LK014 TOTAL SHEETS = 41

LAKE IN THE HILLS AIRPORT LAKE IN THE HILLS, ILLINOIS

FINAL CONSTRUCTION PLANS FOR LAKE IN THE HILLS AIRPORT

REHABILITATE AND REPROFILE RUNWAY 8/26

ILLINOIS PROJECT: 3CK-4814 S.B.G. PROJECT: 3-17-SBGP-144/156/162/TBD

JUNE 18, 2021



JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS

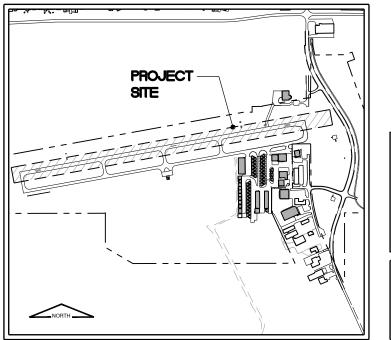
UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO A D D I T I O N A L C O S T T O T H E C O N T R A C T .

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.









DESIGN INFORMATION DESIGN AIRCRAFT APPROACH CATEGORY B

DESIGN AIRCRAFT GROUP II (CITATION EXCEL)

LAKE IN THE HILLS AIRPORT

TOWNSHIP: 43 NORTH RANGE: 8 EAST SECTION: 17 **COUNTY: MCHENRY** TOWNSHIP: ALGONQUIN

LOCATION MAP

SITE PLAN

INDEX TO SHEETS

	<u> </u>
SHEET NUMBER	SHEET TITLE
1	COVER SHEET
2	SUMMARY OF QUANTITIES AND INDEX TO SHEETS
3	SITE PLAN, ALIGNMENT LOCATIONS, CONTROL POINTS AND SITE ACCESS
4	SEQUENCE OF CONSTRUCTION PLAN
5	SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS - 1
6	SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS - 2
7	EXISTING CONDITIONS AND PROPOSED REMOVALS - SHEET 1
8	EXISTING CONDITIONS AND PROPOSED REMOVALS - SHEET 2
9	EXISTING CONDITIONS AND PROPOSED REMOVALS - SHEET 3
10	TYPICAL SECTIONS - 1
11	PLAN AND PROFILE RUNWAY 8-26 - SHEET 1
12	PLAN AND PROFILE RUNWAY 8-26 - SHEET 2
13	PLAN AND PROFILE RUNWAY 8-26 - SHEET 3
14	TAXIWAY A WEST AND RUNWAY 8 RSA PLAN AND PROFILE
15	TAXIWAY A1, A2 AND A3 PLAN AND PROFILE
16	RUNWAY 26 RSA PLAN AND PROFILE
17	ELECTRICAL PLAN - SHEET 1
18	ELECTRICAL PLAN - SHEET 2
19	ELECTRICAL PLAN - SHEET 3
20	ELECTRICAL DETAILS - 1
21	DRAINAGE DETAILS - SHEET 1
22	PAVEMENT MARKING PLAN - SHEET 1
23	PAVEMENT MARKING PLAN - SHEET 2
24	PAVEMENT MARKING DETAILS
25	STORM WATER POLLUTION PREVENTION PLAN - SHEET 1
26	STORM WATER POLLUTION PREVENTION PLAN - SHEET 2
27	STORM WATER POLLUTION PREVENTION PLAN - SHEET 3
28	STORM WATER POLLUTION PREVENTION PLAN NOTES AND DETAILS - SHEET 1
29	STORM WATER POLLUTION PREVENTION PLAN NOTES AND DETAILS - SHEET 2
30	INDEX TO SECTIONS
31	RSA WEST CROSS SECTIONS
32	RUNWAY CROSS SECTIONS - SHEET 1
33	RUNWAY CROSS SECTIONS - SHEET 2
34	RUNWAY CROSS SECTIONS - SHEET 3
35	RUNWAY CROSS SECTIONS - SHEET 4
36	RUNWAY CROSS SECTIONS - SHEET 5
37	RUNWAY CROSS SECTIONS - SHEET 6
38	RUNWAY CROSS SECTIONS - SHEET 7
39 40	RUNWAY CROSS SECTIONS - SHEET 8
40 41	RUNWAY CROSS SECTIONS - SHEET 9
41	RSA EAST CROSS SECTIONS

ITEM	DESCRIPTION	QUANTITY	UNIT
AR108088	1/C #8 XLP-USE	3,500	L.F.
AR108656	3/C #6 600 V UG CABLE IN UD	1,850	L.F.
AR110014	4" DIRECTIONAL BORE	45	L.F.
AR110502	2-WAY CONCRETE ENCASED DUCT	155	L.F.
AR110550	SPLIT DUCT	30	L.F.
AR115610	ELECTRICAL HANDHOLE	2	EACH
AR125100	ELEVATED RETROREFLECTIVE MARKER	5	EACH
AR125106	RELOC. ELEV. RETROREFLECTIVE MARKER	37	EACH
AR125901	REMOVE STAKE MOUNTED LIGHT	12	EACH
AR125931	REPLACE LIGHT LENSE	18	EACH
AR125941	ADJUST STAKE MOUNTED LIGHT	12	EACH
AR150510	ENGINEER'S FIELD OFFICE	1	L.S.
AR150520	MOBILIZATION	1	L.S.
AR152410	UNCLASSIFIED EXCAVATION	15,050	C.Y.
AR152440	BORROW EXCAVATION	3,800	C.Y.
AR152540	SOIL STABILIZATION FABRIC	1,150	S.Y.
AR156510	SILT FENCE	3,580	L.F.
AR156520	INLET PROTECTION	10	EACH
AR156531	EROSION CONTROL BLANKET	7,275	S.Y.
AR208515	POROUS GRANULAR EMBANKMENT	560	C.Y.
AR209608	CRUSHED AGG. BASE COURSE - 8"	32,815	S.Y.
AR401610	BITUMINOUS SURFACE COURSE	4,275	TON
AR401620	R401620 BIT. SURFACE COURSE, LEVELING		TON
AR401630	401630 BITUMINOUS SURFACE TEST SECTION		EACH
AR401650	BITUMINOUS PAVEMENT MILLING	4,250	S.Y.
AR401900	REMOVE BITUMINOUS PAVEMENT	21,015	S.Y.
AR403610	BITUMINOUS BASE COURSE	7,415	TON
AR602510	BITUMINOUS PRIME COAT	9,875	GAL.
AR603510	BITUMINOUS TACK COAT	10,615	GAL.
AR620520	PAVEMENT MARKING - WATERBORNE	19,180	S.F.
AR620525	PAVEMENT MARKING - BLACK BORDER	8,990	S.F.
AR620900	PAVEMENT MARKING REMOVAL	50	S.F.
AR705526	6" PERFORATED UNDERDRAIN W/ SOCK	660	L.F.
AR705640	UNDERDRAIN CLEANOUT	7	EACH
AR705900	REMOVE UNDERDRAIN	485	L.F.
AR705904	REMOVE UNDERDRAIN CLEANOUT	7	EACH
AR901510	SEEDING	10.0	ACRE
AR908515	HEAVY-DUTY HYDRAULIC MULCH	10.0	ACRE

IL. CONTRACT: LK014

IL. LETTING ITEM: 01A IL. PROJECT: 3CK-4814 S.B.G. PROJECT: 3-17-SBGP-144/156/166/TBD

SURVEY BOOK # ----

REVISIONS					
NUMBER	BY	DATE			

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS REHABILITATE AND REPROFILE RUNWAY 8/26 SUMMARY OF QUANTITIES AND INDEX TO SHEETS

VILLAGE OF LAKE IN THE HILLS

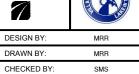
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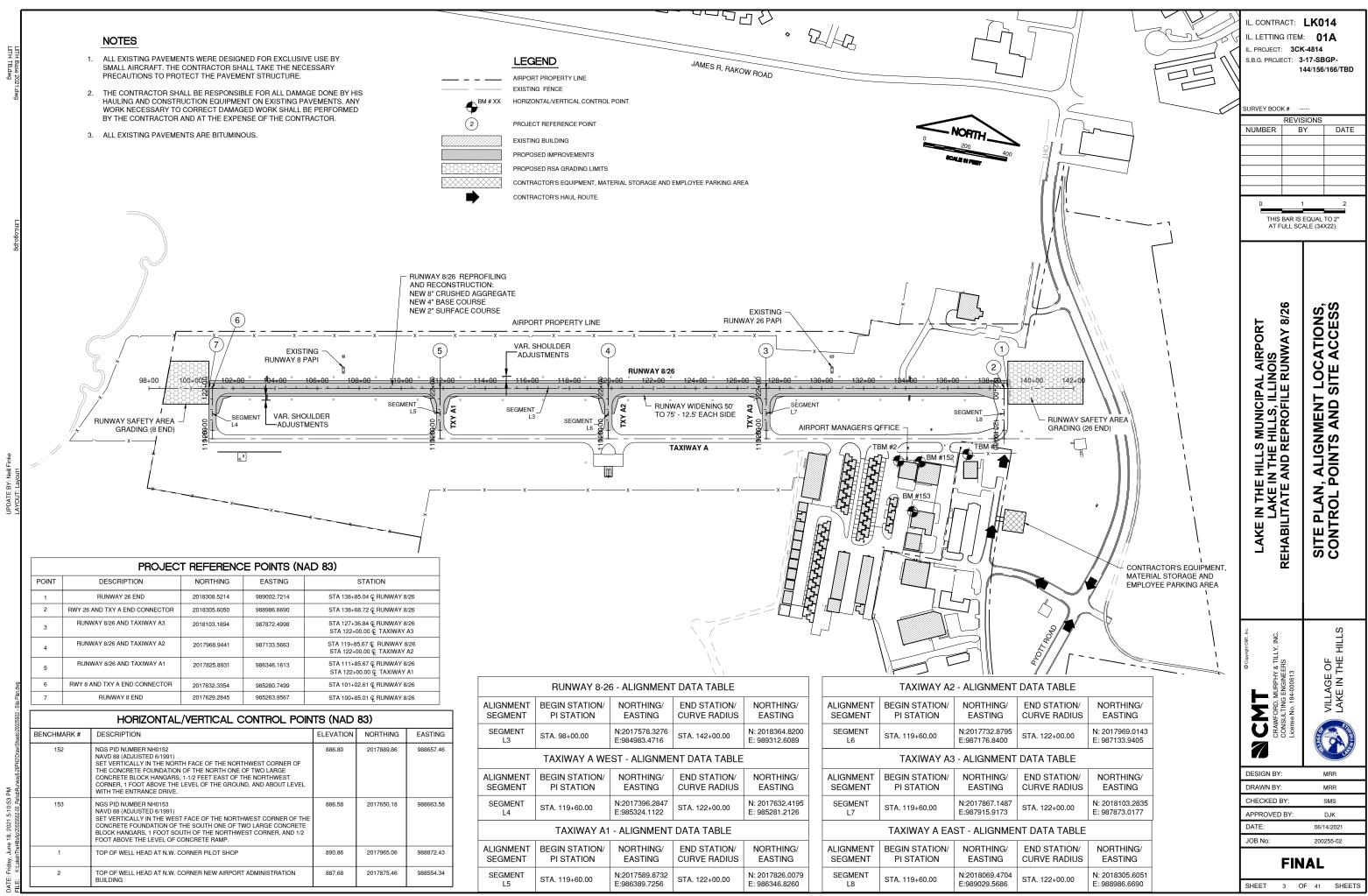


DJK 06/18/2021

JOB No: 200255-02

FINAL

SHEET 2 OF 41 SHEETS



EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS

/	
RUNWAY	8/26
APPROACH CATEGORY	В
DESIGN GROUP	II
DESIGN AIRCRAFT	CITATION EXCE
WINGSPAN	55.7'
TAIL HEIGHT	17.2'
RUNWAY SAFETY AREA WIDTH (RSA)	150'
RUNWAY OBJECT FREE AREA WIDTH (ROFA)	500'
TAXIWAY SAFETY AREA WIDTH (TSA)	79'
TAXIWAY OBJECT FREE AREA WIDTH (TOFA)	131'
TAXILANE OBJECT FREE AREA WIDTH (TOFA)	115'
	•

PRIOR TO REOPENING A CLOSED RUNWAY, THE ENTIRE (RSA) RUNWAY SAFETY AREA (75 FEET FROM CENTERLINE AND INCLUDING BEYOND THE END OF THE RUNWAY WITHIN AIRPORT PROPERTY) AND (TOFA) TAXIWAY OBJECT FREE AREA MUST MEET FAA CRITERIA. FAA CRITERIA REQUIRES THAT THERE BE NO OPEN EXCAVATIONS OR TRENCHES IN THE SAFETY AREA(S), THE MAXIMUM PAVEMENT DROP OFF BE 3 INCHES, AND ALL GRADES IN ANY DIRECTION BE LESS THAN 5 PERCENT, STEEL PLATES, TEMPORARY WEDGING OF BASE COURSE

AND BITUMINOUS CONCRETE MAY BE REQUIRED TO MEET CRITERIA.
ALL NECESSARY TEMPORARY MEASURES SHALL BE CONSIDERED
INCIDENTAL TO THE CONTRACT.

LEGEND

CONTRACTOR'S WORK AREA CONTRACTOR'S STAGING/STORAGE AREA

PAVEMENT CLOSED TO TRAFFIC AIRPORT PROPERTY LINE

EXISTING FENCE LOW PROFILE BARRICADES

ACCESS/HAUL ROUTE RUNWAY SAFETY AREA

> RUNWAY OBJECT FREE AREA OFF PAVEMENT TEMPORARY CLOSED RUNWAY MARKER

> > -ROFA-**TAXIWAY A**

LIGHTED TEMPORARY CLOSED RUNWAY MARKER (LIGHTED "X")

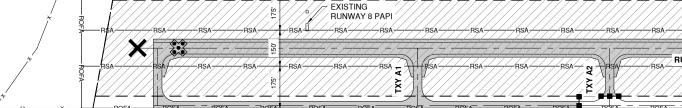
JAMES R. RAKOW ROAD

EXISTING

RUNWAY 26 PAPI

AIRPORT PROPERTY LINE

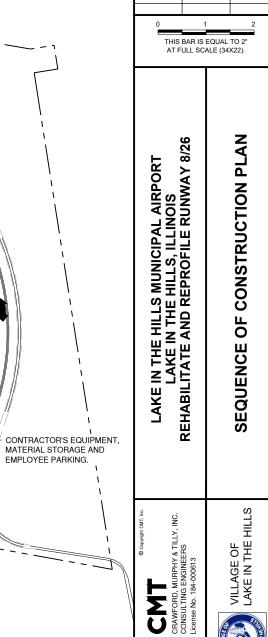
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ALLOWABLE **OPERATIONAL** STATUS/RESTRICTIONS **WORK PERIODS** 67 CALENDAR DAYS (CONSECUTIVE) RUNWAY 8/26 - CLOSED, DAY AND NIGHT (MAIN RUNWAY CONSTRUCTION) RUNWAY 8/26 - OPEN, DAY AND NIGHT (TO ALLOW FOR NEW BITUMINOUS PAVEMENT TO CURE PRIOR TO FULL PAVEMENT MARKING) 30 CALENDAR DAYS OF SUSPENDED WORK (CONSECUTIVE) RUNWAY 8/26 - CLOSED DURING DAY AND OPEN DURING NIGHT (FOR FINAL PAVEMENT MARKING) 2 CALENDAR DAYS

NOTES

- CONTRACTOR SHALL NOTIFY THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER 30 DAYS PRIOR TO THE INITIATION OF ANY WORK WHICH REQUIRES THE CLOSURE OF ACTIVE AIRFIELD PAVEMENT.
- 2. CONTRACTOR SHALL TAKE CARE NOT TO DAMAGE EXISTING AIRFIELD LIGHTING AND DRAINAGE STRUCTURES DURING HAULING AND CONSTRUCTION ACTIVITIES. ANY DAMAGED ITEMS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- 3. CLOSED RUNWAY MARKERS SHALL BE PLACED OFF OF THE ENDS OF THE RUNWAY DURING CONSTRUCTION, AND RELOCATED AS NEEDED DURING RSA GRADING OPERATIONS (COST INCIDENTAL TO THE CONTRACT).
- 4. CONTRACTOR SHALL BE REQUIRED TO DISCONNECT ALL AIRFIELD LIGHTING CIRCUITS FOR THE DURATION OF THE RUNWAY CLOSURE.
- 5. CONTRACTOR SHALL BE REQUIRED TO SHUTDOWN/DISCONNECT THE PAPI UNITS ON BOTH THE RUNWAY 8 AND RUNWAY 26 ENDS. PAPI UNITS WILL NOT RETURN TO OPERATION UNTIL AFTER A FLIGHT CHECK HAS BEEN COMPLETED.



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DESIGN BY:

DRAWN BY:

CHECKED BY

APPROVED BY

JOB No:

MRR

MRR

SMS

D.IK

06/18/2021

200255-02

FINAL SHEET 4 OF 41 SHEETS

L. CONTRACT: **LK014** IL. LETTING ITEM: 01A

IL. PROJECT: 3CK-4814

URVEY BOOK # ---

S.B.G. PROJECT: 3-17-SBGP-

REVISIONS

NUMBER BY DATE

144/156/166/TBD

THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G OR LATEST EDITION, AND

ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS

- PRIOR TO THE NOTICE TO PROCEED, THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT THROUGH THE RESIDENT ENGINEER, FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G OR LATEST EDITION. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRE-CONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRE-CONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT

2. PHASING

- TOTAL CONTRACT TIME SHALL BE XX CALENDAR DAYS.
- PHASING SHALL BE AS SHOWN ON THE SEQUENCE OF CONSTRUCTION SHEET.
- A TYPICAL PHASE OF WORK INCLUDES THE FOLLOWING: CONTRACTOR NOTIFIES AIRPORT SEVEN DAYS PRIOR TO INITIATING THE PHASE; CONTRACTOR DEACTIVATES LIGHTING AND SIGNAGE CIRCUITS AFFECTED BY THE WORK AREA: FAA ATO DEACTIVATES AFFECTED NAVAIDS CONTRACTOR PLACES BARRICADES AND OTHER HAZARD MARKING/LIGHTING: CONTRACTOR INITIATES WORK.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL ALITHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. NAVAIDS THAT COULD BE AFFECTED

- THE CONTRACTOR MUST COORDINATE WITH AIRPORT OPERATIONS/ENGINEER IN ADVANCE FOR ANY WORK WITHIN A NAVAID CRITICAL AREA OR AFFECTING THE VISUAL, TRANSMITTED SIGNAL OR
- EDGE LIGHTS, THRESHOLD LIGHTS, VISUAL AIDS AND ALL ILS EQUIPMENT SHALL BE SHUT OFF FOR THE DURATION OF A CLOSURE PERIOD ON ANY ASSOCIATED PAVEMENTS. IF THE LIGHTING CIRCUIT MUST BE ON FOR OPEN PAVEMENT AREAS, CLOSED PAVEMENT AREA LIGHTS SHALL BE COVERED COMPLETELY
- 3. EXCEPT WHERE NOTED IN THE PLANS, EXISTING COMMUNICATIONS EQUIPMENT AND NAVIGATIONAL AIDS (NAVAIDS) SHALL NOT BE DISTURBED BY THE CONTRACTOR AND SHALL BE PROTECTED FROM DAMAGE
- PRIOR TO BEGINNING SITE WORK, CONTRACTOR SHALL COORDINATE THROUGH AIRPORT OPERATIONS TO LOCATE, AND THE CONTRACTOR TO MARK ALL UNDERGROUND COMMUNICATIONS CABLES AND FACILITIES, WITHIN THE PROJECT AREA
- IF CONTRACTOR CAUSES INTERRUPTION OF POWER OR COMMUNICATIONS TO A NAVAID CONTRACTOR SHALL REPAIR WITHIN 24 HOURS AT THE CONTRACTOR'S COST. CONTRACTOR MUST COORDINATE REPAIR WITH AIRPORT OPERATIONS BEFORE ANY REPAIR IS MADE.

