

ITEM 04A
 LETTING DATE: JUNE 17, 2022

TOTAL SHEETS: 91
 CA 023

CONSTRUCTION PLANS FOR ABRAHAM LINCOLN CAPITAL AIRPORT

SPRINGFIELD AIRPORT AUTHORITY
 SPRINGFIELD, IL

BID SUBMITTAL

IL. PROJ. NO: SPI-4943

AIP PROJ. NO: 3-17-0096-XX

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

APRIL 22, 2022

811 Know what's below.
 Call before you dig.
 COMMON GROUND ALLIANCE
 www.call811.com or
 Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

DESIGN INFORMATION

GEOMETRIC CRITERIA

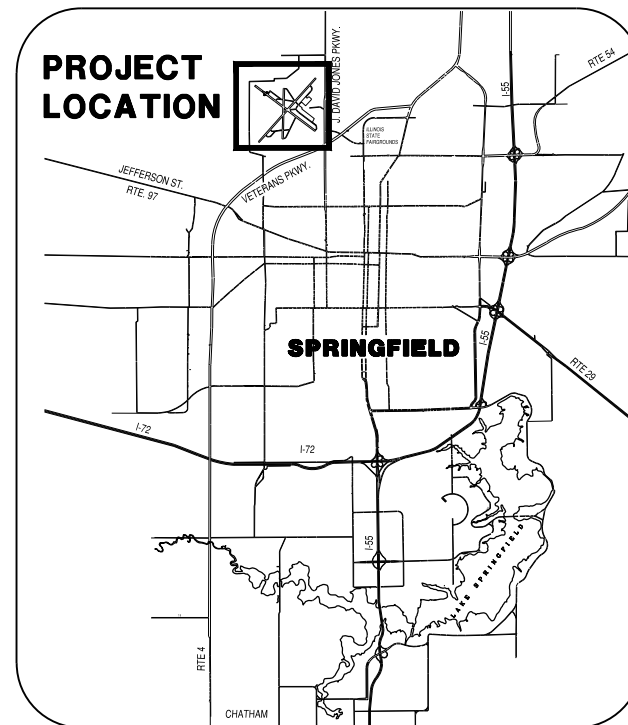
TAXIWAY CONSTRUCTION

APPROACH CATEGORY C
 AIRPLANE DESIGN GROUP III
 TAXIWAY DESIGN GROUP 3

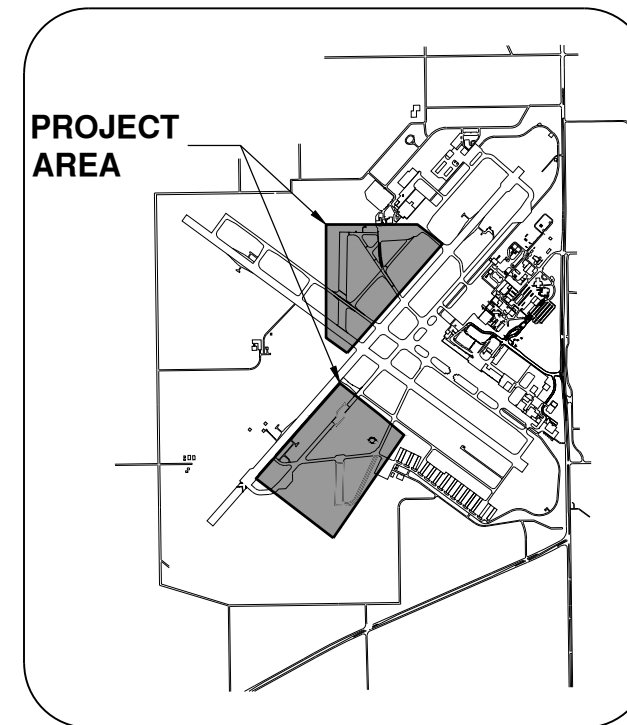
TAXIWAY SAFETY AREA: 118'
 TAXIWAY OBJECT FREE AREA: 186'
 TAXIWAY WIDTH: 50'
 RUNWAY SAFETY AREA: 500'

ABRAHAM LINCOLN CAPITAL
 AIRPORT

TOWNSHIP: 16 NORTH
 RANGE: 5 WEST OF THE 4TH P.M.
 SECTION: 8, 9, 16 & 17
 COUNTY: SANGAMON
 CIVIL TOWNSHIP: CAPITAL



LOCATION MAP



SITE PLAN



ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD AIRPORT AUTHORITY
 SPRINGFIELD, ILLINOIS
 APPROVED *Mark S. Stearns*
 EXECUTIVE DIRECTOR OF AVIATION
 DATE 4/22/2022

CMT
 ■ SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO
 SUBMITTED BY *W. Koerber*
 DATE 4/22/2022
 CMT JOB NUMBER: 180035-05-00



License No. 184-000613

CONSULTANTS

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK DATE DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-GI002.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
COPYRIGHT:	

SHEET TITLE
**INDEX TO SHEETS &
SUMMARY OF
QUANTITIES**

G1002
SHEET 2 OF 91

SUMMARY OF QUANTITIES ADDITIVE ALTERNATE 1			
ITEM NO.	ITEM DESCRIPTION	UNITS	QTY
AX 108108	1/C #8 5 KV UG CABLE	FOOT	225.0
AX 108158	1/C #8 5 KV UG CABLE IN UD	FOOT	5125.0
AX 108258	2/C #8 5 KV UG CABLE IN UD	FOOT	365.0
AX 108706	1/C #6 COUNTERPOISE	FOOT	5250.0
AX 125415	MITL - BASE MOUNTED	EACH	4.0
AX 125416	MITL - BASE MOUNTED - LED	EACH	49.0
AX 125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EACH	2.0
AX 125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	2.0
AX 125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	1.0
AX 125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EACH	4.0
AX 125901	REMOVE STAKE MOUNTED LIGHT	EACH	33.0
AX 125902	REMOVE BASE MOUNTED LIGHT	EACH	40.0
AX 125904	REMOVE TAXI GUIDANCE SIGN	EACH	8.0
AX 150520	MOBILIZATION	L SUM	1.0
AX 150550	CONSTRUCTION ACCESS	L SUM	1.0
AX 150552	RESTORE CONSTRUCTION ACCESS	L SUM	1.0
AX 152419	UNCLASSIFIED DISPOSAL OFFSITE	CU YD	15100.0
AX 152455	EMBANKMENT IN PLACE	CU YD	13500.0
AX 156520	INLET PROTECTION	EACH	3.0
AX 501905	REMOVE PAVEMENT	SQ YD	24800.0
AX 701512	12" RCP, CLASS IV	FOOT	890.0
AX 751410	INLET	EACH	3.0
AX 901510	SEEDING	ACRE	7.3
AX 904510	SODDING	SQ YD	700.0
AX 908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	7.3

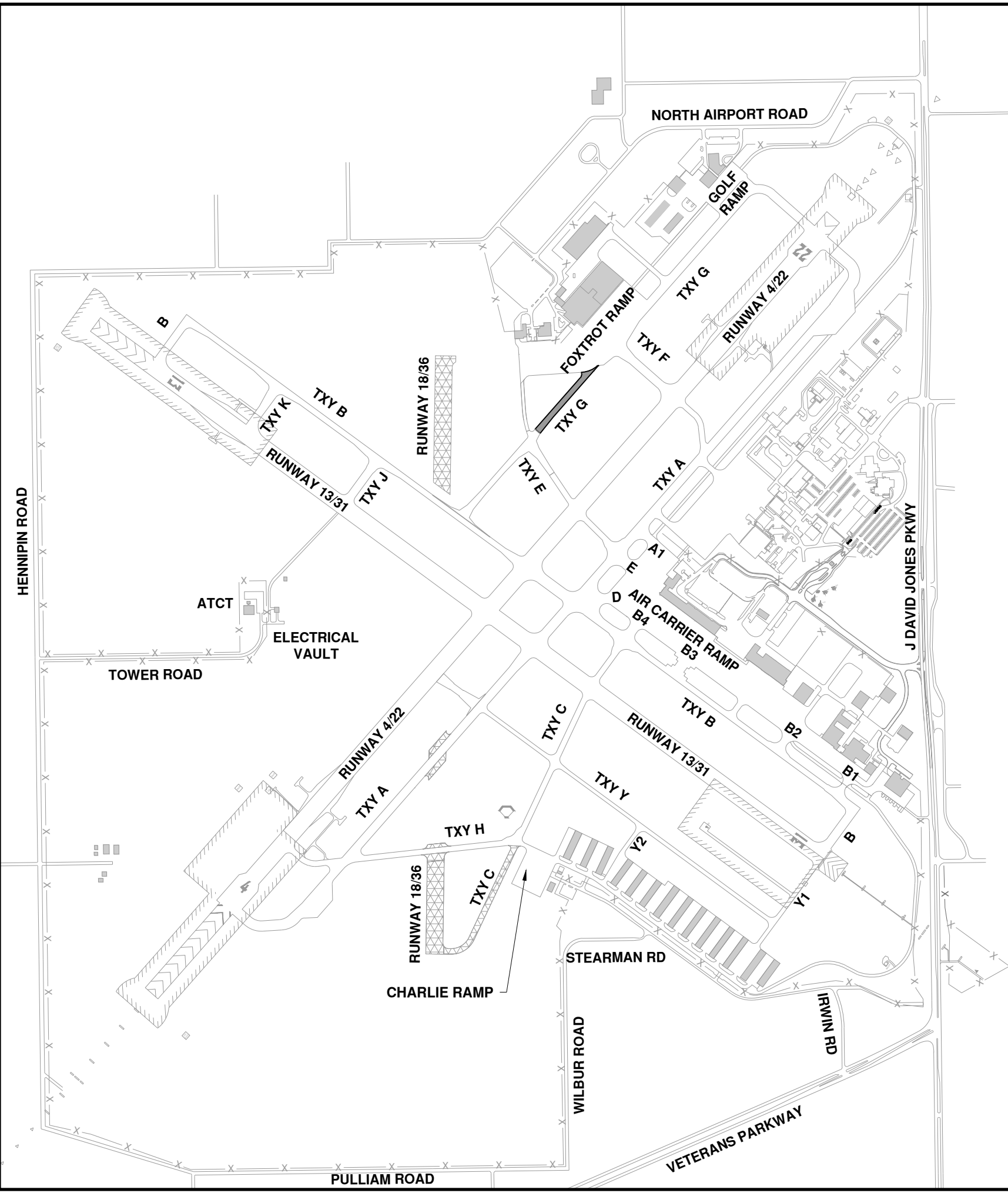
SUMMARY OF QUANTITIES BASE BID			
ITEM NO.	ITEM DESCRIPTION	UNITS	QTY
AW 108108	1/C #8 5 KV UG CABLE	FOOT	200.0
AW 108158	1/C #8 5 KV UG CABLE IN UD	FOOT	1550.0
AW 108258	2/C #8 5 KV UG CABLE IN UD	FOOT	1025.0
AW 108706	1/C #6 COUNTERPOISE	FOOT	2350.0
AW 108800	CONTROL CABLE	FOOT	9000.0
AW 108980	CABLE PROTECTION SLAB	FOOT	200.0
AW 109321	10 KW REGULATOR, STYLE 1	EACH	1.0
AW 109620	LIGHTING CONTROL SYSTEM	L SUM	1.0
AW 110102	DUCT MARKER - IN PAVEMENT	EACH	3.0
AW 110501	1-WAY CONCRETE ENCASED DUCT	FOOT	180.0
AW 110504	4-WAY CONCRETE ENCASED DUCT	FOOT	290.0
AW 110610	ELECTRICAL HANDHOLE	EACH	2.0
AW 125416	MITL - BASE MOUNTED - LED	EACH	15.0
AW 125441	TAXI GUIDANCE SIGN, 1 CHARACTER	EACH	1.0
AW 125740	RUNWAY GUARD LIGHT	EACH	12.0
AW 125902	REMOVE BASE MOUNTED LIGHT	EACH	13.0
AW 125904	REMOVE TAXI GUIDANCE SIGN	EACH	1.0
AW 125912	REMOVE RETROREFLECTIVE MARKER	EACH	9.0
AW 150510	ENGINEER'S FIELD OFFICE	L SUM	1.0
AW 150520	MOBILIZATION	L SUM	1.0
AW 150550	CONSTRUCTION ACCESS	L SUM	1.0
AW 150552	RESTORE CONSTRUCTION ACCESS	L SUM	1.0
AW 152419	UNCLASSIFIED DISPOSAL OFFSITE	CU YD	11800.0
AW 152455	EMBANKMENT IN PLACE	CU YD	17730.0
AW 154506	GRANULAR SUBBASE - 6"	SQ YD	4700.0
AW 156520	INLET PROTECTION	EACH	15.0
AW 209606	CRUSHED AGG. BASE COURSE - 6"	SQ YD	4700.0
AW 401010	CONTRACTOR QUALITY CONTROL PROGRAM	L SUM	1.0
AW 401610	BITUMINOUS SURFACE COURSE	TON	1000.0
AW 401630	BITUMINOUS SURFACE TEST SECTION	EACH	1.0
AW 403610	BITUMINOUS BASE COURSE	TON	2500.0
AW 403630	BITUMINOUS BASE TEST SECTION	EACH	1.0
AW 501905	REMOVE PAVEMENT	SQ YD	20000.0
AW 603510	BITUMINOUS TACK COAT	GALLON	1900.0
AW 620520	PAVEMENT MARKING - WATER BORNE	SQ FT	2000.0
AW 620525	PAVEMENT MARKING - BLACK BORDER	SQ FT	2000.0
AW 620900	PAVEMENT MARKING REMOVAL	SQ FT	4000.0
AW 701518	18" RCP, CLASS IV	FOOT	670.0
AW 701524	24" RCP, CLASS IV	FOOT	1000.0
AW 701900	REMOVE PIPE	FOOT	1000.0
AW 705524	4" PERFORATED UNDERDRAIN W/SOCK	FOOT	1500.0
AW 705640	UNDERDRAIN CLEANOUT	EACH	4.0
AW 705645	UNDERDRAIN CONNECTION	EACH	2.0
AW 751410	INLET	EACH	6.0
AW 751560	MANHOLE 6'	EACH	4.0
AW 751900	REMOVE INLET	EACH	1.0
AW 800308	EXPLORATORY EXCAVATION	EACH	6.0
AW 801505	RUNWAY WEATHER INFORMATION SYSTEM	L SUM	1.0
AW 901510	SEEDING	ACRE	10.0
AW 904510	SODDING	SQ YD	840.0
AW 908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	10.0

Sheet List		
Sheet Index	Sheet Number	Sheet Title
50	EL101	ELECTRICAL PLAN TXY G
51	EL102	ELECTRICAL PLAN TXY H
52	EL103	ELECTRICAL PLAN TXY A
53	EL104	RUNWAY WEATHER INFORMATION SYSTEM (RWIS)
54	EL105	RUNWAY GUARD LIGHT LAYOUT
55	EL401	ELECTRICAL SCHEDULES 1
56	EL402	ELECTRICAL SCHEDULES 2
57	EL501	ELECTRICAL DETAILS 1
58	EL502	ELECTRICAL DETAILS 2
59	EL503	ELECTRICAL DETAILS 3
60	EL504	ELECTRICAL DETAILS 4
61	EL505	ELECTRICAL DETAILS 5
62	EL506	ELECTRICAL DETAILS 6
63	EL507	EXISTING VAULT
64	EL508	PROPOSED VAULT IMPROVEMENTS
65	EL509	ALCMS BLOCK DIAGRAM - EXISTING
66	EL510	ALCMS BLOCK DIAGRAM - PROPOSED
67	EL511	ALCMS DETAILS
68	CM101	PAVEMENT MARKING PLAN 1
69	CM501	PAVEMENT MARKING DETAILS
70	LG101	EROSION CONTROL & TURFING PLANS 1
71	LG102	EROSION CONTROL & TURFING PLANS 2
72	LG103	EROSION CONTROL & TURFING PLANS 3
73	LG104	EROSION CONTROL & TURFING PLANS 4
74	LG501	EROSION CONTROL DETAILS
75	CG600	CROSS SECTION INDEX 1
76	CG601	CROSS SECTION INDEX 2
77	CG602	TAXIWAY G CROSS SECTIONS 1
78	CG603	TAXIWAY G CROSS SECTIONS 2
79	CG604	TAXIWAY G CROSS SECTIONS 3
80	CG605	IN-FIELD DITCH CROSS SECTIONS 1
81	CG606	IN-FIELD DITCH CROSS SECTIONS 2
82	CG607	RWY 18-36 NORTH CROSS SECTIONS 1
83	CG608	RWY 18-36 NORTH CROSS SECTIONS 2
84	CG609	RWY 18-36 NORTH CROSS SECTIONS 3
85	CG610	TAXIWAY A CROSS SECTIONS 1
86	CG611	TAXIWAY A CROSS SECTIONS 2
87	CG612	TAXIWAY H CROSS SECTIONS 1
88	CG613	RUNWAY 18-36 SOUTH CROSS SECTIONS 1
89	CG614	RUNWAY 18-36 SOUTH CROSS SECTIONS 2
90	CG615	TAXIWAY C CROSS SECTIONS 1
91	CG616	TAXIWAY C CROSS SECTIONS 2

Sheet List		
Sheet Index	Sheet Number	Sheet Title
1	GI001	COVER SHEET
2	GI002	INDEX TO SHEETS & SUMMARY OF QUANTITIES
3	GI101	AIRPORT SITE PLAN
4	GC001	CONSTRUCTION ACTIVITY PLAN NOTES 1
5	GC002	CONSTRUCTION ACTIVITY PLAN NOTES 2
6	GC003	CONSTRUCTION ACTIVITY PLAN DETAILS 1
7	GC100	CONST. ACTIVITY PLAN OVERVIEW & SITE ACCESS
8	GC101	CONSTRUCTION ACTIVITY PLAN WORK AREA A
9	GC102	CONSTRUCTION ACTIVITY PLAN WORK AREA B1
10	GC103	CONSTRUCTION ACTIVITY PLAN WORK AREA B2
11	GC104	CONSTRUCTION ACTIVITY PLAN WORK AREAS B3 & B4
12	CD001	EXISTING PAVEMENT STRUCTURES
13	CD002	EXISTING UTILITIES 1
14	CD003	EXISTING UTILITIES 2
15	CD101	EX. CONDITIONS & REMOVALS TXY G EAST
16	CD102	EX. CONDITIONS & REMOVALS RWY 18-36 NORTH
17	CD103	EX. CONDITIONS & REMOVALS TXY A
18	CD104	EX. CONDITIONS & REMOVALS TXY H
19	CD105	EX. CONDITIONS & REMOVAL RWY 18-36 SOUTH
20	CD106	EX. CONDITIONS & REMOVAL TXY C
21	CP100	HORIZONTAL CONTROL
22	CP101	PROPOSED GEOMETRY TXY G
23	CP201	PLAN & PROFILE 1 - TAXIWAY G
24	CP202	PLAN & PROFILE 2 - RUNWAY 18-36 NORTH
25	CP203	PLAN & PROFILE 3 - RUNWAY 18-36 SOUTH
26	CP204	PLAN & PROFILE 4 - TAXIWAY C
27	CP301	TYPICAL SECTIONS
28	CS101	STAKING PLAN TXY G EAST 1
29	CG101	GRADING & DRAINAGE PLAN TXY G
30	CG102	GRADING & DRAINAGE PLAN RWY 1836 N
31	CG103	GRADING & DRAINAGE PLAN RWY 1836 S
32	CG104	GRADING & DRAINAGE PLAN TXY C
33	CG201	DITCH PROFILES 1
34	CG202	DITCH PROFILES 2
35	CG301	GRADING & DRAINAGE DETAILS 1
36	CG302	EMBANKMENT & STOCKPILE AREAS 1
37	CG303	EMBANKMENT & STOCKPILE AREAS 2
38	CU101	STORM SEWER & UNDERDRAIN PLAN TXY G WEST
39	CU102	STORM SEWER & UNDERDRAIN PLAN RWY 1836 S
40	CU201	STORM SEWER PROFILES 1
41	CU202	STORM SEWER PROFILES 2
42	CU203	STORM SEWER PROFILES 3
43	CU401	STORM SEWER & UNDERDRAIN SCHEDULES
44	CU501	UNDERDRAIN DETAILS
45	CU502	STORM SEWER DETAILS 2
46	CU503	STORM SEWER DETAILS 3
47	CU504	STORM SEWER DETAILS 4
48	CU505	STORM SEWER DETAILS 5
49	CU506	STORM SEWER DETAILS 6

Path: K:\Springfield\18035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-GI002.dwg
Date: Monday, May 9, 2022 10:54:50 AM

Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PH3-G1101.dwg
 Date: Tuesday, April 26, 2022 12:38:15 PM



LEGEND

- PAVEMENT REMOVALS
- NEW PAVEMENT
- AIRPORT FENCE

NOT SHOWN
 RUNWAY WEATHER INFORMATION SYSTEM
 AIRFIELD LIGHTING CONTROL MONITORING SYSTEM

GROUND CONTROL FREQUENCY - 121.90
 AIR CONTROL FREQUENCY - 121.30
 MAXIMUM HEIGHT OF EQUIPMENT - 25'

License No. 184-000613
 CONSULTANTS

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
 APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER

SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, IL

MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX CA022		
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-G1101.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: MJD		
APPROVED BY: RLV		
COPYRIGHT:		

SHEET TITLE

AIRPORT SITE PLAN

G1101
 SHEET 3 OF 91

0. GENERAL

- 1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2 (LATEST VERSION), AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2 (LATEST VERSION). NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
4. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RPR. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

- 1. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- 1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS ALLOWED IN THE PLANS.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- 1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

- 1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN IN THE PLANS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS OR THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
3. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
4. CONTRACTOR EMPLOYEES MAY BE REQUIRED TO OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS.
5. ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATIONS AREA (AOA) SHALL ALSO ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.

- 6. CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE MARKED AND FLAGGED PER THE PLAN DETAILS AND SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
7. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) WILL NOT NEED TO OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA, THE TRUCK DRIVERS SHALL BE UNDER THE CONTROL OF AND SUPERVISED BY THE CONTRACTOR.

- 8. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH AIR TRAFFIC AND THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN WITHIN THE MOVEMENT AREA. THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY PASSED THE APPROVED AIRPORT TESTS MAY OPERATE THESE RADIOS.
9. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.

- 10. THE CONTRACTORS STORAGE AND STAGING AREAS WILL BE AS SHOWN IN THE PLANS.
11. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.

- 12. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE RETURNED TO & STORED AT THE STAGING AREA.
13. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.

- 14. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
15. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH ARE OR WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF THE AIRPORT. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.

5. CONTRACTOR ACCESS (CONTINUED)

- 16. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
17. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
18. THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY MAY REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

- 1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF ANY WILDLIFE IS SEEN ON OR ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.
4. THE CONTRACTOR SHALL CONTROL GRASS HEIGHTS THROUGH MOWING TO ASSIST WITH WILDLIFE CONTROL.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- 1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- 1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- 1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO THE AIRPORT PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.

10. INSPECTION REQUIREMENTS

- 1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2 (LATEST VERSION) MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL REQUEST AND ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS SUITABLE TO BE OPENED.

11. UNDERGROUND UTILITIES

- 1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. THE AIRPORT, AND CONTACT THE LOCAL FAA OFFICE (847-294-7336) TO ARRANGE FOR UTILITY LOCATES.

12. PENALTIES

- 1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP, THE CONTRACTOR'S APPROVED SPCD OR THE SECURITY PLAN MAY RESULT IN FINES AS ALLOWED BY LAW.
2. FINES CAN BE LEVIED AGAINST THE CONTRACTOR BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) FOR NEGLIGENCE IF THE AIRPORT SECURITY IS COMPROMISED AND THE AIRPORT PERIMETER FENCE LINE IS NOT MAINTAINED AS SPECIFIED ABOVE. FINES CAN ALSO BE LEVIED AGAINST THE CONTRACTOR FOR FAILURE TO COOPERATE WITH THE AIRPORT MANAGEMENT AS REQUIRED TO MAINTAIN AIRPORT SECURITY.

13. SPECIAL CONDITIONS

- 1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- 1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE PLANS.
2. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF THE PLANS & FAA AC 150/5370-2 (LATEST VERSION.)
3. IMMEDIATELY UPON THE INITIATION OF EACH PROJECT PHASE, THE CONTRACTOR SHALL DE-ENERGIZE OR COVER LIGHTS FOR ALL CLOSED RUNWAYS AND TAXIWAYS DURING PAVEMENT CLOSURES. TEMPORARILY COVER OR DE-ENERGIZE AIRFIELD SIGNAGE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.

15. MARKING AND SIGNS FOR ACCESS ROUTES

- 1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED IN THE PLANS.

16. HAZARD MARKING AND LIGHTING

- 1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5D (OR LATEST) AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN IN THE PLANS OR AS DIRECTED BY THE AIRPORT.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE LIGHTS AND FLAG PLACEMENT.

17. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

- 1. THE CONTRACTOR SHALL PROVIDE ADEQUATE LIGHTING DURING NIGHTTIME CONSTRUCTION.
2. ARTIFICIAL AREA LIGHTING SHALL CONSIST OF VEHICLE OR POLE MOUNTED FLOODLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL ONLY BE ALLOWED IN ADDITION TO THE AREA LIGHTING.
3. ARTIFICIAL AREA LIGHTING SHALL NOT INTERFERE WITH AIR TRAFFIC OR ATCT OPERATIONS.
4. PLACEMENT & AIMING OF ARTIFICIAL LIGHTING SHALL BE APPROVED BY THE AIRPORT PRIOR TO START OF OPERATIONS.

18. PROTECTION OF AREAS & SURFACES

- 1. ALL WORK REQUIRED INSIDE OF THE RUNWAY 4-22 OR 13-31 SAFETY AREAS, WHICH EXTENDS 250' FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 93' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
3. ALL WORK REQUIRED ON AN ACTIVE TAXILANE OBJECT FREE AREA, WHICH EXTENDS 81' FROM THE TAXILANE/APRON CENTERLINE, WILL REQUIRE A PORTION OF THAT APRON TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

(NOTES CONTINUE ON SHEET GC002)



License No. 184-000613

CONSULTANTS

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

Table with columns: MARK, DATE, DESCRIPTION. Includes project details like AIP PROJ. NO: 3-17-0096-XX, IL PROJ. NO: SPI-4943, CMT PROJECT NO: 180035-05, CAD DWG FILE: 18003505-PH3-GC000.DWG, DESIGNED BY: HWI, DRAWN BY: DPA, CHECKED BY: MJD, APPROVED BY: RLV.

CONSTRUCTION ACTIVITY PLAN NOTES 1

GC001
SHEET 4 OF 91

Path: K:\Springfield\A0180035-05_Phrw_18-36\Draw\Sheets\PH3-GC000.dwg
Date: Tuesday, April 26, 2022 12:38:22 PM

19. OTHER LIMITATIONS ON CONSTRUCTION

- 1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS REQUIRED IN THE SPECIFICATIONS.

20. AIRPORT SECURITY REQUIREMENTS

- 1. MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
2. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR. THE SUPERINTENDENT AND FOREMAN THAT ARE ISSUED BADGES SHALL BE DIRECTLY RESPONSIBLE FOR THE IDENTITY AND LOCATION OF THOSE THEY ARE SUPERVISING WHILE ON THE AIRFIELD. BADGES SHALL BE RETURNED TO THE AIRPORT AT THE FINAL INSPECTION OR WHEN THE PERSON IS NO LONGER EMPLOYED BY THE CONTRACTOR. THE CONTRACTOR WILL PAY A FEE OF \$200.00 WITHIN 15 DAYS FOR EACH ACCESS BADGE THAT IS LOST, DESTROYED, STOLEN, OR NOT RETURNED AT THE FINAL INSPECTION.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES.
4. THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR.
5. THE CONTRACTOR SHALL INSTALL AND USE TEMPORARY GATES FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER, THE MAINTENANCE SUPERVISOR, AND THE SECURITY CHIEF. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE RESIDENT ENGINEER. NO ADDITIONAL PAYMENT SHALL BE MADE FOR THE INSTALLATION AND REMOVAL OF TEMPORARY ACCESS GATES.
6. AS A MINIMUM, THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURITY DURING CONSTRUCTION AS FOLLOWS:
a. POSSESS A COPY OF THE AIRPORT'S PROJECT SECURITY PLAN.
b. VISIBLY DELINEATE HIS CONSTRUCTION ZONE BY PLACING A LINE OF BARRICADES OR FLAGGING AROUND THE ENTIRE WORK ZONE DURING EACH PHASE OF THE CONTRACT.
c. COMPLY WITH THE AIRPORT'S SECURITY PLAN ASSOCIATED WITH THE CONSTRUCTION PROJECT AND ENSURE THAT CONSTRUCTION PERSONNEL ARE FAMILIAR WITH SECURITY PROCEDURES AND REGULATIONS ON THE AIRPORT.
d. ENSURE THAT NO CONSTRUCTION EMPLOYEES, EMPLOYEES OF SUBCONTRACTORS OR SUPPLIERS, OR OTHER PERSONS ENTER ANY PART OF THE AIRCRAFT OPERATIONS AREA FROM CONSTRUCTION SITE UNLESS AUTHORIZED.
e. THE AIRPORT MAY REQUIRE THAT ALL SECURITY GUARDS UNDERGO ADDITIONAL TRAINING NECESSARY TO MEET THE AIRPORT'S SECURITY NEEDS.
f. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN SECURITY ON THE AIRPORT AS SPECIFIED OR AS DIRECTED BY THE AIRPORT.
g. THE CONTRACTOR'S SUPERINTENDENT, FOREMAN, SECURITY GUARDS, AND ANY SUPERVISORY PERSONNEL IN CHARGE OF OTHER WORKERS SHALL OBTAIN AN AIRPORT AUTHORITY SECURITY BADGE AND DISPLAY THIS BADGE WHILE ON SITE IN ACCORDANCE WITH FAA AND TSA REGULATIONS. CONTRACTOR PERSONNEL WITH BADGES SHALL BE DIRECTLY RESPONSIBLE FOR THE IDENTITY AND LOCATION OF THOSE THEY ARE SUPERVISING WHILE ON THE AIRFIELD.

20. AIRPORT SECURITY REQUIREMENTS (CONT'D)

- h. TO OBTAIN AIRPORT SECURITY BADGES, CONTRACTORS MUST COMPLETE A CRIMINAL HISTORY RECORDS CHECK TWO WEEKS PRIOR TO EMPLOYEES BEING ALLOWED ACCESS TO THE SITE. THE TWO-WEEK PERIOD IS NECESSARY FOR AN ADEQUATE TIME OF PROCESSING FINGERPRINTS FOR COMPLETING THE CRIMINAL HISTORY CHECK. THE CONTRACTOR IS REQUIRED TO DEPOSIT A \$200 FEE TO THE AIRPORT PER BADGE, WHICH IS 80% REFUNDABLE AFTER EACH BADGE IS RETURNED. THE AIRPORT MUST RECEIVE THIS FEE/DEPOSIT PRIOR TO CONDUCTING ANY OF THE SECURITY BADGE ISSUE PROCESS. THE CONTRACTOR IS REQUIRED TO CONTACT THE AIRPORT AT LEAST THREE CALENDAR DAYS PRIOR TO SCHEDULING FINGERPRINTING AND BADGE TRAINING. TRAINING LASTS APPROXIMATELY TWO HOURS AND CAN BE CONDUCTED INDIVIDUALLY OR WITH A GROUP.
i. THE UNITED STATES DEPARTMENT OF HOMELAND SECURITY HAS DESIGNATED THE SECURITY LEVEL AS HIGH RISK AT ABRAHAM LINCOLN CAPITAL AIRPORT. SUBSEQUENTLY, THE AIRPORT MUST HAVE ALL SECURITY ACTIVITY AND PROGRAMS APPROVED BY THE UNITED STATES DEPARTMENT OF HOMELAND SECURITY TRANSPORTATION SECURITY ADMINISTRATION. TO OBTAIN AIRPORT AUTHORITY ACCESS BADGES, THE CONTRACTOR SHALL PROVIDE COMPLETED ACCESS CONTROL FORMS PROVIDED BY THE AIRPORT FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THE CONTRACTORS WILL NEED TO PROVIDE THE AIRPORT AUTHORITY WITH DOCUMENTATION FROM THE LIST OF ACCEPTABLE DOCUMENTS, WHICH ESTABLISHES BOTH IDENTITY AND EMPLOYMENT AUTHORIZATION TWO WEEKS PRIOR TO EMPLOYEES BEING ALLOWED ACCESS TO THE SITE. THE TWO-WEEK PERIOD IS NECESSARY FOR AN ADEQUATE TIME OF PROCESSING OF A SECURITY THREAT ASSESSMENT (STA) FOR EACH EMPLOYEE REQUESTING ACCESS BY THE HOMELAND SECURITY TRANSPORTATION SECURITY ADMINISTRATION. THE CONTRACTOR IS REQUIRED TO CONTACT THE DIRECTOR OF OPERATIONS AND PUBLIC SAFETY AT 217-494-0787 FOR THE SCHEDULING OF ALL ACCESS BADGE APPLICATIONS.
j. A MINIMUM OF SEVEN (7) DAYS PRIOR TO CONSTRUCTION, A MANDATORY SAFETY MEETING WILL BE CONDUCTED FOR ALL PARTIES INVOLVED. THIS SAFETY MEETING WILL INCLUDE A REVIEW OF A TRANSPORTATION SECURITY ADMINISTRATION (TSA) APPROVED ABRAHAM LINCOLN CAPITAL AIRPORT CHANGE OF CONDITION FOR THIS CONSTRUCTION PROJECT. THIS REVIEW WILL INCLUDE A SET OF SECURITY REQUIREMENTS AND PROCEDURES THAT THE CONTRACTOR WILL BE REQUIRED TO FOLLOW AT ALL TIMES WHILE OPERATING AT THE AIRPORT.
k. THE CONTRACTOR WILL DESIGNATE A MINIMUM OF ONE INDIVIDUAL TO BE THE 24-HOUR POINT OF CONTACT AND ASSUME ALL ON-SITE SECURITY RESPONSIBILITIES FOR ALL EMPLOYEES DURING THE PROJECT. THIS INDIVIDUAL SHALL PROVIDE THE AIRPORT AUTHORITY A 24-HOUR CONTACT FOR EMERGENCY PURPOSES. THIS INDIVIDUAL WILL ALSO BE REQUIRED TO HAVE AVAILABLE AND PRESENT UPON REQUEST AT ANY TIME; A COPY OF THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) APPROVED CHANGE OF CONDITION DURING THE PROJECT.
l. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE CLOSED AND LOCKED DURING WORK HOURS. IF THE CONTRACTOR CHOOSES TO LEAVE THE GATE OPEN, THEN HE SHALL POST A COMPETENT, PROPERLY TRAINED SECURITY GUARD TO PREVENT UNAUTHORIZED ENTRIES. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS IF SO DIRECTED BY THE AIRPORT.
m. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR THIS PADLOCK TO THE RESIDENT ENGINEER AND AIRPORT. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE AIRPORT.
n. THE CONTRACTOR SHALL PROVIDE A SIGN AT ALL ACCESS GATES STATING " AUTHORIZED PERSONNEL ONLY." ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

20. AIRPORT SECURITY REQUIREMENTS (CONT'D)

- o. THE AIRPORT OPERATOR HAS A PROGRAM IN WHICH THE CONTRACTOR HAS THE ABILITY TO HAVE PERSONNEL APPROVED TO ACQUIRE ACCESS TO THE AIR OPERATIONS AREA (AOA) WITHOUT DRIVING PRIVILEGES FOR PURPOSES OF THIS CONTRACT. THOSE PERSON(S) HAVING ACCESS MUST SUCCESSFULLY COMPLETE SECURITY TRAINING AND PROVIDE PROPER DOCUMENTATION AS REQUIRED BY THE AIRPORT. PERSON(S) WITH ACCESS PRIVILEGES MUST SUCCESSFULLY COMPLETE THE NECESSARY SECURITY TRAINING IN ORDER THAT THEY CAN ESCORT ADDITIONAL WORKERS LIMITED TO HAVING ONLY CONTROLLED ACCESS PRIVILEGES.
p. THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) REQUIRED A TEMPORARY MODIFICATION TO THE AIRPORT SECURITY PLAN, KNOWN AS A CHANGE OF CONDITION PLAN (CCP) AND THE CONTRACTOR AND MAJOR SUBCONTRACTORS WILL BE REQUIRED TO SIGN THE PLAN INDICATING THEIR COMPLIANCE WITH THE SECURITY REQUIREMENTS SPECIFIED IN THE CCP. A MANDATORY SAFETY BRIEFING WILL BE HELD AT THE PUBLIC SAFETY FACILITY PRIOR TO THE START OF THE PROJECT.



License No. 184-000613

CONSULTANTS

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

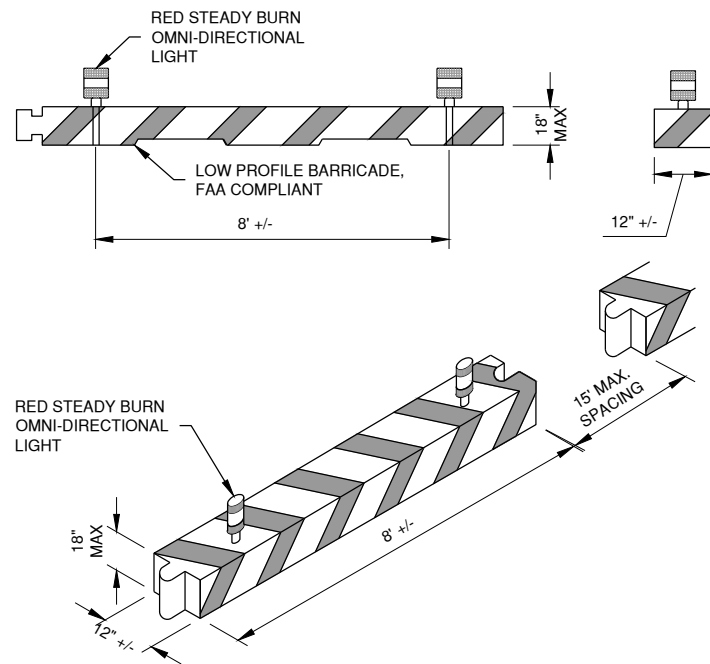
OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

Table with columns: MARK, DATE, DESCRIPTION. Includes project details like AIP PROJ. NO: 3-17-0096-XX, IL PROJ. NO: SPI-4943, CMT PROJECT NO: 180035-05, CAD DWG FILE: 18003505-PH3-GC000.DWG, DESIGNED BY: HWI, DRAWN BY: DPA, CHECKED BY: MJD, APPROVED BY: RLV.

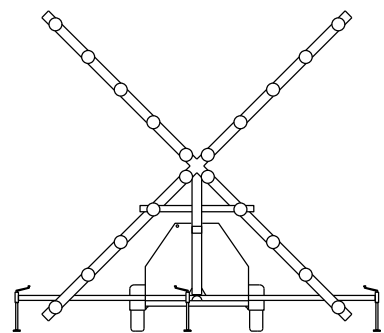
SHEET TITLE
CONSTRUCTION ACTIVITY PLAN NOTES 2



1 BEAM BARRICADE DETAIL
N.T.S.

BEAM BARRICADE NOTES

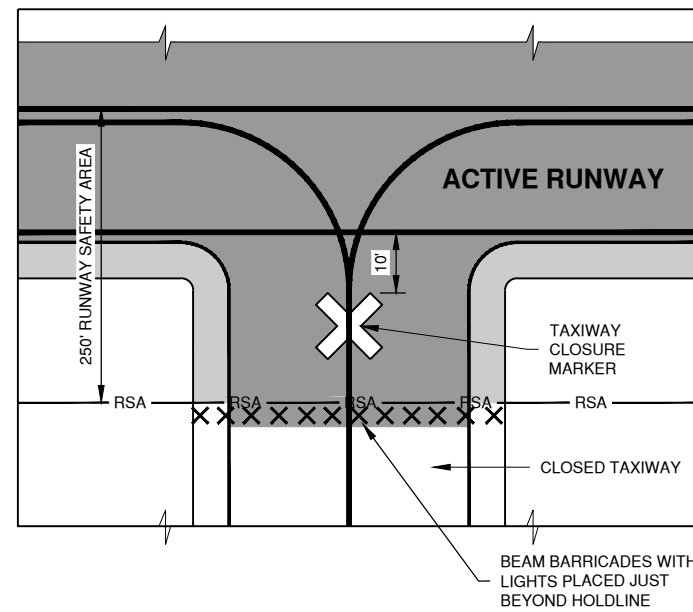
- BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY WIND, JET OR PROP BLAST.
- BARRICADE MUST BE OF LOW MASS AND EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- PLACE AS SHOWN IN PLANS AND AS DIRECTED BY THE ENGINEER.
- BARRICADES SHALL BE COMPLIANT WITH FAA AC 150/5370-2 (LATEST VERSION).



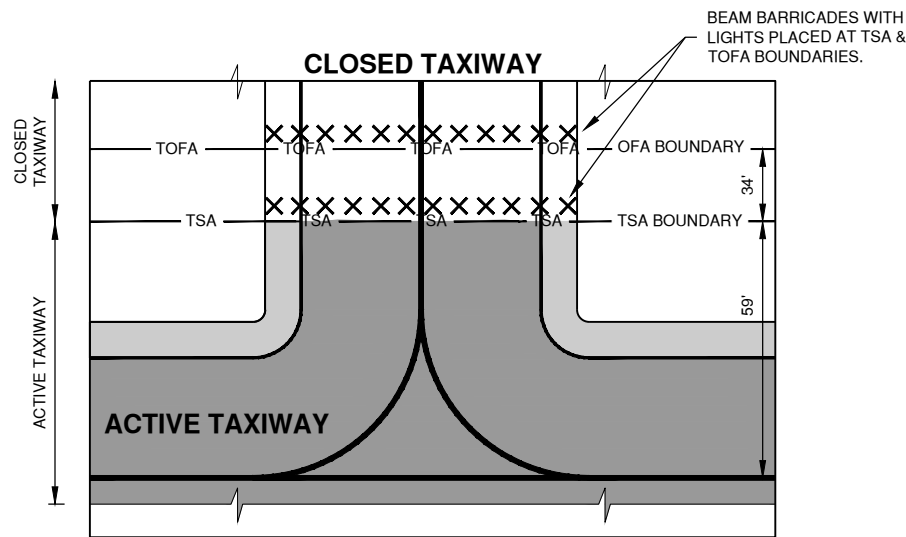
2 LIGHTED RUNWAY CLOSURE MARKER
N.T.S.

LIGHTED RUNWAY CLOSURE MARKER NOTES

- TO BE PLACED ON PAVEMENT AT THE RUNWAY NUMERALS FOR NIGHTTIME CLOSURE.
- THE CONTRACTOR SHALL PROVIDE TWO CLOSURE MARKERS (2 PAIR) AND MAINTAIN THEM (FUEL, OIL, LIGHT BULBS) WHEN USED DURING CONSTRUCTION CLOSURES.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS ASSOCIATED WITH PROVIDING AND MAINTAINING THIS ITEM SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.



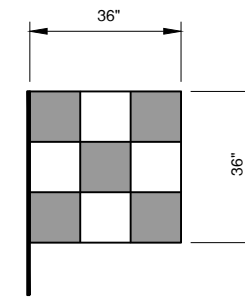
3 CLOSED TAXIWAY/ACTIVE RUNWAY BARRICADE DETAIL
N.T.S.



4 CLOSED TAXIWAY/ACTIVE TAXIWAY BARRICADE DETAIL
N.T.S.

NIGHT WORK RUNWAY CLOSURE NOTES

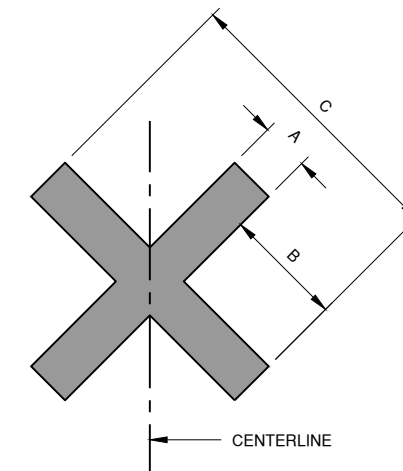
- WORK WITHIN THE RUNWAY SAFETY AREA MAY NOT BEGIN UNTIL THE RUNWAY HAS BEEN CLOSED, NAVAIDS DISABLED, EDGE LIGHTING, CENTERLINE LIGHTING, AND TDZ LIGHTING DISABLED IN THE VAULT AND THE LIGHTED CLOSURE MARKERS ARE IN PLACE.
- UPON COMPLETION OF WORK IN NIGHTTIME OFF-PEAK CLOSURE, NO EXCAVATIONS OR STOCKPILES SHALL EXIST IN EXCESS OF 3" AND ALL GRADES SHALL MEET FAA REQUIREMENTS. BARRICADES AND CLOSURE MARKERS SHALL BE REMOVED. AIRPORT OPERATIONS WILL INSPECT RUNWAY AT 4:30 AM.
- SEE SECTION 80 OF THE SPECIFICATIONS FOR INFORMATION REGARDING LIQUIDATED DAMAGES AND REOPENING THE RUNWAY.
- PRIOR TO REOPENING THE RUNWAY, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE DEBRIS WITHIN THE RUNWAY SAFETY AREA.



5 EQUIPMENT & VEHICLE SIGNAL FLAG
N.T.S. (ORANGE / WHITE)

SIGNAL FLAG NOTES

- ALL CONTRACTOR VEHICLES AND EQUIPMENT SHALL DISPLAY COMPANY LOGO PLACARDS AND FLAG.
- WHEN WORKING PRIOR TO DAWN OR AFTER DUSK, A 360 DEGREE ROTATING AMBER BEACON IS REQUIRED ON ALL EQUIPMENT AND TRUCKS.
- CONTRACTOR SHALL REPLACE FLAGS THAT ARE WORN AND INEFFECTIVE.



DIMENSION		A	B	C
SYMBOL TYPE				
CLOSED TAXIWAY		5'-0"	12'-6"	30'-0"
CLOSED RUNWAY		10'-0"	25'-0"	60'-0"

6 NON-LIGHTED CLOSURE MARKER
N.T.S.

NOTES

- CLOSURE MARKERS SHALL BE SOLID YELLOW.
- MARKERS SHALL BE PLACED ON TAXIWAYS AT THE RUNWAY INTERSECTIONS INSIDE THE RUNWAY SAFETY AREA.
- MARKERS SHALL BE PLACED ON RUNWAYS TO COVER THE NUMERALS ON BOTH ENDS.
- MARKERS MAY BE CONSTRUCTED OF FABRIC, COLORED PLASTIC, PAINTED SHEETS OF PLYWOOD OR SIMILAR MATERIALS.
- MARKERS SHALL BE SECURED TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS. METHODS OF SECURING THE MARKERS SHALL NOT PROTRUDE MORE THAN 3" ABOVE THE PAVEMENT.



License No. 184-000613
CONSULTANTS

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



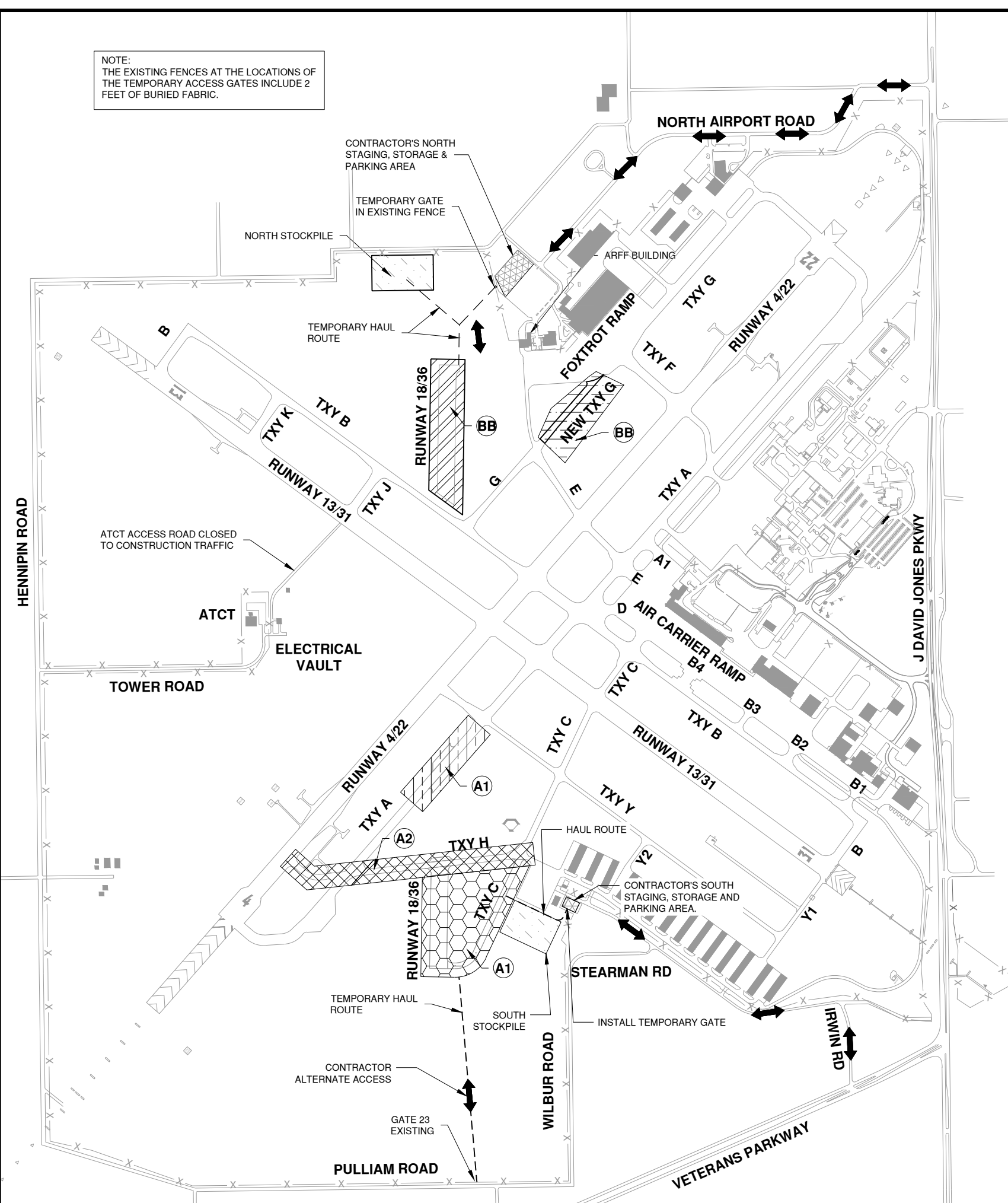
SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-GC000.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
COPYRIGHT:	

SHEET TITLE
**CONSTRUCTION
ACTIVITY PLAN
DETAILS 1**

NOTE:
THE EXISTING FENCES AT THE LOCATIONS OF
THE TEMPORARY ACCESS GATES INCLUDE 2
FEET OF BURIED FABRIC.



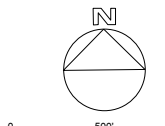
LEGEND

- CONTRACTOR'S STAGING, STORAGE AND PARKING AREA
 - PROPOSED STOCKPILE AREA
 - CONTRACTOR'S ACCESS
 - TEMPORARY HAUL ROUTE
- BASE BID**
- RUNWAY 18/36 NORTH REMOVAL
 - TAXIWAY G
- NOT SHOWN
RUNWAY WEATHER INFORMATION SYSTEM (RWIS)
REPLACE RUNWAY GUARD LIGHTS
LIGHTING CONTROL SYSTEM (ALCMS)
- ADD. ALT. #1**
- TAXIWAY A
 - RUNWAY 18/36 SOUTH REMOVAL & TAXIWAY C
 - TAXIWAY H LIGHTING AND SIGNAGE



License No. 184-000613

CONSULTANTS



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

**RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3**

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

GENERAL NOTES

1. THE CONTRACTOR SHALL PROVIDE 72 HOURS NOTICE PRIOR TO INITIATING WORK IN ANY PHASE.
2. THE CONTRACTOR SHALL PLACE ALL RUNWAY CLOSURE MARKERS, TAXIWAY CLOSURE MARKERS AND BARRICADES PRIOR TO BEGINNING WORK IN ANY PHASE.
3. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN ACTIVE PAVEMENTS DURING HAULING OPERATIONS AND AT THE END OF EACH WORKDAY.
4. AIRCRAFT & AIRPORT VEHICLES SHALL HAVE THE RIGHT OF WAY AT ALL TIMES.
5. THE CONTRACTOR SHALL MAINTAIN ACCESS BETWEEN THE ARFF BUILDING AND THE AIRFIELD AT ALL TIMES.

BIDDING NOTES

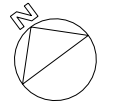
THE FOLLOWING DESCRIBES THE GENERAL WORK TO BE INCLUDED IN THE BASE BID AND ADDITIVE ALTERNATES.

- BASE BID**
- REMOVE, BACKFILL AND TURF RUNWAY 18/36 NORTH OF TAXIWAY B.
 - CONSTRUCT EMBANKMENT, PAVEMENT, DRAINAGE, LIGHTING & SIGNAGE FOR TAXIWAY G BETWEEN TAXIWAY E AND THE FOXTROT RAMP.
 - INSTALL LIGHTING CONTROL SYSTEM (ALCMS).
 - INSTALL RUNWAY WEATHER INFORMATION SYSTEM (RWIS).
 - REPLACE RUNWAY GUARD LIGHTS.
- ADDITIVE ALTERNATE 1**
- REMOVE AND BACKFILL REMAINING 18/36 PAVEMENT AT TAXIWAY A, INSTALL TAXIWAY A LIGHTING.
 - REMOVE, BACKFILL AND TURF RUNWAY 18/36 AND TAXIWAY C SOUTH OF TAXIWAY HOTEL AND REMAINING 18/36 PAVEMENT NORTH OF TAXIWAY H.
 - CONSTRUCT STORM SEWER AT TAXIWAY H.
 - RECONSTRUCT TAXIWAY H LIGHTING.

MARK	DATE	DESCRIPTION
AIP PROJ. NO:	3-17-0096-XX	CA022
IL PROJ. NO:	SPI-4943	
CMT PROJECT NO:	180035-05	
CAD DWG FILE:	18003505-PH3-GC100.DWG	
DESIGNED BY:	HWI	
DRAWN BY:	DPA	
CHECKED BY:	MJD	
APPROVED BY:	RLV	
COPYRIGHT:		

**CONST. ACTIVITY
PLAN OVERVIEW &
SITE ACCESS**

Path: K:\Springfield\180035-05_Phy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-GC100.dwg
Date: Tuesday, May 10, 2022 2:09:41 PM



0 400' 800'
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER

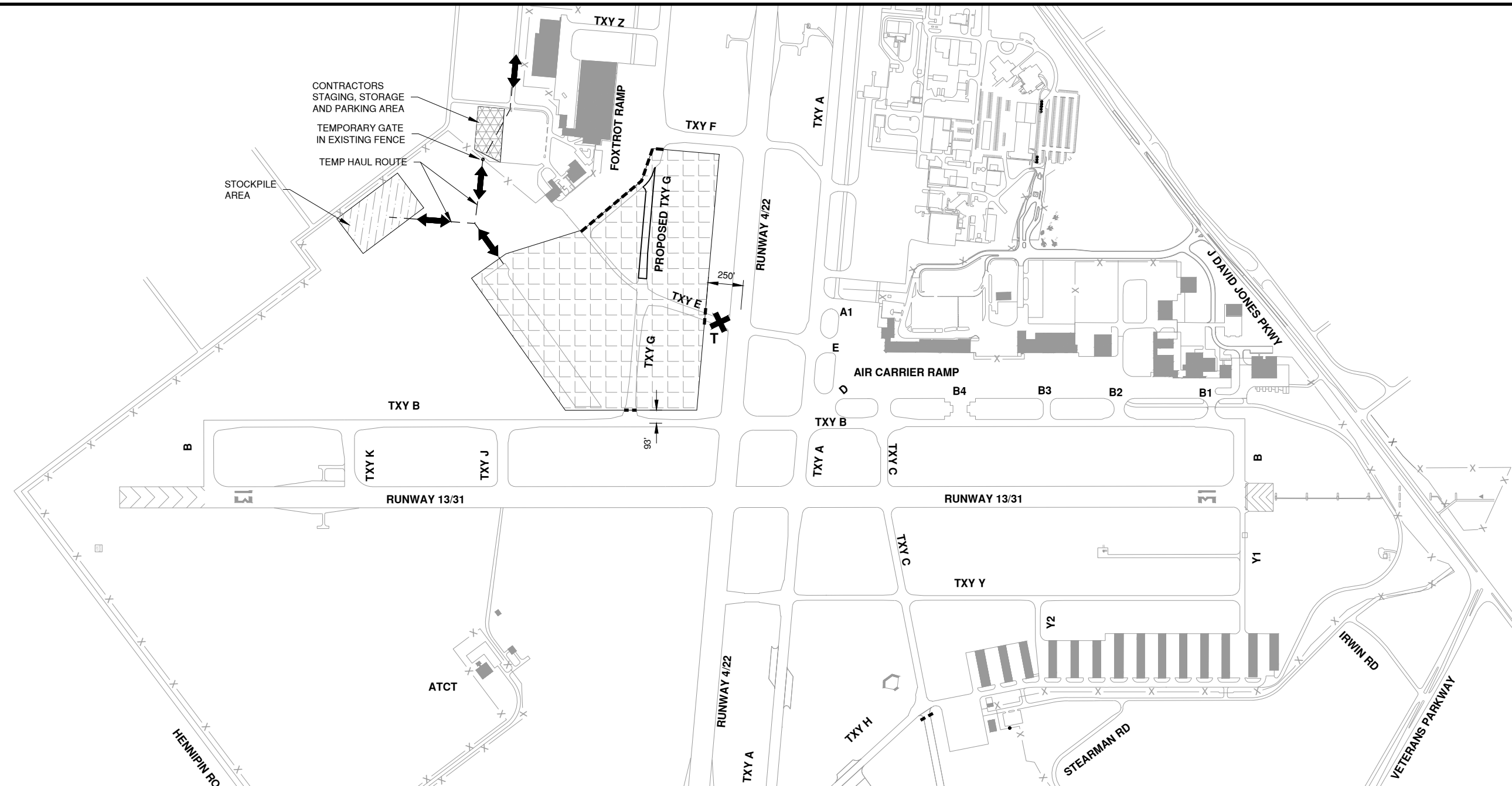


SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-GC101.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
COPYRIGHT:	

CONSTRUCTION ACTIVITY PLAN WORK AREA A



LEGEND

- WORK AREA A
- CONTRACTOR'S STAGING & STORAGE
- STOCKPILE
- BEAM BARRICADES
- LIGHTED RUNWAY CLOSURE MARKER
- TAXIWAY CLOSURE MARKER (TEMPORARY)
- CONTRACTOR'S ACCESS ROUTE

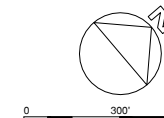
PAVEMENT STATUS TABLE

WORK AREA A	
PAVEMENT	STATUS
RUNWAY 18/36	DECOMMISSIONED
RUNWAY 4/22	OPEN
RUNWAY 13/31	OPEN
TAXIWAY A	OPEN
TAXIWAY B	OPEN
TAXIWAY C	CLOSED S OF TAXIWAY H
TAXIWAY E	CLOSED N OF RUNWAY 4/22
TAXIWAY F	OPEN
EXISTING TAXIWAY G	CLOSED W OF TAXIWAY F
NEW TAXIWAY G	CLOSED
TAXIWAY H	OPEN
TAXIWAY J	OPEN
TAXIWAY K	OPEN
TAXIWAY Y	OPEN

WORK AREA A NOTES

1. WORK AREA A MAY BE CONCURRENTLY ACTIVE WITH ANY OTHER WORK AREA.
2. WORK TO BE COMPLETED WITHIN WORK AREA A, SHALL, AS A MINIMUM, INCLUDE THE FOLLOWING MAJOR WORK ITEMS:
 - A. CONSTRUCT NEW TAXIWAY G BETWEEN TAXIWAY E AND THE FOXTROT RAMP.
 - B. STORM SEWER IMPROVEMENTS.
 - C. INSTALL NEW EDGE LIGHTS AND SIGNS AS INDICATED ON THE PLANS.
 - D. EXTEND THE NEW HOMERUN AND CONNECT THE EXISTING TAXIWAY G EDGE LIGHTS.

