

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------|--------|--------------------|-----------|
| 341 | (109B)BR | COOK | 45 | 1 |
| ILLINOIS | | | CONTRACT NO. 60V27 | |

*45+1=46

D-91-534-12

FOR INDEX OF SHEETS, SEE SHEET NO. 2

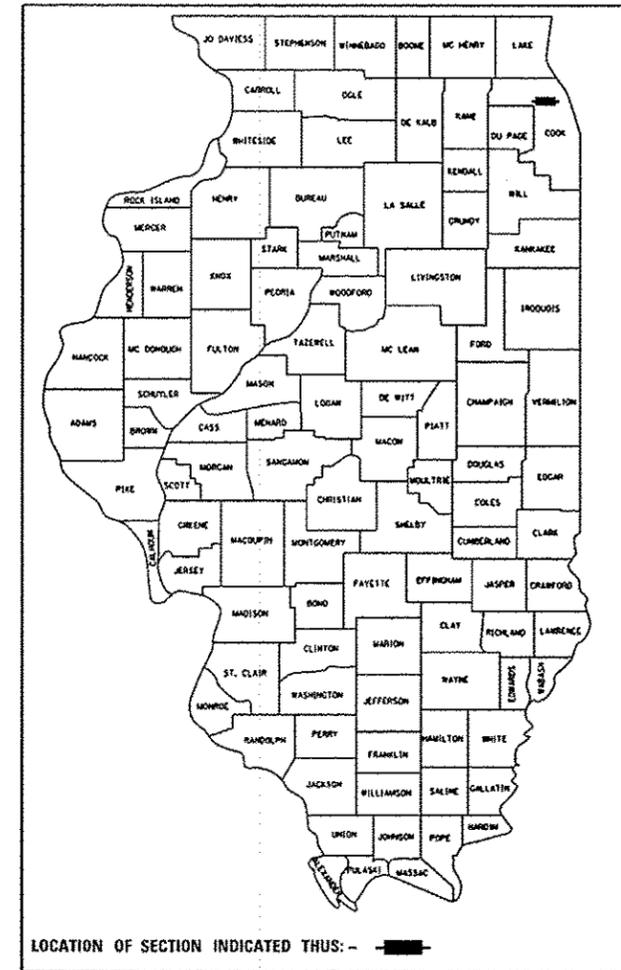
DESIGN DESIGNATION

OTHER PRINCIPAL ARTERIAL
ADT 30600 (2010)
SPEED LIMIT 40 MPH

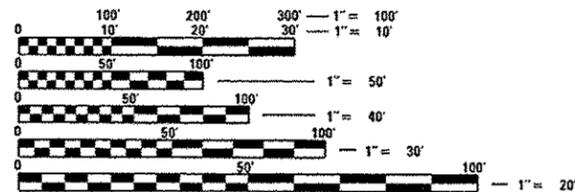
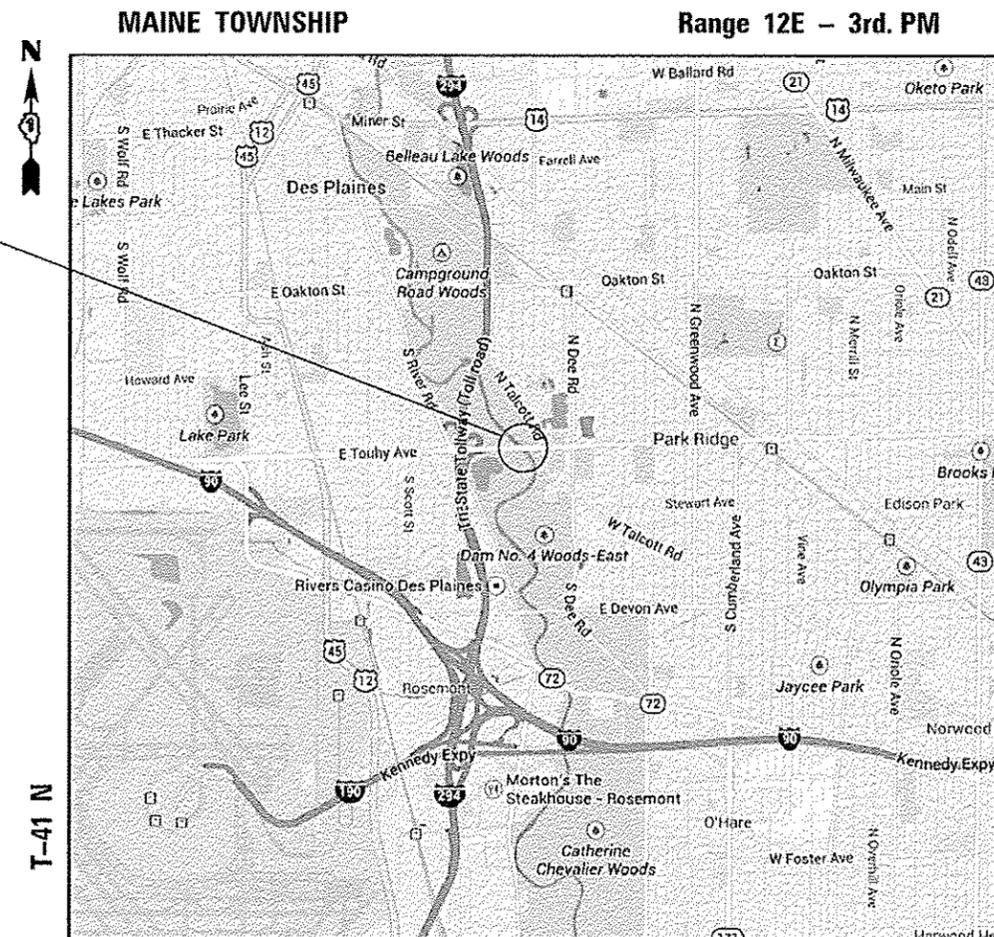
IMPROVEMENT LOCATED IN
THE CITY OF PARK RIDGE

**PROPOSED
HIGHWAY PLANS**
FAP ROUTE 341 (TOUHY AVENUE)
SECTION (109B)BR
OVER DES PLAINES RIVER
BRIDGE REPAIR
COOK COUNTY
PROJECT: ACNHPP-0341(061)

C-91-534-12



IMPROVEMENT LOCATION
TOUHY AVENUE OVER DES PLAINES RIVER
STRUCTURE NO: 016-0829



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT MANAGER: MR. ISSAM RAYYAN, P.E. (847) 705-4178
PROJECT ENGINEER: MS. RAGHAD ADEIS-DAHMAN, P.E. (847) 705-5183
CONTRACT NO. 60V27

GROSS LENGTH = 494 FT. = 0.094 MILE
NET LENGTH = 494 FT. = 0.094 MILE
LOCATION MAP NOT TO SCALE



5-5-2016
LIN ENGINEERING, LTD.
MICHAEL T. HALEY
NO. 081-005991
EXPIRES 11-30-2016
FOR DRAWINGS: 17-36



5/16/16
COLLINS ENGINEERS, INC.
RYAN CALL
NO. 062-064397
EXPIRES 11-30-2017
FOR DRAWINGS: 2-16, 37-45

COLLINS ENGINEERS, INC.
123 N. WACKER DR., SUITE 900
CHICAGO, IL 60606
(312) 704-9300
ILLINOIS PROFESSIONAL DESIGN FIRM
LICENSE NO. 184-000993

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED May 9 2016
John F. Downum Jr.
REGIONAL ENGINEER

July 1 2016
Muneem M. Addis PE
ENGINEER OF DESIGN AND ENVIRONMENT

July 1 2016
Aracelis Alcala
DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

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HIGHWAY STANDARDS

| STANDARD NO. | DESCRIPTION |
|--------------|--|
| 000001-00 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 420001-06 | PAVEMENT JOINTS |
| 515001-03 | NAME PLATE FOR BRIDGE |
| 606001-06 | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER |
| 643001-02 | SAND MODULE IMPACT ATTENUATORS |
| 701011-04 | OFF-RD OPERATIONS, MULTILANE, 15' (4,5 m) TO 24" (600 mm) FROM PAVEMENT EDGE |
| 701101-05 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS |
| 701301-04 | LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS >= 45 MPH |
| 701411-09 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS <= 40 MPH |
| 701427-04 | URBAN LANE CLOSURE MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN |
| 701601-09 | SIDEWALK, CORNER OR CROSSWALK CLOSURE |
| 701801-06 | TRAFFIC CONTROL DEVICES |
| 701901-05 | TEMPORARY CONCRETE BARRIER |
| 704001-08 | OFF-RD MOVING OPERATIONS, |
| 780001-05 | TYPICAL PAVEMENT MARKINGS, 2W, DAY ONLY |
| 781001-04 | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS |
| 782005 | GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS |

GENERAL NOTES

- 10 FT (3 M) TRANSITIONS SHALL BE USED TO MATCH PROPOSED ITEMS OF WORK TO EXISTING ITEMS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEM OF WORK SPECIFIED.
- THE CONTRACTOR MUST CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 AT LEAST 72 HOURS PRIOR TO THE INSTALLATION OF THE TEMPORARY CONTROL DEVICES.
- THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD ENGINEER, AT (773) 685-8386 AT LEAST TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- ALL PAVEMENT MARKINGS AND RAISED REFLECTORS AFFECTED BY THE BRIDGE REPAIRS SHALL BE REPLACED. NOMINAL QUANTITIES HAVE BEEN INCLUDED IN THE CONTRACT FOR THIS WORK.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- DO NOT SCALE THESE PLANS FOR CONSTRUCTION PURPOSES.
- PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED UPON THE UNIT PRICE BID FOR THE WORK.
- DURING CONSTRUCTION OPERATIONS, LOOSE MATERIAL DEPOSITS THAT OBSTRUCT THE FLOW OF WATER IN DRAINING THE AREA SHALL BE REMOVED BEFORE THE END OF EACH WORK DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES (NEW AND EXISTING) SHALL BE FREE FROM ALL DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE ITEM FOR CONCRETE REMOVAL.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES, SYMBOLS AND LETTERS (AND RAISED REFLECTIVE MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS AND RAISED REFLECTIVE MARKERS SHALL BE AS DIRECTED BY THE ENGINEER.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE & GAS UTILITIES.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF PARK RIDGE.
- THE PLANS DO NOT REPRESENT A COMPLETE DEPICTION OF ALL UTILITIES THAT MAY BE IMPACTED BY THE PROPOSED WORK. THE CONTRACTOR SHALL CONDUCT HIS OR HER OWN INVESTIGATION TO DETERMINE THE OWNERSHIP OF IMPACTED UTILITIES. THE CONTRACTOR SHALL COORDINATE WITH THE UTILITY OWNERS AND MAY BE REQUIRED TO PROVIDE TEMPORARY SUPPORT, ADJUST, RELOCATE OR REMOVE UTILITIES THAT ARE IMPACTED BY THE PROPOSED IMPROVEMENT. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE DEPARTMENT HAS DETERMINED THAT IN STREAM WORK IS NOT REQUIRED FOR THE WORK SPECIFIED IN THIS CONTRACT. THE DEPARTMENT HAS NOT OBTAINED A 404 PERMIT. IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING AN USACE 404 PERMIT, IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER USACE PERMITS.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR OBTAINING A PERMIT FROM THE ILLINOIS TOLLWAY TO INSTALL TRAFFIC CONTROL DEVICES ON THEIR PROPERTY. SEE SPECIAL PROVISIONS.
- PRIOR TO ADJUSTING MANHOLES, MWRD MANHOLES SHALL BE LOCATED, PROTECTED, AND/OR ADJUSTED TO GRADE, IF NECESSARY, ACCORDING TO MWRD STANDARD PROCEDURES. PRIOR AUTHORIZATION IS REQUIRED TO MAKE ANY STRUCTURAL MODIFICATIONS INCLUDING MANHOLE FRAME AND LID ADJUSTMENTS. AUTHORIZATION MAY BE OBTAINED BY CONTACTING MS. MANJU SHARMA, DIRECTOR OF MAINTENANCE AND OPERATIONS DEPARTMENT, AT (312) 751-5101.
- ALL RAISED REFLECTIVE PAVEMENT MARKERS (BRIDGE) SHALL BE LOW PROFILE.
- OVERHEAD WIRES ARE NOT INSULATED AND EXTRA CAUTION AND VIGILANCE SHALL BE ADHERED TO WHEN WORKING NEARBY. CONTRACTORS SHALL ALWAYS USE CAUTION WHILE OPERATING CRANES AND OR OTHER EQUIPMENT NEAR OVERHEAD ELECTRICAL FACILITIES. THE OCCUPATIONAL HEALTH AND SAFETY ORGANIZATION (OSHA) RULES REQUIRE THAT WORKERS AND EQUIPMENT SHALL NOT APPROACH WITHIN TEN (10) FEET AWAY OF OVERHEAD ELECTRICAL EQUIPMENT WITHOUT APPROPRIATE SUPPLEMENTAL PROTECTION. BE CERTAIN THAT ALL WORKERS ON THIS PROJECT HAVE BEEN FULLY TRAINED AND CONFORM TO OSHA RULES AND OTHER APPLICABLE GUIDELINES REGARDING WORKING SAFELY AROUND ELECTRICAL POWER LINES.

FILE NAME : I:\B274 - I\B274 - 07 Verlock\B274.B2 - Tonly Ave over Des Plaines River\CA00\CA00\Sheets\016827-std\spennst.dgn

| | | | | | | | | | | | |
|-----------------------------------|------------|-----------|---|---|-----------------|---------------------|-------------|-----------------|-----------------------------|--|--|
| USER NAME : rgall | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TOUGHY AVENUE OVER DES PLAINES RIVER GENERAL NOTES, INDEX OF SHEETS, AND HIGHWAY STANDARDS | F.A.P. RTE. 341 | SECTION 1109B/BR | COUNTY COOK | TOTAL SHEETS 45 | SHEET NO. 2 | | |
| PLDT SCALE : 100.0000' = 1" = 10' | DRAWN - | REVISED - | | | SCALE: | SHEET NO. OF SHEETS | STA. | TO STA. | ILLINOIS CONTRACT NO. 60V27 | | |
| PLDT DATE : 5/7/2016 | CHECKED - | REVISED - | | | | | | | | | |
| | DATE - | REVISED - | | | | | | | | | |

FILE NAME : I:\274 - IDOT P18 188 - B7 - Vertical\B274.B2 - Touhy Ave over Des Plaines River\0600\0600 Sheets\0188\B274-INT-500.dgn

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | URBAN CONSTRUCTION CODE | |
|---------------------|--|------------------|----------------|-------------------------|----------------|
| | | | | 80% FED 20% STATE | |
| | | | | ROADWAY 0004 | BRIDGE 0014 |
| | | | | | S.N. 016-0829 |
| 20200100 | EARTH EXCAVATION | CU YD | 6 | 6 | |
| 40201000 | AGGREGATE FOR TEMPORARY ACCESS | TON | 8 | 8 | |
| 40600275 | BITUMINOUS MATERIALS (PRIME COAT) | POUND | 358 | 158 | 200 |
| 40600290 | BITUMINOUS MATERIALS (TACK COAT) | POUND | 358 | 158 | 200 |
| 40600400 | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS | TON | 2 | 2 | |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 336 | 336 | |
| 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | TON | 67 | 30 | 37 |
| 42001300 | PROTECTIVE COAT | SQ YD | 75 | 75 | |
| 44000200 | DRIVEWAY PAVEMENT REMOVAL | SQ YD | 22 | 22 | |
| 44003510 | MEDIAN REMOVAL PARTIAL DEPTH | SQ FT | 297 | 297 | |
| 48101600 | AGGREGATE SHOULDERS, TYPE B 8" | SQ YD | 37 | 37 | |
| 50102400 | CONCRETE REMOVAL | CU YD | 81.4 | | 81.4 |
| 50157300 | PROTECTIVE SHIELD | SQ YD | 321 | | 321 |
| 50300255 | CONCRETE SUPERSTRUCTURE | CU YD | 76.4 | | 76.4 |
| 50300260 | BRIDGE DECK GROOVING | SQ YD | 1555 | | 1555 |
| 50800205 | REINFORCEMENT BARS, EPOXY COATED | POUND | 11040 | | 11040 |

14

* - DENOTES SPECIALTY ITEM

| | | | | | | | | | | | |
|----------------------------|------------|-----------|---|--|-----------|-----------|-------------|---------|--------------------|--------------|-----------|
| USER NAME : rgeall | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TOUHY AVENUE OVER DES PLAINES RIVER SUMMARY OF QUANTITIES | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| PLOT SCALE : 2,000' / 1" = | DRAWN - | REVISED - | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | COOK | 45 | 3 |
| PLOT DATE : 5/9/2016 | CHECKED - | REVISED - | | | | | | | CONTRACT NO. 60V27 | | |
| | DATE - | REVISED - | | | | | | | ILLINOIS | | |

FILE NAME : I:\B274 - I101 P18 168 - 07 Verticals\B274.B2 - Touhy Ave over Des Plaines River\GDD\COO Sheets\0168V27-ht-500.dgn

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | URBAN CONSTRUCTION CODE 80% FED 20% STATE | |
|---------------------|--|-----------------|----------------|--|-----------------------|
| | | | | ROADWAY | BRIDGE |
| | | | | 0004 | 0014 S.N. 016-0829 |
| 50800515 | BAR SPLICERS | EACH | 168 | | 168 |
| 50800530 | MECHANICAL SPLICERS | EACH | 176 | | 176 |
| 52000110 | PREFORMED JOINT STRIP SEAL | FOOT | 472 | | 472 |
| 52100010 | ELASTOMERIC BEARING ASSEMBLY, TYPE I | EACH | 50 | | 50 |
| 52100520 | ANCHOR BOLTS, 1" | EACH | 100 | | 100 |
| 59000200 | EPOXY CRACK INJECTION | FOOT | 62 | | 62 |
| 60300305 | FRAMES AND LIDS TO BE ADJUSTED | EACH | 3 | 3 | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 6 | 6 | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | |
| 70106800 | CHANGEABLE MESSAGE SIGN | CAL MO | 6 | 6 | |
| 70200100 | NIGHTTIME WORK ZONE LIGHTING | L SUM | 1 | | 1 |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 260 | 260 | |
| 70400100 | TEMPORARY CONCRETE BARRIER | FOOT | 1025 | 1025 | |
| 70400200 | RELOCATE TEMPORARY CONCRETE BARRIER | FOOT | 1025 | 1025 | |
| 70600250 | IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3 | EACH | 3 | 3 | |
| 70600251 | IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST | EACH | 1 | 1 | |

14

* - DENOTES SPECIALTY ITEM

| | | |
|--------------------------|------------|-----------|
| USER NAME : rjgell | DESIGNED - | REVISED - |
| | DRAWN - | REVISED - |
| PLOT SCALE = 2.0000 / in | CHECKED - | REVISED - |
| PLOT DATE = 5/9/2016 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOUHY AVENUE OVER DES PLAINES RIVER
SUMMARY OF QUANTITIES

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|-------------|----------|--------|--------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 341 | (109B)BR | COOK | 45 | 4 |
| | | | CONTRACT NO. 60V27 | |
| ILLINOIS | | | | |

FILE NAME : I:\8274 - IDOT P18 158 - BT Vertical\8274.02 - Touhy Ave over Des Plaines River\CA00\CA00 Sheets\8274-11-501.dgn

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION CODE | |
|------------|---|-------|----------------|----------------------|---------------------------------|
| | | | | 80% FED 20% STATE | |
| | | | | ROADWAY 0004 | BRIDGE 0014 S.N. 016-0829 |
| 70600350 | IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3 | EACH | 2 | 2 | |
| * 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 2858 | 2858 | |
| * 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 230 | 230 | |
| * 78008210 | POLYUREA PAVEMENT MARKING TYPE I - LINE 4" | FOOT | 1765 | 1765 | |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 30 | 30 | |
| * 78100105 | RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) | EACH | 40 | 40 | |
| * 78100200 | TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER | EACH | 126 | 126 | |
| * 78100300 | REPLACEMENT REFLECTOR | EACH | 78 | 78 | |
| * 78200530 | BARRIER WALL MARKERS, TYPE C | EACH | 41 | 41 | |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 23 | 23 | |
| X0326766 | CLEAN & RESEAL RELIEF JOINT | FOOT | 104 | 104 | |
| X0327980 | PAVEMENT MARKING REMOVAL-WATER BLASTING | SQ FT | 3645 | 3645 | |
| X5870015 | BRIDGE DECK CONCRETE SEALER | SQ FT | 1747 | | 1747 |
| X7010216 | TRAFFIC CONTROL AND PROTECTION. (SPECIAL) | L SUM | 1 | 1 | |

14

* - DENOTES SPECIALTY ITEM

| | | | | | | | | | | | |
|-----------------------------|------------|-----------|---|--|---------------------|--------------------|----------|--------|-----------------|--------------|--|
| USER NAME : cge11 | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TOUHY AVENUE OVER DES PLAINES RIVER SUMMARY OF QUANTITIES | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| PLOT SCALE : 2.0000' / 1" = | CHECKED - | REVISED - | | | | 341 | 11098/BR | COOK | 45 | 5 | |
| PLOT DATE : 5/9/2015 | DATE - | REVISED - | | SCALE: | SHEET NO. OF SHEETS | STA. TO STA. | ILLINOIS | | | | |
| | | | | | | CONTRACT NO. 60V27 | | | | | |

FILE NAME : I:\8274 - I001 P1B 68 - 07 Various\8274.02 - Touhy Ave over Des Plaines River\CA00\CA00 Sheets\0168927-sht-500.dgn

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | URBAN CONSTRUCTION CODE | |
|----------|--|-------|----------------|----------------------------|---------------------------------|
| | | | | 80% FED 20% STATE | |
| | | | | ROADWAY 0004 | BRIDGE 0014 S.N. 016-0829 |
| X7030030 | WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH | FOOT | 10058 | 10058 | |
| X7030040 | WET REFLECTIVE TEMPORARY TAPE TYPE III, 6 INCH | FOOT | 648 | 648 | |
| Z0001800 | APPROACH SLAB REPAIR (PARTIAL DEPTH) | SQ YD | 13 | | 13 |
| Z0001899 | JACK AND REMOVE EXISTING BEARINGS | EACH | 50 | | 50 |
| Z0001905 | STRUCTURAL STEEL REPAIR | POUND | 2570 | | 2570 |
| Z0004530 | HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 8" | SQ YD | 22 | 22 | |
| Z0004562 | COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT | FOOT | 267 | 121 | 146 |
| Z0006012 | BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/4 INCHES | SQ YD | 1492 | | 1492 |
| Z0012130 | BRIDGE DECK SCARIFICATION 3/4" | SQ YD | 1492 | | 1492 |
| Z0012754 | STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 IN) | SQ FT | 2798 | | 2798 |
| Z0012755 | STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES) | SQ FT | 45 | | 45 |
| Z0015550 | DEBRIS REMOVAL | CU YD | 22 | | 22 |
| Z0015802 | PLUG EXISTING DECK DRAINS | EACH | 14 | | 14 |
| Z0016001 | DECK SLAB REPAIR (FULL DEPTH, TYPE I) | SQ YD | 7 | | 7 |

19

* - DENOTES SPECIALTY ITEM

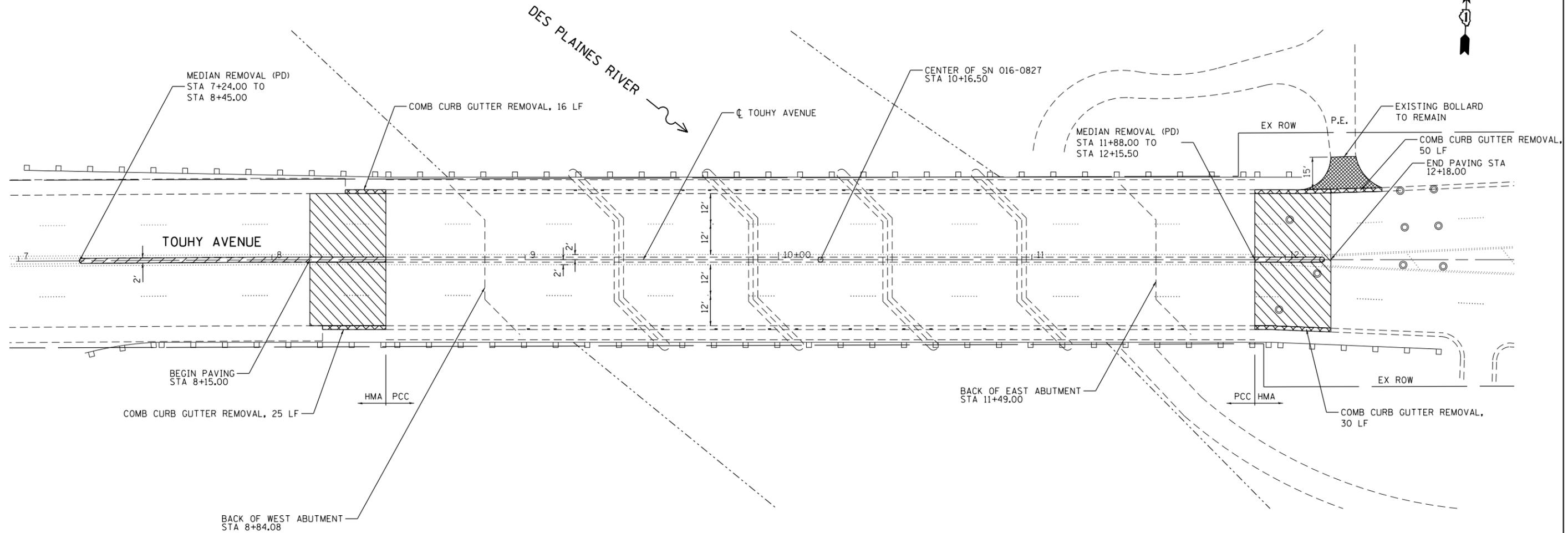
| | | |
|---------------------------|------------|-----------|
| USER NAME : rgojl | DESIGNED - | REVISED - |
| | DRAWN - | REVISED - |
| PLOT SCALE : 2.0000' / 1" | CHECKED - | REVISED - |
| PLOT DATE : 5/9/2016 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOUHY AVENUE OVER DES PLAINES RIVER
SUMMARY OF QUANTITIES

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|-------------|---------|--------|--------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 341 | 109B1BR | COOK | 45 | 6 |
| | | | CONTRACT NO. 60V27 | |
| ILLINOIS | | | | |



LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT
- COMBINATION CURB AND GUTTER REMOVAL
- MEDIAN REMOVAL PARTIAL DEPTH
- DRIVEWAY PAVEMENT REMOVAL

FILE NAME = I:\B274 - IDOT PTB 168 - 07 Verritus\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\168\27-ahtr-removal.dgn

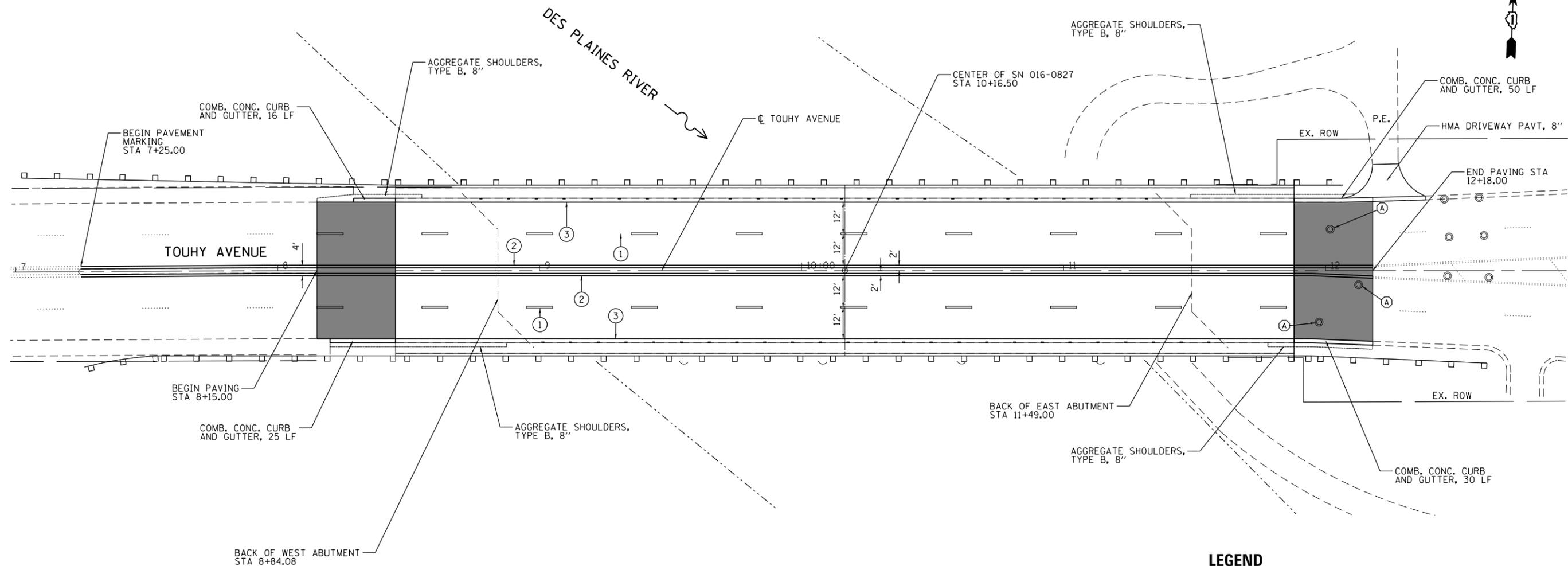
| | | |
|-----------------------------|------------|-----------|
| USER NAME = rge11 | DESIGNED - | REVISED - |
| | DRAWN - | REVISED - |
| PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| PLOT DATE = 5/7/2016 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TOUHY AVENUE OVER DES PLAINES RIVER
REMOVAL PLAN**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|-------------|----------|--------|--------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 341 | (109B)BR | COOK | 45 | 9 |
| ILLINOIS | | | CONTRACT NO. 60V27 | |



LEGEND

- (A) - FRAMES AND LIDS TO BE ADJUSTED
- - PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

PAVEMENT MARKING LEGEND

- ① 4" WHITE LANE LINE (10' DASH, 30' SKIP)
- ② 4" DOUBLE YELLOW LINE (11" C-C)
- ③ 4" WHITE EDGE LINE

NOTES:

1. SEE DISTRICT ONE DETAIL BD-32 FOR BUTT JOINT DETAIL.
2. POLYUREA PAVEMENT MARKING TO BE USED ON PCC PAVEMENT SURFACES.
3. THERMOPLASTIC PAVEMENT MARKINGS TO BE USED ON HMA SURFACES.
4. SEE DISTRICT ONE DETAIL TC-11 FOR RAISED REFLECTIVE PAVEMENT MARKER DETAILS.
5. SEE DISTRICT ONE DETAIL BD-24 FOR COMB. CONC. CURB AND GUTTER REPLACEMENT DETAIL.

