

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DUPAGE	34	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO.	62G59	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROPOSED HIGHWAY PLANS
F.A.P. 870 /IL 53 (ROHLWING RD./COLUMBINE AVE.)
ST. CHARLES RD. TO SIDNEY AVE.
STANDARD OVERLAY.
ADA IMPROVEMENTS
SECTION NO.: 2018-031-RS-SW
PROJECT: NHPP-WSND (326)
DuPAGE COUNTY
C-91-243-18

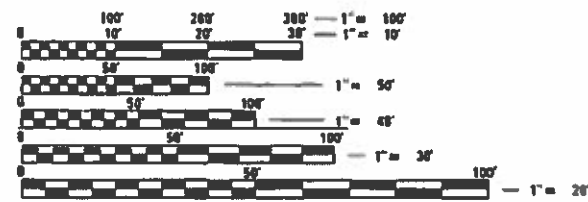
PROJECT IS LOCATED IN THE VILLAGES OF ADDISON AND LOMBARD

TRAFFIC DATA:

2017 ADT - 19000 TO 26300
 SPEED LIMIT - 35 TO 40 MPH

OMISSIONS:

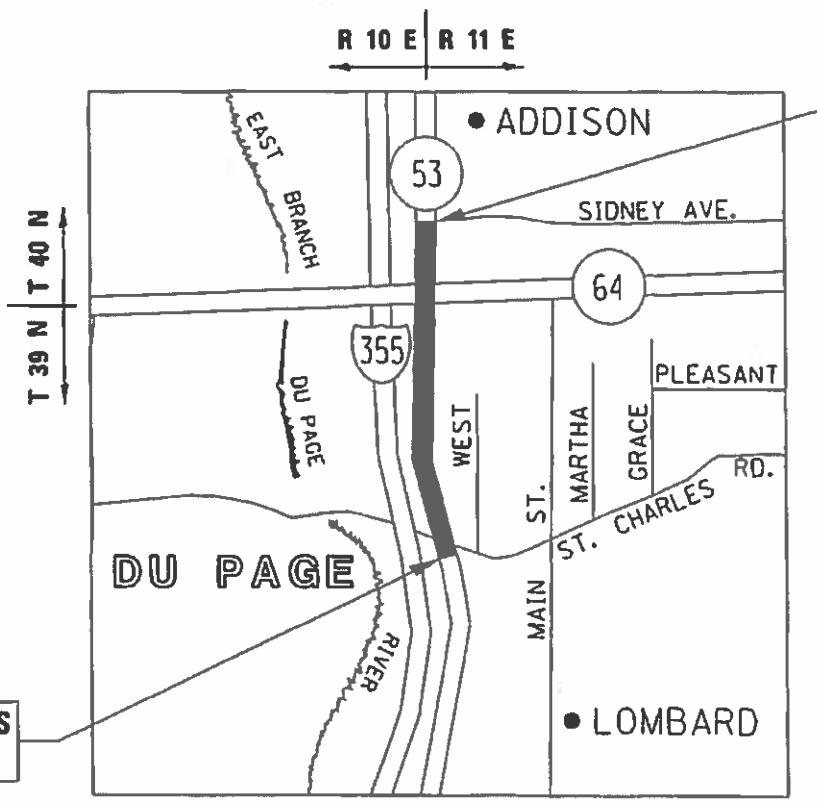
GREAT WESTERN TRAIL
 STA. 40+33 TO STA. 42+05



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811

PROJECT BEGINS STA. 16+54



PROJECT ENDS STA. 100+35

YORK, ADDISON, BLOOMINGDALE & MILTON TOWNSHIPS

GROSS LENGTH OF PROJECT - 8381 FEET - 1.58 MILES
 NET LENGTH OF PROJECT - 8209 FEET - 1.55 MILES

PROJECT ENGINEER: VESELIN VELICHKOV (847) 705-4432
 PROJECT MANAGER: FAWAD AQUEEL (847) 702-4247
 CONTRACT NO. 62G59

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUBMITTED March 19, 2019
Anthony J. Cassidy / *10/13*
 REGIONAL ENGINEER
Timothy J. [Signature]
 ENGINEER OF DESIGN AND ENVIRONMENT
Paul P. [Signature]
 DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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INDEX OF SHEETS		STATE STANDARDS		GENERAL NOTES:	
SHEET NO.	DESCRIPTION:				
1	COVER SHEET	000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS		BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR "CUAN" (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED).
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES	424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS		10 FEET (3 METERS) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
3-5	SUMMARY OF QUANTITIES	424006-04	DIAGONAL CURB RAMPS FOR SIDEWALKS		
6-11	EXISTING AND PROPOSED TYPICAL SECTIONS	442201-03	CLASS C AND D PATCHES		THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES AND THE VILLAGES OF ADDISON AND LOMBARD.
12-15	PROPOSED ROADWAY AND PAVEMENT MARKING PLANS	604001-04	FRAME AND LIDS, TYPE 1		
16-19	ADA SIDEWALK RAMP DETAILS	604006-05	FRAME AND GRATE, TYPE 3		THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
20	DETAILS FOR FRAMES AND LIDS ADJUSTMENTS WITH MILLING (BD-08)	606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER		ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
21	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)	701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE		ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
22	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)	701011-04	OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY		ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
23	BUTT JOINT AND HMA TAPER DETAILS (BD-32)	701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE		LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
24	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)	701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS		DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
25	TYPICAL APPLICATION FOR RAISED REFLECTIVE PAVEMENT MARKERS (TC-11)	701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS, DAY ONLY		IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
26	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)	701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS LESS THAN OR EQUAL TO 40MPH		FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENT SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT IN PART OF THIS CONTRACT.
27	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TC-14)	701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED		BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
28	SHORT-TREM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)	701502-09	URBAN LANE CLOSURE, 2L, 2W, WITH BI-DIRECTIONAL LEFT TURN LANE		WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/ h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/ h), WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING MACHINE IS SLOPED A MINIMUM (1:3).
29	DRIVEWAY ENTRANCE SIGNING (TC-26)	701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRANSVERSABLE MEDIAN		THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL CENTER SUPERVISOR FOR ARTERIALS AT KALPANA.KANNAN-HOSADURGA @ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
30	DISTRICT ONE - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)	701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BI-DIRECTIONAL LEFT TURN LANE		PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL. (TC-13)
31-33	DETECTOR LOOP LOCATION DETAILS	701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN		THE RESIDENT ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER, AT DON.CHIARUGI@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
34	ARTERIAL ROAD INFORMATION SIGNING (TC-22)	701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION		THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
		701801-06	LANE CLOSURE, MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE		THE CONTRACTOR SHALL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING ANY FORESTRY WORK FOR LAYOUT.
		701901-08	TRAFFIC CONTROL DEVICES		
		780001-05	TYPICAL PAVEMENT MARKINGS		
		886001-01	DETECTOR LOOP INSTALLATIONS		
		886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS		

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES 80% FED 20% STATE	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005				
20200100	EARTH EXCAVATION	CU YD	16	16				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	146	146				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	2	2				
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	2	2				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	2	2				
25200110	SODDING, SALT TOLERANT	SO YD	146	146				
25200200	SUPPLEMENTAL WATERING	UNIT	1	1				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	41934	41934				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	94	94				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4, 75, N50	TON	2563	2563				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	389	389				
42001300	PROTECTIVE COAT	SO YD	386	386				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	1146	1146				
*42400800	DETECTABLE WARNINGS	SO FT	104	104				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES 80% FED 20% STATE	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005				
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	62123	62123				
44000600	SIDEWALK REMOVAL	SO FT	1102	1102				
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SO FT	30	30				
44201811	CLASS D PATCHES, TYPE I, 14 INCH	SO YD	125	125				
44201815	CLASS D PATCHES, TYPE II, 14 INCH	SO YD	994	994				
44201819	CLASS D PATCHES, TYPE III, 14 INCH	SO YD	746	746				
44201821	CLASS D PATCHES, TYPE IV, 14 INCH	SO YD	622	622				
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	128	128				
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	5	5				
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	3	3				
*66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	16	16				
*66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2				
*66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1				
*66901002	ON-SITE MONITORING OF REGULATED SUBSTANCES	CAL DA	15	15				

* SPECIALTY ITEMS
 NON PARTICIPATING ITEMS

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES
FAP 870 /IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.
 SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DU PAGE	34	3
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62059	

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES 80% FED 20% STATE	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005				
*66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6				
67100100	MOBILIZATION	L SUM	1	1				
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1				
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1				
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1				
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1				
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	6654	6654				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	1109	1109				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES 80% FED 20% STATE	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005				
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	1009	1009				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	26269	26269				
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	4658	4658				
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	564	564				
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	296	296				
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	613	613				
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	3327	3327				
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	1009	1009				
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	26269	26269				
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	4658	4658				
*78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	564	564				
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	296	296				

* SPECIALTY ITEMS
 NON PARTICIPATING ITEMS

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
FAP 870 /IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DU PAGE	34	4
CONTRACT NO. 62G59				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: SHEET NO. OF SHEETS STA. TO STA.

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES 80% FED 20% STATE	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005				
*7800650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	613	613				
*78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	688	688				
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	646	646				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	581	581				
*88600600	DETECTOR LOOP REPLACEMENT	FOOT	1152	1152				
*89502376	REBUILD EXISTING HANDHOLE	EACH	1	1				
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1				
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	32	32				
*X2700004	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 7"	FOOT	344	344				
X4060004	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80	TON	6089	6089				
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	784	784				
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	18	18				
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	1109	1109				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES 80% FED 20% STATE	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005				
*X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	1032	1032				
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	873	873				
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	12	12				
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4				

* SPECIALTY ITEMS
 NON PARTICIPATING ITEMS

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
FAP 870 /IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.
 SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-R5-SW	DJ PAGE	34	5
CONTRACT NO. 62G59				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LEGEND:

- ① EXISTING P. C. C. PAVEMENT ±10"
 - ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
 - ③ EXISTING P. C. C. BARRIER MEDIAN
 - ④ EXISTING AGGREGATE SHOULDER
 - ⑤ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - ⑥ EXISTING STABILIZED SUB-BASE
 - ⑦ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
 - ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
 - ⑨ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80, 1¾"
 - ⑩ PROPOSED GRADING AND SHAPING SHOULDERS
 - ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes	OMP
PATCHING		
CLASS "D" PATCHES (HMA BINDER COURSE, IL-19MM)	4% @ 70 GYR.	OC / OA
PAVEMENT RESURFACING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80	3.5% @ 80 GYR.	OCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% @ 50 GYR.	OCP
OMP DESIGNATION: QUALITY CONTROL FOR PERFORMANCE (OCP); QUALITY CONTROL / QUALITY ASSURANCE (OC / OA); PAY FOR PERFORMANCE (PFP)		

NOTES:

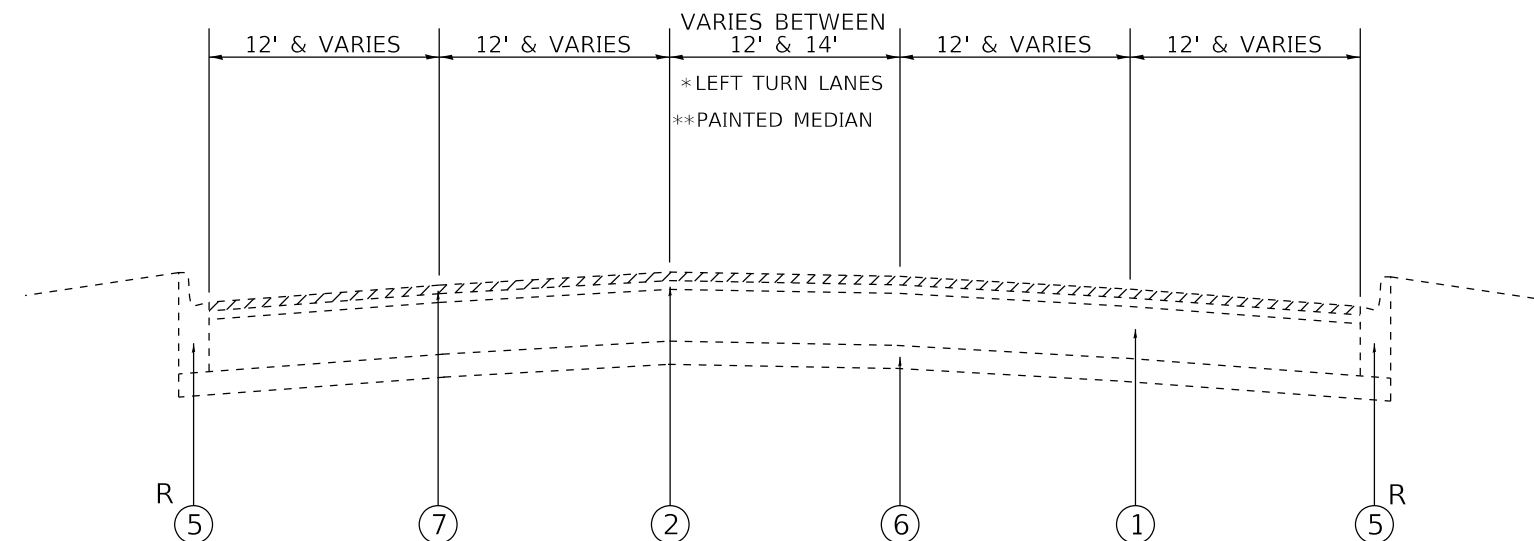
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE QUANTITIES IS 112 LBS./SQ. YD./ IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

THE MILLING SHALL BE DONE PRIOR TO PATCHING

QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

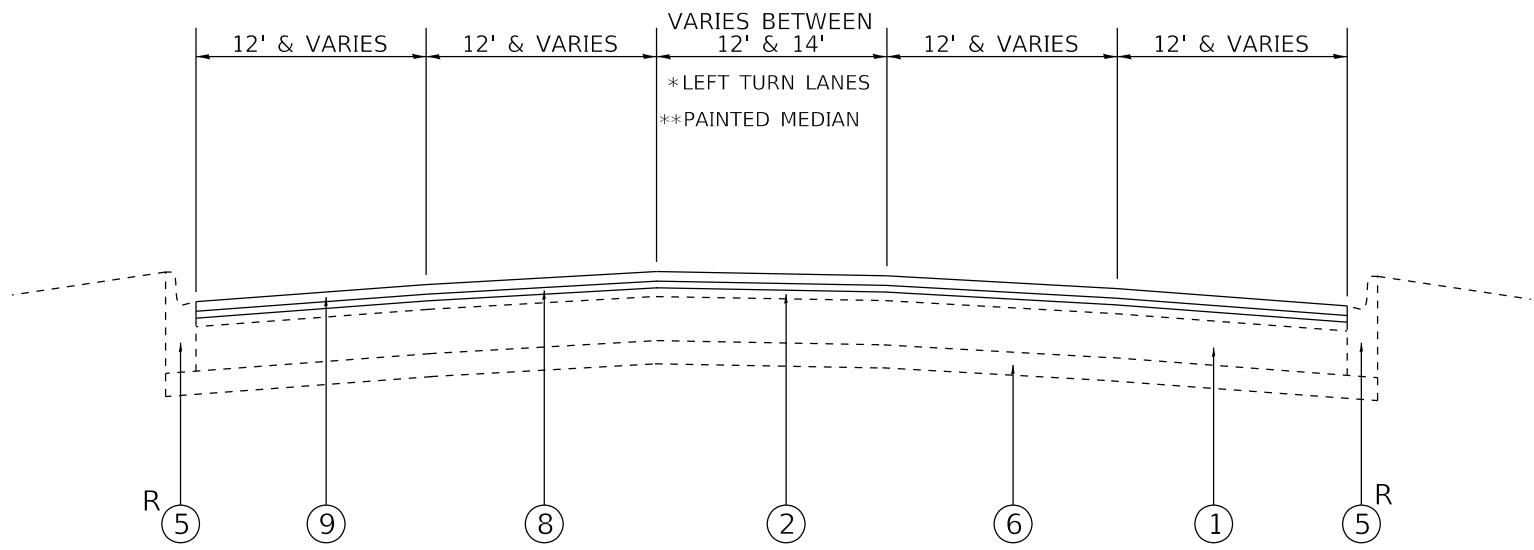
FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS



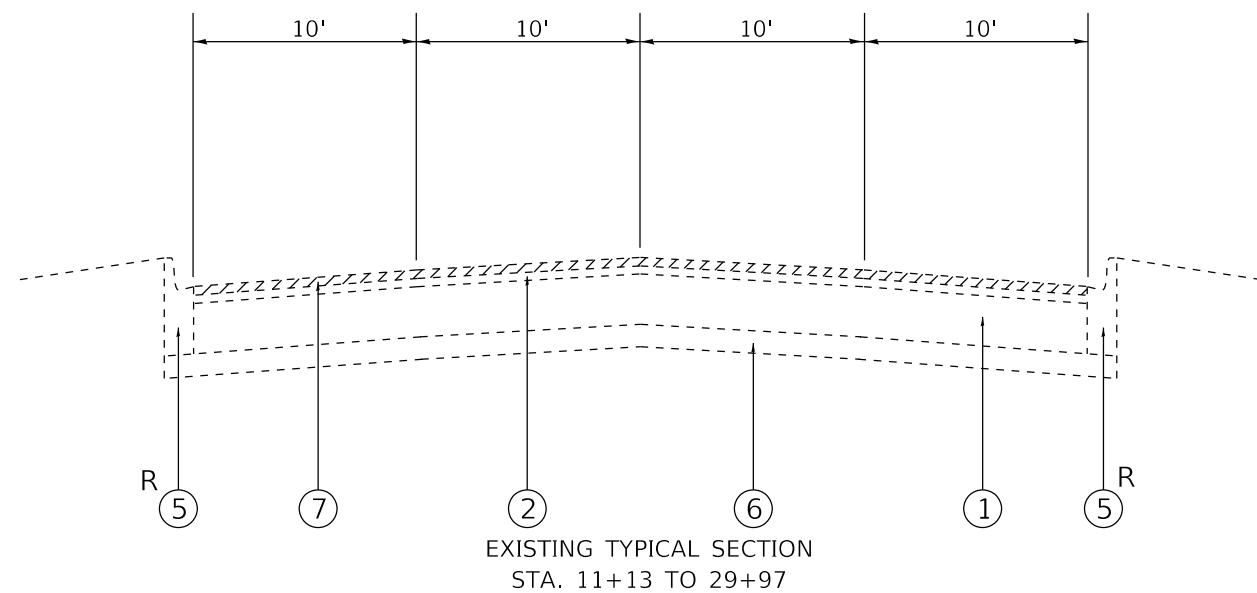
EXISTING TYPICAL SECTION
STA. 3+00 TO 11+13
STA. 70+81 TO 81+35

LEGEND:

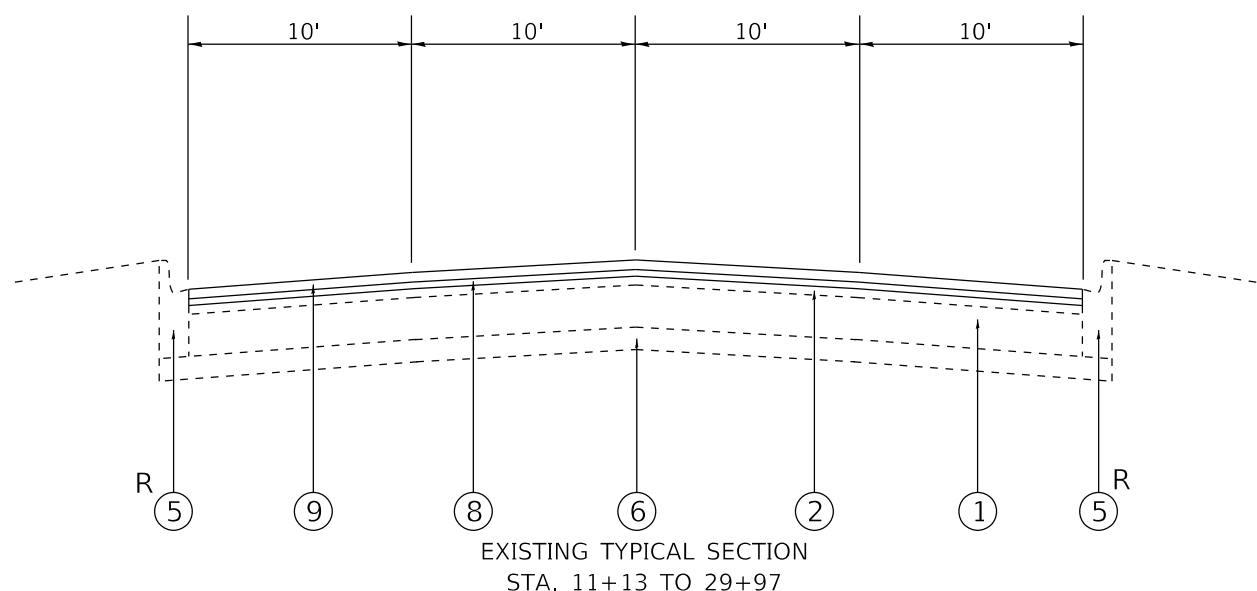
- *STA. 3+11 TO 4+81
- STA. 6+02 TO 7+70
- STA. 75+17 TO 78+37
- STA. 79+43 TO 80+81
- ** STA. 7+70 TO 11+13
- STA. 70+81 TO 73+52



