

CONSTRUCTION PLANS

REHABILITATE RUNWAY 9-27

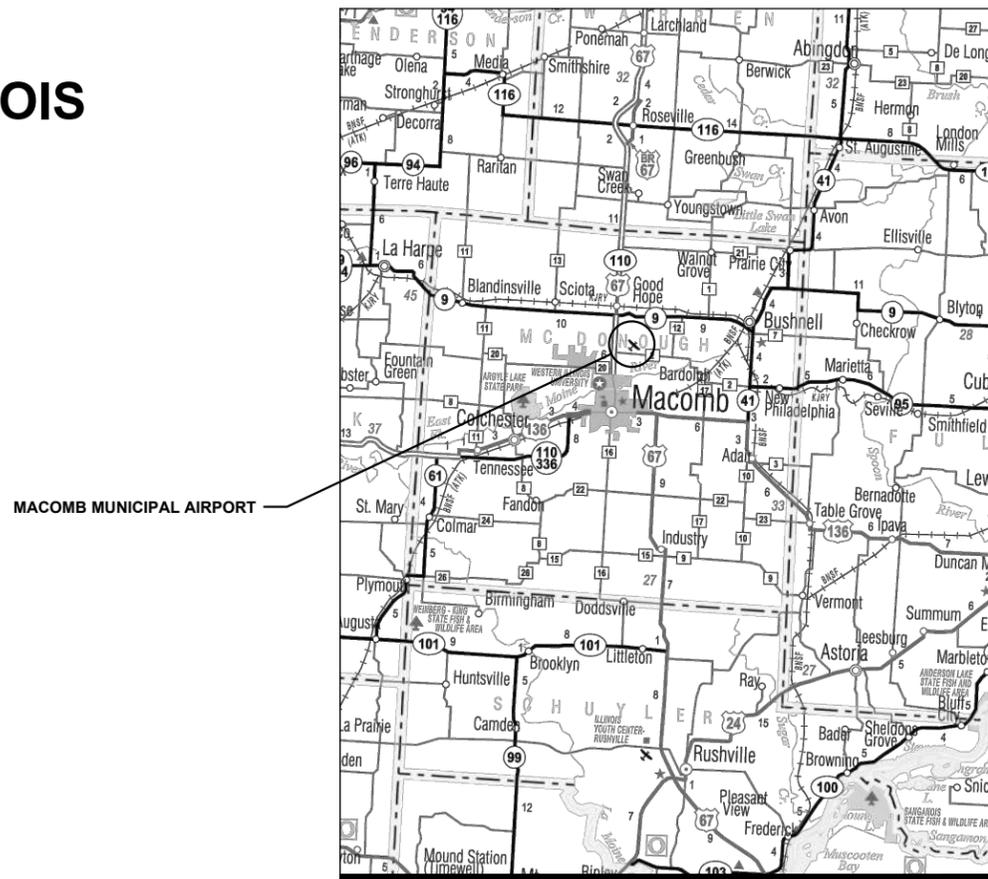
**MACOMB AIRPORT AUTHORITY
MACOMB MUNICIPAL AIRPORT (MQB)
MACOMB, MCDONOUGH COUNTY, ILLINOIS**

**ILLINOIS PROJECT NO. MQB-4945
SBG PROJECT NO. 3-17-SBGP-171/TBD**

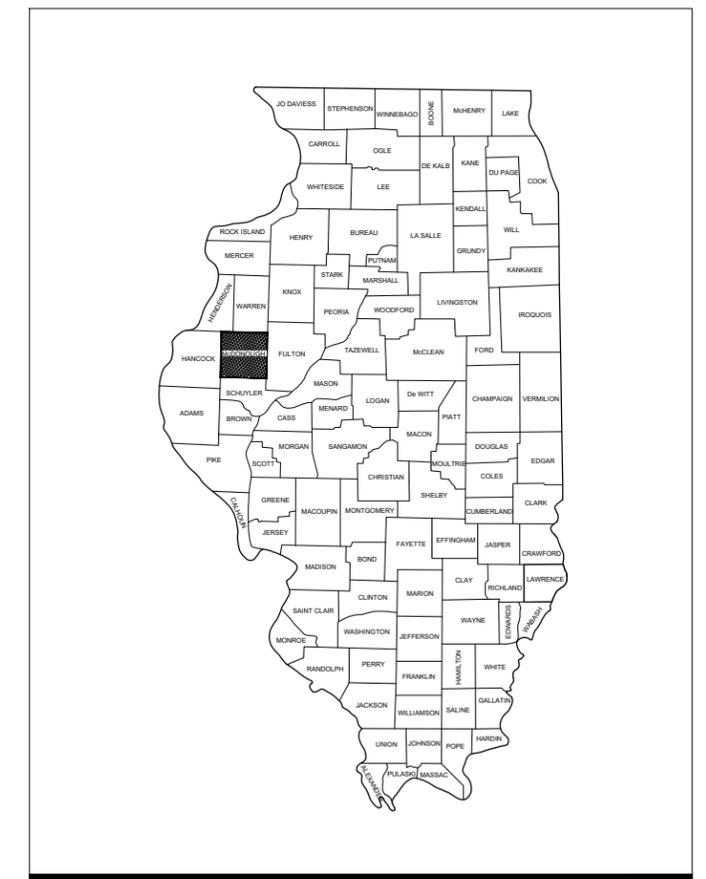
**APRIL 22, 2022
(100% SUBMITTAL)**

NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.



VICINITY MAP



LOCATION MAP

MACOMB MUNICIPAL AIRPORT

No.	Issue/Description	Sheets Changed	Date	By

PREPARED BY:



HANSON
HANSON PROFESSIONAL SERVICES INC.
1525 S. Sixth St.
Springfield, Illinois 62703
Telephone: 217.788.2450
Fax: 217.788.2503

Lindsay Hausman
Lindsay Hausman, P.E. Lic. Exp. 11/30/2023
Project Manager



April 22, 2022
Date

Lee Cobb
Airport Manager

MACOMB MUNICIPAL AIRPORT
MACOMB AIRPORT AUTHORITY
16190 East 1300th Street
Macomb, Illinois 61455
Telephone: 309.833.3324
Fax: 309.836.7721

April 22, 2022
Date

SURVEY NOTES

- ALL COORDINATE VALUES SHOWN IN TABLES ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM, WEST ZONE, NAD-83 (2011). ALL ELEVATIONS ARE REFERENCED TO NAVD 88.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION LAYOUT AND ANY EXTENSION OF THE CONTROL NETWORK NEEDED TO PROPERLY COMPLETE THE WORK.

CONTROL POINTS				
POINT NO.	NORTHING	EASTING	ELEV.	DESCRIPTION
MACPORT	1,403,476.08	2,161,141.29	684.55	DISK IN CONC. MONUMENT
MACPORT AZ MK2	1,403,420.68	2,164,095.73	676.04	DISK IN CONC. MONUMENT

ALIGNMENT DATA				
ALIGNMENT	LOCATION	STATION	EASTING	NORTHING
RUNWAY 9-27	BEGIN	90+00.00	2,158,140.18	1,403,576.76
	END	160+99.32	2,165,239.32	1,403,526.88



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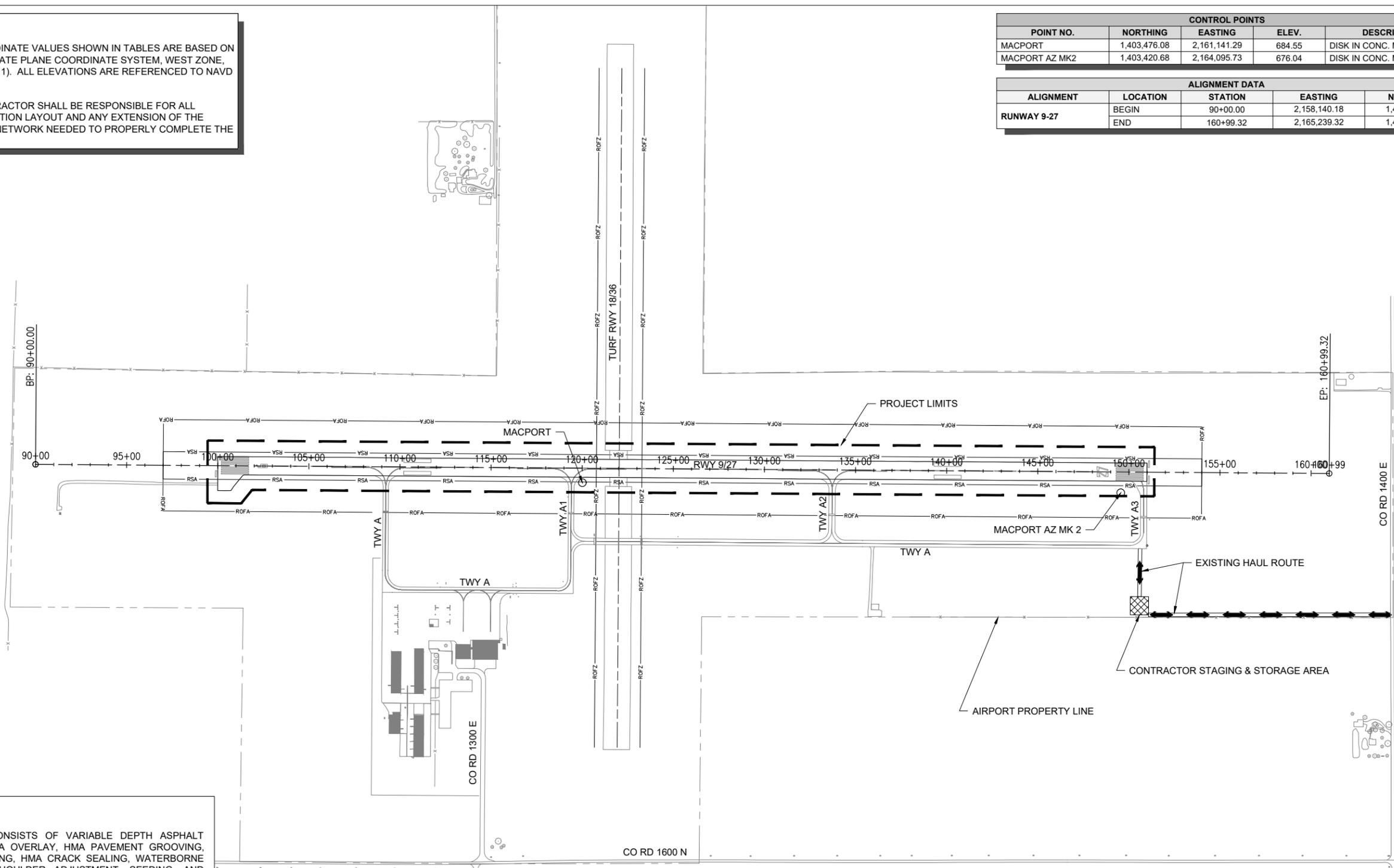
Offices Nationwide
www.hanson-inc.com

Hanson Professional Services Inc.
1525 S. 6th Street
Springfield, IL 62568
phone: 217-788-2450
fax: 217-788-2503

Illinois Licensed
Professional Service Corporation
#184-001084



MACOMB MUNICIPAL AIRPORT
16190 East 1300th Street
Macomb, Illinois 61455
Telephone: 309.833.3324
Fax: 309.836.7721



SCOPE OF WORK

- PROJECT CONSISTS OF VARIABLE DEPTH ASPHALT MILLING, HMA OVERLAY, HMA PAVEMENT GROOVING, HMA PATCHING, HMA CRACK SEALING, WATERBORNE MARKING, SHOULDER ADJUSTMENT, SEEDING, AND MULCHING.

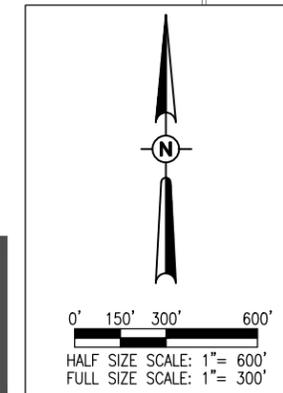
GENERAL

- MACOMB MUNICIPAL AIRPORT IS A NON-TOWER CONTROLLED, GENERAL AVIATION AIRPORT COMPRISED OF ONE PAVED RUNWAY (RUNWAY 9/27) AND ONE TURF RUNWAY (RUNWAY 18/36).
- THE PROPOSED CONSTRUCTION WILL REQUIRE THE CLOSURE OF RUNWAY 9/27 FOR THE DURATION OF THE PROJECT. THE CONNECTING TAXIWAYS AND RUNWAY 18/36 WILL BE CLOSED AT VARIOUS TIMES THROUGHOUT THE PROJECT.
- IN ORDER TO MAINTAIN AN ACTIVE RUNWAY, FLAGGERS WITH TWO-WAY RADIO COMMUNICATION WILL BE REQUIRED DURING PHASE 1.

AIRFIELD SAFETY

- AIRFIELD SAFETY SHALL BE HELD PARAMOUNT AT ALL TIMES. ANY INDIVIDUALS RESPONSIBLE FOR INCURSIONS OR POTENTIAL INCURSIONS WITH AIR TRAFFIC DUE TO NON-COMPLIANCE WITH REQUIREMENTS SET FORTH IN THESE PLANS, SPECIFICATIONS, SPECIAL PROVISIONS, AND FAA ADVISORY CIRCULAR 150/5370-2 (CURRENT EDITION) WILL BE SUBJECT TO AN IMMEDIATE SUSPENSION OF DRIVING PRIVILEGES ON THE AIRPORT OR A COMPLETE RESTRICTION FROM ENTERING THE AIR OPERATIONS AREA ALTOGETHER. THE AIRPORT MANAGER OR RESIDENT ENGINEER/TECHNICIAN MAY STOP THE WORK AT ANY TIME THEY BELIEVE AIRFIELD SAFETY IS BEING COMPROMISED.
- AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. ONLY CONTRACTOR EMPLOYEES SHALL BE ALLOWED WITHIN THE PROJECT LIMITS.

EXISTING	PROPOSED	LEGEND
---	---	RUNWAY SAFETY AREA (RSA)
---	---	RUNWAY OBJECT FREE AREA (ROFA)
---	---	RUNWAY OBSTACLE FREE ZONE (ROFZ)
	▨	STAGING AREA
	↔	TEMPORARY HAUL ROUTE / ACCESS ROUTE (AR150540)



**REHABILITATE
RUNWAY 9-27**

IDA No: MQB-4945
SBGP No:
3-17-SBGP-171/TBD

Contract No.: MB034

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: 04/22/22
PROJECT NO: 20A0074
CAD FILE: G-103-SOW.DWG
DESIGN BY: LDH 3/11/22
DRAWN BY: LDH 3/11/22
REVIEWED BY: KD 03/22/22

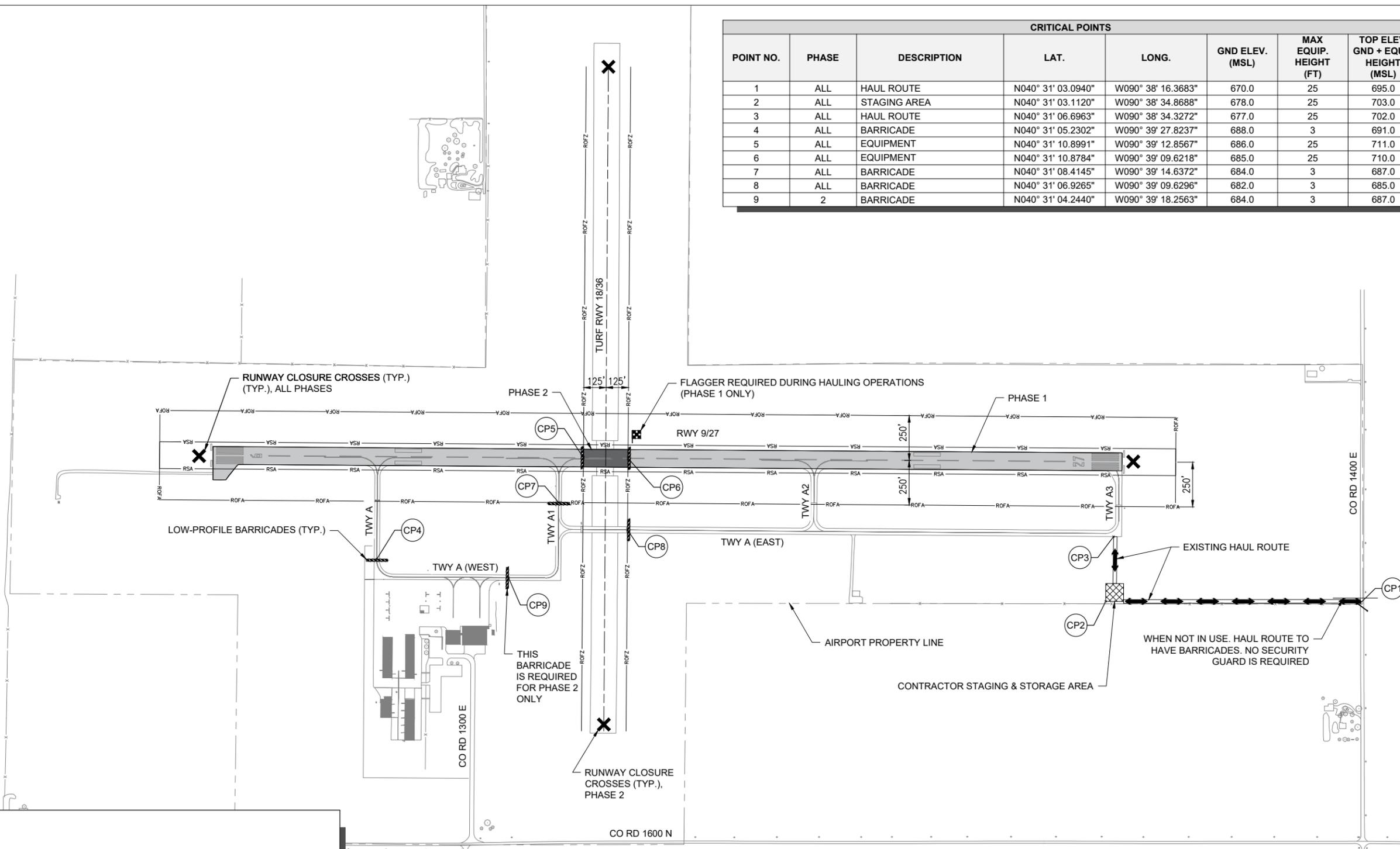
SHEET TITLE

SCOPE OF WORK

APR 20, 2022 2:48 PM HANSON\00682\1\2\JOBS\21A074D\CAD\AIRPORT\SHEETC-103-SOW.DWG



CRITICAL POINTS							
POINT NO.	PHASE	DESCRIPTION	LAT.	LONG.	GND ELEV. (MSL)	MAX EQUIP. HEIGHT (FT)	TOP ELEV. GND + EQUIP HEIGHT (MSL)
1	ALL	HAUL ROUTE	N040° 31' 03.0940"	W090° 38' 16.3683"	670.0	25	695.0
2	ALL	STAGING AREA	N040° 31' 03.1120"	W090° 38' 34.8688"	678.0	25	703.0
3	ALL	HAUL ROUTE	N040° 31' 06.6963"	W090° 38' 34.3272"	677.0	25	702.0
4	ALL	BARRICADE	N040° 31' 05.2302"	W090° 39' 27.8237"	688.0	3	691.0
5	ALL	EQUIPMENT	N040° 31' 10.8991"	W090° 39' 12.8567"	686.0	25	711.0
6	ALL	EQUIPMENT	N040° 31' 10.8784"	W090° 39' 09.6218"	685.0	25	710.0
7	ALL	BARRICADE	N040° 31' 08.4145"	W090° 39' 14.6372"	684.0	3	687.0
8	ALL	BARRICADE	N040° 31' 06.9265"	W090° 39' 09.6296"	682.0	3	685.0
9	2	BARRICADE	N040° 31' 04.2440"	W090° 39' 18.2563"	684.0	3	687.0



REHABILITATE
RUNWAY 9-27

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PROJECT NO: 20A0074
CAD FILE: G-104-CSPP.DWG
DESIGN BY: LDH 3/11/22
DRAWN BY: LDH 3/11/22
REVIEWED BY: KD 03/22/22

SHEET TITLE

SAFETY PLAN -
PHASE 1 AND 2

PHASE 1

WORK TO BE COMPLETED

- MILLING, PATCHING, PAVING, CRACK SEALING, GROOVING, MARKING, SHOULDER ADJUSTMENT, SEEDING, AND MULCHING SHALL BE COMPLETED.

AIRFIELD CLOSURES AND CHANGES

- RUNWAY 9/27 WILL BE CLOSED AND TURF RUNWAY 18/36 WILL REMAIN OPEN.

PHASE 2

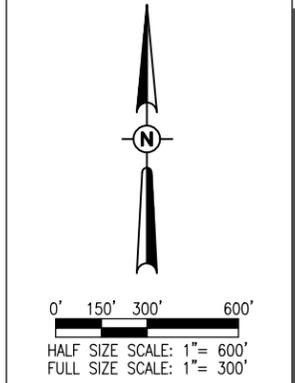
WORK TO BE COMPLETED

- MILLING, PATCHING, PAVING, CRACK SEALING, GROOVING, MARKING, SHOULDER ADJUSTMENT, SEEDING, AND MULCHING SHALL BE COMPLETED.

AIRFIELD CLOSURES AND CHANGES

- RUNWAY 9/27 AND TURF RUNWAY 18/36 WILL BE CLOSED.

