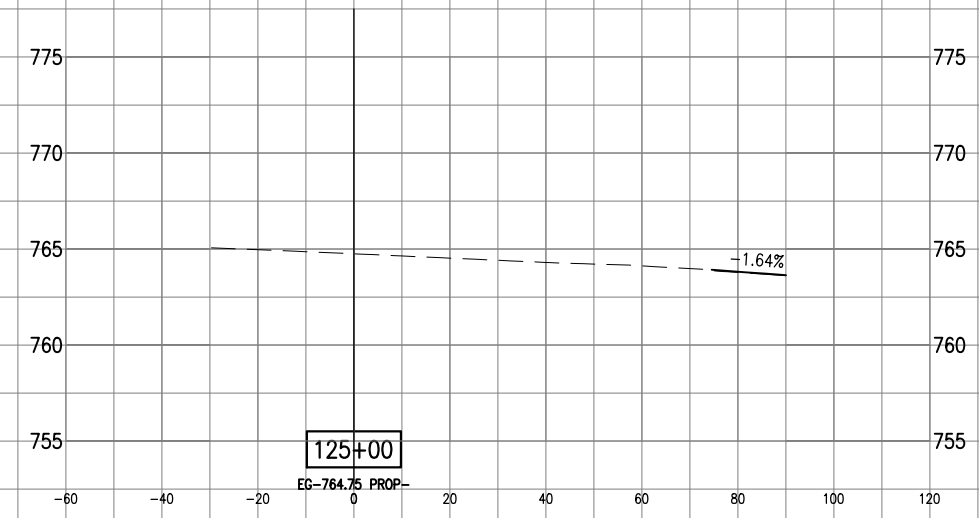
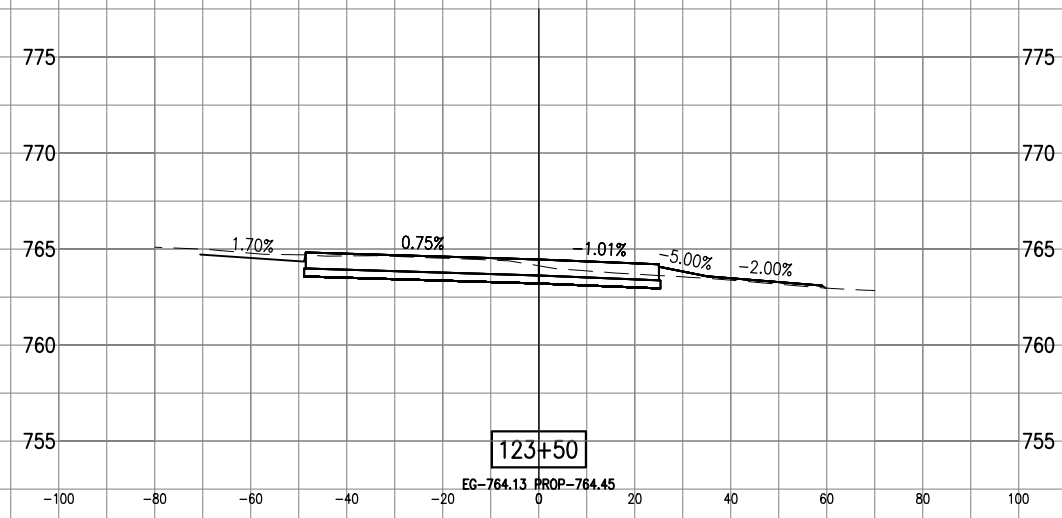
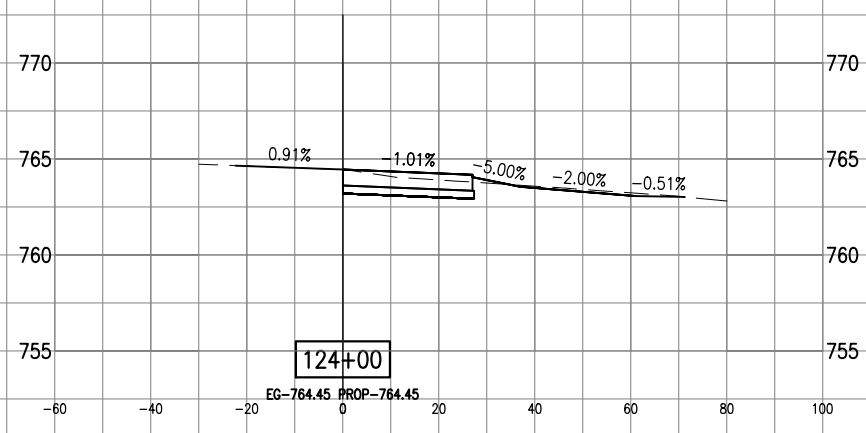
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