CONSTRUCTION PLANS

REHABILITATE RUNWAY 9-27 AND REMARK

KEWANEE AIRPORT AUTHORITY
KEWANEE MUNICIPAL AIRPORT (EZI)
KEWANEE, HENRY COUNTY, ILLINOIS

SBG PROJECT NO. 3-17-SBGP-TBD IDA PROJECT NO. EZI-4528

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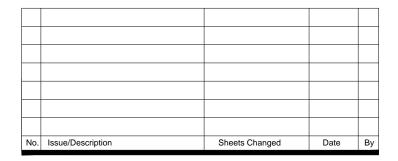


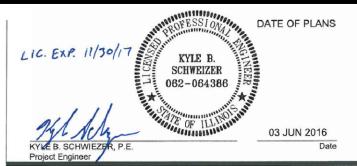
NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

VICINITY MAP

LOCATION MAP







KEWANEE MUNICIPAL AIRPORT	KEWANEE MUNICIPAL AIRPOR 3761 Midland Road Kewanee, Illinois 61443 Telephone: 309.853.8222
Dale Lindstrom	03 JUN 2016 Date

SUMMARY OF QUANTITIES						
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY		
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1			
AR150520	MOBILIZATION	L.S.	1			
AR150540	HAUL ROUTE	L.S.	1			
AR152480	SHOULDER ADJUSTMENT	S.Y.	800			
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	L.F.	2,000			
AR401655	BUTT JOINT CONSTRUCTION	S.Y.	1,353			
AR401910	REMOVE & REPLACE BIT. PAVEMENT	S.Y.	20			
AR402622	POROUS FRICTION COURSE, 0.10'	S.Y.	41,608			
AR603510	BITUMINOUS TACK COAT	GAL.	10,402			
AR620520	PAVEMENT MARKING-WATERBORNE	S.F.	18,544			
AR620525	PAVEMENT MARKING-BLACK BORDER	S.F.	6,976			
AR800400	REFLECTIVE CRACK CONTROL TREATMENT	S.Y.	125			

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IARY OF QUANTITIES AND INDEX TO SHEETS
TY PLAN
TRUCTION PHASING PLAN - AREA 1
TRUCTION PHASING PLAN - AREA 2
IG DETAILS AND NOTES
PLAN - SHEET 1
PLAN - SHEET 2
/AY 9-27 & 1-19 INTERSECTION DETAIL
ING DETAILS AND NOTES
T T IC



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KEWANEE MUNICIPAL AIRPORT

3761 MIDLAND RD. KEWANEE IL, 61443

REHABILITATE RUNWAY 9-27 AND REMARK

IDA No: EZI-4528 SBG No: 3-17-SBGP-TBD

Contract No. KE014

NO.	DATE	DES	CRIPT	ION		
I NO.	DATE	DES	DWN	REV		
ISSUE: JUNE 3, 2016						
PROJECT NO: 16A0020						

PROJECT NO: 16A0020

CAD FILE: G-002-FLP.DWG

DESIGN BY: SJZ 04/11/2016

DRAWN BY: RAD 04/20/2016 REVIEWED BY: KBS 06/03/2016

SHEET TITLE

SUMMARY OF QUANTITIES AND INDEX TO SHEETS

HAUL ROUTE AND STAGING AREA

THE CONTRACTOR WILL BE REQUIRED TO USE AN EXISTING GRAVEL LOT AS A STAGING AREA AND CONSTRUCT AN ALL-WEATHER HAUL ROUTE ALONG THE FIELD LINE EAST OF THE AIRPORT TO THE RUNWAY. SEPARATION FABRIC SHALL BE PLACED OVER THE EXISTING GROUND PRIOR TO ANY STABILIZATION MATERIAL IS PLACED. UPON COMPLETION OF THE PROJECT THE HALL SHALL BE REMOVED AND RESTORED TO ITS ORIGINAL CONDITION. TILLED AREAS SHALL BE CHISEL PLOWED OR DISKED TO AN UNCOMPACTED STATE. TURF AREAS SHALL BE REWORKED AND RESEEDED. ALL WORK AND MATERIALS ASSOCIATED WITH THE HAUL ROUTE AND STAGING AREA SHALL BE PAID FOR UNDER AR150540 "HAUL ROUTE" -- PER LUMP

EROSION CONTROL

THIS PROJECT WILL DISTURB LESS THAN 1 ACRE OF LAND. THEREFORE NO N.P.D.E.S. PERMIT WILL BE REQUIRED.

- 1. THERE ARE RUNWAY AND TAXIWAY LIGHTS ADJACENT TO THE WORK AREA. THE CONTRACTOR SHALL TAKE CARE TO AVOID DAMAGE TO THE LIGHTS. ANY DAMAGE TO THE LIGHTS WILL BE CONTRACTOR'S COST AND RESPONSIBILITY TO REPAIR.
- 2. TO PREVENT DAMAGE TO THE EXISTING THRESHOLD LIGHTS DURING PAVING OPERATIONS, THE CONTRACTOR MAY REMOVE THESE LIGHTS PROVIDED THEY ARE REINSTALLED TO THEIR ORIGINAL CONDITION AT CONTRACTOR'S COST.

CERTIFIED PAYROLLS

THE RESIDENT ENGINEER CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL **CERTIFIED PAYROLLS** FOR THE PERIOD HAVE REEN RECEIVED

MATERIAL CERTIFICATION

MATERIALS TO BE INCORPORATED INTO THE PROJECT CANNOT BE USED WITHOUT PRIOR APPROVAL. ALL MATERIALS TO BE USED IN THE PROJECT MUST BE SUBMITTED TO THE RESIDENT ENGINEER FOR APPROVAL LISE OF MATERIALS WITHOUT PRIOR APPROVAL AND ULTIMATELY DETERMINED TO BE UNACCEPTABLE BY THE ILLINOIS DIVISION OF AERONAUTICS ARE SUBJECT TO REMOVAL AND/OR NON-PAYMENT.

UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE. SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES. INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO

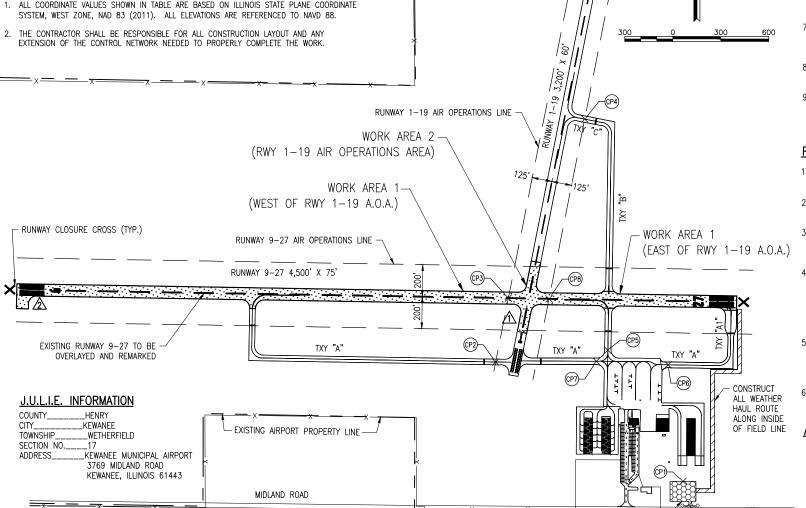
COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

CRITICAL POINTS						
POINT #	DESCRIPTION	WORK AREA	LATITUDE	LONGITUDE	GRND. ELEV. (MSL)	HEIGHT (AGL)
1	CONST. EQUIP.	ALL	N041* 12' 02.836"	W089* 57' 31.283"	849.0	25
2	BARRICADE	1	N041° 12′ 10.233″	W089' 57' 46.574"	854.0	N/A
3	CONST. EQUIP.	1	N041° 12' 14.178"	W089* 57' 45.524"	856.0	25
4	BARRICADE	1	N041* 12' 25.224"	W089* 57' 39.248"	853.0	N/A
5	BARRICADE	ALL	N041' 12' 10.949"	W089° 57' 37.402"	854.0	N/A
6	BARRICADE	ALL	N041° 12' 10.037"	W089° 57′ 32.480″	853.0	N/A
7	BARRICADE	2	N041° 12' 10.262"	W089° 57′ 38.339"	854.0	N/A
8	CONST. EQUIP.	1	N041° 12' 14.130"	W089° 57' 42.201"	858.0	25

HORIZONTAL AND VERTICAL CONTROL DATA						
NO.	DESCRIPTION	NORTHING	EASTING	ELEV.		
\square	"KEWPORT" NGS MONUMENT	1,652,431.74	2,352,728.14	854.2		
<u> </u>	"KEWPORT AZ MK" NGS MONUMENT	1,652,491.68	2,349,792.88	850.6		
	RUNWAY 9/27 ALIGNMENT STA. 218+00.00	1,652,583.99	2,349,650.53			
	RUNWAY 9/27 ALIGNMENT STA. 262+99.60	1,652,508.15	2,354,149.33			

SURVEY NOTES

- 1. ALL COORDINATE VALUES SHOWN IN TABLE ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM, WEST ZONE, NAD 83 (2011). ALL ELEVATIONS ARE REFERENCED TO NAVD 88.