FILE NAME = I:\B274 - IDOT PTB 168 - 07 Verritus\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\168\27-ht-plan.dgn

| | | |
|-----------------------------|------------|-----------|
| USER NAME = rge11 | DESIGNED - | REVISED - |
| | DRAWN - | REVISED - |
| PLOT SCALE = 48.0000' / in. | CHECKED - | REVISED - |
| PLOT DATE = 5/7/2016 | DATE - | REVISED - |

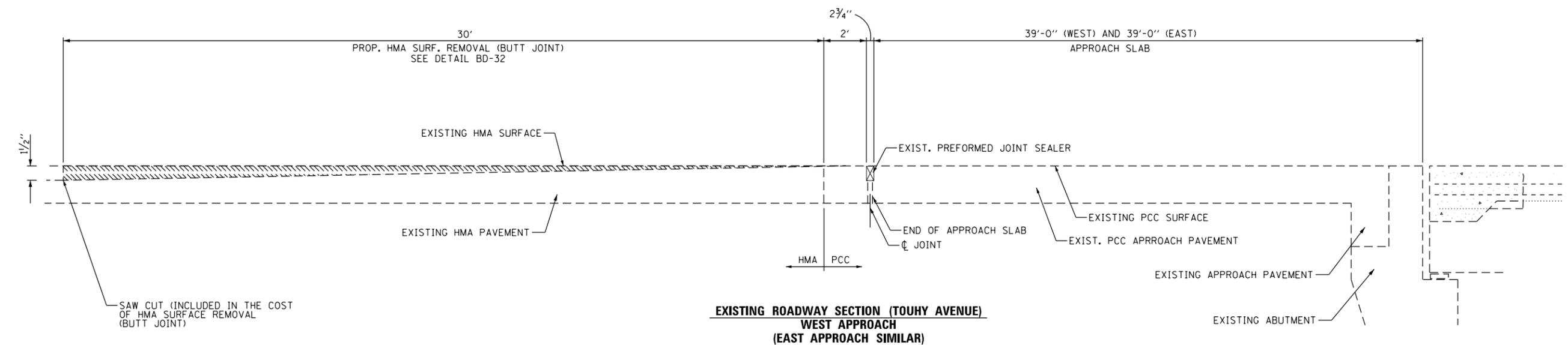
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TOUHY AVENUE OVER DES PLAINES RIVER
ROADWAY PLAN**

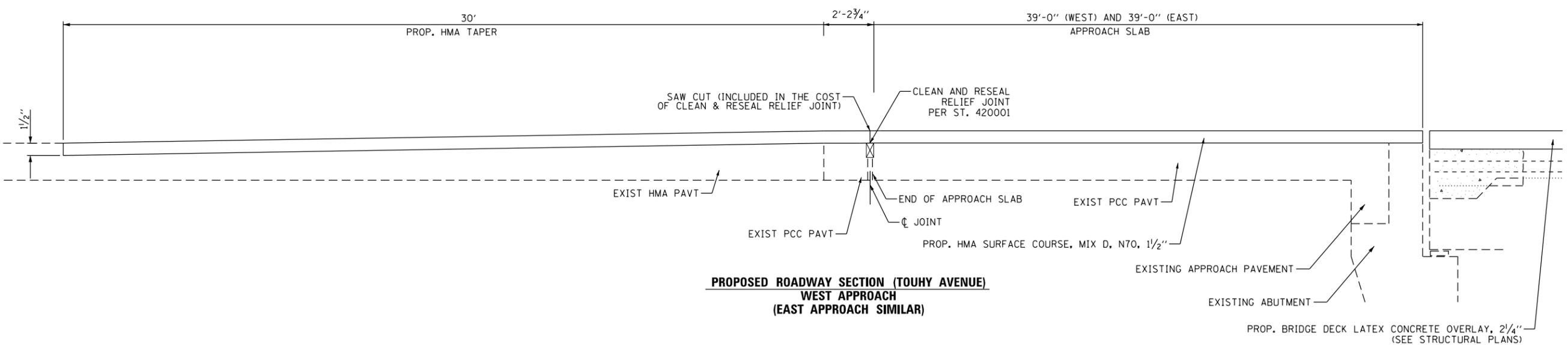
SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|--------------------|----------|--------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 341 | (109B)BR | COOK | 45 | 10 |
| CONTRACT NO. 60V27 | | | | |
| ILLINOIS | | | | |

FILE NAME = I:\B274 - IDOT PTB 168 - 07 Verrious\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\168\27-ht-detail1.dgn



**EXISTING ROADWAY SECTION (TOUHY AVENUE)
WEST APPROACH
(EAST APPROACH SIMILAR)**



**PROPOSED ROADWAY SECTION (TOUHY AVENUE)
WEST APPROACH
(EAST APPROACH SIMILAR)**

- NOTES:**
- SEE DETAIL BD-32 FOR BUTT JOINT AND HMA TAPER DETAILS.

| | | |
|------------------------------|---------------|-----------|
| USER NAME = r9e11 | DESIGNED - MR | REVISED - |
| | DRAWN - | REVISED - |
| PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| PLOT DATE = 5/7/2016 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | | | |
|--|-----------|-----------|------|---------|
| TOUHY AVENUE OVER DES PLAINES RIVER ROADWAY DETAILS | | | | |
| SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. |

| | | | | |
|-------------|----------|--------|--------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 341 | (109B)BR | COOK | 45 | 11 |
| ILLINOIS | | | CONTRACT NO. 60V27 | |

MAINTENANCE OF TRAFFIC - GENERAL NOTES

- SEE SPECIAL PROVISIONS TITLED TRAFFIC CONTROL AND PROTECTION (SPECIAL).
- THE CONTRACTOR SHALL REMOVE AND SAFELY STORE (FREE FROM THEFT OR DAMAGE) OR COVER ALL CONFLICTING EXISTING SIGNS FOR THE DURATION OF THE CONSTRUCTION. ALL SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AT THE END OF CONSTRUCTION.
- THE FOLLOWING APPLY TO CONSTRUCTION SIGNS:
 - THE CONTRACTOR SHALL FURNISH ALL SIGNS.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR AND REPLACE ANY SIGNS THAT ARE SUPPLIED BY OTHERS AND DAMAGED BY THE CONTRACTOR'S WORK FORCE OR SUBCONTRACTORS DURING RELOCATION OR CONSTRUCTION OPERATIONS.
 - ALL SIGNS AND ASSEMBLIES SHALL BE CERTIFIED BY THE CONTRACTOR AS MEETING THE APPLICABLE REQUIREMENTS OF NCHRP REPORT 350, TEST LEVEL 3.
 - ALL SIGNS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE TRAFFIC CONTROL AND PROTECTION (SPECIAL) PAY ITEM, EXCEPT FOR TEMPORARY INFORMATIONAL SIGNING AS NOTED ON THE PLANS.
- OPENINGS THROUGH THE BARRIER FOR CONTRACTOR'S ACCESS TO THE WORK ZONE SHALL BE PROVIDED AS APPROVED BY THE ENGINEER.
- ANY RAISED REFLECTIVE PAVEMENT MARKERS THAT CONFLICT WITH THE TEMPORARY TRAFFIC LANES MUST HAVE THE REFLECTIVE LENSES REMOVED AS DIRECTED BY THE ENGINEER.
- ALL TEMPORARY PAVEMENT MARKINGS ALONG TOUHY AVENUE DURING STAGED CONSTRUCTION SHALL BE WET REFLECTIVE TAPE, TYPE III OF THE WIDTH AND COLOR SPECIFIED ON THE PLAN SHEETS.
- THE CONTRACTOR SHALL MAINTAIN DRAINAGE AND EROSION CONTROL DURING CONSTRUCTION FOR THE DURATION OF THE CONTRACT.
- THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ACCESS TO ALL COMMERCIAL AND RESIDENTIAL ENTRANCES FOR THE ENTIRE DURATION OF THE PROJECT.
- SIDE ROAD, INTERSECTIONS, AND DRIVEWAY TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE TYPICAL ENTRANCE SIGNING DETAIL, DISTRICT DETAILS TC-10 AND TC-26, AND AS SHOWN ON THE PLANS.
- BI-DIRECTIONAL PRISMATIC BARRIER REFLECTORS WILL BE PLACED AT 25' CENTERS ON TOP AND SIDE OF TEMPORARY CONCRETE BARRIER FACING TRAFFIC.
- THE TEMPORARY CONCRETE BARRIER SHALL BE SECURED TO THE PAVEMENT USING THREE (3) ANCHORING PINS ON THE TRAFFIC SIDE OF THE BARRIER AT LOCATIONS WHERE A HAZARD OR SLOPES STEEPER THAN 1V:10H EXISTS WITHIN 3 FT OF THE BARRIER. THE COST OF THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "TEMPORARY CONCRETE BARRIER" OR "RELOCATE TEMPORARY CONCRETE BARRIER" AS APPLICABLE.
- PLACE MAX WITH SIGN (8'-6") FOR STAGE I AND (9'-0") FOR STAGE II AT NORTHWEST CORNER OF TOUHY AVENUE AND IL ROUTE 43 LOCATED 3 MILES EAST OF SN 016-0829 AND SOUTHEAST CORNER OF TOUHY AND US 45 LOCATED 1/4 MILES WEST OF SN 016-0829.

TOUHY AVENUE, STAGING NOTES: STAGE I

WORK IN THIS STAGE CONSISTS OF BRIDGE REPAIRS ALONG THE INNER-MOST EASTBOUND AND WESTBOUND LANES OF TRAFFIC ON TOUHY AVENUE, AND MILLING THE EXISTING PAVEMENT LEADING UP TO THE EAST AND WEST APPROACH SLABS.

MAINTAIN TRAFFIC IN ACCORDANCE WITH STD. 701601 AND DISTRICT ONE DETAIL TC-14.

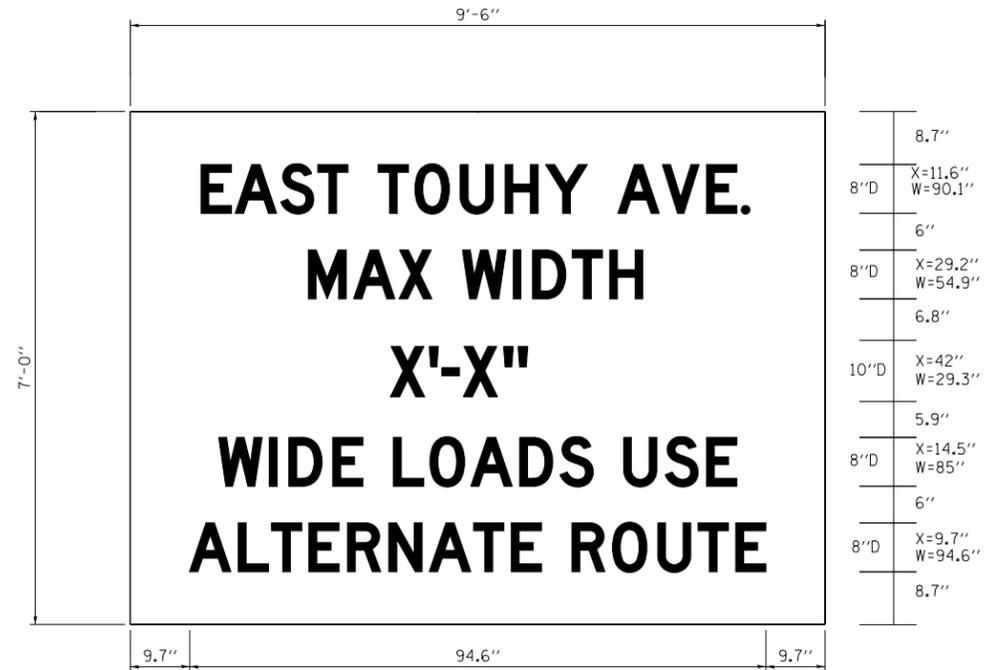
INSTALL TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC-11.

TOUHY AVENUE, STAGING NOTES: STAGE II

WORK IN THIS STAGE CONSISTS OF BRIDGE REPAIRS ALONG THE OUTER-MOST EASTBOUND AND WESTBOUND LANES OF TRAFFIC ON TOUHY AVENUE, AND MILLING THE EXISTING PAVEMENT LEADING UP TO THE EAST AND WEST APPROACH SLABS.

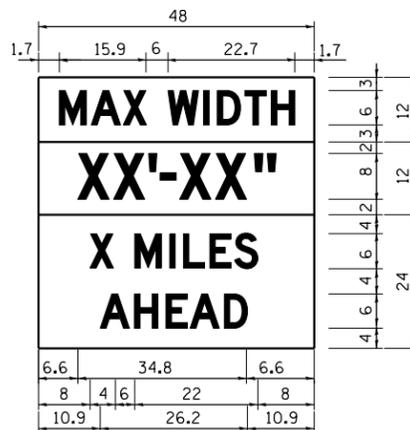
MAINTAIN TRAFFIC IN ACCORDANCE WITH STD. 701411 AND DISTRICT ONE DETAIL TC-14.

INSTALL TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC-11.



DURING STAGE I PLACE MAX WIDTH SIGN (8'-6") AND DURING STAGE II PLACE (9'-0") MAX WIDTH SIGN ON RIGHT SHOULDER 1000 FT NORTH OF I-294 TOLL BOOTH.

NO BORDER
BLACK ON ORANGE



W12-1103 (WIDTH IS 8D)
NO BORDER, BLACK ON WHITE
[MAX WIDTH] D:

NO BORDER, BLACK ON ORANGE
[XX'-XX"] D:

NO BORDER, BLACK ON WHITE
[X MILES] D; [AHEAD] D:

ALL SIGN DIMENSIONS IN INCHES

FILE NAME = I:\B274 - IDOT PTB 168 - 07 Various\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\0160\27-ht-staging.dgn

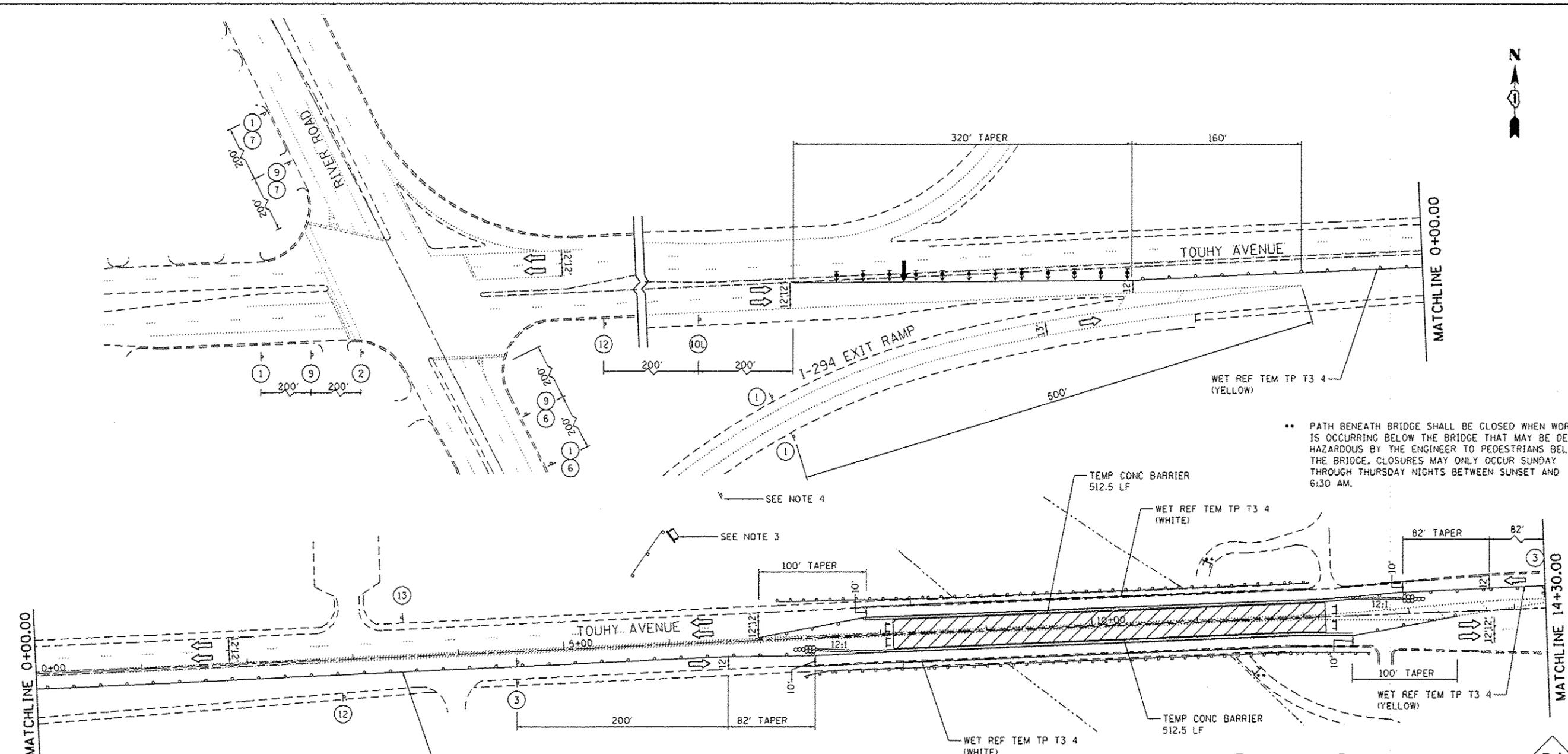
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|-----------------------------|------------|-----------|
| USER NAME = r9e11 | DESIGNED - | REVISED - |
| | DRAWN - | REVISED - |
| PLOT SCALE = 100.0000' / 1" | CHECKED - | REVISED - |
| PLOT DATE = 5/7/2016 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TOUHY AVENUE OVER DES PLAINES RIVER
STAGING NOTES**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|-------------|----------|--------|--------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 341 | (109B)BR | COOK | 45 | 12 |
| ILLINOIS | | | CONTRACT NO. 60V27 | |



•• PATH BENEATH BRIDGE SHALL BE CLOSED WHEN WORK IS OCCURRING BELOW THE BRIDGE THAT MAY BE DEEMED HAZARDOUS BY THE ENGINEER TO PEDESTRIANS BELOW THE BRIDGE. CLOSURES MAY ONLY OCCUR SUNDAY THROUGH THURSDAY NIGHTS BETWEEN SUNSET AND 6:30 AM.

LEGEND:

- WORK AREA
- DRUMS OR TYPE II BARRICADES WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C OR AS NOTED BELOW) DEVICES IN TAPER (20' C-C) DEVICES IN RADII (10' C-C)
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 25' C-C)
- TYPE III BARRICADE W/ 2 FLASHING BEACONS (NO. OF BARRICADES AS DIRECTED BY THE ENGINEER)
- PORTABLE CHANGEABLE MESSAGE SIGN

| | | | | | |
|-----------------|----------------|----------------|----------------|---------------|------------------|
| W2-1115(O)-3618 | R2-1-3648 | R10-19aP-3618 | R2-1106-3618 | W2-1103(O)-48 | G20-1103(O)-3660 |
| W4-2L(O)-48 | W12-1102(O)-48 | W20-1103(O)-48 | W20-1103(O)-48 | W21-5L(O)-48 | W21-1a(O)-48 |
| W6-3(O)-48 | W12-1102(O)-48 | M6-11(O)-2115 | M6-11(O)-2115 | M6-11(O)-2115 | M6-11(O)-2115 |
| W4-2L(O)-48 | W12-1102(O)-48 | M6-11(O)-2115 | M6-11(O)-2115 | M6-11(O)-2115 | M6-11(O)-2115 |
| W20-1103(O)-48 | W21-5L(O)-48 | W21-1a(O)-48 | W21-1a(O)-48 | W21-1a(O)-48 | W21-1a(O)-48 |

• MUST BE REMOVED WHEN FLAGGER OR WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR

NOTES:

1. FOR ADDITIONAL TRAFFIC CONTROL AND SIGN DETAILS SEE HIGHWAY STANDARDS 701601 AND DISTRICT ONE DETAIL TC14.
2. TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS WILL BE INSTALLED ALONG ALL TEMPORARY PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC11.
3. PLACE CMS ON RIGHT SHOULDER 500' NORTH OF I-294 TOLL BOOTH. CMS MESSAGE I: "ROAD WORK AHEAD" CMS MESSAGE II: "WATCH FOR BACKUPS"
4. PLACE WIDE LOAD ADVISORY SIGN ON I-294 SHOULDER, 1000' NORTH OF TOLL BOOTH SEE STAGING NOTES FOR SIGN DETAILS.
5. THE FURNISHING, INSTALLATION, MAINTENANCE, RELOCATION, AND REMOVAL OF WORK ZONE TRAFFIC CONTROL AND PROTECTION ON THE I-294 EXIT RAMP SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL).

| | | |
|---------------------------|------------|-----------|
| USER NAME = tshahs | DESIGNED - | REVISED - |
| PL0T SCALE = 1/8" = 1'-0" | DRAWN - | REVISED - |
| PL0T DATE = 6/17/2016 | CHECKED - | REVISED - |
| | DATE - | REVISED - |

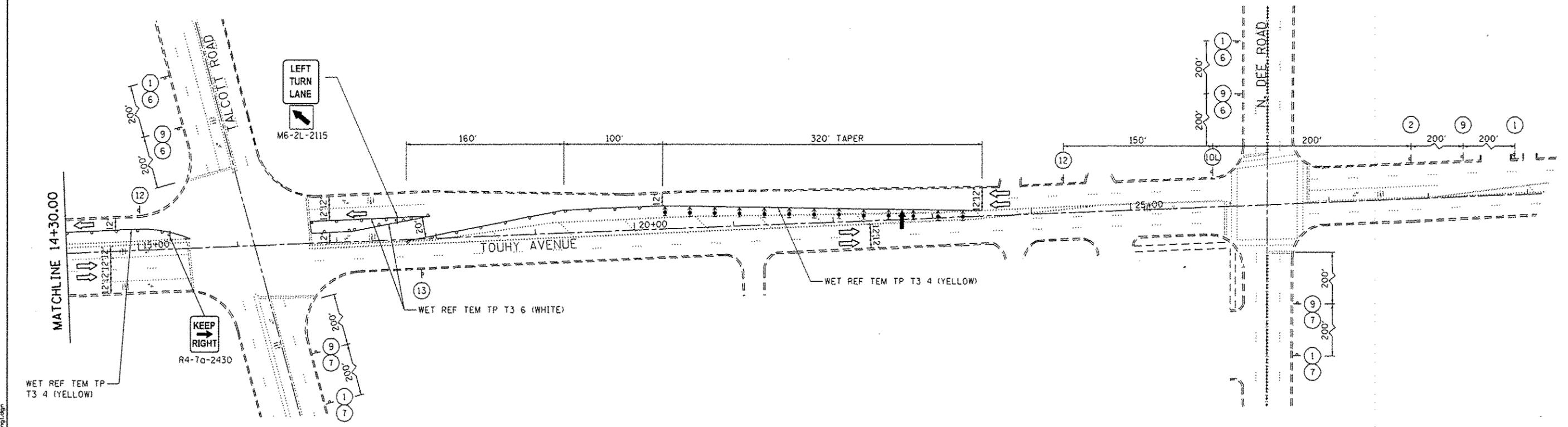
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TOUHY AVENUE OVER DES PLAINES RIVER
MAINTENANCE OF TRAFFIC - STAGE I**

| | | | | |
|--------|-----------|-----------|------|---------|
| SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. |
|--------|-----------|-----------|------|---------|

| | | | | |
|-------------|----------|--------|--------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 341 | 1109B/8R | COOK | 45 | 13 |
| ILLINOIS | | | CONTRACT NO. 60V27 | |

FILE NAME = I:\0274 - 1001 P18 168 - 87 Version\0274_82 - Touhy Ave over Des Plaines River\CADD\CADD Sheet\0168V27-struct.dwg



FILE NAME: E:\6274 - [DOT] P1B 169 - 07 Varian\6274\62 - Touhy Ave over Des Plaines River\000\CAD Sheets\0168\27 - hts\stage1.dgn

- NOTES:**
- FOR ADDITIONAL TRAFFIC CONTROL AND SIGN DETAILS SEE HIGHWAY STANDARDS 701601 AND DISTRICT ONE DETAIL TC14.
 - TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS WILL BE INSTALLED ALONG ALL TEMPORARY PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC11.

