

# CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS

## CONSTRUCTION PLANS FOR

## CHICAGO EXECUTIVE AIRPORT



### REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALCMS

ILLINOIS PROJECT: PWK-4843  
 S.B.G. PROJECT: 3-17-SBGP-TBD

JUNE 25, 2021



LICENSE EXPIRATION  
 DATE: 11/30/2021  
 DATE SIGNED: 06/24/21



LICENSE EXPIRATION  
 DATE: 11/30/2021  
 DATE SIGNED: 06/24/21

**811** Know what's below.  
 Call before you dig.  
 J.U.L.I.E.  
 JOINT UTILITY LOCATING  
 INFORMATION FOR EXCAVATORS  
 www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

**CHICAGO EXECUTIVE AIRPORT**

TOWNSHIP: 42 NORTH      WHEELING TOWNSHIP  
 RANGE: 11 EAST      (SECTION: 13)  
 COOK COUNTY

**CMT** 20029004.00  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 062-069052

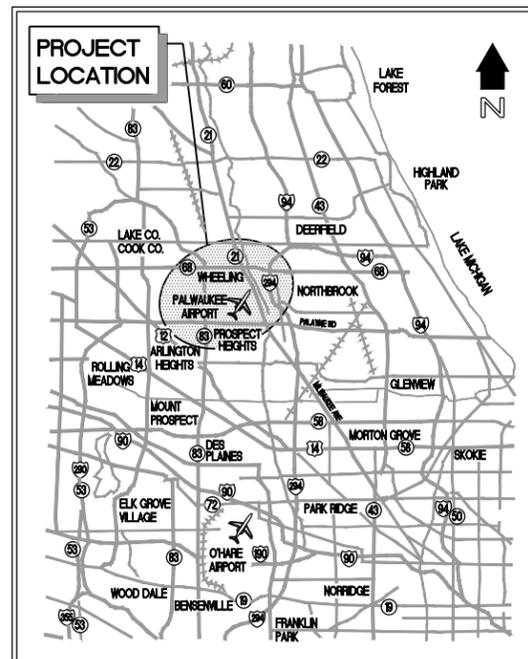
SUBMITTED BY \_\_\_\_\_  
 KRIS SALVATERA, PE

DATE 6/24/21

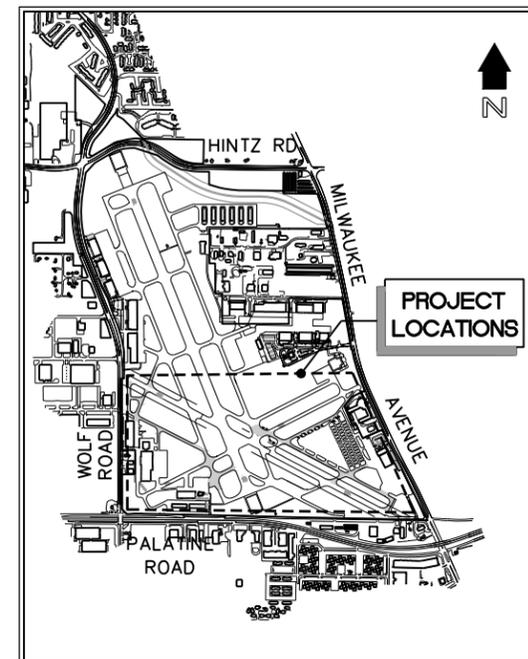
**CHICAGO EXECUTIVE AIRPORT**

APPROVED \_\_\_\_\_  
 GEORGE SAKAS INTERIM EXECUTIVE DIRECTOR

DATE 6/24/21



LOCATION MAP



SITE PLAN

**PROJECT INFORMATION**

CONTRACTOR:  
 RESIDENT ENGINEER:  
 ORIGINAL CONTRACT AMOUNT:  
 FINAL CONSTRUCTION COST:  
 IDOT LETTING DATE:  
 IDOT AWARD DATE:  
 NOTICE TO PROCEED:  
 START OF CONSTRUCTION:  
 SUBSTANTIAL COMPLETION:

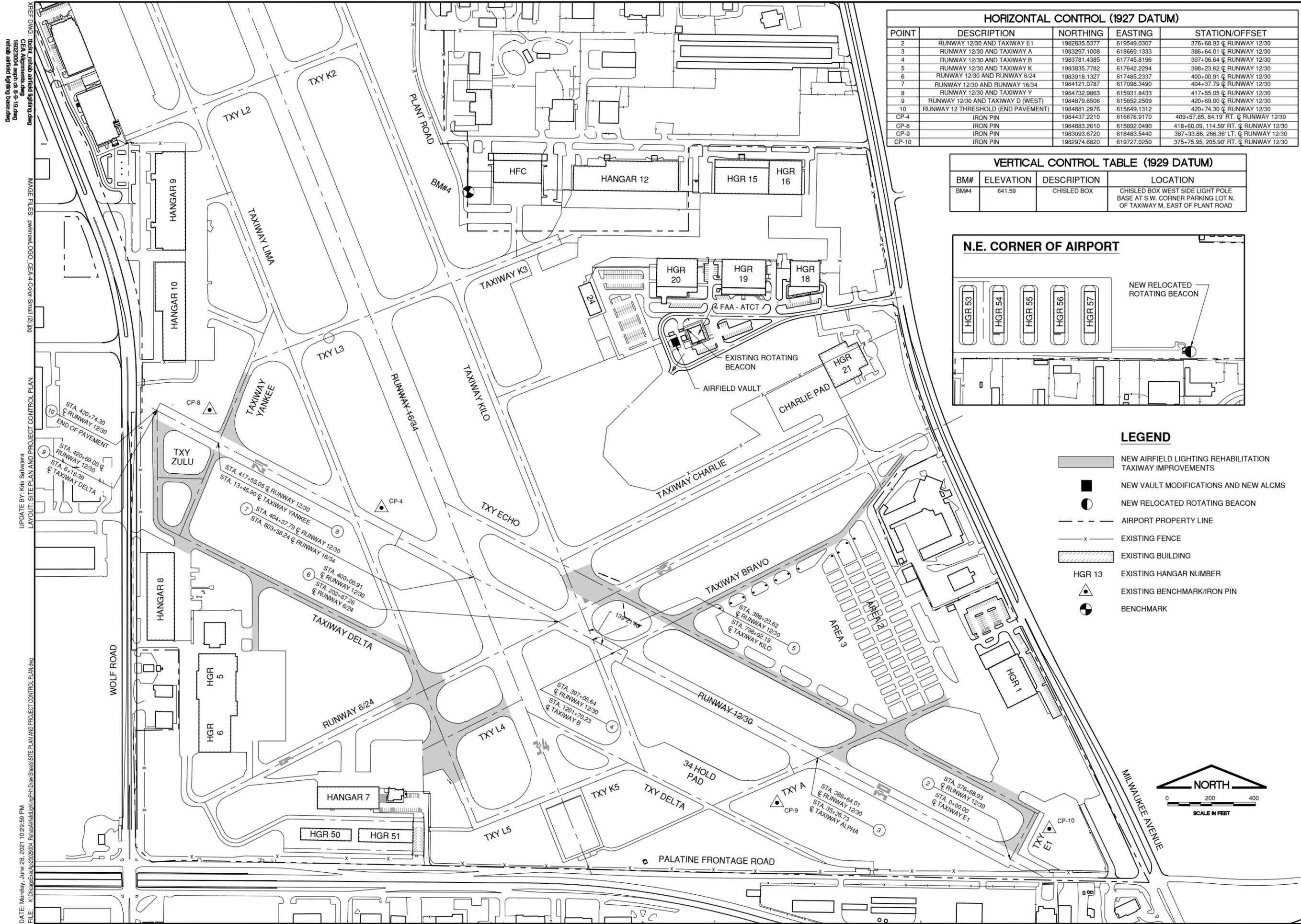
**LOCAL AGENCY CONTACT INFORMATION**

VILLAGE OF WHEELING - 847.459.2600  
 CITY OF PROSPECT HEIGHTS - 847.398.6070

**ENGINEER'S PROJECT PERMIT LOG**

NPDES #  
 FAA AIRSPACE #  
 CCDD LPC-663 DATED  
 MWRDGC PERMIT # 03-246 & RL 09-063  
 VILLAGE APP FOR CONSTRUCTION PERMIT #  
 VILLAGE FLOODPLAIN PERMIT #  
 CONTRACTOR'S REGISTRATION WITH VILLAGE  
 VILLAGE SITE ALTERATION PERMIT #  
 CITY APPLICATION FOR PERMIT #  
 CITY FLOODPLAIN PERMIT #  
 CITY SITE GRADING PERMIT #  
 CONTRACTOR'S REGISTRATION WITH CITY



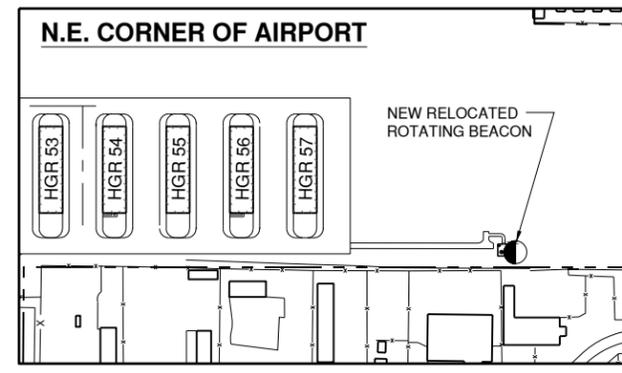


**HORIZONTAL CONTROL (1927 DATUM)**

POINT	DESCRIPTION	NORTHING	EASTING	STATION/OFFSET
2	RUNWAY 12/30 AND TAXIWAY E1	1982835.5377	619549.0307	376+68.93 @ RUNWAY 12/30
3	RUNWAY 12/30 AND TAXIWAY A	1983297.1008	618669.1333	386+64.01 @ RUNWAY 12/30
4	RUNWAY 12/30 AND TAXIWAY B	1983781.4385	617745.8196	397+06.64 @ RUNWAY 12/30
5	RUNWAY 12/30 AND TAXIWAY K	1983835.7782	617642.2294	398+23.62 @ RUNWAY 12/30
6	RUNWAY 12/30 AND RUNWAY 6/24	1983918.1327	617485.2337	400+00.91 @ RUNWAY 12/30
7	RUNWAY 12/30 AND RUNWAY 16/34	1984121.0787	617098.3490	404+37.79 @ RUNWAY 12/30
8	RUNWAY 12/30 AND TAXIWAY Y	1984732.9863	615931.8433	417+55.05 @ RUNWAY 12/30
9	RUNWAY 12/30 AND TAXIWAY D (WEST)	1984879.6506	615652.2509	420+69.00 @ RUNWAY 12/30
10	RUNWAY 12 THRESHOLD (END PAVEMENT)	1984881.2976	615649.1312	420+74.30 @ RUNWAY 12/30
CP-4	IRON PIN	1984437.2210	616676.9170	409+57.85, 84.19' RT. @ RUNWAY 12/30
CP-8	IRON PIN	1984883.2610	615892.0490	418+60.09, 114.59' RT. @ RUNWAY 12/30
CP-9	IRON PIN	1983093.6720	618483.5440	387+33.86, 266.36' L.T. @ RUNWAY 12/30
CP-10	IRON PIN	1982974.6820	619727.0250	375+75.95, 205.90' RT. @ RUNWAY 12/30

**VERTICAL CONTROL TABLE (1929 DATUM)**

BM#	ELEVATION	DESCRIPTION	LOCATION
BM#4	641.59	CHISLED BOX	CHISLED BOX WEST SIDE LIGHT POLE BASE AT S.W. CORNER PARKING LOT N. OF TAXIWAY M, EAST OF PLANT ROAD



- LEGEND**
- NEW AIRFIELD LIGHTING REHABILITATION TAXIWAY IMPROVEMENTS
  - NEW VAULT MODIFICATIONS AND NEW ALCMS
  - NEW RELOCATED ROTATING BEACON
  - AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING BUILDING
  - HGR 13 EXISTING HANGAR NUMBER
  - EXISTING BENCHMARK/IRON PIN
  - BENCHMARK

IL CONTRACT: **PA063**  
 IL LETTING ITEM: **08A**  
 IL PROJECT: **PWK-4843**  
 S.B.G. PROJECT:  
 SURVEY BOOK #  
 REVISIONS  
 NUMBER BY DATE  
 0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**CHICAGO EXECUTIVE AIRPORT**  
**WHEELING/PROSPECT HEIGHTS, ILLINOIS**  
**REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALCMS**  
**SITE PLAN AND PROJECT CONTROL PLAN**

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**CHICAGO EXECUTIVE AIRPORT**  
 DESIGN BY: KWS  
 DRAWN BY: JRO  
 CHECKED BY: KWS  
 APPROVED BY: DKP  
 DATE: 06/25/2021  
 JOB No: 20029004.00  
**FINAL**  
 SHEET 3 OF 35 SHEETS

DATE: Monday, June 28, 2021 10:29:59 PM  
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 UPDATE BY: Kris Savaterra  
 LAYOUT: SITE PLAN AND PROJECT CONTROL PLAN  
 REF: D:\CMT\Projects\2020\PA063\Draw Sheets\Site Plan and Project Control Plan.dwg  
 DATE: Monday, June 28, 2021 10:29:59 PM  
 FILE: K:\Chicago\2020\202004 - Rehabilitation\Draw Sheets\Site Plan and Project Control Plan.dwg



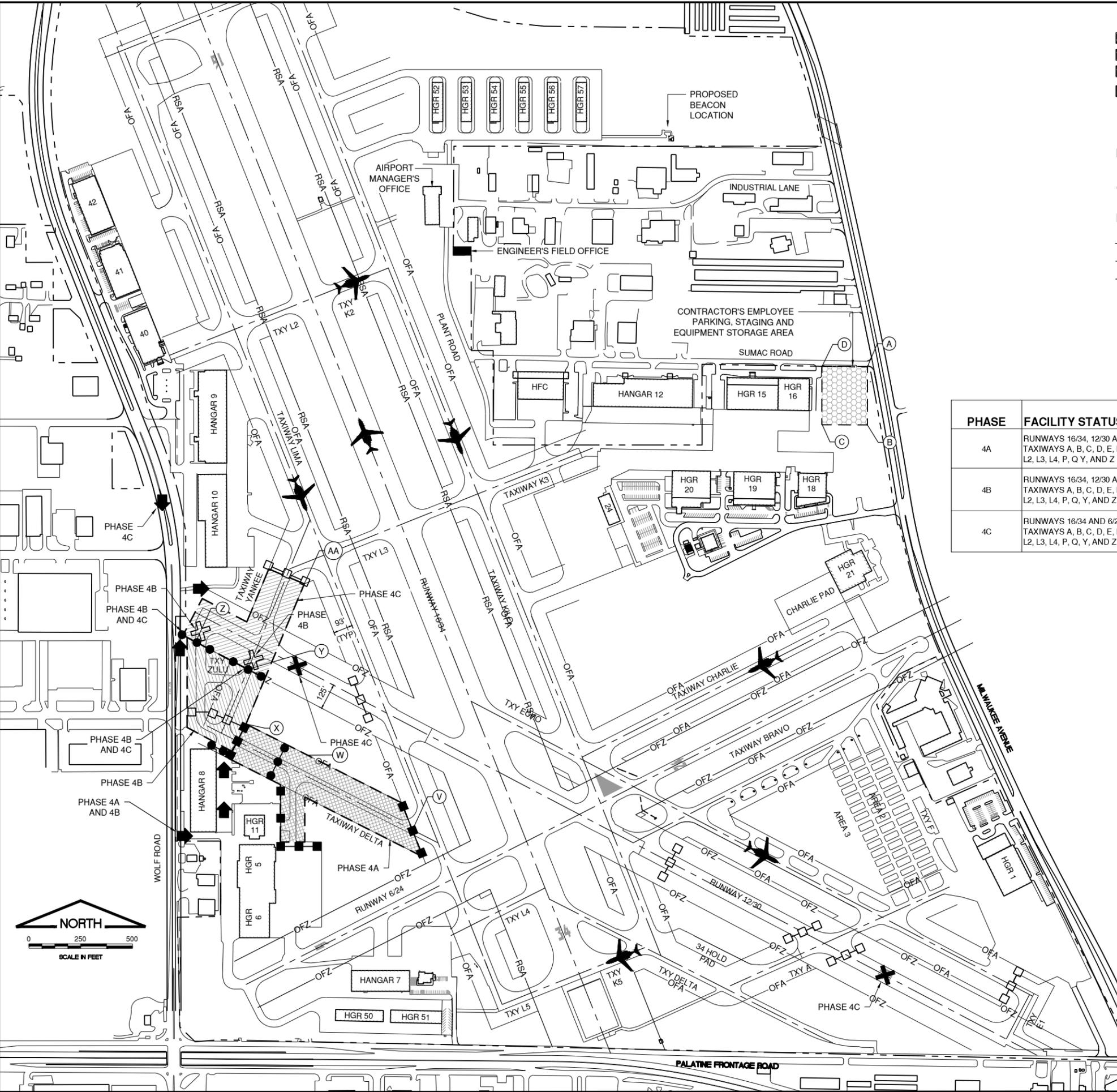








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 Update Date: 6/28/2021 10:30:59 AM  
 Update Reason: Revised lighting bases and  
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**LEGEND**

- PHASE 4A WORK AREA
- PHASE 4B WORK AREA
- PHASE 4C WORK AREA
- CONTRACTOR'S STAGING AREA
- AIRCRAFT MOVEMENT AREA
- LOW PROFILE BARRICADES WITH SIGNS FOR WORK AREA - PHASE 4A ("DO NOT ENTER" AND "AIRCRAFT MOVEMENT AREA")
- LOW PROFILE BARRICADES WITH SIGNS FOR WORK AREA - PHASE 4B ("DO NOT ENTER" AND "AIRCRAFT MOVEMENT AREA")
- LOW PROFILE BARRICADES WITH SIGNS FOR WORK AREA - PHASE 4C ("DO NOT ENTER" AND "AIRCRAFT MOVEMENT AREA")
- RUNWAY OBSTACLE FREE ZONE (OFZ)
- RUNWAY SAFETY AREA (RSA)
- TAXIWAY OBJECT FREE AREA (OFA)
- CRITICAL POINT
- TEMPORARY CLOSED RUNWAY MARKER
- TEMPORARY CLOSED TAXIWAY MARKER
- CONTRACTOR'S ACCESS/HAUL ROAD

PHASE	FACILITY STATUS - OPEN	FACILITY STATUS - CLOSED
4A	RUNWAYS 16/34, 12/30 AND 6/24 TAXIWAYS A, B, C, D, E, K, K1, K2, K3, L, L1, L2, L3, L4, P, Q, Y, AND Z	TAXIWAY D EAST OF TAXIWAY Y
4B	RUNWAYS 16/34, 12/30 AND 6/24 TAXIWAYS A, B, C, D, E, K, K1, K2, K3, L, L1, L2, L3, L4, P, Q, Y, AND Z	TAXIWAY D WEST OF TAXIWAY Y TAXIWAY Y AND Z SOUTH OF RUNWAY 12/30
4C	RUNWAYS 16/34 AND 6/24 TAXIWAYS A, B, C, D, E, K, K1, K2, K3, L, L1, L2, L3, L4, P, Q, Y, AND Z	RUNWAY 12/30 TAXIWAY Y SOUTH OF RUNWAY 12/30 AND NORTH OF RUNWAY 12/30

**PHASE 4A SEQUENCE OF CONSTRUCTION**

- COORDINATE CLOSURE OF TAXIWAY D WITH RESIDENT ENGINEER. GATE ACCESS AND VEHICLE ACCESS TO NON-MOVEMENT AIRCRAFT PAVEMENT AREAS SHALL BE APPROVED BY THE AIRPORT.
- MARK AIR OPERATIONS AREA (A.O.A.) WITH LATHE AND RIBBON AND PLACE BARRICADES FOR TAXIWAY CLOSURES.
- CONSTRUCT PROPOSED IMPROVEMENTS IN PHASE 4A WORK AREA INCLUDING AIRFIELD LIGHTING.
- UPON COMPLETION OF WORK ITEMS, CLEAN PAVEMENTS, REMOVE BARRICADES, SET UP PHASE 4B WORK AREA, AND RE-OPEN PHASE 4A WORK AREA.

**PHASE 4B SEQUENCE OF CONSTRUCTION**

- COORDINATE CLOSURE OF TAXIWAYS D, Y, AND Z WITH RESIDENT ENGINEER. GATE ACCESS AND VEHICLE ACCESS TO NON-MOVEMENT AIRCRAFT PAVEMENT AREAS SHALL BE APPROVED BY THE AIRPORT.
- MARK AIR OPERATIONS AREA (A.O.A.) WITH LATHE AND RIBBON AND PLACE BARRICADES FOR TAXIWAY CLOSURES.
- CONSTRUCT PROPOSED IMPROVEMENTS IN PHASE 4B WORK AREA INCLUDING AIRFIELD LIGHTING.
- UPON COMPLETION OF WORK ITEMS, CLEAN PAVEMENTS, REMOVE BARRICADES, SET UP PHASE 4C WORK AREA, AND RE-OPEN PHASE 4B WORK AREA.