LEGEND EXISTING IMPROVEMENTS

PROPOSED IMPROVEMENTS

EXISTING BUILDINGS

PROPOSED EQUIPMENT AND EMPLOYEE PARKING AREA

PROPOSED HAUL ROUTE

- LIGHTED AND/OR FLAGGED WITH A 3'x3' ORANGE AND WHITE CHECKERED FLAG IN ACCORDANCE WITH AC 150/5210-5D AND 70/7460-1.
- 4. RADIO CONTROL THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT WITH THE AIRPORT UNICOM (122.80 MHz.) ANY TIME HE HAS EQUIPMENT OR PERSONNEL ON THE AIRFIELD.
- AS SHOWN ON THIS SHEET. THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THE STAGING AREA. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE THIS AREA.
- 7. AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL CLOSE THE EXISTING GATE IN THE HAUL ROUTE. WHENEVER HE IS NOT IN A CONTINUOUS HAULING OPERATION.
- 8. ALL WORK PERFORMED SHALL BE DONE IN A ORDERLY AND EFFECTIVE MANNER TO MINIMIZE RUNWAY CLOSURE.
- 9. THE CONTRACTOR SHALL PROVIDE A WRITTEN STATEMENT OF HIS ACCEPTANCE AND ASSURANCE THE HE WILL COMPLY WITH THE PROPOSED CONSTRUCTION SAFETY AND PHASING PLAN PRIOR TO BE

RUNWAY AND TAXIWAY CLOSURE PROCEDURES

- 1 THE AIRPORT MANAGER OR REPRESENTATIVE WILL ISSUE ALL APPLICABLE NOTAM'S TO OPEN AND CLOSE THE RUNWAYS AND TAXIWAYS.
- 3. THE CONTRACTOR WILL OBTAIN APPROVAL FROM THE AIRPORT MANAGER OR REPRESENTATIVE PRIOR TO ENTERING ONTO ANY ACTIVE RUNWAY OR
- 4. THE CONTRACTOR WILL CONTACT THE AIRPORT MANAGER OR REPRESENTATIVE FOR VERIFICATION OF RUNWAY OR TAXIWAY CLOSURE PRIOR TO ENTERING ONTO A RUNWAY OR TAXIWAY TO PLACE EQUIPMENT SUCH AS BARRICADES AND RUNWAY DESIGNATION CROSSES THE AIRPORT MANAGER OR REPRESENTATIVE WILL DIRECT THE LOCATION
- 6. THE AIRPORT MANAGER OR REPRESENTATIVE WILL INSPECT THE AREA PRIOR TO REMOVING EQUIPMENT AND CANCELLING NOTAM'S.

THE AIRCRAFT OPERATION LINE WILL DESIGNATE HOW CLOSE TO A RUNWAY/TAXIWAY ANY CONTRACTOR PERSON/VEHICLE MAY GET WITHOUT CLOSING THE RUNWAY/TAXIWAY, AS THE CONTRACTOR STARTS WORKING IN EACH CONSTRUCTION AREA AS DESCRIBED ON THE FOLLOWING SHEETS, HE WILL PLACE BARRICADES ACROSS THE INACTIVE (CLOSED) PAVEMENT WHERE HIS PERSONNEL WILL BE CROSSING THE ACTIVE RUNWAY/TAXIWAY, THE BARRICADES WILL BE PLACED 10' APART FOR A DISTANCE OF 30' (ON TAXIWAYS) AND 75' (ON RUNWAYS). THE CONTRACTOR WILL MAINTAIN THE AIRCRAFT OPERATION LINES FOR THE DURATION OF THIS PROJECT

PROPOSED SAFETY PLAN AND NOTES

- 1. GENERAL THE KEWANEE MUNICIPAL AIRPORT IS COMPRISED OF TWO RUNWAYS. THE PROPOSED CONSTRUCTION WILL NECESSITATE CLOSING BOTH RUNWAY 9-27 AND 1-19 AND PORTIONS OF TAXIWAY A, TAXIWAY B AND TAXIWAY C. RUNWAY 9-27, TAXIWAY B AND TAXIWAY C WILL BE CLOSED AT THE START OF THIS PROJECT AND WILL REMAIN CLOSED UNTIL THE COMPLETION OF THIS PROJECT, RUNWAY 1-19 WILL BE CLOSED WHEN CONSTRUCTION ACTIVITIES ARE WITHIN 125 FEET OF ITS CENTERLINE. TAXIWAY A WILL BE CLOSED WEST OF RUNWAY 1 END TO RUNWAY 9-27 AND EAST OF APRON TO THE RUNWAY 27 END. THE CLOSURE OF RUNWAY 1-19 WILL CLOSE THE AIRPORT; THEREFORE THE CONTRACTOR WILL BE ALLOWED 7 CALENDAR DAYS TO COMPLETE THE REQUIRED WORK WITHIN AREA 2. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- 2. ALL CONSTRUCTION/OPERATIONS ARE TO BE PERFORMED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR (AC) 150/5370-2F "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" AND AC 150/5300-13A "AIRPORT DESIGN".
- 3. ALL CONSTRUCTION EQUIPMENT ON THE AIRPORT SHALL BE MARKED,
- 5 THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREA WILL BE
- 6. EXCEPT FOR THE PERIODS OF TIME WHEN RUNWAY 1-19 IS ALLOWED TO BE CLOSED, THE CONTRACTOR SHALL MAINTAIN TAXIWAY ACCESS VIA TAXIWAY "A" BETWEEN RUNWAY 1-19 AND ALL HANGARS AND ADMINISTRATIVE AREAS.

- ISSUED THE NOTICE TO PROCEED.
- 2. THE CONTRACTOR WILL PROVIDE NOTICE TO THE AIRPORT MANAGER AT LEAST 72 HOURS IN ADVANCE OF CLOSING ANY RUNWAY OR TAXIWAY.
- OF SUCH FOUIPMENT
- 5. THE CONTRACTOR WILL CONTACT THE AIRPORT MANAGER OR REPRESENTATIVE OF WORK COMPLETION ON THE RUNWAY OR TAXIWAY PRIOR TO REMOVING ANY EQUIPMENT SUCH AS BARRICADES AND RUNWAY DESIGNATION CROSSES.