LEGEND:

- WORK AREA
- DRUMS OR TYPE II BARRICADES WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C OR AS NOTED BELOW) DEVICES IN TAPER (20' C-C) DEVICES IN RADII (10' C-C)
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 25' C-C)
- TYPE III BARRICADE W/ 2 FLASHING BEACONS (NO. OF BARRICADES AS DIRECTED BY THE ENGINEER)
- PORTABLE CHANGEABLE MESSAGE SIGN

| | | | | | | | | | |
|------------------|-------------|----------------|--------------|----------------|----------------|--------------|------------|--------------|--------------|
| W2-1115(O)-3618 | R2-1-3648 | R10-19aP-3618 | R2-1106-3618 | W20-1103(O)-48 | W20-1103(O)-48 | W21-5L(O)-48 | W6-3(O)-48 | M6-1(O)-2115 | M6-1(O)-2115 |
| W20-1103(O)-48 | W4-2L(O)-48 | W12-1102(O)-48 | W20-7(O)-48 | W20-1103(O)-48 | W21-5L(O)-48 | W21-1a(O)-48 | W6-3(O)-48 | M6-1(O)-2115 | M6-1(O)-2115 |
| G20-1103(O)-3660 | W4-2L(O)-48 | W12-1102(O)-48 | W20-7(O)-48 | W20-1103(O)-48 | W21-5L(O)-48 | W21-1a(O)-48 | W6-3(O)-48 | M6-1(O)-2115 | M6-1(O)-2115 |

* MUST BE REMOVED WHEN FLAGGER OR WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR

| | | |
|--------------------------------|------------|-----------|
| USER NAME = tshah | DESIGNED - | REVISED - |
| PLLOT SCALE = 100.0000' / 1" = | DRAWN - | REVISED - |
| PLLOT DATE = 6/17/2016 | CHECKED - | REVISED - |
| | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

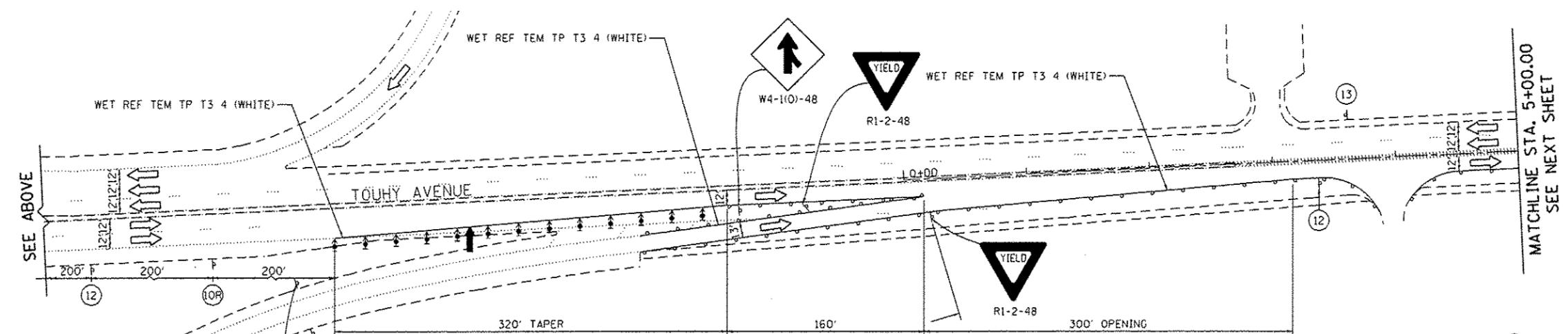
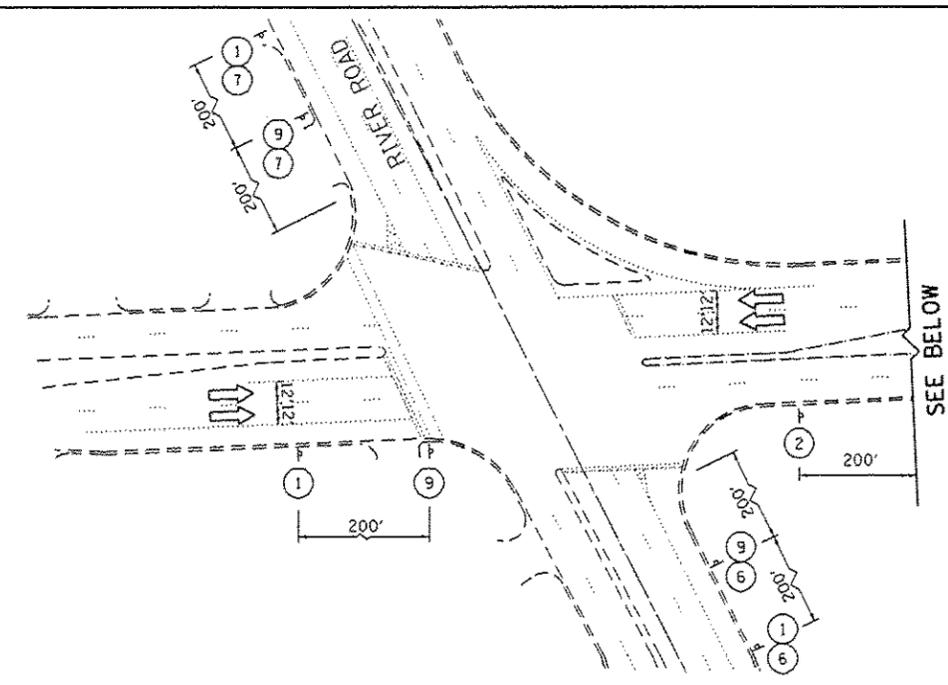
**TOUHY AVENUE OVER DES PLAINES RIVER
MAINTENANCE OF TRAFFIC - STAGE I**

| | | | | |
|---|-----------------|-------------|--------------------|--------------|
| F.A.P. RTE. 341 | SECTION 109B/BR | COUNTY COOK | TOTAL SHEETS 45 | SHEET NO. 14 |
| SCALE: SHEET NO. OF SHEETS STA. TO STA. | | | CONTRACT NO. 60V27 | |



NOTES:

- FOR ADDITIONAL TRAFFIC CONTROL AND SIGN DETAILS SEE HIGHWAY STANDARDS TO1601 AND DISTRICT ONE DETAIL TC14.
- TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS WILL BE INSTALLED ALONG ALL TEMPORARY PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC11.
- PLACE CMS ON RIGHT SHOULDER 500' NORTH OF I-294 TOLL BOOTH. CMS MESSAGE I: "ROAD WORK AHEAD" CMS MESSAGE II: "WATCH FOR BACKUPS"
- PLACE WIDE LOAD ADVISORY SIGN ON I-294 SHOULDER, 1000' NORTH OF TOLL BOOTH SEE STAGING NOTES FOR SIGN DETAILS.
- THE FURNISHING, INSTALLATION, MAINTENANCE, RELOCATION, AND REMOVAL OF WORK ZONE TRAFFIC CONTROL AND PROTECTION ON THE I-294 EXIT RAMP SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL).



LEGEND:

- WORK AREA
- DRUMS OR TYPE II BARRICADES WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C OR AS NOTED BELOW) DEVICES IN TAPER (20' C-C) DEVICES IN RADIUS (10' C-C)
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
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- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 25' C-C)
- TYPE III BARRICADE W/ 2 FLASHING BEACONS (NO. OF BARRICADES AS DIRECTED BY THE ENGINEER)
- PORTABLE CHANGEABLE MESSAGE SIGN

| | | | | | | | |
|------------------|----------------|---------------|--------------|--------------|----------------|--------------|--------------|
| W2-1115(0)-3618 | R2-1-3648 | R10-19aP-3618 | R2-1106-3618 | W4-2R(0)-48 | W12-1102(0)-48 | W20-7(0)-48 | W6-3(0)-48 |
| W20-1103(0)-48 | W20-1103(0)-48 | W21-5R(0)-48 | W21-1a(0)-48 | W21-1a(0)-48 | W21-1a(0)-48 | W21-1a(0)-48 | M6-1(0)-2115 |
| G20-1103(0)-3660 | W20-1103(0)-48 | W21-5R(0)-48 | W21-1a(0)-48 | W21-1a(0)-48 | W21-1a(0)-48 | W21-1a(0)-48 | M6-1(0)-2115 |

* MUST BE REMOVED WHEN FLAGGER OR WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR

FILE NAME: I:\8274 - I001 PTB 168 - B7 Variation\8274.B2 - Touhy Ave over Des Plaines River\CADD Sheets\01818V27.plt:stage.dgn

SEE NOTE 4
SEE NOTE 3

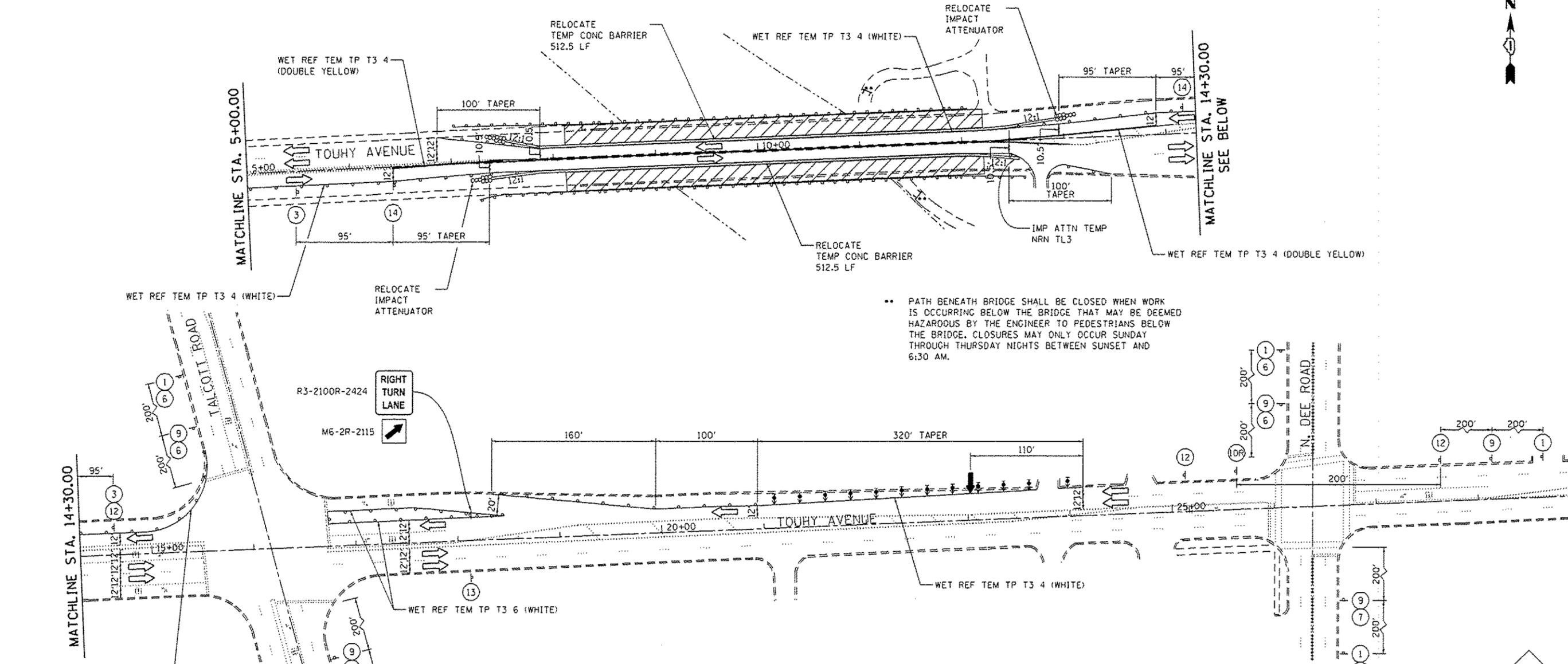
| | | |
|-----------------------------------|-------------|------------|
| USER NAME: tnahs | DESIGNED: - | REVISED: - |
| PLOT SCALE: 100.0000' / 1" = 100' | DRAWN: - | REVISED: - |
| PLOT DATE: 6/17/2016 | CHECKED: - | REVISED: - |
| | DATE: - | REVISED: - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

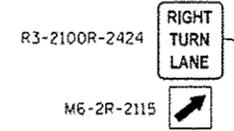
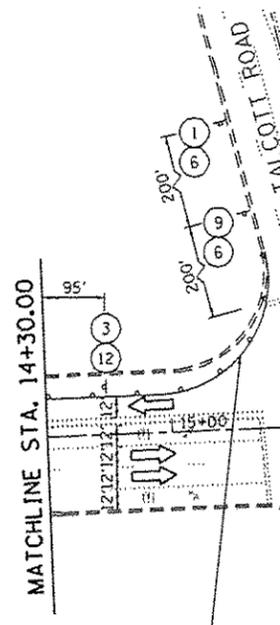
**TOUHY AVENUE OVER DES PLAINES RIVER
MAINTENANCE OF TRAFFIC - STAGE II**

| | | | | |
|--------|-----------|-----------|------|---------|
| SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. |
|--------|-----------|-----------|------|---------|

| | | | | |
|-----------------|-----------------|-------------|--------------------|--------------|
| F.A.P. RTE. 341 | SECTION 1109B/R | COUNTY COOK | TOTAL SHEETS 45 | SHEET NO. 15 |
| ILLINOIS | | | CONTRACT NO. 60V27 | |



.. PATH BENEATH BRIDGE SHALL BE CLOSED WHEN WORK IS OCCURRING BELOW THE BRIDGE THAT MAY BE DEEMED HAZARDOUS BY THE ENGINEER TO PEDESTRIANS BELOW THE BRIDGE. CLOSURES MAY ONLY OCCUR SUNDAY THROUGH THURSDAY NIGHTS BETWEEN SUNSET AND 6:30 AM.



LEGEND:

- WORK AREA
- DRUMS OR TYPE II BARRICADES WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C OR AS NOTED BELOW) DEVICES IN TAPER (20' C-C) DEVICES IN RADIUS (10' C-C)
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 25' C-C)
- TYPE III BARRICADE W/ 2 FLASHING BEACONS (NO. OF BARRICADES AS DIRECTED BY THE ENGINEER)
- PORTABLE CHANGEABLE MESSAGE SIGN

| | | | | | | | | | | | | | |
|-----------------|-----------|---------------|--------------|---------------|------------------|----------------|----------------|----------------|--------------|--------------|--------------|--------------|--------------|
| W2-1115(0)-3618 | R2-1-3648 | R10-19aP-3618 | R2-1106-3618 | W2-1103(0)-48 | G20-1103(0)-3660 | W4-2R(0)-48 | W12-1102(0)-48 | W20-1103(0)-48 | W21-5R(0)-48 | W21-1a(0)-48 | W6-3(0)-48 | M6-1(0)-2115 | M6-1(0)-2115 |
| | | | | | | W12-1102(0)-48 | W21-5R(0)-48 | W21-1a(0)-48 | W6-3(0)-48 | M6-1(0)-2115 | M6-1(0)-2115 | | |

- NOTES:**
- FOR ADDITIONAL TRAFFIC CONTROL AND SIGN DETAILS SEE HIGHWAY STANDARDS 701411, 701601, AND DISTRICT ONE DETAIL TC14.
 - TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS WILL BE INSTALLED ALONG ALL TEMPORARY PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC11.

MUST BE REMOVED WHEN FLAGGER OR WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR

FILE NAME = I:\8274 - I001 P118 B8 - B7 Variable\8274.B2 - Touhy Ave over Des Plaines River\I0001\ROAD Sheets\018127-INT-1103.rpt.dgn

| | | |
|-----------------------|------------|-----------|
| USER NAME = tnhahi | DESIGNED - | REVISED - |
| PLOT SCALE = 100:0000 | DRAWN - | REVISED - |
| PLOT DATE = 6/17/2016 | CHECKED - | REVISED - |
| | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TOUHY AVENUE OVER DES PLAINES RIVER
MAINTENANCE OF TRAFFIC - STAGE II**

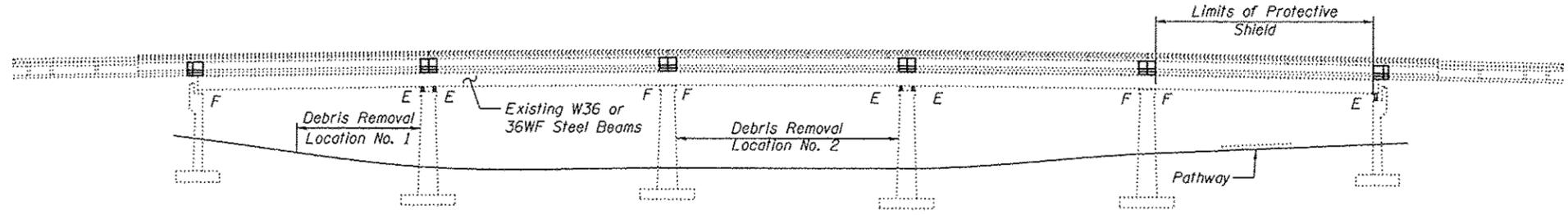
SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|-----------------|------------------|-------------|--------------------|--------------|
| F.A.P. RTE. 341 | SECTION 1109B/BR | COUNTY COOK | TOTAL SHEETS 45 | SHEET NO. 16 |
| ILLINOIS | | | CONTRACT NO. 60V27 | |

Existing Structure: Structure No. 016-0829, was built in 1922 as Rte. 3A under Section 109B-15D as a five span concrete T-Beam Bridge. In 1955, the existing structure was widened on both sides utilizing 36WF steel beams under Section 130-2121 M.F.T. In 1980, under Section 1975-046-BR, the substructure was repaired in addition to removing and replacing the existing superstructure except the 36WF steel beams added during the 1955 structural widening. Currently, the existing structure is a five span steel beam bridge with a 7½" reinforced concrete deck supported by solid wall piers and closed abutments on spread footings. The structure is 265'-0" from bk. to bk. of abutments and 65'-0" o. to o. with a 45° right ahead skew along the widened sections only. Stage construction shall be utilized to maintain two way traffic during construction.

INDEX OF SHEETS

1. General Plan and Elevation
2. General Data
3. Stage Construction Details
4. Temporary Concrete Barrier for Stage Construction
- 5-6. Bridge Deck and Approach Slab Repairs
- 7-9. Joint Replacement Details
10. Structural Steel Repairs
11. Expansion Bearing Details (Beams 3A-3D)
12. Expansion Bearing Details (Beams 1-6)
- 13-17. Substructure Repair Details
18. Debris Removal
19. Preformed Joint Strip Seal
20. Bar Splicer Details



ELEVATION

DESIGN STRESSES
FIELD UNITS

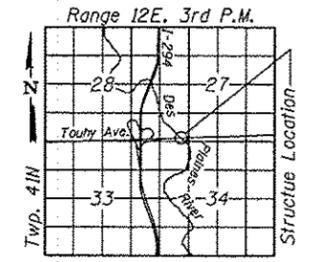
EXISTING CONSTRUCTION (1980)
 $f'c = 3,500$ psi
 $f_y = 60,000$ psi (Reinforcement)
 $f_s = 20,000$ psi (Structural Steel) (M-183)

EXISTING CONSTRUCTION (1955)
 $f'c = 1,200$ psi (Without Earth Pressure)
 $f'c = 800$ psi (With Earth Pressure)
 $f_s = 20,000$ psi (Reinforcement)
 $f_s = 18,000$ psi (Structural Steel)

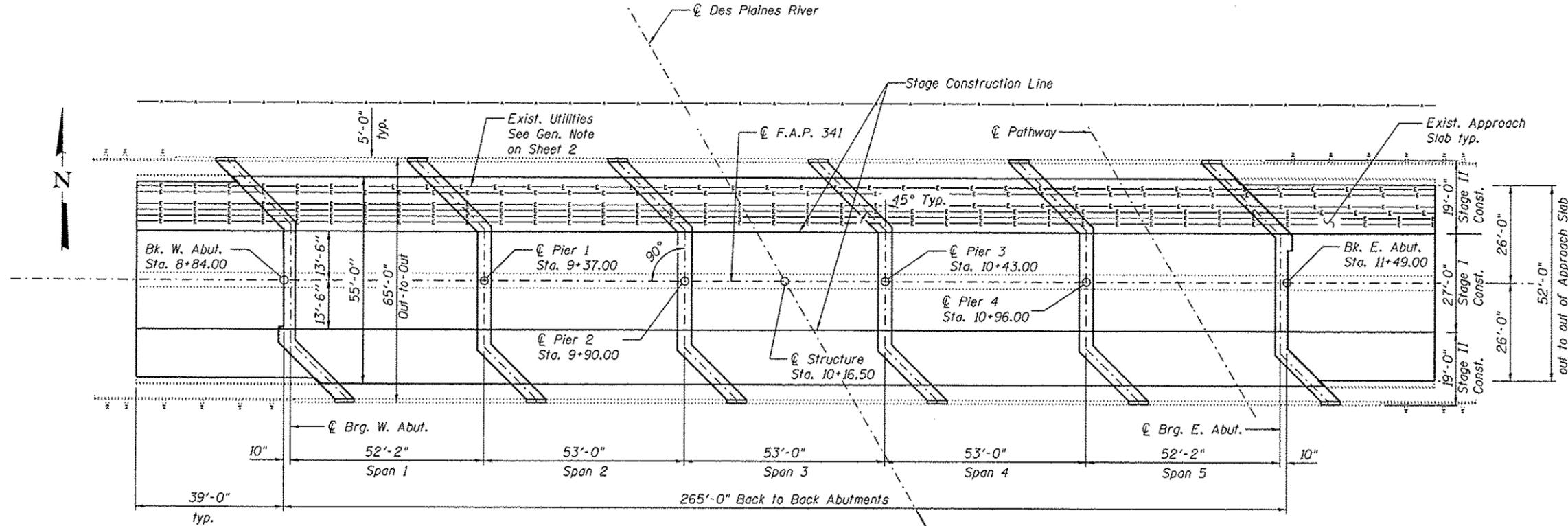
DESIGN SPECIFICATIONS

(New Construction)
 2002 AASHTO Standard Specifications for Highway Bridges, 17th Edition

LOADING HS-20-44
(Original Construction)



LOCATION SKETCH



PLAN



Michael T. Haley 6-16-2016
 Michael T. Haley Date
 Licensed Structural Engineer
 State of Illinois No. 81-5991
 Expires 11/30/2016

GENERAL PLAN AND ELEVATION
TOUHY AVE. OVER DES PLAINES RIVER
F.A.P. RTE. 341-SECTION (109B)BR
COOK COUNTY
STATION 10+16.50
STRUCTURE NO. 016-0829

FILE NAME: ...16.Dgn File: 8168829-60V27.dgn



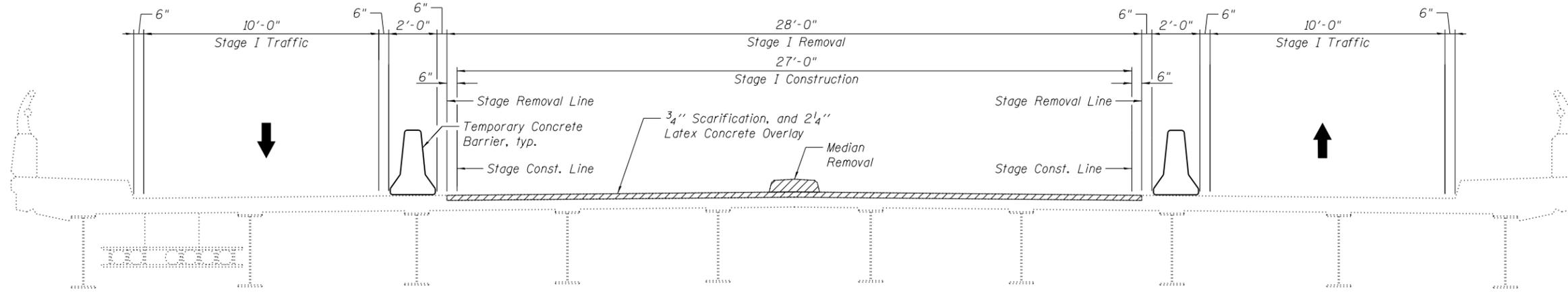
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| PLOT SCALE * | DRAWN - AJF | REVISED - |
| PLOT DATE * 6/16/2016 7:59:03 AM | CHECKED - MTH | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION
STRUCTURE NO. 016-0829

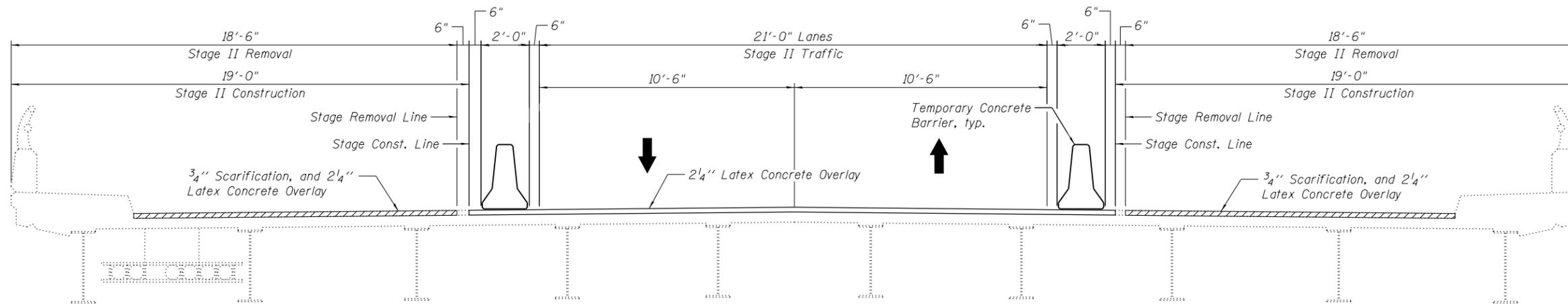
SHEET NO. 1 OF 20 SHEETS

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|----------|--------|---------------------------|-----------|
| 341 | (109B)BR | COOK | 45 | 17 |
| CONTRACT NO. 60V27 | | | ILLINOIS FED. AID PROJECT | |



STAGE I REMOVAL & CONSTRUCTION

(Looking East)

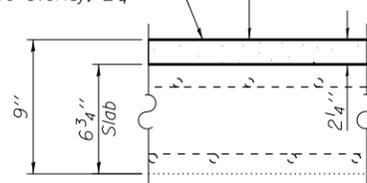


STAGE II REMOVAL & CONSTRUCTION

(Looking East)

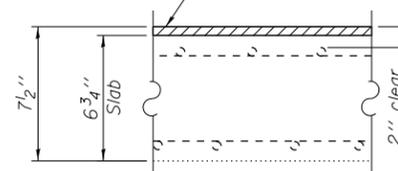
Notes:
See sheet 4 of 20 for details of Temporary Concrete Barrier.
See roadway plan for quantity of Temporary Concrete Barrier.

Proposed Bridge Deck
Latex Concrete Overlay, 2 1/4"

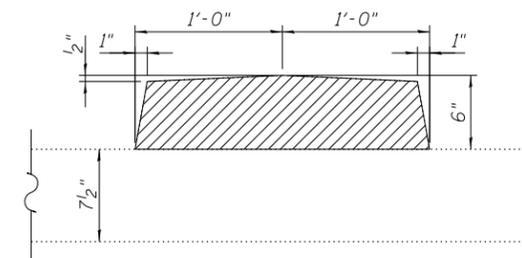


**PROPOSED BRIDGE DECK
CROSS SECTION**

Bridge Deck Scarification 3/4"



**EXISTING BRIDGE DECK
CROSS SECTION**



SECTION THRU MEDIAN

LEGEND:



Limits of Concrete Removal

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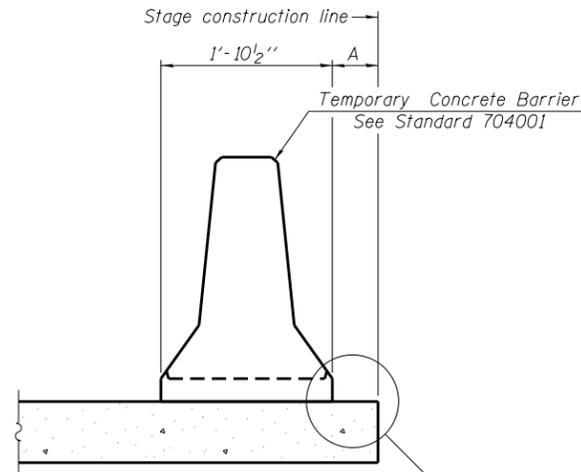
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 016-0829**

SHEET NO. 3 OF 20 SHEETS

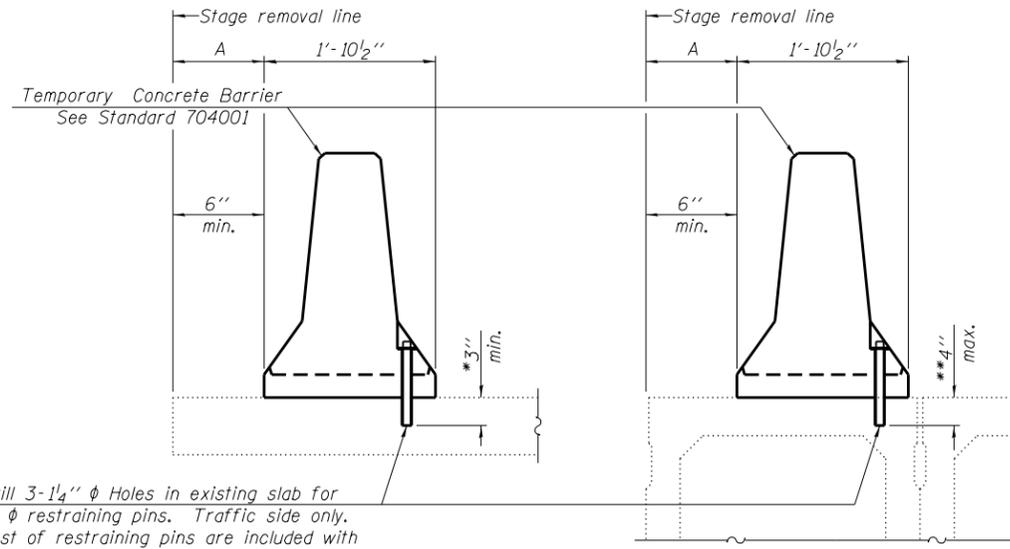
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|--------------------|----------|--------|--------------|-----------|
| 341 | (109B)BR | COOK | 45 | 19 |
| CONTRACT NO. 60V27 | | | | |

ILLINOIS FED. AID PROJECT



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I or Detail II. No restraint is required when "A" is greater than 3'-1".