EXISTING	PROPOSED	LEGEND
— RSA —		RUNWAY SAFETY AREA (RSA)
— ROFA —		RUNWAY OBJECT FREE AREA (ROFA)
— ROFZ —		RUNWAY OBSTACLE FREE ZONE (ROFZ)
		PROPOSED AIRFIELD WORK AREA
		STAGING AREA (AR150520)
		TEMPORARY RUNWAY CLOSURE CROSS (AR150530)
		LOW PROFILE BARRICADE (AR150530)
		TEMPORARY HAUL ROUTE / ACCESS ROUTE (AR150540)
		ESCORT/FLAGMAN LOCATION
		SAFETY CRITICAL POINT



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SAFETY NOTES

- ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER/TECHNICIAN AT THE PRECONSTRUCTION CONFERENCE, OR DURING THE COURSE OF THE CONTRACT.
- THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND PHASING AREAS PRESENTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
- NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE AIRPORT MANAGER RESERVES THE RIGHT TO SUSPEND OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT.
- PRIOR TO ACCESSING THE AIRFIELD, ANY DESIGNATED CONTRACTOR OR SUBCONTRACTOR EMPLOYEES WHO WILL BE OPERATING OR ESCORTING A VEHICLE ON AN ACTIVE AREA OF THE AIRFIELD MUST BE FAMILIAR WITH THE "FAA GUIDE TO GROUND VEHICLE OPERATIONS", AND KEEP A HARD COPY IN THE VEHICLE FOR REFERENCE. THE GUIDE CAN BE FOUND AT: https://www.faa.gov/airports/runway_safety/media/Ground_Vehicle_Guide_Proof_Final.pdf
- NO CONSTRUCTION VEHICLES SHALL BE DRIVEN ACROSS ANY ACTIVE (OPEN) AIRFIELD PAVEMENT AREA WITHOUT AN APPROPRIATE ESCORT. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL NOT BE PERMITTED WITHIN 250' OF RWY 9/27 AND/OR 125' OF RWY 18/36 (DISTANCES MEASURED FROM ACTIVE CENTERLINES) UNLESS CLOSED OR OTHERWISE NOTED. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL ALSO NOT BE PERMITTED WITHIN 65.5' OF ANY ACTIVE AIRPORT TAXIWAY CENTERLINE OR APRON UNLESS OTHERWISE NOTED.
- CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED AT THE STAGING AREA SHOWN ON THE PLAN VIEW, EXCEPT AS OTHERWISE PROVIDED FOR AT THE PRE-CONSTRUCTION CONFERENCE.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION.
- NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 250' OF ANY ACTIVE RUNWAY, WITHIN 65.5' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA, OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE RESIDENT ENGINEER/TECHNICIAN) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY.
- CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION", LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
- NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT WITHOUT THE APPROVAL OF THE AIRPORT MANAGER AND ADDITIONAL AIRSPACE APPROVAL BY THE FAA. AIRSPACE APPROVALS REQUIRE CONSIDERABLE LEAD TIME AND SHOULD BE REQUESTED WELL IN ADVANCE.
- NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER NO FLARE POTS ARE ALLOWED ON THE PROJECT.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEPED, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- CONTRACTOR SHALL TAKE MEASURES TO AVOID TRACKING BITUMINOUS TACK COAT ASSOCIATED WITH PAVING PROJECTS ONTO ADJACENT PAVEMENT AREAS, ESPECIALLY GROOVED RUNWAY PAVEMENTS, UNLESS SUFFICIENT PROTECTION HAS BEEN APPLIED. HEAVY TRACKING OR DAMAGE TO ADJACENT PAVEMENTS AND GROOVED SURFACES MAY BE CAUSE FOR STOPPING THE WORK UNTIL ACCEPTABLE PROTECTION OR CHANGE IN WORK METHODS HAS BEEN PROVIDED.
- EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS. STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED LIGHTS DURING PERIODS OF LOW VISIBILITY AS REQUIRED.

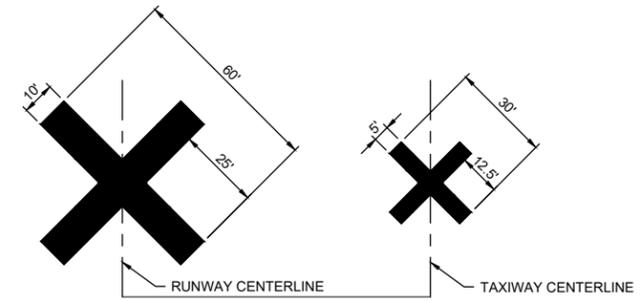
- THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE CHANGE.
- CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER/TECHNICIAN AT NO ADDITIONAL COST.
- CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE.
- THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN AS NECESSARY TO CONTROL DUST.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR IMPLEMENTING MEASURES TO CONTROL OR AVOID CREATING ATTRACTANTS TO WILDLIFE. MEASURES MAY INCLUDE CONTINUOUSLY REMOVING ANY WASTE OR LOOSE MATERIALS, PLACEMENT OF MATERIALS IN APPROPRIATE STORAGE CONTAINERS, PROPERLY MAINTAINING FENCES AND GATES TO PREVENT ACCESS, AND PREVENTING PONDING OF WATER THROUGHOUT THE SITE.
- UNLESS SPECIFIED OTHERWISE, COST FOR SAFETY, STAGING, AND TRAFFIC MAINTENANCE ITEMS IS TO BE CONSIDERED INCIDENTAL TO THE PROJECT. SEPARATE PAYMENT SHALL NOT BE MADE.
- THE CONTRACTOR SHALL HAVE THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD), AS DETAILED IN THE SPECIAL PROVISIONS, SUBMITTED AND APPROVED PRIOR TO BEING ISSUED "NOTICE TO PROCEED".
- ALL RUNWAY/TAXIWAY CLOSURES SHALL BE COORDINATED WITH AIRPORT MANAGEMENT A MINIMUM OF 7 DAYS BEFORE THE DESIRED CLOSING TIME TO ALLOW FOR THE PROPER COORDINATION. AIRPORT MANAGEMENT HAS COMPLETE AUTHORITY IN DETERMINING WHEN THE RUNWAY/TAXIWAY MAY BE CLOSED.
- RUNWAY/TAXIWAY CLOSURE PROCEDURES:
 - CONTACT THE AIRPORT MANAGEMENT OR ASSIGNED REPRESENTATIVE A MINIMUM OF 7 DAYS BEFORE THE DESIRED CLOSING TIME.
 - ISSUANCE OF NOTAM AND DEACTIVATION OF THE APPLICABLE AIRFIELD LIGHTING AND NAVAIDS BY THE AIRPORT MANAGEMENT AND/OR FAA.
 - PLACEMENT OF CROSSES AND BARRICADES.
 - ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS BEGIN WITHIN THE RUNWAY/TAXIWAY AIR OPERATIONS AREA.

RUNWAY/TAXIWAY RE-OPENING PROCEDURES:

- ENSURE ALL PERSONNEL, EQUIPMENT AND MATERIALS ARE CLEAR OF THE AIR OPERATIONS AREA.
- INSPECT THE AREA FOR LOOSE OR TRACKED DEBRIS, PAVEMENT DROP-OFFS, AND OPEN TRENCHES.
- CONTACT AIRPORT MANAGEMENT OR REPRESENTATIVE FOR FINAL INSPECTION OF THE AREA.
- REMOVE BARRICADES AND CROSSES.
- ACTIVATION OF THE AIRFIELD LIGHTING AND NAVAIDS AND CANCELLATION OF THE NOTAM BY THE AIRPORT MANAGEMENT AND/OR FAA.

BARRICADE NOTES

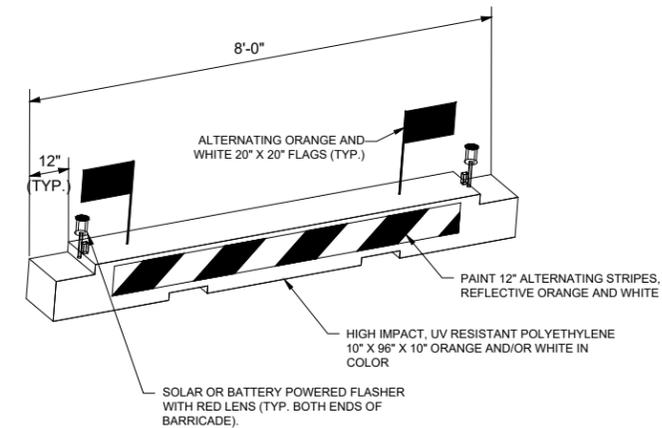
- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- BARRICADES SHALL BE INTERLOCKED END TO END OVER THE LENGTH OF THE PAVEMENT WHERE PROTECTING OPEN RUNWAYS, AND SPACED END TO END A MAXIMUM OF 4 FEET IN OTHER ALL OTHER AREAS. BARRICADES ARE TO BE SET BACK FROM THE ACTIVE RUNWAY OR TAXIWAY CENTERLINE THE DISTANCE AS SHOWN ON THE PLANS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING BARRICADES SHALL BE INCLUDED AS AN INCIDENTAL COST TO THE CONTRACT, UNLESS OTHERWISE NOTED.



TEMPORARY CLOSURE CROSS DETAIL

NOT TO SCALE

- TEMPORARY "CLOSED RUNWAY" AND "CLOSED TAXIWAY" MARKINGS SHALL BE "AVIATION YELLOW"
- TEMPORARY "CLOSED RUNWAY" AND "CLOSED TAXIWAY" MARKINGS SHALL BE CONSTRUCTED OF PLYWOOD, DOUBLE-LAYERED SNOW FENCE OR APPROVED FABRIC AND SHALL BE SECURED TO PAVEMENT BY SANDBAGS OR OTHER APPROVED METHOD.
- TEMPORARY "CLOSED RUNWAY" MARKINGS SHALL BE PLACED OVER THE RUNWAY DESIGNATION NUMBERS UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
- COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING "CLOSED RUNWAY" AND "CLOSED TAXIWAY" MARKINGS SHALL BE INCLUDED AS AN INCIDENTAL COST TO THE CONTRACT, UNLESS OTHERWISE NOTED.



LOW PROFILE AIRCRAFT BARRICADE DETAIL



Engineering | Architecture | Planning | Allied Services

Offices Nationwide
www.hanson-inc.com

Hanson Professional Services Inc.
1525 S. 6th Street
Springfield, IL 62568
phone: 217-788-2450
fax: 217-788-2503

Illinois Licensed
Professional Service Corporation
#184-001084



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16190 East 1300th Street
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SHEET TITLE

**SAFETY PLAN NOTES
& DETAILS**

APR 20, 2022 2:48 PM HALSUM0682 1:12 LOBS121A074D\CAD\AIRPORT\SHHEETG-104-CSPP.DWG



SITE DETAILS

AREA OF CONSTRUCTION SITE: 14.27 ACRES
AREA OF SOIL DISTURBANCE: 2.42 ACRES

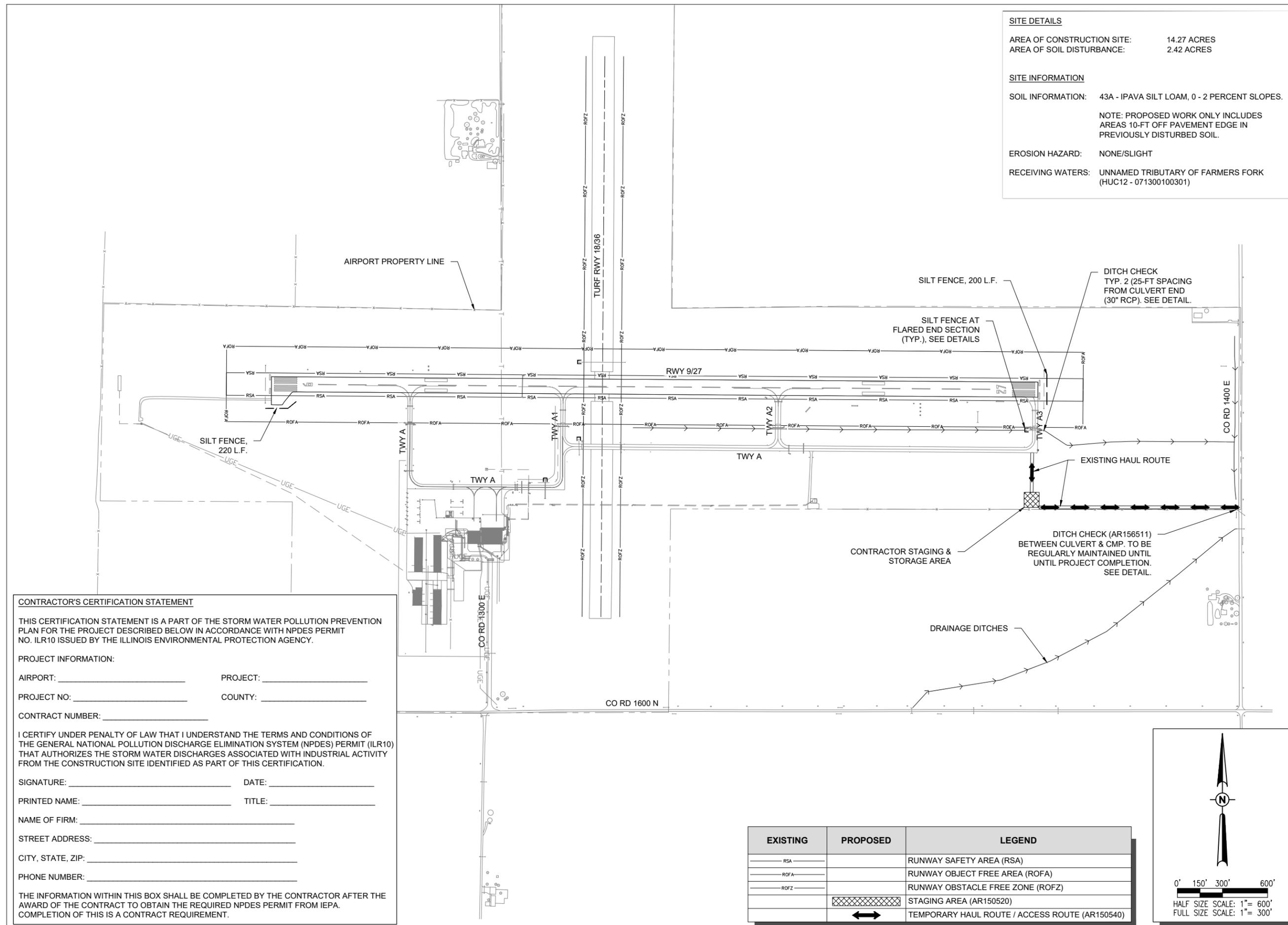
SITE INFORMATION

SOIL INFORMATION: 43A - IPAVA SILT LOAM, 0 - 2 PERCENT SLOPES.

NOTE: PROPOSED WORK ONLY INCLUDES AREAS 10-FT OFF PAVEMENT EDGE IN PREVIOUSLY DISTURBED SOIL.

EROSION HAZARD: NONE/SLIGHT

RECEIVING WATERS: UNNAMED TRIBUTARY OF FARMERS FORK (HUC12 - 071300100301)



CONTRACTOR'S CERTIFICATION STATEMENT

THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PROJECT INFORMATION:

AIRPORT: _____ PROJECT: _____

PROJECT NO: _____ COUNTY: _____

CONTRACT NUMBER: _____

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SIGNATURE: _____ DATE: _____

PRINTED NAME: _____ TITLE: _____

NAME OF FIRM: _____

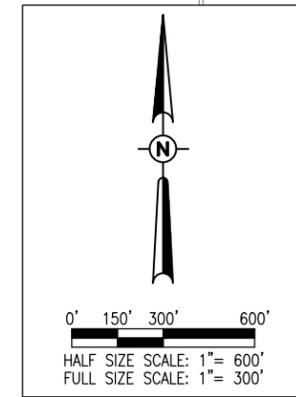
STREET ADDRESS: _____

CITY, STATE, ZIP: _____

PHONE NUMBER: _____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.

EXISTING	PROPOSED	LEGEND
— RSA —		RUNWAY SAFETY AREA (RSA)
— ROFA —		RUNWAY OBJECT FREE AREA (ROFA)
— ROFZ —		RUNWAY OBSTACLE FREE ZONE (ROFZ)
	▨	STAGING AREA (AR150520)
	↔	TEMPORARY HAUL ROUTE / ACCESS ROUTE (AR150540)



**REHABILITATE
RUNWAY 9-27**

IDA No: MQB-4945
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		DES	DWN	REV

ISSUE: 04/22/22
PROJECT NO: 20A0074
CAD FILE: C-104-SWPPP.DWG
DESIGN BY: LDH 3/11/22
DRAWN BY: LDH 3/11/22
REVIEWED BY: KD 03/22/22

SHEET TITLE

**STORMWATER
POLLUTION
PREVENTION PLAN
(SWPPP)**

SEDIMENTATION AND EROSION CONTROL NOTES:

- A. SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
- B. FOR THOSE DEVELOPMENTS THAT REQUIRE A DESIGNATED EROSION CONTROL INSPECTOR (DECI), INSPECTIONS AND DOCUMENTATION SHALL BE PERFORMED, AT A MINIMUM:
 - UPON COMPLETION OF SEDIMENT AND RUNOFF CONTROL MEASURES (INCLUDING PERIMETER CONTROLS AND DIVERSIONS), PRIOR TO PROCEEDING WITH ANY OTHER EARTH DISTURBANCE OR GRADING.
 - AFTER EVERY SEVEN (7) CALENDAR DAYS OR STORM EVENT WITH GREATER THAN 0.5 INCH OF RAINFALL OR LIQUID EQUIVALENT PRECIPITATION.
- C. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. IF STRIPPING, CLEARING, GRADING, OR LANDSCAPING ARE TO BE DONE IN PHASES, THE PERMITTEE SHALL PLAN FOR APPROPRIATE SOIL EROSION AND SEDIMENT CONTROL MEASURES.
- D. A STABILIZED MAT OF CRUSHED STONE MEETING IDOT GRADATION CA-01 UNDERLAIN WITH FILTER FABRIC AND IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL, OR OTHER APPROPRIATE MEASURE(S) AS APPROVED BY THE ENFORCEMENT OFFICER, SHALL BE INSTALLED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE. SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT OF WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
- E. TEMPORARY DIVERSIONS SHALL BE CONSTRUCTED AS NECESSARY TO DIRECT ALL RUNOFF FROM HYDROLOGICALLY DISTURBED AREAS TO AN APPROPRIATE SEDIMENT TRAP OR BASIN.
- F. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE OR REDISTURBANCE.
- G. ALL STOCKPILES SHALL HAVE APPROPRIATE MEASURES TO PREVENT EROSION. STOCKPILES SHALL NOT BE PLACED IN FLOOD PRONE AREAS OR WETLANDS AND DESIGNATED BUFFERS.
- H. SLOPES STEEPER THAN 3H:1V SHALL BE STABILIZED WITH APPROPRIATE MEASURES AS APPROVED BY THE ENFORCEMENT OFFICER.
- I. APPROPRIATE EROSION CONTROL BLANKET SHALL BE INSTALLED ON ALL INTERIOR DETENTION BASIN SIDE SLOPES BETWEEN THE NORMAL WATER LEVEL AND HIGH WATER LEVEL.
- J. STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- K. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION. DISCHARGES SHALL BE ROUTED THROUGH AN APPROVED ANIONIC POLYMER DEWATERING SYSTEM OR A SIMILAR MEASURE AS APPROVED BY THE ENFORCEMENT OFFICER. DEWATERING SYSTEMS SHOULD BE INSPECTED DAILY DURING OPERATIONAL PERIODS. THE ENFORCEMENT OFFICER, OR APPROVED REPRESENTATIVE, MUST BE PRESENT AT THE COMMENCEMENT OF DEWATERING ACTIVITIES.
- L. IF INSTALLED SOIL EROSION AND SEDIMENT CONTROL MEASURES DO NOT MINIMIZE SEDIMENT LEAVING THE DEVELOPMENT SITE, ADDITIONAL MEASURES SUCH AS ANIONIC POLYMERS OR FILTRATION SYSTEMS MAY BE REQUIRED BY THE ENFORCEMENT OFFICER.
- M. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE PROPERTY OWNER SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR.
- N. ALL TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.
- O. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, ENFORCEMENT OFFICER, OR OTHER GOVERNING AGENCY.

STORM WATER POLLUTION PREVENTION NOTES

GENERAL

THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE. PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.

THE MAINTENANCE OF ALL STORM WATER POLLUTION PREVENTION MEASURES IS INCIDENTAL TO THE ASSOCIATED ITEM.

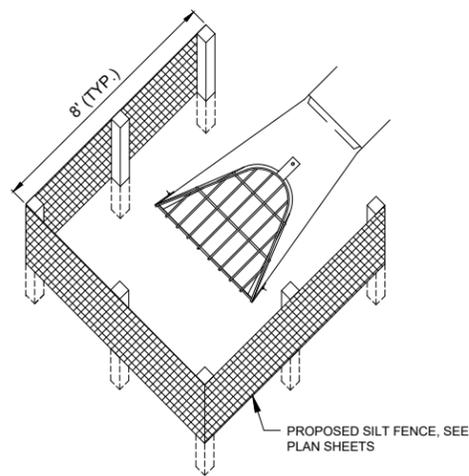
POLLUTION PREVENTION MEASURES

THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHEREVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES.

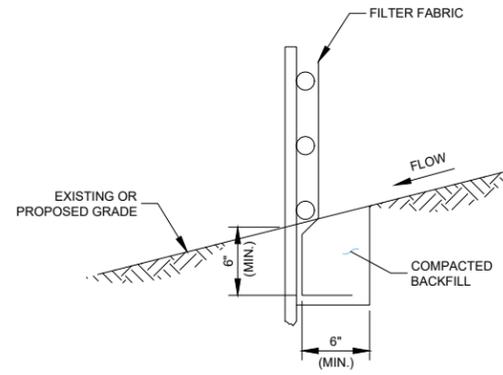
POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EACH ITEM.

THE CONTRACTOR IS TO MAINTAIN AND ADJUST, REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.

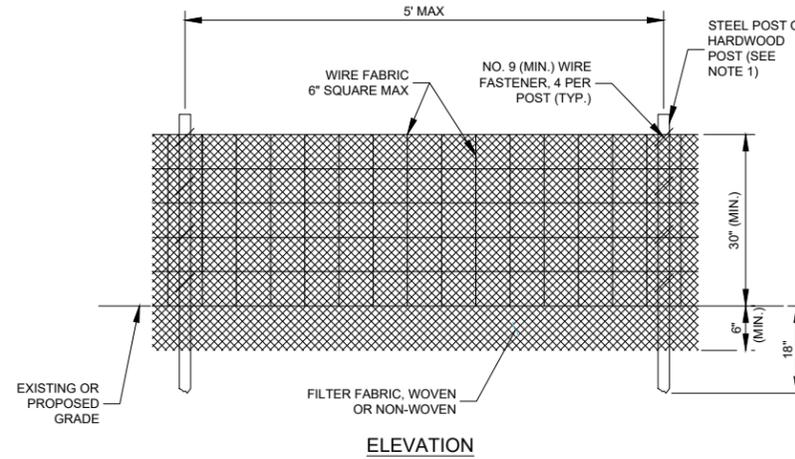
ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES ARE EXISTING ON SITE LOCATED AT DRAINAGE FACILITIES AND ALONG THE PROPERTY LINE.