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS, ALL COSTS BELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATE(S) SHOWN, THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE CLOSED DURING WORK HOURS
- CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
- CONTRACTOR'S VEHICLES AND FOUIPMENT SHALL BE MARKED AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S FOUIPMENT WILL BE 25'.
- DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, ASPHALT, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE BEQUIRED TO SUBMIT THEIR NAME. DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA FENCE, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR PERSONNEL THAT HAS OBTAINED PROPER DRIVING PRIVILEGES.
- CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIRPORT UNICOM (122.70 MHZ) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY SATISFIED THE AIRPORT OF THEIR COMPETENCE MAY OPERATE THESE RADIOS.
- THE CONTRACTORS STORAGE AND STAGING AREAS WILL BE AS SHOWN ON THE SITE PLAN.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREAS.
- DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT, NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTINGNAVIGABLE AIRSPACE EXISTING TUBE AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC 12. SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE COMMON TRAFFIC ADVISORY FREQUENCY. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS. OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION, NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR
- ALL VEHICLE AND FOLIPMENT OPERATORS LISED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY THE AIRPORT OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 10 DAYS NOTICE TO THE FAA AND AIRPORT PRIOR TO THE CLOSURE OF ANY RUNWAY SO THAT THE FAA MAY DEACTIVATE THE FAA - OWNED NAVAIDS.
- THE CONTRACTOR SHALL GIVE A MINIMUM 30 DAYS NOTICE TO THE AIRPORT AND PRIOR TO THE PRE-CONSTRUCTION CONFERENCE PRIOR TO CLOSING ANY RUNWAY OR TAXIWAY PAVEMENT SO THAT THE PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT FOR COORDINATION WITH THE AIRPORT TENANTS
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE FOUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBISTE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2G OR LATEST EDITION MAY BE USED TO AID IN THE INSPECTIONS
- THE CONTRACTOR SHALL REQUEST OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE

11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION ANY LITH ITY INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE LITILITY ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL JULI JE AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION

12. PENALTIES

NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPF AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED

13. SPECIAL CONDITIONS

ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION

14. RUNWAY AND TAXIWAY VISUAL AIDS

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT THE CONTRACTOR SHALL LISE MARKING LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC

15. MARKING AND SIGNS FOR ACCESS ROUTES

BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THE CONSTRUCTION ACTIVITY PLAN SHEETS

16. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, FRECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT
- 2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5C OR LATEST EDITION AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

17. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTION.
- LIGHTS SHALL CONSIST OF VEHICLE OR MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE, LIGHTING SHALL NOT INTERFERE WITH AIR OPERATIONS. ANY WORK BEING PERFORMED UNDER INSUFFICIENT ARTIFICIAL LIGHTING, IN THE RESIDENT ENGINEER'S JUDGEMENT, SHALL BE STOPPED UNTIL SUCH TIME AS ADDITIONAL LIGHTING IS PROVIDED. ALL WORK PERFORMED DURING THAT TIME WILL NOT BE ACCEPTABLE UNTIL PROPER INSPECTION AND TESTING CAN BE MADE

18. PROTECTION

- 1. ALL WORK REQUIRED INSIDE OF A RUNWAY SAFETY AREA, WILL REQUIRE THE
- 2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WILL REQUIRE THE TAXIWAY TO BE CLOSED.

19. OTHER LIMITATIONS ON CONSTRUCTION

- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND FOUIPMENT
- BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE CONTRACT DOCUMENTS.

IL. CONTRACT: **LK014**

144/156/166/TBD

IL. LETTING ITEM: 01A IL. PROJECT: 3CK-4814 S.B.G. PROJECT: 3-17-SBGP-

URVEY BOOK # ---

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THIS BAR IS FOUNDED TO 2" AT FULL SCALE (34X22).

E HILLS MUNICIPAL AIRPORT IN THE HILLS, ILLINOIS E AND REPROFILE RUNWAY 8 Ωŵ TRUCTION SONS! ICE OF CO EQUENCE NERAL LAKE IN THE LAKE II REHABILITATE

8/26

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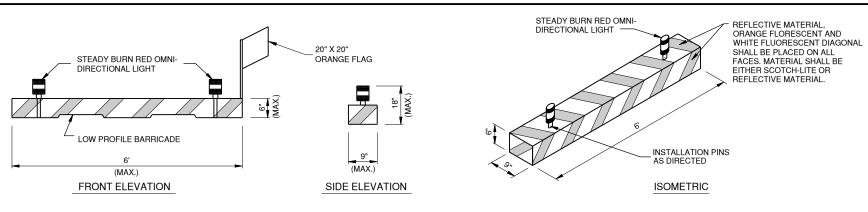
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DESIGN BY DRAWN BY: MRR CHECKED BY SMS APPROVED BY D.IK JOB No: 200255-02

FINAL

SHEET 5 OF 41 SHEETS



LOW PROFILE LIGHTED BARRICADE NOTES

- 1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
- 2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
- 3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- 4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT
- BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
- 6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
- 7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY SAFETY AREAS.
- 8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

IL. PROJECT: 3CK-4814 S.B.G. PROJECT: 3-17-SBGP-144/156/166/TBD

IL. CONTRACT: **LK014**

IL. LETTING ITEM: 01A

URVEY BOOK # ---REVISIONS NUMBER BY

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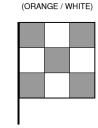
AT FULL SCALE (34X22).

LOW-PROFILE LIGHTED BARRICADE DETAILS

IDOT TYPE 1 BARRICADES WITH LIGHTS PLACED AT LOW-PROFILE BARRICADES WITH LIGHTS PLACED AT TSA TOFA BOUNDARY **BOUNDARY** - OFA BOUNDARY ACTIVE TAXIWAY

CLOSED TAXIWAY/ACTIVE TAXIWAY BARRICADE DETAIL

N.T.S.



CONSTRUCTION EQUIPMENT AND VEHICLE SIGNAL FLAG

SIGNAL FLAG NOTES

- ALL CONTRACTOR VEHICLES AND EQUIPMENT SHALL DISPLAY COMPANY LOGO PLACARDS AND FLAG.
- WHEN WORKING PRIOR TO DAWN OR AFTER DUSK, A 360 DEGREE ROTATING AMBER BEACON IS REQUIRED ON ALL EQUIPMENT AND
- 3. CONTRACTOR SHALL REPLACE FLAGS THAT ARE WORN AND INEFFECTIVE.

LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS EHABILITATE AND REPROFILE RUNWAY 8/26

SEQUENCE OF CONSTRUCTION ENERAL NOTES AND DETAILS -

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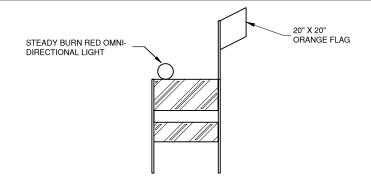
FINAL SHEET 6 OF 41 SHEETS



LIGHTED RUNWAY CLOSURE MARKER

RUNWAY CLOSURE NOTES

- TO BE PLACED ON PAVEMENT AT THE RUNWAY NUMERALS WHEN CONSTRUCTION ACTIVITIES ALLOW 80' FROM THE ENDS OF RUNWAY 8/26 WHEN CONSTRUCTION ACTIVITIES WILL NOT
- THE CONTRACTOR SHALL PROVIDE TWO LIGHTED CLOSURE MARKERS AND MAINTAIN THEM (FUEL, OIL, LIGHT BULBS) DURING THE CLOSURE OF RUNWAY 8/26.
- THE CONTRACTOR WILL BE REQUIRED TO PROVIDE REPLACEMENT BULBS, FUEL, LUBRICANTS, AND DAILY/PERIODIC MAINTENANCE INSPECTIONS AS REQUIRED BY THE AIRPORT.
- 4. THE COST OF SET-UP, FUELING, BULBS, INSPECTION, AND REMOVAL OF THE MARKERS SHALL BE INCIDENTAL TO THE CONTRACT.



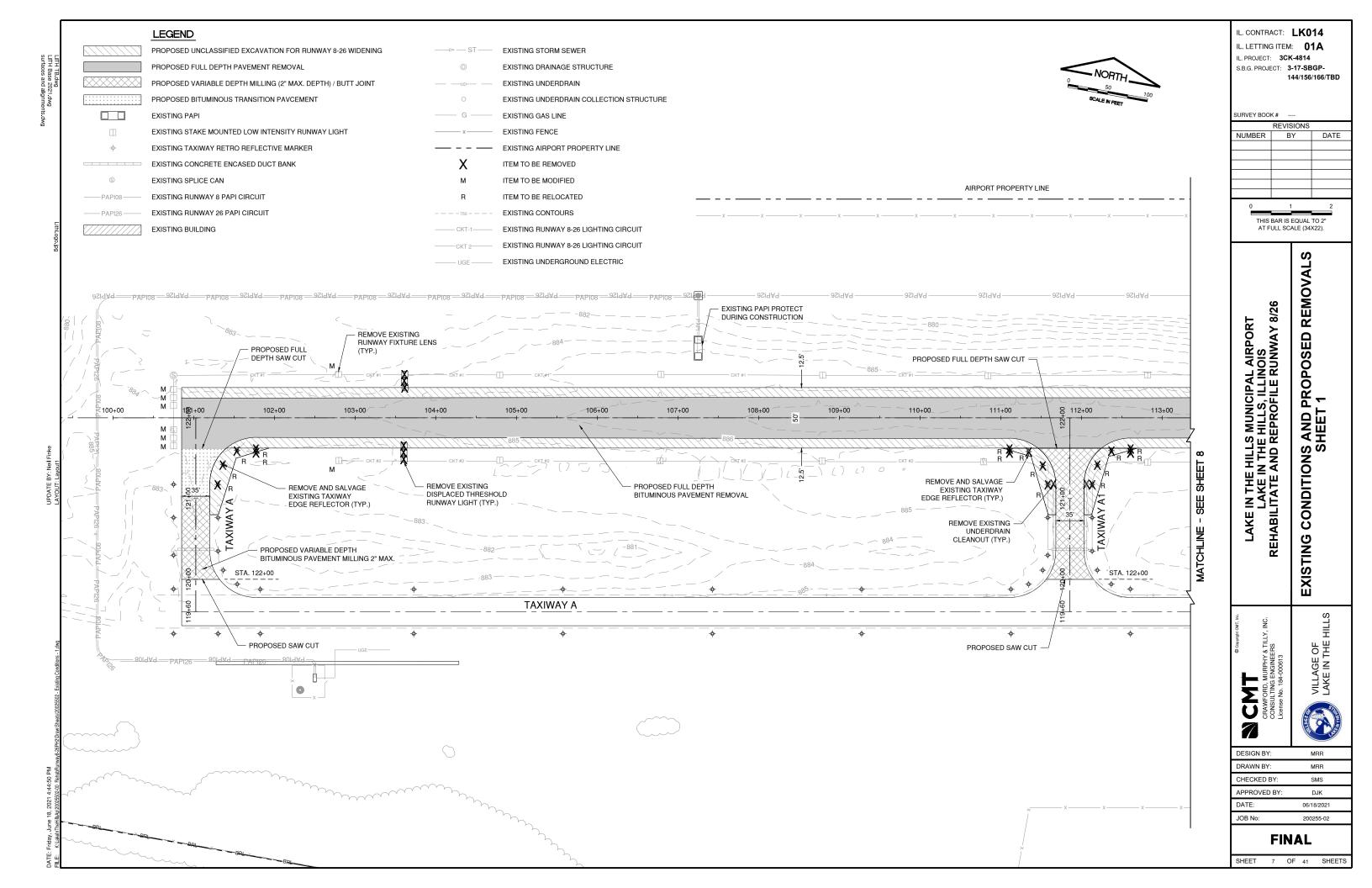
FLASHER BARRICADE DETAIL - IDOT TYPE 1

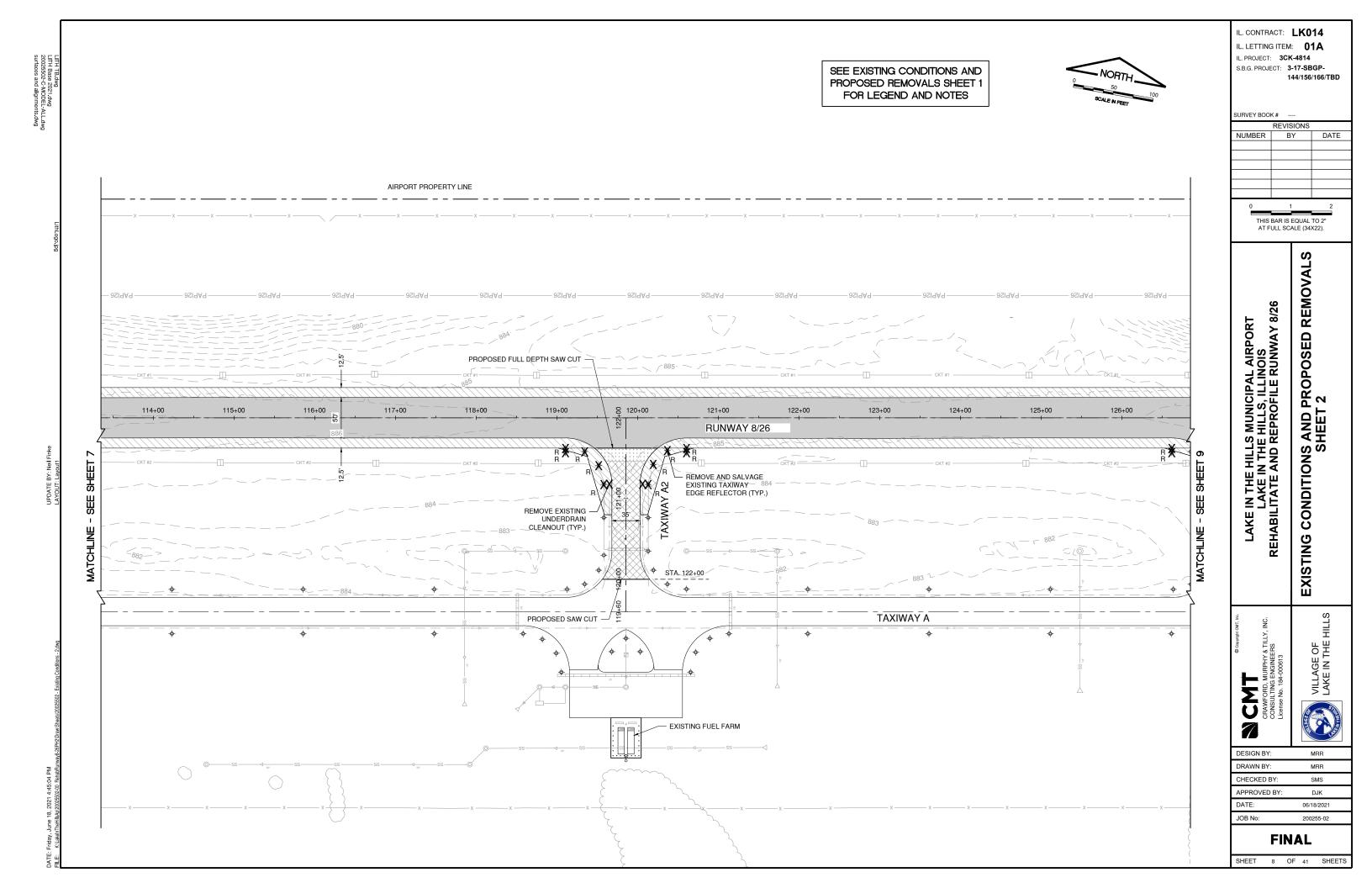
FLASHER BARRICADE NOTES

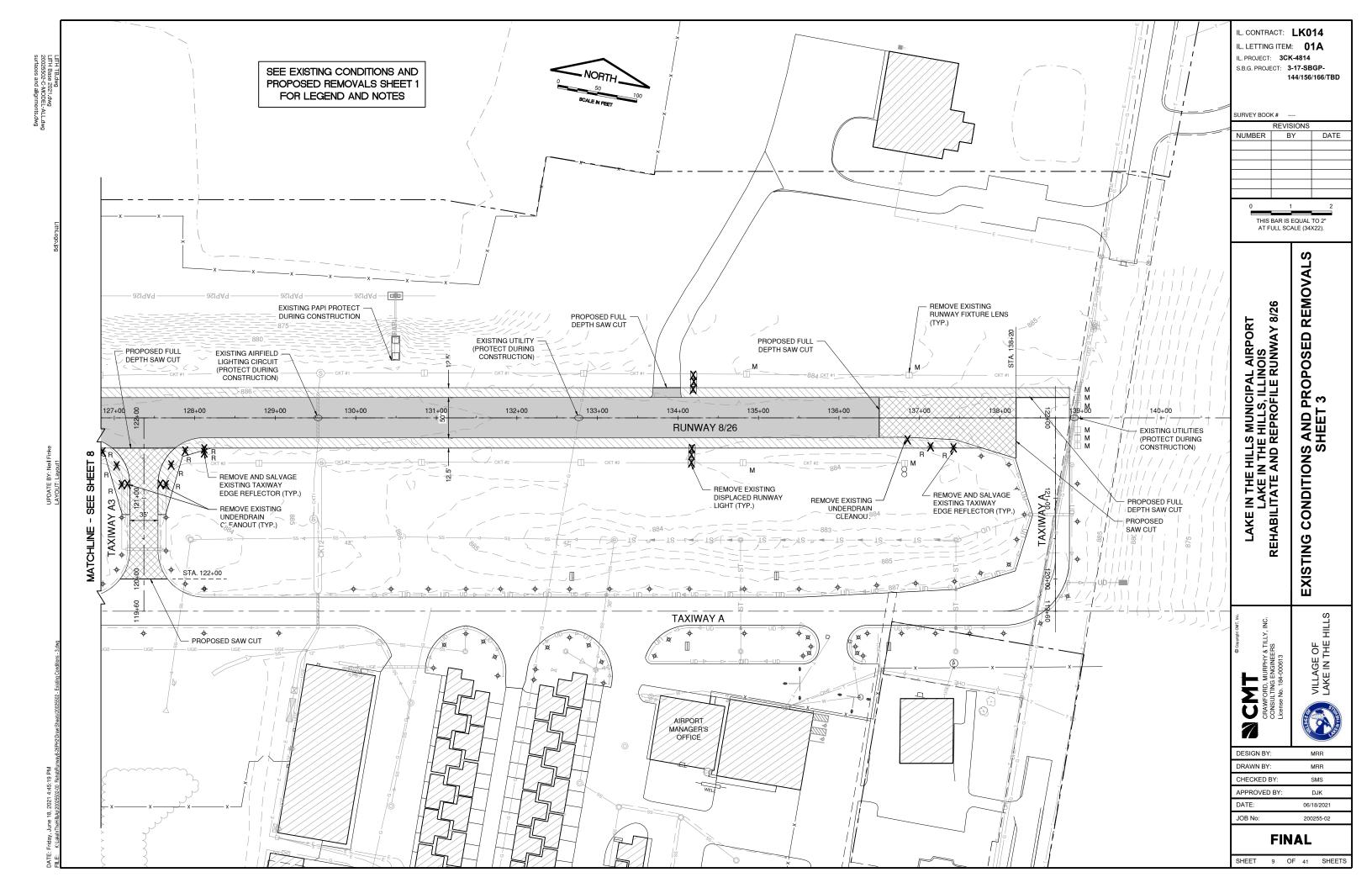
- 1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
- 2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR
- 3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- 4. PLACE AT 15' INTERVALS

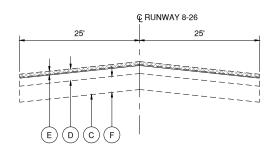


END OF RUNWAY



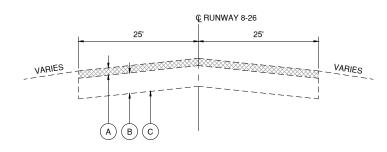






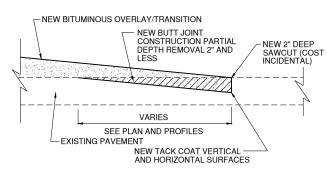
RUNWAY 8-26 EXISTING CONDITIONS AND REMOVALS TYPICAL SECTION A-A TAXIWAY A, A1, A2, A3 EXISTING CONDITIONS AND REMOVALS TYPICAL SECTION A-A

NOT TO SCALE

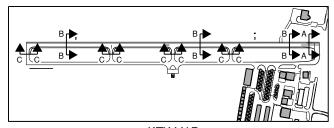


RUNWAY 8-26 EXISTING CONDITIONS AND REMOVALS TYPICAL SECTION B-B

NOT TO SCALE



BUTT JOINT DETAIL NOT TO SCALE

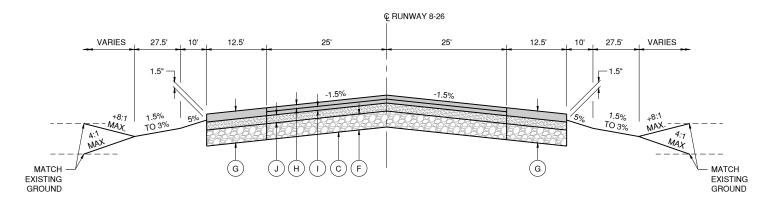


KEY MAP NOT TO SCALE

1. TARGET AIR VOIDS FOR 401 AND 403 ASPHALT DESIGN SHALL BE 3%.

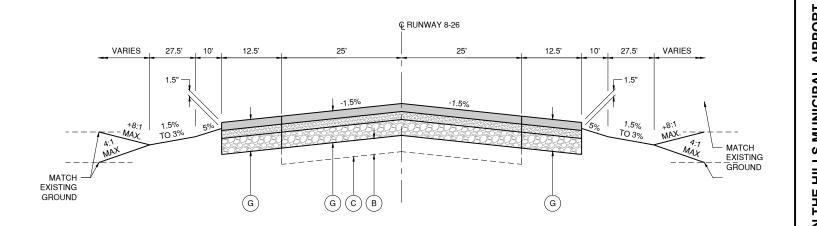
LEGEND

- FULL DEPTH PAVEMENT REMOVAL VARIES
- (B) EXISTING CRUSHED AGGREGATE BASE COURSE - VARIES
- EXISTING SUBBASE
- EXISTING 1.5" BITUMINOUS SURFACE COURSE EXISTING 4.5" BITUMINOUS BASE COURSE
- VARIABLE DEPTH MILLING 2" DEPTH MAX.
- (F) EXISTING 8" CRUSHED AGGREGATE BASE COURSE
- (G) NEW AIRFIELD PAVEMENT STRUCTURE 2" BITUMINOUS SURFACE COURSE (401) 4" BITUMINOUS BASE COURSE (401) 8" CRUSHED AGGREGATE BASE COURSE (209)
- NEW 2" BITUMINOUS SURFACE COURSE (401)
- NEW BITUMINOUS LEVELING COURSE VARIES (401)
- EXISTING 4" BITUMINOUS BASE COURSE