Path: K:\Springfield\0180035-05_Pkwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-GC101.dwg
Date: Tuesday, April 26, 2022 12:38:42 PM



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

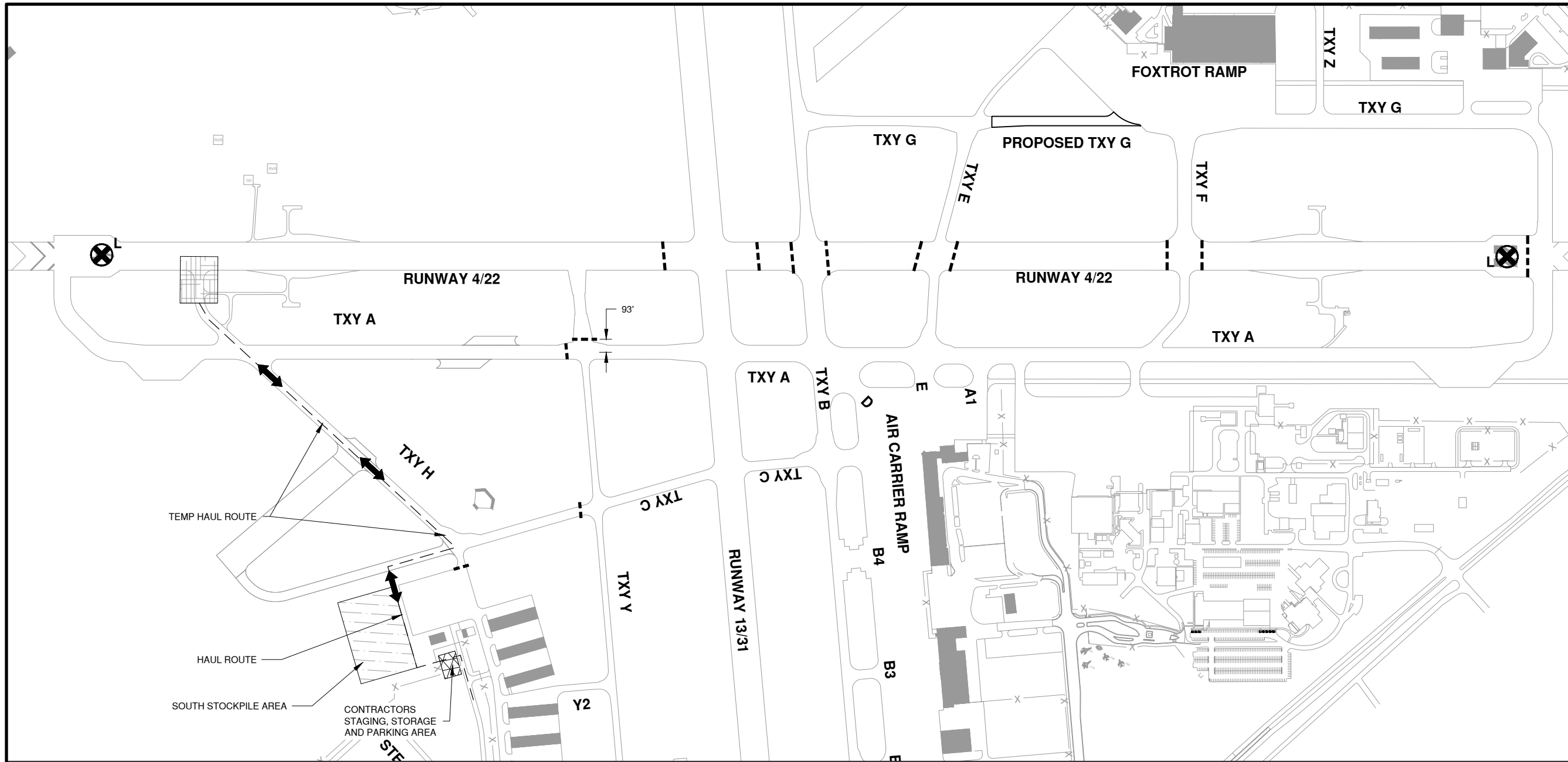
BID SUBMITTAL
APRIL 22, 2022

**RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3**

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL



WORK AREA B1

LEGEND

- WORK AREA B1
- CONTRACTOR'S STAGING & STORAGE
- STOCKPILE
- BEAM BARRICADES
- LIGHTED RUNWAY CLOSURE MARKER
- TAXIWAY CLOSURE MARKER (TEMPORARY)
- CONTRACTOR'S ACCESS ROUTE

PAVEMENT STATUS TABLE

WORK AREA B1	
PAVEMENT	STATUS
RUNWAY 18/36	DECOMMISSIONED
RUNWAY 4/22	CLOSED
RUNWAY 13/31	OPEN
TAXIWAY A	CLOSED S OF TXY Y
TAXIWAY S	OPEN
TAXIWAY C	CLOSED S OF TXY Y
TAXIWAY E	OPEN
TAXIWAY F	OPEN
EXISTING TAXIWAY G	OPEN
NEW TAXIWAY G	CLOSED
TAXIWAY H	CLOSED
TAXIWAY J	OPEN
TAXIWAY X	OPEN
TAXIWAY Y	CLOSED W OF TXY A

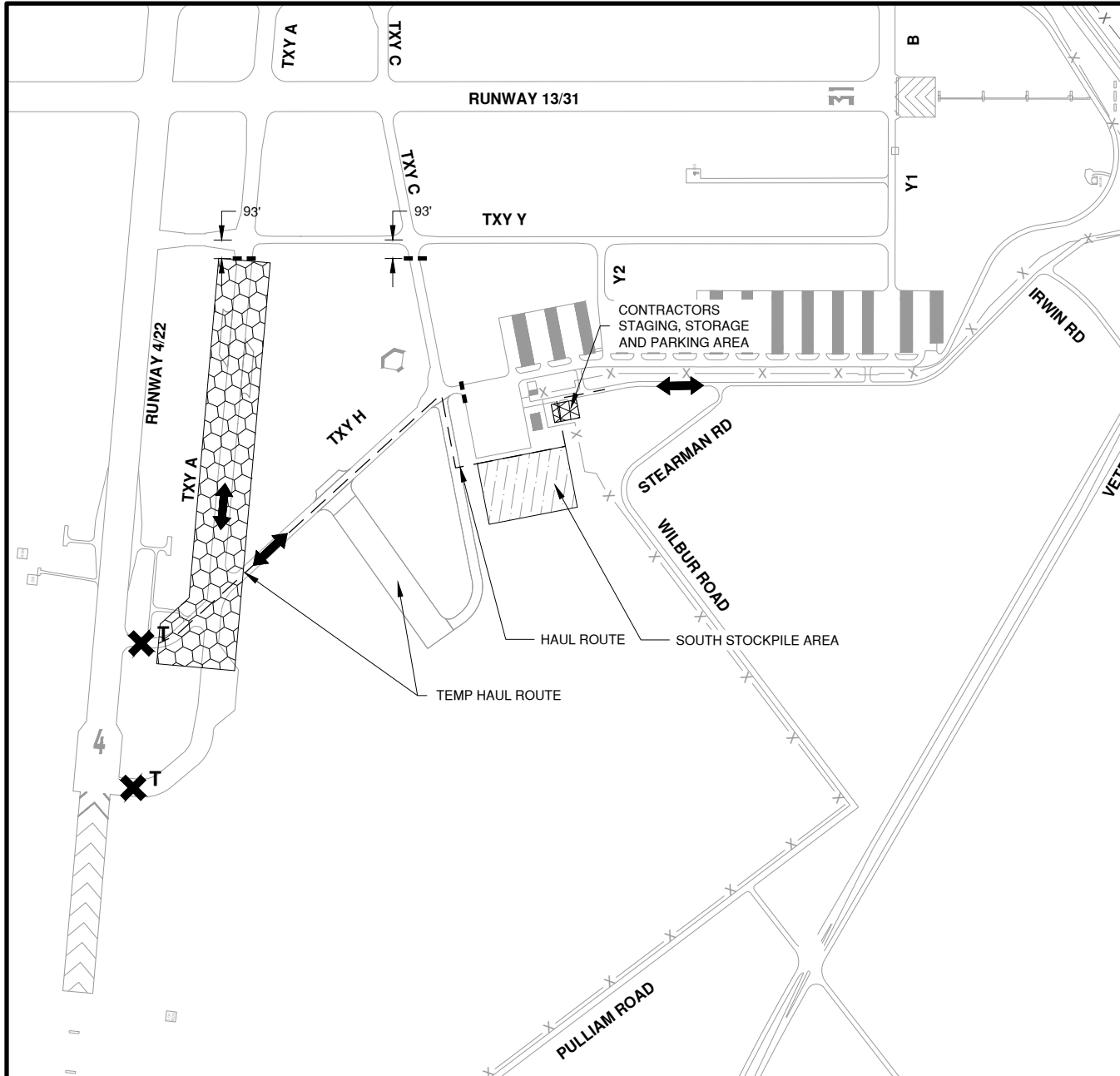
WORK AREA NOTES

1. WORK AREA B1 MAY BE ACTIVE CONCURRENTLY WITH WORK AREA B2.
2. RUNWAY 4/22 MAY BE CLOSED FOR A MAXIMUM OF 5 CALENDAR DAYS TO COMPLETE THE WORK IN THIS WORK AREA.
3. WORK TO BE COMPLETED WITHIN WORK AREA B1 SHALL, AS A MINIMUM, INCLUDE THE FOLLOWING MAJOR WORK ITEMS:
 - A. INSTALL NEW LIGHTS AND SIGNS.

Path: K:\Springfield\18035-05_Rwy_18-36\Draw\Sheets\PHASE 3\1803505-PH3-GC101.dwg
 Date: Tuesday, April 26, 2022 12:38:44 PM

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-GC101.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
COPYRIGHT:	
SHEET TITLE	
CONSTRUCTION ACTIVITY PLAN WORK AREA B1	
SHEET 9	OF 91



License No. 184-000613

CONSULTANTS



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

WORK AREA B2

LEGEND

- WORK AREA B2 - BASE BID
- CONTRACTOR'S STAGING & STORAGE
- STOCKPILE
- BEAM BARRICADES
- LIGHTED RUNWAY CLOSURE MARKER
- TAXIWAY CLOSURE MARKER (TEMPORARY)
- CONTRACTOR'S ACCESS ROUTE

PAVEMENT STATUS TABLE

WORK AREA B2	
PAVEMENT	STATUS
RUNWAY 18/36	DECOMMISSIONED
RUNWAY 4/22	OPEN
RUNWAY 13/31	OPEN
TAXIWAY A	CLOSED S OF TXY Y
TAXIWAY B	OPEN
TAXIWAY C	CLOSED S OF TXY Y
TAXIWAY E	OPEN
TAXIWAY F	OPEN
EXISTING TAXIWAY G	OPEN
NEW TAXIWAY G	CLOSED
TAXIWAY H	CLOSED
TAXIWAY J	OPEN
TAXIWAY K	OPEN
TAXIWAY Y	OPEN

WORK AREA NOTES

1. WORK AREAS B2 AND B3 MAY NOT BE CONCURRENTLY ACTIVE.
2. WORK TO BE COMPLETED WITHIN WORK AREA B2 & B3 SHALL, AS A MINIMUM, INCLUDE THE FOLLOWING MAJOR WORK ITEMS:
 - A. REMOVE REMAINING RWY 18/36 PAVEMENT AT TXY A.
 - B. INSTALL LIGHTS & SIGNS.

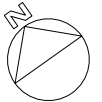
Path: K:\Springfield\0180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-GC101.dwg
Date: Tuesday, April 26, 2022 12:38:46 PM

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX **CA022**
 IL PROJ. NO: SPI-4943
 CMT PROJECT NO: 180035-05
 CAD DWG FILE: 18003505-PH3-GC101.DWG
 DESIGNED BY: HWI
 DRAWN BY: DPA
 CHECKED BY: MJD
 APPROVED BY: RLV
 COPYRIGHT:

SHEET TITLE
**CONSTRUCTION
 ACTIVITY PLAN WORK
 AREA B2**

GC103
 SHEET 10 OF 91



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

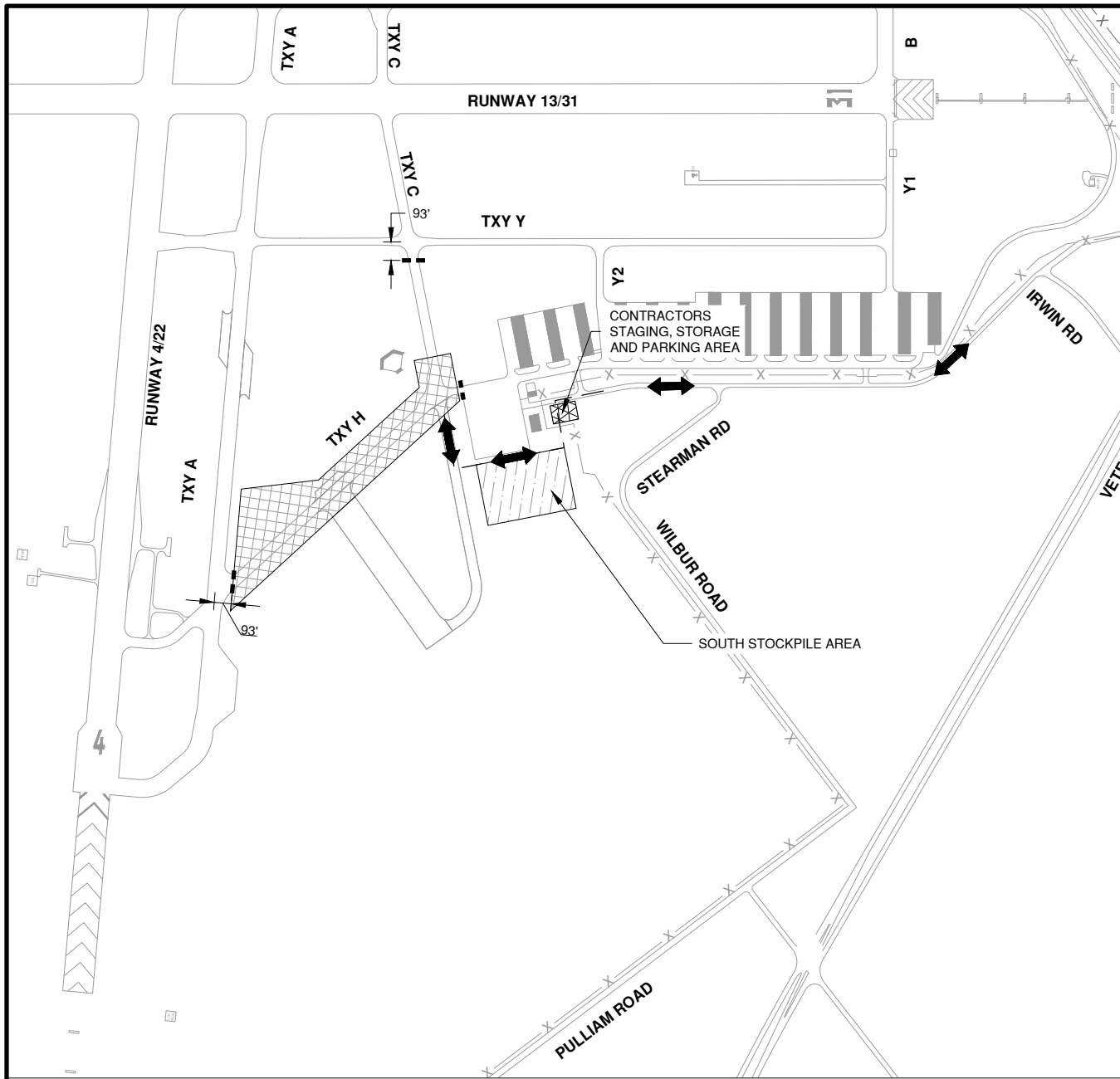
BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

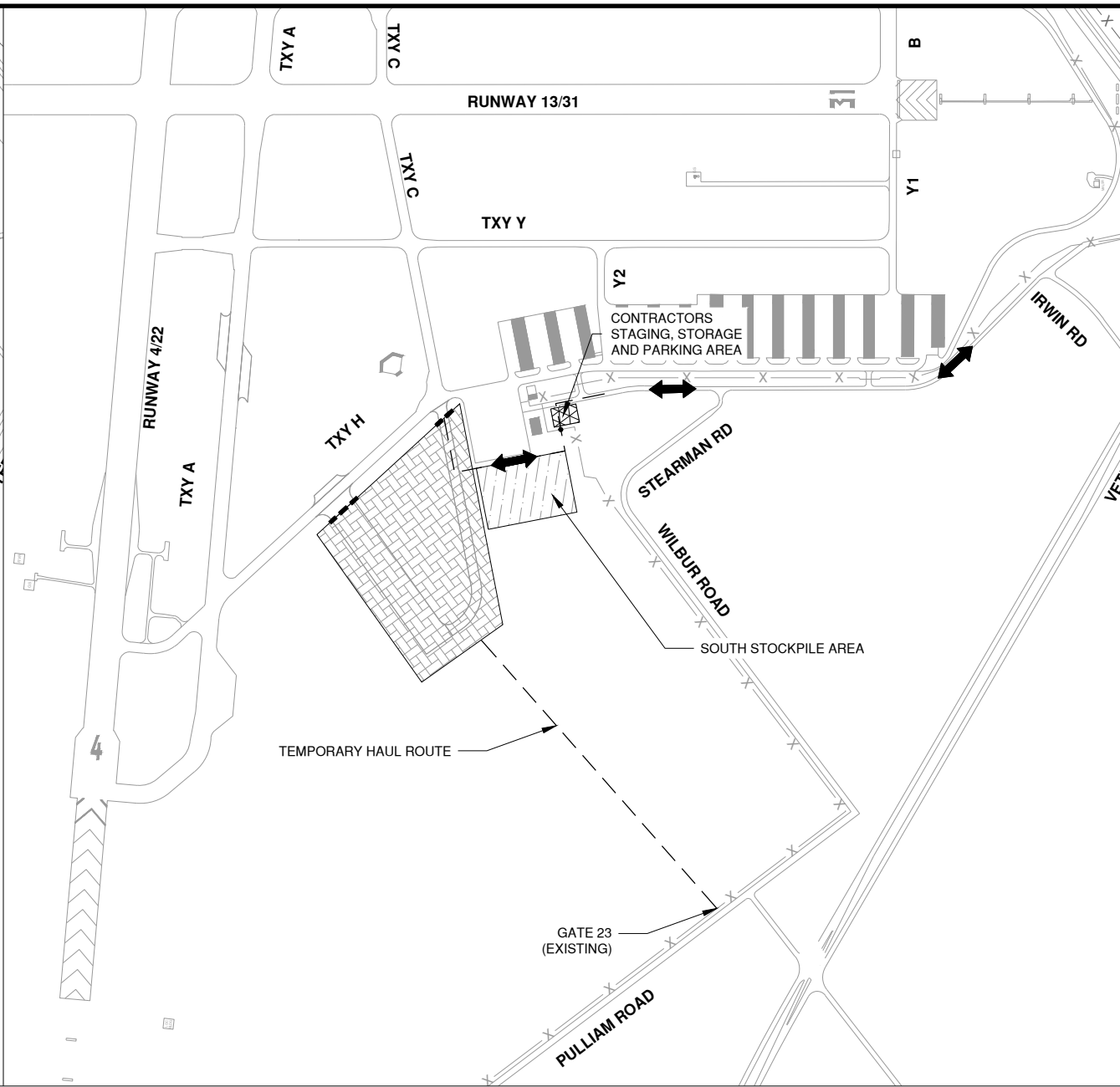
OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL



WORK AREA B3



WORK AREA B4

LEGEND

- WORK AREA B3
- WORK AREA B4
- CONTRACTOR'S STAGING & STORAGE
- STOCKPILE
- BEAM BARRICADES
- LIGHTED RUNWAY CLOSURE MARKER
- TAXIWAY CLOSURE MARKER (TEMPORARY)
- CONTRACTOR'S ACCESS ROUTE

PAVEMENT STATUS TABLE

WORK AREA B3	
PAVEMENT	STATUS
RUNWAY 18/36	DECOMMISSIONED
RUNWAY 4/22	OPEN
RUNWAY 13/31	OPEN
TAXIWAY A	OPEN
TAXIWAY B	OPEN
TAXIWAY C	CLOSED S OF TXY Y
TAXIWAY E	OPEN
TAXIWAY F	OPEN
EXISTING TAXIWAY G	OPEN
NEW TAXIWAY G	CLOSED
TAXIWAY H	CLOSED BETWEEN TXY A & TXY C
TAXIWAY I	OPEN
TAXIWAY K	OPEN
TAXIWAY Y	OPEN

WORK AREA B4	
PAVEMENT	STATUS
RUNWAY 18/36	DECOMMISSIONED
RUNWAY 4/22	OPEN
RUNWAY 13/31	OPEN
TAXIWAY A	OPEN
TAXIWAY B	OPEN
TAXIWAY C	CLOSED S OF TXY Y
TAXIWAY E	OPEN
TAXIWAY F	OPEN
EXISTING TAXIWAY G	OPEN
NEW TAXIWAY G	CLOSED
TAXIWAY H	OPEN
TAXIWAY J	OPEN
TAXIWAY K	OPEN
TAXIWAY Y	OPEN

WORK AREA NOTES

1. WORK AREAS B3 & B4 MAY BE CONCURRENTLY ACTIVE.
2. WORK TO BE COMPLETED WITHIN WORK AREA B3 & B4 SHALL, AS A MINIMUM, INCLUDE THE FOLLOWING MAJOR WORK ITEMS:
 - A. INSTALL NEW LIGHTS AND SIGNS.
 - B. REMOVE AND BACKFILL RUNWAY 18/36 AND TAXIWAY C PAVEMENTS.
 - C. CONSTRUCT NEW STORM SEWER.

Path: K:\Springfield\18035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-GC101.dwg
Date: Tuesday, April 26, 2022 12:38:48 PM

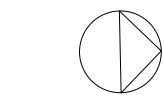
MARK	DATE	DESCRIPTION

SHEET TITLE

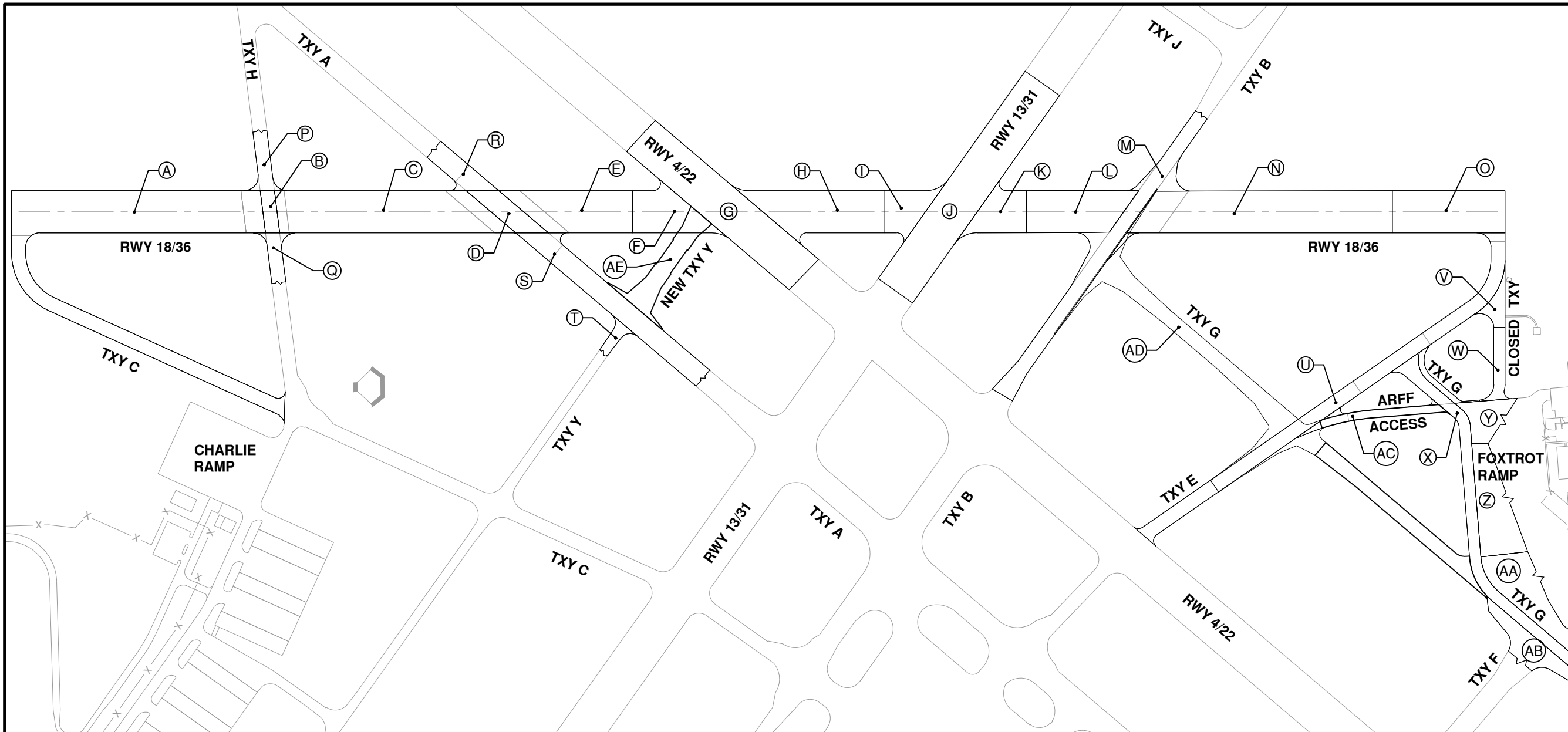
**CONSTRUCTION
ACTIVITY PLAN WORK
AREAS B3 & B4**

GC104

SHEET 11 OF 91



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).


 BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER


 SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

PAVEMENT LEGEND

ID	THICK	MATERIAL	ITEM
A	5"	HMA	401/201
	8.5"	CRACK AND SEAT PCC	501
	7"	GRANULAR BLANKET	154
B	5"	HMA	401/201
	15"	CRACK AND SEAT PCC	501
	6"	AGG BASE	208
	7"	GRANULAR BLANKET	154
C		REMOVED IN PHASE 1	
D	11"	PCC	501
	2.5"	HMA	501
	18.5"	PCC	501
	7"	GRANULAR BLANKET	154
E		REMOVED IN PHASE 1	
F		REMOVED IN PHASE 1	

ID	THICK	MATERIAL	ITEM
G	8"	PCC	501
	1"	HMA	401
	15"	PCC	501
	6"	AGG BASE	208
	7"	GRANULAR BLANKET	154
H		REMOVED IN PHASE 1	
I		REMOVED IN PHASE 1	
J	11"	HMA	401/201
	8"	CRACK AND SEAT PCC	501
	7"	GRANULAR BLANKET	154
K		REMOVED IN PHASE 1	
L		REMOVED IN PHASE 1	

ID	THICK	MATERIAL	ITEM
M	18"	HMA	401/201
	4"	AGG BASE	209
N	5"	HMA	401
	8"	CRACK AND SEAT PCC	501
	7"	GRANULAR BLANKET	154
O	6"	PCC	501
	1"	HMA	201
P	9"	CRACK AND SEAT PCC	501
	7"	GRANULAR BLANKET	154
Q	18.5"	PCC	501
	7"	GRANULAR BLANKET	154
R	16"	PCC	501
	4"	AGG BASE	209
S	8"	LIME MODIFIED SUBGRA	155
	16"	PCC	501
	4"	AGG BASE	209
	8"	LIME MODIFIED SUBGRA	155

SITE NOTES

PAVEMENT STRUCTURES SHOWN WERE TAKEN FROM RECORD DRAWINGS AND ARE CONSIDERED GENERALLY REPRESENTATIVE OF THE "AS CONSTRUCTED" PAVEMENT SECTION WITH SOME VARIABILITY FROM THE THICKNESS INDICATED TO BE EXPECTED. THERE WILL BE NO ADDITIONAL PAYMENT TO THE CONTRACTOR DUE TO VARIATIONS IN SIZE, THICKNESS, OR QUANTITY OF EXISTING FEATURES.

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX CA022

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-CD001.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: MJD

APPROVED BY: RLV

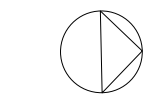
COPYRIGHT:

SHEET TITLE

EXISTING PAVEMENT STRUCTURES

CD001

SHEET 12 OF 91



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

 BID SUBMITTAL
APRIL 22, 2022

**RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3**

OWNER


 SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
----------------------------	--------------

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-CD002.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: MJD

APPROVED BY: RLV

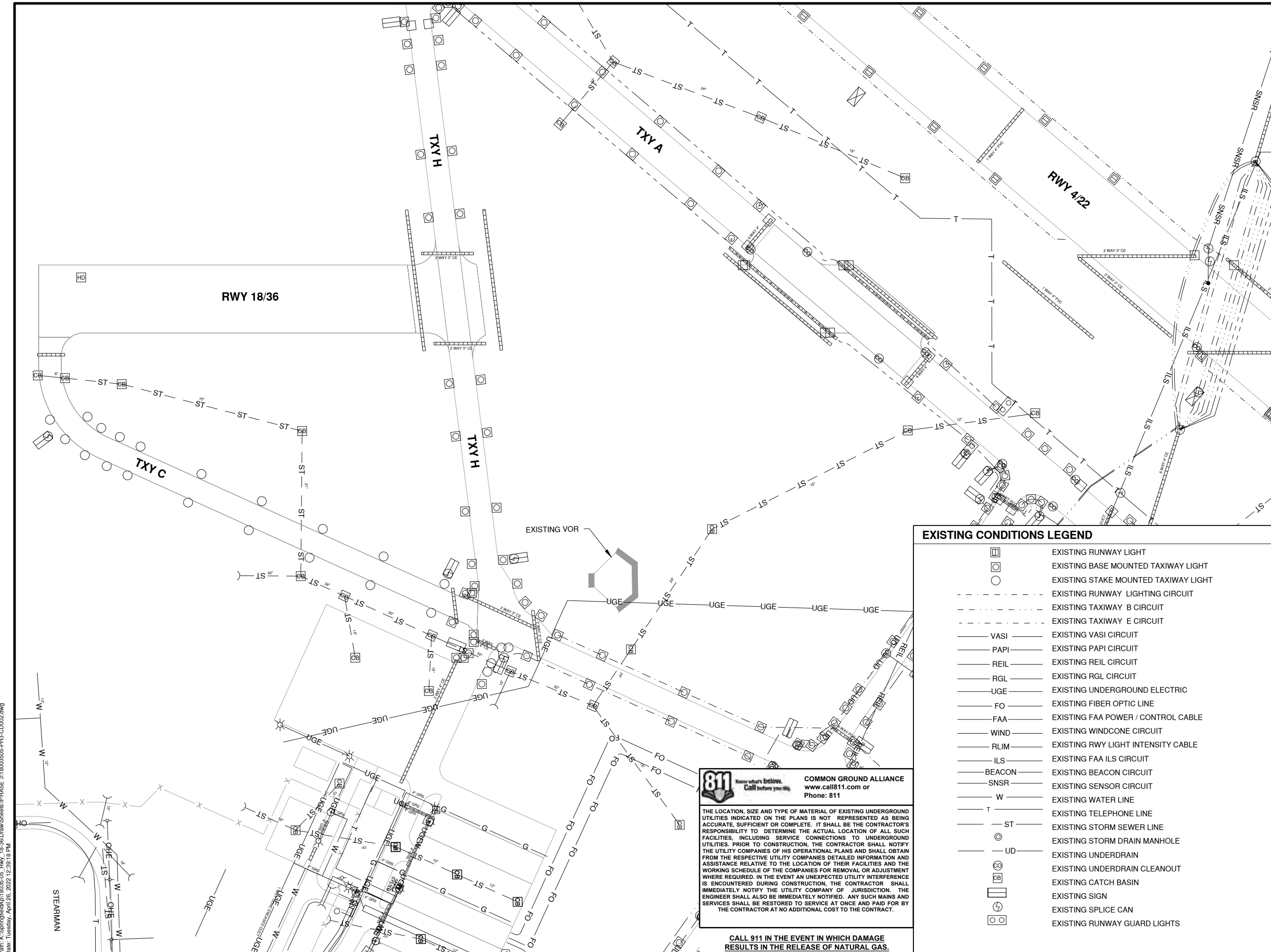
COPYRIGHT:

SHEET TITLE

EXISTING UTILITIES 2

CD003

SHEET 14 OF 91


EXISTING CONDITIONS LEGEND

- EXISTING RUNWAY LIGHT
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING RUNWAY LIGHTING CIRCUIT
- EXISTING TAXIWAY B CIRCUIT
- EXISTING TAXIWAY E CIRCUIT
- VASI EXISTING VASI CIRCUIT
- PAPI EXISTING PAPI CIRCUIT
- REIL EXISTING REIL CIRCUIT
- RGL EXISTING RGL CIRCUIT
- UGE EXISTING UNDERGROUND ELECTRIC
- FO EXISTING FIBER OPTIC LINE
- FAA EXISTING FAA POWER / CONTROL CABLE
- WIND EXISTING WINDCONE CIRCUIT
- RLIM EXISTING RWY LIGHT INTENSITY CABLE
- ILS EXISTING FAA ILS CIRCUIT
- BEACON EXISTING BEACON CIRCUIT
- SNSR EXISTING SENSOR CIRCUIT
- W EXISTING WATER LINE
- T EXISTING TELEPHONE LINE
- ST EXISTING STORM SEWER LINE
- EXISTING STORM DRAIN MANHOLE
- UD EXISTING UNDERDRAIN
- EXISTING UNDERDRAIN CLEANOUT
- EXISTING CATCH BASIN
- EXISTING SIGN
- EXISTING SPLICE CAN
- EXISTING RUNWAY GUARD LIGHTS

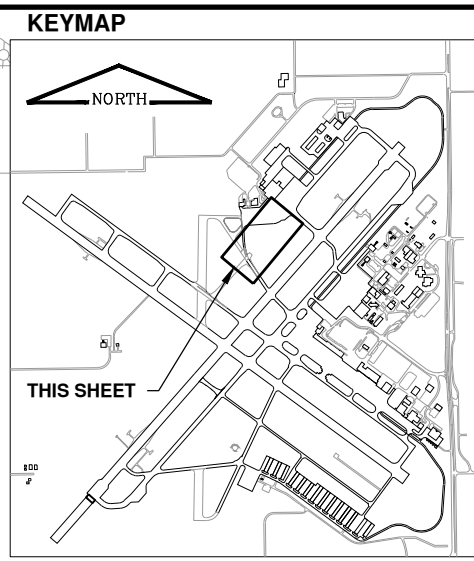
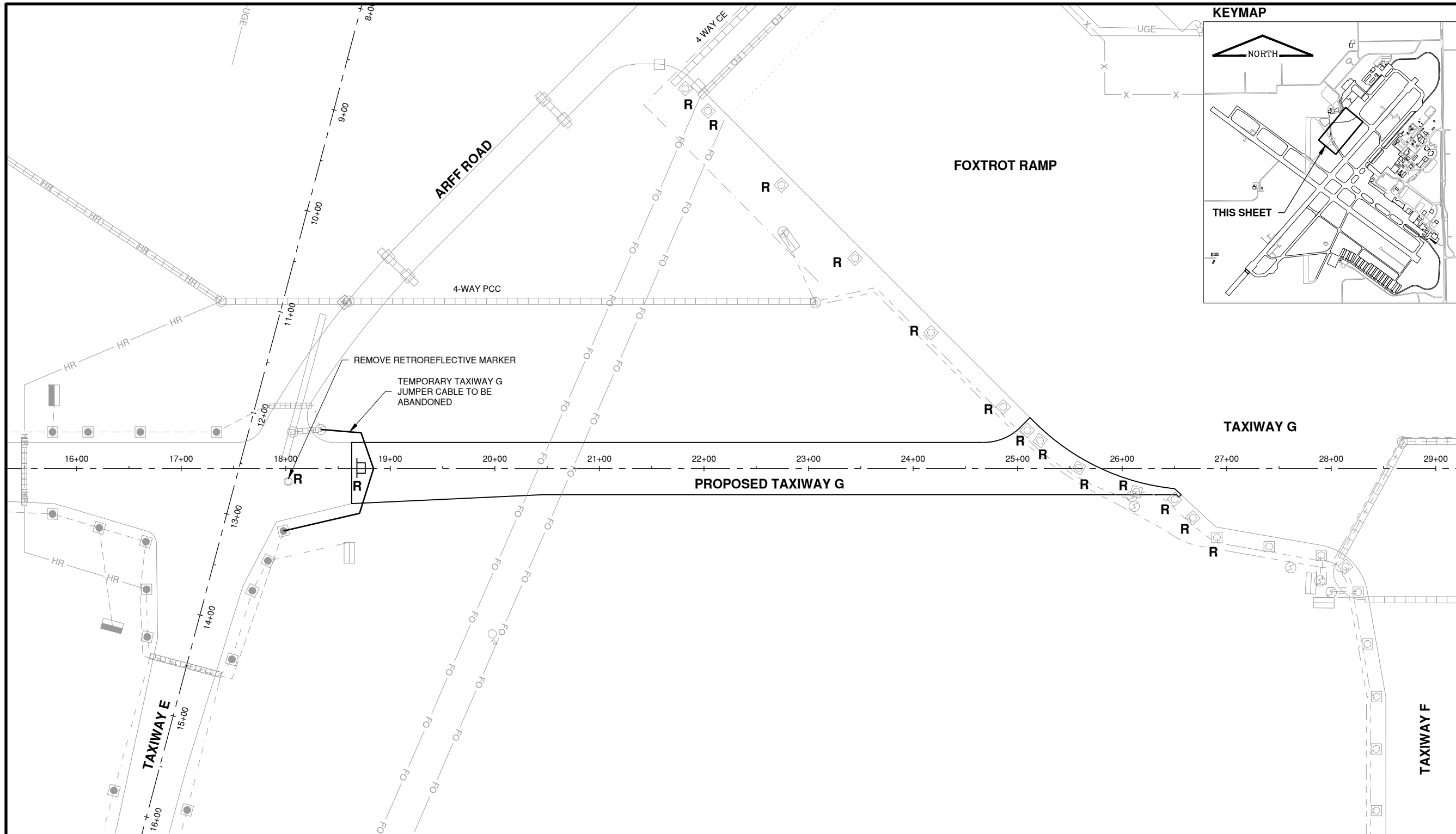
811 Know what's below. Call before you dig. **COMMON GROUND ALLIANCE**
www.call811.com or Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

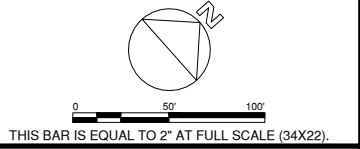
CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

 Path: K:\Springfield\0180035-05_Rwy_18-36\Draw\Sheets\PH3-CD002.dwg
Date: Tuesday, April 26, 2022 12:39:18 PM

Path: K:\Springfield\18035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CD100.dwg
 Date: Tuesday, April 26, 2022 12:39:37 PM



License No. 184-000613
CONSULTANTS



BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



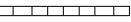
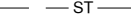






SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL













NOTES

1. REMOVED PAVEMENT SHALL BE DISPOSED OF OFFSITE AT NO ADDITIONAL COST TO THE CONTRACT.
2. CONTRACTOR SHALL VERIFY THAT ALL DUCTS ARE ABANDONED PRIOR TO REMOVALS.
3. CONTRACTOR SHALL PROTECT EXISTING PAVEMENT EDGES WHICH TO REMAIN IN PLACE. ADDITIONAL SAWING MAY BE REQUIRED TO PROTECT THESE EDGES.
4. REMOVED LIGHT FIXTURES AND SIGNS SHALL BE GIVEN TO THE AIRPORT. REMOVED CANS SHALL BE DISPOSED OF AT NO ADDITIONAL COST TO THE CONTRACT.

LEGEND

-  REM. PVMT STRUCTURE (HMA / PCC)
-  FULL DEPTH SAWCUT
-  EXIST. DUCT BANK
-  EXIST. STORM SEWER
-  TO BE REMOVED
-  RETRO-REFLECTIVE MARKERS
-  SPLICE CAN
-  EXIST FIBER-OPTIC CABLE

M

-  MODIFY SIGN PANEL
-  EXIST. RUNWAY THRESHOLD STAKE-MOUNTED LIGHT
-  EXIST. RUNWAY BASE-MOUNTED LIGHT
-  EXIST. RUNWAY STAKE-MOUNTED LIGHT
-  EXIST. TAXIWAY BASE-MOUNTED LIGHT
-  EXIST. TAXIWAY STAKE-MOUNTED LIGHT
-  EXIST. RUNWAY GUARD LIGHT
-  EXIST. SIGN
-  EXIST. DISTANCE REMAINING SIGN
-  EXIST. INLET / CATCH BASIN
-  EXIST. CLEANOUT
-  EXIST. DUCT MARKER

UTILITY DISCLAIMER

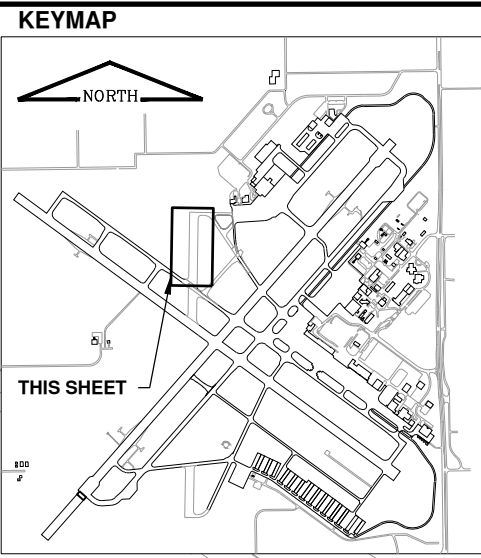
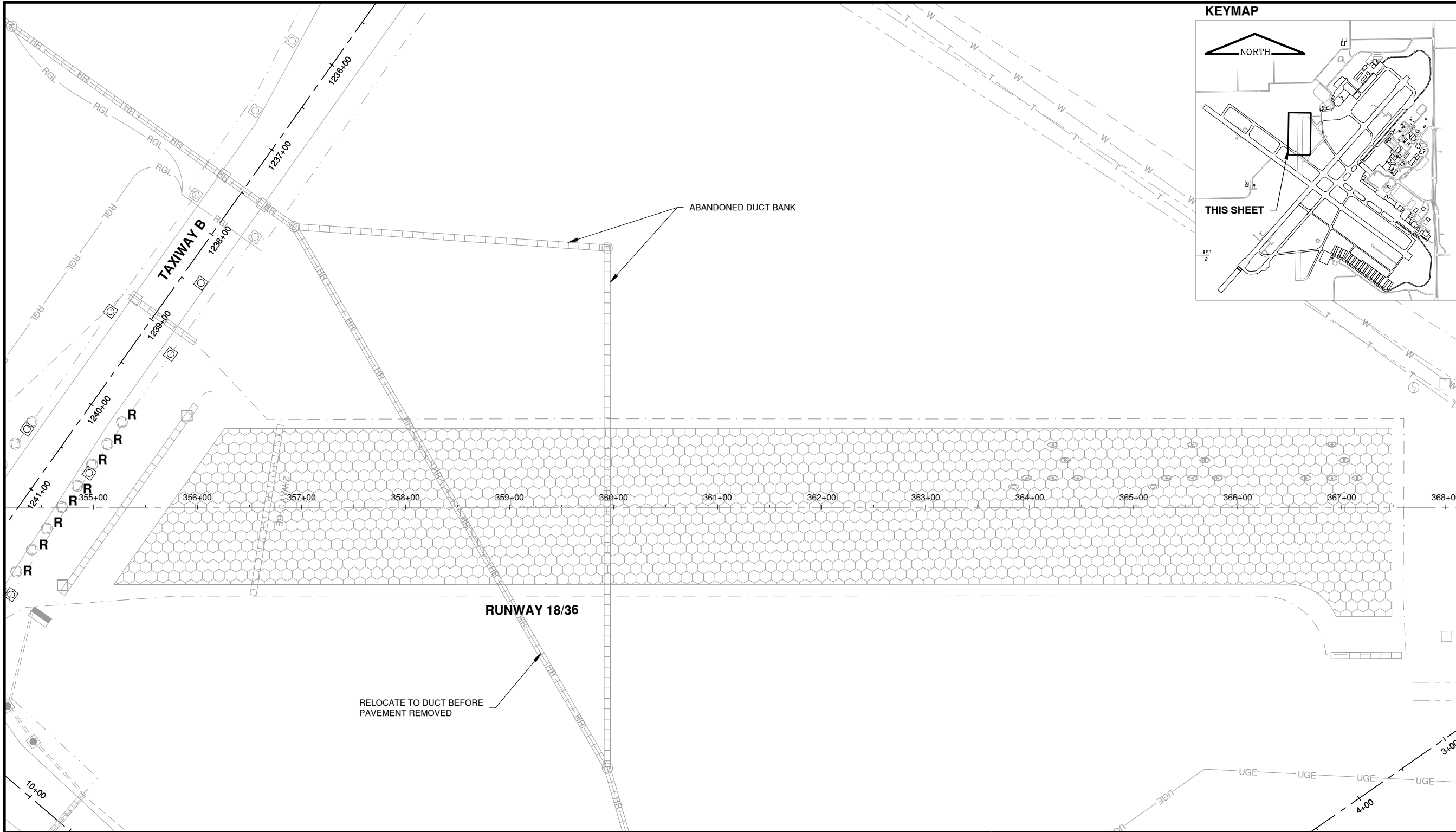
THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY AND FAA OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

MARK	DATE	DESCRIPTION

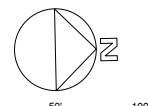
AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CD100.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
COPYRIGHT:	

EX. CONDITIONS & REMOVALS TXY G EAST



License No. 184-000613

CONSULTANTS



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

NOTES

1. REMOVED PAVEMENT SHALL BE DISPOSED OF OFFSITE AT NO ADDITIONAL COST TO THE CONTRACT.
2. CONTRACTOR SHALL VERIFY THAT ALL DUCTS ARE ABANDONED PRIOR TO REMOVALS.
3. CONTRACTOR SHALL PROTECT EXISTING PAVEMENT EDGES WHICH TO REMAIN IN PLACE. ADDITIONAL SAWING MAY BE REQUIRED TO PROTECT THESE EDGES.
4. REMOVED LIGHT FIXTURES AND SIGNS SHALL BE GIVEN TO THE AIRPORT. REMOVED CANS SHALL BE DISPOSED OF AT NO ADDITIONAL COST TO THE CONTRACT.

LEGEND

- REM. PVMT STRUCTURE (HMA / PCC)
- FULL DEPTH SAWCUT
- EXIST. DUCT BANK
- EXIST. STORM SEWER
- TO BE REMOVED
- RETRO-REFLECTIVE MARKERS
- SPLICE CAN
- EXIST FIBER-OPTIC CABLE
- EXISTING PAVEMENT TIE DOWN

M

- MODIFY SIGN PANEL
- EXIST. RUNWAY THRESHOLD STAKE-MOUNTED LIGHT
- EXIST. RUNWAY BASE-MOUNTED LIGHT
- EXIST. RUNWAY STAKE-MOUNTED LIGHT
- EXIST. TAXIWAY BASE-MOUNTED LIGHT
- EXIST. TAXIWAY STAKE-MOUNTED LIGHT
- EXIST. RUNWAY GUARD LIGHT
- EXIST. SIGN
- EXIST. DISTANCE REMAINING SIGN
- EXIST. INLET / CATCH BASIN
- EXIST. CLEANOUT
- EXIST. DUCT MARKER

UTILITY DISCLAIMER

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

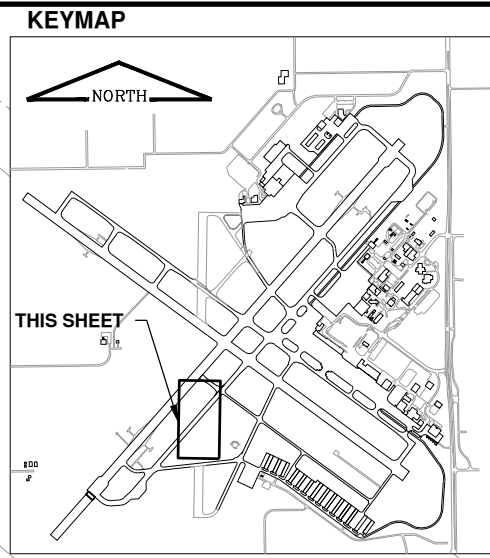
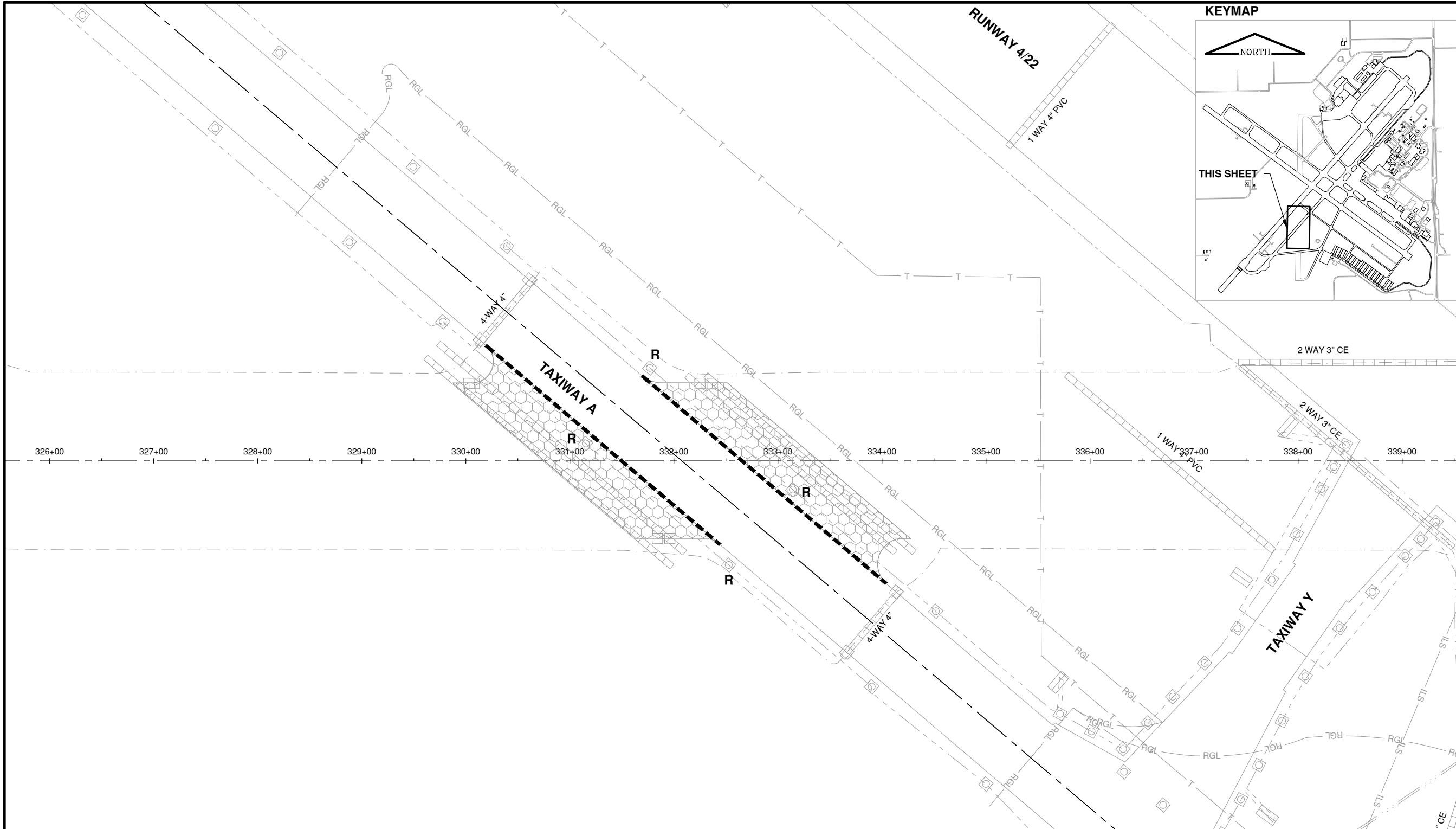
IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY AND FAA OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CD100.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
COPYRIGHT:	

EX. CONDITIONS & REMOVALS RWY 18-36 NORTH

Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CD100.dwg
Date: Tuesday, April 26, 2022 12:39:52 PM



License No. 184-000613

CONSULTANTS



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

NOTES

1. REMOVED PAVEMENT SHALL BE DISPOSED OF OFFSITE AT NO ADDITIONAL COST TO THE CONTRACT.
2. CONTRACTOR SHALL VERIFY THAT ALL DUCTS ARE ABANDONED PRIOR TO REMOVALS.
3. CONTRACTOR SHALL PROTECT EXISTING PAVEMENT EDGES WHICH TO REMAIN IN PLACE. ADDITIONAL SAWING MAY BE REQUIRED TO PROTECT THESE EDGES.
4. REMOVED LIGHT FIXTURES AND SIGNS SHALL BE GIVEN TO THE AIRPORT. REMOVED CANS SHALL BE DISPOSED OF AT NO ADDITIONAL COST TO THE CONTRACT.

LEGEND

- | | | | |
|----------|---------------------------------|----------|---|
| | REM. PVMT STRUCTURE (HMA / PCC) | M | MODIFY SIGN PANEL |
| | FULL DEPTH SAWCUT | | EXIST. RUNWAY THRESHOLD STAKE-MOUNTED LIGHT |
| | EXIST. DUCT BANK | | EXIST. RUNWAY BASE-MOUNTED LIGHT |
| | EXIST. STORM SEWER | | EXIST. RUNWAY STAKE-MOUNTED LIGHT |
| R | TO BE REMOVED | | EXIST. TAXIWAY BASE-MOUNTED LIGHT |
| | RETRO-REFLECTIVE MARKERS | | EXIST. TAXIWAY STAKE-MOUNTED LIGHT |
| | SPLICE CAN | | EXIST. RUNWAY GUARD LIGHT |
| | EXIST FIBER-OPTIC CABLE | | EXIST. SIGN |
| | | | EXIST. DISTANCE REMAINING SIGN |
| | | | EXIST. INLET / CATCH BASIN |
| | | | EXIST. CLEANOUT |
| | | | EXIST. DUCT MARKER |

UTILITY DISCLAIMER

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

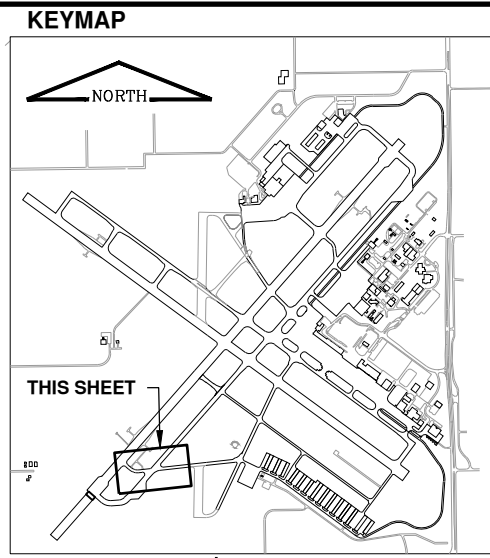
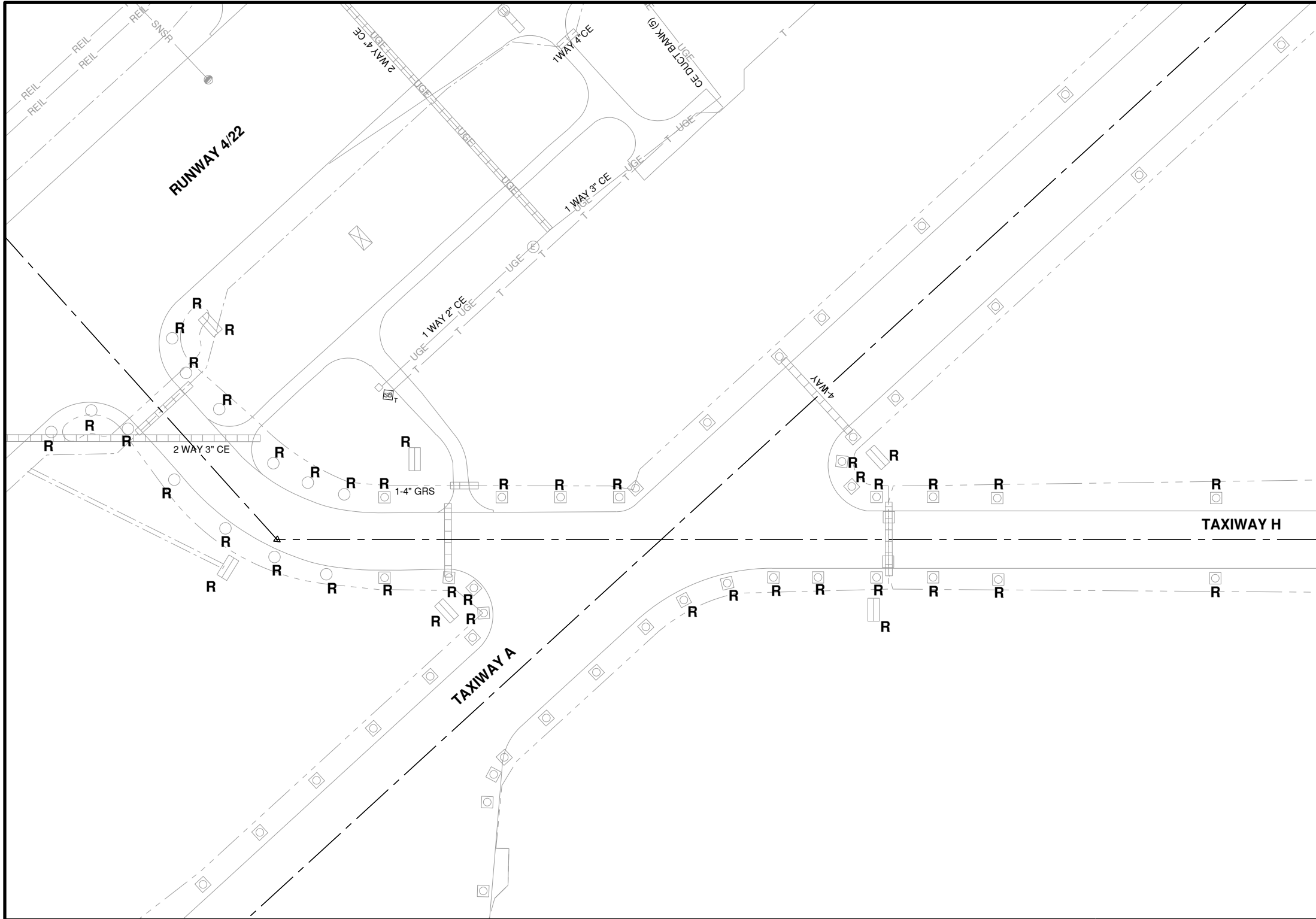
IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY AND FAA OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CD100.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
COPYRIGHT:	

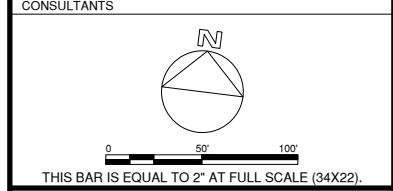
SHEET TITLE
EX. CONDITIONS & REMOVALS TXY A

CD103
SHEET 17 OF 91

Path: K:\Springfield\180035-05_Prv_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CD100.dwg
Date: Tuesday, April 26, 2022 12:40:07 PM



CMT
 License No. 184-000613
 CONSULTANTS



BID SUBMITTAL
 APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER

 SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, IL

MATCHLINE (SEE CD105)

Path: K:\Springfield\18035-05_Phase 3\Draw\Sheets\PHASE 3\18003505-PH3-CD100.dwg
 Date: Tuesday, April 26, 2022 12:40:22 PM

- NOTES**
- REMOVED PAVEMENT SHALL BE DISPOSED OF OFFSITE AT NO ADDITIONAL COST TO THE CONTRACT.
 - CONTRACTOR SHALL VERIFY THAT ALL DUCTS ARE ABANDONED PRIOR TO REMOVALS.
 - CONTRACTOR SHALL PROTECT EXISTING PAVEMENT EDGES WHICH TO REMAIN IN PLACE. ADDITIONAL SAWING MAY BE REQUIRED TO PROTECT THESE EDGES.
 - REMOVED LIGHT FIXTURES AND SIGNS SHALL BE GIVEN TO THE AIRPORT. REMOVED CANS SHALL BE DISPOSED OF AT NO ADDITIONAL COST TO THE CONTRACT.