**PHASE 4C SEQUENCE OF CONSTRUCTION**

- COORDINATE CLOSURE OF RUNWAY 12/30 AND TAXIWAY Y WITH RESIDENT ENGINEER. GATE ACCESS AND VEHICLE ACCESS TO NON-MOVEMENT AIRCRAFT PAVEMENT AREAS SHALL BE APPROVED BY THE AIRPORT.
- MARK AIR OPERATIONS AREA (A.O.A.) WITH LATHE AND RIBBON AND PLACE BARRICADES FOR TAXIWAY CLOSURES.
- CONSTRUCT PROPOSED IMPROVEMENTS IN PHASE 2C WORK AREA INCLUDING AIRFIELD LIGHTING.
- UPON COMPLETION OF WORK ITEMS, CLEAN PAVEMENTS, REMOVE BARRICADES, AND RE-OPEN TAXIWAYS AND RUNWAY 12/30.

IL CONTRACT: **PA063**  
 IL LETTING ITEM: **08A**  
 IL PROJECT: **PWK-4843**  
 S.B.G. PROJECT:

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**CHICAGO EXECUTIVE AIRPORT**  
**WHEELING/PROSPECT HEIGHTS, ILLINOIS**  
**REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALOMS**  
**SEQUENCE OF CONSTRUCTION - SHEET 6**

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**CHICAGO EXECUTIVE AIRPORT**

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	KWS
APPROVED BY:	DKP
DATE:	06/25/2021
JOB No:	20029004.00

**FINAL**

SHEET 9 OF 35 SHEETS

**PHASE 4A, 4B AND 4C**





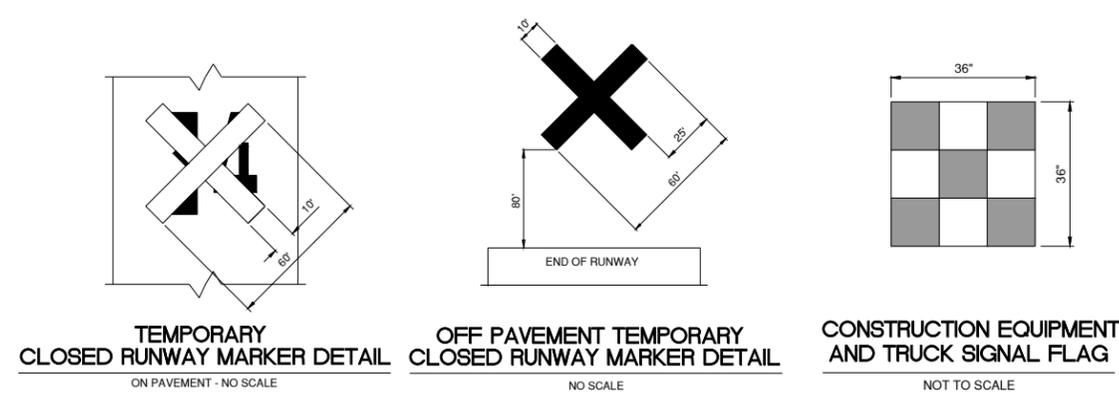
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 UPDATE BY: Kris Salvatera  
 LAYOUT: SEQUENCE OF CONSTRUCTION - GENERAL NOTES AND DETAILS - SHEET 2.dwg  
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**PHASING NOTES (ALL PHASES)**

1. THE INTENT OF THE PHASING PLANS IS TO MINIMIZE THE IMPACT OF CONSTRUCTION ON THE OPERATION OF THE AIRPORT. THE CONTRACTOR SHALL CONSTRUCT THE PROJECT IN CONSECUTIVE PHASES AS OUTLINED IN THE PLANS UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT EXECUTIVE DIRECTOR.
2. PRIOR TO REOPENING A CLOSED RUNWAY, THE ENTIRE (RSA) RUNWAY SAFETY AREA (250 FEET FROM CENTERLINE AND INCLUDING BEYOND THE END OF THE RUNWAY WITHIN AIRPORT PROPERTY) AND (TOFA) TAXIWAY OBJECT FREE AREA MUST MEET FAA CRITERIA. FAA CRITERIA REQUIRES THAT THERE BE NO OPEN EXCAVATIONS OR TRENCHES IN THE SAFETY AREA(S), THE MAXIMUM PAVEMENT DROP OFF BE 3 INCHES, AND ALL GRADES IN ANY DIRECTION BE LESS THAN 3 PERCENT. STEEL PLATES, TEMPORARY WEDGING OF BASE COURSE AND BITUMINOUS CONCRETE MAY BE REQUIRED TO MEET CRITERIA. ALL NECESSARY TEMPORARY MEASURES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
3. THE CONTRACTOR SHALL SUBMIT A CONSTRUCTION SCHEDULE. STRICT ADHERENCE TO THE APPROVED SCHEDULE WILL BE ENFORCED TO AVOID CONFLICTS WITH OTHER CONSTRUCTION ACTIVITIES ON THE AIRPORT AND THE ADVERSE EFFECTS THEY COULD HAVE ON AIRPORT OPERATIONS.
4. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT STAFF TO SCHEDULE THE RUNWAY/TAXIWAY CLOSURES. ITEMS SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT DEPENDABILITY AND MANPOWER AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THIS CRITICAL CLOSURE. THE AIRPORT EXECUTIVE DIRECTOR AND THE CONTRACTOR SHALL MUTUALLY AGREE ON THE EXACT DATES AND TIMES OF THE CLOSURE(S).
5. CONTRACTOR MUST MAINTAIN ACCESS TO ALL ACTIVE AND OPEN AREAS AT ALL TIMES. CONTRACTOR SHALL RELOCATE EQUIPMENT AT NO ADDITIONAL COST TO CONTRACT TO ALLOW AIRCRAFT TO PASS. CONTRACTOR SHALL COORDINATE CONSTRUCTION OPERATIONS AT ALL ACTIVE AND OPEN AREAS TO PROVIDE MINIMAL DISRUPTIONS TO AIRCRAFT MOVEMENT IN THAT AREA.
6. FAA AND AIRPORT ACCESS ROAD(S) SHALL NOT BE USED AS A HAUL ROAD BY THE CONTRACTOR WITHOUT PRIOR APPROVAL.
7. TO THE EXTENT POSSIBLE THE CONTRACTOR SHALL HAVE ALL EMPLOYEE PARKING OUTSIDE OF AIRPORT FENCE OR AS INDICATED AT THE LOCATION SHOWN.
8. THE AIRPORT RESERVES THE RIGHT TO MODIFY THE SEQUENCE OF CONSTRUCTION INCLUDING BUT NOT LIMITED TO PHASING, WORK AREAS, BARRICADE PLACEMENT, ACCESS AND HAUL ROUTES, AND CONTRACTOR MOVEMENTS AT ANY TIME DURING THE PROJECT WITH FAA, IDA AND ATCT APPROVAL.
9. CONTRACTOR MAY REQUEST TO COMBINE WORK PHASES/AREAS. THE AIRPORT WILL DETERMINE IF THE REQUEST IS ACCEPTABLE.
10. WHEN HAUL ROUTE IS IN USE ON ACTIVE AIRFIELD PAVEMENTS, THE CONTRACTOR WILL BE REQUIRED TO BE UNDER CONTROL BY A CROSSING GUARD OR ESCORT IN RADIO CONTACT WITH THE ATCT FOR CONSTRUCTION PERSONNEL, CONSTRUCTION TRAFFIC, CONTRACTORS VEHICLES AND EQUIPMENT CROSSING BY, TO OR FROM WORK ZONE. STOP SIGNS SHALL BE IN PLACE AT ALL TIMES IN THIS AREA. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.
11. TEMPORARY CLOSED TAXIWAY MARKERS ARE ONLY REQUIRED WHEN THE TAXIWAY WILL REMAIN CLOSED FOR 3 CONSECUTIVE DAYS OR MORE.

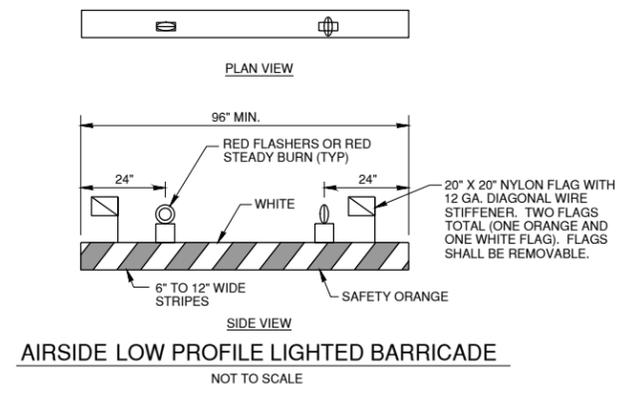
**AIRFIELD LIGHTS AND SIGNS NOTES**

1. CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
2. CONTRACTOR SHALL TURN OFF RUNWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES, CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY. THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.



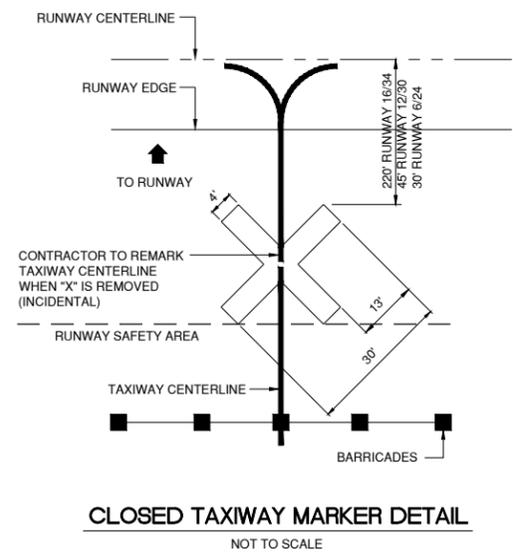
**CLOSED RUNWAY MARKER NOTES**

1. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL.
2. FOR RUNWAY CLOSURES INVOLVING A SINGLE RUNWAY, IT IS ANTICIPATED THAT THE AIRPORT SHALL MOBILIZE THE AIRPORT OWNED LIGHTED "X'S" ON EACH END OF THE CLOSED RUNWAY. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN THE LIGHTED "X'S" DURING EACH CLOSURE PERIOD. CONTRACTOR SUPPLIED MAINTENANCE SHALL INCLUDE, BUT NOT BE LIMITED TO, FUELING, REPLACING LAMPS, CHECKING AND SUPPLYING OIL, ETC. IF ANY DAMAGE IS INCURRED TO THE AIRPORT LIGHTED "X'S" DURING CONSTRUCTION BY THE CONTRACTOR, THE CONTRACTOR SHALL REPLACE THE LIGHTED "X" IN KIND AT NO COST TO THE CONTRACT OR AIRPORT.
3. FOR RUNWAY CLOSURES INVOLVING MORE THAN ONE RUNWAY, OR IF THE AIRPORT OWNED LIGHTED "X'S" ARE NOT AVAILABLE, THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING YELLOW CROSSES AT THE LOCATION AND DIMENSIONS DETAILED ON THE SEQUENCE OF CONSTRUCTION AND PER AC 150/5370-2 (LATEST EDITION), THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.
4. TEMPORARY CLOSED RUNWAY MARKERS SHALL BE YELLOW.
5. TEMPORARY MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
6. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
7. MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
8. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



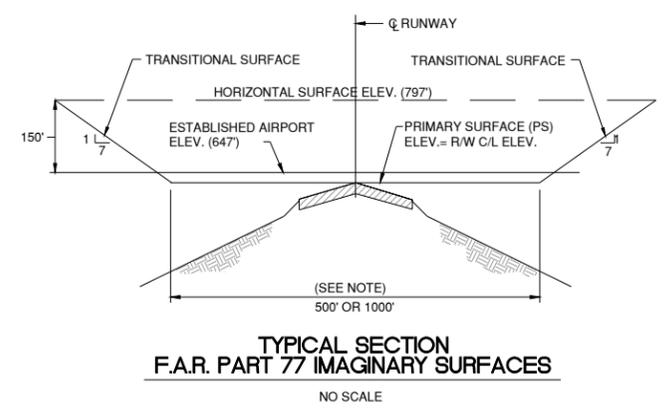
**BARRICADE NOTES**

1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

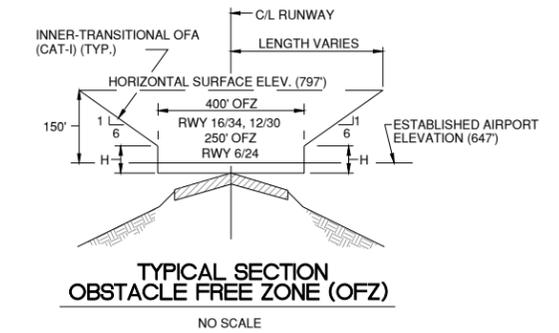


**CLOSED TAXIWAY MARKER DETAIL NOTES**

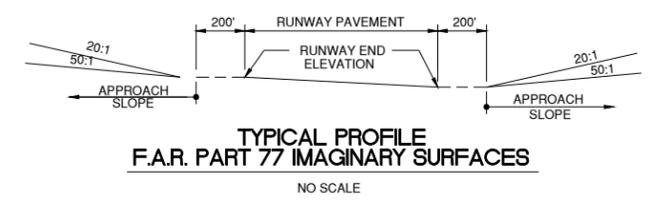
1. CLOSED TAXIWAY MARKERS SHALL BE PAINTED YELLOW WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OBJECT FREE AREA CRITERIA AND RUNWAY SAFETY AREA CRITERIA PER ADVISORY CIRCULAR 150/5300-13A (LATEST EDITION) AND ARE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT.
2. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
3. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. PLACE MARKERS OVER TAXIWAY CENTERLINE.
5. MARKERS SHALL BE ADEQUATELY SECURED TO PREVENT MOVEMENT BY PROPELLER WASH, JET BLAST OR OTHER WIND CURRENTS.
6. MARKERS ARE ONLY REQUIRED FOR CLOSURES EXCEEDING 72 HOURS.



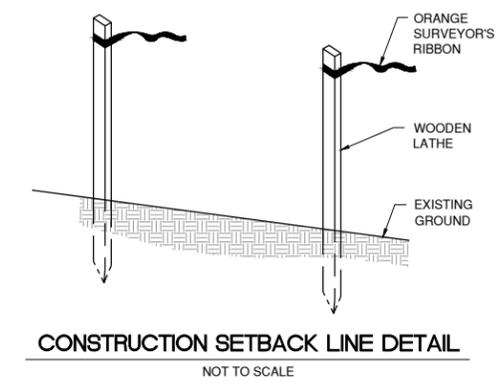
**NOTE:**  
IMAGINARY SURFACE REQUIREMENTS FOR EXISTING ACTIVE RUNWAYS (R/W) ARE SIMILAR EXCEPT PRIMARY SURFACE (PS) DIMENSIONS VARY R/W 6/24 & 12/30 500' PS (250' LT & RT OF C/L) R/W 16/34 1000' PS (500' LT & RT OF C/L)



RUNWAY	TYPE OF RUNWAY	H (FEET)
16-34	PRECISION (CAT I)	48
6-24	VISUAL	150



RUNWAY END	ELEVATION	APPROACH SLOPE
16	643	50:1
34	644	20:1
12	645	20:1
30	640	20:1
6	647	20:1
24	638	20:1



**CONSTRUCTION SETBACK NOTES**

1. CONTRACTOR SHALL MARKER THE RUNWAY SAFETY AREA, TAXIWAY OBJECT FREE AREA AND RUNWAY OBSTACLE FREE ZONE PER THE CONSTRUCTION SETBACK DETAIL AS DIRECTED BY THE RESIDENT ENGINEER.
2. ALL COST ASSOCIATED WITH THE CONSTRUCTION SETBACK LINE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

IL CONTRACT: **PA063**  
 IL LETTING ITEM: **08A**  
 IL PROJECT: **PWK-4843**  
 S.B.G. PROJECT:

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT  
 WHEELING/PROSPECT HEIGHTS, ILLINOIS  
 REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALOMS  
 SEQUENCE OF CONSTRUCTION  
 GENERAL NOTES AND DETAILS - SHEET 2

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**CHICAGO EXECUTIVE AIRPORT**

DESIGN BY: KWS  
 DRAWN BY: JRO  
 CHECKED BY: KWS  
 APPROVED BY: DKP  
 DATE: 06/25/2021  
 JOB No: 20029004.00

**FINAL**

SHEET 11 OF 35 SHEETS

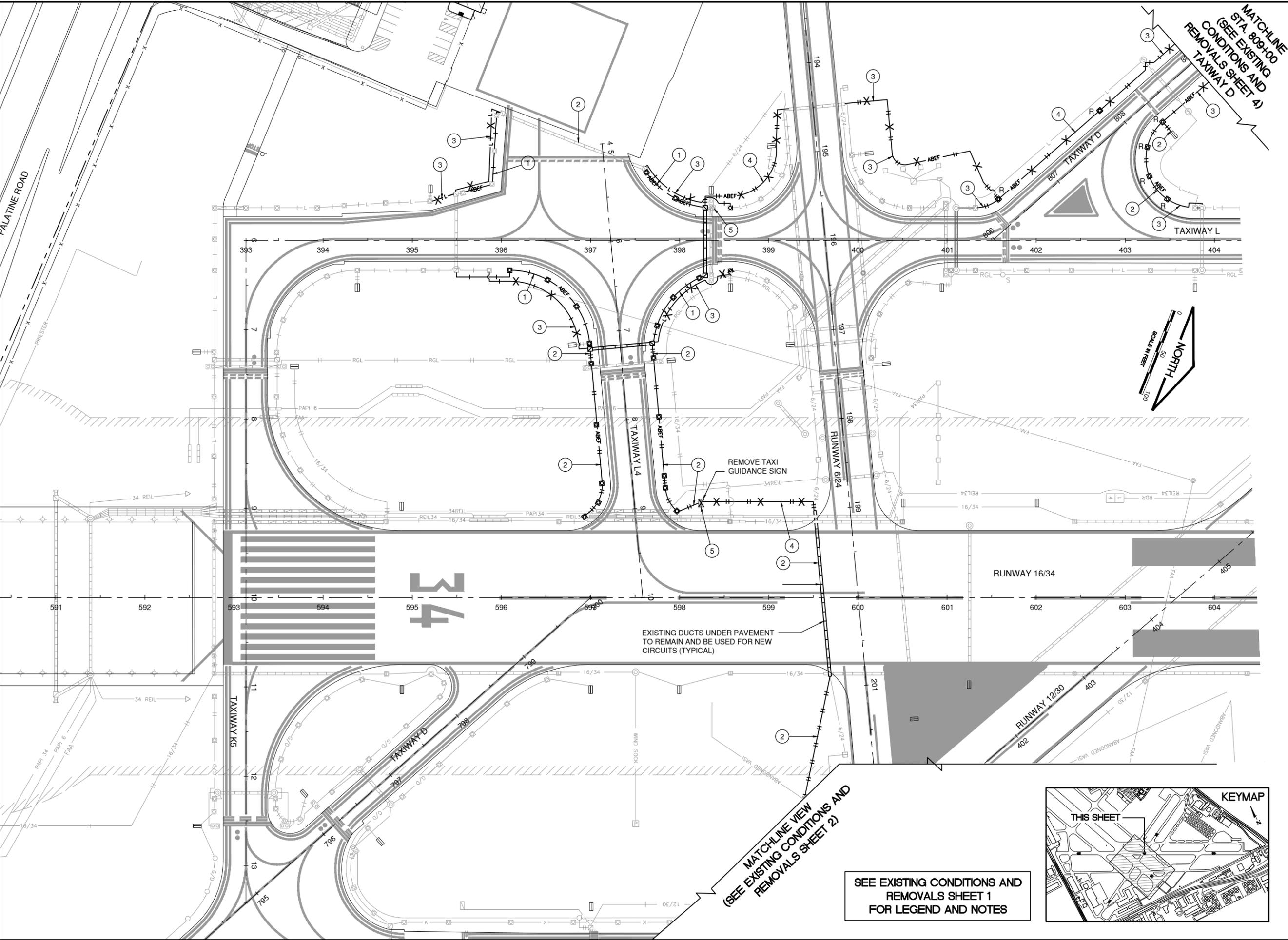








DATE: Monday, June 28, 2021 10:32:04 PM  
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 UPDATE BY: Kris Salvatera  
 LAYOUT: EXISTING CONDITIONS AND REMOVALS - SHEET 3  
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 retable airfield lighting bases.dwg  
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 PALATINE ROAD  
 PRESTER



IL CONTRACT: PA063  
 IL LETTING ITEM: 08A  
 IL PROJECT: PWK-4843  
 S.B.G. PROJECT:

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT  
 WHEELING/PROSPECT HEIGHTS, ILLINOIS  
 REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALOMS

**EXISTING CONDITIONS AND REMOVALS - SHEET 3**

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**CHICAGO EXECUTIVE AIRPORT**

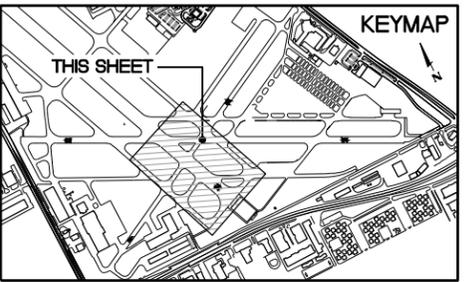
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 DRAWN BY: JRO  
 CHECKED BY: KWS  
 APPROVED BY: DKP  
 DATE: 06/25/2021  
 JOB No: 20029004.00

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SHEET 16 OF 35 SHEETS

MATCHLINE VIEW  
 (SEE EXISTING CONDITIONS AND REMOVALS SHEET 2)

SEE EXISTING CONDITIONS AND REMOVALS SHEET 1 FOR LEGEND AND NOTES



EXISTING DUCTS UNDER PAVEMENT TO REMAIN AND BE USED FOR NEW CIRCUITS (TYPICAL)