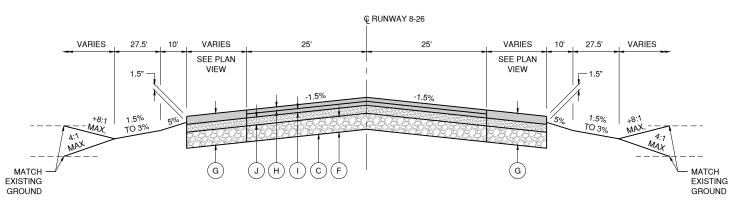
PROPOSED VARIBALE DEPTH BITUMINOUS MILLING (2" DEPTH) / BUTT JOINT AND 12.5' WIDENING RUNWAY 8-26 TYPICAL SECTION A-A STA. 136+50 TO STA. 138+86

NOT TO SCALE



RUNWAY 8-26 PROPOSED FULL DEPTH BITUMINOUS PAVEMENT AND 12.5' WIDENING STA. 100+85 TO STA. 136+50 TYPICAL SECTION B-B

NOT TO SCALE



PROPOSED TAXIWAY A, A1, A2, A3 BITUMINOUS PAVEMENT TRANSITION TYPICAL SECTION C-C

NOT TO SCALE

IL. CONTRACT: **LK014** IL. LETTING ITEM: 01A IL. PROJECT: 3CK-4814 S.B.G. PROJECT: 3-17-SBGP-144/156/166/TBD

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	REVISIONS	
NUMBER	BY	DATE
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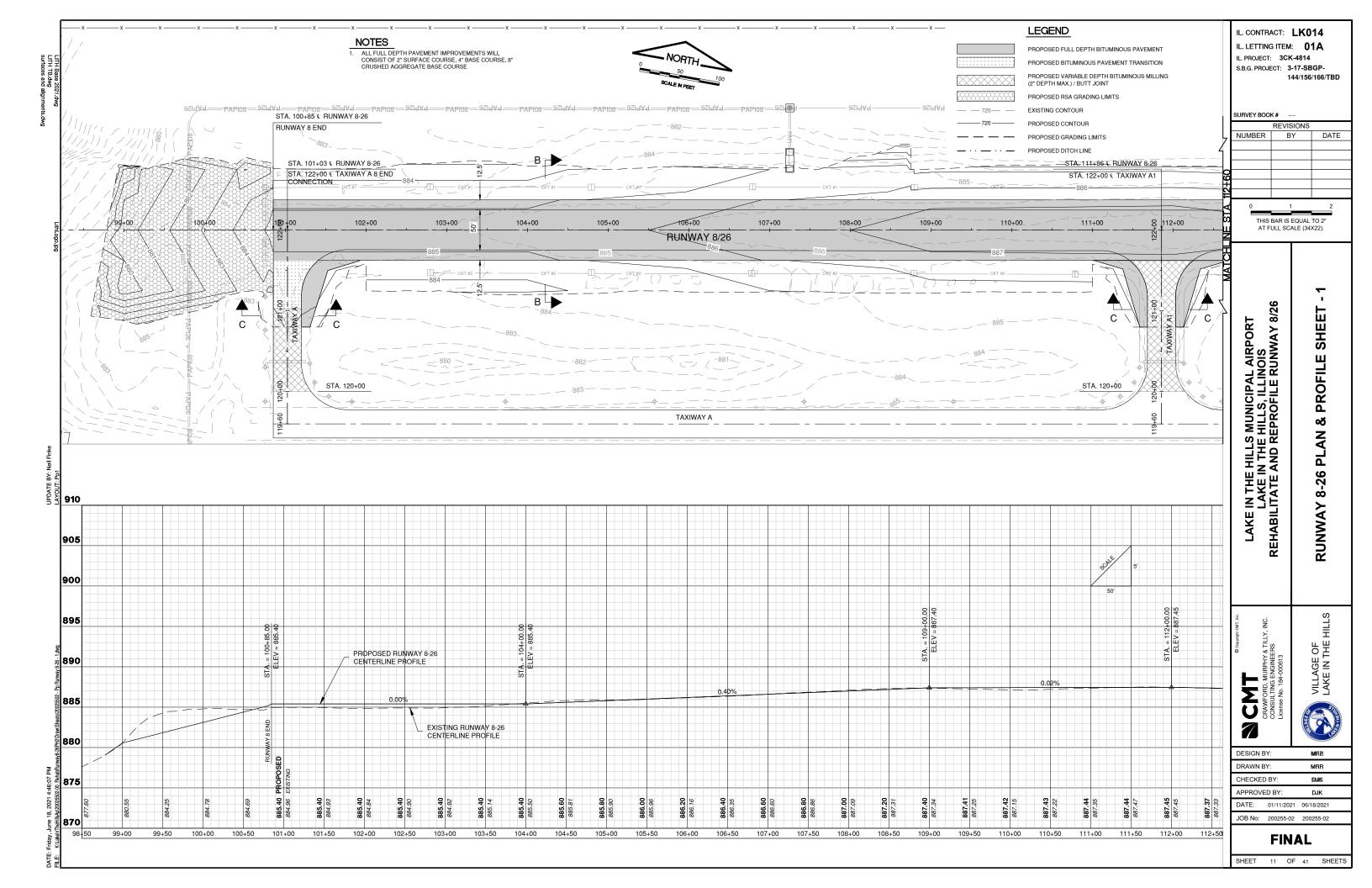
8/26 LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS EHABILITATE AND REPROFILE RUNWAY 8 TYPICAL SECTIONS

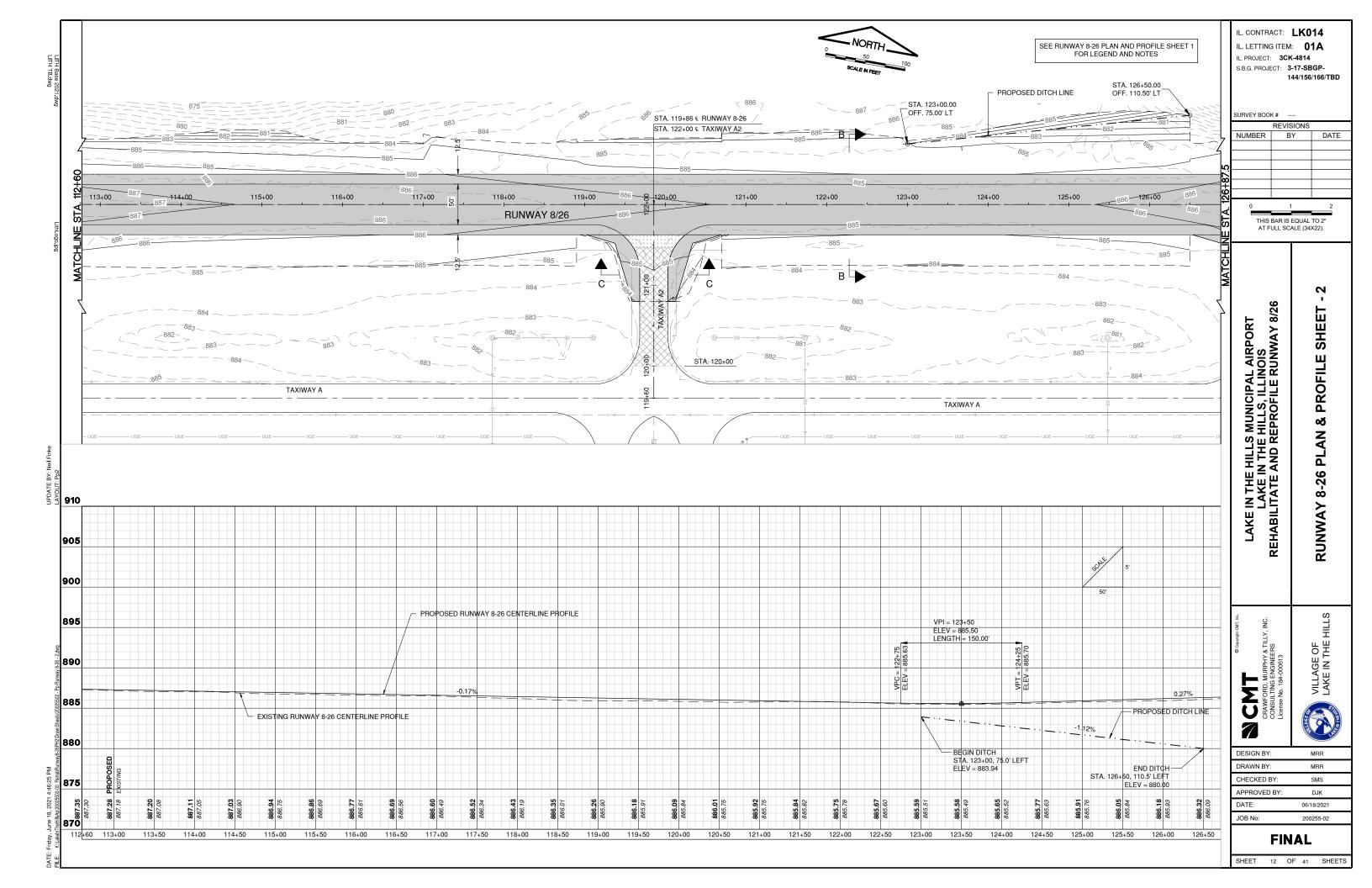
VILLAGE OF LAKE IN THE HILL L W

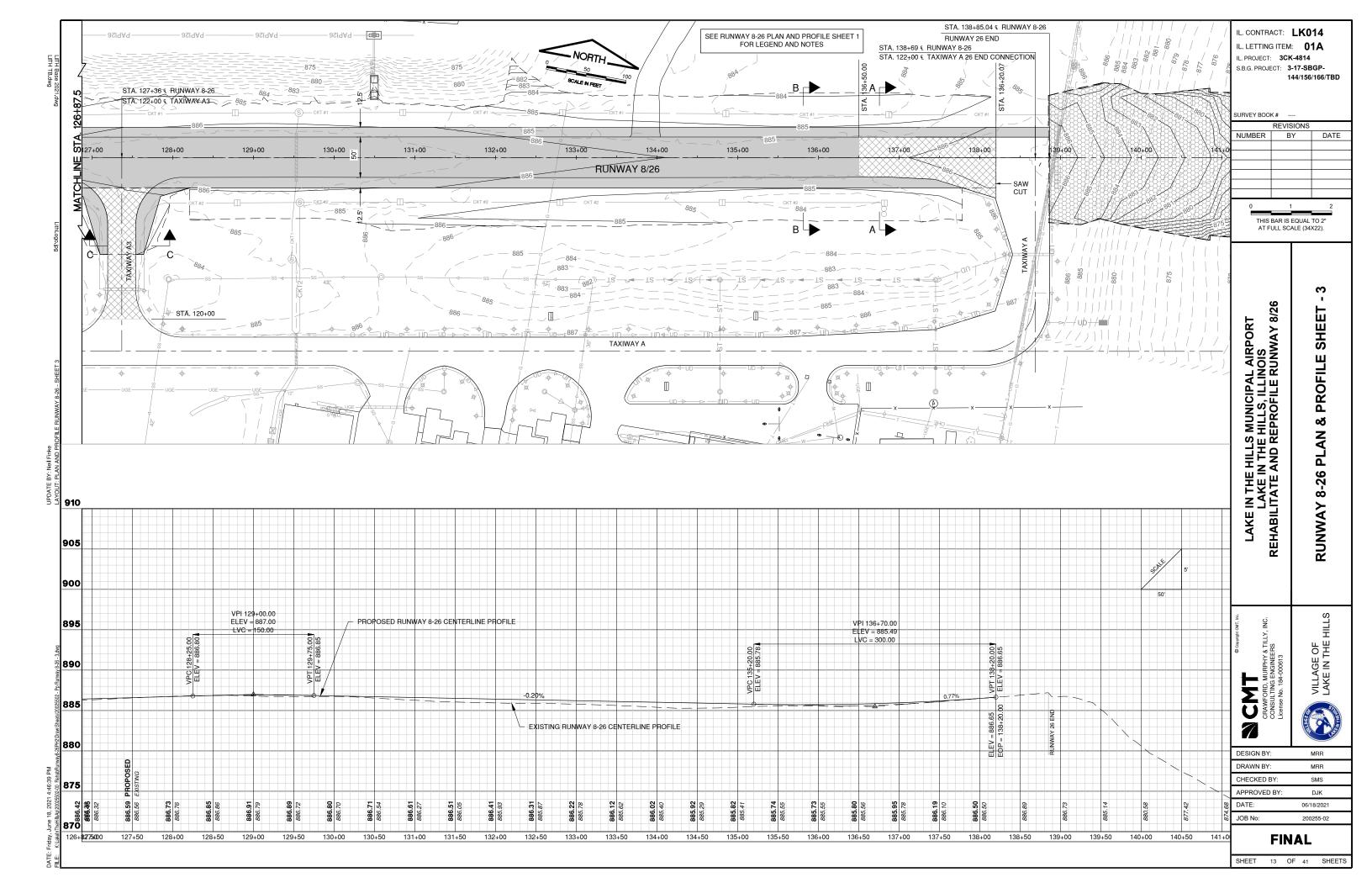
DESIGN BY: DRAWN BY: MRR CHECKED BY SMS APPROVED BY JOB No: 200255-02

FINAL

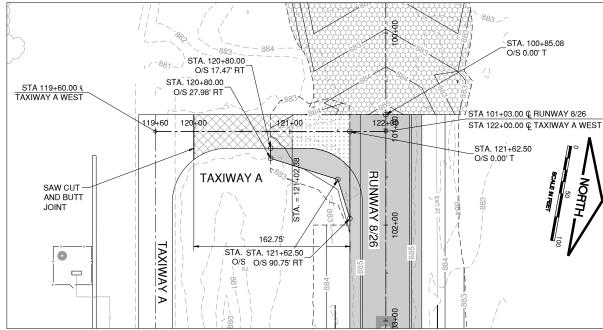
SHEET 10 OF 41 SHEETS



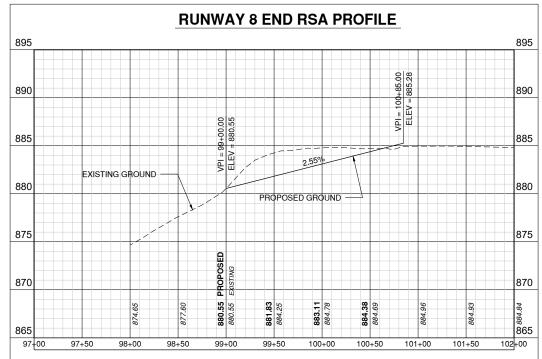


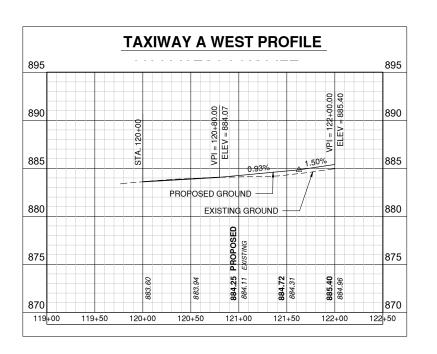


STA 101+03.00 © RUNWAY 8/26 STA 122+00.00 © TAXIWAY A WEST STA, 100+85.08 O/S 0.00' T STA. 121+62.50 O/S 0.00' T STA. 98+78.52 O/S 0.00' T 98+00/ 102+00 **RUNWAY 8/26** STA. 121+62.50 O/S 90.75' RT <u>. ⊭ 121+02.38</u> STA. 121+50.00 O/S 50.00' RT STA, 120+80,00 TAXIWAY O/S 17.47' RT _-881--STA. 120+80.00 O/S 27.98' RT **RUNWAY 8 END**



TAXIWAY A END





SEE RUNWAY 8/26 PLAN AND PROFILE SHEET 1 FOR ADDITIONAL LEGEND AND NOTES

IL. CONTRACT: **LK014** IL. LETTING ITEM: 01A IL. PROJECT: 3CK-4814 S.B.G. PROJECT: 3-17-SBGP-144/156/166/TBD URVEY BOOK # ---REVISIONS NUMBER BY DATE THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22). TAXIWAY A WEST AND RUNWAY 8 RSA PLAN AND PROFILE LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS REHABILITATE AND REPROFILE RUNWAY 8/26 VILLAGE OF LAKE IN THE HILLS

DESIGN BY: MRR

DRAWN BY: MRR

CHECKED BY: SMS

APPROVED BY: DJK

DATE: 06/18/2021

JOB No: 200255-02

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FINAL
SHEET 14 OF 41 SHEETS

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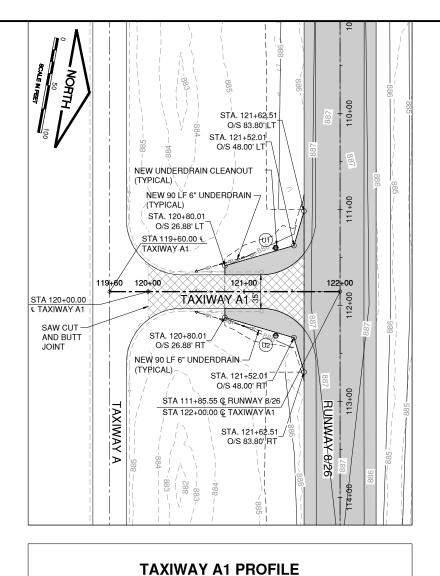
880

875

119+00

119+50

120+00



PROPOSED GROUND

120+50

2 8

121+00

121+50

895

890

885

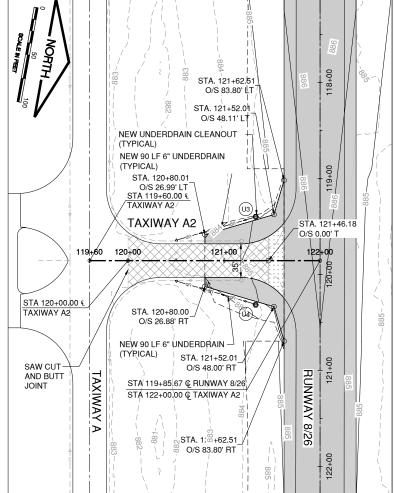
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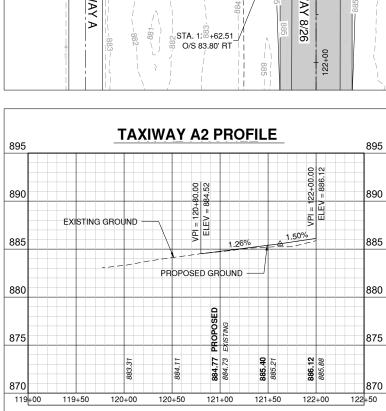
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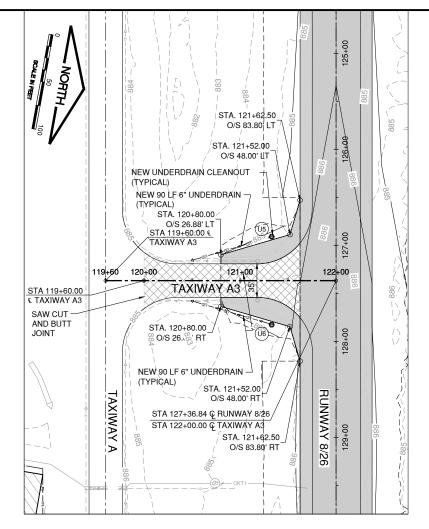
122+50

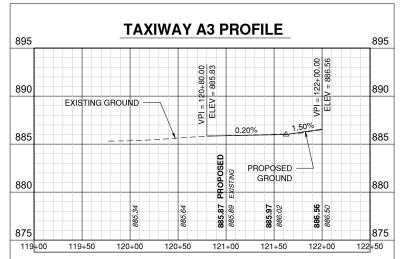
887.

122+00









SEE RUNWAY 8/26 PLAN AND PROFILE SHEET 1 FOR ADDITIONAL LEGEND AND NOTES



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LEGEND EXISTING STORM SEWER

EXISTING DRAINAGE STRUCTURE

NEW UNDERDRAIN CLEANOUT - SEE SCHEDULE

NEW 6" PERFORATED UNDERDRAIN

UNDERDRAIN NOTES:

- A. CONTRACTOR SHALL FIELD VERIFY UNDERDRAIN INSPECTION HOLES INVERTS BEFORE
- INSTALLING UNDERDRAIN B. ALL UNDERDRAIN CONNECTIONS, TEES, BENDS, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE UNDERDRAIN.
- C. UNDERDRAIN CONFLICTS WITH THE EXISTING CONDITIONS SHALL BE RESOLVED AND SHALL BE INCIDENTAL TO THE COST OF THE UNDERDRAIN.
- D. SEE UNDERDRAIN STRUCTURE SCHEDULE AND UNDERDRAIN PIPE SCHEDULE ON DRAINAGE DETAILS SHEET.