AIRCRAFT OPERATION LINE

- FXISTING

SWING GATE

EQUIPMENT AND EMPLOYEE

PARKING AREA (EXISTING

GRAVEL)

Offices Nationwide

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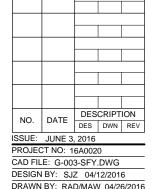
KEWANEE MUNICIPAL AIRPORT

3761 MIDLAND RD. KEWANEE IL, 61443

REHABILITATE RUNWAY 9-27 AND REMARK

IDA No: EZI-4528 SBG No: 3-17-SBGP-TBD

Contract No. KE014



SHEET TITLE

REVIEWED BY: KBS 06/03/2016

SAFETY PLAN

PROPOSED SEQUENCE OF WORK

AREA 1

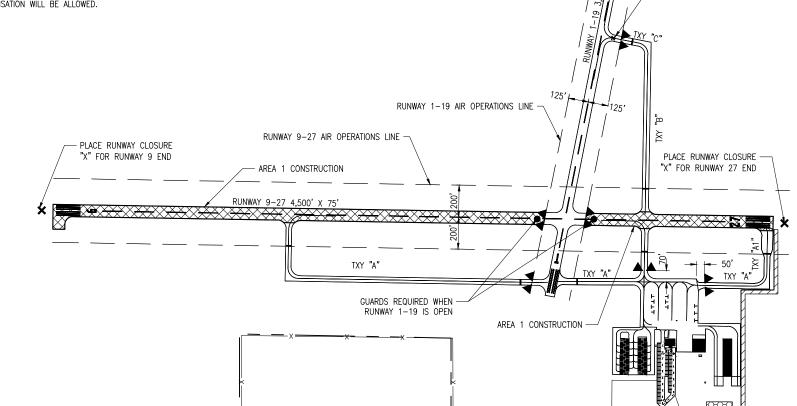
- 1. AREA 1 CONSTRUCTION SHALL CONSIST OF ALL WORK LOCATED ON RUNWAY 9-27 AND PORTIONS OF TAXIWAY CONNECTORS 125' FROM RUNWAY 1-19 CENTERLINE.
- 2. AT THE START OF CONSTRUCTION ACTIVITIES THE CONTRACTOR SHALL CLOSE RUNWAY 9-27, TAXIWAY "B" AND TAXIWAY "C" IN ACCORDANCE WITH THE PROCEDURE SET FORTH ON THE PROPOSED SAFETY PLAN. THE ASSOCIATED RUNWAY LIGHTING CIRCUIT AND NAVAIDS WILL BE TURNED OFF.
- 3. RUNWAY 9-27 AND TAXIWAYS "B" AND "C" WILL BE CLOSED FOR THE DURATION OF THIS PROJECT.
- 4. TAXIWAY "A" WILL BE CLOSED FROM RUNWAY 9-27 TO RUNWAY 1 END AND FROM THE APRON TO RUNWAY 27 END FOR THE DURATION FOR THIS PROJECT
- 5. ONLY AFTER THE CROSSES AND BARRICADES HAVE BEEN PLACED WILL THE CONTRACTOR BE ALLOWED TO MOVE ONTO RUNWAY 9-27 AND BEGIN WORKING
- 6. ONCE ALL THE CRACK CLEANING AND SEALING, PAVEMENT REPAIRS AND OTHER PREPARATORY WORK WITHIN AREA 1 HAVE BEEN COMPLETED, THE CONTRACTOR WILL BE ALLOWED TO START WORK WITHIN AREA 2.
- 7. THE CONTRACTOR MAY FINISH SHOULDER ADJUSTMENT AND BALANCE OF MARKINGS ON RUNWAY 9-27 AND ADJACENT TAXIWAYS (AREA 1) AFTER THE COMPLETION OF THE CONSTRUCTION IN THE AREA 2.
- 8. WHEN WORK IS OCCURRING IN AREA 1 AND RUNWAY 1-19 IS OPEN, ALL CROSSING OF RUNWAY 1-19 SHALL OCCUR AT THE INTERSECTION WITH RUNWAY 9-27. THE CONTRACTOR SHALL SUPPLY GUARDS AT THE CROSSING TO ENSURE THERE ARE NO AIRCRAFT PRESENT BEFORE CONSTRUCTION TRAFFIC MAY CROSS RUNWAY 1-19. AIRCRAFT HAVE THE RIGHT OF WAY AT ALL TIMES.

TAXIWAY LIGHTING CLOSURE NOTES

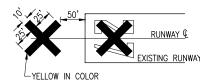
- 1. WHEN AREA 1 CONSTRUCTION ACTIVITIES BEGIN, THE ENTIRE TAXIWAY B AND TAXIWAY C AND SECTIONS OF TAXIWAY A WILL BE CLOSED (BARRICADED) TO AIRCRAFT MOVEMENT. THOSE SECTIONS OF TAXIWAY A, TAXIWAY B AND TAXIWAY C THAT ARE CLOSED TO AIRCRAFT MOVEMENT WILL NEED TO HAVE THEIR LIGHTING CIRCUIT DEACTIVATED.
- 2. TAXIWAY A, B AND C ARE ON THE SAME LIGHTING CIRCUIT. THE CONTRACTOR WILL "BAG" THE EXISTING TAXIWAY LIGHTS THAT ARE LOCATED ON THE SECTION OF TAXIWAY A, TAXIWAY B AND TAXIWAY C THAT ARE BEING CLOSED. THE "BAG" WILL NOT ALLOW ANY LIGHT TO EMIT FROM THE TAXIWAY LIGHT. THERE ARE OVER 80 TAXIWAY LIGHTS THAT WILL REQUIRE "BAGGING". ANY TAXI GUIDANCE SIGNS LOCATED WITHIN THE CLOSED AREA WILL ALSO BE "BAGGED".
- 3. THE "BAGGING" OF TAXIWAY LIGHTS AND SIGNS WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

BARRICADES

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AS SHOWN ON THIS SHEET AND AS DIRECTED BY THE AIRPORT MANAGER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING OR RED STEADY-BURN LIGHTS AND 20" SQUARE ORANGE FLAGS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



MIDLAND ROAD



DETAIL OF CROSS FOR CLOSED RUNWAY

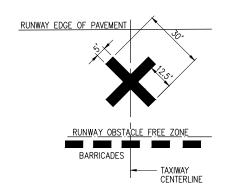
NOT TO SCALE

NOTE:

PLACE TAXIWAY CLOSURE

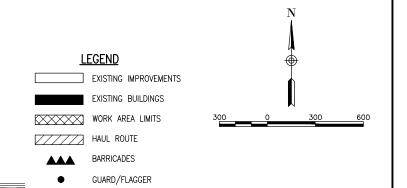
"X" FOR TAXIWAY "C"

COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE AIRPORT MANAGER. THE CROSSES WILL BE PLACED AS SHOWN IN THE ABOVE DETAIL AND SECURED IN A MANNER APPROVED BY THE AIRPORT MANAGER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING, PLACING, AND REMOVING THE CROSSES AS AN INCIDENTAL COST TO THE PROJECT.



- 1. TEMPORARY "CLOSED TAXIWAY" MARKINGS SHALL BE "AVIATION YELLOW"
- TEMPORARY "CLOSED TAXIWAY" MARKINGS SHALL BE CONSTRUCTED USING TEMPORARY PAINT SUCH AS SEYMOUR TEMPORARY MARKER, PART NO. 20-636, OR AN EQUIVALENT PAINT EASILY REMOVED WITH WATER WITHOUT DEFACING THE PAVEMENT.
- 3. TEMPORARY "CLOSED TAXIWAY" MARKINGS SHALL BE PLACED WHEN THE RUNWAY IS OPEN AND THE TAXIWAY IS CLOSED MORE THAN 72 HOURS. THE "CLOSED TAXIWAY" MARKINGS SHALL BE PLACED AS SHOWN ON THIS SHEET. IT IS ANTICIPATED THAT ONLY TAXIWAY "O" WILL REQUIRE A TEMPORARY "CLOSED TAXIWAY" MARKING.
- 4. THE PROPOSED CROSS WILL BE REMAIN IN PLACE EACH DAY THE TAXIWAY IS CLOSED AND REMOVED WHEN THE TAXIWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. "CLOSED TAXIWAY" MARKINGS SHALL NOT BE A PAY ITEM AND SHALL BE INCIDENTAL TO OTHER CONTRACT BID ITEMS.