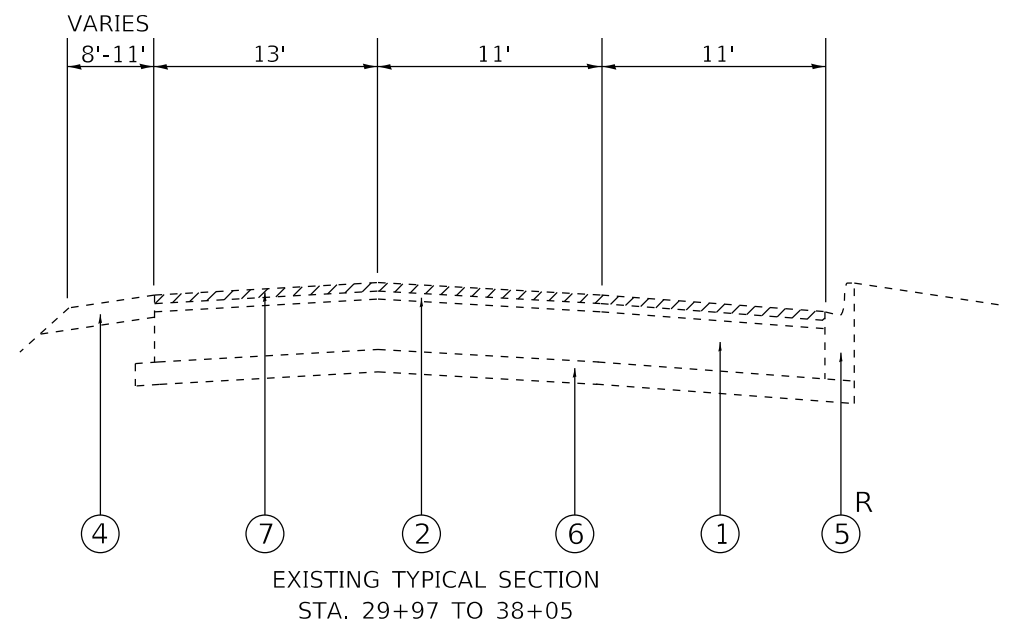
PROPOSED TYPICAL SECTION
STA. 3+00 TO 11+13
STA. 70+81 TO 81+35



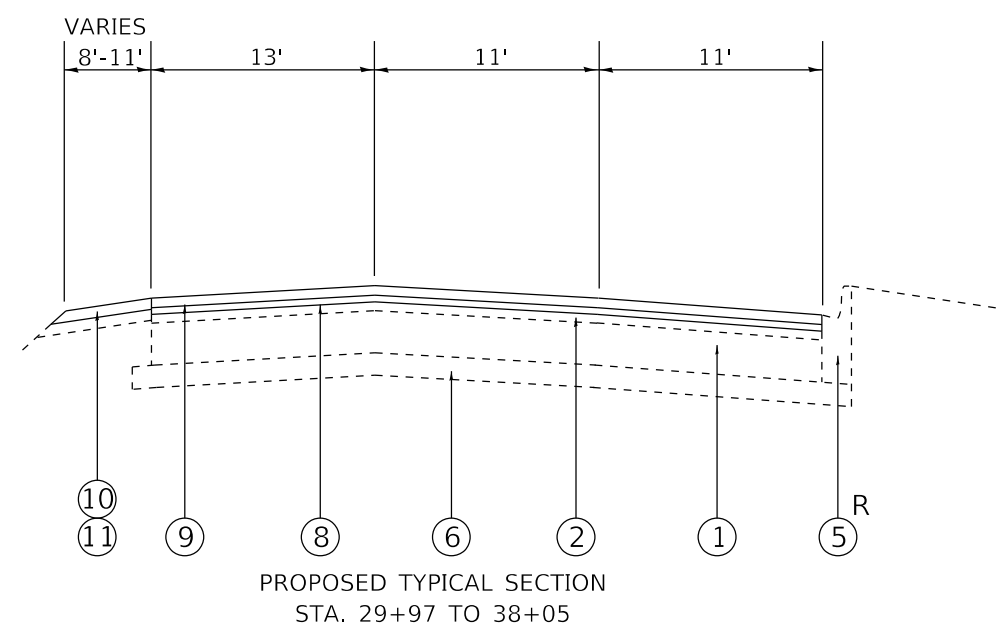
EXISTING TYPICAL SECTION
STA. 11+13 TO 29+97



EXISTING TYPICAL SECTION
STA. 11+13 TO 29+97



EXISTING TYPICAL SECTION
STA. 29+97 TO 38+05



PROPOSED TYPICAL SECTION
STA. 29+97 TO 38+05

LEGEND:

- ① EXISTING P. C. C. PAVEMENT ±10"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
- ③ EXISTING P. C. C. BARRIER MEDIAN
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑥ EXISTING STABILIZED SUB-BASE
- ⑦ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
- ⑨ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80, 1¾"
- ⑩ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

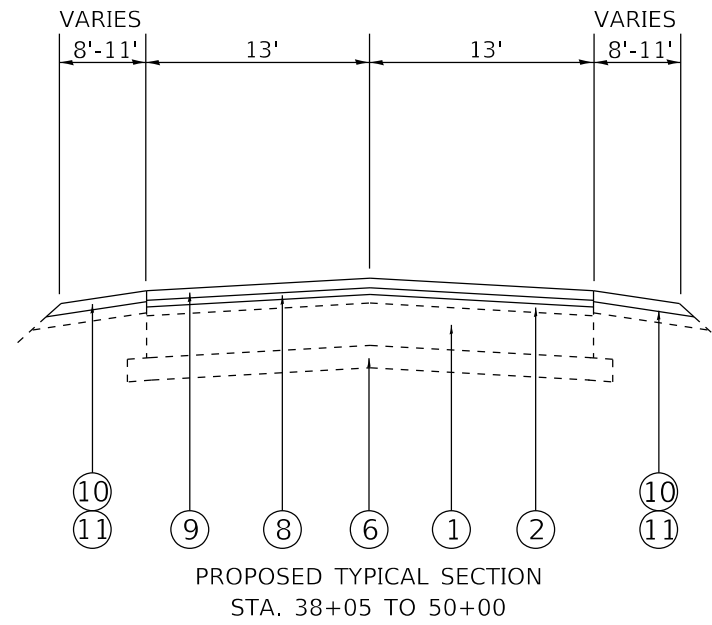
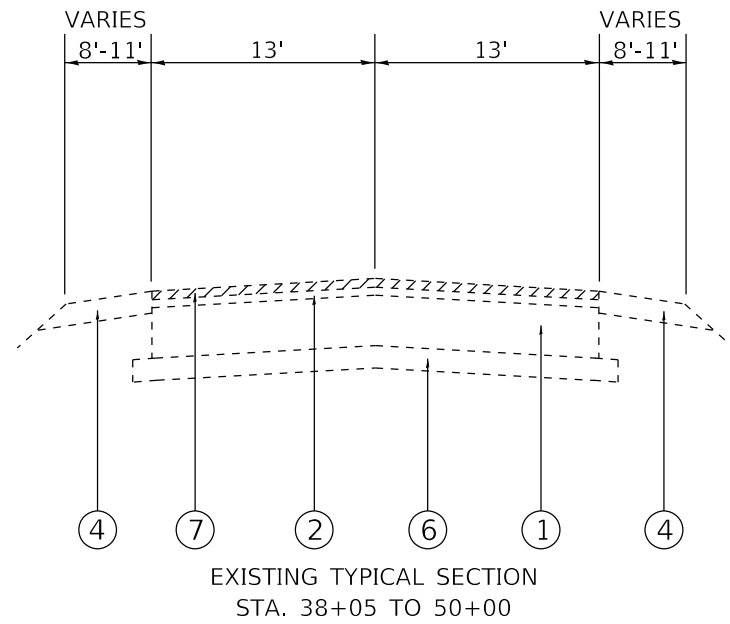
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

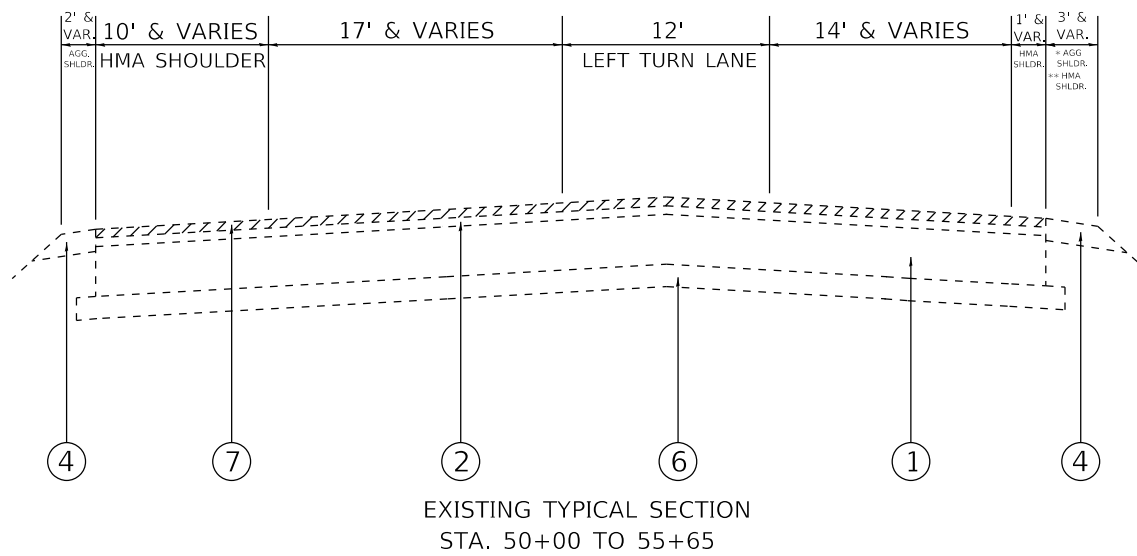
**EXISTING AND PROPOSED TYPICAL SECTIONS
FAP 870 /IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DU PAGE	34	7
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62G59	

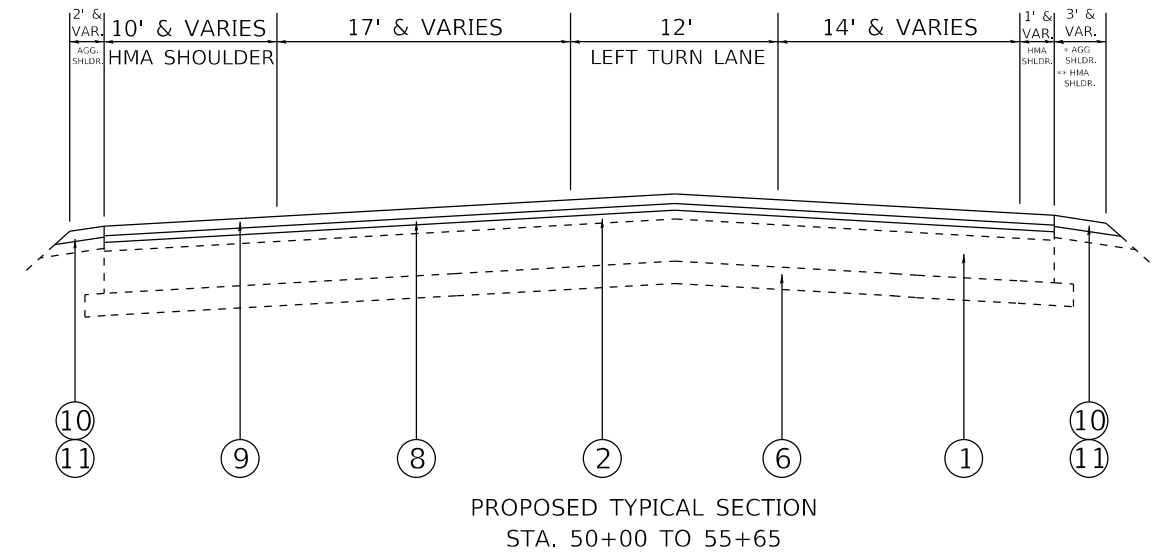
SHEET NO. 1 OF 1 SHEETS STA. TO STA.



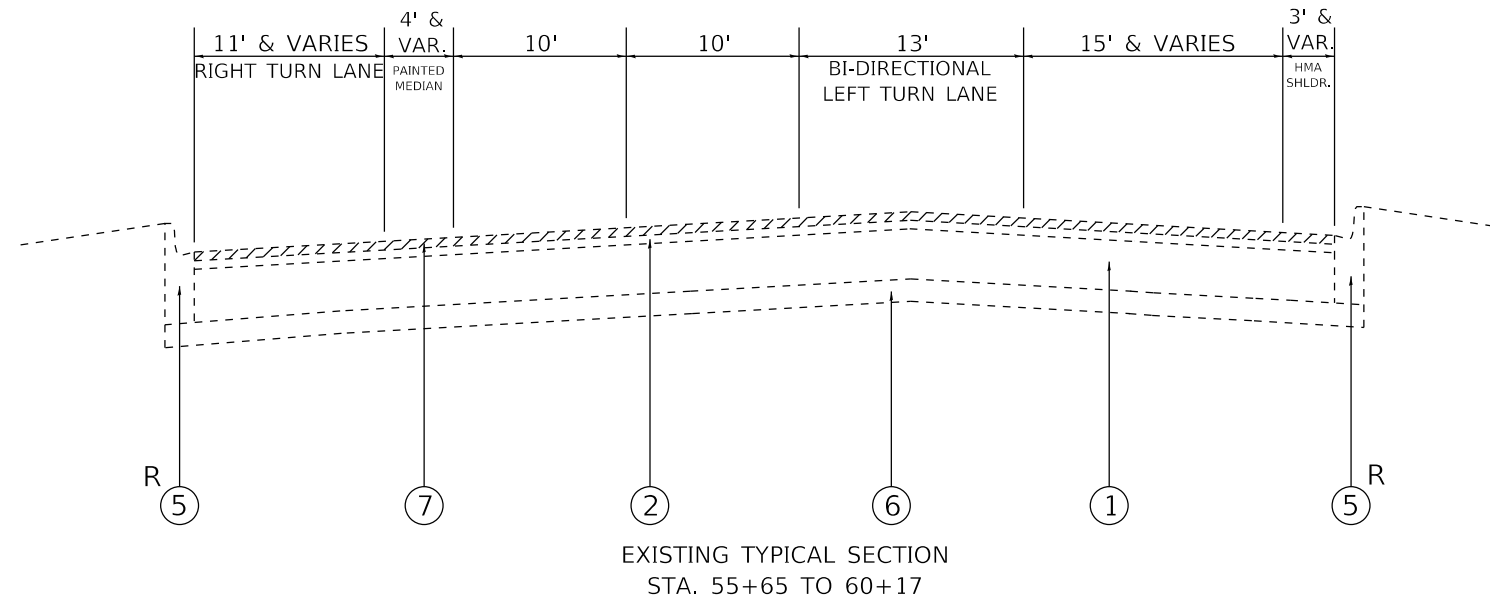
- LEGEND:**
- ① EXISTING P.C.C. PAVEMENT ±10"
 - ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
 - ③ EXISTING P.C.C. BARRIER MEDIAN
 - ④ EXISTING AGGREGATE SHOULDER
 - ⑤ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - ⑥ EXISTING STABILIZED SUB-BASE
 - ⑦ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
 - ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1L-4.75, N50, ¾"
 - ⑨ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80, 1¾"
 - ⑩ PROPOSED GRADING AND SHAPING SHOULDERS
 - ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



- LEGEND:**
- *STA. 50+00 TO 53+00
 - **STA. 53+00 TO 55+65

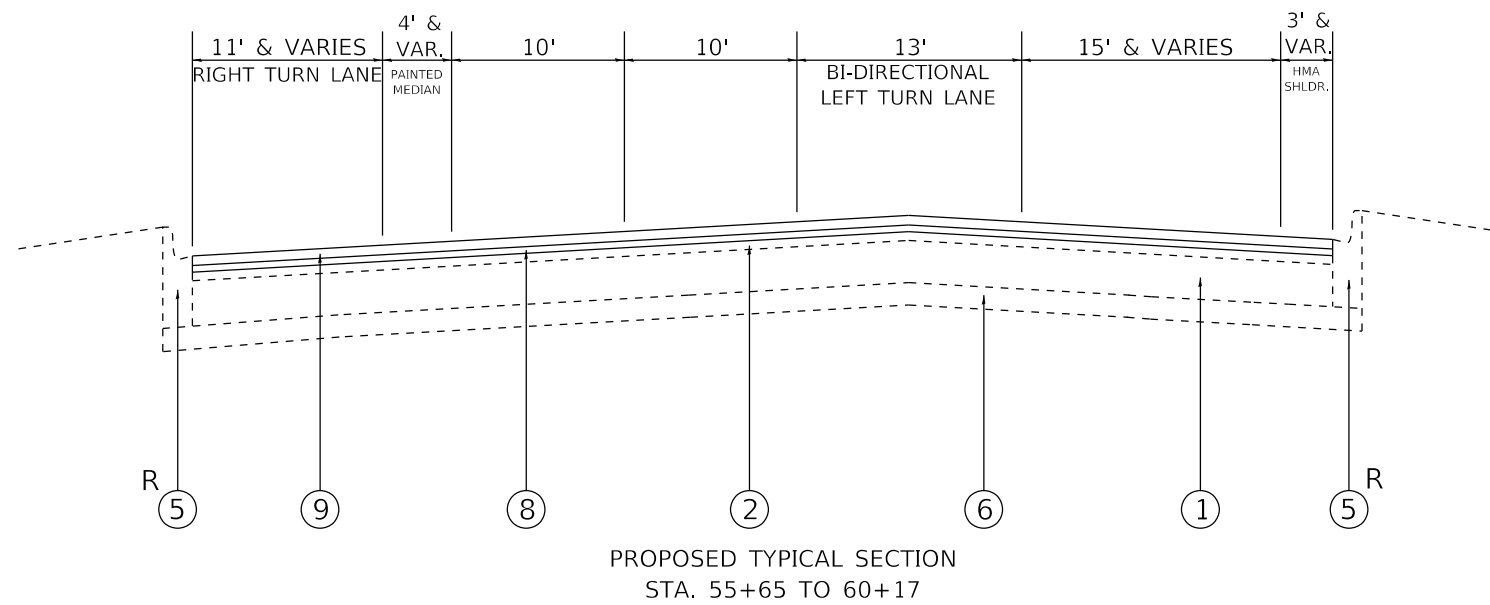


FILE NAME =	USER NAME = steedp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS FAP 870 /IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.		F.A.P. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 5/22/2019	DATE -	REVISED -	REVISED -		SHEET NO. 1 OF 1 SHEETS			STA.	TO STA.	CONTRACT NO. 62G59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT											

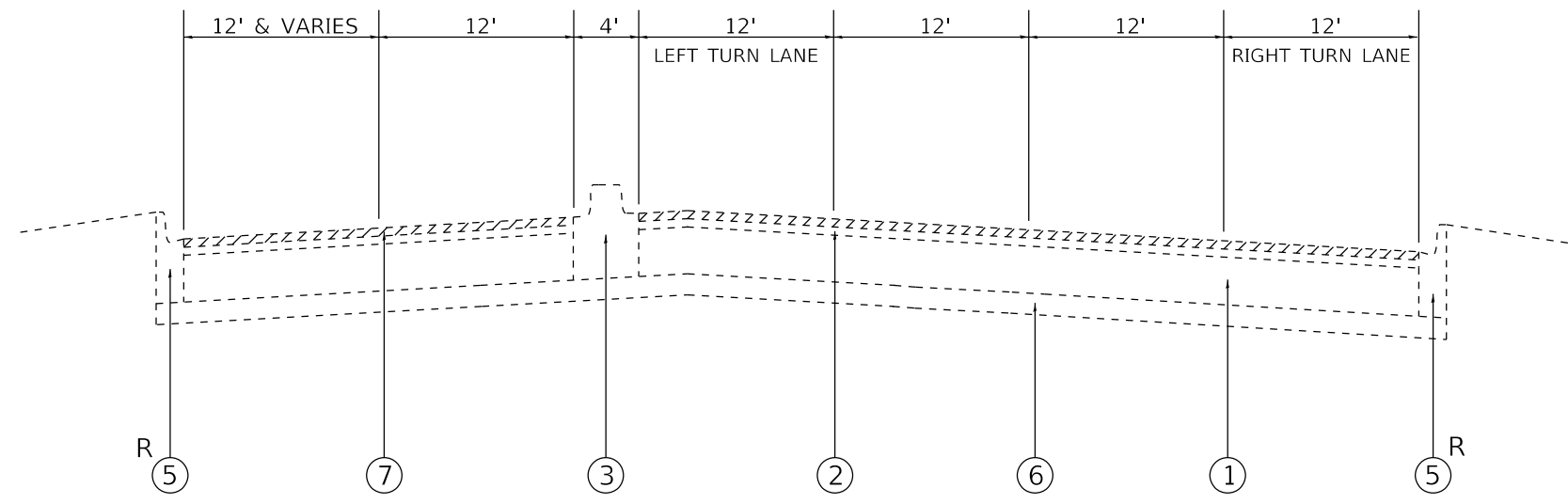


LEGEND:

- ① EXISTING P. C. C. PAVEMENT ±10"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
- ③ EXISTING P. C. C. BARRIER MEDIAN
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑥ EXISTING STABILIZED SUB-BASE
- ⑦ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
- ⑨ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80, 1¾"
- ⑩ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

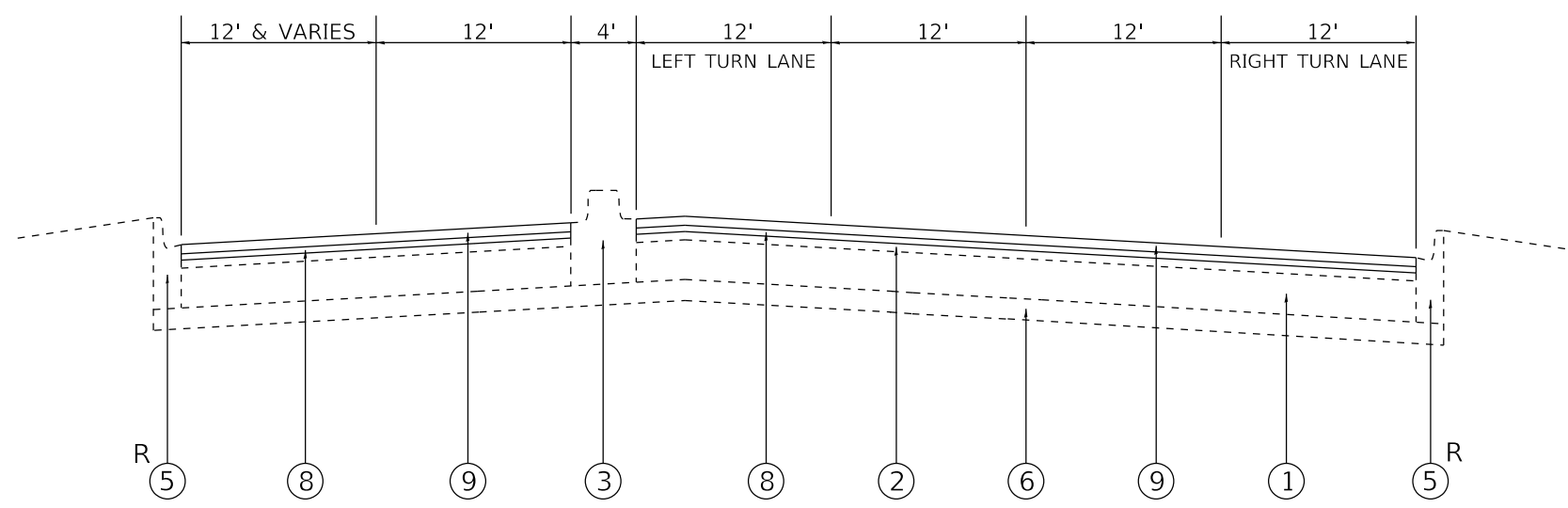


FILE NAME =	USER NAME = steedpa	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS FAP 870 /IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0002' / in.	CHECKED -	REVISED -	REVISED -			CONTRACT NO. 62G59				
PLOT DATE = 3/22/2019	DATE -	REVISED -	REVISED -			SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.	
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



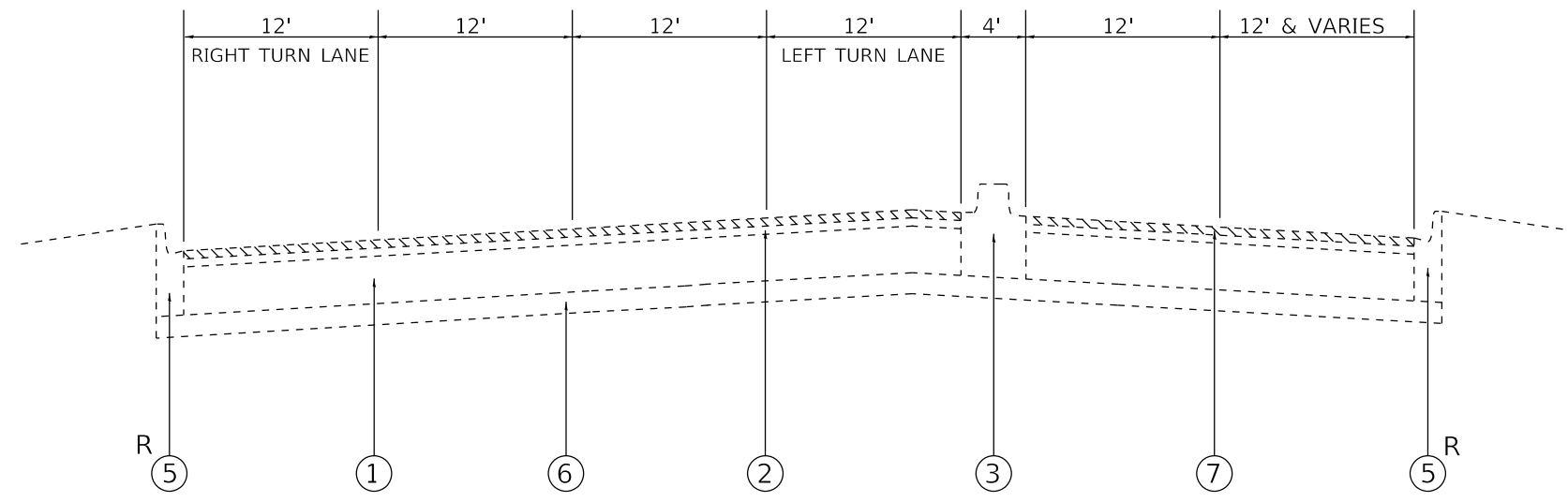
EXISTING TYPICAL SECTION
STA. 60+17 TO 65+48

- LEGEND:**
- ① EXISTING P. C. C. PAVEMENT ±10"
 - ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
 - ③ EXISTING P. C. C. BARRIER MEDIAN
 - ④ EXISTING AGGREGATE SHOULDER
 - ⑤ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - ⑥ EXISTING STABILIZED SUB-BASE
 - ⑦ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
 - ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
 - ⑨ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80, 1¾"
 - ⑩ PROPOSED GRADING AND SHAPING SHOULDERS
 - ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
 - R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



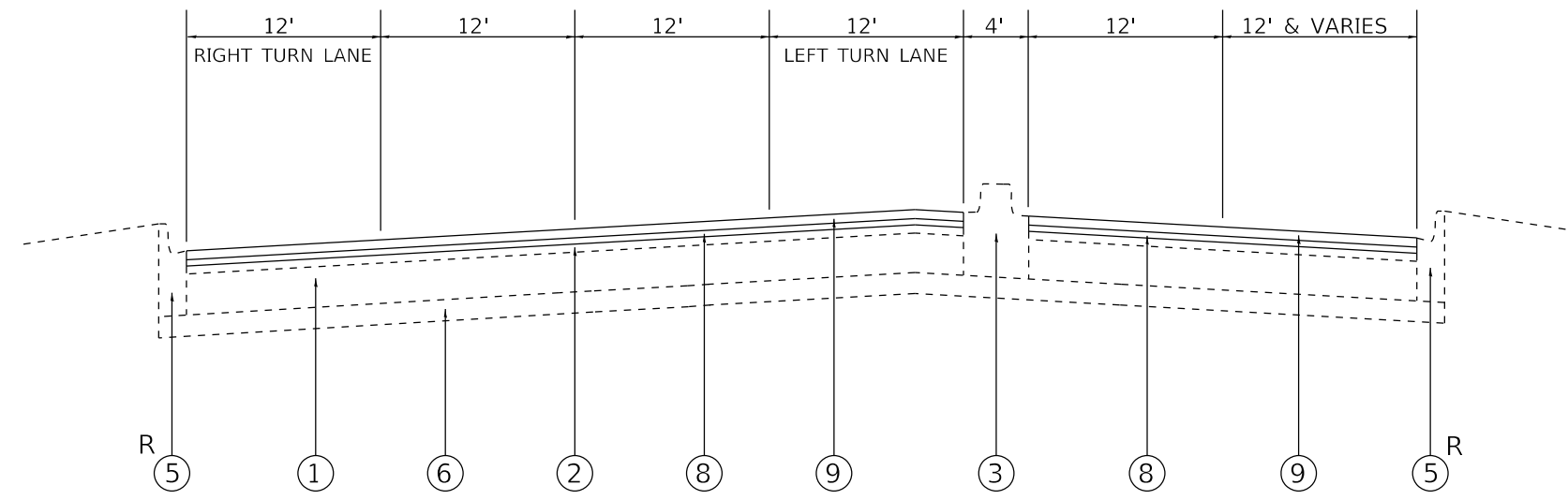
PROPOSED TYPICAL SECTION
STA. 60+17 TO 65+48

FILE NAME =	USER NAME = steedpa	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS FAP 870 /IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLLOT DATE = 5/22/2019	DATE -	REVISED -	REVISED -			SHEET NO. 1 OF 1 SHEETS			STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT
						CONTRACT NO. 62G59					



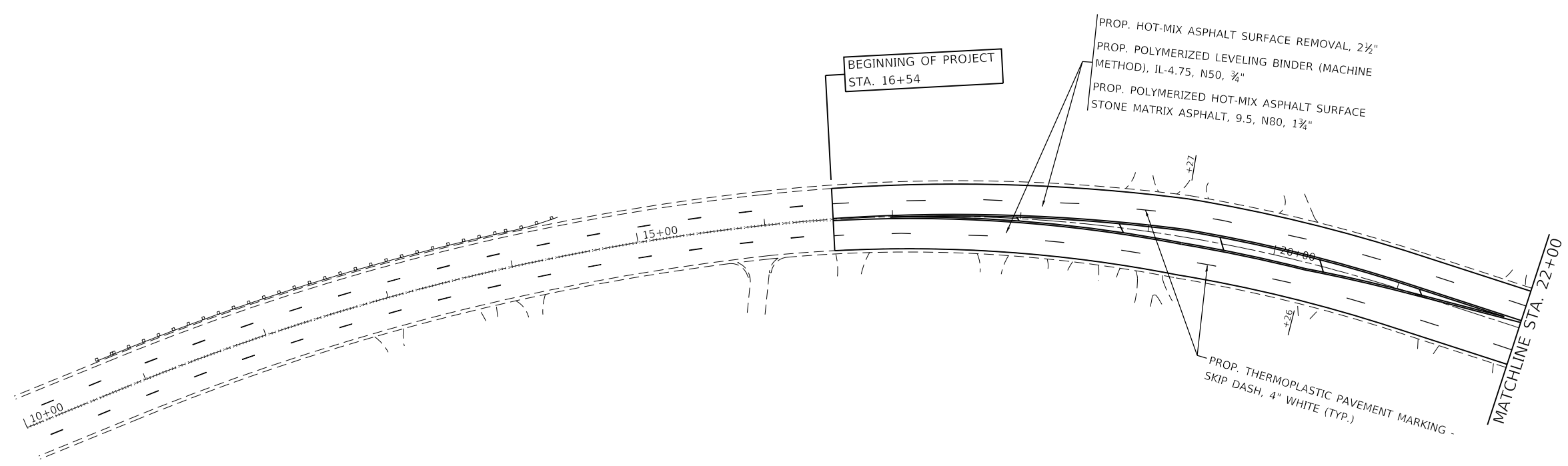
EXISTING TYPICAL SECTION
STA. 65+48 TO 70+81

- LEGEND:**
- ① EXISTING P. C. C. PAVEMENT ±10"
 - ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
 - ③ EXISTING P. C. C. BARRIER MEDIAN
 - ④ EXISTING AGGREGATE SHOULDER
 - ⑤ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - ⑥ EXISTING STABILIZED SUB-BASE
 - ⑦ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
 - ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
 - ⑨ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80, 1¾"
 - ⑩ PROPOSED GRADING AND SHAPING SHOULDERS
 - ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
 - R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



PROPOSED TYPICAL SECTION
STA. 65+48 TO 70+81

FILE NAME =	USER NAME = steedpa	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS FAP 870 /IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLLOT DATE = 5/22/2019	DATE -	REVISED -	REVISED -			SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.	CONTRACT NO. 62G59	
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



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PLOT DATE = 5/22/2019	DATE -	REVISED -

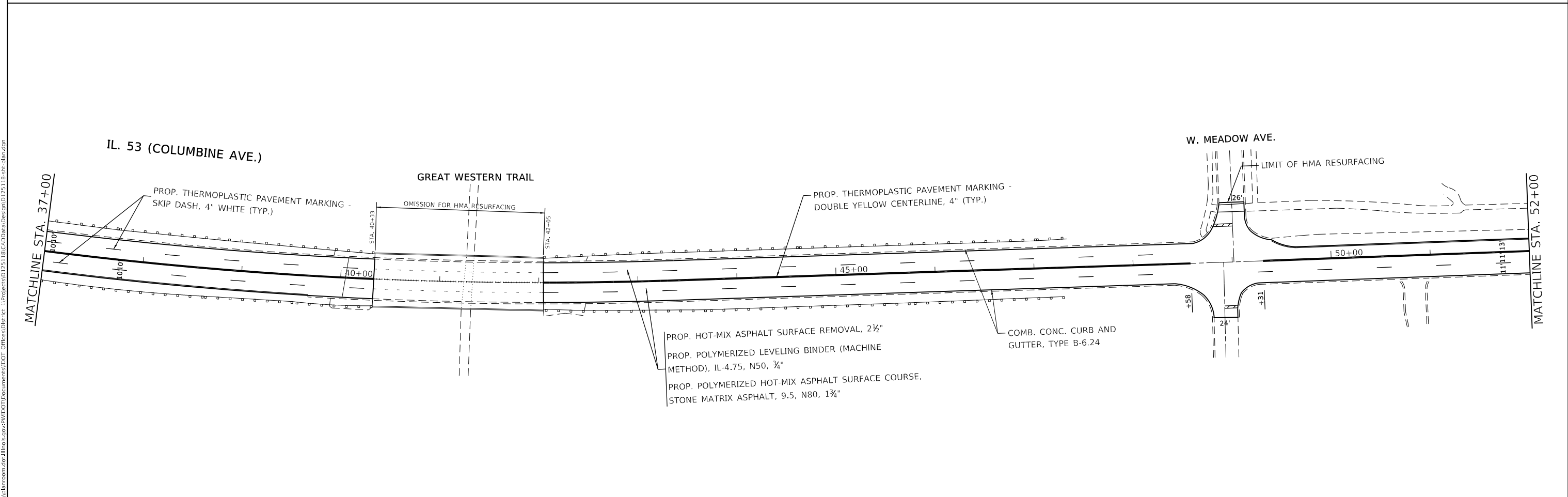
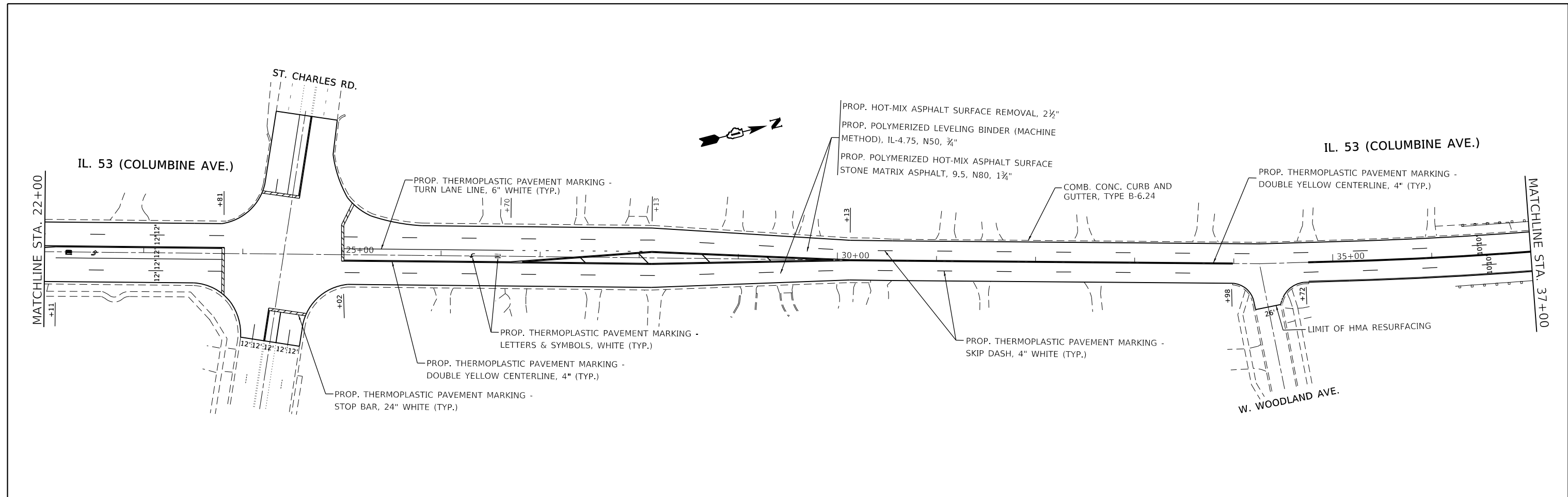
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN
 FAP 870 /IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.**

SCALE: 1"=50' SHEET OF SHEETS STA. 10+00 TO STA. 22+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-R5-SW	DUPAGE	34	12
			CONTRACT NO. 62G59	
		ILLINOIS	FED. AID PROJECT	

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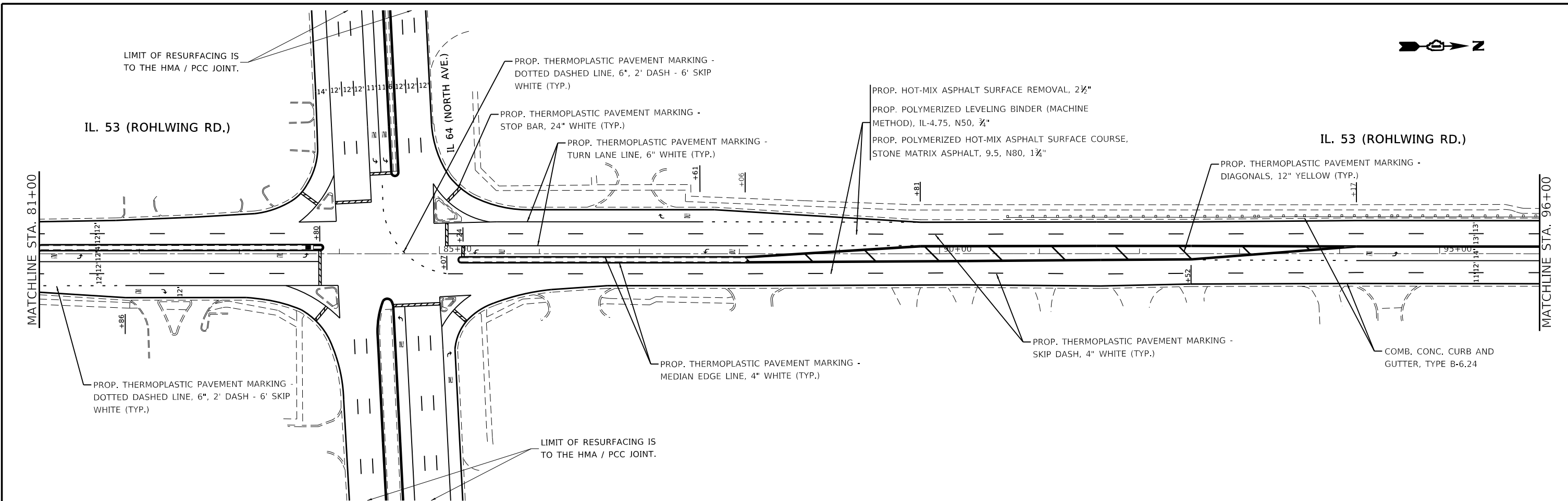


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PLOT DATE = 3/25/2019	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLAN
FAP 870 / IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.