IL. CONTRACT: LK014
IL. LETTING ITEM: 01A
IL. PROJECT: 3CK-4814
S.B.G. PROJECT: 3-17-SBGP-
144/156/166/TBD

SURVEY BOO	K#	
	REVISIONS	
NUMBER	BY	DATE

THIS BAR IS FOUNDED TO 2" AT FULL SCALE (34X22).

8/26 LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS REHABILITATE AND REPROFILE RUNWAY 8.

TAXIWAY A1, A2 AND A3 PLAN AND PROFILE

VILLAGE OF LAKE IN THE HILLS

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DESIGN BY: MRR DRAWN BY: MRR CHECKED BY SMS APPROVED BY DJK DATE: 06/18/2021 JOB No: 200255-02

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SHEET 15 OF 41 SHEETS





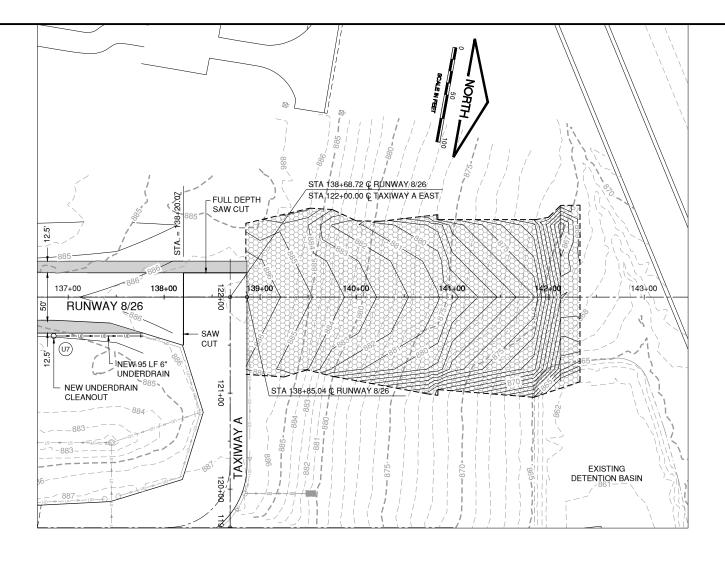
EXISTING DRAINAGE STRUCTURE

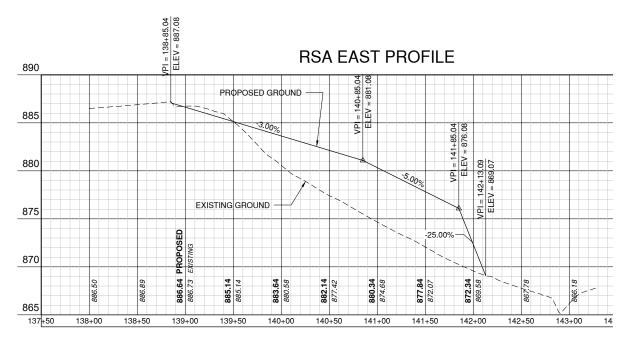
NEW 6" PERFORATED UNDERDRAIN

NEW UNDERDRAIN CLEANOUT - SEE SCHEDULE

UNDERDRAIN NOTES:

- A. CONTRACTOR SHALL FIELD VERIFY UNDERDRAIN INSPECTION HOLES INVERTS BEFORE INSTALLING UNDERDRAIN.
- B. ALL UNDERDRAIN CONNECTIONS, TEES, BENDS, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE UNDERDRAIN.
- C. UNDERDRAIN CONFLICTS WITH THE EXISTING CONDITIONS SHALL BE RESOLVED AND SHALL BE INCIDENTAL TO THE COST OF THE UNDERDRAIN.
- D. SEE UNDERDRAIN STRUCTURE SCHEDULE AND UNDERDRAIN PIPE SCHEDULE ON DRAINAGE DETAILS SHEET.





SEE RUNWAY 8/26 PLAN AND PROFILE SHEET 1 FOR ADDITIONAL LEGEND AND NOTES

IL. CONTRACT: **LK014** IL. LETTING ITEM: 01A IL. PROJECT: 3CK-4814 S.B.G. PROJECT: 3-17-SBGP-

SURVEY BOOK #					
REVISIONS					
NUMBER	BY	DATE			

144/156/166/TBD

THIS BAR IS EQUAL TO 2"

RUNWAY 26 RSA PLAN AND PROFILE

LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS REHABILITATE AND REPROFILE RUNWAY 8/26 VILLAGE OF LAKE IN THE HILLS

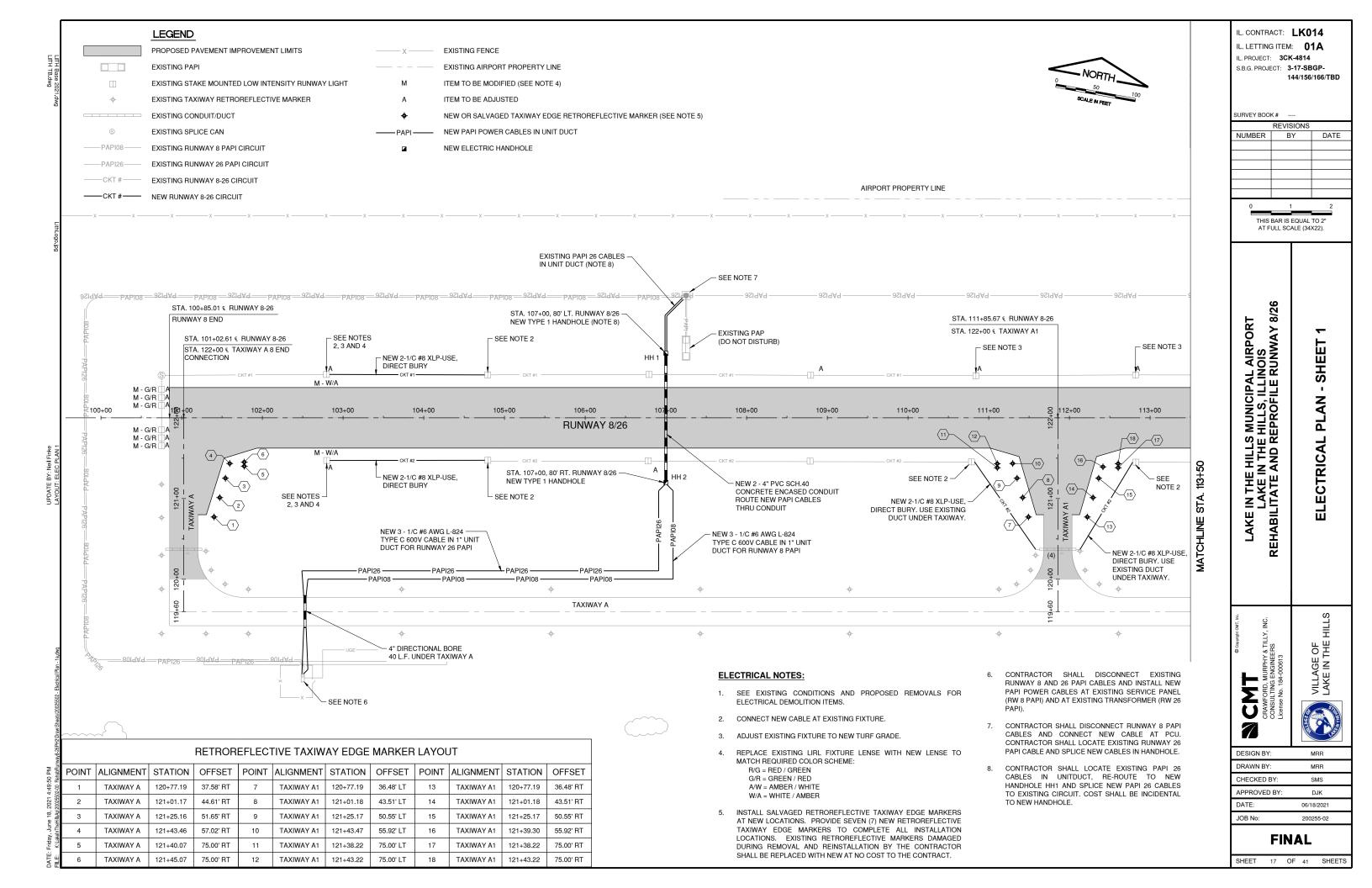
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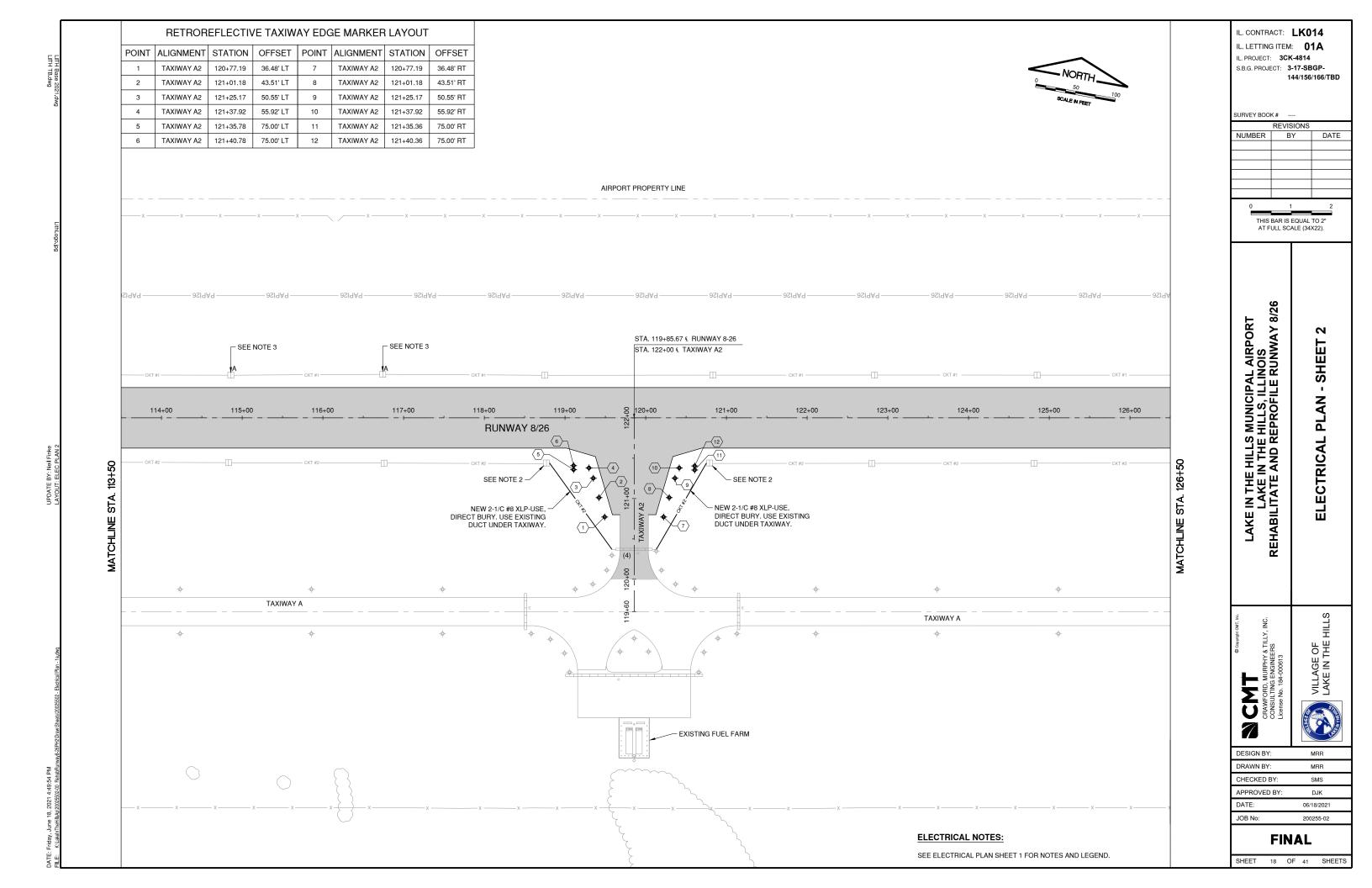


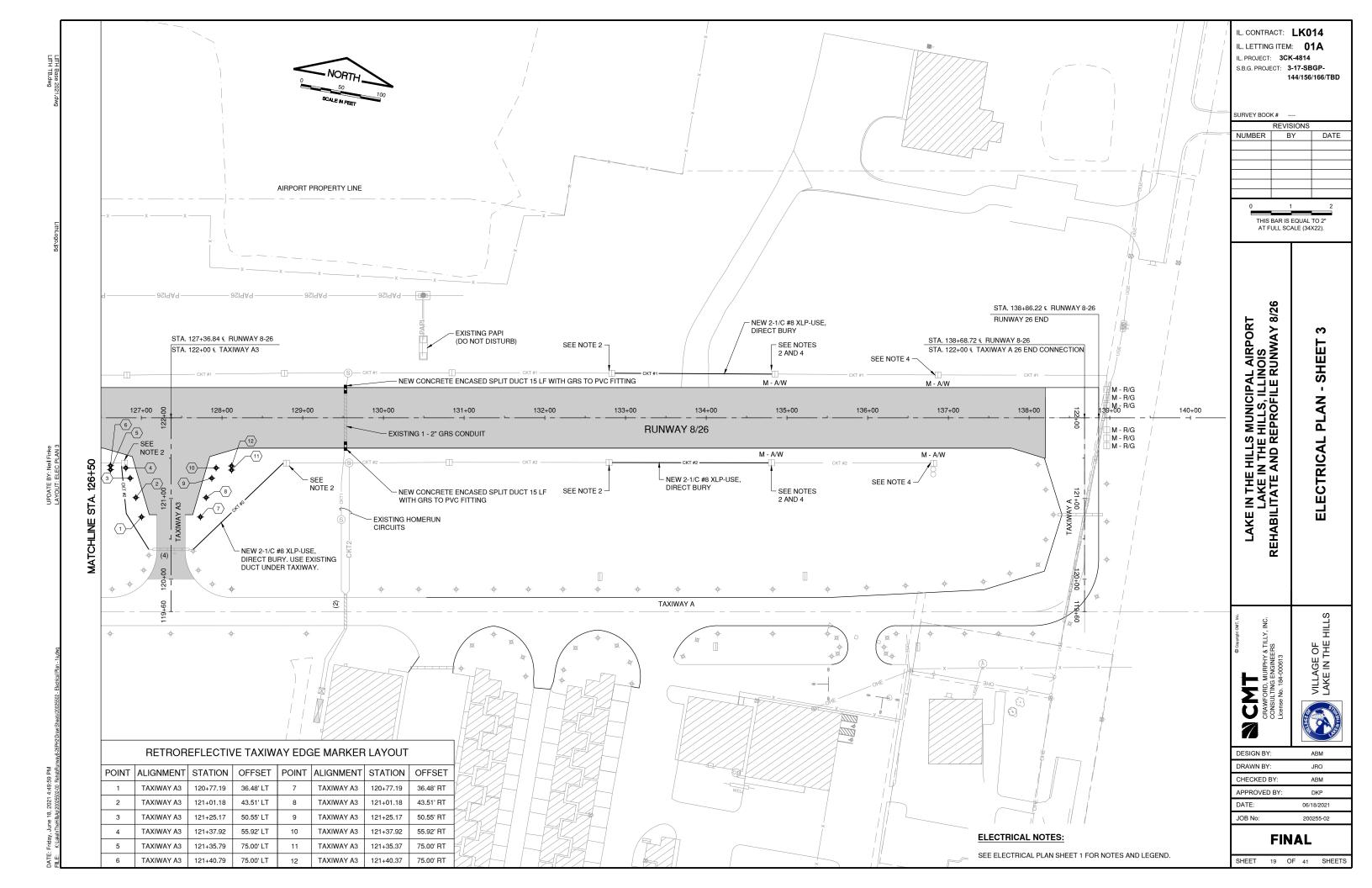
DESIGN BY: MRR DRAWN BY: MRR CHECKED BY SMS APPROVED BY: DJK JOB No: 200255-02

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SHEET 16 OF 41 SHEETS

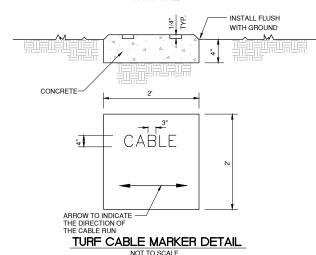






CONCRETE ENCASED DUCT BACKFILL

NOT TO SCALE

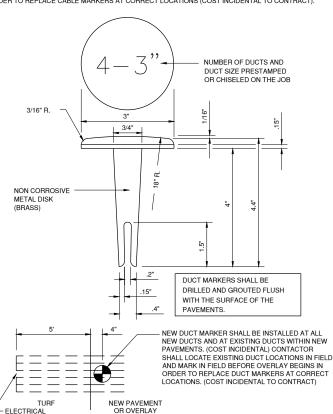


NOTES

- CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE CABLE RUN.
- 2. ITEM 610 CONCRETE SHALL BE USED
- 3. ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL
- THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- 0.049 CU, YD, CONCRETE PER MARKER,

DUCT/CONDUIT

CONTRACTOR SHALL LOCATE EXISTING CABLE MARKERS IN THE FIELD BEFORE SHOULDER ADJUSTMENT BEGINS IN ORDER TO REPLACE CABLE MARKERS AT CORRECT LOCATIONS (COST INCIDENTAL TO CONTRACT).



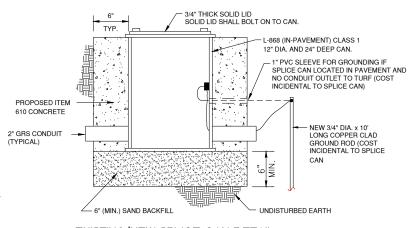
DUCT/CONDUIT MARKER DETAIL

MOUND SLIGHTLY, GRADE AND SEED MIN. MIN. CDADE BACKFILL RESTORATION PER SPEC. SAND OR EARTH BACKFILL (TYP.) (SEE SPECIFICATIONS) -INSULATED CABLE AS SHOWN ON LIGHTING LAYOUT SHEETS IN UNIT DUCT, PVC, GRS CONDUIT OR

TRENCH DETAIL NOT TO SCALE

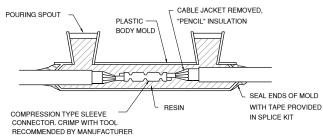
NOTES

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE
- 2. DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- 4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION



EXISTING/NEW SPLICE CAN DETAIL

NOT TO SCALE

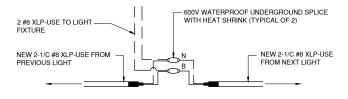


DIRECT BURIED CABLE SPLICE

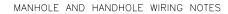
NOT TO SCALE

CABLE SPLICE NOTES

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- 4. THE CONTRACTOR SHALL HAVE A MINIMUM OF FIVE (5) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.



120V LIGHTING CIRCUIT CONNECTION DETAIL



ALL WIRING IN MANHOLES AND HANDHOLES SHALL BE LABELED INSIDE MANHOLES AND HANDHOLES. LABELING MATERIALS SHALL BE SUITABLE FOR USE IN THE ENVIRONMENT AND SHALL BE WATERPROOF, LABELS SHALL IDENTIFY EACH 480V, 240V AND 120V AND MONITOR CIRCUIT PER MASTER CIRCUIT LABELING SCHEDULE DEVELOPED BY ELECTRICAL CONTRACTOR.

WHEREVER POSSIBLE, WIRING TO INDIVIDUAL STRUCTURES AND PIECES OF EQUIPMENT SHALL BE GROUPED TOGETHER IN MANHOLES AND HANDHOLES MULTIPLE CONDUCTORS OF INDIVIDUAL CIRCUITS SHALL BE TIE-WRAPPED TOGETHER AND LABELED.

WHEREVER POSSIBLE, 480V WIRING, 120V/240V WIRING AND MONITOR/ALARM WIRING SHALL BE SEPARATED FROM EACH OTHER IN MANHOLES AND HANDHOLES. MONITOR WIRING SHALL BE ROUTED ABOVE 120V/240V WIRING WHICH, IN TURN, SHALL BE ROUTED ABOVE 480V WIRING.

ALL WIRING THROUGH MANHOLES SHALL BE ATTACHED TO CABLE RACKS.

NOTE

BLUE POST

(HEIGHT ABOVE

GROUND)

18" SOII ANCHOI

BE L-853 CERTIFIED.