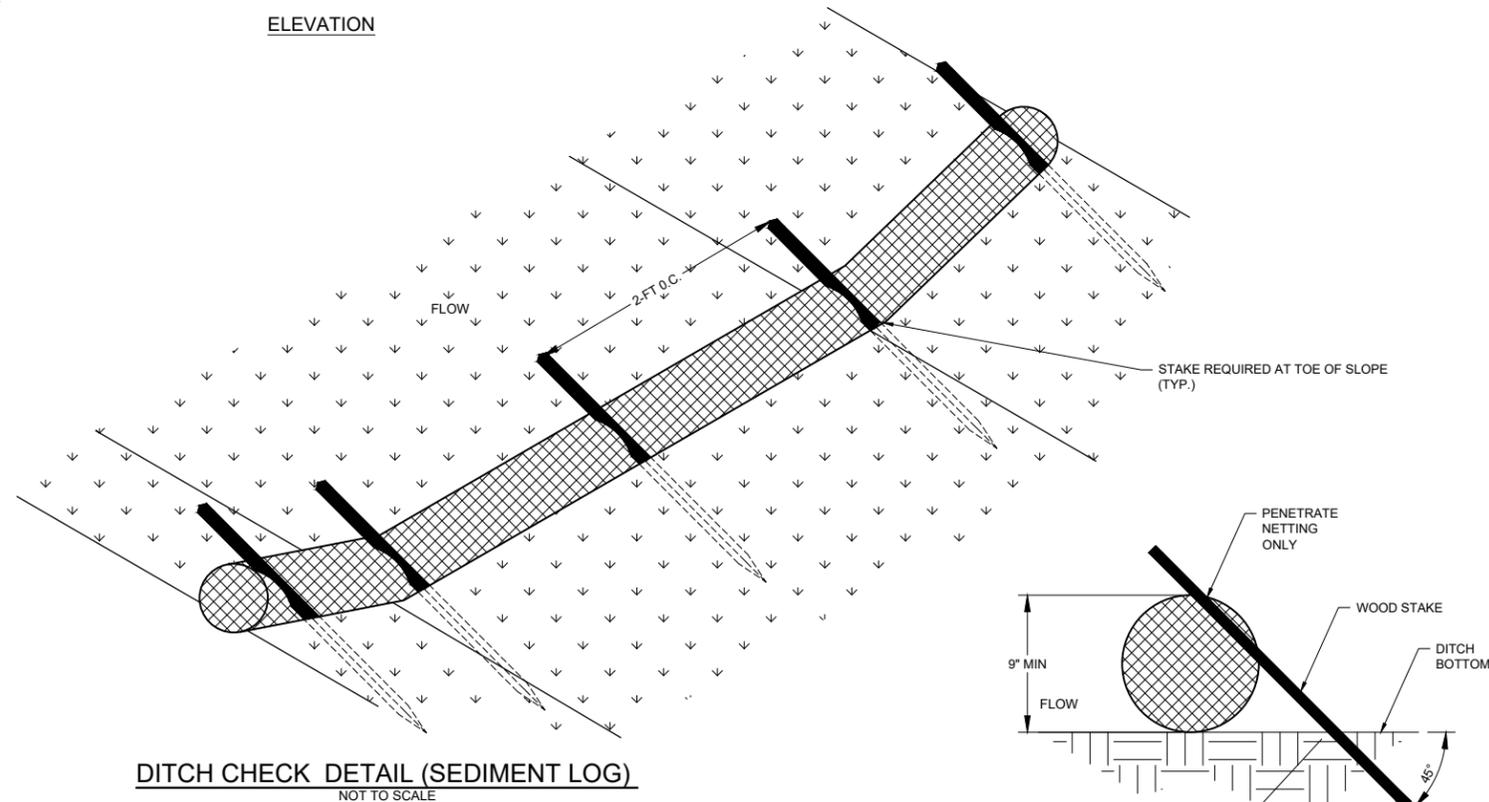
SILT FENCE PLACEMENT AT FLARED END SECTIONS (FES)



FABRIC ANCHOR DETAIL



ELEVATION



DITCH CHECK DETAIL (SEDIMENT LOG)
NOT TO SCALE

DITCH CHECK STAKING DETAIL (SEDIMENT LOG)
NOT TO SCALE

NOTES:

1. FENCE POST SHALL BE EITHER STEEL "T" LINE POST OR HARDWOOD POST WITH A MINIMUM SECTIONAL AREA OF 2.0 SQUARE INCHES. A CARPENTER'S (NOMINAL) 2"x2" POST WILL MEET SPECIFICATIONS.
2. TOP AND BOTTOM WIRE OF WIRE FABRIC SHALL BE MINIMUM GAGE NO. 9. INTERMEDIATE WIRES OF THE WIRE FABRIC SHALL BE MINIMUM GAGE NO. 11.
3. WIRE FABRIC SHALL BE SECURELY FASTENED TO FENCE POSTS WITH NO. 9 GAGE WIRE MINIMUM. FOUR (4) FASTENERS PER POST REQUIRED.
4. FILTER FABRIC SHALL BE SECURELY FASTENED TO WIRE FABRIC AND POSTS WITH TIES OR STAPLES SPACED AT 12" APART AT THE TOP, MIDDLE AND BOTTOM.
5. WHEN TWO SECTIONS OF FILTER FABRIC MEET, THEY SHALL BE OVERLAPPED BY 6" AND FOLDED AND ATTACHED TO THE WIRE FABRIC AT A POST.
6. FILTER FABRIC SHALL BE IN ACCORDANCE WITH SPECIAL PROVISIONS WITH APPARENT OPENING SIZE (AOS) OF AT LEAST 40 FOR NONWOVEN AND WOVEN. THE FABRIC MUST MEET THE APPLICABLE STANDARDS OF AASHTO 288-00 (Article IV, Section B.1.1.1, AS AMENDED), OR EQUIVALENT.
7. A MAXIMUM OF 5 FEET IS USED FOR POST-TO-POST SPACING.
8. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
9. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
10. SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. PERIODIC INSPECTION SHALL BE PERFORMED AND REQUIRED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN EVENT.
11. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED AND REPLACED WHEN BULGES DEVELOP IN THE SILT FENCE.
12. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE).
13. FENCE POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
14. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.



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 Macomb, Illinois 61455
 Telephone: 309.833.3324
 Fax: 309.836.7721

REHABILITATE
 RUNWAY 9-27

IDA No: MQB-4945
 SBGP No:
 3-17-SBGP-171/TBD

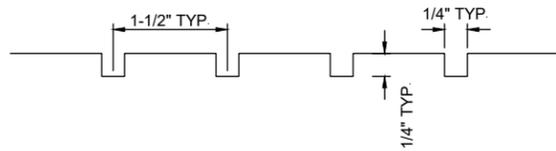
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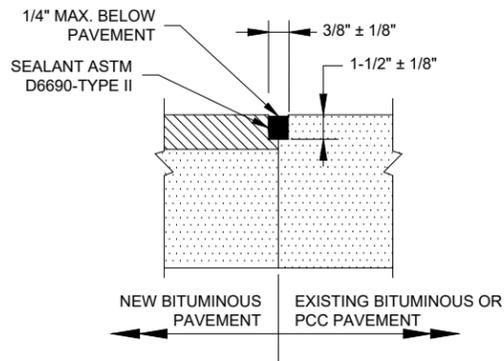
SWPPP NOTES &
 DETAILS



GROOVING DETAIL
NOT TO SCALE

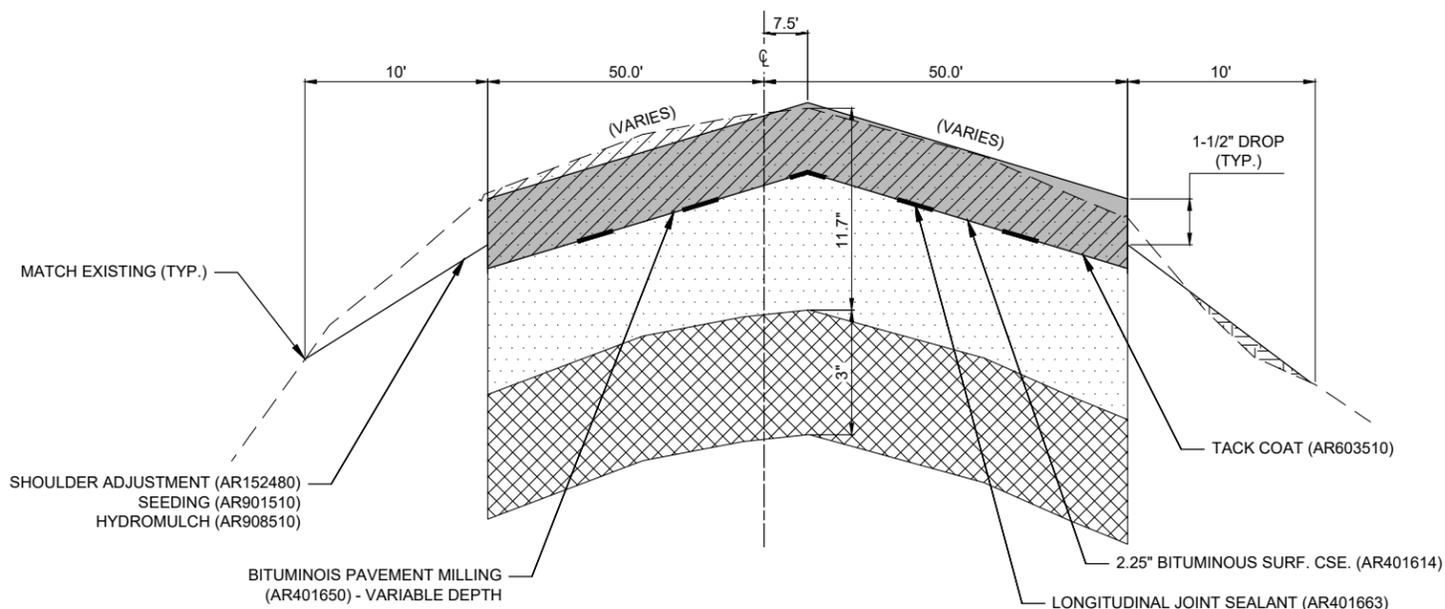
AR401640 - BITUMINOUS PAVEMENT GROOVING

1. THE PROPOSED GROOVES WILL BE CUT PERPENDICULAR TO THE RUNWAY CENTERLINE AND WILL TERMINATE 10' FROM THE RUNWAY PAVEMENT EDGE.
2. THE GROOVES WILL BE 1/4" WIDE, 1/4" DEEP AND PLACED ON 1 1/2" CENTERS
3. TOLERANCES ON THE GROOVING WILL BE 1 1/2" IN ALIGNMENT FOR 75', ±1/16" IN DEPTH, +1/16" IN WIDTH, -1/8" IN GROOVE SPACING.
4. GROOVES SHALL BE SAWED NO LESS THAN 6" AND NO MORE THAN 18" FROM AN IN-PAVEMENT LIGHT.
5. GROOVES SHALL NOT BE CLOSER THAN 3" OR MORE THAN 9" FROM TRANSVERSE JOINTS IN CONCRETE PAVEMENTS.
6. ALL SOLID MATERIAL GENERATED FROM GROOVING OPERATIONS WILL BE PICKED UP AND DISPOSED OF AT AN APPROVED LAND FILL OFF THE AIRPORT SITE, AT THE CONTRACTOR'S OWN EXPENSE.
7. AT THE END OF EACH WORK DAY THE RUNWAY WILL BE FLUSHED, CLEANED AND CLEARED OF ALL MATERIAL GENERATED BY THE GROOVING OPERATION.

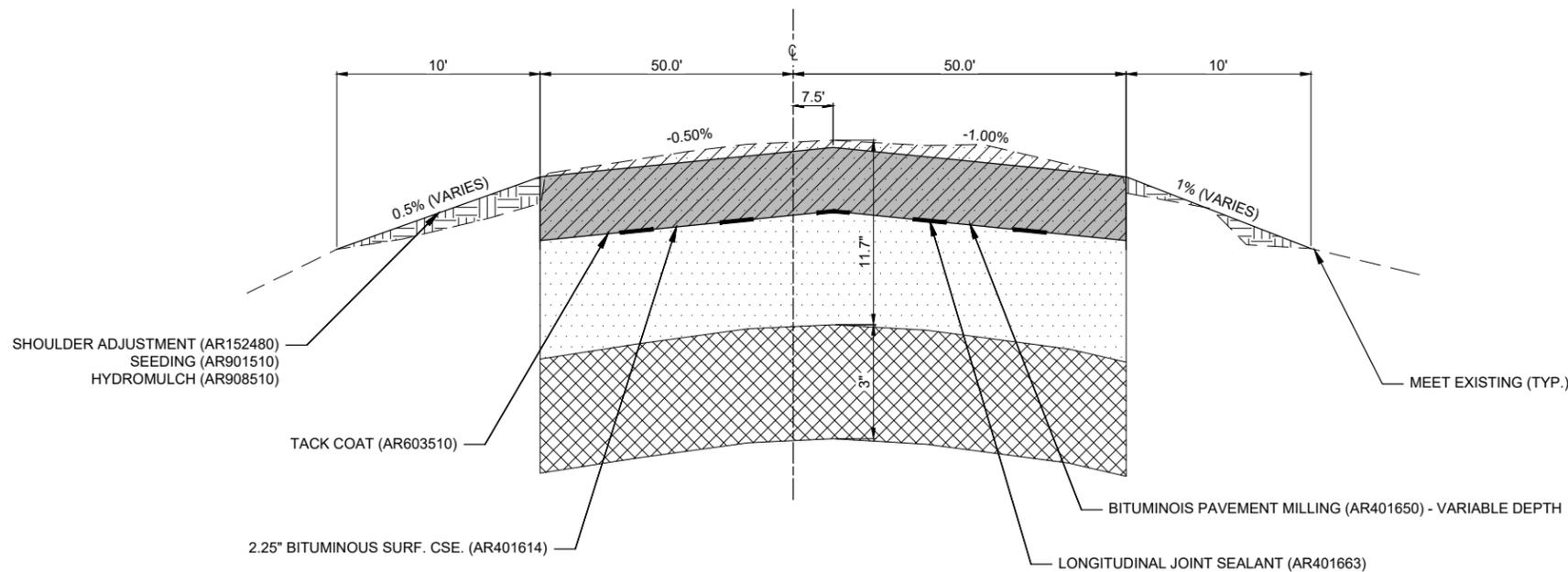


NOTE:
ALL BITUMINOUS/BITUMINOUS JOINT SEALING TO BE PAID UNDER SAW AND SEAL BITUMINOUS JOINTS, ITEM AR401660.

BITUMINOUS/BITUMINOUS SEAL



RUNWAY 9/27 TYPICAL SECTION
NOT TO SCALE



RUNWAY 9/27 TYPICAL SECTION THROUGH TURF RUNWAY
NOT TO SCALE

EXISTING	PROPOSED	LEGEND
		BIT. SURF. CSE.-METHOD I, SUPERPAVE
		BIT. PAVEMENT MILLING (VARIABLE)
		SHOULDER ADJUSTMENT (FILL)
		BITUMINOUS PAVEMENT
		CRUSHED AGGREGATE

**REHABILITATE
RUNWAY 9-27**

IDA No: MQB-4945
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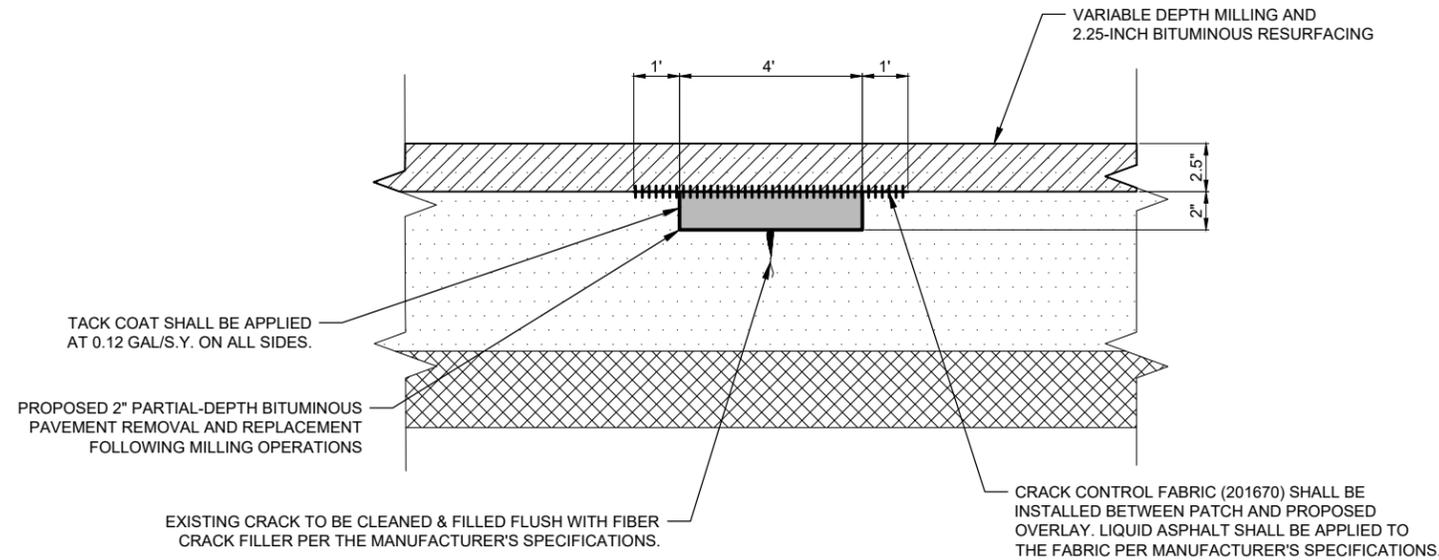
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SHEET TITLE

TYPICAL SECTIONS



AR401915 REMOVE & REPLACE BITUMINOUS PAVEMENT - TYPE A (PARTIAL DEPTH)

NOT TO SCALE

**REHABILITATE
RUNWAY 9-27**

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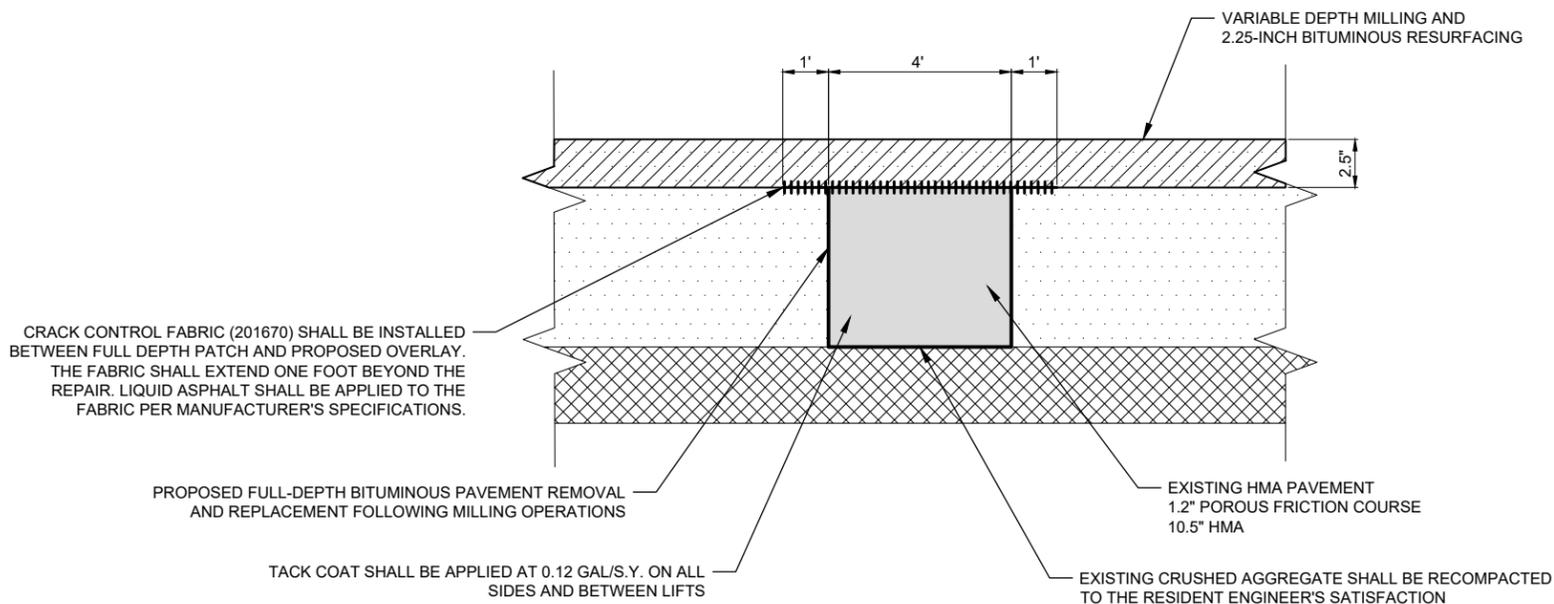
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SHEET TITLE

**PAVEMENT
PATCHING DETAILS**



AR401916 REMOVE & REPLACE BITUMINOUS PAVEMENT - TYPE B (FULL DEPTH)

NOT TO SCALE

EXISTING	PROPOSED	LEGEND
		BIT. PAVEMENT
		BIT. PAVEMENT MILLING (VARIABLE)
		BITUMINOUS PAVEMENT
		CRUSHED AGGREGATE



ELECTRICAL NOTES

1. THE CONTRACTOR SHALL EXAMINE THE SITE TO CONFIRM AND FIELD VERIFY EXISTING SITE CONDITIONS. THE CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING, DISCONNECTING, WORKING ON, RELOCATING, RECONNECTING, AND/OR INSTALLING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, OR OTHER DEVICES. CONTRACTOR SHALL REPORT ANY VARIATIONS, DEFICIENCIES, AND/OR APPARENT SAFETY CONCERNS TO THE RESIDENT ENGINEER/TECHNICIAN.
2. WHEN A RUNWAY IS CLOSED THE RESPECTIVE LIGHTING AND NAVAIDS SHALL BE SHUT OFF. WHEN A TAXIWAY IS CLOSED THE RESPECTIVE LIGHTING SYSTEM SHALL BE SHUT OFF.
3. ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER/DIRECTOR AND RESIDENT ENGINEER/TECHNICIAN.