F.A.P. RTE. 870	SECTION 2018-031-RS-SW	COUNTY DuPAGE	TOTAL SHEETS 34	SHEET NO. 13
SCALE: 1"=50'			CONTRACT NO. 62G59	
SHEET OF SHEETS STA. 22+00 TO STA. 52+00		ILLINOIS FED. AID PROJECT		



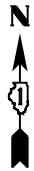
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 DATE: 5/22/2019

USER NAME = Velichkovv	DESIGNED -	REVISED -
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PLOT DATE = 5/22/2019	CHECKED -	REVISED -
	DATE -	REVISED -

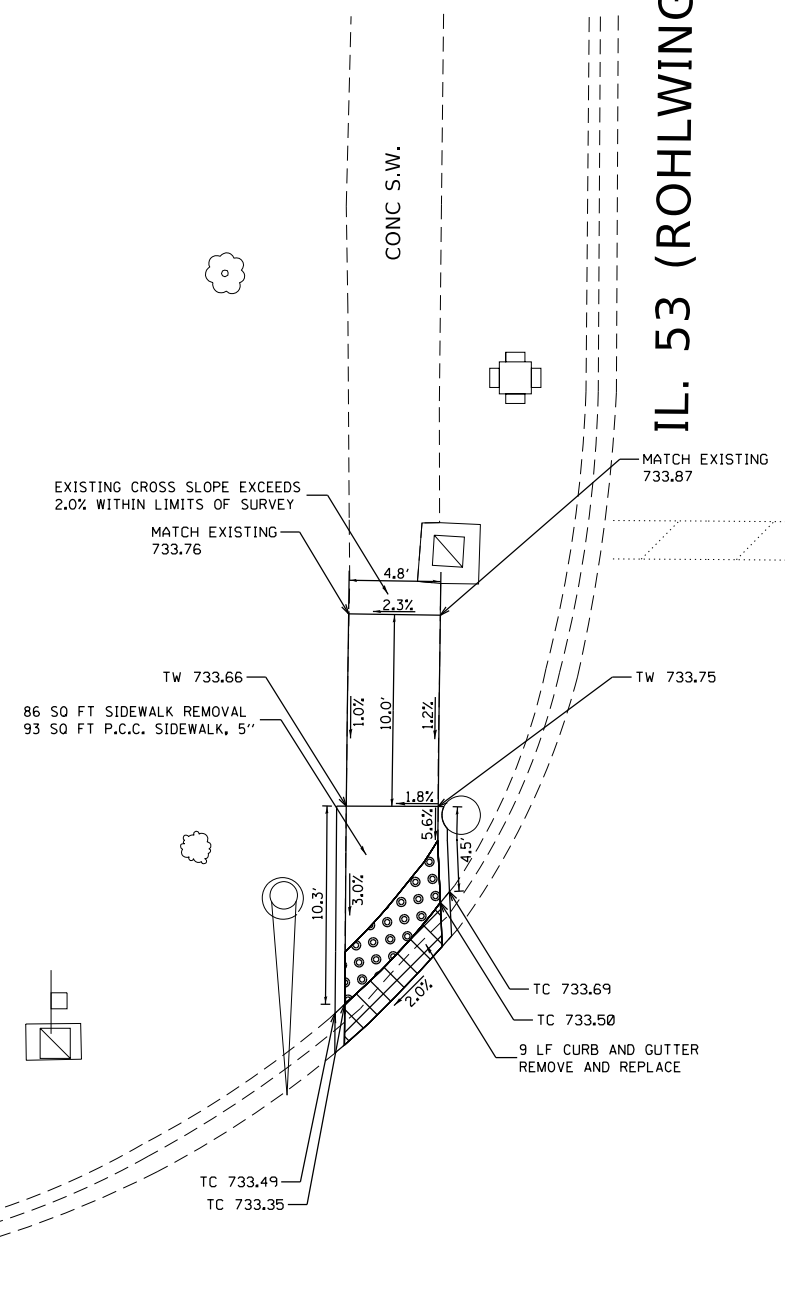
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLAN
FAP 870 / IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.
 SCALE: 1"=50' SHEET OF SHEETS STA. 81+00 TO STA. 101+28.71

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DuPAGE	34	15
			CONTRACT NO. 62G59	
ILLINOIS FED. AID PROJECT				



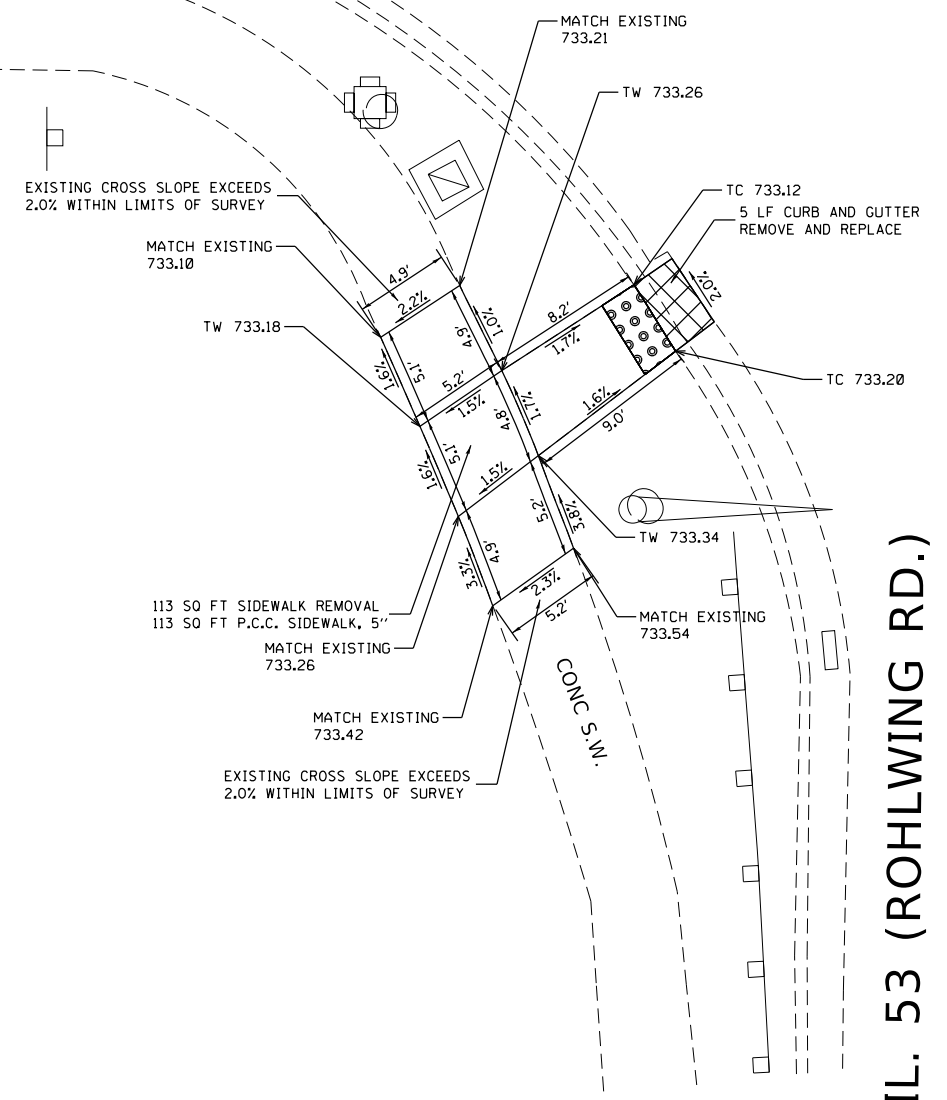
IL. 53 (ROHLWING RD.)



SIDNEY AVE.



SIDNEY AVE.



IL. 53 (ROHLWING RD.)

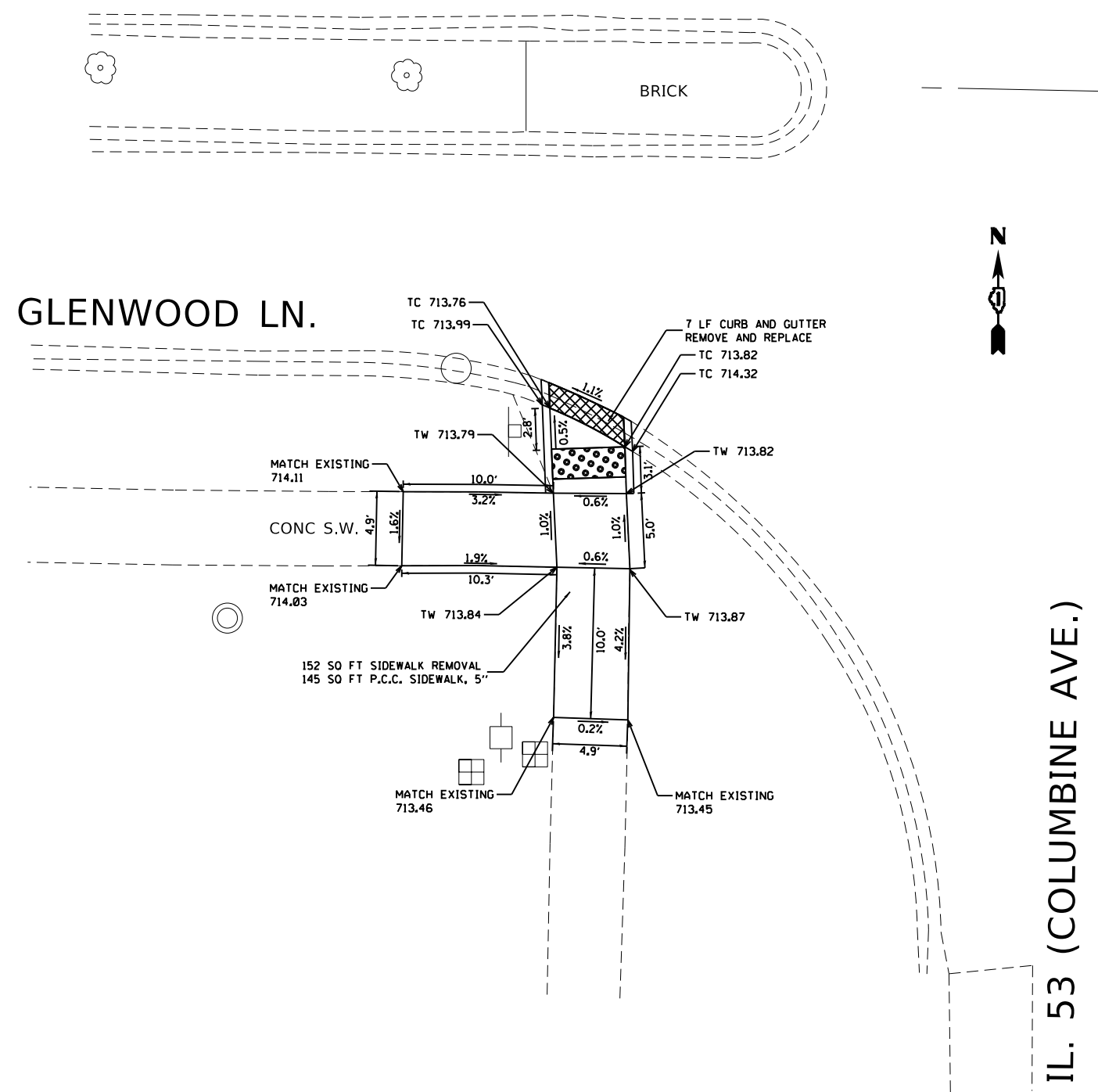
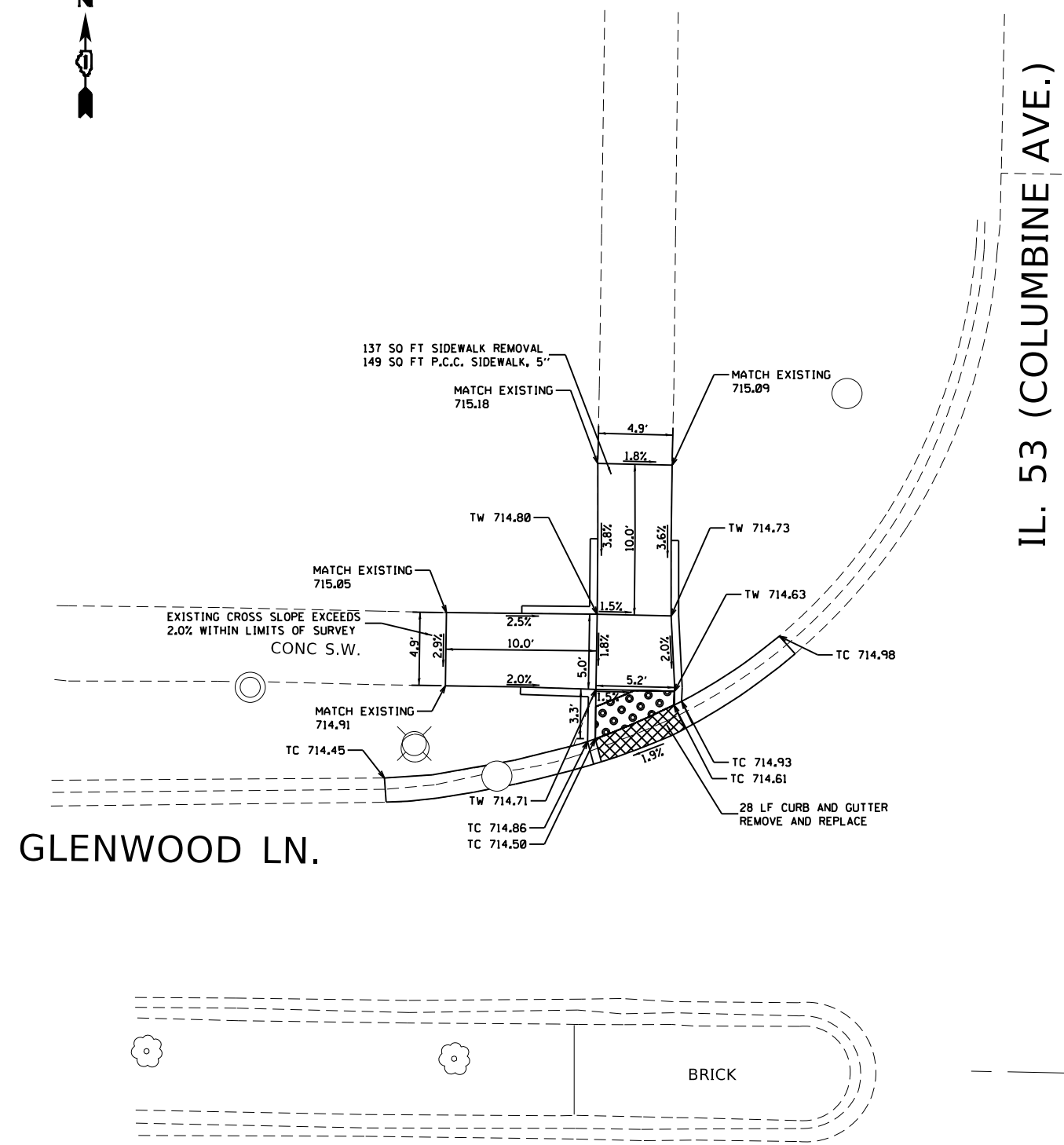
REFERENCE BENCH MARK ELEV 734.67
 BENCHMARK : SET X ON TOP SPINDAL OF FIRE HYDRANT
 LOCATION : FIRE HYDRANT AT SW CORNER OF IL 53 (ROHLWING RD.) AND SIDNEY AVE.

LEGEND

- xx.xx' EXISTING LENGTH
- ===== PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- [] PROPOSED SIDEWALK
- [] DETECTABLE WARNINGS
- [X] DEPRESSED CURB

REFERENCE BENCH MARK ELEV 734.67
 BENCHMARK : SET X ON TOP SPINDAL OF FIRE HYDRANT
 LOCATION : FIRE HYDRANT AT SW CORNER OF IL 53 (ROHLWING RD.) AND SIDNEY AVE.

FILE NAME =	USER NAME = steedp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ADA DETAILS (AT SIDNEY AVE.-NW & SW CORNERS) FAP 870 /IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 62G59				
	PLOT DATE = 5/17/2019	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								



REFERENCE BENCH MARK ELEV 718.32
 BENCHMARK : SET X ON TOP OF FIRE HYDRANT
 LOCATION : FIRE HYDRANT AT SW CORNER OF IL 53 (COLUMBINE AVE.) AND GLENWOOD LN.

LEGEND

xx.xx'	EXISTING LENGTH		PROPOSED SIDEWALK
	PROPOSED SIDE CURB		DETECTABLE WARNINGS
()	EXISTING ELEVATION/SLOPE		DEPRESSED CURB

REFERENCE BENCH MARK ELEV 718.32
 BENCHMARK : SET X ON TOP OF FIRE HYDRANT
 LOCATION : FIRE HYDRANT AT SW CORNER OF IL 53 (COLUMBINE AVE.) AND GLENWOOD LN.

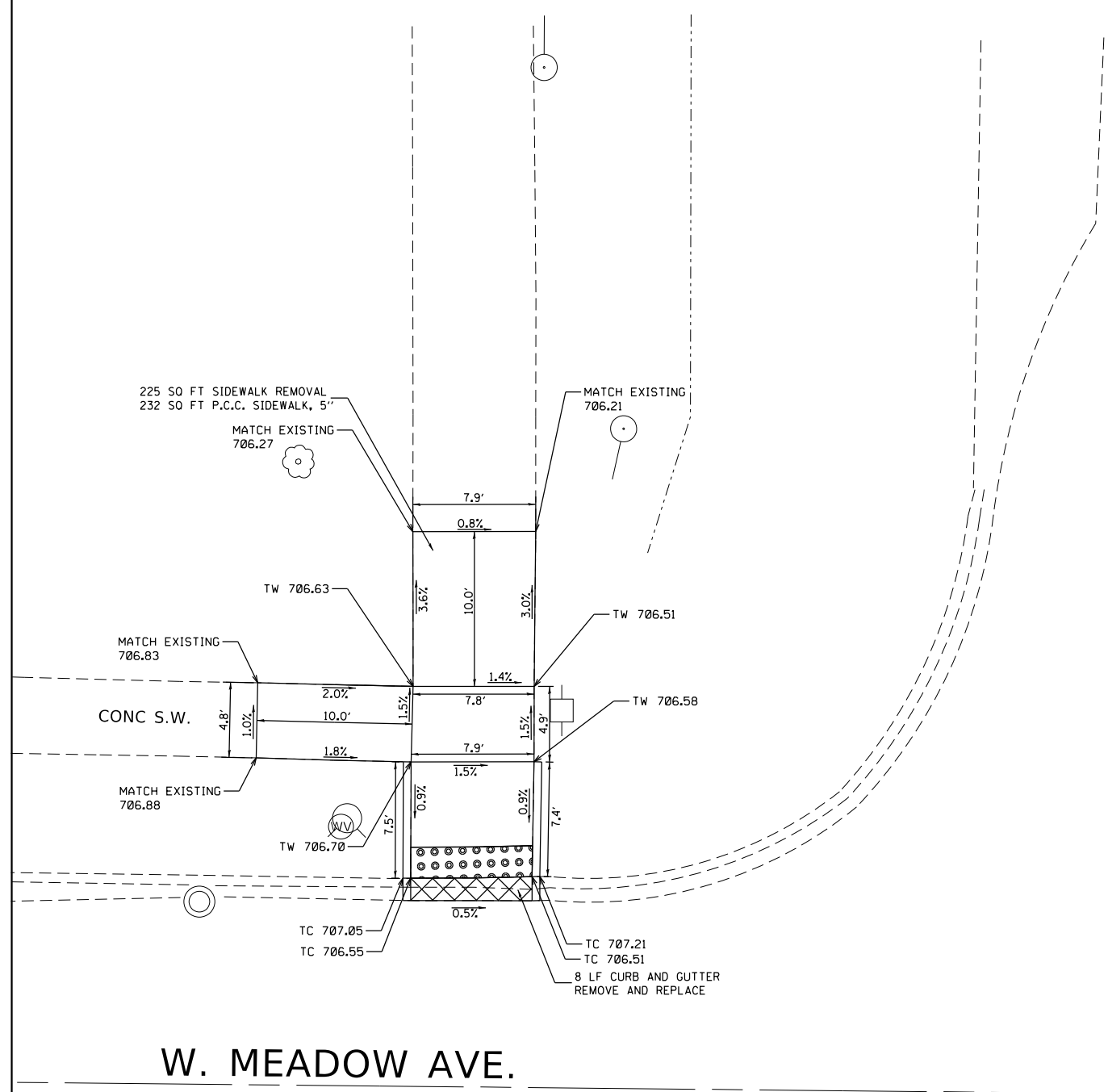
FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -
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Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/22/2019	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ADA DETAILS (AT GLENWOOD LN.-NW & SW CORNERS)
 FAP 870 /IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.**