ELEVATED

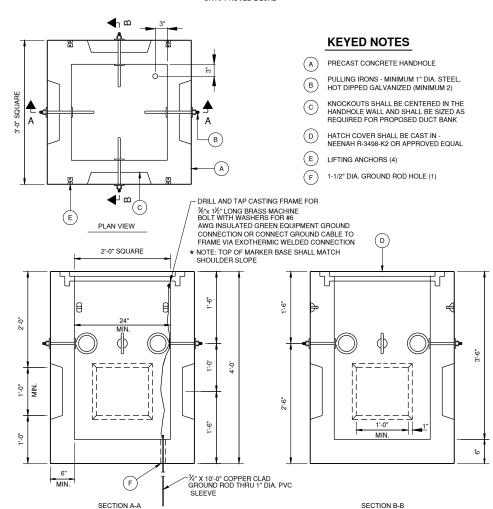
RETROREFLECTIVE MARKER

(SOIL ANCHOR MOUNT)

NOT TO SCALE

NOTE: RETROREFLECTIVE MARKER SHALL

- 1. CONCRETE SHALL MEET THE REQUIREMENTS OF ITEM P-610.
- REBAR MEETING ASTM A-615, GRADE 60, SHALL BE INSTALLED ON 8 INCH CENTERS BOTH WAYS. REBAR SHALL BE MINIMUM #4 BARS. 3" CLEARANCE SHALL BE MAINTAINED BETWEEN THE REBAR AND THE FINISHED FACE OF THE
- 3. A 6-INCH BED OF CA-7 OR CA-11 SHALL BE PLACED BENEATH THE HANDHOLE.
- 4. INSTALL GROUND ROD IN EACH HANDHOLE
- ANY UNUSED DUCT OPENINGS SHALL BE FITTED WITH APPROVED PLUGS PRIOR TO BACKFILLING.
- 6. HANDHOLES SHALL HAVE A WEEP HOLE CONSTRUCTED IN THE BOTTOM OF HANDHOLE.
- 7. HANDHOLE COVER SHALL BE LABELED "ELECTRIC" AS SHOWN ON PLAN SHEETS
- 8. 4-WAY DIRECTIONAL BORE SHALL BE CONNECTED IN HANDHOLES IN MANNER THAT CREATES A SINGLE 4-WAY DUCT BANK.
- PRECAST HANDHOLE SHALL MEET UTILITY CONCRETE PRODUCTS ITEM #2238HH OR APPROVED EQUAL



TYPE 1 HANDHOLE DETAIL

NOT TO SCALE

IL. CONTRACT: **LK014** IL. LETTING ITEM: 01A

IL. PROJECT: 3CK-4814 S.B.G. PROJECT: 3-17-SBGP-144/156/166/TBD

URVEY BOOK # ---

REVISIONS NUMBER BY DATE

> THIS BAR IS FOUNDED TO 2" AT FULL SCALE (34X22).

 ∞ E HILLS MUNICIPAL AIRPORT IN THE HILLS, ILLINOIS E AND REPROFILE RUNWAY 8 **DETAILS ECTRICAL** E IN THE LAKE I

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DESIGN BY DRAWN BY MRR CHECKED B APPROVED BY D.IK 06/18/202 JOB No 200255-02

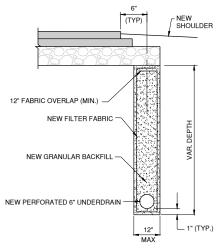
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SHEET 20 OF 41 SHEETS

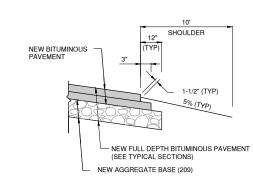
UNDERDRAIN STRUCTURE SCHEDULE						
STRUCTURE						
NUMBER	STATION/OFFSET	RIM ELEV.	INV. ELEV.	NOTES		
U1	STA. 111+40, 64.5' RT	886.25	882.89	TYPE 1 CLEANOUT		
U2	STA. 112+32, 64.5' RT	886.24	882.88	TYPE 1 CLEANOUT		
U3	STA.119+40, 64.5' RT	884.76	881.40	TYPE 1 CLEANOUT		
U4	STA. 120+31.5, 64.5' RT	884.65	881.29	TYPE 1 CLEANOUT		
U5	STA. 126+91, 64.5' RT	885.26	881.90	TYPE 1 CLEANOUT		
U6	STA. 127+82.5, 64.5' RT	885.47	882.11	TYPE 1 CLEANOUT		
U7	STA. 136+85, 38' RT	885.06	881.70	TYPE 1 CLEANOUT		

UNDERDRAIN PIPE SCHEDULE							
UD	UPSTREAM	DOWNSTREAM	UPSTREAM	DOWNSTREAM	PIPE	PIPE	
LINE	STRUCTURE	STRUCTURE	INVERT	INVERT	LENGTH	SLOPE	
1	U1	EXIST. UD	882.89	M.E.	90	*	
2	U2	EXIST. UD	882.88	M.E.	90	*	
3	U3	EXIST. UD	881.40	M.E.	90	*	
4	U5	EXIST. UD	881.29	M.E.	90	*	
5	U4	EXIST. UD	881.90	M.E.	90	*	
6	U6	EXIST. UD	882.11	M.E.	90	*	
7	U7	EXIST. UD	881.70	M.E.	95	*	

M.E. = MATCH EXISTING ELEVATION



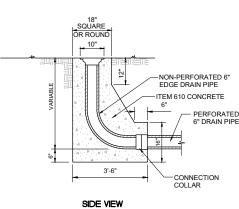
UNDERDRAIN DETAIL EDGE OF PAVEMENT AREAS
NO SCALE



AIRFIELD PAVEMENT EDGE DETAIL



TOP VIEW



UNDERDRAIN CLEANOUT DETAIL NOT TO SCALE NOTE:

CLEANOUTS TO BE REPLACED SHALL BE COMPLETELY REMOVED AND A NEW TYPE 1 CLEANOUT INSTALLED AT THAT LOCATION. REMOVAL AND REPLACEMENT TO BE PAID UNDER AR705924. REPLACE UNDERDRAIN CLEANOUT.

INSIDE DIAMETER OF STORM SEWER (INCHES)

MAXIMUM TRENCH WIDTH

6 3'-7" 5'-7"

8 3'-9" 5'-9"

12 4'-2" 6'-2" 15 4'-6" 6'-6"

21 5'-0" 7'-0" 24 5'-4" 7'-4"

5'-7"

30 5'-11" 7'-11" 36 6'-6" 8'-6"

42 7'-1" 9'-1"

48 7'-8" 9'-8"

54 8'-3" 10'-3"

60 8'-10" 10'-10" 66 9'-5" 11'-5"

72 10'-0" 12'-0"

78 10'-7" 12'-7" 84 11'-2" 13'-2"

11'-9"

96 12'-4" 14'-4"

102 12'-11" 14'-11"

108 13'-6" 15'-6"

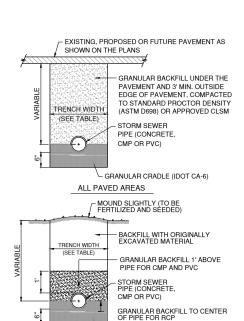
18 4'-9"

MAXIMUM PAVEMEN REMOVAL WIDTH

6'-9"

7'-7"

13'-9"



NON-PAVED AREAS

TRENCH DETAILS

NOT TO SCALE
(INCIDENTAL)

GRANULAR CRADLE (IDOT CA-6)



DESIGN BY:

DRAWN BY:

CHECKED BY:

APPROVED BY

DATE:

JOB No:

MRR

MRR

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06/18/2021

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FINAL

SHEET 21 OF 41 SHEETS

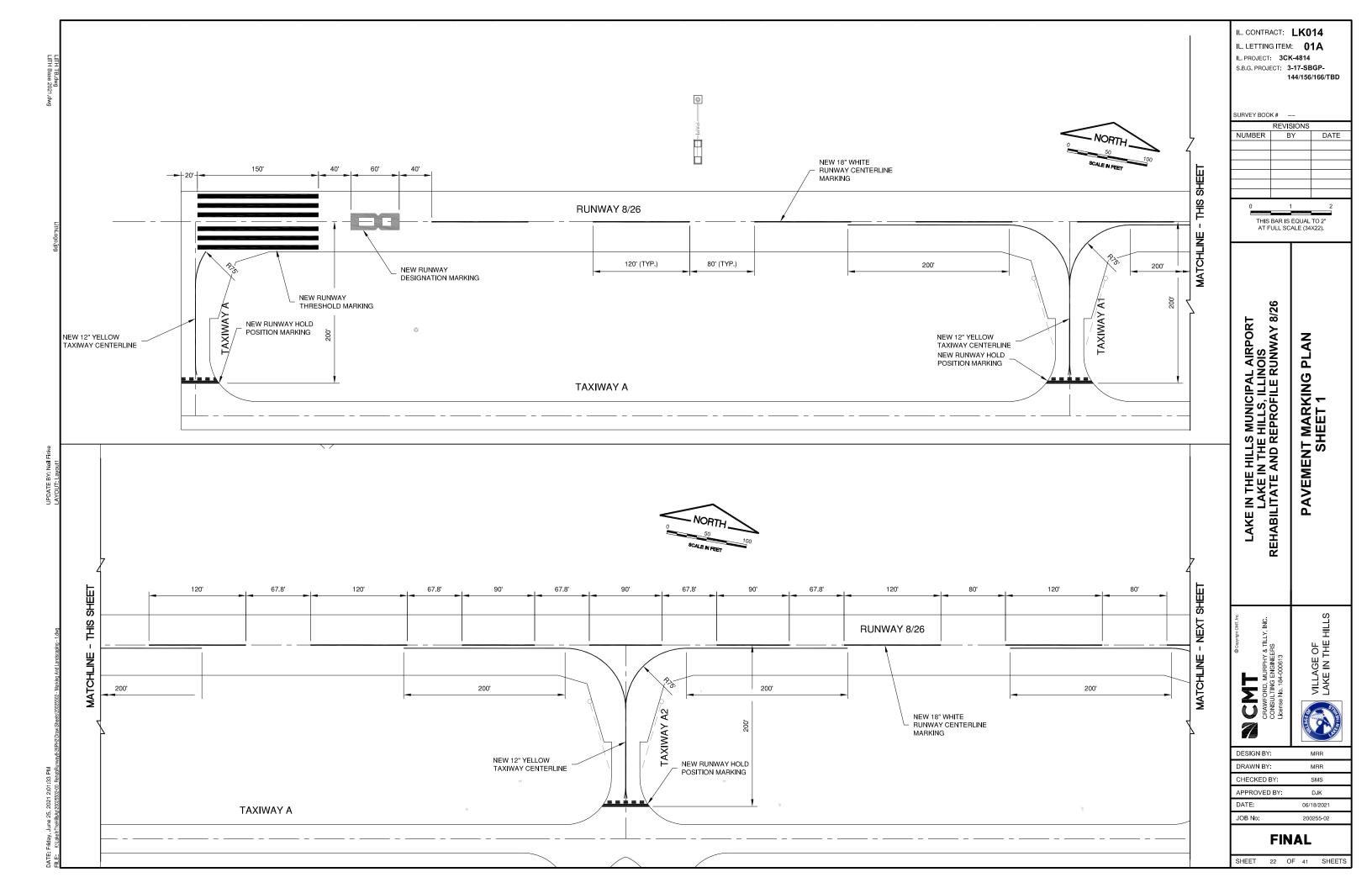
IL. CONTRACT: LK014
IL. LETTING ITEM: 01A
IL. PROJECT: 3CK-4814
S.B.G. PROJECT: 3-17-SBGP-

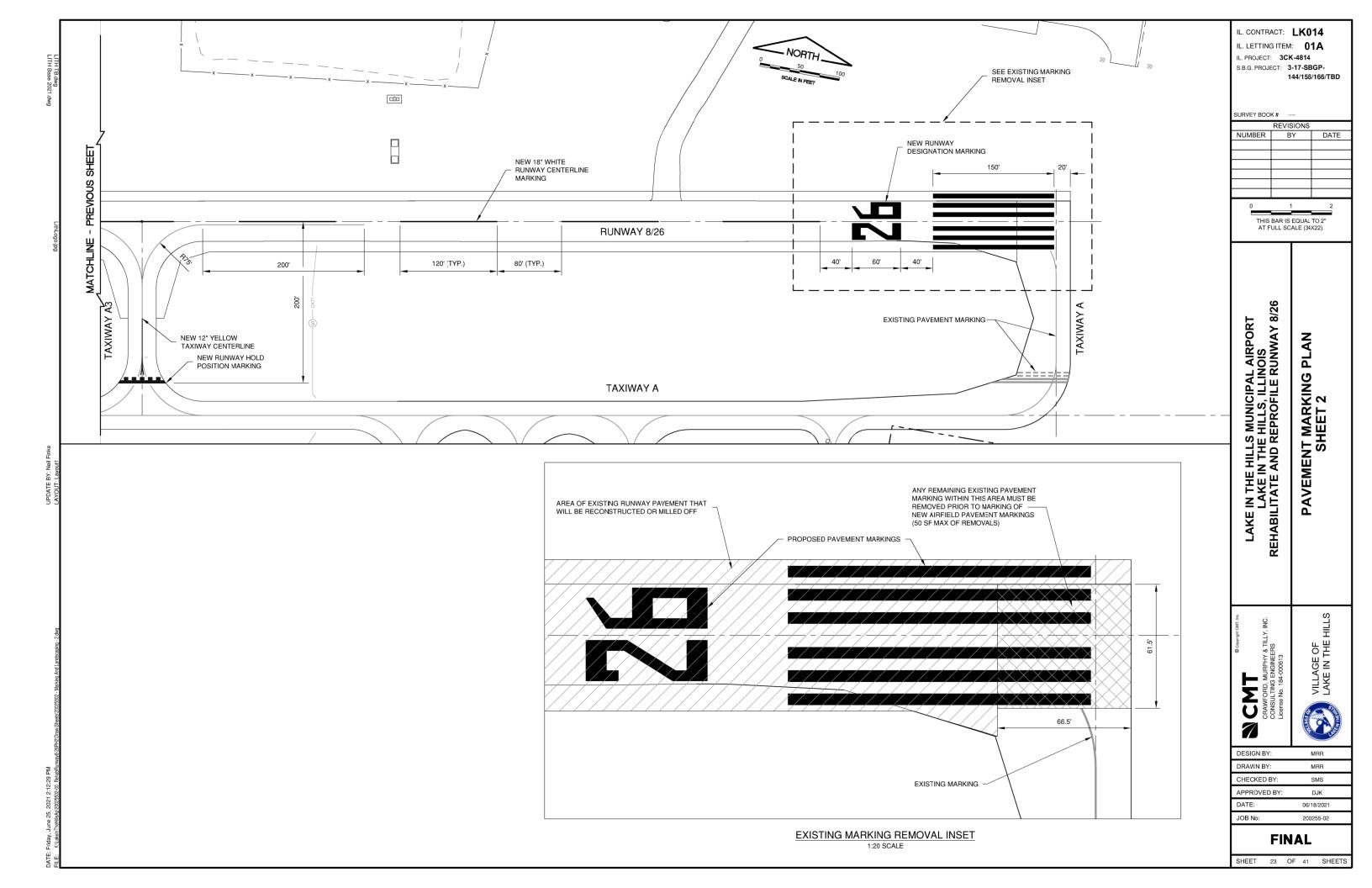
SURVEY BOOK # ----

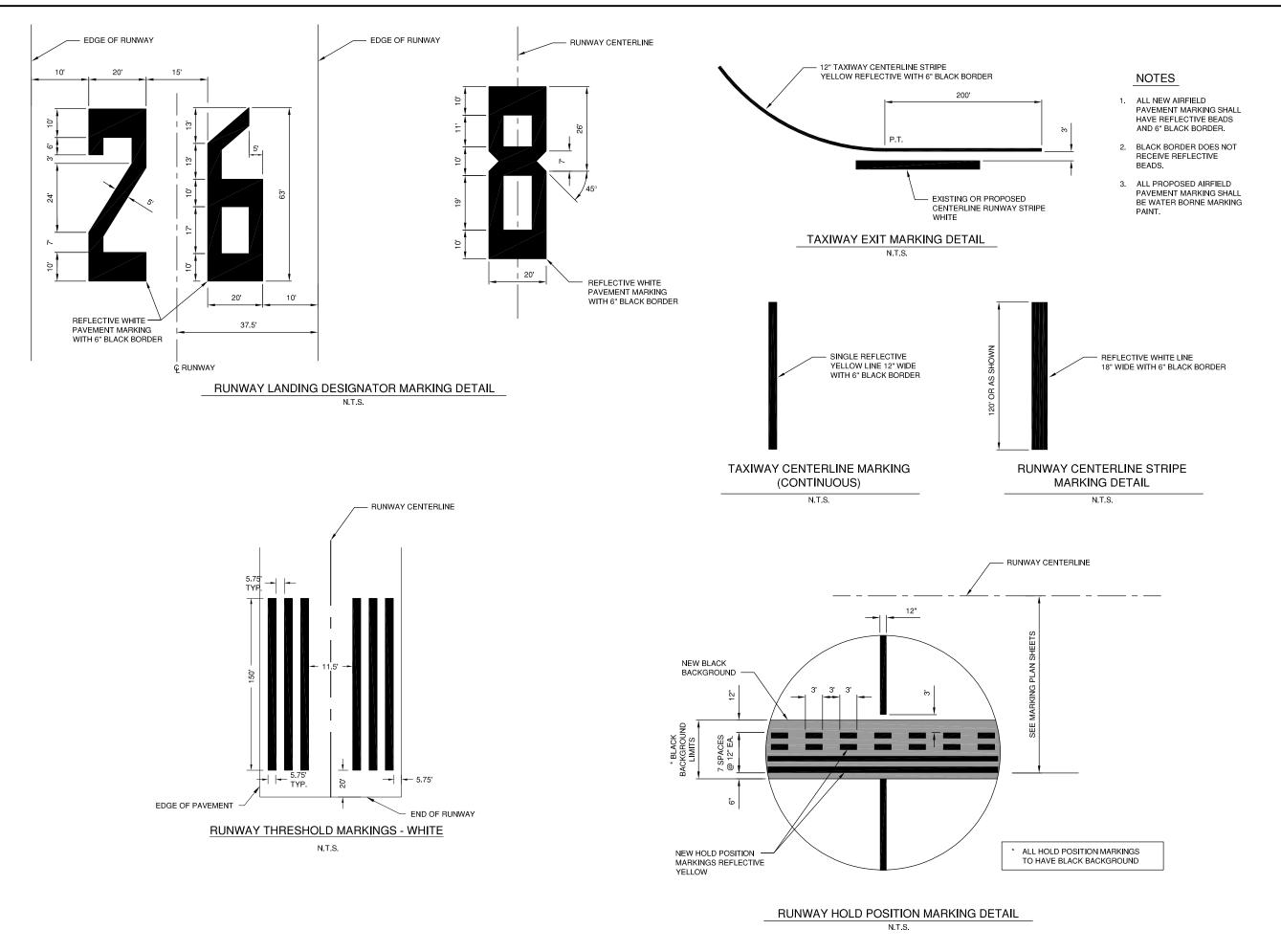
REVISIONS
NUMBER BY DATE

144/156/166/TBD

^{*} PIPE SLOPE UNKNOWN BASED ON UNKNOWN EXISTING INVERT ELEVATIONS







IL. PROJECT: 3CK-4814 S.B.G. PROJECT: 3-17-SBGP-144/156/166/TBD

SURVEY BOOK # ---

REVISIONS
NUMBER BY DATE

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

LAKE IN THE HILLS MUNICIPAL AIRPORT
LAKE IN THE HILLS, ILLINOIS
REHABILITATE AND REPROFILE RUNWAY 8/26
PAVEMENT MARKING DETAILS