REMOVE TAXI GUIDANCE SIGN

MATCHLINE  
 STA. 809+00  
 (SEE EXISTING CONDITIONS AND REMOVALS SHEET 4)

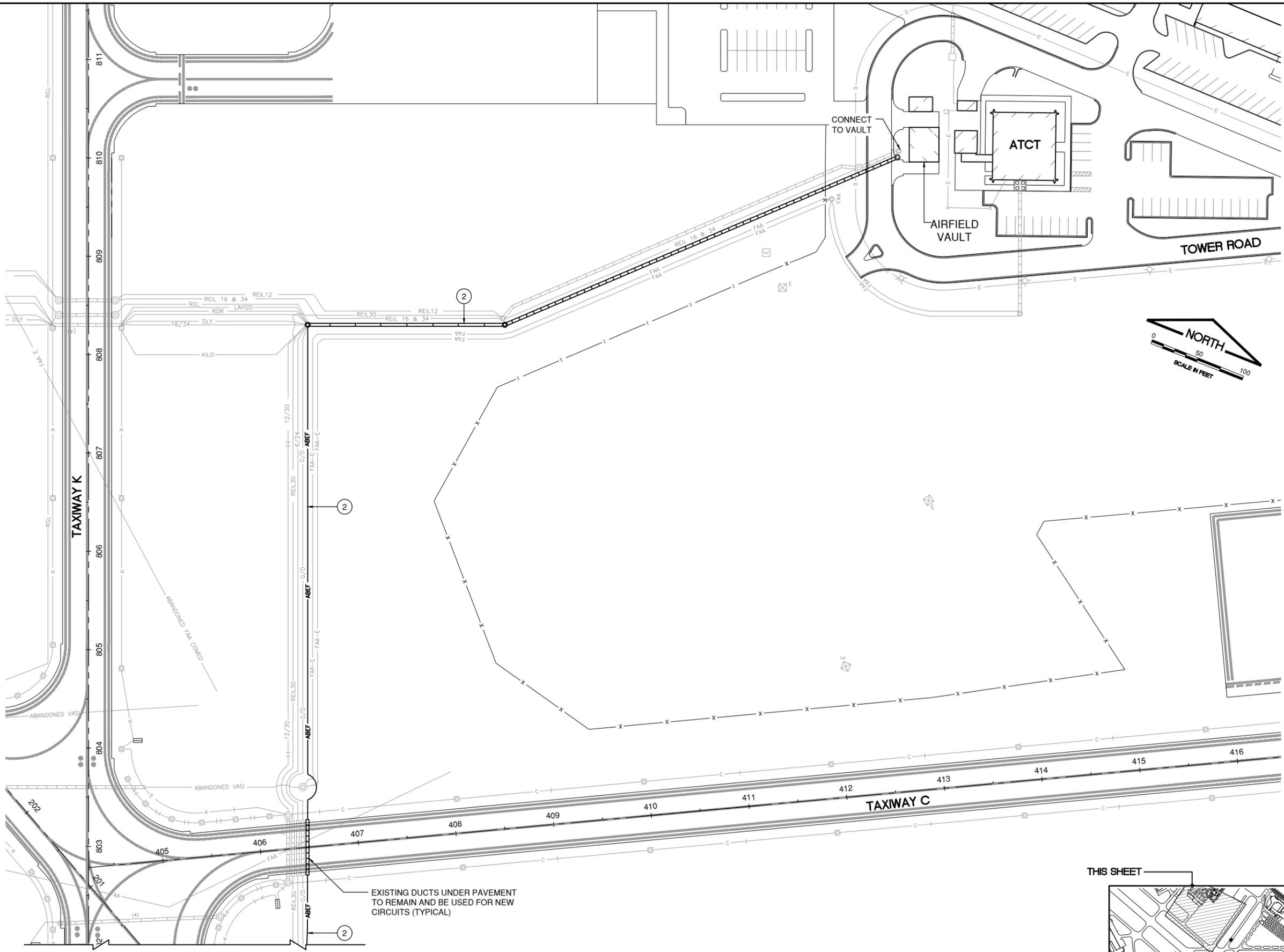


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 Rehab Airfield Lighting Base.dwg

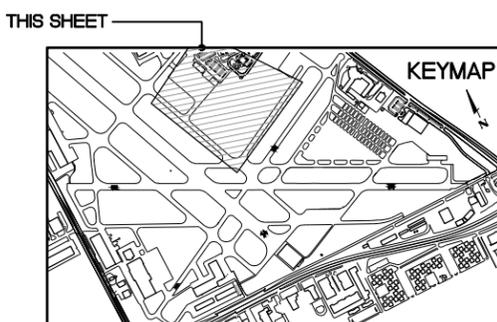
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UPDATE BY: **Kris Salvatera**  
 LAYOUT: **Layout6**

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SEE EXISTING CONDITIONS AND REMOVALS SHEET 1 FOR LEGEND AND NOTES



IL CONTRACT: **PA063**  
 IL LETTING ITEM: **08A**  
 IL PROJECT: **PWK-4843**  
 S.B.G. PROJECT:

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
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CHICAGO EXECUTIVE AIRPORT  
 WHEELING/PROSPECT HEIGHTS, ILLINOIS  
 REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALOMS  
**EXISTING CONDITIONS AND REMOVALS - SHEET 5**

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**CHICAGO EXECUTIVE AIRPORT**

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CHECKED BY:	KWS
APPROVED BY:	DKP
DATE:	06/25/2021
JOB No:	20029004.00
<b>FINAL</b>	
SHEET	18 OF 35 SHEETS

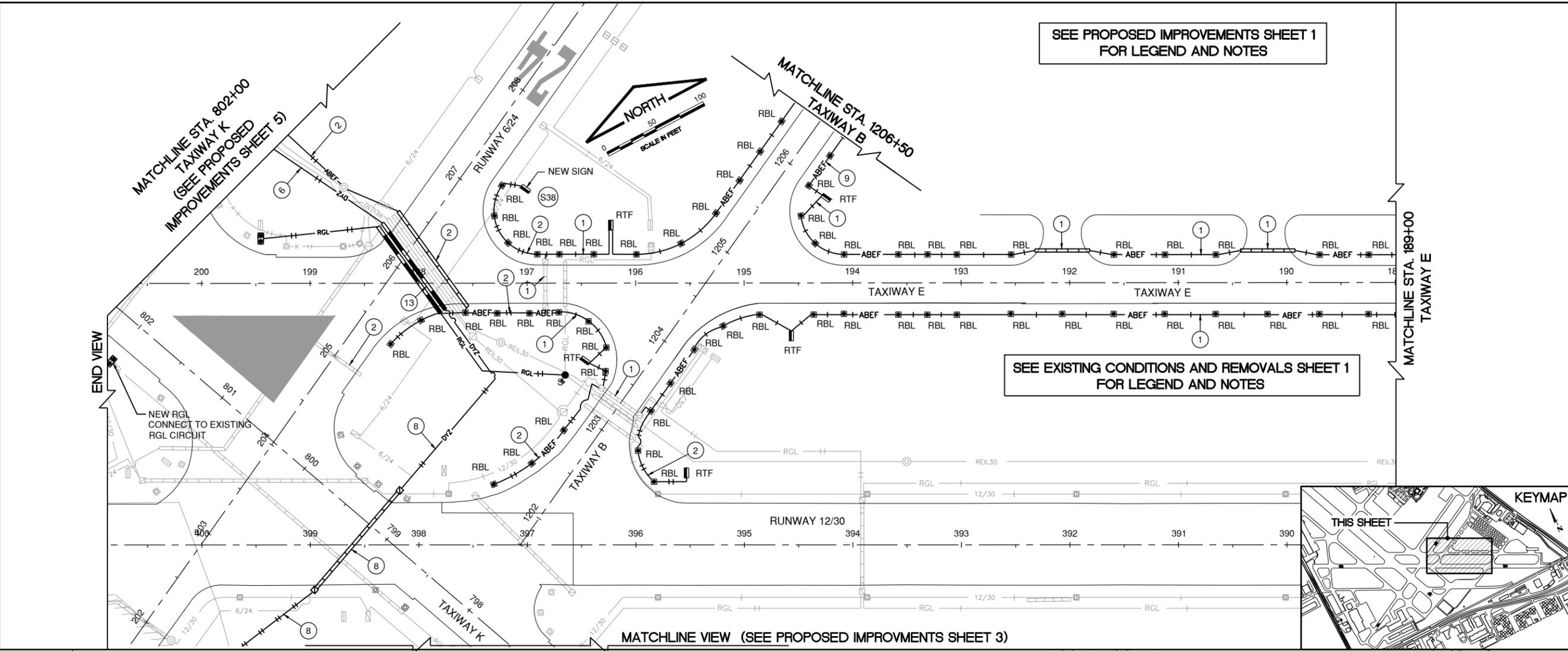


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UPDATE BY: Kris Salvatera  
 LAYOUT: PROPOSED IMPROVEMENTS PLAN - SHEET 2

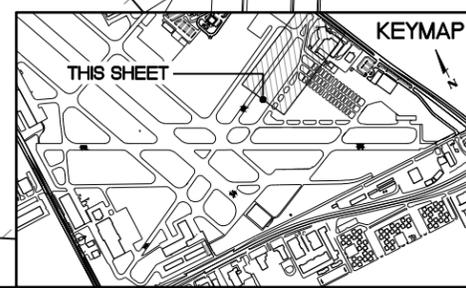
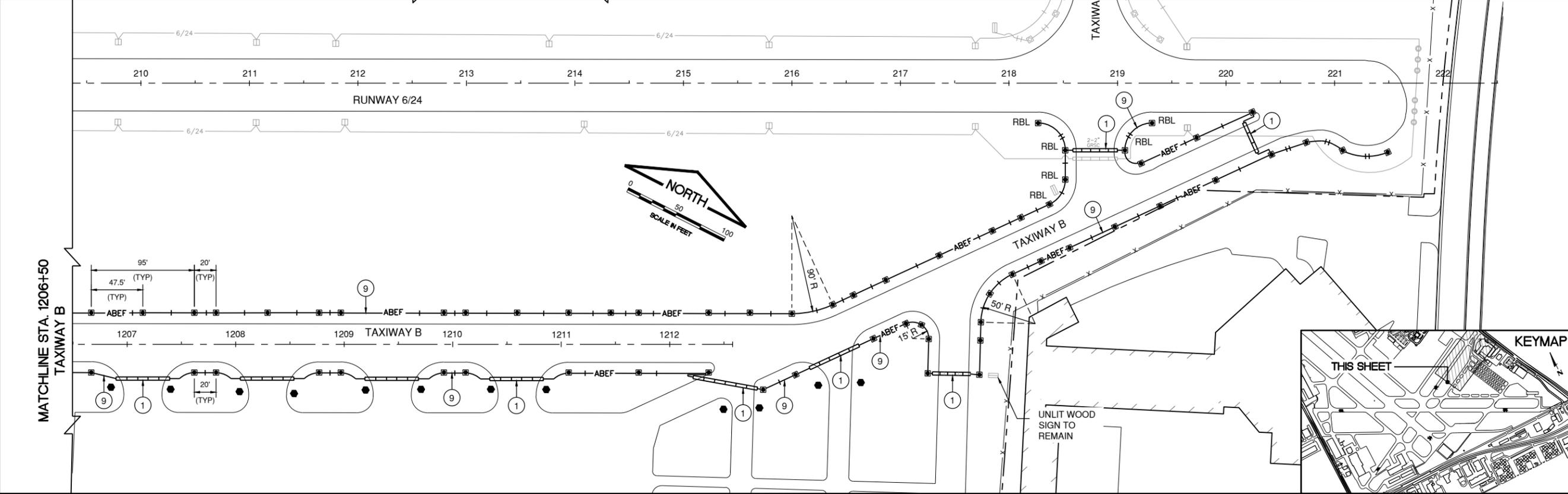
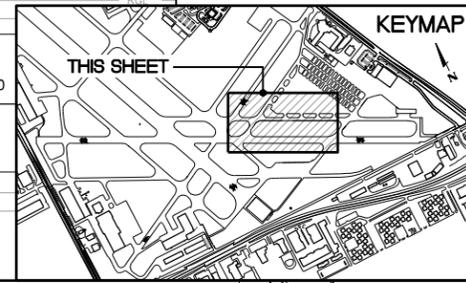
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SEE PROPOSED IMPROVEMENTS SHEET 1 FOR LEGEND AND NOTES

SEE EXISTING CONDITIONS AND REMOVALS SHEET 1 FOR LEGEND AND NOTES



IL CONTRACT: PA063  
 IL LETTING ITEM: 08A  
 IL PROJECT: PWK-4843  
 S.B.G. PROJECT:

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
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CHICAGO EXECUTIVE AIRPORT  
 WHEELING/PROSPECT HEIGHTS, ILLINOIS  
 REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALOMS

**PROPOSED IMPROVEMENTS PLAN - SHEET 2**

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**CHICAGO EXECUTIVE AIRPORT**

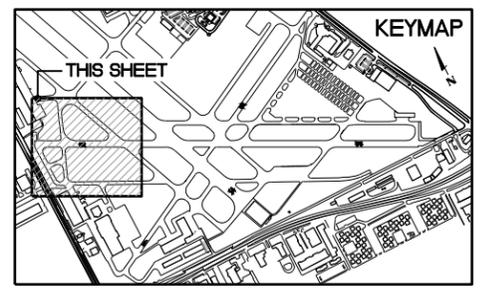
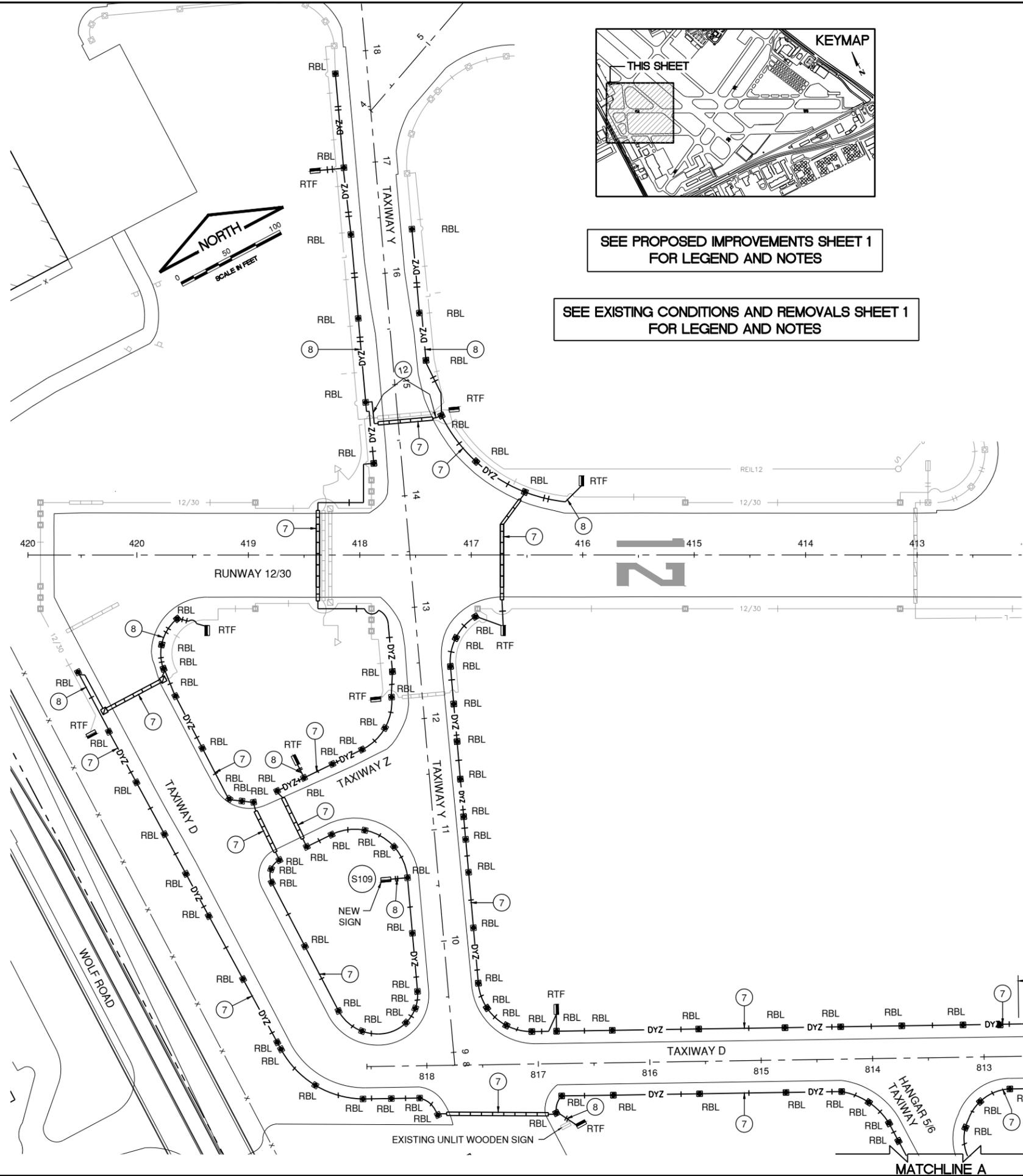
DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	KWS
APPROVED BY:	DKP
DATE:	06/25/2021
JOB No:	20029004.00

**FINAL**

SHEET 20 OF 35 SHEETS

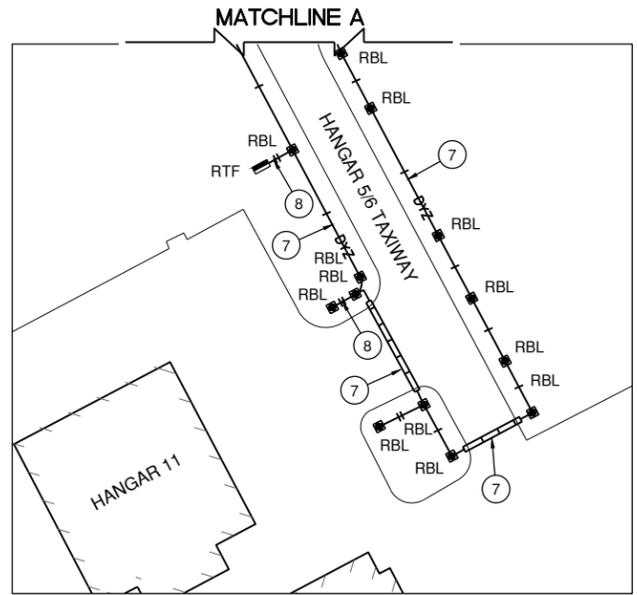
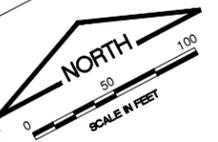


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 UPDATE BY: Kris Salvatera  
 LAYOUT: PROPOSED IMPROVEMENTS PLAN - SHEET 4  
 REF: DWG: 22\_Airfield\_Electrical\_Improvements\_Plan\_Sheet\_4.dwg  
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SEE PROPOSED IMPROVEMENTS SHEET 1 FOR LEGEND AND NOTES

SEE EXISTING CONDITIONS AND REMOVALS SHEET 1 FOR LEGEND AND NOTES



INSET - MATCHLINE A  
SCALE = 1" = 50'

MATCHLINE - TAXIWAY D  
STA. 809+00  
(SEE PROPOSED IMPROVEMENTS SHEET 3)

IL CONTRACT: PA063  
 IL LETTING ITEM: 08A  
 IL PROJECT: PWK-4843  
 S.B.G. PROJECT:

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT  
 WHEELING/PROSPECT HEIGHTS, ILLINOIS  
 REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALOMS

**PROPOSED IMPROVEMENTS PLAN - SHEET 4**

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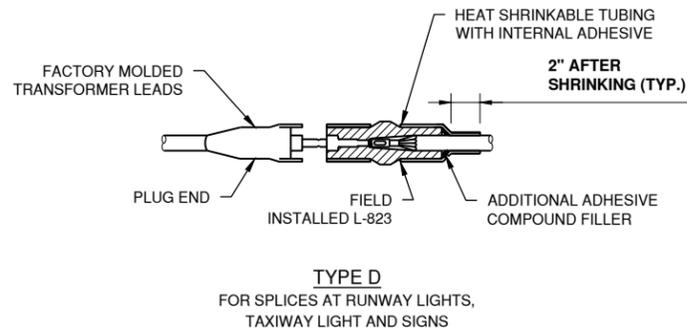
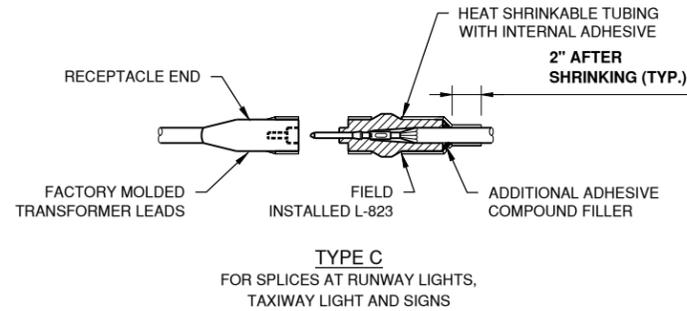
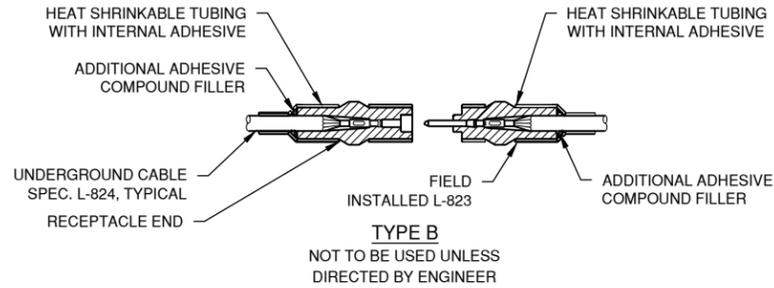
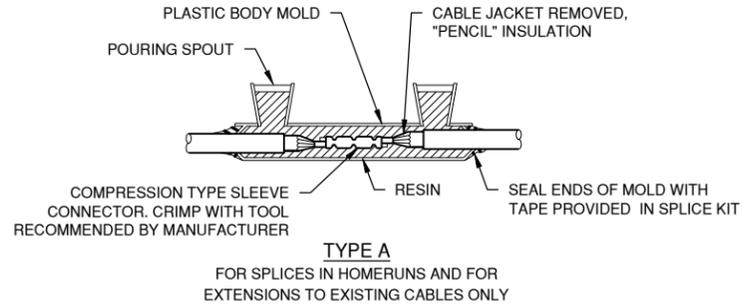
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**CHICAGO EXECUTIVE AIRPORT**