NEW SLAB

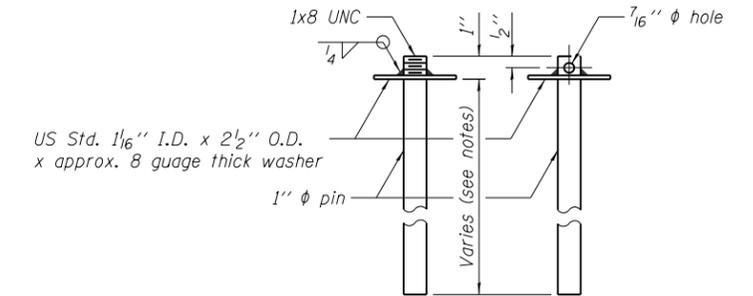


Drill 3-1/4" ϕ Holes in existing slab for 1" ϕ restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

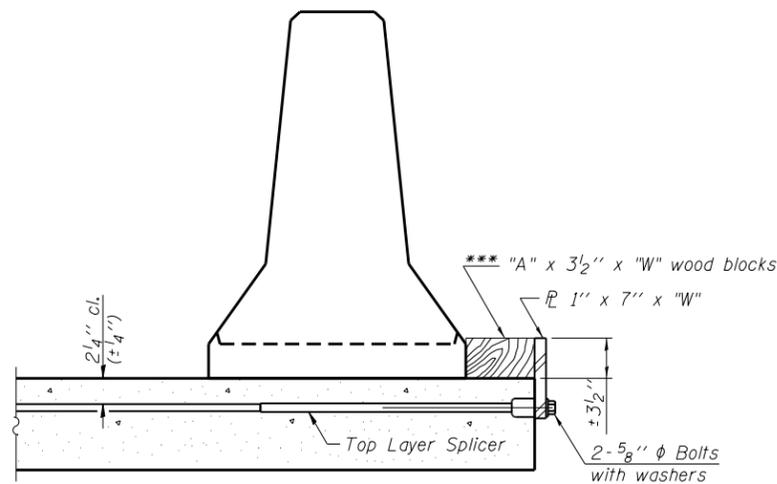
EXISTING DECK BEAM

* Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.
 ** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.

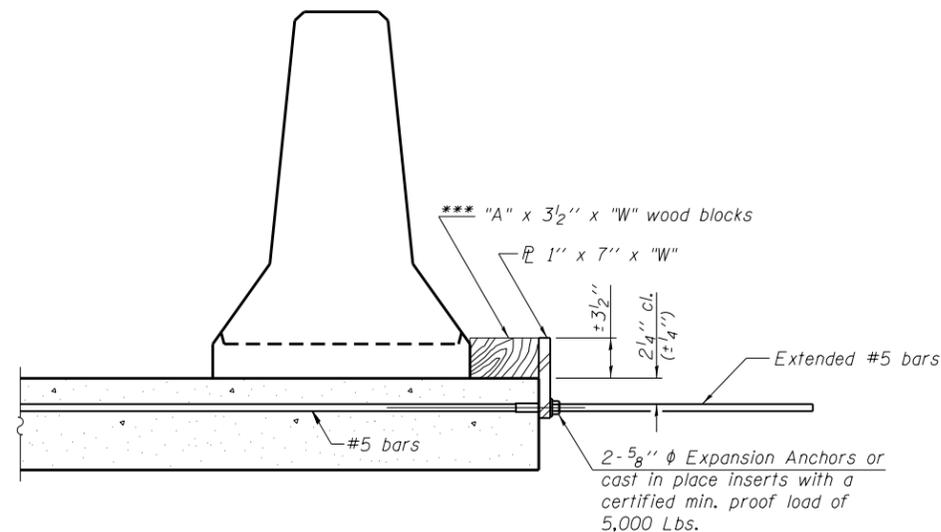


RESTRAINING PIN

SECTIONS THRU SLAB OR DECK BEAM



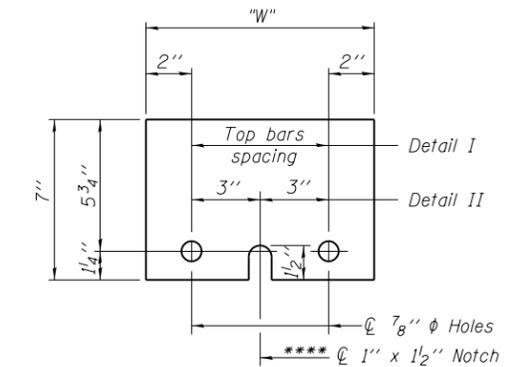
DETAIL I



DETAIL II

RETAINER ASSEMBLY

*** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.



STEEL RETAINER 1" x 7" x "W"

**** Required only with Detail II

NOTES

Detail I - With Bar Splicer or Couplers:
 Connect one (1) 1" x 7" x "W" steel \mathbb{R} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \mathbb{C} of each barrier panel.
 Detail II - With Extended Reinforcement Bars:
 Connect one (1) 1" x 7" x "W" steel \mathbb{R} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \mathbb{C} of each barrier panel.
 Cost of retainer assembly is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

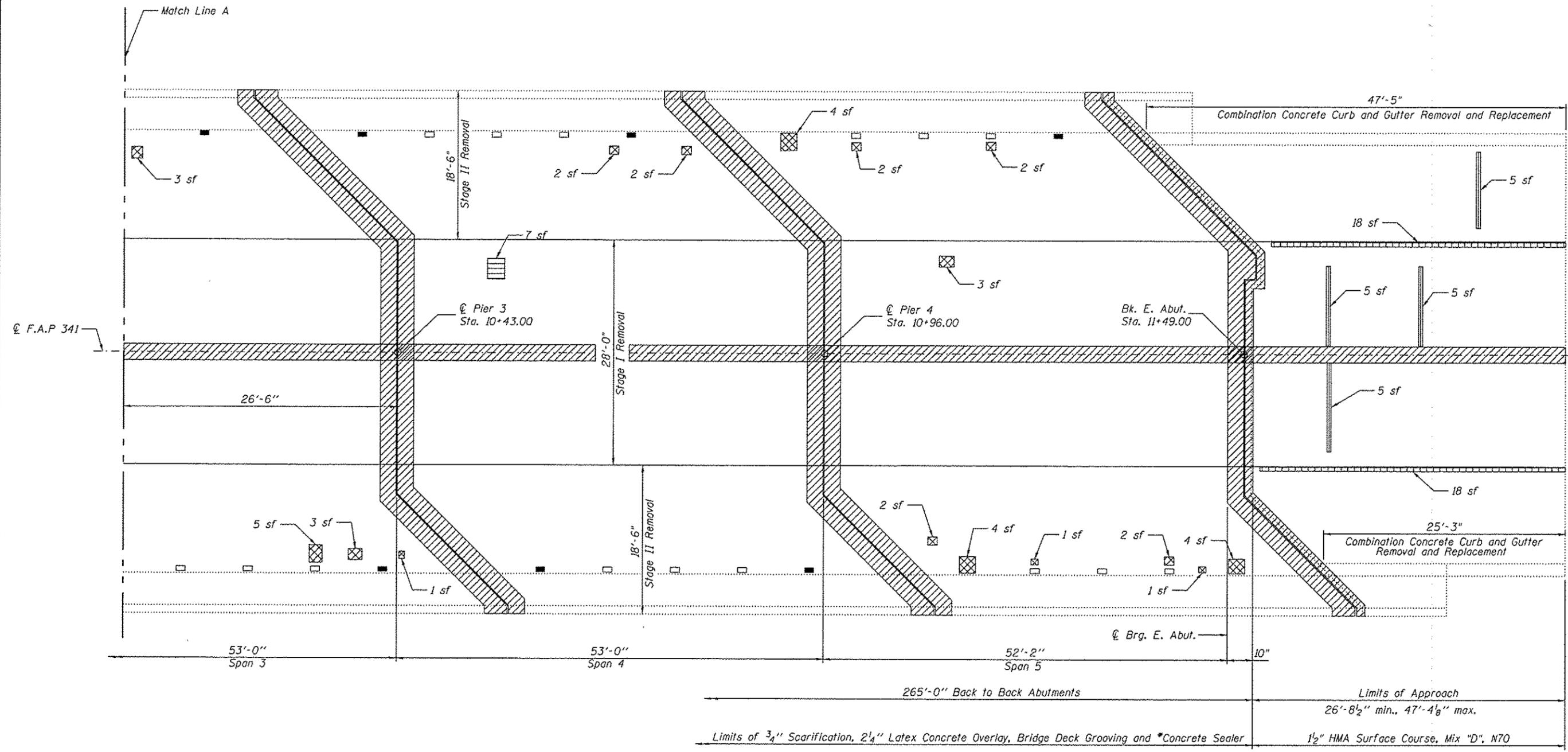
R-27

2-19-16

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|--------|--------------|-----------|
| 341 | (109B)BR | COOK | 45 | 20 |
| CONTRACT NO. 60V27 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



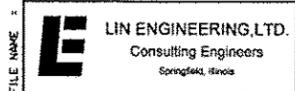
BRIDGE DECK PARTIAL PLAN
 (Partial Span 3, Span 4, Span 5, E. Approach)

LEGEND:

- sf Square Feet
- Concrete Removal
- Deck Slab Repair (Full Depth, Type I)
- Deck Slab Repair (Full Depth, Type II)
- Approach Slab Repair (Partial Depth)
- Plug Existing Deck Drains

Notes:
 Protective Shield required at Span 5 from Fascia Beam to Fascia Beam.
 Deck drains within the limits of a full depth deck slab repair shall be removed. Cost of deck drain removal included in the type of full depth deck slab repair identified in the Plan view.
 *Bridge Deck Concrete Sealer shall not be applied to Latex Concrete areas.

(Sheet 2 of 2)



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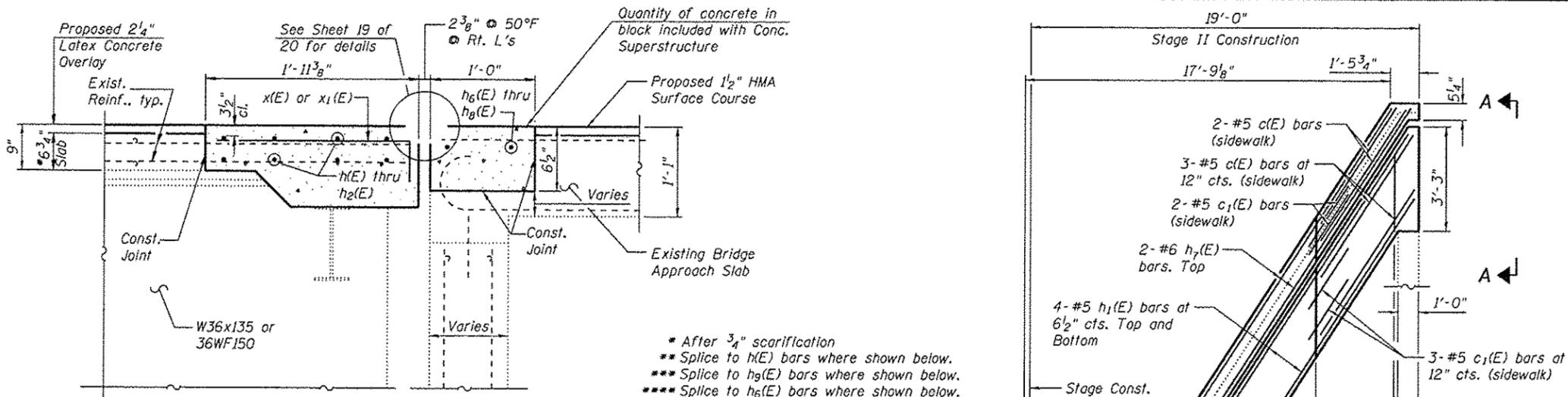
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BRIDGE DECK AND APPROACH SLAB REPAIRS
 STRUCTURE NO. 016-0829

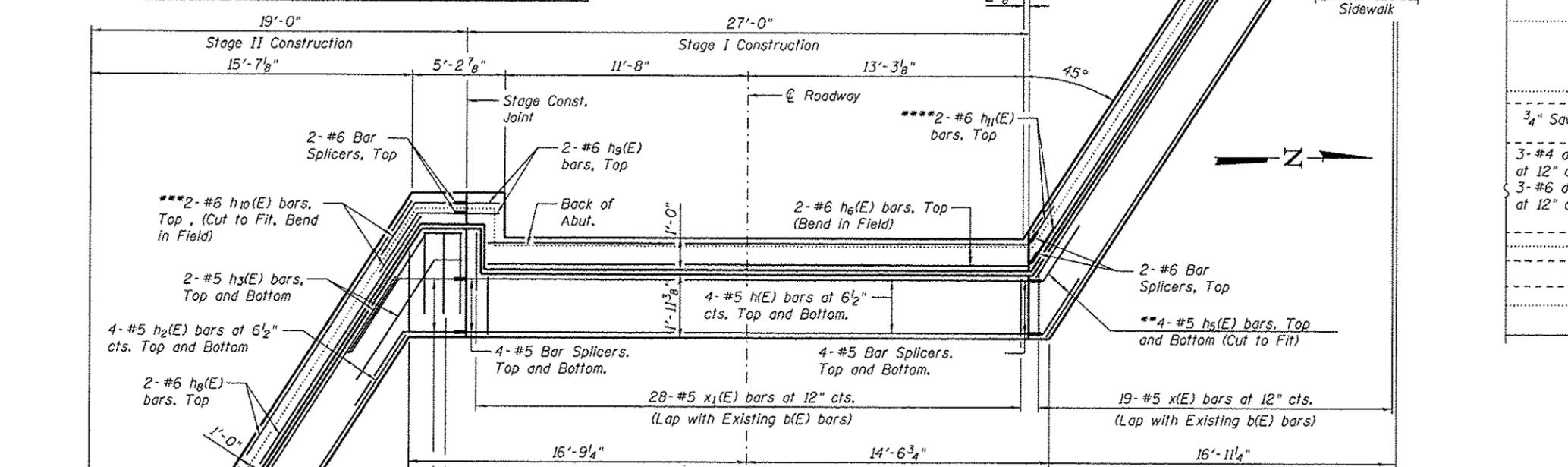
SHEET NO. 6 OF 20 SHEETS

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| CONTRACT NO. 60V27 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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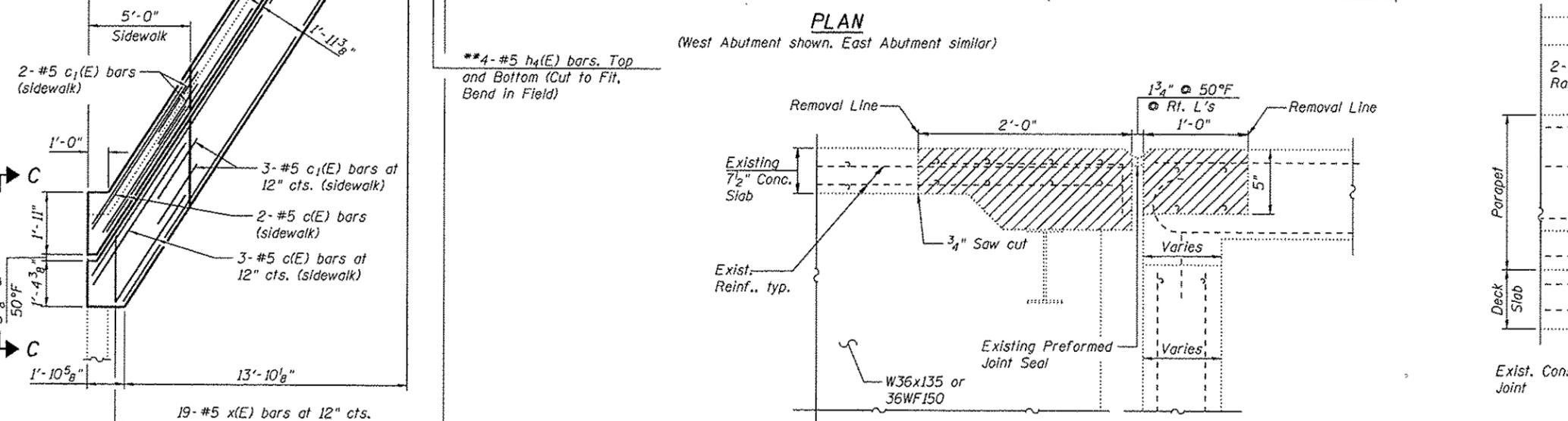


SECTION THRU PROPOSED ABUTMENT JOINT



PLAN

(West Abutment shown. East Abutment similar)

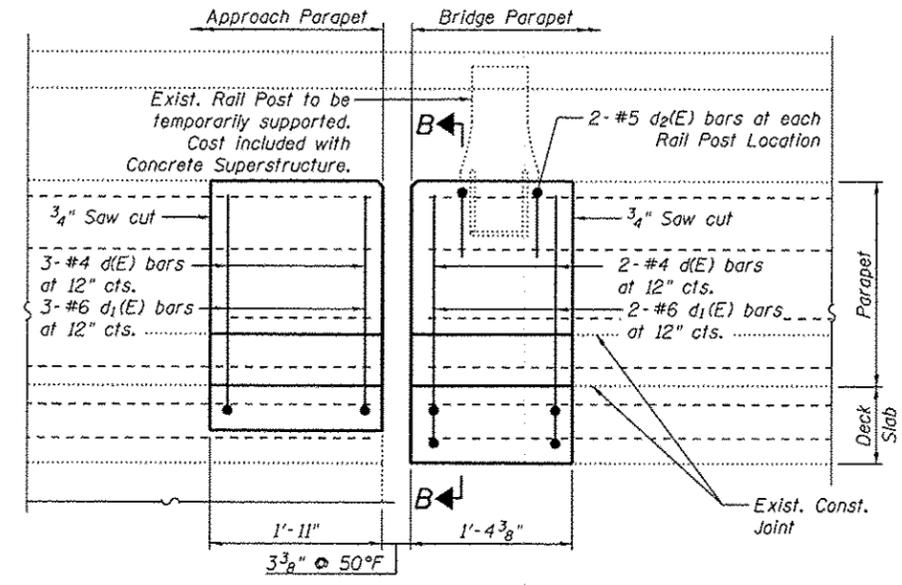


SECTION THRU EXISTING ABUTMENT JOINT

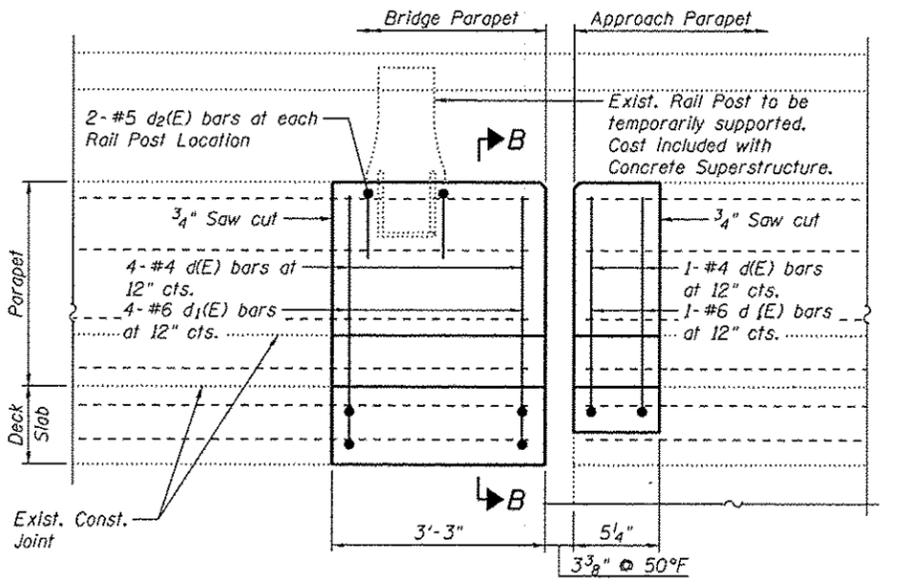
(Dimensions at Rt. L's)

Notes:
 Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
 For Section B-B, Total Bill of Material and Bar Details, see sheet 9 of 20.
 Proposed d(E) bars are required for the Approach Parapets only if the Concrete Removal fully exposes the equivalent existing bars that the proposed d(E) bars are intended to replace.

LEGEND:
 Limits of Concrete Removal



VIEW C-C



VIEW A-A

(Sheet 1 of 3)



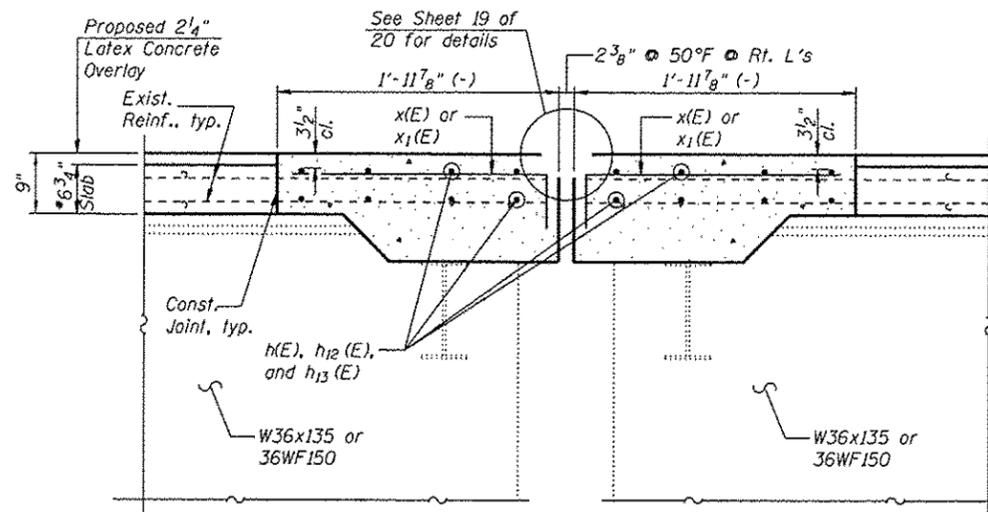
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**JOINT REPLACEMENT DETAILS
 STRUCTURE NO. 016-0829**

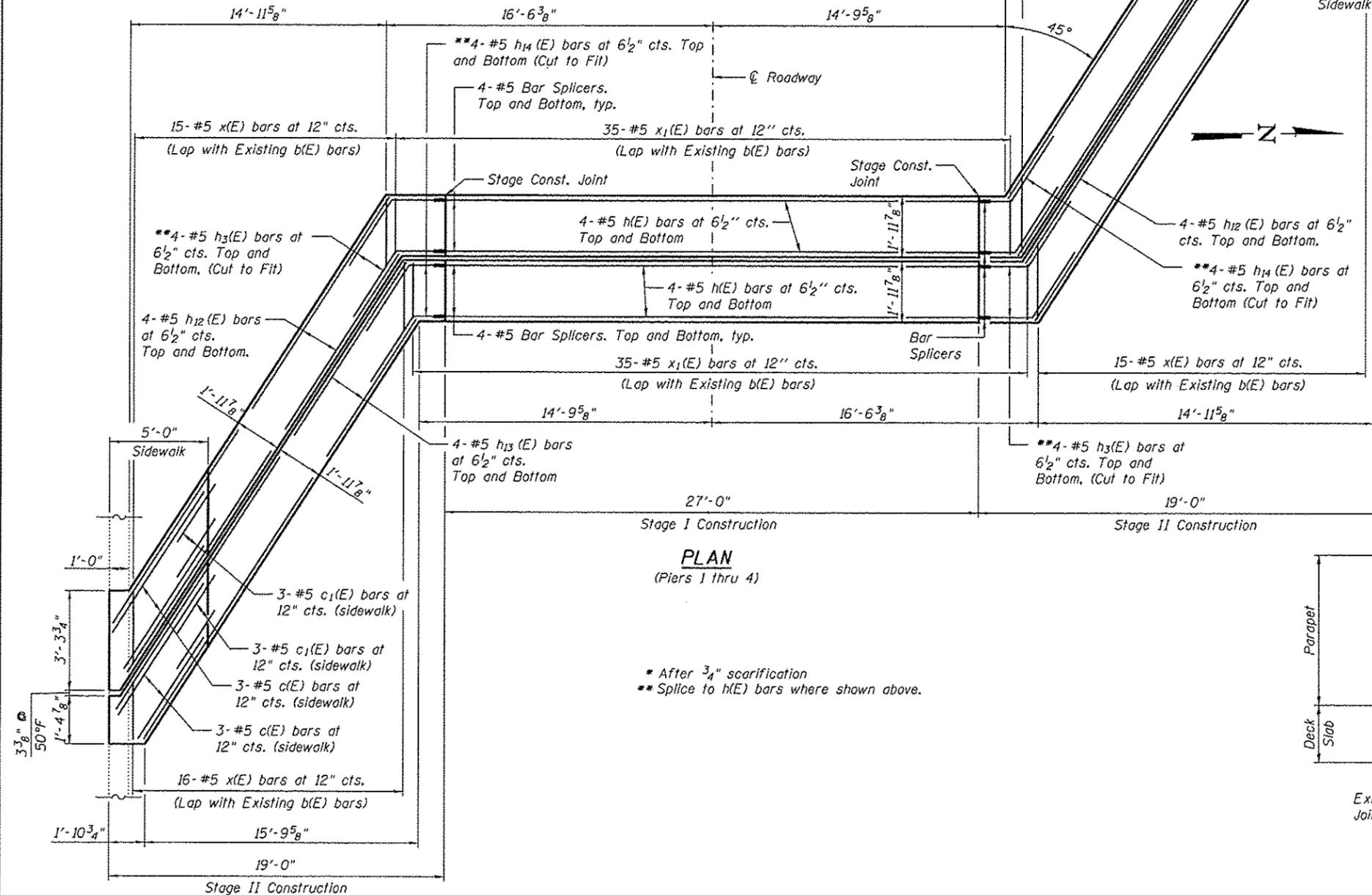
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| CONTRACT NO. 60V27 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



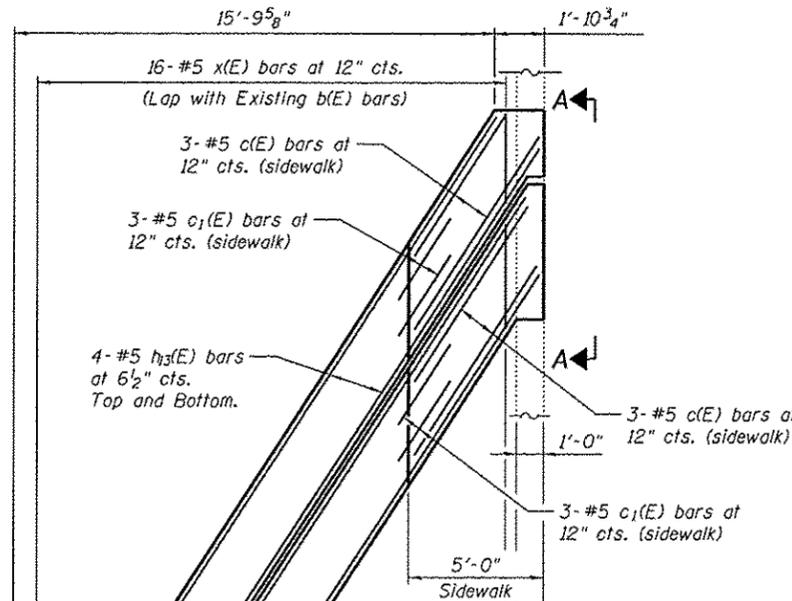
SECTION THRU PROPOSED PIER

(Dimensions at Rt. L's)



PLAN
(Piers 1 thru 4)

* After 3/4" scarification
** Splice to h(E) bars where shown above.

