

LETTING ITEM NO. 9A  
 LETTING DATE: JULY 31, 2015

TOTAL SHEETS: 39  
 PE099

# CONSTRUCTION PLANS FOR PEORIA INTERNATIONAL AIRPORT

GENERAL WAYNE A. DOWNING  
 PEORIA, ILLINOIS

IL. PROJ. NO. PIA-4485  
 AIP PROJ. NO. 3-17-0080-XX

## RECONSTRUCT TERMINAL APRON PHASE 1

JULY 31, 2015

**811** Know what's below.  
 Call before you dig. COMMON GROUND ALLIANCE  
 www.call811.com or  
 Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

**DESIGN INFORMATION**

GEOMETRIC CRITERIA

AIRPLANE DESIGN GROUP IV  
 TAXIWAY DESIGN GROUP IV

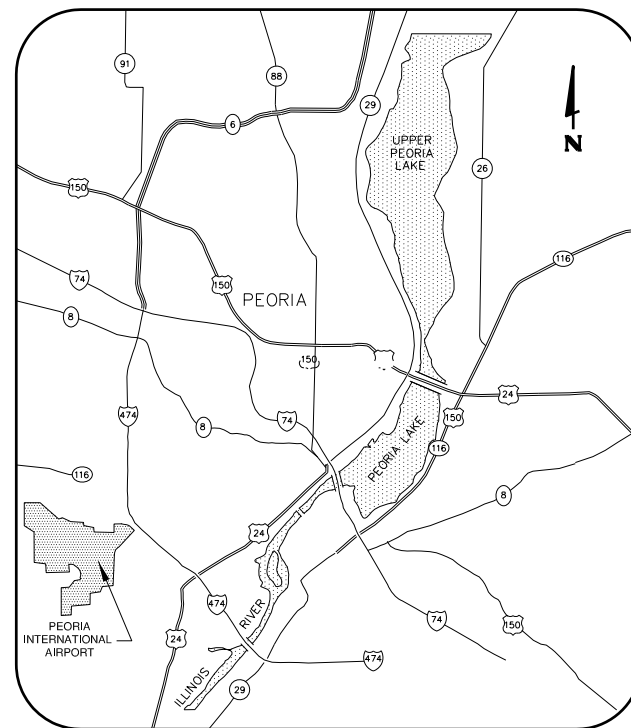
PAVEMENT DESIGN CRITERIA

AIRCRAFT DUAL WHEEL GEAR  
 DEPARTURE WEIGHT = 161,000 LBS  
 32 EQUIV. ANNUAL DEPARTURES

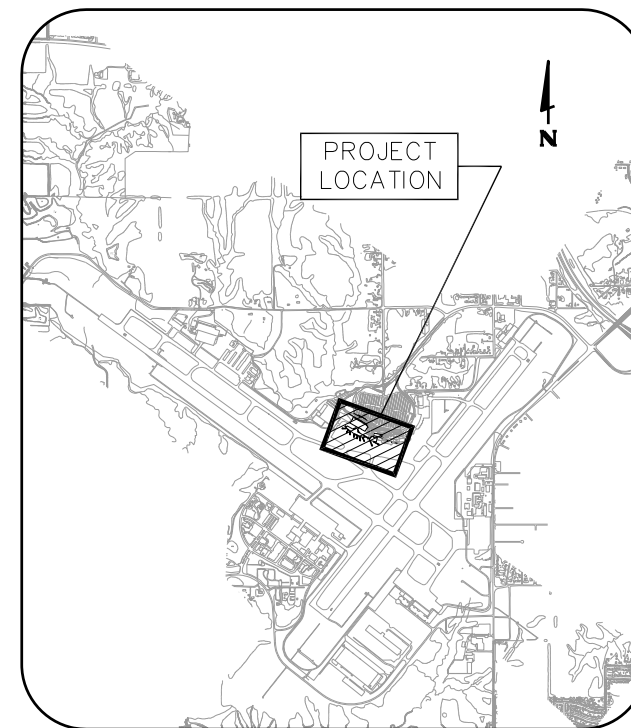
MAXIMUM EQUIPMENT HEIGHT = 25'  
 GROUND FREQUENCY 121.85

GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT

TOWNSHIP: 8 NORTH  
 RANGE: 7 EAST OF THE 4TH P.M.  
 SECTION: 22  
 COUNTY: PEORIA  
 CIVIL TOWNSHIP: LIMESTONE



LOCATION MAP



SITE PLAN



GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT

GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT

APPROVED *General Downing*  
 DATE April 23, 2015

**CMT**

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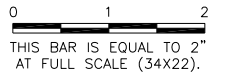
SUBMITTED BY *Christopher B. Gault*  
 DATE 5/22/15

CMT JOB NUMBER: 14061-03

K:\Peoria\14061-03\_TermApronRehab\Draw\Sheets  
 FILE: G000.dwg  
 UPDATE BY: Jeremy Wasilewski  
 PLOT DATE: 7/1/2015 1:31 PM

**PE099**

REVISIONS		
NUMBER	BY	DATE



SUMMARY OF QUANTITIES			
ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
AR150520	MOBILIZATION	LS	1
AR152410	UNCLASSIFIED EXCAVATION	CY	4,975
AR156513	SEPARATION FABRIC	SY	16,650
AR208540	OVERSIZE AGGREGATE	TON	14,750
AR209604	CRUSHED AGGREGATE BASE COURSE - 4"	SY	16,525
AR209606	CRUSHED AGGREGATE BASE COURSE - 6"	SY	110
AR501510	10" PCC PAVEMENT	SY	110
AR501518	18" PCC PAVEMENT	SY	16,525
AR501900	REMOVE PCC PAVEMENT	SY	16,635
AR605510	JOINT SEALING FILLER	LF	17,500
AR620510	PAVEMENT MARKING	SF	12,750
AR620595	TEMPORARY MARKING & REMOVAL	SF	1,700
AR620900	PAVEMENT MARKING REMOVAL	SF	1,225
AR800204	MOVE AIRLINE WORKSTATION	EA	9
AR800258	CONSTRUCTION ACCESS	LS	1

ADDITIVE ALTERNATE 1			
ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
AS152410	UNCLASSIFIED EXCAVATION	CY	450
AS156513	SEPARATION FABRIC	SY	1,475
AS208540	OVERSIZE AGGREGATE	TON	1,325
AS209604	CRUSHED AGGREGATE BASE COURSE - 4"	SY	1,470
AS501518	18" PCC PAVEMENT	SY	1,470
AS501900	REMOVE PCC PAVEMENT	SY	1,470
AS605510	JOINT SEALING FILLER	LF	1,380

ADDITIVE ALTERNATE 2			
ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
AT152410	UNCLASSIFIED EXCAVATION	CY	450
AT156513	SEPARATION FABRIC	SY	1,475
AT208540	OVERSIZE AGGREGATE	TON	1,325
AT209604	CRUSHED AGGREGATE BASE COURSE - 4"	SY	1,470
AT501518	18" PCC PAVEMENT	SY	1,470
AT501900	REMOVE PCC PAVEMENT	SY	1,470
AT605510	JOINT SEALING FILLER	LF	1,380

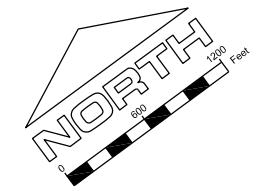
ADDITIVE ALTERNATE 3			
ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
AU152410	UNCLASSIFIED EXCAVATION	CY	450
AU156513	SEPARATION FABRIC	SY	1,475
AU208540	OVERSIZE AGGREGATE	TON	1,325
AU209604	CRUSHED AGGREGATE BASE COURSE - 4"	SY	1,470
AU501518	18" PCC PAVEMENT	SY	1,470
AU501900	REMOVE PCC PAVEMENT	SY	1,470
AU605510	JOINT SEALING FILLER	LF	1,380

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06	CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS
07	CONSTRUCTION ACCESS
08	CONSTRUCTION ACTIVITY PLAN OVERVIEW
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15	FIRST FLOOR FIS BUILDING EXISTING TECHNOLOGY LAYOUT
16	SECOND FLOOR FIS BUILDING EXISTING TECHNOLOGY LAYOUT
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**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**RECONSTRUCT TERMINAL APRON PHASE 1**  
**INDEX TO SHEETS & SUMMARY OF**  
**QUANTITIES**






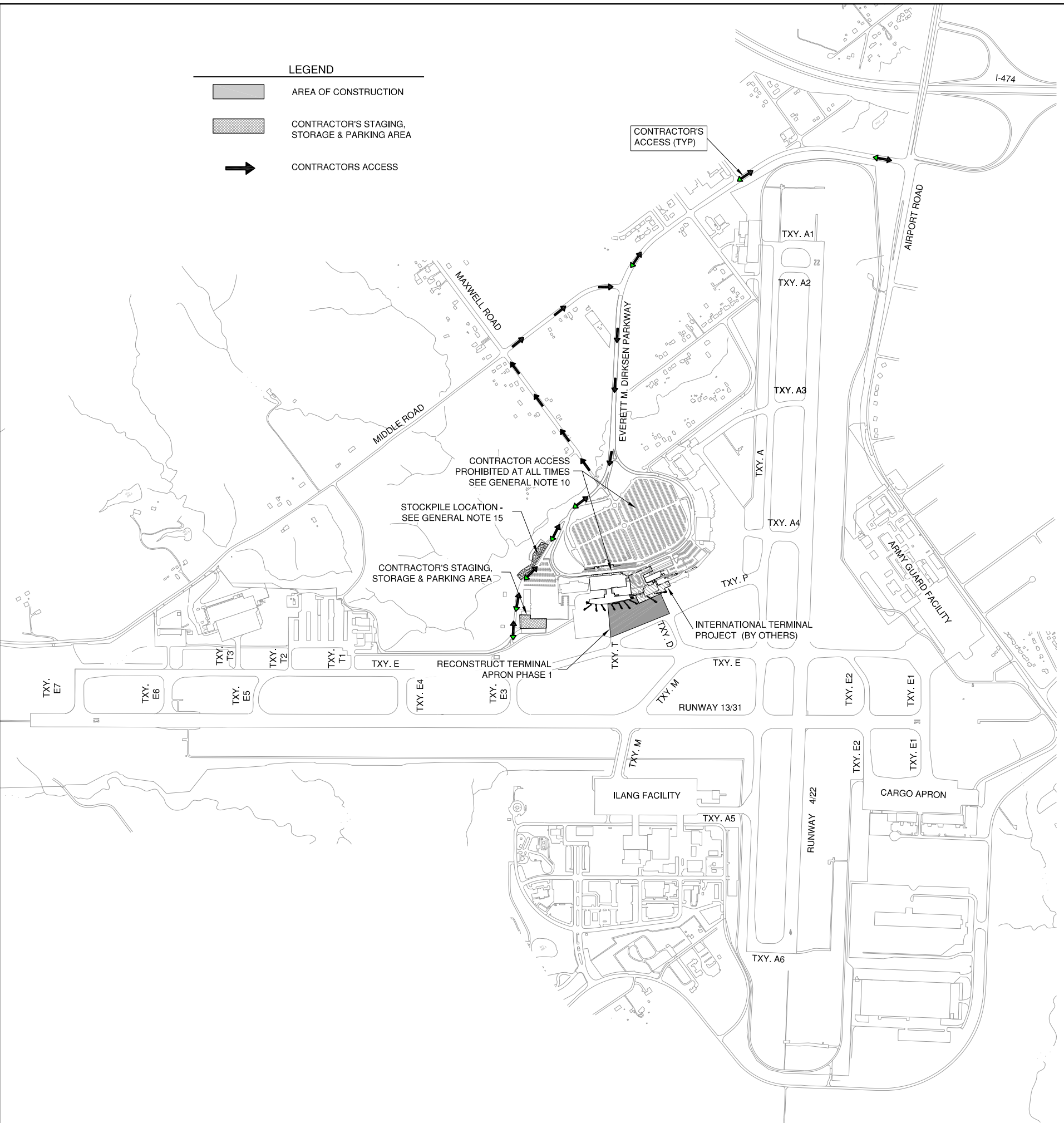
DESIGN BY:	JMW/CET
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	02 OF 39 SHEETS



FILE: CS101.dwg  
 UPDATE BY: Jeremey Wasilewski  
 PLOT DATE: 7/1/2015 1:34 PM  
 PIA\_BASE\_EXISTING  
 FloorPlan-A-FP-01-OVERALLPLAN

**LEGEND**

-  AREA OF CONSTRUCTION
-  CONTRACTOR'S STAGING, STORAGE & PARKING AREA
-  CONTRACTORS ACCESS



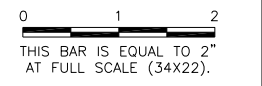
**GENERAL NOTES**

1. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATION SHOWN ON THE CONSTRUCTION ACTIVITY PLAN OVERVIEW. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS, AND STOCKPILES SHALL BE 25 FEET ABOVE GROUND ELEVATION AND SHALL BE STORED A MINIMUM OF 10 FEET FROM THE AIRPORT FENCE.
2. ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF AN ESCORT OR A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED MAAP/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
3. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT DIRECTOR OF OPERATIONS.
4. EXCESS EARTH, BROKEN ASPHALT, AND CONCRETE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
5. ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
6. ALL DISTURBED AREAS WITHIN THE PROJECT LIMITS SHALL BE SEEDED AND STABILIZED. ANY AREAS DISTURBED BY THE CONTRACTOR OUTSIDE THE LIMITS SHOWN SHALL BE SEEDED & STABILIZED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
7. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
8. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE RESIDENT ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
9. SEE CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS SHEET AND CONSTRUCTION ACCESS SHEET FOR NOTES REGARDING THE CONTRACTOR'S ACCESS AND CONSTRUCTION ACTIVITY PLANS FOR DETAILED PHASING REQUIREMENTS.
10. CONTRACTOR SHALL BE EXPLICITLY RESTRICTED FROM USING THE RING ROAD DIRECTLY IN FRONT OF THE MAIN TERMINAL BUILDING AND THE MAIN TERMINAL PARKING LOT FOR CONSTRUCTION ACCESS OF ANY KIND UNLESS OTHERWISE NOTED IN THE PLANS. THE CONTRACTOR SHALL BE REQUIRED TO STAGE CONSTRUCTION AROUND THE USE OF THESE AREAS AT ALL TIMES. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR'S VEHICLES OR EQUIPMENT BE ALLOWED TO USE THESE AREAS.
11. CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE LIGHTED, MARKED, AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25 FEET.
12. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT PUBLIC AND PRIVATE PROPERTY INCLUDING UTILITIES AND STRUCTURES TO REMAIN. IF AT ANY TIME, THE CONTRACTOR DAMAGES OR DESTROYS ANY OTHER PUBLIC OR PRIVATE PROPERTY, THE CONTRACTOR SHALL, AT HIS OWN EXPENSE, RESTORE SUCH PROPERTY TO A CONDITION EQUAL TO THAT OF EXISTING BEFORE SUCH DAMAGE. PUBLIC ROADWAYS, PRIVATE ENTRANCES, AND OTHER FACILITIES SHALL REMAIN ACCESSIBLE AND FREE OF CONSTRUCTION DEBRIS. ANY MATERIAL TO BE STAGED OR STOCKPILED SHALL BE COORDINATED WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER.
13. IF EXISTING ROADWAY SIGNS INTERFERE WITH THE INSTALLATION OF THE WORK FOR THIS PROJECT, SUCH SIGNS AND POSTS SHALL BE REMOVED AND REPLACED AFTER CONSTRUCTION. THE COST OF REMOVING AND STORING EXISTING SIGNS WILL NOT BE PAID FOR DIRECTLY. THE COST SHALL BE INCLUDED IN OTHER PAY ITEMS.
14. ACCESS TO ALL ENTRANCES SHALL BE MAINTAINED AT ALL TIMES UNLESS NOTED OTHERWISE ON THE PLANS.
15. STOCKPILE LOCATION SHALL BE PLACED IN THE LOCATION AS SHOWN ON THE PLANS UNLESS OTHERWISE DIRECTED BY THE AIRPORT. EXCESS EARTH MATERIAL FROM THE PROJECT SITE SHALL BE TESTED TO ENSURE THAT IT IS CLEAN PRIOR TO PLACING IN THE STOCKPILE LOCATION. THE EXCESS EARTHWORK SHALL BE PLACED IN ACCORDANCE WITH ITEM 152 OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS PER THIS PROJECT.

**PE099**

**REVISIONS**

NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
 AIRPORT SITE PLAN**



DESIGN BY:	JMW/CET
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	03 OF 39 SHEETS

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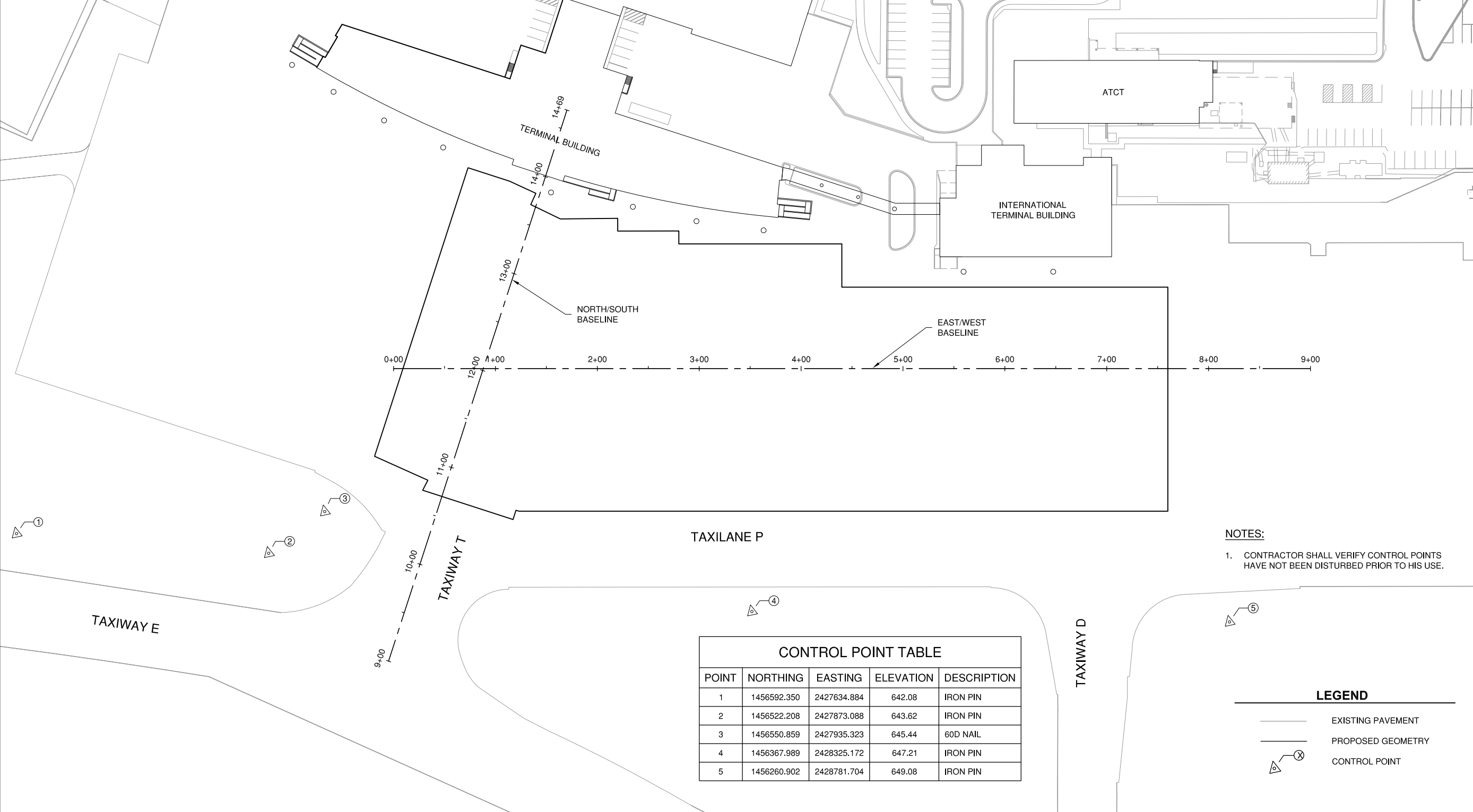
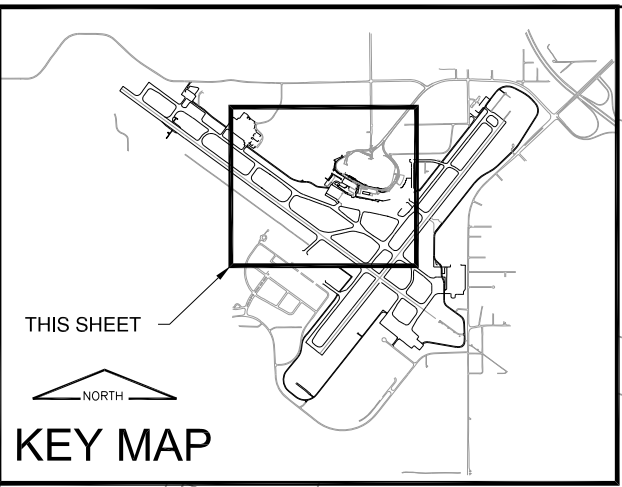
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 UPDATE BY: Jeremy Wasilewski  
 PLOT DATE: 7/1/2015 1:49 PM

KEYMAP  
 PIA\_BASE\_PROP\_GEO  
 PIA\_BASE\_EXISTING

**PE099**

REVISIONS		
NUMBER	BY	DATE

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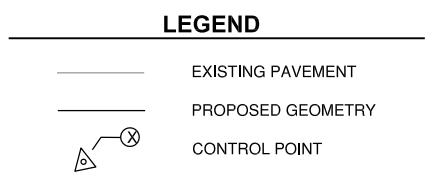


**NOTES:**

1. CONTRACTOR SHALL VERIFY CONTROL POINTS HAVE NOT BEEN DISTURBED PRIOR TO HIS USE.

**CONTROL POINT TABLE**

POINT	NORTHING	EASTING	ELEVATION	DESCRIPTION
1	1456592.350	2427634.884	642.08	IRON PIN
2	1456522.208	2427873.088	643.62	IRON PIN
3	1456550.859	2427935.323	645.44	60D NAIL
4	1456367.989	2428325.172	647.21	IRON PIN
5	1456260.902	2428781.704	649.08	IRON PIN



**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1**

**CONTROL POINTS**



DESIGN BY: JMW/CET

DRAWN BY: CMT

CHECKED BY: CBG

APPROVED BY: CBG

DATE: JULY 31, 2015

JOB No: 14061-03

IL PROJ. NO. PIA-4485  
 AIP PROJ. NO. 3-17-0080-XX

SHEET 04 OF 39 SHEETS

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**GENERAL**

1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F (OR LATEST VERSION), AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F (OR LATEST VERSION). NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP IS SPECIALIZED TO AIRPORT OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEANS AND METHODS THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

**1. COORDINATION**

1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

**2. PHASING**

1. TOTAL CONTRACT TIME SHALL BE 120 CALENDAR DAYS IF ONLY THE BASE BID IS AWARDED. HOWEVER, IF ALL THREE ADDITIVE ALTERNATES ARE AWARDED IN CONJUNCTION WITH THE BASE BID, THE CONTRACT TIME SHALL BE 150 CALENDAR DAYS. EACH ADDITIVE ALTERNATE SHALL BE A TOTAL OF 10 ADDITIONAL CALENDAR DAYS.
2. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

**3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY**

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

**4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)**

1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

**5. CONTRACTOR ACCESS**

1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN.
3. CERTAIN CONTRACTOR EMPLOYEES SHALL OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
4. ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATIONS AREA (AOA) SHALL ALSO ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
5. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA, THE TRUCK DRIVERS MUST BE ESCORTED BY THE CONTRACTOR.
6. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN DRIVING/WORKING IN MOVEMENT AREAS OF THE AIRFIELD. THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS.
7. FLAGMEN TO BE USED DURING THE CONSTRUCTION OF THE PROJECT IN THE LOCATIONS CALLED OUT IN THE PLAN SET SHALL BE FIELD ENDORSED.
8. THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN.
9. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
10. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
11. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
12. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
13. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
14. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
15. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
16. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF NORMAL EMERGENCY ACCESS TO THE AIRPORT. CHANGES SHALL BE MADE TO MAINTAIN EMERGENCY ACCESS TO THE AIRPORT AT ALL TIMES WITH THE APPROVAL OF THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY.

**6. WILDLIFE MANAGEMENT**

1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL ALWAYS BE MONITORED WITH A GATE GUARD WHEN IN USE.
3. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED AND LOCKED WHEN THE CONTRACTOR IS NOT WORKING.
4. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

**7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT**

1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

**8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT**

1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

**9. NOTIFICATION OF CONSTRUCTION ACTIVITIES**

1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
5. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

<b>DIRECTOR OF OPERATIONS</b> DOUG PALMER	309-303-0994
<b>AIRPORT MAINTENANCE</b> GREG HUSER	309-303-1005
<b>AIRPORT OPERATIONS</b> OPS CELL	309-3031001
<b>ENGINEER</b> CHUCK TAYLOR, P.E. - PROJECT ENGINEER CMT - RESIDENT ENGINEER	217-787-8050 217-787-8050

**10. INSPECTION REQUIREMENTS**

1. THE CONTRACTOR SHALL INSPECT THE JOB SITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F (OR LATEST VERSION) MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL REQUEST AN OPERATIONAL INSPECTION WITH THE AIRPORT OPERATIONS THROUGH THE RESIDENT ENGINEER OF EACH PHASE WORK AREA PRIOR TO THE AREA BEING REOPENED FOR AIRCRAFT USE. AIRPORT OPERATIONS SHALL MAKE THE FINAL DETERMINATION OF THE ACCEPTANCE TO REOPEN THIS WORK AREA. THIS WILL NOT BE CONSIDERED AS FINAL OR PARTIAL ACCEPTANCE OF THE WORK IN THIS PHASE.
3. THE CONTRACTOR SHALL SUBMIT TO THE RESIDENT ENGINEER AN OFFICIAL 'SUBSTANTIALLY COMPLETE' LETTER WHEN HE FEELS THE PROJECT IS COMPLETE AND AT THE POINT OF HAVING A FINAL ACCEPTANCE. UPON THE RESIDENT ENGINEERS RECOMMENDATION TO IDA, A FINAL ACCEPTANCE OF THE PROJECT WILL THEN BE SCHEDULED.

**11. UNDERGROUND UTILITIES**

1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

**12. PENALTIES**

1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

**13. SPECIAL CONDITIONS**

1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

**14. RUNWAY AND TAXIWAY VISUAL AIDS**

1. NO RUNWAY CLOSURES WILL BE REQUIRED FOR THIS PROJECT. HOWEVER, A TAXIWAY CLOSURE IS REQUIRED FOR THIS PROJECT. WHEN THE TAXIWAY CLOSURE IS COORDINATED AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2F (OR LATEST VERSION).
2. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

**15. HAZARD MARKING AND LIGHTING**

1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F (OR LATEST VERSION) AND 150/5210-5C (OR LATEST VERSION) AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

**16. PROTECTION**

1. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 129.5' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED.
2. ALL WORK REQUIRED ON AN ACTIVE TAXILANE OR INSIDE AN ACTIVE TAXILANE OBJECT FREE AREA, WHICH EXTENDS 112.5' FROM THE TAXILANE CENTERLINE, WILL REQUIRE THE TAXILANE TO BE CLOSED.
3. TAXIWAY/TAXILANE OBJECT FREE AREA DISTANCES MAY BE LESS THAN STATED IN NOTES 1 AND 2 FOR CERTAIN PHASES AND NOTED IN THIS PLAN SET. THOSE OBJECT FREE AREA DISTANCES HAVE BEEN DETERMINED BASED ON SPECIFIC AIRCRAFT THAT TAXI IN THOSE PHASE LOCATIONS.
4. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

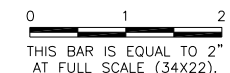
**17. OTHER LIMITATIONS ON CONSTRUCTION**

1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

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UPDATE BY: Jeremey Wasilewski  
PLOT DATE: 7/1/2015 1:34 PM

**PE099**

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NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL AIRPORT  
PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
CONSTRUCTION SAFETY PHASING PLAN  
NOTES**

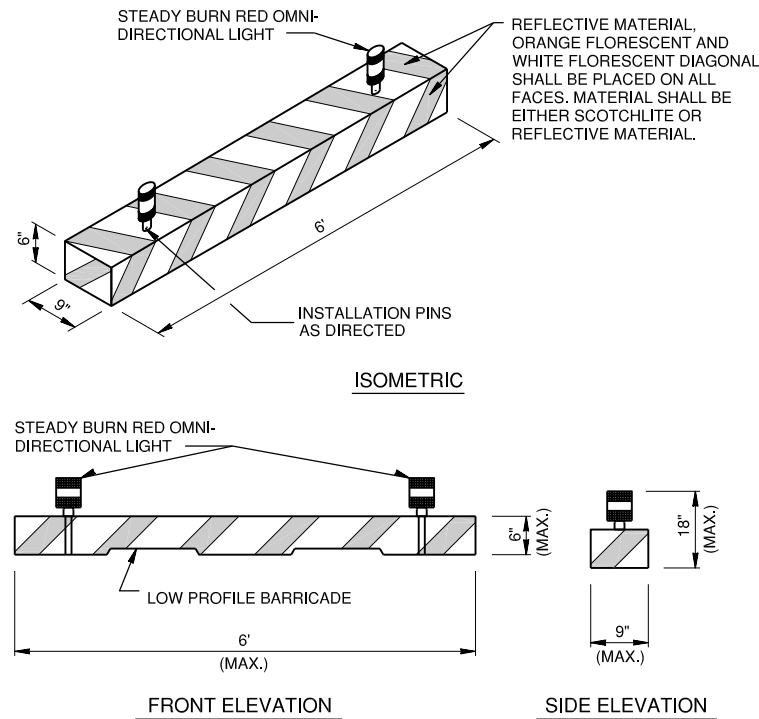
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**GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL AIRPORT**

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APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO. PIA-4485 AIP PROJ. NO. 3-17-0080-XX	
SHEET 05 OF 39 SHEETS	

**BEAM BARRICADE NOTES**

1. BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
2. BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
3. PLACE BARRICADES END TO END AROUND CONSTRUCTION PHASE LIMITS UNLESS NOTED IN THE PLANS TO ALLOW ACCESS FOR ARFF VEHICLES AND GROUND CREW OPERATIONS.
4. BARRICADES CLOSING TAXIWAYS AND CONSTRUCTION ACCESS ROUTES SHALL BE PLACED AT 10 FOOT INTERVALS.
5. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.