TEMPORARY TAXIWAY CLOSURE CROSS DETAIL NOT TO SCALE



ESSESSE EQUIPMENT AND EMPLOYEE PARKING AREA

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NO.	DATE	DESCRIPTION				
NO.	DATE	DES	DWN	REV		
SSUE: JUNE 3, 2016						
PROJECT NO: 16A0020						
CAD FILE: G-004-PHS.DWG						

SHEET TITLE

DESIGN BY: SJZ 04/18/2016

DRAWN BY: RAD/MAW 05/02/2016

REVIEWED BY: KBS 06/03/2016

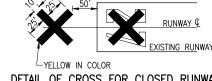
CONSTRUCTION PHASING PLAN -AREA 1

PROPOSED SEQUENCE OF WORK

- 1. AREA 2 CONSTRUCTION SHALL CONSIST OF CLEANING AND SEALING CRACKS, REMOVE AND REPLACE BITUMINOUS PAVEMENT, BUTT JOINT AND MILLING AND POROUS FRICTION COURSE. MATCH INTO EXISTING GRADE ON RUNWAY 1-19 AND PAYEMENT MARKINGS THAT ARE LOCATED WITHIN 125 FEET OF RUNWAY 1-19 CENTERLINE ON BOTH RUNWAY 1-19 AND RUNWAY 9-27.
- 2. THE CONTRACTOR WILL PLACE CROSSES ON RUNWAY ENDS 1, 19, AND BEYOND THE ENDS 9 AND 27 AS SHOWN ON THIS SHEET.
- 3. SINCE THIS STAGE WILL CLOSE THE AIRPORT TO ALL AIRCRAFT ACTIVITY THE CONTRACTOR WILL HAVE SEVEN CALENDAR DAYS TO COMPLETE ALL THE WORK WITHIN AREA 2.
- 4. THE CONTRACTOR MAY DELAY THE AREA 1 POROUS FRICTION COURSE OVERLAY UNTIL THE AREA 2 CONSTRUCTION. HOWEVER, DUE TO THE SEVEN CALENDAR DAY CLOSURE LIMITATION, ALL WORK - INCLUDING THE SHOULDER ADJUSTMENT AND MARKING - WITHIN 125' OF THE RUNWAY 1-19 CENTERLINE MUST BE COMPLETED BY THE END OF THE SEVENTH DAY.
- 5. UPON COMPLETION OF AREA 2 CONSTRUCTION ACTIVITIES THE CONTRACTOR WILL RE-OPEN RUNWAY 1-19 AND THE PORTION OF TAXIWAY A BETWEEN RUNWAY 1 END AND THE APRON.

BARRICADES

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AS SHOWN ON THIS SHEET AND AS DIRECTED BY THE AIRPORT MANAGER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING OR RED STEADY-BURN LIGHTS AND 20" SQUARE ORANGE FLAGS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



DETAIL OF CROSS FOR CLOSED RUNWAY

NOT TO SCALE

NOTE:

COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE AIRPORT MANAGER. THE CROSSES WILL BE PLACED AS SHOWN IN THE ABOVE DETAIL AND SECURED IN A MANNER APPROVED BY THE AIRPORT MANAGER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



Offices Nationwide

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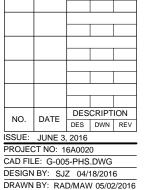
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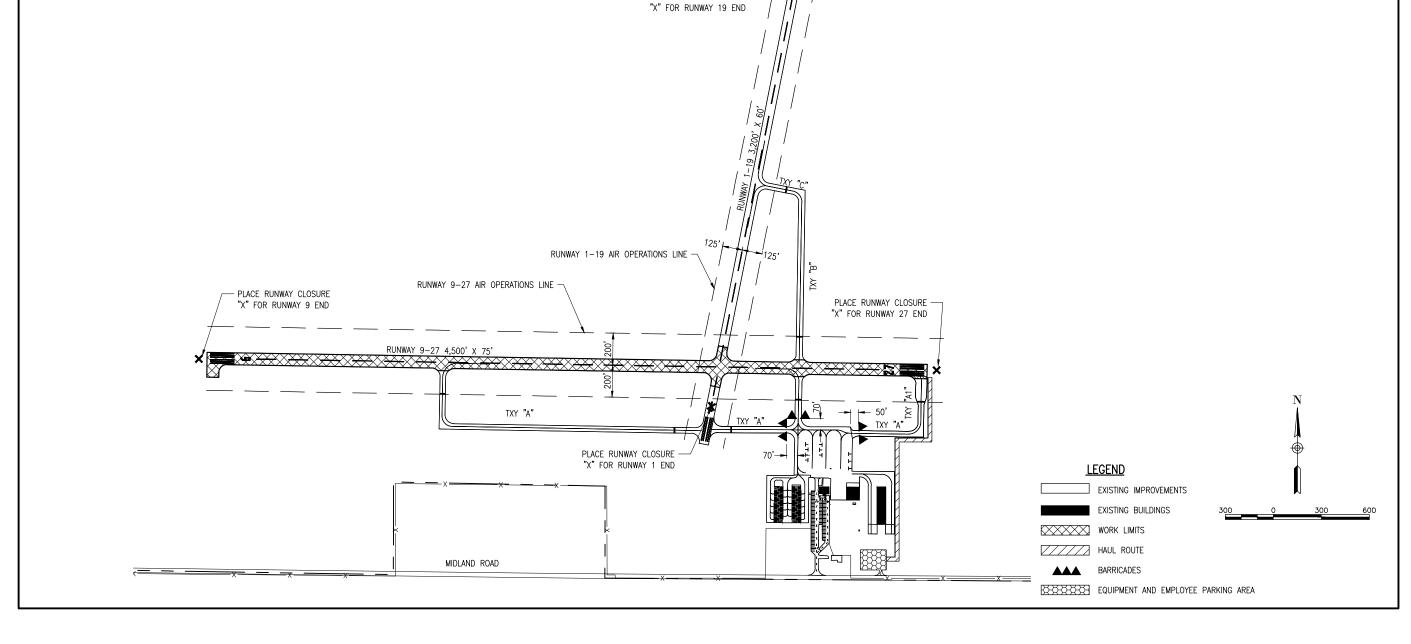
Contract No. KE014



CONSTRUCTION PHASING PLAN -AREA 2

REVIEWED BY: KBS 06/03/2016

SHEET TITLE



PLACE RUNWAY CLOSURE

 THIS ITEM OF WORK WILL BE PAID FOR AT THE CONTRACT PRICE PER LINEAR FOOT OF CLEANING AND SEALING CRACKS, COMPLETE; WHICH PRICE AND PAYMENT SHALL CONSTITUTE FULL COMPENSATION FOR ALL ROUTING, CLEANING, PREPARATION AND DISPOSAL OF ALL LOOSE MATERIALS; AND FOR ALL MATERIALS. LABOR. EQUIPMENT. TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM.

 PAYMENT FOR THIS ITEM OF WORK WILL BE UNDER ITEM: AR201661 "CLEAN & SEAL BITUMINOUS CRACKS" - PER LINEAR FOOT.

REMOVE & REPLACE BITUMINOUS PAVEMENT

 THE AREA SHOWN ON THE CONSTRUCTION SHEETS WILL HAVE THE EXISTING PAVEMENT SAWED, REMOVED, AND REPLACED (FULL DEPTH).

2. THE REPLACEMENT BITUMINOUS MATERIAL SHALL BE AN IDOT BITUMINOUS SURFACE COURSE MIX OF MINIMUM QUALITY "C" AND "N" DESIGN OF 50. THE MIX SHALL BE APPROVED BY THE IDOT-DIVISION OF AERONAUTICS (IDA) PRIOR TO INSTALLATION. AN AVERAGE OF TWO NUCLEAR DENSITY TESTS PER LIFT PER PATCH WILL BE REQUIRED FOR ACCEPTANCE TESTING. TO BE ACCEPTABLE THE AVERAGE OF THESE TWO TESTS MUST BE 93%. THE FINAL LIFT OF SURFACE COURSE SHALL BE INSTALLED FLUSH WITH THE EXISTING ADJACENT PAVEMENT. MAXIMUM LIFT THICKNESS IS THREE INCHES.

 EACH REMOVAL AND REPLACEMENT AREA WILL BE TESTED WITH A STRAIGHT EDGE TO DETERMINE IF THE AREA IS FLUSH WITH THE ADJACENT PAVEMENT. ALL IRREGULARITIES WILL BE CORRECTED BY THE CONTRACTOR AS AN INCIDENTAL ITEM TO THIS WORK.