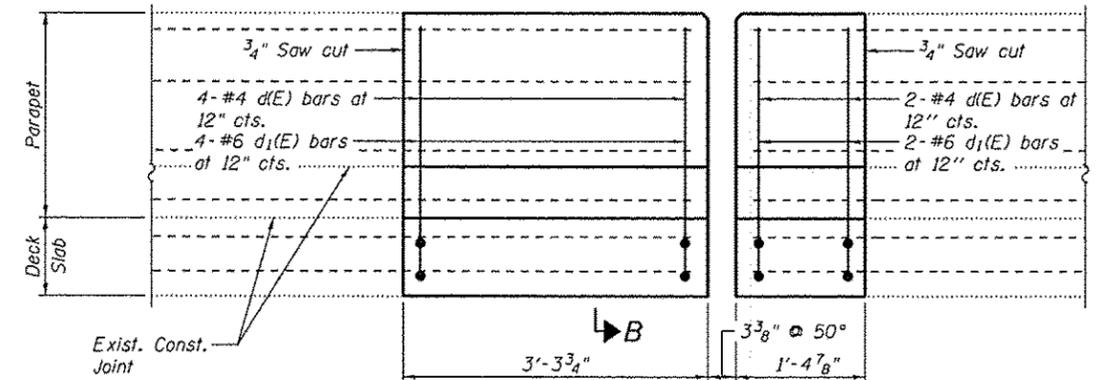
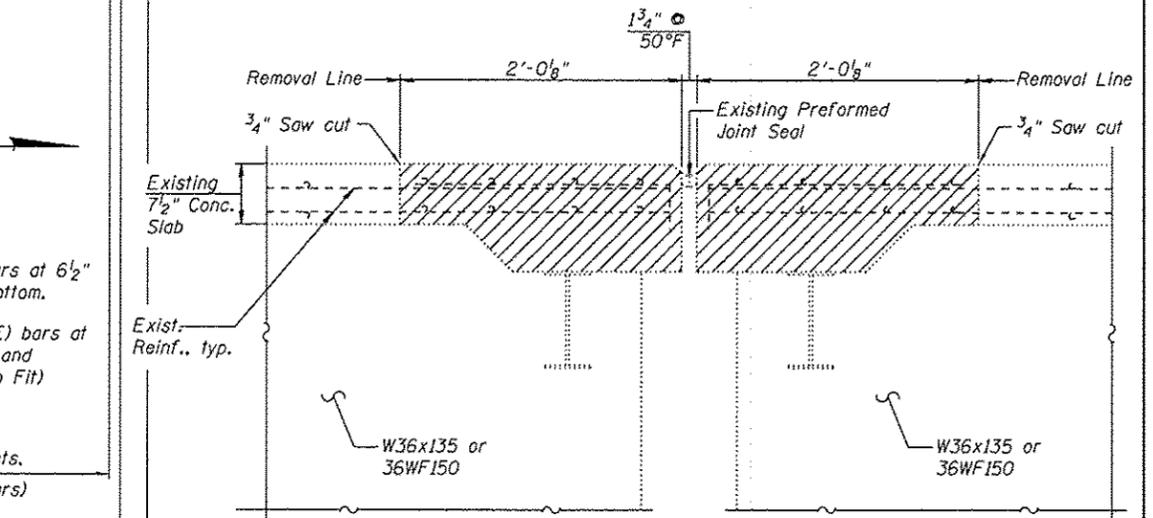


LEGEND:

Limits of Concrete Removal

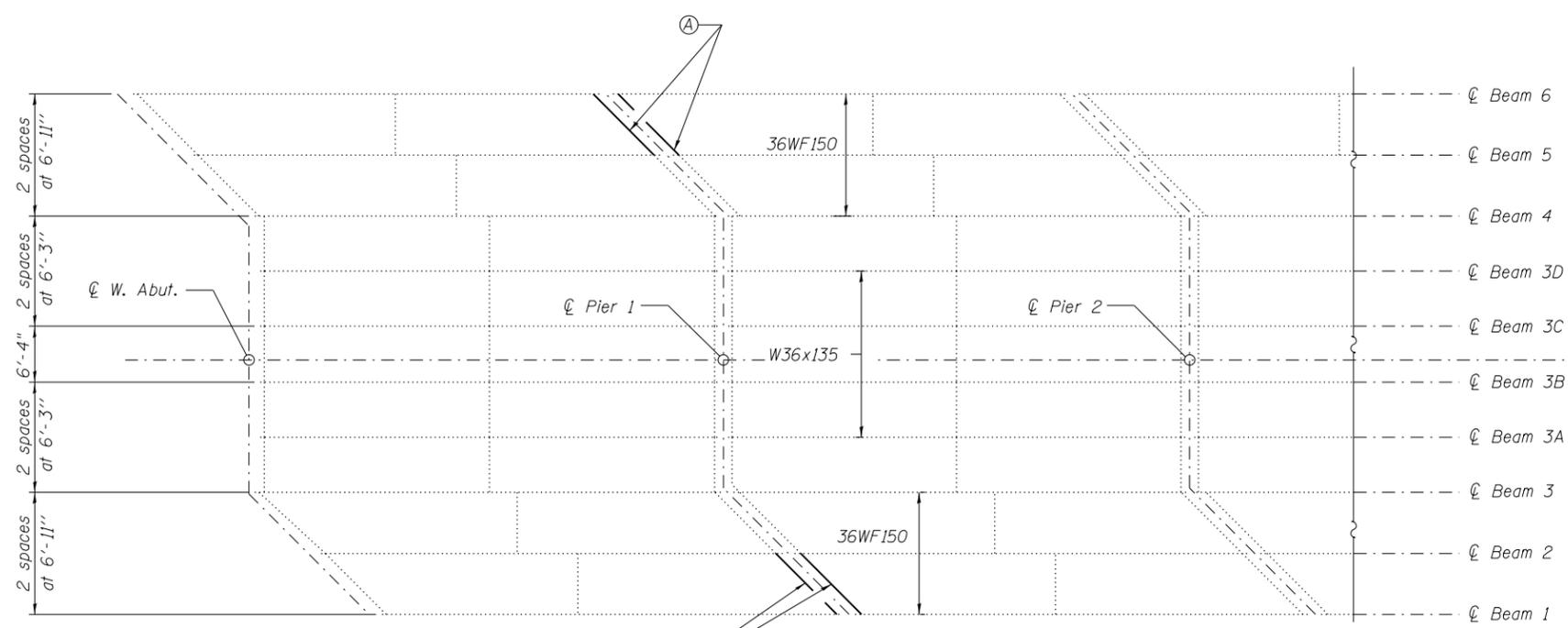
SECTION THRU EXISTING PIER

(Dimensions at Rt. L's)

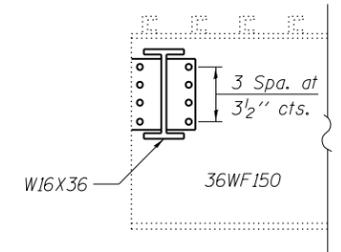


VIEW A-A

(Sheet 2 of 3)



PARTIAL FRAMING PLAN



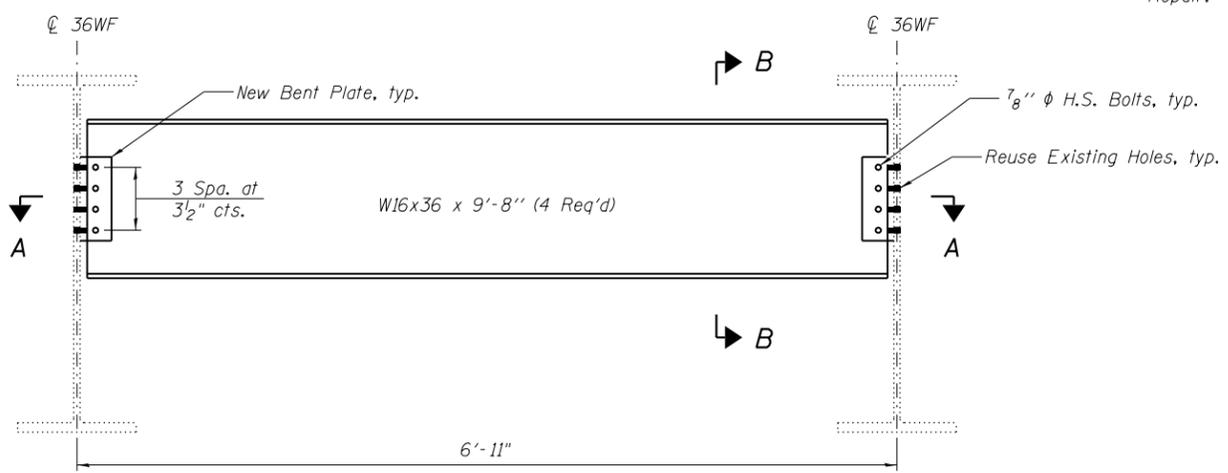
SECTION B-B

BILL OF MATERIAL

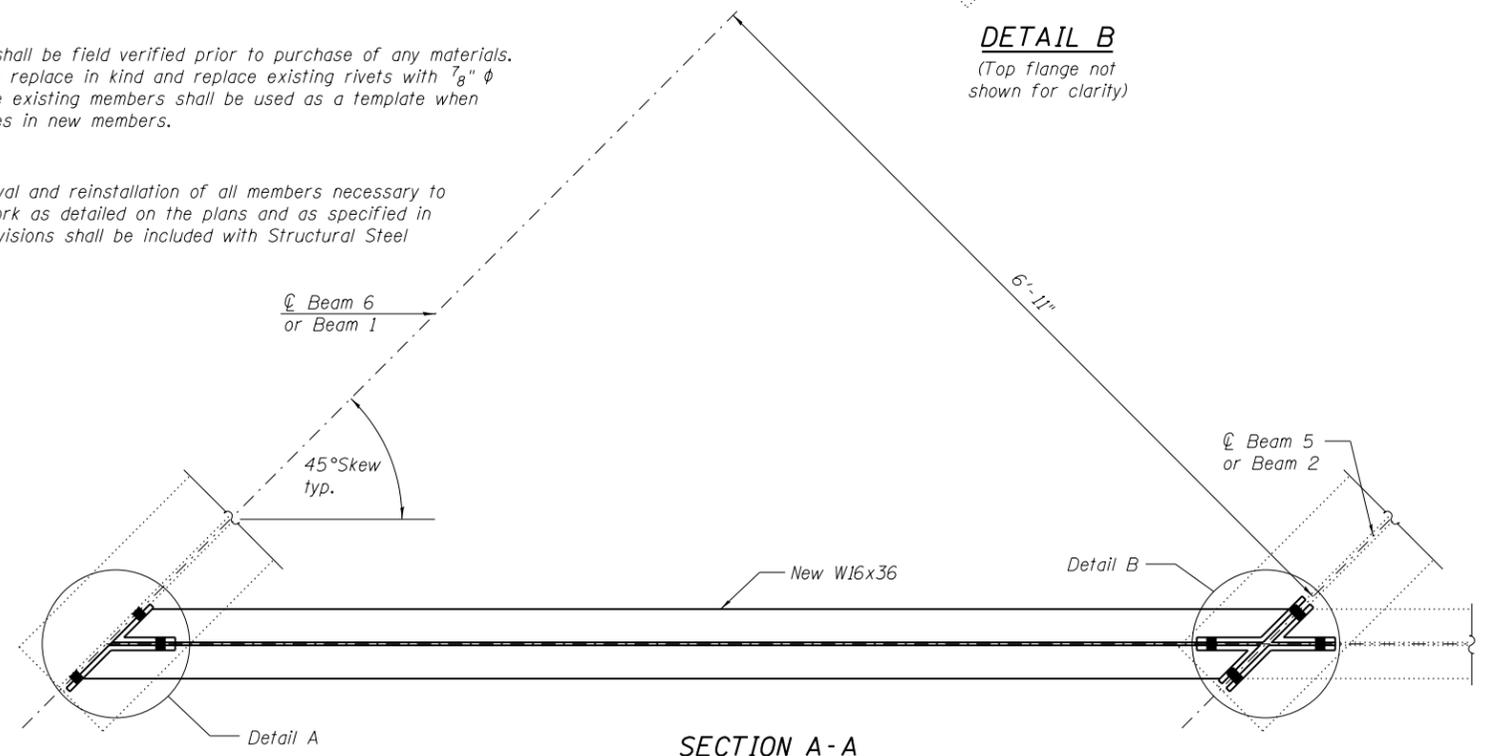
| Item | Unit | Total |
|-------------------------|-------|-------|
| Structural Steel Repair | Pound | 2,570 |

*All dimensions shall be field verified prior to purchase of any materials. The intent is to replace in kind and replace existing rivets with 7/8" φ H.S. bolts. The existing members shall be used as a template when drilling new holes in new members.

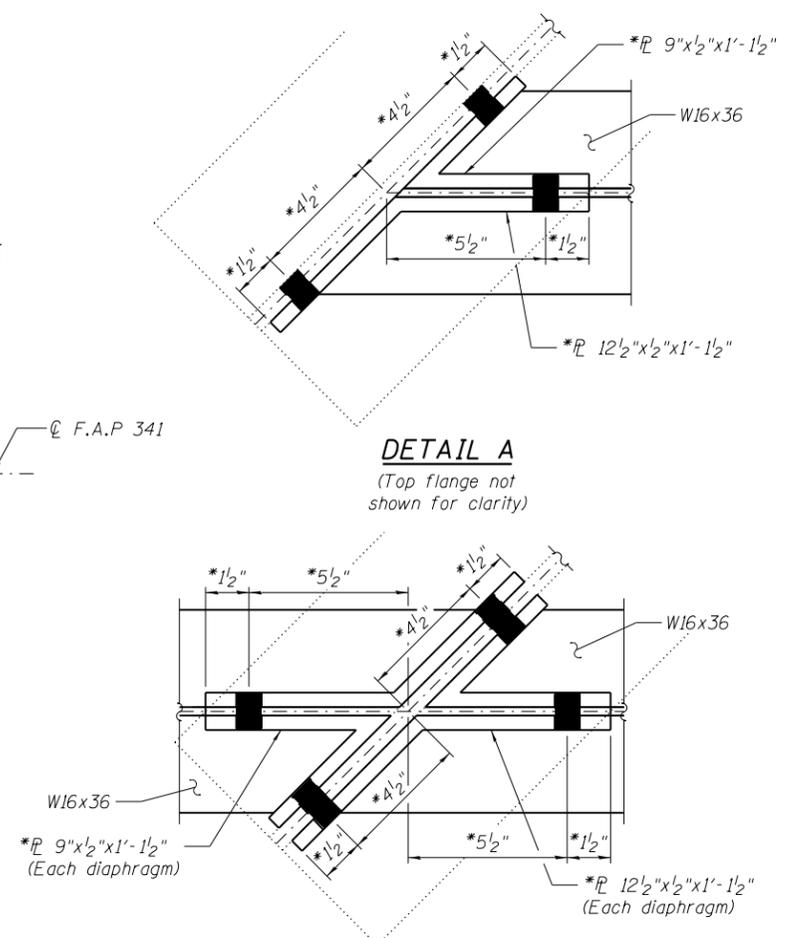
Notes:
Cost of removal and reinstallation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Structural Steel Repair.



DIAPHRAGM ELEVATION



SECTION A-A



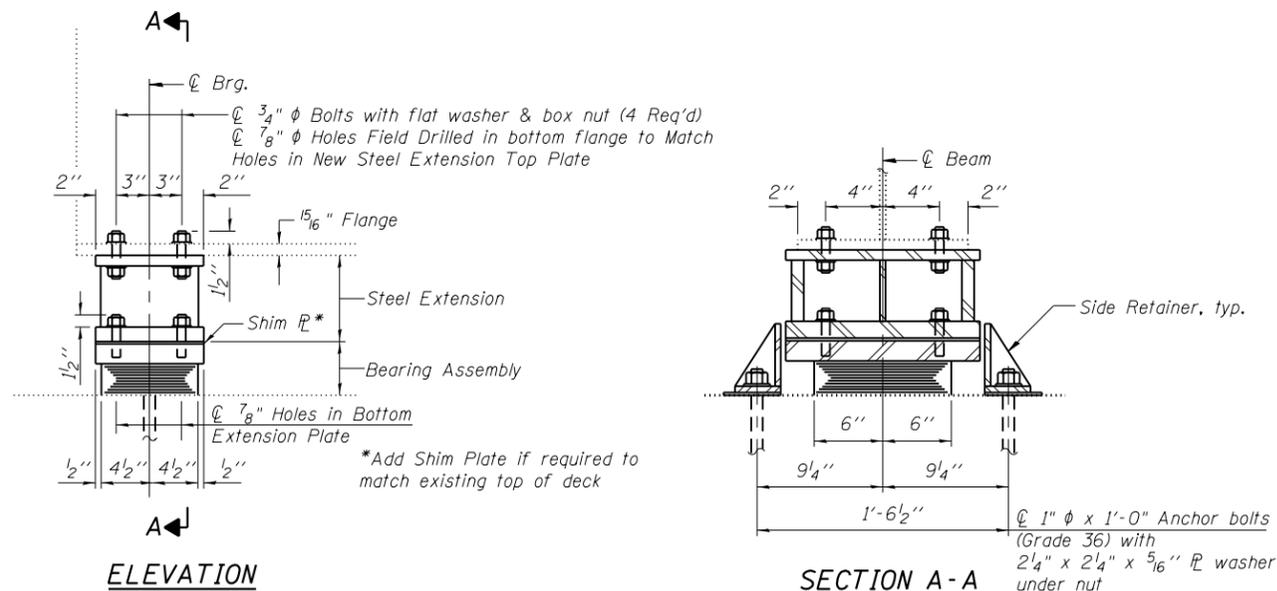
DETAIL A

(Top flange not shown for clarity)

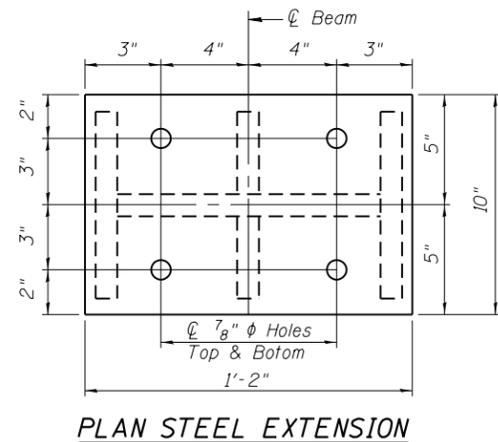
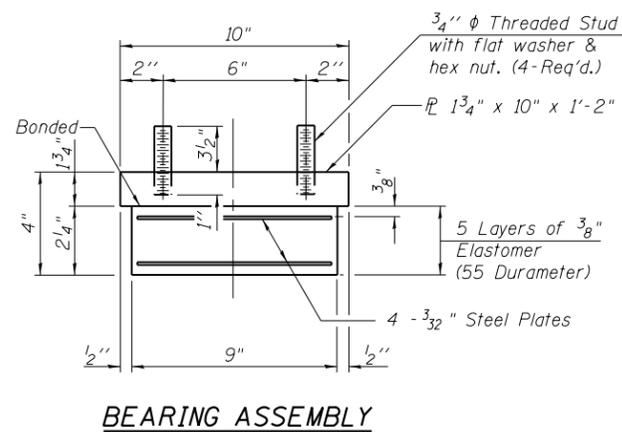
DETAIL B

(Top flange not shown for clarity)

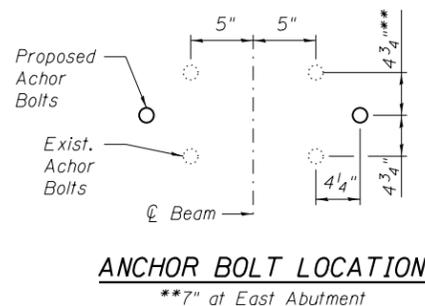
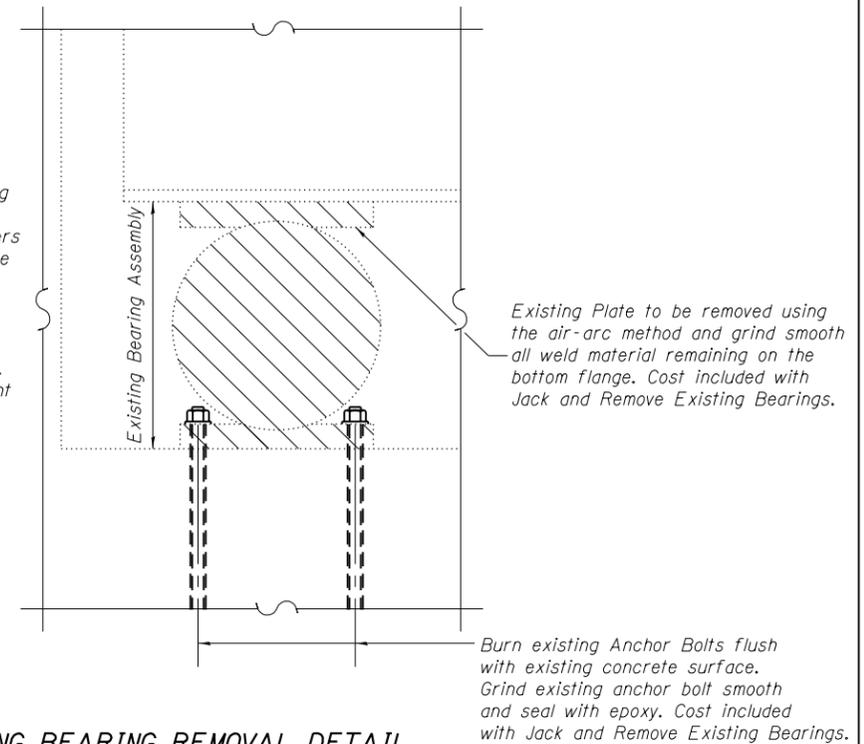
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TYPE I ELASTOMERIC EXP. BRG.
 (Beams 1-3, 4-6 at Pier 1, Pier 3 & East Abutment)



Notes:
 Hatch area indicates Bearing removal. See special provision for Jack and Remove Existing Bearings.
 Anchor bolts shall be ASTM F1554 all-thread (or an Engineer approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
 Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
 Side retainers, steel extensions and other steel members required for the bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.
 Fasteners shall be AASHTO M164 Type 1, mechanically galvanized bolts.
 The Contractor is to verify the existing dimensions prior to fabricating the steel extensions and shim plates. It is intended to keep the existing beams at their current elevation. Therefore, if field measurements differ from heights shown, adjust shim plates accordingly.

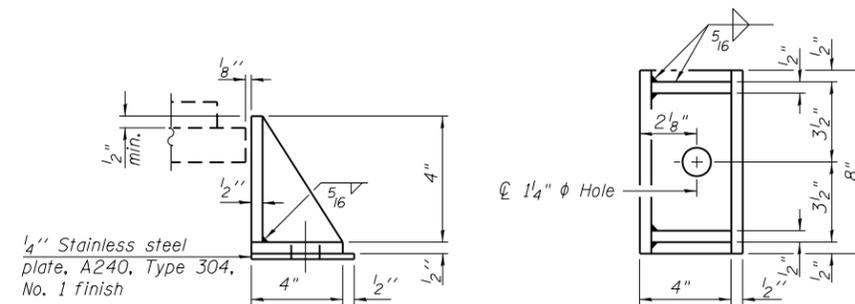
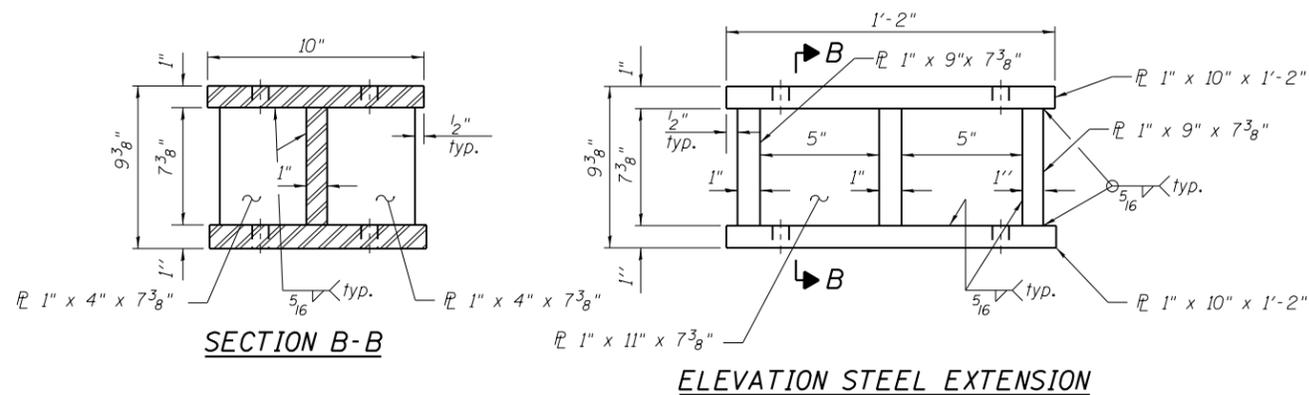


FIELD MEASURED BEARING HEIGHTS

| Beam No. | Pier 1 Span 1 | Pier 1 Span 2 | Pier 3 Span 3 | Pier 3 Span 4 | East Abut. |
|----------|---------------|---------------|---------------|---------------|------------|
| 1 | 14" | 14 5/16" | 14 5/16" | 13 7/8" | 14 5/16" |
| 2 | 14 3/16" | 14 1/4" | 14 1/16" | 13 1/2" | 14 7/16" |
| 3 | 13 1/2" | 14" | 14 3/8" | 14 3/8" | 14 7/16" |
| 4 | 14 1/8" | 14 5/16" | 14 1/8" | 14 1/16" | 14 5/16" |
| 5 | 13 7/8" | 14 3/8" | 14 1/4" | 14 1/8" | 14 1/16" |
| 6 | 14 3/8" | 14 3/4" | NA | 14 5/16" | 14 5/8" |

BEAM REACTION TABLE
 (Per Bearing)

| | |
|---------------------------|----|
| Dead Load (K) | 36 |
| Live Load (K) | 38 |
| Impact (K) | 11 |
| Total (K) | 85 |
| Min. Jack Capacity (Tons) | 46 |



BILL OF MATERIAL

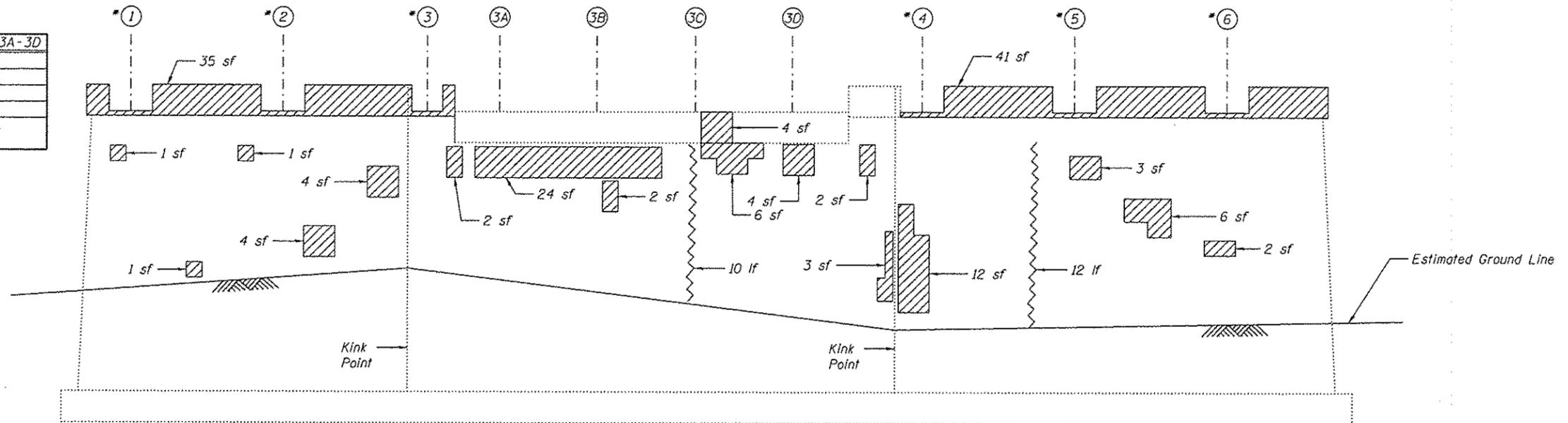
| Item | Unit | Total |
|--------------------------------------|------|-------|
| Elastomeric Bearing Assembly, Type I | Each | 30 |
| Anchor Bolts, 1" | Each | 60 |
| Jack and Remove Existing Bearings | Each | 30 |