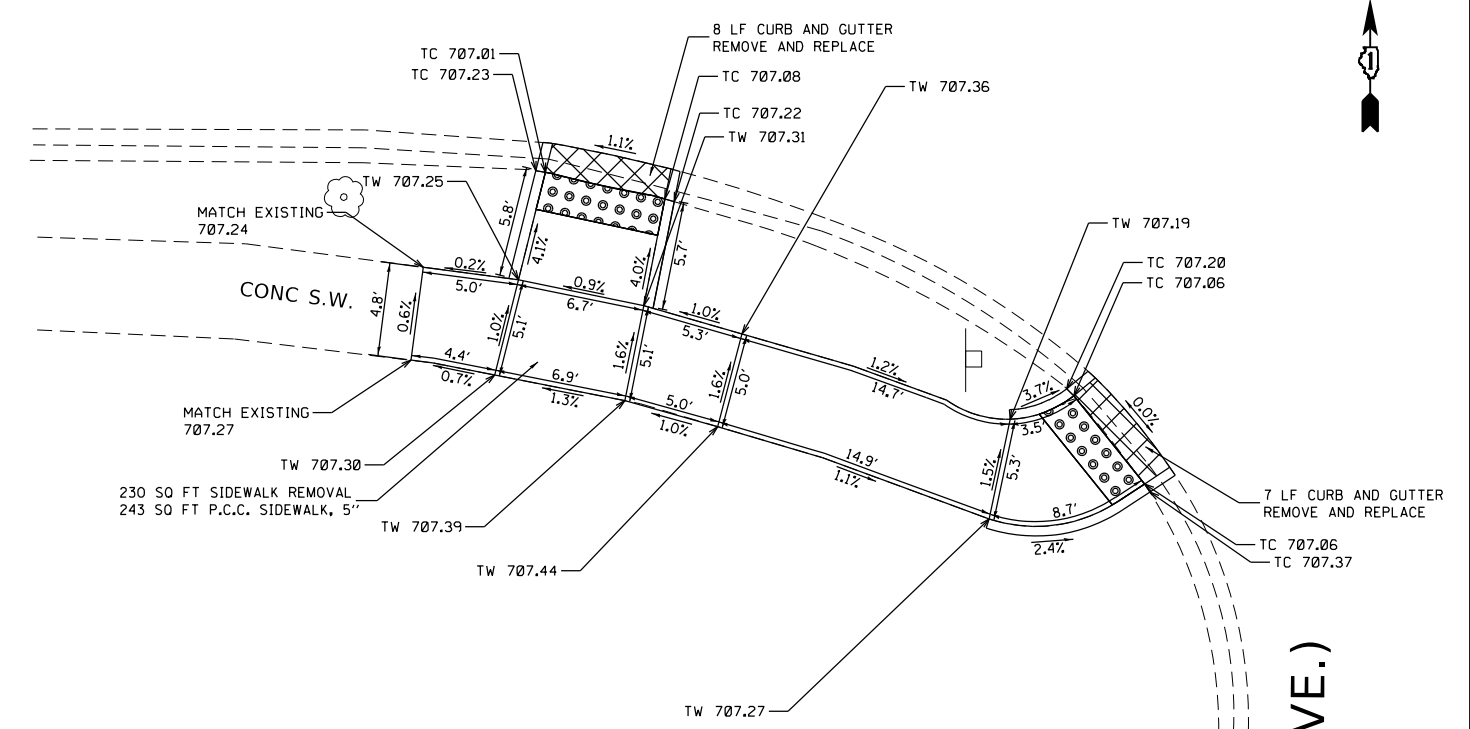
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DUPAGE	34	17
CONTRACT NO. 62059				
ILLINOIS FED. AID PROJECT				



IL. 53 (COLUMBINE AVE.)

W. MEADOW AVE.



IL. 53 (COLUMBINE AVE.)

W. MEADOW AVE.

REFERENCE BENCH MARK ELEV 709.57
 BENCHMARK : SET X ON TOP OF FIRE HYDRANT
 LOCATION : FIRE HYDRANT AT NW CORNER OF IL 53 (COLUMBINE AVE.) AND MEADOW AVE.

LEGEND

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- DEPRESSED CURB

REFERENCE BENCH MARK ELEV 709.57
 BENCHMARK : SET X ON TOP OF FIRE HYDRANT
 LOCATION : FIRE HYDRANT AT NW CORNER OF IL 53 (COLUMBINE AVE.) AND MEADOW AVE.

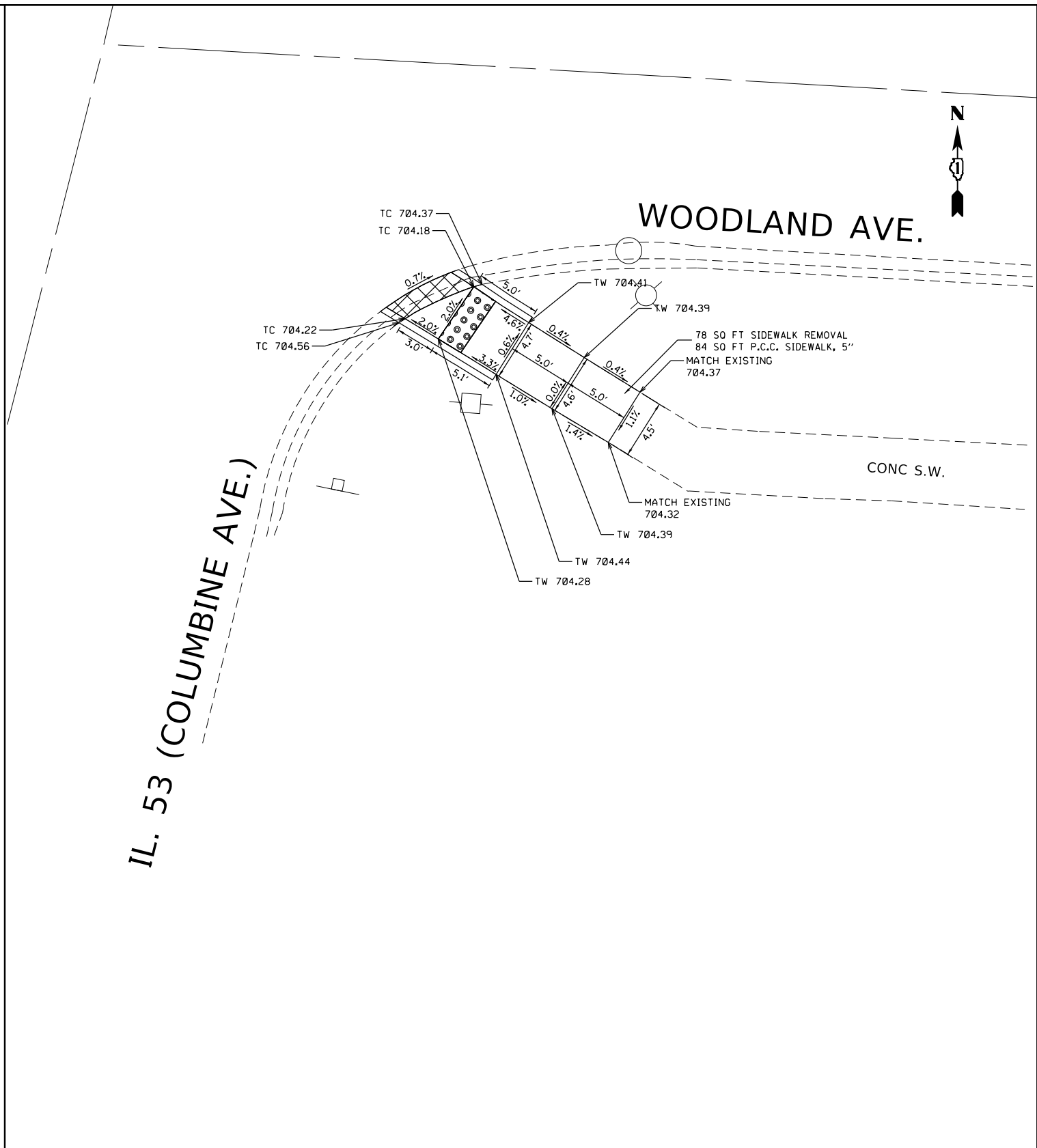
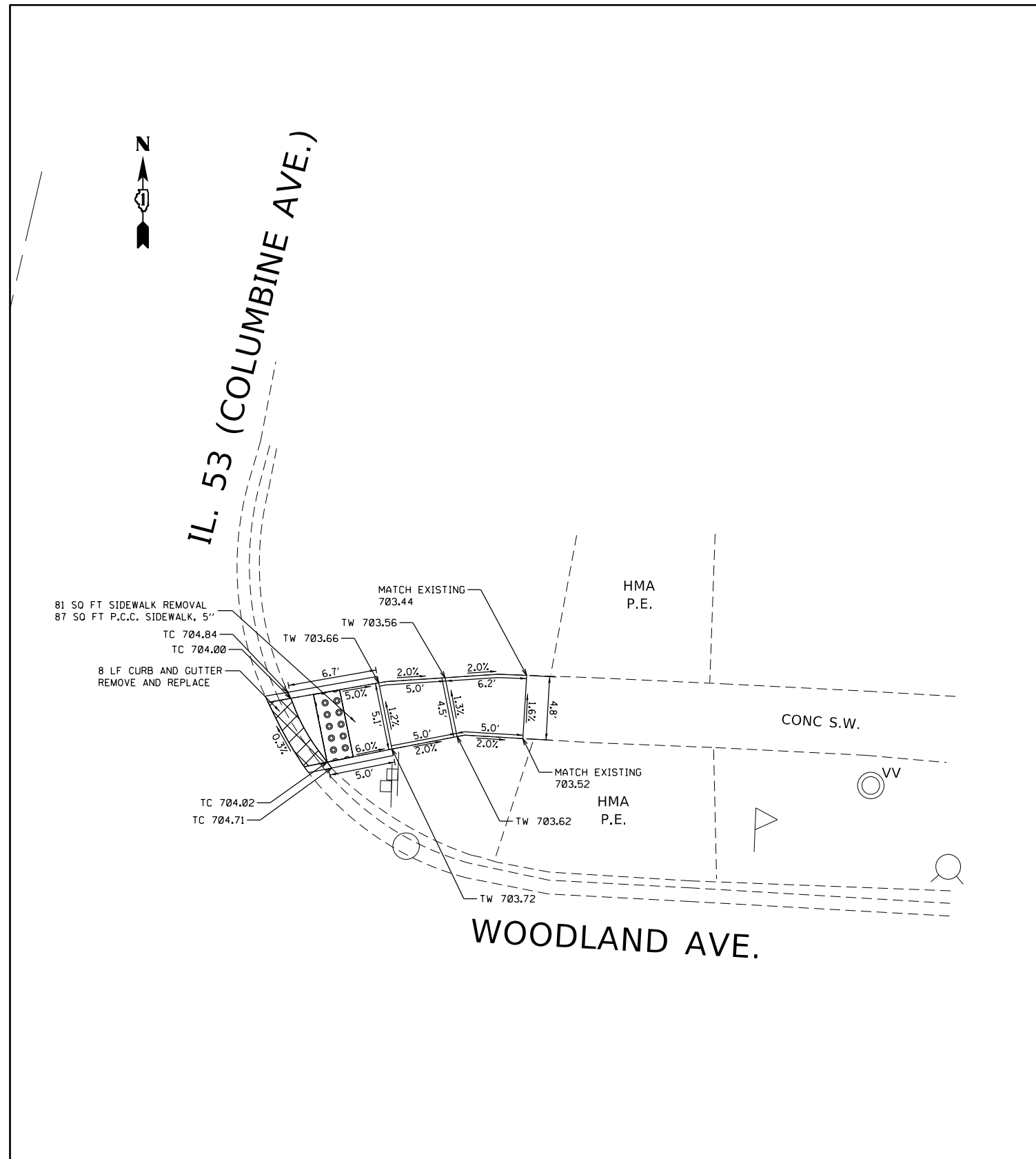
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Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/20/2019	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ADA DETAILS (AT MEADOW AVE.-NW & SW CORNERS)
 FAP 870 /IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DUPAGE	34	18
CONTRACT NO. 62G59				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.



REFERENCE BENCH MARK ELEV 705.34

BENCHMARK : SET X ON RAILROAD SPIKE IN POWERPOLE

LOCATION : POWERPOLE AT SE CORNER OF IL 53 (COLUMBINE AVE.) AND WOODLAND AVE.

LEGEND

xx.xx'	EXISTING LENGTH		PROPOSED SIDEWALK
	PROPOSED SIDE CURB		DETECTABLE WARNINGS
()	EXISTING ELEVATION/SLOPE		DEPRESSED CURB

REFERENCE BENCH MARK ELEV 705.34

BENCHMARK : SET X ON RAILROAD SPIKE IN POWERPOLE

LOCATION : POWERPOLE AT SE CORNER OF IL 53 (COLUMBINE AVE.) AND WOODLAND AVE.

FILE NAME =	USER NAME = steedp	DESIGNED -	REVISED -
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Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/17/2019	DATE -	REVISED -

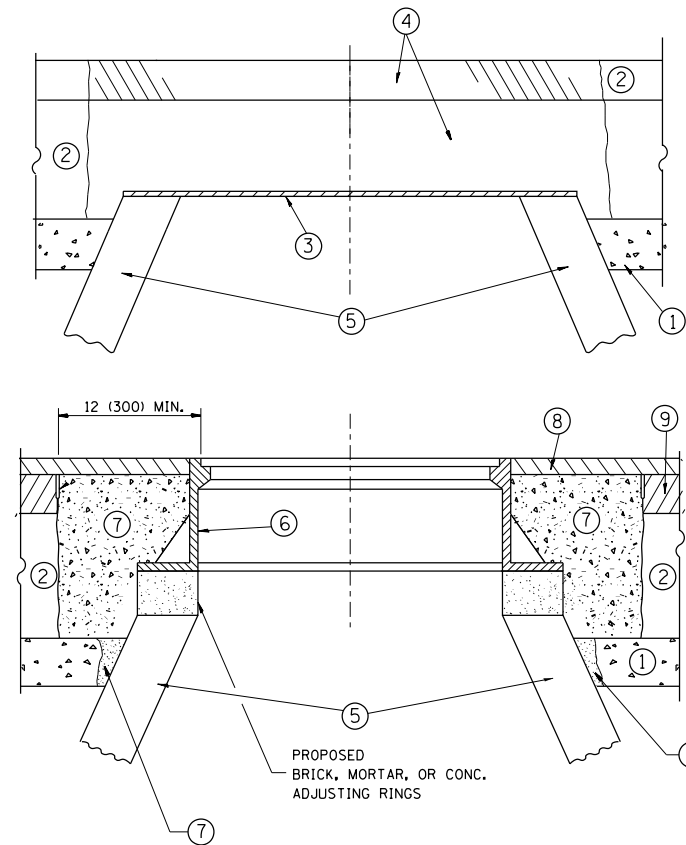
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA DETAILS (AT WOODLAND AVE.-NE & SE CORNERS)
FAP 870 /IL 53 (COLUMBINE AVE.)—ST. CHARLES RD. TO SIDNEY AVE.**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW		34	19
CONTRACT NO. 62G59				

SCALE: SHEET OF SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

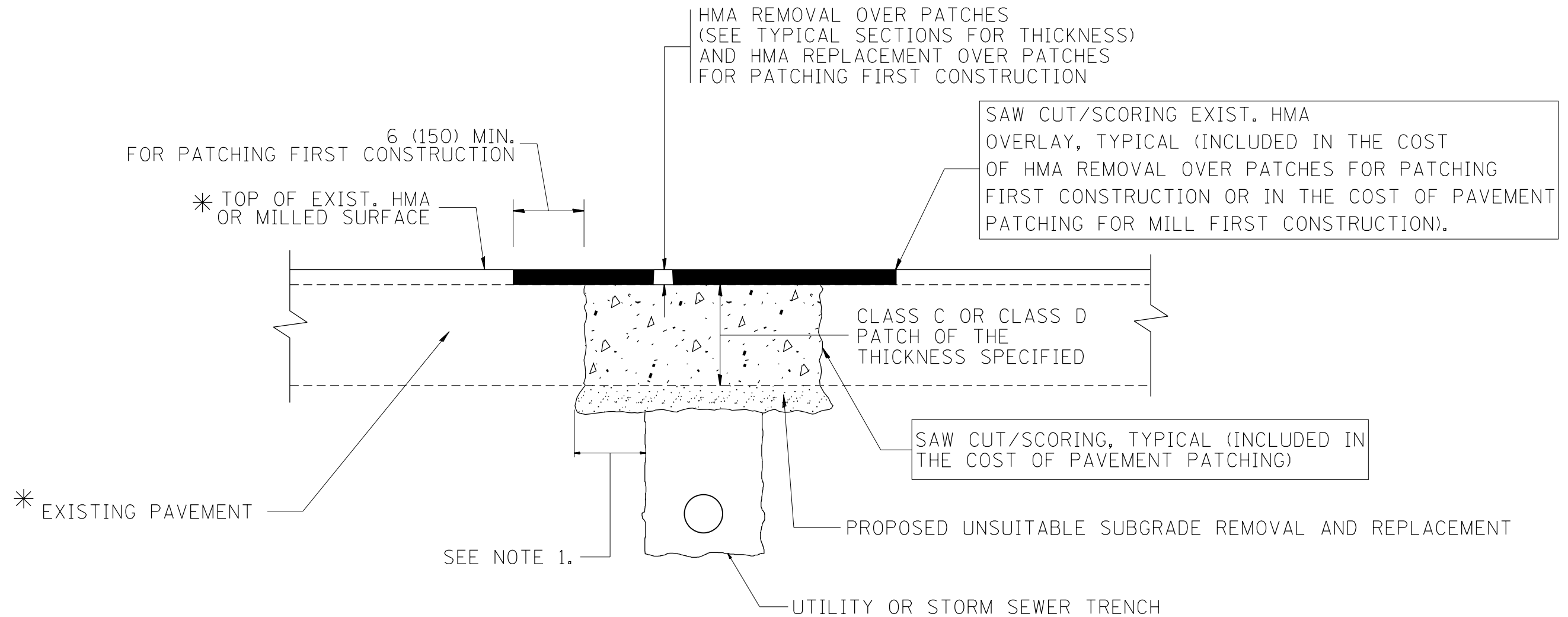
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	PLOT SCALE = 100.0000' / 1in.	DATE - 10-25-94	REVISED - R. BORO 03-09-11
	PLOT DATE = 3/22/2019		REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DJ	34	20
BD600-03 (BD-8)			CONTRACT NO. 62059	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = steedpo	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0125118\DRAWING\Design\DistStd.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 01-01-07					870	2018-031-RS-SW	DJ PAGE	34	21
PLOT DATE = 3/22/2019	DATE - 10-25-94	REVISED - R. BORO 09-04-07	REVISED - K. ENG 10-27-08		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			BD400-04 (BD-22)		CONTRACT NO. 62059		
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

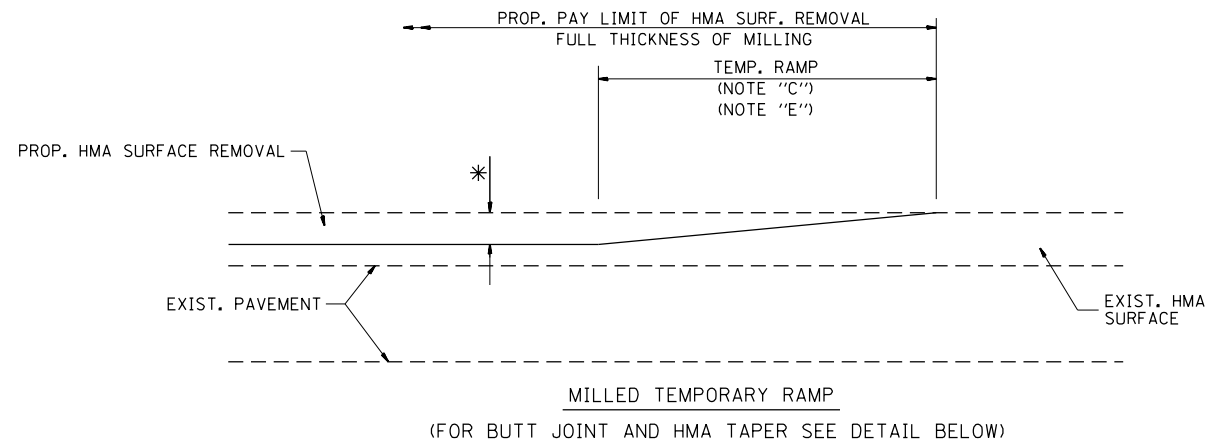
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