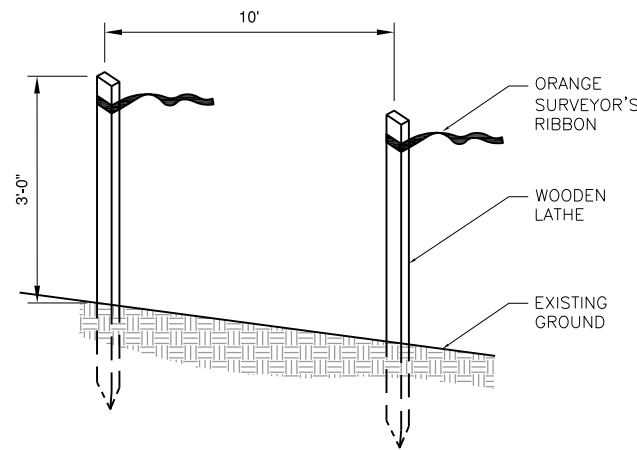
**LOW PROFILE - BEAM BARRICADE DETAILS**  
N.T.S.

**SECURITY NOTES**

1. MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
2. AT NO TIME ARE UNSUPERVISED GATE OPENINGS PERMITTED AT THE AIRPORT. IF THE PROPOSED WORK REQUIRES GATE OPENING, THE CONTRACTOR SHALL PROVIDE A GUARD TO PREVENT UNAUTHORIZED ACCESS. THE CONTRACTOR SHALL SECURE THE GATE AT THE COMPLETION OF THE WORK DAY.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES DURING THE COURSE OF THE WORK.
4. FINES CAN BE LEVIED AGAINST THE CONTRACTOR BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) FOR NEGLIGENCE IF THE AIRPORT SECURITY IS COMPROMISED AND THE AIRPORT PERIMETER FENCE LINE IS NOT MAINTAINED AS SPECIFIED ABOVE. FINES CAN ALSO BE LEVIED AGAINST THE CONTRACTOR FOR FAILURE TO COOPERATE WITH THE AIRPORT MANAGEMENT AS REQUIRED TO MAINTAIN AIRPORT SECURITY.

**MOVE AIRLINE WORKSTATION NOTES:**

1. PHASE 1, 3, 4, AND 5 WILL REQUIRE MOVING OF AIRLINE COMPUTER WORKSTATIONS IN ORDER TO FACILITATE THE WORK LIMITS NEEDED TO CONSTRUCT EACH PHASE OF THIS PROJECT.
2. IN THE PREVIOUS CONSTRUCTION ACTIVITY PLAN SHEETS, IT HAS BEEN OUTLINED WHICH AIRLINES ARE TO MOVE FROM WHICH GATES TO THE THE TEMPORARY GATE LOCATIONS.
3. THE FOLLOWING STEPS WILL BE INVOLVED IN THE MOVING OF AN AIRLINE:
  - 3.1. WHEN AIRCRAFT ARRIVES FROM LAST FLIGHT IT SHALL BE PARKED AT NEW TEMPORARY GATE LOCATION PRIOR TO STARTING WORK IN NEW PHASE WORK AREA PER THAT PHASE NOTES (SEE CONSTRUCTION ACTIVITY PLAN SHEET 'X').
  - 3.2. PRIOR TO ANY DISCONNECTIONS, THE RESIDENT ENGINEER SHALL BE PRESENT AND DOCUMENT WITH PICTURES AND SKETCHES OF ALL CONNECTIONS AT THE WORKSTATION LOCATIONS, INTERMEDIATE DISTRIBUTION FRAME (IDF) ROOM(S) AND MAIN DISTRIBUTION FRAME (MDF) ROOM (LAN ROOM LOCATIONS ARE CALLED OUT ON SHEET 14, 15, 16, 17, AND 18).
  - 3.3. DISCONNECT WORKSTATION (COMPUTER, KEYBOARD, MOUSE, MONITOR, SCANNER, PRINTER) FROM TERMINAL NETWORK AT EXISTING GATE.
  - 3.4. MOVE WORKSTATION TO NEW TEMPORARY GATE LOCATION.
  - 3.5. CONNECT WORKSTATION TO TERMINAL NETWORK AT THE PODIUM.
  - 3.6. MAKE NECESSARY CHANGES TO THE PATCH PANELS AND SWITCHES IN THE IDF ROOM(S) AND MDF ROOM FOR THE FOLLOWING SYSTEMS: INTERNET/AIRLINE NETWORK CONNECTION, PAGING SYSTEM, DOOR ACCESS CONTROL, AND MUFIDS.
  - 3.7. TEST SYSTEM WITH AN AIRLINE REPRESENTATIVE ONCE THE MOVING OF THE WORKSTATION AND THE CONNECTIONS IN THE IDF AND MDF ROOMS HAVE BEEN FINISHED TO ENSURE PROPER OPERATION PRIOR TO USING THE SYSTEM IN THE MORNING FOR THE FIRST DEPARTING FLIGHT AT THE NEW TEMPORARY GATE LOCATION.
4. IF THE SYSTEM DOES NOT WORK AFTER BEING MOVED THEN CONSTRUCTION WORK ON THE APRON SHALL NOT INITIATE UNTIL THE AIRLINE WORKSTATION ISSUE HAS BEEN RESOLVED.



**CONSTRUCTION SETBACK LINE DETAIL**  
N.T.S.

**CONSTRUCTION SETBACK LINE NOTES**

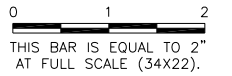
1. CONSTRUCTION SETBACK LINE SHALL BE INSTALLED WHERE SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.
2. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

**CONSTRUCTION ACTIVITY PLAN GENERAL NOTES**

1. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
2. THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
3. IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT ARE THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
4. WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT.
5. DELAYS, CHANGES IN SCHEDULING OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. VEHICLE ACCESS ON PERIMETER ROADS SHALL BE MAINTAINED THROUGHOUT THE PROJECT.
7. THE CONTRACTOR SHALL BE AWARE THAT DAILY OPERATION OF THE AIRPORT WILL CONTINUE THROUGHOUT THIS PROJECT. AS A RESULT, DAILY COORDINATION WILL BE NECESSARY TO LIMIT DISRUPTION TO AIRPORT/TENTANT AND CONTRACTOR OPERATIONS.
8. CONTRACTOR'S STAGING, STORAGE, AND PARKING WILL BE AS SHOWN ON THE AIRPORT SITE PLAN.
9. THE CONTRACTOR SHALL PLACE ALL BARRICADES, CONSTRUCTION SETBACK LINES, AND EROSION CONTROL ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
10. CONSTRUCTION PHASING IS OF CRITICAL IMPORTANCE TO THE AIRPORT FOR THIS PROJECT. LIQUIDATED DAMAGES MAY BE SOUGHT ON A PHASE BY PHASE BASIS.
11. CONSTRUCTION RELATED ITEMS REQUIRING THE CLOSURE OF RUNWAYS AND/OR TAXIWAYS SHALL REQUIRE A MINIMUM OF 72 HOURS NOTICE TO COORDINATION WITH THE AIRPORT AND FAA. NO EXTENSION TO CONTRACT TIME WILL BE GIVEN FOR DELAYS CAUSED BY LACK OF ADEQUATE COORDINATION.
12. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
  - A. THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
  - B. THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE AIRPORT OPERATIONS BY THE CONTRACTOR.
  - C. THE CONTRACTOR SHALL USE AN EXISTING GATE(S), OR NEW GATES AS CALLED OUT IN THE PLANS, FOR ACCESS TO THE AIRFIELD.
  - D. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
  - E. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
  - F. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED AND LOCKED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
  - G. THE CONTRACTOR SHALL CLOSE AND SECURE THE ACCESS GATE(S) UPON LEAVING THE SITE.
  - H. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
  - I. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
  - J. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE USED BY HIS VEHICLES AND CONSTRUCTION EQUIPMENT ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE OR CONSTRUCTION EQUIPMENT.
  - K. THE CONTRACTOR SHALL EVALUATE AND PRESERVE THE CONDITION OF THE EXISTING AIRPORT PERIMETER ROAD AND ALL OTHER ACCESS ROUTES UTILIZED BY THE CONTRACTOR FOR ACCESS TO THE SITE. THE CONTRACTOR SHALL DOCUMENT THE CONDITION OF THE EXISTING PAVEMENT IN THE PRESENCE OF THE AIRPORT AND RESIDENT ENGINEER PRIOR TO THE INITIATION OF CONSTRUCTION. AT THE COMPLETION OF CONSTRUCTION, ANY PAVEMENT USED BY THE CONTRACTOR THAT IS SAID TO BE DAMAGED BY HIS OPERATIONS SHALL BE REPAIRED TO ITS ORIGINAL CONDITION AND/OR TO THE SATISFACTION OF THE AIRPORT AT THE CONTRACTOR'S EXPENSE.

**PE099**

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NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**RECONSTRUCT TERMINAL APRON PHASE 1**  
**CONSTRUCTION ACTIVITY PLAN NOTES AND**  
**DETAILS**



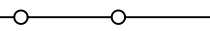



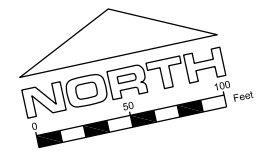
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APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	06 OF 39 SHEETS

**CONSTRUCTION ACCESS NOTES**

1. PLACEMENT OF BARRICADES AND TRAFFIC SIGNAGE NECESSARY FOR THE REDUCTION OF TRAFFIC FROM TWO LANES TO ONE LANE SHALL MEET IDOT STANDARD 701606 REQUIREMENTS FOR LONG TERM LANE CLOSURES. CONTRACTOR SHALL MAINTAIN MINIMUM 11 FOOT WIDE LANES AT ALL TIMES.
2. CONTRACTOR SHALL USE EXISTING TEMPORARY ACCESS ROAD IN PLACE. CONTRACTOR SHALL MAINTAIN THIS TEMPORARY ROAD THROUGHOUT THE DURATION OF THIS PROJECT. THIS TEMPORARY ROAD IS TO BE USED BY PUBLIC/NON-CONSTRUCTION TRAFFIC ONLY.
3. THE SIGNAGE SHOWN ON THIS SHEET SHALL BE IN ADDITION TO ALL TEMPORARY CONSTRUCTION TRAFFIC SIGNAGE REQUIRED BY IDOT STANDARDS FOR LANE CLOSURES WHICH ARE NOT SHOWN.
4. CONTRACTOR SHALL PLACE A TEMPORARY STOP SIGN DIRECTING ALL CONSTRUCTION TRAFFIC EXITING THE SITE TO STOP AND ALLOW ALL ONCOMING CONSTRUCTION TRAFFIC TO PROCEED PRIOR TO PROCEEDING THROUGH THE 2-WAY 1-LANE ZONE. THE CONTRACTOR SHALL USE EXTREME CAUTION IN THIS AREA AND SHALL REQUIRE COORDINATED COMMUNICATION WITH ALL INDIVIDUALS USING THIS ACCESS.
5. CONTRACTOR SHALL PROVIDE SIGNAGE TO TEMPORARILY CLOSE AIRPORT ACCESS FOR THE GENERAL PUBLIC FROM MAXWELL ROAD. MAXWELL ROAD SHALL BE CLOSED TO THRU TRAFFIC IN ACCORDANCE WITH IDOT STANDARD 701901.
6. MAINTENANCE OF TRAFFIC CONTROL ITEMS AND TEMPORARY ROAD MILLINGS SHALL BE MAINTAINED AT ALL TIMES THROUGHOUT THE PROJECT DURATION AND SHALL BE PAID FOR UNDER ITEM AR800258 CONSTRUCTION ACCESS.

**LEGEND**

-  TEMPORARY PUBLIC PARKING ACCESS
-  CONTRACTOR'S ACCESS
-  CONSTRUCTION BARRICADES PER IDOT STD 701901
-  TEMPORARY SIGNAGE



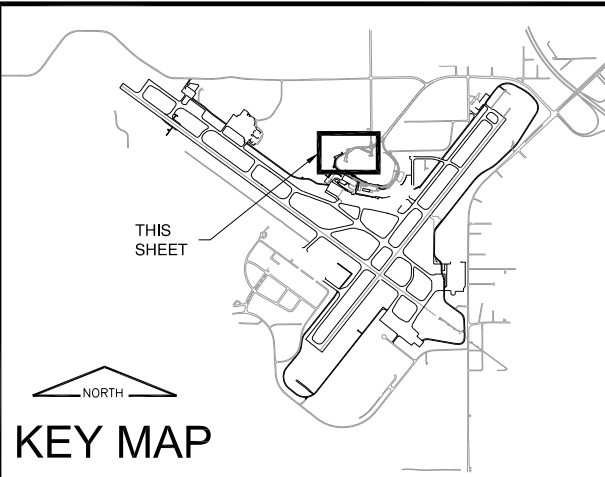
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 KEYMAP

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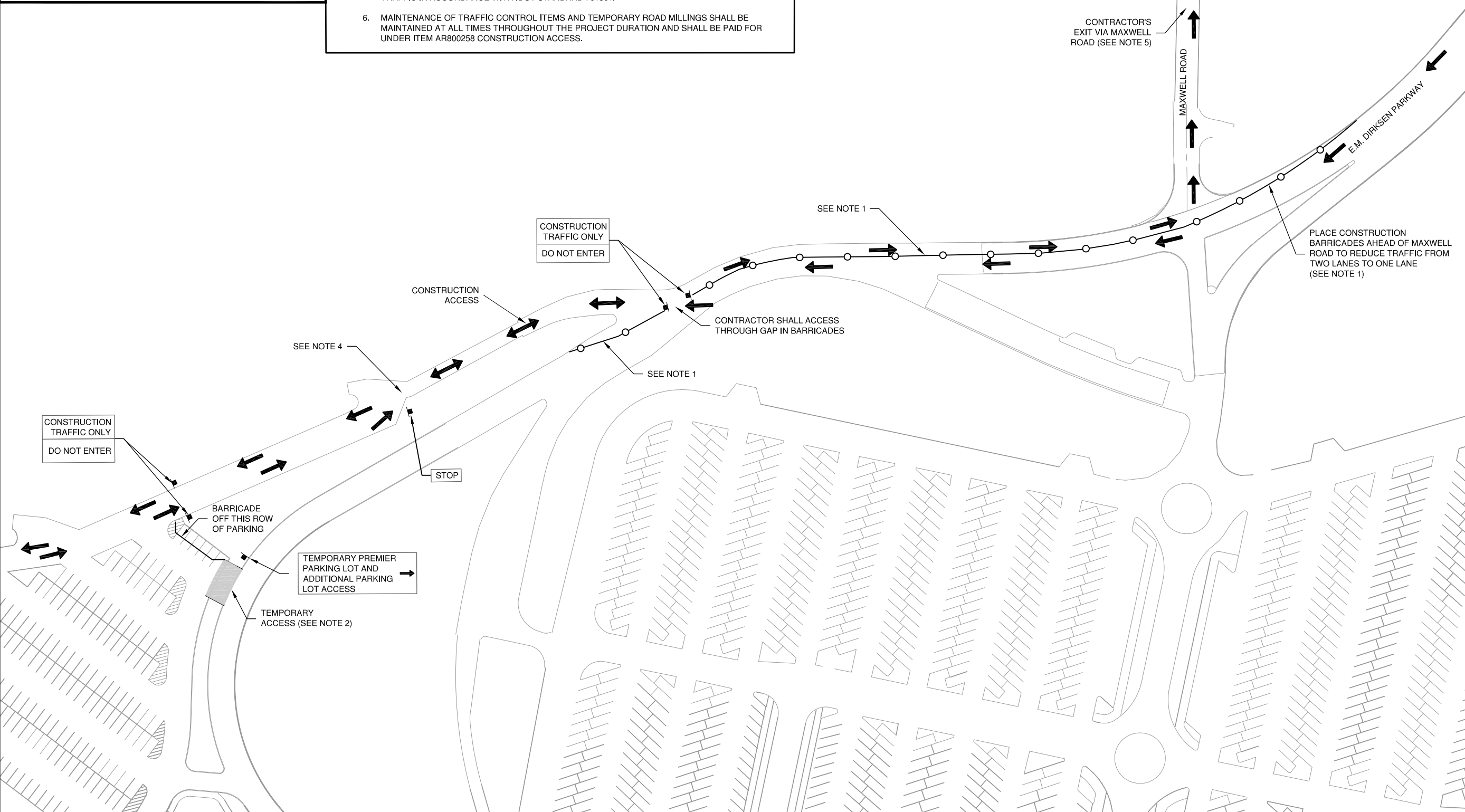
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**KEY MAP**



**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
 CONSTRUCTION ACCESS**



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JOB No:	14061-03
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AIP PROJ. NO.	3-17-0080-XX
SHEET	07 OF 39 SHEETS

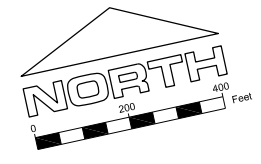
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**CRITICAL POINT TABLE**

POINT	LATITUDE	LONGITUDE	ELEVATION	HEIGHT ABOVE	TOP ELEVATION
1	N40° 40' 00.81"	W89° 41' 46.71"	631.75	25'	656.75
2	N40° 39' 58.81"	W89° 41' 48.78"	632.32	25'	657.32
3	N40° 39' 57.74"	W89° 41' 46.91"	635.13	25'	660.13
4	N40° 39' 56.80"	W89° 41' 39.00"	641.27	25'	666.27
5	N40° 39' 54.60"	W89° 41' 35.20"	644.72	25'	669.72
6	N40° 39' 53.66"	W89° 41' 33.16"	646.94	25'	671.94
7	N40° 39' 52.80"	W89° 41' 27.48"	649.91	25'	674.91
8	N40° 39' 52.24"	W89° 41' 24.44"	650.11	25'	675.11
9	N40° 39' 55.35"	W89° 41' 24.01"	653.00	25'	678.00
10	N40° 39' 56.39"	W89° 41' 28.22"	652.02	25'	677.02
11	N40° 39' 57.92"	W89° 41' 32.71"	650.40	25'	675.40
12	N40° 39' 55.76"	W89° 41' 35.60"	646.23	25'	671.23
13	N40° 39' 59.13"	W89° 41' 41.94"	640.65	25'	665.65
14	N40° 40' 00.47"	W89° 41' 40.63"	642.00	25'	667.00
15	N40° 40' 02.09"	W89° 41' 43.67"	641.16	25'	666.16
16	N40° 40' 00.87"	W89° 41' 44.95"	638.12	25'	663.12

**NOTES**

1. CONSTRUCTION ACCESS ROAD SHALL BE CONSTRUCTED OF MILLINGS OR SUITABLE EQUIVALENT MATERIAL AS SHOWN IN THE DETAIL ON TYPICAL SECTIONS 3.
2. THE PAVEMENT STRUCTURE SHALL BE CAPABLE OF HANDLING HEAVY TRUCK TRAFFIC FOR THE DURATION OF THE PROJECT WITHOUT TRACKING FOD ONTO THE APRON. SHOULD FOD ACCUMULATE ON THE PAVEMENT, THE CONTRACTOR SHALL BROOM OFF AND MAKE CORRECTIVE ACTION TO PREVENT IT FROM REOCCURRING. SEE TYPICAL SECTIONS 3, SHEET 26.
3. A MINIMUM OF 75 FEET OF THE CONSTRUCTION ACCESS ROAD ADJACENT TO THE THE APRON PAVEMENT EDGE SHALL BE PAVED BITUMINOUS SURFACE COURSE MATERIAL WITH A MINIMUM OF 4" IN THICKNESS.
4. STOP SIGN SHALL BE MOUNTED ON A FRANGIBLE BASE AND APPROVED FOR USE ON AIRFIELDS. MAXIMUM HEIGHT FROM BASE OF SIGN TO TOP OF SIGN SHALL BE 24". SIGN SHALL BE PLACED 45FT FROM EDGE OF APRON PAVEMENT ALONG CONSTRUCTION ACCESS ROAD.



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 UPDATE BY: Jeremy Wasilewski  
 PLOT DATE: 7/1/2015 1:54 PM  
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 PIA\_BASE\_EXISTING  
 PIA\_BASE\_UTILITY- From Lighting

**PE099**

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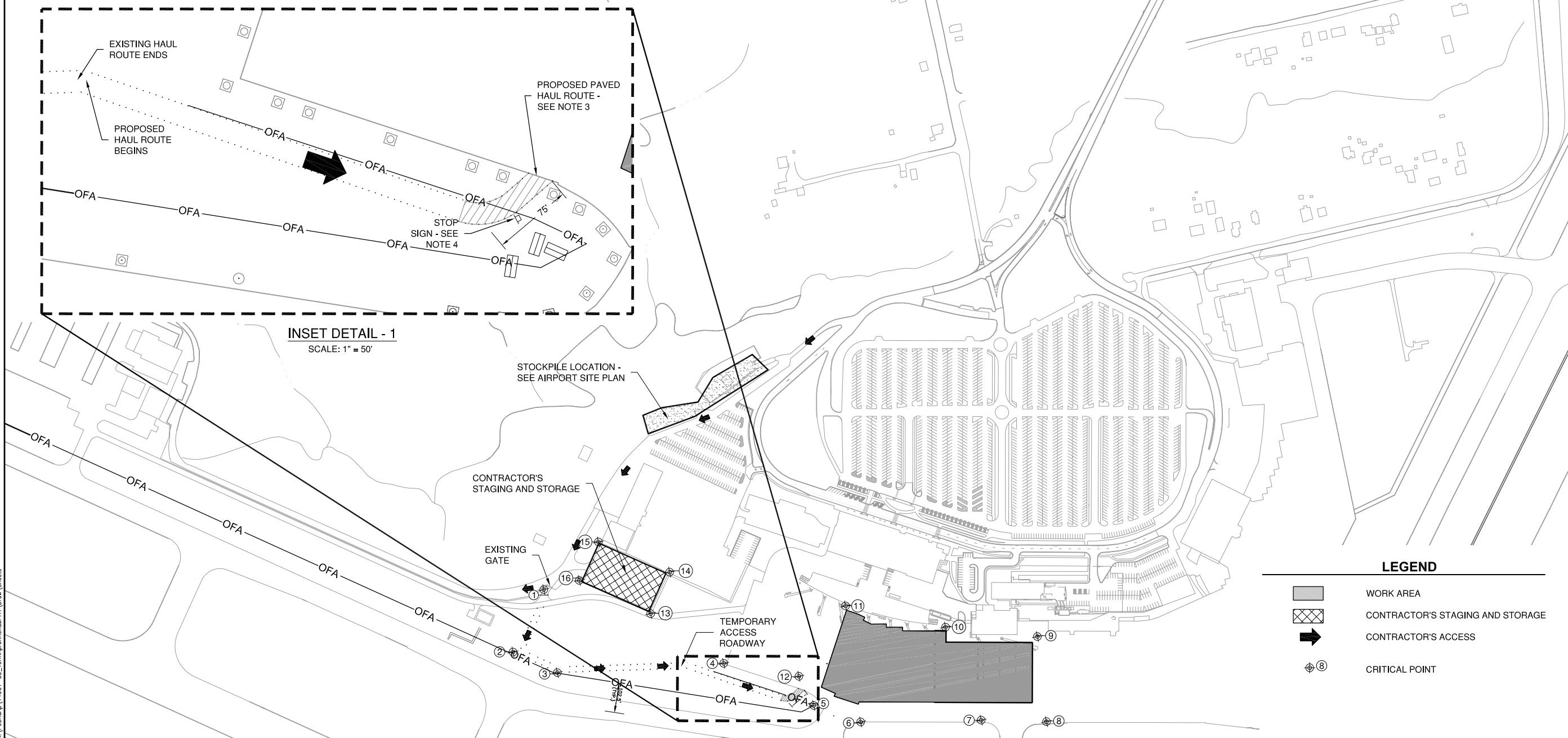
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**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
 CONSTRUCTION ACTIVITY PLAN OVERVIEW**



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 DRAWN BY: CMT  
 CHECKED BY: CBG  
 APPROVED BY: CBG  
 DATE: JULY 31, 2015  
 JOB No: 14061-03  
 IL. PROJ. NO. PIA-4485  
 AIP PROJ. NO. 3-17-0080-XX  
 SHEET 08 OF 39 SHEETS

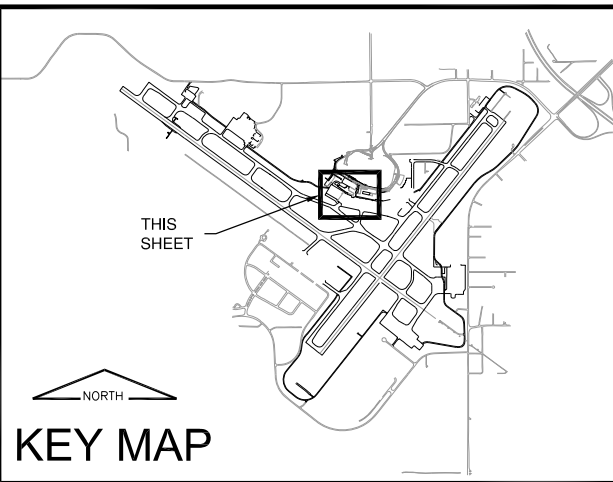


**LEGEND**

	WORK AREA
	CONTRACTOR'S STAGING AND STORAGE
	CONTRACTOR'S ACCESS
	CRITICAL POINT

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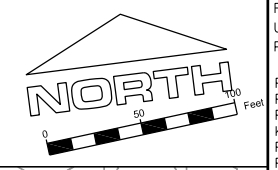




KEY MAP

**PHASE 1 NOTES**

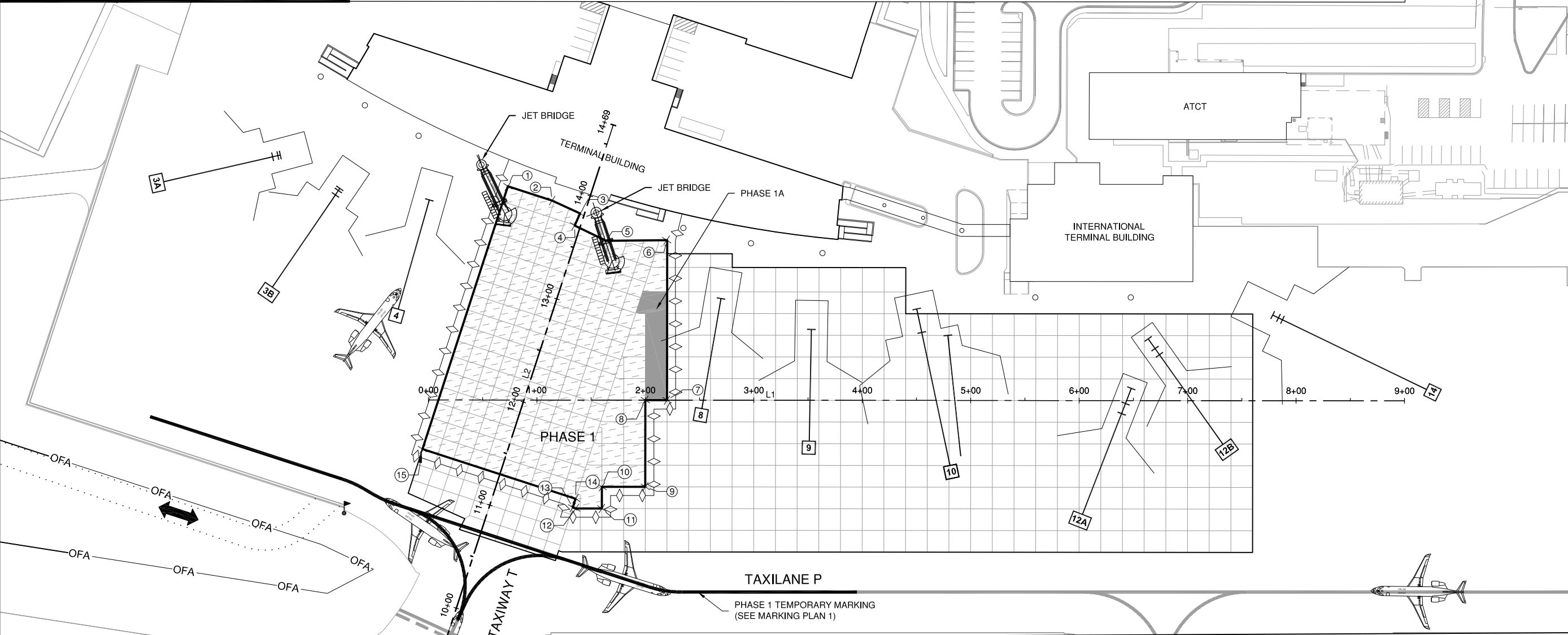
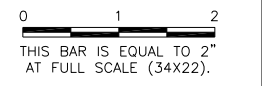
1. PRIOR TO STARTING ANY WORK, THE INTERNATIONAL TERMINAL BUILDING MUST BE COMPLETED AND GATES 12 AND 14 OPERABLE. (ESTIMATED COMPLETION MARCH 2016)
2. THE CONTRACTOR SHALL HAVE IN PLACE TRAFFIC CONTROL AND CONSTRUCTION ACCESS ROUTES TO THE SITE BEFORE STARTING WORK ON THE TERMINAL APRON.
3. THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR 7 DAYS PRIOR TO THE INITIATION OF PHASE 1.
4. PAVEMENT MARKING FOR GATES 12A AND 12B, AND PHASE 1 TEMPORARY CENTERLINE SHALL BE PAINTED; WITH THE REMOVAL OF A PORTION OF THE TAXILANE CENTERLINE. SEE MARKING PLAN 1 AND TERMINAL GATE MARKING PLAN 2 FOR ADDITIONAL DETAILS AND LAYOUT INFORMATION.
5. ON THE NIGHT BEFORE THE FIRST DAY OF CONSTRUCTION FOR PHASE 1 THE AIRCRAFT THAT PARK AT GATES 5 AND 6 (AMERICAN) SHALL PARK AT GATES 12A AND 14 FOR THE LAST ARRIVING FLIGHTS THAT NIGHT. CONTRACTOR SHALL COORDINATE WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER.
6. IN ADDITION ON THE NIGHT BEFORE THE FIRST DAY OF CONSTRUCTION FOR PHASE 1 THE COMPUTER WORKSTATIONS SHALL BE MOVED FOR GATES 5 AND 6 AND SHALL BE INSTALLED AT GATES 12A AND 12B. SEE CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS, SHEET 6 FOR DETAILED INFORMATION FOR MOVING WORKSTATIONS.
7. ALL BARRICADES SHALL BE SET UP PRIOR TO THE INITIATION OF THIS PHASE AND SHALL REMAIN IN PLACE UNTIL COMPLETE.
8. BARRICADES SHALL BE PLACED ON EDGE OF PAVEMENT TO BE REMOVED. WHEN CONSTRUCTION OPERATIONS ARE ALONG SIDE THE PHASE THE BARRICADES MAY BE MOVED TO ALLOW THE WORK TO CONTINUE BUT SHALL BE PUT BACK IN PLACE THERE AFTER.
9. JET BRIDGES FOR GATES 5 AND 6 SHALL BE MOVED, BY THE AIRPORT, TO THE LOCATIONS SHOWN ON THIS SHEET PRIOR TO DEMOLITION OF THE EXISTING PCC PAVEMENT.
10. AFTER PCC PAVEMENT ON THE SOUTHERN HALF OF THE PHASE HAS CURED PER SPECIFICATIONS, THE JET BRIDGES SHALL BE EXTENDED OUT AWAY FROM THE TERMINAL TO ALLOW A WORKABLE AREA TO RECONSTRUCT THE PCC PAVEMENT UNDER THE JET BRIDGES ADJACENT TO THE TERMINAL.
11. THE JET BRIDGES SHALL ALWAYS BE POSITIONED ON A SOLID PCC PAVEMENT SURFACE.
12. ALL WORK IN PHASE 1 TO BE COMPLETED AND PCC CURED PER SPECIFICATIONS PRIOR TO STARTING PHASE 1A. A CLEAR ROUTE SHALL BE OPEN AT ALL TIMES AROUND THE WEST SIDE OF THE AIRCRAFT AT GATE 8 FOR GROUND CREW OPERATIONS.
13. THIS PHASE SHALL BE COMPLETED IN 36 CALENDAR DAYS.