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REACTION TABLE

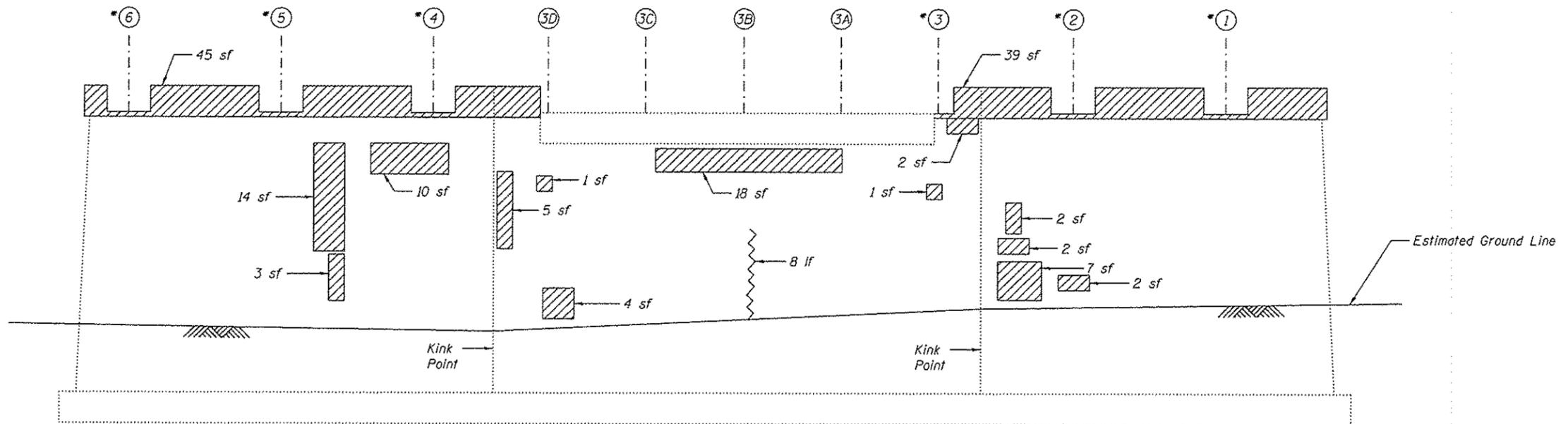
(Per Bearing)

| | Girders 1-6 | Girders 3A-3D |
|---------------------------|-------------|---------------|
| Dead Load (K) | 36 | 34 |
| Live Load (K) | 38 | 36 |
| Impact (K) | 11 | 10 |
| Total (K) | 85 | 80 |
| Min. Jack Capacity (Tons) | 46 | 43 |



PIER 1

(Looking West)



PIER 1

(Looking East)

LEGEND:

- Structural Repair of Concrete (Depth Equal to or Less Than 5")
- Epoxy Crack Injection
- sf Square Feet
- lf Linear Feet

BILL OF MATERIAL

| Item | Unit | Total |
|--|---------|-------|
| Structural Repair of Concrete (Depth Equal to or Less Than 5") | Sq. Ft. | 312 |
| Epoxy Crack Injection | Foot | 30 |
| Temporary Shoring and Cribbing | Each | 12 |

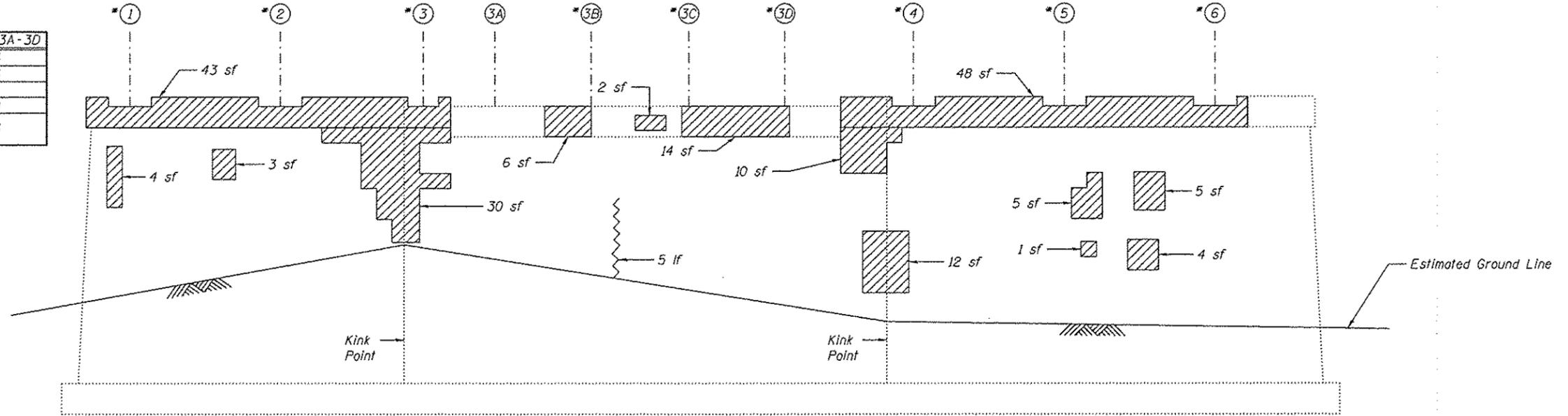
Notes:
 Repair of the existing piers shall include but may not be limited to the areas shown. The actual areas to be repaired will be determined by the Engineer at the time of construction.
 *Beams Nos. 1-3, 4-6 will require temporary shoring for structural repairs on the west face of Pier 1. Beam Nos. 1-3, 4-6 will require temporary shoring on the east face of Pier 1. Cost included with Temporary Shoring and Cribbing.

(Sheet 1 of 5)

REACTION TABLE

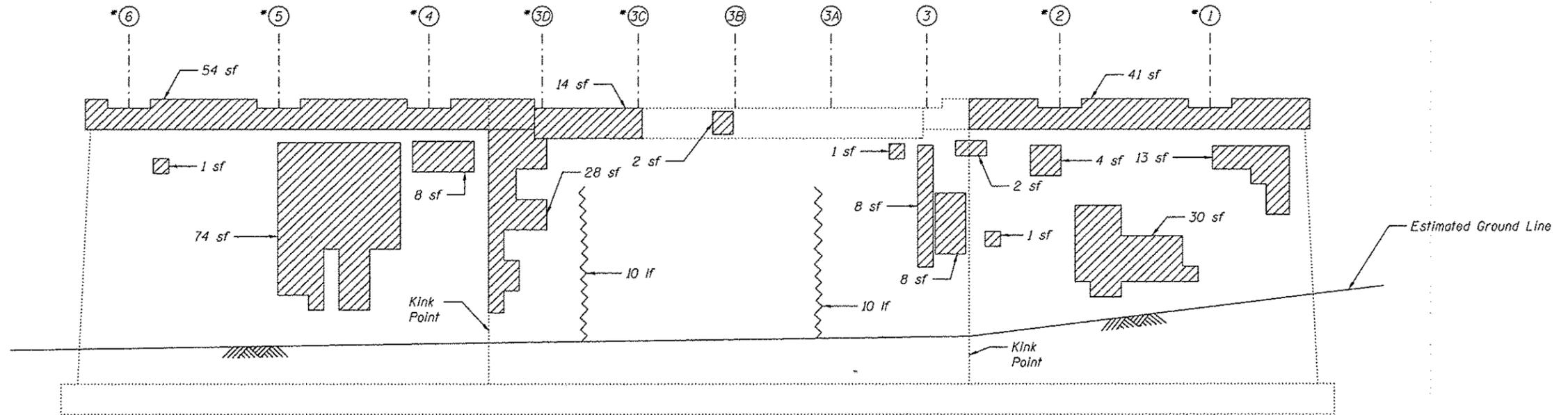
(Per Bearing)

| | Girders 1-6 | Girders 3A-3D |
|---------------------------|-------------|---------------|
| Dead Load (K) | 36 | 34 |
| Live Load (K) | 38 | 36 |
| Impact (K) | 11 | 10 |
| Total (K) | 85 | 80 |
| Min. Jack Capacity (Tons) | 46 | 43 |



PIER 2

(Looking West)



PIER 2

(Looking East)

LEGEND:

- Structural Repair of Concrete (Depth Equal to or Less Than 5")
- Epoxy Crack Injection
- sf Square Feet
- lf Linear Feet

BILL OF MATERIAL

| Item | Unit | Total |
|--|---------|-------|
| Structural Repair of Concrete (Depth Equal to or Less Than 5") | Sq. Ft. | 476 |
| Epoxy Crack Injection | Foot | 25 |
| Temporary Shoring and Cribbing | Each | 15 |

Notes:
 Repair of the existing piers shall include but may not be limited to the areas shown. The actual areas to be repaired will be determined by the Engineer at the time of construction.
 *Beams Nos. 1-2, 3C-6 will require temporary shoring for structural repairs on the west face of Pier 2. Beam Nos. 1-3, 3B-6 will require temporary shoring on the east face of Pier 2. Cost included in Temporary Shoring and Cribbing.

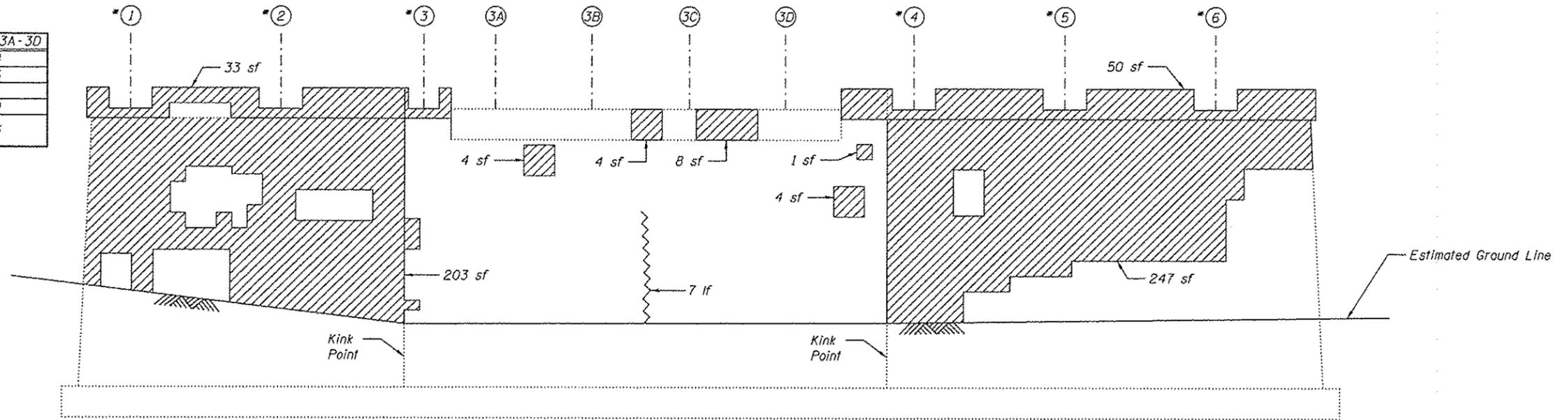
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REACTION TABLE

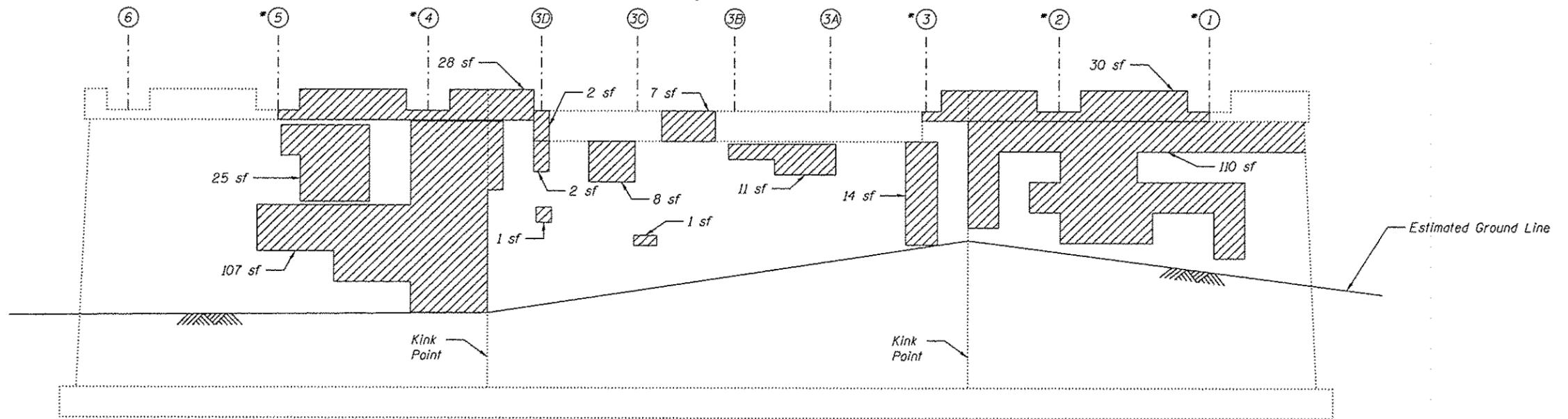
(Per Bearing)

| | Girders 1-6 | Girders 3A-3D |
|---------------------------|-------------|---------------|
| Dead Load (K) | 36 | 34 |
| Live Load (K) | 38 | 36 |
| Impact (K) | 11 | 10 |
| Total (K) | 85 | 80 |
| Min. Jack Capacity (Tons) | 46 | 43 |



PIER 3

(Looking West)



PIER 3

(Looking East)

LEGEND:

- Structural Repair of Concrete (Depth Equal to or Less Than 5")
- Epoxy Crack Injection
- sf Square Feet
- lf Linear Feet

BILL OF MATERIAL

| Item | Unit | Total |
|--|---------|-------|
| Structural Repair of Concrete (Depth Equal to or Less Than 5") | Sq. Ft. | 900 |
| Epoxy Crack Injection | Foot | 7 |
| Temporary Shoring and Cribbing | Each | 11 |

Notes:

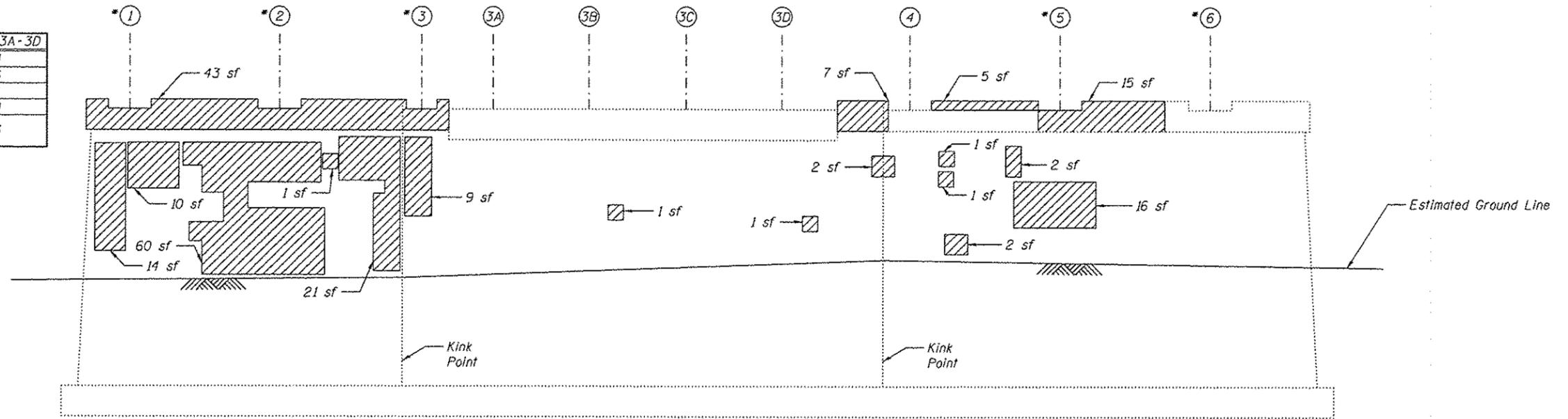
Repair of the existing piers shall include but may not be limited to the areas shown. The actual areas to be repaired will be determined by the Engineer at the time of construction.
 *Beams Nos. 1-3, 4-5 will require temporary shoring for structural repairs on the west face of Pier 3. Beam Nos. 1-3, 4-6 will require temporary shoring on the east face of Pier 3. Cost included with Temporary Shoring and Cribbing.

(Sheet 3 of 5)

REACTION TABLE

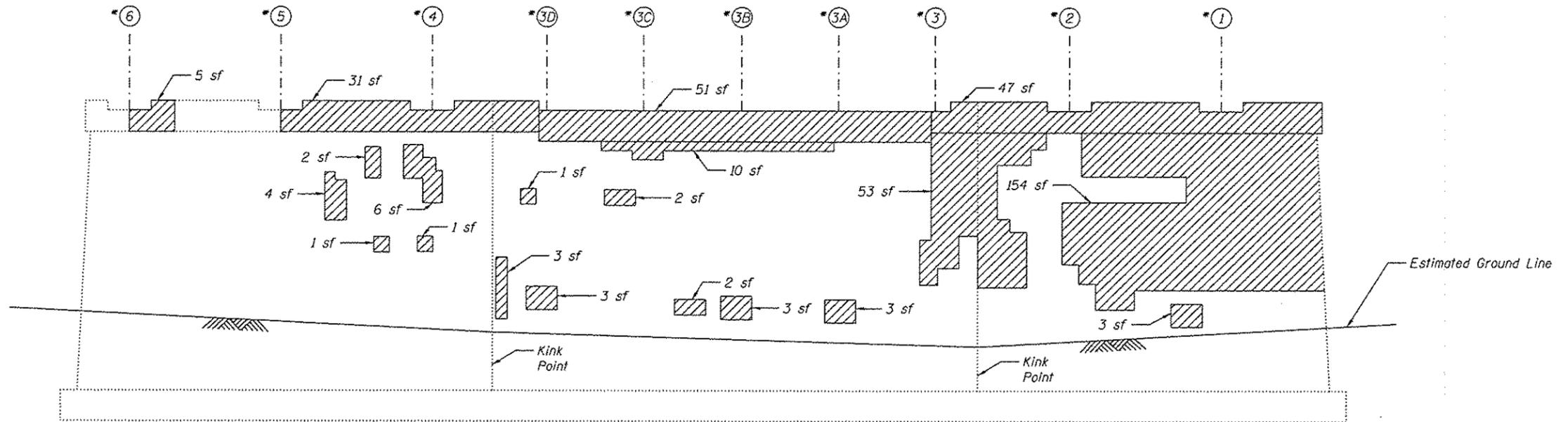
(Per Bearing)

| | Girders 1-6 | Girders 3A-3D |
|---------------------------|-------------|---------------|
| Dead Load (K) | 36 | 34 |
| Live Load (K) | 38 | 36 |
| Impact (K) | 11 | 10 |
| Total (K) | 85 | 80 |
| Min. Jack Capacity (Tons) | 46 | 43 |



PIER 4

(Looking West)



PIER 4

(Looking East)

LEGEND:

Structural Repair of Concrete
(Depth Equal to or Less Than 5")

sf Square Feet

BILL OF MATERIAL

| Item | Unit | Total |
|---|---------|-------|
| Structural Repair of Concrete (Depth Equal to or Less Than 5") | Sq. Ft. | 596 |
| Temporary Shoring and Cribbing | Each | 15 |

Notes:

Repair of the existing piers shall include but may not be limited to the areas shown. The actual areas to be repaired will be determined by the Engineer at the time of construction

*All Beams will require temporary shoring for structural repairs on the west face of Pier 4, Beam Nos. 1-3, 5-6 will require temporary shoring on the east face of Pier 4. Cost Included in Temporary Shoring and Cribbing.

(Sheet 4 of 5)

FILE NAME: \\s.dgn F:\1144\1168823-60V27.dgn

LE LIN ENGINEERING, LTD.
Consulting Engineers
Springfield, Illinois

| | | | | | |
|------------|----------------------|----------|-----|---------|---|
| USER NAME | L1128_Nick | DESIGNED | PSS | REVISED | - |
| FILE NAME | | CHECKED | LMS | REVISED | - |
| PLOT SCALE | | DRAWN | AJF | REVISED | - |
| PLOT DATE | 6/16/2016 7:59:08 AM | CHECKED | MTH | REVISED | - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBSTRUCTURE REPAIR DETAILS
STRUCTURE NO. 016-0829

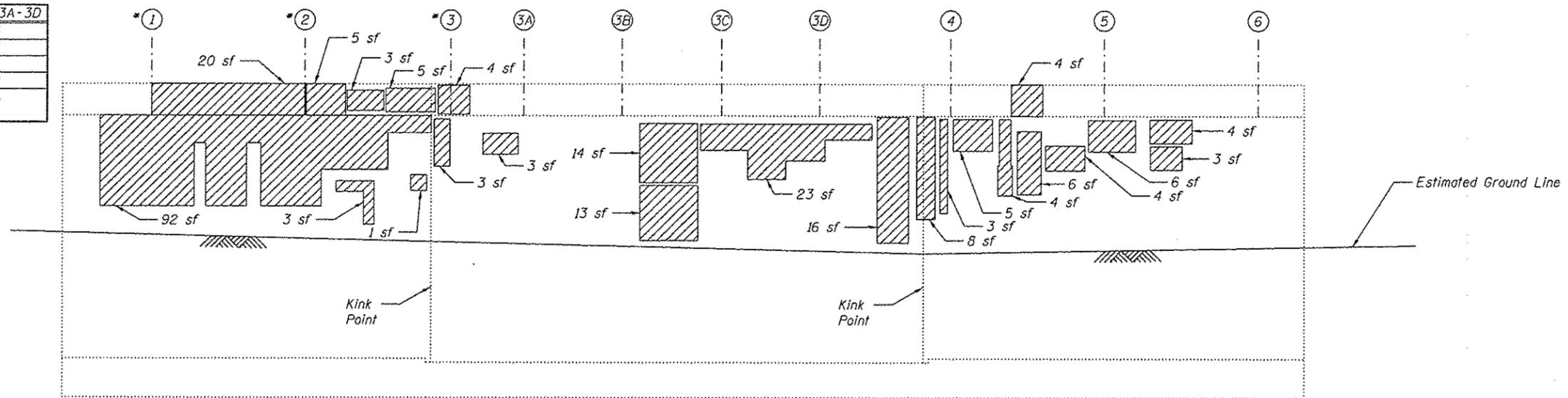
SHEET NO. 16 OF 20 SHEETS

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|--------|--------------|-----------|
| 341 | (109B)BR | COOK | 45 | 32 |
| CONTRACT NO. 60V27 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

REACTION TABLE

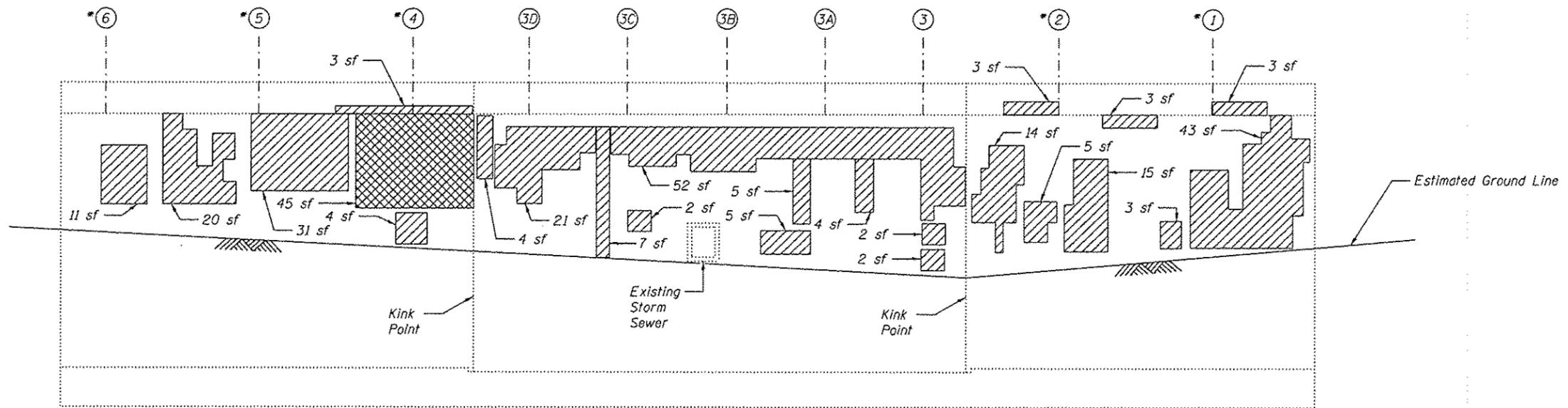
(Per Bearing)

| | Girders 1-6 | Girders 3A-3D |
|---------------------------|-------------|---------------|
| Dead Load (K) | 36 | 34 |
| Live Load (K) | 38 | 36 |
| Impact (K) | 11 | 10 |
| Total (K) | 85 | 80 |
| Min. Jack Capacity (Tons) | 46 | 43 |



WEST ABUTMENT

(Looking West)



EAST ABUTMENT

(Looking East)

LEGEND:

Structural Repair of Concrete (Depth Equal to or Less Than 5")

Structural Repair of Concrete (Depth Greater Than 5")

sf Square Feet

BILL OF MATERIAL

| Item | Unit | Total |
|--|---------|-------|
| Structural Repair of Concrete (Depth Equal to or Less Than 5") | Sq. Ft. | 514 |
| Structural Repair of Concrete (Depth Greater Than 5") | Sq. Ft. | 45 |
| Temporary Shoring and Cribbing | Each | 8 |

Notes:

Repair of the existing abutments shall include but may not be limited to the areas shown. The actual areas to be repaired will be determined by the Engineer at the time of construction.

*Beams Nos. 1-3 will require temporary shoring for structural repairs on the east face of the West Abutment. Beam Nos. 1-2, 4-6 will require Temporary Shoring on the west face of the East Abutment. Cost included in Temporary Shoring and Cribbing.

(Sheet 5 of 5)

FILE NAME: ...\\s.dgn F:\11\016823\6V27.dgn

LE LIN ENGINEERING, LTD.
Consulting Engineers
Springfield, Illinois

| | | |
|----------------------------------|----------------|-----------|
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| FILE NAME • | CHECKED - LMS | REVISED - |
| PLOT SCALE • | DRAWN - AJF | REVISED - |
| PLOT DATE • 6/16/2016 7:59:00 AM | CHECKED - MTH | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBSTRUCTURE REPAIR DETAILS
STRUCTURE NO. 016-0829

SHEET NO. 17 OF 20 SHEETS

| | | | | |
|---------------------------|------------------|-------------|--------------------|--------------|
| F.A.P. RTE. 341 | SECTION 1109B/BR | COUNTY COOK | TOTAL SHEETS 45 | SHEET NO. 33 |
| | | | CONTRACT NO. 60V27 | |
| ILLINOIS FED. AID PROJECT | | | | |



LOCATION 1
Debris in span 1.



LOCATION 2
Debris at nose of Pier 3.



LOCATION 2
Debris along west side of Pier 3.

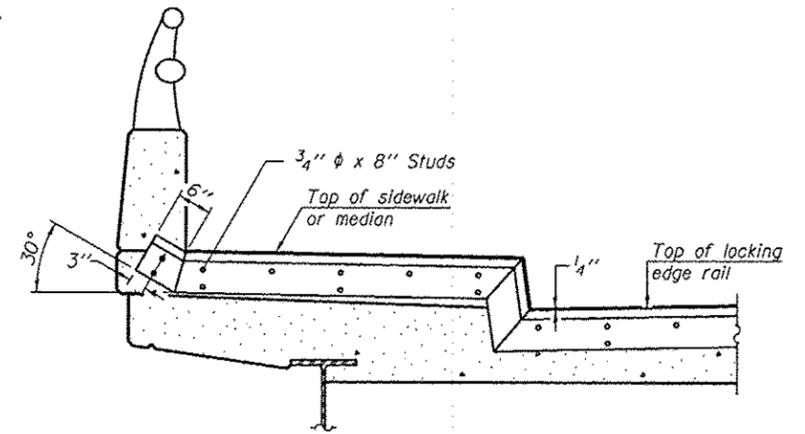
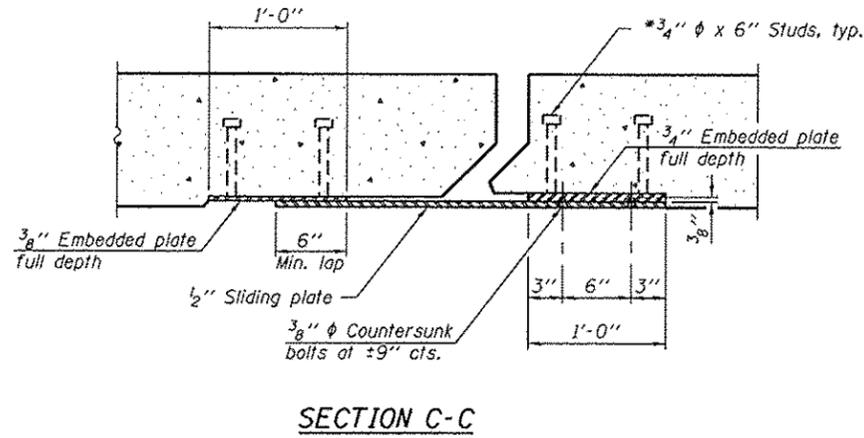
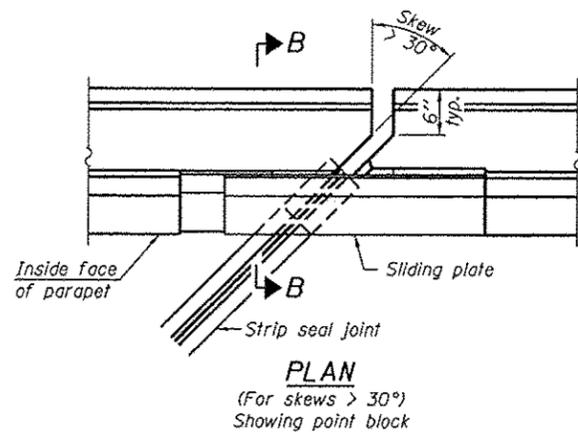
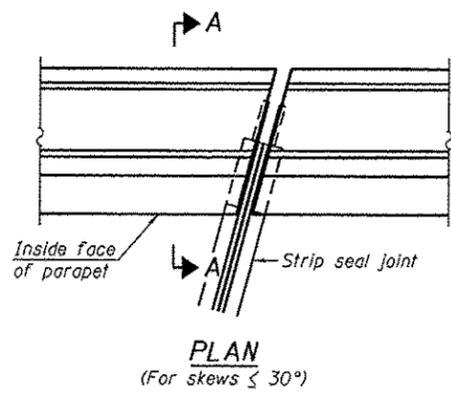
BILL OF MATERIAL

| Item | Unit | Total |
|----------------|---------|-------|
| Debris Removal | Cu. Yd. | 22 |

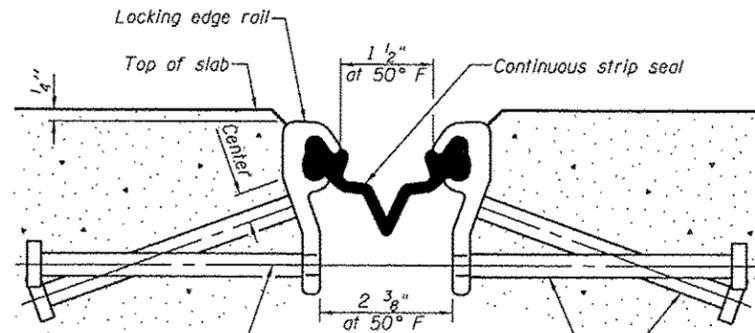
Notes:

The Contractor is responsible for cleaning debris located within structural limits after repairs are completed.
Debris quantity shown is subject to routine variations. Such variations shall not be cause for additional compensation for a change in scope of the work. However, the Contractor will be paid for the quantity actually removed based upon the unit price bid for the work.