REHABIL

CAMPORD, MURPHY & TILLY CONSULTING ENGINEERS License No. 184-000613

 DESIGN BY:
 MRR

 DRAWN BY:
 MRR

 CHECKED BY:
 SMS

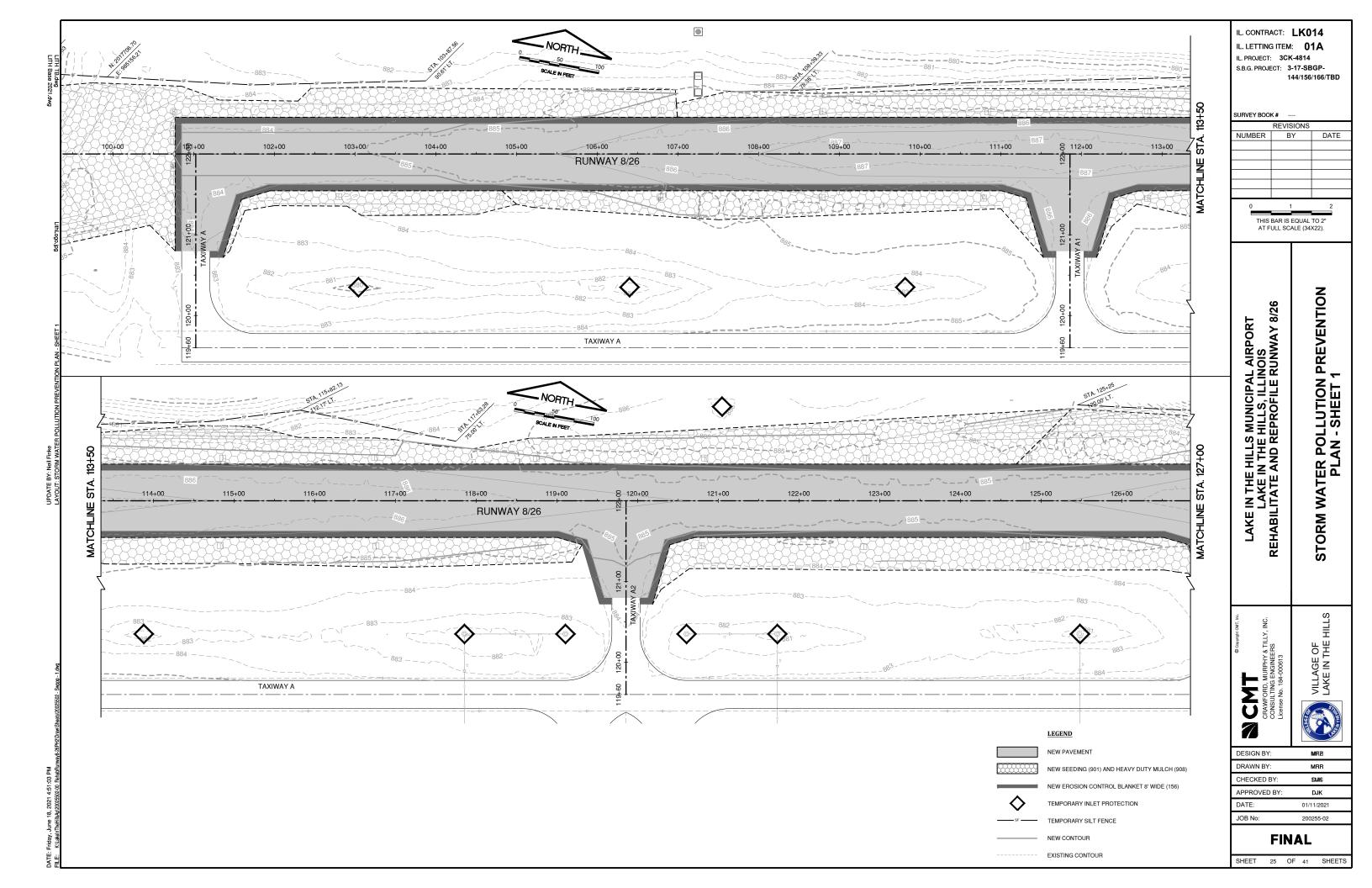
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 DJK

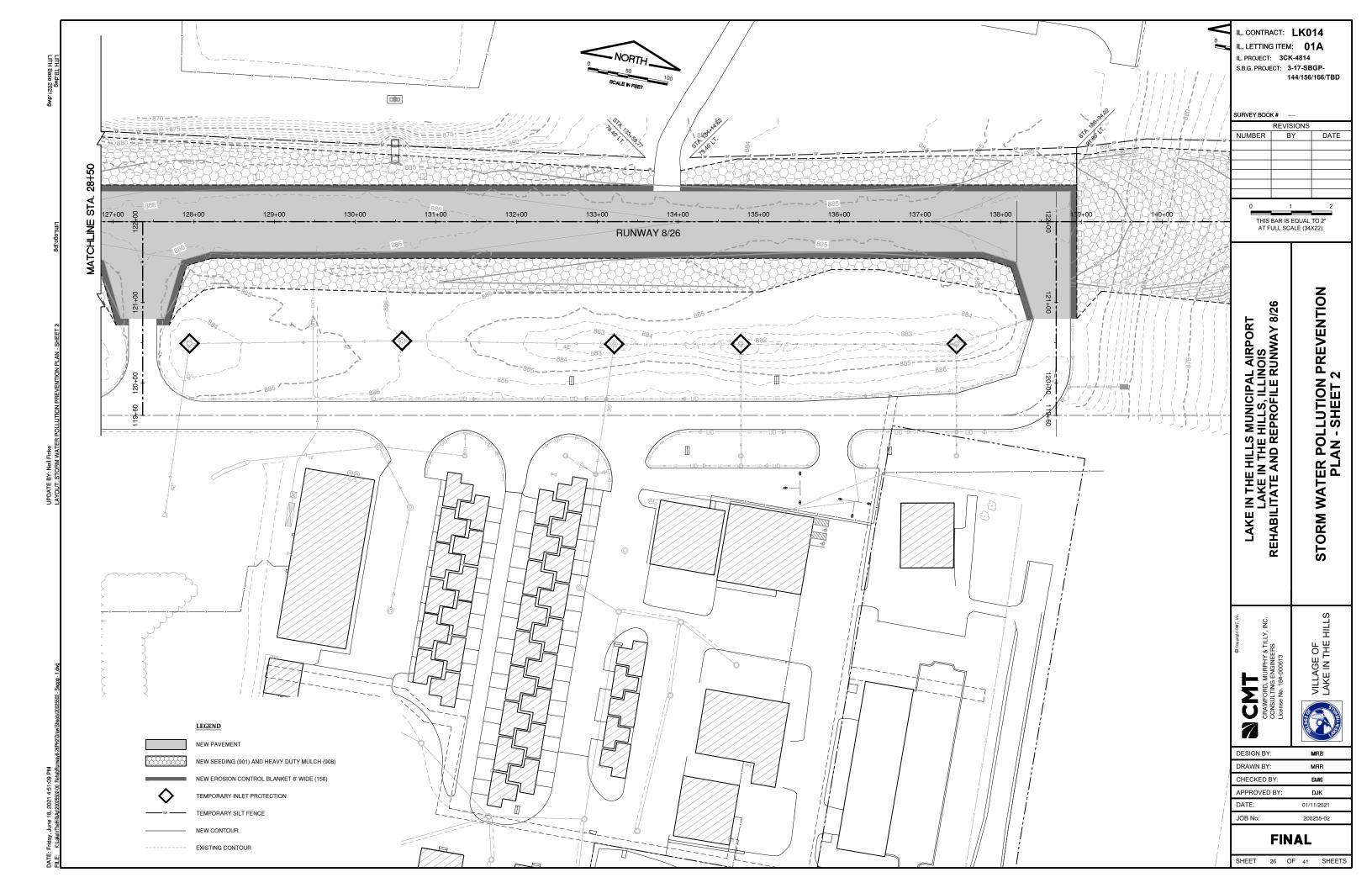
 DATE:
 06/18/2021

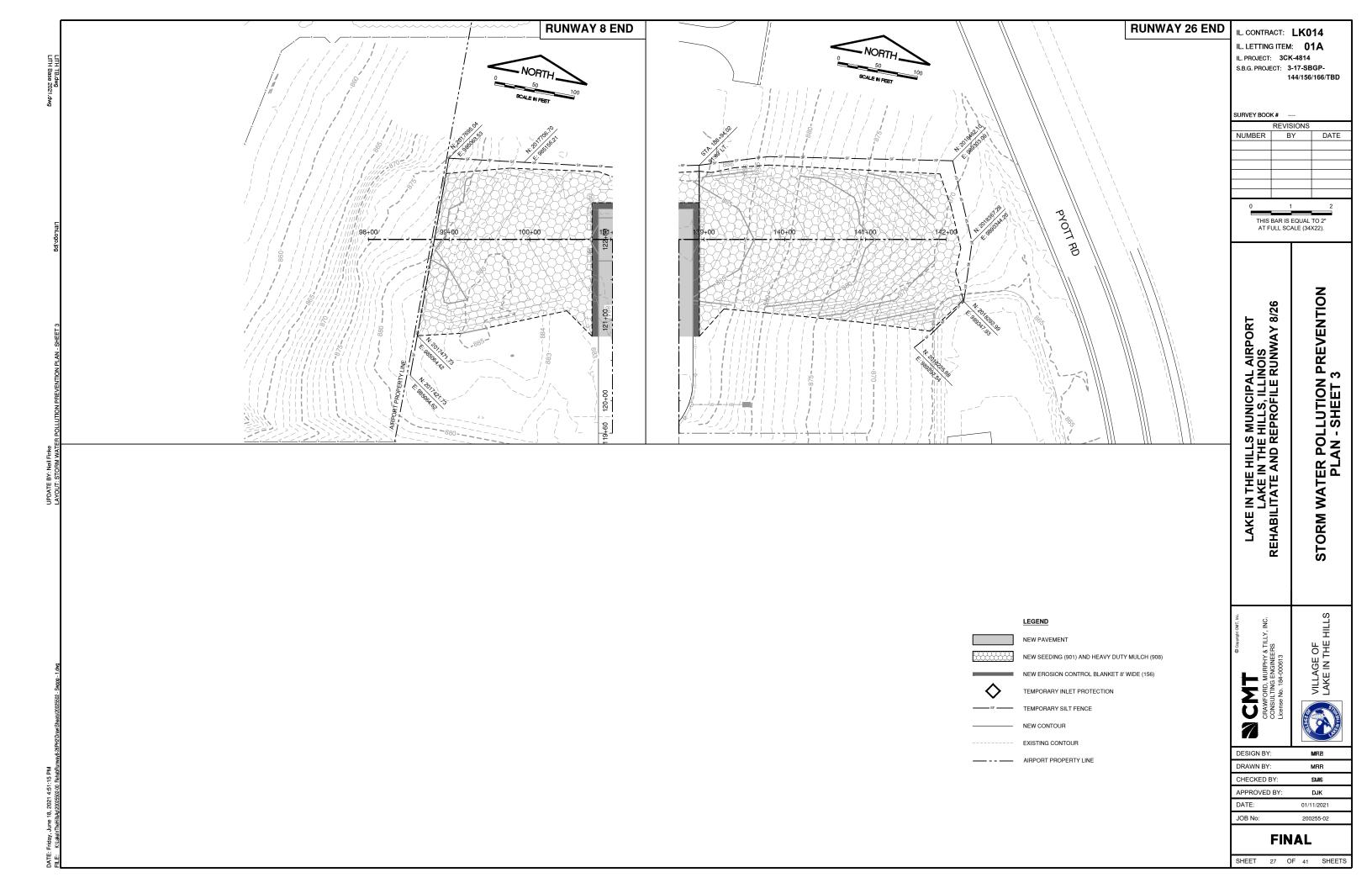
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 200255-02

FINAL

SHEET 24 OF 41 SHEETS







- LINESS OTHERWISE INDICATED ALL VEGETATIVE AND STRUCTURAL FROSION AND SEDIMENT CONTROL PRACTICES WILL BE CONSTRUCTED ACCORDING TO MINIMUM STANDARDS AND SPECIFICATIONS IN THE ILLINOIS URBAN MANUAL REVISED
- A COPY OF THE APPROVED EROSION CONTROL AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED ON THE SITE AT ALL TIME.
- THE CONTRACTOR WILL BE REQUIRED TO IMPLEMENT AND MAINTAIN EROSION
- NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE SITE OTHER THAN THROUGH EROSION CONTROL PROTECTIVE MEASURES. THE CONTRACTOR WILL ADJUST HIS OPERATIONS AND IMPLEMENT EROSION CONTROL MEASURES ACCORDINGLY.
- MAINTENANCE AND REPAIR OF ALL EROSION CONTROL MEASURES SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE RESPECTIVE EROSION CONTROL PAY AREA.
- THE CONTRACTOR SHALL INSTALL SILT FILTER FENCE AT ALL EARTH STOCKPILES WHICH SHALL BE PAID FOR AS SILT FENCE.
- ALL ADJACENT STREETS MUST BE KEPT CLEAR OF DEBRIS, INSPECTED DAILY, AND
- EROSION CONTROL MEASURES SHALL BE INSPECTED 24 HOURS AFTER ANY STORM OF PRECIPITATION OF 0.5" OR GREATER.
- ALL CONCRETE TRUCK WASHOUT LOCATIONS SHALL BE LOCATED WITHIN THE CONTRACTOR'S STAGING AREA. THE DESIGNATED AREA SHALL BE APPROVED BY THE ENGINEER.
- A.) A CONCRETE WASHOUT SIGN SHALL BE INSTALL WITHIN 20 FEET OF THE TEMPORARY CONCRETE TRUCK WASHOUT FACILITY. AT A MINIMUM, THE SIGN SHALL READ "CONCRETE WASHOUT" IN 6" TALL LETTERS.
- B.) INSPECTION SHALL OCCUR ONCE A WEEK AND DAILY DURING CONCRETE OPERATIONS, REPAIR/REPLACEMENT OF THE FACILITY SHALL BE MADE SUCH THAT CONCRETE WASTE IS CONTAINED.
- C.) THE CONCRETE WASHOUT FACILITY MUST BE CLEANED AND ALL OF THE CONTAINED MATERIALS SHALL BE REMOVED AND DISPOSED OF AT A LEGAL OFF-SITE LOCATION WHEN THE FACILITY HAS REACHED TWO-THIRDS CAPACITY. UPON COMPLETION OF CONCRETE OPERATIONS, THE CONCRETE TRUCK WASHOUT AND ALL MATERIALS CONTAINED WITHIN SHALL BE DISPOSED OF AT A LEGAL OFF-SITE LOCATION
- D.) WASHOUT TO BE LOCATED ON LEVEL GROUND AND A MINIMUM OF 50' FROM INLETS, DRAINAGE FACILITIES OR WATER BODIES. IF REQUIRED CONTRACTOR'S BUILD A LEVEL PAD FOR THE WASHOUT (COST INCIDENTAL TO CONTRACT).
- E.) ALL LABOR, EQUIPMENT, TOOLS, MATERIAL, EXCAVATION, MAINTENANCE AND DISPOSAL SHALL NOT BE MEASURED FOR PAYMENT BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- F.) CONTRACTOR SHALL REMOVE ACCUMULATED LIQUIDS PRIOR TO IMPENDING STORMS TO PREVENT OVERFLOW OF FACILITY, OTHERWISE COVER FACILITY.
- G.) CONTRACTOR MAY PROPOSE ALTERNATE WASHOUT FACILITIES IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL FOR REVIEW AND APPROVAL BY THE ENGINEER
- THE CONTRACTOR IS RESPONSIBLE FOR PROPER INSTALLATION, INSPECTION, AND THE CONTRACTOR IS RESPONSIBLE FOR PROPER INSTALLATION, INSPECTION, AND MAINTENANCE OF ALL EROSION CONTROL DEVICES. ALL EROSION CONTROL DEVICES SHALL BE MAINTAINED, REPAIRED, AND REPLACED THROUGHOUT THE ENTIRE CONSTRUCTION OF PROJECT. AFTER ACHIEVING PERMANENT VEGETATION, ALL EROSION CONTROL DEVICES SHALL BE REMOVED, ALL DRAINAGE STRUCTURES CLEANED, AND ALL AREAS DISTURBED BY INSTALLATION OF EROSION CONTROL DEVICES RESTORED.
- 12. SILT BASKETS SHALL BE INSTALLED AT ALL IN PAVEMENT AND TURF DRAINAGE STRUCTURES. SILT BASKETS SHALL BE CLEANED AS RECOMMENDED BY THE MANUFACTURER OR AT THE DIRECTION OF THE ENGINEER. COSTS FOR INSTALLATION AND CLEANING OF SILT BASKETS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR INLET PROTECTION.
- 13. TEMPORARY EROSION CONTROL SEEDING SHALL BE APPLIED BY HAND BROADCASTING TO ACHIEVE REASONABLY UNIFORM COVERAGE AT A RATE OF
- 4. DITCH CHECKS SHALL BE FROM IDOT'S APPROVED LIST OR BOLLED EXCELSION. ROLLED EXCELSION SHALL BE IN CONFORMANCE WITH SECTIONS 280,04 AND 1081.15 (f) OF THE STD. SPECIFICATION, FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, METAL STAKES WILL NOT BE PERMITTED.
- 15. DITCH CHECK SPACING BASED ON 10-INCH HIGH DITCH CHECK, SPACING AND
- 16. CONTRACTOR SHALL DETERMINE AND IMPLEMENT THE NECESSARY PRECAUTIONS TO MINIMIZE FUGITIVE DUST DURING BUILDING AND FOUNDATION DEMOLITION, AT A MINIMUM SURFACES SHOULD BE WETTED.
- . EROSION CONTROL BLANKET WILL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURERS INSTRUCTIONS. WOOD STAKES OR BIODEGRADABLE PLASTIC STAKES ARE TO BE USED ONLY. METAL STAKES ARE NOT ALLOWED.

MCHENRY COUNTY STANDARD SOIL EROSION AND SEDIMENT CONTROL NOTES

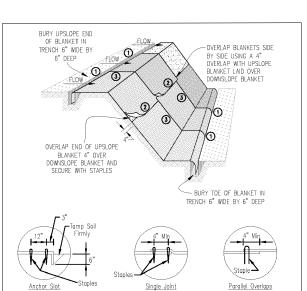
- 1. CONTROL MEASURES SHALL MEET THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE *ILLINOIS URBAN MANUAL* (WWW.AISWCD.ORG/IUM) UNLESS STATED OTHERWISE.
- 2. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE <u>EROSION</u>. AREAS OF THE DEVELOPMENT SITE THAT ARE NOT TO BE DISTURBED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL STABILIZATION IS ACHIEVED.
- 3. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, <u>DEVELOPMENT SITE</u> CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
- 4. STABILIZATION BY SEEDING SHALL INCLUDE TOPSOIL PLACEMENT AND FERTILIZATION, AS NECESSARY.
- 5. NATIVE SEED MIXTURES SHALL INCLUDE RAPID-GROWING ANNUAL GRASSES OR SMALL GRAINS TO PROVIDE INITIAL, TEMPORARY SOIL STABILIZATION.
- 6. OFFSITE PROPERTY SHALL BE PROTECTED FROM <u>EROSION</u> AND <u>SEDIMENTATION</u>. VELOCITY DISSIPATION DEVICES SHALL BE PLACED AT CONCENTRATED DISCHARGE LOCATIONS AND ALONG THE LENGTH OF ANY OUTFALL CHANNEL, AS NECESSARY TO PREVENT EROSION.
- 7. SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE DISTURBANCE OF TRIBUTARY
- 8. STABILIZATION OF DISTURBED AREAS SHALL BE INITIATED IMMEDIATELY WHENEVER ANY CLEARING GRADING, EXCAVATING OR OTHER EARTH DISTURBING ACTIVITIES HAVE PERMANENTLY CEASED ON ANY PORTION OF THE <u>DEVELOPMENT SITE</u>, OR TEMPORARILY CEASED ON ANY PORTION OF THE <u>DEVELOPMENT SITE</u> AND WILL NOT RESUME FOR A PERIOD EXCEEDING 14 CALENDAR DAYS. STABILIZATION OF DISTURBED AREAS SHALL BE INITIATED WITHIN 1 WORKING DAY OF PERMANENT OR TEMPORARY CESSATION OF EARTH DISTURBING ACTIVITIES AND SHALL BE COMPLETED AS SOON AS POSSIBLE, BUT NOT LATER THAN 14 CALENDAR DAYS FROM THE INITIATION OF STABILIZATION WORK IN AN AREA. EXCEPTIONS TO THESE TIME FRAMES ARE SPECIFIED BELOW:
- A. WHERE THE INITIATION OF STABILIZATION MEASURES IS PRECLUDED BY SNOW COVER, STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE; AND
- B. IN AREAS WHERE CONSTRUCTION ACTIVITY HAS TEMPORARILY CEASED AND WILL RESUME AFTER 14 DAYS, A TEMPORARY STABILIZATION METHOD MAY BE USED.
- 9. DISTURBANCE OF STEEP SLOPES SHALL BE MINIMIZED, AREAS OR EMBANKMENTS HAVING SLOPES STEEPER THAN 3:1 SHALL BE STABILIZED WITH STAKED IN PLACE SOD, <u>EROSION</u> CONTROL BLANKET IN COMBINATION WITH SEEDING, OR AN EQUIVALENT CONTROL MEASURE
- 10. PERIMETER CONTROL MEASURES SHALL BE PROVIDED DOWNSLOPE AND PERPENDICULAR TO THE FLOW OF <u>RUNOFF</u> FROM DISTURBED AREAS, WHERE THE <u>TRIBUTARY AREA</u> IS GREATER THAN 5,000 SQUARE FEET, AND WHERE <u>RUNOFF</u> WILL FLOW IN A SHEET FLOW MANNER. PERIMETER EROSION CONTROL SHALL ALSO BE PROVIDED AT THE BASE OF $\underline{\mathsf{SOIL}}$ STOCKPILES.
- 11. THE STORMWATER MANAGEMENT SYSTEM SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION DOWNSLOPE FROM DISTURBED AREAS, INLET PROTECTION THAT REDUCES SEDIMENT LOADING, WHILE ALLOWING RUNOFF TO ENTER THE INLET SHALL BE REQUIRED FOR ALL STORM SEWERS. CHECK DAMS, OR AN EQUIVALENT CONTROL MEASURE, SHALL BE REQUIRED FOR ALL CHANNELS. FILTER FABRIC INLET PROTECTION AND STRAW BALE DITCH CHECKS ARE NOT ACCEPTABLE CONTROL MEASURES.
- 12. IF DEWATERING SERVICES ARE USED, DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G., SEDIMENT TRAP OR AN EQUIVALENT CONTROL MEASURE). THE ENFORCEMENT OFFICER SHALL BE NOTIFIED PRIOR TO THE COMMENCEMENT OF DEWATERING ACTIVITIES.
- 13. ALL TEMPORARY SOIL <u>EROSION</u> AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION OF THE <u>DEVELOPMENT SITE</u> IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NECESSARY. TRAPPED SEDIMENT SHALL BE REMOVED AND DISTURBED AREAS SHALL BE PERMANENTLY STABILIZED.
- 14. STOCKPILED SOIL AND MATERIALS SHALL BE REMOVED FROM FLOOD HAZARD AREAS AT THE END OF EACH WORK DAY. SOIL AND MATERIALS STOCKPILED IN IWMC OR BUFFER AREAS SHALL BE PLACED ON TIMBER MATS, OR AN EQUIVALENT CONTROL MEASURE.
- 15. EFFECTIVE CONTROL MEASURES SHALL BE UTILIZED TO MINIMIZE THE DISCHARGE OF POLLUTANTS FROM THE $\underline{\text{DEVELOPMENT SITE}}.$ AT A MINIMUM, CONTROL MEASURES SHALL BE IMPLEMENTED IN ORDER TO:
- A. MINIMIZE THE DISCHARGE OF POLLUTANTS FROM EQUIPMENT AND VEHICLE WASHING, WHEEL WASH WATER, AND OTHER WASH WATER; AND
- B. MINIMIZE THE EXPOSURE OF BUILDING MATERIALS, BUILDING PRODUCTS, CONSTRUCTION WASTES, TRASH, LANDSCAPE MATERIALS, FERTILIZERS, PESTICIDES, HERBICIDES, DETERGENTS, VEHICLE FLUIDS, SANITARY WASTE, AND OTHER MATERIALS PRESENT ON THE <u>DEVELOPMENT SITE</u> TO PRECIPITATION AND TO STORMWATER.
- 16. ADEQUATE RECEPTACLES SHALL BE PROVIDED FOR THE DEPOSITING OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE <u>DEVELOPMENT</u> PROCESS. THE <u>APPLICANT</u> SHALL NOT ION WHEN IN DEBRIS GENERATED DURING THE <u>DEVELOPMENT</u> PROCESS. THE <u>APPLICANT</u> SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL DEBRIS UPON OR INTO ANY <u>DEVELOPMENT SITE</u>, CHANNEL, OR <u>IWMC</u>. THE <u>DEVELOPMENT SITE</u> SHALL BE MAINTAINED FREE OF CONSTRUCTION MATERIAL DEBRIS.
- 17. THE <u>ENFORCEMENT OFFICER</u> MAY REQUIRE ADDITIONAL OR ALTERNATE SOIL <u>EROSION</u> AND SEDIMENT CONTROL MEASURES, BASED ON <u>DEVELOPMENT SITE</u> SPECIFIC CONSIDERATIONS AND THE EFFECTIVENESS OF THE INSTALLED CONTROL MEASURES.