4. THE BITUMINOUS SURFACE COURSE WILL BE INSTALLED AS PER THE SPECIFICATIONS EXCEPT AS STATED IN THESE PLANS. THE REMOVAL AND DISPOSAL OF THE EXISTING PAVEMENT; PROVIDING AND INSTALLING THESE MATERIALS TO GRADE IS CONSIDERED PART OF THIS PAY ITEM AND NO OTHER COMPENSATION WILL BE ALLOWED.

5. THE PAVEMENT CONDITIONS AND CRACKS WERE IDENTIFIED DURING A SURVEY IN THE SPRING 2016.

 THE REMOVAL AND REPLACEMENT OF BITUMINOUS PAVEMENT WILL BE PAID FOR UNDER ITEM: AR401910 "REMOVE & REPLACE BIT. PAVEMENT" - PER SQUARE YARD.

REFLECTIVE CRACK CONTROL TREATMENT NOTES

 PRIOR TO THE OVERLAY OF RUNWAY 9-27, REFLECTIVE CRACK CONTROL TREATMENT IS TO BE COMPLETED IN THREE AREAS ALONG RUNWAY 9-27 BETWEEN STA. 250+00 AND STA. 262+00. THESE AREAS WILL BE LOCATED BY THE RESIDENT ENGINEER.

2. THE CONTRACTOR SHALL MILL EACH AREA CENTERED ON THE CRACK TO A WIDTH OF 5 FEET AND A DEPTH OF 4 INCHES

5. THE MILLED AREA SHALL BE DRY, CLEAN AND DUST FREE. CRACK CONTROL REPAIR MATERIAL AND TACK COAT SHALL BE INSTALLED ACCORDING TO CRACK CONTROL REPAIR MATERIAL MANUFACTURER'S SPECIFICATIONS AND INSTRUCTIONS BETWEEN THE TWO LIFTS OF ASPHALT. PRIOR TO THE INSTALLATION OF THE FIRST LIFT OF BITUMINOUS SURFACE COURSE A HEAVY FOG (APPROXIMATELY 0.10 GAL/S.Y.) SHALL BE APPLIED TO THE EXISTING SURFACES.

4. THE REPLACEMENT BITUMINOUS MATERIAL SHALL BE AN IDOT BITUMINOUS SURFACE COURSE MIX OF MINIMUM QUALITY "C" AND "N" DESIGN OF 50. THE MIX SHALL BE APPROVED BY THE IDOT-DIVISION OF AERONAUTICS (IDA) PRIOR TO INSTALLATION. AN AVERAGE OF TWO NUCLEAR DENSITY TESTS PER LIFT PER PATCH AREA WILL BE REQUIRED FOR ACCEPTANCE TESTING. TO BE ACCEPTABLE THE AVERAGE OF THESE TWO TESTS MUST BE 93% OR HIGHER. THE SURFACE COURSE SHALL BE INSTALLED IN TWO LIFTS AND FLUSH WITH THE EXISTING ADJACENT PAVEMENT. ANY DEVIATIONS BETWEEN THE EXISTING PAVEMENT AND THE REPAIR SURFACE OF 1/8" OR MORE SHALL BE CORRECTED AT THE CONTRACTOR'S EXPENSE

 THE REFLECTIVE CRACK CONTROL TREATMENT WILL BE PAID FOR UNDER ITEM: AR800400 "REFLECTIVE CRACK CONTROL TREATMENT" - PER SQUARE YARD.

BUTT JOINT CONSTRUCTION NOTES

THE PAVEMENT AREAS DESIGNATED ON THESE DRAWINGS SHALL BE CUT OR MILLED TO ACCOMMODATE
THE PROPOSED POROUS FRICTION COURSE OVERLAY.

2. THE EXISTING PAVEMENT WILL BE SAWED AT THE LOCATIONS SHOWN ON THESE PLANS. THE SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO "BUTT JOINT CONSTRUCTION"

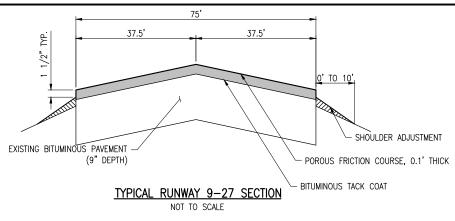
3. ALL MILLED MATERIAL WILL BE DISPOSED OF BY THE CONTRACTOR, OFF THE AIRPORT SITE, UNLESS OTHERWISE DIRECTED BY THE AIRPORT MANAGER AT THE TIME OF CONSTRUCTION.

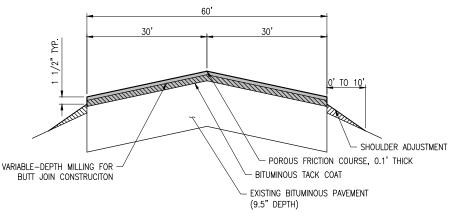
4. ANY ADJACENT PAVEMENT OR EQUIPMENT THAT IS DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER.

 PRIOR TO THE APPLICATION OF THE PFC OVERLAY, ALL MILLED AREAS WILL BE BROOMED AND BLOWN CLEAN OF LOOSE MATERIALS AND DEBRIS. A BITUMINOUS TACK COAT WILL BE APPLIED TO THE JOINT SURFACE AND VERTICAL EDGES IN ACCORDANCE WITH ITEM 603.

6. THE EXISTING PAVEMENT WILL BE MILLED TO A DEPTH OF 0.10' AT THE BUTT JOINT

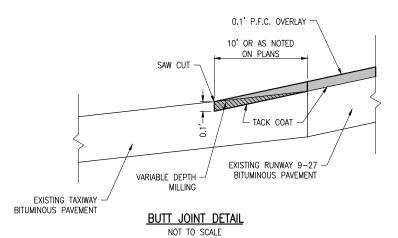
 PAYMENT FOR THIS ITEM OF WORK WILL BE UNDER ITEM: AR401655 "BUTT JOINT CONSTRUCTION" — PER SQUARE YARD

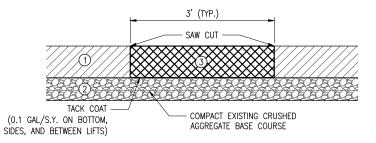




TYPICAL RUNWAY 1-19 SECTION

NOT TO SCALE





REMOVE & REPLACE BITUMINOUS PAVEMENT DETAIL

) EXISTING BIT. SURFACE COURSE (9.5" AVG.)

(2) EXISTING CRUSHED AGGREGATE BASE COURSE (7" AVG.)

PROPOSED BITUMINOUS SURFACE COURSE (9.5" DEPTH)
PLACE IN MINIMUM OF 3 LIFTS

BITUMINOUS TACK COAT NOTES

1. THE BITUMINOUS TACK COAT SHALL BE PLACED IN ACCORDANCE WITH ITEM 603.

THE TACK COAT SHALL BE PLACED ON THE EXISTING BITUMINOUS PAVEMENT AND ALLOWED TO CURE FOR 48 HOURS PRIOR TO THE PLACEMENT OF THE POROUS FRICTION COURSE.

 THE BITUMINOUS TACK COAT WILL BE PAID FOR UNDER ITEM: AR603510 "BITUMINOUS TACK COAT" – PER GALLON

POROUS FRICTION COURSE NOTES

. THE PROPOSED POROUS FRICTION COURSE WILL BE CONSTRUCTED IN ONE LAYER, HAVING A COMPACTED NOMINAL THICKNESS OF ONE TENTH OF A FOOT (0.10') IN ACCORDANCE WITH ITEM 402.

. THE POROUS FRICTION COURSE SHALL BE PLACED ON A CLEAN AND PREPARED SURFACE ONLY AFTER THE APPROVAL OF THE RESIDENT ENGINEER. THE SURFACES TO BE OVERLAYED WILL BE SPRAYED WITH AN APPLICATION OF A BITUMINOUS TACK COAT. AN APPLICATION RATE OF 0.25 GAL/S.Y. (DILUTED) WAS USED FOR CALCULATING THE QUANTITY OF TACK FOR THIS APPLICATION. THE EXACT RATE OF APPLICATION WILL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

. THE PROPOSED POROUS FRICTION COURSE WILL BE PAID FOR UNDER ITEM: AR402622 "POROUS FRICTION COURSE, 0.10' " - PER SQUARE YARD

SHOULDER ADJUSTMENT NOTES

I. APPROXIMATE SHOULDER ADJUSTMENT AREAS SHOWN ON SHEETS 7 AND 8 ARE BASED ON SPRING 2016 EVALUATION OF AREAS EXPECTED TO HAVE AN EDGE DROP GREATER THAN 1-1/2" AFTER OVERLAY. ACTUAL AREAS AND QUANTITIES OF SHOULDER ADJUSTMENT WILL BE DELINEATED BY THE RESIDENT ENGINEER AFTER PAVING.