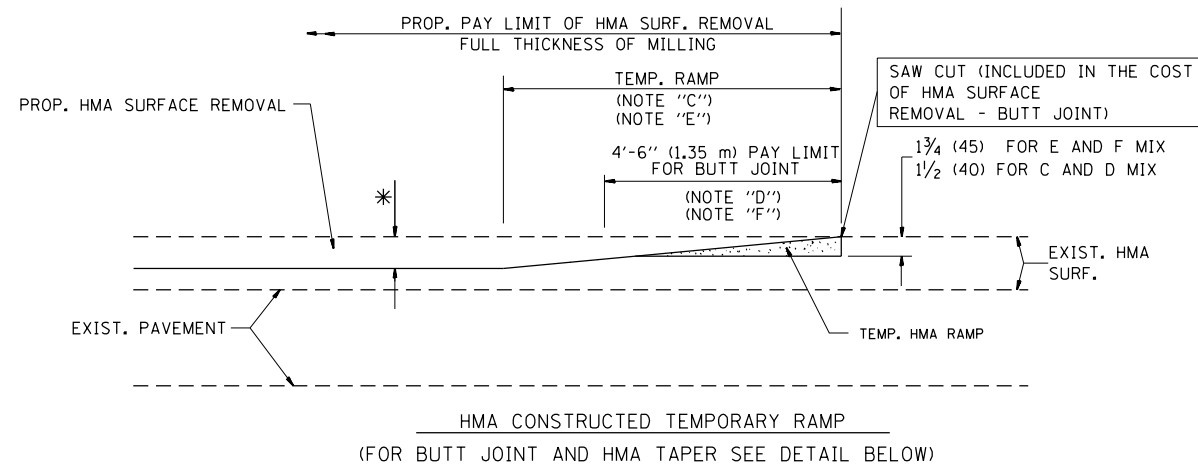
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = steedpa	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0125118\DRAWING\Design\DistStd.dgn		REVISION	REVISED - A. ABBAS 03-21-97			870	2018-031-RS-SW	DU PAGE	34	22
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 01-22-01				BD600-06 (BD-24)		CONTRACT NO. 62059		
PLOT DATE = 3/22/2019	DATE - 03-11-94	REVISED - R. BORO 12-15-09	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

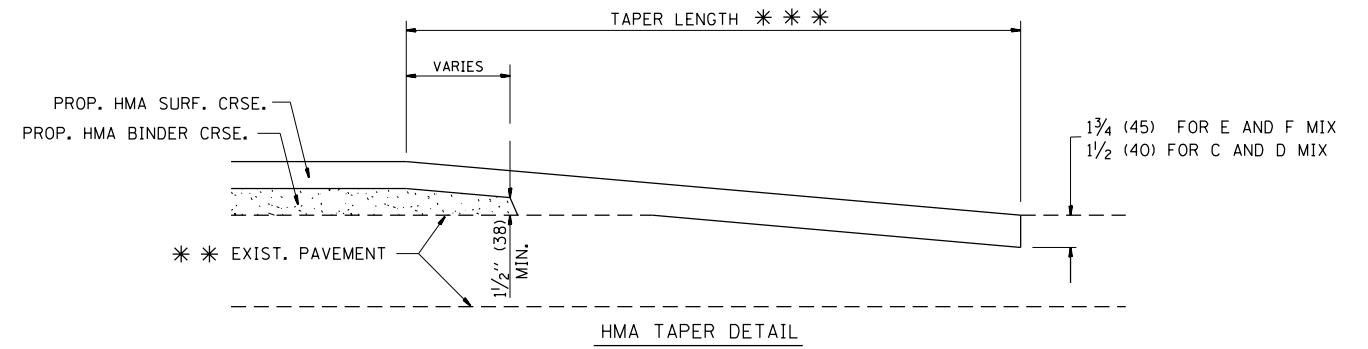
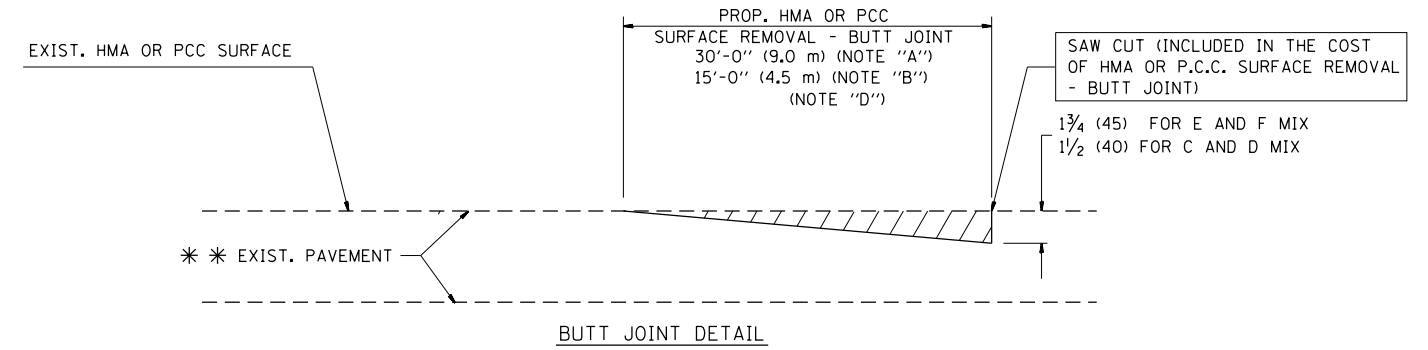


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

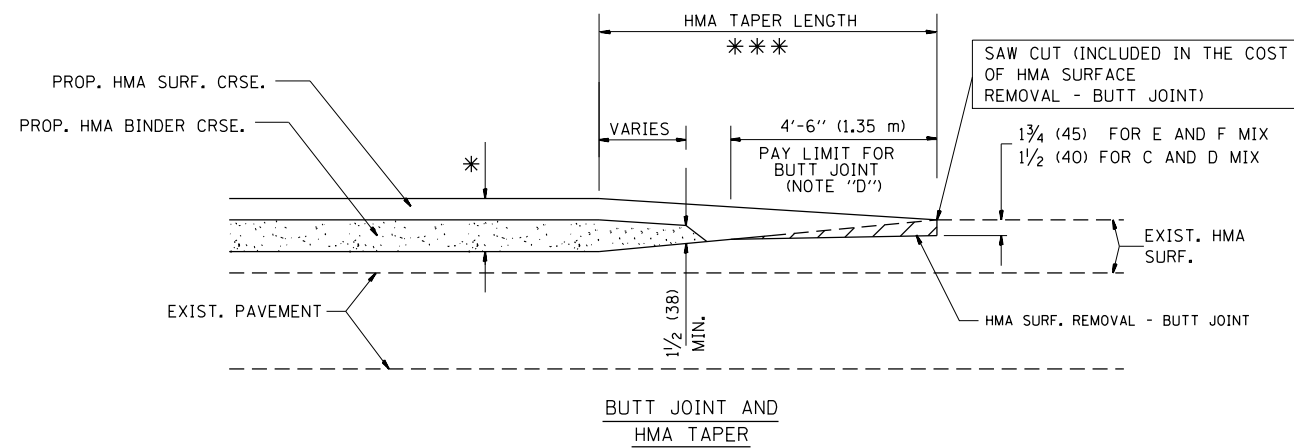
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

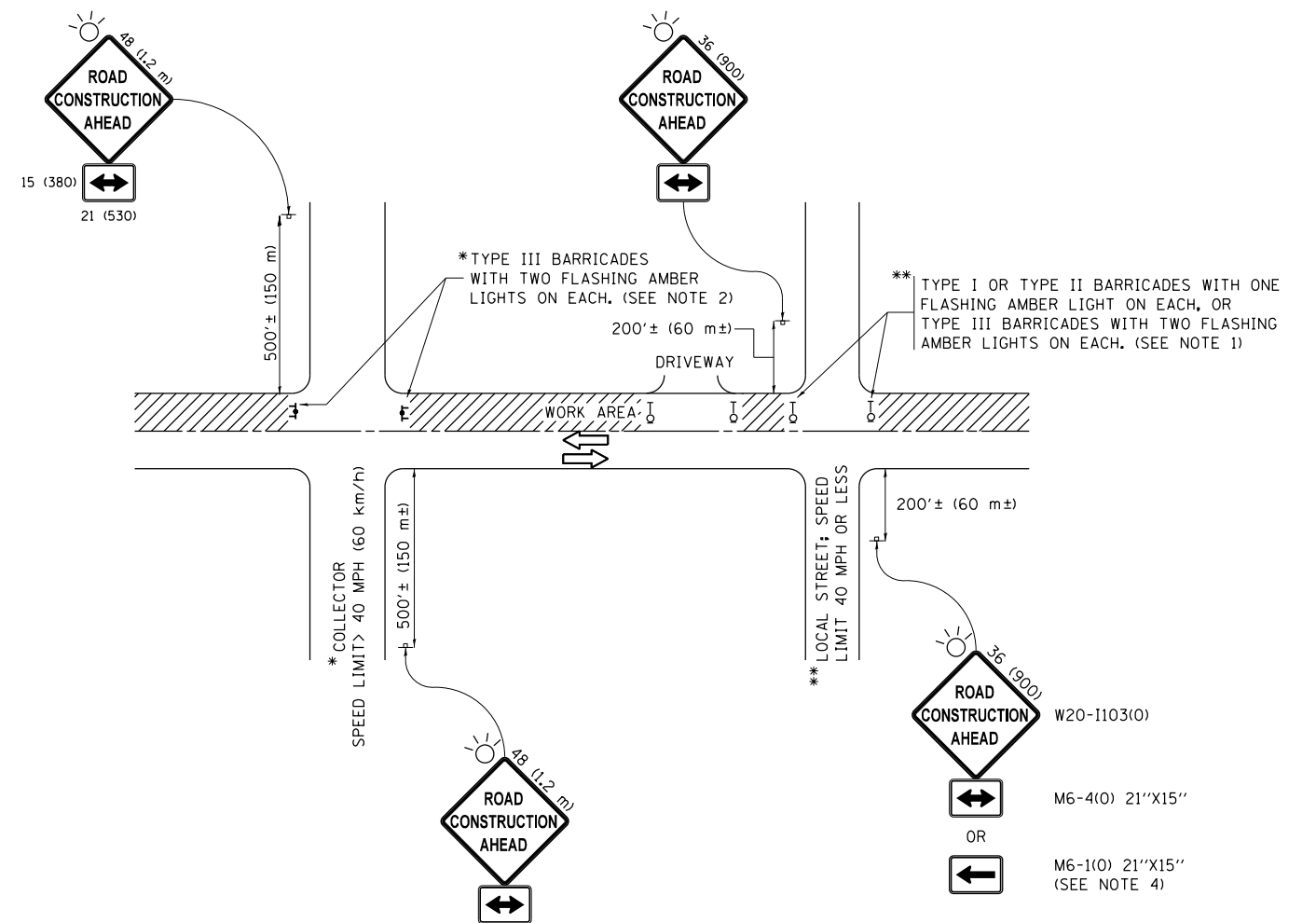
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	PLOT SCALE = 100.0000' / 1in.	DATE - 06-13-90	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 3/22/2019		REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DJ PAGE	34	23
BD400-05 BD32		CONTRACT NO. 62G59		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

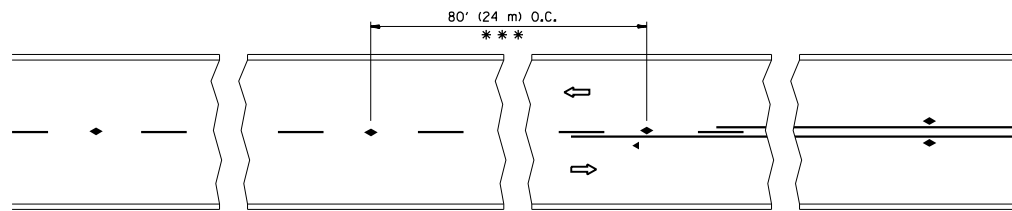
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 3/22/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DJ	34	24
TC-10			CONTRACT NO. 62G59	
ILLINOIS FED. AID PROJECT				

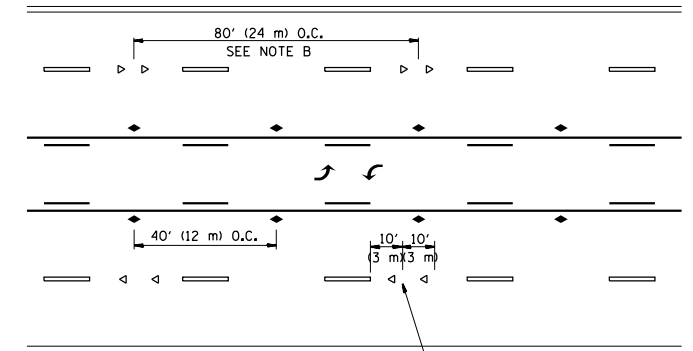


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

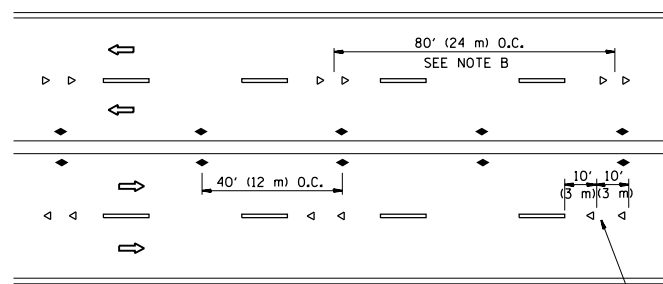
TWO-LANE/TWO-WAY



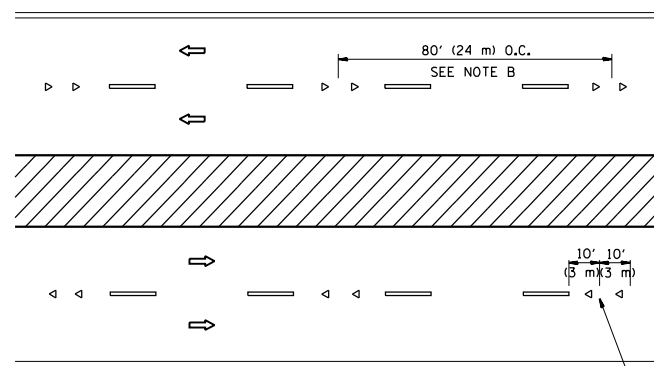
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

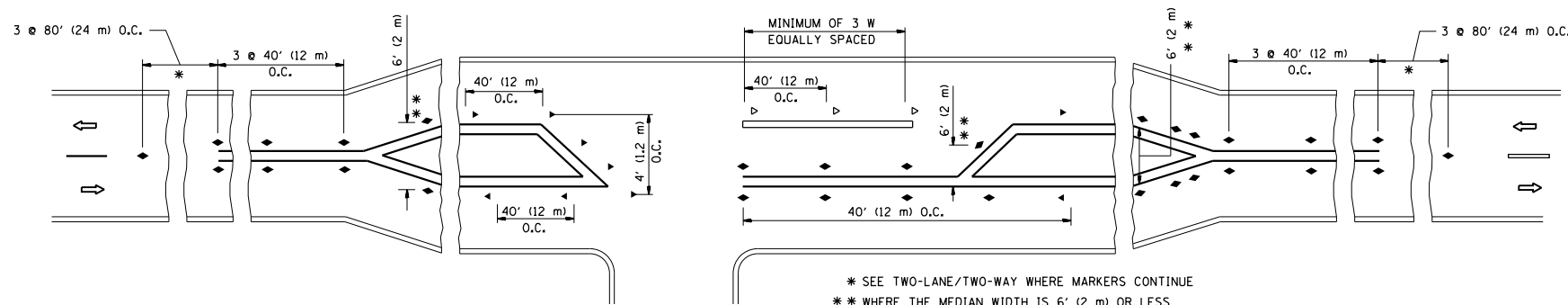
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

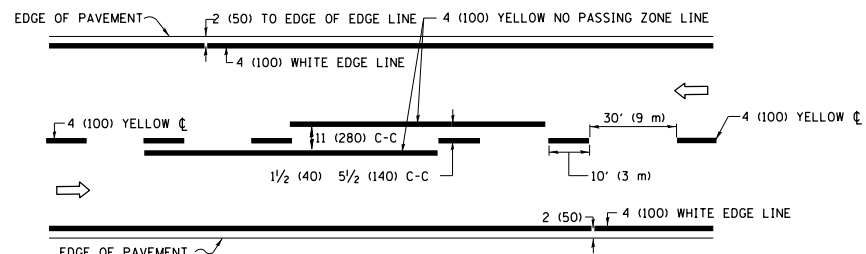
All dimensions are in inches (millimeters) unless otherwise shown.

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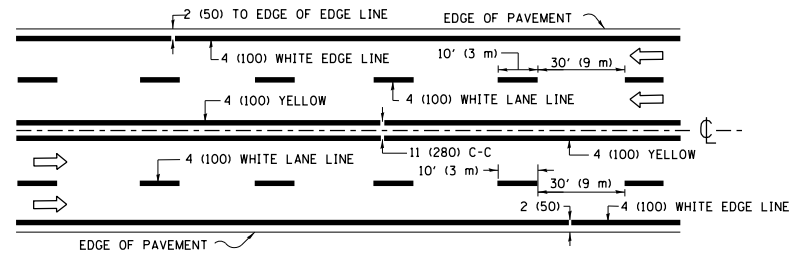
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

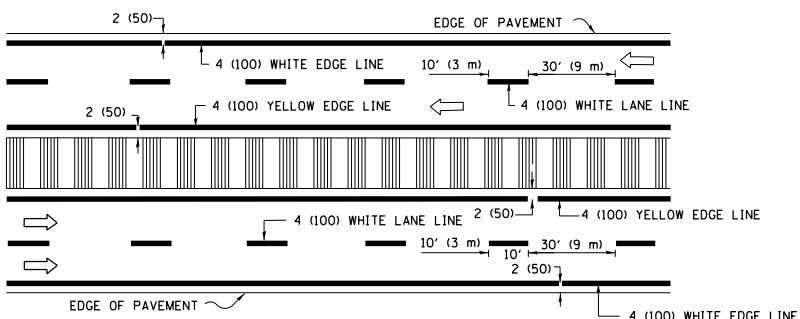
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DJ	34	25
TC-11		CONTRACT NO. 62G59		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

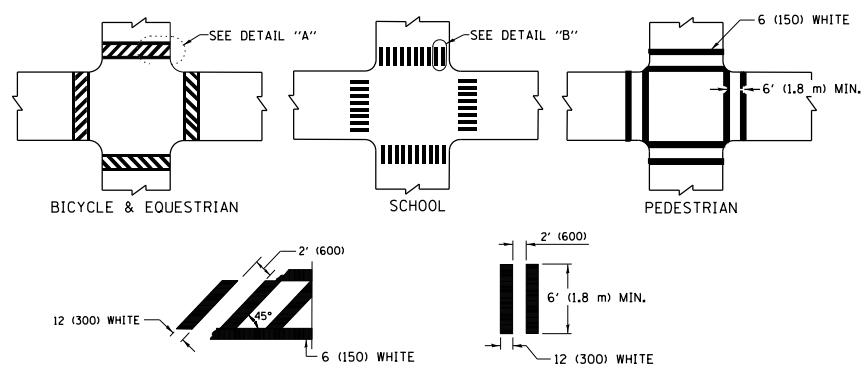


MULTI-LANE UNDIVIDED



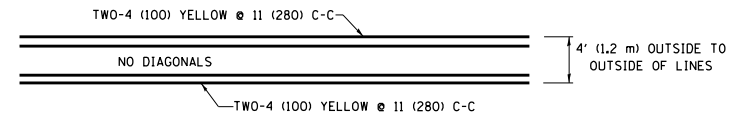
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

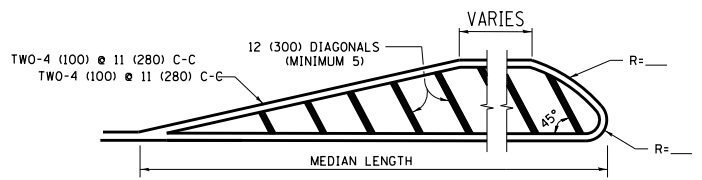


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

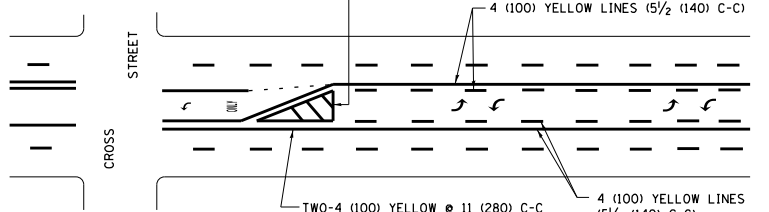


4' (1.2 m) WIDE MEDIANS ONLY



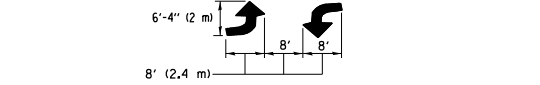
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



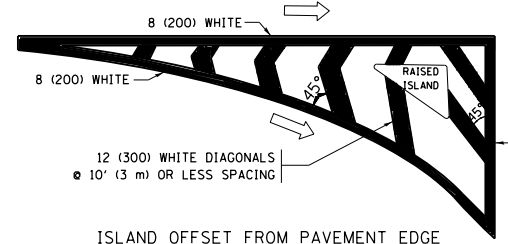
**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**

A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

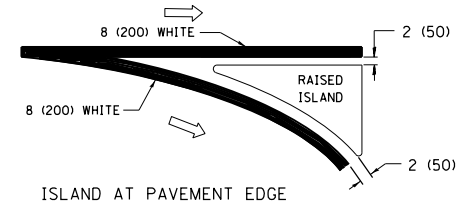


**TYPICAL LEFT (OR RIGHT) TURN LANE
TYPICAL TURN LANE MARKING**

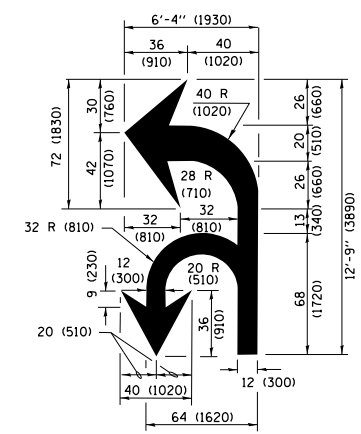
FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