NOTES

CLEAN & SEAL BITUMINOUS CRACKS

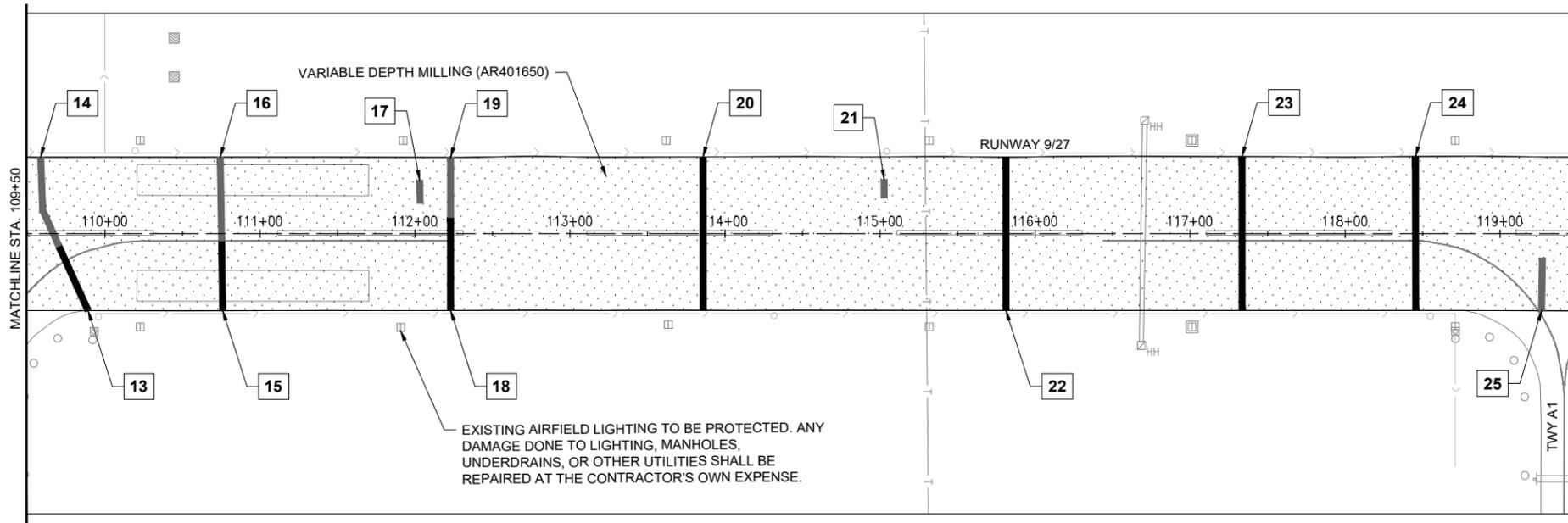
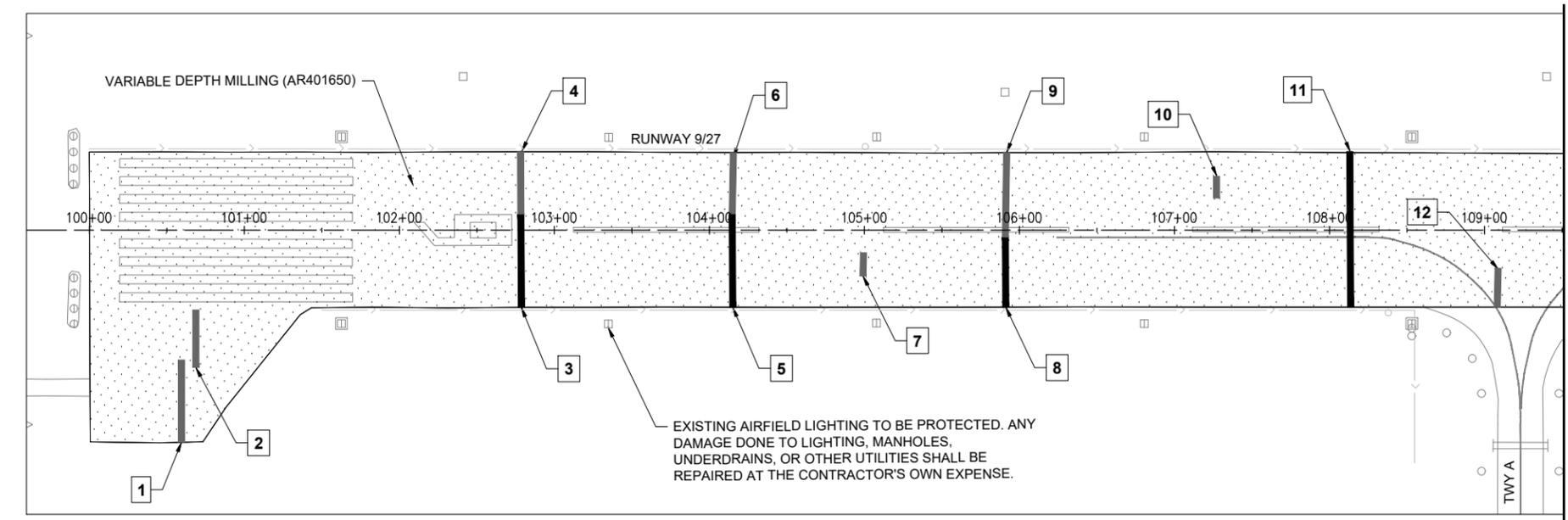
1. A NOMINAL QUANTITY OF AR201661 "CLEAN AND SEAL BITUMINOUS CRACKS" HAS BEEN INCLUDED AND WILL BE LAID OUT BY THE RESIDENT ENGINEER FOLLOWING PAVEMENT MILLING. THE NOMINAL QUANTITY WAS DETERMINED BY A FIELD SURVEY (CONDUCTED ON 02/22/22) AND ASSUMES THAT 50% OF SURVEYED CRACKS WILL BE ELIMINATED AFTER THE COMPLETION OF MILLING.

BITUMINOUS PAVEMENT MILLING

2. CONTRACTOR SHALL PROTECT ALL ADJACENT PAVEMENT EDGES DURING MILLING AND PAVING OPERATIONS. ANY DAMAGE DONE TO EXISTING PAVEMENT OR MARKINGS SHALL BE REPAIRED OR REMARKED AT THE CONTRACTOR'S EXPENSE.
3. ALL MILLED MATERIAL SHALL BE HAULED OFFSITE UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN. MILLED MATERIAL MAY BE USED TO MAKE REPAIRS TO THE HAUL ROUTE AS NEEDED AND APPROVED BY THE RESIDENT ENGINEER.

REMOVE & REPLACE BITUMINOUS PAVEMENT

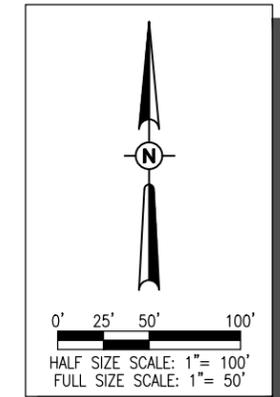
1. SEE DETAILS SHEET FOR PATCHING. PATCHING LOCATIONS ARE APPROXIMATE AND WILL BE CONFIRMED AND LAID OUT BY THE RESIDENT ENGINEER/TECHNICIAN FOLLOWING PAVEMENT MILLING.
2. ALL PATCHES INDICATED AS PARTIAL (TYPE A) OR FULL DEPTH (TYPE B) WERE DETERMINED DURING A FIELD SURVEY AND PRIOR TO MILLING. FOLLOWING MILLING, AND UPON INSPECTION BY RESIDENT ENGINEER/TECHNICIAN, PARTIAL PATCHES MAY BE CONVERTED TO TYPE B (FULL-DEPTH) PATCH TO BE MEASURED AND PAID FOR UNDER PAY ITEM AR401916 OR REMOVED.



EXISTING AIRFIELD LIGHTING TO BE PROTECTED. ANY DAMAGE DONE TO LIGHTING, MANHOLES, UNDERDRAINS, OR OTHER UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

EXISTING AIRFIELD LIGHTING TO BE PROTECTED. ANY DAMAGE DONE TO LIGHTING, MANHOLES, UNDERDRAINS, OR OTHER UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

EXISTING	PROPOSED	LEGEND
		AR401650 - BITUMINOUS PAVEMENT MILLING (VARIABLE) - BASE
		REMOVE AND REPLACE BIT. PAVEMENT - FULL DEPTH
		REMOVE AND REPLACE BIT. PAVEMENT - PARTIAL DEPTH
		AIRPORT PROPERTY LINE
		AIRPORT FENCE (AIRFIELD)
		UNDERGROUND ELECTRIC LINE
		TELEPHONE/COMMUNICATION LINE
		UNDERDRAIN
		MANHOLE
		AIRFIELD LIGHTING



REHABILITATE RUNWAY 9-27

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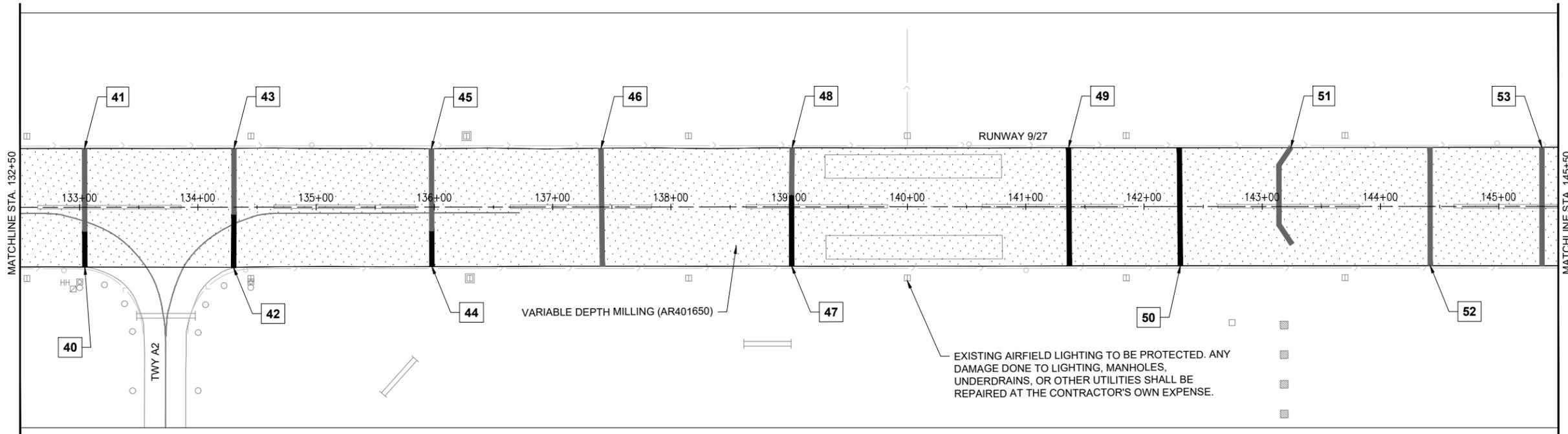
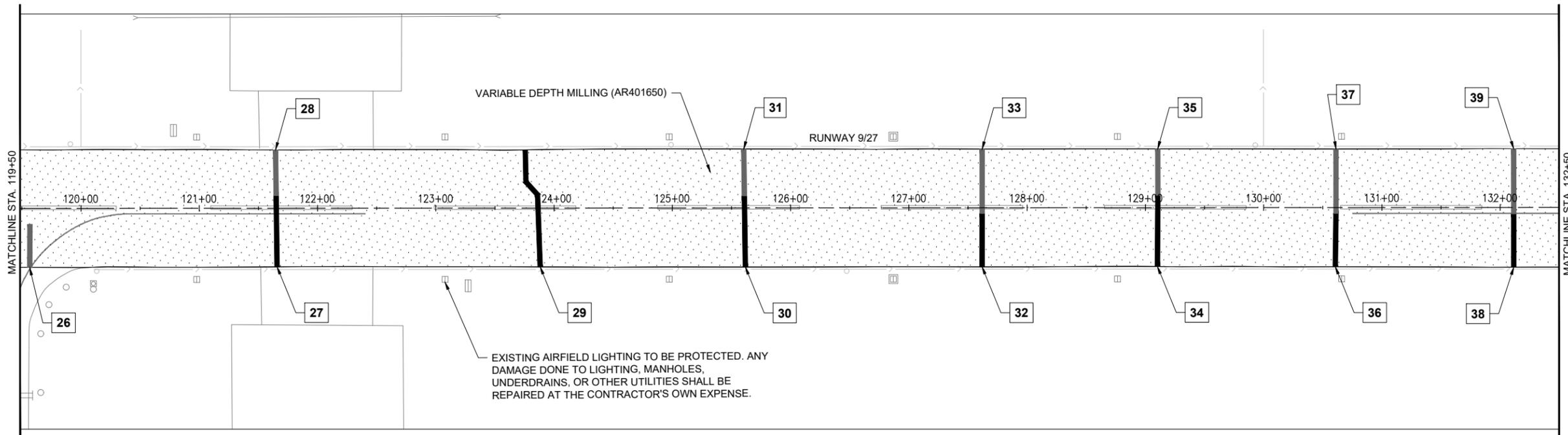
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SHEET TITLE

PAVEMENT PREPARATION PLAN - 1



REHABILITATE
RUNWAY 9-27

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Contract No.: MB034

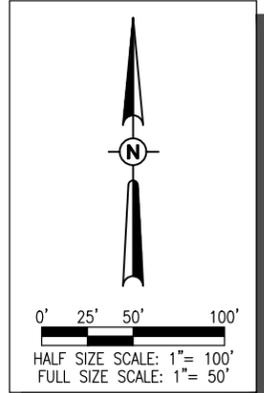
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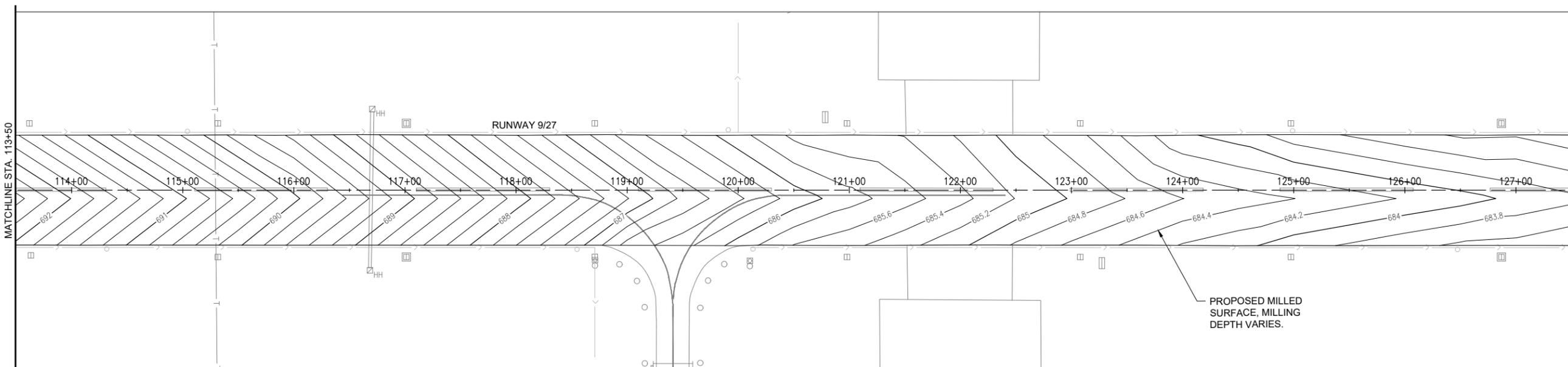
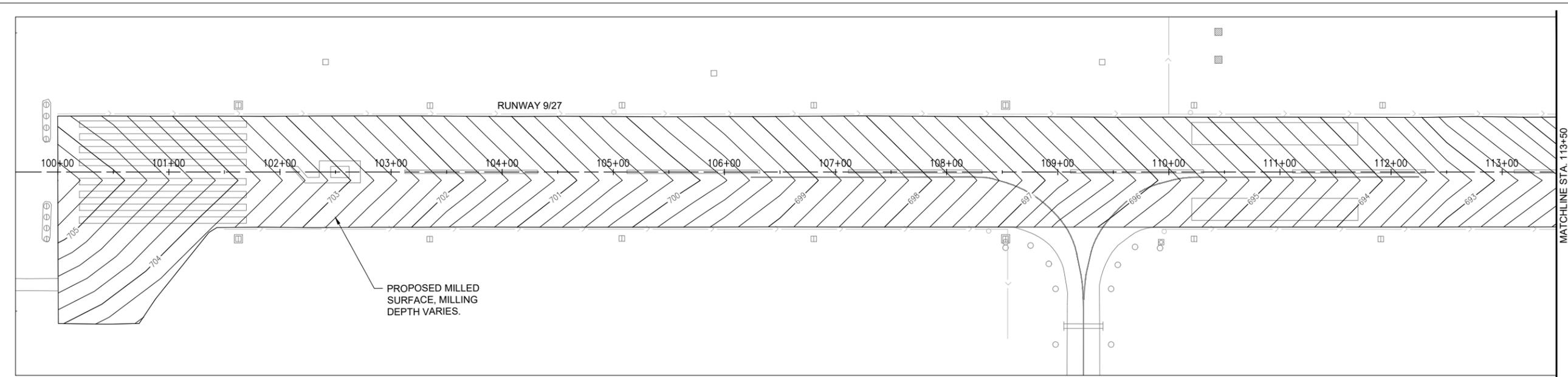
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SHEET TITLE

PAVEMENT
PREPARATION PLAN -
2

EXISTING	PROPOSED	LEGEND
		AR401650 - BITUMINOUS PAVEMENT MILLING (VARIABLE) - BASE
		REMOVE AND REPLACE BIT. PAVEMENT - FULL DEPTH
		REMOVE AND REPLACE BIT. PAVEMENT - PARTIAL DEPTH
		AIRPORT PROPERTY LINE
		AIRPORT FENCE (AIRFIELD)
		UNDERGROUND ELECTRIC LINE
		TELEPHONE/COMMUNICATION LINE
		UNDERDRAIN
		MANHOLE
		AIRFIELD LIGHTING





REHABILITATE
RUNWAY 9-27

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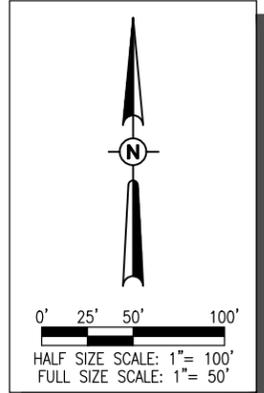
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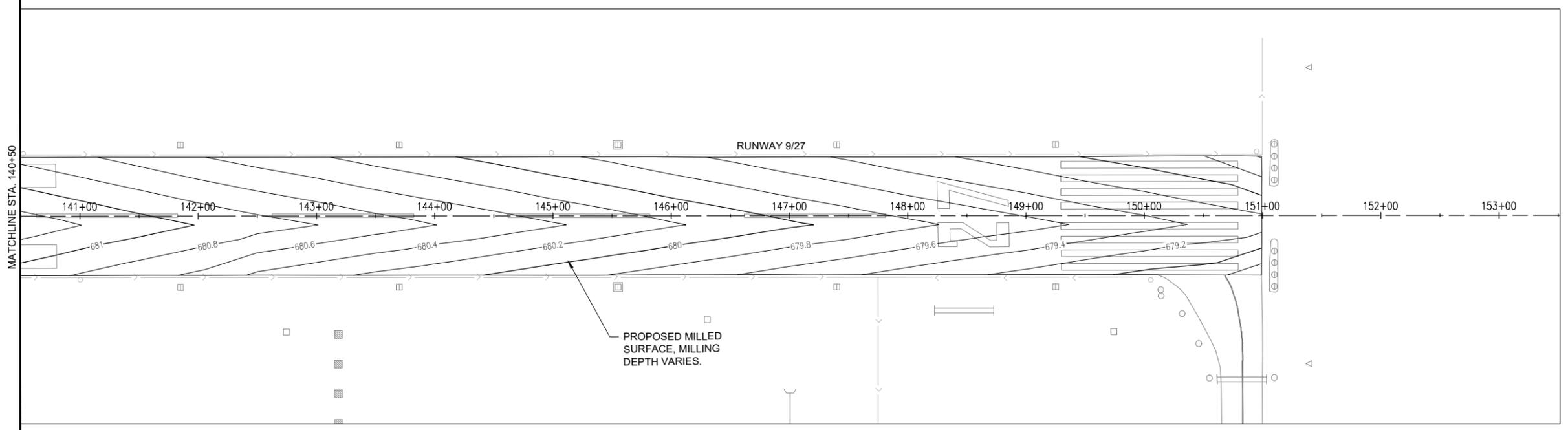
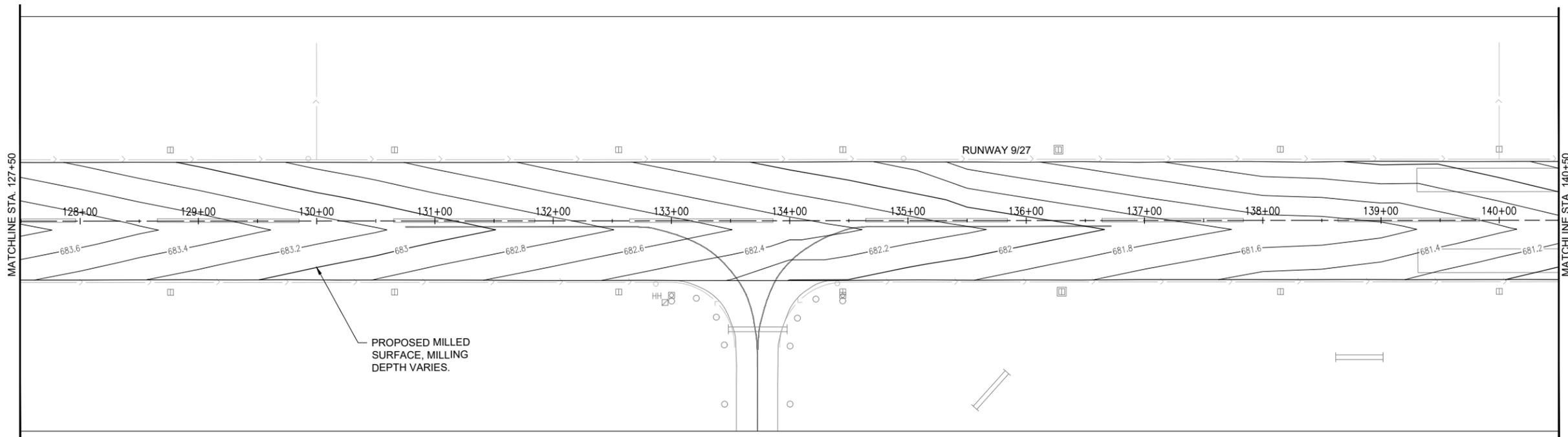
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SHEET TITLE

MILLING GRADE PLAN
1

EXISTING	PROPOSED	LEGEND
---		AIRPORT PROPERTY LINE
-x-x-		AIRPORT FENCE (AIRFIELD)
-e-e-		UNDERGROUND ELECTRIC LINE
--->---		UNDERDRAIN
⊙		MANHOLE
□ ○		AIRFIELD LIGHTING