VEGETATIVE BUFFER NOTES

- 1. CONTRACTOR SHALL MARK THE VEGETATIVE BUFFER WITH LATHE AND RIBBON. THE AREA SHALL NOT BE DISTURBED BY CONSTRUCTION EQUIPMENT OR VEHICLES
- VEGETATIVE BUFFER SHALL BE INSPECTED FOR PROPER DISTRIBUTION OF FLOWS, SEDIMENT ACCUMULATION AND SIGNS OF RILL FORMATION. THEY SHALL AT A MINIMUM BE INSPECTED WEEKLY AND WITHIN 24 HOURS AFTER EVERY PRECIPITATION EVENT THAT PROVIDED 0.5 INCHES OF RAIN OR MORE DURING A 24 HOUR PERIOD.
- IF THE BUFFER BECOMES SILT COVERED, CONTAINS RILLS OR IS OTHERWISE RENDERED INEFFECTIVE, OTHER PERIMETER SEDIMENT CONTROL MEASURES SHALL BE INSTALLED. ERODED AREAS SHALL BE REPAIRED AND STABILIZED, REPAIR SHALL BE COMPLETED AS SOON AS POSSIBLE WITH CONSIDERATION TO SITE CONDITIONS.
- 4. THE BUFFER VEGITATION SHALL BE MAINTAINED BETWEEN A HEIGHT OF 4-12 INCHES.
- 5. MAINTENANCE OF THE BUFFER AREA WILL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

STABILIZATION TYPE	JAN.	FEB.	MAR.	APR.	MAY.	JUNE	JULY	AUG.	SEPT.	ост.	NOV.	DEC.
PERMANENT SEEDING				А				А				
DORMANT SEEDING	N/A	N/A	N/A	N/A								
TEMPORARY SEEDING			В									
MULCHING											_	
TEMPORARY MULCHING												
EROSION CONTROL BLANKET												
A SEFTING AFRONAUTICS MIX (SEE IDOT SID. SPEC. AIRPORTS SECTION 901-217 FOR SEFTING MIXTURE)												

A.SEEDING AERONAUTICS MIX (SEE IDOT STD. SPEC. AIRPORTS SECTION 901-2,17 FOR SEEDING MIXTURE) B.TEMPORARY SEEDING (SEE IDOT STD. SPEC. ART. 280.04 f.)



STAPLE DETAIL

NOTES:

DETAIL 1

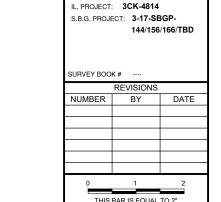
- Staples shall be placed in a diamond pattern at 2 per s.y. for stiched blankets. Non-stiched shall use 4 staples per s.y. of material. This equates to 200 staples with stiched blanket and 400 staples with non-stiched blanket per 100 s.y. of material.
- 2. Staple or push pin lengths shall be selected based on soil type and conditions. (minimum staple

PUSH PIN DETAIL

DETAIL 2

DETAIL 3

- 3. Erosion control material shall be placed in contact with the soil over a prepared seedbed.
- 4. All anchor slots shall be stapled at approximately 12" intervals.



IL. CONTRACT: **LK014**

IL. LETTING ITEM: 01A

AT FULL SCALE (34X22)

ᇫ 8/26 Z AIRPORT VOIS RUNWAY 8/ ₫-ĔΨ ⋝ェ S MUNICIPAL / E HILLS, ILLING REPROFILE RU ЫM LUTI DET, E HILLS POLI AND LAKE IN THE LAKE II REHABILITATE മ് WATER

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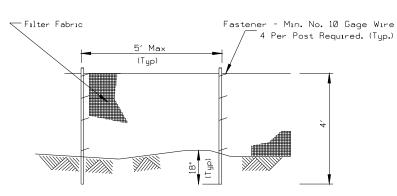
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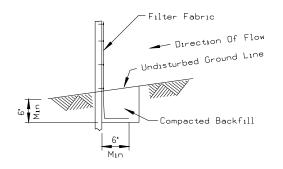
FINAL

SHEET 28 OF 41 SHEETS





ELEVATION

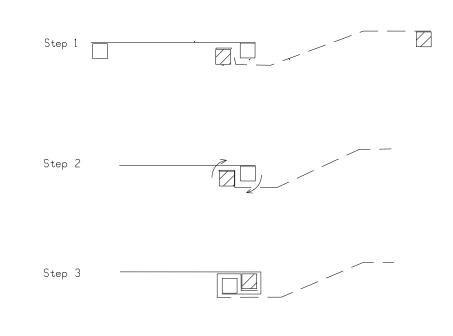


FABRIC ANCHOR DETAIL

NOTES:

- TEMPORARY SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. THEY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD AND REMOVED IN CONJUNCTION WITH THE FINAL GRADING AND SITE STABILIZATION.
- 2. FILTER FABRIC SHALL BE A WOVEN FABRIC MEETING THE REQUIREMENTS OF AASHTO M288 FOR UNSUPPORTED SILT FENCE WITH LESS THAN 50 PERCENT GEOTEXTILE ELONGATION.
- 3. FENCE POSTS SHALL BE WOOD POST WITH A MINIMUM CROSS-SECTIONAL AREA OF 3.0 SO. IN.

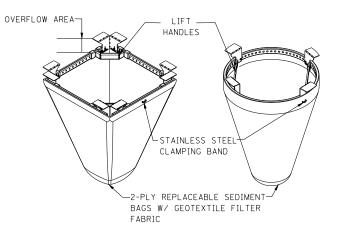
SILT FENCE DETAIL



ATTACHING TWO SILT FENCES

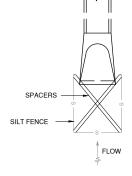
- 1. PLACE THE END POST OF THE SECOND FENCE INSIDE THE END POST OF THE FIRST FENCE
- 2. ROTATE BOTH POSTS AT LEAST 180 DEGREES IN A CLOCKWISE DIRECTION TO CREATE A TIGHT SEAL WITH THE FABRIC MATERIAL.
- 3. DRIVE BOTH POSTS A MINIMUM OF 18 INCHES INTO THE GROUND AND BURY THE FLAP.
- 4. SILT FILTER J-HOOK PLACEMENT SHALL BE IN ACCORDANCE WITH IDOT STD. 280001-07.

ATTACHING TWO SILT FENCES DETAIL

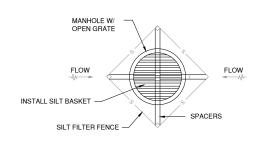


INLET PROTECTION - SILT BASKET (PAVEMENT AND TURF)

FOR ALL RECTANGULAR AND CIRCULAR INLETS

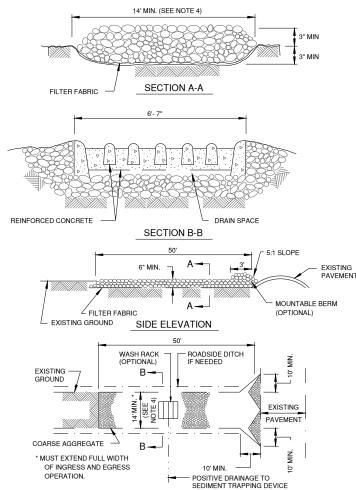






INLET PROTECTION (INLET/MANHOLES - IN TURF)

NOT TO SCALE IDOT STANDARD 280001-07

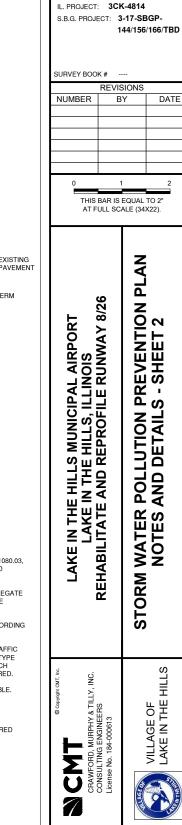


PLAN VIEW

STABILIZED CONSTRUCTION ENTRANCE

FROM NRCS STANDARD DRAWING NO. IL-630

- FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED UNDER SECTION 1080.03,
 OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED
- ROCK OR RECLAIMED CONCRETE SHALL MEET ONE OF THE FOLLOWING IDOT COARSE AGGREGATE GRADATION, CA-1, CA-2, CA-3 OR CA-4. COMPACTION SHALL BE TO THE SATISFACTION OF THE
- 3. ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHALL BE CONSTRUCTED ACCORDING TO MANUFACTURERS SPECIFICATIONS AND SHALL BE INCIDENTAL TO THE CONTRACT.
- 4. MINIMUM WIDTH IS 14' FOR ONE-WAY TRAFFIC AND 20' FOR TWO WAY TRAFFIC. TWO-WAY TRAFFIC WIDTHS SHALL BE INCREASED A MINIMUM OF 4' FOR TRAILER TRAFFIC, DEPENDING ON THE TYPE OF VEHICLE OR EQUIPMENT, SPEED, LOADS, CLIMATIC AND OTHER CONDITIONS UNDER WHICH VEHICLES AND EQUIPMENT OPERATE AN INCREASE IN THE MINIMUM WIDTHS MAY BE REQUIRED.
- 5. ROADWAY SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE.
- 6. IF WASH RACK ARE USED THEY SHALL BE INSTALLED ACCORDING TO THE MANUFACTURER'S
- 7. THE STABILIZED CONSTRUCTION ENTRANCE SHALL NOT BE PAID FOR BUT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



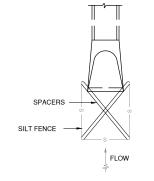
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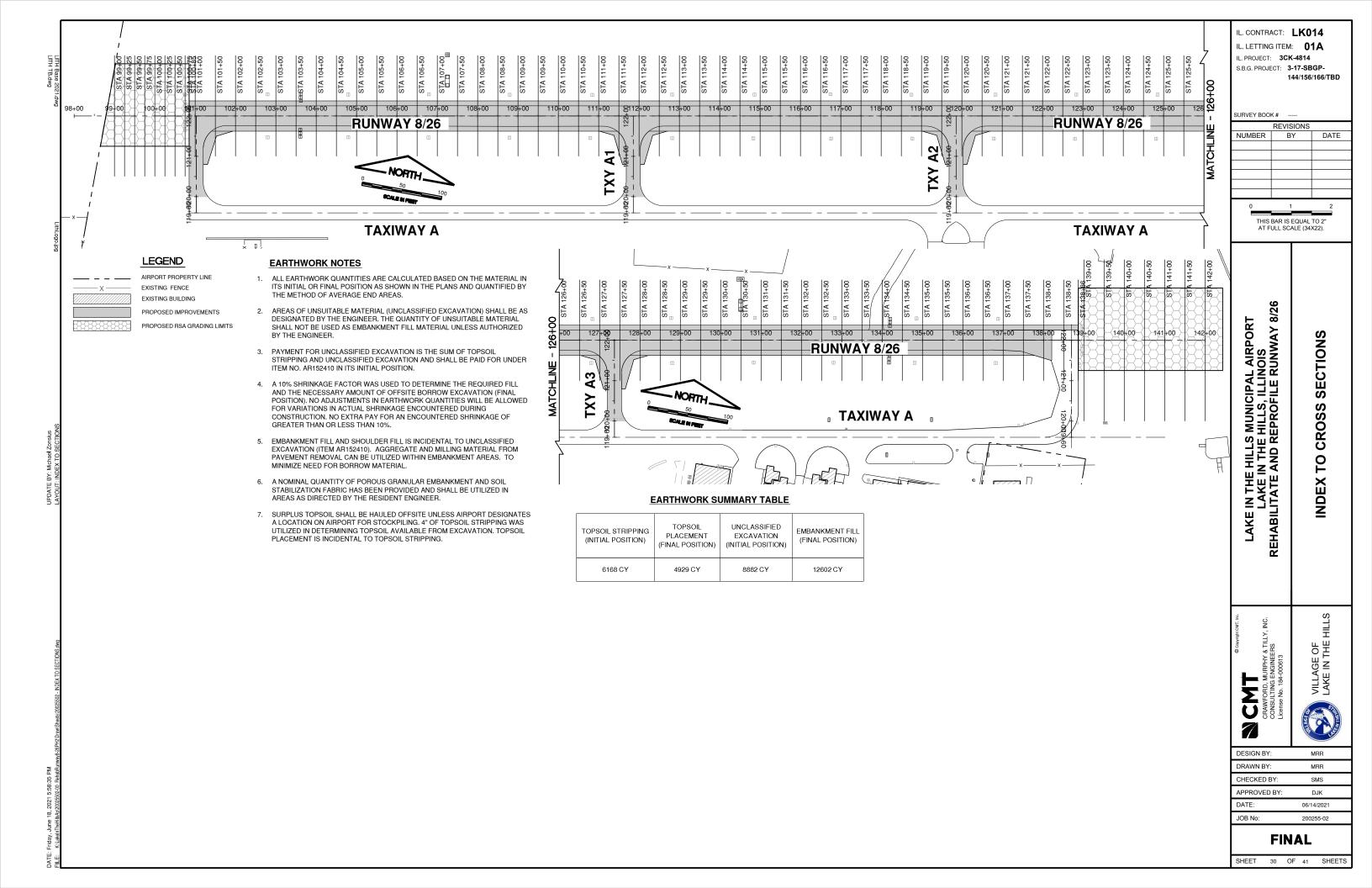


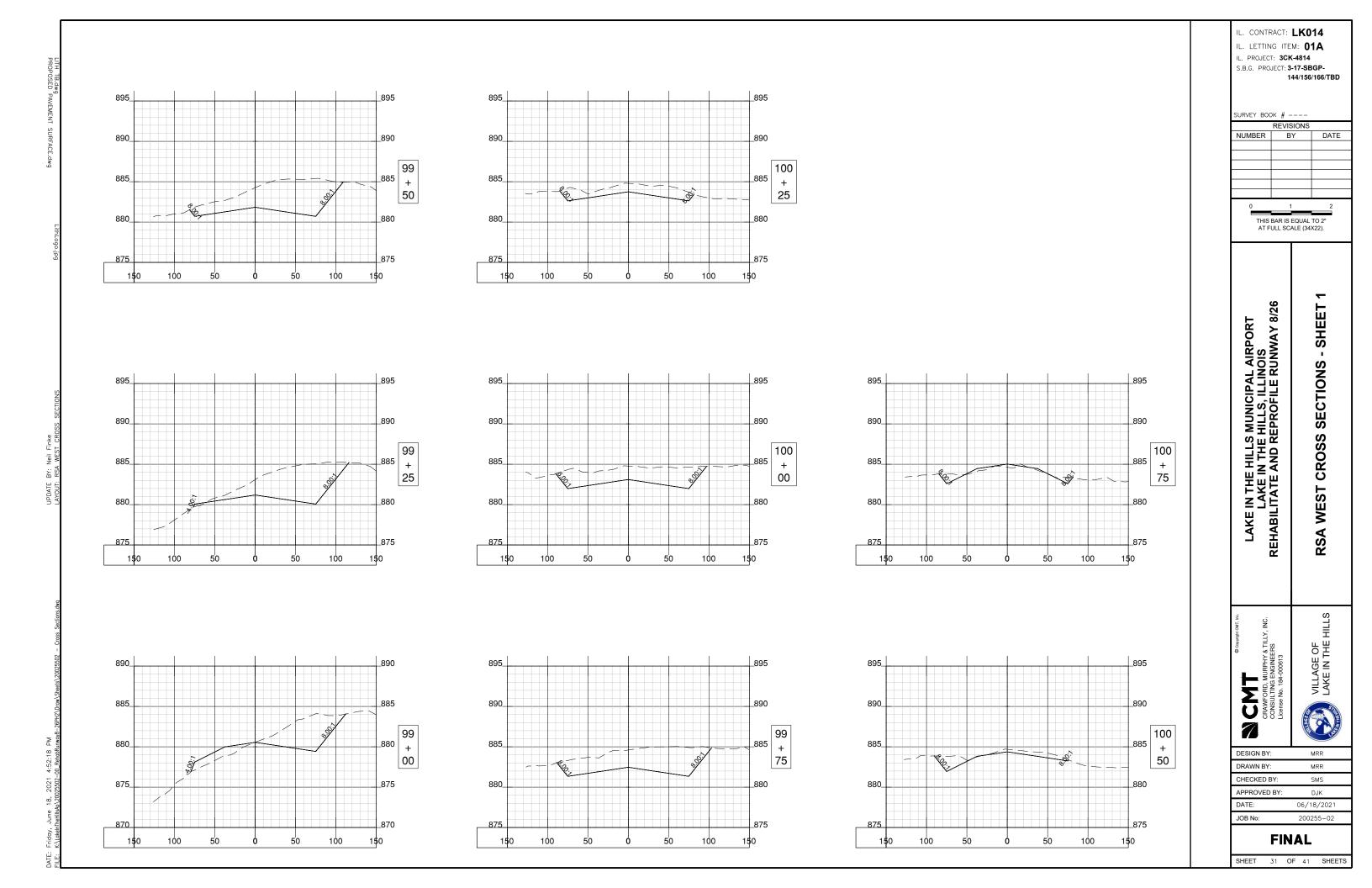
DESIGN BY: DRAWN BY: CHECKED BY JOB No: 200255-02