LEGEND

	REM. PVMT STRUCTURE (HMA / PCC)		MODIFY SIGN PANEL
	FULL DEPTH SAWCUT		EXIST. RUNWAY THRESHOLD STAKE-MOUNTED LIGHT
	EXIST. DUCT BANK		EXIST. RUNWAY BASE-MOUNTED LIGHT
	EXIST. STORM SEWER		EXIST. RUNWAY STAKE-MOUNTED LIGHT
	TO BE REMOVED		EXIST. TAXIWAY BASE-MOUNTED LIGHT
	RETRO-REFLECTIVE MARKERS		EXIST. TAXIWAY STAKE-MOUNTED LIGHT
	SPLICE CAN		EXIST. RUNWAY GUARD LIGHT
	EXIST FIBER-OPTIC CABLE		EXIST. SIGN
			EXIST. DISTANCE REMAINING SIGN
			EXIST. INLET / CATCH BASIN
			EXIST. CLEANOUT
			EXIST. DUCT MARKER

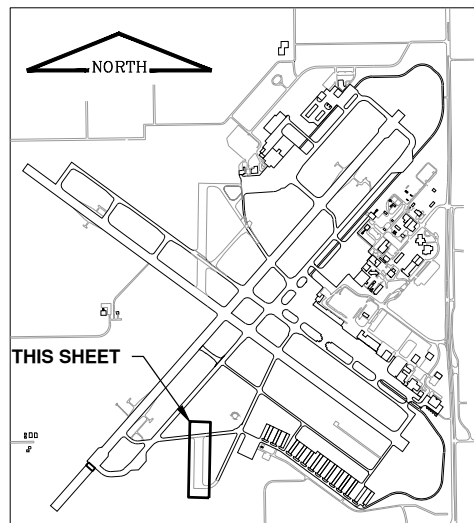
UTILITY DISCLAIMER

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY AND FAA OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA022
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-CD100.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: MJD		
APPROVED BY: RLV		
COPYRIGHT:		
SHEET TITLE		
EX. CONDITIONS & REMOVALS TXY H		
CD104		
SHEET	18	OF 91

KEYMAP



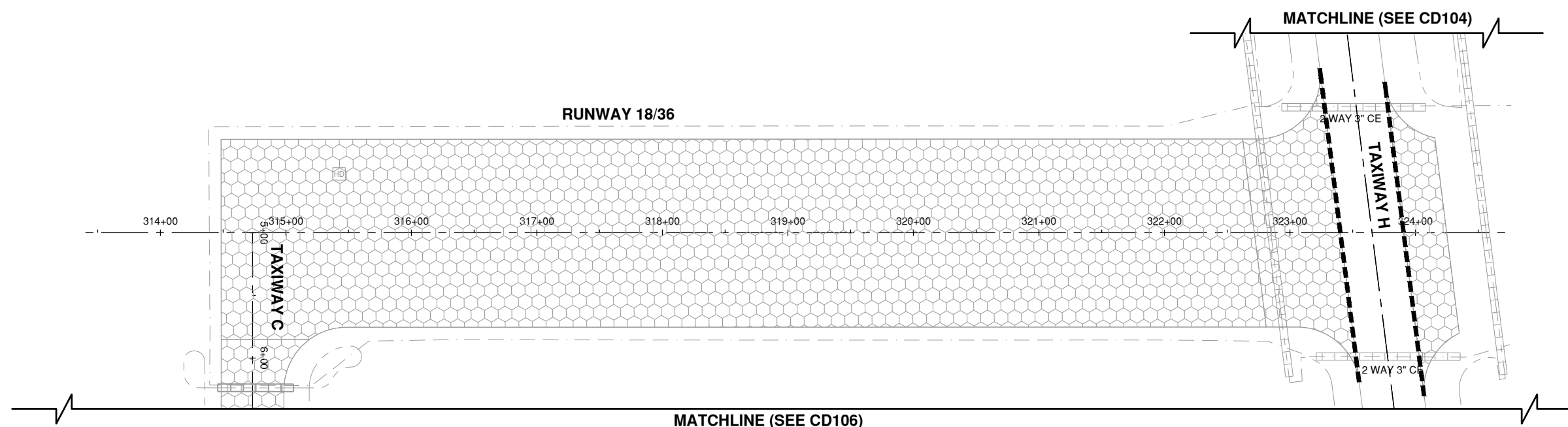
License No. 184-000613

CONSULTANTS



0 50' 100'

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

NOTES

1. REMOVED PAVEMENT SHALL BE DISPOSED OF OFFSITE AT NO ADDITIONAL COST TO THE CONTRACT.
2. CONTRACTOR SHALL VERIFY THAT ALL DUCTS ARE ABANDONED PRIOR TO REMOVALS.
3. CONTRACTOR SHALL PROTECT EXISTING PAVEMENT EDGES WHICH TO REMAIN IN PLACE. ADDITIONAL SAWING MAY BE REQUIRED TO PROTECT THESE EDGES.
4. REMOVED LIGHT FIXTURES AND SIGNS SHALL BE GIVEN TO THE AIRPORT. REMOVED CANS SHALL BE DISPOSED OF AT NO ADDITIONAL COST TO THE CONTRACT.

LEGEND

	REM. PVMT STRUCTURE (HMA / PCC)	M	MODIFY SIGN PANEL
	FULL DEPTH SAWCUT		EXIST. RUNWAY THRESHOLD STAKE-MOUNTED LIGHT
	EXIST. DUCT BANK		EXIST. RUNWAY BASE-MOUNTED LIGHT
	ST EXIST. STORM SEWER		EXIST. RUNWAY STAKE-MOUNTED LIGHT
R	TO BE REMOVED		EXIST. TAXIWAY BASE-MOUNTED LIGHT
	RETRO-REFLECTIVE MARKERS		EXIST. TAXIWAY STAKE-MOUNTED LIGHT
	SPLICE CAN		EXIST. RUNWAY GUARD LIGHT
	FO EXIST FIBER-OPTIC CABLE		EXIST. SIGN
			EXIST. DISTANCE REMAINING SIGN
			EXIST. INLET / CATCH BASIN
			EXIST. CLEANOUT
			EXIST. DUCT MARKER

UTILITY DISCLAIMER

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY AND FAA OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CD100.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
COPYRIGHT:	

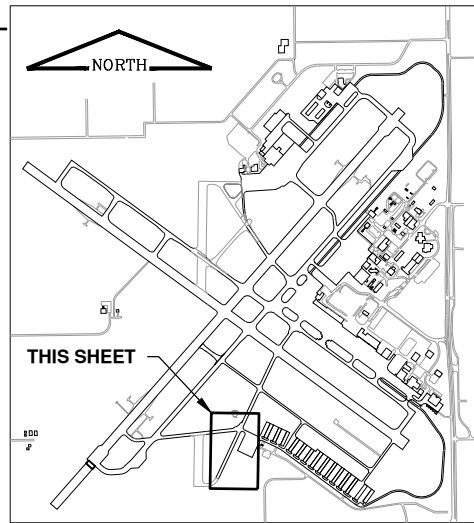
EX. CONDITIONS & REMOVAL RWY 18-36 SOUTH

SHEET 19 OF 91

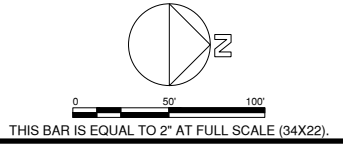
Path: K:\Springfield\0180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CD100.dwg
Date: Tuesday, April 26, 2022 12:40:37 PM

MATCHLINE (SEE CD105)

KEYMAP



License No. 184-000613
CONSULTANTS



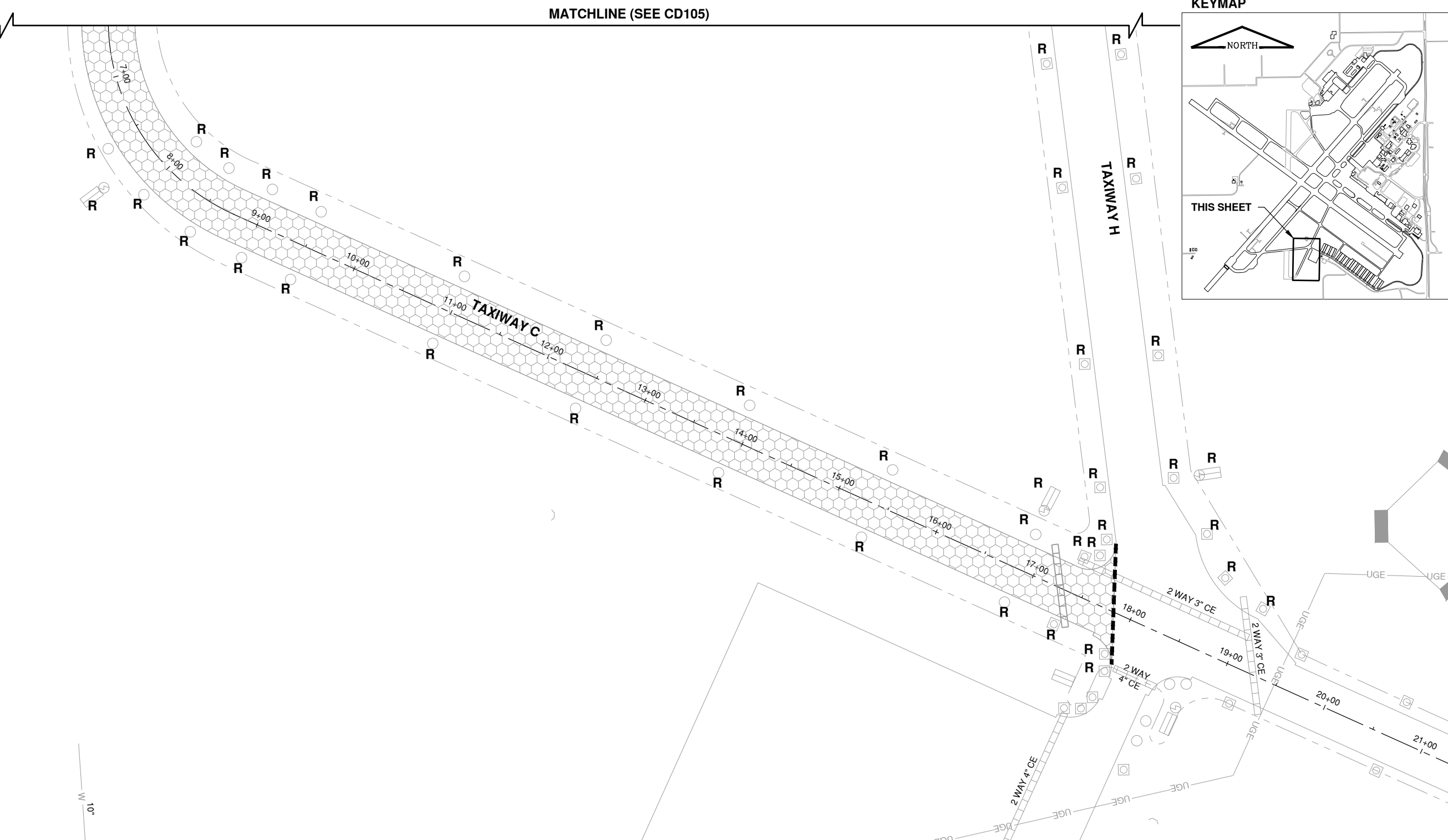
BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL



NOTES

1. REMOVED PAVEMENT SHALL BE DISPOSED OF OFFSITE AT NO ADDITIONAL COST TO THE CONTRACT.
2. CONTRACTOR SHALL VERIFY THAT ALL DUCTS ARE ABANDONED PRIOR TO REMOVALS.
3. CONTRACTOR SHALL PROTECT EXISTING PAVEMENT EDGES WHICH TO REMAIN IN PLACE. ADDITIONAL SAWING MAY BE REQUIRED TO PROTECT THESE EDGES.
4. REMOVED LIGHT FIXTURES AND SIGNS SHALL BE GIVEN TO THE AIRPORT. REMOVED CANS SHALL BE DISPOSED OF AT NO ADDITIONAL COST TO THE CONTRACT.

LEGEND

	REM. PVMT STRUCTURE (HMA / PCC)		MODIFY SIGN PANEL
	FULL DEPTH SAWCUT		EXIST. RUNWAY THRESHOLD STAKE-MOUNTED LIGHT
	EXIST. DUCT BANK		EXIST. RUNWAY BASE-MOUNTED LIGHT
	EXIST. STORM SEWER		EXIST. RUNWAY STAKE-MOUNTED LIGHT
	TO BE REMOVED		EXIST. TAXIWAY BASE-MOUNTED LIGHT
	RETRO-REFLECTIVE MARKERS		EXIST. TAXIWAY STAKE-MOUNTED LIGHT
	SPLICE CAN		EXIST. RUNWAY GUARD LIGHT
	EXIST FIBER-OPTIC CABLE		EXIST. SIGN
			EXIST. DISTANCE REMAINING SIGN
			EXIST. INLET / CATCH BASIN
			EXIST. CLEANOUT
			EXIST. DUCT MARKER

UTILITY DISCLAIMER

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY AND FAA OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

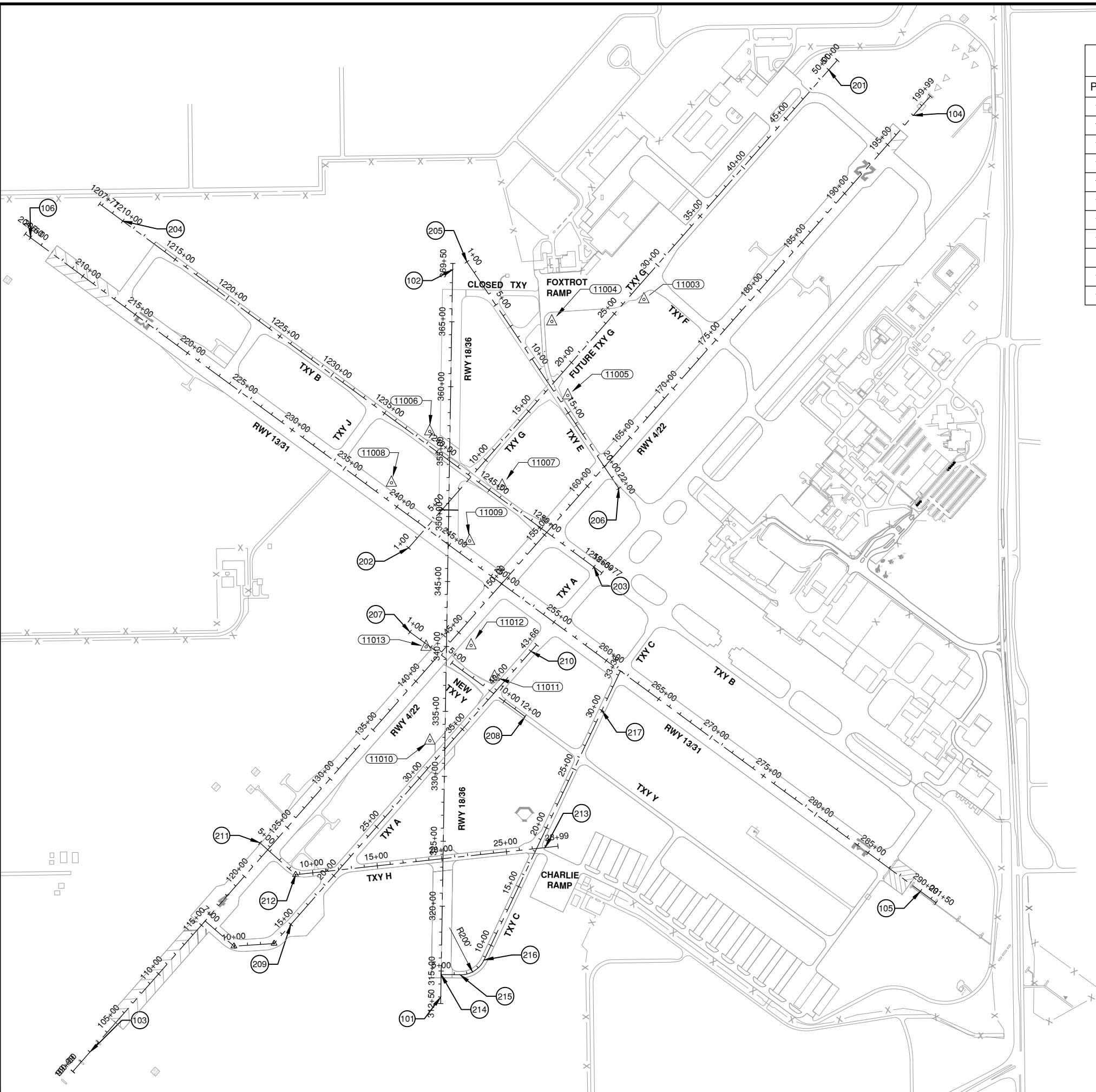
MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CD100.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
COPYRIGHT:	

SHEET TITLE
**EX. CONDITIONS &
REMOVAL TXY C**

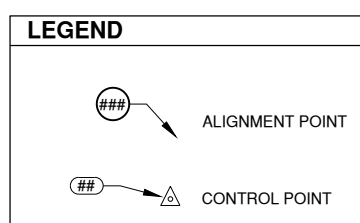
CD106
SHEET 20 OF 91

Path: K:\Springfield\0180035-05_Rwy_18-36\DrawSheets\PHASE 3\18003505-PH3-CP100.dwg
 Date: Tuesday, April 26, 2022 12:41:12 PM



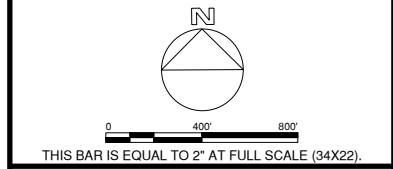
CONTROL POINT TABLE				
POINT	DESCRIPTION	NORTHING	EASTING	ELEVATION
11003	103 IPIN	1159636.849'	2434842.840'	587.19'
11004	103 IPIN	1159466.767'	2434133.844'	587.66'
11005	103 IPIN	1158897.523'	2434256.542'	586.66'
11006	103 IPIN	1158619.807'	2433193.514'	588.43'
11007	103 IPIN	1158202.064'	2433751.538'	586.45'
11008	103 IPIN	1158224.510'	2432899.265'	588.36'
11009	103 IPIN	1157776.500'	2433501.190'	589.49'
11010	103 IPIN	1156239.181'	2433195.251'	588.13'
11011	103 IPIN	1156730.933'	2433693.303'	588.79'
11012	103 IPIN	1156972.836'	2433512.577'	589.43'
11013	103 IPIN	1156961.517'	2433165.713'	589.72'

ALIGNMENT POINT TABLE			
POINT	DESCRIPTION	NORTHING	EASTING
101	RWY 18/36 STA 313+00	1154265.684'	2433279.034'
102	RWY 18/36 STA 369+00	1159864.931'	2433370.878'
103	RWY 4/22 STA 102+00	1153842.673'	2430578.774'
104	RWY 4/22 STA 198+00	1161053.405'	2436916.387'
105	RWY 13/31 STA 290+00	1155079.996'	2436971.296'
106	RWY 13/31 STA 205+00	1160112.085'	2430120.888'
201	TXY G STA 50+00	1161404.112'	2436266.057'
202	TXY G STA 1+00	1157723.634'	2433031.234'
203	TXY B STA 1255+00	1157576.668'	2434458.006'
204	TXY B STA 1210+00	1160240.715'	2430831.320'
205	TXY E STA 1+00	1159926.168'	2433480.525'
206	TXY E STA 22+00	1158181.938'	2434649.996'
207	TXY Y STA 1+00	1157075.813'	2433041.737'
208	TXY Y STA 12+00	1156424.601'	2433928.260'
209	TXY A STA 15+00	1154824.510'	2432125.248'
210	TXY A STA 43+00	1156927.715'	2433973.640'
211	TXY H STA 5+00	1155453.737'	2431886.769'
212	TXY H STA 8+67.09	1155207.697'	2432161.607'
213	TXY H STA 28+00	1155416.441'	2434083.208'
214	TXY C STA 5+00	1154439.039'	2433281.878'
215	TXY C STA 6+64.40	1154436.660'	2433428.255'
216	TXY C STA 8+75.59	1154551.247'	2433612.362'
217	TXY C STA 30+00	1156472.321'	2434519.339'



License No. 184-000613

CONSULTANTS



BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER

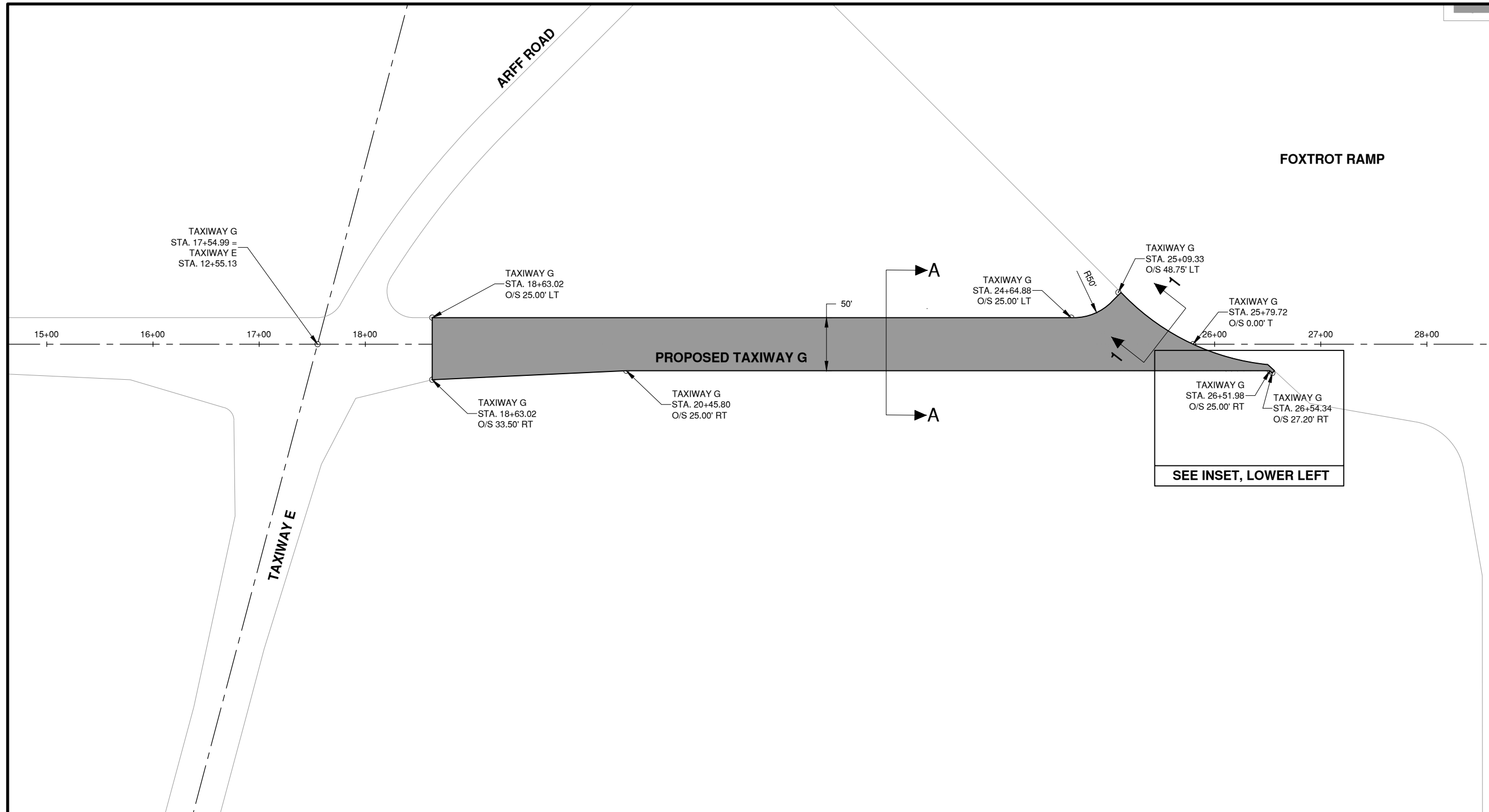
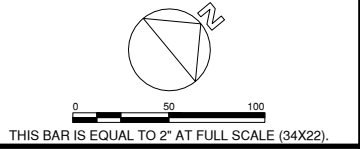


SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION
AIP PROJ. NO:	3-17-0096-XX	CA022
IL PROJ. NO:	SPI-4943	
CMT PROJECT NO:	180035-05	
CAD DWG FILE:	18003505-PH3-CP100.DWG	
DESIGNED BY:	HWI	
DRAWN BY:	DPA	
CHECKED BY:	MJD	
APPROVED BY:	RLV	
COPYRIGHT:		

SHEET TITLE
**HORIZONTAL
CONTROL**

CP100
SHEET 21 OF 91



BID SUBMITTAL
APRIL 22, 2022

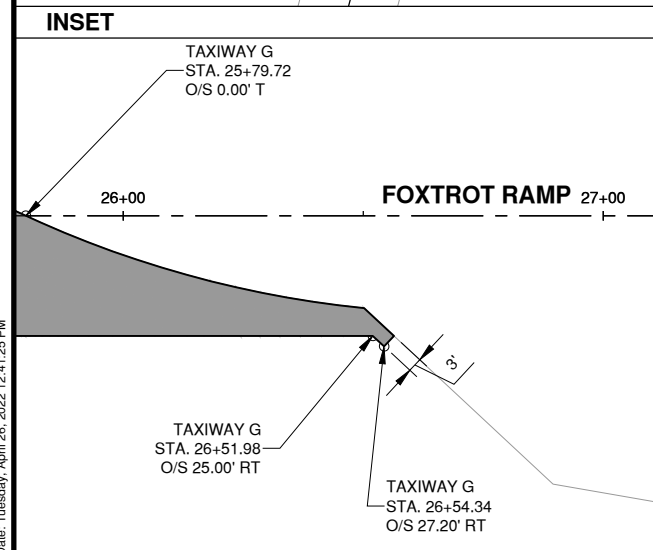
RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA022
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-CP101.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: MJD		
APPROVED BY: RLV		
COPYRIGHT:		

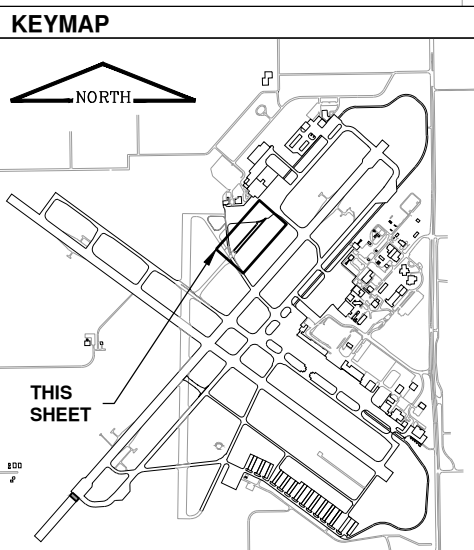


NOTES

- SEE SHEET CP301 FOR PAVEMENT TRANSITION DETAILS.

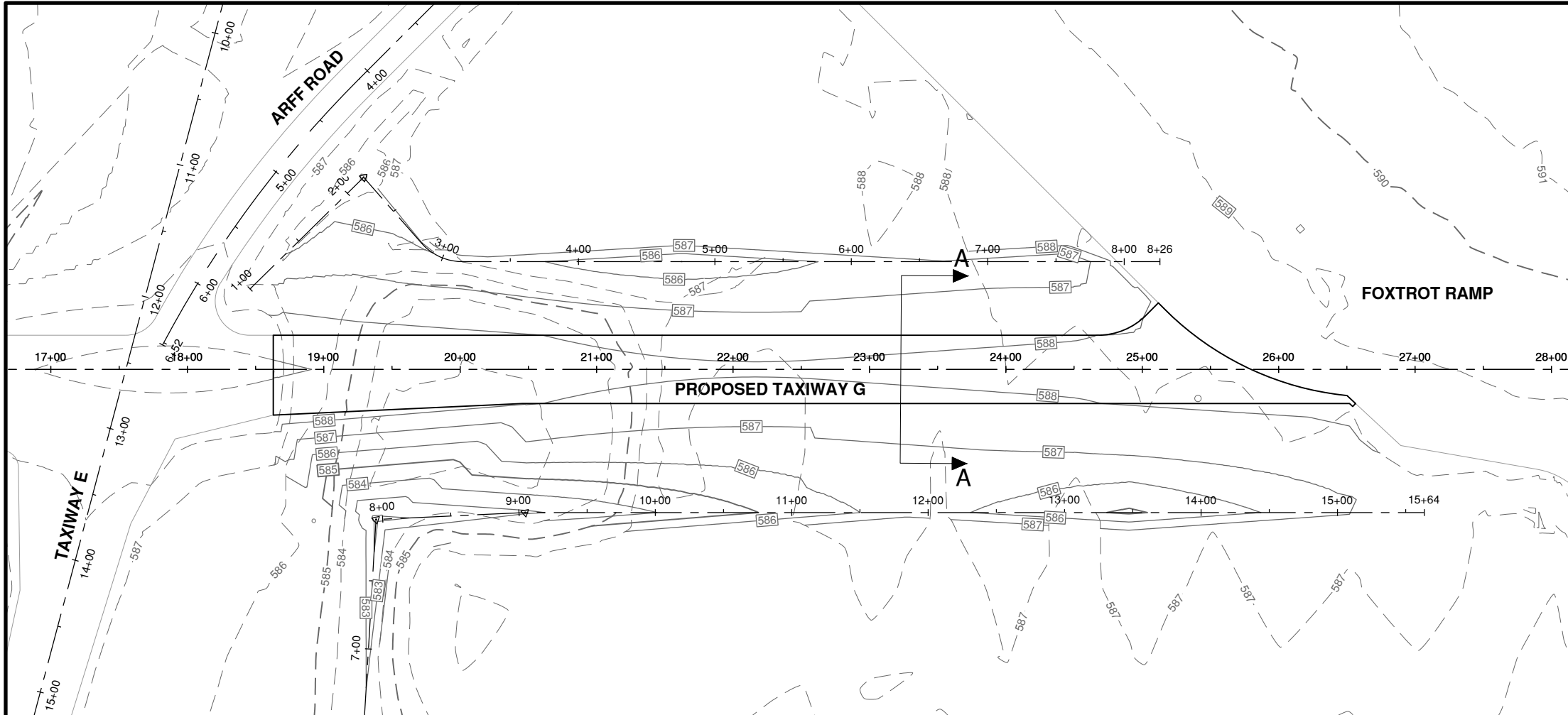
LEGEND

- NEW PAVEMENT
- EXISTING PAVEMENT EDGE
- SEE SECTIONS SHEET CP301



PROPOSED GEOMETRY TXY G

Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CP101.dwg
Date: Tuesday, April 26, 2022 12:41:25 PM



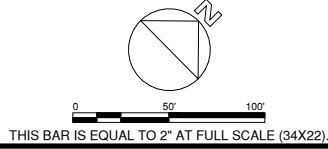
LEGEND

- 645.55 NEW (PROFILE) ELEVATION
- 648.67 EXIST. (PROFILE) ELEVATION
- 589 NEW CONTOURS
- 589 EXISTING CONTOURS
- PROPOSED PAVEMENT
- EXISTING PAVEMENT
- TYPICAL PAVEMENT SECTION

SEE SHEET CP301 FOR TYPICAL PAVEMENT SECTION



License No. 184-000613
CONSULTANTS



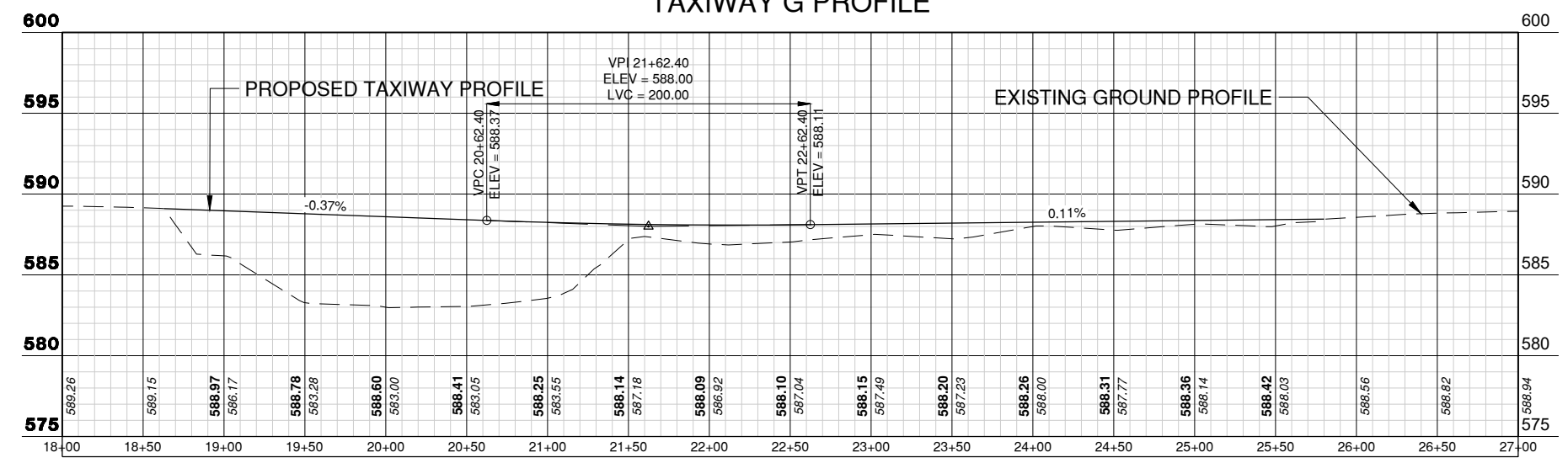
BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

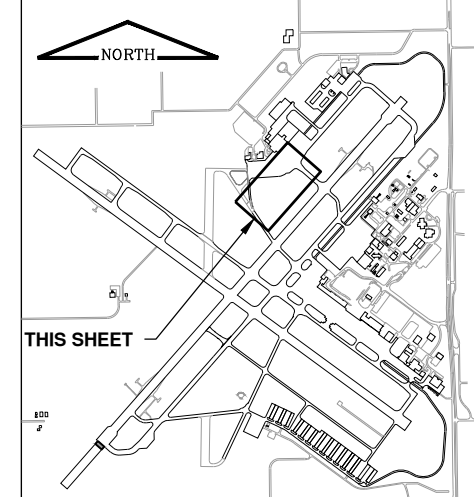


SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

TAXIWAY G PROFILE



KEYMAP



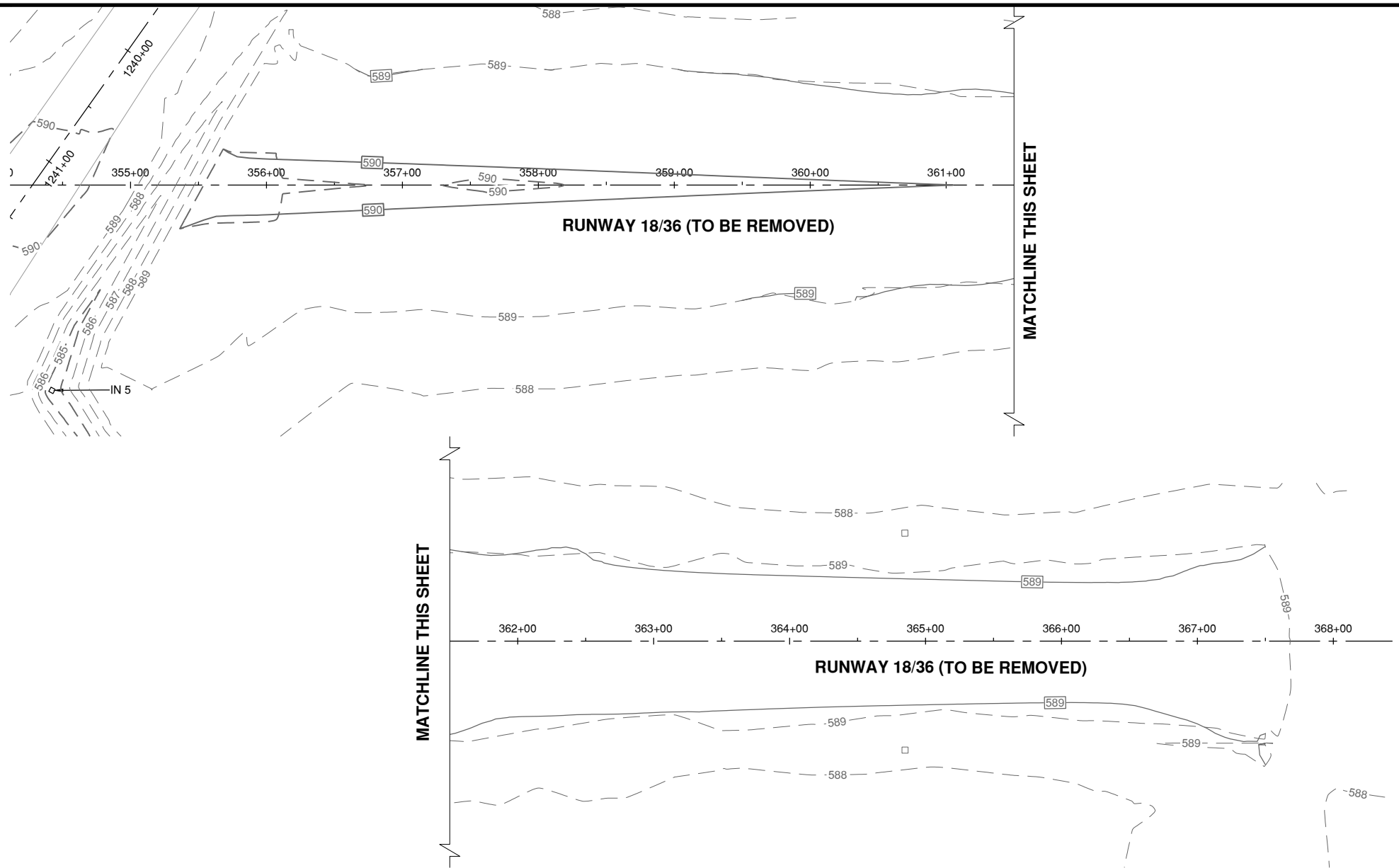
MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CP201.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

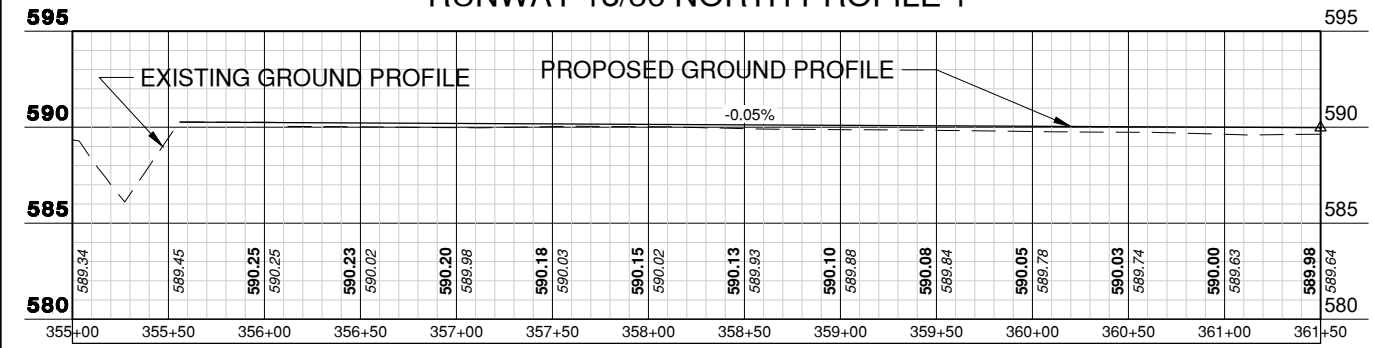
SHEET TITLE
PLAN & PROFILE 1 - TAXIWAY G

Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CP201.dwg
Date: Tuesday, April 26, 2022 12:41:57 PM

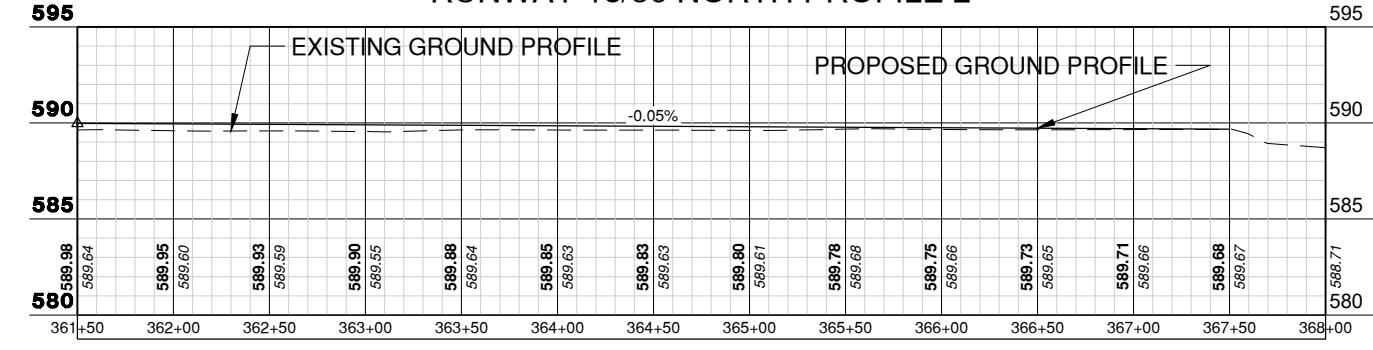
Path: K:\Springfield\18003505_Phase 3\18-36\DrawSheets\PHASE 3\18003505-PH3-CP201.dwg
 Date: Tuesday, April 26, 2022 12:42:15 PM



RUNWAY 18/36 NORTH PROFILE 1



RUNWAY 18/36 NORTH PROFILE 2



LEGEND

- NEW (PROFILE) ELEVATION
- EXIST. (PROFILE) ELEVATION
- NEW CONTOURS
- EXISTING CONTOURS
- PROPOSED PAVEMENT
- EXISTING PAVEMENT

SEE SHEET CP301 FOR TYPICAL PAVEMENT SECTION

CMT
 License No. 184-000613
 CONSULTANTS

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
 APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER

SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, IL

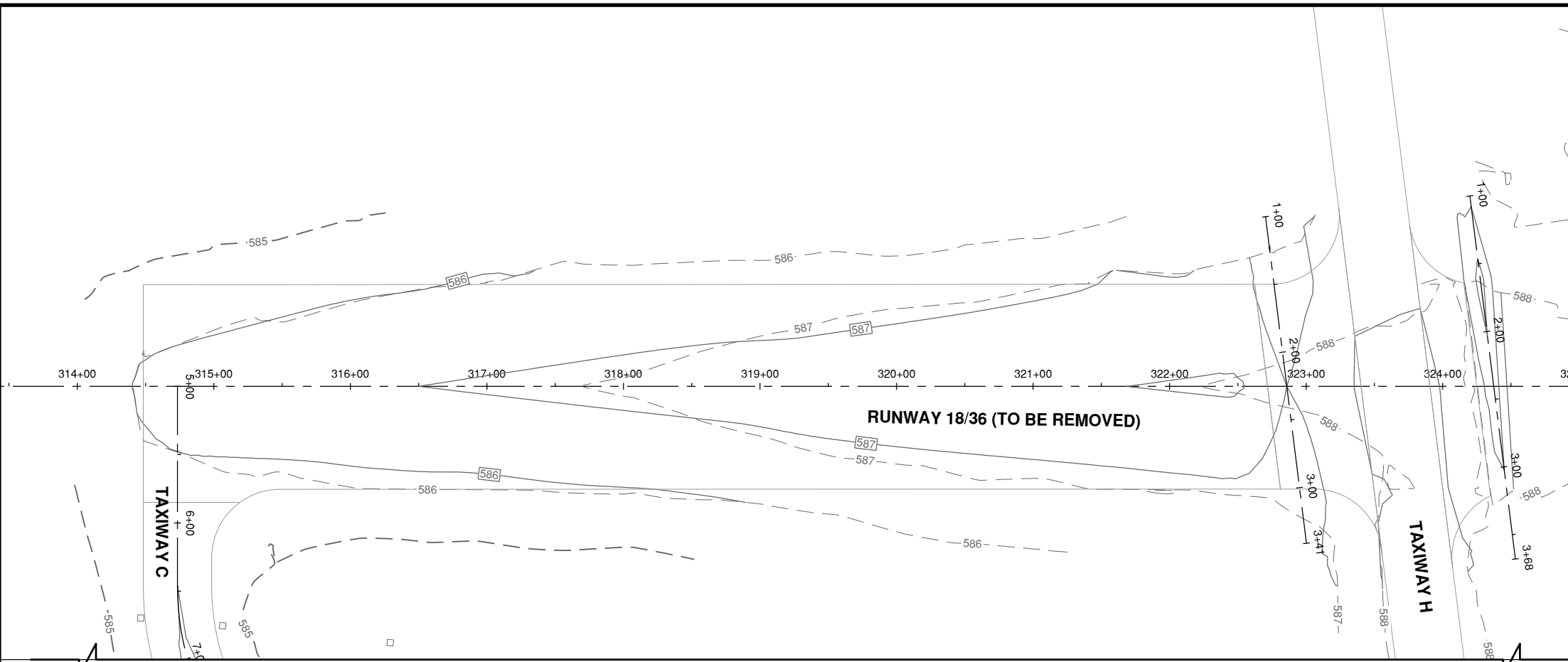
KEYMAP

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CP201.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

SHEET TITLE
PLAN & PROFILE 2 - RUNWAY 18-36 NORTH

Path: K:\Springfield\180035-05_Rwy_18-36\DrawSheets\PHASE 3\18003505-PH3-CP201.dwg
 Date: Tuesday, April 26, 2022 12:42:31 PM



MATCHLINE (SEE CP204)

LEGEND

- NEW (PROFILE) ELEVATION
- EXIST. (PROFILE) ELEVATION
- NEW CONTOURS
- EXISTING CONTOURS
- PROPOSED PAVEMENT
- EXISTING PAVEMENT

SEE SHEET CP301 FOR TYPICAL PAVEMENT SECTION

License No. 184-000613
 CONSULTANTS

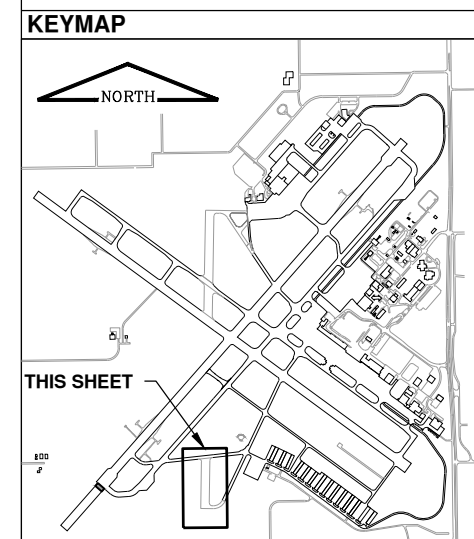
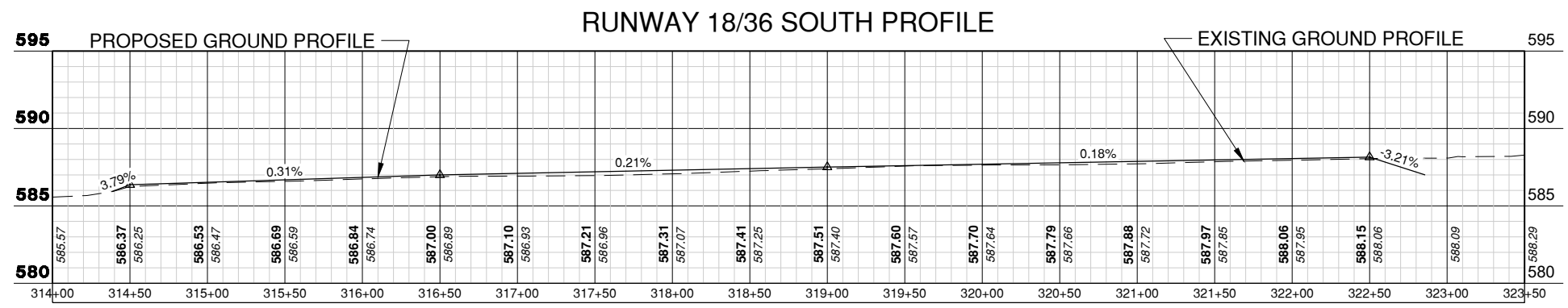
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
 APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER

SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, IL

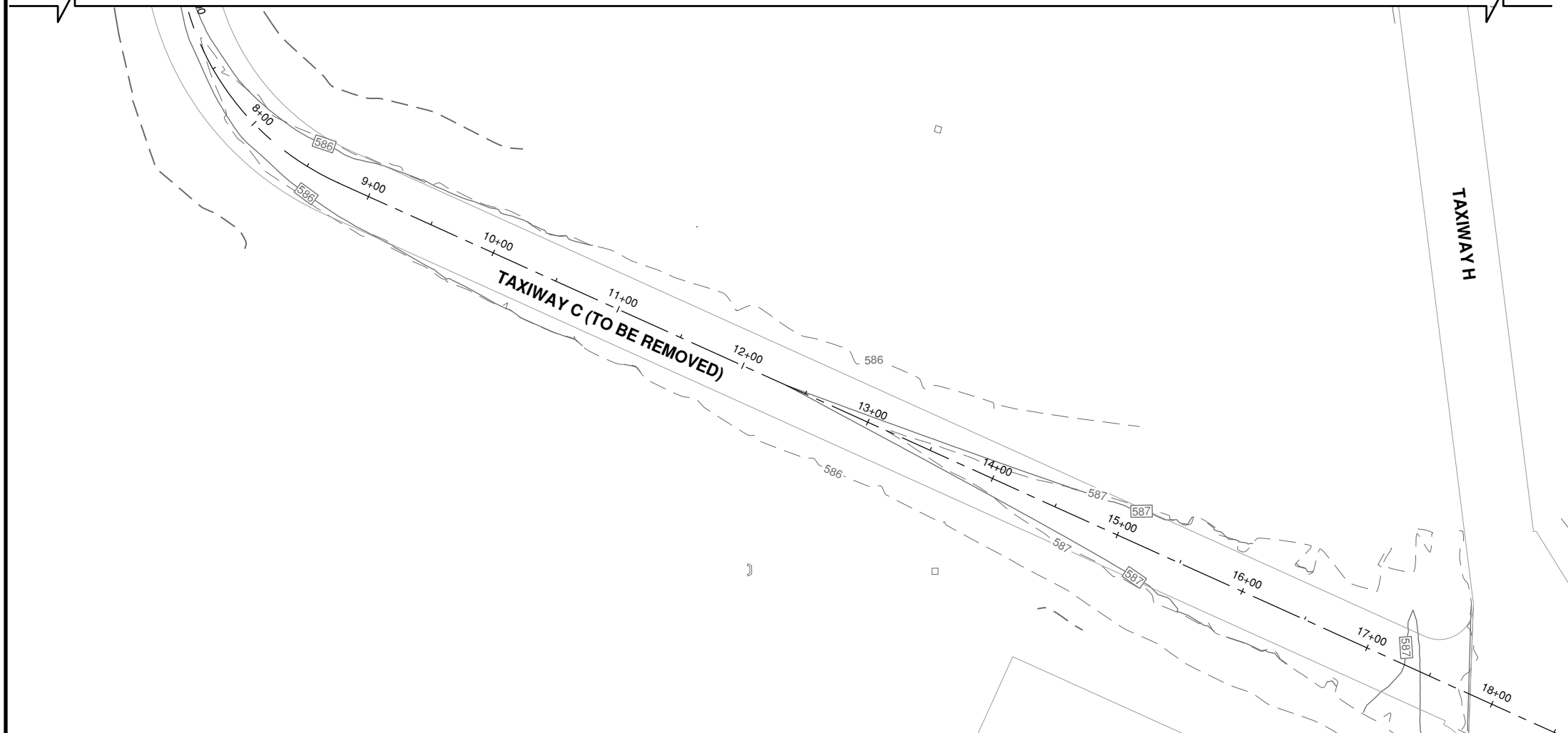


MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CP201.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

PLAN & PROFILE 3 - RUNWAY 18-36 SOUTH

MATCHLINE (SEE CP203)



LEGEND

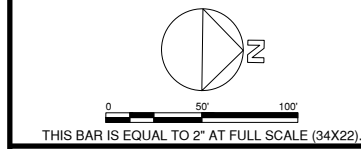
- NEW (PROFILE) ELEVATION
- EXIST. (PROFILE) ELEVATION
- NEW CONTOURS
- EXISTING CONTOURS
- PROPOSED PAVEMENT
- EXISTING PAVEMENT

SEE SHEET CP301 FOR TYPICAL PAVEMENT SECTION



License No. 184-000613

CONSULTANTS



BID SUBMITTAL
APRIL 22, 2022

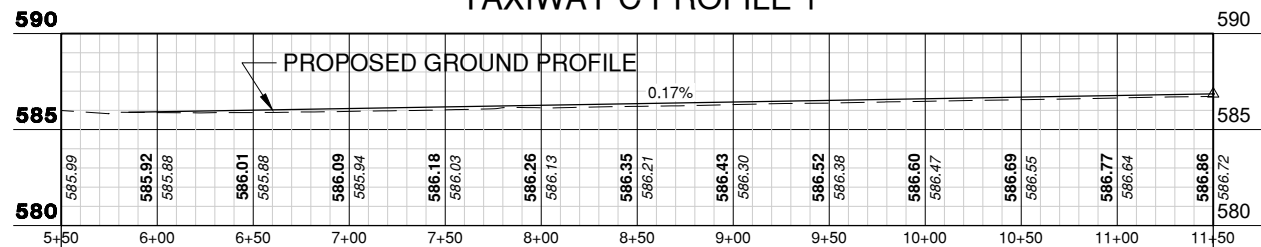
RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER

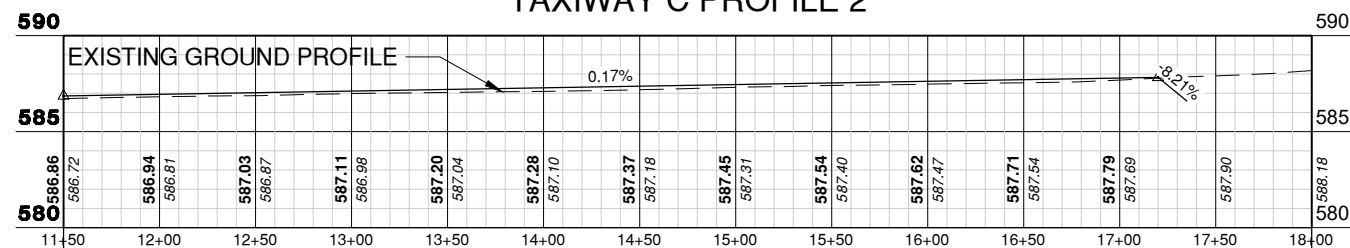


SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

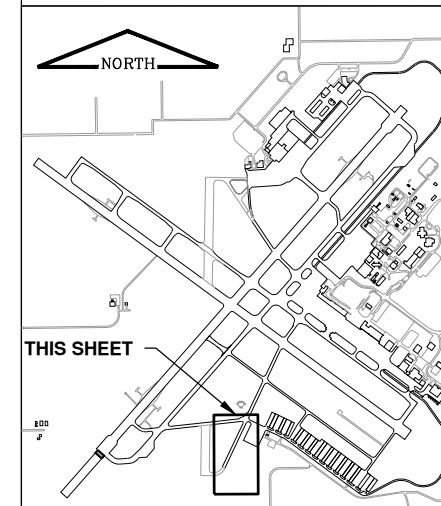
TAXIWAY C PROFILE 1



TAXIWAY C PROFILE 2



KEYMAP



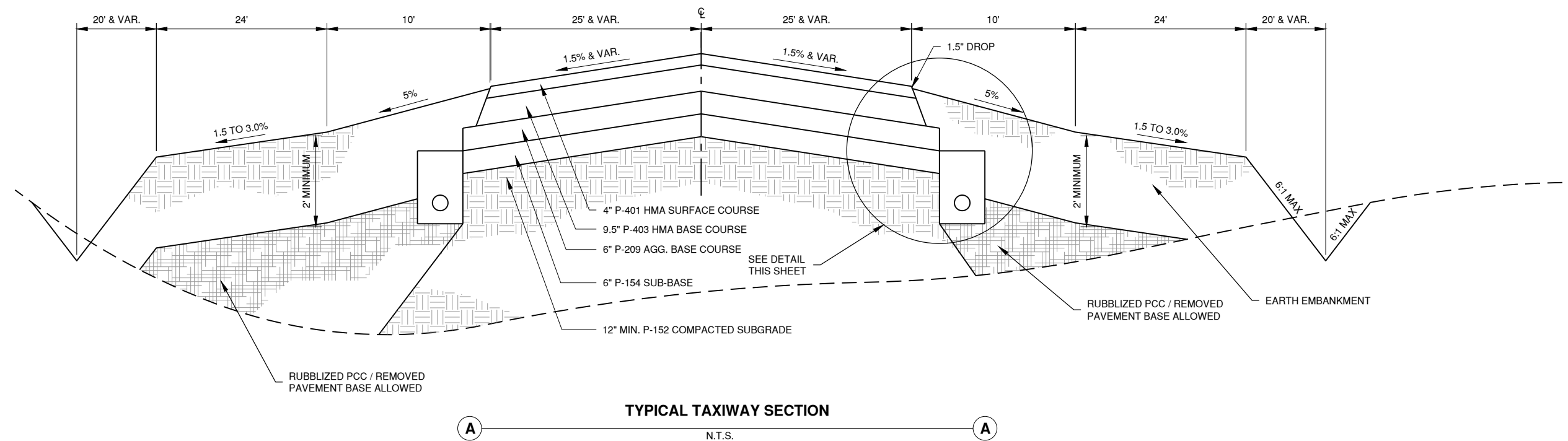
MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CP201.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

SHEET TITLE
**PLAN & PROFILE 4 -
TAXIWAY C**

CP204
SHEET 26 OF 91

Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CP201.dwg
Date: Tuesday, April 26, 2022 12:42:46 PM



TYPICAL TAXIWAY SECTION

N.T.S.