REHABILITATE
RUNWAY 9-27

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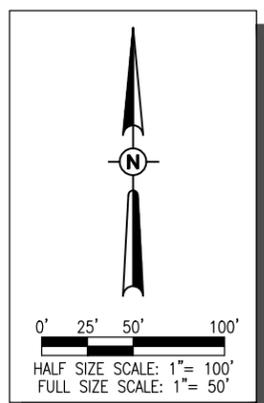
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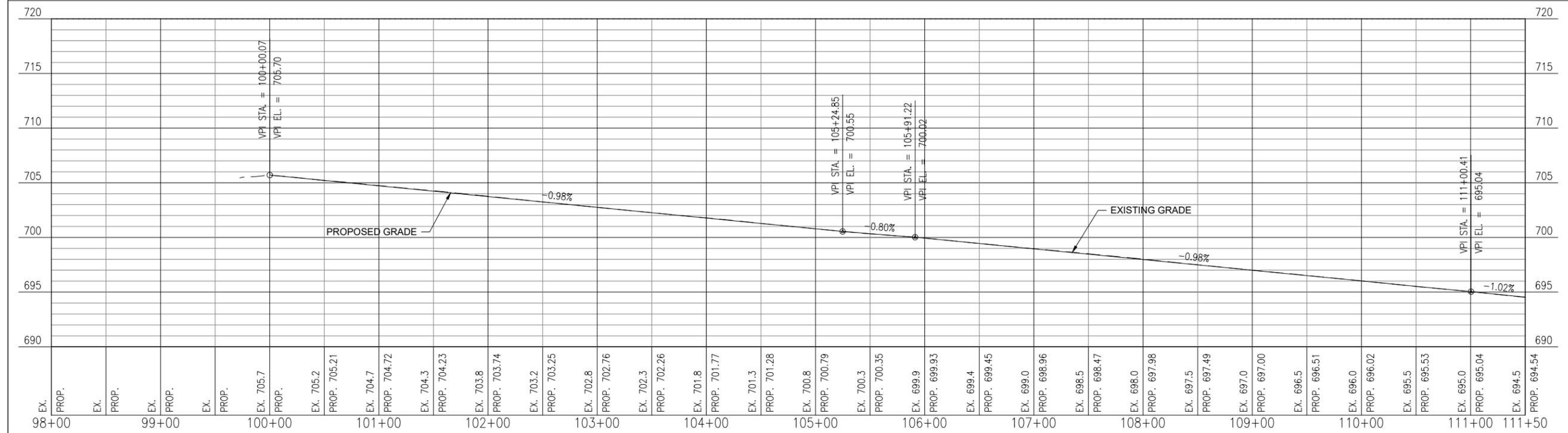
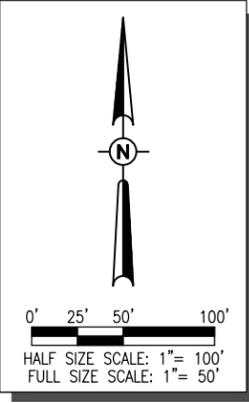
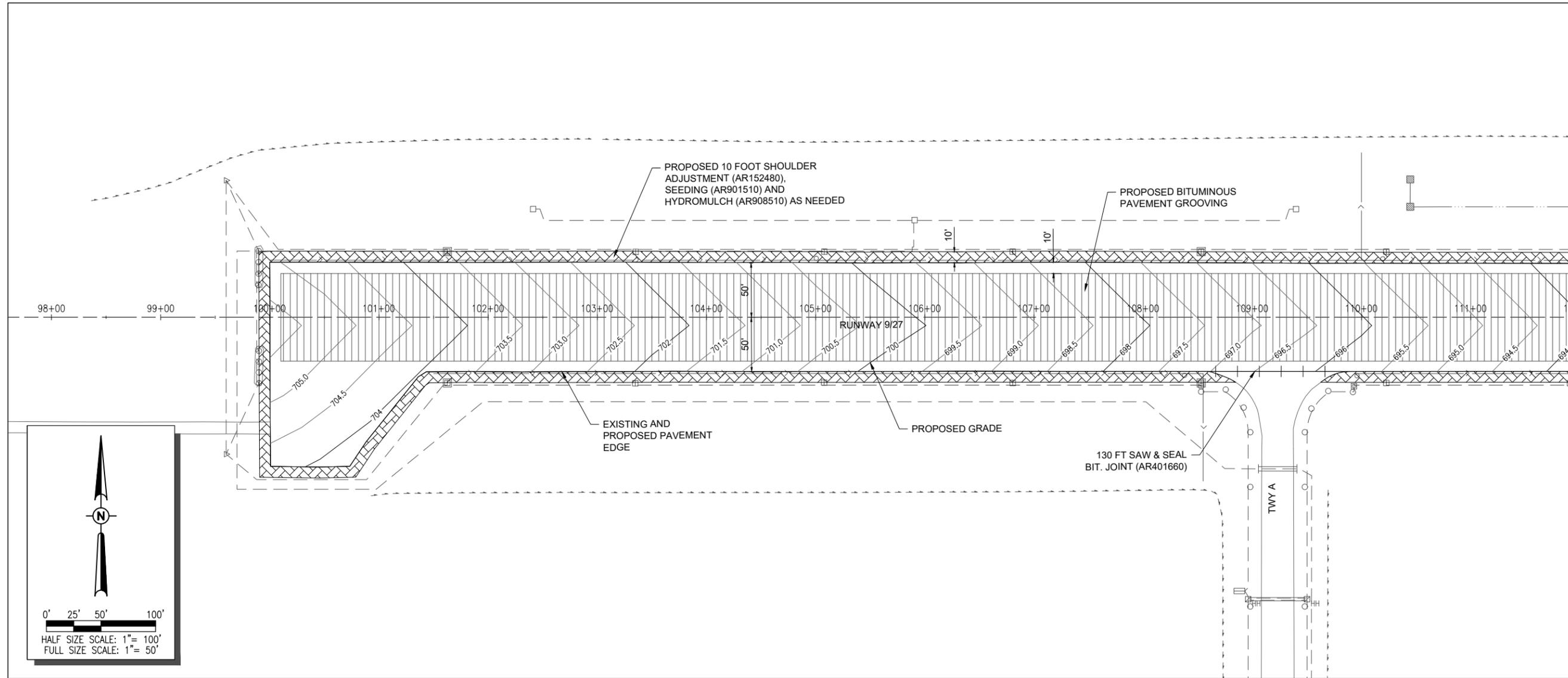
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PROJECT NO: 20A0074
CAD FILE: C-201-MILL.DWG
DESIGN BY: LDH 3/13/22
DRAWN BY: LDH 3/13/22
REVIEWED BY: KD 03/22/22

SHEET TITLE

MILLING GRADE PLAN
2





REHABILITATE
RUNWAY 9-27

IDA No: MQB-4945
SBGP No:
3-17-SBGP-171/TBD

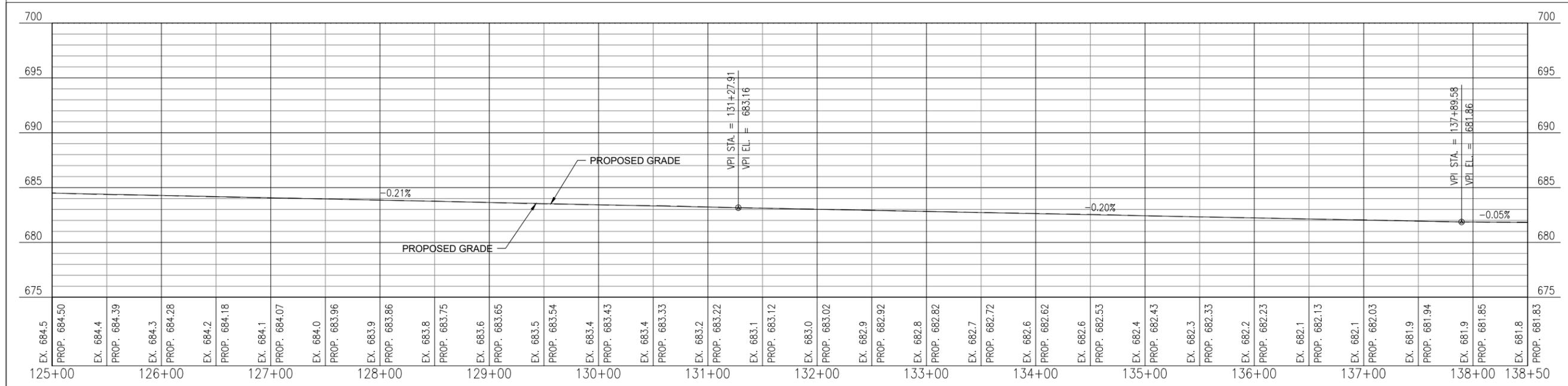
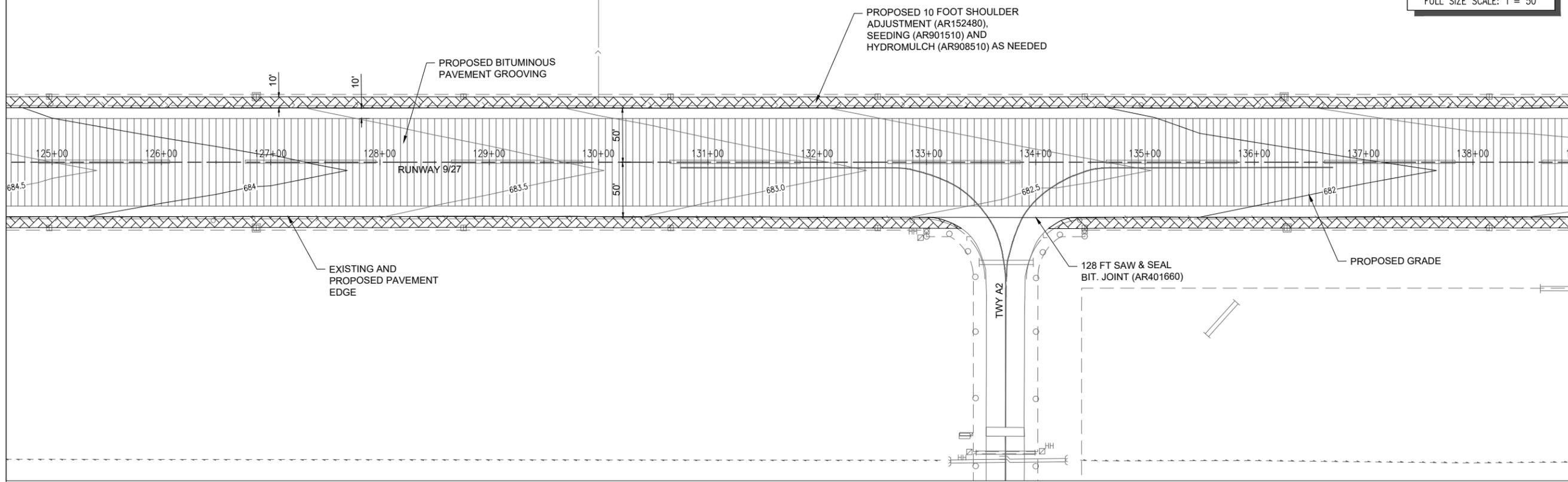
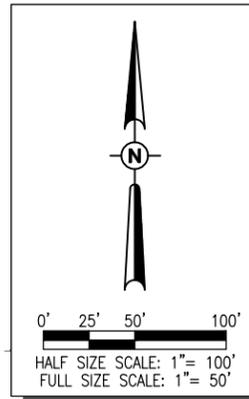
Contract No.: MB034

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: 04/22/22
PROJECT NO: 20A0074
CAD FILE: P&P - (1).DWG
DESIGN BY: LDH 3/13/22
DRAWN BY: LDH 3/13/22
REVIEWED BY: KD 03/22/22

SHEET TITLE

PLAN & PROFILE 1 -
RUNWAY 9-27



REHABILITATE
RUNWAY 9-27

IDA No: MQB-4945
SBGP No:
3-17-SBGP-171/TBD

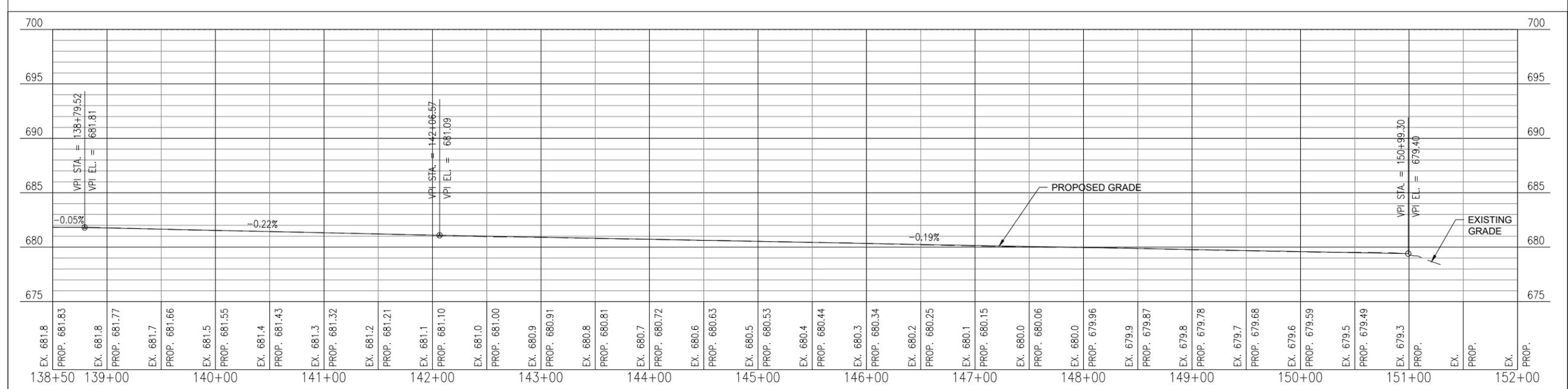
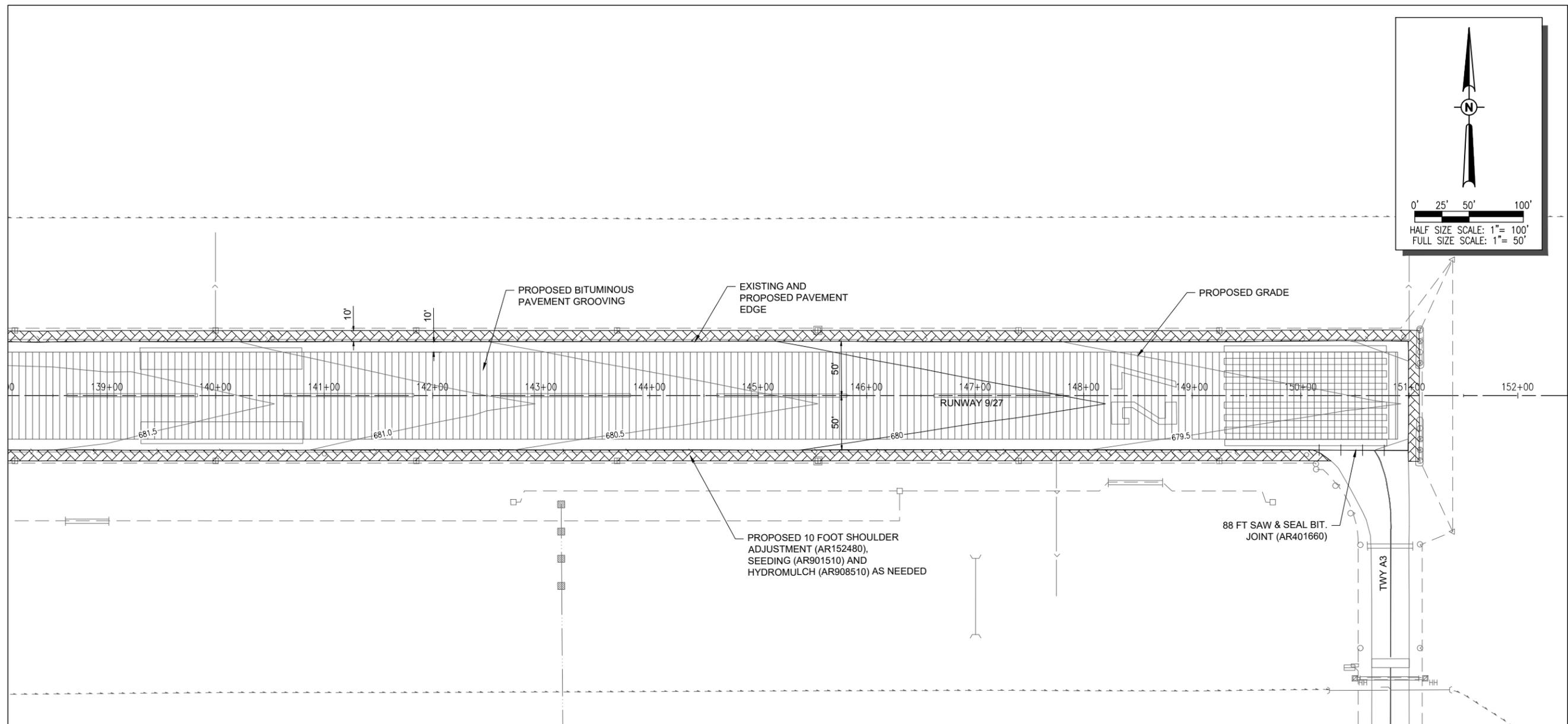
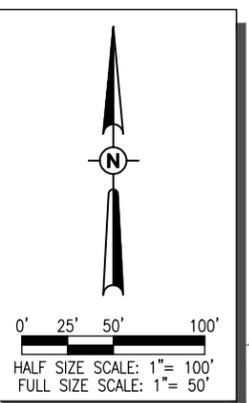
Contract No.: MB034

NO.	DATE	DESCRIPTION

NO.	DATE	DESCRIPTION

SHEET TITLE

PLAN & PROFILE 3 -
RUNWAY 9-27



**REHABILITATE
RUNWAY 9-27**

IDA No: MQB-4945
SBGP No:
3-17-SBGP-171/TBD

Contract No.: MB034

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: 04/22/22
PROJECT NO: 20A0074
CAD FILE: P&P - (4).DWG
DESIGN BY: LDH 3/14/22
DRAWN BY: LDH 3/14/22
REVIEWED BY: KD 03/22/22

SHEET TITLE

**PLAN & PROFILE 4 -
RUNWAY 9-27**

APR 20, 2022 2:50 PM HANSON00682
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REHABILITATE
RUNWAY 9-27

IDA No: MQB-4945
SBGP No:
3-17-SBGP-171/TBD

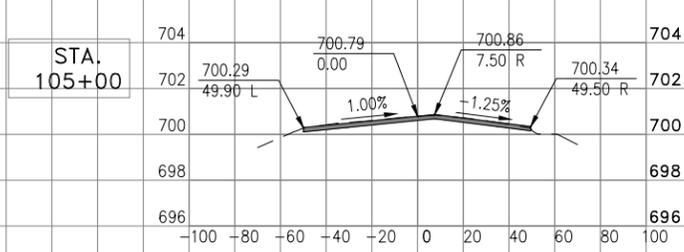
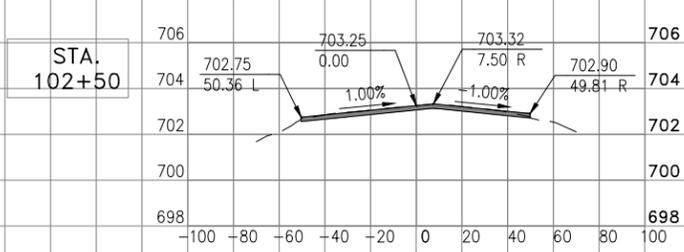
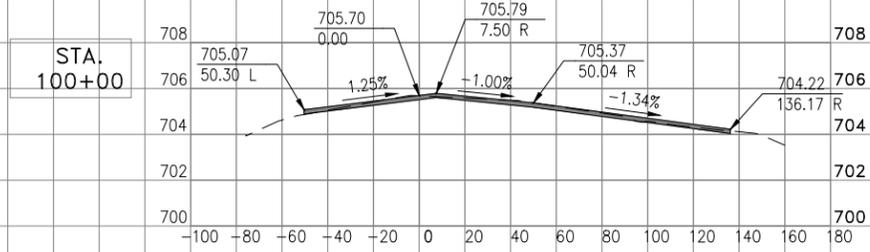
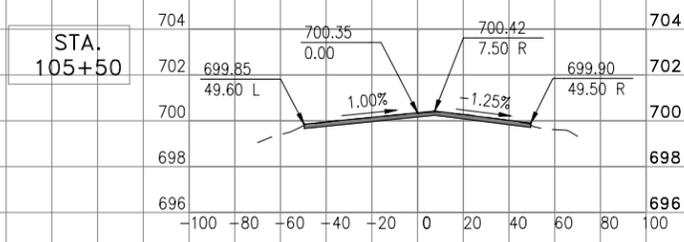
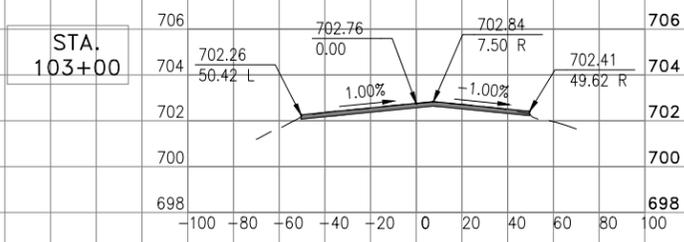
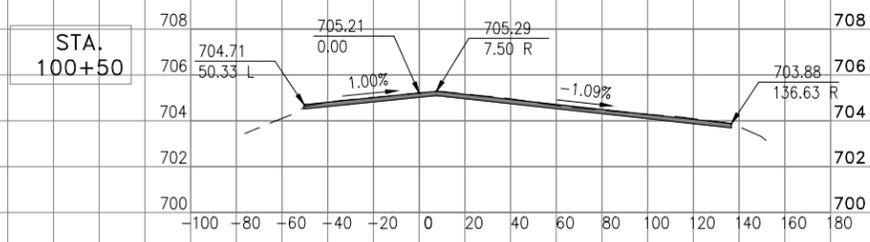
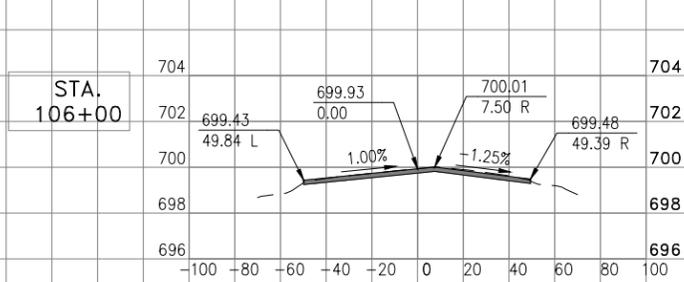
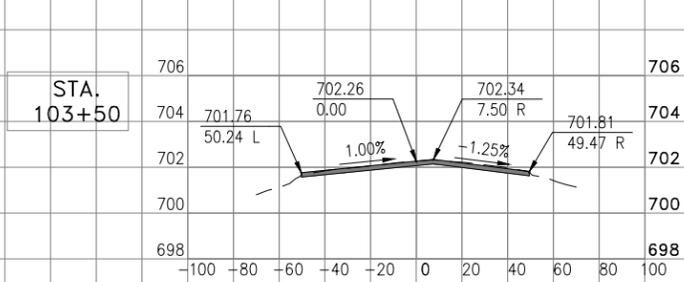
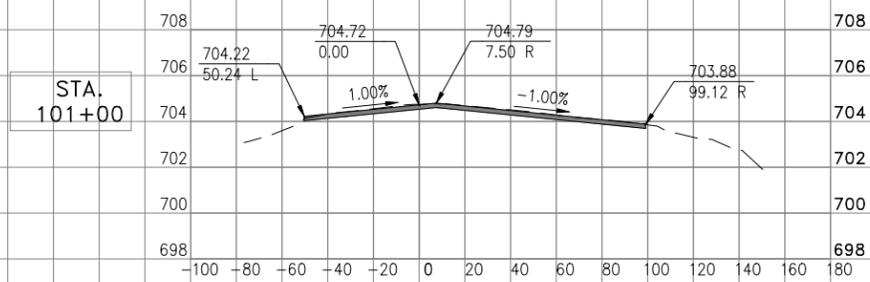
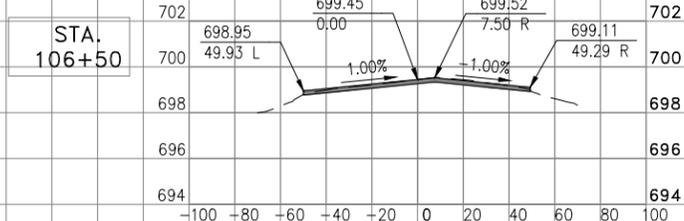
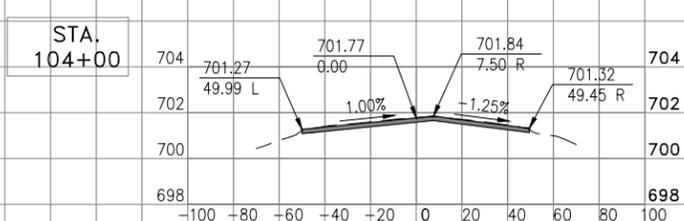
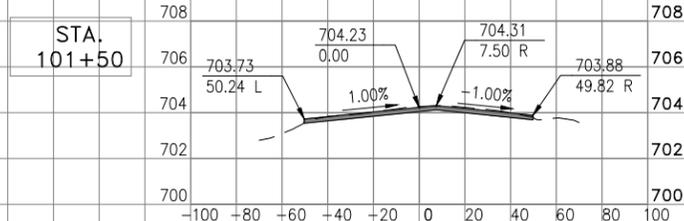
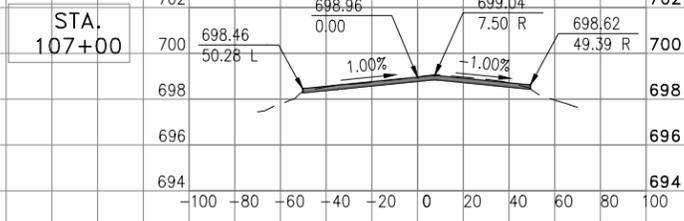
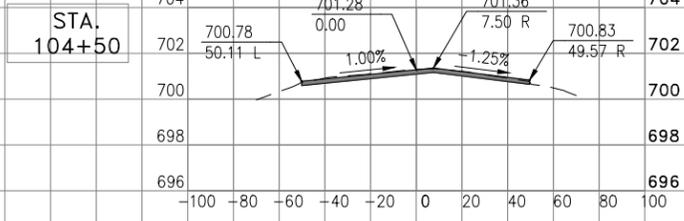
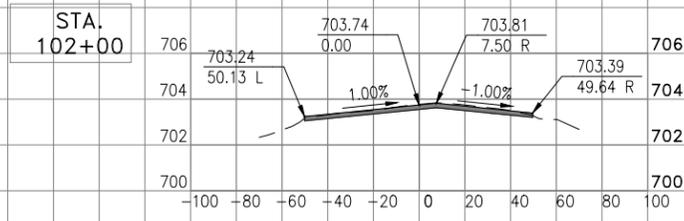
Contract No.: MB034