FILE: GC101.dwg  
 UPDATE BY: Jeremey Wasilewski  
 PLOT DATE: 7/1/2015 1:50 PM  
 PIA\_BASE\_PROP\_GEO  
 PIA\_BASE\_PHASING  
 PIA\_BASE\_MARKING  
 KEYMAP  
 PIA\_PROP\_JOINTS  
 PIA\_BASE\_EXISTING

**PE099**

REVISIONS		
NUMBER	BY	DATE

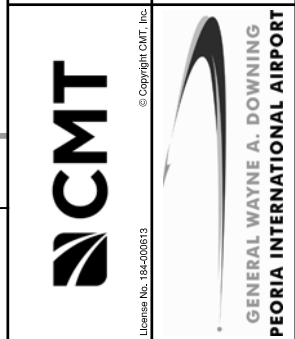
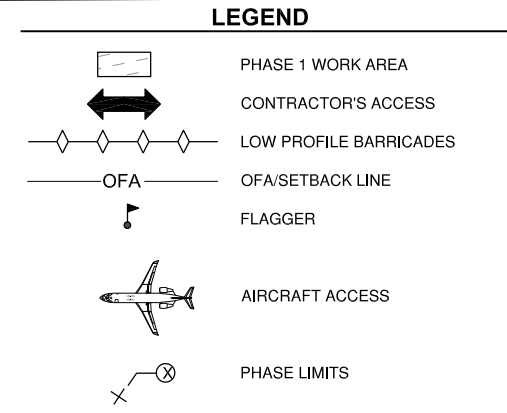


**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**RECONSTRUCT TERMINAL APRON PHASE 1**  
**CONSTRUCTION ACTIVITY PLAN 1**

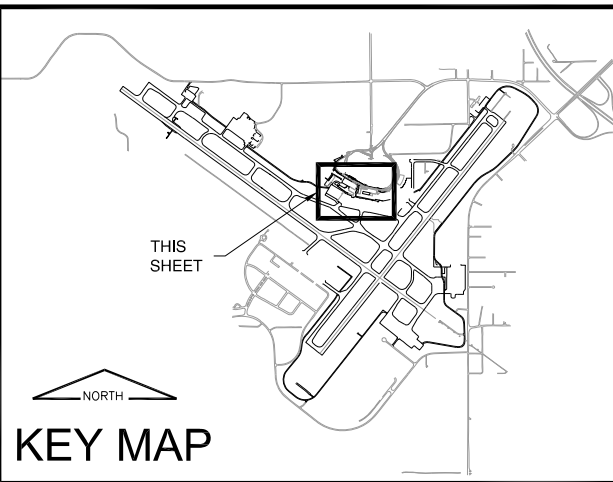
K:\Peoria\14061-03\_TermApronRebPH1\Drawn\Sheets

POINT	NORTHING	EASTING
1	1456852.055	2428142.830
2	1456830.682	2428180.025
3	1456813.750	2428202.845
4	1456803.896	2428195.585
5	1456784.125	2428221.144
6	1456773.156	2428275.828
7	1456628.641	2428245.758
8	1456632.783	2428226.192

POINT	NORTHING	EASTING
9	1456554.517	2428209.623
10	1456562.802	2428170.490
11	1456543.235	2428166.348
12	1456548.213	2428142.834
13	1456549.708	2428140.233
14	1456557.027	2428144.439
15	1456630.959	2428015.781

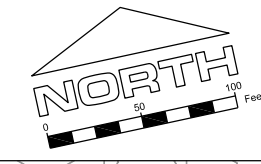


DESIGN BY: JMW  
 DRAWN BY: CMT  
 CHECKED BY: CBG  
 APPROVED BY: CBG  
 DATE: JULY 31, 2015  
 JOB No: 14061-03  
 IL PROJ. NO. PIA-4485  
 AIP PROJ. NO. 3-17-0080-XX  
 SHEET 09 OF 39 SHEETS



**PHASE 2 NOTES**

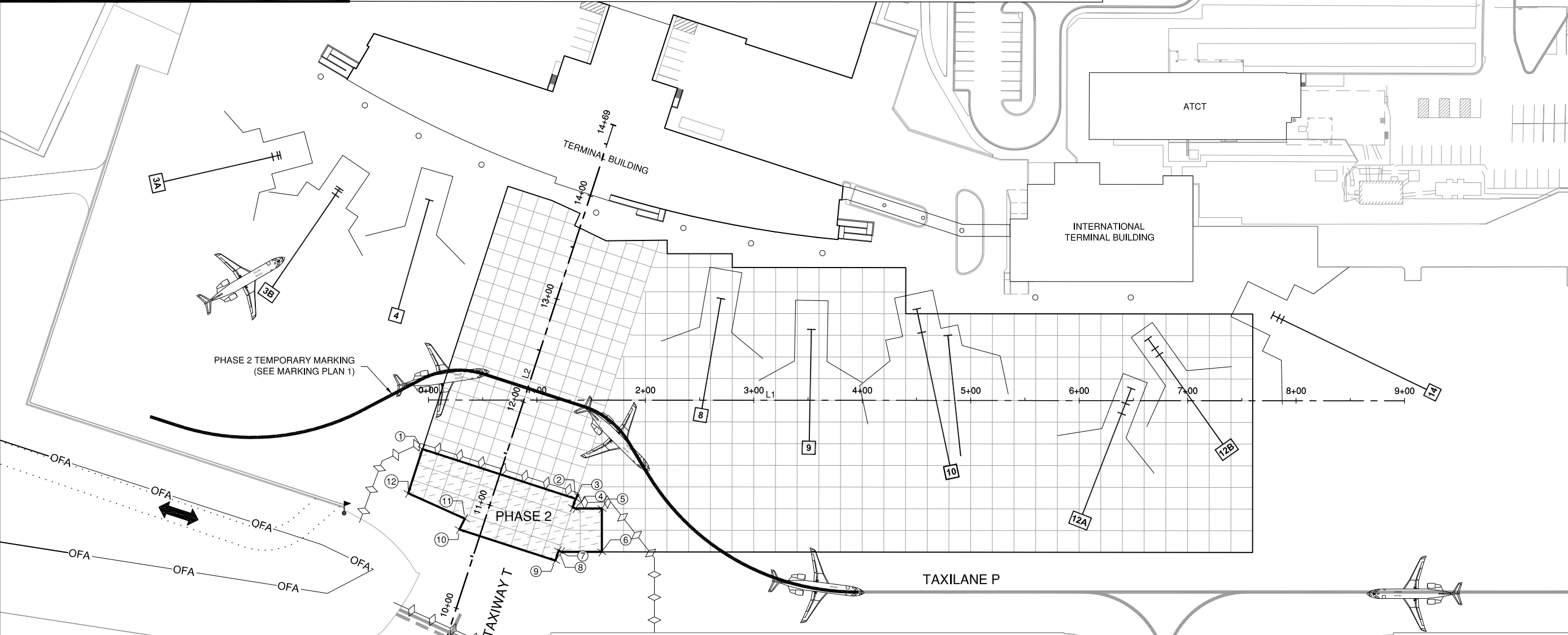
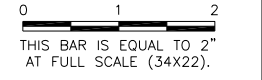
1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR 7 DAYS PRIOR TO THE INITIATION OF PHASE 2.
2. PRIOR TO THE START OF PHASE 2; THE PHASE 1 TEMPORARY MARKING SHALL BE REMOVED AND THE PHASE 2 TEMPORARY MARKING CENTERLINE SHALL BE PAINTED. SEE MARKING PLAN 1 FOR DETAILS.
3. ALL BARRICADES SHALL BE SET UP PRIOR TO THE INITIATION OF THIS PHASE AND SHALL REMAIN IN PLACE UNTIL COMPLETE.
4. BARRICADES SHALL BE PLACED ON EDGE OF PAVEMENT TO BE REMOVED. WHEN CONSTRUCTION OPERATIONS ARE ALONG SIDE THE PHASE THE BARRICADES MAY BE MOVED TO ALLOW THE WORK TO CONTINUE BUT SHALL BE PUT BACK IN PLACE THERE AFTER.
5. THIS PHASE REQUIRES THE CLOSURE OF TAXIWAY T.
6. JET BRIDGES FOR GATES 5 AND 6 SHALL BE MOVED, BY THE AIRPORT, TO THE POSITIONS AS SHOWN ON CONSTRUCTION ACTIVITY PLAN 1 SHEET.
7. THE PHASE 2 TEMPORARY CENTERLINE SHALL BE REMOVED AT THE COMPLETION OF THIS PHASE.
8. THE PAVEMENT MARKING FOR GATES 5 AND 6 SHALL BE PAINTED AS SHOWN ON TERMINAL GATE MARKING PLAN 1.
9. THIS PHASE TO BE COMPLETED IN 19 CALENDAR DAYS.



FILE: GC102.dwg  
 UPDATE BY: Jeremy Wasilewski  
 PLOT DATE: 7/1/2015 1:56 PM  
 PIA\_BASE\_PROP\_GEO  
 PIA\_BASE\_PHASING  
 PIA\_BASE\_MARKING  
 KEYMAP  
 PIA\_PROP\_JOINTS  
 PIA\_BASE\_EXISTING

**PE099**

REVISIONS		
NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

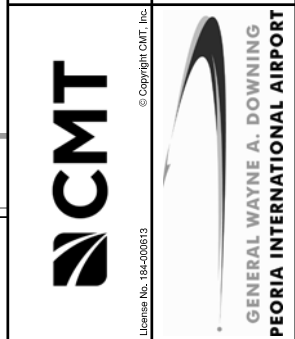
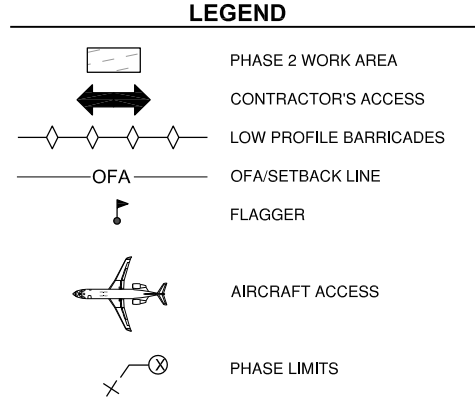
**RECONSTRUCT TERMINAL APRON PHASE 1  
 CONSTRUCTION ACTIVITY PLAN 2**

**PHASE LIMITS POINT TABLE**

POINT	NORTHING	EASTING
1	1456630.959	2428015.781
2	1456557.027	2428144.439
3	1456549.708	2428140.233
4	1456548.213	2428142.834
5	1456543.235	2428166.348
6	1456504.103	2428158.064
7	1456511.761	2428121.887
8	1456513.256	2428119.286

**PHASE LIMITS POINT TABLE**

POINT	NORTHING	EASTING
9	1456505.005	2428114.545
10	1456551.448	2428033.722
11	1456560.038	2428040.777
12	1456593.791	2427994.541



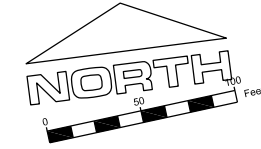
DESIGN BY: JMW  
 DRAWN BY: CMT  
 CHECKED BY: CBG  
 APPROVED BY: CBG  
 DATE: JULY 31, 2015  
 JOB No: 14061-03  
 IL PROJ. NO. PIA-4485  
 AIP PROJ. NO. 3-17-0080-XX  
 SHEET 10 OF 39 SHEETS

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**PE099**

REVISIONS		
NUMBER	BY	DATE

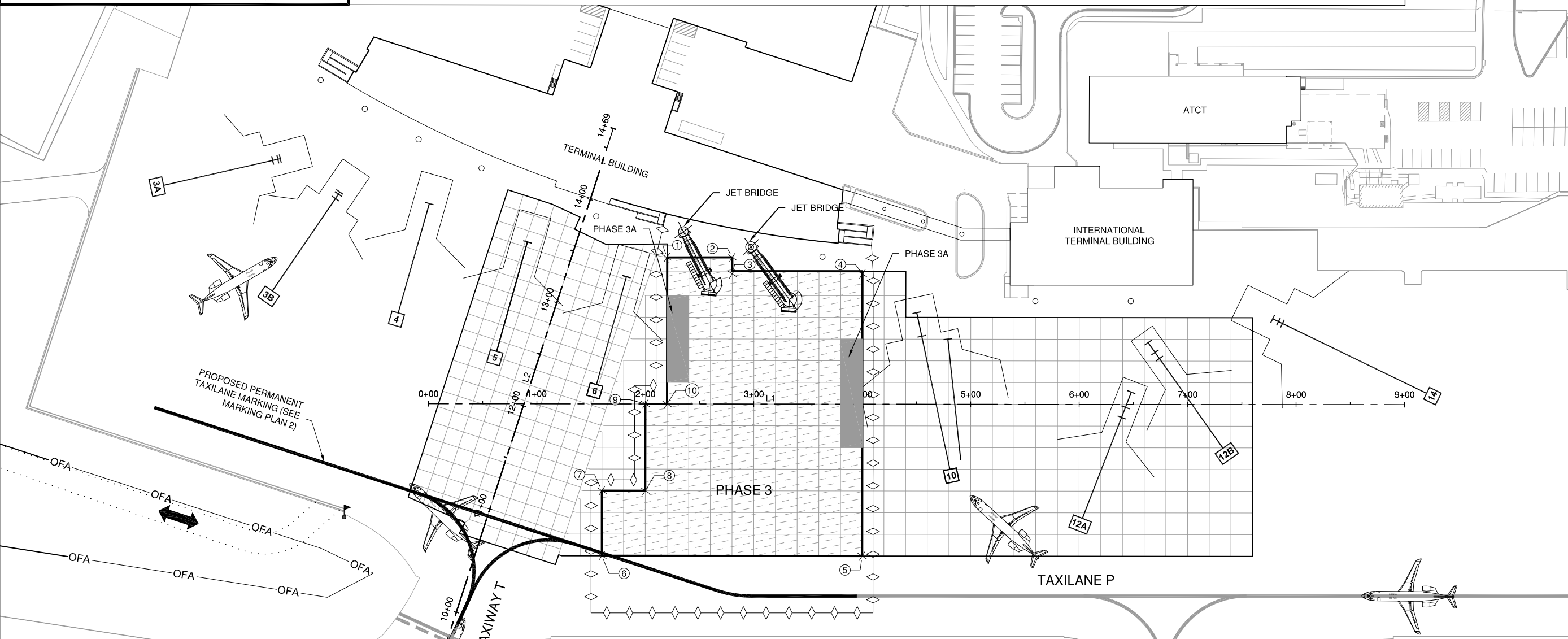
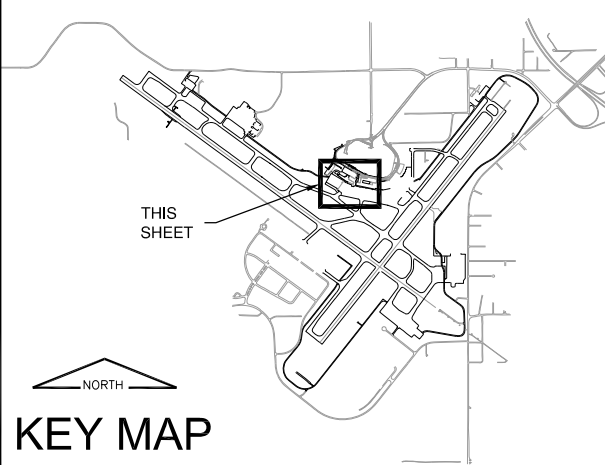
0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).



**PHASE 3 NOTES**

1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR 7 DAYS PRIOR TO THE INITIATION OF PHASE 3.
2. PRIOR TO THE START OF PHASE 3; THE PROPOSED PERMANENT TAXILANE CENTER LINE SHALL BE PAINTED AS SHOWN ON MARKING PLAN 2. THE TAXILANE CENTER SHALL BE PAINTED ONLY TO THE LIMITS OF PHASE 3 WORK AREA.
3. ON THE NIGHT BEFORE THE FIRST DAY OF CONSTRUCTION FOR PHASE 3 THE AIRCRAFT FROM GATES 8 AND 9 (DELTA) SHALL BE PARKED AT GATES 5 AND 6. CONTRACTOR SHALL COORDINATE WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER.
4. THE COMPUTER WORKSTATIONS FROM GATES 8 AND 9 SHALL BE MOVED TO GATES 5 AND 6 ON THE NIGHT BEFORE THE FIRST DAY OF CONSTRUCTION FOR PHASE 3.
5. ALL BARRICADES SHALL BE SET UP PRIOR TO THE INITIATION OF THIS PHASE AND SHALL REMAIN IN PLACE UNTIL COMPLETE.
6. BARRICADES SHALL BE PLACED ON EDGE OF PAVEMENT TO BE REMOVED. WHEN CONSTRUCTION OPERATIONS ARE ALONG SIDE THE PHASE THE BARRICADES MAY BE MOVED TO ALLOW THE WORK TO CONTINUE BUT SHALL BE PUT BACK IN PLACE THERE AFTER.
7. TAXILANE P ALONG THE SOUTH EDGE OF PHASE 3 SHALL BE CLOSED.
8. TAXIWAY T AND TAXIWAY D ARE TO REMAIN OPEN.
9. JET BRIDGES FOR GATES 8 AND 9 SHALL BE MOVED, BY THE AIRPORT, TO THE LOCATIONS SHOWN ON THIS SHEET PRIOR TO DEMOLITION OF THE EXISTING PCC PAVEMENT.
10. AFTER PCC PAVEMENT ON THE SOUTHERN HALF OF THE PHASE HAS CURED PER SPECIFICATIONS, THE JET BRIDGES SHALL BE EXTENDED OUT AWAY FROM THE TERMINAL TO ALLOW A WORKABLE AREA TO RECONSTRUCT THE PCC PAVEMENT UNDER THE JET BRIDGES ADJACENT TO THE TERMINAL.
11. THE JET BRIDGES SHALL ALWAYS BE POSITIONED ON A SOLID PCC PAVEMENT SURFACE.
12. ALL WORK IN PHASE 3 TO BE COMPLETED AND PCC CURED PER SPECIFICATIONS PRIOR TO STARTING PHASE 3A. A CLEAR ROUTE SHALL BE OPEN AT ALL TIMES AROUND THE WEST SIDE OF THE AIRCRAFT AT GATE 10 FOR GROUND CREW OPERATIONS.
13. MINIMUM OF 15 FEET ACCESS SHALL REMAIN AT ALL TIMES ON THE EAST SIDE OF THE AIRCRAFT AT GATE 6 FOR GROUND CREW OPERATIONS.
14. PAVEMENT MARKING FOR GATES 8 AND 9 SHALL BE PAINTED AS SHOWN ON THE TERMINAL GATE MARKING PLAN 1 PRIOR TO OPENING PHASE 3.
15. THIS PHASE SHALL BE COMPLETED IN 30 CALENDAR DAYS.

**KEY MAP**

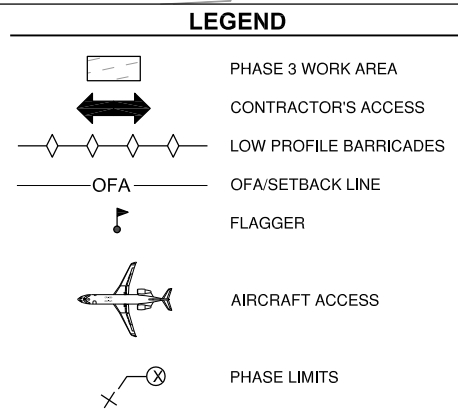


**PHASE LIMITS POINT TABLE**

POINT	NORTHING	EASTING
1	1456760.883	2428273.476
2	1456748.468	2428332.455
3	1456733.612	2428329.310
4	1456708.759	2428446.708
5	1456454.397	2428392.860
6	1456504.103	2428158.064
7	1456562.802	2428170.490
8	1456554.517	2428209.623

**PHASE LIMITS POINT TABLE**

POINT	NORTHING	EASTING
9	1456632.783	2428226.192
10	1456628.641	2428245.758



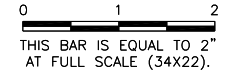
**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**RECONSTRUCT TERMINAL APRON PHASE 1**  
**CONSTRUCTION ACTIVITY PLAN 3**



DESIGN BY:	JMW
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	11 OF 39 SHEETS

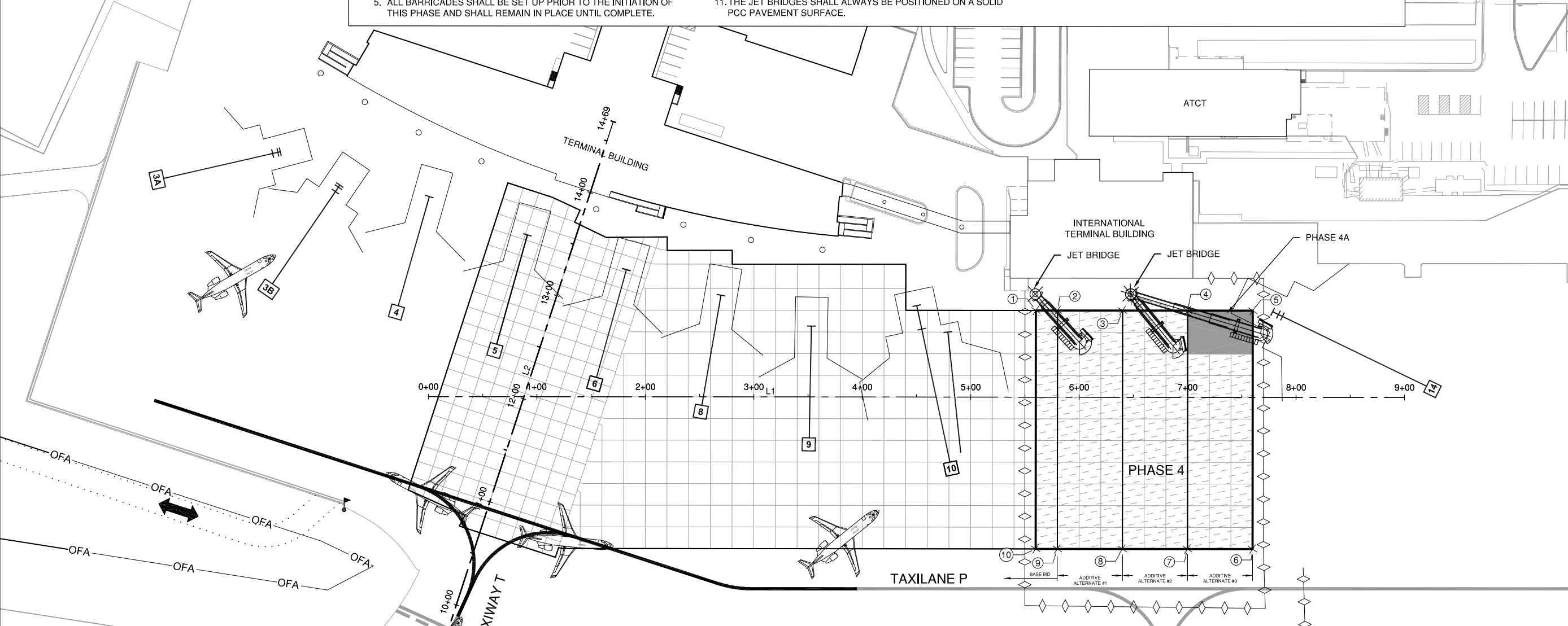
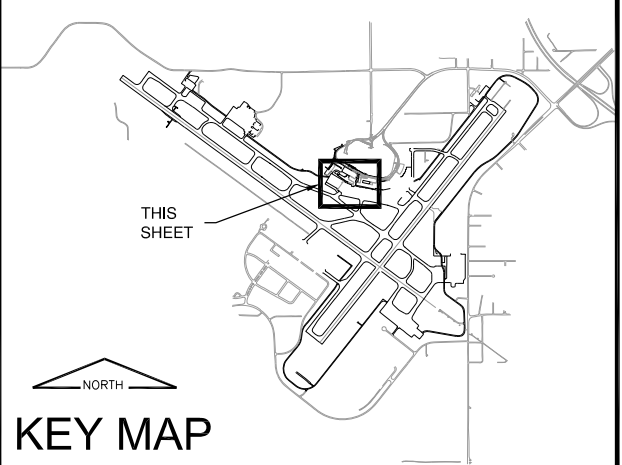
**PE099**

REVISIONS		
NUMBER	BY	DATE



**PHASE 4 NOTES**

1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR 7 DAYS PRIOR TO THE INITIATION OF PHASE 4.
2. IF ONLY THE BASE BID IS AWARDED FOR THIS PROJECT, THEN THE LIMITS OF PHASE 4 BASE BID SHALL BE ADDED TO PHASE 5. OTHERWISE THE LIMITS OF PHASE 4 SHALL BE AS SHOWN TO THE LIMITS OF THE APPROPRIATE NUMBER OF ADDITIVE ALTERNATES, IF ANY SHALL BE AWARDED.
3. ON THE NIGHT BEFORE THE FIRST DAY OF CONSTRUCTION FOR PHASE 4 THE AIRCRAFT FROM GATES 5 AND 6 (DELTA) SHALL BE PARKED AT GATES 8 AND 9 FOR THE LAST ARRIVING FLIGHT. IN ADDITION THE AIRCRAFT PARKED AT GATES 12A AND 12B (AMERICAN) SHALL BE PARKED AT GATES 5 AND 6 FOR THE LAST ARRIVING FLIGHT. CONTRACTOR SHALL COORDINATE WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER.
4. THE COMPUTER WORKSTATIONS SHALL BE MOVED FROM GATES 5 AND 6 (DELTA) TO GATES 8 AND 9; AND FROM GATES 12A AND 12B (AMERICAN) TO GATES 5 AND 6 DURING THE NIGHT BEFORE THE FIRST DAY OF CONSTRUCTION FOR PHASE 4.
5. ALL BARRICADES SHALL BE SET UP PRIOR TO THE INITIATION OF THIS PHASE AND SHALL REMAIN IN PLACE UNTIL COMPLETE.
6. BARRICADES SHALL BE PLACED ON EDGE OF PAVEMENT TO BE REMOVED. WHEN CONSTRUCTION OPERATIONS ARE ALONG SIDE THE PHASE THE BARRICADES MAY BE MOVED TO ALLOW THE WORK TO CONTINUE BUT SHALL BE PUT BACK IN PLACE THERE AFTER.
7. TAXIWAY D SHALL BE CLOSED FOR THIS PHASE.
8. TAXILANE P SHALL BE CLOSED ALONG THE SOUTH EDGE OF PHASE 4 FOR THIS PHASE.
9. JET BRIDGE FOR GATE 12 SHALL BE MOVED, BY THE AIRPORT, TO THE LOCATION AS SHOWN ON THIS SHEET PRIOR TO DEMOLITION OF THE EXISTING PCC PAVMENT. GATE 14 SHALL REMAIN IN OPERATION.
10. AFTER THE PCC PAVEMENT HAS CURED PER SPECIFICATIONS, GATE 12 JET BRIDGE SHALL BE EXTENDED OUT AWAY FROM THE TERMINAL TO ALLOW A WORKABLE AREA TO RECONSTRUCT THE PCC PAVEMENT UNDER THE JET BRIDGE ADJACENT TO THE TERMINAL.
11. THE JET BRIDGES SHALL ALWAYS BE POSITIONED ON A SOLID PCC PAVEMENT SURFACE.
12. AT ALL TIMES A MINIMUM 15 FOOT LANE SHALL REMAIN OPEN AND CLEAR ALONG THE WEST SIDE OF THE PHASE 4 LIMITS TO ALLOW ARFF VEHICLE AND BAGGAGE CART ACCESS.
13. ALL WORK IN PHASE 4 TO BE COMPLETED PRIOR TO PHASE 4A. PRIOR TO STARTING PHASE 4A, GATE 12 SHALL BE PAINTED AS SHOWN ON TERMINAL GATE MARKING PLAN 2 SHEET AND AIRCRAFT PARKING FOR GATE 14 SHALL BE MOVED TO GATE 12 DURING PHASE 4A.
14. PAVEMENT MARKING FOR GATE 14 SHALL BE PAINTED AT THE COMPLETION OF PHASE 4/A.
11. IF ONLY THE BASE BID IS AWARDED THEN 5 CALENDAR DAYS SHALL BE ADDED TO PHASE 5 AND PHASE 4 WILL BE ELIMINATED. IF ANY ONE OF THE ADDITIVE ALTERNATES ARE AWARDED THEN THERE SHALL BE A PHASE 4 THAT CONSIST OF 5 CALENDAR DAYS ADDED TO THE APPROPRIATE NUMBER OF ADDITIVE ALTERNATES AWARDED. EACH ADDITIVE ALTERNATE SHALL CONSIST OF 10 CALENDAR DAYS.