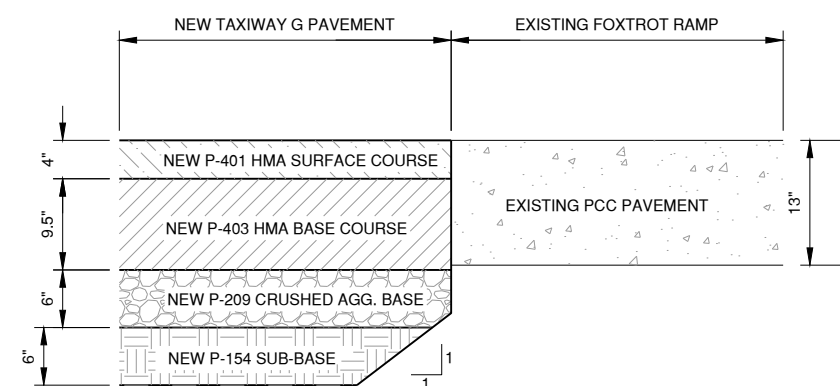
BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER

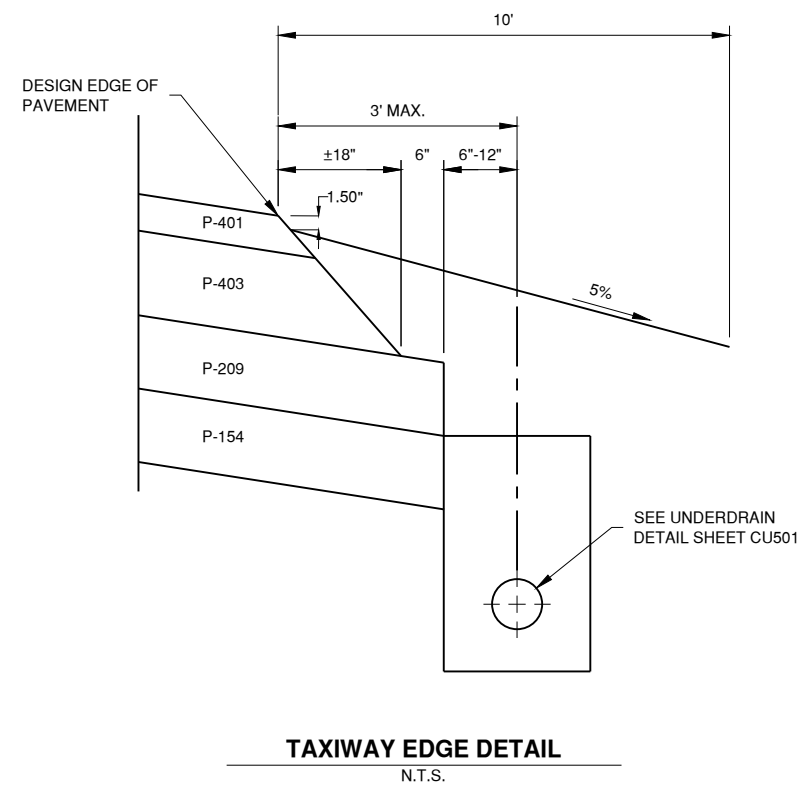


SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL



**PAVEMENT TRANSITION DETAIL
NEW TAXIWAY G TO
EXISTING FOXTROT RAMP**

N.T.S.



TAXIWAY EDGE DETAIL

N.T.S.

MARK DATE DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX **CA022**

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-CP300.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

APPROVED BY: APR

COPYRIGHT:

SHEET TITLE

TYPICAL SECTIONS



License No. 184-000613

CONSULTANTS



THIS BAR IS EQUAL TO 2' AT FULL SCALE (34X22).

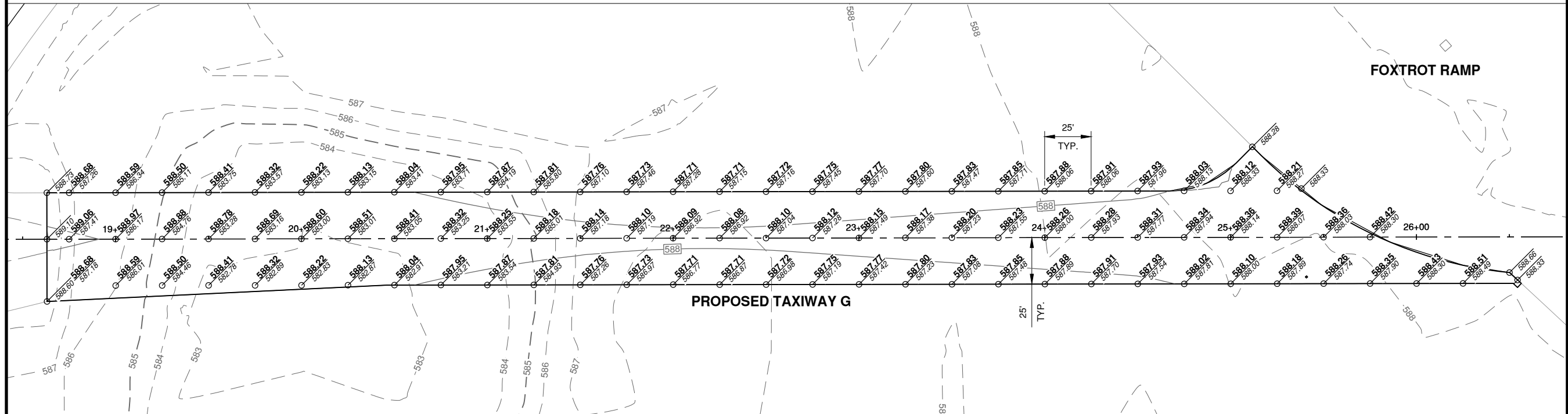
BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



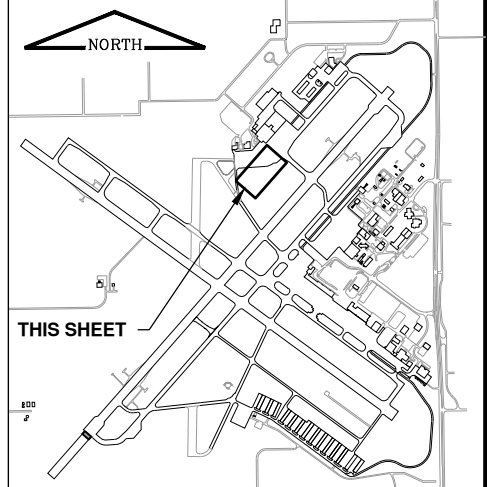
SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL



LEGEND

- NEW ELEVATION
- EXISTING ELEVATION
- NEW CONTOUR
- EXISTING CONTOUR

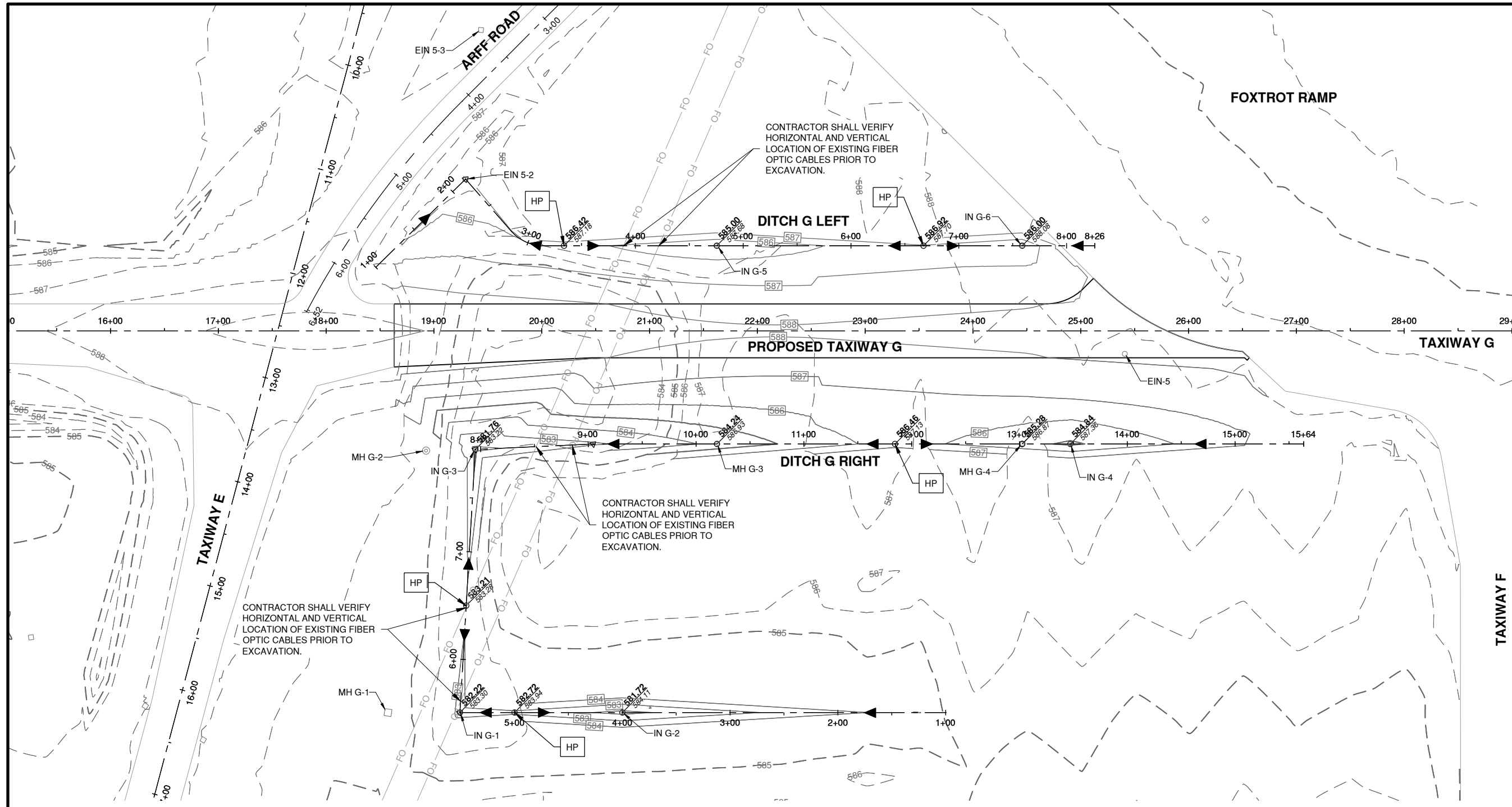
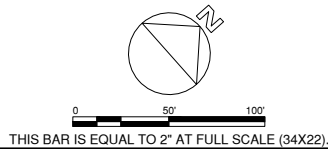
KEYMAP



Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CS101.dwg
Date: Tuesday, April 26, 2022 12:43:22 PM

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX CA022
IL PROJ. NO: SPI-4943
CMT PROJECT NO: 180035-05
CAD DWG FILE: 18003505-PH3-CS101.DWG
DESIGNED BY: HWI
DRAWN BY: DPA
CHECKED BY: CHK
APPROVED BY: APR
COPYRIGHT:
SHEET TITLE
**STAKING PLAN TXY G
EAST 1**
CS101
SHEET 28 OF 91



BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



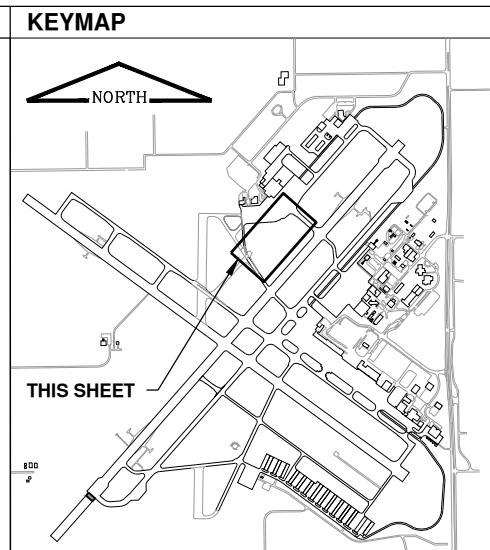
SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX CA022		
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-CG100.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: CHK		
APPROVED BY: APR		
COPYRIGHT:		

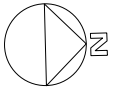
SHEET TITLE
GRADING & DRAINAGE PLAN TXY G

- NOTES**
- PRIOR TO EXCAVATION, THE CONTRACTOR SHALL PERFORM EXPLORATORY EXCAVATIONS OF THE EXISTING FIBER OPTIC CABLE AT THE INDICATED LOCATIONS AND PROVIDE THE ENGINEER WITH HORIZONTAL AND VERTICAL INFORMATION.
 - CONSTRUCT CABLE PROTECTION SLAB WHEN FIBER OPTIC CABLES ARE LESS THAN 36" BELOW FINISHED GRADE. SEE EL501

- LEGEND**
- NEW PAVEMENT EDGE
 - 589 — PROPOSED CONTOUR
 - - - 589 — EXISTING CONTOUR
 - ← - - - → PROPOSED DITCH FLOWLINE
 - 585.48 ○ PROPOSED SPOT ELEVATION
 - IN - # □ NEW INLET & NUMBER
 - HP □ HIGH POINT
 - MH - # ⊙ MANHOLE & NUMBER

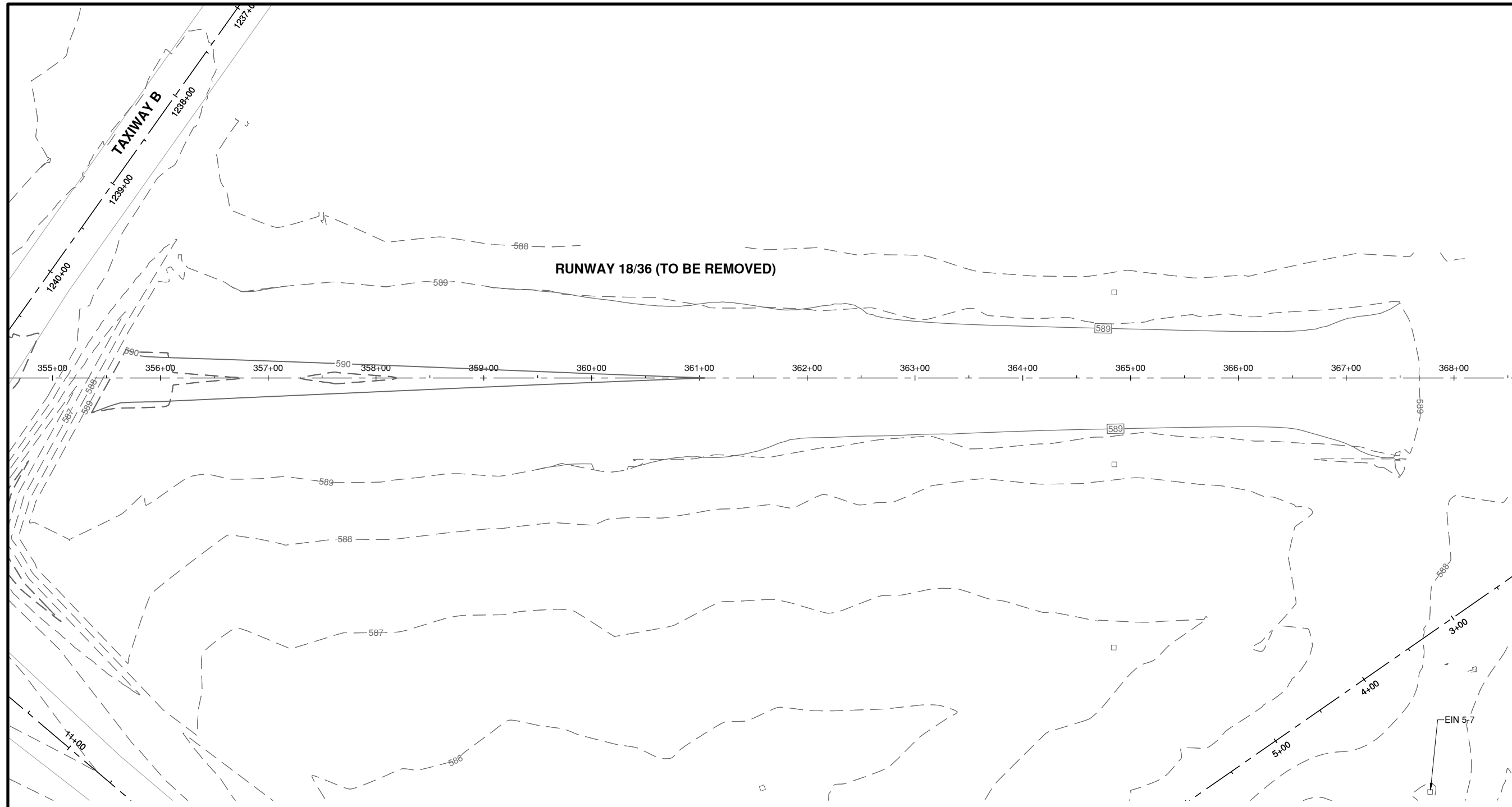


Path: K:\Springfield\0118035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CG100.dwg
Date: Tuesday, April 26, 2022 12:43:54 PM



THIS BAR IS EQUAL TO 2' AT FULL SCALE (34X22).

RUNWAY 18/36 (TO BE REMOVED)



BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER

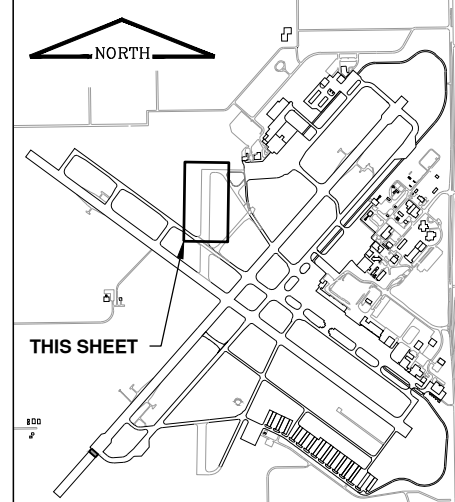


SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

LEGEND

- NEW PAVEMENT EDGE
- PROPOSED CONTOUR
- EXISTING CONTOUR
- PROPOSED DITCH FLOWLINE
- PROPOSED SPOT ELEVATION
- NEW INLET & NUMBER
- HIGH POINT
- MANHOLE & NUMBER

KEYMAP

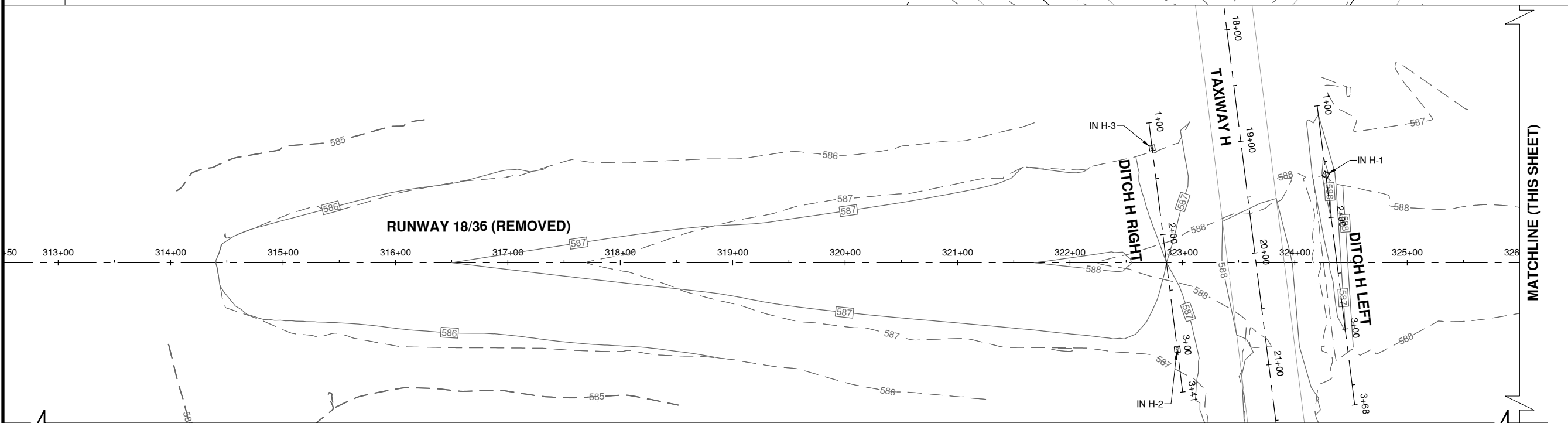
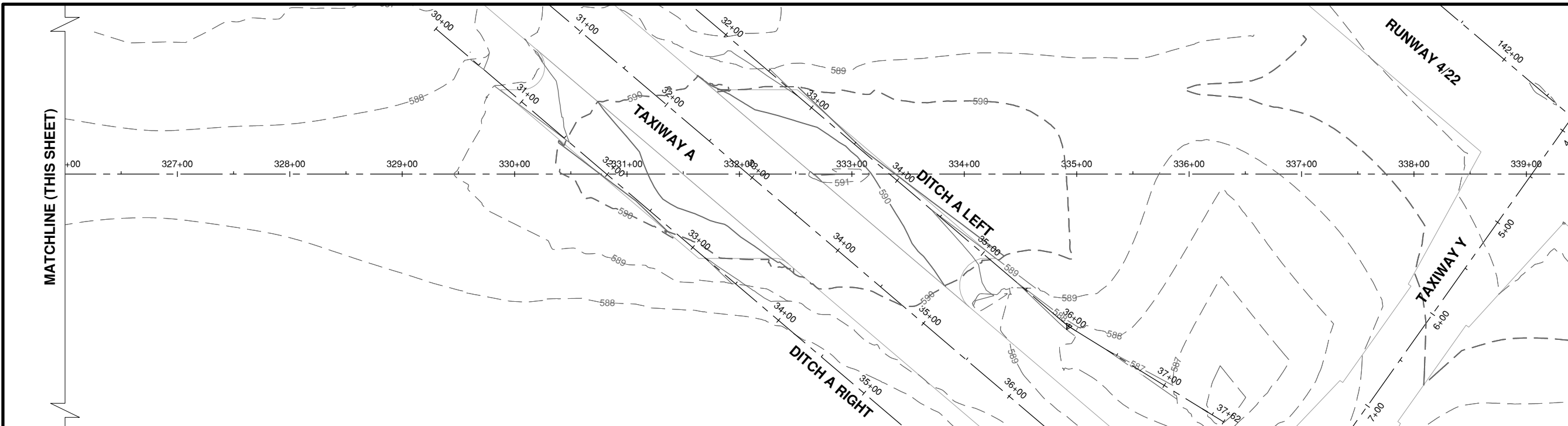


MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA022
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-CG100.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: CHK		
APPROVED BY: APR		
COPYRIGHT:		

SHEET TITLE
GRADING & DRAINAGE PLAN RWY 1836 N

CG102
SHEET 30 OF 91

MATCHLINE (THIS SHEET)

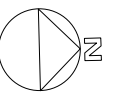


MATCHLINE (SEE CG104)



License No. 184-000613

CONSULTANTS



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



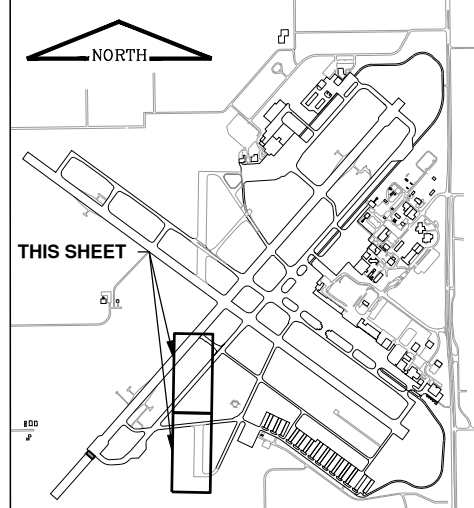
SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

Path: K:\Springfield\18035-05_Rwy_18-36\Draw\Sheets\PHASE 3\1803505-05-PH3-CG100.dwg
Date: Tuesday, April 26, 2022 12:44:26 PM

LEGEND

- NEW PAVEMENT EDGE
- 589 PROPOSED CONTOUR
- 589 EXISTING CONTOUR
- PROPOSED DITCH FLOWLINE
- 585.48 PROPOSED SPOT ELEVATION
- IN - # NEW INLET & NUMBER
- HP HIGH POINT
- MH - # MANHOLE & NUMBER

KEYMAP



MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA022
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-CG100.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: CHK		
APPROVED BY: APR		
COPYRIGHT:		

SHEET TITLE

**GRADING &
DRAINAGE PLAN RWY
1836 S**

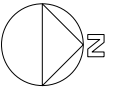
CG103

SHEET 31 OF 91



License No. 184-000613

CONSULTANTS



0 50' 100'

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

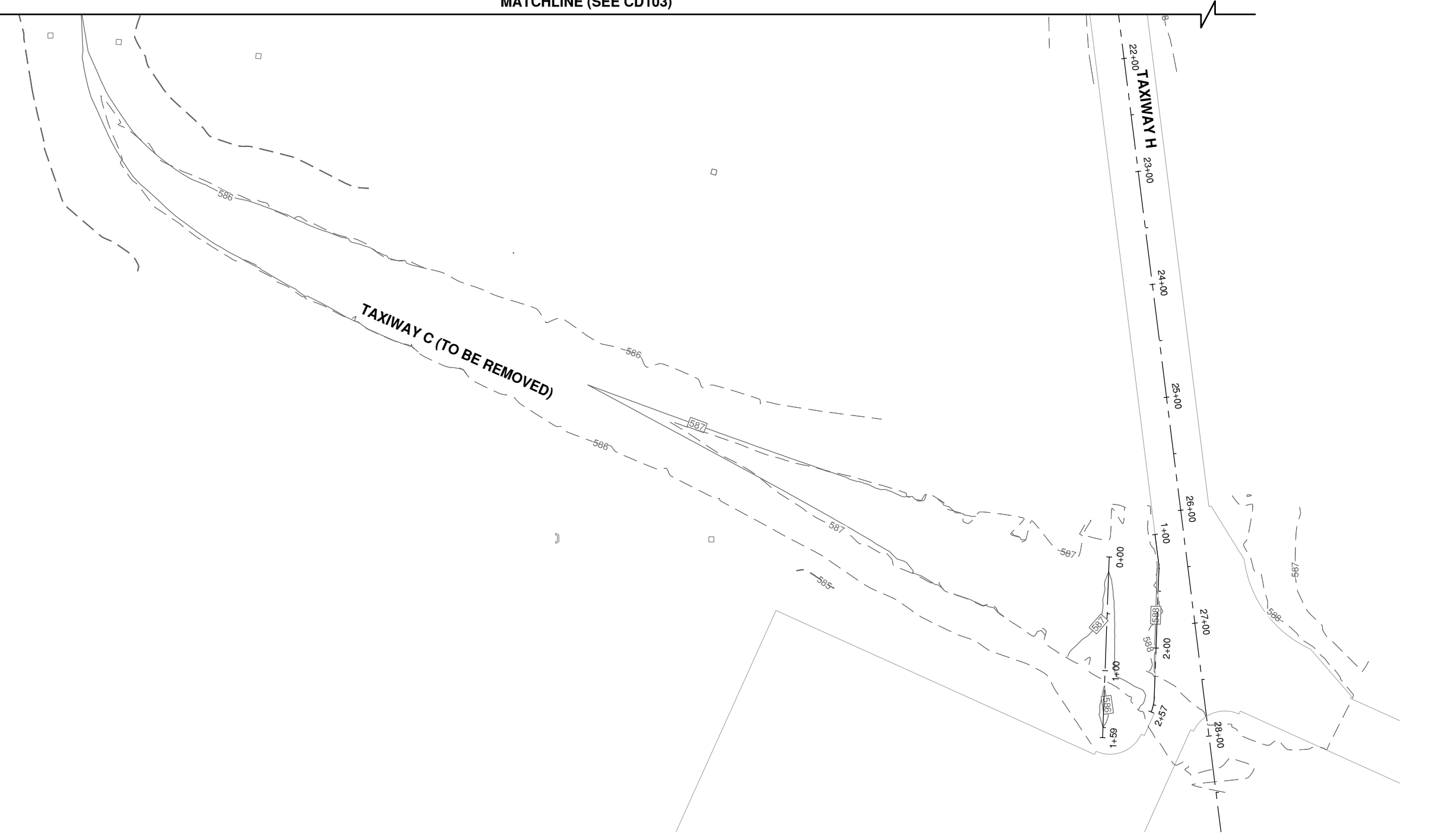
BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



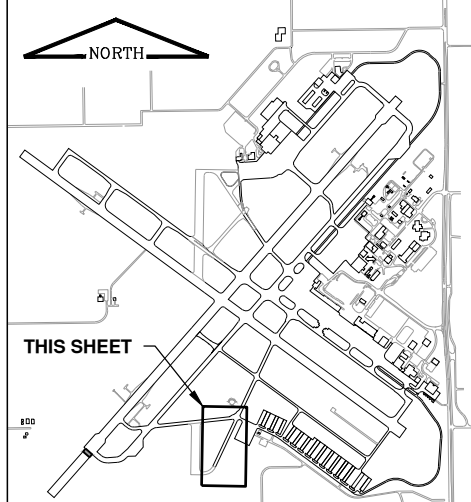
SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL



LEGEND

- NEW PAVEMENT EDGE
- 589— PROPOSED CONTOUR
- - -589- - EXISTING CONTOUR
- ← - - - → PROPOSED DITCH FLOWLINE
- 585.48 PROPOSED SPOT ELEVATION
- CB IN - # NEW INLET & NUMBER
- HP HIGH POINT
- ⊙ MH - # MANHOLE & NUMBER

KEYMAP



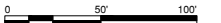
MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CG100.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

SHEET TITLE
GRADING &
DRAINAGE PLAN TXY
C

CG104
SHEET 32 OF 91

Path: K:\Springfield\A0180035-05_Rwy_18-36\DrawSheets\PHASE 3\18003505-PH3-CG100.dwg
Date: Tuesday, April 26, 2022 12:44:41 PM



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

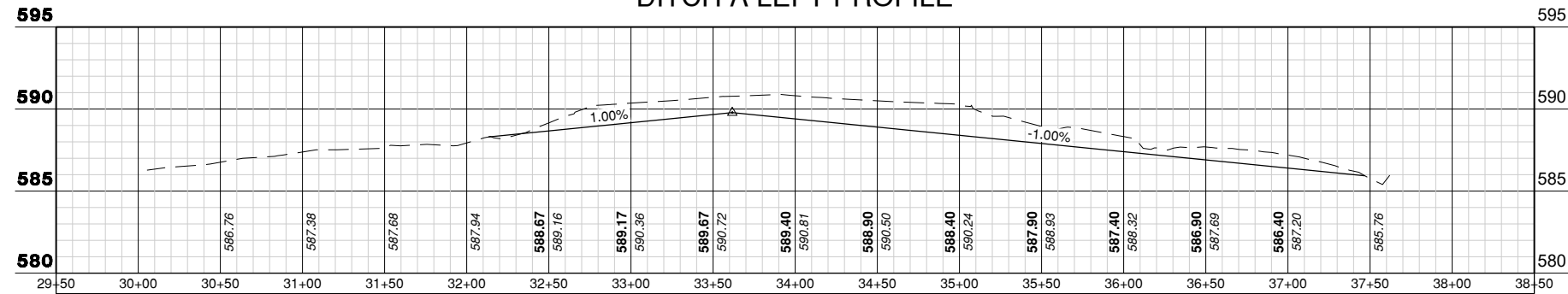
MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CG201.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

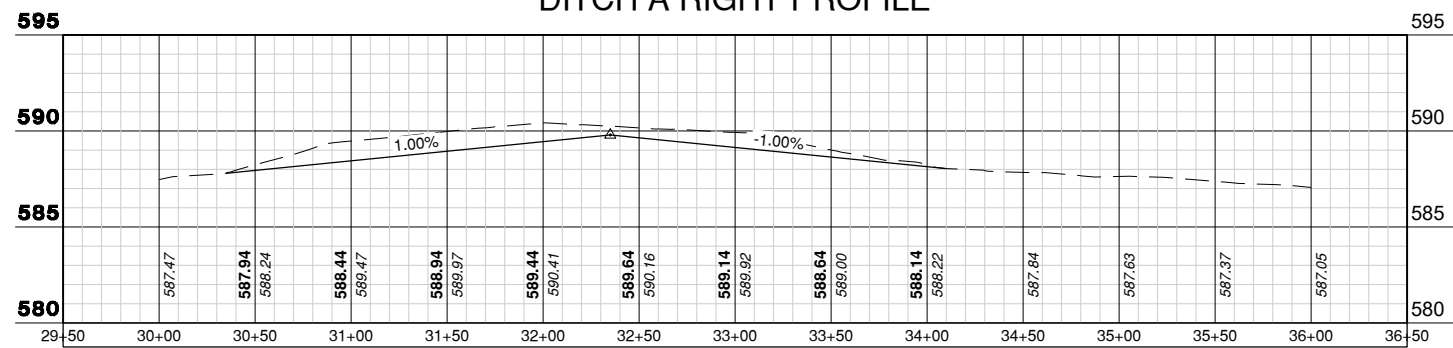
SHEET TITLE
DITCH PROFILES 1

CG201
SHEET 33 OF 91

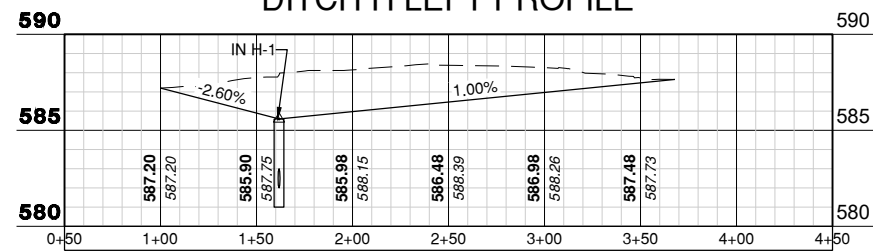
DITCH A LEFT PROFILE



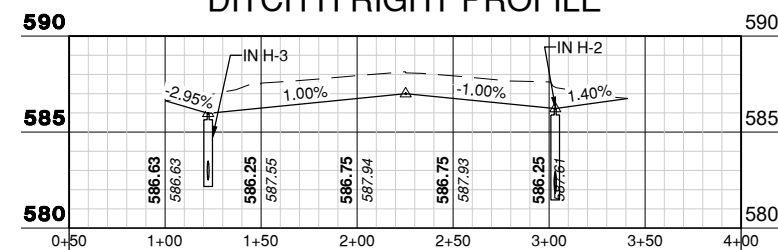
DITCH A RIGHT PROFILE



DITCH H LEFT PROFILE



DITCH H RIGHT PROFILE





THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER

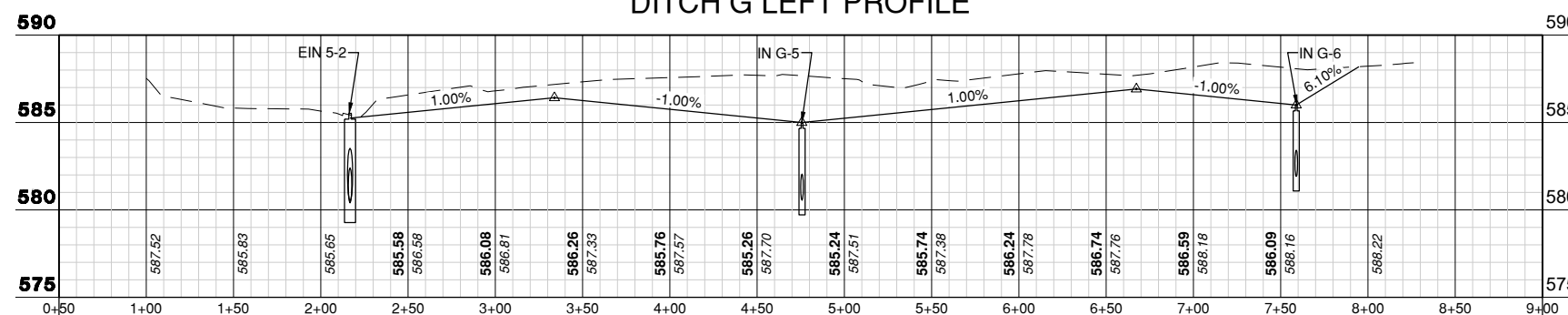


SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

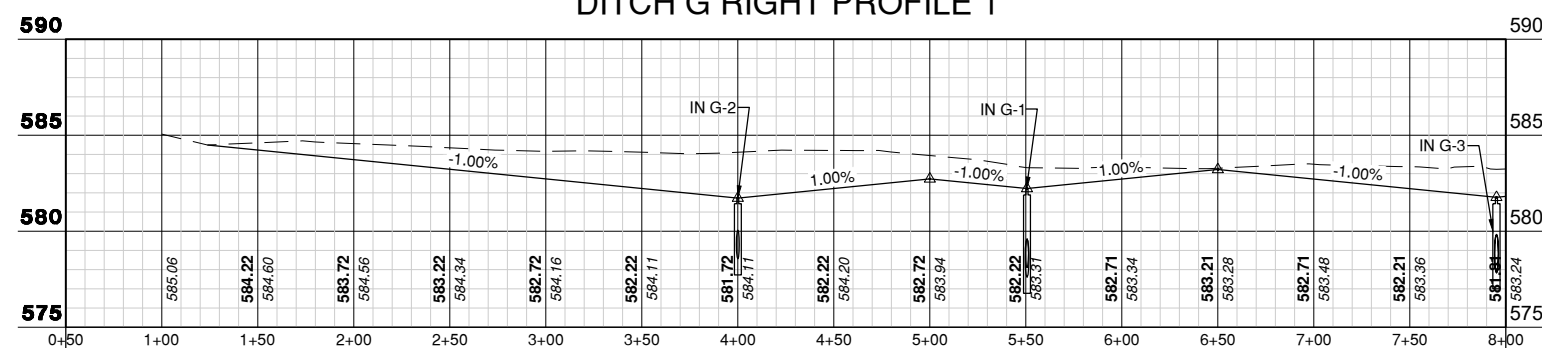
MARK	DATE	DESCRIPTION
AIP PROJ. NO:	3-17-0096-XX	CA022
IL PROJ. NO:	SPI-4943	
CMT PROJECT NO:	180035-05	
CAD DWG FILE:	18003505-PH3-CG201.DWG	
DESIGNED BY:	HWI	
DRAWN BY:	DPA	
CHECKED BY:	CHK	
APPROVED BY:	APR	
COPYRIGHT:		

SHEET TITLE
DITCH PROFILES 2

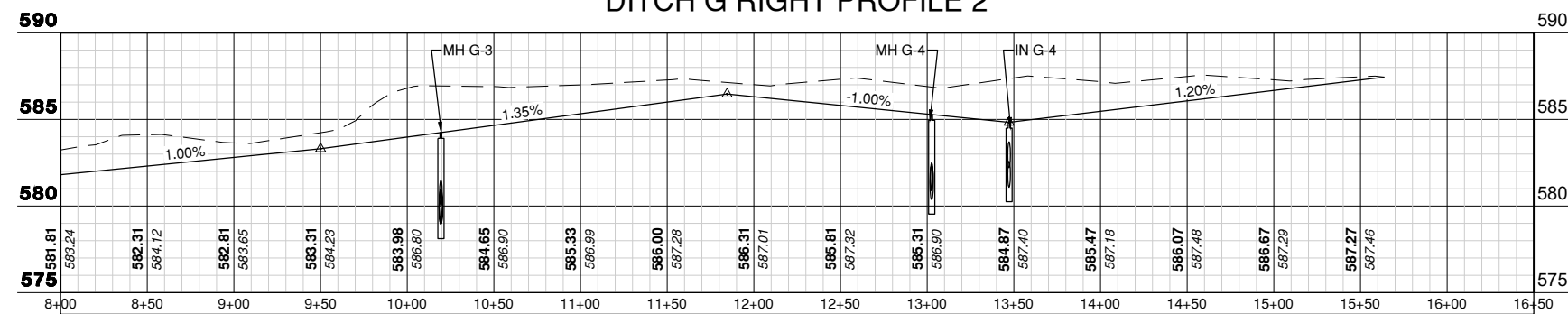
DITCH G LEFT PROFILE



DITCH G RIGHT PROFILE 1



DITCH G RIGHT PROFILE 2



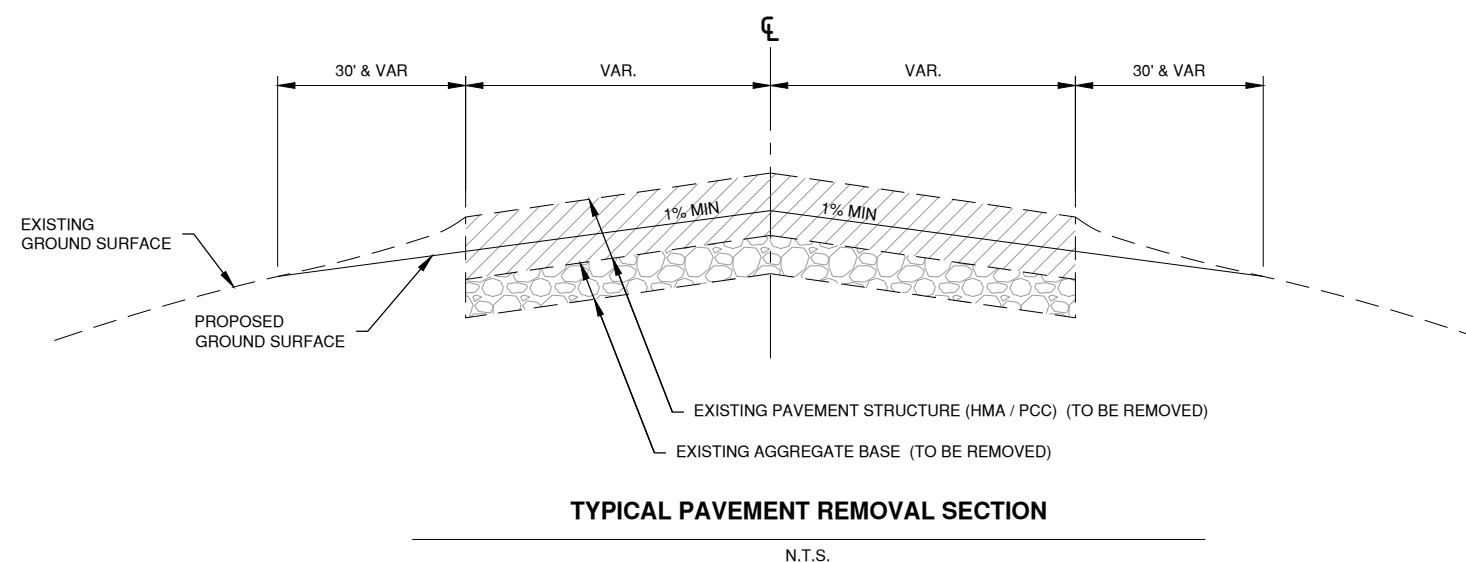
BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



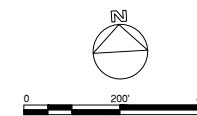
SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL



Path: K:\Springfield\18035-05_Rwy_18-36\Draw\Sheets\PH3-CG300.dwg
Date: Tuesday, April 26, 2022 12:45:17 PM

MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA022
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-CG300.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: CHK		
APPROVED BY: APR		
COPYRIGHT:		

SHEET TITLE
**GRADING &
DRAINAGE DETAILS 1**



BID SUBMITTAL
APRIL 22, 2022

**RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3**

OWNER



**SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL**

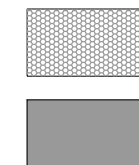
MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CG300.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

SHEET TITLE
**EMBANKMENT &
STOCKPILE AREAS 1**

CG302
SHEET 36 OF 91

- (A)** REMOVE 18/36 NORTH
11400 CY PAVEMENT / BASE REMOVAL
13700 CY BORROW NEEDED
- (B)** CONSTRUCT TAXIWAY G EAST
0 CY PAVEMENT / BASE REMOVAL
950 CY BORROW NEEDED



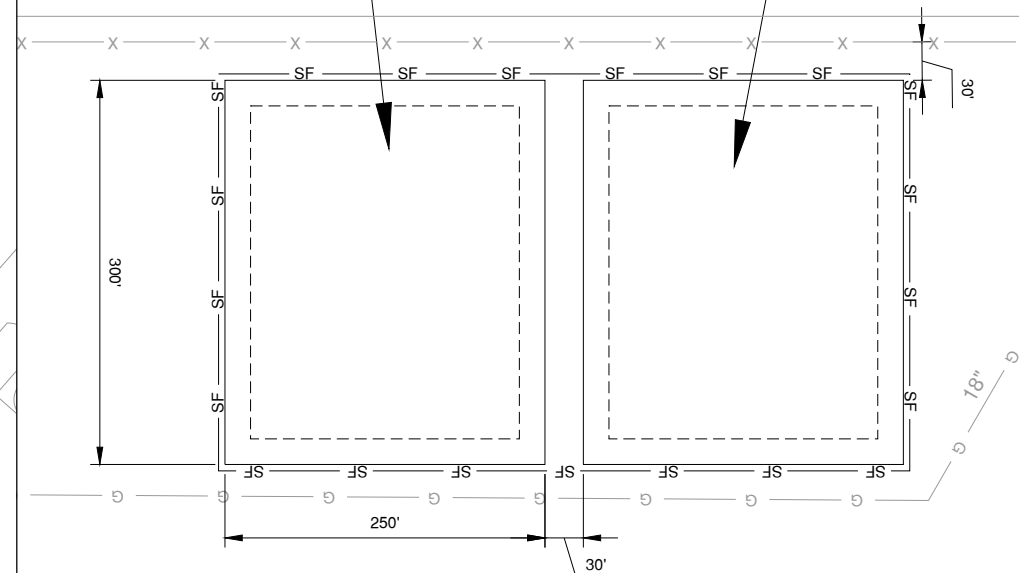
PAVEMENT REMOVAL
NEW PAVEMENT

NORTH STOCKPILE NOTES

1. PAVEMENT AND BASE REMOVED DURING THIS PHASE SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
2. MAINTAIN A 30' BUFFER BETWEEN THE EXISTING FENCE AND THE STOCKPILE.
3. MAINTAIN A MINIMUM 20' BUFFER BETWEEN THE EXISTING GAS MAIN AND THE STOCKPILE.
4. UPON PROJECT COMPLETION, THE CONTRACTOR SHALL DISPOSE OF ALL STOCKPILED PAVEMENT AND BASE MATERIALS AND RESTORE THE STOCKPILE AREA TO A CONDITION SUITABLE FOR AGRICULTURAL USE.
5. APPROXIMATE STOCKPILED MATERIALS (NORTH STOCKPILE) FROM PHASES 1 & 2:
 - 5.1. SOIL - 17,000 CY
 - 5.2. RUBBLE - 11,500 CY

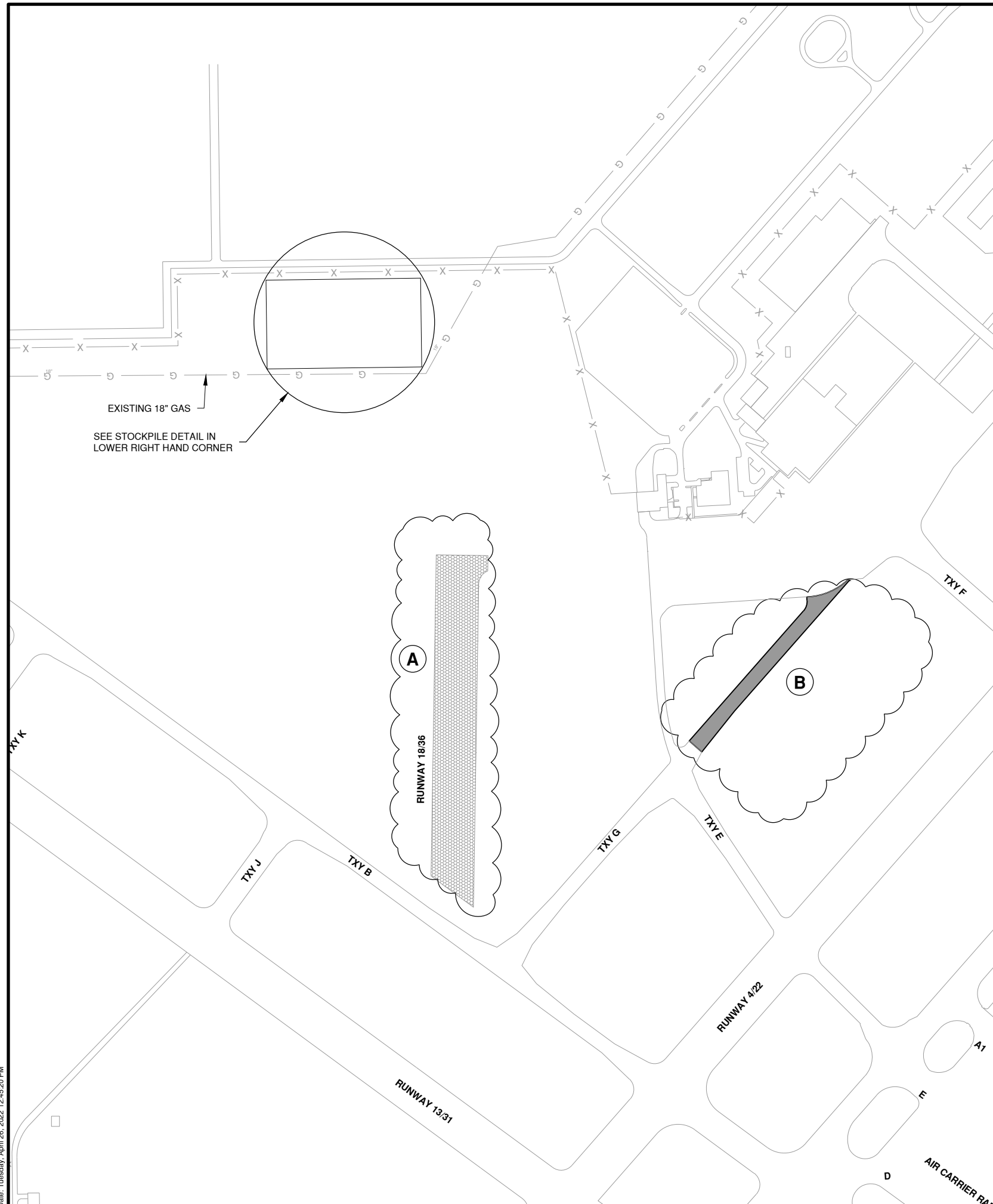
NORTH PCC/BASE STOCKPILE
SIDE SLOPES = 2H:1V MAX
MAX HEIGHT = 10'
CAPACITY = 24000 CY
MAX STORED VOL = ±19700 CY

NORTH SOIL STOCKPILE
SIDE SLOPES = 2H:1V MAX
MAX HEIGHT = 10'
CAPACITY = 24000 CY
MAX STORED VOL = ±17670 CY



NORTH STOCKPILE AREA

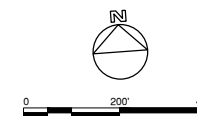
Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CG300.dwg
Date: Tuesday, April 26, 2022 12:45:20 PM





License No. 184-000613

CONSULTANTS



BID SUBMITTAL
APRIL 22, 2022

**RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3**

OWNER



**SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL**

MARK DATE DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX CA022

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-CG300.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

APPROVED BY: APR

COPYRIGHT:

SHEET TITLE

**EMBANKMENT &
STOCKPILE AREAS 2**

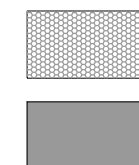
CG303

SHEET 37 OF 91

C TAXIWAY A
1300 CY PAVEMENT / BASE REMOVAL
650 CY UNCLASSIFIED EXCAVATION

D REMOVE 18/36 SOUTH
7700 CY PAVEMENT / BASE REMOVAL
9200 CY BORROW NEEDED

E REMOVE TAXIWAY C
3450 CY PAVEMENT / BASE REMOVAL
4300 CY BORROW NEEDED



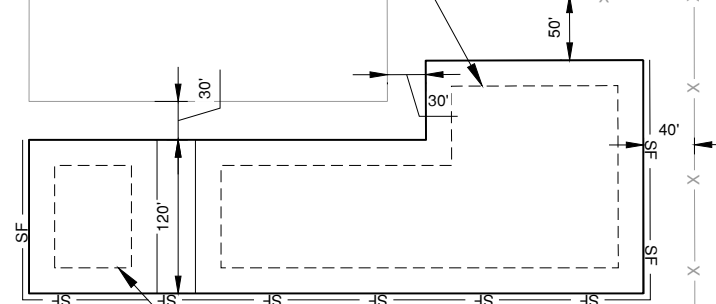
PAVEMENT REMOVAL

NEW PAVEMENT

SOUTH STOCKPILE NOTES

- PAVEMENT AND BASE REMOVED DURING THIS PHASE SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
- MAINTAIN A 30' BUFFER BETWEEN THE EXISTING FENCE AND THE STOCKPILE.
- MAINTAIN A MINIMUM 20' BUFFER BETWEEN THE EXISTING GAS MAIN AND THE STOCKPILE.
- UPON PROJECT COMPLETION, THE CONTRACTOR SHALL DISPOSE OF ALL STOCKPILED PAVEMENT AND BASE MATERIALS AND RESTORE THE STOCKPILE AREA TO A CONDITION SUITABLE FOR AGRICULTURAL USE.
- ANTICIPATED STOCKPILED MATERIALS (SOUTH STOCKPILE) FROM PHASES 1 & 2:
 - 5.1. SOIL - 2,000 CY
 - 5.2. RUBBLE - 14,000 CY

SOUTH PCC/BASE STOCKPILE
SIDE SLOPES = 2H:1V MAX
MAX HEIGHT = 10'
CAPACITY = 15,800 CY
MAX STORED VOL = ±14,700 CY



SOUTH SOIL STOCKPILE
SIDE SLOPES = 2H:1V MAX
MAX HEIGHT = 10'
CAPACITY = 3,000 CY
MAX STORED VOL = ±2,000 CY

SOUTH STOCKPILE AREA



THIS BAR IS EQUAL TO 2' AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



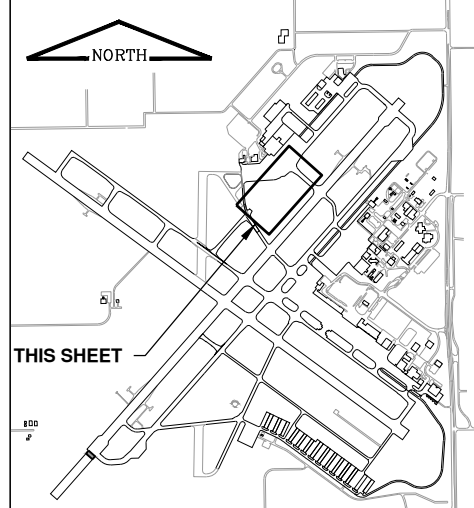
SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

NOTES

LEGEND

EXISTING	PROPOSED	DESCRIPTION
— ST —	P-XX	STORM SEWER
— UD —	UD-XX	UNDERDRAIN
■	IN - #	INLET & NUMBER
○	MH - #	MANHOLE & NUMBER
○	CO	UNDERDRAIN CLEANOUT & NUMBER
○	CS	UNDERDRAIN COLLECTION STRUCTURE
○	DC	UNDERDRAIN DIRECT CONNECTION
○	○	UNDERDRAIN NODE
///		PIPE REMOVAL

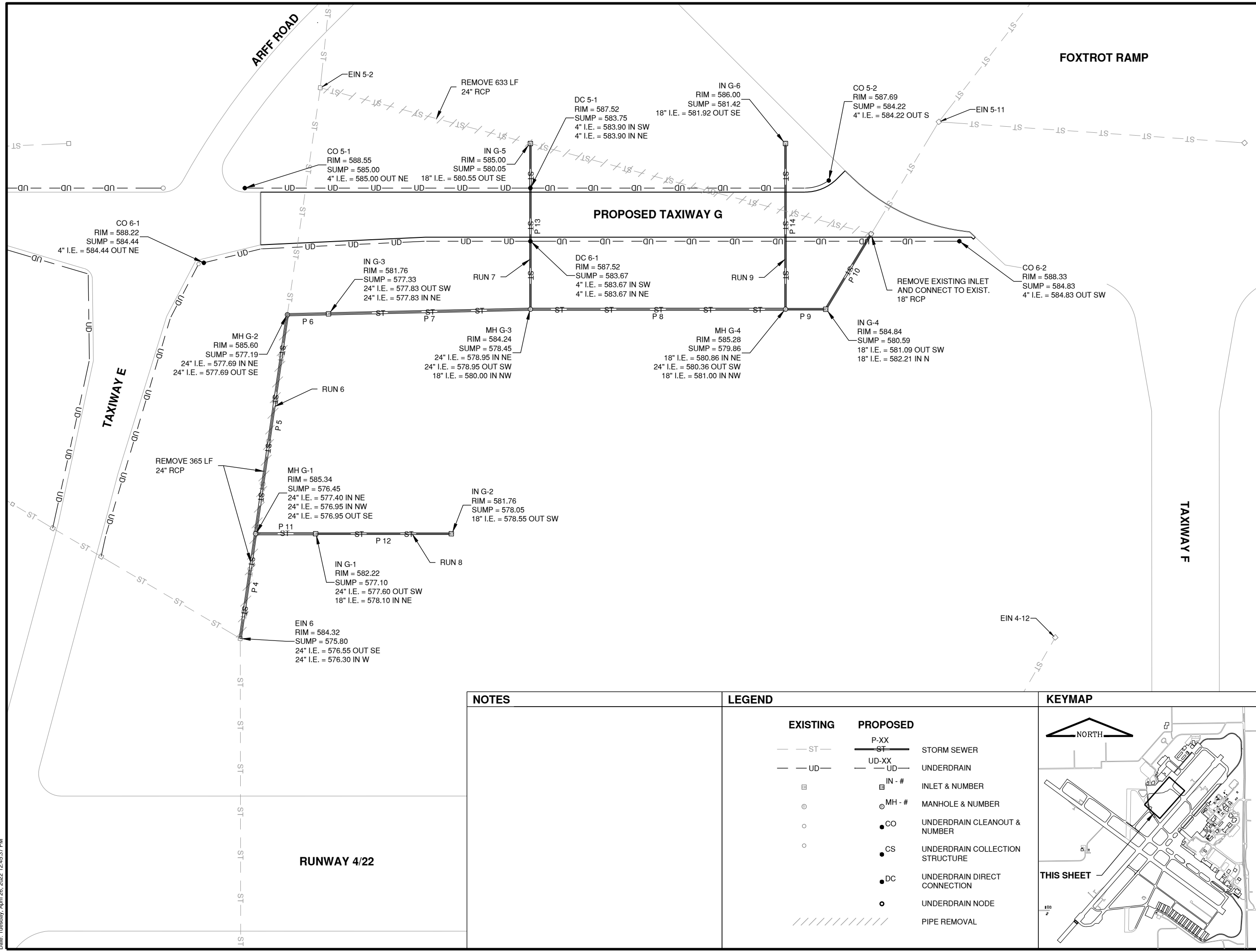
KEYMAP

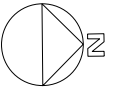


MARK	DATE	DESCRIPTION

STORM SEWER & UNDERDRAIN PLAN TXY G WEST

Path: K:\Springfield\18035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CU100.dwg
Date: Tuesday, April 26, 2022 12:45:37 PM





THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

**RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3**

OWNER



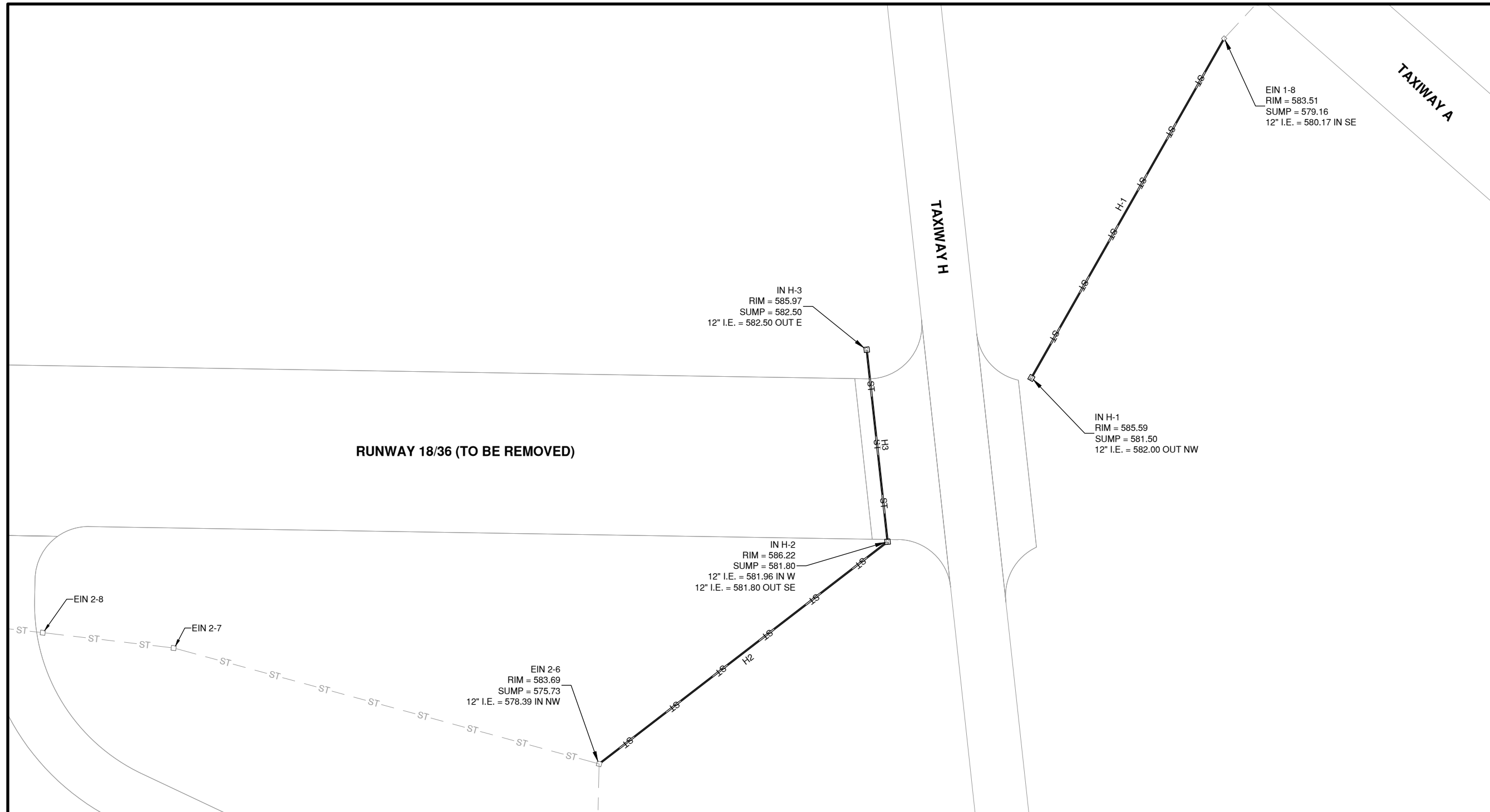
SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CU100.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

**STORM SEWER &
UNDERDRAIN PLAN
RWY 1836 S**

CU102
SHEET 39 OF 91



IN H-3
RIM = 585.97
SUMP = 582.50
12" I.E. = 582.50 OUT E

EIN 1-8
RIM = 583.51
SUMP = 579.16
12" I.E. = 580.17 IN SE

IN H-1
RIM = 585.59
SUMP = 581.50
12" I.E. = 582.00 OUT NW

IN H-2
RIM = 586.22
SUMP = 581.80
12" I.E. = 581.96 IN W
12" I.E. = 581.80 OUT SE

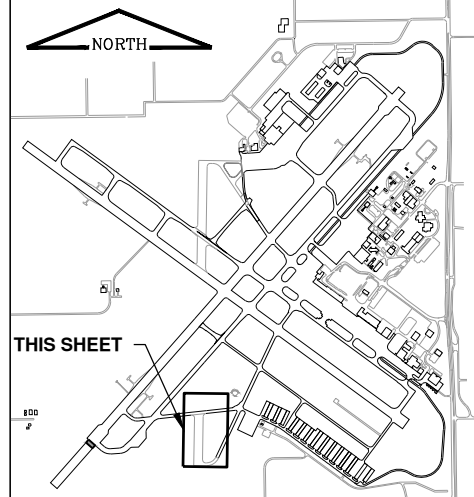
EIN 2-6
RIM = 583.69
SUMP = 575.73
12" I.E. = 578.39 IN NW

NOTES

LEGEND

EXISTING	PROPOSED	
— ST —	P-XX	STORM SEWER
— UD —	UD-XX	UNDERDRAIN
⊠	⊠	INLET & NUMBER
⊙	⊙	MANHOLE & NUMBER
○	● CO	UNDERDRAIN CLEANOUT & NUMBER
○	● CS	UNDERDRAIN COLLECTION STRUCTURE
○	● DC	UNDERDRAIN DIRECT CONNECTION
○	○	UNDERDRAIN NODE
////	////	PIPE REMOVAL

KEYMAP



Path: K:\Springfield\18035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CU100.dwg
Date: Tuesday, April 26, 2022 12:45:51 PM



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX CA022

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-CU200.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

APPROVED BY: APR

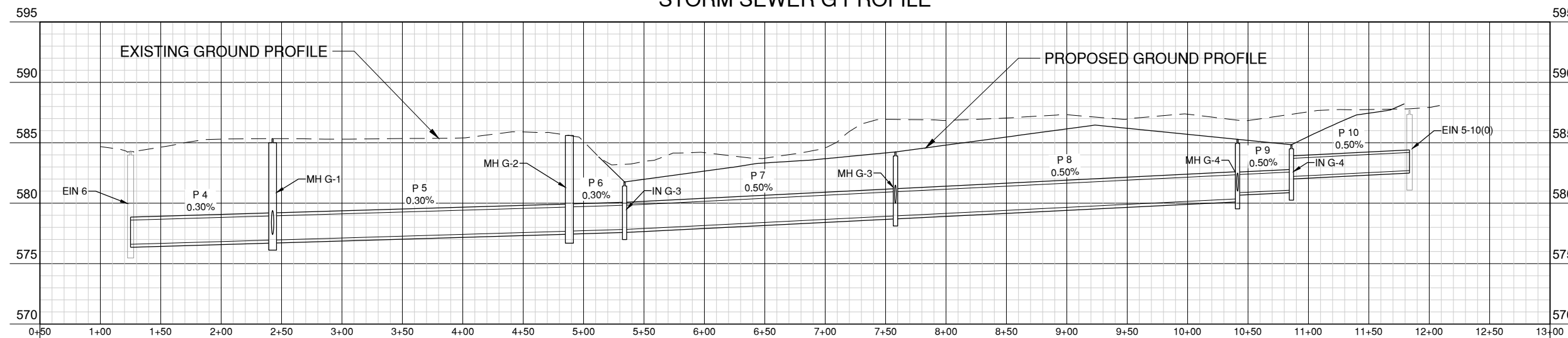
COPYRIGHT:

SHEET TITLE

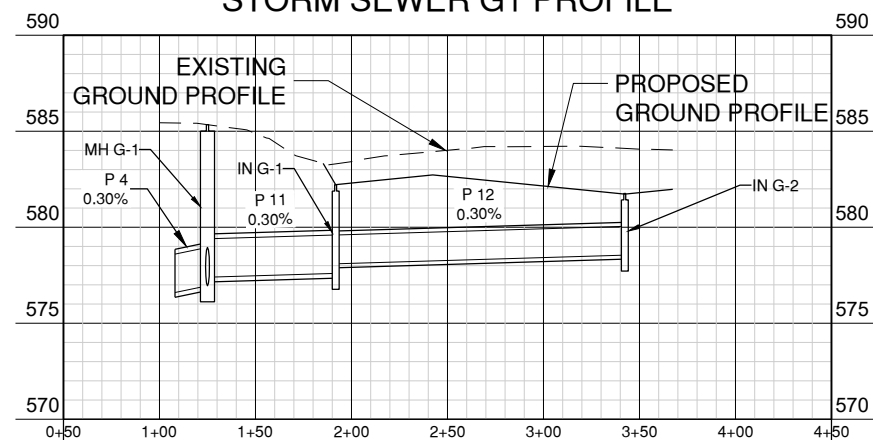
**STORM SEWER
PROFILES 1**

CU201
SHEET 40 OF 91

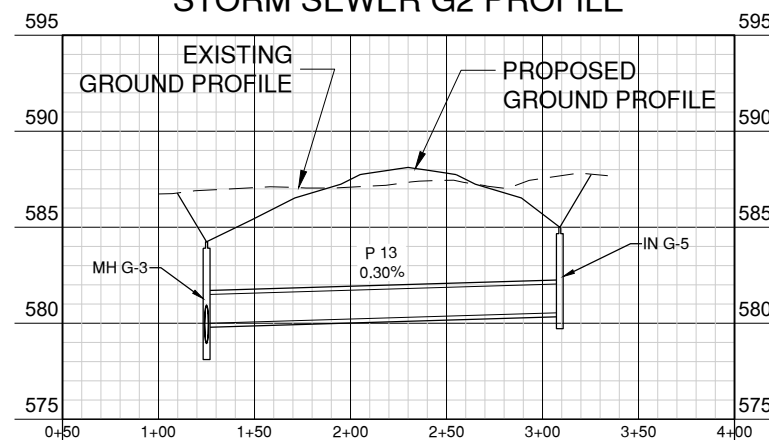
STORM SEWER G PROFILE



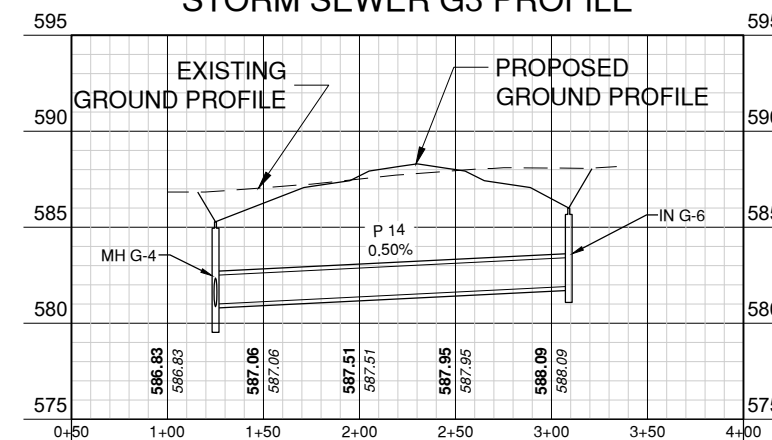
STORM SEWER G1 PROFILE

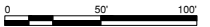


STORM SEWER G2 PROFILE



STORM SEWER G3 PROFILE





THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX **CA022**

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-CU200.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

APPROVED BY: APR

COPYRIGHT:

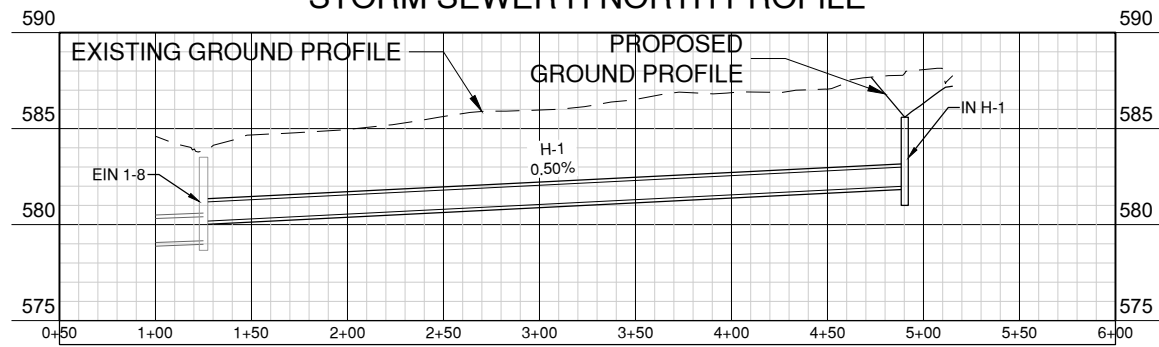
SHEET TITLE

**STORM SEWER
PROFILES 2**

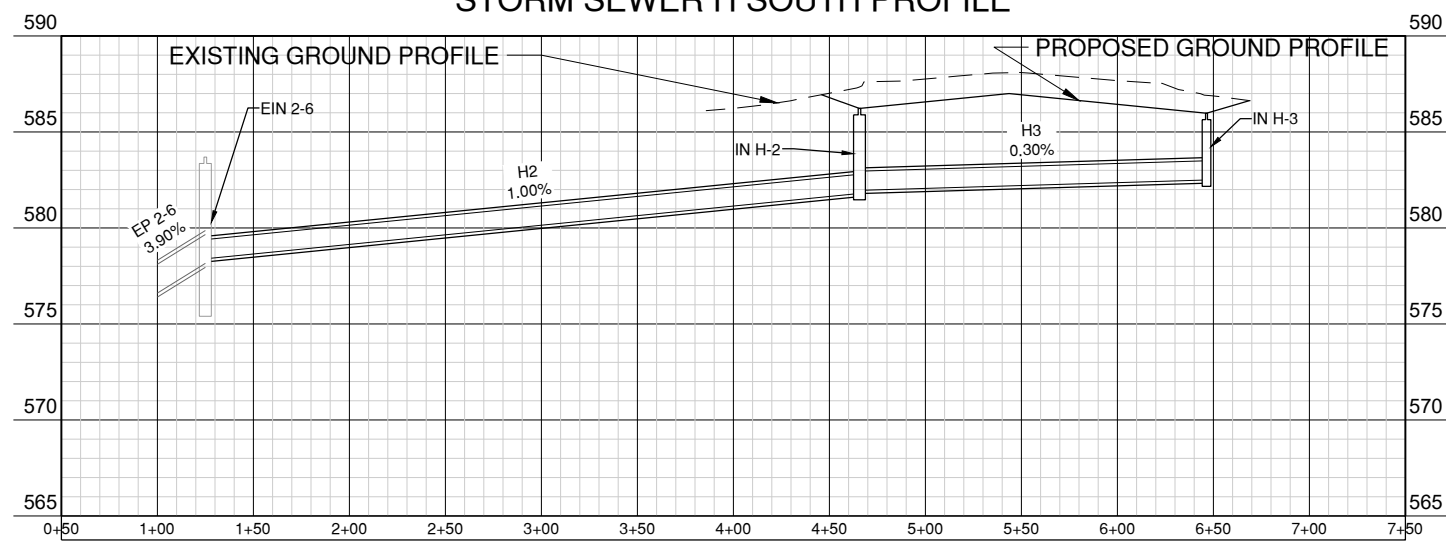
CU202

SHEET **41** OF **91**

STORM SEWER H NORTH PROFILE



STORM SEWER H SOUTH PROFILE





License No. 184-000613

CONSULTANTS

0 50 100

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK | DATE | DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX CA022

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-CU200.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

APPROVED BY: APR

COPYRIGHT:

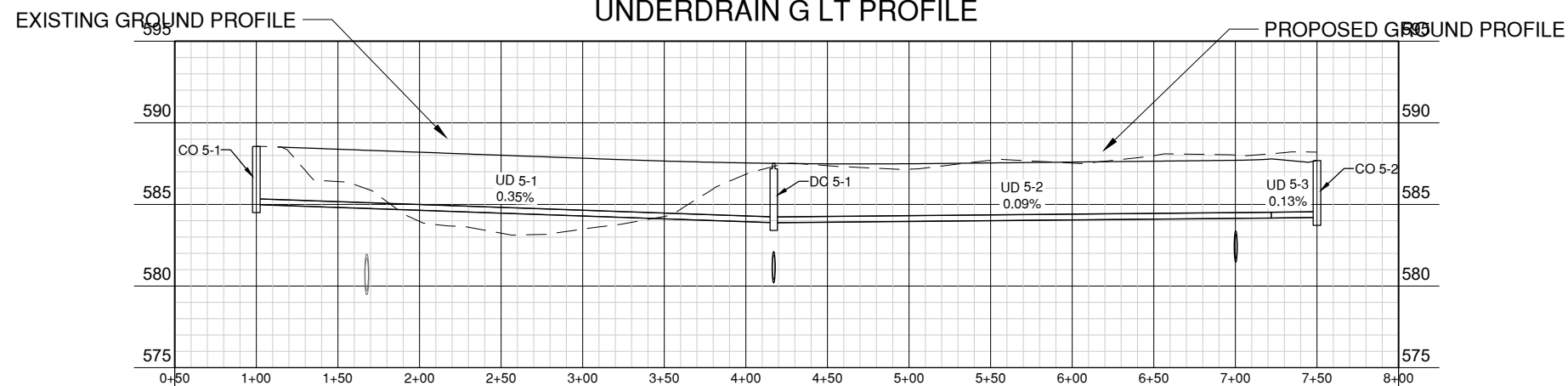
SHEET TITLE

STORM SEWER
PROFILES 3

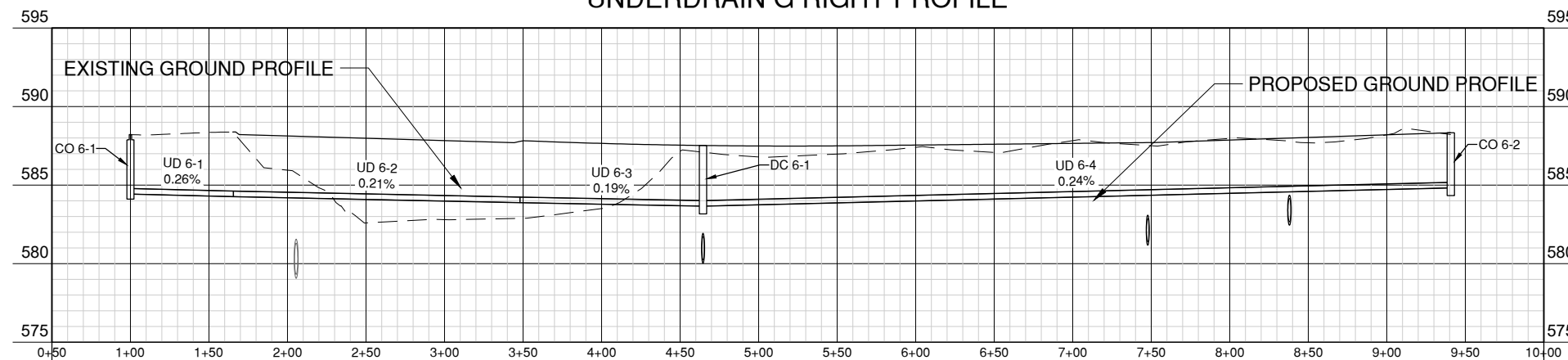
CU203

SHEET 42 OF 91

UNDERDRAIN G LT PROFILE



UNDERDRAIN G RIGHT PROFILE





License No. 184-000613

CONSULTANTS

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK | DATE | DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX **CA022**

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-CU400.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

APPROVED BY: APR

COPYRIGHT:

SHEET TITLE

**STORM SEWER &
UNDERDRAIN
SCHEDULES**

CU401
SHEET **43** OF **91**

STRUCTURE TABLE G EAST STORM SEWER BB			PIPE SCHEDULE G EAST STORM SEWER BB							
STRUCTURE NAME & TYPE	STRUCTURE DETAILS	HORIZONTAL CONTROL	PIPE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	INVERT	INVERT	LENGTH (FT)	SLOPE	TYPE
EIN-5 CONNECT TO EXISTING PIPE	RIM = 585.41 SUMP = 582.70 P 10 INV OUT = 582.70	TAXIWAY G STA 25+40.80 OFFSET 21.35 R	P 4	MH G-1	EIN-6	576.95	576.60	118	0.30%	RCP - 24"
EIN-6 CONNECT TO EXIST. STRUCTURE	RIM = 579.85 SUMP = 576.60 P 4 INV IN = 576.60	TAXIWAY E STA 16+88.10 OFFSET -203.15 L	P 5	MH G-2	MH G-1	577.69	576.95	246	0.30%	RCP - 24"
IN G-1 NEW 36" INLET	RIM = 582.22 SUMP = 577.10 P 12 INV IN = 578.10 P 11 INV OUT = 577.60	TAXIWAY E STA 15+54.18 OFFSET -254.19 L	P 6	IN G-3	MH G-2	577.83	577.69	46	0.30%	RCP - 24"
IN G-2 NEW 36" INLET	RIM = 581.76 SUMP = 578.05 P 12 INV OUT = 578.55	TAXIWAY E STA 15+15.59 OFFSET -399.76 L	P 7	MH G-4	MH G-3	580.36	578.95	283	0.50%	RCP - 24"
IN G-3 NEW 36" INLET	RIM = 581.76 SUMP = 577.33 P 7 INV IN = 577.83 P 6 INV OUT = 577.83	TAXIWAY E STA 13+14.56 OFFSET -205.45 L	P 8	IN G-4	MH G-4	581.09	580.86	45	0.50%	RCP - 18"
IN G-4 NEW 36" INLET	RIM = 584.84 SUMP = 580.59 P 10 INV IN = 582.21 P 9 INV OUT = 581.09	TAXIWAY G STA 24+90.31 OFFSET 105.00 R	P 9	IN G-4	MH G-4	582.70	582.21	98	0.50%	RCP - 18"
IN G-5 NEW 36" INLET	RIM = 585.00 SUMP = 580.05 P 13 INV OUT = 580.55	TAXIWAY G STA 21+62.50 OFFSET -79.00 L	P 11	MH G-1	IN G-1	577.40	577.60	67	0.30%	RCP - 24"
IN G-6 NEW 36" INLET	RIM = 586.00 SUMP = 581.42 P 14 INV OUT = 581.92	TAXIWAY G STA 24+45.67 OFFSET -79.00 L	P 12	IN G-2	IN G-1	578.55	578.10	151	0.30%	RCP - 18"
MH G-1 NEW 72" DIA MANHOLE	RIM = 585.34 SUMP = 576.45 P 11 INV IN = 577.40 P 5 INV IN = 576.95 P 4 INV OUT = 576.95	TAXIWAY E STA 15+71.31 OFFSET -189.69 L	P 13	IN G-5	MH G-3	580.55	580.00	184	0.30%	RCP - 18"
MH G-2 NEW 72" DIA MANHOLE	RIM = 585.60 SUMP = 577.19 P 6 INV IN = 577.69 P 5 INV OUT = 577.69	TAXIWAY G STA 18+92.67 OFFSET 111.12 R	P 14	IN G-6	MH G-4	581.92	581.00	184	0.50%	RCP - 18"
MH G-3 NEW 72" DIA MANHOLE	RIM = 584.24 SUMP = 578.45 P 8 INV IN = 578.95 P 13 INV IN = 580.00 P 7 INV OUT = 578.95	TAXIWAY G STA 21+62.50 OFFSET 105.00 R								
MH G-4 NEW 72" DIA MANHOLE	RIM = 585.28 SUMP = 579.86 P 9 INV IN = 580.86 P 14 INV IN = 581.00 P 8 INV OUT = 580.36	TAXIWAY G STA 24+45.67 OFFSET 105.00 R								

STRUCTURE TABLE STORM SEWER H LT A1			PIPE SCHEDULE STORM SEWER H LT A1							
STRUCTURE NAME & TYPE	STRUCTURE DETAILS	HORIZONTAL CONTROL	PIPE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	INVERT	INVERT	LENGTH (FT)	SLOPE	TYPE
EIN 1-8 EXISTING INLET	RIM = 583.51 SUMP = 579.16 H-1 INV IN = 580.17	TAXIWAY H STA 16+42.54 OFFSET -284.97 L	H-1	IN H-1	EIN 1-8	582.00	580.17	365	0.50%	CONCRETE PIPE - 12"
IN H-1 NEW 36" INLET	RIM = 585.59 SUMP = 581.50 H-1 INV OUT = 582.00	TAXIWAY H STA 19+38.84 OFFSET -71.37 L								

STRUCTURE TABLE STORM SEWER H RT A1			PIPE SCHEDULE STORM SEWER H RT A1							
STRUCTURE NAME & TYPE	STRUCTURE DETAILS	HORIZONTAL CONTROL	PIPE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	INVERT	INVERT	LENGTH (FT)	SLOPE	TYPE
EIN 2-6 EXISTING INLET	RIM = 583.69 SUMP = 575.73 H2 INV IN = 578.39	TAXIWAY H STA 22+54.25 OFFSET 369.77 R	H2	IN H-2	EIN 2-6	581.80	578.39	341	1.00%	CONCRETE PIPE - 12"
IN H-2 NEW 36" INLET	RIM = 586.22 SUMP = 581.80 H3 INV IN = 581.96 H2 INV OUT = 581.80	TAXIWAY H STA 20+76.71 OFFSET 79.00 R	H3	IN H-3	IN H-2	582.50	581.96	181	0.30%	CONCRETE PIPE - 12"
IN H-3 NEW 36" INLET	RIM = 585.97 SUMP = 582.50 H3 INV OUT = 582.50	TAXIWAY H STA 18+95.92 OFFSET 79.00 R								

STRUCTURE TABLE G EAST UNDERDRAIN BB		
STRUCTURE NAME & TYPE	STRUCTURE DETAILS	HORIZONTAL CONTROL
CO 5-1 UNDERDRAIN CLEANOUT TYPE 1	RIM = 588.55 SUMP = 585.00 UD 5-1 INV OUT = 585.00	TAXIWAY G STA 18+45.42 OFFSET -29.50 L
CO 5-2 UNDERDRAIN CLEANOUT TYPE 1	RIM = 587.69 SUMP = 584.22 UD 5-3 INV OUT = 584.22	TAXIWAY G STA 24+93.68 OFFSET -37.87 L
CO 6-1 UNDERDRAIN CLEANOUT TYPE 1	RIM = 588.22 SUMP = 584.44 UD 6-1 INV OUT = 584.44	TAXIWAY G STA 17+99.96 OFFSET 53.47 R
CO 6-2 UNDERDRAIN CLEANOUT TYPE 1	RIM = 588.33 SUMP = 584.83 UD 6-4 INV OUT = 584.83	TAXIWAY G STA 26+38.67 OFFSET 29.50 R
DC 5-1 DIRECT CONNECTION	RIM = 587.52 SUMP = 583.75 UD 5-1 INV IN = 583.90 UD 5-2 INV IN = 583.90	TAXIWAY G STA 21+62.50 OFFSET -29.50 L
DC 6-1 DIRECT CONNECTION	RIM = 587.52 SUMP = 583.67 UD 6-3 INV IN = 583.67 UD 6-4 INV IN = 583.67	TAXIWAY G STA 21+62.50 OFFSET 29.50 R
NODE 5-1	RIM = 585.53 SUMP = 584.18 UD 5-3 INV IN = 584.18 UD 5-2 INV OUT = 584.18	TAXIWAY G STA 24+67.38 OFFSET -29.50 L
NODE 6-1	RIM = 585.62 SUMP = 584.27 UD 6-1 INV IN = 584.27 UD 6-2 INV OUT = 584.27	TAXIWAY G STA 18+63.66 OFFSET 37.97 R
NODE 6-2	RIM = 585.24 SUMP = 583.89 UD 6-2 INV IN = 583.89 UD 6-3 INV OUT = 583.89	TAXIWAY G STA 20+45.90 OFFSET 29.50 R

PIPE SCHEDULE G EAST UNDERDRAIN BB							
PIPE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	INVERT	INVERT	LENGTH (FT)	SLOPE	TYPE
UD 5-1	CO 5-1	DC 5-1	585.00	583.90	317	0.35%	PERFORATED PVC - 4"
UD 5-2	NODE 5-1	DC 5-1	584.18	583.90	305	0.09%	PERFORATED PVC - 4"
UD 5-3	CO 5-2	NODE 5-1	584.22	584.18	28	0.13%	PERFORATED PVC - 4"
UD 6-1	CO 6-1	NODE 6-1	584.44	584.27	66	0.26%	PERFORATED PVC - 4"
UD 6-2	NODE 6-1	NODE 6-2	584.27	583.89	182	0.21%	PERFORATED PVC - 4"
UD 6-3	NODE 6-2	DC 6-1	583.89	583.67	117	0.19%	PERFORATED PVC - 4"
UD 6-4	CO 6-2	DC 6-1	584.83	583.67	476	0.24%	PERFORATED PVC - 4"

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

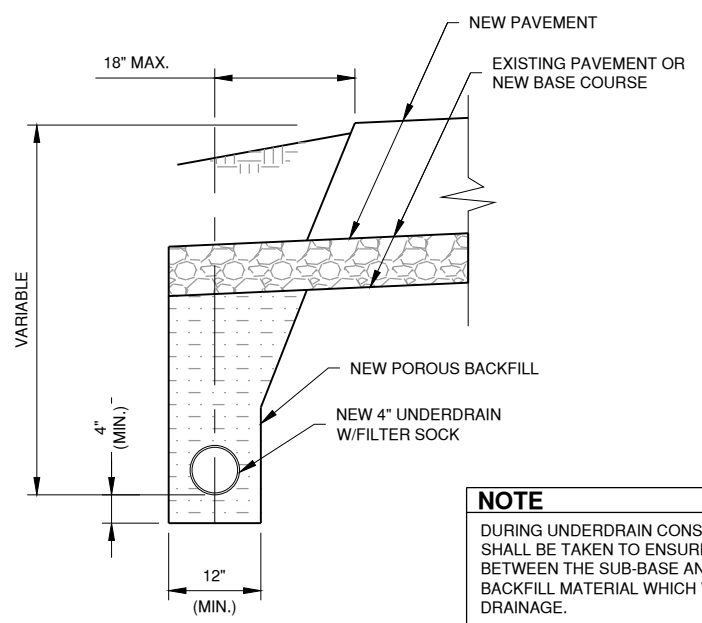
OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

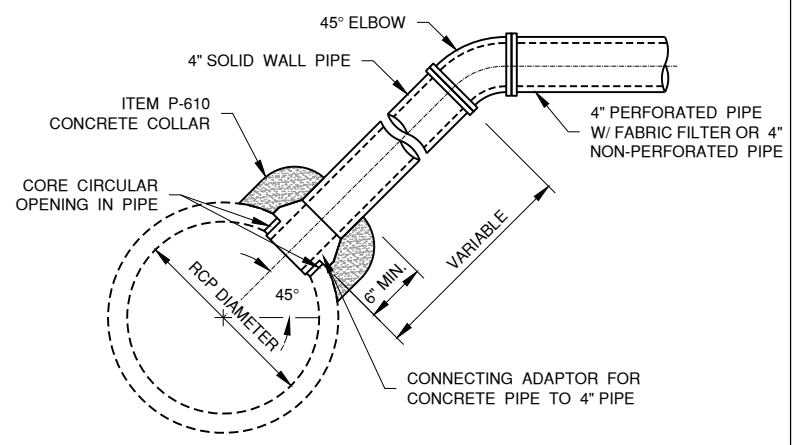
MARK	DATE	DESCRIPTION

SHEET TITLE
**UNDERDRAIN
DETAILS**



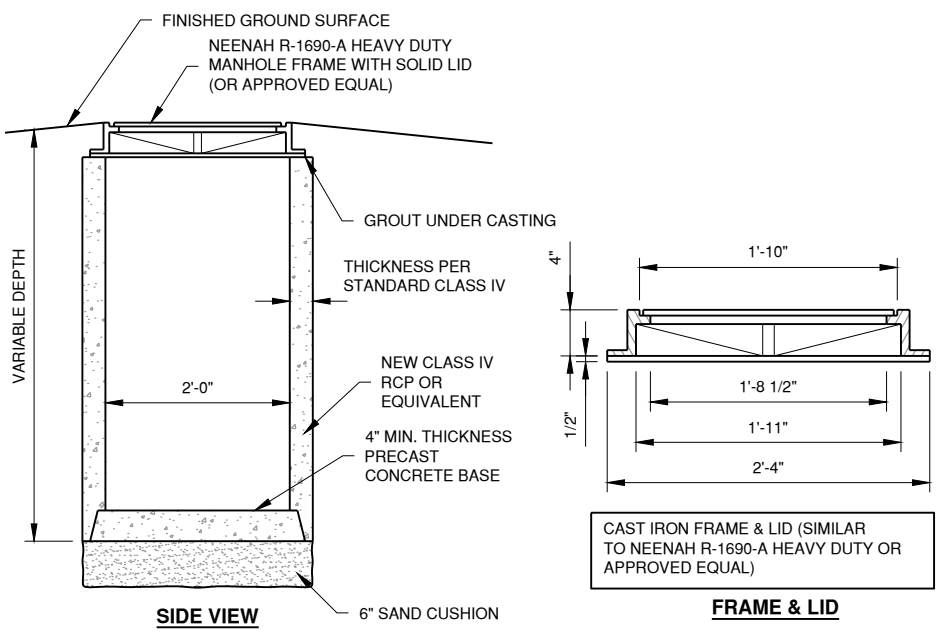
NOTE
DURING UNDERDRAIN CONSTRUCTION, CARE SHALL BE TAKEN TO ENSURE GOOD DRAINAGE BETWEEN THE SUB-BASE AND THE POROUS BACKFILL MATERIAL WHICH WILL AFFECT DRAINAGE.

1 UNDERDRAIN PAVEMENT EDGE
N.T.S.

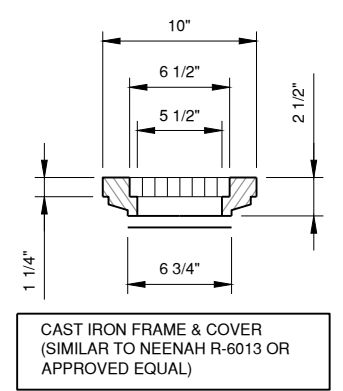


NOTE
ANGLE MEASURED FROM HORIZONTAL

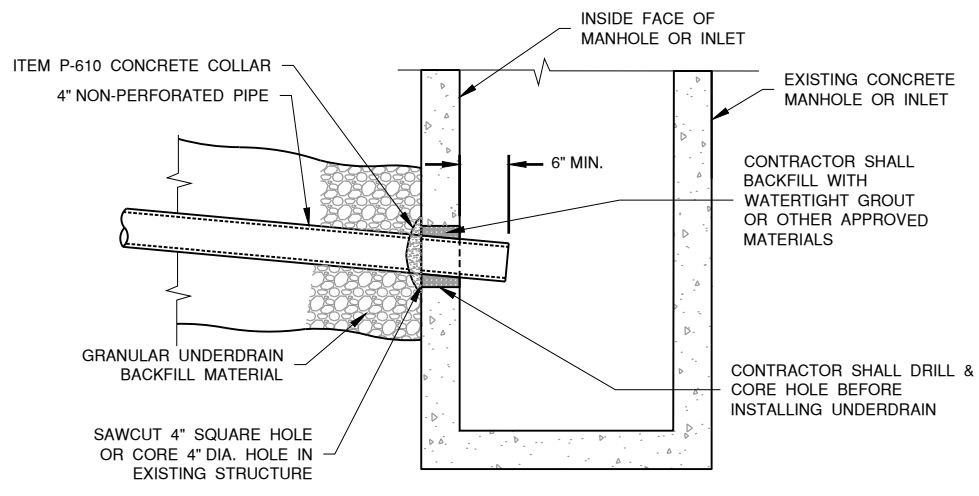
2 UNDERDRAIN DIRECT CONNECTION
N.T.S.



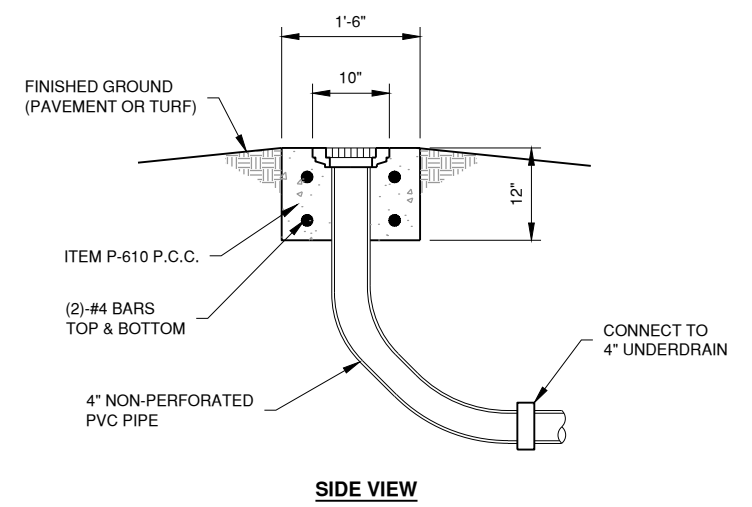
3 UNDERDRAIN COLLECTION STRUCTURE
N.T.S.



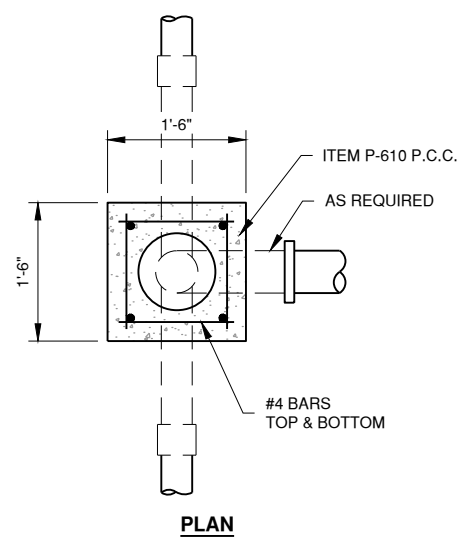
FRAME & LID



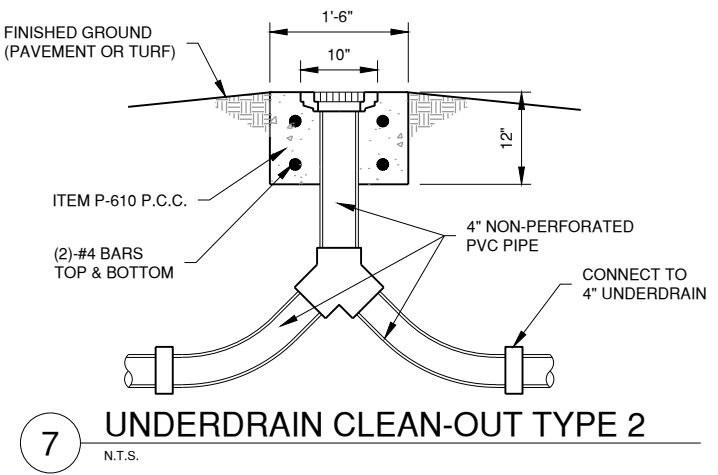
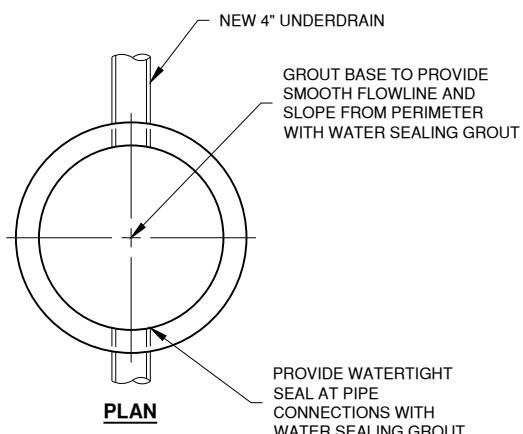
4 UNDERDRAIN DIRECT CONNECTION TO STRUCTURE
N.T.S.



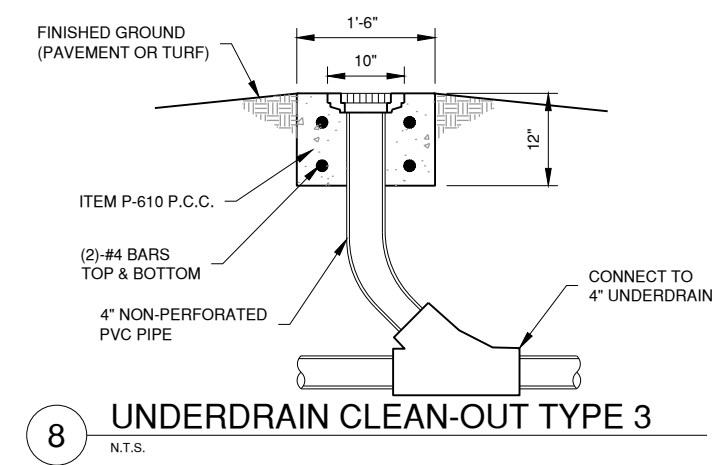
6 UNDERDRAIN CLEAN-OUT TYPE 1
N.T.S.



5 UNDERDRAIN CLEAN-OUT
N.T.S.



7 UNDERDRAIN CLEAN-OUT TYPE 2
N.T.S.



8 UNDERDRAIN CLEAN-OUT TYPE 3
N.T.S.

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



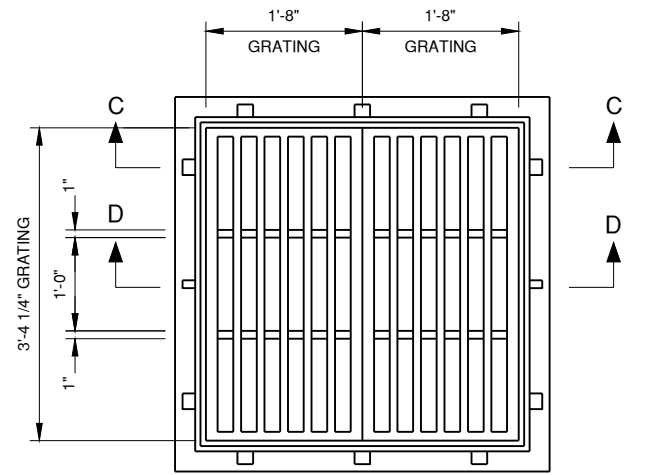
SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

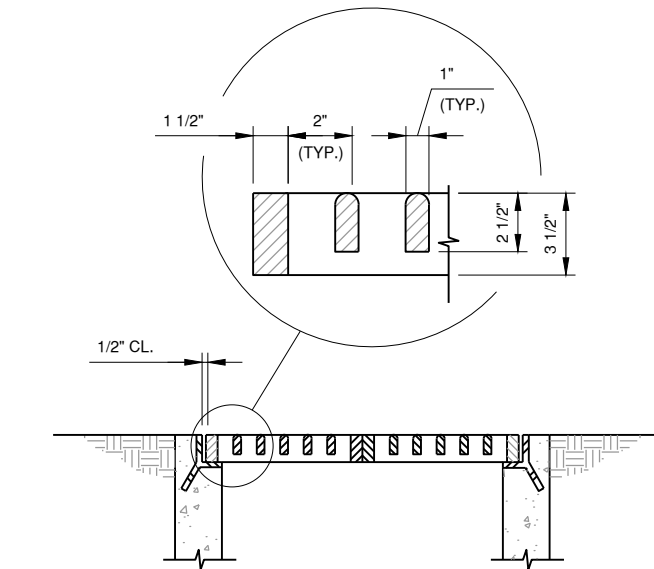
AIP PROJ. NO:	3-17-0096-XX	CA022
IL PROJ. NO:	SPI-4943	
CMT PROJECT NO:	180035-05	
CAD DWG FILE:	18003505-PH3-CU500.DWG	
DESIGNED BY:	HWI	
DRAWN BY:	DPA	
CHECKED BY:	CHK	
APPROVED BY:	APR	
COPYRIGHT:		

SHEET TITLE
**STORM SEWER
DETAILS 2**

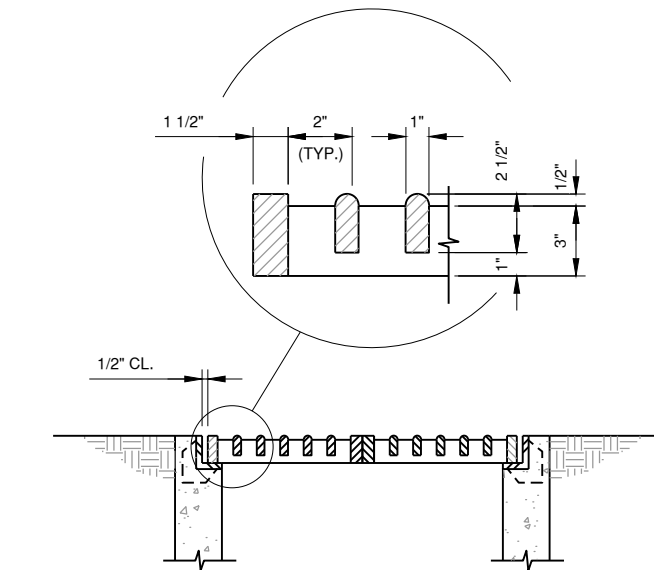
CU502
SHEET 45 OF 91



PLAN
N.T.S.

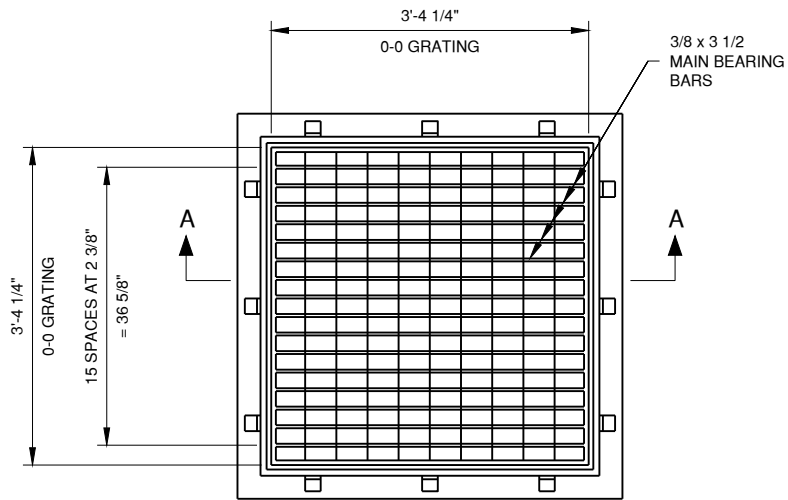


SECTION C-C
N.T.S.

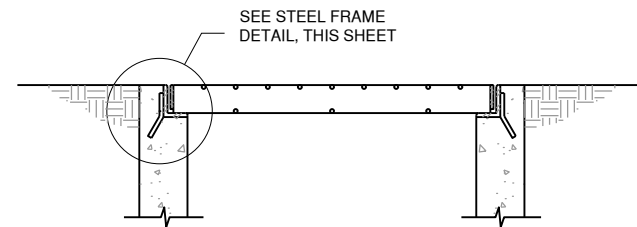


SECTION D-D
N.T.S.

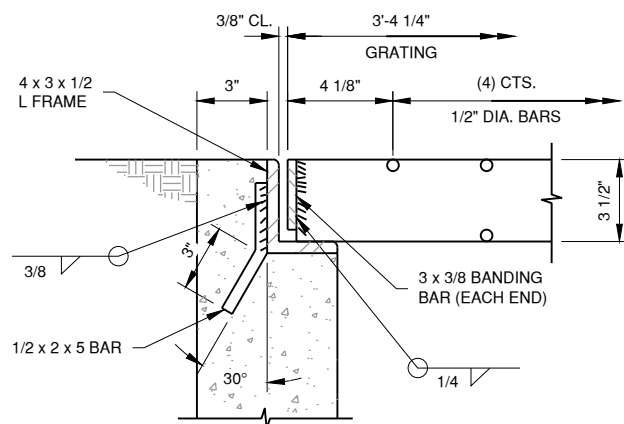
4 CAST GRATE DETAILS
N.T.S.



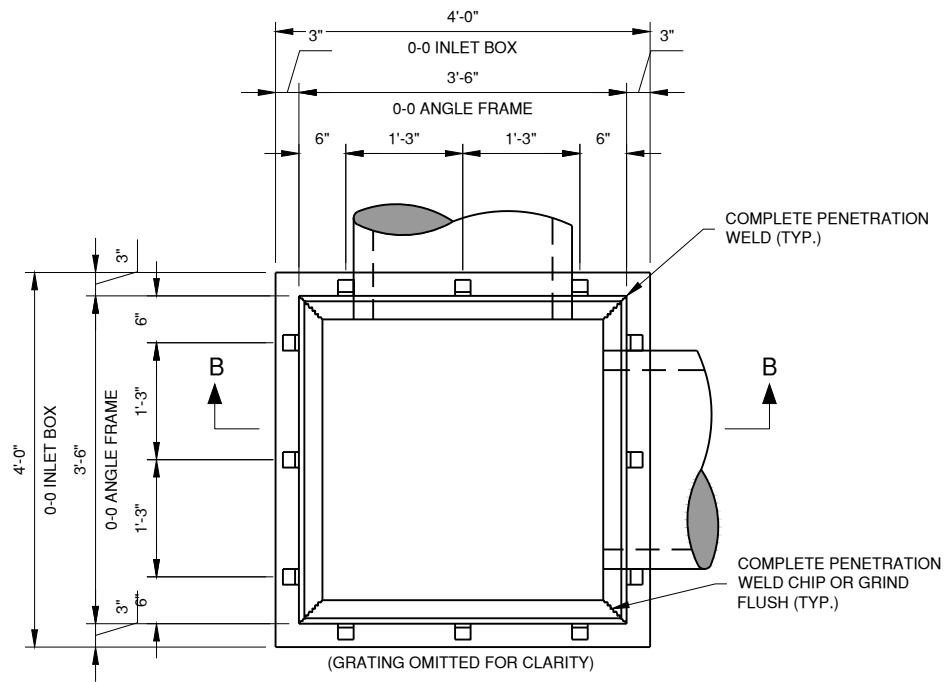
PLAN
N.T.S.



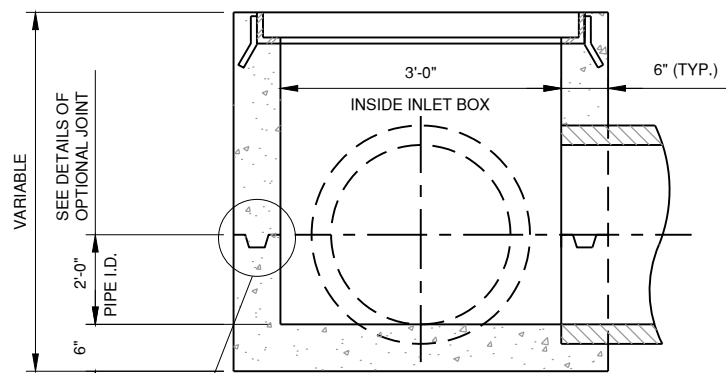
SECTION A-A
N.T.S.



3 STEEL FRAME & GRATE DETAIL
N.T.S.



PLAN
N.T.S.

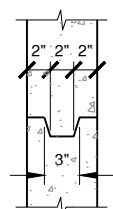


SECTION B-B
N.T.S.

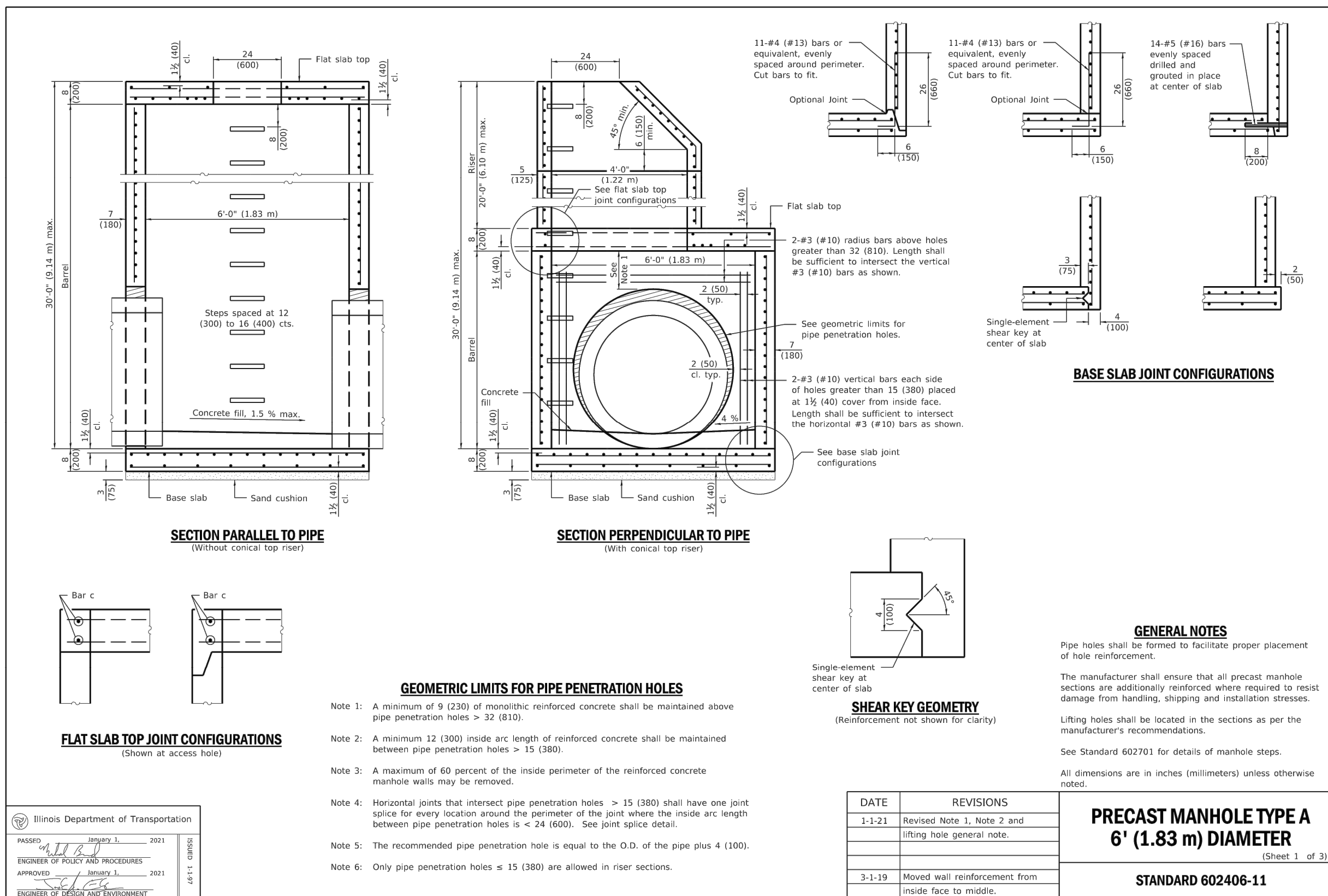
36" INLET NOTE

INLET IS AN IDOT STD. 542546, FLUSH INLET BOX FOR MEDIAN. FRAME & GRATES ARE NEENAH R-3807 OR APPROVED EQUAL.

1 36" INLET DETAIL
N.T.S.



2 OPTIONAL JOINT DETAIL
N.T.S.



BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA022
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-CU500.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: CHK		
APPROVED BY: APR		
COPYRIGHT:		

SHEET TITLE
**STORM SEWER
DETAILS 3**

CU503
SHEET 46 OF 91

Illinois Department of Transportation

PASSED January 1, 2021
ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2021
ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 46-1-17

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX **CA022**

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-CU500.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

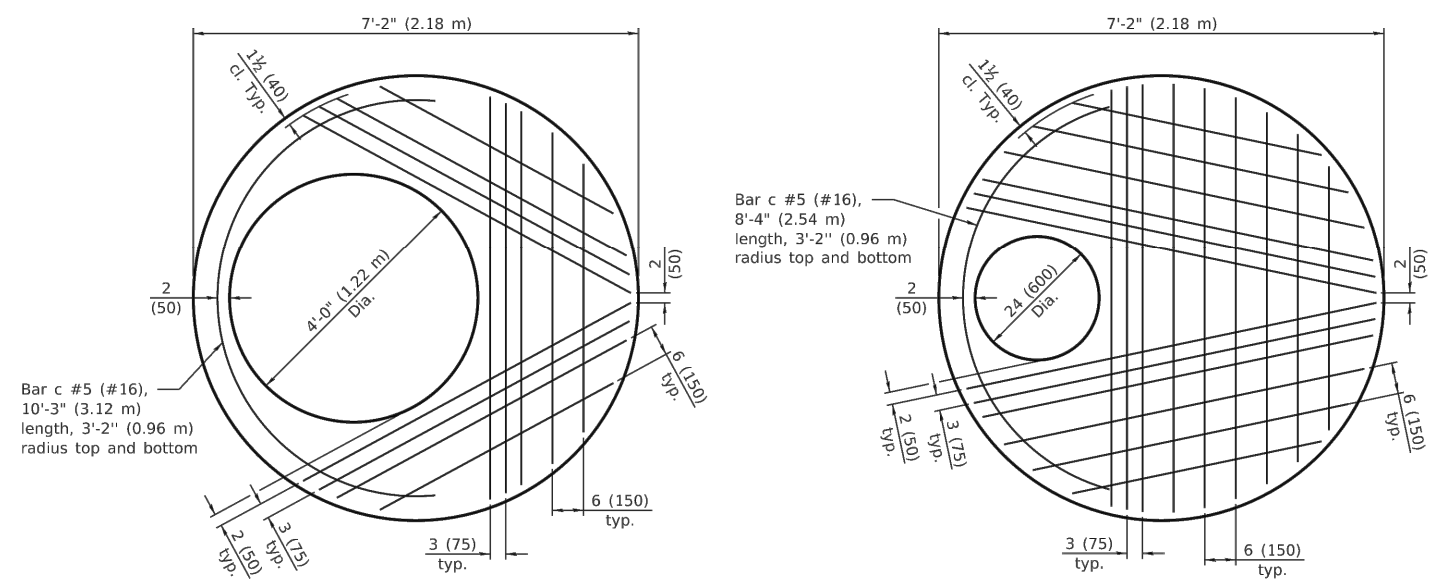
APPROVED BY: APR

COPYRIGHT:

SHEET TITLE

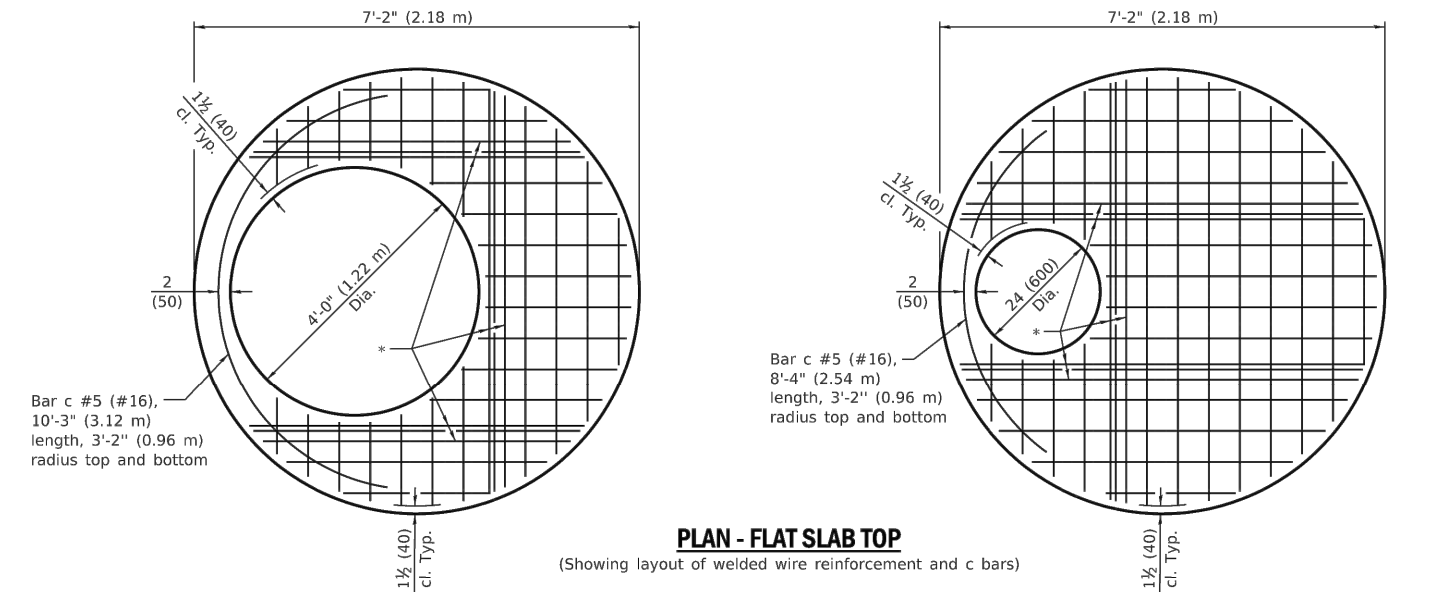
**STORM SEWER
DETAILS 4**

CU504
SHEET 47 OF 91



PLAN - FLAT SLAB TOP

(Showing layout of bottom reinforcement bars and c bars)



PLAN - FLAT SLAB TOP

(Showing layout of welded wire reinforcement and c bars)

* #5 (#16) bars for risers ≤ 10 ft. (3.05 m) tall or
#6 (#19) bars for risers > 10 ft. (3.05 m) tall bottom.
Bundle first bar with closest WWR bar to the opening
and place second bar ±3 (75) away.