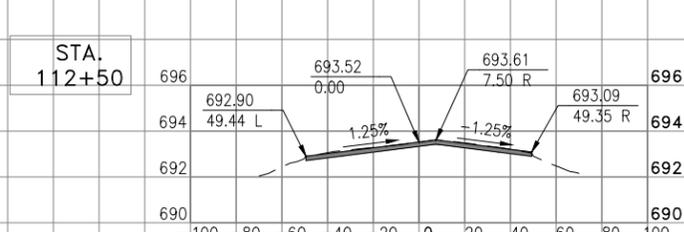
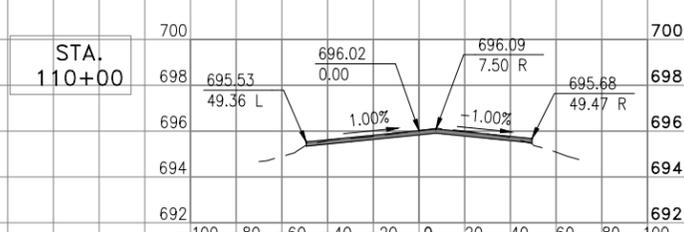
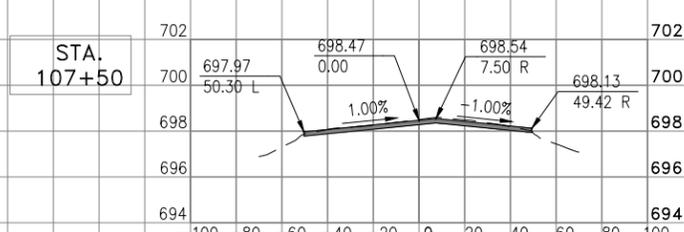
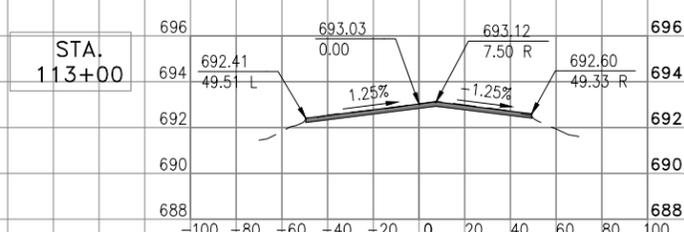
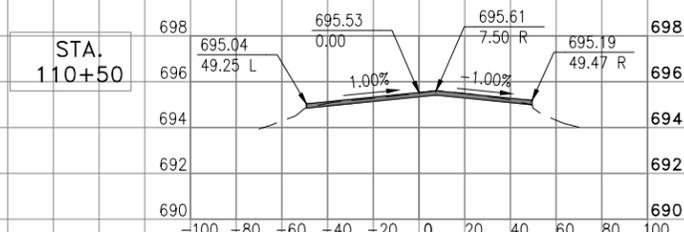
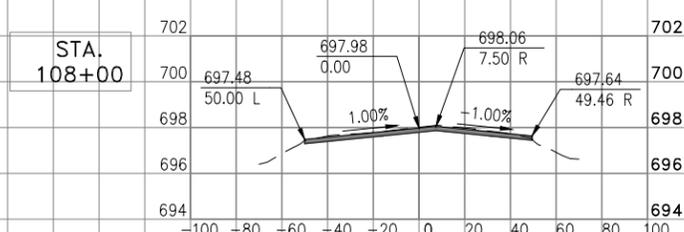
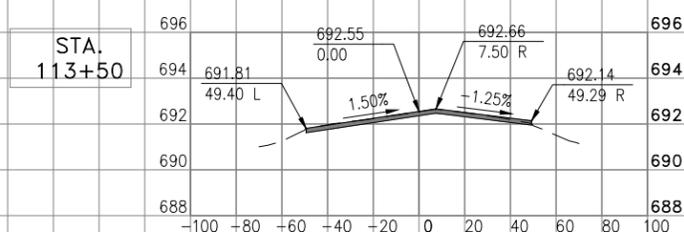
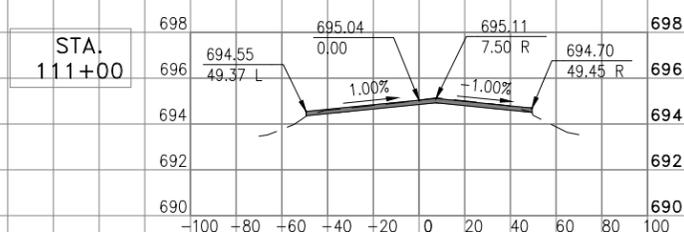
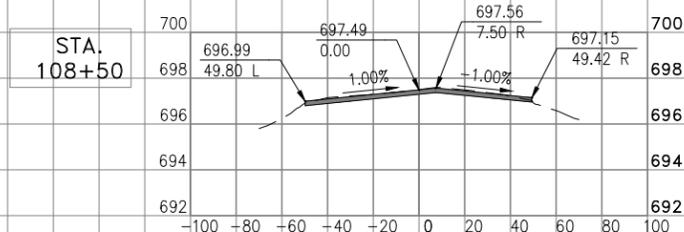
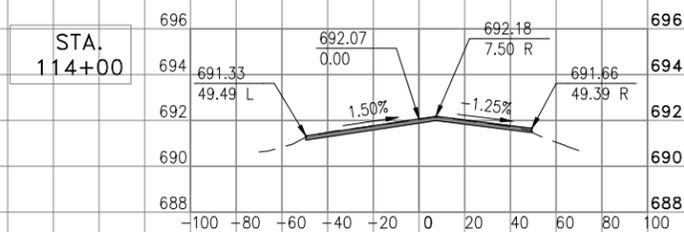
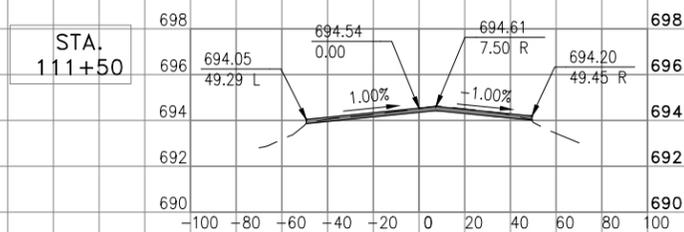
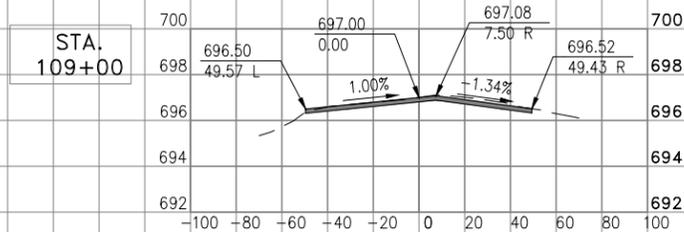
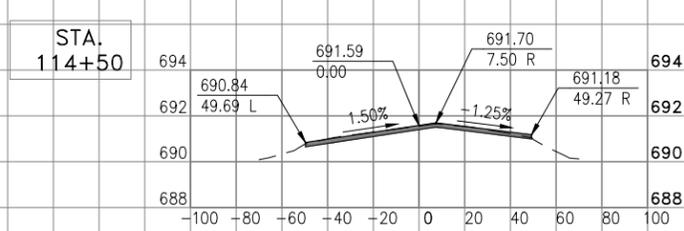
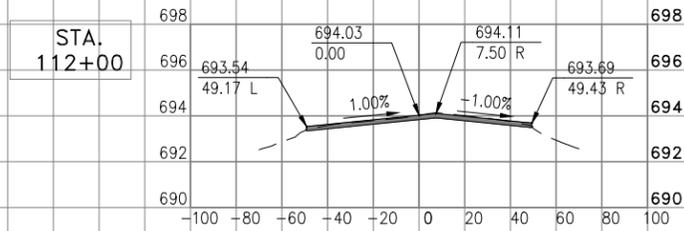
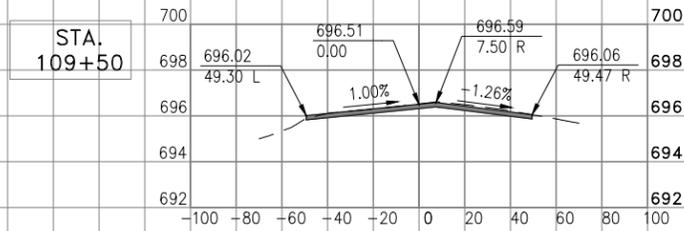
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ISSUE: 04/22/22
PROJECT NO: 20A0074
CAD FILE: C-401-XSECT.DWG
DESIGN BY: LDH 3/14/22
DRAWN BY: LDH 3/14/22
REVIEWED BY: KD 03/22/22

SHEET TITLE

CROSS SECTIONS 1 -
RUNWAY 9-27





REHABILITATE
RUNWAY 9-27

IDA No: MQB-4945
SBGP No:
3-17-SBGP-171/TBD

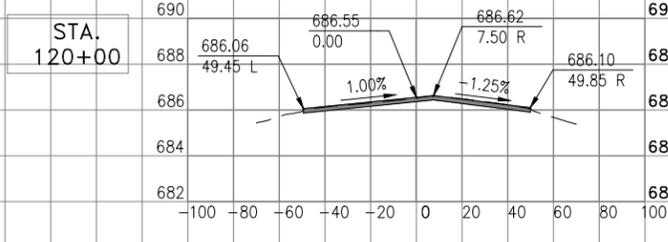
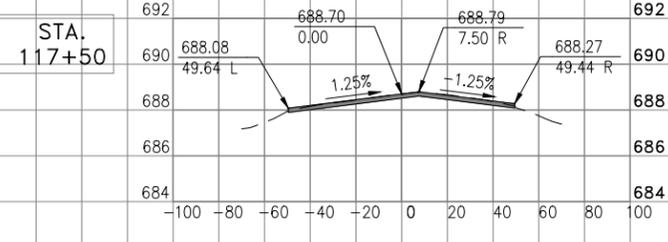
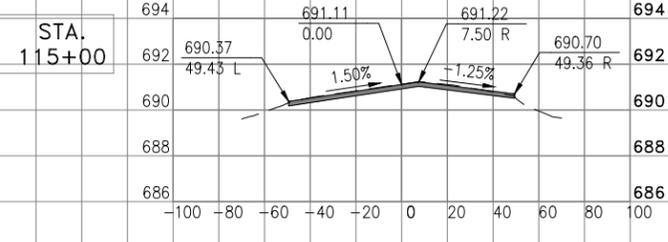
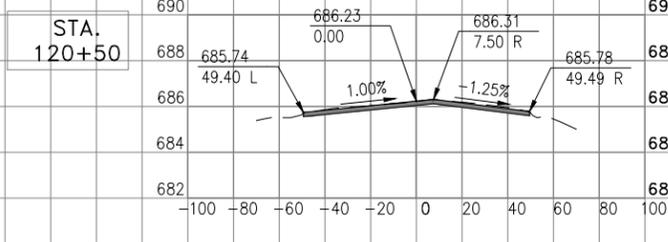
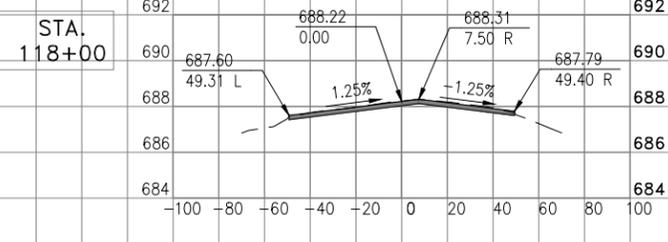
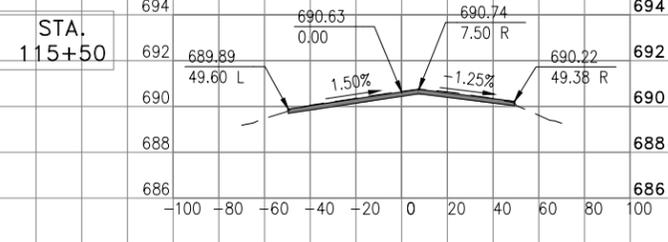
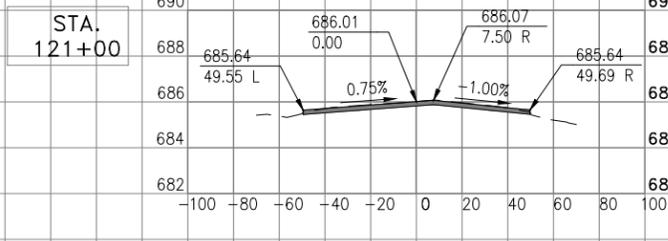
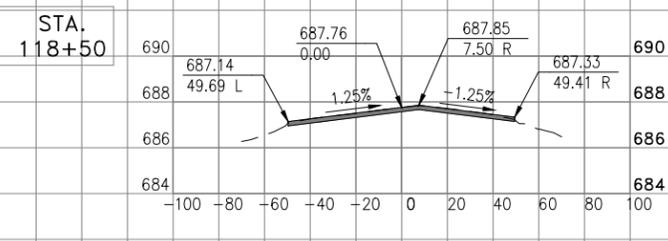
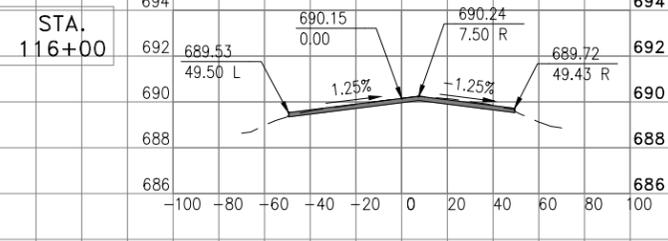
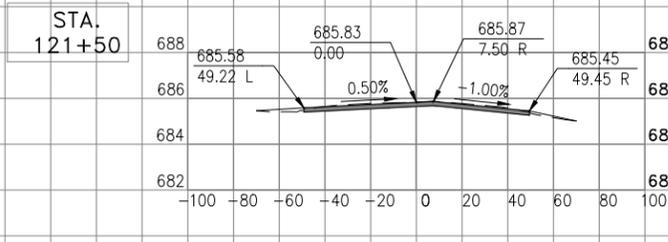
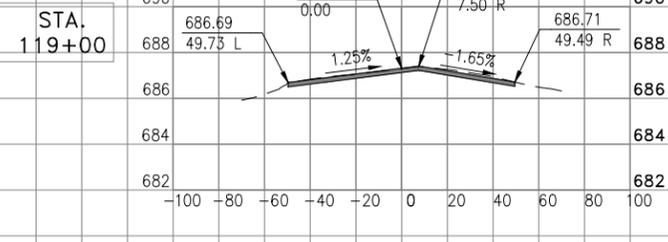
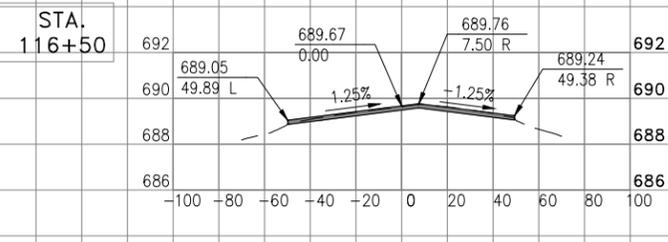
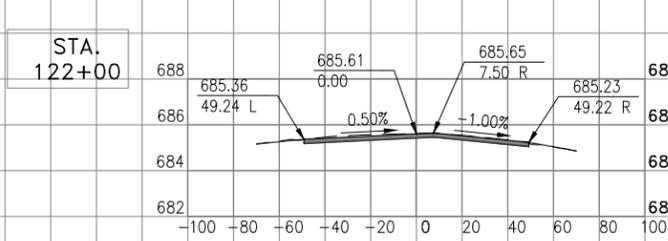
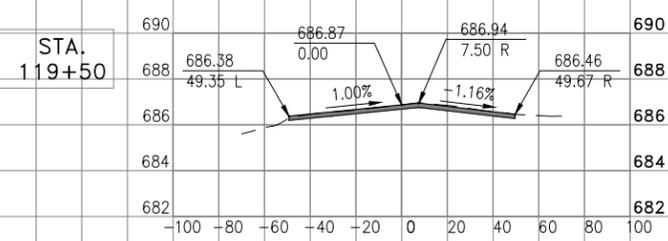
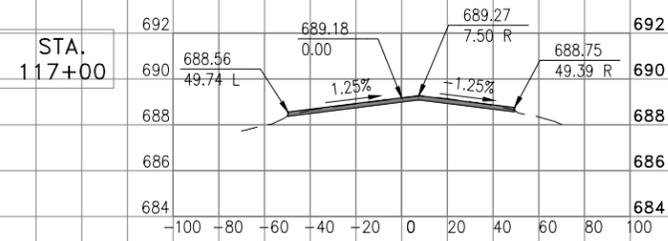
Contract No.: MB034

NO.	DATE	DESCRIPTION		
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ISSUE: 04/22/22
PROJECT NO: 20A0074
CAD FILE: C-401-XSECT.DWG
DESIGN BY: LDH 3/14/22
DRAWN BY: LDH 3/14/22
REVIEWED BY: KD 03/22/22

SHEET TITLE

CROSS SECTIONS 2 -
RUNWAY 9-27



**REHABILITATE
RUNWAY 9-27**

IDA No: MQB-4945
SBGP No:
3-17-SBGP-171/TBD

Contract No.: MB034

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: 04/22/22
PROJECT NO: 20A0074
CAD FILE: C-401-XSECT.DWG
DESIGN BY: LDH 3/14/22
DRAWN BY: LDH 3/14/22
REVIEWED BY: KD 03/22/22