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	KWS
APPROVED BY:	DKP
DATE:	06/25/2021
JOB No:	20029004.00

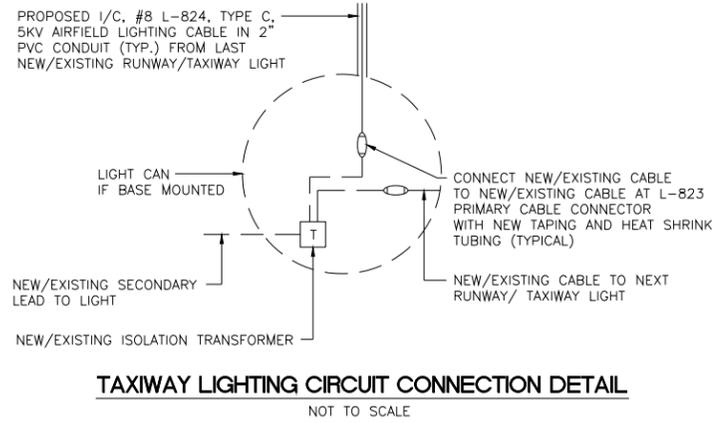


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 UPDATE BY: Kris Solvetera  
 LAYOUT: ELECTRICAL DETAILS - SHEET 1  
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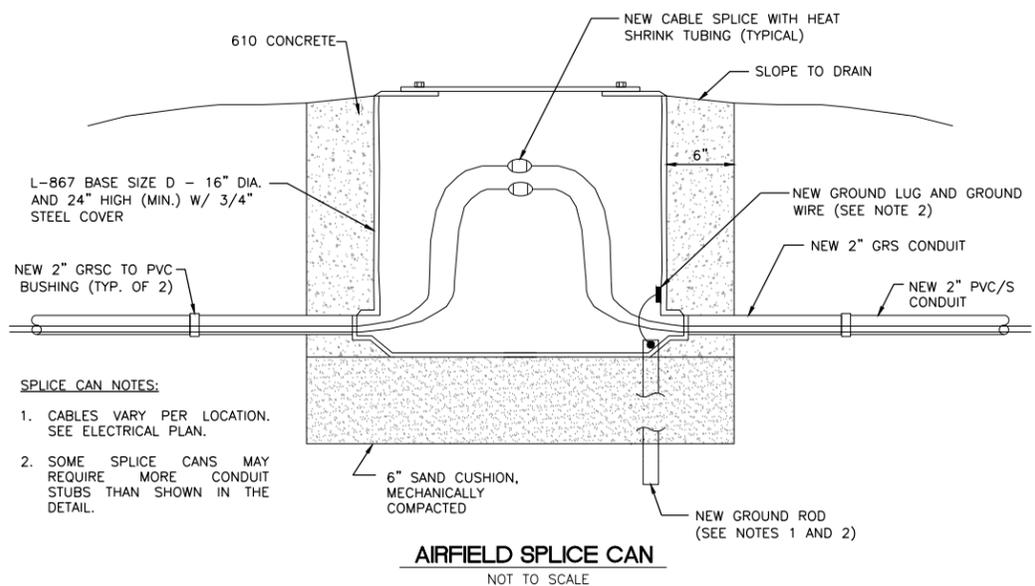
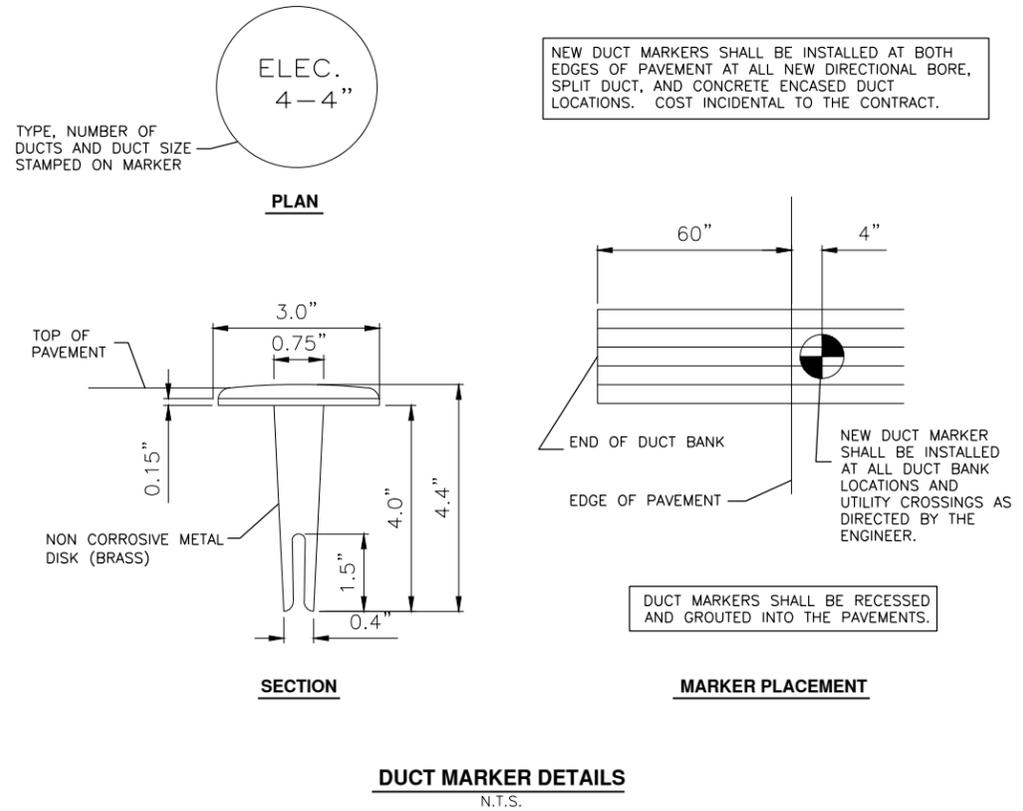
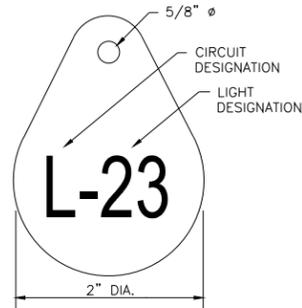
**SPLICE NOTES**

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF FIVE (5) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.
- THE CONTRACTOR MAY ELECT TO USE AN FAA APPROVED "COMPLETE" KIT IN LIEU OF HEAT SHRINK KIT AT NO ADDITIONAL COST.



**NOTES**

- INSTALL A NON-CORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
- NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIWAY AND RUNWAY LIGHTS AND SIGNS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (PROPOSED OR RELOCATED LIGHTS) SHALL BE RETAGGED.
- COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- COORDINATE WITH RESIDENT ENGINEER FOR TAXIWAY LIGHTING TAG NUMBERS.



**SPLICE CAN NOTES:**

- CABLES VARY PER LOCATION. SEE ELECTRICAL PLAN.
- SOME SPLICE CANS MAY REQUIRE MORE CONDUIT STUBS THAN SHOWN IN THE DETAIL.

**NOTES:**

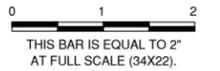
- INSTALL SAFETY GROUND TO EXISTING SIGNS AND L-867 CANS. ATTACH GROUND LUG TO EXISTING CAN AND INSTALL GROUND ROD AS SHOWN ON PLANS.
- INSTALL 1/C #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN AND EXOTHERMICALLY WELDED TO GROUND ROD. INSTALL GROUND LUG FOR EXISTING CANS, IF REQUIRED.

IL CONTRACT: **PA063**  
 IL LETTING ITEM: **08A**  
 IL PROJECT: **PWK-4843**  
 S.B.G. PROJECT:

SURVEY BOOK #

**REVISIONS**

NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT  
 WHEELING/PROSPECT HEIGHTS, ILLINOIS  
 REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALOMS

**ELECTRICAL DETAILS - SHEET 1**

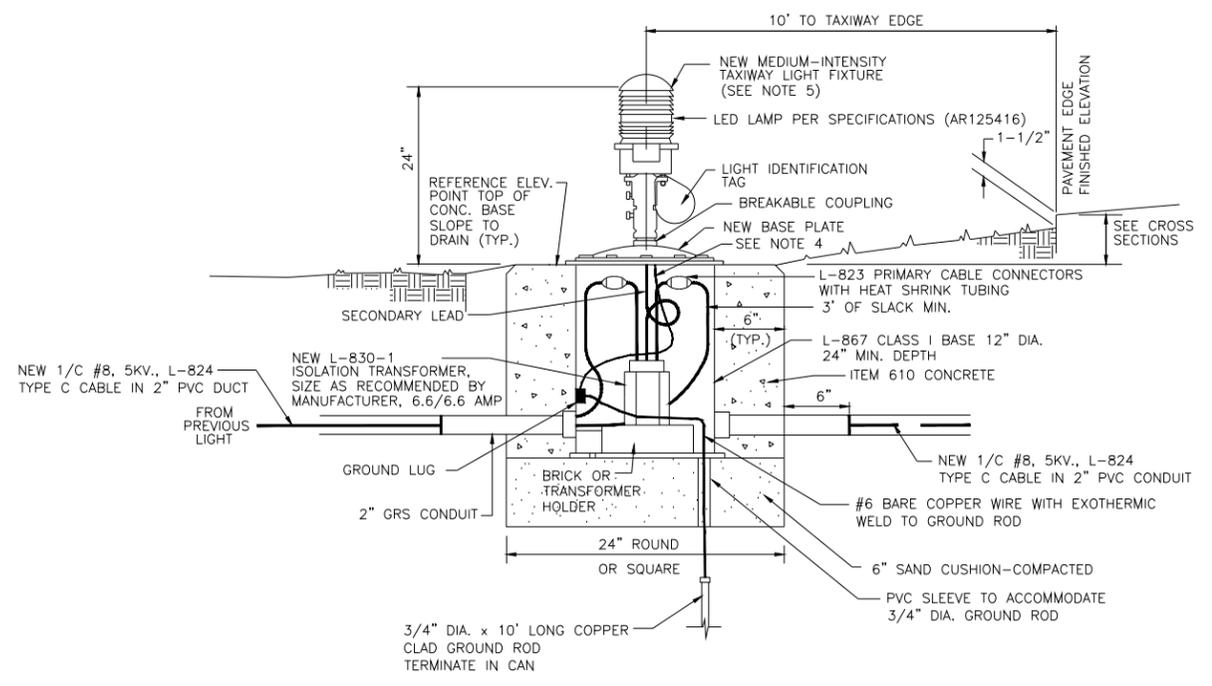
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**CHICAGO EXECUTIVE AIRPORT**

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	KWS
APPROVED BY:	DKP
DATE:	06/25/2021
JOB No:	20029004.00

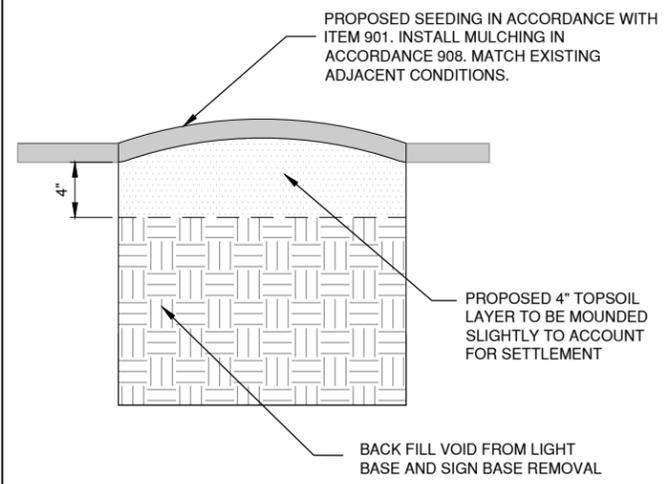
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 UPDATE BY: Kris Solvetero  
 LAYOUT: ELECTRICAL DETAILS - SHEET 2  
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 XREF DWG: \chicago\rehab\lighting\ph1\Draw\Sheet\27 Electrical Details - Sheet 5.dwg



**NEW BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT**  
NOT TO SCALE

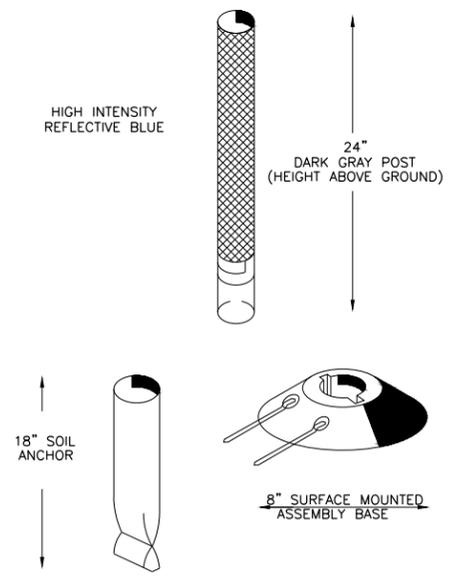
- LIGHTING NOTES**
1. TRANSFORMER WATTAGE AS RECOMMENDED BY MANUFACTURER FOR EACH LAMP TYPE.
  2. NON-LED FIXTURES SHALL UTILIZE QUARTZ LAMPS.
  3. ARCTIC KIT SHALL NOT BE REQUIRED FOR LED FIXTURES.
  4. THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS WITH GREEN XHHW INSULATION. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.
  5. LED LIGHT FIXTURES SHALL BE L-861T AS INDICATED ON THE PLANS AND SPECIFICATIONS.
  6. AFTER INSTALLATION OF CABLE IN UNIT DUCT, SEAL END OF CONDUIT TO MAKE WATERTIGHT.
  7. PRECAST BASE MAY BE USED.



**COMPLETED STAKE MOUNTED LIGHT, BASE MOUNTED LIGHT AND SIGN REMOVAL**  
N.T.S.

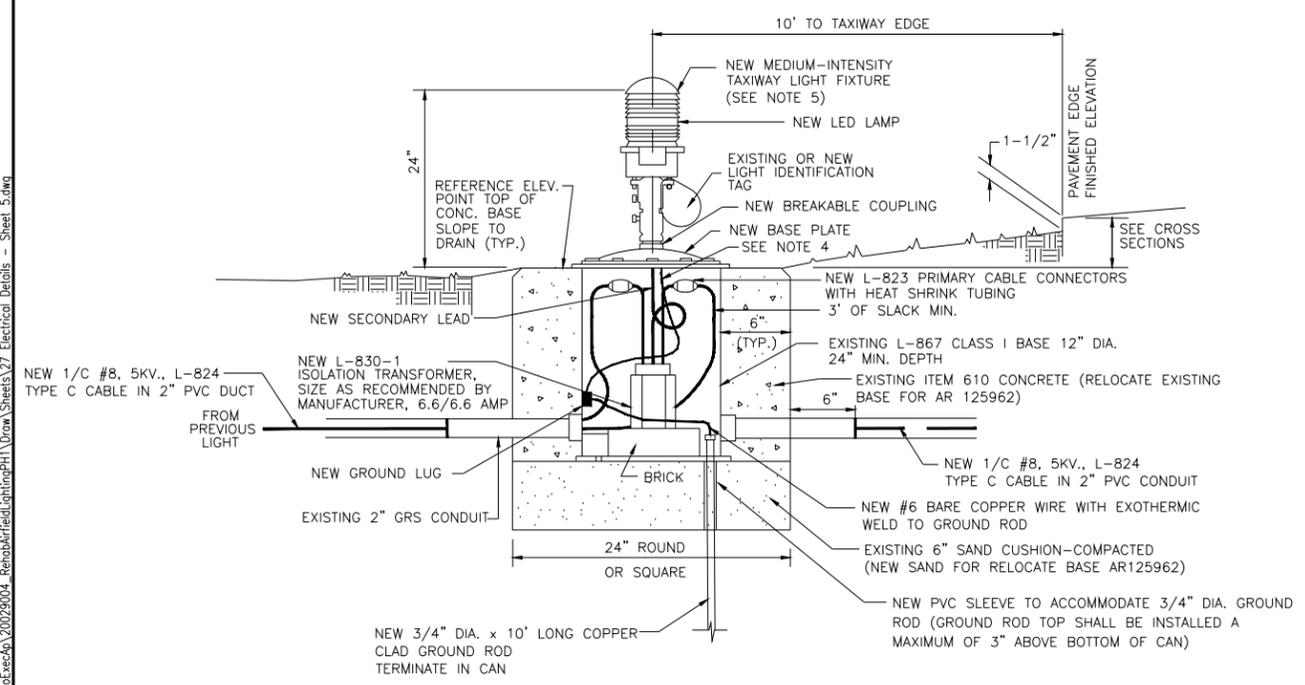
NOTE: COST OF BACKFILL AND RESTORATION SHALL BE INCIDENTAL TO THE CONTRACT.

- GENERAL NOTES**
1. TRANSFORMER HOLDER MAY BE ANY COMMERCIALY AVAILABLE BRICK.
  2. BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
  3. ISOLATION TRANSFORMERS SHALL HAVE A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9), A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/C, No. 8, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
  4. TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
  5. AT THE CONTRACTOR'S OPTION, IN LIEU OF TAPE AND HEAT SHRINKABLE TUBING, A SELF-SEALING STYLE CONNECTOR L-823 "COMPLETE KITS" OR FAA APPROVED EQUAL MAY BE USED.
  6. LAMP FOR FIXTURES SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS. ISOLATION TRANSFORMERS SHALL BE SIZED PER THE FIXTURE MANUFACTURER, 6.6 AMP.
  7. THE CONCRETE BASE FOR BASE MOUNTED LIGHTS AND SIGNS SHALL BE TROWEL FINISHED WITH A 45° BEVELED EDGE AND SLOPED TO DRAIN.