**PRECAST MANHOLE TYPE A
6' (1.83 m) DIAMETER**

(Sheet 2 of 3)

STANDARD 602406-11

Illinois Department of Transportation

PASSED January 1, 2021
ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2021
ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-97

Path: K:\Springfield\0180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CU500.dwg
Date: Tuesday, April 26, 2022 12:46:40 PM

BID SUBMITTAL
 APRIL 22, 2022

 RUNWAY 18/36 REMOVAL AND
 ASSOCIATED IMPROVEMENTS,
 PHASE 3

OWNER


 SPRINGFIELD AIRPORT
 AUTHORITY
 ABRAHAM LINCOLN CAPITAL
 AIRPORT
 SPRINGFIELD, IL

MARK | DATE | DESCRIPTION

 AIP PROJ. NO: 3-17-0096-XX **CA022**

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-CU500.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

APPROVED BY: APR

COPYRIGHT:

SHEET TITLE

**STORM SEWER
 DETAILS 5**

 SHEET **48** OF **91**

CU505

FLAT SLAB TOP REINFORCEMENT

Location	Riser Height (RH)	WWR (each direction)		Rebar (each direction except as noted)		Bar Size
		A _s (min.)	Spacing (max.)	A _s (min.)	Spacing (max.)	
Top Mat	All	0.11 sq. in./ft. (233 sq. mm/m)	18 (450)	0.11 sq. in./ft. (233 sq. mm/m)	18 (450)	#3 or #4 (#10) (#13)
Bottom Mat	RH ≤ 10 ft. (3.05 m)	** 0.62 sq. in./ft. (1312 sq. mm/m)	6 (150)	See plan view for rebar orientation and spacing and this table for bar size		#5 (#16)
	RH > 10 ft. (3.05 m)	** 0.88 sq. in./ft. (1863 sq. mm/m)	6 (150)			#6 (#19)

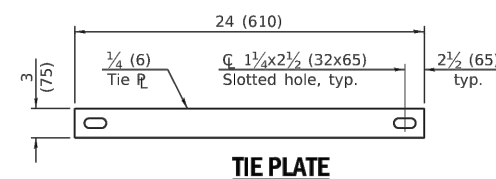
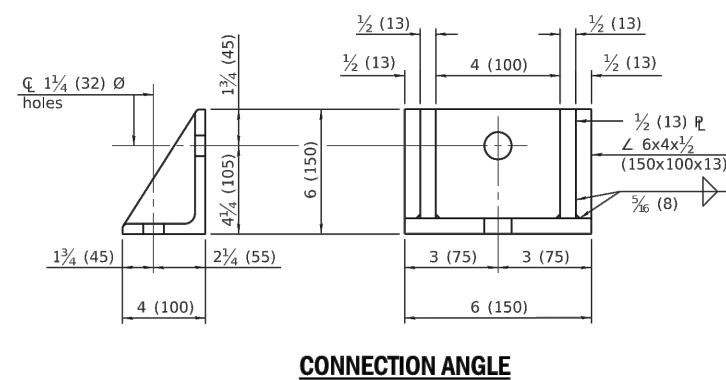
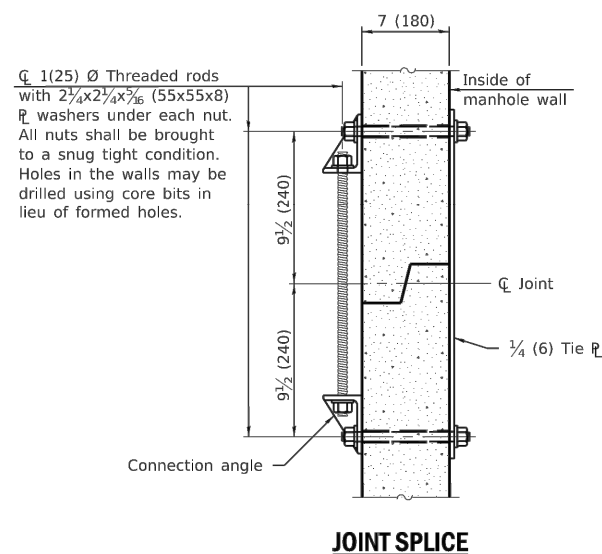
** Only one layer of WWR permitted to avoid congestion.

WALL REINFORCEMENT

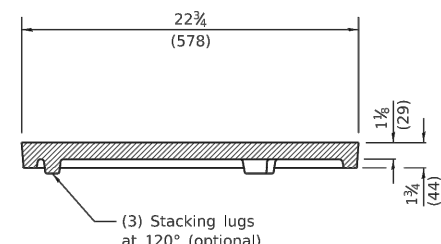
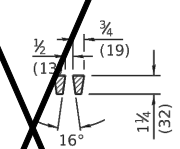
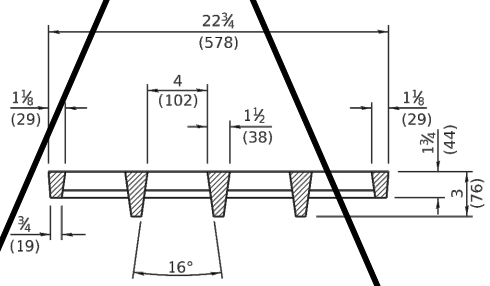
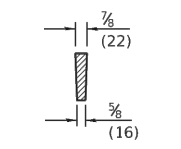
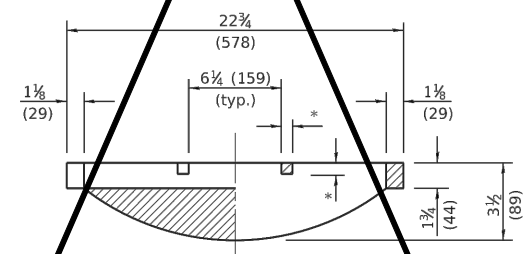
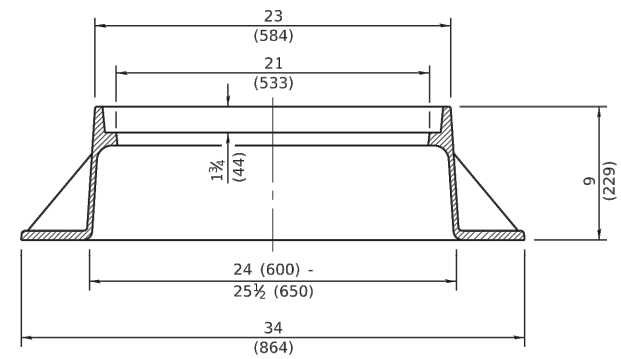
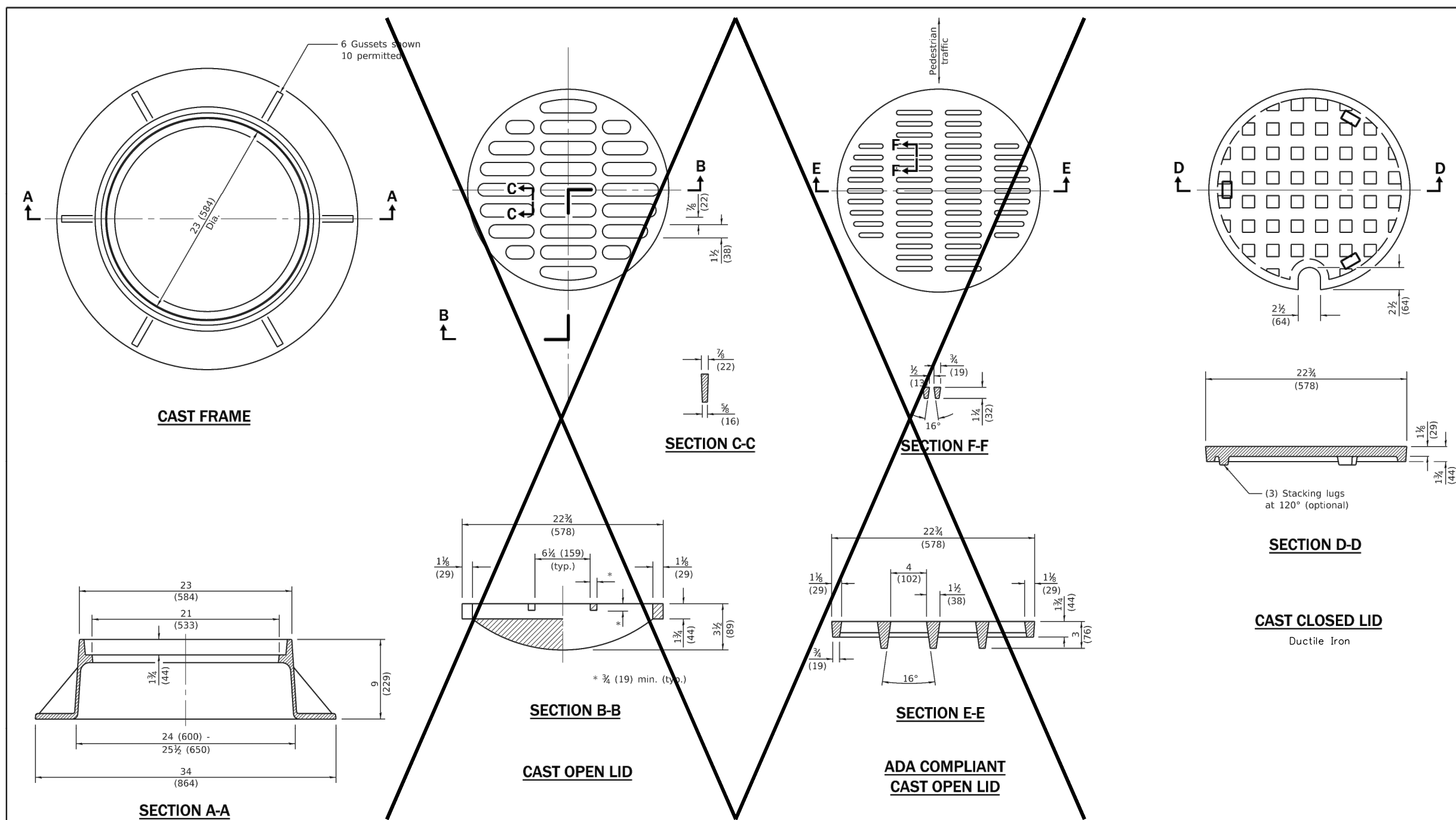
Location	Orientation	WWR or Rebar	
		A _s (min.)	Spacing (max.)
4 ft. (1.22 m) Ø Riser	Circumferential	0.12 sq. in./ft. (254 sq. mm/m)	6 (150)
	Vertical	0.045 sq. in./ft. (95 sq. mm/m)	8 (200)
6 ft. (1.83 m) Ø Barrel	Circumferential	0.18 sq. in./ft. (381 sq. mm/m)	6 (150)
	Vertical	0.045 sq. in./ft. (95 sq. mm/m)	8 (200)

BASE SLAB REINFORCEMENT

Location	Riser Height (RH)/ Total Height (TH)	WWR or Rebar (each direction)	
		A _s (min.)	Spacing (max.)
Top Mat	RH ≤ 10 ft. (3.05 m) & TH ≤ 20 ft. (6.10 m)	0.28 sq. in./ft. (593 sq. mm/m)	6 (150)
	RH > 10 ft. (3.05 m) or TH > 20 ft. (6.10 m)	0.40 sq. in./ft. (847 sq. mm/m)	6 (150)
Bottom Mat	All	0.11 sq. in./ft. (233 sq. mm/m)	18 (450)



Illinois Department of Transportation	
PASSED	January 1, 2021
ENGINEER OF POLICY AND PROCEDURES	
APPROVED	January 1, 2021
ENGINEER OF DESIGN AND ENVIRONMENT	
ISSUED	1-1-97



(3) Stacking lugs at 120° (optional)

All dimensions are in inches (millimeters) unless otherwise shown.

Illinois Department of Transportation
 PASSED January 1, 2020
 ENGINEER OF POLICY AND PROCEDURES
 APPROVED January 1, 2020
 ENGINEER OF DESIGN AND ENVIRONMENT

DATE	REVISIONS
1-1-20	Revised dimension in Section B-B of cast open lid.
1-1-15	Revised dimensioning of frame. Added ADA compliant open lid.
1-1-09	Switched units to English (metric).

**FRAME AND LIDS
TYPE 1**

STANDARD 604001-05

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

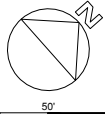
OWNER

 SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CU500.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

SHEET TITLE
**STORM SEWER
 DETAILS 6**

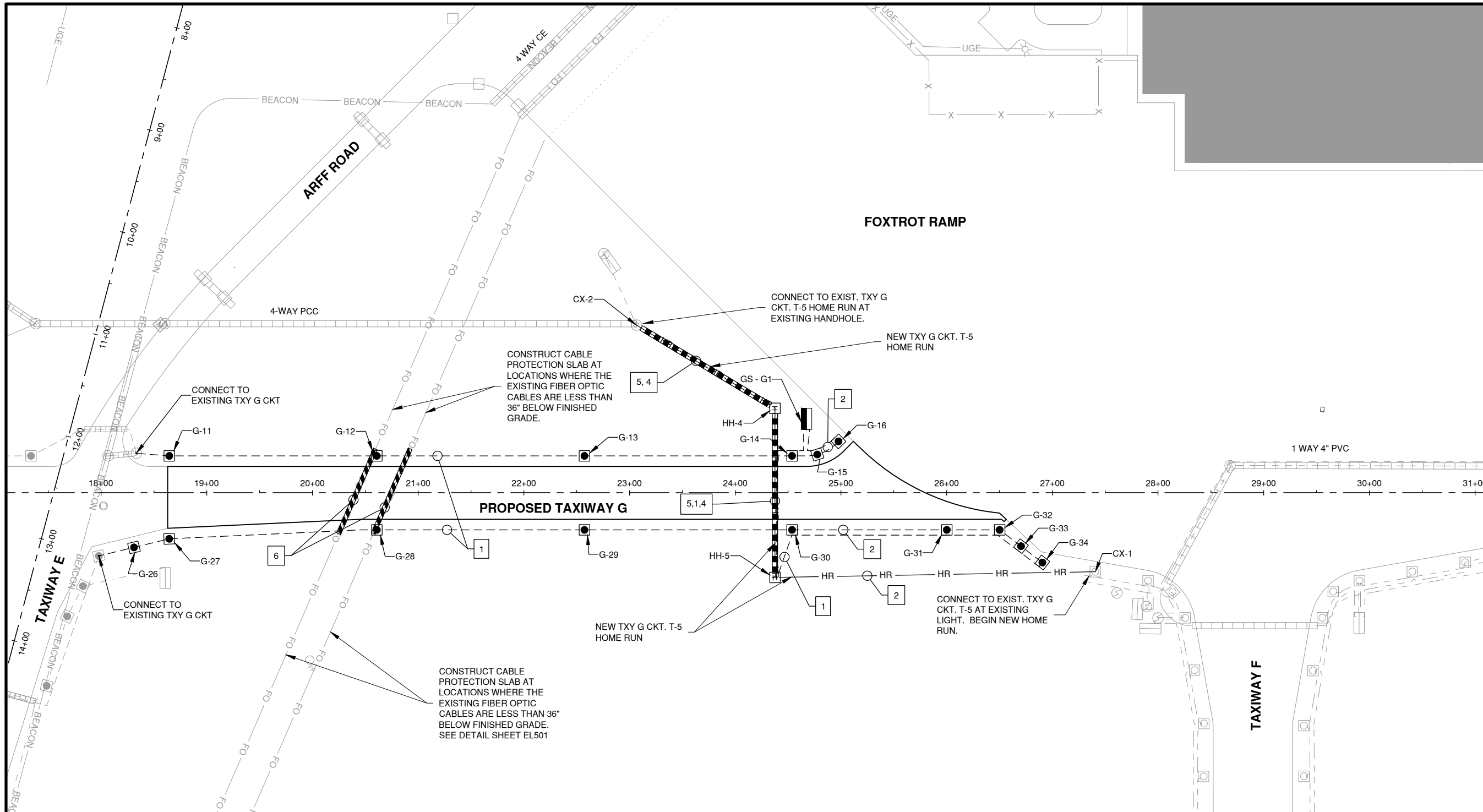


THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

 BID SUBMITTAL
APRIL 22, 2022

 RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER


 SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

CABLE NOTES

- | | |
|---|---|
| 1 | 1/C #8 5KV UG CABLE IN UD WITH COUNTERPOISE |
| 2 | 2/C #8 5KV UG CABLE IN UD WITH COUNTERPOISE |
| 3 | 1/C #8 5KV UG CABLE WITH COUNTERPOISE |
| 4 | 2/C #8 5KV UG CABLE WITH COUNTERPOISE |
| 5 | 4-WAY CONC. ENCASED DUCT |
| 6 | 1-WAY CONC. ENCASED DUCT |
| 7 | NEW CABLE IN EXISTING DUCT |

GENERAL NOTES

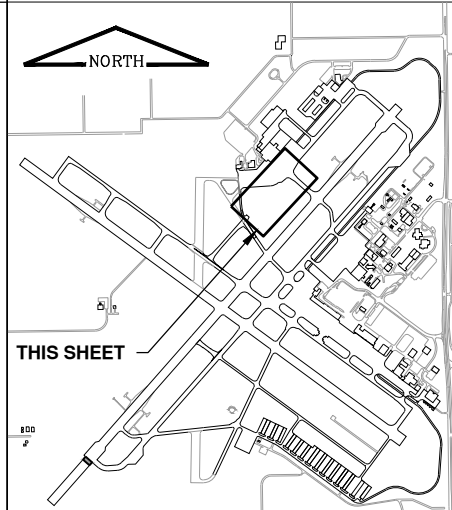
- LIGHTS SHALL BE LOCATED NO MORE THAN 10' AND NO LESS THAN 2" FROM THE EXISTING PAVEMENT EDGE, IN A STRAIGHT LINE.
- THE CONTRACTOR SHALL VERIFY LAYOUT OF LIGHTS WITH THE RPR PRIOR TO INSTALLING.
- LIGHTS SHALL BE INSTALLED IN A STRAIGHT LINE FROM THE I.P., P.T., P.C., OR TAPER POINT.

CIRCUIT LEGEND

EXISTING	NEW	CIRCUIT
		R-1 (RWY 4/22)
		R-2 (RWY 13/31)
		T-8 (TXY A, H-WEST)
		T-6 (TXY B)
		T-1 (TXY C, H-EAST)
		T-5 (EX. TXY G)
		T-4 (TXY E)
		T-10 (NEW TXY G)
		HR HOMERUN

LEGEND

EXISTING	NEW	
		SPLICE CAN
		TAXIWAY GUIDANCE SIGN
		TAXIWAY EDGE LIGHT - STAKE MOUNTED
		TAXIWAY EDGE LIGHT - BASE MOUNTED
		RUNWAY BASE MOUNTED EDGE LIGHT
		HANDHOLE
		4-WAY DUCT BANK
		RUNWAY EDGE LIGHT - FLUSH MOUNTED
		ELEVATED RETROREFLECTIVE MARKER
		TAXIWAY EDGE LIGHT - FLUSH MOUNTED
		TAXIWAY ENDING MARKER
		PAVEMENT SENSOR
		1-WAY DUCT BANK - 2" DIA.

KEYMAP


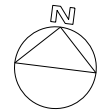
MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA022
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-EL100.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: MJD		
APPROVED BY: RLV		
COPYRIGHT:		

**ELECTRICAL PLAN
TXY G**



License No. 184-000613

CONSULTANTS



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).

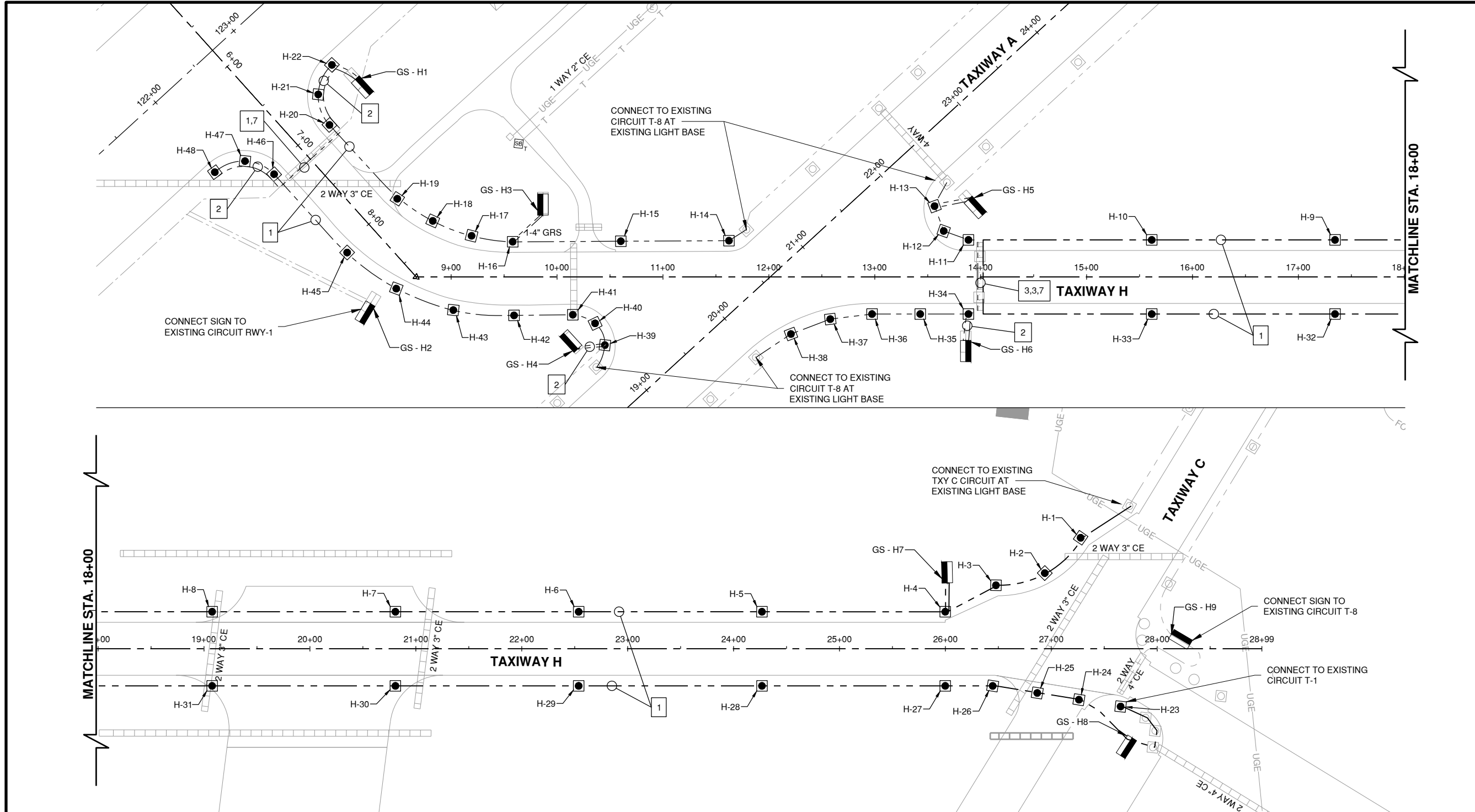
BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL



CABLE NOTES

- 1 1/C #8 5KV UG CABLE IN UD WITH COUNTERPOISE
- 2 2/C #8 5KV UG CABLE IN UD WITH COUNTERPOISE
- 3 1/C #8 5KV UG CABLE WITH COUNTERPOISE
- 4 2/C #8 5KV UG CABLE WITH COUNTERPOISE
- 5 4-WAY CONC. ENCASED DUCT
- 6 1-WAY CONC. ENCASED DUCT
- 7 NEW CABLE IN EXISTING DUCT

GENERAL NOTES

1. LIGHTS SHALL BE LOCATED NO MORE THAN 10' AND NO LESS THAN 2' FROM THE EXISTING PAVEMENT EDGE, IN A STRAIGHT LINE.
2. THE CONTRACTOR SHALL VERIFY LAYOUT OF LIGHTS WITH THE RPR PRIOR TO INSTALLING.
3. LIGHTS SHALL BE INSTALLED IN A STRAIGHT LINE FROM THE I.P., P.T., P.C., OR TAPER POINT.

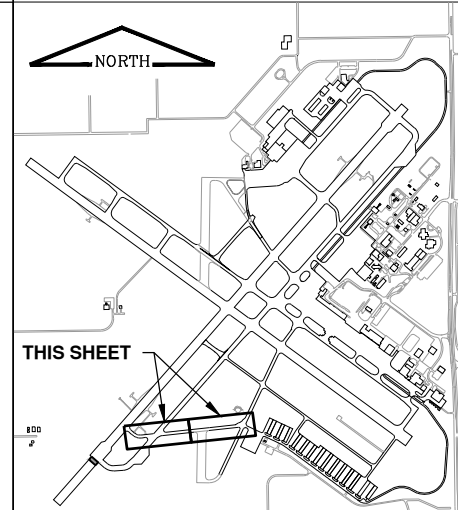
CIRCUIT LEGEND

EXISTING	NEW	CIRCUIT
(Symbol)	(Symbol)	R-1 (RWY 4/22)
(Symbol)	(Symbol)	R-2 (RWY 13/31)
(Symbol)	(Symbol)	T-8 (TXY A, H-WEST)
(Symbol)	(Symbol)	T-6 (TXY B)
(Symbol)	(Symbol)	T-1 (TXY C, H-EAST)
(Symbol)	(Symbol)	T-5 (EX. TXY G)
(Symbol)	(Symbol)	T-4 (TXY E)
(Symbol)	(Symbol)	T-10 (NEW TXY G)
(Symbol)	(Symbol)	HR HOMERUN

LEGEND

EXISTING	NEW	DESCRIPTION
(Symbol)	(Symbol)	SPLICE CAN
(Symbol)	(Symbol)	TAXIWAY GUIDANCE SIGN
(Symbol)	(Symbol)	TAXIWAY EDGE LIGHT - STAKE MOUNTED
(Symbol)	(Symbol)	TAXIWAY EDGE LIGHT - BASE MOUNTED
(Symbol)	(Symbol)	RUNWAY BASE MOUNTED EDGE LIGHT
(Symbol)	(Symbol)	HANDHOLE
(Symbol)	(Symbol)	4-WAY DUCT BANK
(Symbol)	(Symbol)	RUNWAY EDGE LIGHT - FLUSH MOUNTED
(Symbol)	(Symbol)	ELEVATED RETROREFLECTIVE MARKER
(Symbol)	(Symbol)	TAXIWAY EDGE LIGHT - FLUSH MOUNTED
(Symbol)	(Symbol)	TAXIWAY ENDING MARKER
(Symbol)	(Symbol)	PAVEMENT SENSOR
(Symbol)	(Symbol)	1-WAY DUCT BANK - 2" DIA.

KEYMAP

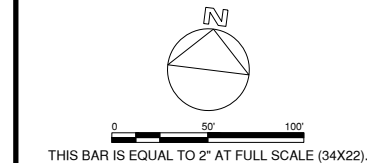


Path: K:\Springfield\01180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-EL100.dwg
Date: Tuesday, April 26, 2022 12:47:32 PM

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-EL100.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
COPYRIGHT:	

SHEET TITLE
ELECTRICAL PLAN
TXY H



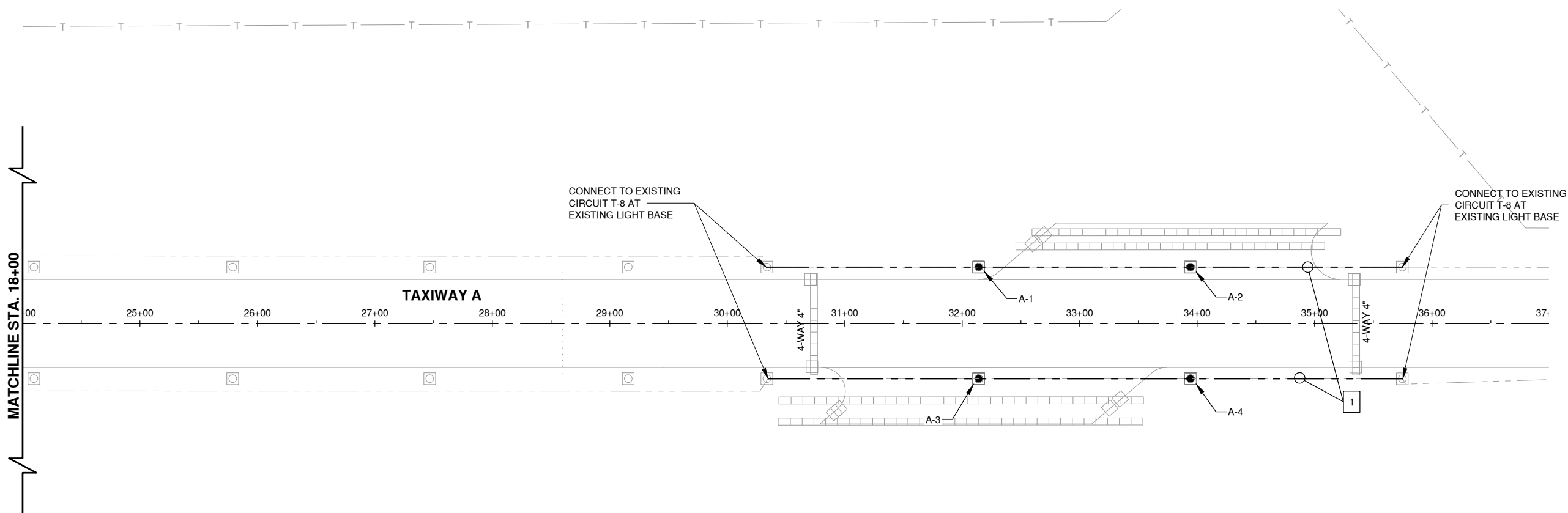
BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL



CABLE NOTES

- 1 ○ 1/C #8 5KV UG CABLE IN UD WITH COUNTERPOISE
- 2 ○ 2/C #8 5KV UG CABLE IN UD WITH COUNTERPOISE
- 3 ○ 1/C #8 5KV UG CABLE WITH COUNTERPOISE
- 4 ○ 2/C #8 5KV UG CABLE WITH COUNTERPOISE
- 5 ○ 4-WAY CONC. ENCASED DUCT
- 6 ○ 1-WAY CONC. ENCASED DUCT
- 7 ○ NEW CABLE IN EXISTING DUCT

GENERAL NOTES

1. LIGHTS SHALL BE LOCATED NO MORE THAN 10' AND NO LESS THAN 2' FROM THE EXISTING PAVEMENT EDGE, IN A STRAIGHT LINE.
2. THE CONTRACTOR SHALL VERIFY LAYOUT OF LIGHTS WITH THE RPR PRIOR TO INSTALLING.
3. LIGHTS SHALL BE INSTALLED IN A STRAIGHT LINE FROM THE I.P., P.T., P.C., OR TAPER POINT.

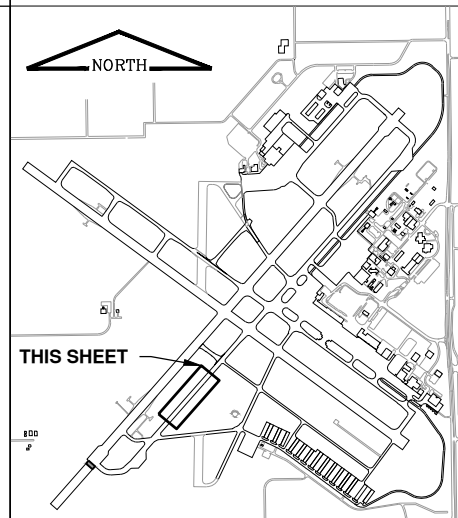
CIRCUIT LEGEND

EXISTING	NEW	CIRCUIT
---	---	R-1 (RWY 4/22)
---	---	R-2 (RWY 13/31)
---	---	T-8 (TXY A, H-WEST)
---	---	T-6 (TXY B)
---	---	T-1 (TXY C, H-EAST)
---	---	T-5 (EX. TXY G)
---	---	T-4 (TXY E)
---	---	T-10 (NEW TXY G)
---	---	HR HOMERUN

LEGEND

EXISTING	NEW	DESCRIPTION
○	○	SPLICE CAN
▬	▬	TAXIWAY GUIDANCE SIGN
○	●	TAXIWAY EDGE LIGHT - STAKE MOUNTED
○	●	TAXIWAY EDGE LIGHT - BASE MOUNTED
□	□	RUNWAY BASE MOUNTED EDGE LIGHT
□	□	HANDHOLE
▬	▬	4-WAY DUCT BANK
○	○	RUNWAY EDGE LIGHT - FLUSH MOUNTED
○	○	ELEVATED RETROREFLECTIVE MARKER
○	○	TAXIWAY EDGE LIGHT - FLUSH MOUNTED
○	○	TAXIWAY ENDING MARKER
○	○	PAVEMENT SENSOR
▬	▬	1-WAY DUCT BANK - 2" DIA.

KEYMAP



MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-EL100.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
COPYRIGHT:	

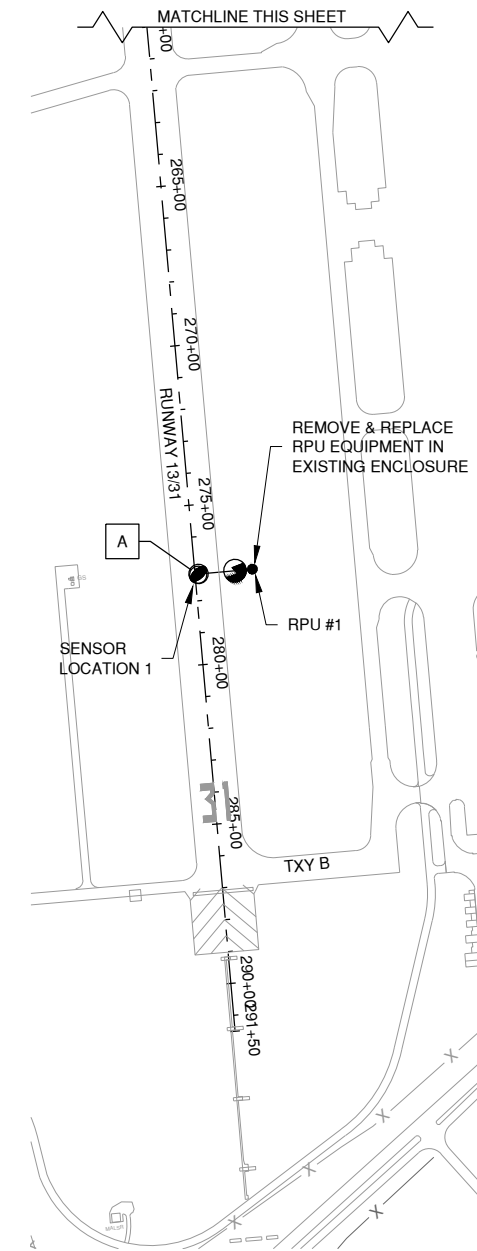
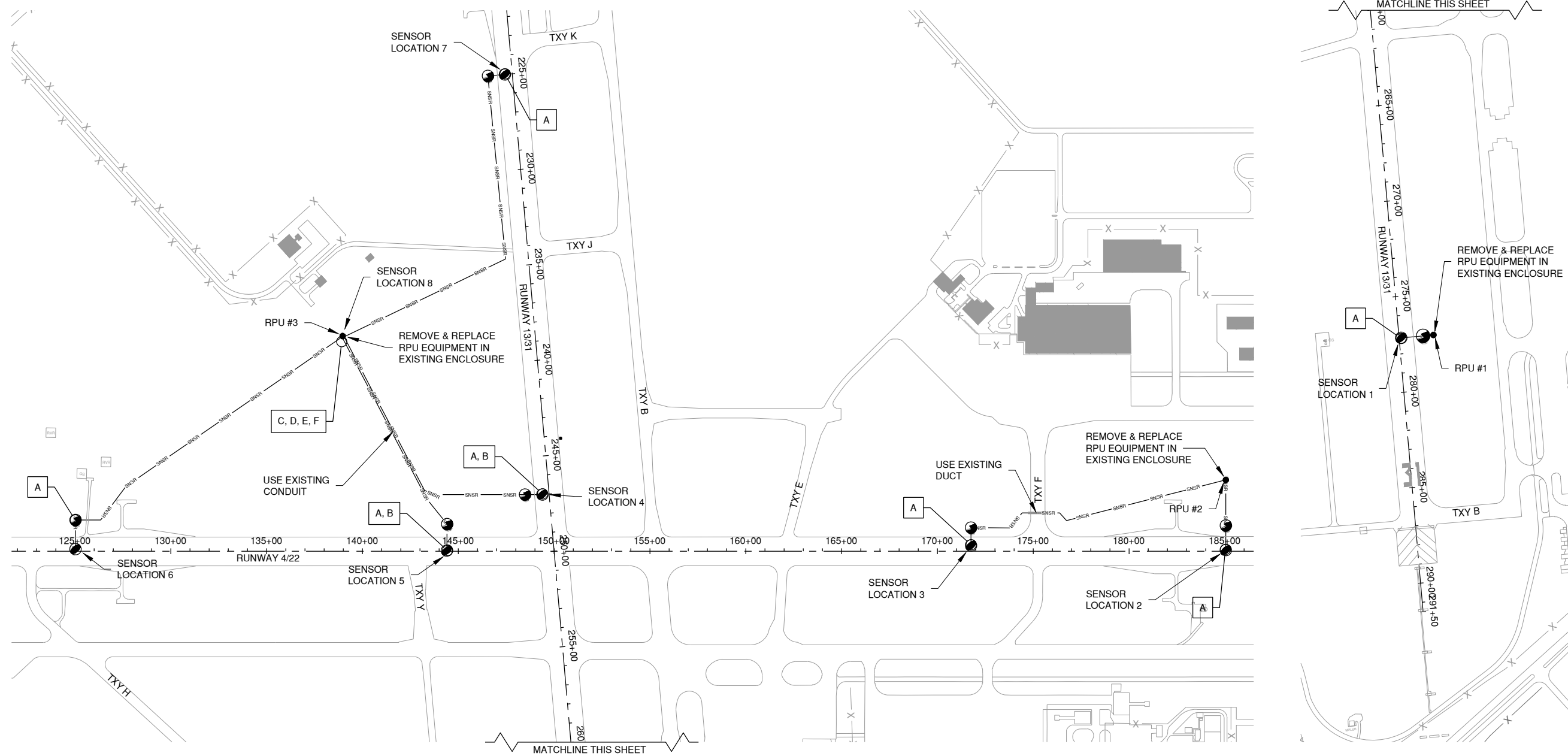
SHEET TITLE

**ELECTRICAL PLAN
TXY A**

EL103

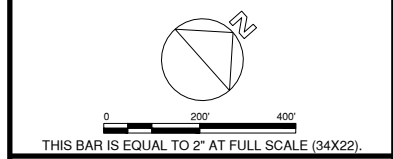
SHEET 52 OF 91

Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-EL100.dwg
 Date: Tuesday, April 26, 2022 12:47:51 PM



License No. 184-000613

CONSULTANTS



BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

SENSOR EQUIPMENT NOTES

- A ○ PASSIVE PAVEMENT SURFACE SENSOR (7 EACH)
- B ○ SUBSURFACE TEMPERATURE PROBE (2 EACH)
- C ○ CLASSIFYING PRECIPITATION AND VISIBILITY SENSOR (1 EACH)
- D ○ AIR TEMPERATURE / RELATIVE HUMIDITY SENSOR (1 EACH)
- E ○ HEATED ULTRASONIC WIND SPEED / DIRECTION SENSOR (1 EACH)
- F ○ BAROMETRIC PRESSURE SENSOR (1 EACH)

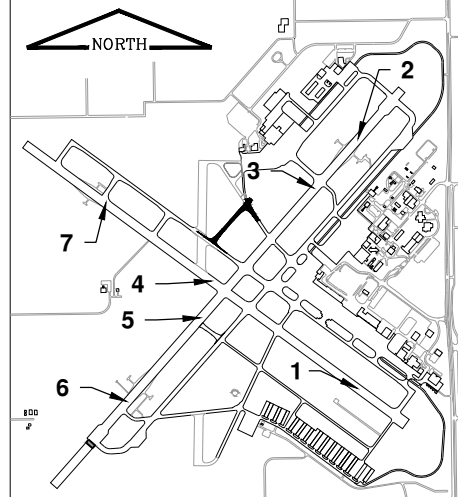
NOTES

1. SENSORS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
2. SENSOR AND RPU LOCATIONS ARE APPROXIMATE.
3. ALL NEW SENSORS SHALL INCLUDE A NEW SPLICE CAN. SPLICE CANS FOR SENSORS SHALL BE CONSIDERED INCIDENTAL TO THE RWIS PAY ITEM.
4. ALL RPU ELECTRONICS TO BE REMOVED AND REPLACED WITHIN EXISTING PEDESTAL MOUNTED ENCLOSURES.
5. NEW SENSOR CABLE AS REQUIRED BY RPU MANUFACTURER SHALL BE INSTALLED BETWEEN THE RPU AND THE PAVEMENT SENSOR.

LEGEND

- SENSOR
- RPU
- SPLICE CAN
- SNSR— NEW SENSOR CABLE IN UD

KEYMAP

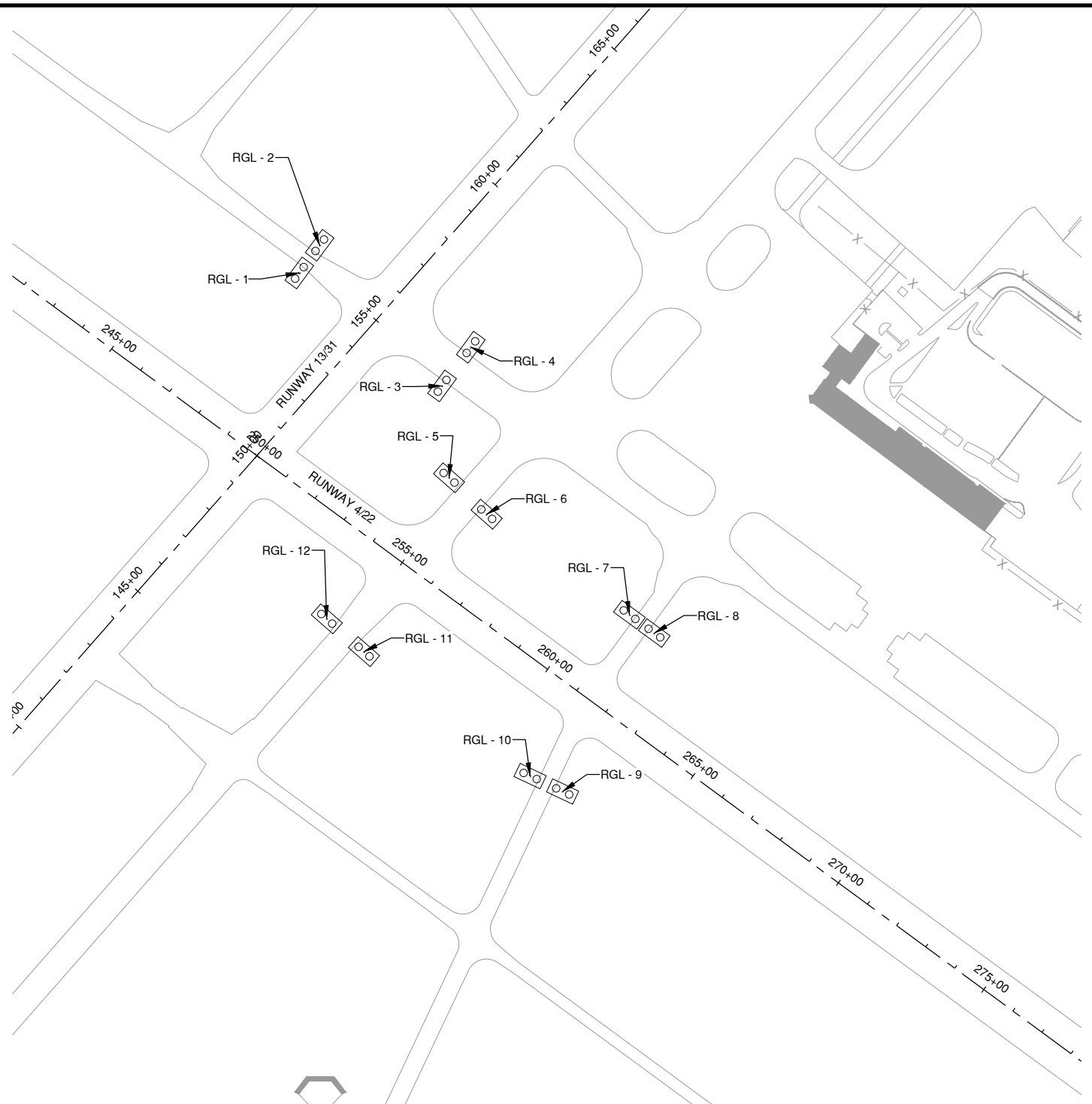


MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA022
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-EL103.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: CHK		
APPROVED BY: APR		
COPYRIGHT:		

SHEET TITLE
RUNWAY WEATHER INFORMATION SYSTEM (RWIS)

EL104
SHEET 53 OF 91

Path: K:\Springfield\0118035-05_Rwy_18-36\DrawSheets\PHASE 3\18003505-PH3-EL103.dwg
Date: Tuesday, April 26, 2022 12:48:14 PM

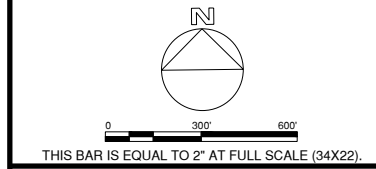


FIXTURE TABLE RUNWAY GUARD LIGHTS

FIXTURE #	FIXTURE TYPE	HORIZONTAL CONTROL
RGL - 1	RUNWAY GUARD LIGHT # 1	RUNWAY 4-22 STA 154+58.25
RGL - 2	RUNWAY GUARD LIGHT # 2	RUNWAY 4-22 STA 155+52.91
RGL - 3	RUNWAY GUARD LIGHT # 3	RUNWAY 4-22 STA 154+84.02
RGL - 4	RUNWAY GUARD LIGHT # 4	RUNWAY 4-22 STA 156+17.41
RGL - 5	RUNWAY GUARD LIGHT # 5	RUNWAY 13-31 STA 254+65.48
RGL - 6	RUNWAY GUARD LIGHT # 6	RUNWAY 13-31 STA 256+09.14
RGL - 7	RUNWAY GUARD LIGHT # 7	RUNWAY 13-31 STA 260+93.23
RGL - 8	RUNWAY GUARD LIGHT # 8	RUNWAY 13-31 STA 261+78.75
RGL - 9	RUNWAY GUARD LIGHT # 9	RUNWAY 13-31 STA 262+33.90
RGL - 10	RUNWAY GUARD LIGHT # 10	RUNWAY 13-31 STA 261+36.11
RGL - 11	RUNWAY GUARD LIGHT # 11	RUNWAY 13-31 STA 255+60.68
RGL - 12	RUNWAY GUARD LIGHT # 12	RUNWAY 13-31 STA 254+23.70



License No. 184-000613
CONSULTANTS



BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

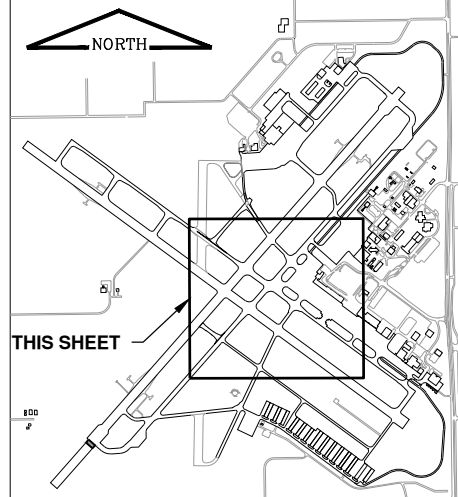
NOTES

1. RUNWAY GUARD LIGHTS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
2. NEW RGLS WITH TRANSFORMERS SHALL BE INSTALLED ON EXISTING BASES.
3. THE RUNWAY GUARD LIGHT PAY ITEM SHALL INCLUDE THE REMOVAL AND DISPOSAL OF THE EXISTING RGL & TRANSFORMER.

LEGEND

RGL

KEYMAP



MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA022
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-EL104.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: CHK		
APPROVED BY: APR		
COPYRIGHT:		
SHEET TITLE		
RUNWAY GUARD LIGHT LAYOUT		
EL105		
SHEET 54	OF	91

Path: K:\Springfield\0418035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-EL104.dwg
 Date: Tuesday, April 26, 2022 12:48:35 PM



License No. 184-000613
CONSULTANTS

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK | DATE | DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX CA022

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-EL400.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

APPROVED BY: APR

COPYRIGHT:

SHEET TITLE

ELECTRICAL
SCHEDULES 1

EL401
SHEET 55 OF 91

FIXTURE TABLE HOME RUN

FIXTURE #	FIXTURE TYPE	HORIZONTAL CONTROL
CX-1	CONNECT TO EX. TXY G CKT	TAXIWAY G STA 27+40.60
CX-2	EXISTING HANDHOLE	TAXIWAY G STA 23+06.52
HH-4	NEW HANDHOLE	TAXIWAY G STA 24+37.57
HH-5	NEW HANDHOLE	TAXIWAY G STA 24+37.57

FIXTURE TABLE TXY A LIGHTS - CKT T-8

FIXTURE #	FIXTURE TYPE	HORIZONTAL CONTROL
A-1	L-861T (QUARTZ)	TAXIWAY A STA 32+13.99
A-2	L-861T (QUARTZ)	TAXIWAY A STA 33+94.34
A-3	L-861T (QUARTZ)	TAXIWAY A STA 32+13.99
A-4	L-861T (QUARTZ)	TAXIWAY A STA 33+94.34

FIXTURE TABLE TXY H LIGHTS - CKT T-1

FIXTURE #	FIXTURE TYPE	HORIZONTAL CONTROL
H-1	L-861T (LED)	TAXIWAY H STA 27+27.67
H-2	L-861T (LED)	TAXIWAY H STA 26+93.81
H-3	L-861T (LED)	TAXIWAY H STA 26+47.69
H-4	L-861T (LED)	TAXIWAY H STA 25+99.82
H-5	L-861T (LED)	TAXIWAY H STA 24+26.77
H-6	L-861T (LED)	TAXIWAY H STA 22+53.72
H-7	L-861T (LED)	TAXIWAY H STA 20+80.67
H-8	L-861T (LED)	TAXIWAY H STA 19+07.61
H-9	L-861T (LED)	TAXIWAY H STA 17+34.56
H-10	L-861T (LED)	TAXIWAY H STA 15+61.51
H-23	L-861T (LED)	TAXIWAY H STA 27+65.41
H-24	L-861T (LED)	TAXIWAY H STA 27+26.06
H-25	L-861T (LED)	TAXIWAY H STA 26+86.71
H-26	L-861T (LED)	TAXIWAY H STA 26+44.64
H-27	L-861T (LED)	TAXIWAY H STA 25+99.82
H-28	L-861T (LED)	TAXIWAY H STA 24+26.77
H-29	L-861T (LED)	TAXIWAY H STA 22+53.72
H-30	L-861T (LED)	TAXIWAY H STA 20+80.67
H-31	L-861T (LED)	TAXIWAY H STA 19+07.61
H-32	L-861T (LED)	TAXIWAY H STA 17+34.56
H-33	L-861T (LED)	TAXIWAY H STA 15+61.51

FIXTURE TABLE TXY H LIGHTS - CKT T-8

FIXTURE #	FIXTURE TYPE	HORIZONTAL CONTROL
H-11	L-861T (LED)	TAXIWAY H STA 13+88.46
H-12	L-861T (LED)	TAXIWAY H STA 13+65.05
H-13	L-861T (LED)	TAXIWAY H STA 13+56.44
H-14	L-861T (LED)	TAXIWAY H STA 11+62.32
H-15	L-861T (LED)	TAXIWAY H STA 10+60.22
H-16	L-861T (LED)	TAXIWAY H STA 9+58.12
H-17	L-861T (LED)	TAXIWAY H STA 9+19.28
H-18	L-861T (LED)	TAXIWAY H STA 8+38.12
H-19	L-861T (LED)	TAXIWAY H STA 8+00.20
H-20	L-861T (LED)	TAXIWAY H STA 7+05.53
H-21	L-861T (LED)	TAXIWAY H STA 6+77.00
H-22	L-861T (LED)	TAXIWAY H STA 6+64.94
H-34	L-861T (LED)	TAXIWAY H STA 13+88.46
H-35	L-861T (LED)	TAXIWAY H STA 13+42.93

FIXTURE TABLE TXY H LIGHTS - CKT T-8

FIXTURE #	FIXTURE TYPE	HORIZONTAL CONTROL
H-36	L-861T (LED)	TAXIWAY H STA 12+97.40
H-37	L-861T (LED)	TAXIWAY H STA 12+57.97
H-38	L-861T (LED)	TAXIWAY H STA 12+20.83
H-39	L-861T (LED)	TAXIWAY H STA 10+45.17
H-40	L-861T (LED)	TAXIWAY H STA 10+35.90
H-41	L-861T (LED)	TAXIWAY H STA 10+14.89
H-42	L-861T (LED)	TAXIWAY H STA 9+59.40
H-43	L-861T (LED)	TAXIWAY H STA 9+02.05
H-44	L-861T (LED)	TAXIWAY H STA 8+62.61
H-45	L-861T (LED)	TAXIWAY H STA 8+06.25
H-46	L-861T (LED)	TAXIWAY H STA 7+05.15
H-47	L-861T (LED)	TAXIWAY H STA 6+77.59
H-48	L-861T (LED)	TAXIWAY H STA 6+66.34

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK DATE DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-EL400.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

SHEET TITLE
**ELECTRICAL
SCHEDULES 2**

FIXTURE TABLE TXY G - CKT T-10

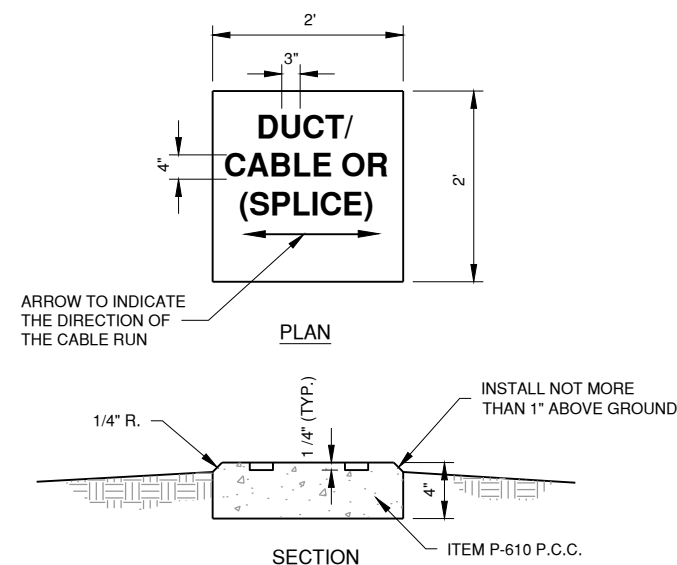
FIXTURE #	FIXTURE TYPE	HORIZONTAL CONTROL
CX-1	CONNECT TO EXISTING TXY G CKT	TAXIWAY G STA 18+33.33
G-11	L-861T (LED)	TAXIWAY G STA 18+64.44
G-12	L-861T (LED)	TAXIWAY G STA 20+60.87
G-13	L-861T (LED)	TAXIWAY G STA 22+57.29
G-14	L-861T (LED)	TAXIWAY G STA 24+53.72
G-15	L-861T (LED)	TAXIWAY G STA 24+77.57
G-16	L-861T (LED)	TAXIWAY G STA 24+97.69
G-26	L-861T (LED)	TAXIWAY G STA 18+31.11
G-27	L-861T (LED)	TAXIWAY G STA 18+64.44
G-28	L-861T (LED)	TAXIWAY G STA 20+60.87
G-29	L-861T (LED)	TAXIWAY G STA 22+57.29
G-30	L-861T (LED)	TAXIWAY G STA 24+53.72
G-31	L-861T (LED)	TAXIWAY G STA 26+00.14
G-32	L-861T (LED)	TAXIWAY G STA 26+50.14
G-33	L-861T (LED)	TAXIWAY G STA 26+70.36
G-34	L-861T (LED)	TAXIWAY G STA 26+90.58
G-CX-2	CONNECT TO EXISTING TXY G CKT	TAXIWAY G STA 17+97.78

FIXTURE TABLE TAXIWAY GUIDANCE SIGNS

FIXTURE #	FIXTURE TYPE	HORIZONTAL CONTROL
GS - G1	GUIDANCE SIGN	TAXIWAY G STA 24+67.38
GS - H1	GUIDANCE SIGN	TAXIWAY H STA 6+89.88
GS - H2	HOLDLINE SIGN	TAXIWAY H STA 8+56.87
GS - H3	GUIDANCE SIGN	TAXIWAY H STA 9+86.79
GS - H4	GUIDANCE SIGN	TAXIWAY H STA 10+19.74
GS - H5	GUIDANCE SIGN	TAXIWAY H STA 13+88.14
GS - H6	GUIDANCE SIGN	TAXIWAY H STA 13+85.94
GS - H7	GUIDANCE SIGN	TAXIWAY H STA 26+01.84
GS - H8	GUIDANCE SIGN	TAXIWAY H STA 27+76.26
GS - H9	GUIDANCE SIGN	TAXIWAY H STA 28+13.46

GUIDANCE SIGN SCHEDULE

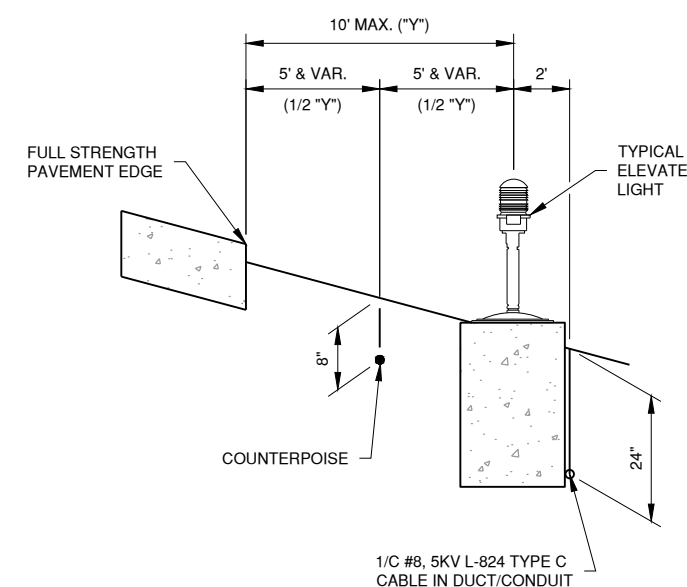
SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND (L-858R)	BLACK LEGEND ON YELLOW BACKGROUND (L-858Y)	YELLOW LEGEND ON BLACK BACKGROUND (L-858L)	NUMBER OF CHARACTERS	POWER CIRCUIT	SIGN TYPE	SIGN SIZE	SIGN STYLE	SIGN CLASS	SIGN MODE
GS- G1	NE SW				G G		TXY G	L-858	2	2	2	2
GS- H1	NE SW	← H 4	4	← H		2	TXY A	L-858	2	2	2	2
GS- H2	E W	H 4 H	4		H H	2	TXY A	L-858	2	2	2	2
GS- H3	E W	H H H A A A		H A A	H A	5	TXY A	L-858	2	2	2	2
GS- H4	NE SW	A A H A A H		A H	A	5	TXY A	L-858	2	2	2	2
GS- H5	NE SW	A A A H		A H	A	5	TXY A	L-858	2	2	2	2
GS- H6	E W	H H H A A 4 ^		H A A	H	5	TXY A	L-858	2	2	2	2
GS- H7	E W	4 ^ H C H		4 ^ H C	H	3	TXY H CKT T-1	L-858	2	2	2	2
GS- H8	E W	H C H H C →		H C →		4	TXY C CKT T-1	L-858	2	2	2	2
GS- H9	NE SW	C H H >		H >	C	3	TXY C CKT T-1	L-858	2	2	2	2



1 TURF CABLE / DUCT / SPLICE MARKER
N.T.S.

MARKER NOTES

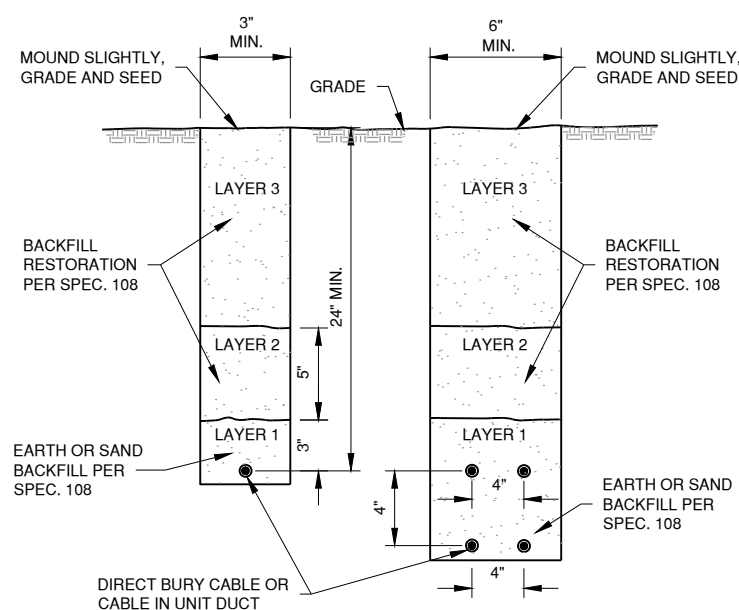
- DUCT MARKERS SHALL BE INSTALLED AT BOTH EDGES OF PAVEMENT WHERE PROPOSED ELECTRICAL DUCTS CROSS BOTH NEW AND EXISTING PAVEMENTS.
- CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE HOMERUN.
- ITEM 610 CONCRETE SHALL BE USED.
- ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
- THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED ITEMS.
- 0.049 CU. YD. CONCRETE PER MARKER.
- A MARKER CONFORMING TO THIS DETAIL MARKED "SPLICE" SHALL BE INSTALLED AT ALL SPLICE LOCATIONS NOT IN LIGHT CANS OR MANHOLES.
- ADDITIONAL REQUIREMENTS FOR CONTRACTOR FURNISHED CONCRETE DUCT MARKERS ARE PRESENTED IN SECTION L-110 OF THE PROJECT SPECIFICATION.
- INSTALLED MARKER TO HAVE A COAT OF ORANGE PAINT.
- DIGITS SHALL BE IMPRESSED INTO THE CONCRETE.