THE GRADING WILL HAVE A MAXIMUM 1-1/2" DROP FROM THE PAVEMENT EDGE AND TAPERING TO THE EXISTING GROUND WITHIN THE DISTANCE SHOWN ON THIS SHEET. THE EARTH FILLETS WILL NOT REQUIRE COMPACTING, OTHER THAN LIGHT ROLLING AND SHAPING. THE TOPSOIL FOR THE PROPOSED EARTH FILLETS WILL BE OBTAINED FROM OFF-SITE.

 THE GRADING OF THE ENDS OF RUNWAY 9-27 WILL BE FLUSH WITH THE PAVEMENT ELEVATION AND TAPER IN THE DISTANCE SHOWN ON THIS SHEET.

4. THE EXISTING SHOULDER AREA WILL BE MOWED PRIOR TO PLACING THE TOPSOIL MATERIAL.

. THE SHOULDER ADJUSTMENT MATERIAL WILL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO INSTALLATION. THE MATERIAL WILL NOT BE REQUIRED TO BE TESTED FOR pH, ORGANIC CONTENT, NOR GRADATION PROVIDED THE TOPSOIL MATERIAL HAS SUFFICIENT AMENITIES TO SUPPORT A STAND OF GRASS.

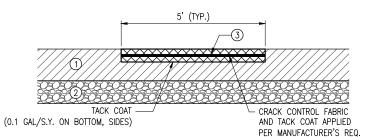
5. THE CONTRACTOR SHALL TAKE SPECIAL PRECAUTIONS WHEN HAULING BORROW MATERIAL SO AS NOT TO CREATE RUTS IN ADJACENT EARTH AREAS. ALL EXISTING GRADED OR TURFED AREAS OUTSIDE THE GRADING LIMITS WHICH ARE DISTURBED OR RUTTED BY THE CONTRACTOR DURING THE HAULING OPERATION SHALL BE REGRADED AND RETURFED AT HIS OWN EXPENSE TO THE SATISFACTION OF THE FAICHIEFED.

7. THE CONTRACTOR SHALL SEED ALL SHOULDER ADJUSTMENT AREAS WITH AN IDOT CLASS 2 ROADSIDE SEED MIXTURE AT A RATE OF 200 POUNDS PER ACRE. THE CONTRACTOR WILL BE REQUIRED TO ESTABLISH A GOOD STAND OF GRASS OF UNIFORM COLOR AND DENSITY TO THE SATISFACTION OF THE RESIDENT ENGINEER. IF AT THE TIME WHEN THE CONTRACT HAS BEEN OTHERWISE COMPLETED IT IS NOT POSSIBLE TO MAKE AN ADEQUATE DETERMINATION OF COLOR, DENSITY, AND UNIFORMITY OF SUCH STAND OF GRASS, THE ITEM OF WORK WILL BE REVIEWED AT A LATER DATE DETERMINED BY THE IDOT—DIVISION OF AERONAUTICS.

8. THE CONTRACTOR SHALL APPLY A COMMERCIALLY AVAILABLE 3-1-2 RATIO FERTILIZER MIXTURE AT A RATE OF 270 POUNDS PER ACRE TO ALL SEEDED AREAS.

THE CONTRACTOR SHALL WORK AROUND OR ADJUST ANY UNDERDRAIN STRUCTURES AS NEEDED AS AN INCIDENTAL COST TO THE SHOULDER ADJUSTMENT.

10. ALL MATERIAL FOR THE PROPOSED EARTH FILLETS, INCLUDING TOPSOIL, SEEDING AND FERTILIZER WILL BE PAID FOR UNDER ITEM: AR152480 "SHOULDER ADJUSTMENT" — PER SQUARE YARD.



REFLECTIVE CRACK CONTROL TREATMENT DETAIL

(1) EXISTING BIT. SURFACE COURSE (9" AVG.)

2) EXISTING CRUSHED AGGREGATE BASE COURSE (4" AVG.)

PROPOSED BITUMINOUS SURFACE COURSE (2-2" LIFTS)

HANSON Engineering | Planning | Allied Service:

Offices Nationwide

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KEWANEE MUNICIPAL AIRPORT

3761 MIDLAND RD. KEWANEE IL, 61443

REHABILITATE RUNWAY 9-27 AND REMARK

IDA No: EZI-4528 SBG No: 3-17-SBGP-TBD

Contract No. KE014

NO. DATE DESCRIPTION
DES DWN REV

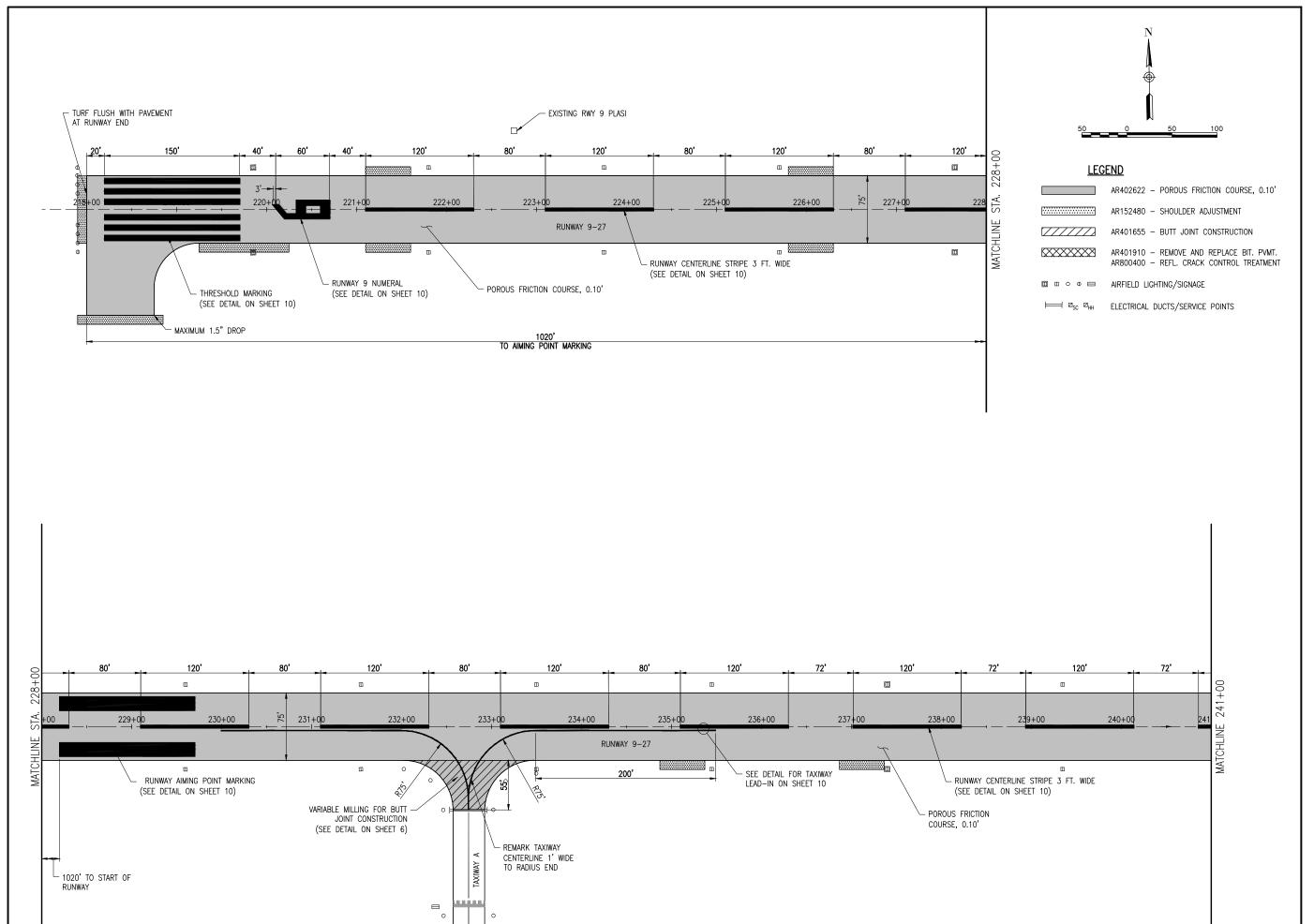
ISSUE: JUNE 3, 2016
PROJECT NO: 16A0020
CAD FILE: G-006-NOTE.DWG
DESIGN BY: SJZ 04/12/2016