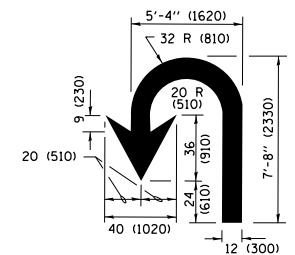
ISLAND OFFSET FROM PAVEMENT EDGE



**ISLAND AT PAVEMENT EDGE
TYPICAL ISLAND MARKING**



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = steedp	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\planroom.dot\illinois.gov\PWIDOT\Documents\1007 Offices\District 1\Projects\012518\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 3/22/2019		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DJ PAGE	34	26
TC-13		CONTRACT NO.	62G59	
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

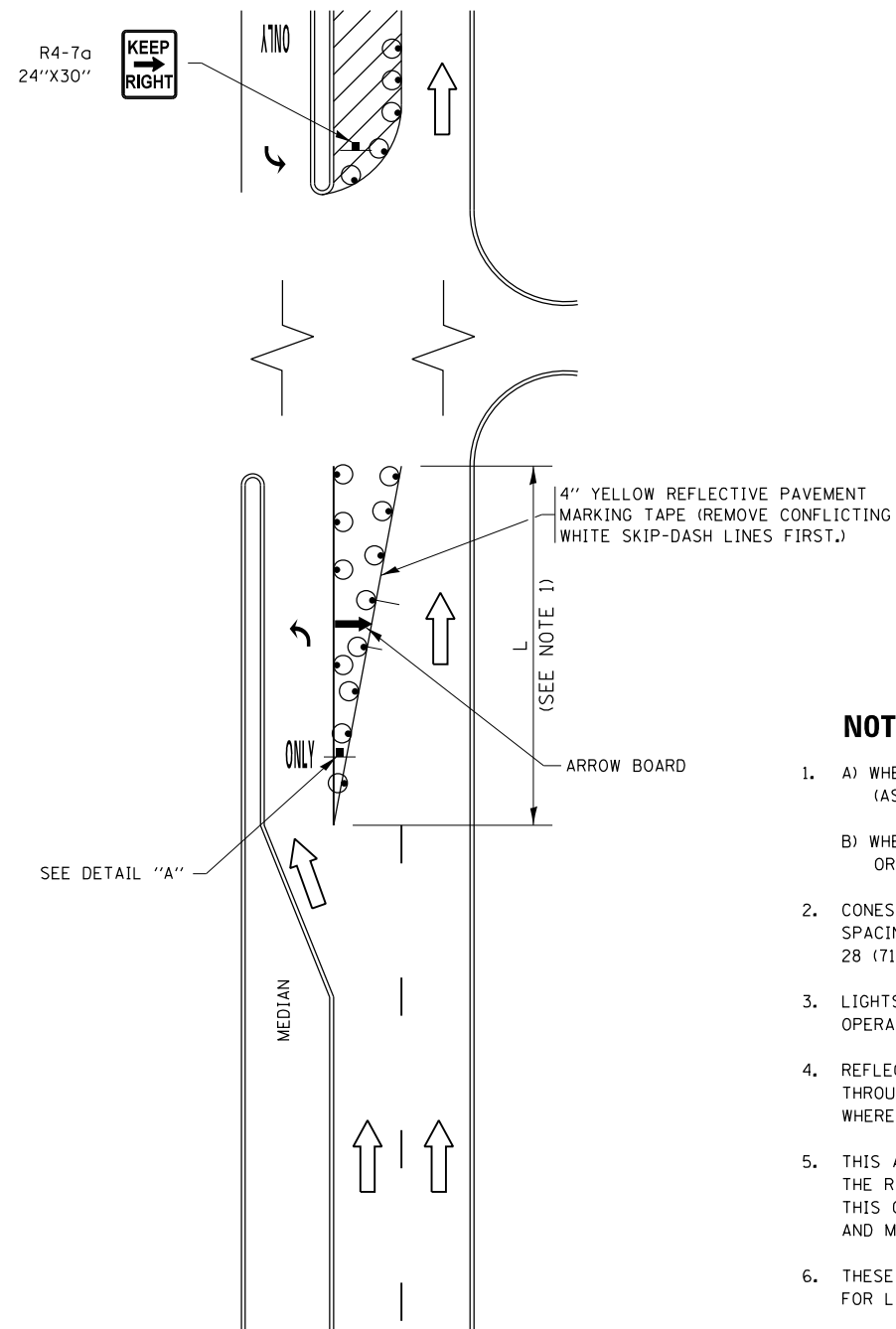


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

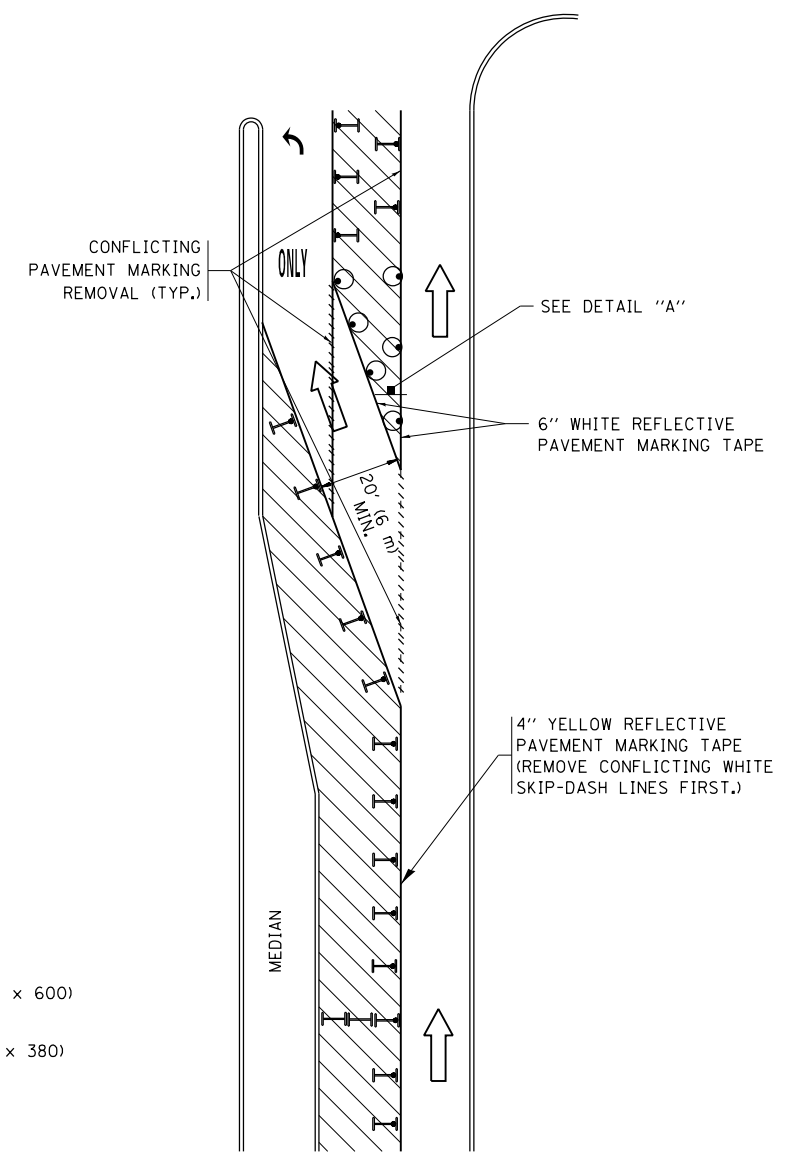
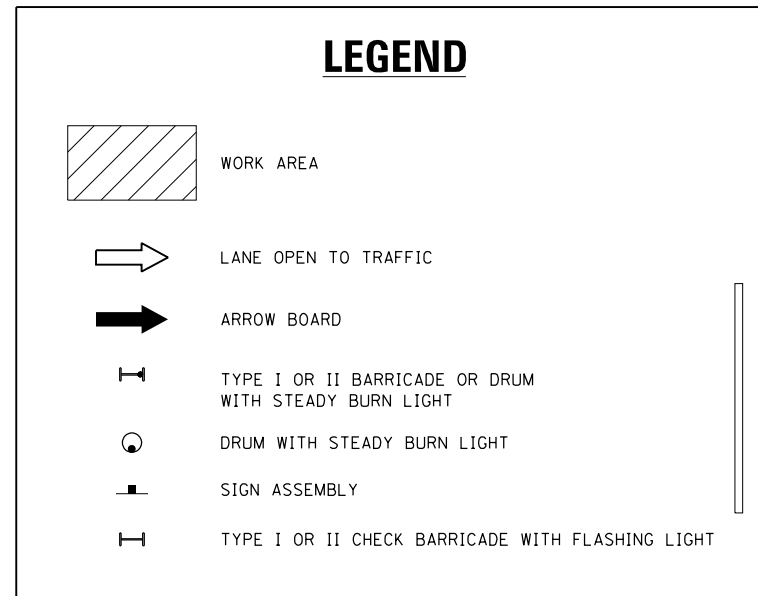
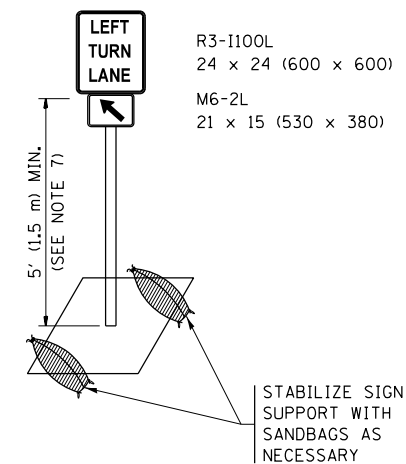


FIGURE 2



NOTES:

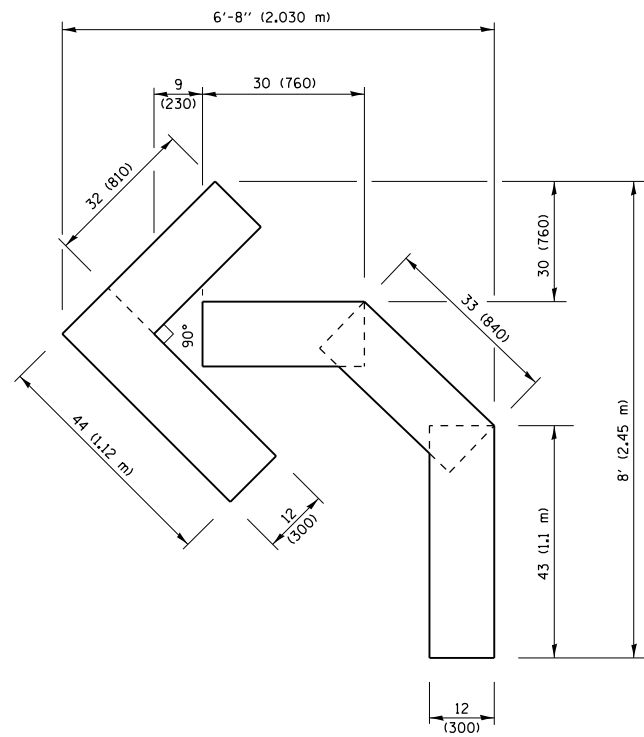
1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PRE REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



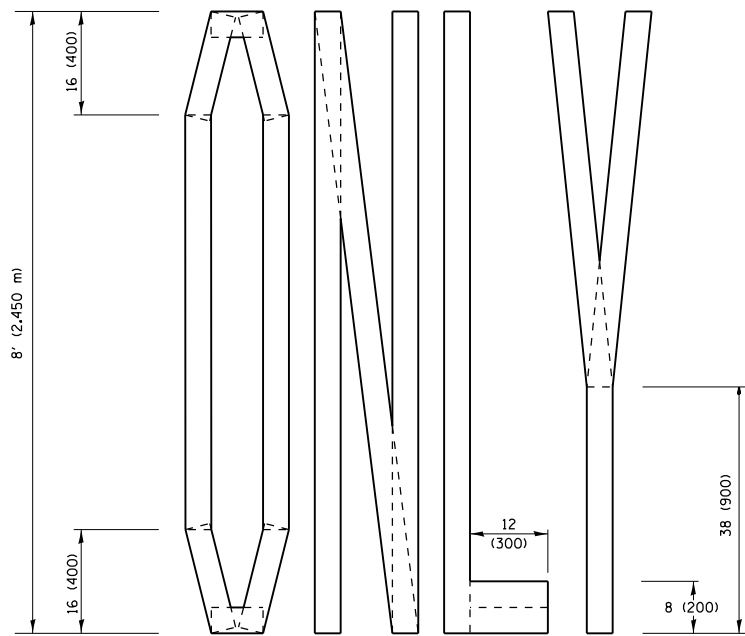
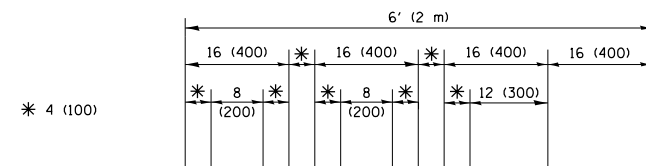
DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

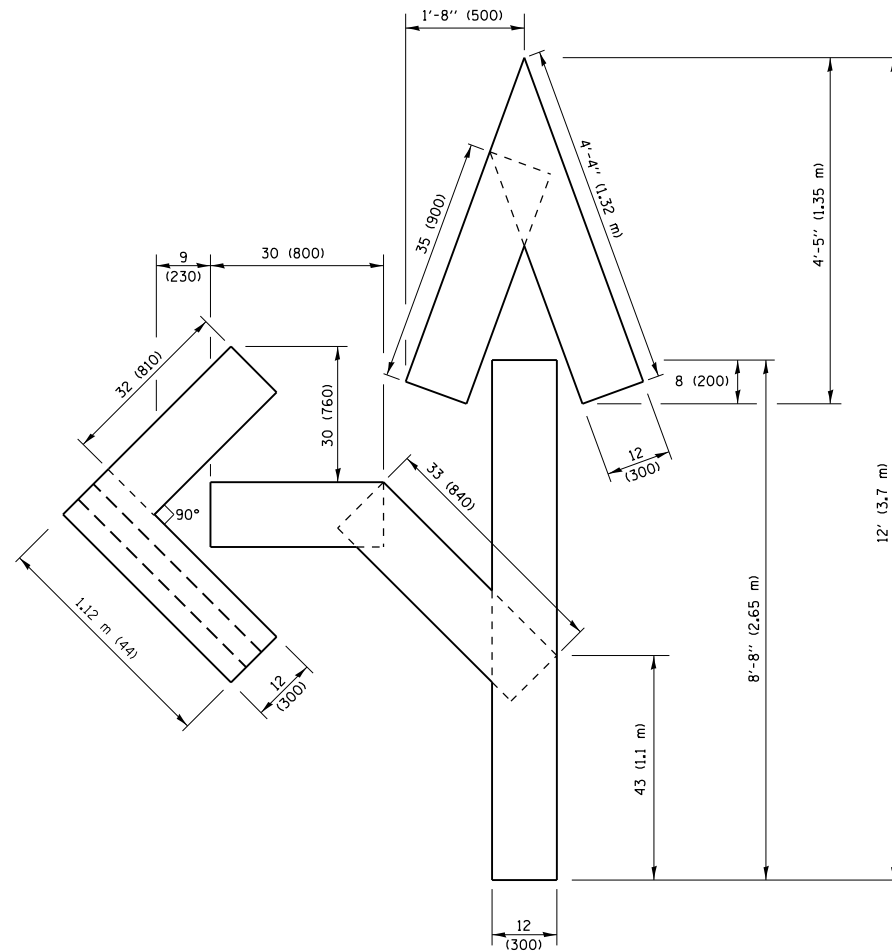
FILE NAME =	USER NAME = steedpa	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default		REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13			870	2018-031-RS-SW	DU PAGE	34	27	
	PLOT SCALE = 100.0000' / 1in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			TC-14		CONTRACT NO. 62059			
	PLOT DATE = 3/22/2019	REVISED - T. RAMMACHER 01-06-00	REVISED -			ILLINOIS FED. AID PROJECT					
				SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.			



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)



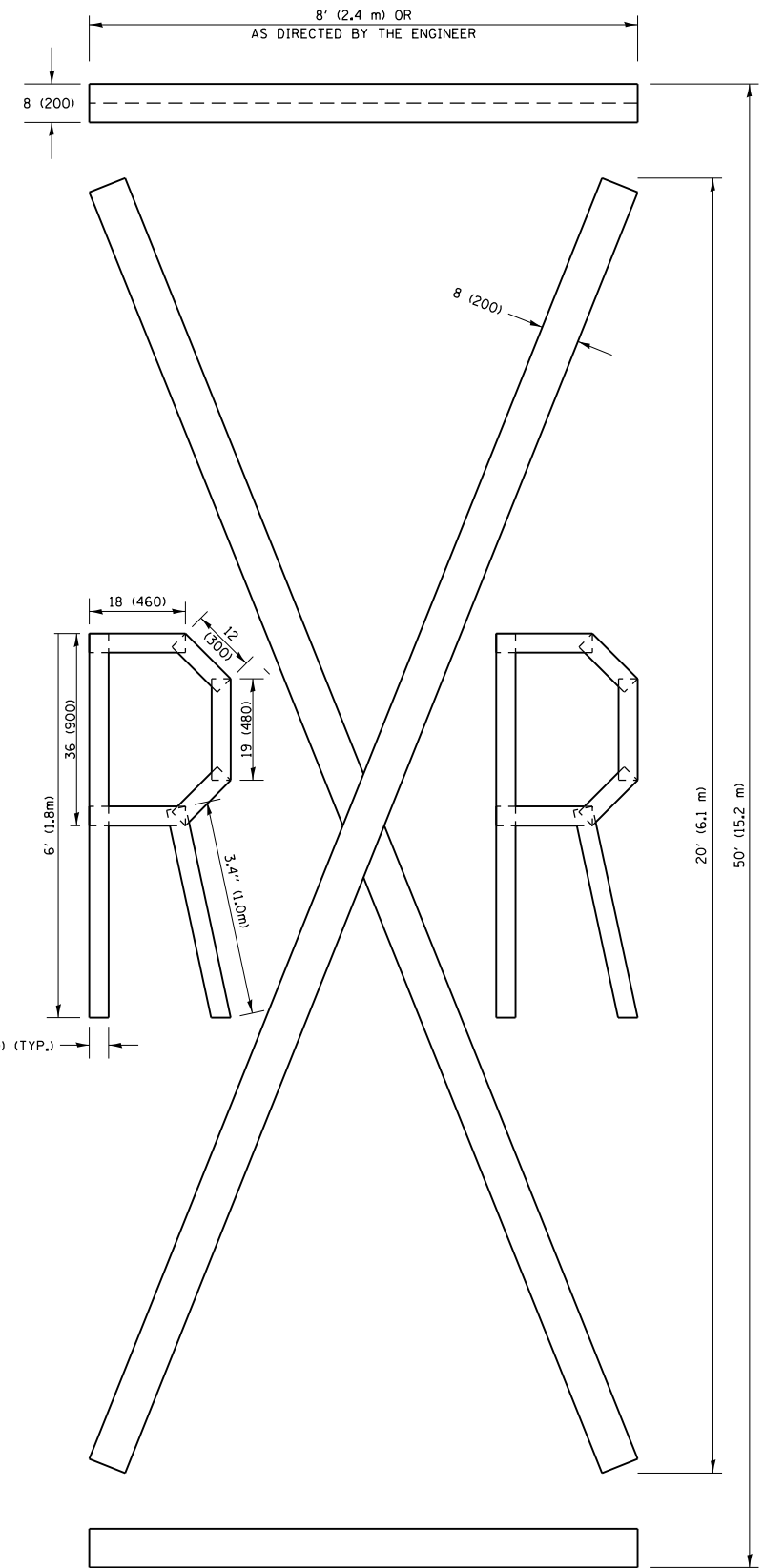
QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