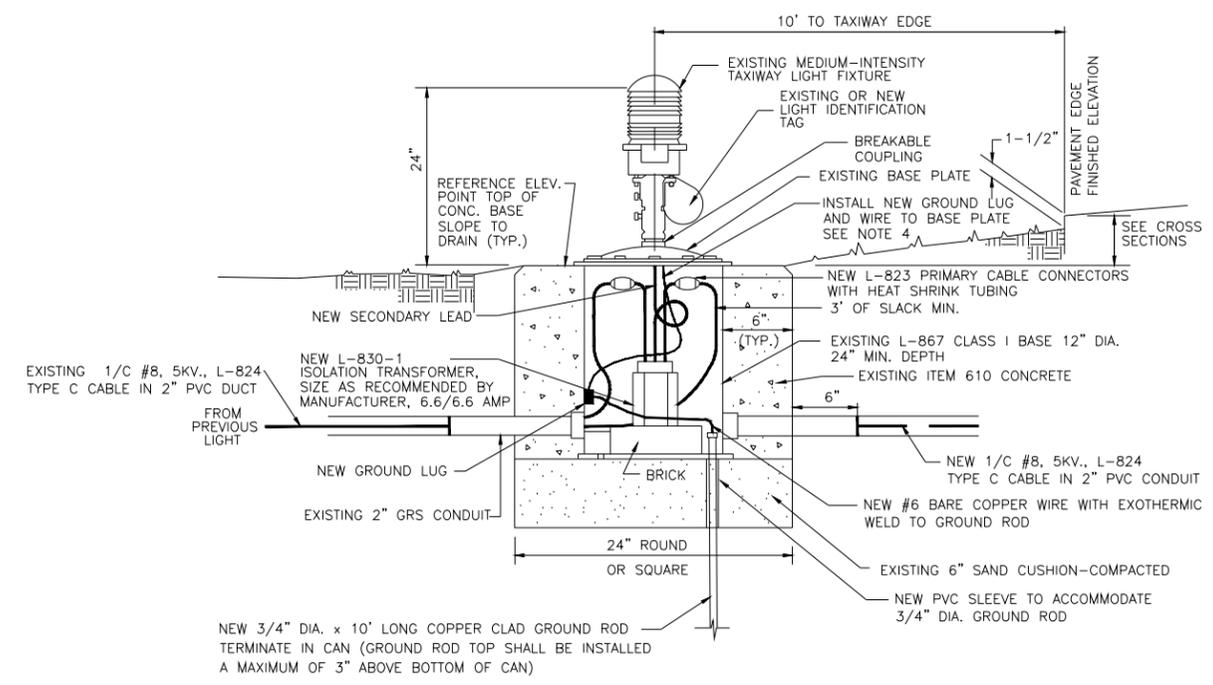


**ELEVATED RETROREFLECTIVE MARKER DETAIL**  
NOT TO SCALE

NOTE: RETROREFLECTIVE MARKER SHALL MET FAA AC 150/5345-39D AIRFIELD LIGHTING EQUIPMENT CERTIFICATION PROGRAM



**RELOCATE/REPLACE BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT**  
NOT TO SCALE



**RETROFIT BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT**  
NOT TO SCALE

IL CONTRACT: **PA063**  
IL LETTING ITEM: **08A**  
IL PROJECT: **PWK-4843**  
S.B.G. PROJECT:

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**CHICAGO EXECUTIVE AIRPORT**  
**WHEELING/PROSPECT HEIGHTS, ILLINOIS**  
**REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALOMS**  
**ELECTRICAL DETAILS - SHEET 2**

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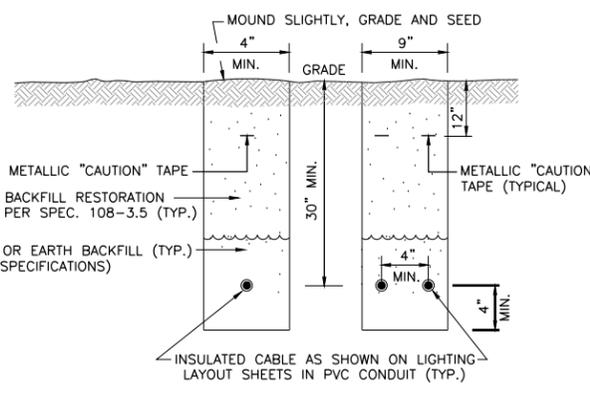
DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	KWS
APPROVED BY:	DKP
DATE:	06/25/2021
JOB No:	20029004.00

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SHEET 25 OF 35 SHEETS

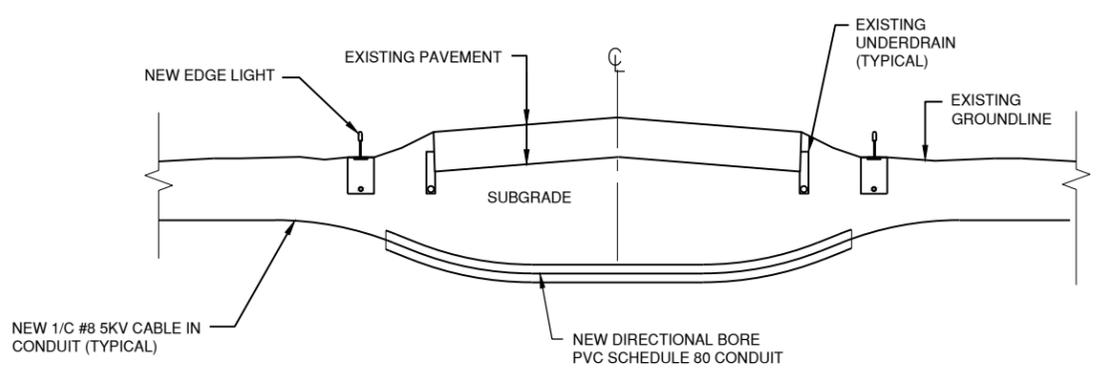


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 LAYOUT: ELECTRICAL DETAILS - SHEET 4  
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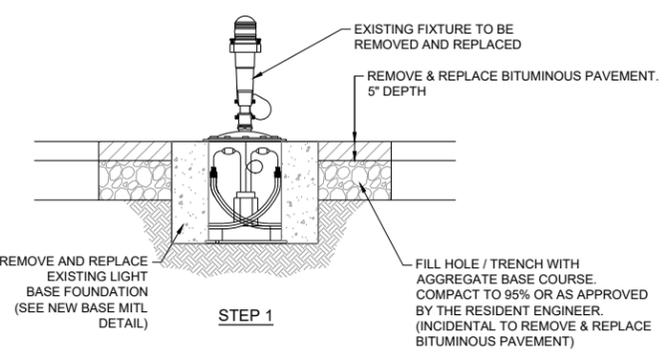
- TRENCH NOTES**
1. TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 4" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
  2. DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
  3. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
  4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO THE CONDUIT AND CABLE INSTALLATION.

**TRENCH DETAIL**  
NOT TO SCALE

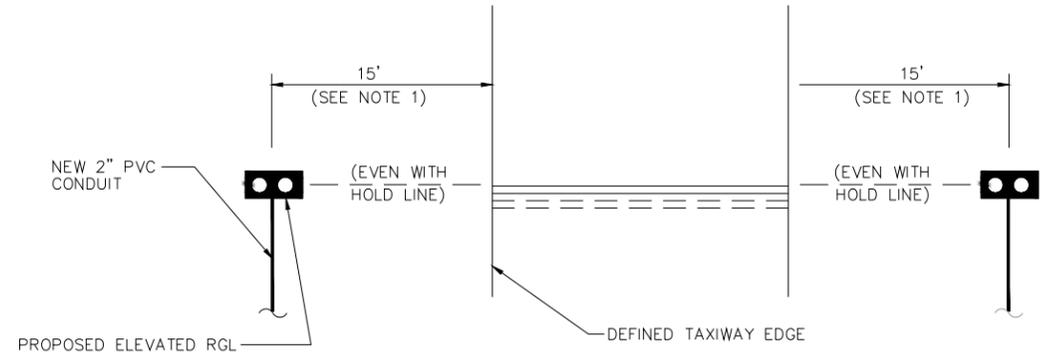


**DIRECTIONAL BORE DETAIL**  
N.T.S.

- NOTES**
1. THE DEPTH OF THE DIRECTIONAL BORE SHALL BE NO LESS THAN 4.0' FROM THE PAVEMENT SURFACE AND SHALL NOT DISTURB EXISTING UNDERDRAINS/UTILITIES OR NEW LIGHTS/CABLING.
  2. REFER TO LIGHTING PLANS FOR ADDITIONAL CABLE AND CONDUIT INFORMATION.

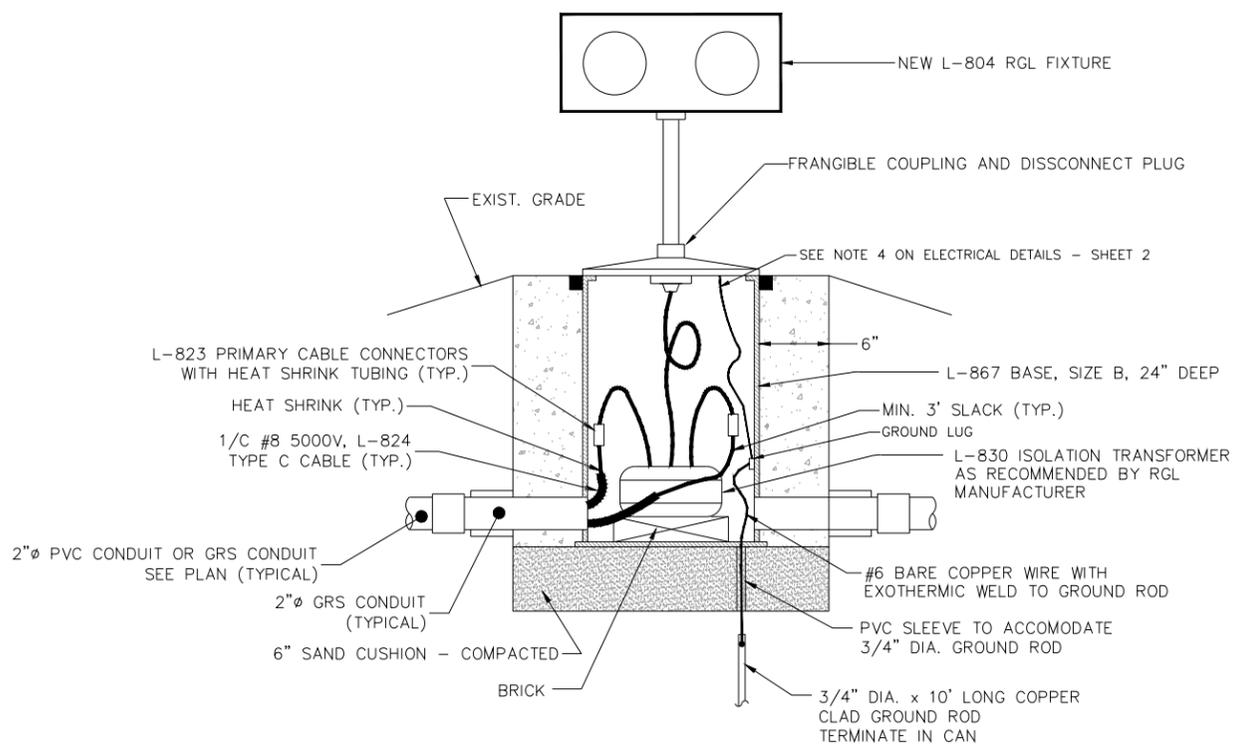


**LIGHT BASE REMOVAL IN ASPHALT PAVEMENT (NEW/EXISTING)**  
NOT TO SCALE



**ELEVATED RGL LAYOUT DETAIL (TYPICAL)**  
NOT TO SCALE

- NOTES**
1. DISTANCE FROM TAXIWAY EDGE MAY BE INCREASED UP TO A MAXIMUM OF 17' AND A MINIMUM OF 12'. KEEP BOTH FIXTURES AT SAME DISTANCE FROM TAXIWAY PAVEMENT. CONTRACTOR SHALL VERIFY THE LOCATION LAYOUT WITH THE RESIDENT ENGINEER.



**INSTALLATION OF ELEVATED RGL (TYPICAL)**  
NOT TO SCALE

IL CONTRACT: **PA063**  
 IL LETTING ITEM: **08A**  
 IL PROJECT: **PWK-4843**  
 S.B.G. PROJECT:

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT  
 WHEELING/PROSPECT HEIGHTS, ILLINOIS  
 REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALOMS  
 ELECTRICAL DETAILS - SHEET 4

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**CHICAGO EXECUTIVE AIRPORT**

DESIGN BY: KWS  
 DRAWN BY: JRO  
 CHECKED BY: KWS  
 APPROVED BY: DKP  
 DATE: 06/25/2021  
 JOB No: 20029004.00

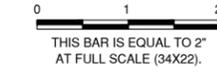
**FINAL**  
 SHEET 27 OF 35 SHEETS

IL CONTRACT: **PA063**  
 IL LETTING ITEM: **08A**  
 IL PROJECT: **PWK-4843**  
 S.B.G. PROJECT:

SURVEY BOOK #

REVISIONS

NUMBER	BY	DATE



**CHICAGO EXECUTIVE AIRPORT**  
**WHEELING/PROSPECT HEIGHTS, ILLINOIS**  
**REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALCMS**  
**AIRFIELD ELECTRICAL VAULT IMPROVEMENTS**  
**AND NEW ALCMS**

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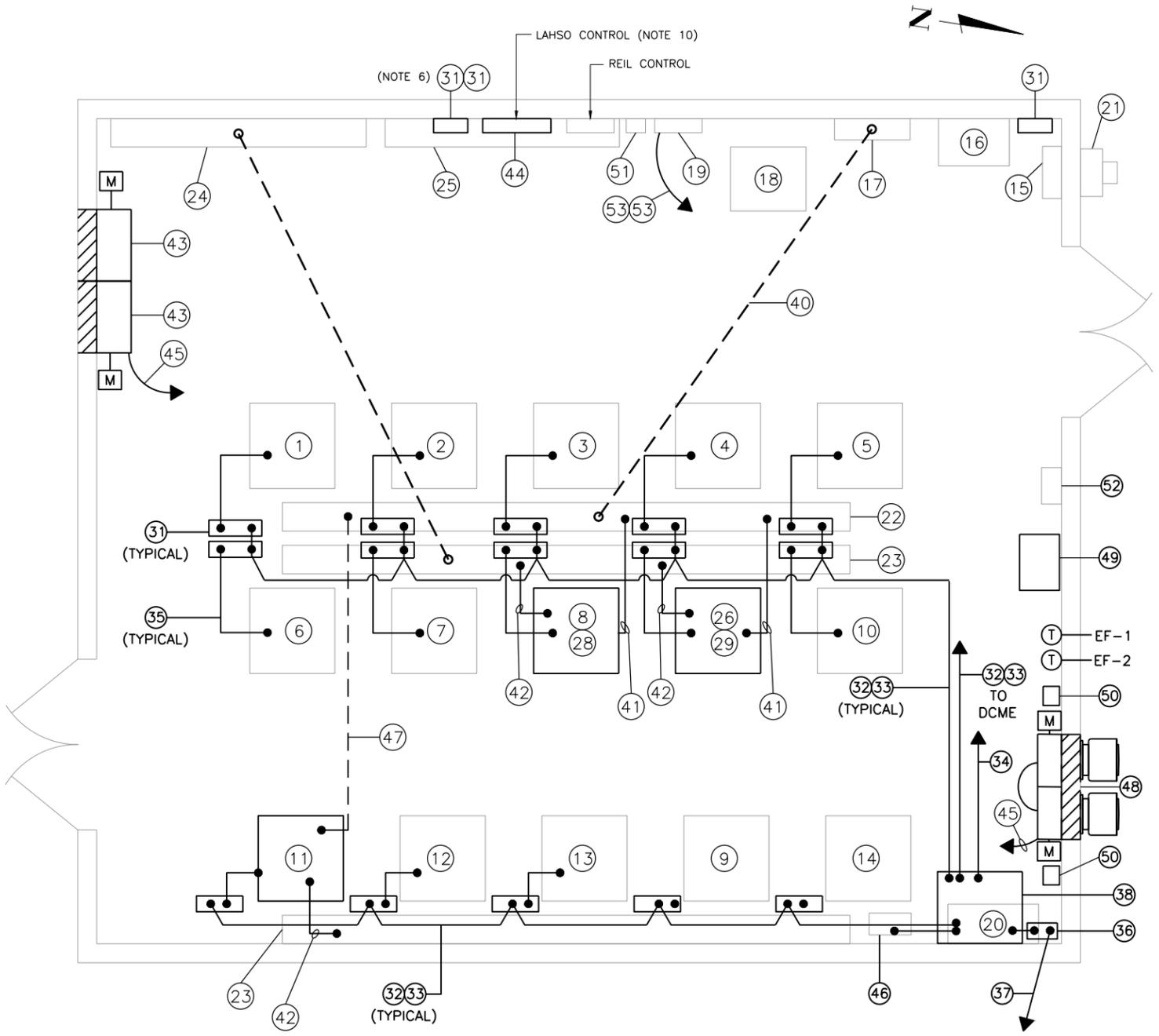
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DESIGN BY:	AB
DRAWN BY:	JRO
CHECKED BY:	AB
APPROVED BY:	DKP
DATE:	06/25/2021
JOB No:	20029004.00

**FINAL**

**VAULT NOMENCLATURE**

- 1 EXISTING 30KW (5-STEP) REGULATOR FOR RUNWAY 16/34.
- 2 EXISTING 20KW (5-STEP) REGULATOR FOR RUNWAY 6/24.
- 3 EXISTING 10KW (3-STEP) REGULATOR FOR RUNWAY 12/30
- 4 EXISTING 10KW (3-STEP) REGULATOR FOR TAXIWAY G,D AND 34 HOLD APRON.
- 5 EXISTING 10KW (3-STEP) REGULATOR FOR RUNWAY 16/34 RDR.
- 6 EXISTING 15KW (3-STEP) REGULATOR FOR RUNWAY 16/34 RGL.
- 7 EXISTING 30 KW (5-STEP) REGULATOR (SPARE).
- 8 EXISTING 30 KW (3-STEP) REGULATOR FOR A, B, D, E, F, Y AND L4 TO BE REMOVED (SEE NOTE 7).
- 9 EXISTING 30KW (5-STEP) SPARE.
- 10 EXISTING 20KW (5-STEP) REGULATOR FOR RUNWAY 16 APPROACH.
- 11 EXISTING 30 KW (3-STEP) REGULATOR FOR LIMA TO BE REPLACED WITH NEW 30KW REGULATOR.
- 12 EXISTING 30KW (3-STEP) REGULATOR FOR TAXIWAY K.
- 13 EXISTING 10KW (3-STEP) REGULATOR FOR TAXIWAY C.
- 14 EXISTING 30kW (3-STEP) FUTURE AIRFIELD LIGHTING REGULATOR.
- 15 EXISTING MAIN UTILITY SERVICE CIRCUIT BREAKER DISCONNECT. 800A, 480V, 3-POLE.
- 16 EXISTING AUTOMATIC TRANSFER SWITCH. 800A, 480V, 3-POLE. PROVIDE ALCMS INTERFACE.
- 17 EXISTING HIGH VOLTAGE POWER DISTRIBUTION PANEL. 480V, 3-PHASE WITH 800AMP MAIN CIRCUIT BREAKER (SEE NOTE 3).
- 18 EXISTING 150KVA, 480V-280Y/120V, 3ø, 4-WIRE TRANSFORMER.
- 19 EXISTING LOW VOLTAGE LIGHTING PANEL. 208Y/120V, 3-PHASE WITH 400AMP MAIN CIRCUIT BREAKER (SEE NOTE 3).
- 20 EXISTING PLC CONTROL CABINET TO BE REMOVED.
- 21 EXISTING 800AMP CT CABINET.
- 22 EXISTING 12"x12"x12' LONG LOW VOLTAGE WIREWAY.
- 23 EXISTING 12"x12"x12' LONG HIGH VOLTAGE WIREWAY.
- 24 EXISTING 12"x12"x9' LONG HIGH VOLTAGE WIREWAY (SEE NOTE 8).
- 25 EXISTING 12"x12"x9' LONG LOW VOLTAGE WIREWAY.
- 26 EXISTING 10KW (3-STEP) REGULATOR FOR HFC APRON TO BE REMOVED (SEE NOTE 7).
- 27 INSTALL NEW 2-1/C #8 5KV, L-824 CABLES IN EXISTING CONDUIT FROM NEW 15KW ABEF REGULATOR TO HIGH VOLTAGE WIREWAY, INSTALL L-823 CONNECTORS.
- 28 NEW 15KW, 480V (3-STEP) L-829 FERRORESONANT REGULATOR FOR TAXIWAY A, B, E, F CIRCUIT.
- 29 NEW 15KW, 480V (3-STEP) L-829 FERRORESONANT REGULATOR FOR TAXIWAY D, Y, Z CIRCUIT.
- 30 CONNECT NEW REGULATOR TO POWER DISTRIBUTION PANEL.
- 31 NEW DISTRIBUTED CONTROL AND MONITORING EQUIPMENT (DCME) MOUNTED ON EXISTING EQUIPMENT PLATE OR WALL. TYPICAL FOR EACH CCR (TOTAL OF 15), A.T.S., LAHSO AND REIL CONTROLLER (TOTAL OF 3)(SEE NOTE 9).
- 32 NEW (2) #24 AWG, SHIELDED, TWO TWISTED PAIR, BELDEN 9842 OR AS REQUIRED BY ALCMS MANUFACTURER IN 1" GRS CONDUIT (TYPICAL FOR ALL DCME UNITS).
- 33 NEW 2 #10 THWN, 1 #10 GND. FOR DCME UNIT UPS POWER OR AS REQUIRED BY ALCMS MANUFACTURER IN 1" GRS CONDUIT (TYPICAL FOR ALL DCME).
- 34 NEW 2 #12 THWN, 1 #12 GND. IN 1" GRS CONDUIT TO LOW VOLTAGE LIGHTING PANEL (SEE NOTE 3).
- 35 NEW 14 #18 AWG OR AS REQUIRED BY ALCMS IN 3/4" FLEXIBLE CONDUIT (TYPICAL FOR ALL CCR'S).
- 36 NEW FIBER OPTIC PATCH PANEL AND FIBER OPTIC JUMPER CABLES AS REQUIRED BY ALCMS MANUFACTURER.
- 37 NEW 1-12 STRAND MULTI-MODE FIBER OPTIC CABLE IN EXISTING CONDUIT TO ATCT.
- 38 NEW ALCMS RACK (SEE NOTE 2).
- 40 NEW 2 #2, 4 #4 THWN, 3 #6 GND. IN EXISTING CONDUIT.
- 41 NEW 2 #4 THWN, 1 #6 IN FLEXIBLE CONDUIT.
- 42 NEW 2 #1/C #8, 5KV L-824 AIRFIELD LIGHTING CABLE IN FLEXIBLE CONDUIT. (SEE NOTE 9).
- 43 REMOVE EXISTING LOUVERS, MOTORIZED DAMPERS AND FILTERS AND REPLACE WITH NEW STATIONARY LOUVERS SL-1 AND SL-2 WITH NEW MOTORIZED DAMPERS AND FILTERS. FURNISH AND INSTALL NEW ALUMINUM SLEEVE TO MOUNT MOTORIZED DAMPERS AND FILTERS IN BEHIND LOUVER. SEE STATIONARY LOUVER SCHEDULE SHEET 29.
- 44 REMOVE EXISTING LAHSO CONTROLLER AND REPLACE WITH NEW LAHSO CONTROLLER.
- 45 NEW 2 #10 THWN, 1 #10 GND. IN 1" CONDUIT TO LIGHTING PANEL.
- 46 EXISTING L-854 RADIO CONTROLLER, PROVIDE ALCMS INTERFACE.
- 47 NEW 2 #2 THWN, 1 #6 GND. IN EXISTING CONDUIT.
- 48 REMOVE EXISTING LOUVER, PLENUM AND EXHAUST FAN. INSTALL NEW ANGLE IRON FRAME AND NEW EXHAUST FANS EF-1 AND EF-2. SEE EXHAUST FAN SCHEDULE SHEET 29. SEE EXHAUST FAN MOUNTING DETAIL SHEET 29.
- 49 REUSE EXISTING INTERLOCK CONTROL BOX AND CONDUIT TO WIRE NEW LINE VOLTAGE THERMOSTAT CONTROL FROM EF-1 TO SL-1 AND SEPARATE THERMOSTAT CONTROL FROM EF-2 TO SL-2.
- 50 MOUNT DISCONNECT AND SPEED CONTROL ADJACENT TO NEW EXHAUST FAN
- 51 EXISTING TOWER ROAD STREET LIGHTS SWITCH TO REMAIN.
- 52 EXISTING TYCO FIRE ALARM AND VIDEO SURVEILLANCE EQUIPMENT TO REMAIN.
- 53 NEW 2 #12 THWN, 1 #12 GND. IN 1" CONDUIT TO EXHAUST FAN.