**PHASE LIMITS POINT TABLE**

POINT	NORTHING	EASTING
1	1456636.490	2428594.955
2	1456632.347	2428614.521
3	1456619.921	2428673.220
4	1456607.494	2428731.919
5	1456595.068	2428790.618
6	1456379.838	2428745.055
7	1456392.264	2428686.356
8	1456404.691	2428627.657

**PHASE LIMITS POINT TABLE**

POINT	NORTHING	EASTING
9	1456417.117	2428568.958
10	1456421.260	2428549.391

**LEGEND**

- PHASE 4 WORK AREA
- CONTRACTOR'S ACCESS
- LOW PROFILE BARRICADES
- OFA/SETBACK LINE
- FLAGGER
- AIRCRAFT ACCESS
- PHASE LIMITS

PLACE BARRICADES ON TAXIWAY D AT TAXIWAY E/D INTERSECTION

**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1**

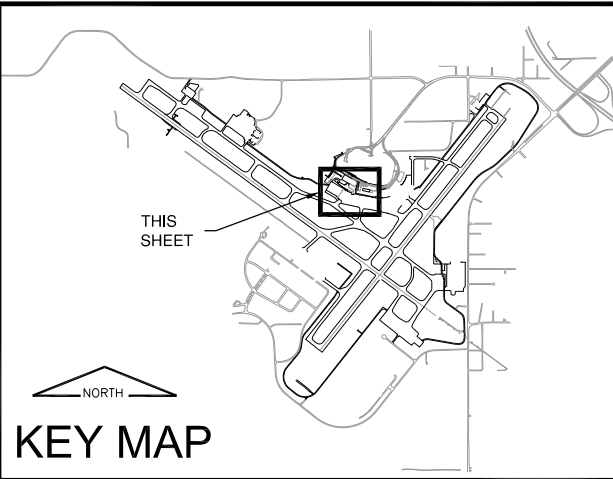
**CONSTRUCTION ACTIVITY PLAN 4**



DESIGN BY:	JMW/CET
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	12 OF 39 SHEETS

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**PHASE 5 NOTES**

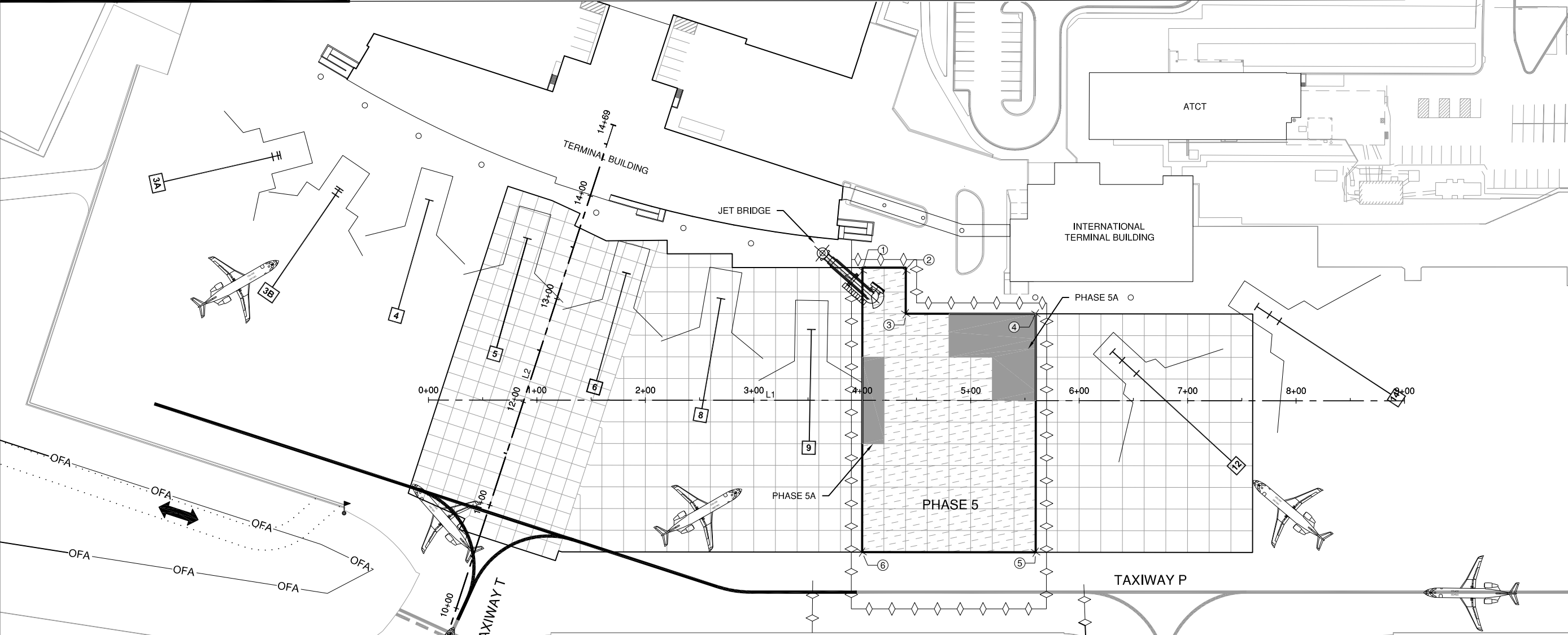
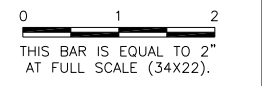
1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR 7 DAYS PRIOR TO THE INITIATION OF PHASE 5.
2. ON THE NIGHT BEFORE THE FIRST DAY OF CONSTRUCTION FOR PHASE 5 THE AIRCRAFT FROM GATE 10 (ALLEGIANT) SHALL BE PARKED AT GATE 12 FOR THE LAST ARRIVING FLIGHT. CONTRACTOR SHALL COORDINATE WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER.
3. THE COMPUTER WORKSTATIONS FROM GATE 10 SHALL BE MOVED TO GATE 12 DURING THE NIGHT BEFORE THE CONSTRUCTION OF PHASE 5.
4. ALL BARRICADES SHALL BE SET UP PRIOR TO THE INITIATION OF THIS PHASE AND SHALL REMAIN IN PLACE UNTIL COMPLETE.
5. BARRICADES SHALL BE PLACED ON EDGE OF PAVEMENT TO BE REMOVED. WHEN CONSTRUCTION OPERATIONS ARE ALONG SIDE THE PHASE THE BARRICADES MAY BE MOVED TO ALLOW THE WORK TO CONTINUE BUT SHALL BE PUT BACK IN PLACE THERE AFTER.
6. TAXILANE P ALONG THE SOUTH EDGE OF PHASE 5 SHALL BE CLOSED.
7. TAXIWAY T AND TAXIWAY D SHALL REMAIN OPEN.
8. JET BRIDGE FOR GATE 10 SHALL BE MOVED, BY THE AIRPORT, TO THE LOCATION AS SHOWN ON THIS SHEET PRIOR TO DEMOLITION OF THE EXISTING PCC PAVEMENT.
9. AFTER THE PCC PAVEMENT HAS CURED PER SPECIFICATIONS, GATE 10 JET BRIDGE SHALL BE EXTENDED OUT AWAY FROM THE TERMINAL TO ALLOW A WORKABLE AREA TO RECONSTRUCT THE PCC PAVEMENT UNDER THE JET BRIDGE ADJACENT TO THE TERMINAL.
10. THE JET BRIDGE SHALL ALWAYS BE POSITIONED ON A SOLID PCC PAVEMENT SURFACE.
11. A MINIMUM 15 FOOT LANE SHALL REMAIN CLEAR AT ALL TIMES AROUND THE NORTH SIDE OF PHASE 5 FOR ARFF VEHICLE ACCESS AND THE EAST SIDE AROUND GATE 9 FOR GROUND CREW OPERATIONS.
12. ALL WORK IN PHASE 5 TO BE COMPLETED PRIOR TO PHASE 5A. A CLEAR ROUTE FOR THE ARFF VEHICLE ACCESS UNDER THE CONNECTING BRIDGE AND GROUND CREW OPERATIONS SHALL BE MAINTAINED AT ALL TIMES DURING PHASE 5A.
13. PAVEMENT MARKING FOR GATE 10 SHALL BE PAINTED AS SHOWN ON THE TERMINAL GATE MARKING PLAN 1 SHEET AND MARKING FOR ARFF/BAGGAGE CART PATHWAY SHALL BE PAINTED AS SHOWN ON MARKING PLAN 2 PRIOR TO OPENING PHASE 5.
14. THIS PHASE SHALL BE COMPLETED IN 30 CALENDAR DAYS.



FILE: GC105.dwg  
 UPDATE BY: Jeremy Wasilewski  
 PLOT DATE: 7/1/2015 1:53 PM  
 PIA\_BASE\_PROP\_GEO  
 PIA\_BASE\_PHASING  
 PIA\_BASE\_MARKING  
 KEYMAP  
 PIA\_PROP\_JOINTS  
 PIA\_BASE\_EXISTING

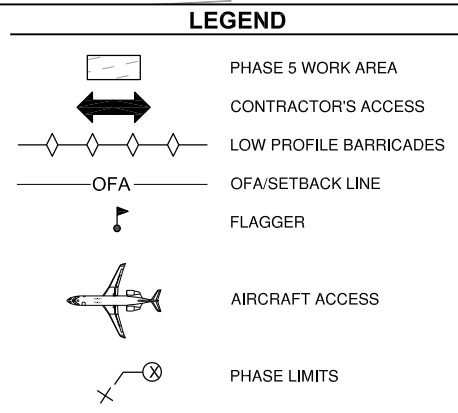
**PE099**

REVISIONS		
NUMBER	BY	DATE

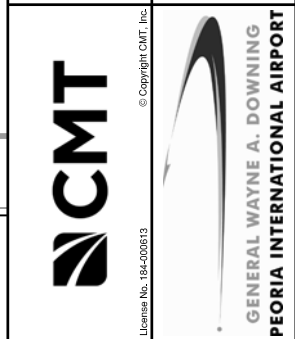


**PHASE LIMITS POINT TABLE**

POINT	NORTHING	EASTING
1	1456708.759	2428446.708
2	1456700.475	2428485.841
3	1456661.342	2428477.557
4	1456636.490	2428594.955
5	1456421.260	2428549.391
6	1456454.397	2428392.860



**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**RECONSTRUCT TERMINAL APRON PHASE 1**  
**CONSTRUCTION ACTIVITY PLAN 5**



DESIGN BY: JMW  
 DRAWN BY: CMT  
 CHECKED BY: CBG  
 APPROVED BY: CBG  
 DATE: JULY 31, 2015  
 JOB No: 14061-03  
 IL PROJ. NO. PIA-4485  
 AIP PROJ. NO. 3-17-0080-XX  
 SHEET 13 OF 39 SHEETS

K:\Peoria\14061-03\_TermApronRebPH1\Drawn\_Sheets

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FILE: GC501.dwg  
UPDATE BY: Jeremy Wasilewski  
PLOT DATE: 7/1/2015 1:37 PM  
PIA\_BASE\_TERMINAL TECH 1ST FLOOR

### PE099

REVISIONS		
NUMBER	BY	DATE

0 1 2  
THIS BAR IS EQUAL TO 2"  
AT FULL SCALE (34X22).

## S.S.I. SECURITY SENSITIVE INFORMATION

1. THIS SHEET CONTAINS SECURITY SENSITIVE INFORMATION OF THE TERMINAL BUILDING ROOM LAYOUT AND NETWORK/COMMUNICATION CONNECTIONS.
2. THESE LAYOUTS ARE TO BE ONLY A VISUAL AID TO THE NOTES ON SHEET 6 CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS, DURING THE PHASING FOR THE MOVING OF AIRLINE WORKSTATIONS.
3. NO INFORMATION ON THIS SHEET IS DIRECTLY RELATED TO A BID ITEM.
4. CONTACT JEREMY WASILEWSKI-CMT 309.680.1320 IF THE CONTRACTOR WOULD LIKE TO REVIEW PRIOR TO BID.
5. THIS SHEET WILL BE PROVIDED TO THE AWARDED CONTRACTOR AFTER THE PROJECT HAS BEEN BID AND THE CONTRACTOR HAS SIGNED A LETTER OF CONFIDENTIALITY.

GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL AIRPORT  
PEORIA, ILLINOIS

RECONSTRUCT TERMINAL APRON PHASE 1  
FIRST FLOOR MAIN TERMINAL EXISTING  
TECHNOLOGY LAYOUT



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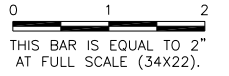


DESIGN BY:	JMW/CET
DRAWN BY:	ADD/EMH
CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL. PROJ. NO. PIA-4485 AIP PROJ. NO. 3-17-0080-XX	
SHEET	14 OF 39 SHEETS

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**PE099**

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NUMBER	BY	DATE



**S.S.I. SECURITY SENSITIVE INFORMATION**

1. THIS SHEET CONTAINS SECURITY SENSITIVE INFORMATION OF THE TERMINAL BUILDING ROOM LAYOUT AND NETWORK/COMMUNICATION CONNECTIONS.
2. THESE LAYOUTS ARE TO BE ONLY A VISUAL AID TO THE NOTES ON SHEET 6 CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS, DURING THE PHASING FOR THE MOVING OF AIRLINE WORKSTATIONS.
3. NO INFORMATION ON THIS SHEET IS DIRECTLY RELATED TO A BID ITEM.
4. CONTACT JEREMY WASILEWSKI-CMT 309.680.1320 IF THE CONTRACTOR WOULD LIKE TO REVIEW PRIOR TO BID.
5. THIS SHEET WILL BE PROVIDED TO THE AWARDED CONTRACTOR AFTER THE PROJECT HAS BEEN BID AND THE CONTRACTOR HAS SIGNED A LETTER OF CONFIDENTIALITY.

**GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL AIRPORT  
PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
FIRST FLOOR FIS BUILDING EXISTING  
TECHNOLOGY LAYOUT**



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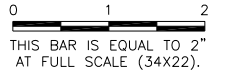


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PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
SECOND FLOOR FIS BUILDING EXISTING  
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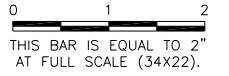
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PEORIA INTERNATIONAL AIRPORT  
PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
COMMUNICATION EQUIPMENT RACK  
ELEVATIONS-MAIN TERMINAL**



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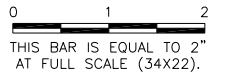


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PEORIA INTERNATIONAL AIRPORT  
PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
COMMUNICATION EQUIPMENT RACK  
ELEVATIONS-FIS BUILDING**



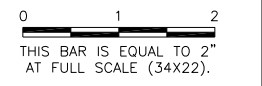
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SHEET	18 OF 39 SHEETS

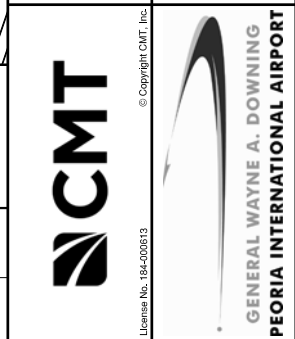
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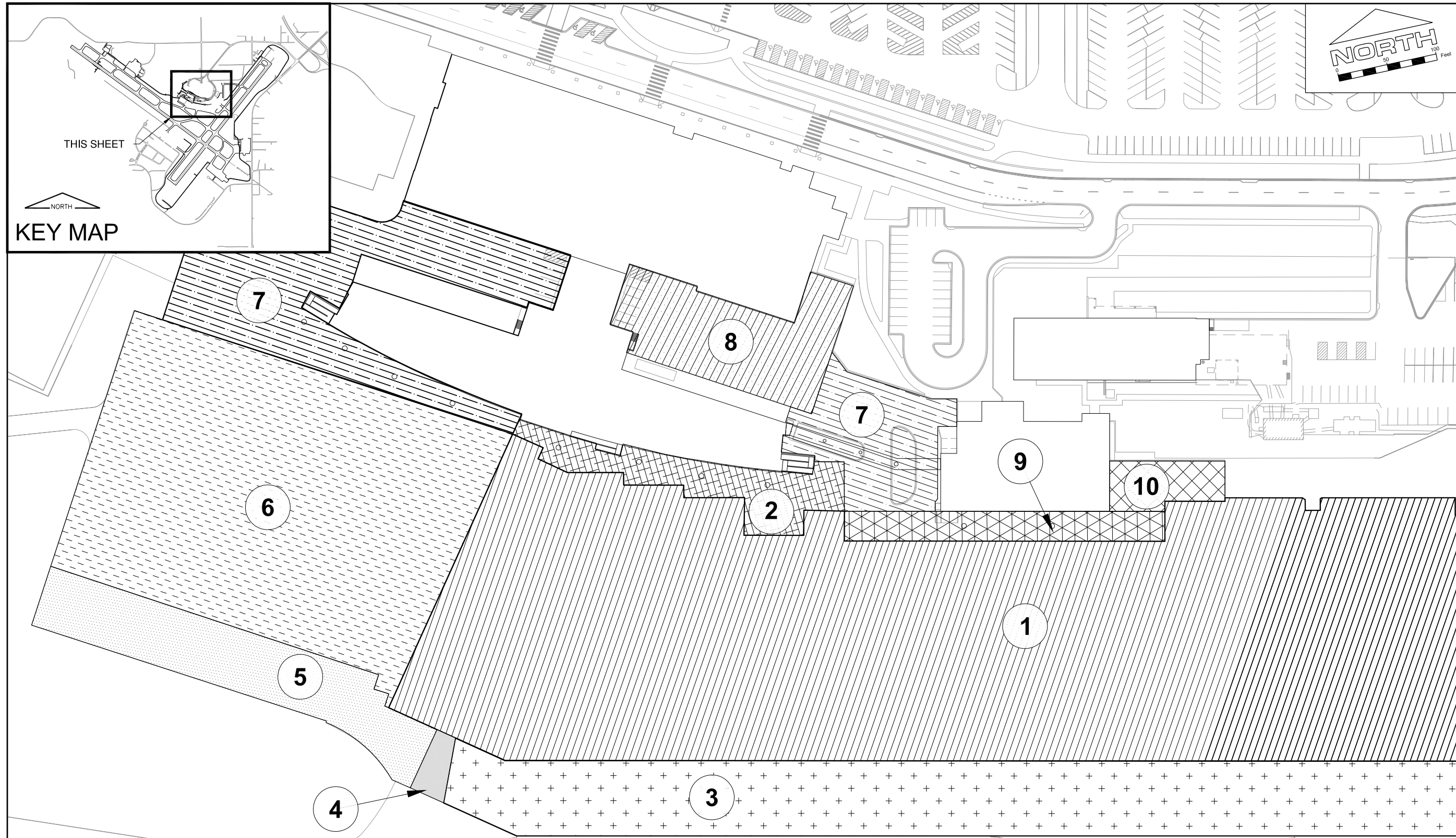


**GENERAL WAYNE A. DOWNING  
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 PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
 EXISTING PAVEMENT STRUCTURES**



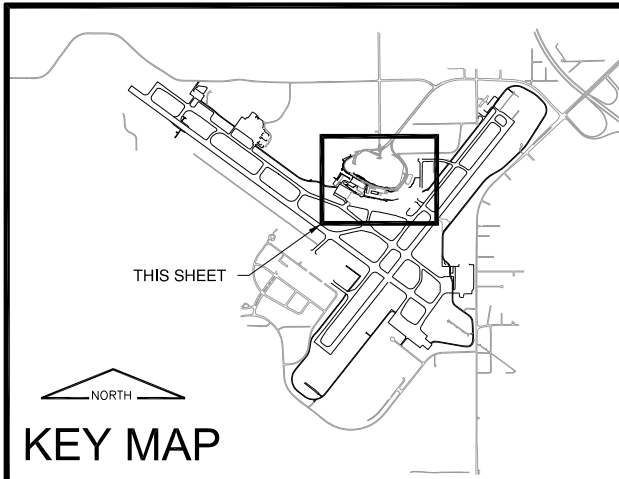
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DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	19 OF 39 SHEETS



**PAVEMENT STRUCTURE LEGEND**

	<b>1</b>	9" PCC PAVEMENT (501) 10" PCC PAVEMENT (501) 8" GRANULAR SUBBASE (154)		<b>4</b>	4.5" BITUMINOUS INLAY 3.5" BITUMINOUS SURFACE COURSE (401) 3" BITUMINOUS BASE COURSE (201)		<b>6</b>	14" PCC PAVEMENT (501) 6" ECONOCRETE SUBBASE (306) 8" CRUSHED AGGREGATE BASE COURSE (209) 12" LIME-MODIFIED SOIL		<b>9</b>	14" PCC PAVEMENT (501) 12" CRUSHED AGGREGATE BASE COURSE (209) 12" OVERSIZE AGGREGATE (208)
	<b>2</b>	10" PCC PAVEMENT (610) 6" CRUSHED AGGREGATE BASE COURSE (209)		<b>5</b>	18" PCC PAVEMENT (501) 8" CRUSHED AGGREGATE BASE COURSE (209) 12" OVERSIZE AGGREGATE (208)		<b>7</b>	8" PCC PAVEMENT (501) 6" CRUSHED AGGREGATE BASE COURSE (209)		<b>10</b>	9" PCC PAVEMENT (501) 12" CRUSHED AGGREGATE BASE COURSE (209)
	<b>3</b>	18" PCC PAVEMENT (501) 8" CRUSHED AGGREGATE BASE COURSE (209)					<b>8</b>	8" PCC PAVEMENT (610) 4" CRUSHED AGGREGATE BASE COURSE (209)			

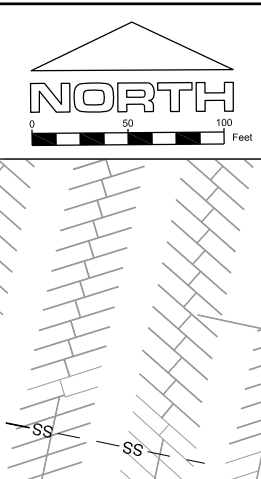
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**LEGEND**

	EXISTING PAVEMENT		EXISTING ELECTRICAL DUCT
	EXISTING BASE MOUNTED TAXIWAY LIGHT		EXISTING STORM SEWER
	EXISTING ELECTRICAL MANHOLE		EXISTING PAVEMENT JOINTS
	EXISTING TRANSFORMER		EXISTING LOCALIZER CABLE
	EXISTING JET BRIDGE		EXISTING FAA CABLE
	EXISTING INLET		EXISTING EDGE LIGHT CIRCUIT 1
	EXISTING TAXIWAY GUIDANCE SIGN		EXISTING EDGE LIGHT CIRCUIT 2
	EXISTING LIGHT POLE		EXISTING JOINT
	PAVEMENT REMOVAL LIMITS		EXISTING WATER LINE
			EXISTING UNDERDRAIN

- NOTES**
- DEMOLITION AND REMOVALS SHALL BE COMPLETED ONLY WITHIN THE LIMITS OF EACH PHASE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.
  - THE CONTRACTOR SHALL COORDINATE WITH AIRPORT OPERATIONS SEVEN DAYS PRIOR TO PAVEMENT REMOVAL.
  - RECORD DRAWINGS INDICATE AN EXISTING FAA DUCT BENEATH THE EXISTING PAVEMENT THAT RUNS FROM AN EXISTING ELECTRICAL MANHOLE TO THE NORTH. THE DEPTH OF THE DUCT SHALL BE VERIFIED AND PAVEMENT REMOVAL OPERATIONS SHALL TAKE NECESSARY PRECAUTIONS TO PROTECT THE DUCT DURING CONSTRUCTION. HAND REMOVAL OF MATERIAL OVER EXISTING FAA DUCT SHALL BE PERFORMED IN THE SHADED REGION AS SHOWN ON THIS SHEET. THE DUCT HOUSES CONTROL CABLES FOR THE FAA RADAR AND LOCALIZER CONTROL. DAMAGE TO THIS DUCT AND CABLES MAY RESTRICT THE AIRPORTS ABILITY TO LAND AIRCRAFT. ANY DAMAGE TO THESE ITEMS SHALL BE REPAIRED BY THE CONTRACTOR WITH ALL COSTS TO BE BORNE BY THE CONTRACTOR.
  - SEE CONSTRUCTION ACTIVITY PLAN SHEETS FOR MORE INFORMATION ON CONSTRUCTION PHASING AROUND JET BRIDGES.

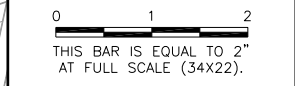


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 UPDATE BY: Jeremy Wasilewski  
 PLOT DATE: 7/1/2015 1:38 PM

PIA\_EXIST\_JOINTS  
 PIA\_BASE\_PROP\_GEO  
 KEYMAP  
 140610300-V-UTIL  
 PIA\_BASE\_UTILITY- From Lighting  
 PIA\_BASE\_EXISTING

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**811** Know what's below. Call before you dig.

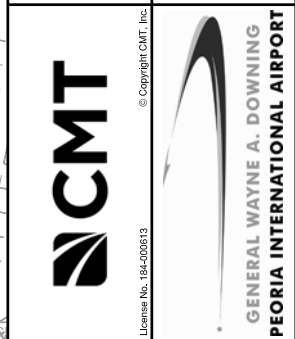
**COMMON GROUND ALLIANCE**  
 www.ca811.com or  
 Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH UTILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

**CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.**

**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
 EXISTING CONDITIONS AND REMOVALS**

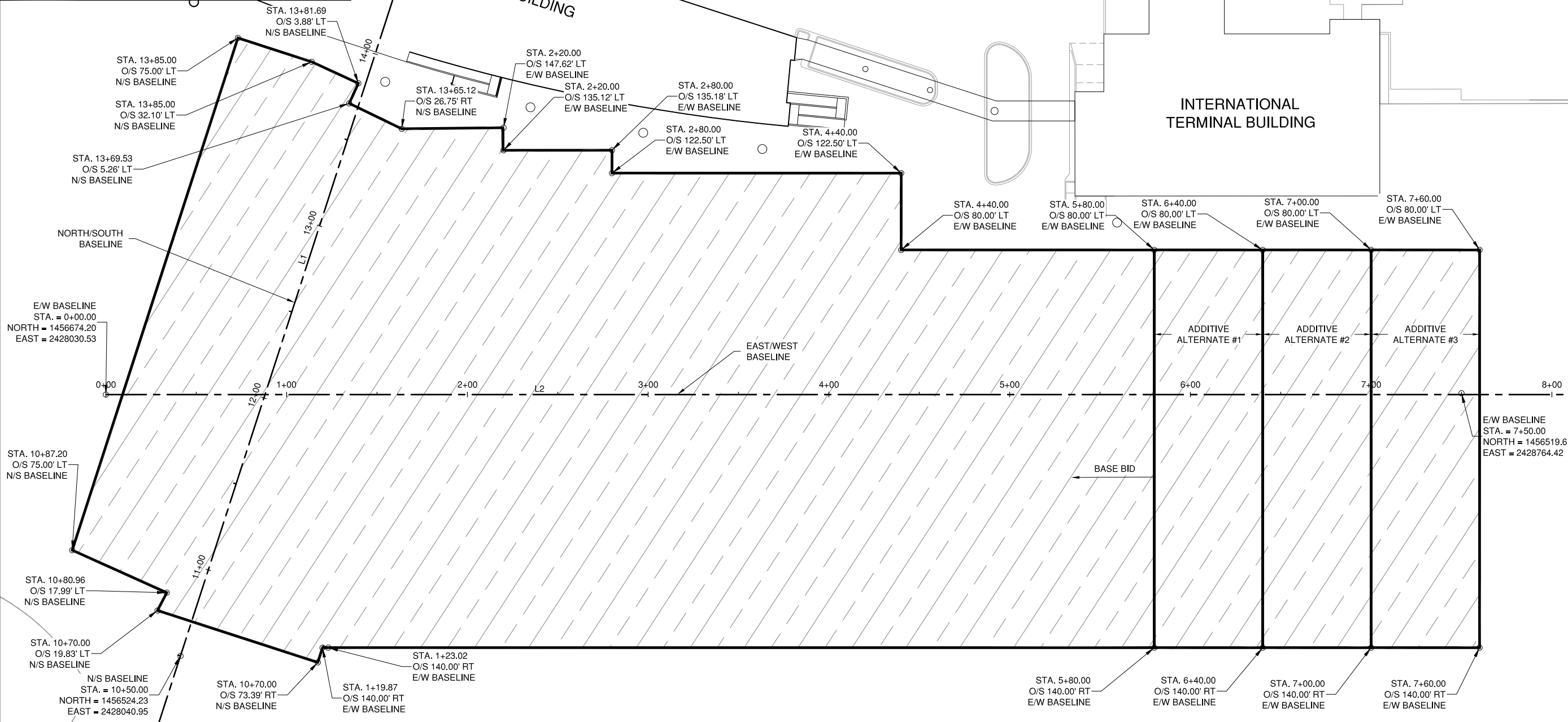
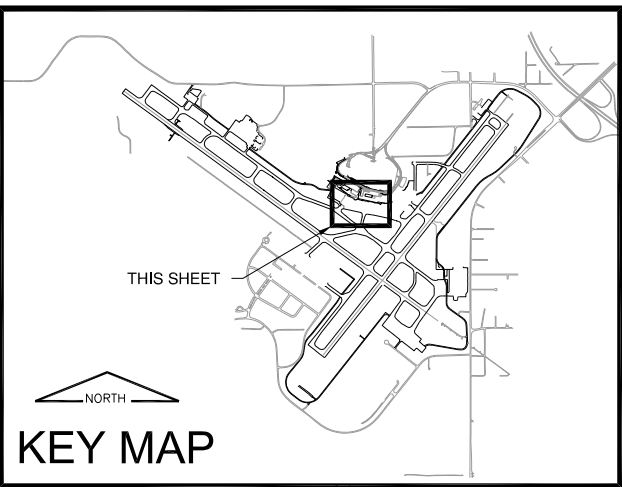
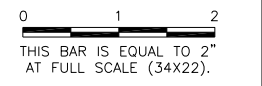


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DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	20 OF 39 SHEETS



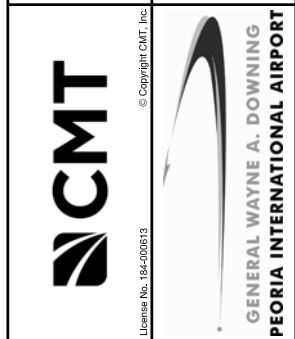
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**GENERAL WAYNE A. DOWNING  
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**RECONSTRUCT TERMINAL APRON PHASE 1  
 DEMOLITION PLAN**



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SHEET	21 OF 39 SHEETS

ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
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L1	N/S BASELINE	STA. 10+00.00	N:1456480.8760 E:2428016.0384	STA. 14+68.80	N: 1456887.3486 E: 2428249.6119

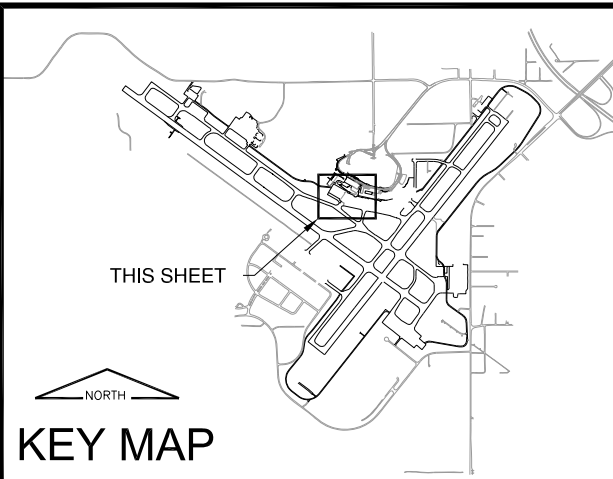
**NOTES:**

- ALL PROPOSED PAVEMENT REMOVAL ADJACENT TO EXISTING PAVEMENT TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH ALONG THE LIMITS OF THE REMOVAL PRIOR TO THE PAVEMENT DEMOLITION.
- USE CAUTION WHEN PERFORMING DEMOLITION OPERATIONS NEAR BUILDINGS AND EXISTING FAA FACILITY. DAMAGE, OR NOISE AND VIBRATIONAL DISTURBANCE WILL REQUIRE THE CONTRACTOR TO MODIFY HIS REMOVAL METHODS.
- WHERE SAW CUTTING IS SHOWN ADJACENT TO EXISTING PAVEMENT TO REMAIN IN PLACE, A SECOND SAWCUT LINE SHALL BE MADE ON THE PAVEMENT TO BE REMOVED TO PREVENT DAMAGE TO THE EXISTING DURING BREAKING OPERATIONS.
- PROJECT LIMITS SHALL BE SAWCUT TO ISOLATE PANELS TO BE REMOVED FROM PANELS THAT ARE TO REMAIN. A MINIMUM OF 2' OFFSET SAWCUTS WILL BE REQUIRED AT THE PERIMETER OF THE REMOVAL.