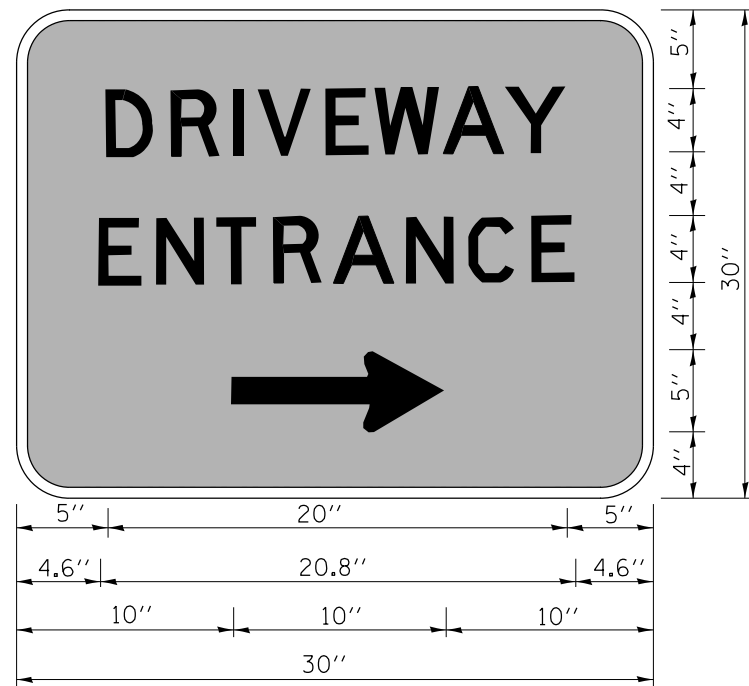
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p:\planroom.dot\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\0125118\DRAWING\Design\DistStd.dgn		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DU PAGE	34	28
TC-16			CONTRACT NO. 62059	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = steedpa	DESIGNED -	REVISED - C. JUCIUS 02-15-07
p:\planroom.dot\illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0125118\DRAWING\Design\DistStd.dgn		DRAWN	REVISED -
	PLOT SCALE = 100.0010' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/22/2019	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

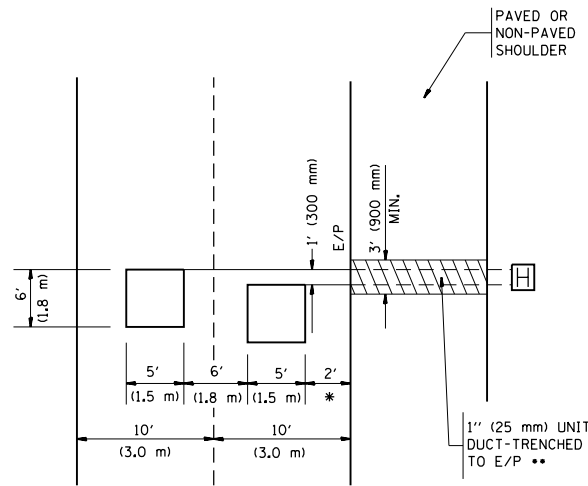
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW		34	29
TC-26			CONTRACT NO. 62059	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



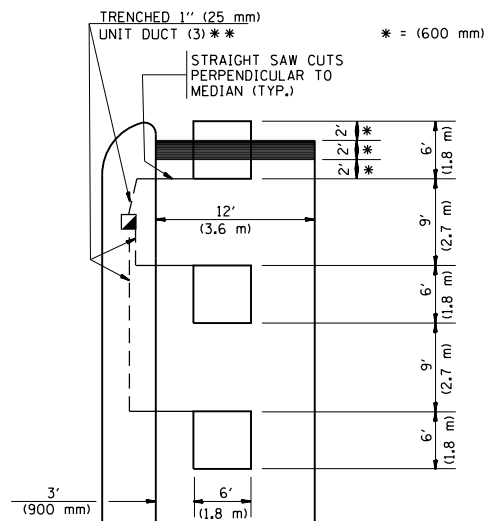
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

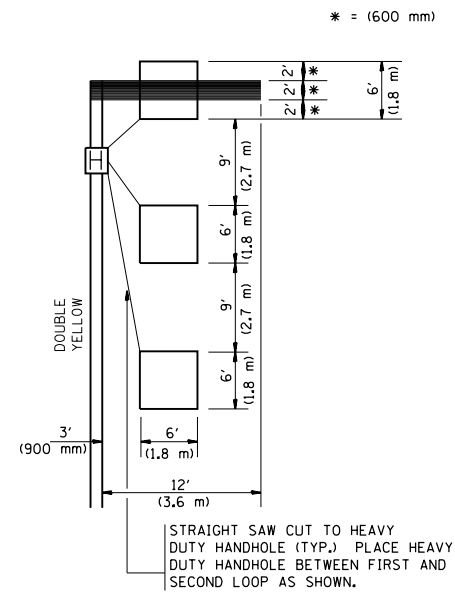


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

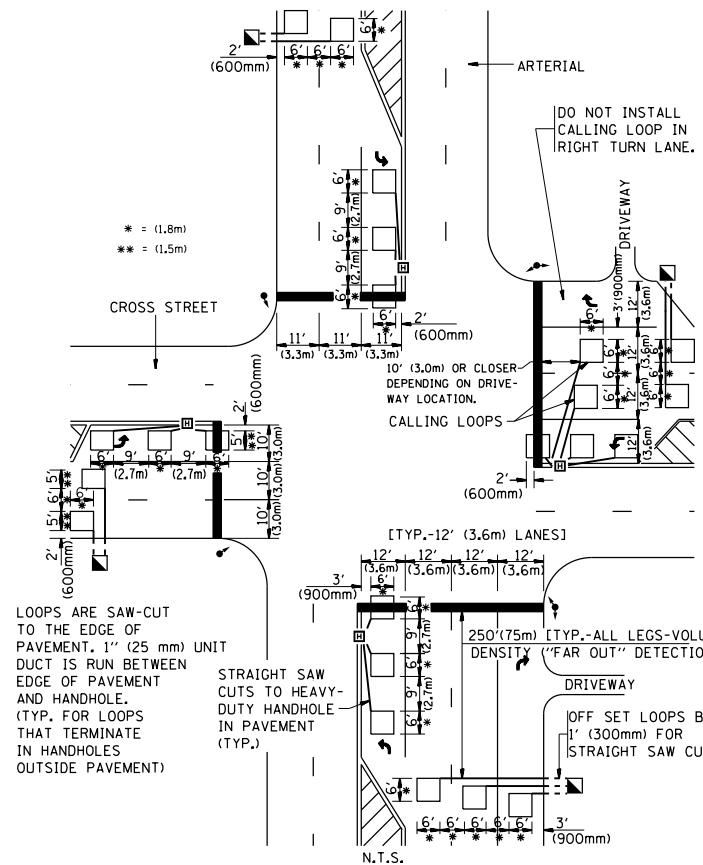
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



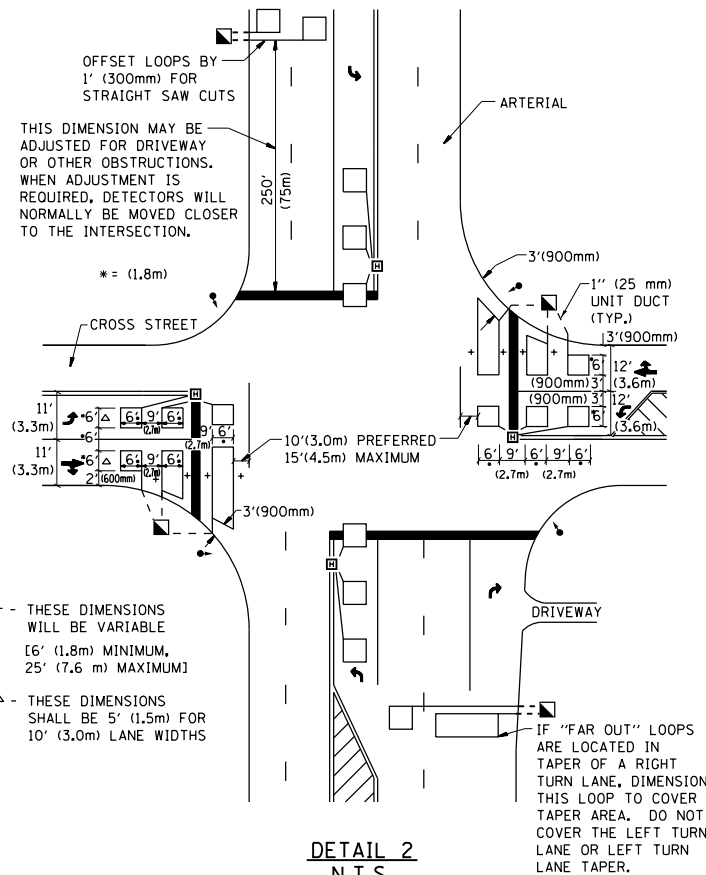
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

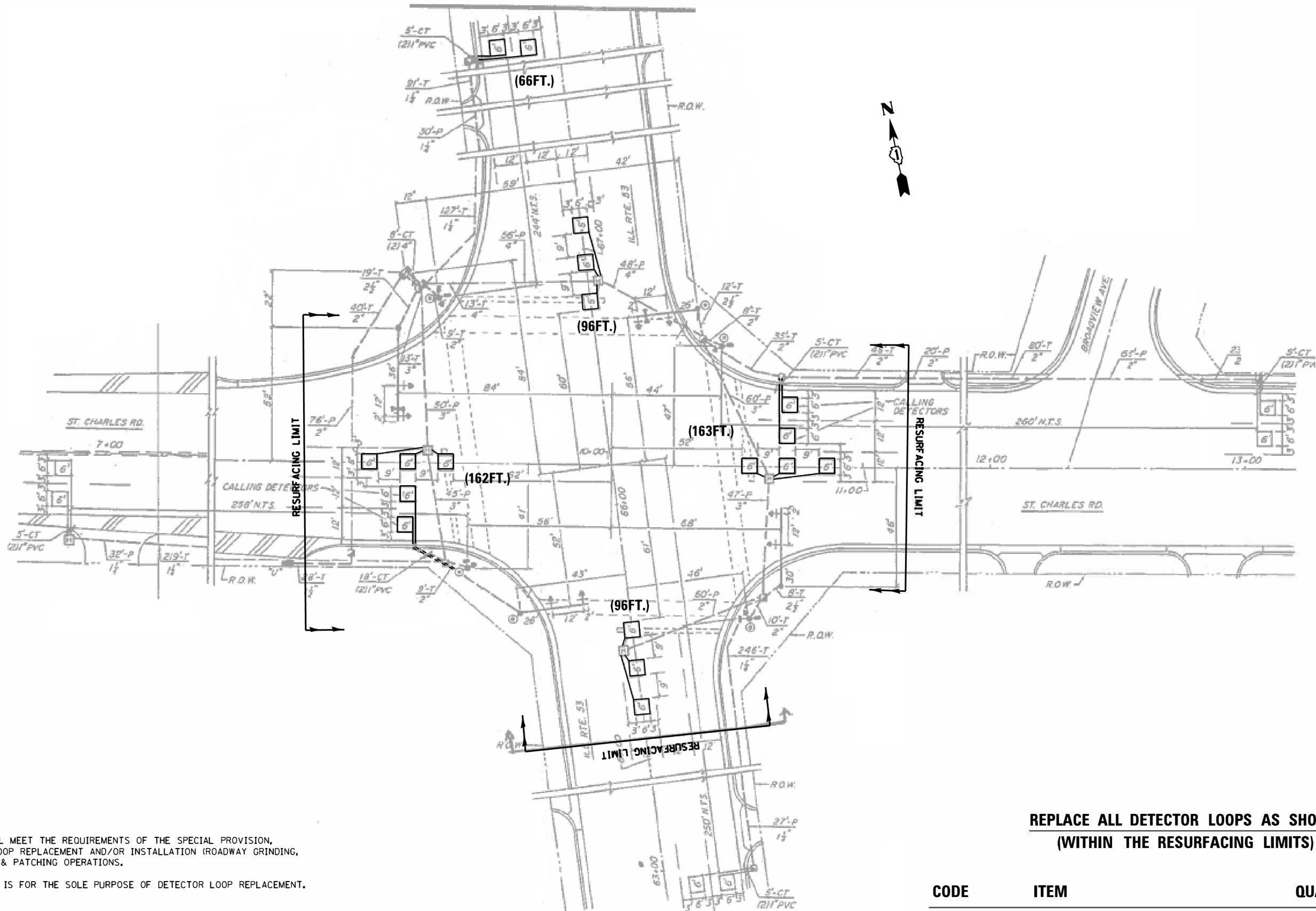
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = steedpo	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0125118\Drawings\Design\DistStd.dgn		DRAWN -	REVISED -			870	2018-031-RS-SW	DJ PAGE	34	30
PLOT SCALE = 100.0010 / 1 in.		CHECKED - R.K.F.	REVISED -			TS-07		CONTRACT NO. 62059		
PLOT DATE = 3/22/2019		DATE -	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	583	FOOT

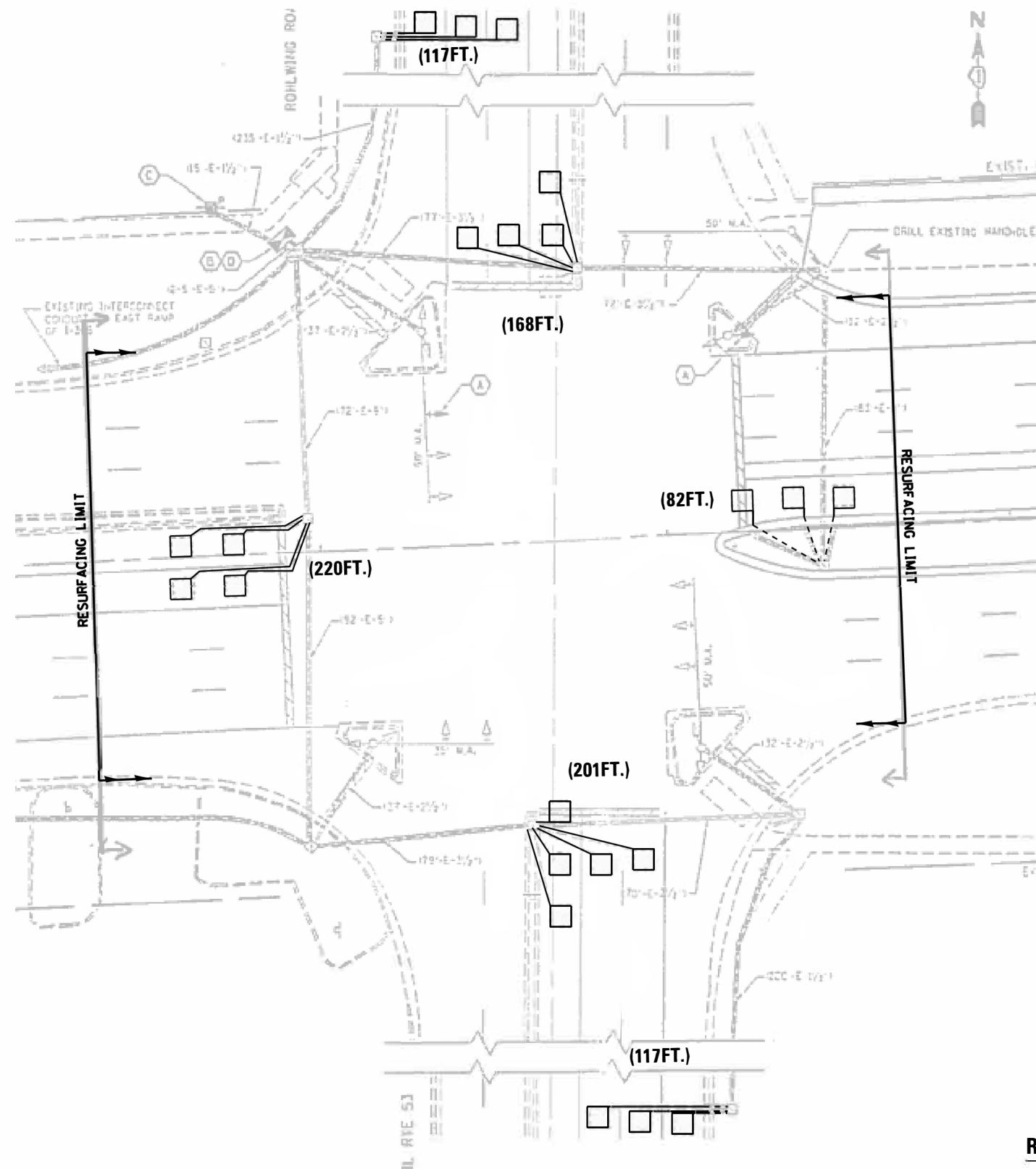
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Default	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/13/2019	DATE - 03/13/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
IL 53 AT ST. CHARLES RD.**

SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DUPAGE	34	31
CONTRACT NO. 62659				
ILLINOIS FED. AID PROJECT				



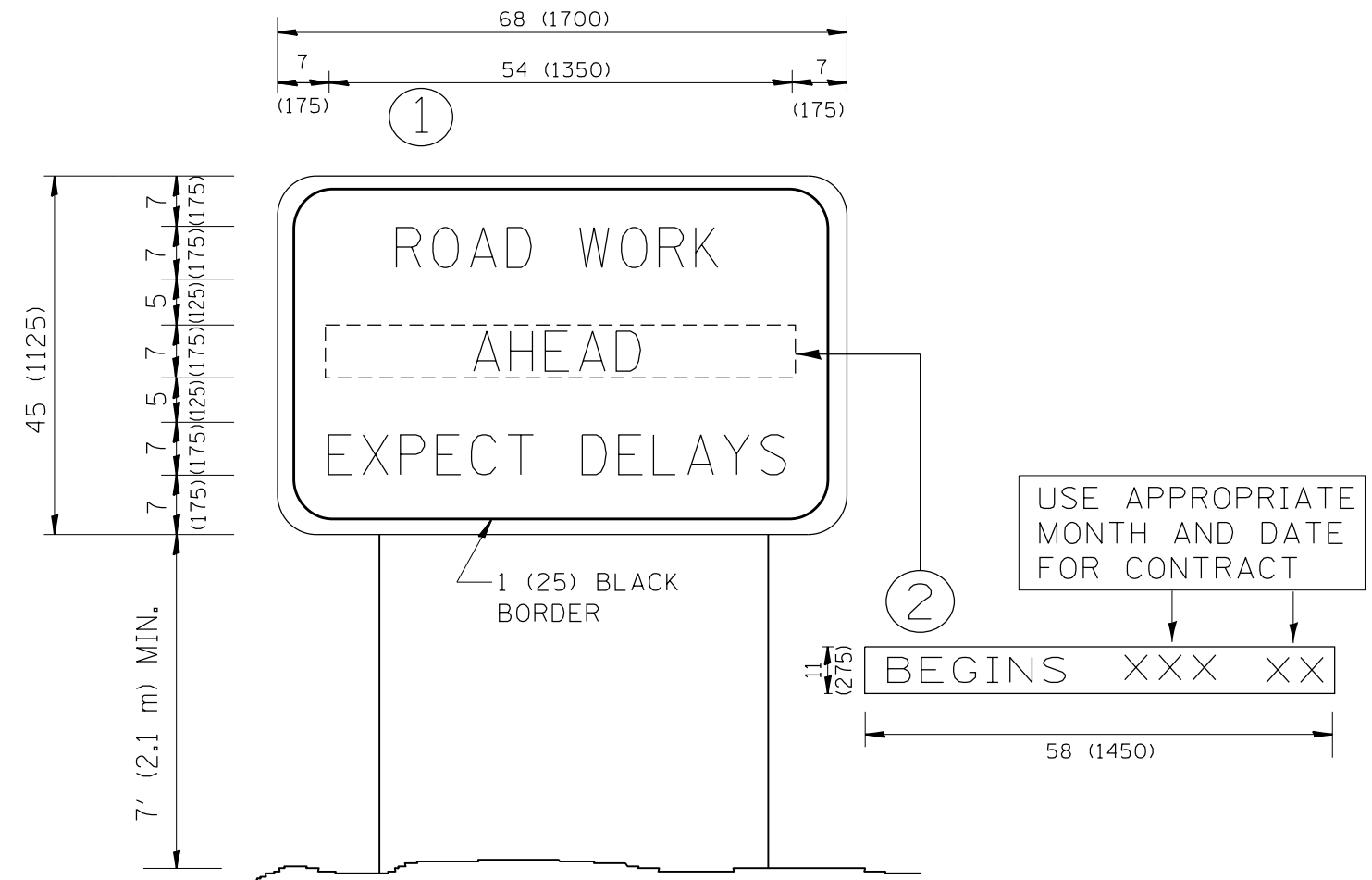
NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	905	FOOT

FILE NAME = P:\Detector Loops\2019\62059\CADD\IL 53	USER NAME = mezag AT IL 64.dgn	DESIGNED - DRAWN - G.M.	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN IL 53 AT IL 64 (NORTH AVE.)		F.A.P. RTE. 870	SECTION 2018-031-RS-SW	COUNTY DUPAGE	TOTAL SHEETS 34	SHEET NO. 32
Default	PLOT SCALE = 40.0000' / 1" / in.	CHECKED - DATE - 03/13/2019	SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____		ILLINOIS FED. AID PROJECT			CONTRACT NO. 62059			



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = steedpa	DESIGNED -	REVISED - R. MIRS 09-15-97
p:\planroom.dot\illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0125118\DRAWING\Design\DistStd.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0010' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 3/22/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2018-031-RS-SW	DU PAGE	34	34
TC-22			CONTRACT NO. 62059	
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				