**AIRFIELD ELECT. VAULT PLAN VIEW**  
 NOT TO SCALE

**NOTES:**

1. ALL PROPOSED WORK OR ITEMS BEING MODIFIED ARE SHOWN IN BOLD. ALL OTHER ITEMS SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY.
2. PROPOSED ALCMS RACK SHALL BE SUPPLIED WITH WHEELS. NEW CONTROL SYSTEM SHALL BE OPERATIONAL AND TESTED PRIOR TO THE REMOVAL OF EXISTING PLC CABINET. PROVIDE J-BOX, FLEX CONDUIT AND SUFFICIENT CABLE SLACK REQUIRED FOR ALCMS RACK TO BE OPERATIONAL.
3. SEE PANEL SCHEDULE FOR REGULATOR, ALCMS AND HVAC CIRCUITS.
4. INSTALL POWER AND CONTROL WIRES BETWEEN INTERFACE CONTROL PANEL AND A.T.S., BEACON CONTROL, LAHSO CONTROL, L-854 RADIO CONTROLLER AND REIL CONTROLLER PER ALCMS MANUFACTURER.
5. INSTALL AND WIRE PROPOSED DCME FOR FUTURE REGULATORS.
6. RELOCATE EXISTING WALL MOUNTED TRANSFORMER BELOW LAHSO CONTROLLER. MOUNT (2) ACE-II UNITS ON TOP OF EACH OTHER BETWEEN LAHSO AND REIL CONTROLLERS.
7. REMOVE EXISTING REGULATOR AND INSTALL PROPOSED REGULATOR. INSTALL PROPOSED POWER AND CONTROL WIRES IN FLEXIBLE CONDUIT. MATCH EXISTING CONDITIONS.
8. INSTALL L-823 CONNECTORS IN HIGH VOLTAGE WIREWAY.
9. RELOCATE EXISTING INDICATOR UNIT ON EXISTING PANEL TO PROVIDE SPACE FOR NEW DCME UNIT.
10. CONNECT EXISTING INPUT POWER AND OUTPUT AIRFIELD CIRCUIT TO NEW LAHSO CONTROLLER.

DATE: Monday, June 28, 2021 10:33:59 PM  
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 UPDATE BY: Kris Salvatori  
 LAYOUT: VAULT MODIFICATIONS - AIRFIELD LIGHTING  
 IMAGE FILES: DSC00769.jpg  
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DATE: Monday, June 28, 2021 10:34:02 PM  
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 UPD BY: Kri Salvatoro  
 LAYOUT: PANEL SCHEDULE  
 XREF DWG: \PanelSchedule\Lighting.dwg  
 IMAGE FILES: DSC0072A.jpg DSC0077A.jpg  
 DSC0077A.jpg  
 P:\new\LOCO\_CEA-4-Color-Small (2).jpg

PANELBOARD SCHEDULE																	
PANEL DESIGNATION: <b>PDP</b>						BOND NEUTRAL AND GROUND BAR: <b>NO</b>						POLE: <b>30</b>					
LOCATION: <b>VAULT</b>						NEUTRAL BUS RATING:						SHORT CIRCUIT RATING:					
MFR & TYPE: <b>SQUARE D</b>						SERVICE ENTRANCE RATED: <b>NO</b>						SERIES OR FULLY RATED: <b>SERIES</b>					
						TVSS & DISCONNECT REQUIRED: <b>NO</b>											
VOLTS: <b>480/277V</b>						MOUNTING: <b>SURFACE</b>						BUS RATING (AMPS): <b>800</b>					
PHASE: <b>3</b>						ENCL RATING: <b>NEMA 1</b>						BUS: <b>COPPER</b>					
WIRE: <b>3</b>						MAIN CIRCUIT BREAKER: AMP/POLE <b>800</b>											
CKT NO.	LOAD	BREAKER SIZE	LOAD AMPS	USAGE FACTOR	PHASE AMPS (USAGE)			POLE NO.			PHASE AMPS (USAGE)			USAGE FACTOR	LOAD AMPS	BREAKER SIZE	CKT NO.
1	TRANSFORMER	225A/3P	10.2	0.5	5.1			1	2	7.5			0.5	15	100A/2P	RWY 16/34	2
3			10.2	0.5		5.1		3	4		7.5		0.5	15			4
5			10.2	0.5		5.1		5	6		7.5		0.5	15			6
7	TWY LIMA	100A/2P	6.8	0.5	3.4			7	8	6			0.5	12	30A/2P	RWY 12/30	8
9			6.8	0.5		3.4		9	10		6		0.5	12			10
11	TWY DYZ	60A/2P	6.8	0.5			3.4	11	12			6	0.5	12	50A/2P	RGL	12
13			4.2	0.5	2.1			13	14	10			0.5	20			14
15	TWY AEBF	100A/2P	4.2	0.5		2.1		15	16		10		0.5	20	100A/2P	STAND BY	16
17			4.2	0.5		2.1	17	18		10		0.5	20	18			
19	UNIT HEATER	50A/3P	27	0.5	13.5			19	20	10			0.5	20	50A/3P	UNIT HEATER	20
21			27	0.5		13.5		21	22		10		0.5	20			22
23			27	0.5		13.5		23	24		10		0.5	20			24
25	STREET LIGHTS	20A/3P	3.4	0.5	1.7			25	26	10			0.5	20	60A/2P	RWY 6/24	26
27			3.4	0.5		1.7		27	28		10		0.5	20			28
29			3.4	0.5		1.7		29	30		10		0.5	20			30
31	KILO	100A/2P						31	32					30A/2P	RDR	32	
33								33	34							34	
35	REIL 12	20A/2P						35	36					20A/2P	REIL 16	36	
37								37	38							38	
39	REIL 30	20A/2P						39	40					20A/2P	REIL 34	40	
41								41	42							42	
43	TWY C	30A/2P						43	44					20A/2P	LAHSO STEP DOWN TRANSFORMER	44	
45								45	46							46	
SECTION TOTAL:					25.8	25.8	25.8			43.5	43.5	43.5		TOTAL USAGE LOAD: 57588.3 VA			
					PHASE TOTAL AMPS:			A B C									
					69.3			69.3			69.3						
					A B C												
					19196.1			19196.1			19196.1						

PANELBOARD SCHEDULE																		
PANEL DESIGNATION: <b>LP</b>						BOND NEUTRAL AND GROUND BAR: <b>NO</b>						POLE: <b>30</b>						
LOCATION: <b>VAULT</b>						NEUTRAL BUS RATING:						SHORT CIRCUIT RATING:						
MFR & TYPE: <b>SQUARE D</b>						SERVICE ENTRANCE RATED: <b>NO</b>						SERIES OR FULLY RATED: <b>SERIES</b>						
						TVSS & DISCONNECT REQUIRED: <b>NO</b>												
VOLTS: <b>208Y/120V</b>						MOUNTING: <b>SURFACE</b>						BUS RATING (AMPS): <b>400</b>						
PHASE: <b>3</b>						ENCL RATING: <b>NEMA 1</b>						BUS: <b>COPPER</b>						
WIRE: <b>4</b>						MAIN CIRCUIT BREAKER: AMP/POLE <b>400</b>												
CKT NO.	LOAD	BREAKER SIZE	LOAD AMPS	USAGE FACTOR	PHASE AMPS (USAGE)			POLE NO.			PHASE AMPS (USAGE)			USAGE FACTOR	LOAD AMPS	BREAKER SIZE	LOAD	CKT NO.
1	REGULATOR TWY GD , 34 HOLD	20A/2P	0	0	0			1	2	0			0	0	20A/2P	EXHAUST FAN	2	
3			0	0		0		3	4		0		0	0			4	
5			0	0		0		5	6		0		0	0			6	
7	GEN- SET-PANEL	100A/2P	0	0	0			7	8	0			0	0	20A/1P	LOUVERS - FAN	8	
9			0	0		0		9	10		0		0	0			10	
11	OUTLET - WEST	20A/1P	0	0			0	11	12			0	0	20A/1P	OUTSIDE LIGHTS	12		
13	OUTLET - SOUTH	20A/1P	0	0			0	13	14	0		0	0	20A/1P	VAULT LIGHTS	14		
15	ALCMS POWER	20A/1P	0	0			0	15	16	0		0	0	20A/1P	BEACON CONTROLLER	16		
17	RADIO CONTROL UNIT	20A/1P	0	0			0	17	18			0	0	20A/1P	SPARE	18		
19	SPARE	20A/1P	0	0			0	19	20			0	0	60A/2P	TRAILER TEMP SERVICE	20		
21	ST LIGHTING CONTROL	20A/1P	0	0			0	21	22			0	0	60A/2P	RWY 6/24	22		
23	LAHSO CONTROL	20A/1P	0	0			0	23	24			0	0	60A/2P		24		
25	BLANK	20A/1P	0	0			0	25	26	0		0	0	20A/2P	LAHSO	26		
27	REIL CONTROL	15A/1P	0	0			0	27	28			0	0	20A/2P		28		
29	EXHAUST FAN	20A/2P					0	29	30			0	0	15A/1P	DAMPER MOTORS	30		
SECTION TOTAL:					0	0	0			0	0	0		TOTAL USAGE LOAD: 0 VA				
					PHASE TOTAL AMPS:			A B C										
					0			0			0							
					A B C													
					0			0			0							

NOTES:

**POWER DISTRIBUTION NOTES**

- CONNECT NEW TAXIWAY LIMA REGULATOR CABLES TO EXISTING CIRCUIT BREAKER.
- REPLACE EXISTING 60A SPARE CIRCUIT BREAKER WITH NEW 50A, 2-POLE BREAKER FOR TAXIWAY D,Y,Z REGULATOR.
- REPLACE EXISTING 100A TAXIWAY A,B,E,F CIRCUIT BREAKER WITH NEW 50A, 2-POLE BREAKER FOR TAXIWAY A,B,E,F REGULATOR.

**LIGHTING PANEL NOTES**

- REPLACE EXISTING 20A PLC-POWER CIRCUIT BREAKER WITH NEW 20A, 1-POLE BREAKER FOR ALCMS.
- REPLACE HFC CIRCUIT BREAKER WITH 20A, 2-POLE BREAKER FOR EXHAUST FAN.

**EXHAUST FAN SCHEDULE**

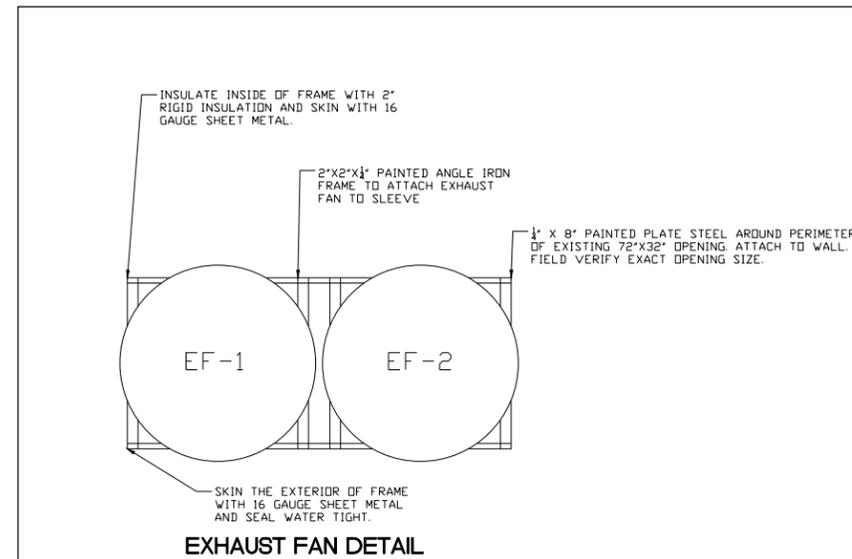
MARK	EF-1	EF-2
SERVICE	VAULT	VAULT
LOCATION	SIDEWALL	SIDEWALL
MANUFACTURER	GREENHECK	GREENHECK
MODEL	CUE-180-VG	CUE-180-VG
TYPE	SIDEWALL	SIDEWALL
CFM	4000	4000
S.P.	0.75"	0.75"
FAN RPM	1239	1239
MOTOR	HP	2
	VOLT	208
	PHASE	1
	RPM	1325
ACCESSORIES	SEE NOTES	SEE NOTES
	1 THRU 6	1 THRU 6
REMARKS	INTERLOCK WITH SL-1	INTERLOCK WITH SL-2

- NOTES:
- WEATHER TIGHT DISCONNECT SWITCH
  - ALUMINUM BIRDSCREEN
  - LOW LEAK INSULATED 115V MOTORIZED DAMPER
  - SPEED CONTROL
  - LINE VOLTAGE COOLING THERMOSTAT
  - MAKE/MODEL NUMBER IS SHOWN FOR INFORMATION ONLY. APPROVED EQUAL EQUIPMENT SHALL BE ACCEPTABLE.

**STATIONARY LOUVER SCHEDULE**

MARK	SL-1	SL-2
SERVICE	VAULT	VAULT
TYPE	DRAINABLE	DRAINABLE
MANUFACTURER	GREENHECK	GREENHECK
MODEL	ESD-635	ESD-635
CFM	4000	4000
MAX. Δ S.P. (IN)	0.1	0.1
NOM. SIZE	WIDTH	36
	HEIGHT	32
	DEPTH	6"
AREA (SQ. FT.)	NET	7.50
	FREE	3.845
FRAME	CHANNEL	CHANNEL
MATERIAL	ALUMINUM	ALUMINUM
FINISH	KYNAR	KYNAR
ACCESSORIES	SEE NOTES	SEE NOTES
	1 THRU 7	1 THRU 7
REMARKS	INTERLOCK WITH EF-1	INTERLOCK WITH EF-2

- NOTES:
- COLOR TO BE SELECTED BY OWNER
  - BIRD SCREEN
  - EXTENDED SILL
  - 115V LOW LEAK INSULATED MOTORIZED DAMPER
  - WASHABLE FILTER
  - FIELD VERIFY LOUVER SIZE
  - MAKE/MODEL NUMBER IS SHOWN FOR INFORMATION ONLY. APPROVED EQUAL EQUIPMENT SHALL BE ACCEPTABLE.



IL CONTRACT: **PA063**  
 IL LETTING ITEM: **08A**  
 IL PROJECT: **PWK-4843**  
 S.B.G. PROJECT:

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CHICAGO EXECUTIVE AIRPORT  
 WHEELING/PROSPECT HEIGHTS, ILLINOIS  
 REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALCMS

**PANEL SCHEDULE**

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**CHICAGO EXECUTIVE AIRPORT**

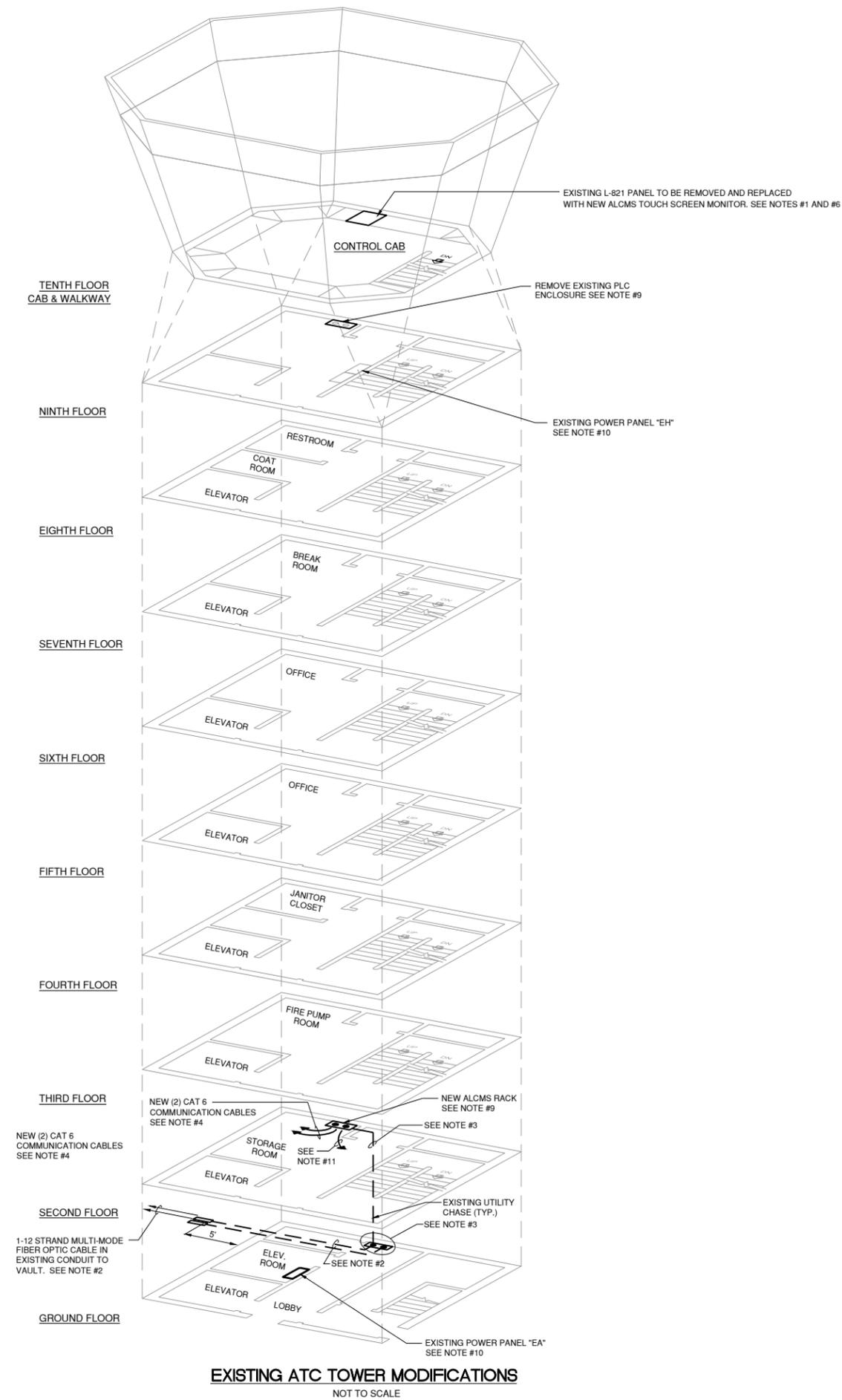
DESIGN BY: AB  
 DRAWN BY: JRO  
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 APPROVED BY: DKP  
 DATE: 06/25/2021  
 JOB No: 20029004.00

**FINAL**

DATE: Monday, June 28, 2021 10:34:09 PM  
 FILE: K:\ChicagoExecAirport\202006\RehabilitateAirfieldLighting\PH1\Draw\Sheet28 ALCMS - ATCT MODIFICATIONS.dwg  
 UPDATE BY: Kris Salvatera  
 LAYOUT: ALCMS - ATCT MODIFICATIONS  
 IMAGE FILES: p:\0000\GEAR\Comp\Sheet\28 ALCMS - ATCT MODIFICATIONS  
 REF: DWG: RehabilitateAirfieldLighting\PH1\Draw\Sheet28 ALCMS - ATCT MODIFICATIONS

**NOTES:**

- CONTRACTOR SHALL COORDINATE ALL WORK IN THE EXISTING CONTROL TOWER WITH THE FAA AIRWAYS FACILITIES REPRESENTATIVES AND THE RESIDENT ENGINEER. CONTRACTOR SHALL GIVE A MINIMUM OF 2 DAYS NOTICE PRIOR TO BEGINNING WORK IN THE EXISTING TOWER.
- CONTRACTOR SHALL REMOVE EXISTING FIBER OPTIC CABLES FROM CONDUIT AFTER NEW FIBER OPTIC CABLES AND ALCMS HAVE BEEN INSTALLED AND OPERATIONAL.
- CONTRACTOR SHALL PULL NEW FIBER OPTIC CABLES THROUGH EXISTING CABLE CHASE.
- CONTRACTOR SHALL REMOVE EXISTING CONTROL CABLES IN TOWER FOR EXISTING AIRFIELD LIGHTING CONTROLS ONCE NEW AIRFIELD LIGHTING CONTROL SYSTEM IN TOWER IS COMPLETELY OPERATIONAL. CONTRACTOR SHALL ROUTE NEW FIBER OPTIC CABLES IN EXISTING CABLE CHASE. INSTALL DATA CABLES FOR BEACON DCME AS REQUIRED BY ALCMS MANUFACTURER.
- CONTRACTOR SHALL REMOVE EXISTING CONTROL CABLES AND FLEXIBLE CONDUIT FROM TOWER CAB AFTER ALCMS HAS BEEN INSTALLED AND OPERATIONAL. CONTRACTOR SHALL RUN NEW FIBER OPTIC CABLES IN 2-2" FLEXIBLE CONDUITS ALONG ROUTE OF EXISTING CONTROL CABLES.
- CONTRACTOR SHALL REMOVE EXISTING CONTROL CONSOLE AND INSTALL NEW 18" x 24" COUNTERTOP AND FLUSH MOUNTED TOUCH SCREEN IN ITS PLACE. CONTRACTOR SHALL MAKE NECESSARY MODIFICATIONS TO EXISTING CABINET WORK TO PROVIDE COMPLETE AND OPERATIONAL SYSTEM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND TOWER CHIEF. NEW COUNTERTOP SHOULD BE CUSTOM WOOD CONSOLE OF SIZE AND FINISH TO MATCH THE EXISTING COUNTER OR AS DIRECTED BY TOWER CHIEF.
- PROVIDE UPS AND RECEPTACLE FOR TOUCH SCREEN MONITOR IN THE TOWER CAB.
- ALL NEW WORK IN EXISTING TOWER SHALL BE INCLUDED IN THE UNIT BID PRICE FOR L-890 ALCMS.
- NEW CONTROL SYSTEM SHALL BE OPERATIONAL AND TESTED PRIOR TO THE REMOVAL OF THE EXISTING PLC CABINET. PROVIDE SUFFICIENT CABLE SLACK REQUIRED FOR ALCMS RACK TO BE OPERATIONAL.
- CONTRACTOR SHALL INSTALL 1-20A, 1-POLE CIRCUIT BREAKER IN PANEL "EA", LOCATED ON FIRST FLOOR
- NEW #2 12THWN, 1 #12 GND. IN 3/4" GRS CONDUIT TO EXISTING PANEL "EA".
- CONTRACTOR SHALL INSTALL A BEACON CONTACTOR AND DCME OR PLC FOR BEACON CONTROL IN ATCT CAB. EXACT LOCATION SHALL BE COORDINATED WITH THE FAA.