**LEGEND**

- EXISTING PAVEMENT
- PROPOSED FULL DEPTH SAWCUTS
- PROPOSED APRON REMOVAL LIMITS

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**LEGEND**

	BASE BID		ADDITIVE ALTERNATE 1
	NEW 18" PCC PAVEMENT (501)		NEW 18" PCC PAVEMENT (501)
	NEW 4" CRUSHED AGGREGATE (209)		NEW 4" CRUSHED AGGREGATE (209)
	NEW 16" OVERSIZED AGGREGATE (208)		NEW 16" OVERSIZED AGGREGATE (208)
	NEW SEPARATION FABRIC (156)		NEW SEPARATION FABRIC (156)
	BASE BID		ADDITIVE ALTERNATE 2
	NEW 10" PCC PAVEMENT (501)		NEW 18" PCC PAVEMENT (501)
	NEW 6" CRUSHED AGGREGATE (209)		NEW 4" CRUSHED AGGREGATE (209)
	NEW 12" OVERSIZED AGGREGATE (208)		NEW 16" OVERSIZED AGGREGATE (208)
	NEW SEPARATION FABRIC (156)		NEW SEPARATION FABRIC (156)
	ADDITIVE ALTERNATE 3		ADDITIVE ALTERNATE 3
	NEW 18" PCC PAVEMENT (501)		NEW 18" PCC PAVEMENT (501)
	NEW 4" CRUSHED AGGREGATE (209)		NEW 4" CRUSHED AGGREGATE (209)
	NEW 16" OVERSIZED AGGREGATE (208)		NEW 16" OVERSIZED AGGREGATE (208)
	NEW SEPARATION FABRIC (156)		NEW SEPARATION FABRIC (156)



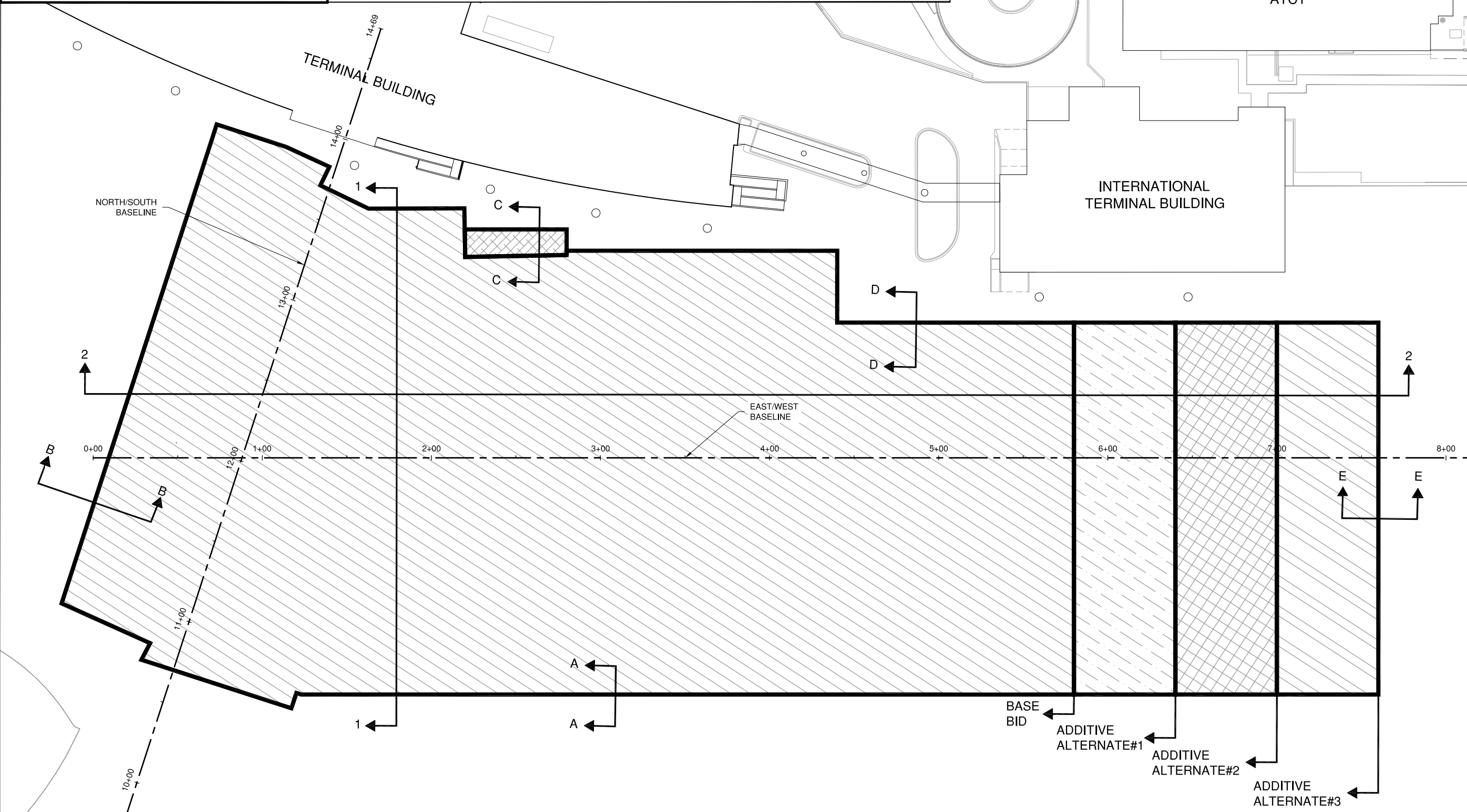
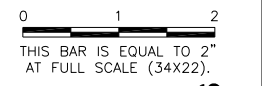
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PIA\_BASE\_PROP\_GEO  
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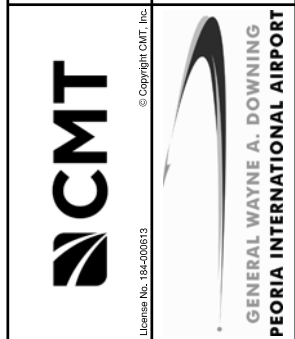
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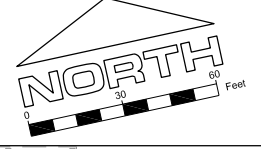
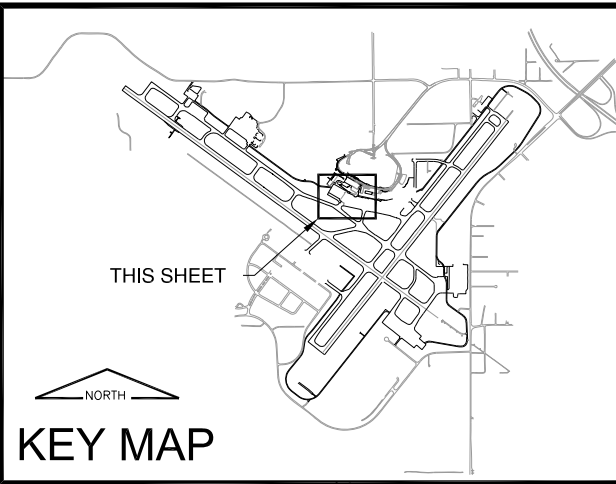
**RECONSTRUCT TERMINAL APRON PHASE 1**

**PROPOSED IMPROVEMENTS**



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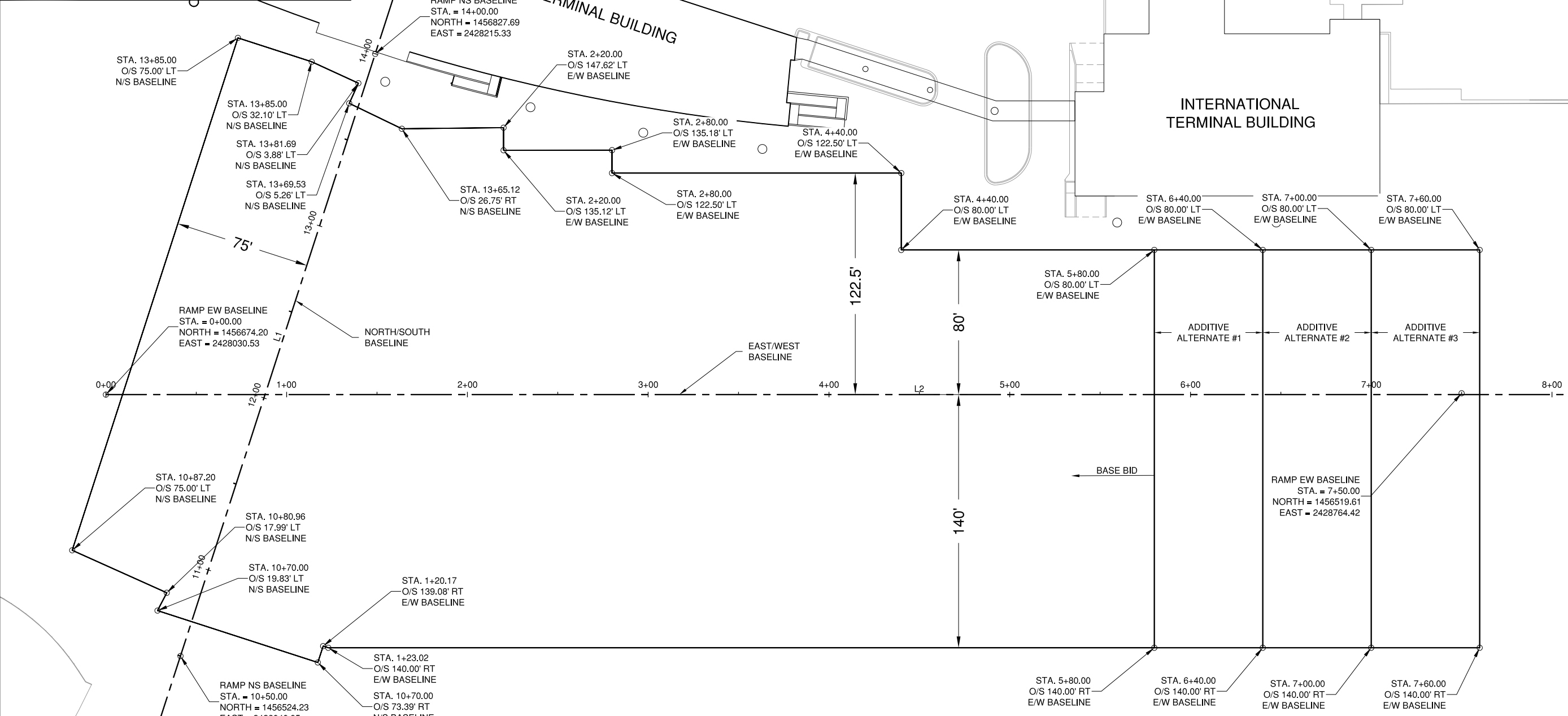
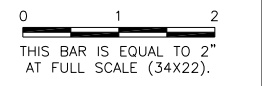
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**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**RECONSTRUCT TERMINAL APRON PHASE 1**  
**PROPOSED GEOMETRY**



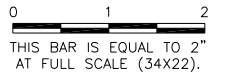
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L1	N/S BASELINE	STA. 10+00.00	N:1456480.8760 E:2428016.0384	STA. 14+68.80	N: 1456887.3486 E: 2428249.6119

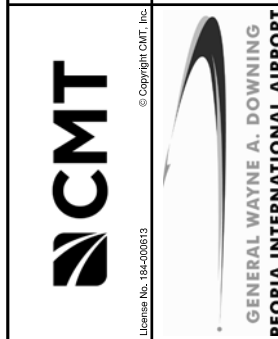
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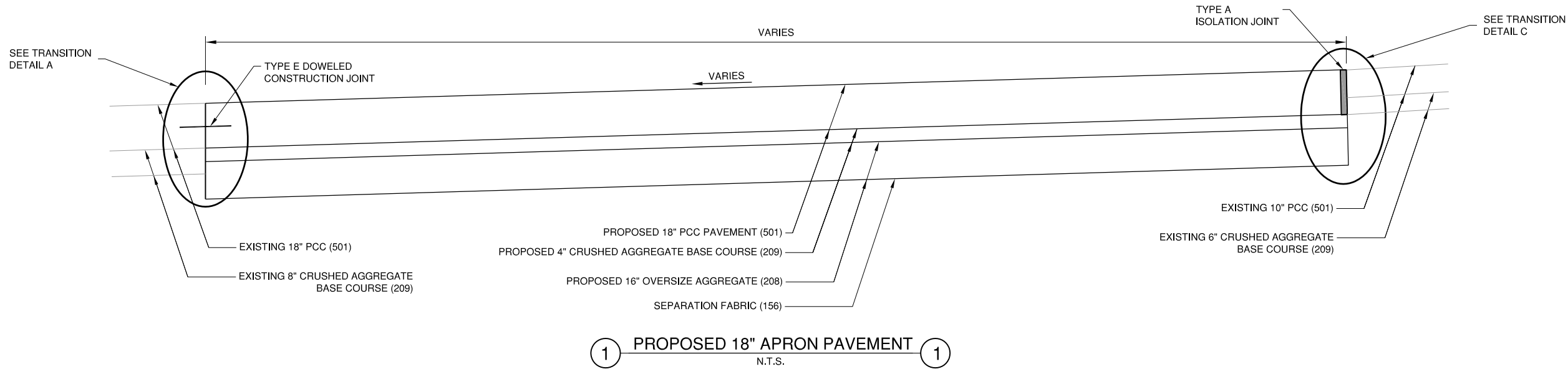
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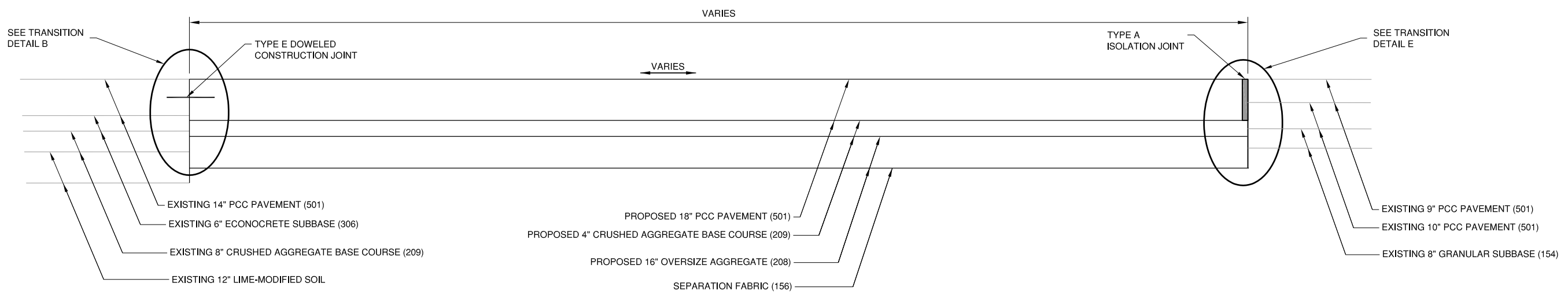
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**RECONSTRUCT TERMINAL APRON PHASE 1  
 TYPICAL SECTIONS 1**



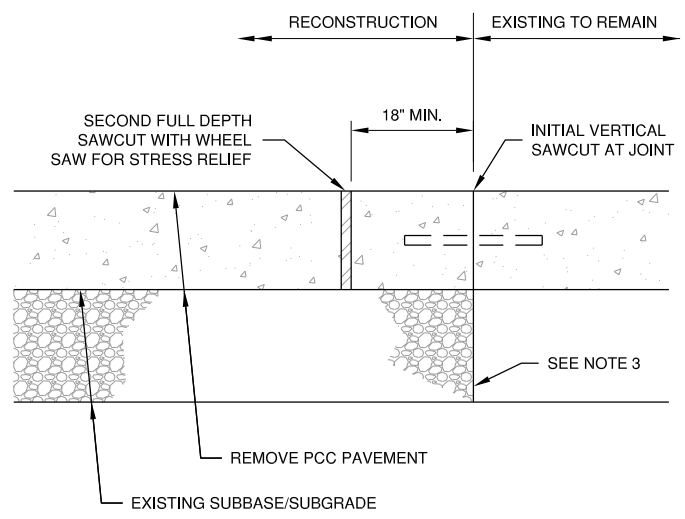
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AIP PROJ. NO.	3-17-0080-XX
SHEET	24 OF 39 SHEETS



**1 PROPOSED 18" APRON PAVEMENT**  
 N.T.S.

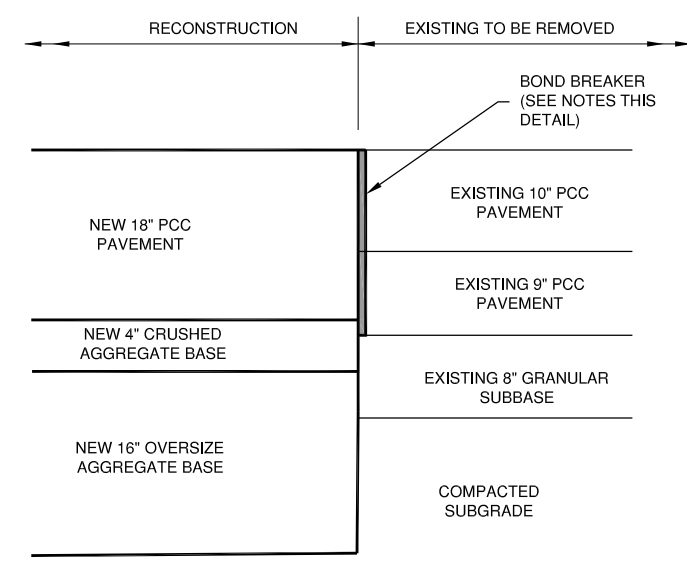


**2 PROPOSED 18" APRON PAVEMENT**  
 N.T.S.



**PCC PAVEMENT REMOVAL DETAIL**  
 N.T.S.

- NOTES:**
- SEE JOINTING PLAN FOR JOINT SPACING AND JOINT TYPES.
  - TWO VERTICAL SAWCUTS SHALL BE MADE AS INDICATED PRIOR TO STARTING ANY PAVEMENT REMOVAL ADJACENT TO EXISTING PAVEMENTS TO REMAIN IN PLACE.
  - CONTRACTOR SHALL TAKE ACTION AS NEEDED TO PREVENT LOSS OF AGGREGATE FROM UNDERNEATH PAVEMENT TO REMAIN IN PLACE.



- NOTES:**
- LIQUID BOND BREAKER SHALL NOT BE USED.
  - THICKNESS OF BOND BREAKER SHALL NOT CHANGE PROPOSED JOINT SPACING OR PANEL DIMENSIONS.

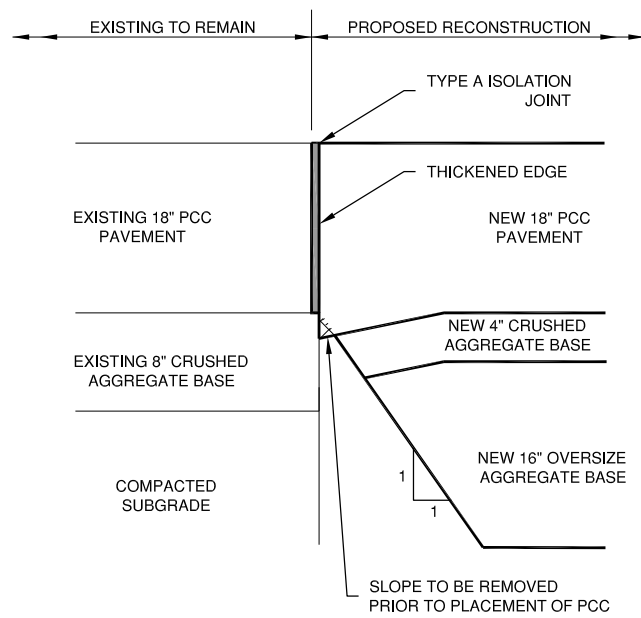
**PHASE BOND BREAKER DETAIL**  
 N.T.S.

K:\Peoria\14061-03\_TermApronRehabPH1\Drawn Sheets

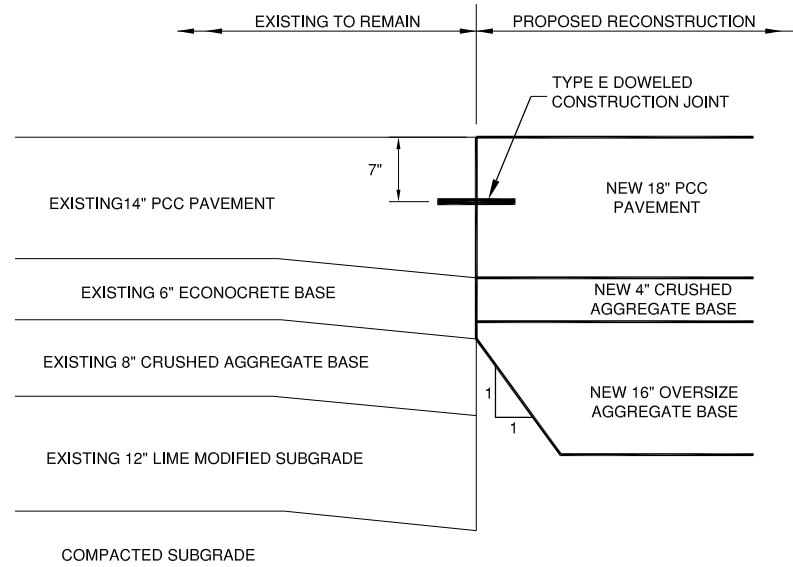
**PE099**

REVISIONS		
NUMBER	BY	DATE

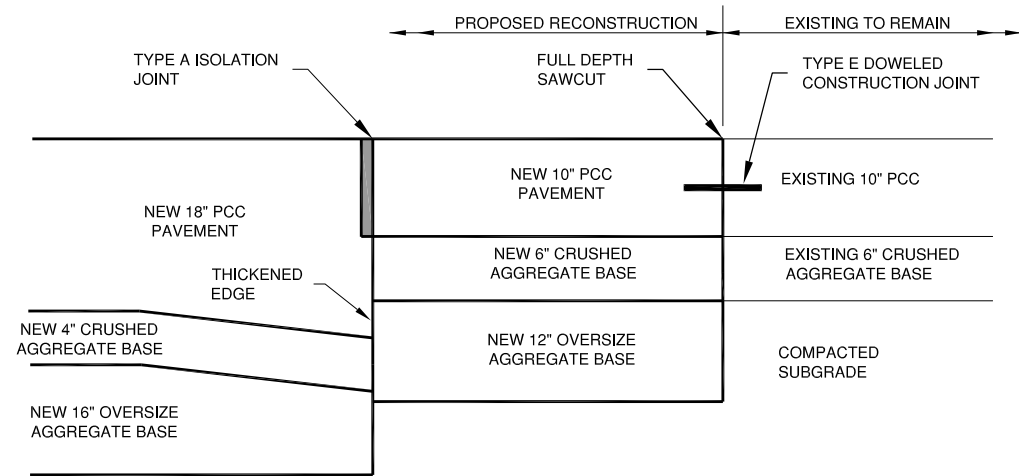
0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).



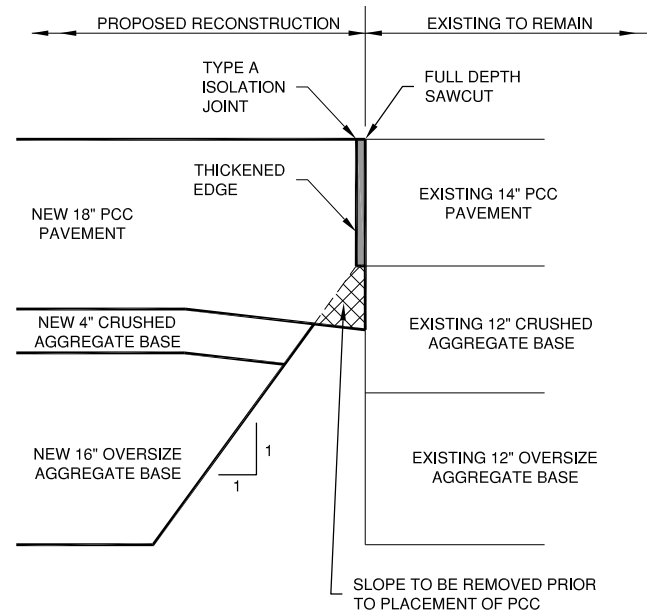
**A** TRANSITION DETAIL **A**  
 N.T.S.



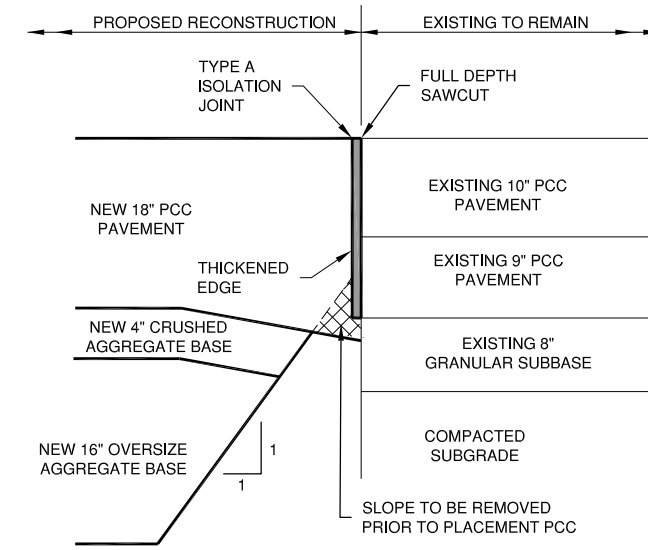
**B** TRANSITION DETAIL **B**  
 N.T.S.