FILE NAME = ...16_Dgn_F11as\0160829-60V27.dgn



TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

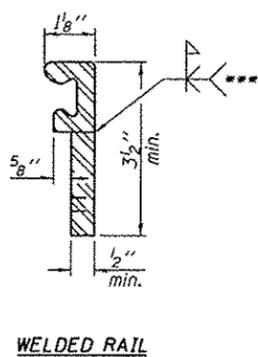
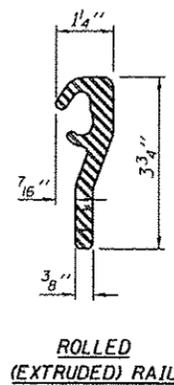


$7/16$ " ϕ holes at 4'-0" cts. for $3/8$ " ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

Place $1/2$ " ϕ x 6" granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded at 1'-0" alt. cts.

Notes:

- The strip seal shall be made continuous and shall have a minimum thickness of $1/4$ ". The configuration of the strip seal shall match the configuration of the Locking Edge Rails.
- The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed.
- The inside of the Locking Edge Rail groove shall be free of weld residue. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.
- The manufacturer's recommended installation methods shall be followed.
- All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
- Maximum space between rail segments at stage lines shall be $3/16$ ", sealed with a suitable sealant.
- Parapet plates and anchorage studs for skews $> 30^\circ$ included in the cost of Preformed Joint Strip Seal.
- The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified accordingly to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.



LOCKING EDGE RAIL SPLICE

Rolled rail shown, welded rail similar.

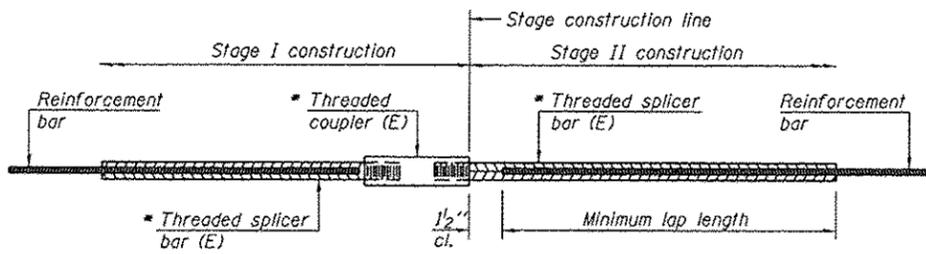
- * Omit weld at seal opening.
- *** Back gouge not required if complete joint penetration is verified by mock-up.

LOCKING EDGE RAIL

BILL OF MATERIAL

| Item | Unit | Total |
|----------------------------|------|-------|
| Preformed Joint Strip Seal | Foot | 472 |

FILE NAME : \\S:\Bgn. Files\B16682\60V27.dgn

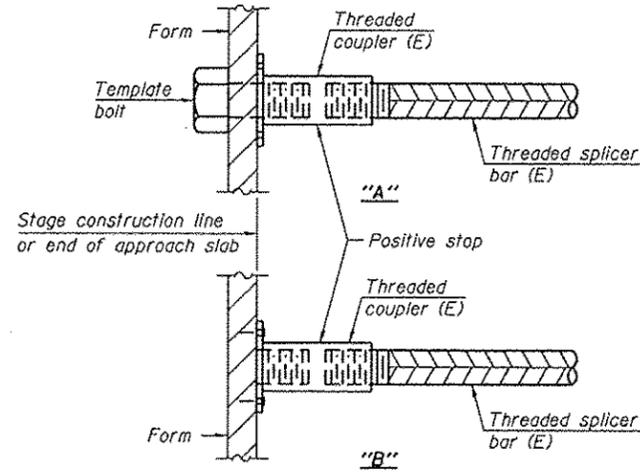


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1/2" + thread length

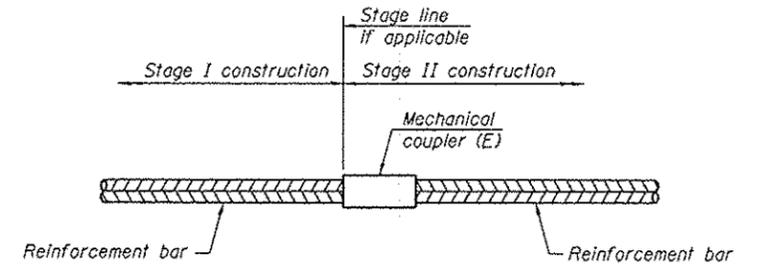
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

| Location | Bar size | No. assemblies required | Minimum lap length |
|----------|----------|-------------------------|--------------------|
| W. Abut. | #5 | 16 | 3'-4" |
| W. Abut. | #6 | 4 | 4'-9" |
| Pier 1 | #5 | 32 | 3'-4" |
| Pier 2 | #5 | 32 | 3'-4" |
| Pier 3 | #5 | 32 | 3'-4" |
| Pier 4 | #5 | 32 | 3'-4" |
| E. Abut. | #5 | 16 | 3'-4" |
| E. Abut. | #6 | 4 | 4'-9" |



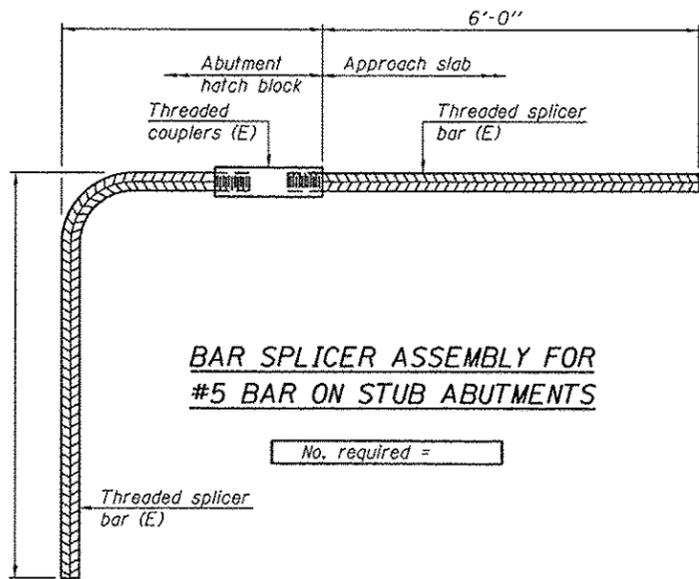
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

| Location | Bar size | No. assemblies required |
|----------|----------|-------------------------|
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |



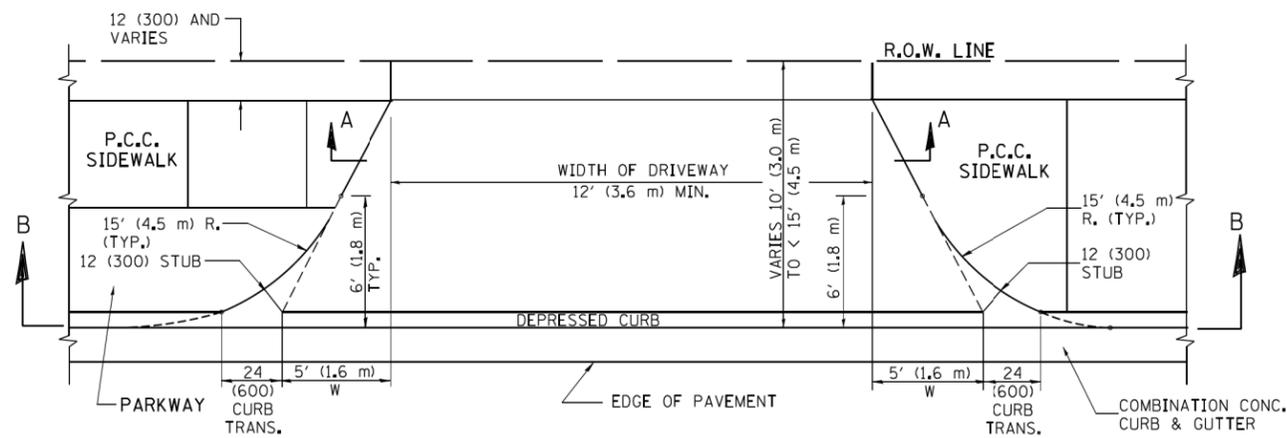
NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

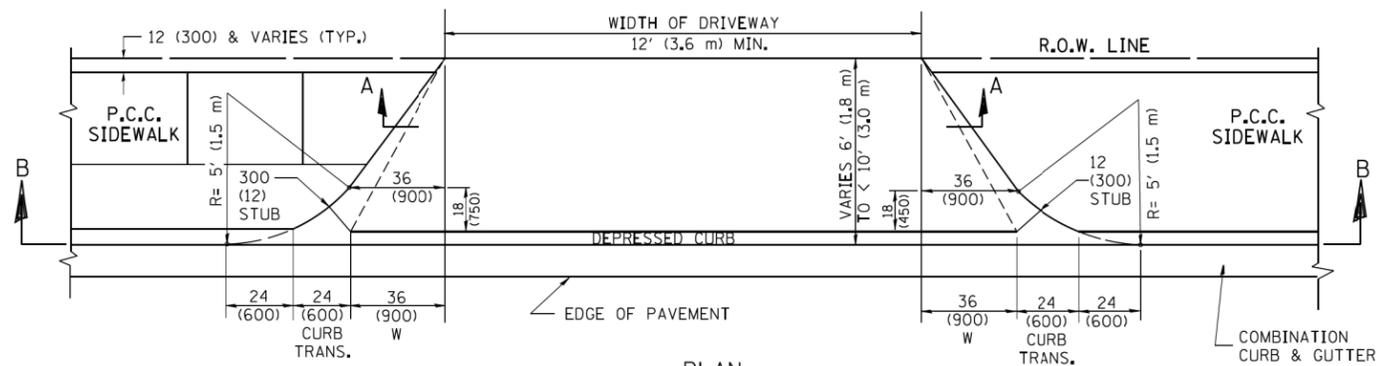
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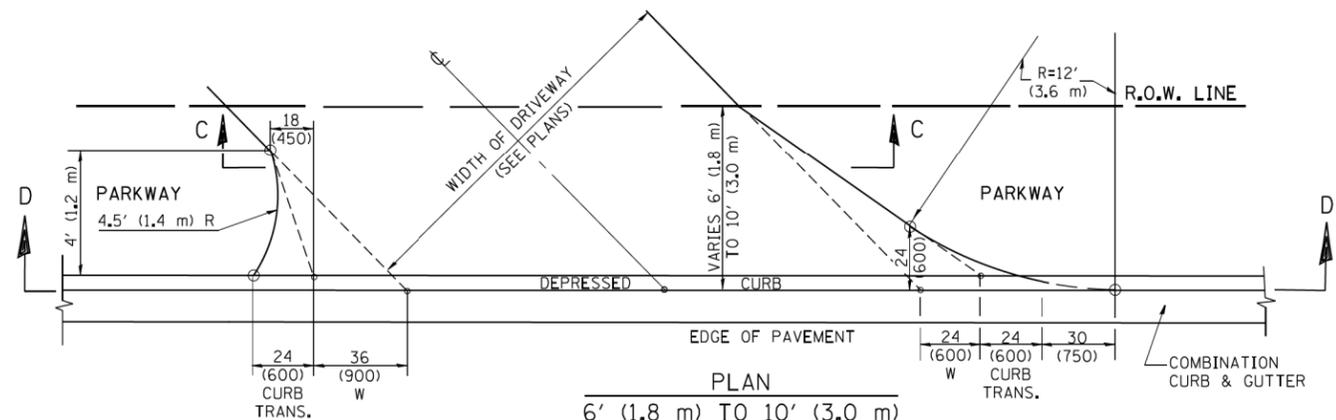
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|--------|--------------|-----------|
| 341 | (109B)BR | COOK | 45 | 36 |
| CONTRACT NO. 60V27 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



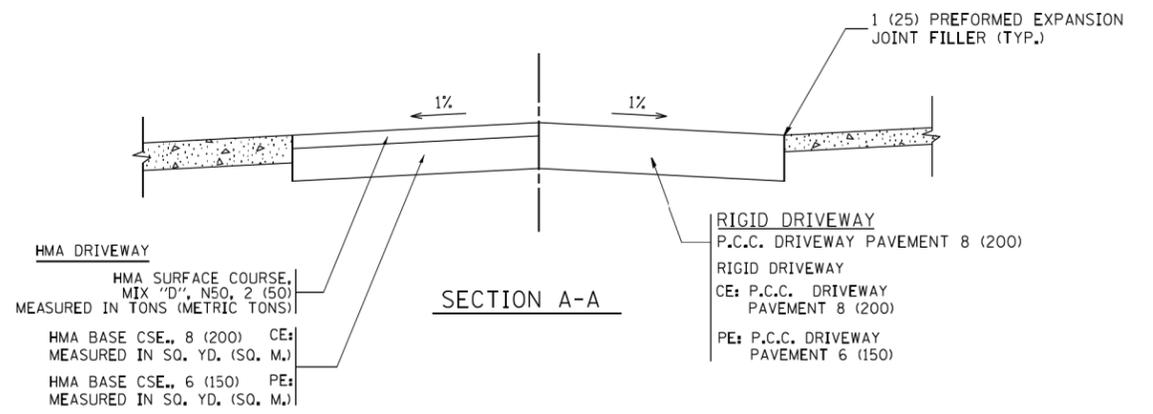
PLAN
10' (3.0 m) TO < 15' (4.5 m)



PLAN
6' (1.8 m) TO < 10' (3.0 m)

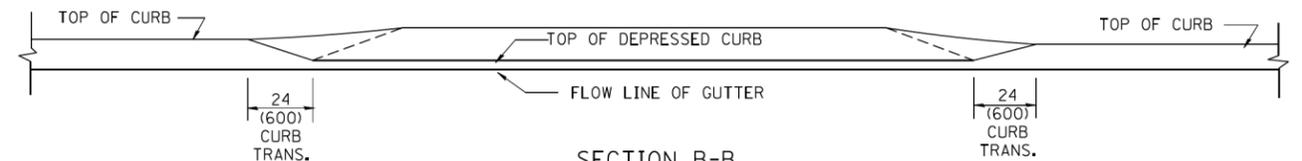


PLAN
6' (1.8 m) TO 10' (3.0 m)

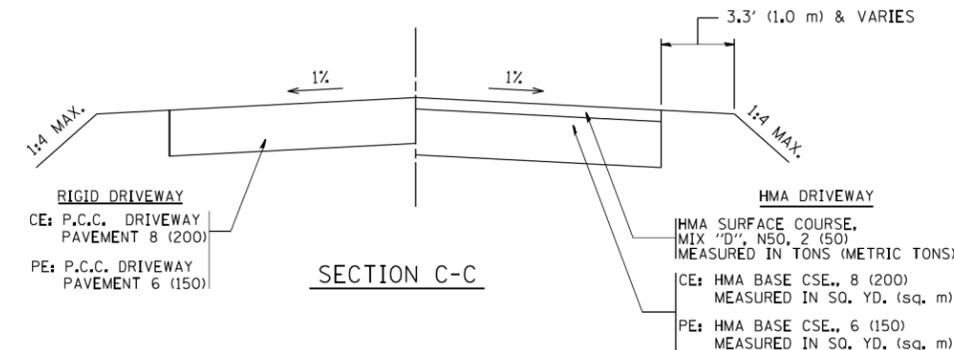


HMA DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)
HMA BASE CSE., 8 (200) MEASURED IN SQ. YD. (SQ. M.)
HMA BASE CSE., 6 (150) MEASURED IN SQ. YD. (SQ. M.)

RIGID DRIVEWAY
P.C.C. DRIVEWAY PAVEMENT 8 (200)
RIGID DRIVEWAY CE: P.C.C. DRIVEWAY PAVEMENT 8 (200)
PE: P.C.C. DRIVEWAY PAVEMENT 6 (150)

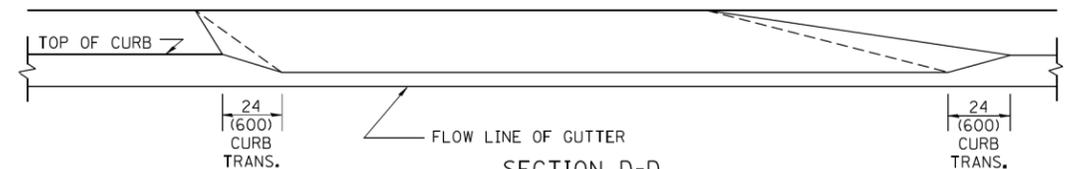


SECTION B-B



RIGID DRIVEWAY
CE: P.C.C. DRIVEWAY PAVEMENT 8 (200)
PE: P.C.C. DRIVEWAY PAVEMENT 6 (150)

HMA DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)
CE: HMA BASE CSE., 8 (200) MEASURED IN SQ. YD. (SQ. M.)
PE: HMA BASE CSE., 6 (150) MEASURED IN SQ. YD. (SQ. M.)



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME = I:\B274 - IDOT PTB 168 - 07 Various\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\DI60V27-ht-DISTANDARD.dgn

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| | | DRAWN - | REVISED - P. LOFLEUR 04-15-03 |
| | PLOT SCALE = 50.0000' / 1" | CHECKED - | REVISED - R. BORO 01-01-07 |
| | PLOT DATE = 10/28/2011 | DATE - 11-06-95 | REVISED - R. BORO 09-06-11 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| DRIVEWAY DETAILS | | | |
|---|-------------------------|--------------|--|
| DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m) | | | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|--------|--------------------|-----------|
| 341 | (109)BR | COOK | 45 | 37 |
| BD400-02 (BD-02) | | | CONTRACT NO. 60V27 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) * *

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

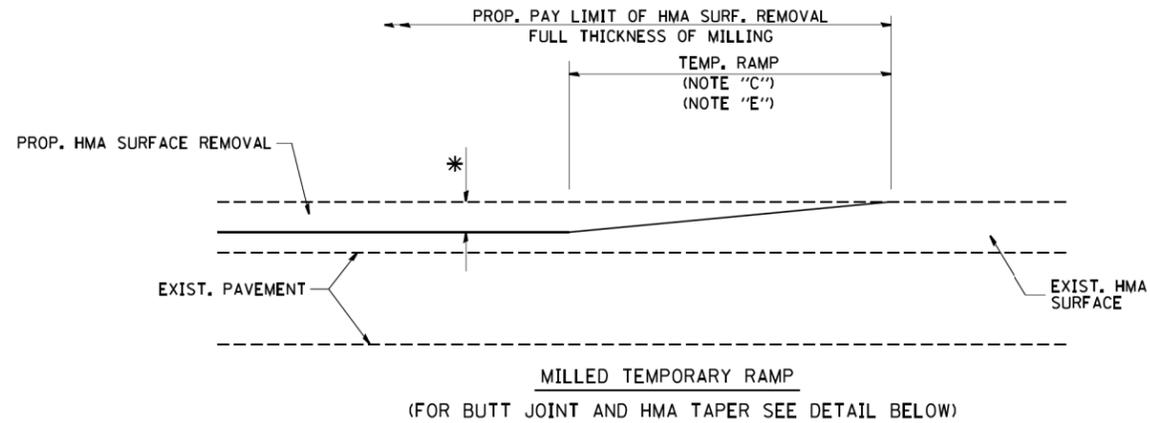
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| | PLOT DATE = 12/15/2009 | DATE - 03-11-94 | REVISED - R. BORO 12-15-09 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

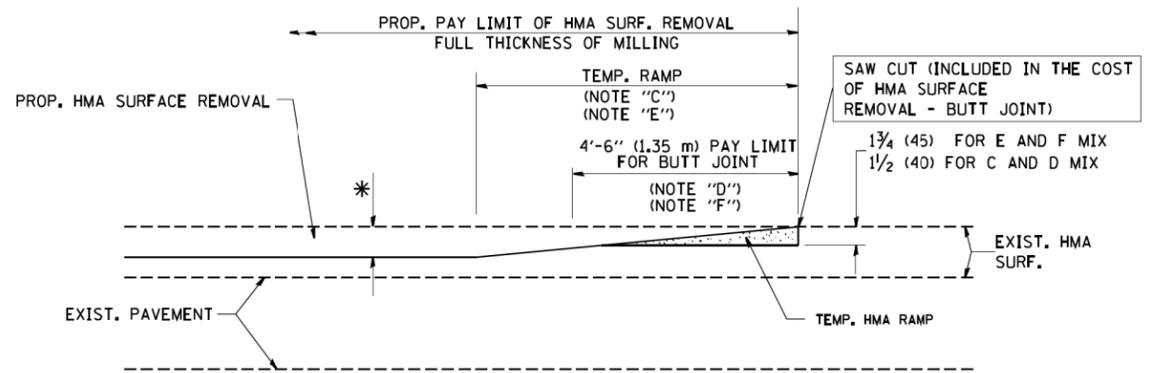
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| CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS STA. TO STA. |

| | | | | |
|---|----------|--------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 341 | (109B)BR | COOK | 45 | 38 |
| BD600-06 (BD-24) | | CONTRACT NO. 60V27 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



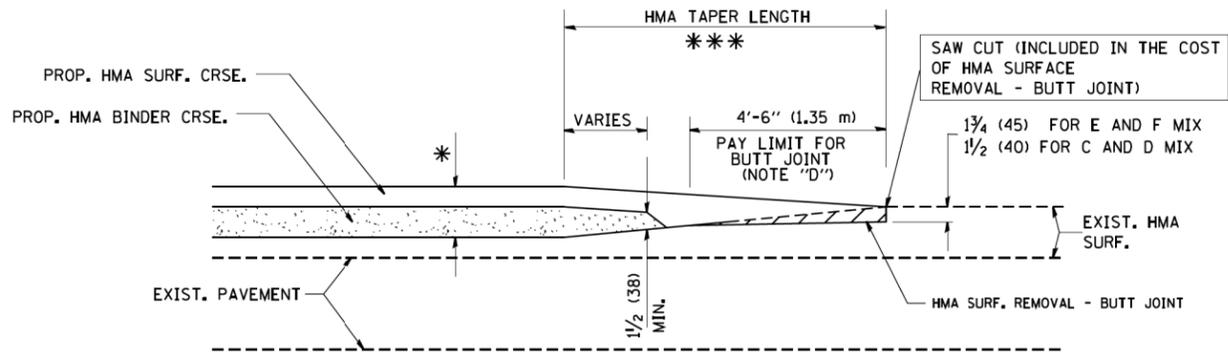
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

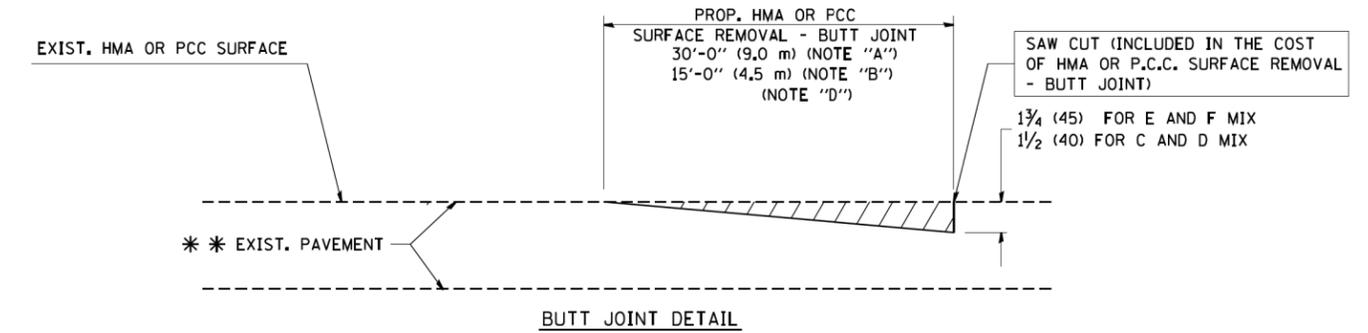


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

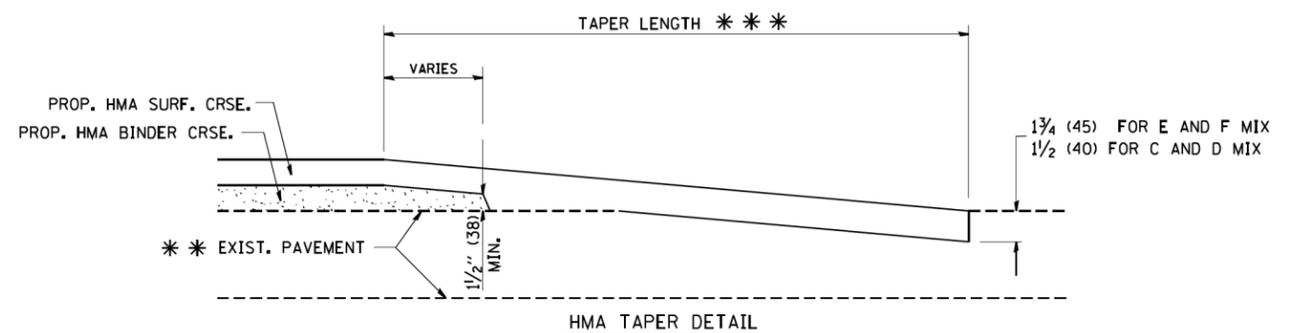
OPTION 2
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = I:\B274 - IDOT PTB 168 - 07 Various\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\DI60\27-ht-DISTANDARD.dgn