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 AIR TRAFFIC CONTROL TOWER MODIFICATIONS

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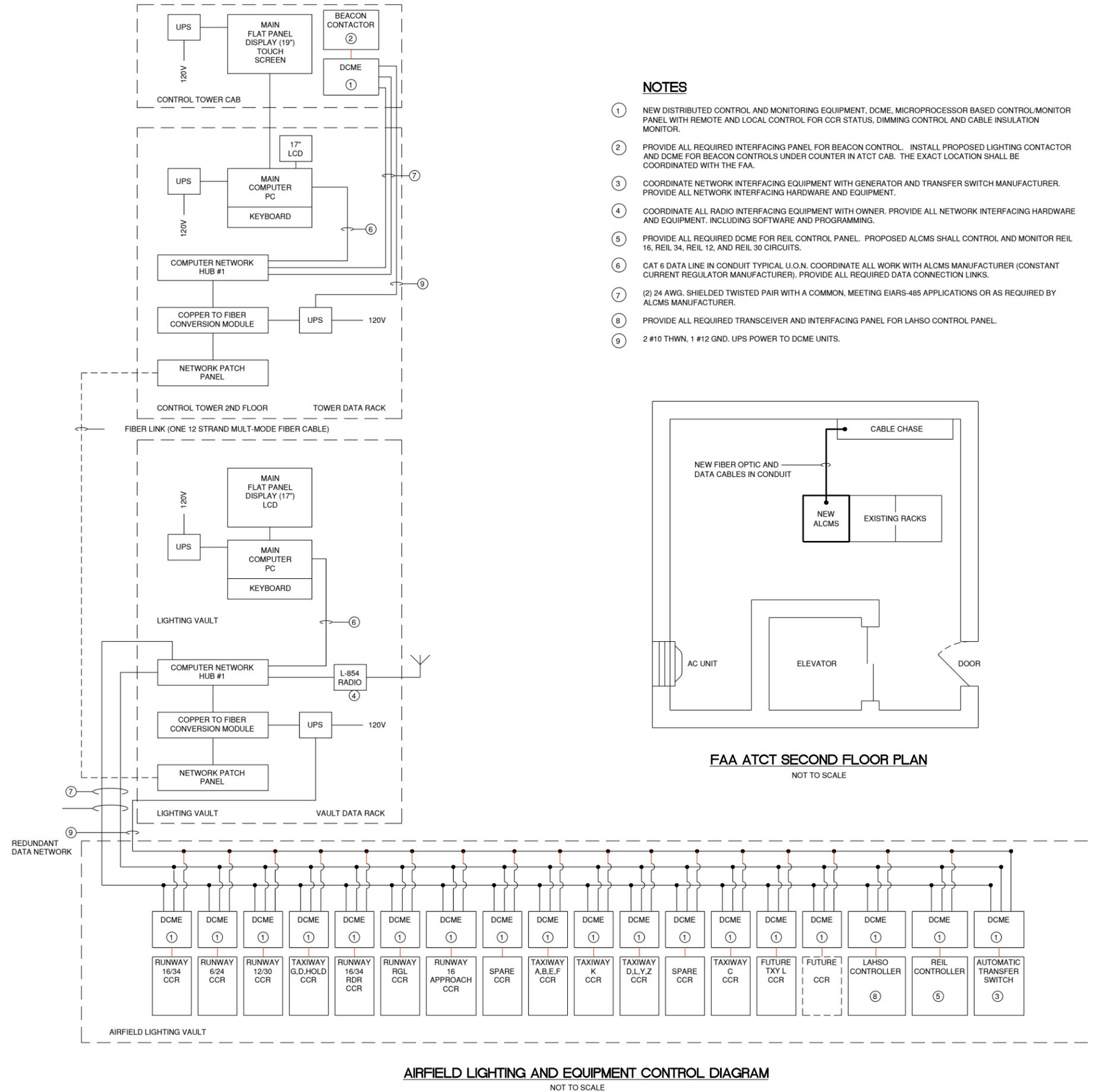
**CHICAGO EXECUTIVE AIRPORT**

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	KWS
APPROVED BY:	DKP
DATE:	06/25/2021
JOB No:	20029004.00

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SHEET 30 OF 35 SHEETS

DATE: Monday, June 28, 2021 10:34:14 PM  
 FILE: K:\ChicagoExecAirport\202006\RehabilitateAirfieldLighting\Draw\Sheets\ALCMS - EQUIPMENT BLOCK DIAGRAM.dwg  
 UPDATE BY: Kris Salvatera  
 LAYOUT: ALCMS - EQUIPMENT BLOCK DIAGRAM  
 REF: DWG: RehabilitateAirfieldLighting\Draw\Sheets\ALCMS - EQUIPMENT BLOCK DIAGRAM

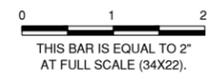


- NOTES**
- NEW DISTRIBUTED CONTROL AND MONITORING EQUIPMENT, DCME, MICROPROCESSOR BASED CONTROL/MONITOR PANEL WITH REMOTE AND LOCAL CONTROL FOR CCR STATUS, DIMMING CONTROL AND CABLE INSULATION MONITOR.
  - PROVIDE ALL REQUIRED INTERFACING PANEL FOR BEACON CONTROL. INSTALL PROPOSED LIGHTING CONTACTOR AND DCME FOR BEACON CONTROLS UNDER COUNTER IN ATCT CAB. THE EXACT LOCATION SHALL BE COORDINATED WITH THE FAA.
  - COORDINATE NETWORK INTERFACING EQUIPMENT WITH GENERATOR AND TRANSFER SWITCH MANUFACTURER. PROVIDE ALL NETWORK INTERFACING HARDWARE AND EQUIPMENT.
  - COORDINATE ALL RADIO INTERFACING EQUIPMENT WITH OWNER. PROVIDE ALL NETWORK INTERFACING HARDWARE AND EQUIPMENT, INCLUDING SOFTWARE AND PROGRAMMING.
  - PROVIDE ALL REQUIRED DCME FOR REIL CONTROL PANEL. PROPOSED ALCMS SHALL CONTROL AND MONITOR REIL 16, REIL 34, REIL 12, AND REIL 30 CIRCUITS.
  - CAT 6 DATA LINE IN CONDUIT TYPICAL U.O.N. COORDINATE ALL WORK WITH ALCMS MANUFACTURER (CONSTANT CURRENT REGULATOR MANUFACTURER), PROVIDE ALL REQUIRED DATA CONNECTION LINKS.
  - (2) 24 AWG, SHIELDED TWISTED PAIR WITH A COMMON, MEETING EIA-485 APPLICATIONS OR AS REQUIRED BY ALCMS MANUFACTURER.
  - PROVIDE ALL REQUIRED TRANSCEIVER AND INTERFACING PANEL FOR LAHSO CONTROL PANEL.
  - 2 #10 THWN, 1 #12 GND. UPS POWER TO DCME UNITS.

IL CONTRACT: **PA063**  
 IL LETTING ITEM: **08A**  
 IL PROJECT: **PWK-4843**  
 S.B.G. PROJECT:

SURVEY BOOK #

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**CHICAGO EXECUTIVE AIRPORT**  
**WHEELING/PROSPECT HEIGHTS, ILLINOIS**  
**REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALCMS**  
**ALCMS - EQUIPMENT BLOCK DIAGRAM**

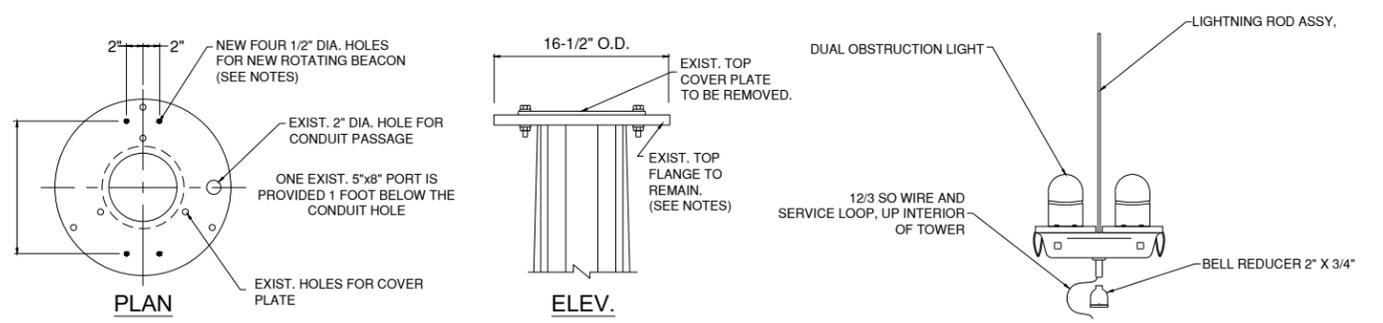
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**CHICAGO EXECUTIVE AIRPORT**

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DATE: Monday, June 28, 2021 10:34:25 PM  
 FILE: K:\ChicagoExecAirg\2020\004 - RehabilitateAirfieldLighting\Plan\Draw\Sheet\03 BEACON - LOCATION PLAN.dwg  
 UPDATE BY: Kris Salvatera  
 LAYOUT: BEACON - LOCATION PLAN  
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**EXISTING TOP FLANGE DETAIL**  
NOT TO SCALE

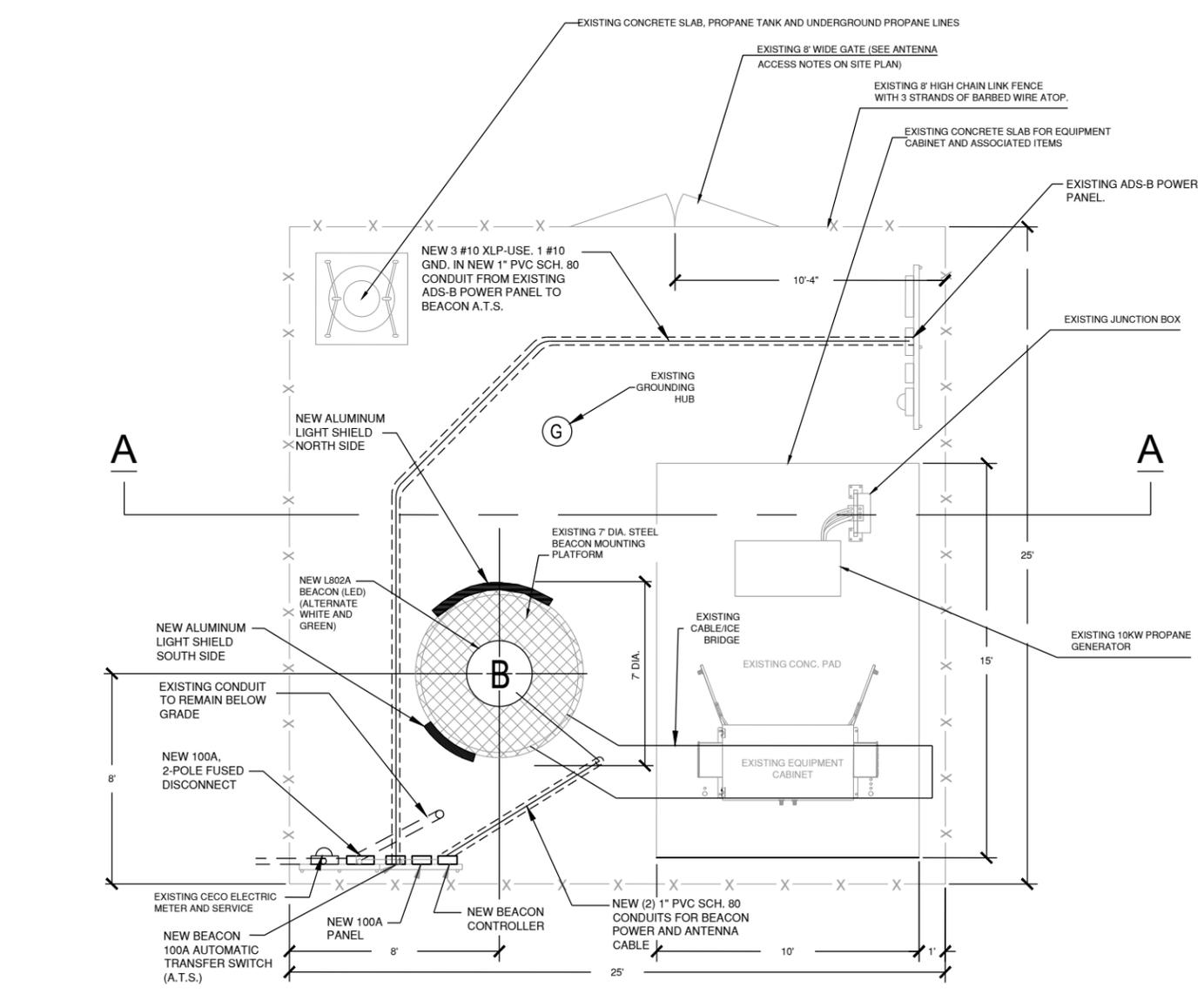
THE EXISTING TOP FLANGE BOLT PATTERN SHALL BE MODIFIED PER MANUFACTURER RECOMMENDATIONS. CONTRACTOR SHALL VERIFY CORRECT BOLT PATTERN AND REQUIREMENTS FOR NEW BEACON.

**EXISTING FAA OBSTRUCTION LIGHT MOUNT DETAIL (TO BE RELOCATED)**  
NOT TO SCALE

1. THE RELOCATION OF THE EXISTING FAA OBSTRUCTION LIGHTS SHALL BE INCIDENTAL TO THE NEW BEACON PAY ITEM.
2. OBSTRUCTION LIGHT SHALL BE ON 24 HOURS (DAY AND NIGHT).

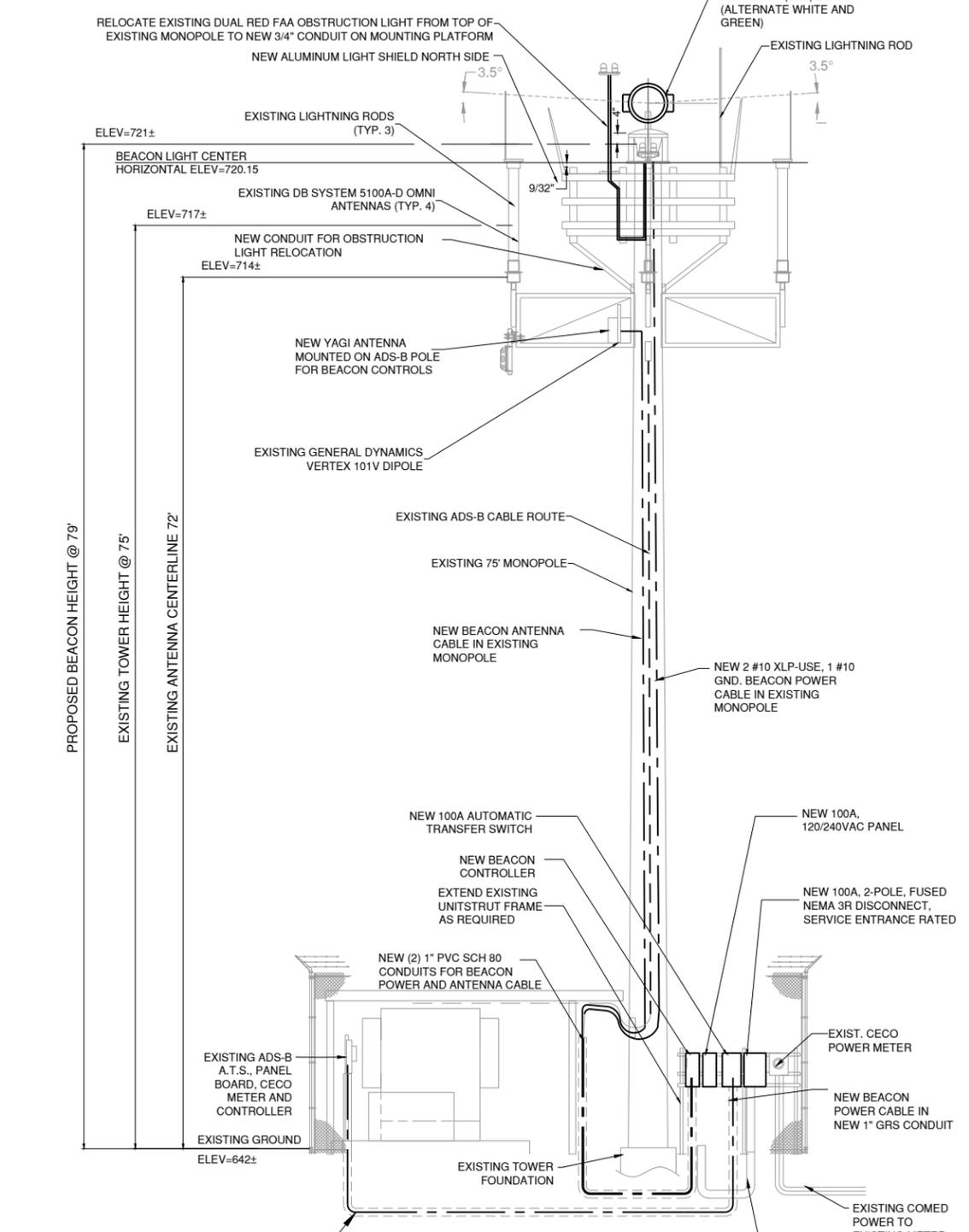
**NOTES:**

1. ANY EXISTING UTILITY, CABLE, GROUNDING, AND CONDUIT LOCATIONS SHALL BE LOCATED BY THE CONTRACTOR PRIOR TO NEW INSTALLATION. LOCATIONS NOT SHOWN FOR CLARITY.
2. ALL EXISTING UTILITY VERIFICATION AND NEW UNDERGROUND INSTALLATION SHALL BE COMPLETED BY HAND DIGGING TRENCHES.
3. NEW L802A BEACON (LED) SHALL BE HALI-BRITE L802AL216 CIVILIAN VERSION, 120 VAC, 60 HZ, CLASS II, OR APPROVED EQUAL.
4. THE MOUNTING PLATFORM SHALL BE HALI-BRITE 440C98-1 7 FOOT DIA. ROUND BEACON BASKET OR APPROVED EQUAL. ALL PLATFORM SUPPORTS AND INSTALLATION SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
5. THE NEW LIGHT SHIELDS SHALL PROTECT GLARE FROM THE NEW BEACON TO THE FAA AIR TRAFFIC CONTROL TOWER AND THE RESIDENCE TO THE NORTH OF HINTZ ROAD AS SHOWN ON SHEET 3, SEQUENCE OF CONSTRUCTION. THE LIGHT SHIELDS SHALL BE VARIOUS SIZED ALUMINUM PANELS OR APPROVED EQUAL AND SHALL BE INSTALLED TO THE PLATFORM TO THE SATISFACTION OF THE AIRPORT AND ENGINEER. AFTER INSTALLATION, THE CONTRACTOR MAY NEED TO ADJUST THE SHIELDS UP TO TWO TIMES TO THE SATISFACTION OF THE FAA TOWER CHIEF, THE AIRPORT AND THE ENGINEER.
6. THE BEACON LIGHT BEAM ANGLE SHALL BE SET TO 3.5 DEGREES ABOVE HORIZONTAL OR AS DETERMINED BY THE AIRPORT.
7. COORDINATE ALL WORK AT ADSB TOWER WITH L3 HARRIS TECHNOLOGIES, INC.



**ADS-B ANTENNA SITE**  
NOT TO SCALE

ALL NEW ITEMS AND ASSOCIATED WORK SHOWN ARE INCIDENTAL TO AS101510, AIRPORT ROTATING BEACON.



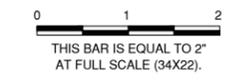
**ADS-B ANTENNA ELEVATION A-A**  
NOT TO SCALE

ALL NEW ITEMS AND ASSOCIATED WORK SHOWN ARE INCIDENTAL TO AS101510, AIRPORT ROTATING BEACON.