**C** TRANSITION DETAIL **C**  
 N.T.S.



**D** TRANSITION DETAIL **D**  
 N.T.S.



**E** TRANSITION DETAIL **E**  
 N.T.S.

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 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

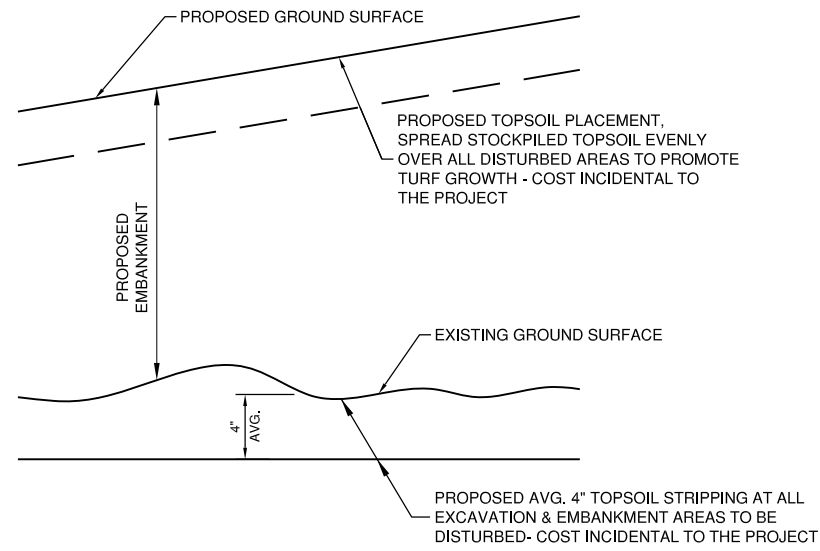
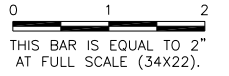
**RECONSTRUCT TERMINAL APRON PHASE 1  
 TYPICAL SECTIONS 2**



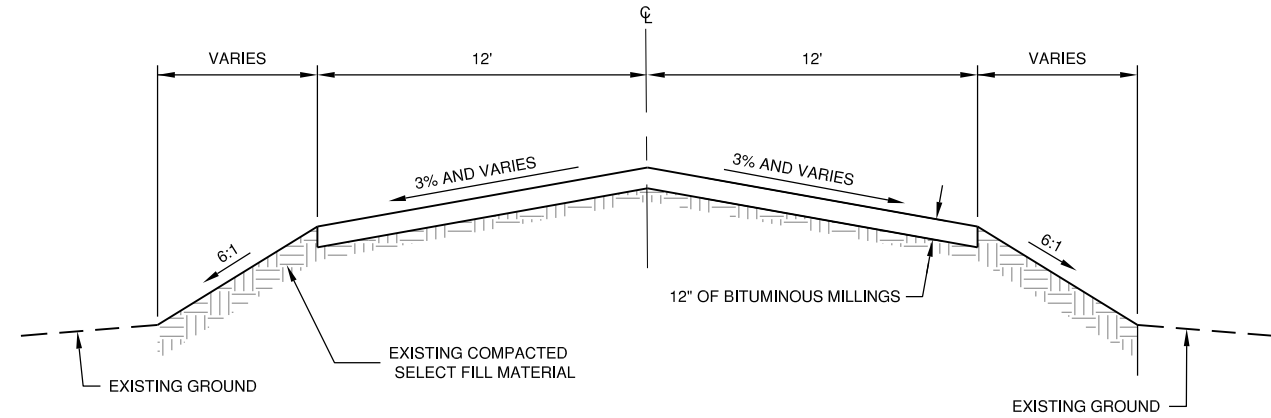
DESIGN BY:	EMH
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO. PIA-4485 AIP PROJ. NO. 3-17-0080-XX	
SHEET	25 OF 39 SHEETS

**PE099**

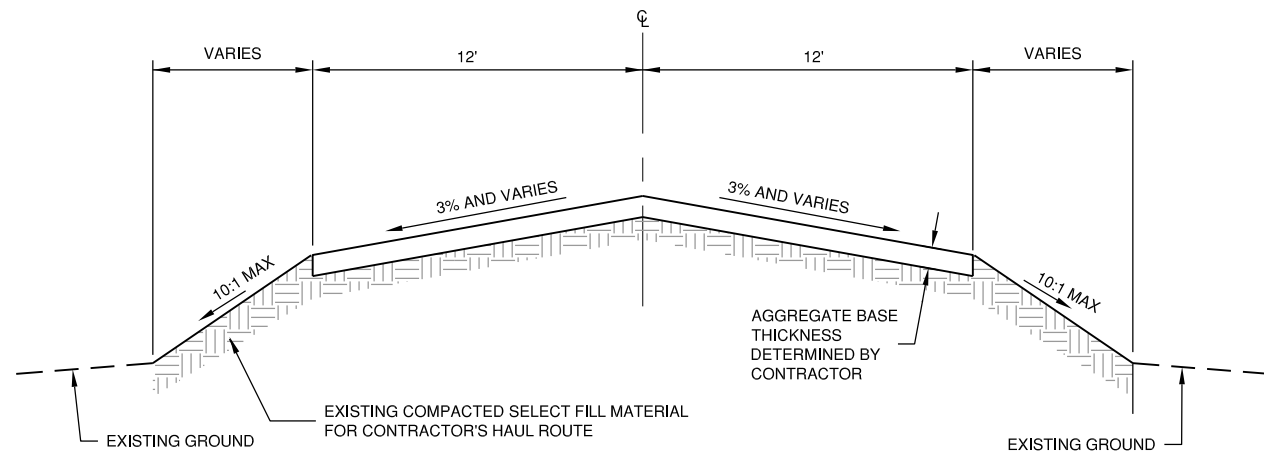
REVISIONS		
NUMBER	BY	DATE



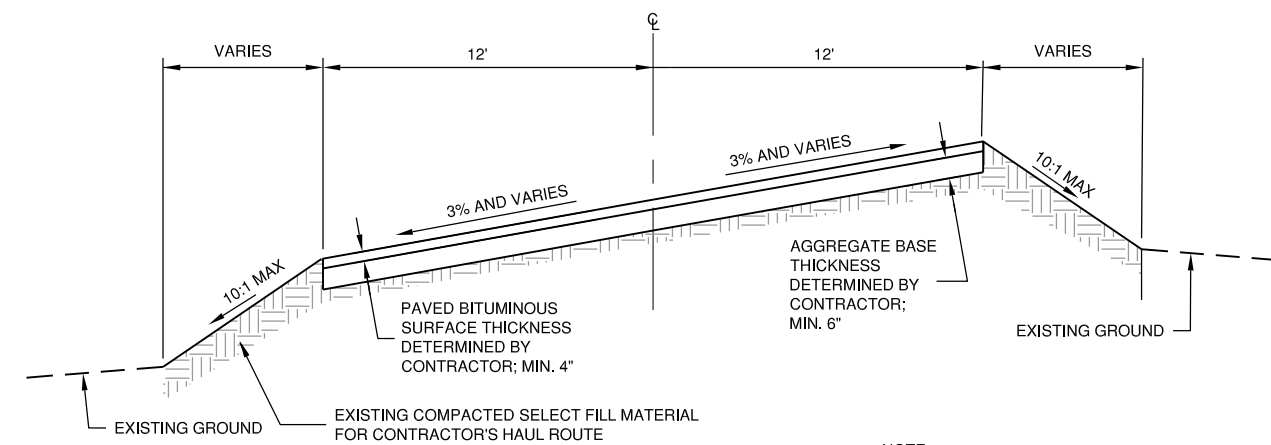
**TOPSOIL STRIPPING DETAIL**  
N.T.S.



**EXISTING TERMINAL APRON  
HAUL ROUTE SECTION**  
N.T.S.



**PROPOSED HAUL ROUTE  
SECTION TO TERMINAL APRON**  
N.T.S.



**PROPOSED PAVED HAUL ROUTE  
SECTION TO TERMINAL APRON**  
N.T.S.

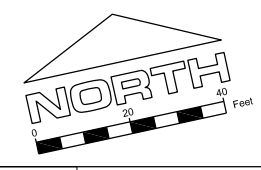
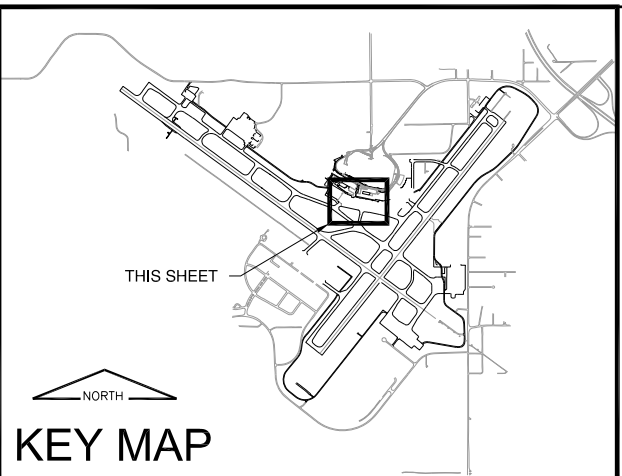
**NOTE:**  
 1. CONSTRUCTION ACCESS HAUL ROUTE ROAD SHALL BE SLOPED TO BE THE SAME AS THE EXISTING TURF GRADING OFF OF AIRFIELD PAVEMENT.

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 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**  
  
**RECONSTRUCT TERMINAL APRON PHASE 1  
 TYPICAL SECTIONS 3**



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DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	26 OF 39 SHEETS

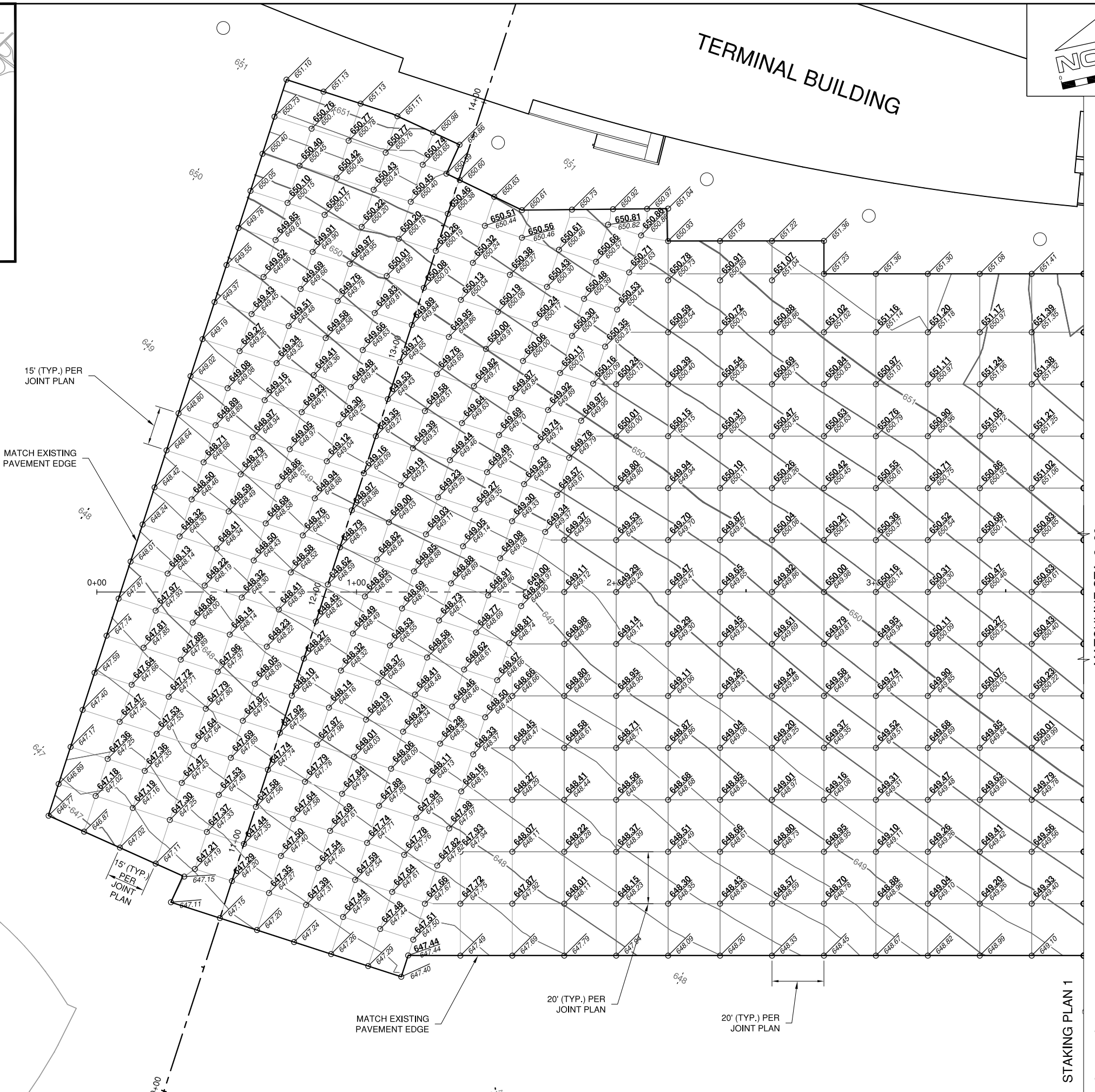
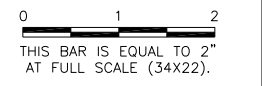
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FILE: CG101.dwg  
 UPDATE BY: Jeremy Wasilewski  
 PLOT DATE: 7/1/2015 1:40 PM  
 PIA\_PROP\_JOINTS  
 PIA\_BASE\_EXISTING  
 140610300-C-SPOT  
 KEYMAP

**PE099**

REVISIONS		
NUMBER	BY	DATE



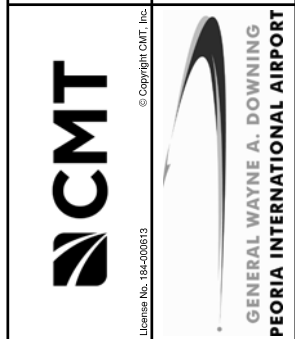
**LEGEND**

	PROPOSED ELEVATION
	EXISTING ELEVATION
	PROPOSED CONTOURS
	EXISTING CONTOURS

**NOTES:**  
 1. SEE JOINTING PLANS FOR MORE INFORMATION ON JOINT TYPES.

**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
 STAKING PLAN 1**

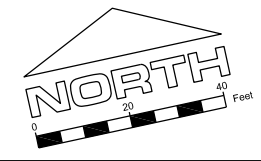


DESIGN BY:	CBG
DRAWN BY:	CMT
CHECKED BY:	CET
APPROVED BY:	CET
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO. PIA-4485 AIP PROJ. NO. 3-17-0080-XX	
SHEET	27 OF 39 SHEETS

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# INTERNATIONAL TERMINAL BUILDING



FILE: CG101.dwg  
UPDATE BY: Jeremy Wasilewski  
PLOT DATE: 7/1/2015 1:40 PM

PIA\_PROP\_JOINTS  
PIA\_BASE\_EXISTING  
140610300-C-SPOT  
KEYMAP

## PE099

### REVISIONS

NUMBER	BY	DATE

0 1 2  
THIS BAR IS EQUAL TO 2"  
AT FULL SCALE (34X22).

### LEGEND

- PROPOSED ELEVATION
- EXISTING ELEVATION
- PROPOSED CONTOURS
- EXISTING CONTOURS

### NOTES:

- SEE JOINTING PLANS FOR MORE INFORMATION ON JOINTS.
- STAKING PLAN SHOWS GRADES FOR THE BASE BID AND ALL 3 ADDITIVE ALTERNATES. GRADES MAY REQUIRE REVISIONS DEPENDING ON THE COMBINATION OF ADDITIVE ALTERNATES THAT ARE AWARDED.



MATCHLINE STA. 3-80

MATCH EXISTING PAVEMENT EDGE

8+00

20' (TYP.) PER JOINT PLAN

20' (TYP.) PER JOINT PLAN

MATCH EXISTING PAVEMENT EDGE

BASE BID

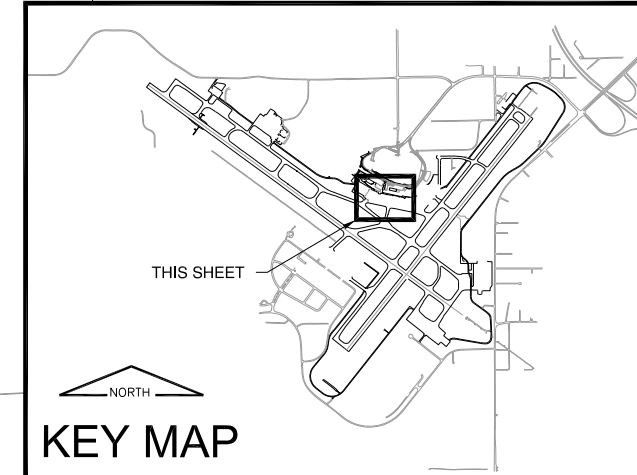
ADDITIVE ALTERNATE #1

ADDITIVE ALTERNATE #2

ADDITIVE ALTERNATE #3

STAKING PLAN 1

STAKING PLAN 2



KEY MAP

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PEORIA INTERNATIONAL AIRPORT  
PEORIA, ILLINOIS

RECONSTRUCT TERMINAL APRON PHASE 1  
STAKING PLAN 2



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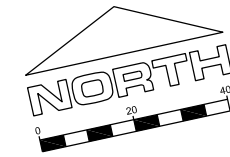
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DRAWN BY:	CMT
CHECKED BY:	CET
APPROVED BY:	CET
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	28 OF 39 SHEETS

**LEGEND**

- A — TYPE A ISOLATION JOINT (3/4")
- B — TYPE B HINGED (TIED) CONTRACTION JOINT
- C — TYPE C DOWELED CONTRACTION JOINT
- D — TYPE D DUMMY CONTRACTION JOINT
- E — TYPE E DOWELED CONSTRUCTION JOINT
- R REINFORCED PANEL
- THICKENED EDGE

**NOTES:**

1. THE CONTRACTOR MAY SUBMIT AN ALTERNATE JOINTING/PAVING PLAN FOR REVIEW AND CONSIDERATION PRIOR TO INITIATING THE PAVING OPERATIONS. THE PAVING PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO, THE CONTRACTOR'S PROPOSED CHANGES TO PAVING DIRECTION, PROPOSED CHANGES TO PAVING LANE WIDTHS, PROPOSED CHANGES TO THE JOINTING TYPE, CONTRACTOR'S PLAN FOR "BOXING OUT" AROUND INLETS, LIGHTS, HAND-HOLES, OR OTHER PAVEMENT PENETRATIONS, PLANS TO PREVENT PREMATURE CRACKING IN "BOXED OUT" PANELS, CONTRACTOR'S PLAN FOR PAVING AGAINST ADJACENT STRUCTURES, AND ANY OTHER REVISION OR ALTERATION THAT THE CONTRACTOR SHOULD PROPOSE THAT DIFFERS FROM THE PLAN SHEETS.

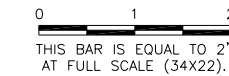


FILE: CP101.dwg  
 UPDATE BY: Jeremey Wasilewski  
 PLOT DATE: 7/1/2015 1:40 PM

PIA\_PROP\_JOINTS  
 PIA\_EXIST\_JOINTS  
 PIA\_BASE\_EXISTING  
 KEYMAP  
 PIA\_BASE\_PROP\_GEO

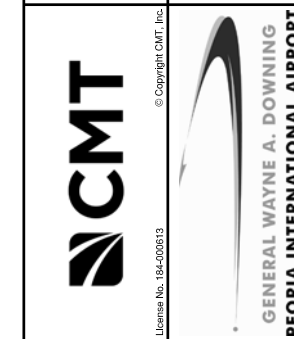
**PE099**

REVISIONS		
NUMBER	BY	DATE

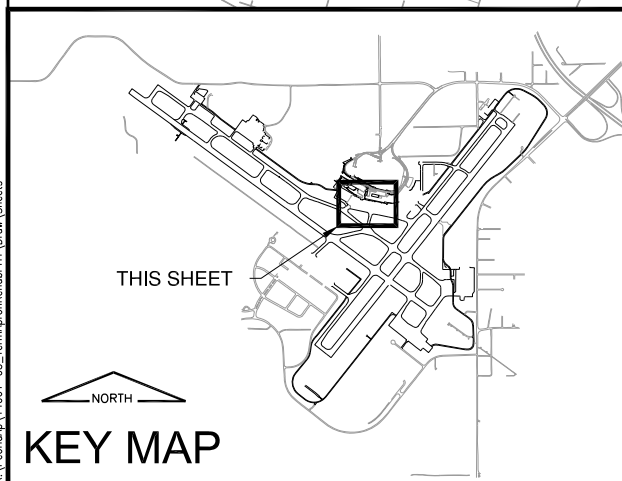


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 PEORIA, ILLINOIS**

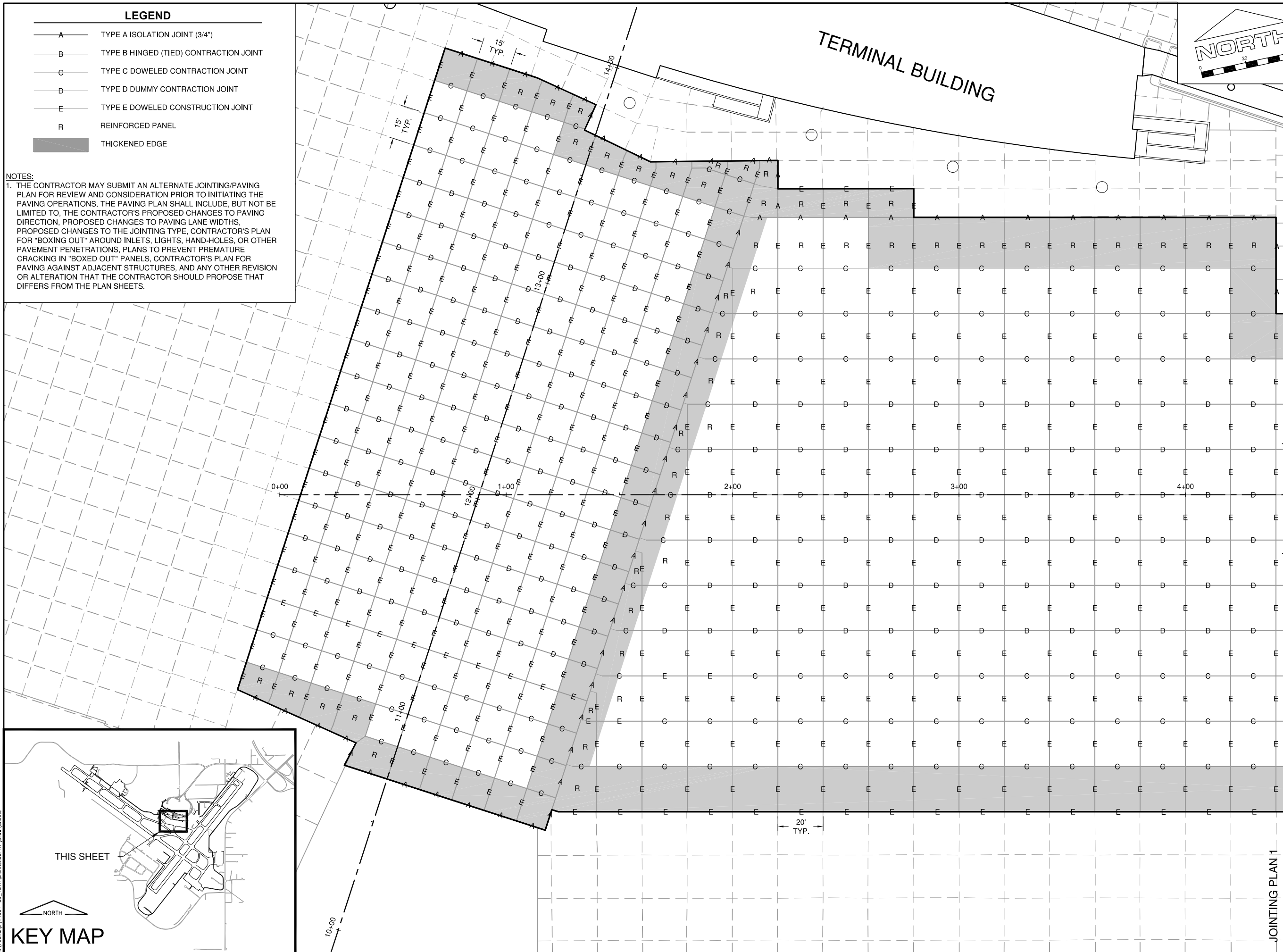
**RECONSTRUCT TERMINAL APRON PHASE 1  
 JOINTING PLAN 1**



DESIGN BY:	JMW
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	29 OF 39 SHEETS

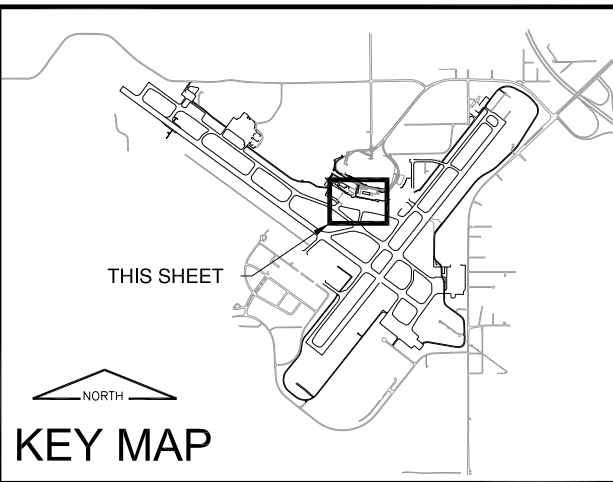


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JOINTING PLAN 1

JOINTING PLAN 2



# INTERNATIONAL TERMINAL BUILDING

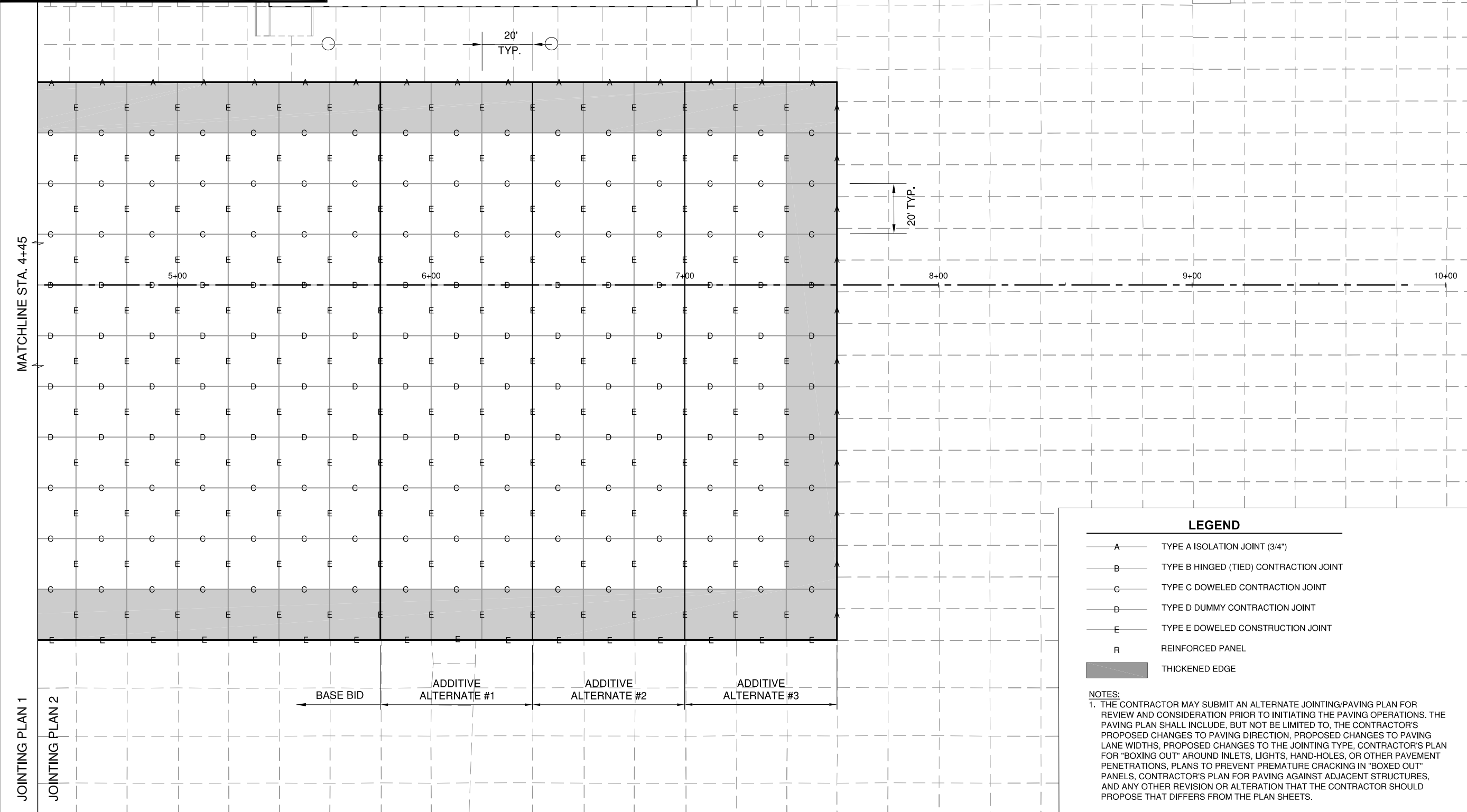
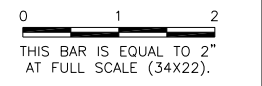


FILE: CP102.dwg  
 UPDATE BY: Jeremy Wasilewski  
 PLOT DATE: 7/1/2015 2:06 PM

PIA\_PROP\_JOINTS  
 PIA\_EXIST\_JOINTS  
 KEYMAP  
 PIA\_BASE\_PROP\_GEO  
 PIA\_BASE\_EXISTING

## PE099

REVISIONS		
NUMBER	BY	DATE



**LEGEND**

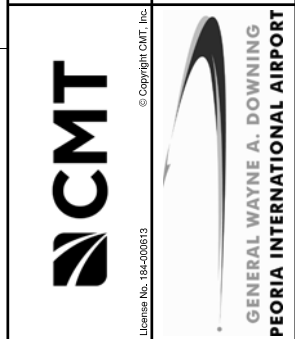
— A —	TYPE A ISOLATION JOINT (3/4")
— B —	TYPE B HINGED (TIED) CONTRACTION JOINT
— C —	TYPE C DOWELED CONTRACTION JOINT
— D —	TYPE D DUMMY CONTRACTION JOINT
— E —	TYPE E DOWELED CONSTRUCTION JOINT
— R —	REINFORCED PANEL
	THICKENED EDGE

**NOTES:**

1. THE CONTRACTOR MAY SUBMIT AN ALTERNATE JOINTING/PAVING PLAN FOR REVIEW AND CONSIDERATION PRIOR TO INITIATING THE PAVING OPERATIONS. THE PAVING PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO, THE CONTRACTOR'S PROPOSED CHANGES TO PAVING DIRECTION, PROPOSED CHANGES TO PAVING LANE WIDTHS, PROPOSED CHANGES TO THE JOINTING TYPE, CONTRACTOR'S PLAN FOR "BOXING OUT" AROUND INLETS, LIGHTS, HAND-HOLES, OR OTHER PAVEMENT PENETRATIONS, PLANS TO PREVENT PREMATURE CRACKING IN "BOXED OUT" PANELS, CONTRACTOR'S PLAN FOR PAVING AGAINST ADJACENT STRUCTURES, AND ANY OTHER REVISION OR ALTERATION THAT THE CONTRACTOR SHOULD PROPOSE THAT DIFFERS FROM THE PLAN SHEETS.