4 COUNTERPOISE LOCATION DETAIL
N.T.S.

CABLE TRENCH NOTES

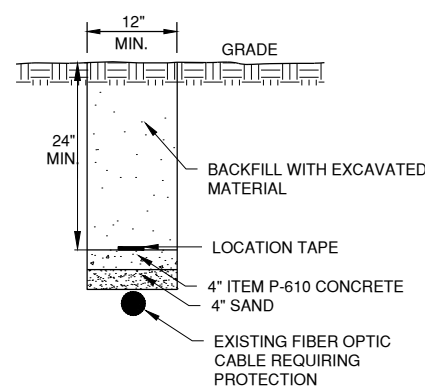
- #6 BARE COUNTERPOISE WITH 3/4" x 10' GROUND ROD INSTALLED AT MAX. 500' SPACING. ALSO USE GROUND ROD TO TERMINATE THE COUNTERPOISE AT BOTH ENDS OF DUCT. GROUND RODS SHALL BE CONSIDERED INCIDENTAL TO OTHER PAY ITEMS.



2 CABLE TRENCH DETAIL
N.T.S.

CABLE TRENCH NOTES

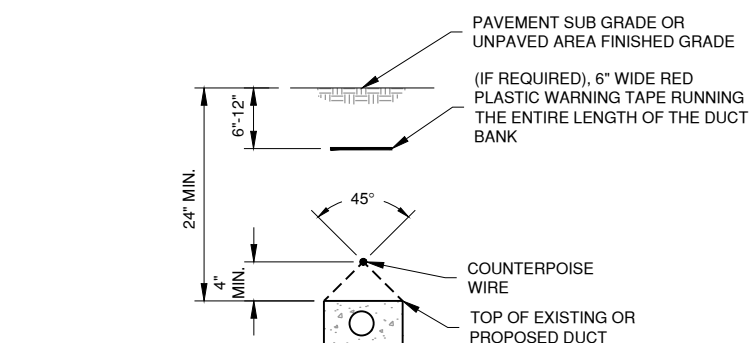
- CABLES SHALL NOT BE PLACED LESS THAN 24" DEEP IN ANY ONE TRENCH UNLESS PERMITTED BY ENGINEER.
- WHERE PERMITTED, CONTRACTOR MAY INSTALL CABLE IN UNIT DUCT BY PLOWING METHOD.



3 CABLE PROTECTION SLAB
N.T.S.

CABLE PROTECTION SLAB NOTES

- THIS DETAIL SHALL APPLY UNTIL THE DEPTH OF CABLE IS KNOWN TO BE 36" OR GREATER AT THE LOCATIONS SHOWN ON THE PLANS.



5 LOCATION OF COUNTERPOISE (DUCT BANK)
N.T.S.

COUNTERPOISE NOTES

- THE HEIGHT ABOVE THE CABLE AND/OR CONDUIT IS CALCULATED TO ENSURE THE CABLES AND/OR CONDUITS TO BE PROTECTED ARE WITHIN THE 45° ZONE OF PROTECTION BELOW THE COUNTERPOISE.
- COUNTERPOISE WIRES MUST BE INSTALLED ABOVE MULTIPLE CONDUITS/DUCT BANKS FOR AIRFIELD LIGHTING CABLES, WITH THE INTENT BEING TO PROVIDE A COMPLETE CONE OF PROTECTION OVER THE AIRFIELD LIGHTING CABLES. WHEN MULTIPLE CONDUITS AND/OR DUCT BANKS FOR AIRFIELD CABLE ARE INSTALLED IN THE SAME TRENCH, THE NUMBER AND LOCATION OF THE COUNTERPOISE WIRES ABOVE THE CONDUITS SHALL BE ADEQUATE TO PROVIDE A COMPLETE ZONE OF PROTECTION MEASURED 22-1/2" EACH SIDE OF VERTICAL.
- REFER TO THE CURRENT VERSIONS OF FAA AC 150/5340-30 AND AC 150/5370-10 FOR MORE DETAILS ON COUNTERPOISE INSTALLATION.

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-EL500.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

SHEET TITLE
ELECTRICAL DETAILS
1
SHEET **57** OF **91**
EL501

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX **CA022**

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-EL500.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

APPROVED BY: APR

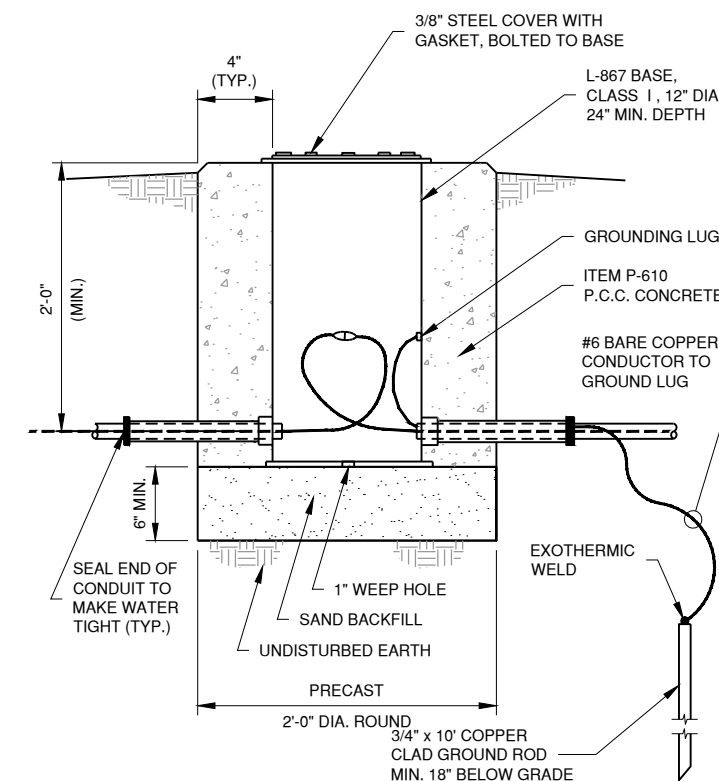
COPYRIGHT:

SHEET TITLE

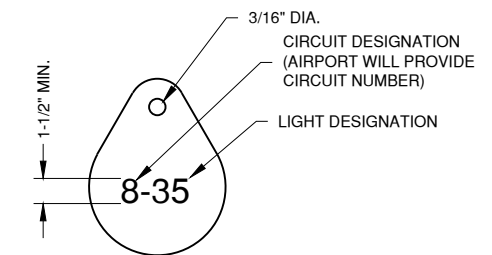
ELECTRICAL DETAILS

2

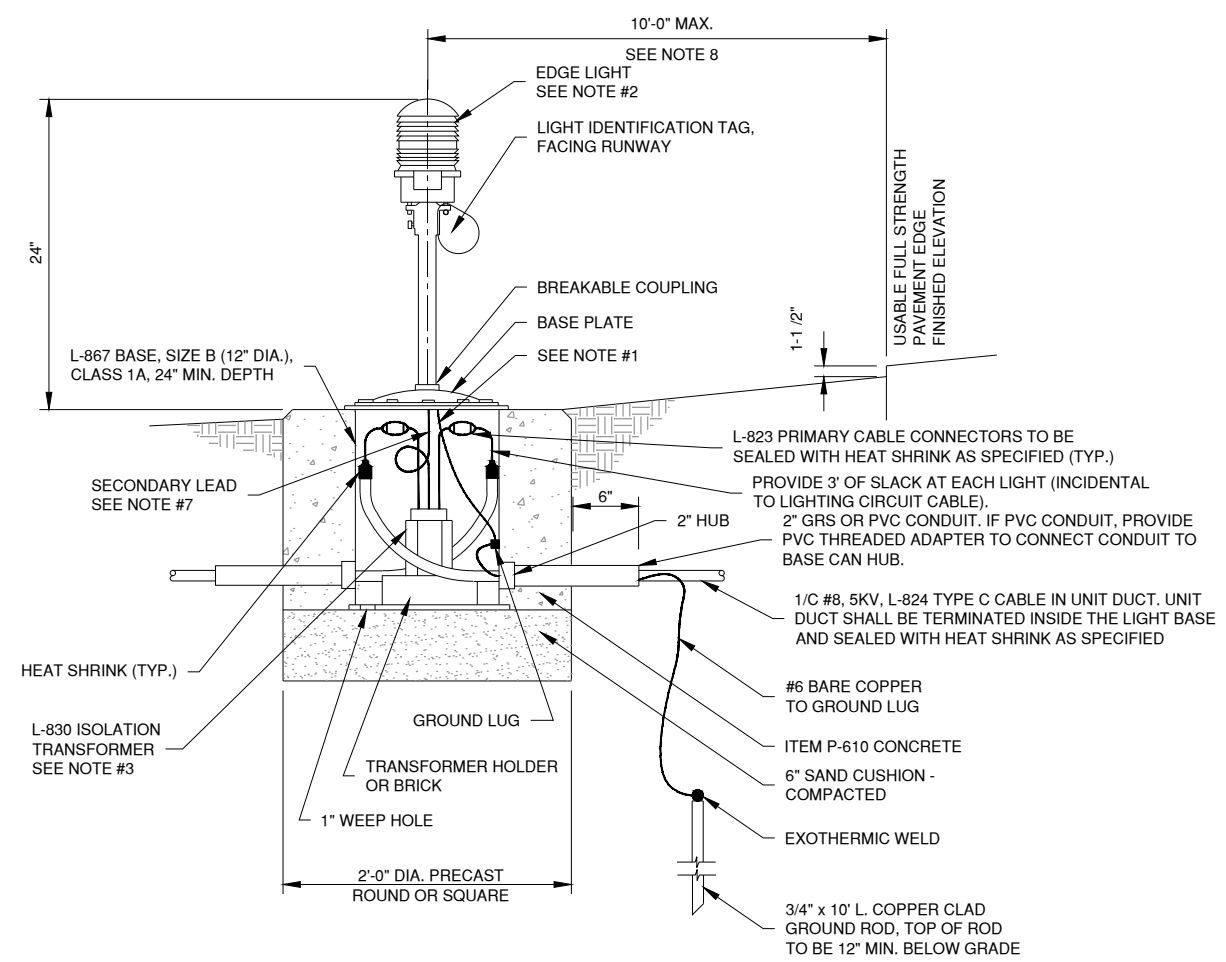
EL502
SHEET **58** OF **91**



3 SPLICE CAN
N.T.S.



2 LIGHT IDENTIFICATION TAG
N.T.S.



1 BASE MOUNTED ELEVATED EDGE LIGHT
N.T.S.

NOTE: SEE COUNTERPOISE LOCATION DETAIL SHEET EL501

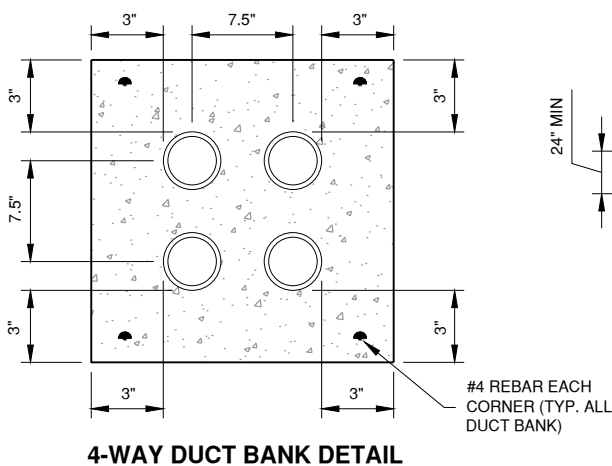
BASE-MOUNTED ELEVATED EDGE LIGHT NOTES

1. THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS WITH GREEN XHHW INSULATION. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.
2. LIGHT FIXTURES SHALL BE L-861T AS INDICATED ON THE PLANS AND SPECIFICATIONS.
3. THE TOLERANCE FOR THE HEIGHT OF RUNWAY/TAXIWAY EDGE LIGHTS MUST BE ± 1 INCH. THE TOLERANCE FOR THE LATERAL SPACING (LIGHT LANE TO RUNWAY/TAXIWAY CENTERLINE) OF RUNWAY/TAXIWAY EDGE LIGHTS MUST BE ± 1 INCH.
4. DIRECTION OF PRIMARY CABLES MUST BE IDENTIFIED BY COLOR CODING AS FOLLOWS: WHEN FACING LIGHT WITH BACK FACING PAVEMENT, CABLE TO THE LEFT IS CODED RED AND CABLE TO THE RIGHT IS CODED BLUE.
5. APPLY A CORROSION INHIBITING, ANTI-SEIZE COMPOUND TO ALL SCREWS, NUTS AND FRANGIBLE COUPLING THREADS. IF COATED BOLTS ARE USED PER ENGINEERING BRIEF #83, DO NOT APPLY ANTI-SEIZE COMPOUND.
6. ELECTRICAL INSULATING GREASE MUST BE APPLIED WITHIN THE L-830 ISOLATION TRANSFORMER SECONDARY TWO CONDUCTOR CONNECTORS TO PREVENT WATER ENTRANCE. THE CONNECTORS MUST NOT BE TAPED.
7. ENTRANCES IN L-867 BASES MUST BE PLUGGED FROM THE INSIDE WITH DUCT SEAL TO MAKE WATERTIGHT.
8. EDGE LIGHTS SHALL BE LOCATED NO MORE THAN 10' AND NO LESS THAN 2' FROM THE EXISTING PAVEMENT EDGE, IN A STRAIGHT LINE PARALLEL WITH CENTERLINE. THE CONTRACTOR SHALL VERIFY LAYOUT OF LIGHTS WITH THE RPR PRIOR TO INSTALLATION.

LIGHT IDENTIFICATION NOTES

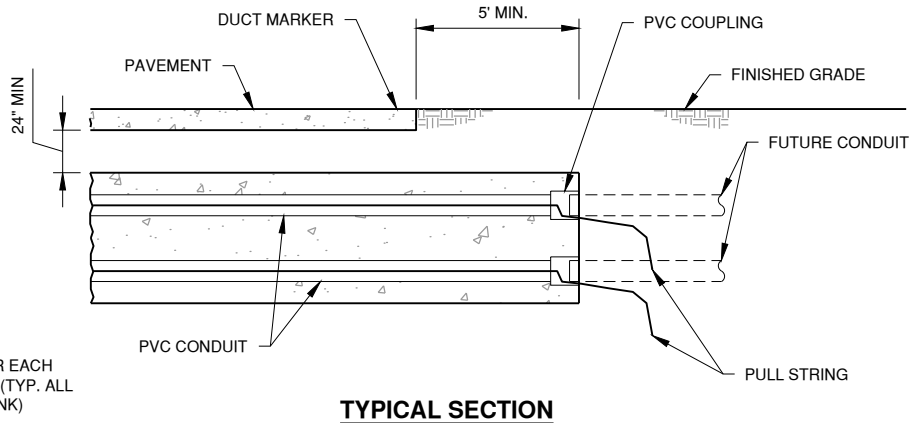
1. INSTALL A NON-CORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH SET SCREW.
2. LEGENDS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. CONTRACTOR TO COORDINATE LEGEND WITH AIRPORT.
3. THE CONTRACTOR SHALL NUMBER THE EXISTING/ PROPOSED LIGHTS AND SIGNS IN EACH CIRCUIT STARTING AT THE HOMERUN CONTINUING AROUND THE ENTIRE CIRCUIT BACK TO THE HOMERUN.
4. AIRFIELD SIGNS SHALL BE TAGGED & NUMBERED.

Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-EL500.dwg
Date: Tuesday, April 26, 2022 12:49:03 PM

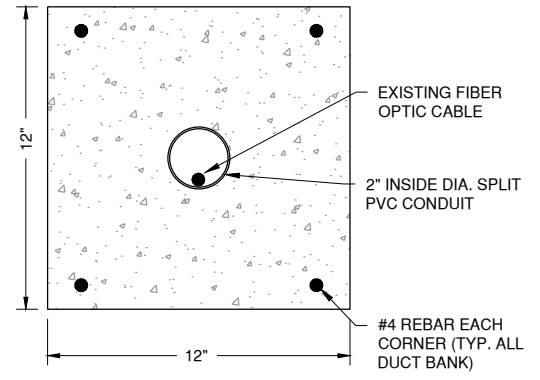


4-WAY DUCT BANK DETAIL

1 4-WAY DUCT BANK DETAIL
N.T.S.



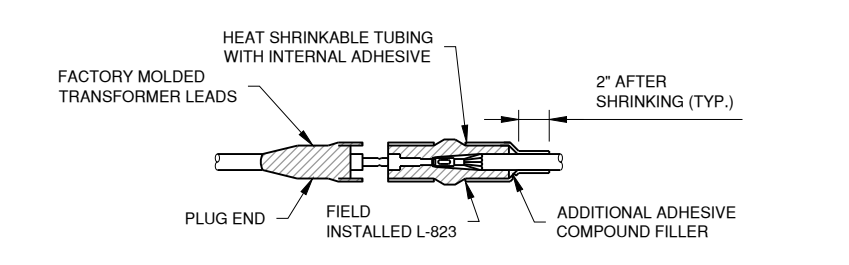
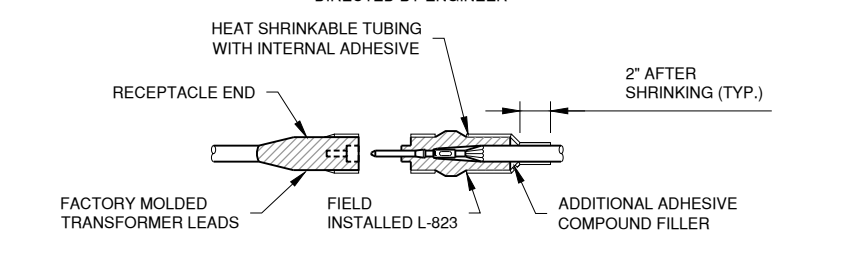
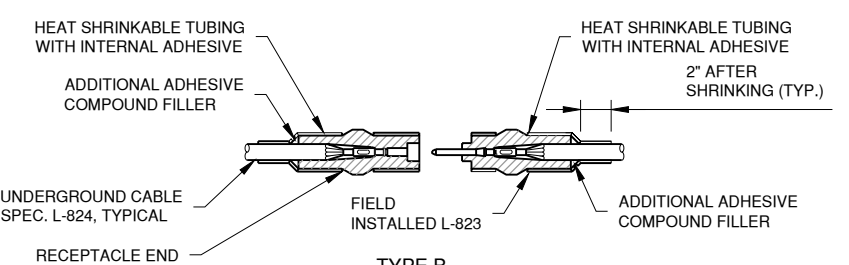
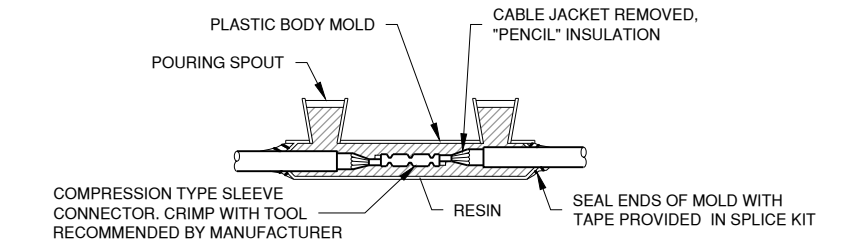
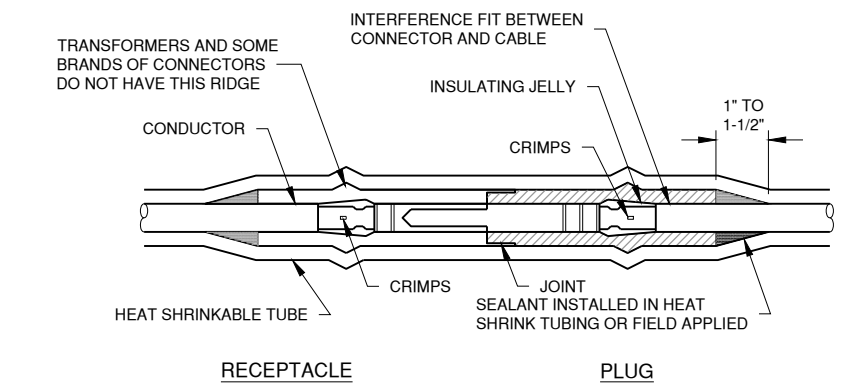
TYPICAL SECTION



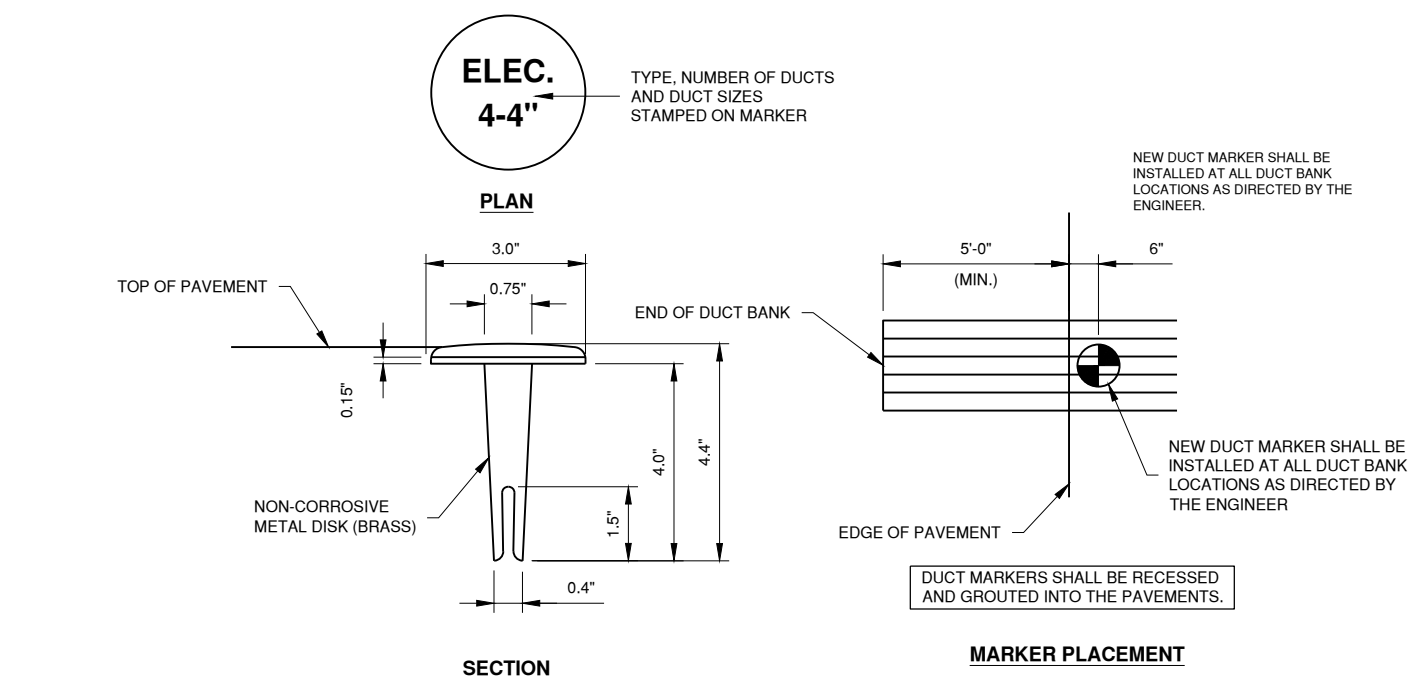
1-WAY DUCT BANK DETAIL

2 N.T.S.

- DUCT BANK NOTES**
- DIMENSIONS SHOWN ARE MINIMUM.
 - TOP OF CONCRETE ENCASEMENT SHALL BE NOT LESS THAN 24" BELOW FINISHED SUBGRADE BELOW PAVEMENTS AND NOT LESS THAN 24" BELOW FINISHED GRADE IN UNPAVED AREAS, EXCEPT WHERE DIRECTED OTHERWISE BY ENGINEER. AVOID ALL CONFLICTS WITH OTHER UTILITIES (UNDERDRAINS, WATER LINES, SEWER LINES, TELEPHONE, ELECTRICAL) OR OTHER OBSTACLES, ADJUSTING DEPTH AS NECESSARY.
 - CONCRETE SHALL BE ITEM P-610.
 - CONDUIT FOR CONCRETE ENCASEMENT SHALL BE SCHEDULE 40 PVC, 4" NOMINAL DIAMETER, OR AS INDICATED ON THE PLANS.
 - CONCRETE ENCASEMENT SHALL EXTEND A MINIMUM OF 5'-0" BEYOND EDGES OF PAVEMENT, OR AS SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER.
 - #4 REBAR SHALL BE INSTALLED CONTINUOUS THE LENGTH OF THE CONCRETE ENCASEMENT.
 - DUCT BANK SHALL BE STACKED NO MORE THAN THREE CONDUITS HIGH UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
 - AT ENDS OF DUCT BANKS, INSTALL A PVC COUPLING FLUSH WITH END OF CONCRETE FOR CONNECTING FUTURE CONDUIT. INSTALL POLYETHYLENE PULL STRING, GREENLEE, OR EQUIVALENT. PLUG THE ENDS OF UNUSED SPARE CONDUITS WITH WOODEN PLUGS.
 - HIGH VOLTAGE WIRING, RUNWAY & TAXIWAY SERIES CIRCUIT WIRING, ETC., AND POWER WIRING OVER 480V SHALL BE INSTALLED IN SEPARATE CONDUITS FROM LOW VOLTAGE WIRING, 480V OR LESS.
 - IF POSSIBLE, INSTALL FIBER OPTIC CABLES AND COMMUNICATION CABLES (FAA, ETC.) IN THEIR OWN CONDUITS; OTHERWISE, INSTALL THEM IN THE CONDUITS WITH LOW VOLTAGE WIRING.
 - DEPTH AS REQUIRED TO NOT IMPACT EDGE DRAIN.



4 CABLE PROTECTION DETAIL
N.T.S.



3 IN-PAVEMENT DUCT MARKER DETAIL
N.T.S.

- NOTES**
- MATCH THE OUTSIDE DIAMETER OF CABLE INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY
 - WRAP WITH AT LEAST ONE LAYER OF RUBBER OR TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.



MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA022
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-EL500.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: CHK		
APPROVED BY: APR		
COPYRIGHT:		

Path: K:\Springfield\018035-05_Rwy_18-36\DrawSheets\PHASE 3\18003505-PH3-EL500.dwg
Date: Tuesday, April 26, 2022 12:49:04 PM

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

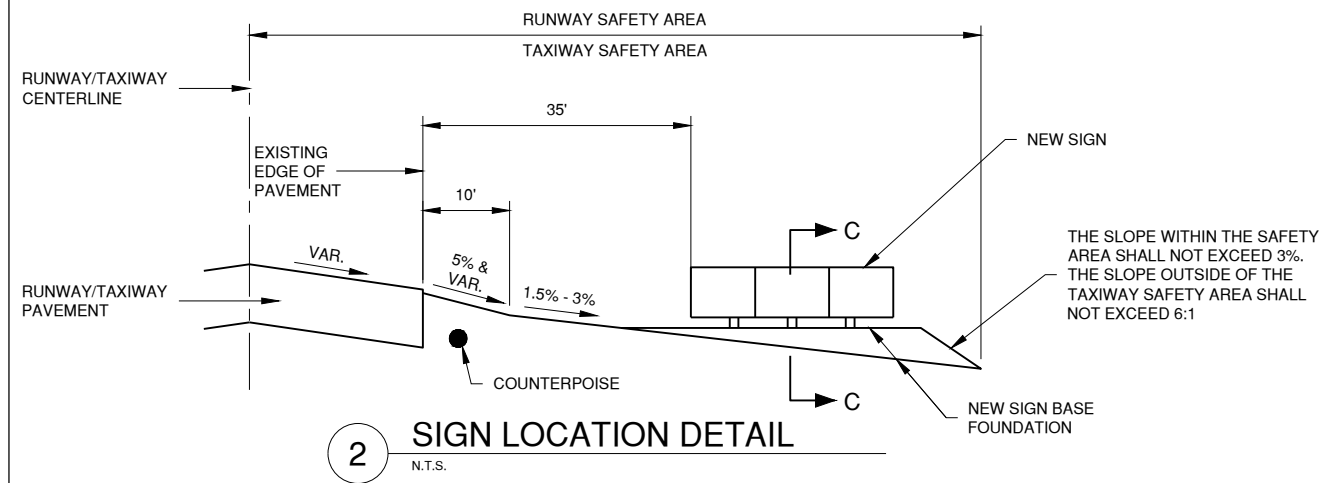
OWNER



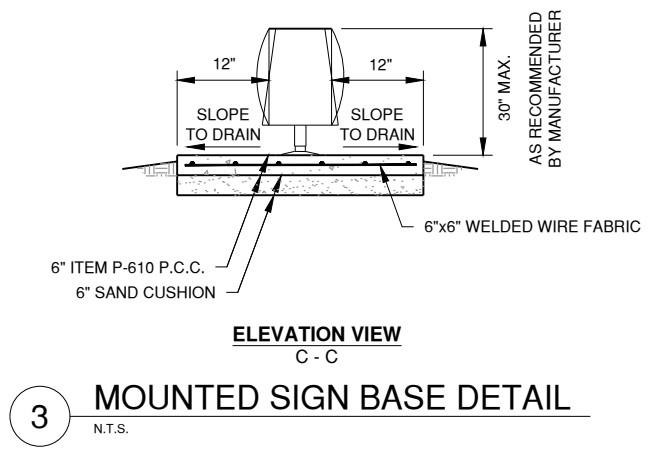
SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

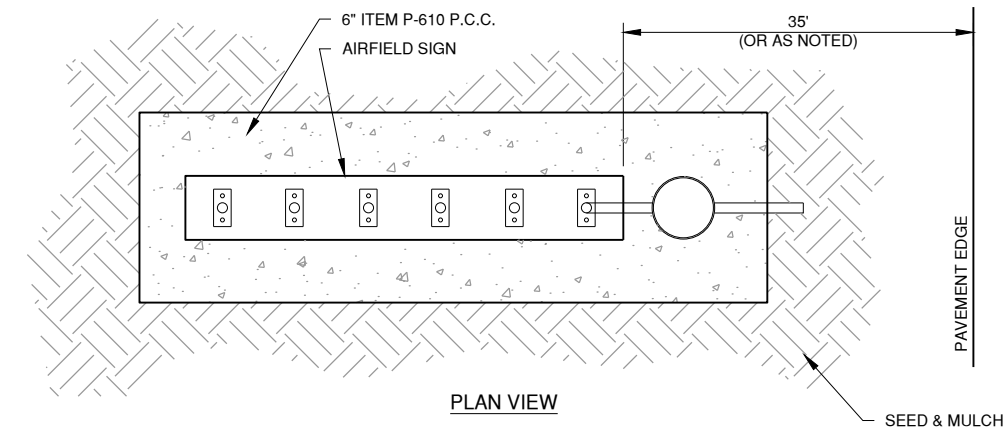
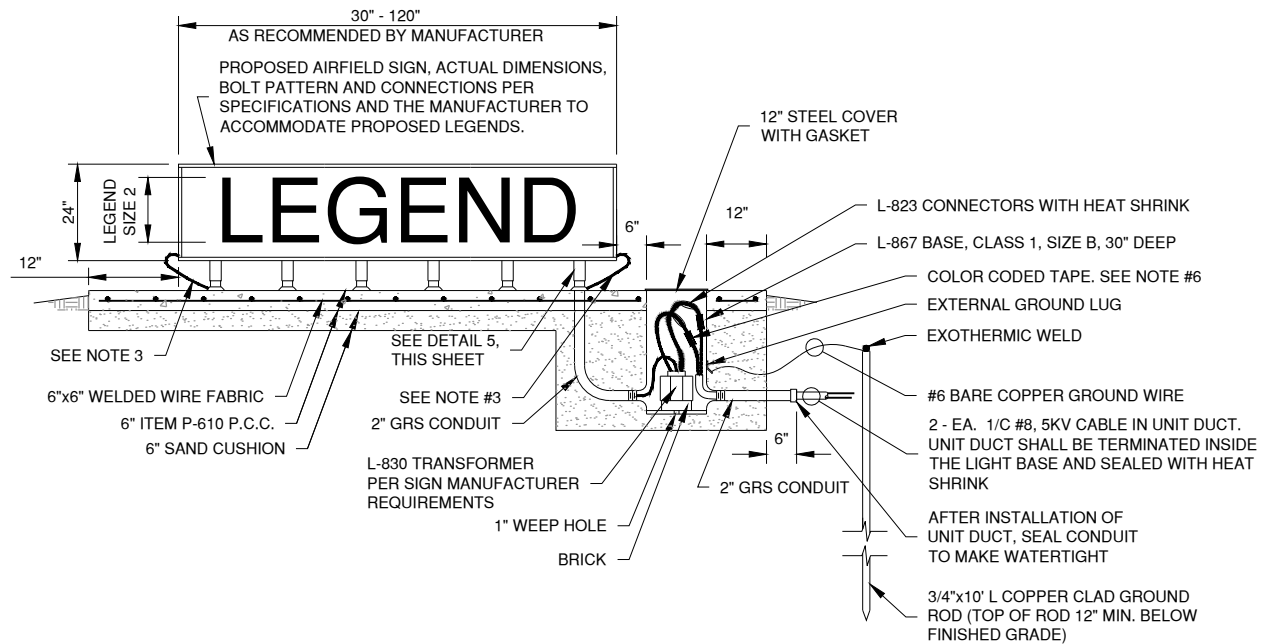
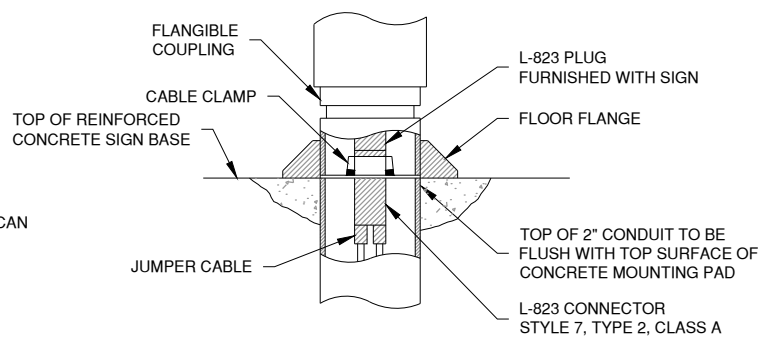
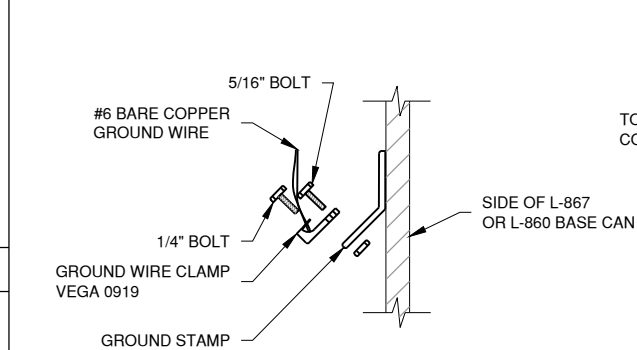
SHEET TITLE
ELECTRICAL DETAILS
4



- SIGN NOTES**
- SLOPES SHOWN ARE FROM FAA STANDARDS AND MAY NOT REFLECT THE ACTUAL GRADES IN THE FIELD
 - ESTIMATED 1 C.Y. OF EMBANKMENT MAY BE REQUIRED TO CONSTRUCT SIGN BASE FOUNDATION. COSTS TO CONSTRUCT SHALL BE INCIDENTAL TO SIGN PAY ITEM.
 - ACTUAL LOCATION OF THE SIGN WITHIN THE TAXIWAY SAFETY AREA WILL VARY DUE TO PAVEMENT WIDTHS AND VARIANCES IN SIGN FOUNDATION LENGTHS.
 - 4" OF KNITTED STRAW MAT SHALL BE PLACED AROUND THE PROTECTION APRON. COST FOR MAT SHALL BE INCIDENTAL TO SIGN PAY ITEM.



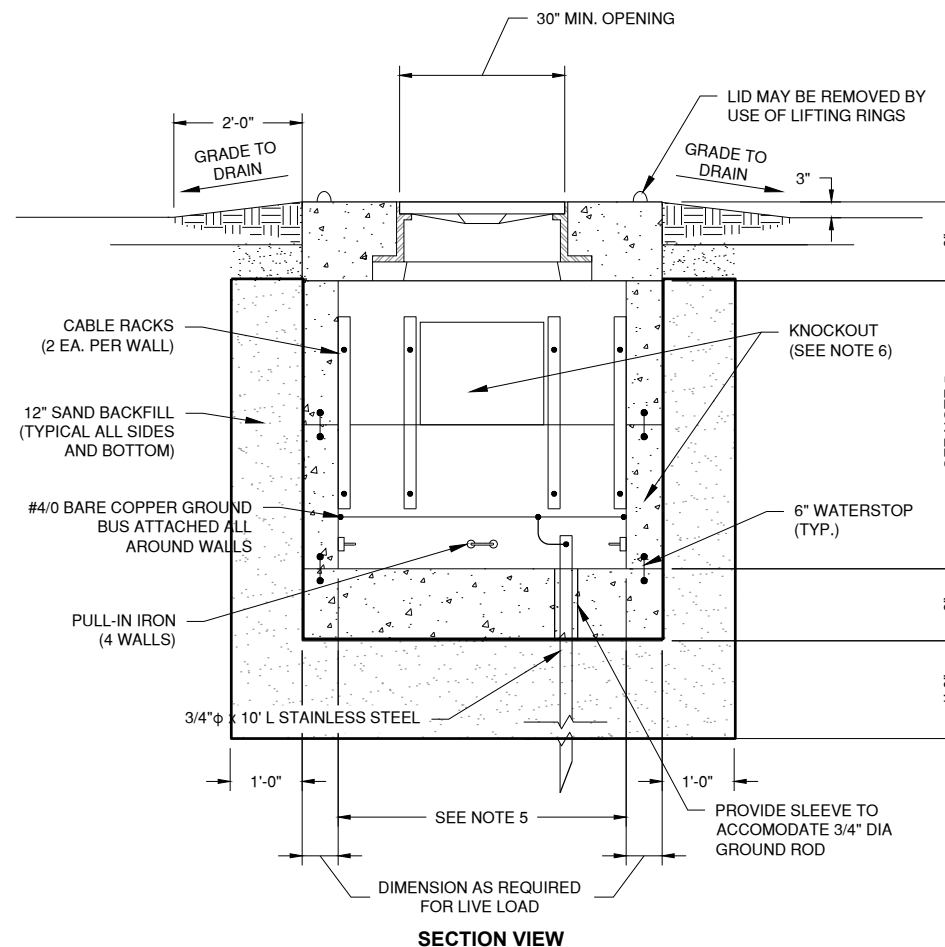
NOTE: SEE COUNTERPOISE LOCATION DETAIL SHEET EL501



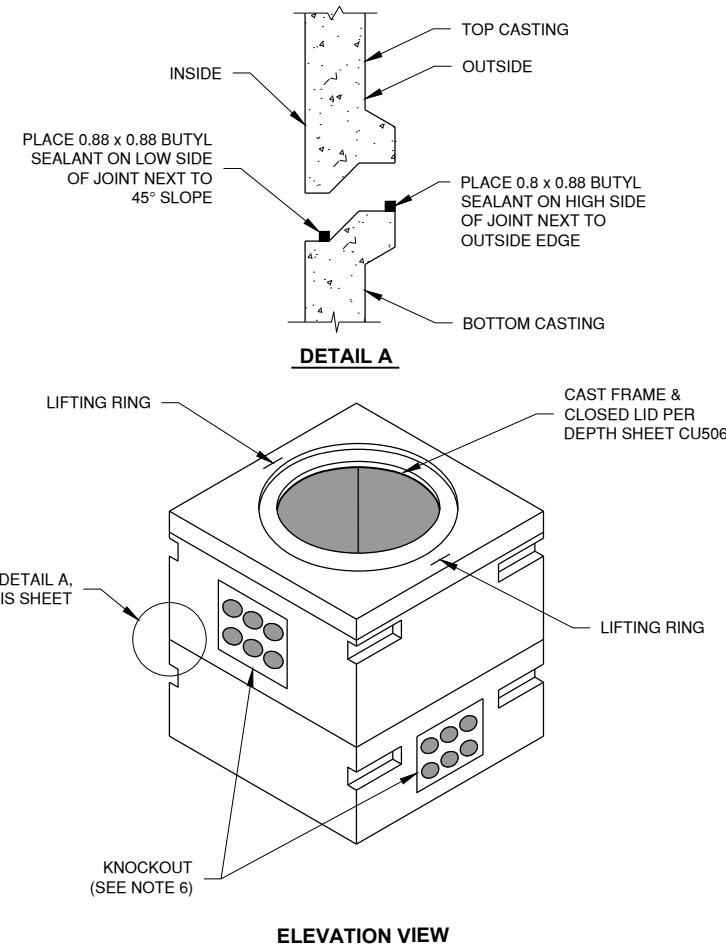
- AIRFIELD SIGN NOTES**
- TRANSFORMER WATTAGE SHALL BE AS REQUIRED BY SIGN MANUFACTURER. SIGNS ON RUNWAY CIRCUITS SHALL BE STYLE 2 OR 3 DEPENDING ON REGULATOR.
 - SIGN LEGEND SHALL BE AS SHOWN IN THE PLANS. SIGN SCHEDULE IS SUBJECT TO FAA APPROVAL OF THE SIGNAGE PLAN. CHANGES TO NEW LEGENDS MAY OCCUR DURING CONSTRUCTION.
 - SIGN ANCHOR TETHERS AND GROUND WIRES ARE REQUIRED. SEE SPECIFICATIONS.
 - SIGNS SHALL BE SIZE 2, STYLE 2 OR 3, CLASS 2, AND MODE 2. SEE SIGN SCHEDULE FOR DETAILS
 - LIGHT I.D. TAG FOR SIGN SHALL INCLUDE SIGN DESIGNATOR SHOWN IN THE PLAN TABLES.
 - DIRECTION OF PRIMARY CABLES MUST BE IDENTIFIED BY COLOR CODING AS FOLLOWS: WHEN FACING SIGN IN BACK FACING THE RELATED RUNWAY OR TAXIWAY PAVEMENT, THE CABLE FOR THE CIRCUIT TO THE LEFT IS CODED RED AND CABLE FOR THE CIRCUIT TO THE RIGHT IS CODED BLUE.

NOTES

1. THE HANDHOLE/GRADE RING/HANDHOLE LID ASSEMBLY SHALL BE CONSTRUCTED TO MEET OR EXCEED THE FOLLOWING LOADINGS:
 - A. EARTHLOAD = 2 FEET FILL AT 130 LBS/FT .
 - B. SURCHARGE = 2 FEET FILL AT 130 LBS/FT .
 - C. LIVE LOAD = A.A.S.H.T.O. HS-20 TRUCK WITH 20% IMPACT
 - D. $f_c = 4,500$ P.S.I.
 - E. $f_y = 60,000$ P.S.I.
 - F. ULTIMATE STRENGTH DESIGN METHOD
 THE SUPPLIER SHALL PROVIDE CERTIFICATION THAT THE HANDHOLES MEET OR EXCEED THESE REQUIREMENTS PRIOR TO INSTALLATION.
2. THE HANDHOLE CONSTRUCTION AND INSTALLATION SHALL BE WATERTIGHT. ALL CONSTRUCTION JOINTS AND DUCTS SHALL BE SEALED TO PREVENT WATER ENTRY. ALL UNUSED DUCT BANK OPENINGS IN HANDHOLE SHALL BE SEALED WITH METAL PLATES TREATED FOR CORROSION RESISTANCE AND BOLTED INTO PLACE. MATING SURFACES SHALL BE SEALED USING BUTYL SEALANT.
3. THE HANDHOLE LID ASSEMBLY SHALL BE INSTALLED SLIGHTLY ABOVE THE SURROUNDING FINAL GRADE AND THE EARTH SHALL BE GRADED TO IT.
4. THE HANDHOLE COVER SHALL BE LOCKABLE UTILIZING A PENTAGON BOLT ASSEMBLY.
5. PROPOSED ELECTRICAL HANDHOLE SHALL BE THE FOLLOWING INTERIOR DIMENSIONS: 4' L x 4' W x 4' H
6. SINGLE HANDHOLES: KNOCKOUTS SHALL BE CENTERED IN THE HANDHOLE WALL AND SHALL BE SIZED AS REQUIRED FOR PROPOSED DUCT BANK.
7. HANDHOLES THAT MAKE UP A HANDHOLE PLAZA: THE WALL KNOCKOUTS FOR THE NORTH/SOUTH WALLS SHALL BE PLACED AT HIGHER OR LOWER ELEVATIONS THAN THE WALL KNOCKOUTS FOR THE EAST/WEST WALLS TO ALLOW THE DUCTS TO CROSS. KNOCKOUTS SHALL BE SIZED AS REQUIRED FOR PROPOSED DUCT BANK.
8. THE HANDHOLE CONCRETE TOP LID SHALL BE SET THAT IF DESIRED, THE CONCRETE TOP LID MAY BE REMOVED BY USE OF THE LIFTING RINGS.
9. CAST FRAME & CLOSED LID SHALL BE INCLUDED.



1 ELECTRICAL HANDHOLE DETAIL
N.T.S.



ELEVATION VIEW



License No. 184-000613
CONSULTANTS

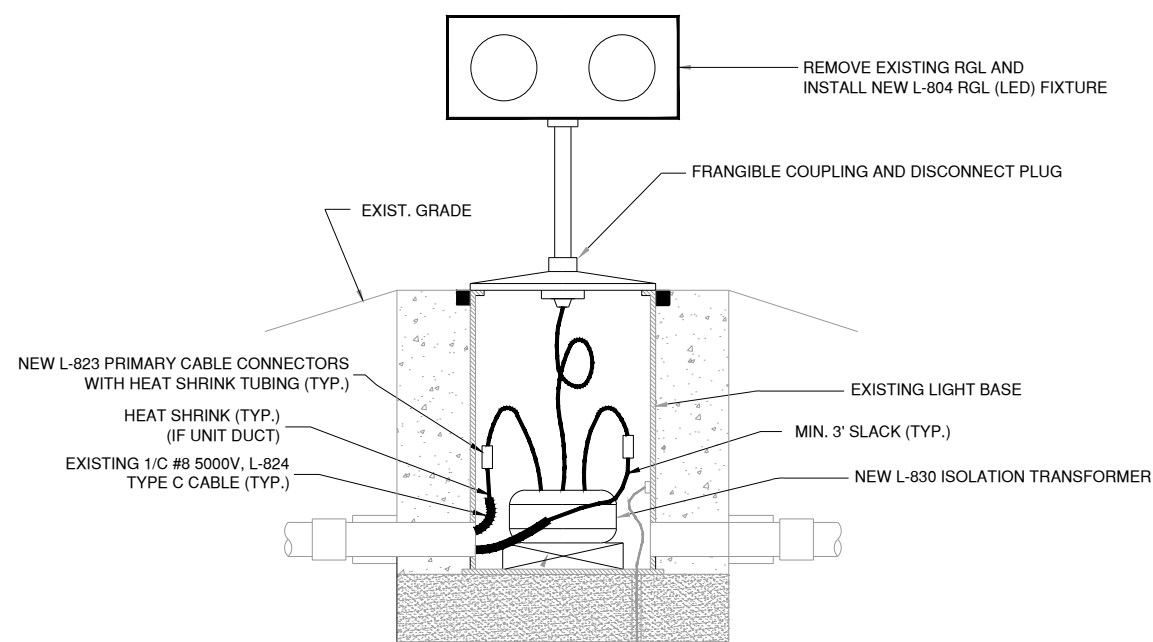
BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL



NOTES

1. THIS ITEM WILL INCLUDE THE REMOVAL OF THE EXISTING RGL FIXTURE AND TRANSFORMER AND INSTALLING A NEW RGL AND TRANSFORMER.

2 RUNWAY GUARD LIGHT DETAIL
N.T.S.

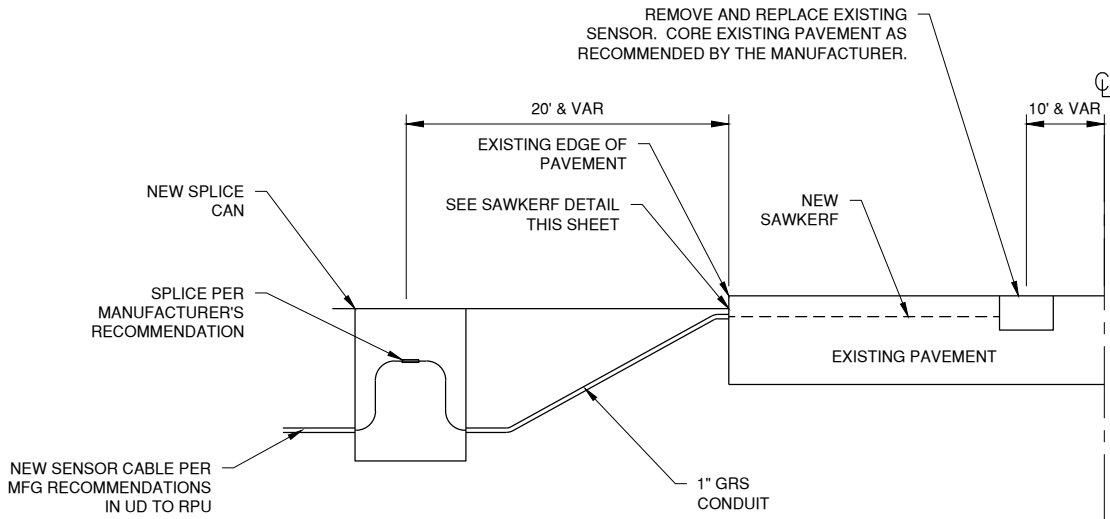
Path: K:\Springfield\0118035-05_Rwy_18-36\Draw\Sheets\PH3-EL500.dwg
Date: Tuesday, April 26, 2022 12:49:05 PM

MARK	DATE	DESCRIPTION

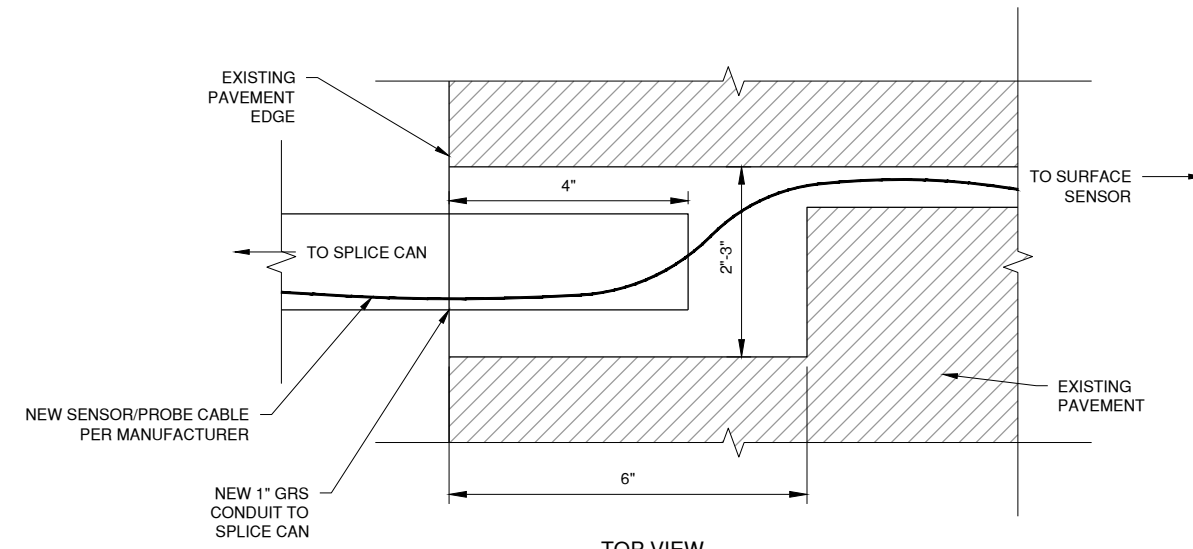
AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-EL500.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

SHEET TITLE
ELECTRICAL DETAILS
5

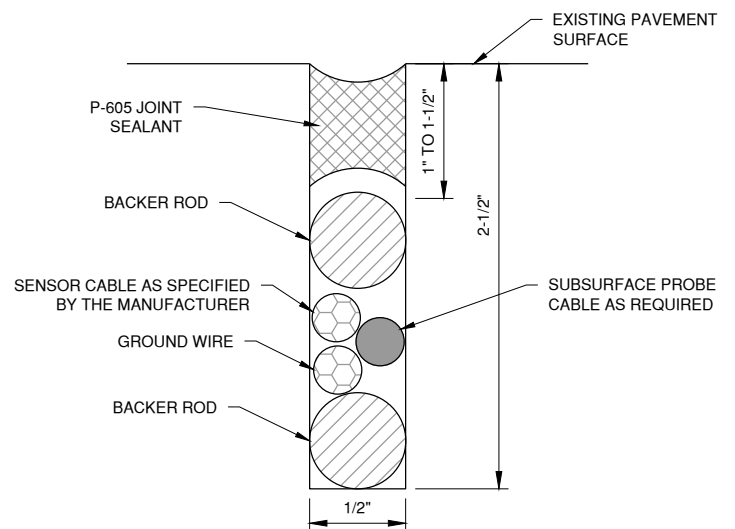
EL505
SHEET **61** OF **91**



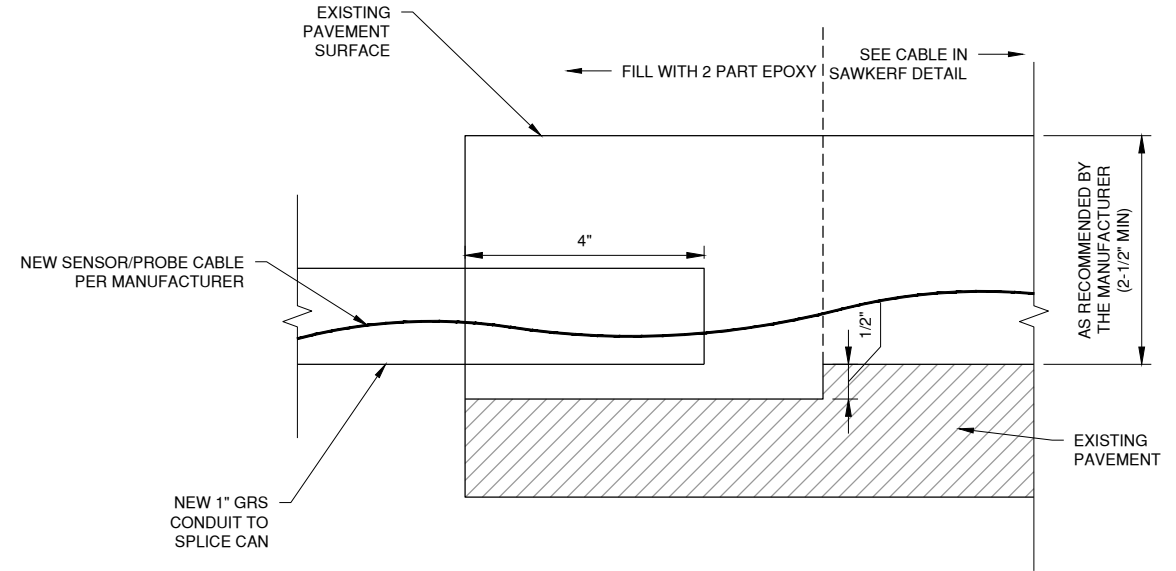
SENSOR INSTALLATION OVERVIEW
N.T.S.



TOP VIEW



CABLE IN SAWKERF
N.T.S.



SECTION VIEW

SAWKERF DETAIL
N.T.S.

SURFACE SENSOR NOTES

1. SENSOR INSTALLATION SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
2. SENSORS ARE TO BE REPLACED IN THEIR CURRENT LOCATION. SAWKERFS SHALL BE LAID OUT AND APPROVED BY THE RPR PRIOR TO BEGINNING INSTALLATION.
3. REMOVAL AND REPLACEMENT OF SENSOR SPLICE CANS SHALL BE CONSIDERED INCIDENTAL TO THE RUNWAY WEATHER INFORMATION SYSTEM PAY ITEM.