IL CONTRACT: **PA063**  
 IL LETTING ITEM: **08A**  
 IL PROJECT: **PWK-4843**  
 S.B.G. PROJECT:

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**CHICAGO EXECUTIVE AIRPORT**  
**WHEELING/PROSPECT HEIGHTS, ILLINOIS**  
**REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALOMS**  
**ADDITIVE ALTERNATE #1**  
**BEACON - LOCATION PLAN**

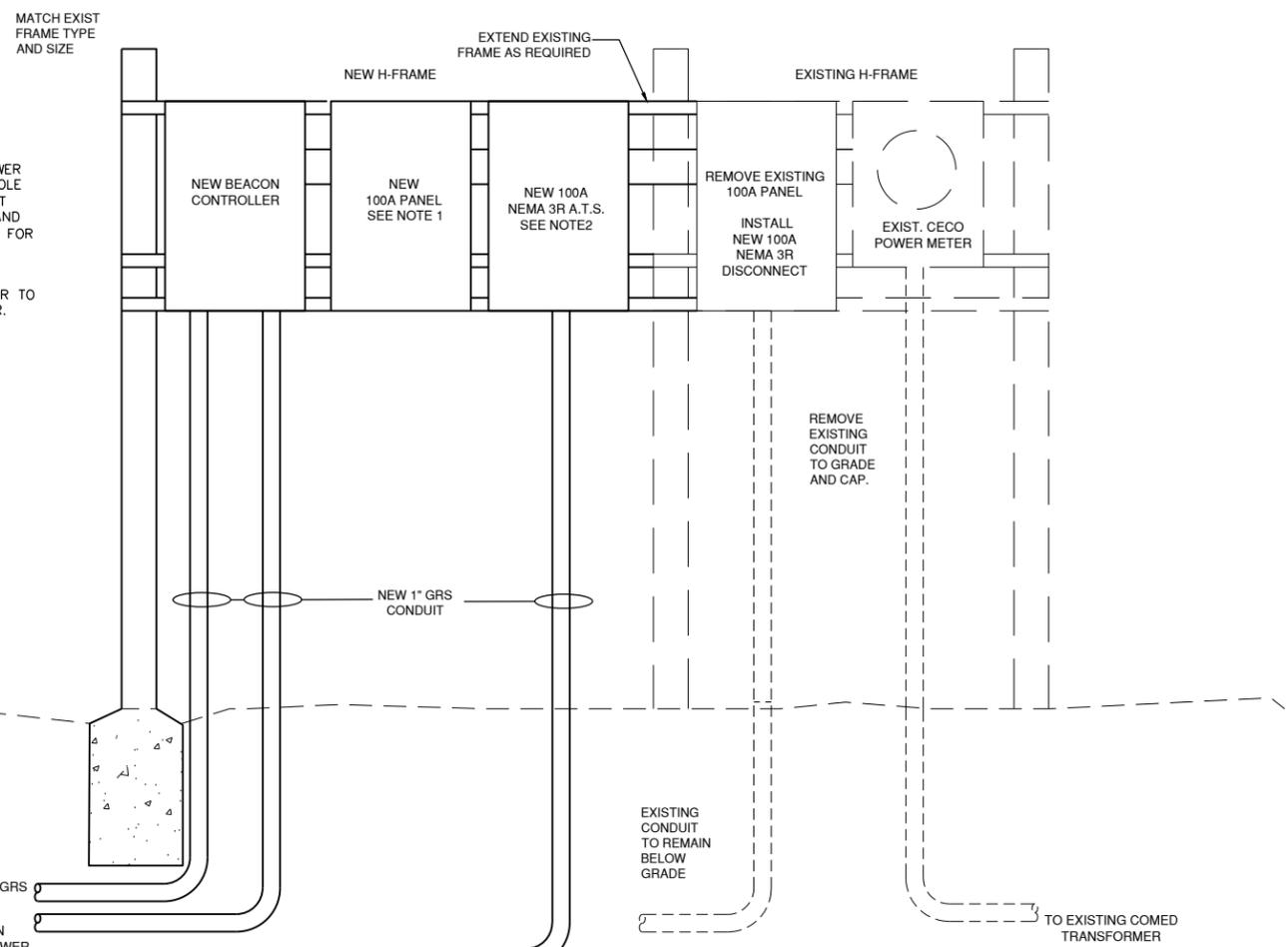
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**NOTES**

1. INSTALL NEW 100A, 120/240V POWER PANEL, NEMA 3R WITH 100A, 2-POLE MCB AND (1) 20A, 1-POLE CIRCUIT BREAKER FOR BEACON CONTROL AND (1) 30A 1-POLE CIRCUIT BREAKER FOR BEACON POWER.
2. NEW 100A, 2-POLE, AUTOMATIC TRANSFER SWITCH SHALL TRANSFER TO EMERGENCY WHEN LOSS OF POWER.
3. INSTALL NEW CONDUCTORS AND CONDUITS AS SHOWN ON POWER ONE-LINE.



**BEACON H-FRAME DETAILS**  
NOT TO SCALE

ALL NEW ITEMS AND ASSOCIATED WORK SHOWN ARE INCIDENTAL TO AS101510, AIRPORT ROTATING BEACON.

MATCH EXIST FRAME TYPE AND SIZE

EXTEND EXISTING FRAME AS REQUIRED

EXISTING H-FRAME

REMOVE EXISTING 100A PANEL

INSTALL NEW 100A NEMA 3R DISCONNECT

EXIST. CECCO POWER METER

REMOVE EXISTING CONDUIT TO GRADE AND CAP.

EXISTING CONDUIT TO REMAIN BELOW GRADE

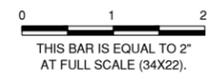
TO EXISTING COMED TRANSFORMER

NEW 1\"/>

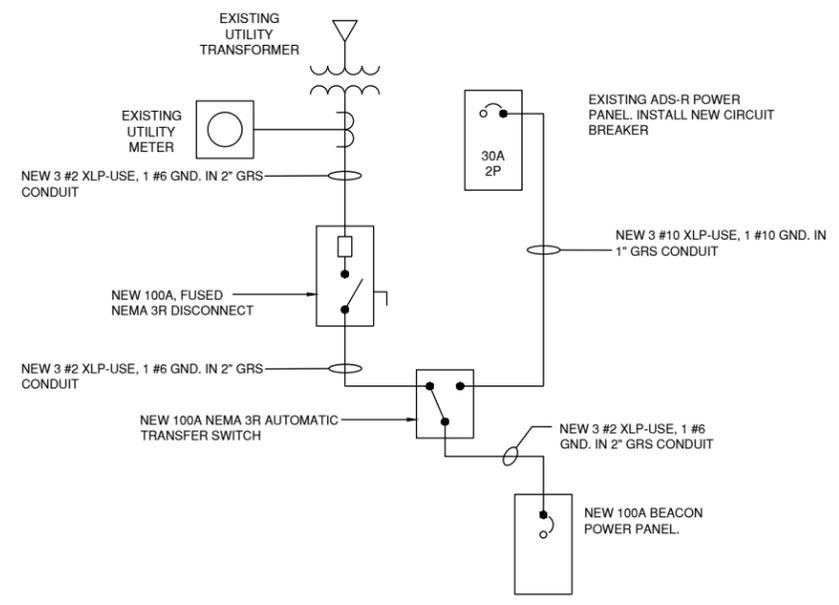
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IL. PROJECT: **PWK-4843**  
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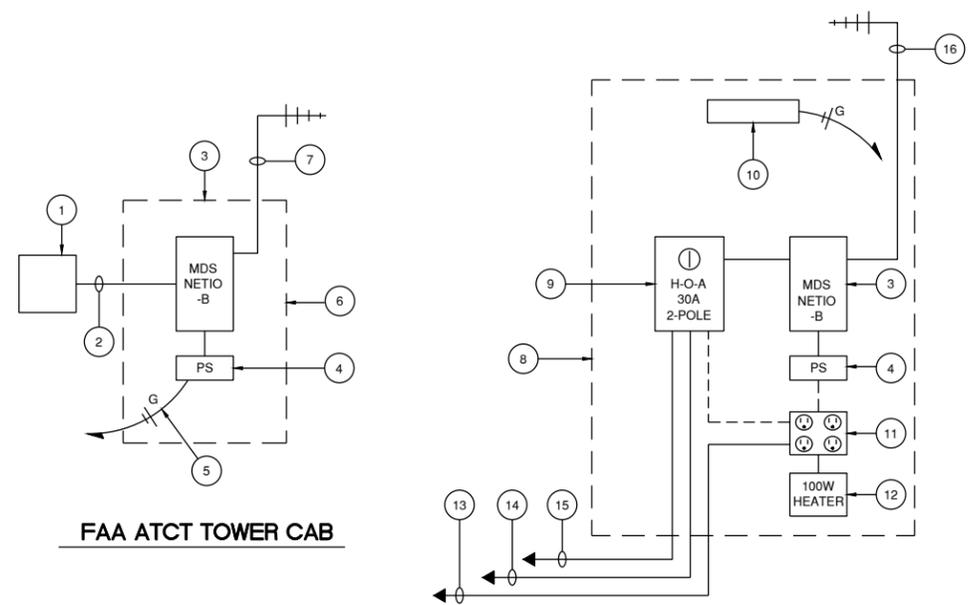
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CHICAGO EXECUTIVE AIRPORT  
WHEELING/PROSPECT HEIGHTS, ILLINOIS  
REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALCMS  
**ADDITIVE ALTERNATE #1  
BEACON - ELECTRICAL DETAILS**



**BEACON POWER ONE-LINE**



**BEACON CONTROL DIAGRAM**

**EQUIPMENT CONDUIT NOMENCLATURE**

- 1 NEW DCME (ALCMS) CONTROL FOR BEACON.
- 2 NEW 2 #12 THWN CONTROLS FROM DCME TO BEACON CONTROLLER.
- 3 NEW FCC LICENSED WIRELESS TRANSMITTER/RECEIVER WITH ANALOG AND DIGITAL I/O. CONTRACTOR SHALL OBTAIN LICENSE AND DESIGN EQUIPMENT..
- 4 NEW 24V DC POWER SUPPLY OR AS REQUIRED FOR WIRELESS TRANSMITTER/RECEIVER.
- 5 NEW 2 #12 THWN, 1 #12 GND. IN 3/4" CONDUIT, 120 VAC POWER FROM EXISTING CIRCUIT UNDER CABINET.
- 6 NEW 18"Hx12"Wx12"D NEMA 1 ENCLOSURE, SIZED FOR TRANSMITTER AND POWER SUPPLY, HOFFMAN OR EQUAL. INSTALL ENCLOSURE INSIDE CABINET UNDER L-890 TOUCHSCREEN.
- 7 NEW ANTENNA CABLE AND YAGI ANTENNA MOUNTED ON TOP OF FAA ATCT TOWER CAB. ANTENNA CABLE AND ANTENNA SHALL BE AS RECOMMENDED BY WIRELESS EQUIPMENT MANUFACTURER.
- 8 NEW 36"Hx24"Wx12"D NEMA 3R ENCLOSURE SIZED TO ACCOMMODATE RECEIVER, POWER SUPPLY, CONTRACTOR AND HEATER, HOFFMAN OR EQUAL.
- 9 NEW 30A, 2-POLE, 240V LIGHTING CONTRACTOR WITH 120 VAC COIL AND 3-POSITION HAND-OFF-AUTO SELECTOR SWITCH MOUNTED ON ENCLOSURE DOOR.
- 10 NEW ENCLOSURE LIGHT
- 11 NEW QUAD RECEPTACLE
- 12 NEW 100W HEATER WITH THERMOSTAT
- 13 NEW 2 #12 XLP-USE, 1 #12 GND. 120 VAC POWER FROM BEACON POWER PANEL.
- 14 NEW 2 #10 XLP-USE, 1 #10 GND. 120 VAC POWER FROM BEACON POWER PANEL.
- 15 NEW 2 #10 XLP-USE, 1 #10 GND. 120 VAC POWER TO BEACON.
- 16 NEW ANTENNA AND YAGI ANTENNA MOUNTED ON BEACON TOWER. ANTENNA CABLE AND ANTENNA SHALL BE AS RECOMMENDED BY WIRELESS EQUIPMENT MANUFACTURER.

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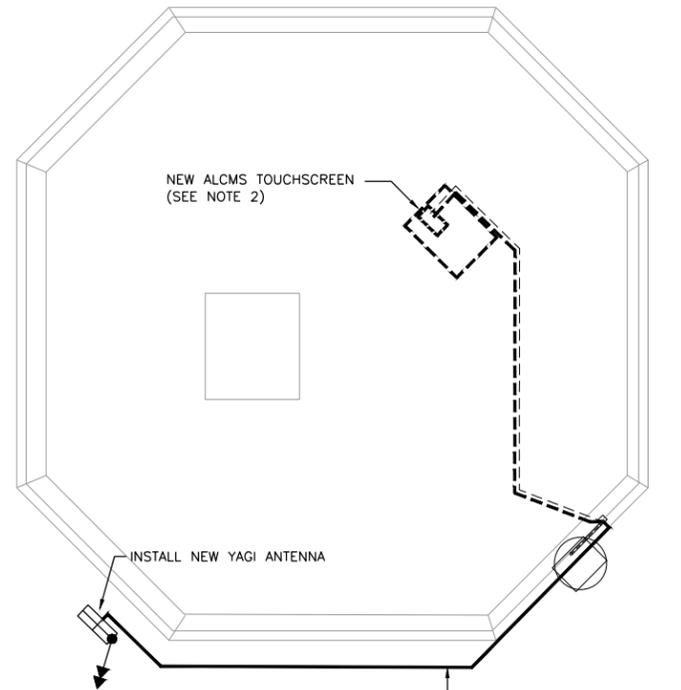
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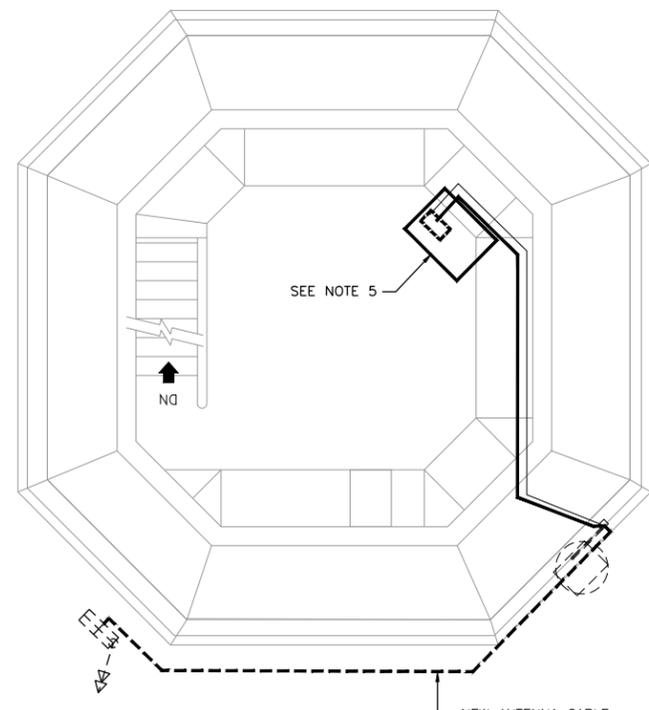
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**EXISTING BEACON**

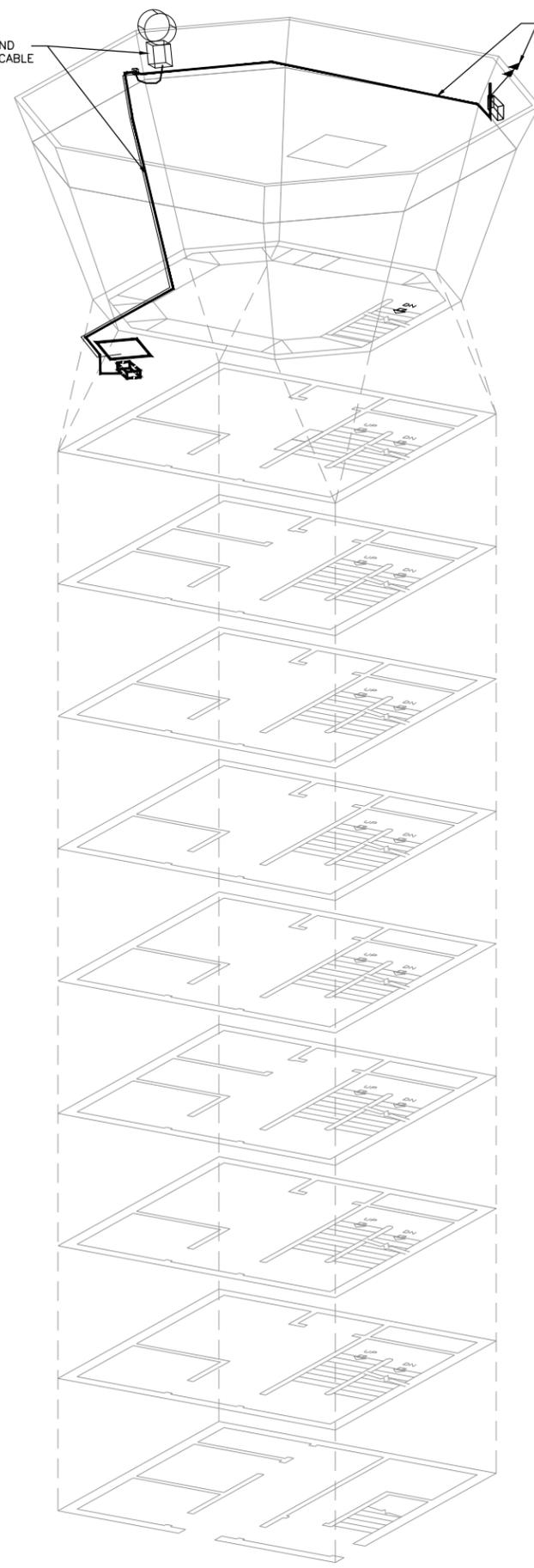


**ROOF PLAN**  
NOT TO SCALE

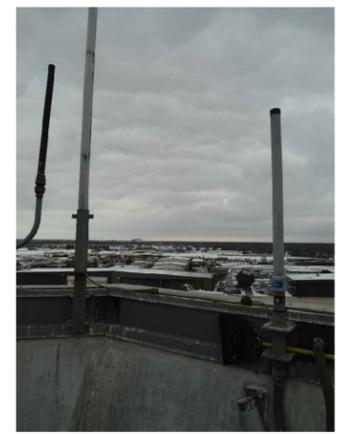


**ATCT TOWER CAB PLAN**  
NOT TO SCALE

REMOVE EXISTING BEACON AND BEACON POWER CABLES IN CABLE TROUGH / WIREWAY



**ATCT ELEVATION**  
NOT TO SCALE



**VIEW FROM WIRELESS ANTENNA TOWARDS ADS-B SITE**

INSTALL NEW YAGI ANTENNA AND CABLES. ANTENNA MOUNTING SHALL BE AS RECOMMENDED BY MANUFACTURER

**NOTES:**

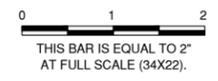
1. CONTRACTOR SHALL COORDINATE ALL WORK IN THE EXISTING CONTROL TOWER WITH THE FAA AIRWAYS FACILITIES REPRESENTATIVES, THE AIRPORT AND THE RESIDENT ENGINEER. CONTRACTOR SHALL GIVE A MINIMUM OF 7 DAYS NOTICE PRIOR TO BEGINNING WORK IN THE EXISTING TOWER.
2. CONTRACTOR SHALL INSTALL NEW BEACON WIRELESS TRANSMITTER IN ENCLOSURE INSIDE CABINET UNDER NEW ALCMS TOUCH SCREEN. CONNECT DIGITAL INPUT FROM BEACON PUSH BUTTON TO WIRELESS TRANSMITTER.
3. CONTRACTOR SHALL REMOVE EXISTING BEACON AND ASSOCIATED CONDUCTORS AFTER NEW BEACON IS OPERATIONAL.
4. CONTRACTOR TO PERFORM RADIO PATH STUDY FOR BEACON WIRELESS FOR ICC LICENSED COMMUNICATION AND ADJUST ANTENNA TYPE, HEIGHT AND DIRECTION AS NECESSARY, INCIDENTAL TO AS101510.
5. CONTRACTOR SHALL INSTALL NEW BEACON CONTROLLER, DCME, CONTROLLER POWER, ANTENNA, ANTENNA CABLE AND REMOVAL OF EXISTING BEACON TO PROVIDE COMPLETE AND OPERATIONAL SYSTEM TO THE SATISFACTION OF THE TOWER CHIEF, AIRPORT AND ENGINEER. ALL WORK TO BE PAID UNDER AS101510.
6. CONTRACTOR SHALL OBTAIN FCC LICENSE FROM RADIO CONTROL RADIO SERVICE (RCRS) FOR AUTHORIZED FREQUENCY TO OPERATE RADIO CONTROL. NEW WIRELESS EQUIPMENT SHALL BE DESIGNED TO OPERATE ON SPECIFIC FREQUENCY.

ALL NEW ITEMS AND ASSOCIATED WORK SHOWN ARE INCIDENTAL TO AS101510, AIRPORT ROTATING BEACON.

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IL LETTING ITEM: **08A**  
IL PROJECT: **PWK-4843**  
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**CHICAGO EXECUTIVE AIRPORT**  
**WHEELING/PROSPECT HEIGHTS, ILLINOIS**  
**REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALCMS**  
**ADDITIVE ALTERNATE #1**  
**BEACON - ATCT MODIFICATIONS**

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