PAVING DETAILS AND NOTES

DRAWN BY: RAD/MAW 04/26/2016

REVIEWED BY: KBS 06/03/2016

SHEET TITLE





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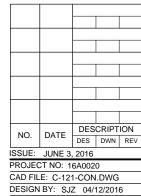
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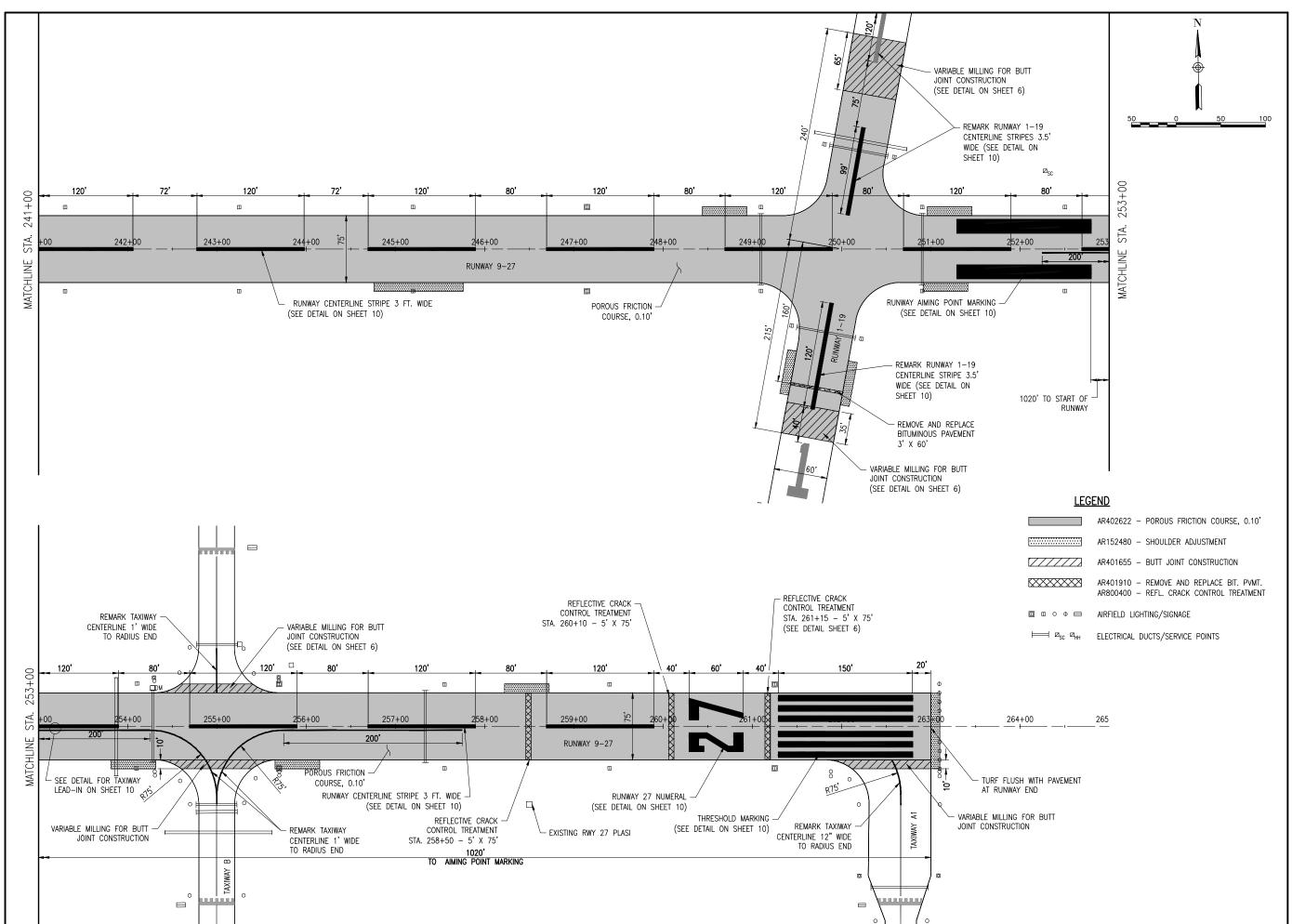
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SHEET TITLE

SITE PLAN - SHEET 1

DRAWN BY: RAD/MAW 04/19/2016 REVIEWED BY: KBS 06/03/2016





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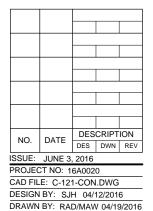
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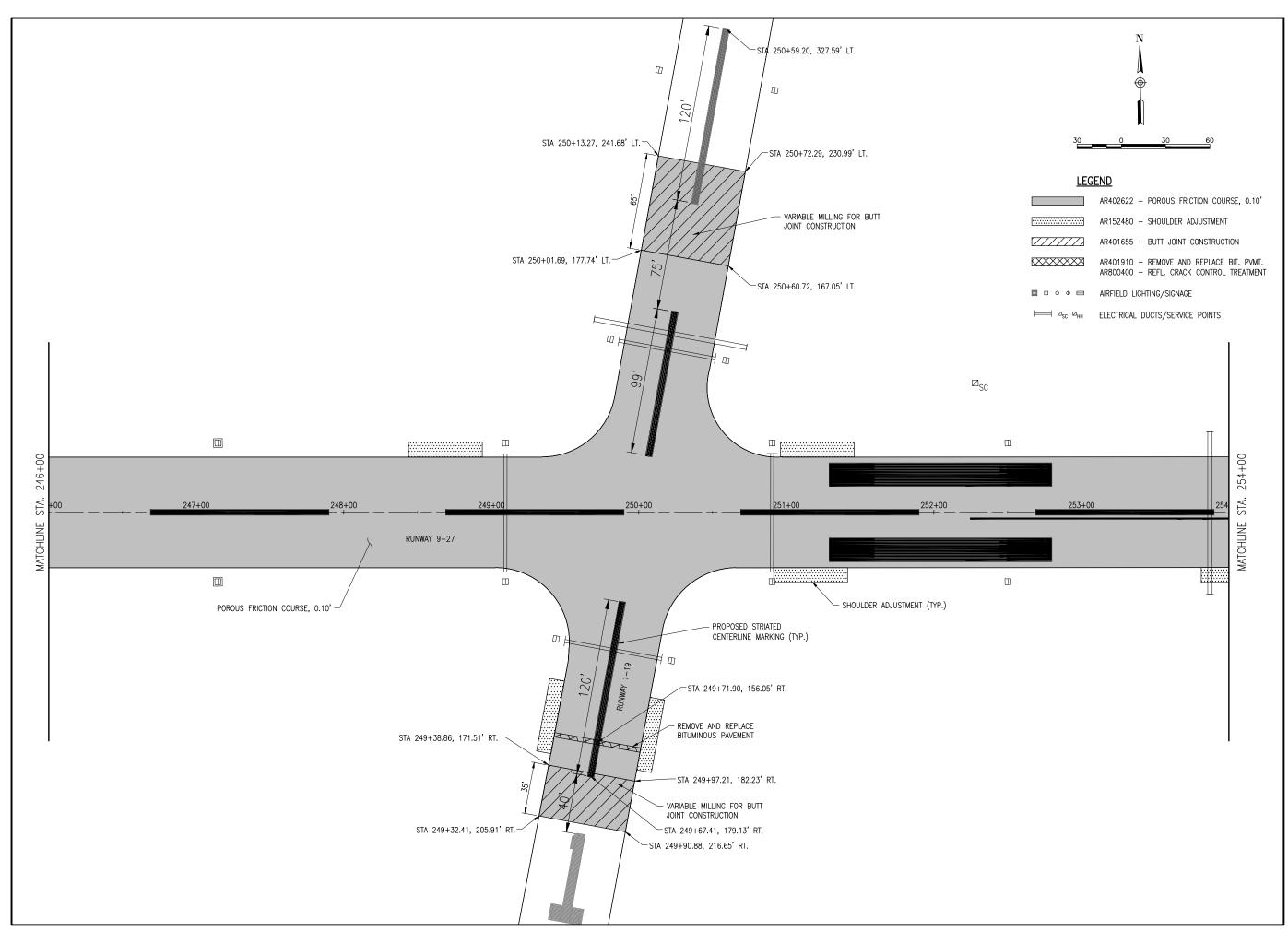
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SHEET TITLE