SHEET TITLE

**CROSS SECTIONS 3 -
RUNWAY 9-27**



REHABILITATE
RUNWAY 9-27

IDA No: MQB-4945
SBGP No:
3-17-SBGP-171/TBD

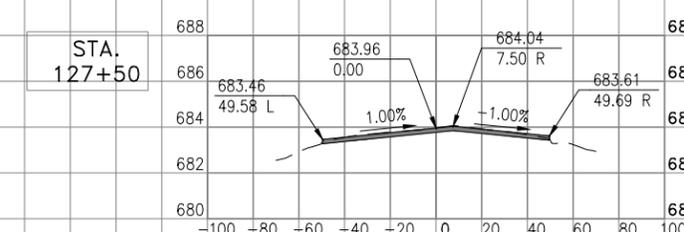
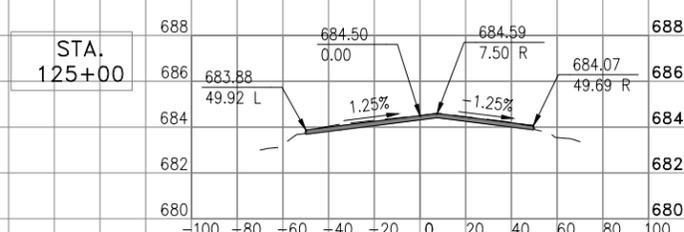
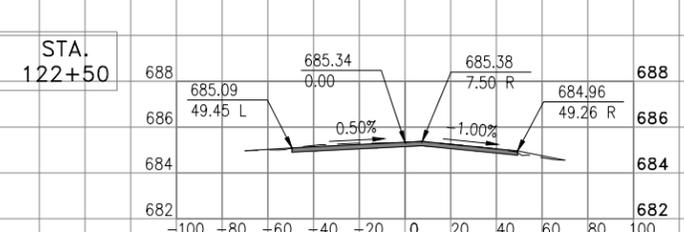
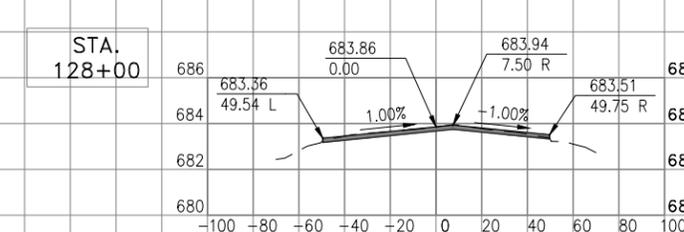
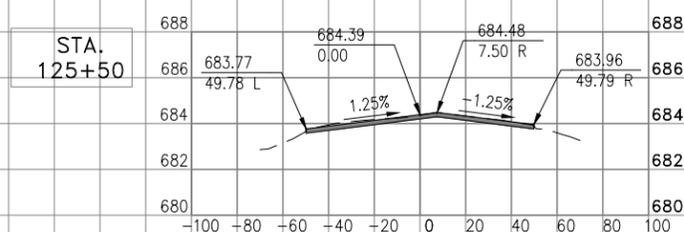
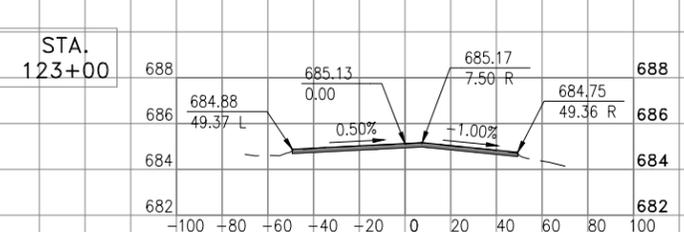
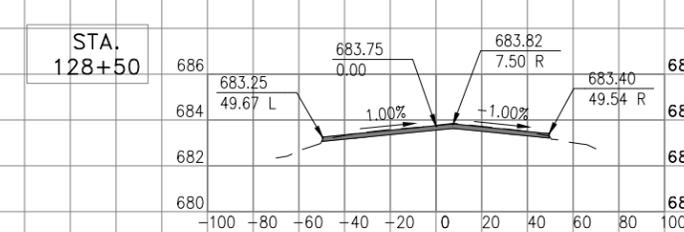
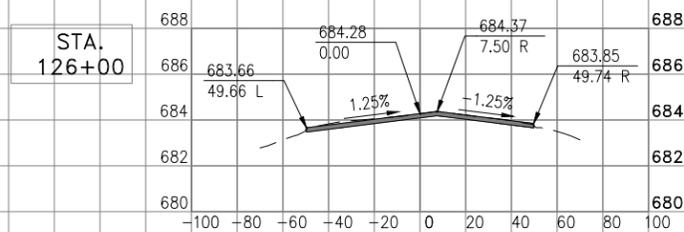
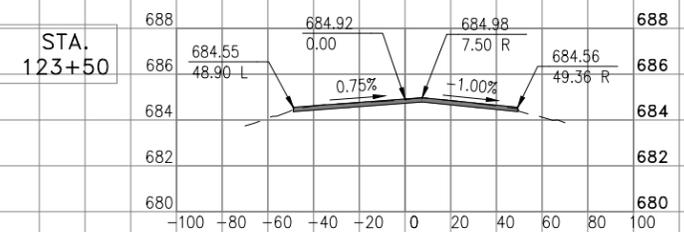
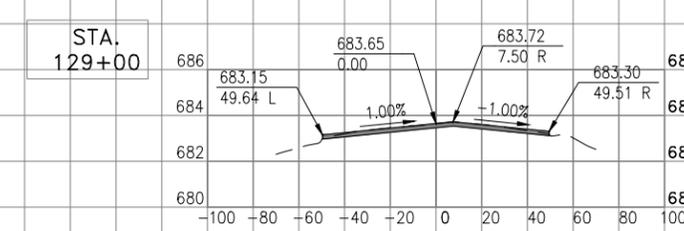
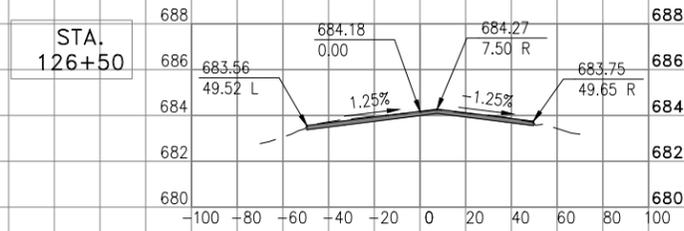
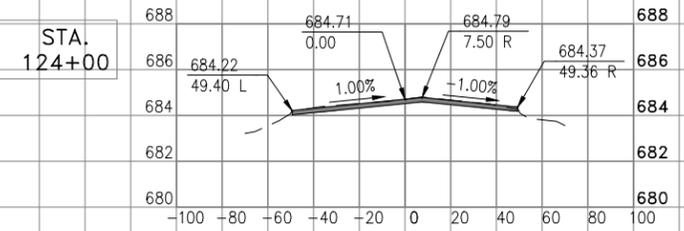
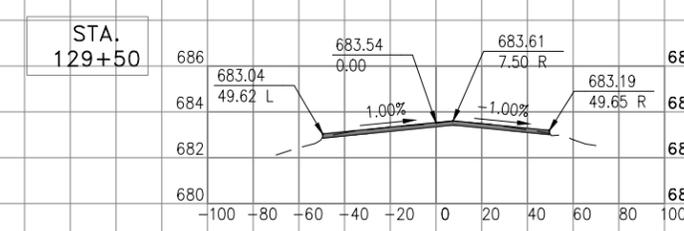
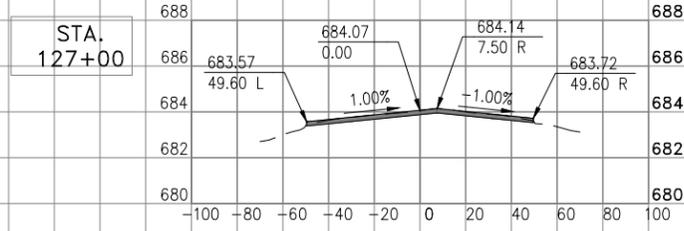
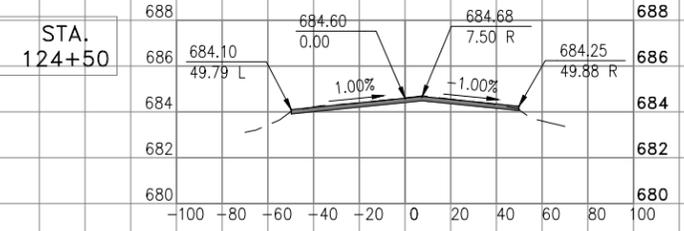
Contract No.: MB034

NO.	DATE	DESCRIPTION		
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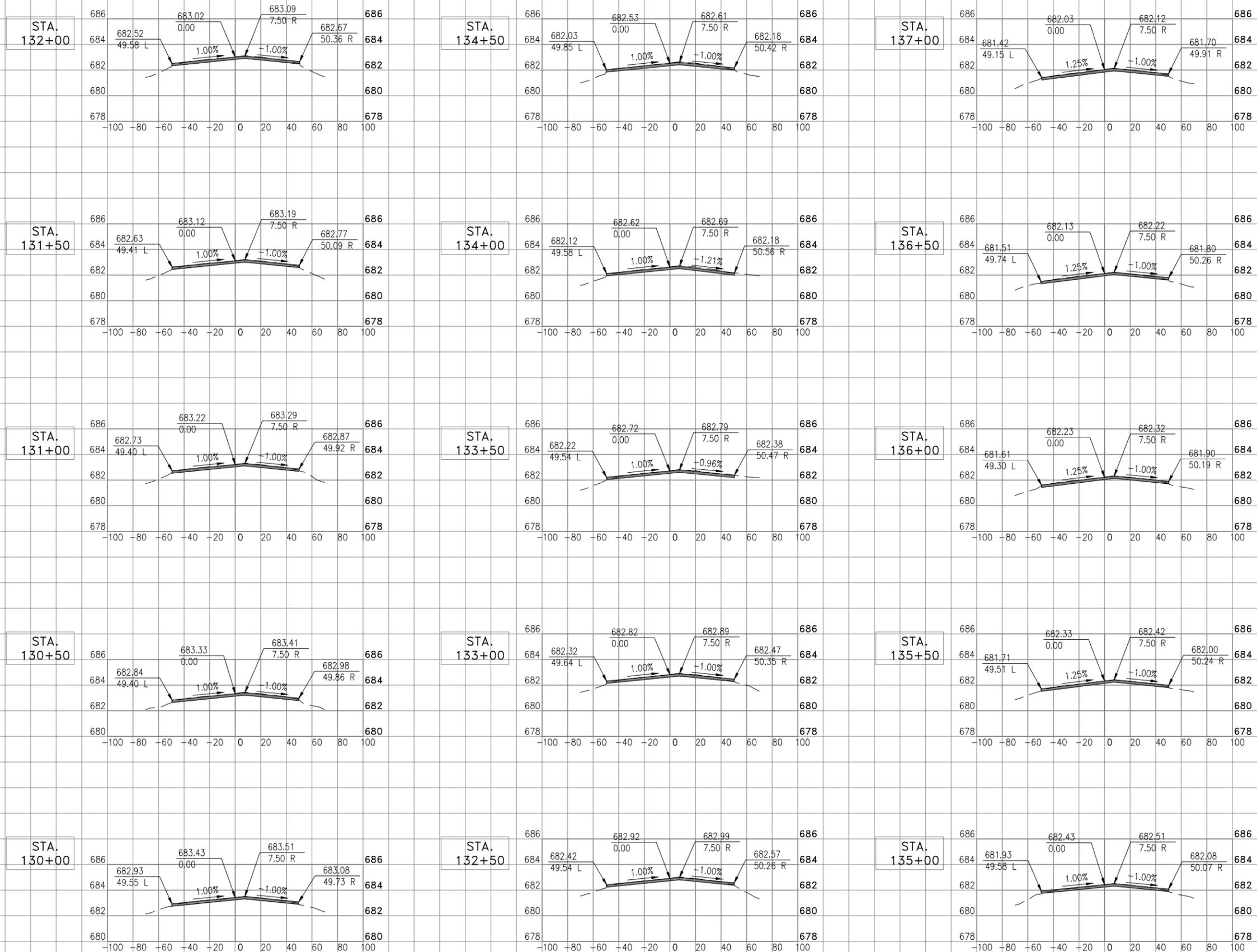
ISSUE: 04/22/22
PROJECT NO: 20A0074
CAD FILE: C-401-XSECT.DWG
DESIGN BY: LDH 3/14/22
DRAWN BY: LDH 3/14/22
REVIEWED BY: KD 03/22/22

SHEET TITLE

CROSS SECTIONS 4 -
RUNWAY 9-27



APR 20, 2022 2:50 PM HANSON00682
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REHABILITATE
RUNWAY 9-27

IDA No: MQB-4945
SBGP No:
3-17-SBGP-171/TBD

Contract No.: MB034

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: 04/22/22
PROJECT NO: 20A0074
CAD FILE: C-401-XSECT.DWG
DESIGN BY: LDH 3/14/22
DRAWN BY: LDH 3/14/22
REVIEWED BY: KD 03/22/22

SHEET TITLE

CROSS SECTIONS 5 -
RUNWAY 9-27



REHABILITATE
RUNWAY 9-27

IDA No: MQB-4945
SBGP No:
3-17-SBGP-171/TBD

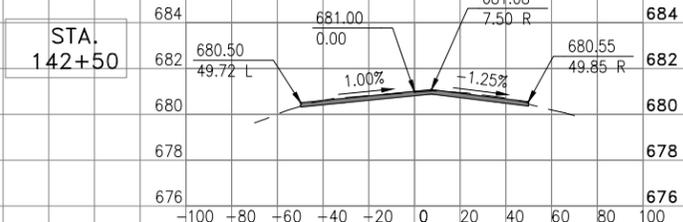
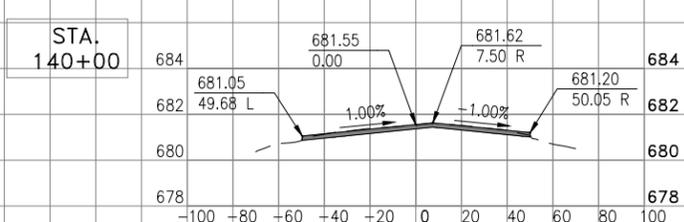
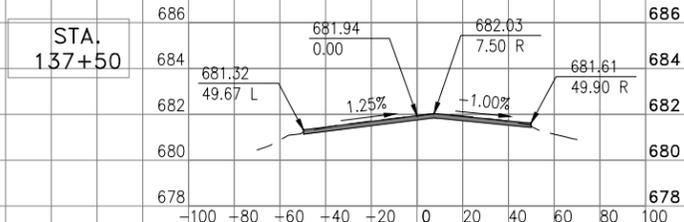
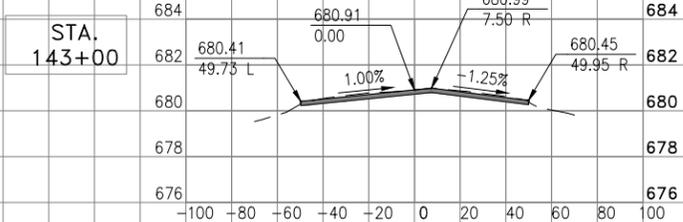
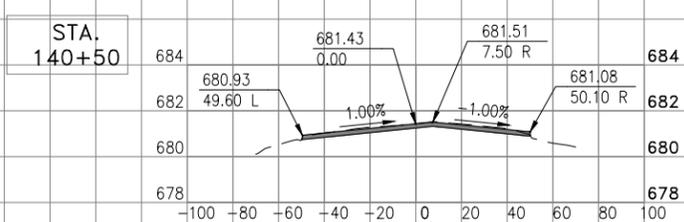
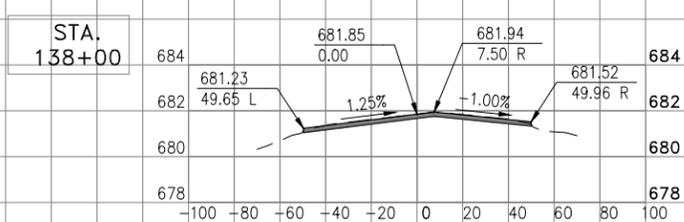
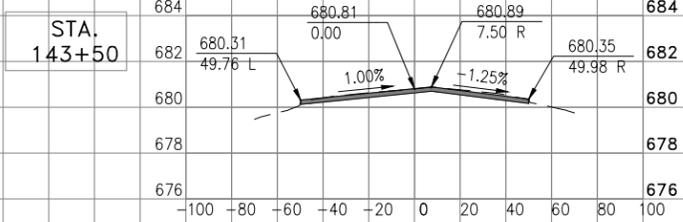
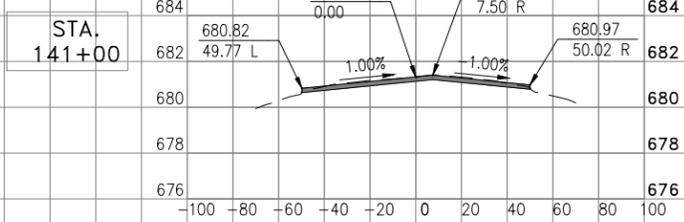
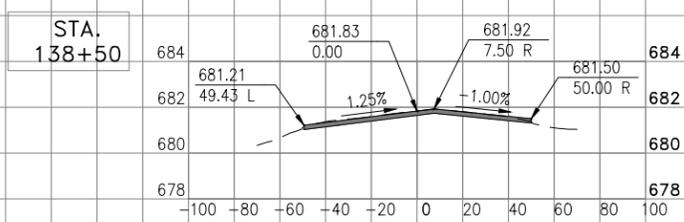
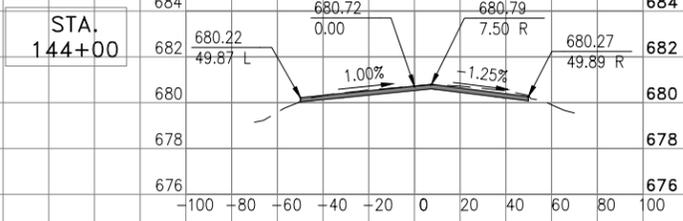
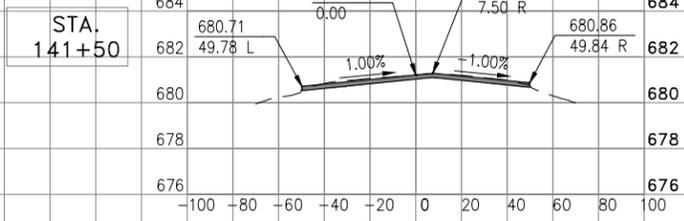
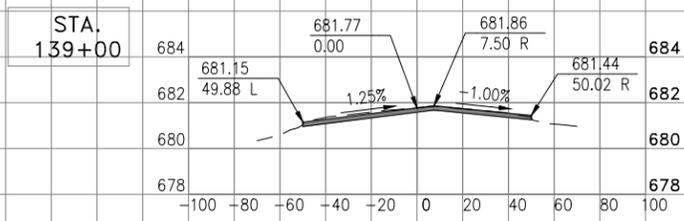
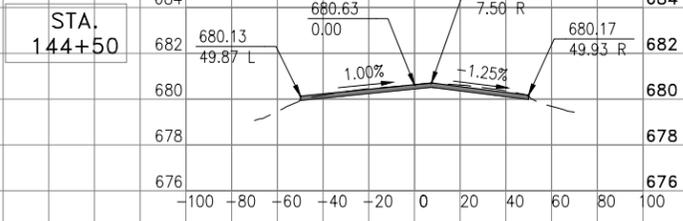
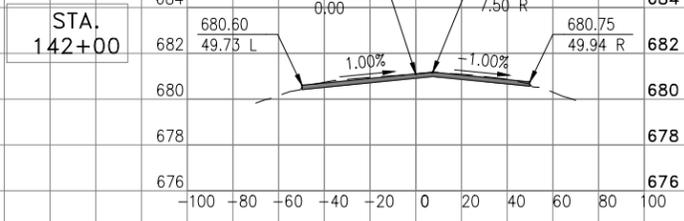
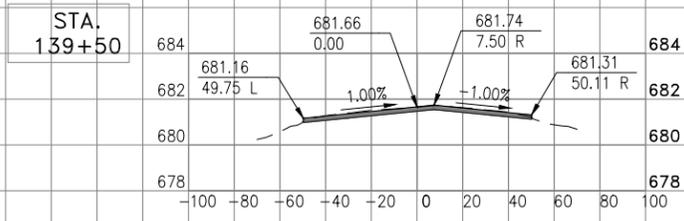
Contract No.: MB034