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 PEORIA, ILLINOIS

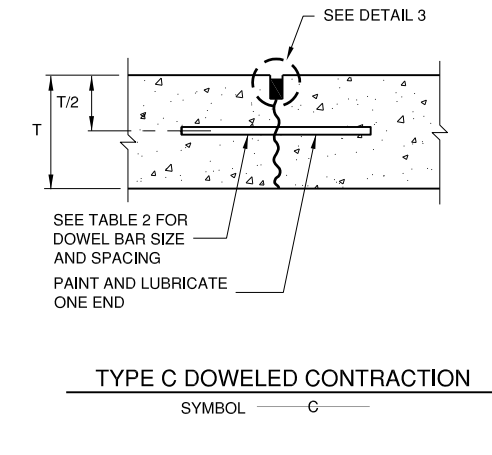
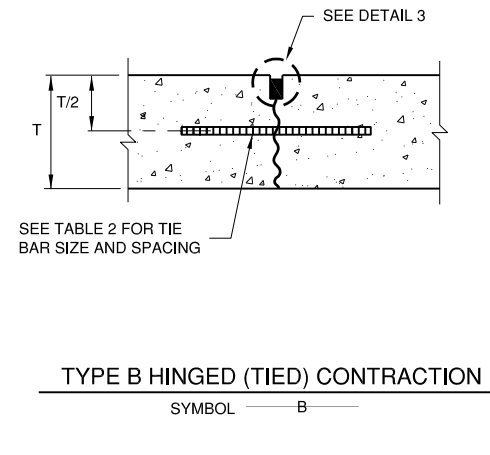
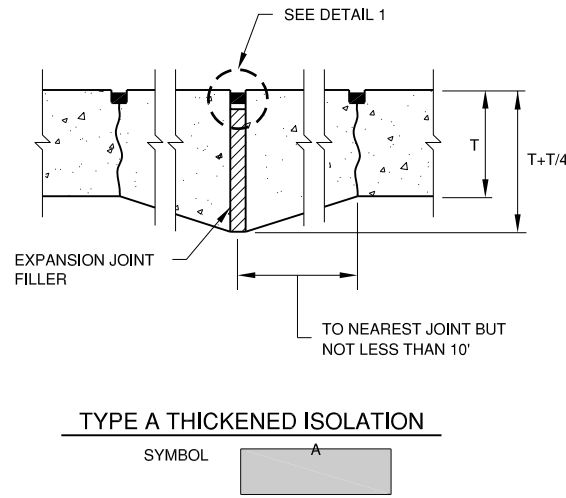
RECONSTRUCT TERMINAL APRON PHASE 1  
 JOINTING PLAN 2



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CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	30 OF 39 SHEETS

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TABLE 1	
PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES $I=(T/3) \pm 1/4"$
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"



**PE099**

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NUMBER	BY	DATE

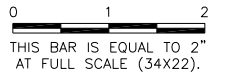
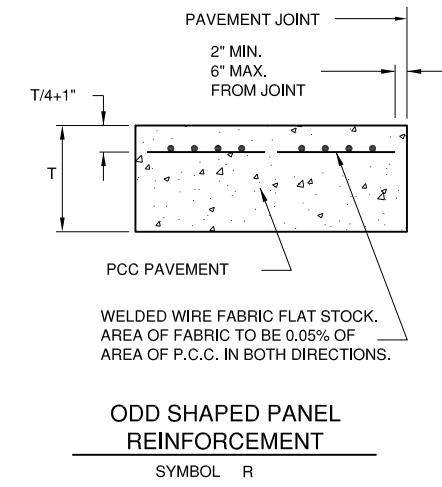
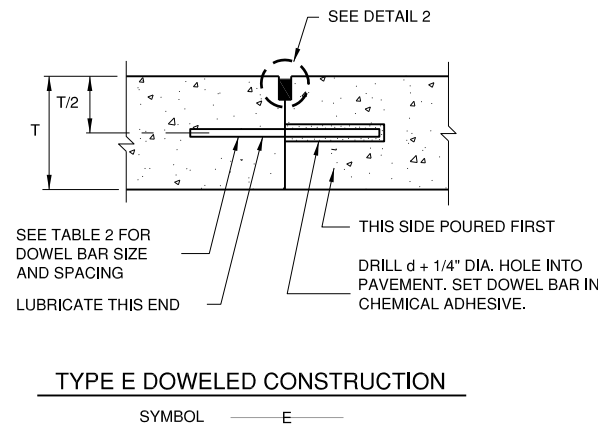
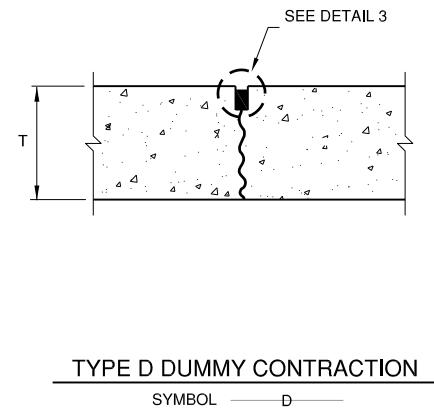


TABLE 2						
PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"



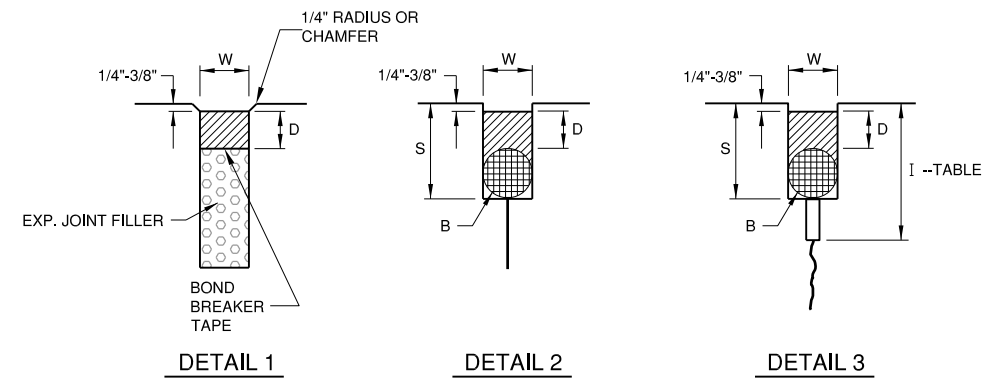
**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**RECONSTRUCT TERMINAL APRON PHASE 1**  
**JOINTING DETAILS**

JOINT SEALING DIMENSIONS			
	DETAIL 1	DETAIL 2	DETAIL 3
W=WIDTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/2
D=DEPTH OF SEALANT RESERVOIR (IN.)	3/8	1/4	1/4
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-1/8	1-1/8

**JOINT NOTES**

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.

**JOINT SEALING DETAILS**



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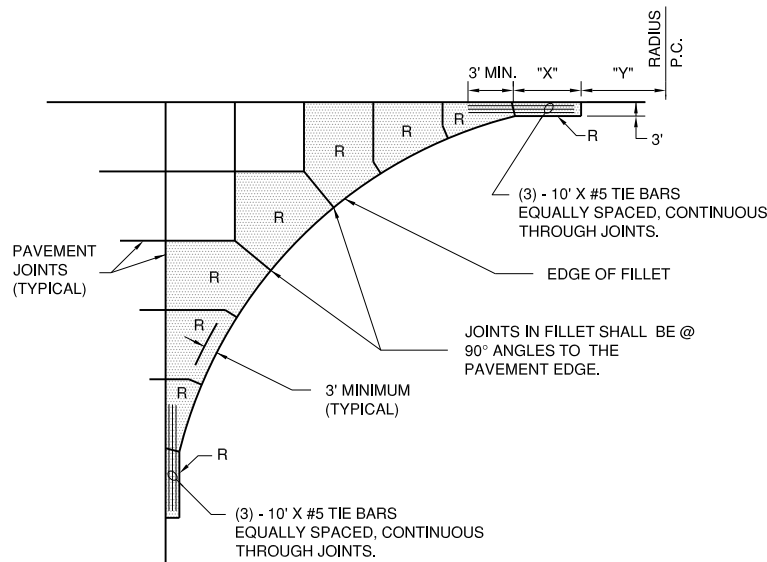
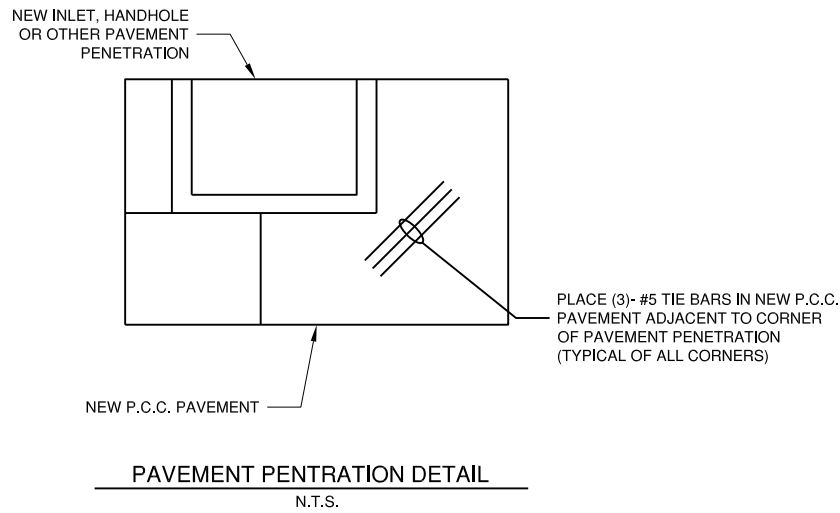


DESIGN BY:	JMW
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	31 OF 39 SHEETS

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NUMBER	BY	DATE

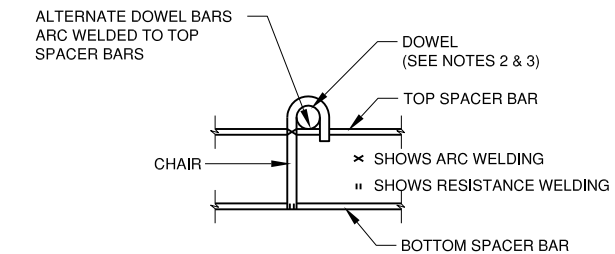
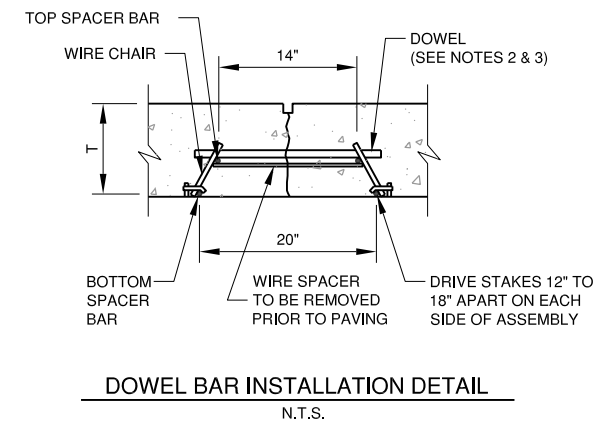
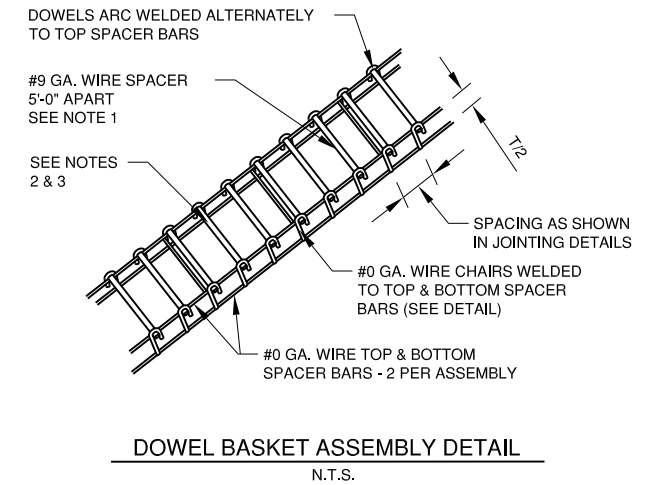
0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



⊕ DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH WELDED WIRE FABRIC AS SHOWN ON JOINTING DETAILS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN)

FILLET RADIUS	"X" (IN FEET)	"Y" (IN FEET)
20	4.30	6.24
25	4.88	7.00
30	5.40	7.68
50	7.11	9.95
75	8.79	12.21
85	9.38	13.00
100	10.21	14.11
125	11.44	15.78
150	12.56	17.29
175	13.58	18.68
200	14.53	19.98

**FILLET DETAIL & FILLET REINFORCING LAYOUT**  
 N.T.S.



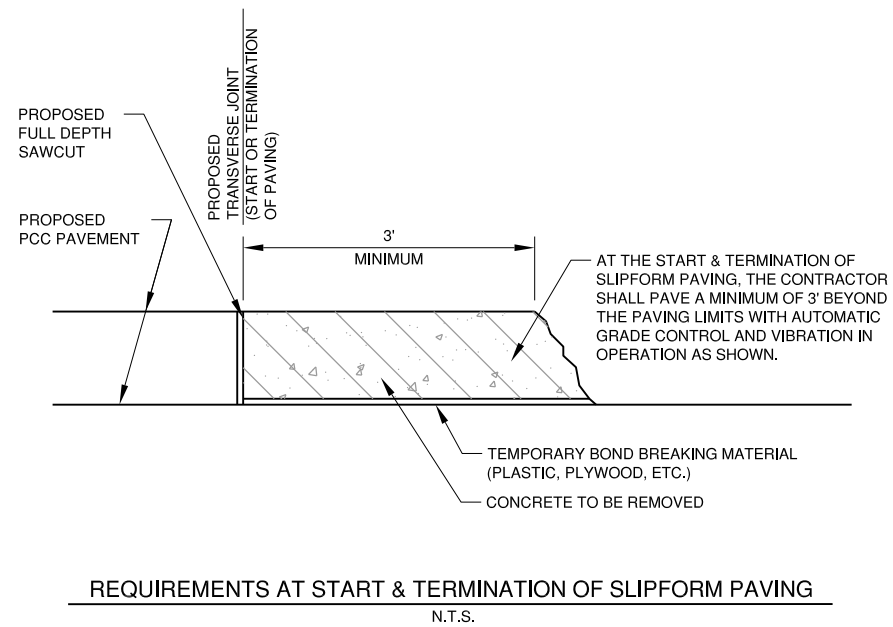
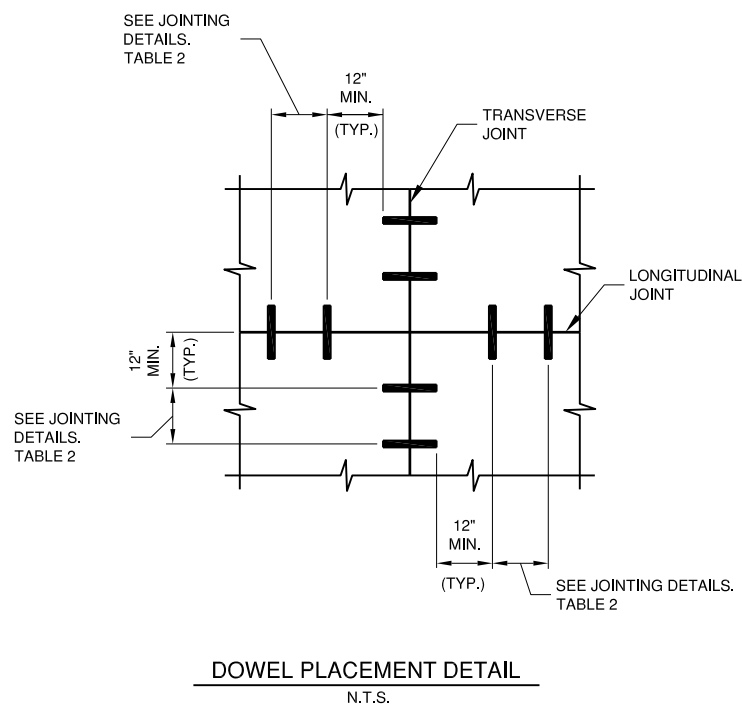
**DOWEL BASKET DETAILS**  
 N.T.S.

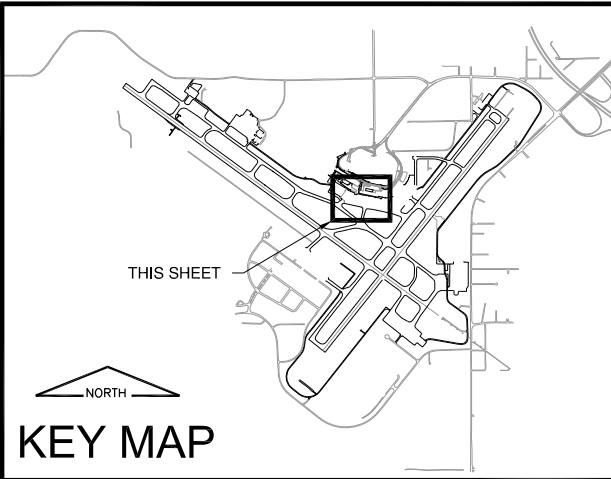
- DOWEL BASKET NOTES**
- #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
  - DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 2.
  - DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.

**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**RECONSTRUCT TERMINAL APRON PHASE 1**  
**PAVING & MISCELLANEOUS DETAILS**



DESIGN BY:	JMW
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	32 OF 39 SHEETS





**NOTES**

1. PRIOR TO STARTING PHASE 1 REMOVE THE TAXILANE CENTERLINE AS SHOWN. PHASE 1 TEMPORARY MARKING SHALL BE PAINTED AS LAID OUT.
2. PHASE 1 TEMPORARY MARKING SHALL REMAIN IN PLACE FOR CONSTRUCTION PHASE 1 AND BE REMOVED PRIOR TO STARTING PHASE 2.
3. PHASE 2 TEMPORARY MARKING SHALL BE PAINTED PRIOR TO CONSTRUCTION PHASE 2 AND REMOVED THERE AFTER PRIOR TO STARTING CONSTRUCTION PHASE 3.
4. PRIOR TO STARTING CONSTRUCTION PHASE 3 THE PROPOSED PERMANENT TAXILANE MARKING SHALL BE PAINTED.
5. THE TAXILANE CENTERLINE SHALL BE REMARKED AS SHOWN ON THE MARKING PLAN 2 SHEET.



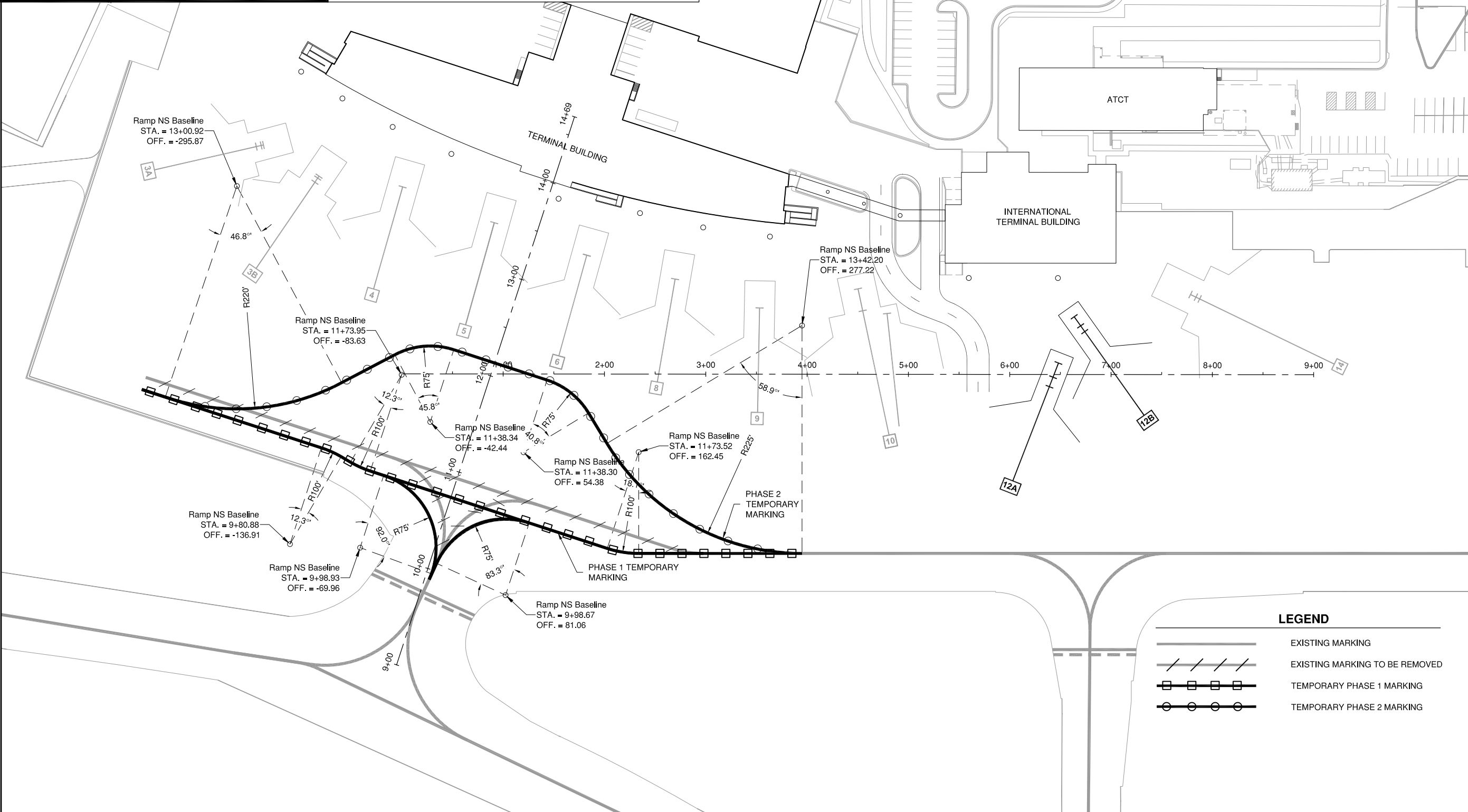
FILE: CM101.dwg  
 UPDATE BY: Jeremy Wasilewski  
 PLOT DATE: 7/1/2015 1:41 PM

KEYMAP  
 PIA\_BASE\_EXISTING  
 PIA\_BASE\_MARKING

**PE099**

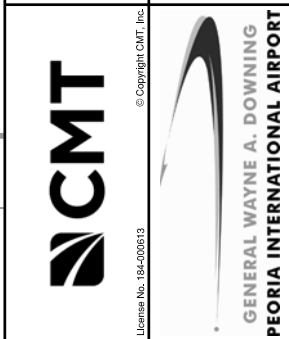
REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).



**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
 MARKING PLAN 1**



**LEGEND**

	EXISTING MARKING
	EXISTING MARKING TO BE REMOVED
	TEMPORARY PHASE 1 MARKING
	TEMPORARY PHASE 2 MARKING

DESIGN BY:	KLB/JMW
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	33 OF 39 SHEETS

K:\Peoria\14061-03\_TermApronRebPH1\Drawn\_Sheets



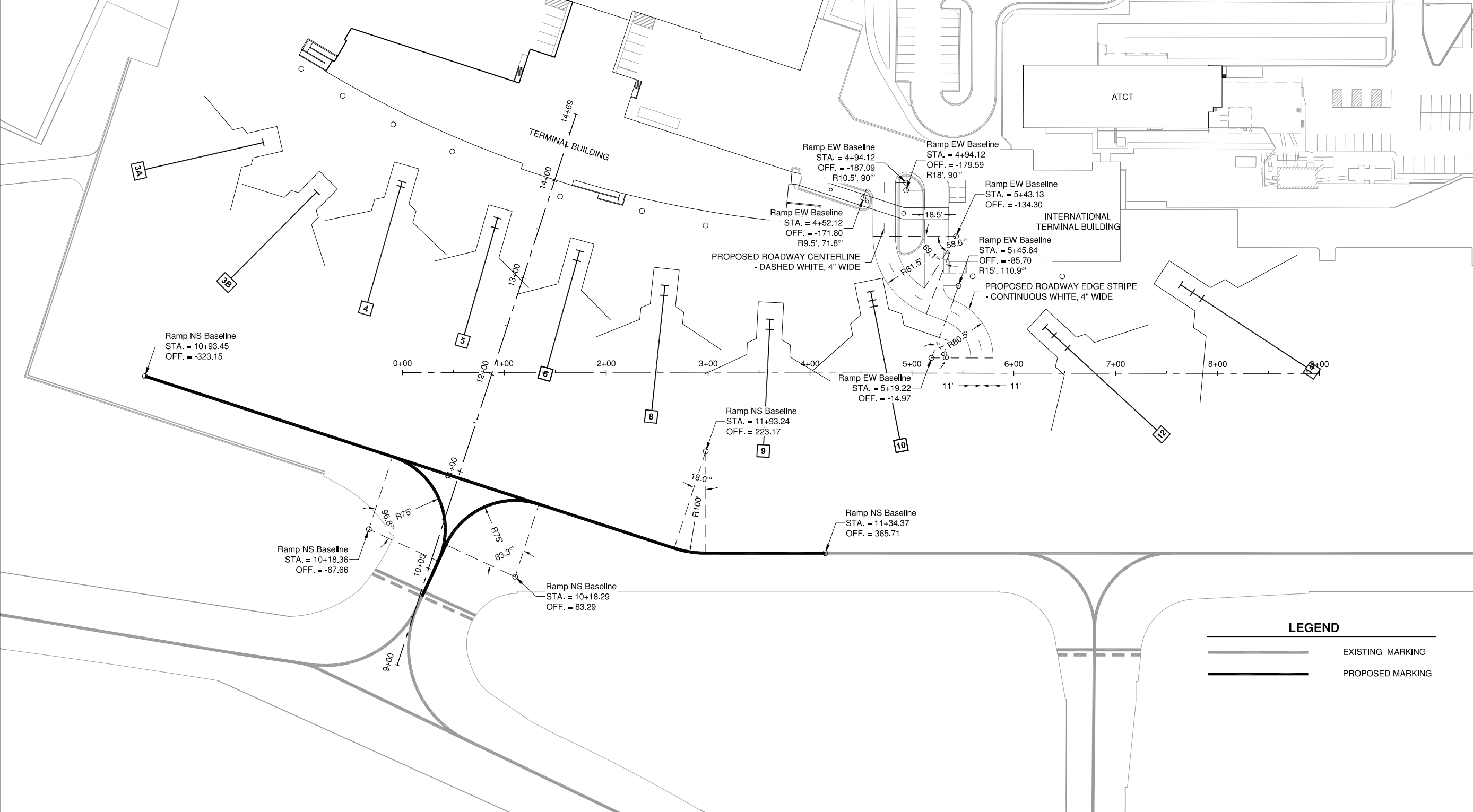
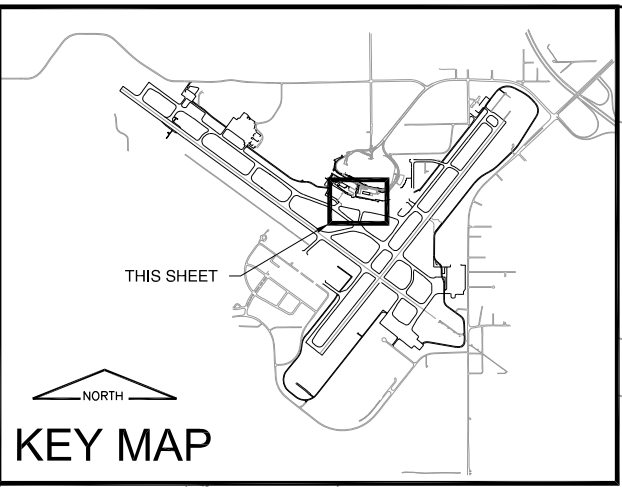
FILE: CM102.dwg  
UPDATE BY: Jeremy Wasilewski  
PLOT DATE: 7/1/2015 1:42 PM

KEYMAP  
PIA\_BASE\_MARKING  
PIA\_BASE\_EXISTING

**PE099**

REVISIONS		
NUMBER	BY	DATE

0 1 2  
THIS BAR IS EQUAL TO 2"  
AT FULL SCALE (34X22).



**LEGEND**

	EXISTING MARKING
	PROPOSED MARKING

**GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL AIRPORT  
PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
MARKING PLAN 2**



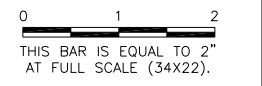
DESIGN BY:	KLB/JMW
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	34 OF 39 SHEETS

K:\Peoria\14061-03\_TermApronRebPH1\Drawn\Sheets



**PE099**

REVISIONS		
NUMBER	BY	DATE



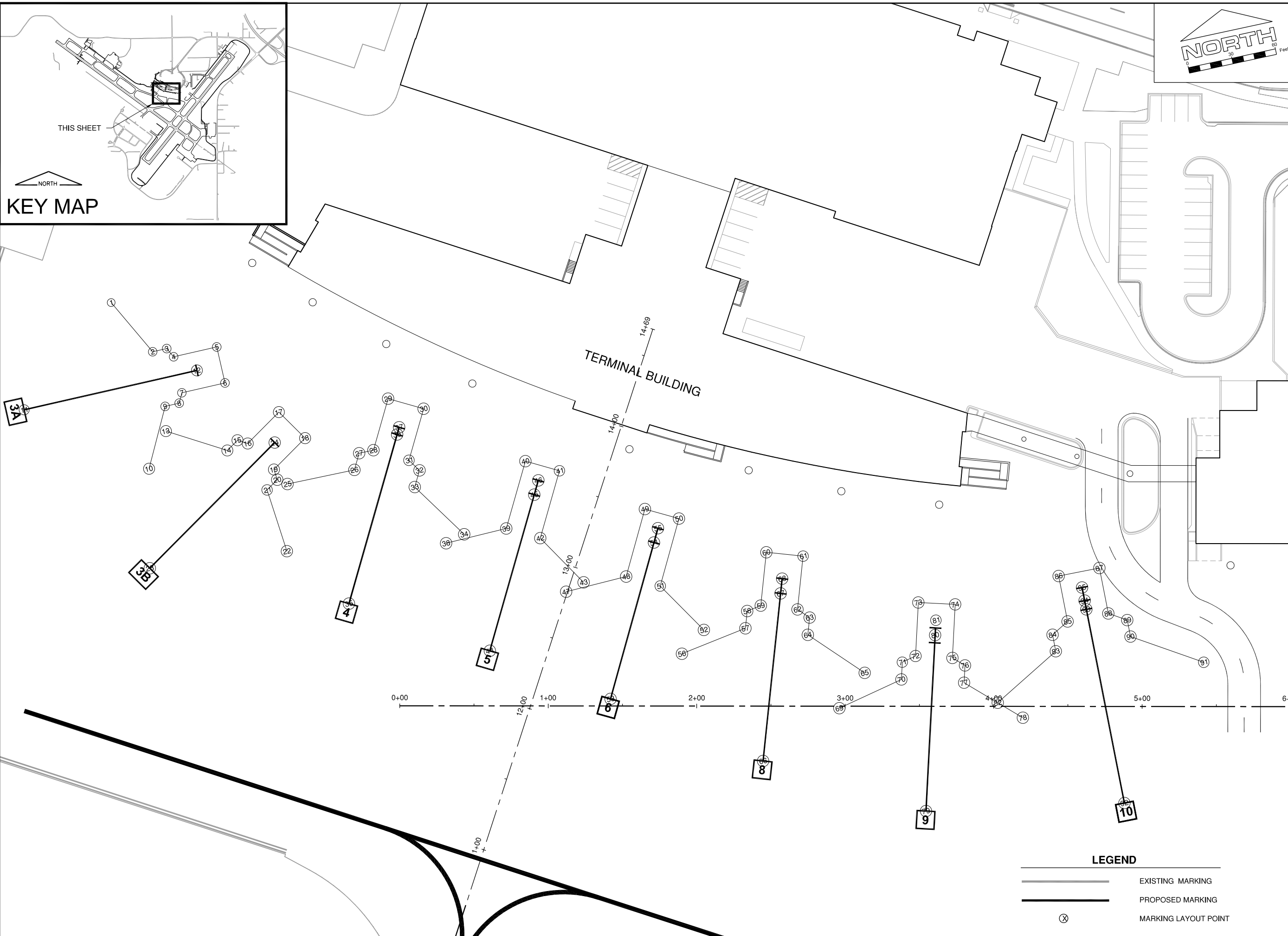
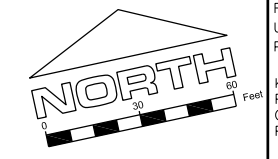
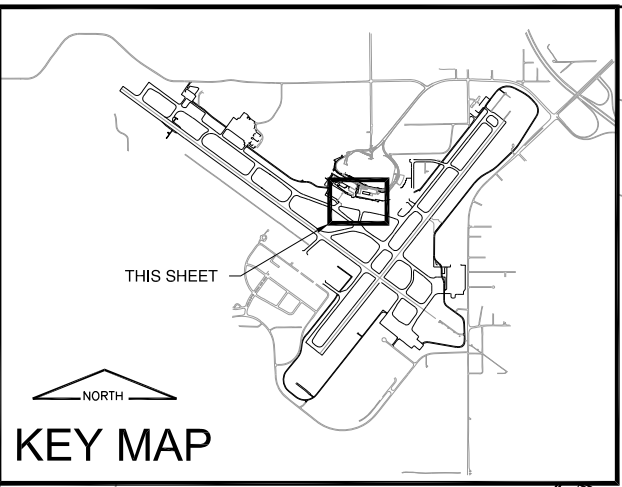
**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1**

**TERMINAL GATE MARKING PLAN 1**



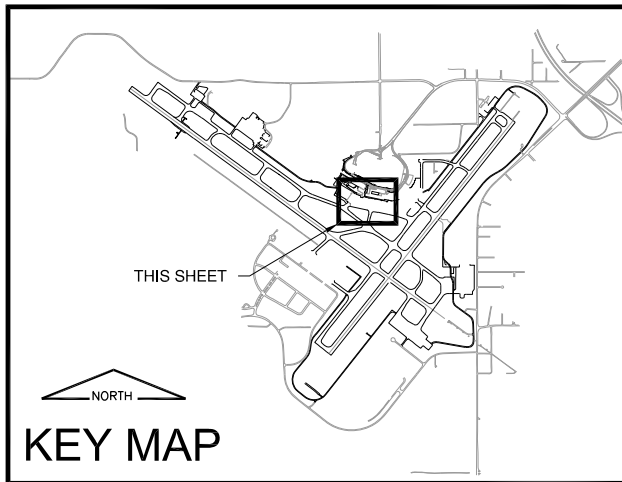
DESIGN BY:	KLB/JMW
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	35 OF 39 SHEETS



**LEGEND**

	EXISTING MARKING
	PROPOSED MARKING
	MARKING LAYOUT POINT

K:\Peoria\14061-03\_TermApronRebPH1\Drawn\_Sheets



INTERNATIONAL TERMINAL BUILDING



FILE: CM104.dwg  
 UPDATE BY: Jeremey Wasilewski  
 PLOT DATE: 7/1/2015 1:42 PM

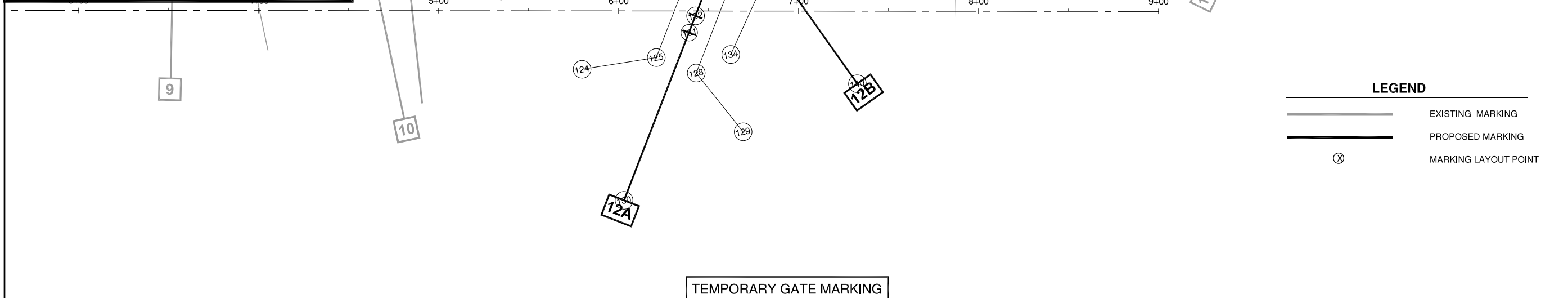
PIA\_BASE\_EXISTING  
 KEYMAP  
 PIA\_BASE\_MARKING  
 CM601

PE099

REVISIONS

NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).