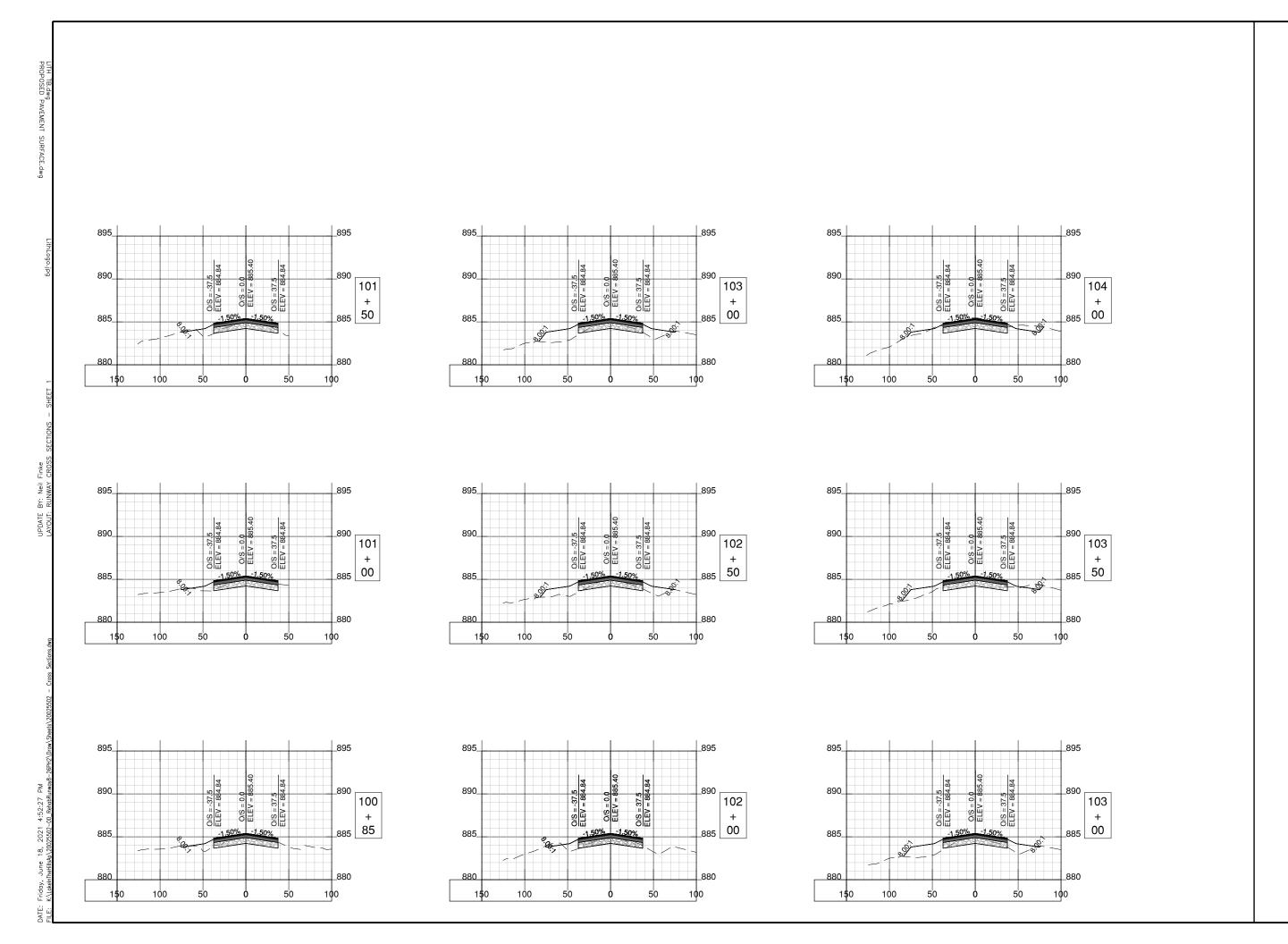
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SHEET 29 OF 41 SHEETS









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RUNWAY CROSS SECTIONS - SHEET

LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS REHABILITATE AND REPROFILE RUNWAY 8/26

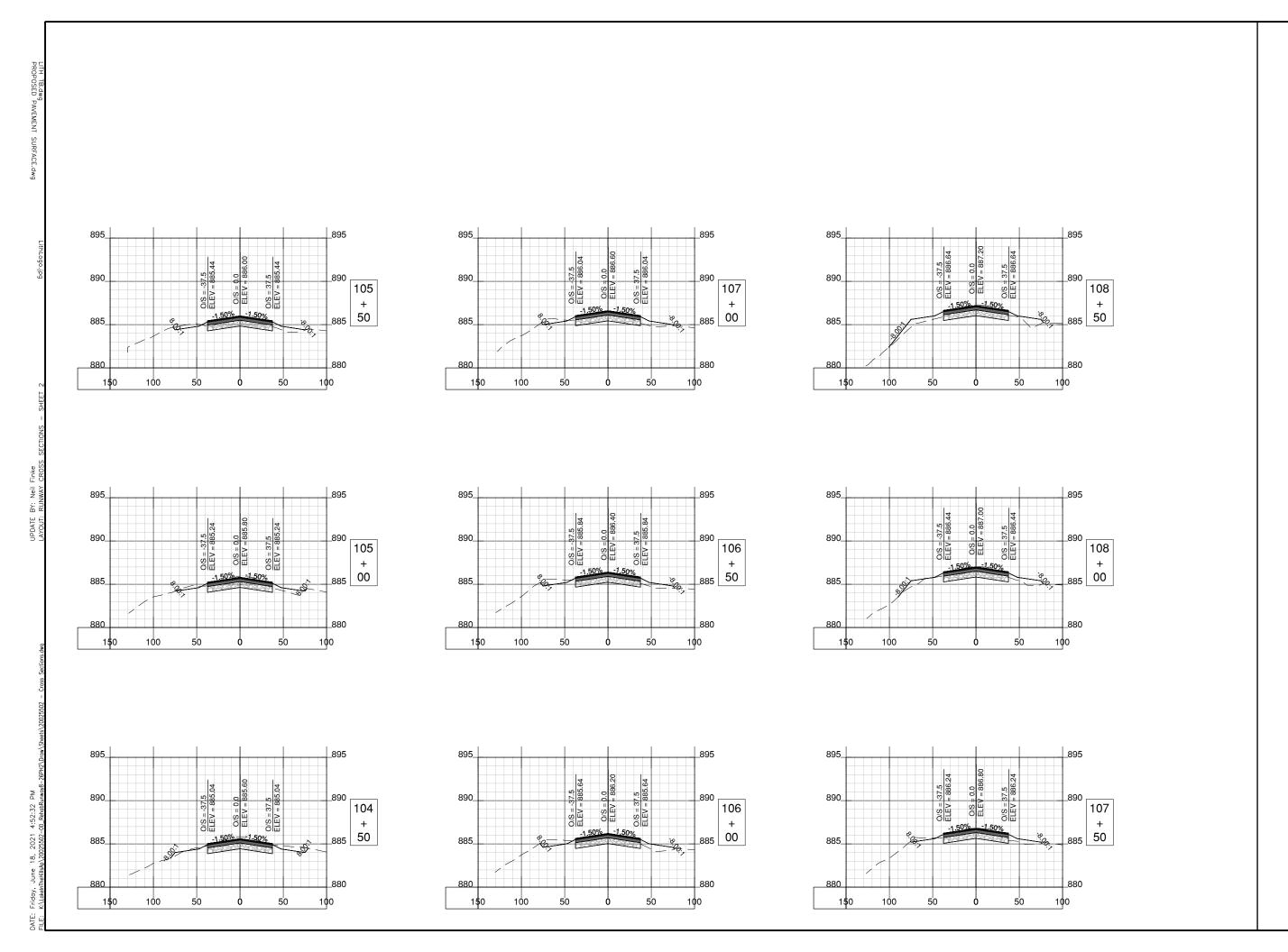
VILLAGE OF LAKE IN THE HILLS



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DRAWN BY:	MRR		
CHECKED BY:	SMS		
APPROVED BY:	DJK		
DATE:	06/18/2021		
JOB No:	200255-02		

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SHEET 32 OF 41 SHEETS



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RUNWAY CROSS SECTIONS - SHEET

LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS REHABILITATE AND REPROFILE RUNWAY 8/26

VILLAGE OF LAKE IN THE HILLS

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DRAWN BY:	MRR
CHECKED BY:	SMS
APPROVED BY:	DJK
DATE:	06/18/2021
JOB No:	200255-02

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SHEET 33 OF 41 SHEETS

__895 __895 895_ _895 O/\$ = 0.0 ELEV = 887.44 O/\$ = 0.0 ELEV = 887.42 ELEV = 886.72 O.S = 0.0 1.20% 1.20% O/S = -37.5 ELEV = 886.8 890_ 890_ 890_ 890 110 111 O/S = 113 _885 00 885 00 885 50 885_ 885_ 885_ .880 _880 _880 50 100 50 100 50 100 100 50 100 UPDATE BY: Neil Finke LAYOUT: RUNWAY CROS _895 __895 895_ _895 895_ O/S = 0.0 ELEV = 887.41 890_ _890 890_ 890_ _890 111 109 112 _885 50 _885 50 00 885_ 885_ _885 885_ 880 _880 _880 100 50 100 50 50 100 50 150 50 _895 _895 __895 895_ 895_ 895_ O/S = 0.0 ELEV = 887.40 O/\$ = 0.0 ELEV = 887.43 O/\$ = 0.0 ELEV = 887.45 June 18, 2021 4:52:37 PM 890_ 890 890_ 890_ _890 109 110 112 _885 00 00 50 885 885_ _885 885_ 885_ 880 _880 _880 100 50 100 50 50 100

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က LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS REHABILITATE AND REPROFILE RUNWAY 8/26

RUNWAY CROSS SECTIONS - SHEET

VILLAGE OF LAKE IN THE HILLS

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JOB No:

SHEET 34 OF 41 SHEETS

895_ __895 O/S = 0.0 ELEV = 887.0 890 895_ _895 895_ __895 890 00 = 0.0 ELEV = 886.57 114 885_ 885 890_ 890_ _890 116 117 50 _885 50 00 880 880 885_ 885_ 875 _880 _880 100 50 50 100 100 50 50 100 100 50 100 UPDATE BY: Neil Finke LAYOUT: RUNWAY CROS _895 __895 895_ 895_ _895 895_ ELEV = 886.04 O/S = 0.0 O/S = 37.5 O/S = 0.0 ELEV = 887.11 890_ _890 890_ 890_ _890 117 114 115 _885 00 50 _885 00 885_ 885_ 885 885_ 880 _880 _880 100 50 100 50 50 100 50 50 _895 _890 _895 895_ 890_ 895_ O/S= O/S = 0.0 ELEV = 887.20 O/S = 0.0 ELEV = 86 890_ 890 885_ 890_ _890 113 115 116 50 00 _885 50 885 880_ 885_ _880 [880 _875 _880 100 50 100 50 50 100

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RUNWAY CROSS SECTIONS - SHEET

4

LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS REHABILITATE AND REPROFILE RUNWAY 8/26

VILLAGE OF LAKE IN THE HILLS

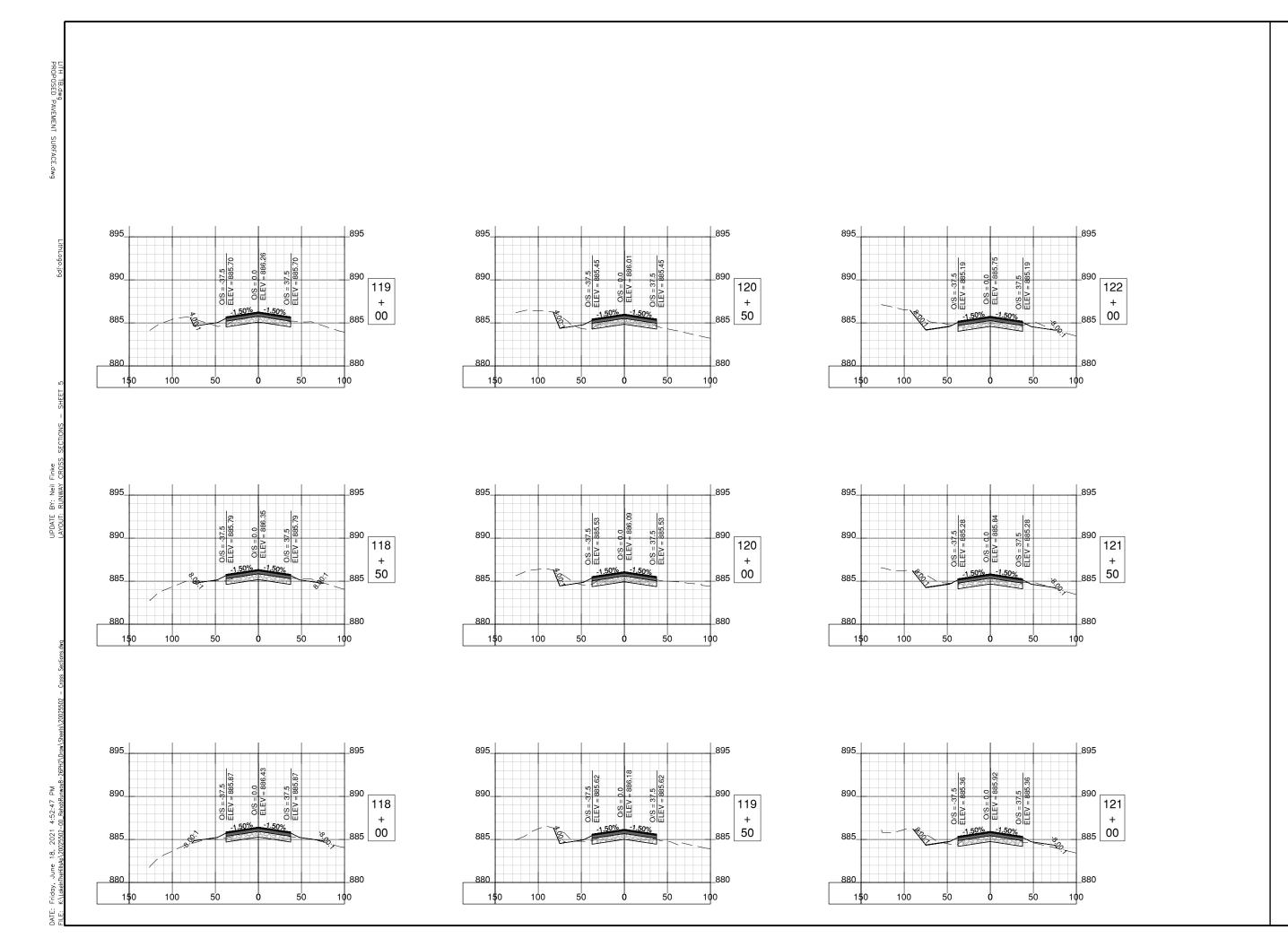


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5

LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS REHABILITATE AND REPROFILE RUNWAY 8/26 RUNWAY CROSS SECTIONS - SHEET

VILLAGE OF LAKE IN THE HILLS

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SHEET 36 OF 41 SHEETS

895_ __895 O/S = 0.0 ELEV = 886.32 __895 895_ _895 890_ 890 895_ 126 890_ 885_ _885 890_ 123 125 50 885 50 00 885_ 885_ 885 880_ _880 .880 _880 _875 100 100 50 100 50 50 100 50 UPDATE BY: Neil Finke LAYOUT: RUNWAY CROS _895 __895 895_ 895_ _895 895_ 890_ 890_ 890_ _890 _890 O/S = 0.0 ELEV = 8 123 124 126 50 _885 00 _885 00 885_ 885_ 885 885_ 880 _880 _880 100 50 50 100 50 50 100 50 50 _895 _895 _895 895_ 895_ 895_ O/S = 0.0 ELEV = 886.0 890_ 890 890_ 890_ _890 122 124 125 _885 50 00 _885 50 885_ 885_ 885 880 880 _880 _880 100 100 50 50

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9 LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS REHABILITATE AND REPROFILE RUNWAY 8/26

RUNWAY CROSS SECTIONS - SHEET

VILLAGE OF LAKE IN THE HILLS

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SHEET 37 OF 41 SHEETS

__895 895_ _895 __895 O/S = 0.0 ELEV = 886.7 890_ 890_ 890_ 890 _890 128 129 131 _885 00 885 00 50 885_ 885_ 885_ _880 _880 _880 50 100 50 100 100 50 100 50 100 UPDATE BY: Neil Finke LAYOUT: RUNWAY CROS _895 __895 895_ 895_ _895 895_ 890_ _890 890_ 890_ _890 127 129 130 _885 50 _885 | 50 00 885_ 885_ 885_ 880 _880 _880 100 50 100 50 50 100 50 50 150 _895 _895 _895 895_ 895_ 895_ 890_ 890 890_ 890_ _890 127 128 130 _885 00 _885 00 50 885_ 885_ 885 880 880 _880 _880 100 50 100 50 50 100

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7

LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS REHABILITATE AND REPROFILE RUNWAY 8/26 RUNWAY CROSS SECTIONS - SHEET

VILLAGE OF LAKE IN THE HILLS

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SHEET 38 OF 41 SHEETS

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RUNWAY CROSS SECTIONS - SHEET

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LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS REHABILITATE AND REPROFILE RUNWAY 8/26

VILLAGE OF LAKE IN THE HILLS



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DATE:	06/18/2021	
JOB No:	200255-02	

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SHEET 39 OF 41 SHEETS

895_ __895 895_ _895 890_ 890_ 890 137 138 885 00 885 50 885 885_ LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS REHABILITATE AND REPROFILE RUNWAY 8/26 _880 _880 50 100 50 50 100 100 50 100 UPDATE BY: Neil Finke LAYOUT: RUNWAY CROS _895 _895 895_ 895_ 890_ _890 890_ _890 136 138 _885 50 _885 00 885_ 885_ _880 _880 50 100 50 100 50 **MCMT** _895 _895 _895 895_ 895_ 895_ 890_ 890 890_ 890 890_ _890 136 137 138 _885 00 50 _885 86 885_ 885 885_ 885_ DATE: JOB No: 880 880 _880 _880 100 50 100 50 50 100

IL. CONTRACT: **LK014** IL. LETTING ITEM: 01A

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RUNWAY CROSS SECTIONS - SHEET

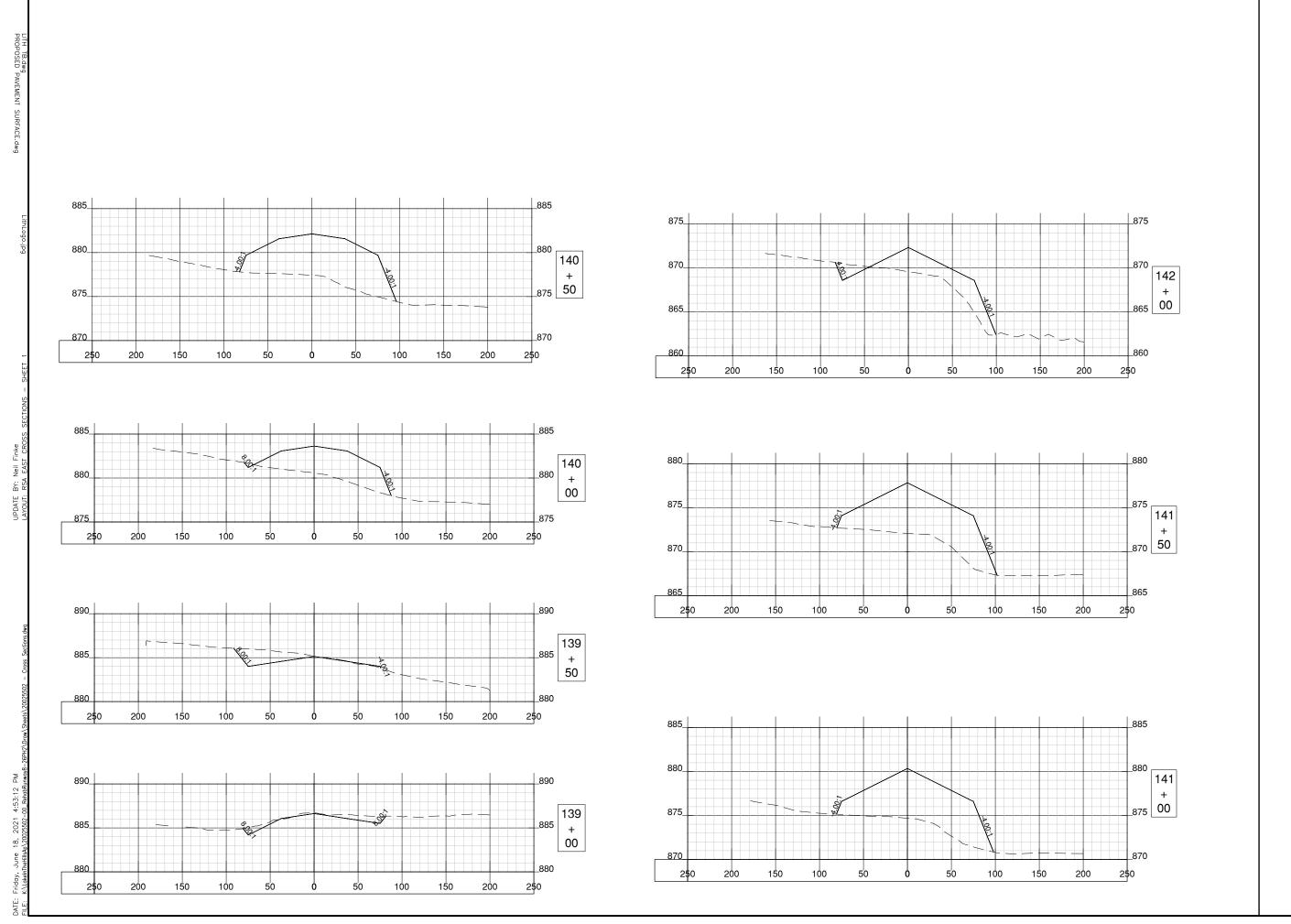
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VILLAGE OF LAKE IN THE HILLS

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SHEET 40 OF 41 SHEETS



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 $\overline{}$ LAKE IN THE HILLS MUNICIPAL AIRPORT LAKE IN THE HILLS, ILLINOIS REHABILITATE AND REPROFILE RUNWAY 8/26 RSA EAST CROSS SECTIONS - SHEET

VILLAGE OF LAKE IN THE HILLS

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