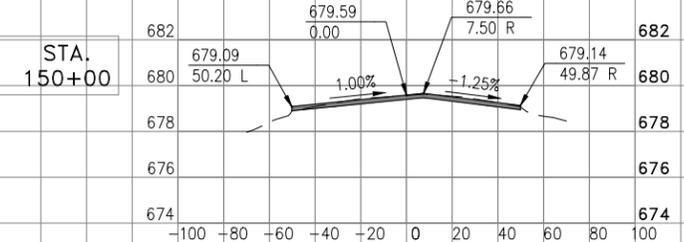
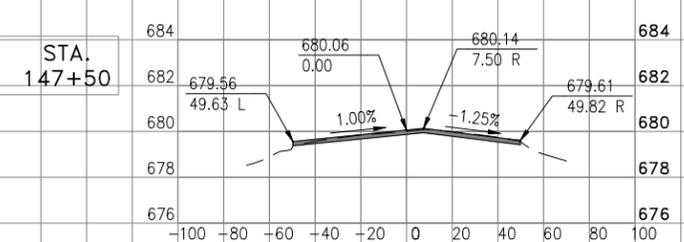
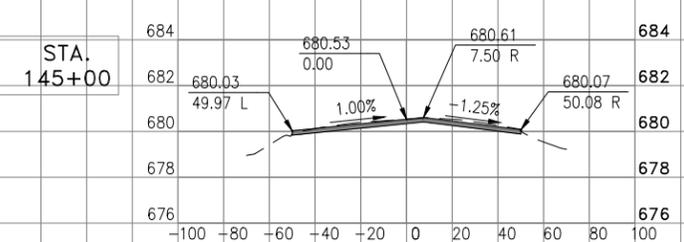
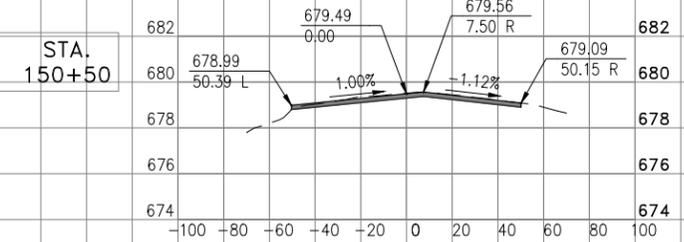
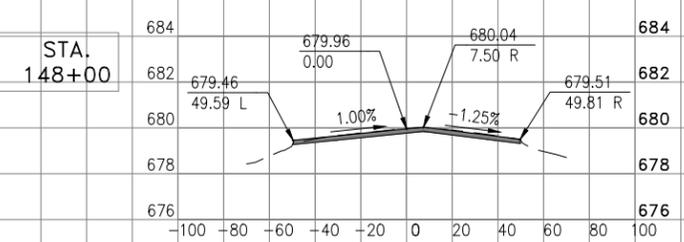
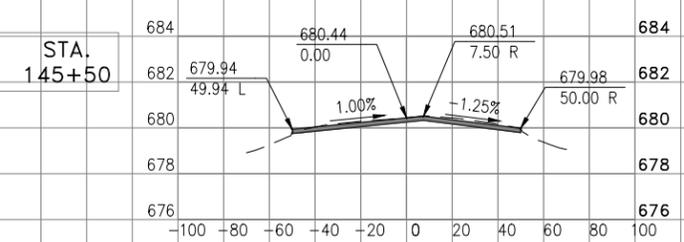
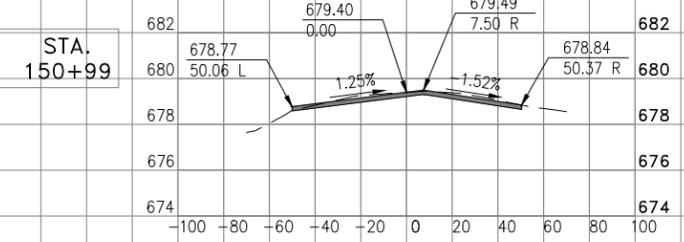
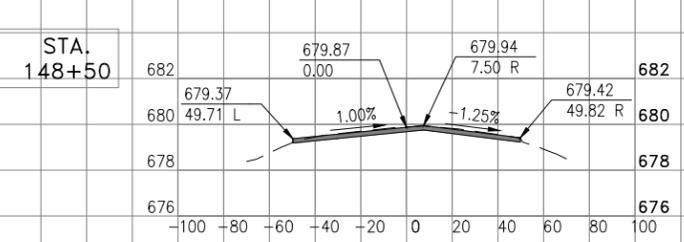
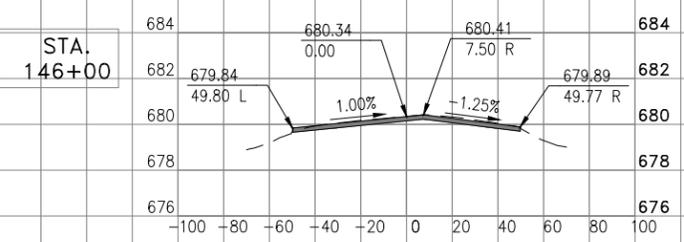
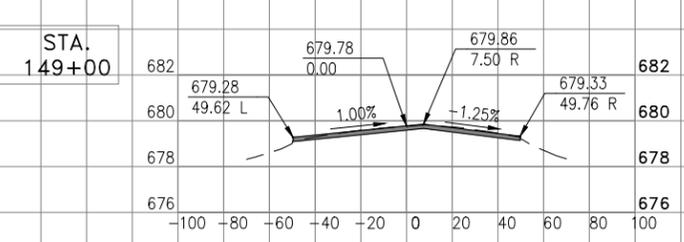
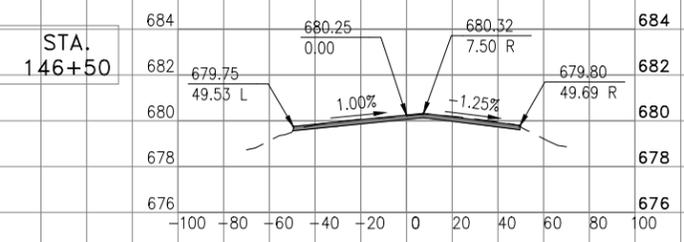
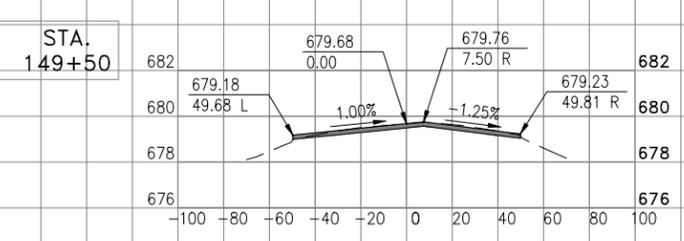
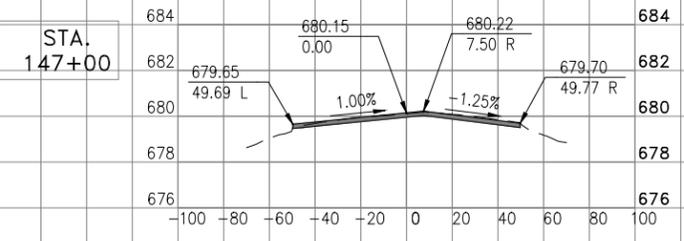
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ISSUE: 04/22/22
PROJECT NO: 20A0074
CAD FILE: C-401-XSECT.DWG
DESIGN BY: LDH 3/14/22
DRAWN BY: LDH 3/14/22
REVIEWED BY: KD 03/22/22

SHEET TITLE

CROSS SECTIONS 6 -
RUNWAY 9-27





REHABILITATE
RUNWAY 9-27

IDA No: MQB-4945
SBGP No:
3-17-SBGP-171/TBD

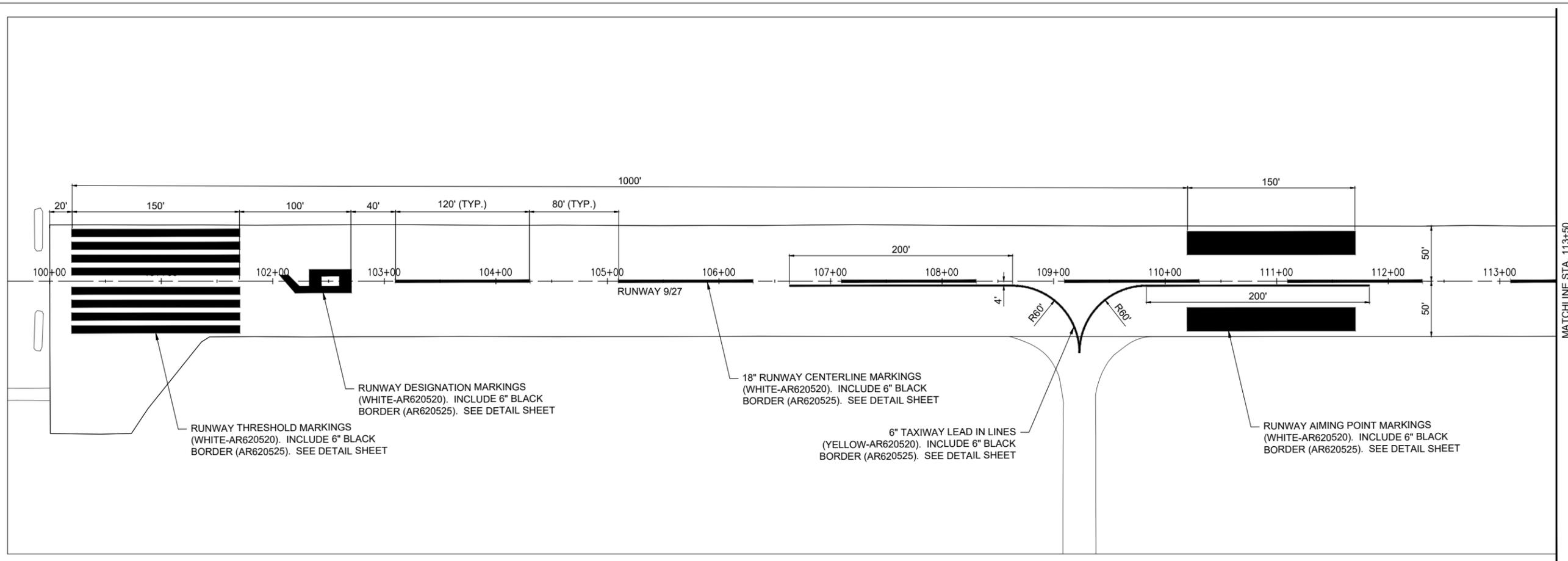
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		DES	DWN	REV

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DRAWN BY: LDH 3/14/22
REVIEWED BY: KD 03/22/22

SHEET TITLE

CROSS SECTIONS 7 -
RUNWAY 9-27



MATCHLINE STA. 113+50

- WATERBORNE PAVEMENT MARKING NOTES**
- A 30-DAY WAITING PERIOD IS REQUIRED BETWEEN THE PLACEMENT OF BITUMINOUS SURFACE COURSE AND THE FINAL APPLICATION OF WATERBORNE PAINT. AFTER THE PLACEMENT OF SURFACE COURSE AND PRIOR TO THE 30-DAY CURE PERIOD AN INITIAL COAT OF MARKINGS SHALL BE PLACED AT 50% OF THE NORMAL APPLICATION RATE. BLACK MARKINGS SHALL NOT BE PLACED DURING THE INITIAL COAT. THE REMAINDER OF THE MARKINGS, INCLUDING THE BLACK BORDERS, SHALL BE PLACED AFTER THE 30-DAY CURE PERIOD HAS ELAPSED AND GROOVING HAS BEEN COMPLETED. (IF ONLY GROOVING AND MARKING REMAIN, THE CALENDAR DAYS MAY BE SUSPENDED DURING THE 30 DAY CURE PERIOD.)
 - WATERBORNE BLACK PAINT SHALL BE USED TO OUTLINE A MINIMUM 6-INCH BORDER AROUND ALL MARKINGS AND WILL BE APPLIED IN ONE APPLICATION.
 - GLASS BEADS SHALL BE APPLIED ON THE SECOND COAT OF MARKING. GLASS BEADS SHALL NOT BE APPLIED TO BLACK PAINT.
 - CUT-OFF SHEETS WILL BE REQUIRED TO ENSURE STRAIGHT EDGES.
 - THE CONTRACTOR WILL BE RESPONSIBLE FOR RE-MARKING ANY MARKINGS DAMAGED BY CONSTRUCTION ACTIVITIES THAT ARE OUTSIDE OF THE PROPOSED WORK LIMITS.

MARKING SCHEDULE			
AR620520 - PAVEMENT MARKING - WATERBORNE (WHITE)			
DESCRIPTION	UNIT AREA (S.F.)	NO. REQUIRED	TOTAL AREA (S.F.)
THRESHOLD MARKING	862.5	16	13,800.0
DESIGNATION MARKING			
9	718.8	1	718.8
2	651.5	1	651.5
7	482.0	1	482.0
AIMING POINT MARKING	3,000.0	4	12,000.0
CENTERLINE MARKING	180.0	23	4,140.0
AR620520 - PAVEMENT MARKING - WATERBORNE (YELLOW)			
DESCRIPTION	UNIT AREA (S.F.)	NO. REQUIRED	TOTAL AREA (S.F.)
6" TAXIWAY LEAD IN LINES	900.0	1	900.0
AR620525 PAVEMENT MARKING - BLACK BORDER			
DESCRIPTION	UNIT AREA (S.F.)	NO. REQUIRED	TOTAL AREA (S.F.)
THRESHOLD MARKING	156.8	16	2,508.0
DESIGNATION MARKING			
9	113.2	1	113.2
2	106.3	1	106.3
7	84.7	1	84.7
AIMING POINT MARKING	171.0	4	684.0
CENTERLINE MARKING	122.5	23	2,817.5
6" TAXIWAY LEAD IN LINES	1,800.0	1	1,800.0

**REHABILITATE
RUNWAY 9-27**

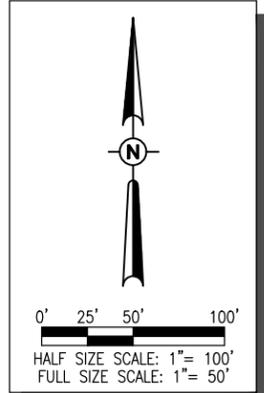
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3-17-SBGP-171/TBD

Contract No.: MB034

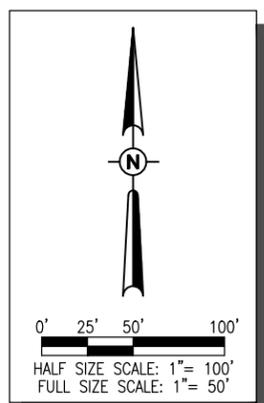
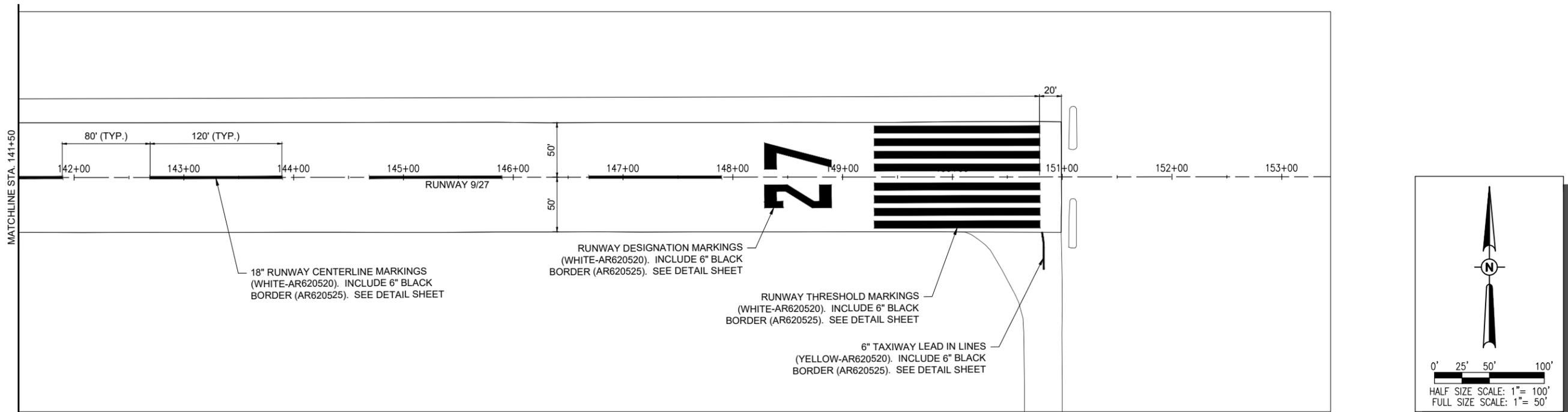
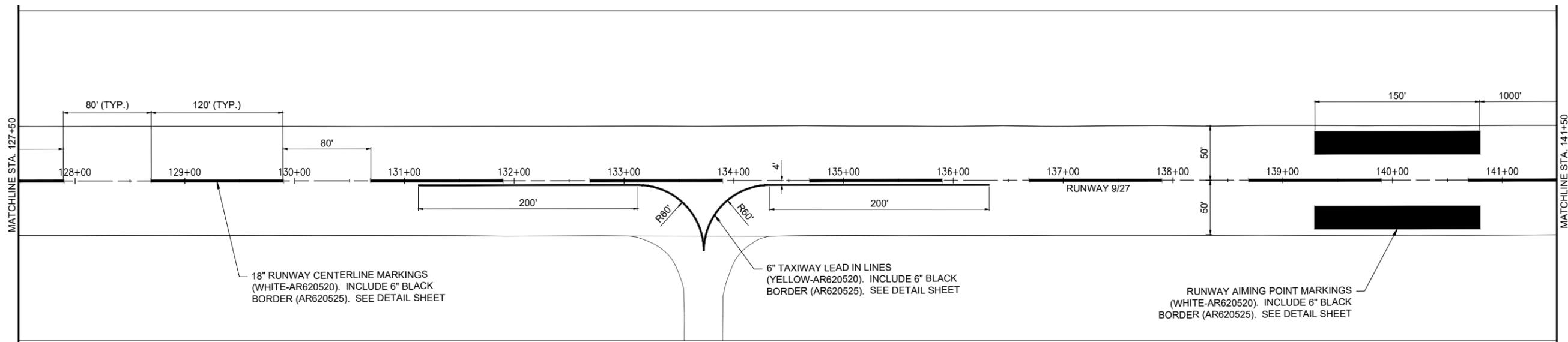
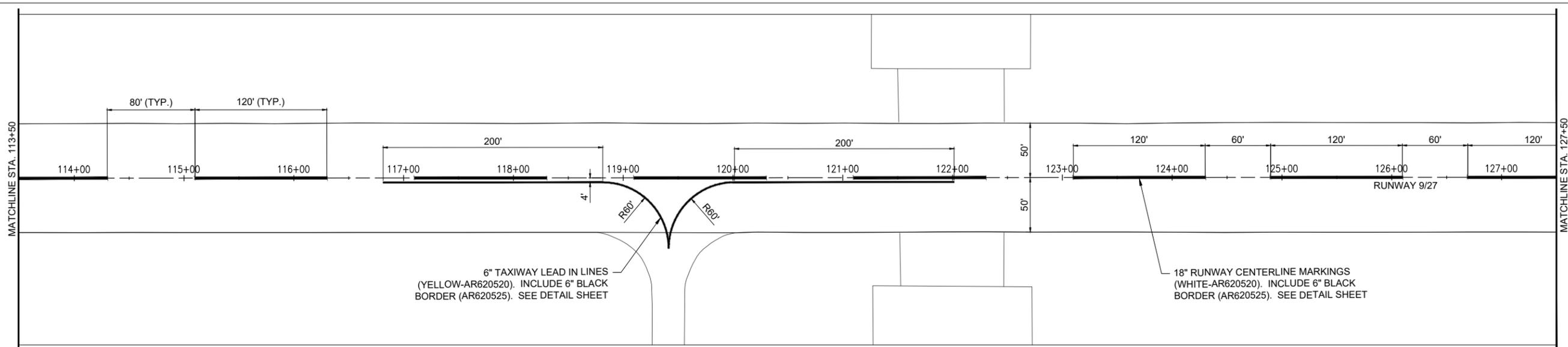
NO.	DATE	DESCRIPTION		
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ISSUE: 04/22/22
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SHEET TITLE



MARKING PLAN - 1



REHABILITATE
RUNWAY 9-27

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SBGP No:
3-17-SBGP-171/TBD

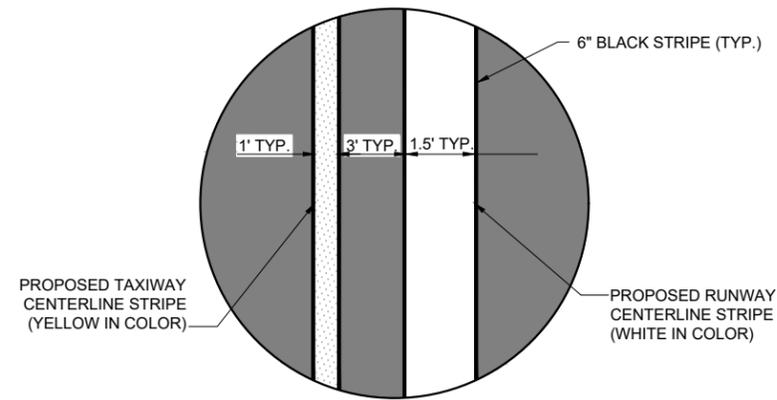
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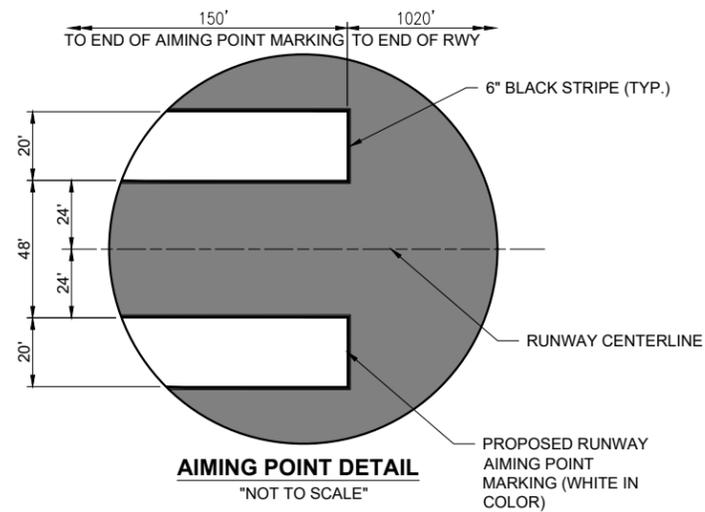
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REVIEWED BY: KD 03/22/22

SHEET TITLE

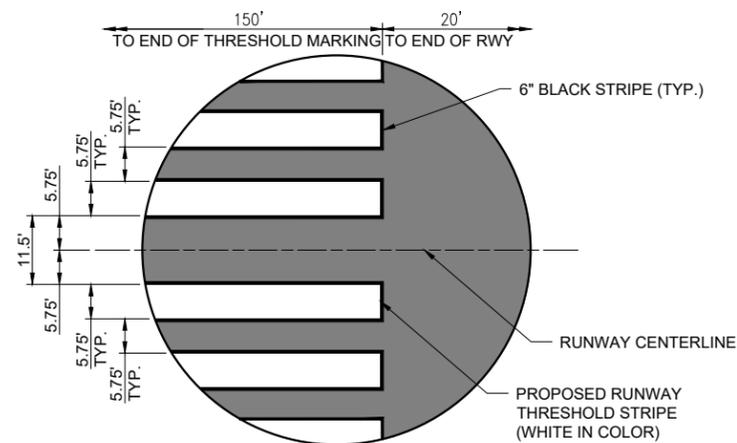
MARKING PLAN - 2



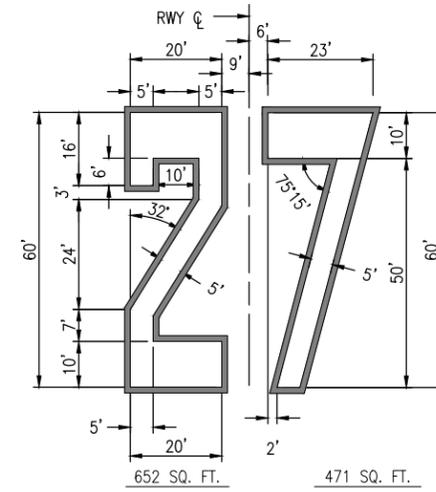
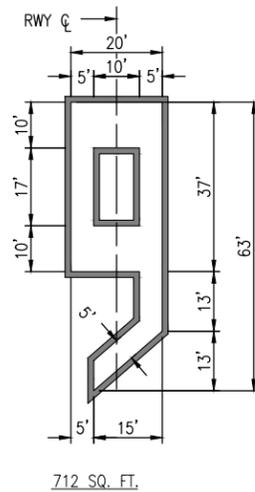
TAXIWAY CENTERLINE LEAD-IN DETAIL
"NOT TO SCALE"



AIMING POINT DETAIL
"NOT TO SCALE"



THRESHOLD STRIPE DETAIL
"NOT TO SCALE"



RUNWAY NUMERAL DIMENSIONING NOTES:

1. NUMERALS EXCEPT FOR THE NUMERAL ELEVEN (AS SHOWN) ARE HORIZONTALLY SPACED 15 FEET APART.
2. ALL DIMENSIONS ARE EXPRESSED IN INCREMENTS OF FEET.
3. DOUBLE DIGIT NUMERAL DESIGNATIONS ARE CENTERED ON THE RUNWAY PAVEMENT CENTERLINE BASED ON THE CENTER OF THE OUTER EDGES OF THE TWO NUMERALS.
4. CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

**REHABILITATE
RUNWAY 9-27**

IDA No: MQB-4945
SBGP No:
3-17-SBGP-171/TBD

Contract No.: MB034

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MARKING DETAILS