LEGEND

- EXISTING MARKING
- PROPOSED MARKING
- MARKING LAYOUT POINT

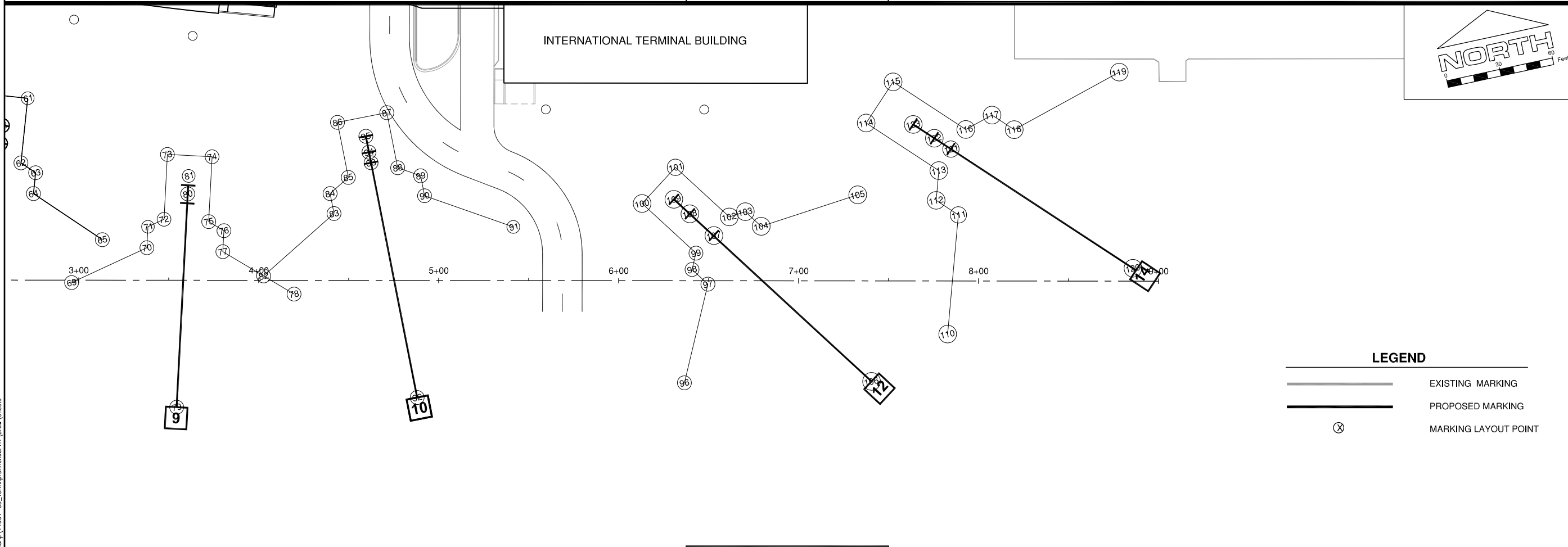
TEMPORARY GATE MARKING

INTERNATIONAL TERMINAL BUILDING



GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS

RECONSTRUCT TERMINAL APRON PHASE 1  
 TERMINAL GATE MARKING PLAN 2



LEGEND

- EXISTING MARKING
- PROPOSED MARKING
- MARKING LAYOUT POINT

FINAL GATE MARKING



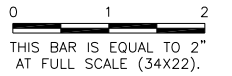
DESIGN BY: KLB/JMW  
 DRAWN BY: CMT  
 CHECKED BY: CBG  
 APPROVED BY: CBG  
 DATE: JULY 31, 2015  
 JOB No: 14061-03

IL PROJ. NO. PIA-4485  
 AIP PROJ. NO. 3-17-0080-XX

SHEET 36 OF 39 SHEETS

**PE099**

REVISIONS		
NUMBER	BY	DATE



LAYOUT POINT TABLE		
POINT	NORTHING	EASTING
1	1456980.115	2427896.338
2	1456941.902	2427916.862
3	1456942.063	2427926.560
4	1456935.690	2427929.983
5	1456936.185	2427959.766
6	1456911.313	2427960.179
7	1456910.819	2427930.396
8	1456904.336	2427927.187
9	1456904.175	2427917.489
10	1456865.301	2427898.245
11	1456921.492	2427824.095
12	1456923.474	2427943.425
13	1456887.777	2427914.575
14	1456866.474	2427952.360
15	1456871.799	2427960.467
16	1456868.247	2427966.768
17	1456884.600	2427991.665
18	1456863.809	2428005.321
19	1456847.455	2427980.424
20	1456840.262	2427981.181
21	1456834.937	2427973.075
22	1456791.799	2427977.616
23	1456799.598	2427884.908
24	1456865.119	2427984.660
25	1456835.996	2427987.338
26	1456836.031	2428033.405
27	1456846.360	2428038.847
28	1456846.367	2428048.682
29	1456878.321	2428065.516
30	1456866.727	2428087.524

LAYOUT POINT TABLE		
POINT	NORTHING	EASTING
31	1456834.773	2428070.690
32	1456826.657	2428076.246
33	1456816.328	2428070.804
34	1456778.315	2428096.826
35	1456748.616	2428011.242
36	1456853.458	2428066.476
37	1456857.882	2428068.806
38	1456774.925	2428083.656
39	1456776.312	2428125.341
40	1456818.037	2428147.323
41	1456806.961	2428168.417
42	1456765.207	2428146.420
43	1456730.043	2428168.849
44	1456697.971	2428097.533
45	1456794.713	2428148.500
46	1456803.560	2428153.161
47	1456726.308	2428156.022
48	1456727.966	2428197.697
49	1456769.862	2428219.423
50	1456758.864	2428240.558
51	1456716.998	2428218.848
52	1456681.980	2428241.504
53	1456649.475	2428170.413
54	1456746.546	2428220.751
55	1456755.423	2428225.355
56	1456669.340	2428223.728
57	1456677.306	2428269.102
58	1456688.418	2428272.683
59	1456690.119	2428282.370
60	1456724.494	2428293.451

LAYOUT POINT TABLE		
POINT	NORTHING	EASTING
61	1456716.862	2428317.127
62	1456682.487	2428306.046
63	1456675.449	2428312.916
64	1456664.337	2428309.334
65	1456631.372	2428341.513
66	1456587.380	2428262.320
67	1456695.408	2428297.143
68	1456704.926	2428300.211
69	1456611.574	2428319.900
70	1456621.904	2428364.794
71	1456633.188	2428367.789
72	1456635.393	2428377.374
73	1456670.301	2428386.641
74	1456663.919	2428410.683
75	1456629.011	2428401.417
76	1456622.342	2428408.645
77	1456611.058	2428405.650
78	1456579.822	2428439.510
79	1456531.746	2428362.728
80	1456646.494	2428393.189
81	1456656.159	2428395.755
82	1456593.188	2428425.047
83	1456619.067	2428470.470
84	1456630.375	2428470.625
85	1456637.069	2428482.375
86	1456668.261	2428482.802
87	1456667.878	2428510.758
88	1456636.687	2428510.332
89	1456629.674	2428521.894
90	1456618.365	2428521.740

LAYOUT POINT TABLE		
POINT	NORTHING	EASTING
91	1456591.253	2428566.438
92	1456509.467	2428494.606
93	1456642.438	2428496.429
94	1456648.437	2428496.511
95	1456657.437	2428496.634
96	1456486.829	2428641.570
97	1456537.822	2428665.672
98	1456547.594	2428658.716
99	1456556.079	2428662.727
100	1456589.225	2428639.131
101	1456605.078	2428661.401
102	1456571.931	2428684.996
103	1456572.943	2428694.327
104	1456563.172	2428701.283
105	1456569.252	2428757.356
106	1456466.181	2428743.499
107	1456563.561	2428674.497
108	1456578.134	2428663.932
109	1456587.849	2428656.889
110	1456483.186	2428790.290
111	1456546.787	2428809.786
112	1456557.242	2428799.374
113	1456573.131	2428804.245
114	1456607.337	2428770.180
115	1456626.627	2428789.549
116	1456592.420	2428823.614
117	1456597.225	2428839.523
118	1456586.769	2428849.935
119	1456606.001	2428913.616
120	1456497.765	2428898.588

LAYOUT POINT TABLE		
POINT	NORTHING	EASTING
121	1456583.450	2428813.259
122	1456591.244	2428805.497
123	1456601.164	2428795.618
124	1456522.435	2428590.976
125	1456520.320	2428632.644
126	1456566.044	2428662.423
127	1456553.041	2428682.387
128	1456507.317	2428652.608
129	1456470.063	2428671.391
130	1456446.502	2428598.786
131	1456530.021	2428653.385
132	1456538.438	2428658.784
133	1456551.063	2428666.884
134	1456513.420	2428673.516
135	1456547.013	2428698.257
136	1456597.069	2428676.533
137	1456606.554	2428698.389
138	1456556.499	2428720.113
139	1456551.624	2428761.548
140	1456483.048	2428739.003
141	1456576.016	2428698.656
142	1456582.438	2428695.869
143	1456592.529	2428691.490

**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
 TERMINAL GATE MARKING LAYOUT POINTS**



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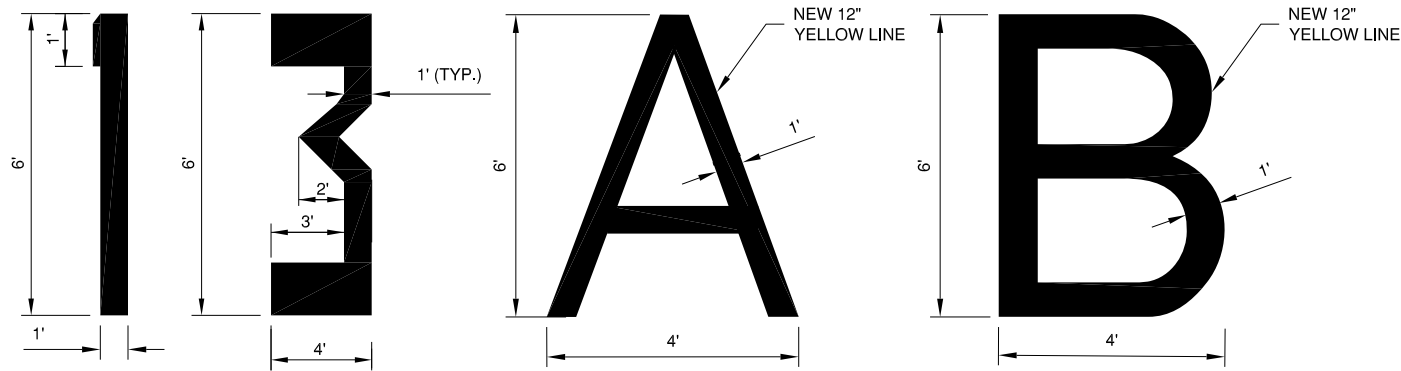


DESIGN BY:	KLB/JMW
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO. PIA-4485 AIP PROJ. NO. 3-17-0080-XX	
SHEET	37 OF 39 SHEETS

**PE099**

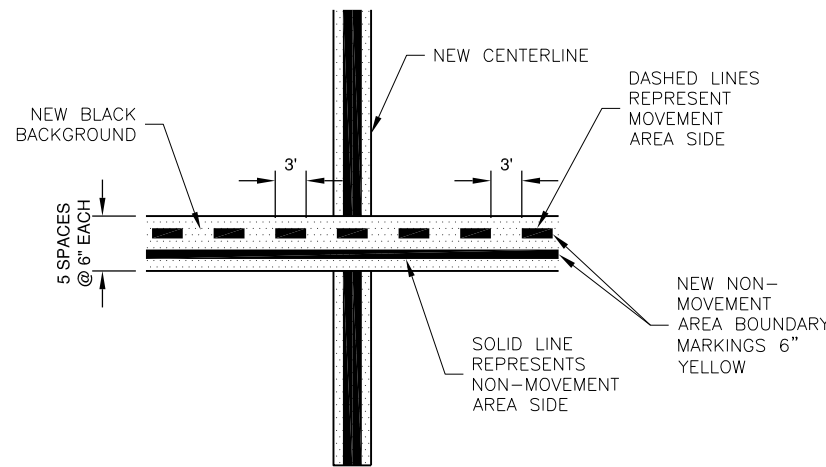
REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

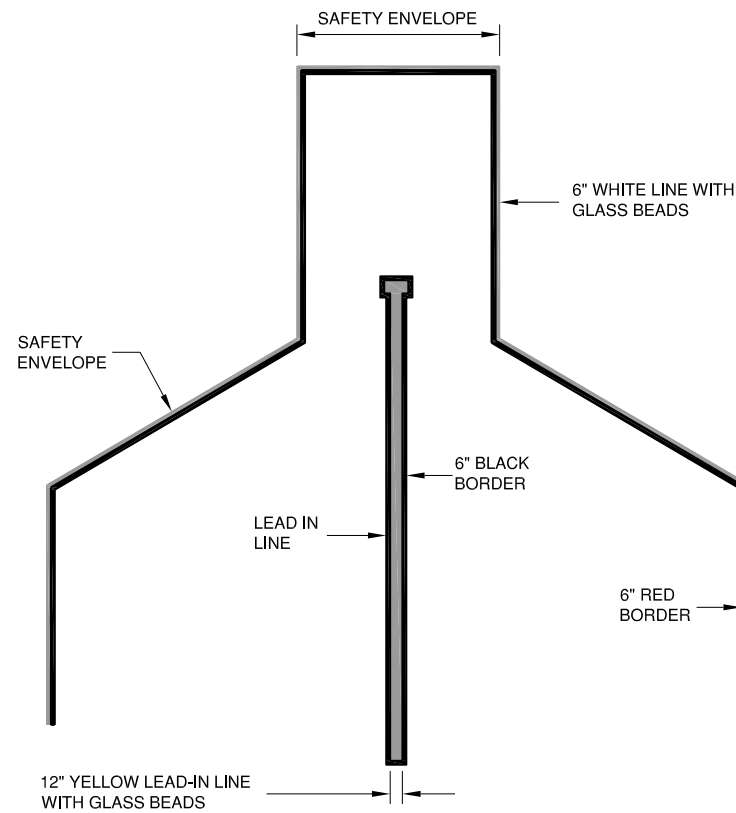


**NOTE:**  
 1. ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE MEDIA & 6" BLACK BORDER.  
 2. BLACK BORDER SHALL NOT REQUIRE REFLECTIVE MEDIA.

**J-LINE NUMERALS (TYP.)**  
 N.T.S.

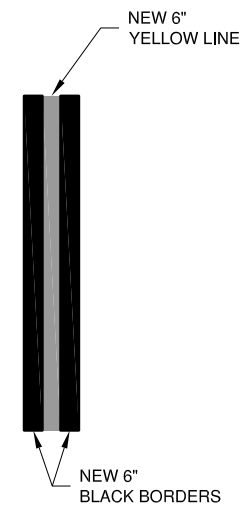


**NON-MOVEMENT AREA BOUNDARY MARKING**  
 N.T.S.

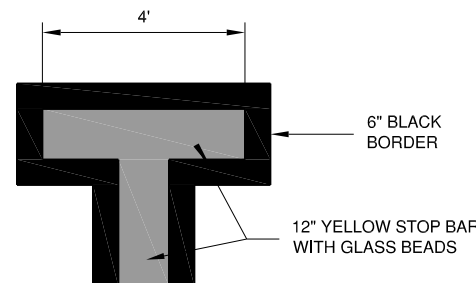


**TYPICAL LEAD-IN AND SAFETY ENVELOPE FOR CONCRETE SURFACE**  
 N.T.S.

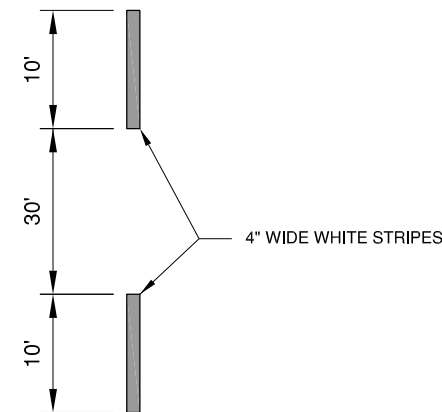
**NOTE:**  
 1. SEE MARKING PLANS FOR EXACT DIMENSIONS OF SAFETY ENVELOPES



**TAXIWAY CENTERLINE CONTINUOUS (YELLOW)**  
 N.T.S.



**TYPICAL STOP BAR FOR CONCRETE SURFACE**  
 N.T.S.



**DASHED ROADWAY CENTERLINE MARKING**  
 N.T.S.

**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
 MARKING DETAILS 1**



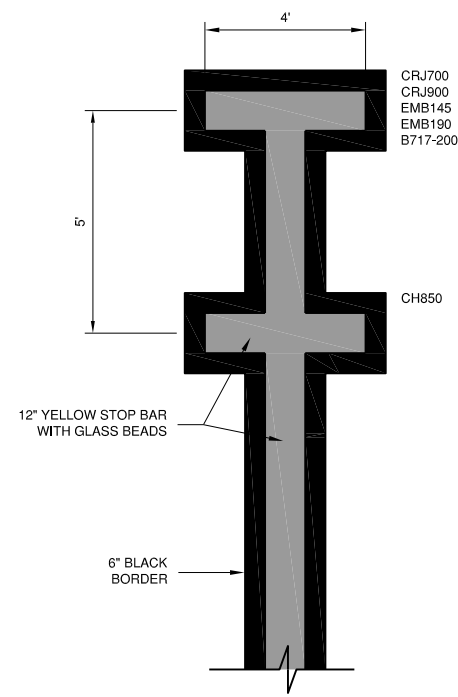
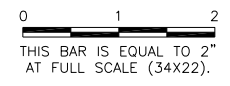
DESIGN BY: KLB  
 DRAWN BY: CMT  
 CHECKED BY: CBG  
 APPROVED BY: CBG  
 DATE: JULY 31, 2015  
 JOB No: 14061-03

IL PROJ. NO. PIA-4485  
 AIP PROJ. NO. 3-17-0080-XX

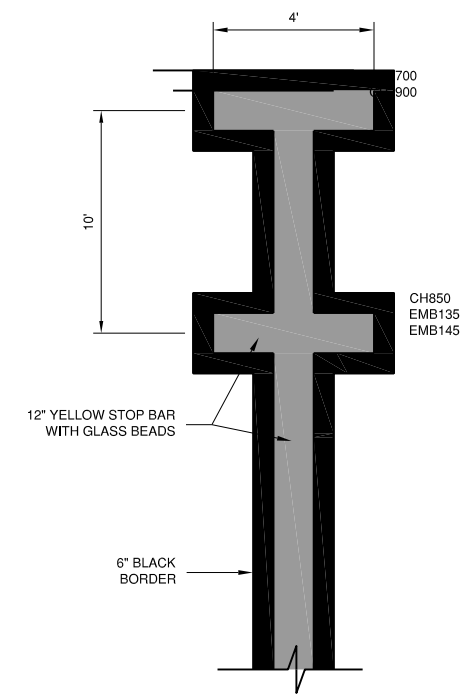
K:\Peoria\14061-03\_TermApronRehab\1\Draw\Sheets

**PE099**

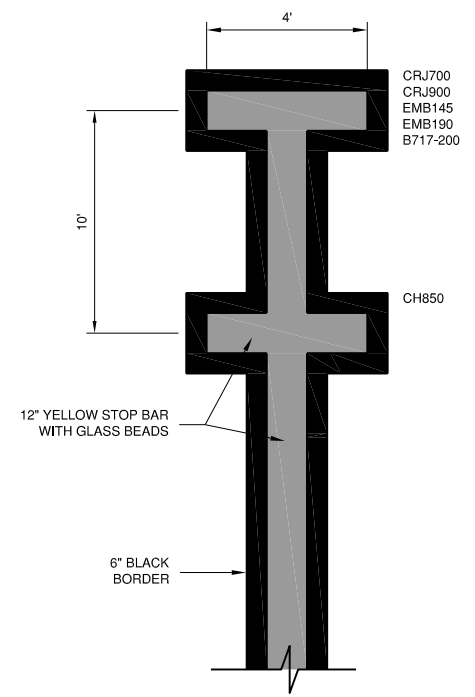
REVISIONS		
NUMBER	BY	DATE



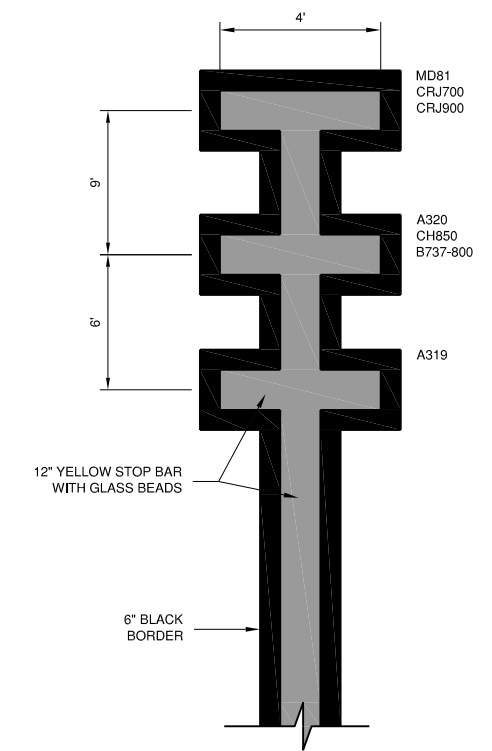
**STOP BAR DETAIL - GATE 4**  
N.T.S.



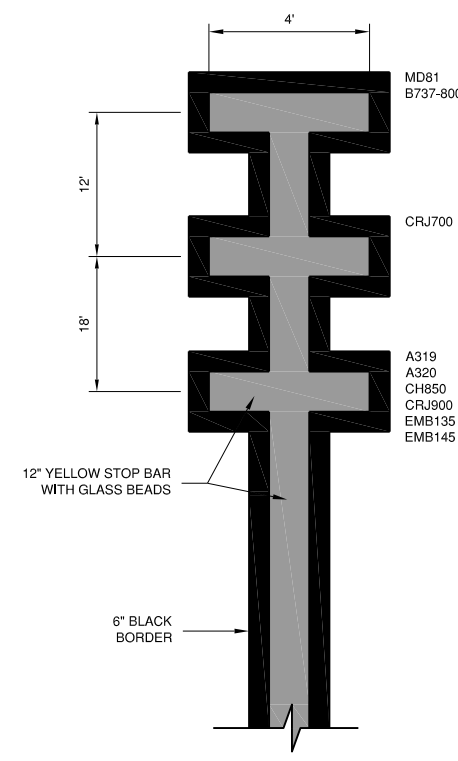
**STOP BAR DETAIL - GATES 5 & 6**  
N.T.S.



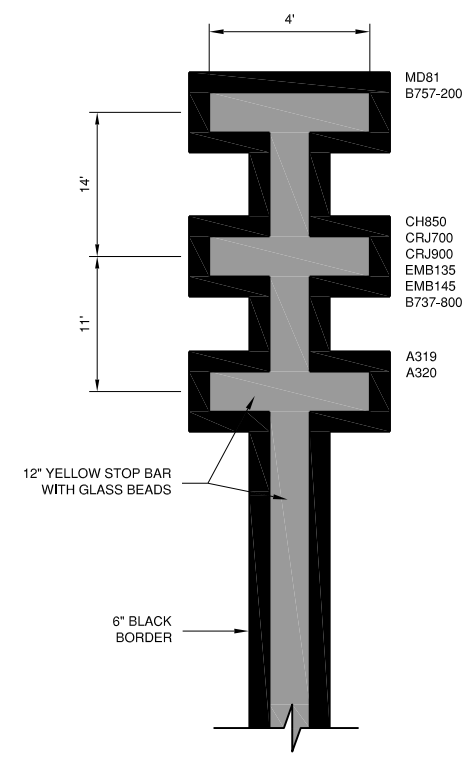
**STOP BAR DETAIL - GATES 8 & 9**  
N.T.S.



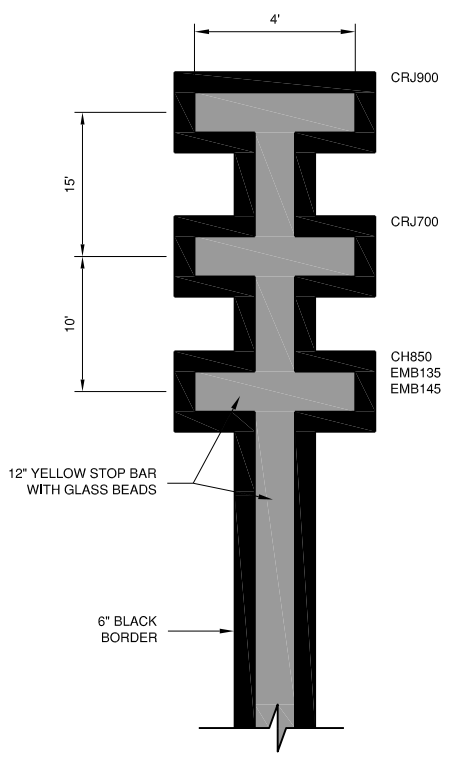
**STOP BAR DETAIL - GATE 10**  
N.T.S.



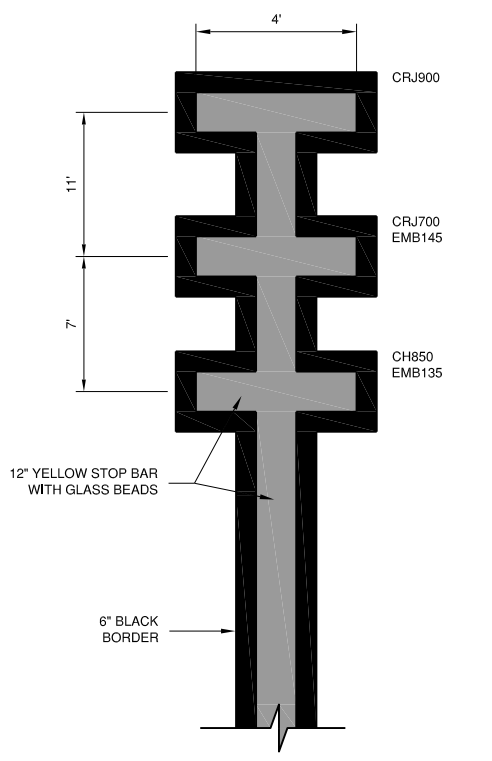
**STOP BAR DETAIL - GATE 12**  
N.T.S.



**STOP BAR DETAIL - GATE 14**  
N.T.S.



**STOP BAR DETAIL - GATE 12A**  
N.T.S.



**STOP BAR DETAIL - GATE 12B**  
N.T.S.

- NOTE:**
1. VERIFY WITH AIRPORT PRIOR TO MARKING THE AIRCRAFT TYPE AT STOP BARS.
  2. AIRCRAFT TYPE MARKING TO BE WHITE LETTERS WITH BLACK BACKGROUND.
  3. AIRCRAFT TYPE MARKING SHALL BE PAINTED 180° ROTATED FROM WHAT IS SHOWN ON THIS SHEET.

**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**RECONSTRUCT TERMINAL APRON PHASE 1  
 MARKING DETAILS 2**



DESIGN BY:	KLB
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CBG
DATE:	JULY 31, 2015
JOB No:	14061-03
IL PROJ. NO.	PIA-4485
AIP PROJ. NO.	3-17-0080-XX
SHEET	39 OF 39 SHEETS

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