SITE PLAN - SHEET 2

REVIEWED BY: KBS 06/03/2016





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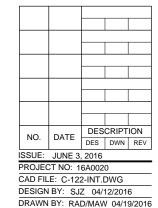
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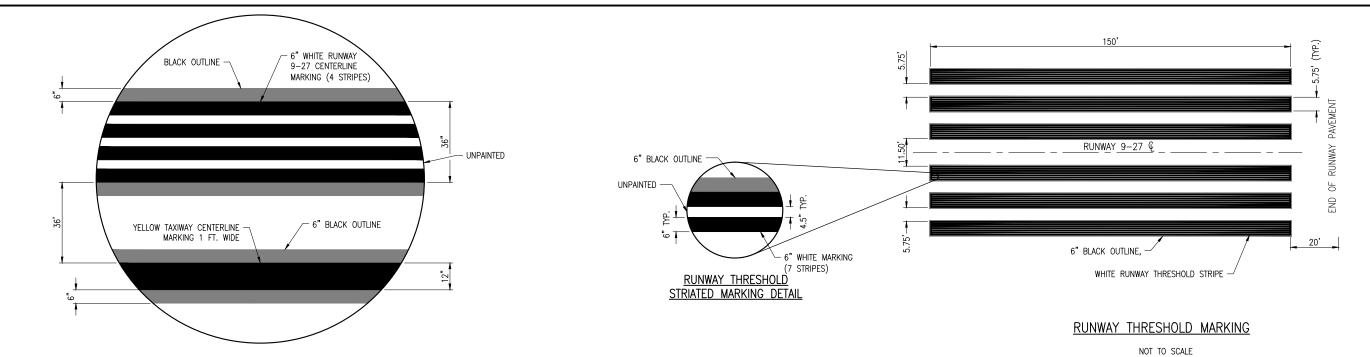
Contract No. KE014



SHEET TITLE

RUNWAY 9-27 & 1-19 INTERSECTION DETAIL

REVIEWED BY: KBS 06/03/2016



MARKING QUANTITIES							
MARKING DESCRIPTION	LENGTH (FT.)	WIDTH (FT.)	NUMBER	WHITE (S.F.)	YELLOW (S.F.)	BLACK (S.F.)	
AIMING POINTS	150	15	4	4832		664	
THRESHOLD BARS	150	5.75	12	6342		1890	
CENTERLINE MARKING RWY 9-27	120	3	20	4840		2480	í
CENTERLINE MARKING RWY 1-19	120(2) 99(1)	3.5	3	684		855	
RUNWAY 9 NUMERAL				407		114	
RUNWAY 2 NUMERAL				378		107	
RUNWAY 7 NUMERAL				280		85	
TAXIWAY CENTERLINE	781	1			781	781	
AR620520 - PAVEMENT MARKING-V	AR620520 - PAVEMENT MARKING-WATERBORNE 18544						
AR620525 - PAVEMENT MARKING-BLACK BORDER						6976	

CENTERLINE LEAD-IN DETAIL

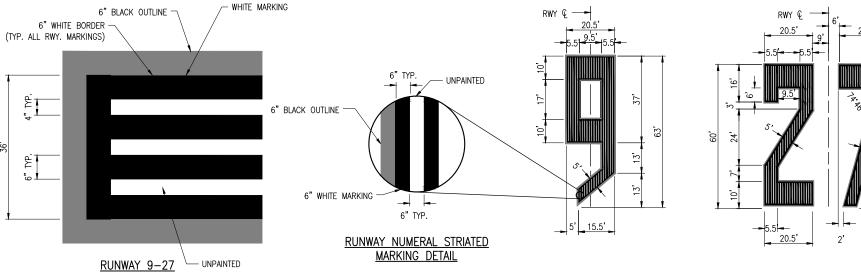
NOT TO SCALE

620-PAVEMENT MARKING-WATERBORNE NOTES

- 1. THE PAVEMENT MARKING-WATERBORNE (620) SHALL BE PLACED IN ACCORDANCE WITH ITEM 620 "PAVEMENT MARKING".
- ALL MARKINGS WILL BE WHITE, BLACK, OR YELLOW IN COLOR AS NOTED ON THE PLANS.
- 3. ALL WHITE RUNWAY MARKINGS SHALL BE STRIATED AND LAID OUT AS SHOWN PER THE PLAN DETAILS. ALL MARKINGS WILL HAVE A 6 INCH WHITE BORDER INSIDE THE NOMINAL DIMENSIONS OF THE FEATURE AND 6 INCH BLACK OUTLINE. WHITE INTERIOR STRIPES WILL BE 6" WIDE WITH UNPAINTED GAPS AT THE WIDTHS SHOWN ON THE DETAILS.
- THE MARKINGS SHALL BE APPLIED IN TWO APPLICATIONS. GLASS BEADS ARE REQUIRED ONLY ON THE SECOND APPLICATION OF YELLOW AND WHITE MARKINGS.
- CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.
- 6. REMARK TAXIWAY CENTERLINE MARKINGS TO RADIUS RETURN.
- ONLY THE BLACK MARKING THAT IS VISIBLE AT THE COMPLETION OF MARKING IS ELIGIBLE FOR PAYMENT. AREAS COVERED UP AS A RESULT OF "BLOCK PAINTING" PRIOR TO WHITE OR YELLOW MARKING WILL NOT BE MEASURED FOR PAYEMENT.
- 8. THE MARKINGS WILL BE PAID FOR UNDER ITEMS:

WHITE AND YELLOW AR620520 PAVEMENT MARKING-WATERBORNE ___ PER SQUARE FEET

AR620525 PAVEMENT MARKING-BLACK BORDER ___ PER SQUARE FEET



CENTERLINE DETAIL PROPOSED NUMERAL DETAIL NOT TO SCALE FOR RUNWAY END 9 NOT TO SCALE 6" BLACK OUTLINE ¬ 6" WHITE BORDER

<u>RUNWAY 1-19</u>

CENTERLINE DETAIL (MATCH EXISTING)

NOT TO SCALE

(TYP. ALL RWY. MARKINGS) 6" WHITE MARKING (4 STRIPES) 6" BLACK MARKING - 6" BLACK OUTLINE RUNWAY 9-27 € 6" BLACK OUTLINE UNPAINTED 6" WHITE MARKING (16 STRIPES) WHITE AIMING POINT **RUNWAY AIMING POINT**

STRIATED MARKING DETAIL

AIMING POINT MARKINGS

NOT TO SCALE

PROPOSED NUMERAL DETAIL

FOR RUNWAY END 27

NOT TO SCALE

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REHABILITATE RUNWAY 9-27 AND REMARK

IDA No: EZI-4528 SBG No: 3-17-SBGP-TBD

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ISSUE: JUNE 3, 2016					
PROJECT NO: 16A0020					
CAD FILE: C-551-DETL.DWG					

DESIGN BY: SJZ 04/12/2016 DRAWN BY: RAD 04/26/2016 REVIEWED BY: KBS 06/03/2016

SHEET TITLE

MARKING DETAILS AND NOTES

10