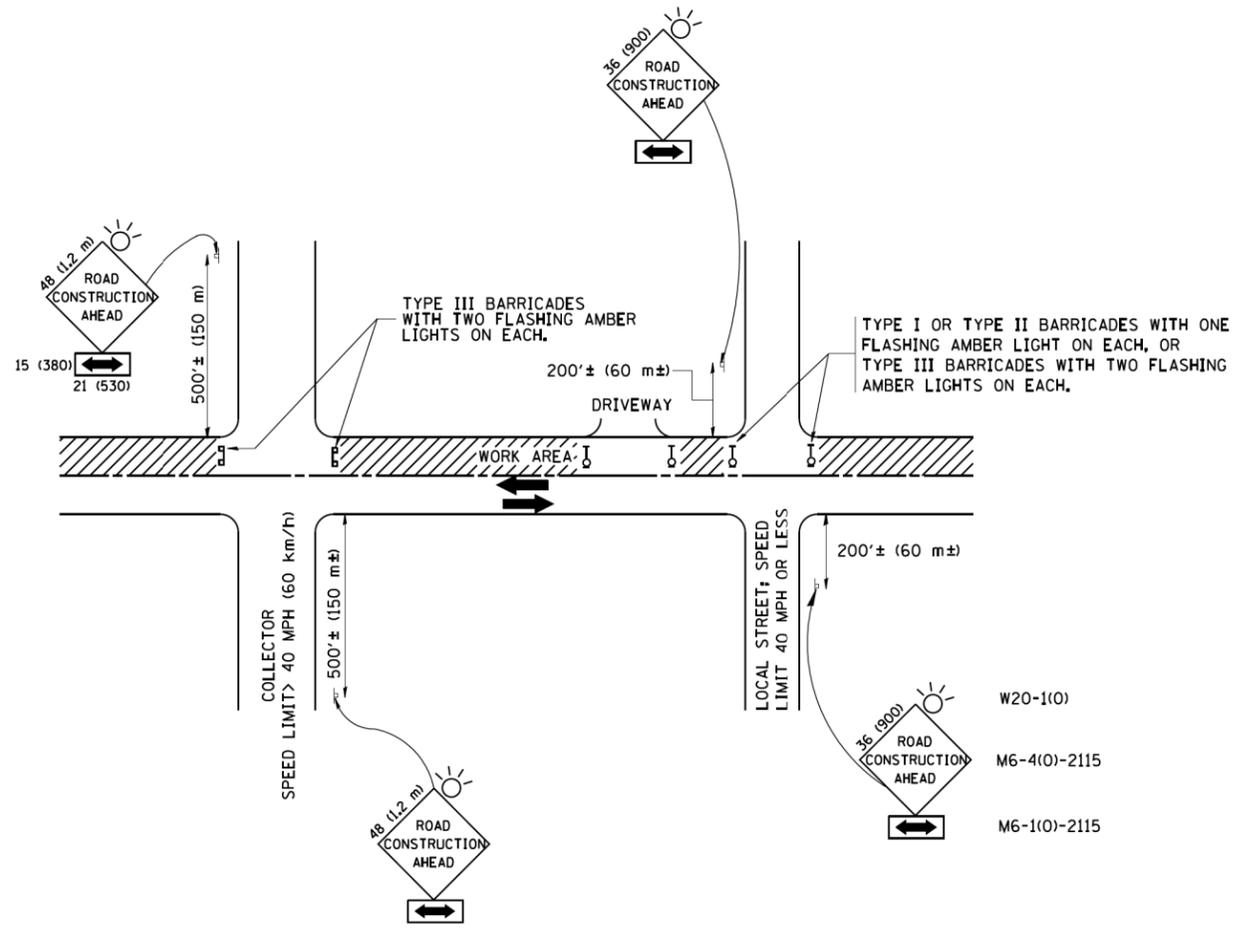
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| FILE NAME = I:\B274 - IDOT PTB 168 - 07 Various\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\DI60\27-ht-DISTANDARD.dgn | USER NAME = geglennob | DESIGNED - M. DE YONG | REVISED - R. SHAH 10-25-94 |
| | | DRAWN - | REVISED - A. ABBAS 03-21-97 |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - M. GOMEZ 04-06-01 |
| | PLOT DATE = 1/4/2008 | DATE - 06-13-90 | REVISED - R. BORO 01-01-07 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | |
|---|-------------------------|
| BUTT JOINT AND HMA TAPER DETAILS | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS |
| STA. | TO STA. |

| | | | | |
|---|------------------|--------------------|-----------------|--------------|
| F.A.P. RTE. 341 | SECTION (109B)BR | COUNTY COOK | TOTAL SHEETS 45 | SHEET NO. 39 |
| BD400-05 BD32 | | CONTRACT NO. 60V27 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

FILE NAME = I:\B274 - IDOT PTB 168 - 07 Verritus\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\DI60V27-ht-DISTANDARD.dgn



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

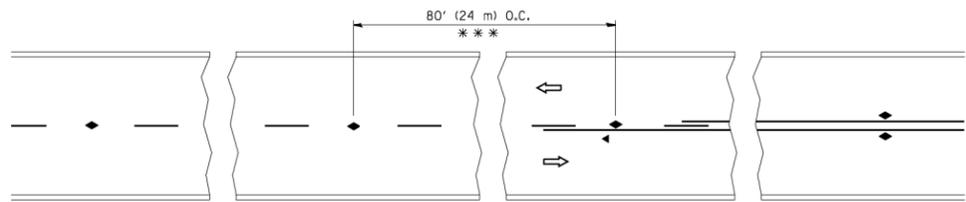
All dimensions are in millimeters (Inches) unless otherwise shown.

| | | | |
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| FILE NAME = I:\B274 - IDOT PTB 168 - 07 Verritus\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\DI60V27-ht-DISTANDARD.dgn | USER NAME = gegl1enobt | DESIGNED - LHA | REVISED - J. OBERLE 10-18-95 |
| | | DRAWN - | REVISED - A. HOUSEH 03-06-96 |
| | PLOT SCALE = 50.000' / IN. | CHECKED - | REVISED - A. HOUSEH 10-15-96 |
| | PLOT DATE = 1/4/2008 | DATE - 06-89 | REVISED - T. RAMMACH 01-06-00 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

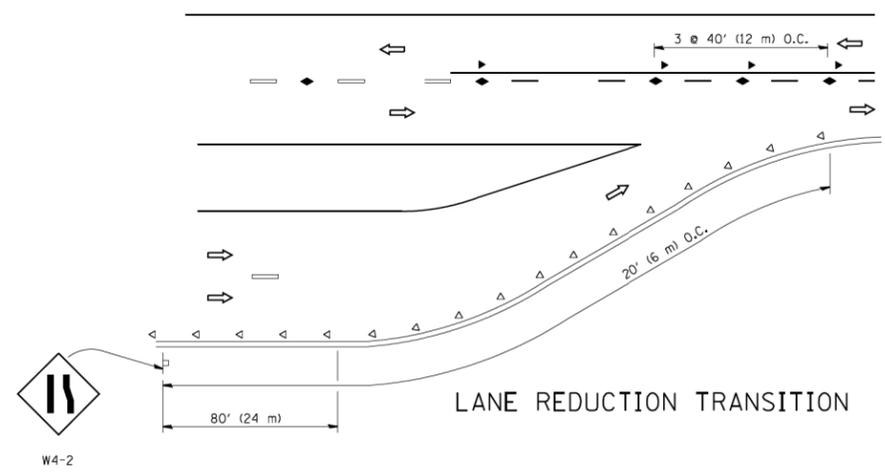
| | | | |
|--|-------------------------|------|---------|
| TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS | | | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

| | | | | |
|---|------------------|-------------|---------------------------|--------------|
| F.A.P. RTE. 341 | SECTION (109B)BR | COUNTY COOK | TOTAL SHEETS 45 | SHEET NO. 40 |
| TC-10 | | | CONTRACT NO. 60V27 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

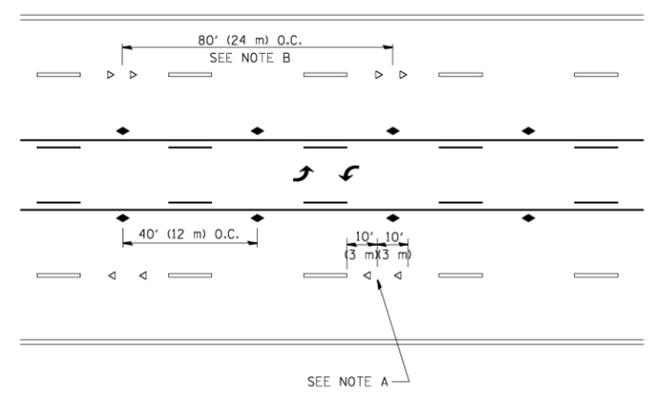


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

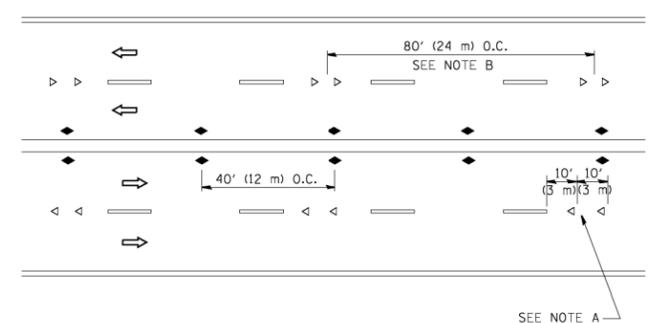
TWO-LANE/TWO-WAY



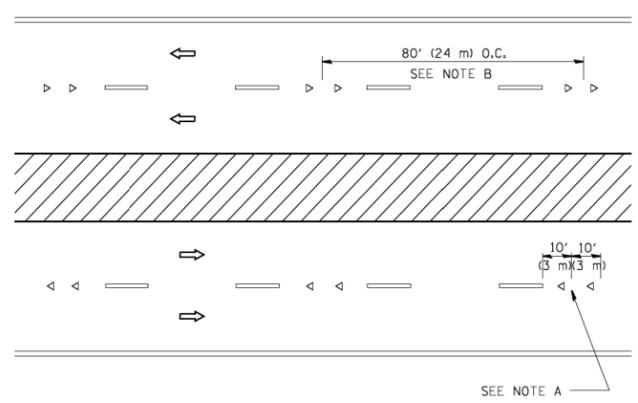
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

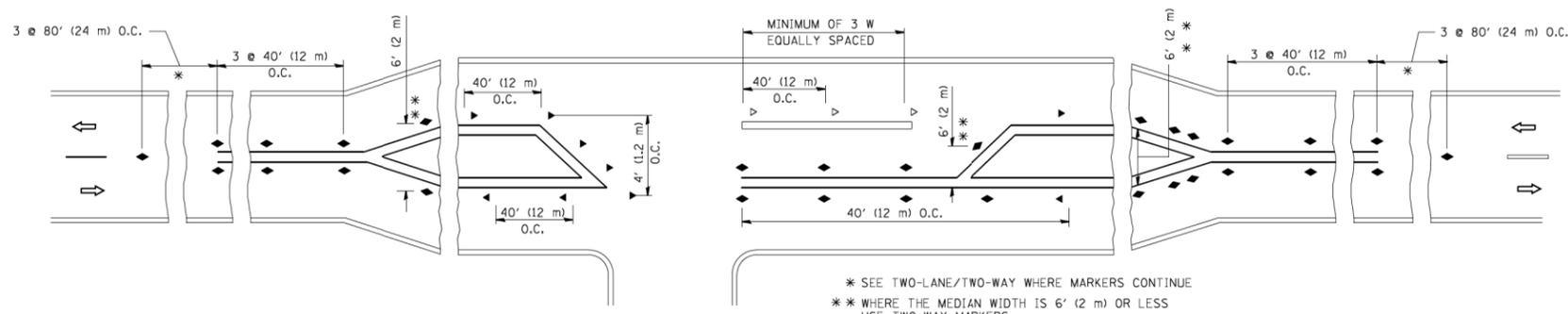
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

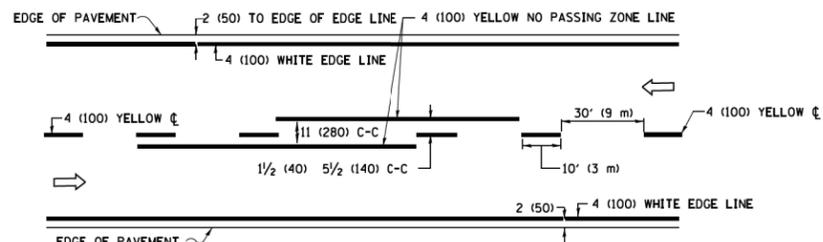
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| FILE NAME = I:\B274 - IDOT PTB 168 - 07 Varricus\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\DI60V27-ht-DISTANDSTD.dgn | USER NAME = lveysa | DESIGNED - | REVISED - T. RAMMACHER 09-19-94 |
| | | DRAWN - | REVISED - T. RAMMACHER 03-12-99 |
| | PLOT SCALE = 50.000' / IN. | CHECKED - | REVISED - T. RAMMACHER 01-06-00 |
| | PLOT DATE = 3/2/2011 | DATE - | REVISED - C. JUCIUS 09-09-09 |

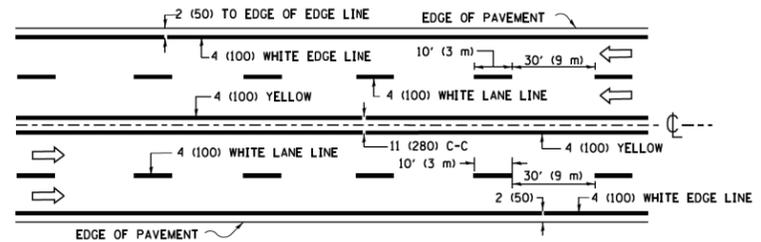
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

| TYPICAL APPLICATIONS | |
|--|--------------------------------------|
| RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS STA. TO STA. |

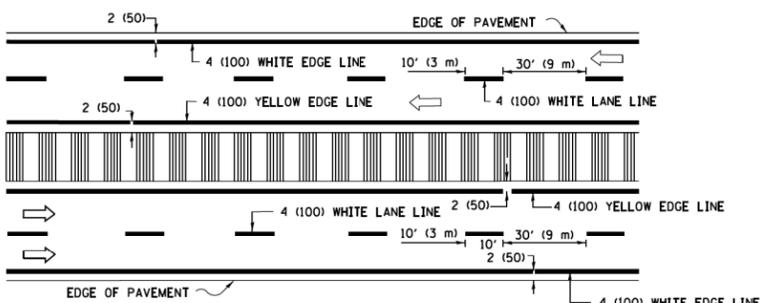
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------|--------|--------------------|-----------|
| 341 | (109B)BR | COOK | 45 | 41 |
| TC-11 | | | CONTRACT NO. 60V27 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



2-LANE ROADWAY

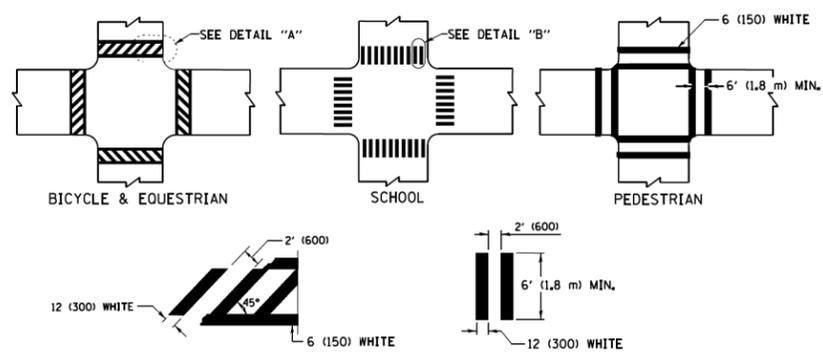


MULTI-LANE UNDIVIDED



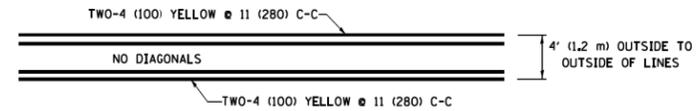
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

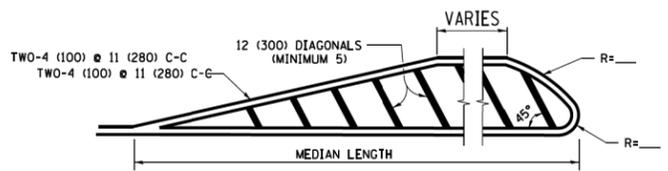


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

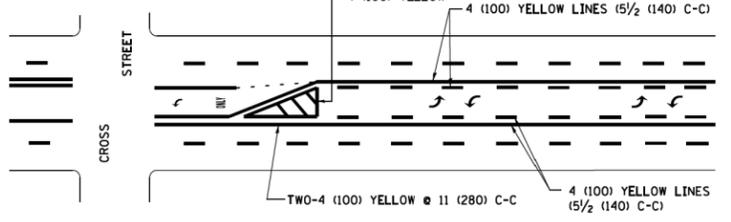


4' (1.2 m) WIDE MEDIANS ONLY



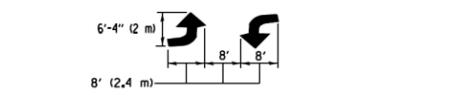
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**

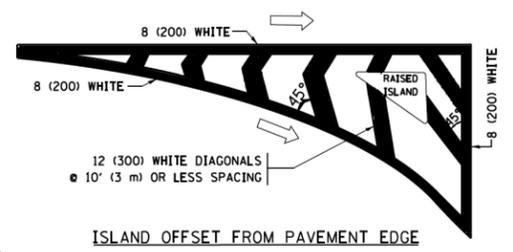
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



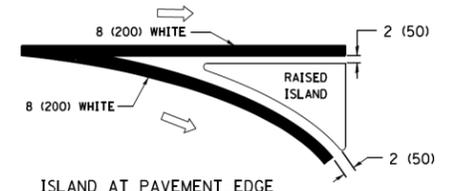
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

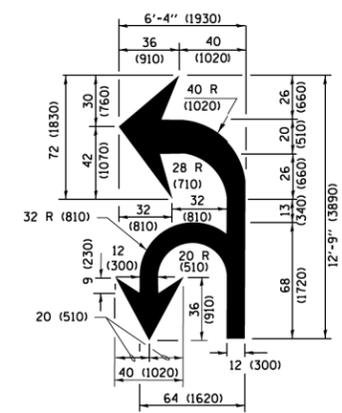
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



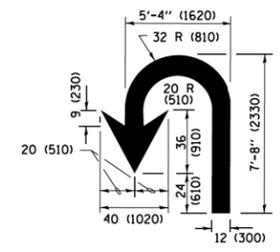
ISLAND OFFSET FROM PAVEMENT EDGE



**ISLAND AT PAVEMENT EDGE
TYPICAL ISLAND MARKING**



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

| D(FT) | SPEED LIMIT |
|-------|-------------|
| 345 | 30 |
| 425 | 35 |
| 500 | 40 |
| 580 | 45 |
| 665 | 50 |
| 750 | 55 |

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING /REMARKS |
|---|--|------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 @ 4 (100) | SOLID SOLID | YELLOW YELLOW | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MEDIANS IN YELLOW |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| CORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" |
| SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8') | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h)) |
| U TURN ARROW | SEE DETAIL | SOLID | WHITE | 16.3 SF |
| 2 ARROW COMBINATION LEFT AND U TURN | SEE DETAIL | SOLID | WHITE | 30.4 SF |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = ia.B274 - IDOT PTB 168 - 07 Varricus\B274.02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\DI60V27-ht-DISTANDARD.dgn

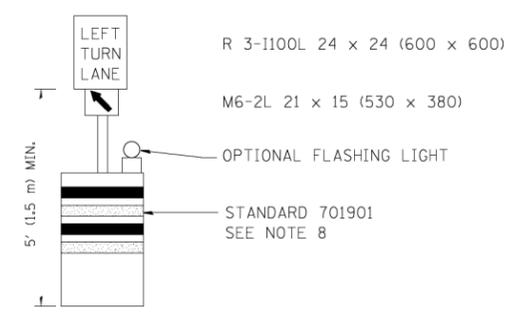
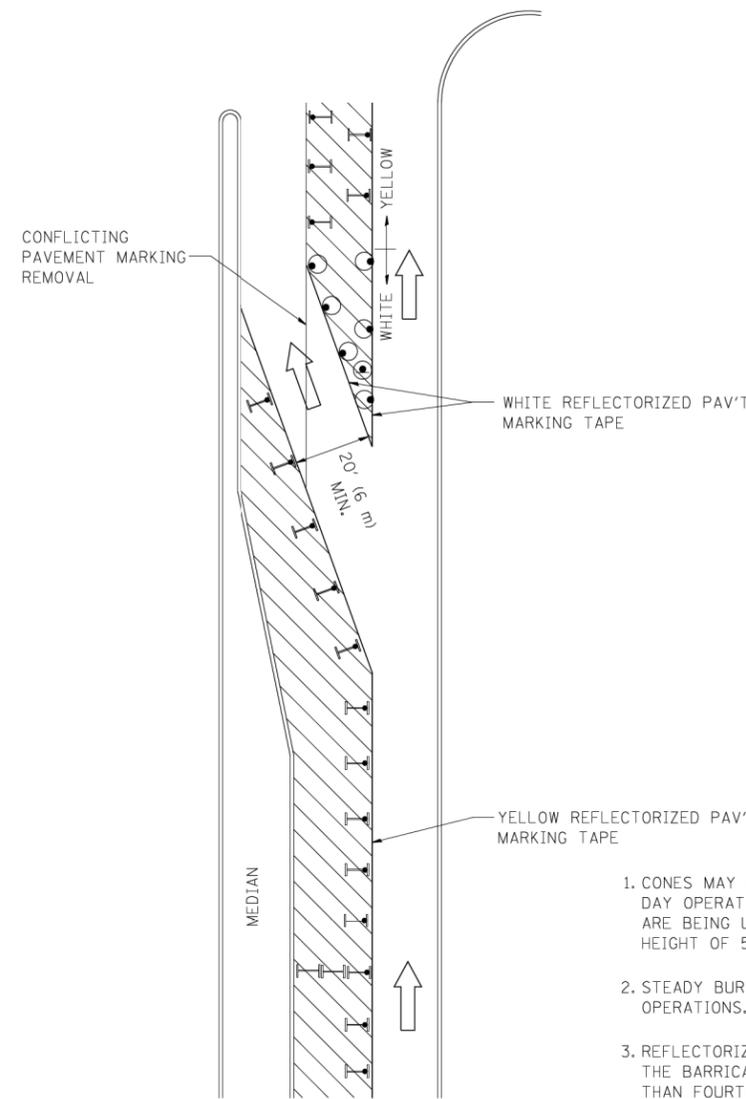
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| pw\1\084E8ID\INTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\Dist 1\CADD\CADD Sheets\to13.dgn | | DRAWN - | REVISED - C. JUCIUS 07-01-13 |
| Default | PLOT SCALE = 50.000' / in. | CHECKED - | REVISED - C. JUCIUS 12-21-15 |
| | PLOT DATE = 4/13/2016 | DATE - 03-19-90 | REVISED - C. JUCIUS 04-12-16 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | | |
|----------------------------------|---------|-------------|--------------|
| DISTRICT ONE | | | |
| TYPICAL PAVEMENT MARKINGS | | | |
| SCALE: NONE | SHEET 1 | OF 1 SHEETS | STA. TO STA. |

| | | | | |
|---------------------------|----------|--------|--------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 341 | (109)B/R | COOK | 45 | 42 |
| TC-13 | | | CONTRACT NO. 60V27 | |
| ILLINOIS FED. AID PROJECT | | | | |

FILE NAME = I:\B274 - IDOT PTB 168 - 07 Various\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\DI60V27-ht-DISTANDS.dgn



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

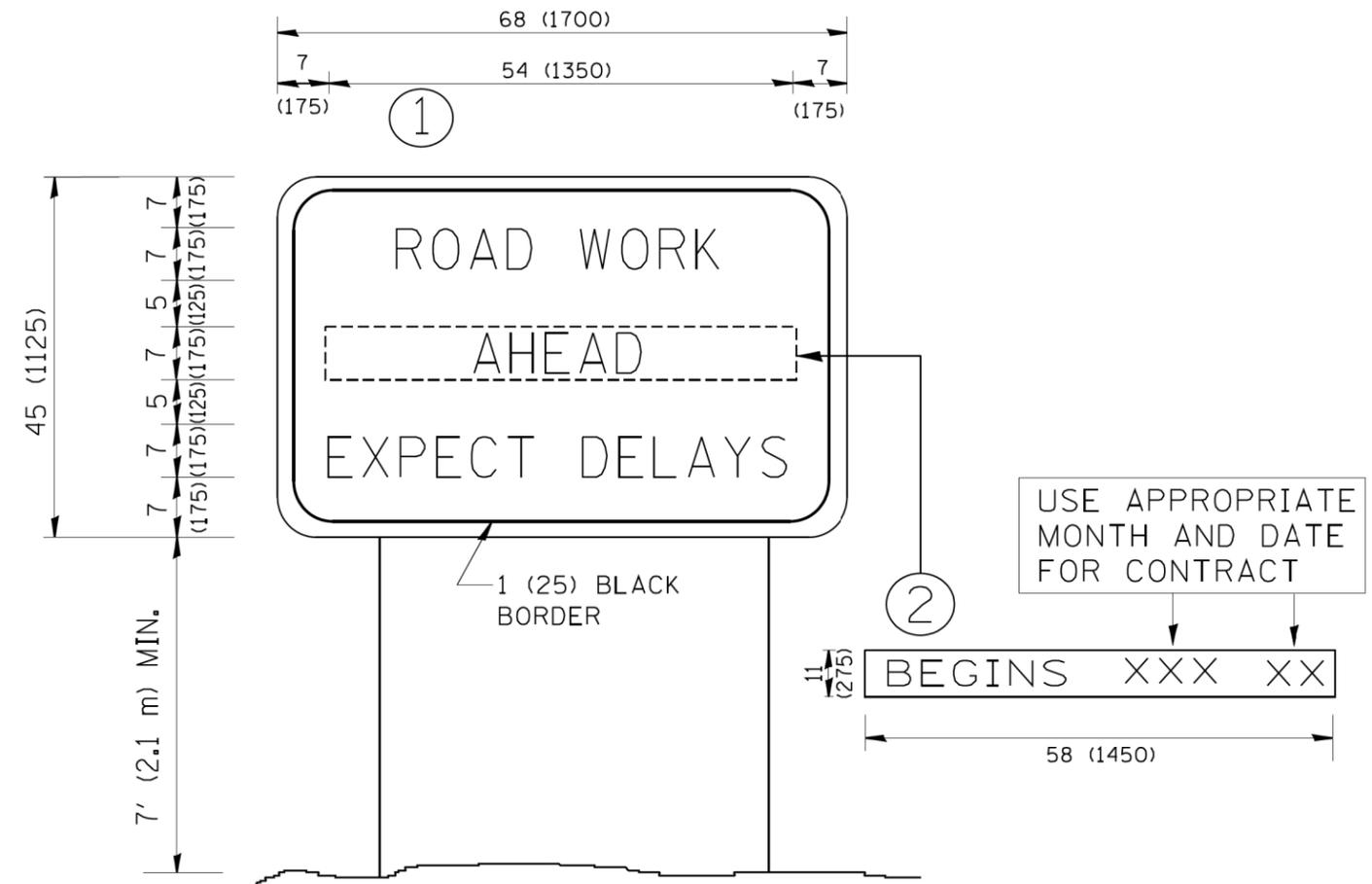
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| | PLOT SCALE = 49.9999 / / IN. | REVISED - A. HOUSEH 10-12-96 | REVISED - |
| | PLOT DATE = 9/14/2009 | REVISED -T. RAMMACHER 01-06-00 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | | |
|--|-------------------------|------|---------|
| TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) | | | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

| | | | | |
|---|----------|--------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 341 | (109B)BR | COOK | 45 | 43 |
| TC-14 | | CONTRACT NO. 60V27 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

FILE NAME = I:\B274 - IDOT PTB 168 - 07 Verritus\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\DI60V27-ht-DISTANDS.dgn



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | |
|--|-----------------------------|------------|---------------------------------|
| FILE NAME = I:\B274 - IDOT PTB 168 - 07 Verritus\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\DI60V27-ht-DISTANDS.dgn | USER NAME = gegl1enobt | DESIGNED - | REVISED - R, MIRS 09-15-97 |
| | | DRAWN - | REVISED - R, MIRS 12-11-97 |
| | PLOT SCALE = 50.000 ' / IN. | CHECKED - | REVISED - T. RAMMACHER 02-02-99 |
| | PLOT DATE = 1/4/2008 | DATE - | REVISED - C. JUCIUS 01-31-07 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------|--------|--------------------|-----------|
| 341 | (109B)BR | COOK | 45 | 44 |
| TC-22 | | | CONTRACT NO. 60V27 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = I:\B274 - IDOT PTB 168 - 07 Various\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\DI60V27-ht-DISTANDS.dgn

| | | | |
|---|----------------------------|------------|------------------------------|
| FILE NAME = I:\B274 - IDOT PTB 168 - 07 Various\B274\02 - Touhy Ave over Des Plaines River\CADD\CADD Sheets\DI60V27-ht-DISTANDS.dgn | USER NAME = gaglianobt | DESIGNED - | REVISED - C. JUCIUS 02-15-07 |
| | | DRAWN - | REVISED - |
| | PLOT SCALE = 50.000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 12/13/2012 | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---|---------------------|--------------------|--------------------|-----------------|
| F.A.P. RTE. 341 | SECTION (109B)BR | COUNTY COOK | TOTAL SHEETS 45 | SHEET NO. 45 |
| TC-26 | | CONTRACT NO. 60V27 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