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

SHEET TITLE
ELECTRICAL DETAILS
6

Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\EL500.dwg
Date: Tuesday, April 26, 2022 12:49:07 PM



MARK	DATE	DESCRIPTION
AIP PROJ. NO:	3-17-0096-XX	CA022
IL PROJ. NO:	SPI-4943	
CMT PROJECT NO:	180035-05	
CAD DWG FILE:	18003505-PH3-EL500.DWG	
DESIGNED BY:	HWI	
DRAWN BY:	DPA	
CHECKED BY:	CHK	
APPROVED BY:	APR	
COPYRIGHT:		

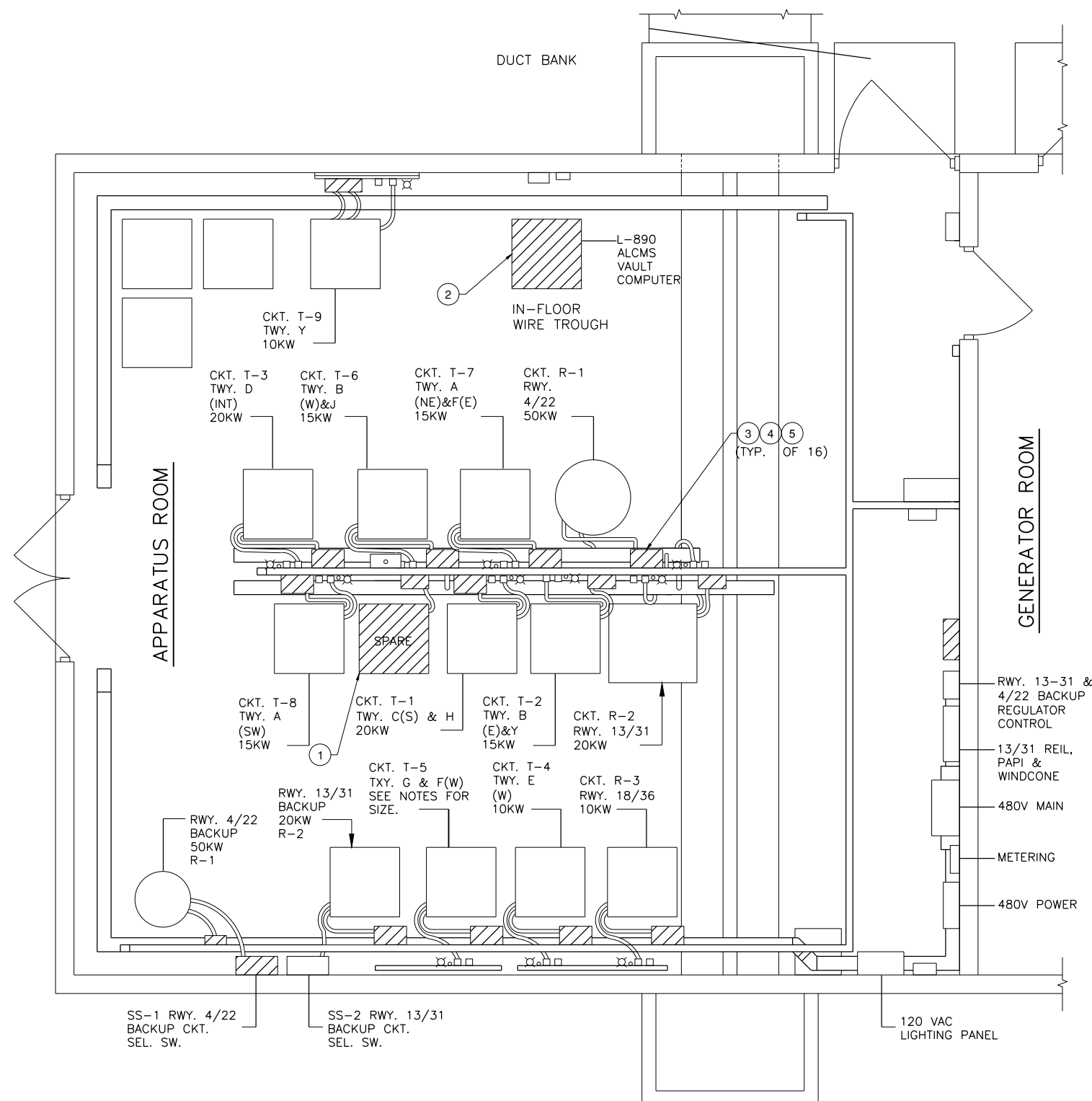
SHEET TITLE
EXISTING VAULT

GENERAL NOTES

- EQUIPMENT NOT CALLED OUT TO BE REMOVED SHALL REMAIN UNLESS NOTED OTHERWISE.
- EXISTING VAULT GROUNDING SYSTEM SHALL REMAIN AND SHALL BE PROTECTED FROM ANY DAMAGE DURING CONSTRUCTION.
- EXISTING AIRFIELD LIGHTING CIRCUITS SHALL REMAIN OPERATIONAL UNTIL NEW EQUIPMENT IS READY TO BE INSTALLED.

KEYED NOTES #

- EXISTING SPARE REGULATOR TO BE REMOVED & LEFT IN VAULT AT LOCATION ACCEPTABLE TO THE AIRPORT.
- EXISTING ALCMS VAULT COMPUTER TO BE REMOVED.
- EXISTING ACE-2 UNITS FOR ALCMS TO BE REMOVED.
- REMOVE ALL CONTROL CONDUCTORS BETWEEN EXISTING ACE-2 UNITS AND REGULATORS. CONDUIT TO REMAIN AND SHALL BE RE-USED.
- REMOVE ALL COMMUNICATION CONDUCTORS BETWEEN ACE-2 UNITS AND ALCMS COMPUTER RACK. CONDUIT TO REMAIN AND SHALL BE RE-USED.



EXISTING VAULT PLAN
N.T.S.

Path: K:\Springfield\0180035-05 Runway 18-36 Draw\Sheets\PH3\EL500.dwg
Date: Tuesday, April 26, 2022 12:49:08 PM

GENERAL NOTES

1. THE CONTRACTOR SHALL FURNISH, INSTALL, RELOCATE, CONNECT AND TEST ALL EQUIPMENT ACCESSORIES, CONDUIT CABLES, WIRES, BUSES, GROUNDS AND SUPPORT NECESSARY TO INSURE A COMPLETE AND OPERABLE ELECTRICAL DISTRIBUTION FACILITY FOR THE AIRPORT LIGHTING POWER AND CONTROL SYSTEM AS SPECIFIED IN THE SUBMITTAL PACKAGE.
2. THE ALCMS MANUFACTURER SHALL BE LISTED IN THE FAA APPROVED EQUIPMENT LIST, AC 150/5345-53 (CURRENT EDITION) AS A FAA APPROVED SUPPLIER OF L-890 AIRFIELD LIGHTING COMPUTER AND MONITORING SYSTEMS IN ACCORDANCE WITH AC 150/5345-56 (CURRENT EDITION).
3. EXISTING EQUIPMENT IS SHOWN FOR INFORMATION ONLY. NEW WORK IS SHOW IN BOLD.
4. CONTRACTOR SHALL COORDINATE WITH NEW ALCMS MANUFACTURER AND PROVIDE ANY REQUIRED COMPONENTS FOR EXISTING AND NEW REGULATORS FOR A COMPLETE L-829 AND L-890 ALCMS SYSTEM.
5. THE CONSTANT CURRENT REGULATOR MANUFACTURER SHALL BE LISTED AS AN FAA APPROVED SUPPLIER OF L-827/L-829 CONSTANT CURRENT REGULATORS IN ACCORDANCE WITH AC 150/5345-10 (CURRENT EDITION).

KEYED NOTES #

1. NEW FAA L-890 ALCMS VAULT COMPUTER RACK. PROPOSED ALCMS RACK SHALL BE SUPPLIED WITH WHEELS. NEW CONTROL SYSTEM SHALL BE OPERATIONAL AND TESTED PRIOR TO REMOVAL OF EXISTING SYSTEM.
2. NEW 10KW L-829 3-STEP REGULATOR, 480VAC INPUT/ 6.6A OUTPUT FOR TAXIWAY G(E) CIRCUIT T-10.
3. EXISTING REGULATOR TO BE REMOVED TO SERVE AS A SPARE. CONTRACTOR SHALL COORDINATE STORAGE LOCATION AND DELIVERY WITH AIRPORT.
4. NEW DIGITAL CONTROL INTERFACE UNIT FOR EACH REGULATOR MOUNTED ON EXISTING EQUIPMENT PANEL.
5. INSTALL NEW CONTROLS CONDUCTORS AS REQUIRED BY NEW ALCMS MANUFACTURER BETWEEN NEW INTERFACE UNITS AND EACH REGULATOR IN EXISTING CONDUITS. CONTRACTOR SHALL COORDINATE WITH NEW ALCMS MANUFACTURER AND PROVIDE ANY REQUIRED COMPONENTS FOR EXISTING AND NEW REGULATORS FOR A COMPLETE L-829 AND L-890 ALCMS SYSTEM.
6. INSTALL NEW COMMUNICATION CONDUCTORS AS REQUIRED BY NEW ALCMS MANUFACTURER BETWEEN NEW INTERFACE UNITS AND NEW ALCMS IN EXISTING CONDUITS.
7. CONTRACTOR TO FIELD SURVEY ALL EXISTING REGULATORS FOR MAKE/MODEL OF REGULATORS. FURNISH AND INSTALL ALL REQUIRED COMPONENTS TO BE COMPATIBLE WITH NEW ALCMS FOR A COMPLETE L-829 AND L-890 SYSTEM.

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

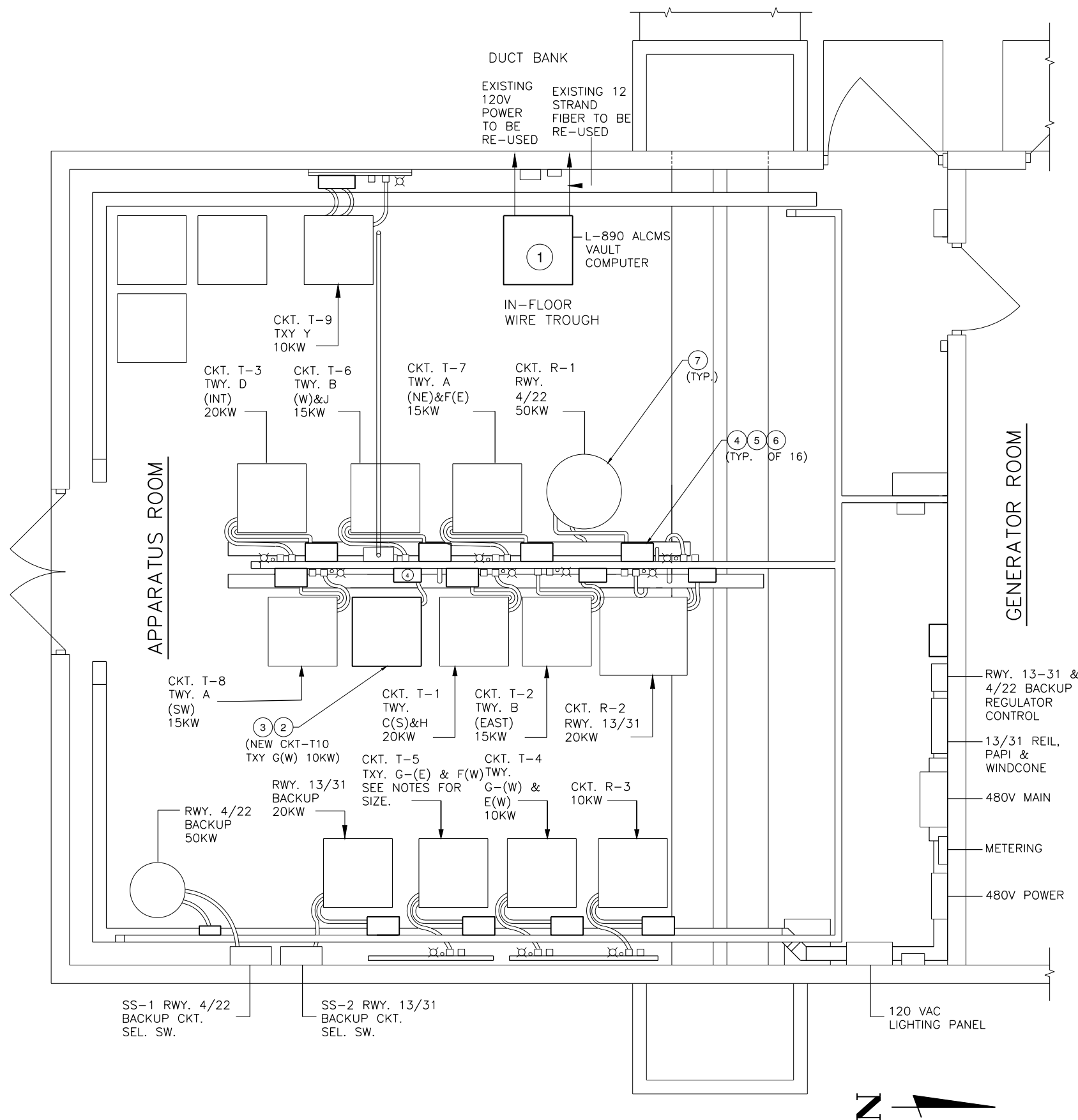
OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA022
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-EL500.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: CHK		
APPROVED BY: APR		
COPYRIGHT:		

SHEET TITLE
PROPOSED VAULT IMPROVEMENTS



VAULT ELECTRICAL PLAN
N.T.S.



Path: K:\Springfield\0118035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-EL500.dwg
Date: Tuesday, April 26, 2022 12:49:09 PM

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION
		AIP PROJ. NO: 3-17-0096-XX CA022
		IL PROJ. NO: SPI-4943
		CMT PROJECT NO: 180035-05
		CAD DWG FILE: 18003505-PH3-EL500.DWG
		DESIGNED BY: HWI
		DRAWN BY: DPA
		CHECKED BY: CHK
		APPROVED BY: APR
		COPYRIGHT:

SHEET TITLE
**ALCMS BLOCK
DIAGRAM - EXISTING**

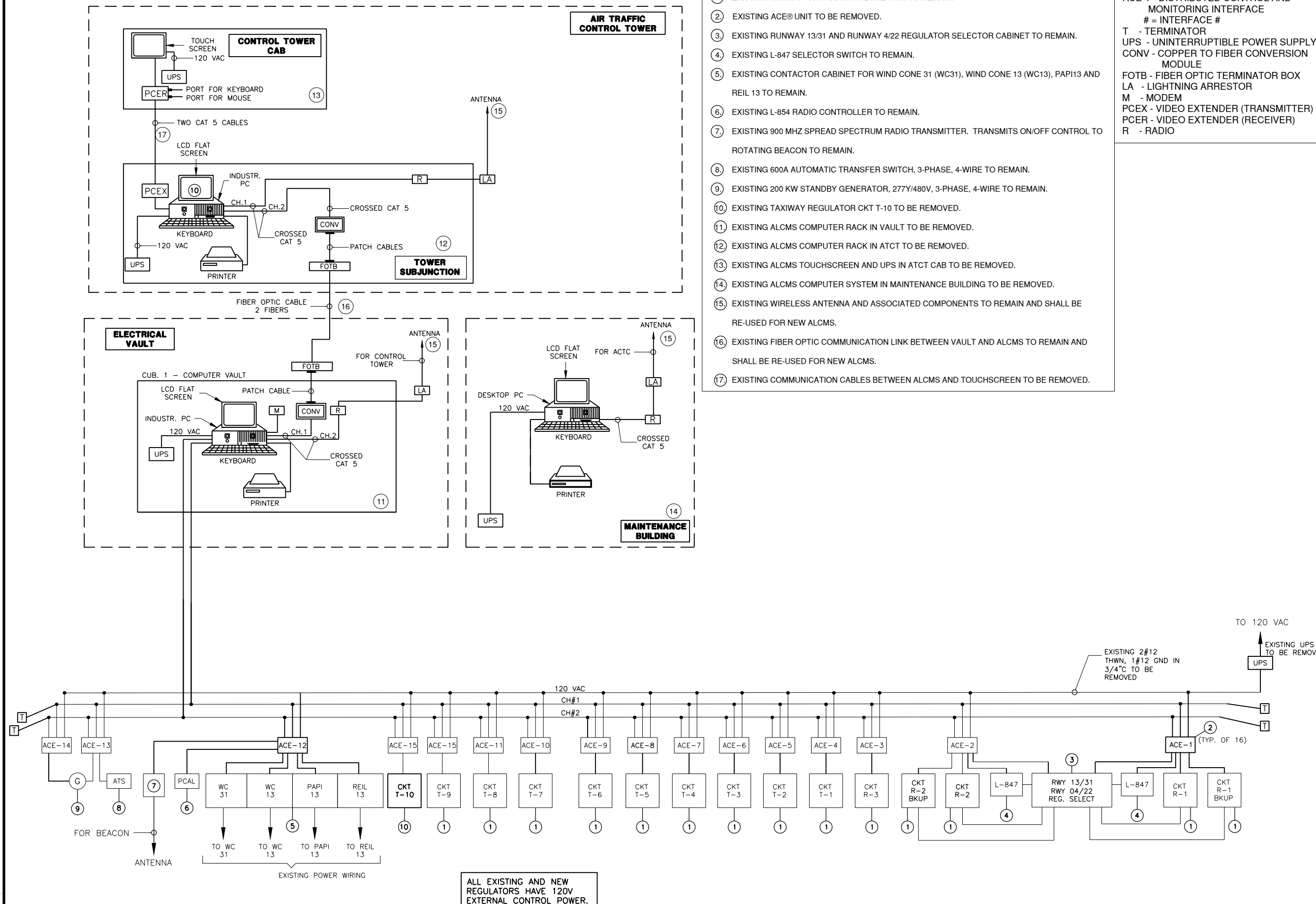
EL509
SHEET 65 OF 91

KEYED NOTES

- ① EXISTING RUNWAY OR TAXIWAY REGULATOR TO REMAIN.
- ② EXISTING ACE® UNIT TO BE REMOVED.
- ③ EXISTING RUNWAY 13/31 AND RUNWAY 4/22 REGULATOR SELECTOR CABINET TO REMAIN.
- ④ EXISTING L-847 SELECTOR SWITCH TO REMAIN.
- ⑤ EXISTING CONTACTOR CABINET FOR WIND CONE 31 (WC31), WIND CONE 13 (WC13), PAPI13 AND REIL 13 TO REMAIN.
- ⑥ EXISTING L-854 RADIO CONTROLLER TO REMAIN.
- ⑦ EXISTING 900 MHZ SPREAD SPECTRUM RADIO TRANSMITTER. TRANSMITS ON/OFF CONTROL TO ROTATING BEACON TO REMAIN.
- ⑧ EXISTING 600A AUTOMATIC TRANSFER SWITCH, 3-PHASE, 4-WIRE TO REMAIN.
- ⑨ EXISTING 200 KW STANDBY GENERATOR, 277Y/480V, 3-PHASE, 4-WIRE TO REMAIN.
- ⑩ EXISTING TAXIWAY REGULATOR CKT T-10 TO BE REMOVED.
- ⑪ EXISTING ALCMS COMPUTER RACK IN VAULT TO BE REMOVED.
- ⑫ EXISTING ALCMS COMPUTER RACK IN ATCT TO BE REMOVED.
- ⑬ EXISTING ALCMS TOUCHSCREEN AND UPS IN ATCT CAB TO BE REMOVED.
- ⑭ EXISTING ALCMS COMPUTER SYSTEM IN MAINTENANCE BUILDING TO BE REMOVED.
- ⑮ EXISTING WIRELESS ANTENNA AND ASSOCIATED COMPONENTS TO REMAIN AND SHALL BE RE-USED FOR NEW ALCMS.
- ⑯ EXISTING FIBER OPTIC COMMUNICATION LINK BETWEEN VAULT AND ALCMS TO REMAIN AND SHALL BE RE-USED FOR NEW ALCMS.
- ⑰ EXISTING COMMUNICATION CABLES BETWEEN ALCMS AND TOUCHSCREEN TO BE REMOVED.

L-890 BLOCK DIAGRAM LEGEND

- ACE-1 - DISTRIBUTED CONTROL AND MONITORING INTERFACE
= INTERFACE
- T - TERMINATOR
- UPS - UNINTERRUPTIBLE POWER SUPPLY
- CONV - COPPER TO FIBER CONVERSION MODULE
- FOTB - FIBER OPTIC TERMINATOR BOX
- LA - LIGHTNING ARRESTOR
- M - MODEM
- PCEX - VIDEO EXTENDER (TRANSMITTER)
- PCER - VIDEO EXTENDER (RECEIVER)
- R - RADIO



ALL EXISTING AND NEW
REGULATORS HAVE 120V
EXTERNAL CONTROL POWER.

EXISTING L-890 ALCMS BLOCK DIAGRAM

N.T.S.

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA022
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-EL500.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: CHK		
APPROVED BY: APR		
COPYRIGHT:		

SHEET TITLE
**ALCMS BLOCK
DIAGRAM -
PROPOSED**

EL510
SHEET 66 OF 91

KEYED NOTES

- NEW DISTRIBUTED CONTROL AND MONITORING UNIT (DCMU), COMBO AS MANUFACTURED BY NEW ALCMS MANUFACTURER. MICROPROCESSOR BASED CONTROL/ MONITOR PANEL WITH REMOTE AND LOCAL CONTROL FOR CCR STATUS, DIMMING CONTROL AND CABLE INSULATION MONITOR.
- NEW FAA L-890 COMPUTER RACK AND ASSOCIATED EQUIPMENT. RE-CONNECT EXISTING POWER, FIBER OPTIC CABLE AND WIRELESS COMMUNICATION LINK, INSTALL NEW COMMUNICATION TO DCMU'S
- NEW ALCMS REMOTE COMPUTER SYSTEM FOR MAINTENANCE ACCESS. RE-CONNECT EXISTING POWER AND WIRELESS COMMUNICATION LINK.
- NEW ALCMS COMPUTER RACK AND ASSOCIATED EQUIPMENT. RECONNECT EXISTING POWER, FIBER OPTIC CABLE AND WIRELESS COMMUNICATION LINK. INSTALL NEW DATA CABLES TO TOUCH SCREEN IN CAB. (SEE GENERAL NOTE 3)
- NEW ALCMS TOUCHSCREEN AND UPS IN ATCT CAB. (SEE GENERAL NOTE 3)
- NEW ALCMS LAPTOP WITH CELLULAR COMMUNICATION AND ALCMS SOFTWARE.

GENERAL NOTES

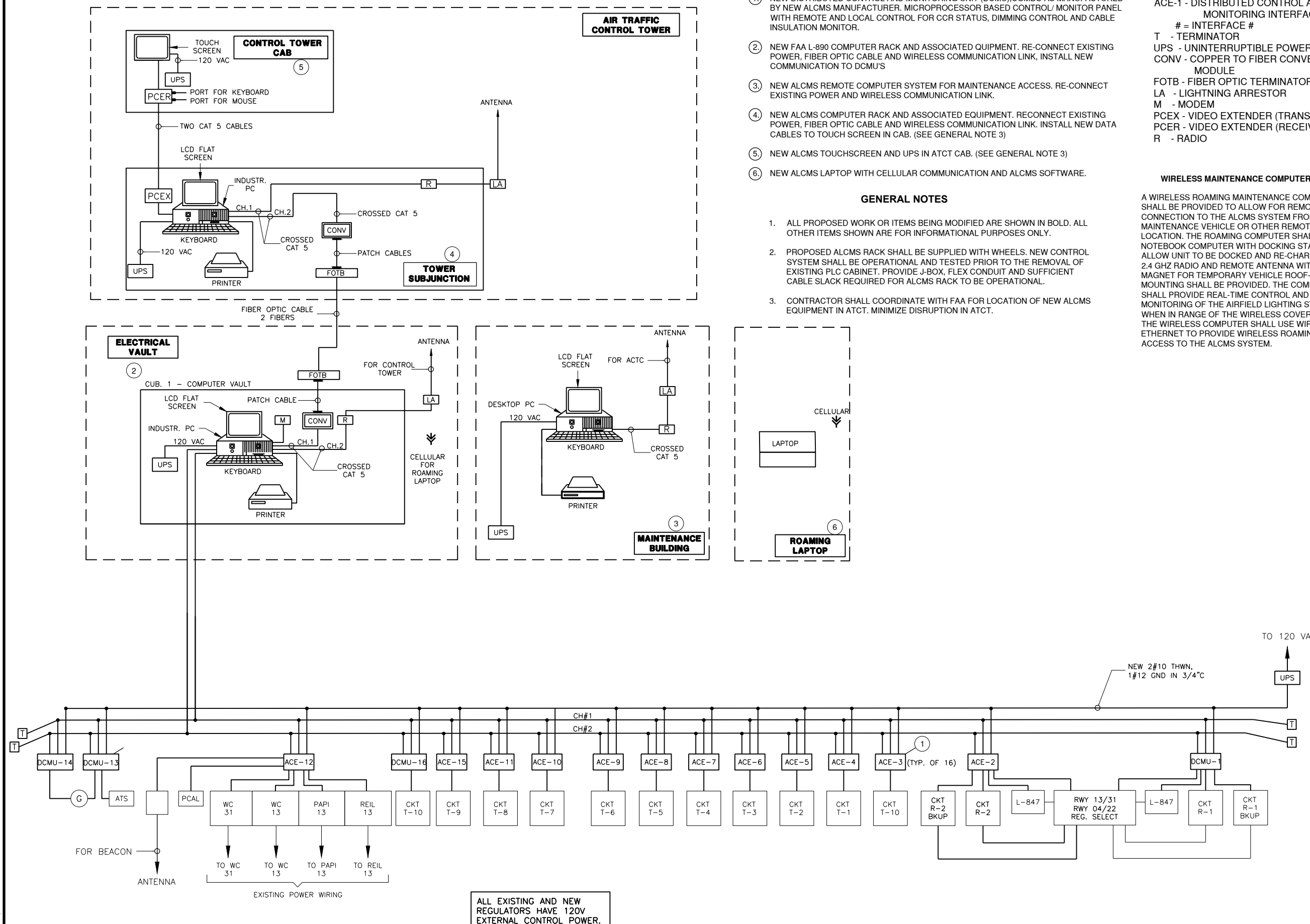
- ALL PROPOSED WORK OR ITEMS BEING MODIFIED ARE SHOWN IN BOLD. ALL OTHER ITEMS SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY.
- PROPOSED ALCMS RACK SHALL BE SUPPLIED WITH WHEELS. NEW CONTROL SYSTEM SHALL BE OPERATIONAL AND TESTED PRIOR TO THE REMOVAL OF EXISTING PLC CABINET. PROVIDE J-BOX, FLEX CONDUIT AND SUFFICIENT CABLE SLACK REQUIRED FOR ALCMS RACK TO BE OPERATIONAL.
- CONTRACTOR SHALL COORDINATE WITH FAA FOR LOCATION OF NEW ALCMS EQUIPMENT IN ATCT. MINIMIZE DISRUPTION IN ATCT.

L-890 BLOCK DIAGRAM LEGEND

ACE-1 - DISTRIBUTED CONTROL AND MONITORING INTERFACE
= INTERFACE #
T - TERMINATOR
UPS - UNINTERRUPTIBLE POWER SUPPLY
CONV - COPPER TO FIBER CONVERSION MODULE
FOTB - FIBER OPTIC TERMINATOR BOX
LA - LIGHTNING ARRESTOR
M - MODEM
PCEX - VIDEO EXTENDER (TRANSMITTER)
PCER - VIDEO EXTENDER (RECEIVER)
R - RADIO

WIRELESS MAINTENANCE COMPUTER

A WIRELESS ROAMING MAINTENANCE COMPUTER SHALL BE PROVIDED TO ALLOW FOR REMOTE CONNECTION TO THE ALCMS SYSTEM FROM A MAINTENANCE VEHICLE OR OTHER REMOTE LOCATION. THE ROAMING COMPUTER SHALL BE A NOTEBOOK COMPUTER WITH DOCKING STATION TO ALLOW UNIT TO BE DOCKED AND RE-CHARGED. A 2.4 GHZ RADIO AND REMOTE ANTENNA WITH MAGNET FOR TEMPORARY VEHICLE ROOF-TOP MOUNTING SHALL BE PROVIDED. THE COMPUTER SHALL PROVIDE REAL-TIME CONTROL AND MONITORING OF THE AIRFIELD LIGHTING SYSTEM WHEN IN RANGE OF THE WIRELESS COVERAGE. THE WIRELESS COMPUTER SHALL USE WIRELESS ETHERNET TO PROVIDE WIRELESS ROAMING ACCESS TO THE ALCMS SYSTEM.



ALL EXISTING AND NEW
REGULATORS HAVE 120V
EXTERNAL CONTROL POWER.

NEW L-890 ALCMS BLOCK DIAGRAM

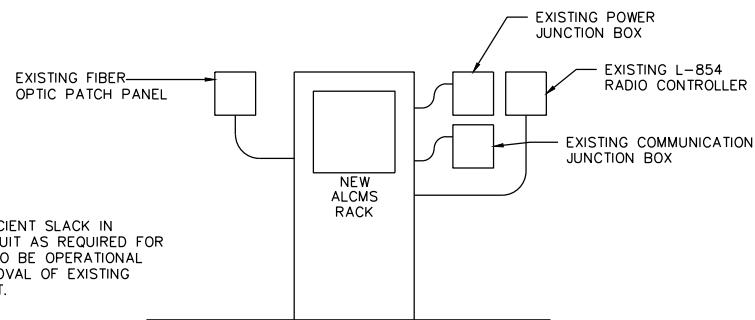
N.T.S.

NOTES

- 1. INSTALL NEW DISTRIBUTED CONTROL AND MONITORING UNIT (DCMU) ON UNITSTRUT. MATCH EXISTING CONDITIONS. COORDINATE SIZE AND CONDUIT OPENINGS WITH ALCMS MANUFACTURER.
- 2. INSTALL MALE AND FEMALE L-823 CONNECTOR TO CONNECT AIRFIELD LIGHTING CIRCUITS IN HIGH VOLTAGE WIREWAY.

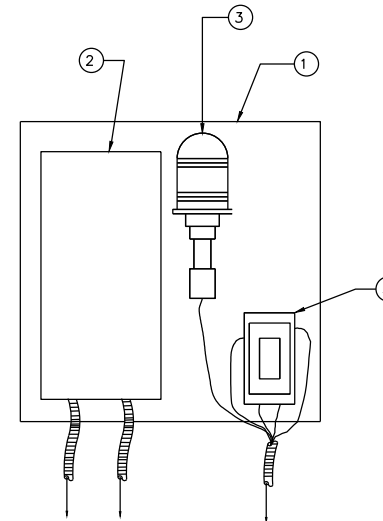
EQUIPMENT NOMENCLATURE

- ① EXISTING EQUIPMENT MOUNTING PLATE TO REMAIN.
- ② EXISTING REGULATOR DISCONNECT SWITCH TO REMAIN.
- ③ EXISTING RUNWAY/TAXIWAY INDICATOR LIGHT.
- ④ EXISTING S-1 CUT-OUT.
- ⑤ NEW DISTRIBUTED CONTROL AND MONITORING UNIT (DCMU) - SEE NOTE 1.
- ⑥ EXISTING DATA/CONTROL CABLES (AS REQUIRED BY ALCMS MANUFACTURER) IN EXISTING/NEW CONDUIT TO EXISTING REGULATOR.
- ⑦ EXISTING LOW VOLTAGE AND HIGH VOLTAGE 12"x12" WIREWAY - SEE NOTE 2.
- ⑧ EXISTING INCOMING AND OUTGOING 600V POWER CABLES IN FLEX CONDUIT TO LOW VOLTAGE WIREWAY.
- ⑨ EXISTING 4 #8 5KV AIRFIELD LIGHTING CABLES IN FLEX CONDUIT TO HIGH VOLTAGE WIREWAY.
- ⑩ EXISTING (2) 24 AWG SHIELDED TWISTED PAIRS OR AS REQUIRED BY ALCMS MANUFACTURER IN EXISTING CONDUIT.
- ⑪ NOT USED
- ⑫ EXISTING 2 #8 5KV, L-824 TYPE C AIRFIELD LIGHTING CABLES IN 1" FLEX CONDUIT TO HIGH VOLTAGE WIREWAY.
- ⑬ NEW 2 #10 THWN, 1 #12 GND. IN EXISTING CONDUIT TO LOW VOLTAGE WIREWAY FOR UPS POWER.
- ⑭ EXISTING 2 #8 5KV, L-824 TYPE C CABLES IN CONDUIT TO S-1 PLUG CUT-OUT.
- ⑮ NEW 2 #10 THWN, 1 #12 GND. IN EXISTING CONDUIT.

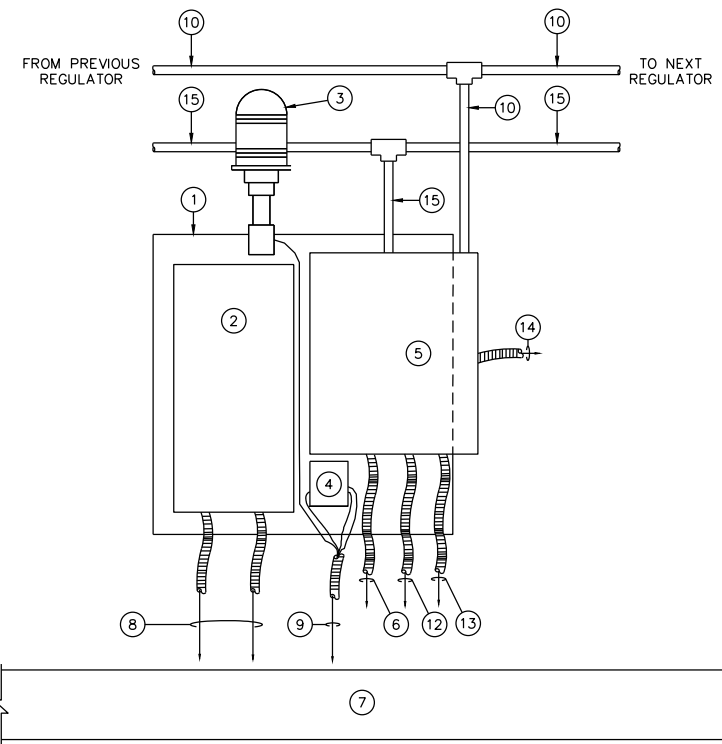


NOTE:
PROVIDE SUFFICIENT SLACK IN FLEXIBLE CONDUIT AS REQUIRED FOR ALCMS RACK TO BE OPERATIONAL PRIOR TO REMOVAL OF EXISTING ALCMS CABINET.

VAULT ALCMS RACK ELEVATION
NOT TO SCALE



TYPICAL EQUIPMENT MOUNTING PLATE DETAIL
NOT TO SCALE



TYPICAL INTERFACE CONTROL AND IRMS PANEL (ACE-II) MOUNTING DETAIL
NOT TO SCALE

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER

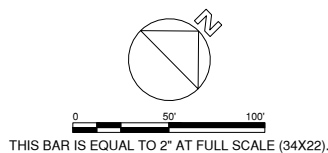


SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

SHEET TITLE
ALCMS DETAILS

Path: K:\Springfield\0180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-EL500.dwg
Date: Tuesday, April 26, 2022 12:48:12 PM



BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

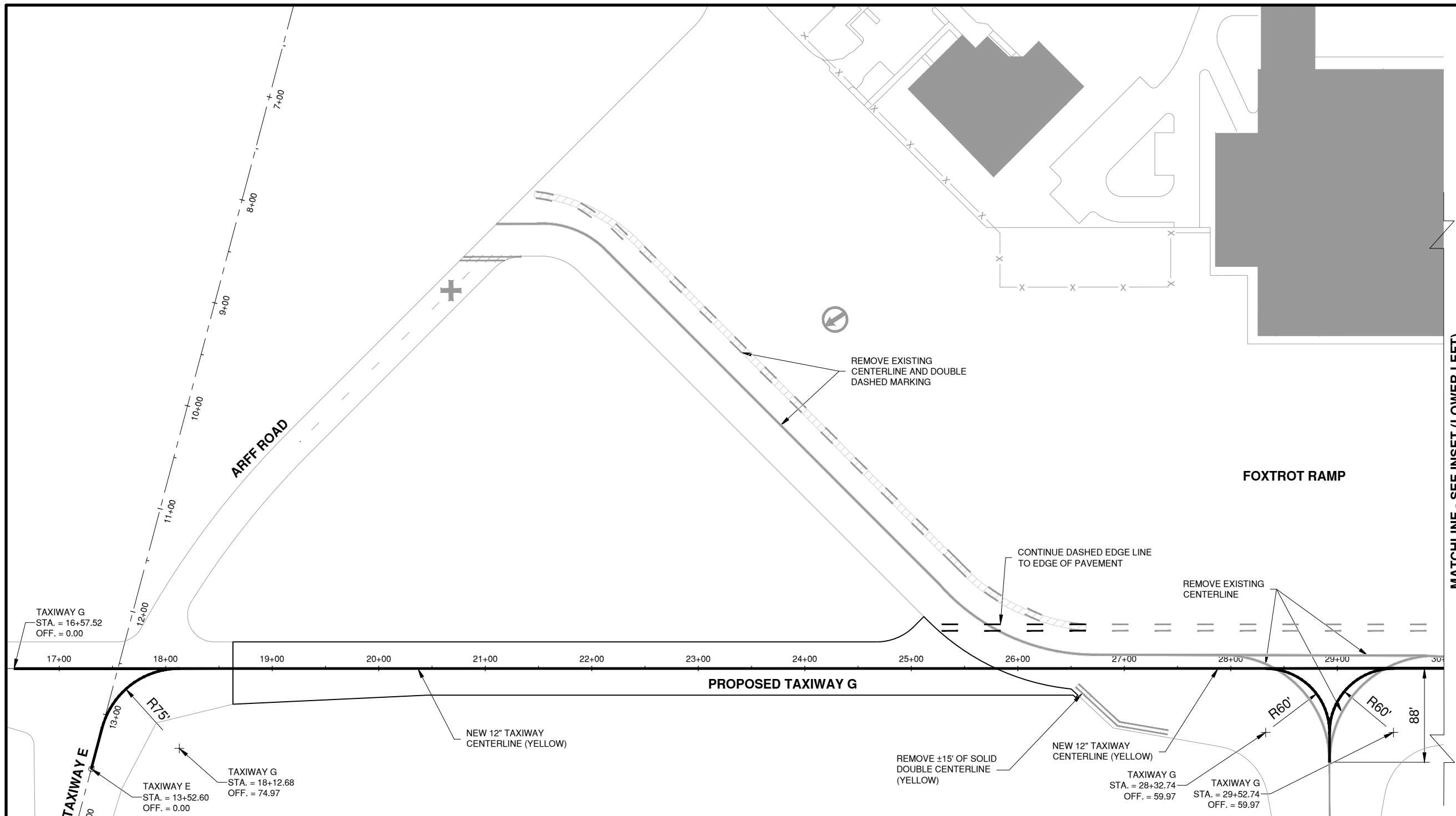
OWNER



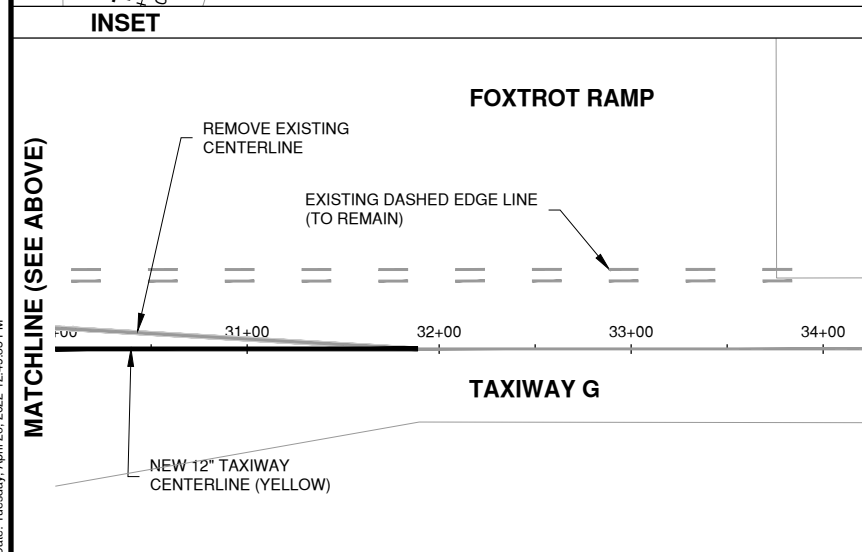
SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA022
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-CM100.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: CHK		
APPROVED BY: APR		
COPYRIGHT:		

SHEET TITLE
**PAVEMENT MARKING
PLAN 1**

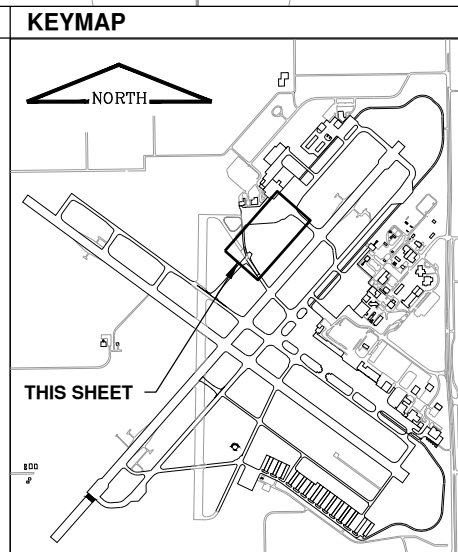


MATCHLINE - SEE INSET (LOWER LEFT)

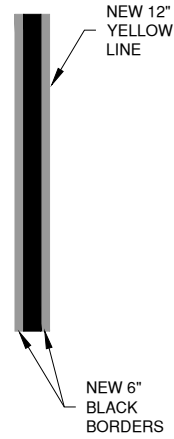


- NOTES**
1. MARKING LOCATIONS SHOWN FOR REFERENCE ONLY. NEW MARKINGS SHALL BE AT THE LOCATION OF EXISTING MARKINGS IN THE FIELD. PROVIDED THE EXISTING LOCATIONS CONFORM TO THE DIMENSIONS SHOWN ON THE THE DETAIL PROVIDED.
 2. MARKING SHALL BE APPLIED AFTER ALL PAVING IS COMPLETE.
 3. REFER TO CONSTRUCTION ACTIVITY PLAN FOR PHASED REMOVAL OF PAVEMENT MARKINGS.

- LEGEND**
- PROPOSED TAXIWAY CENTERLINE MARKING
 - - - EXISTING MARKING



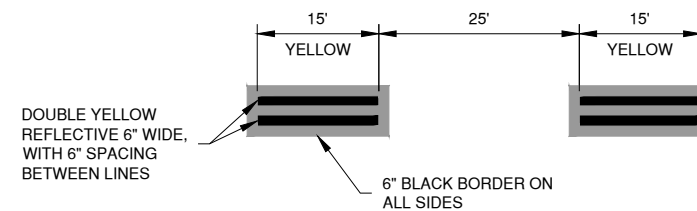
Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CM100.dwg
Date: Tuesday, April 26, 2022 12:49:35 PM



1 TAXIWAY CENTERLINE CONTINUOUS
N.T.S.

NOTES:

1. ALL NEW WATER BORNE PAVEMENT MARKING (RED, YELLOW, WHITE) WILL BE PAID UNDER WATERBORNE MARKING PAY ITEM AND SHALL HAVE REFLECTIVE BEADS.
2. ALL NEW AIRFIELD PAVEMENT MARKING SHALL HAVE 6" BLACK BORDER UNLESS OTHERWISE NOTED. BLACK BORDER DOES NOT RECEIVE REFLECTIVE BEADS.



2 DASHED TAXIWAY EDGE LINE
N.T.S.



License No. 184-000613
CONSULTANTS

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER

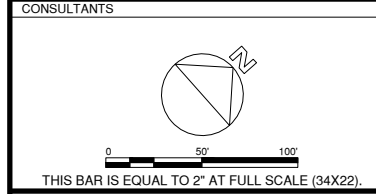


SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX **CA022**
IL PROJ. NO: SPI-4943
CMT PROJECT NO: 180035-05
CAD DWG FILE: 18003505-PH3-CM500.DWG
DESIGNED BY: HWI
DRAWN BY: DPA
CHECKED BY: CHK
APPROVED BY: APR
COPYRIGHT:

SHEET TITLE
**PAVEMENT MARKING
DETAILS**



BID SUBMITTAL
APRIL 22, 2022

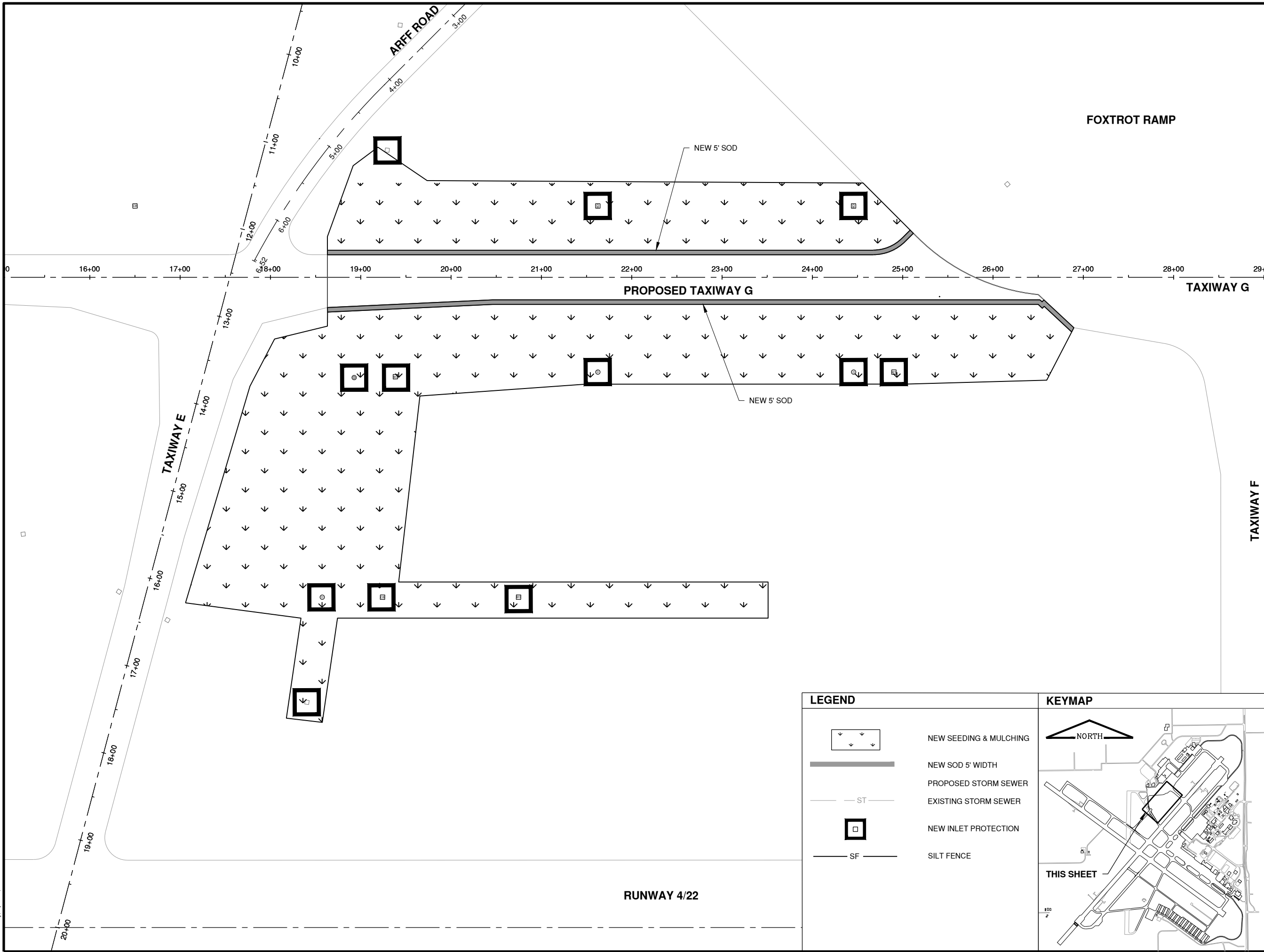
RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

Path: K:\Springfield\0180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-LG100.dwg
Date: Tuesday, April 26, 2022 12:50:11 PM



LEGEND		KEYMAP	
	NEW SEEDING & MULCHING		NORTH
	NEW SOD 5' WIDTH		THIS SHEET
	PROPOSED STORM SEWER		
	EXISTING STORM SEWER		
	NEW INLET PROTECTION		
	SILT FENCE		

MARK	DATE	DESCRIPTION

SHEET TITLE	
EROSION CONTROL & TURFING PLANS 1	
SHEET	70 OF 91

LG101



0 50 100
THIS BAR IS EQUAL TO 2' AT FULL SCALE (34X22).

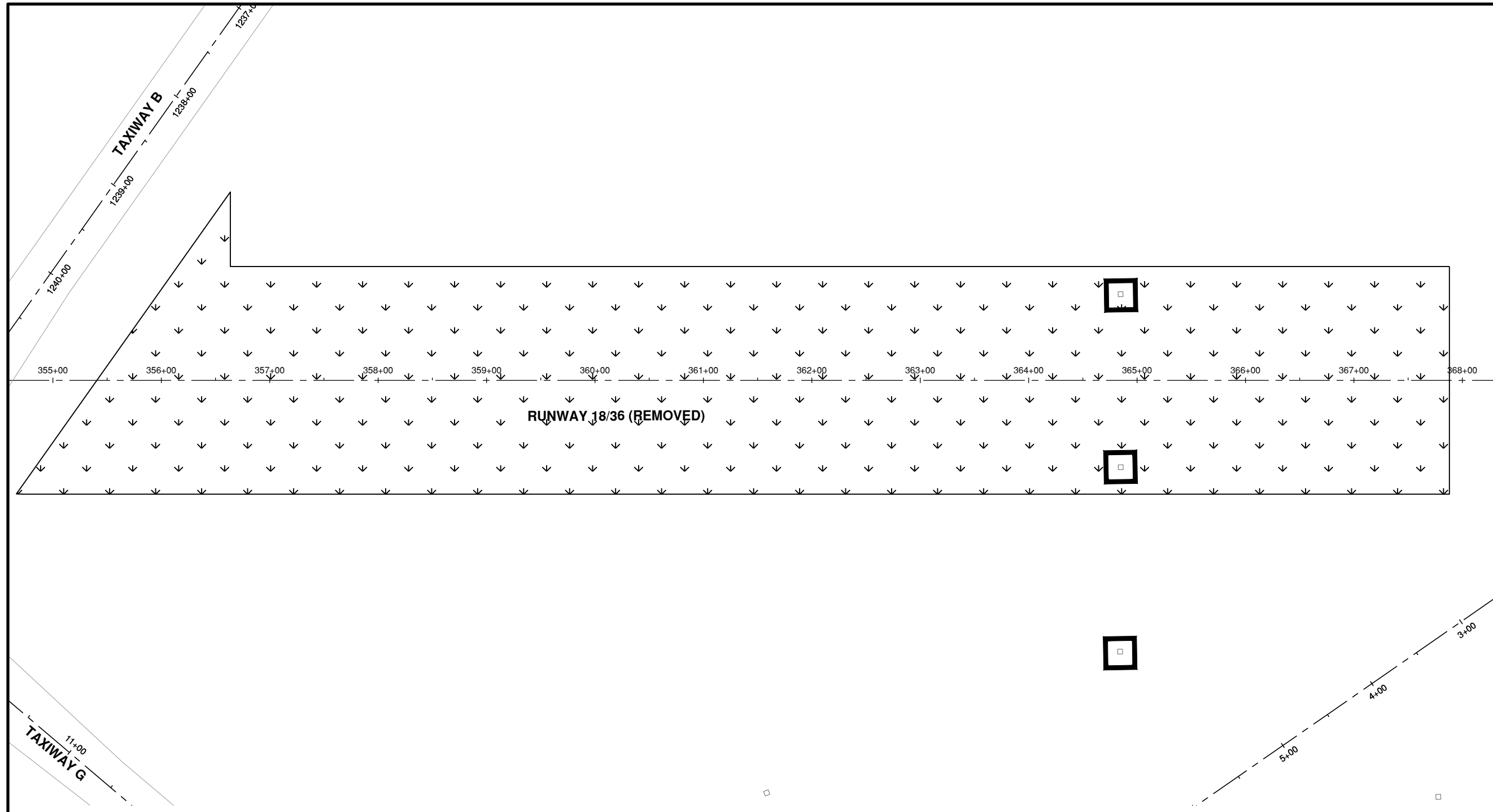
BID SUBMITTAL
APRIL 22, 2022

**RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3**

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL



LEGEND

	NEW SEEDING & MULCHING
	NEW SOD 5' WIDTH
	PROPOSED STORM SEWER
	EXISTING STORM SEWER
	NEW INLET PROTECTION
	SILT FENCE

KEYMAP

MARK	DATE	DESCRIPTION

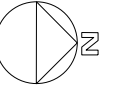
AIP PROJ. NO: 3-17-0096-XX CA022
IL PROJ. NO: SPI-4943
CMT PROJECT NO: 180035-05
CAD DWG FILE: 18003505-PH3-LG100.DWG
DESIGNED BY: HWI
DRAWN BY: DPA
CHECKED BY: CHK
APPROVED BY: APR
COPYRIGHT:

Path: K:\Springfield\A180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-LG100.dwg
Date: Tuesday, April 26, 2022 12:50:24 PM



License No. 184-000613

CONSULTANTS



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK | DATE | DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX CA022

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-LG100.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

APPROVED BY: APR

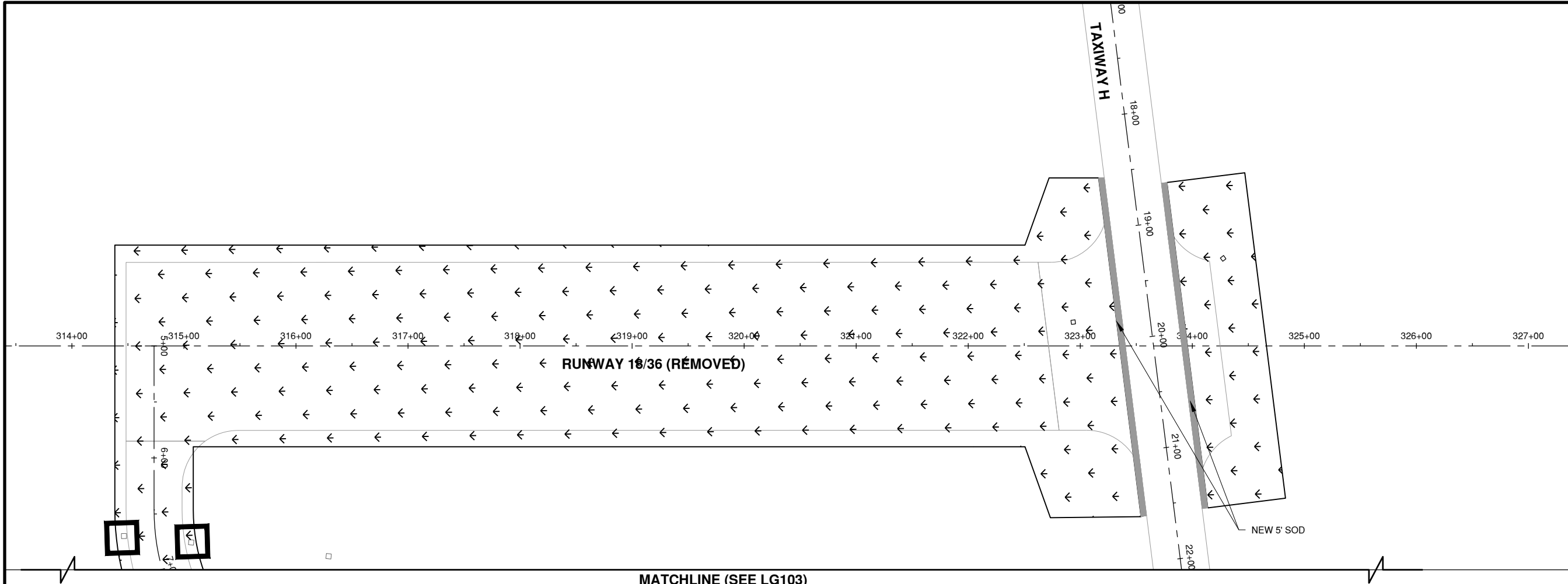
COPYRIGHT:

SHEET TITLE

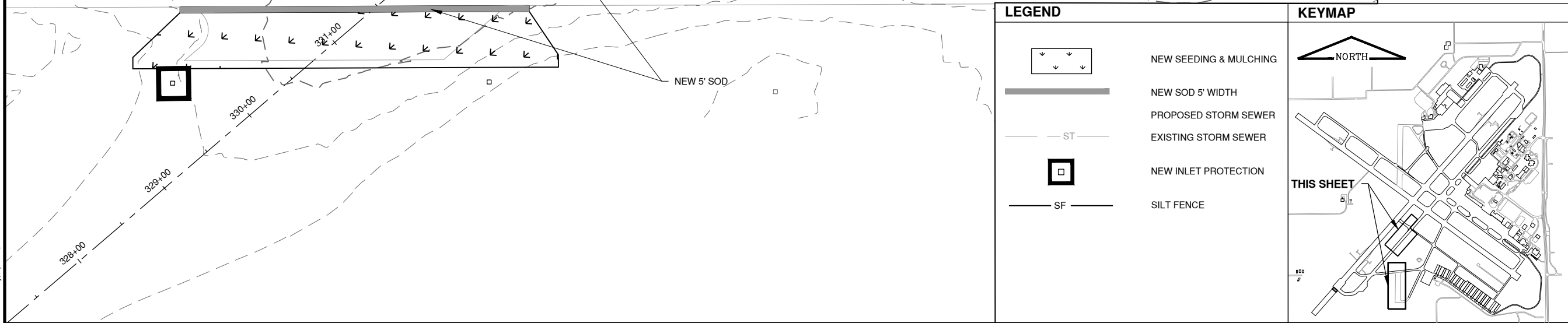
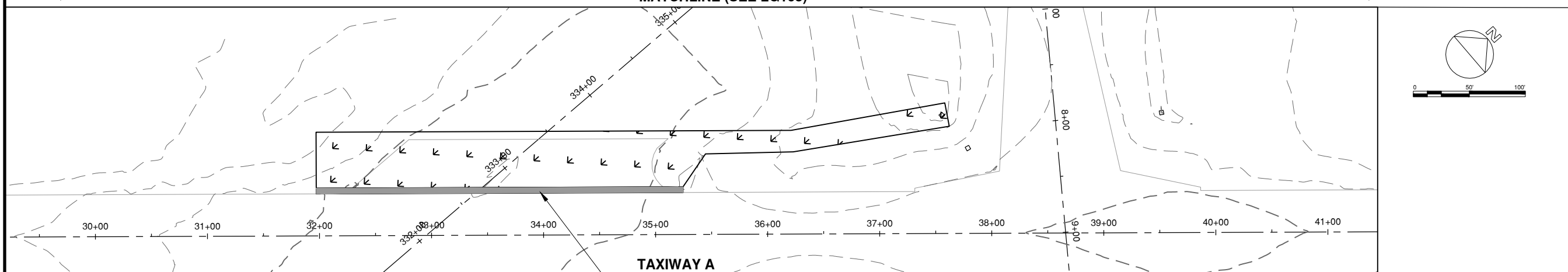
EROSION CONTROL & TURFING PLANS 3

LG103

SHEET 72 OF 91



MATCHLINE (SEE LG103)



LEGEND

	NEW SEEDING & MULCHING
	NEW SOD 5' WIDTH
	PROPOSED STORM SEWER
	EXISTING STORM SEWER
	NEW INLET PROTECTION
	SILT FENCE

KEYMAP

THIS SHEET

MATCHLINE (SEE LG103)



License No. 184-000613

CONSULTANTS



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID SUBMITTAL
APRIL 22, 2022

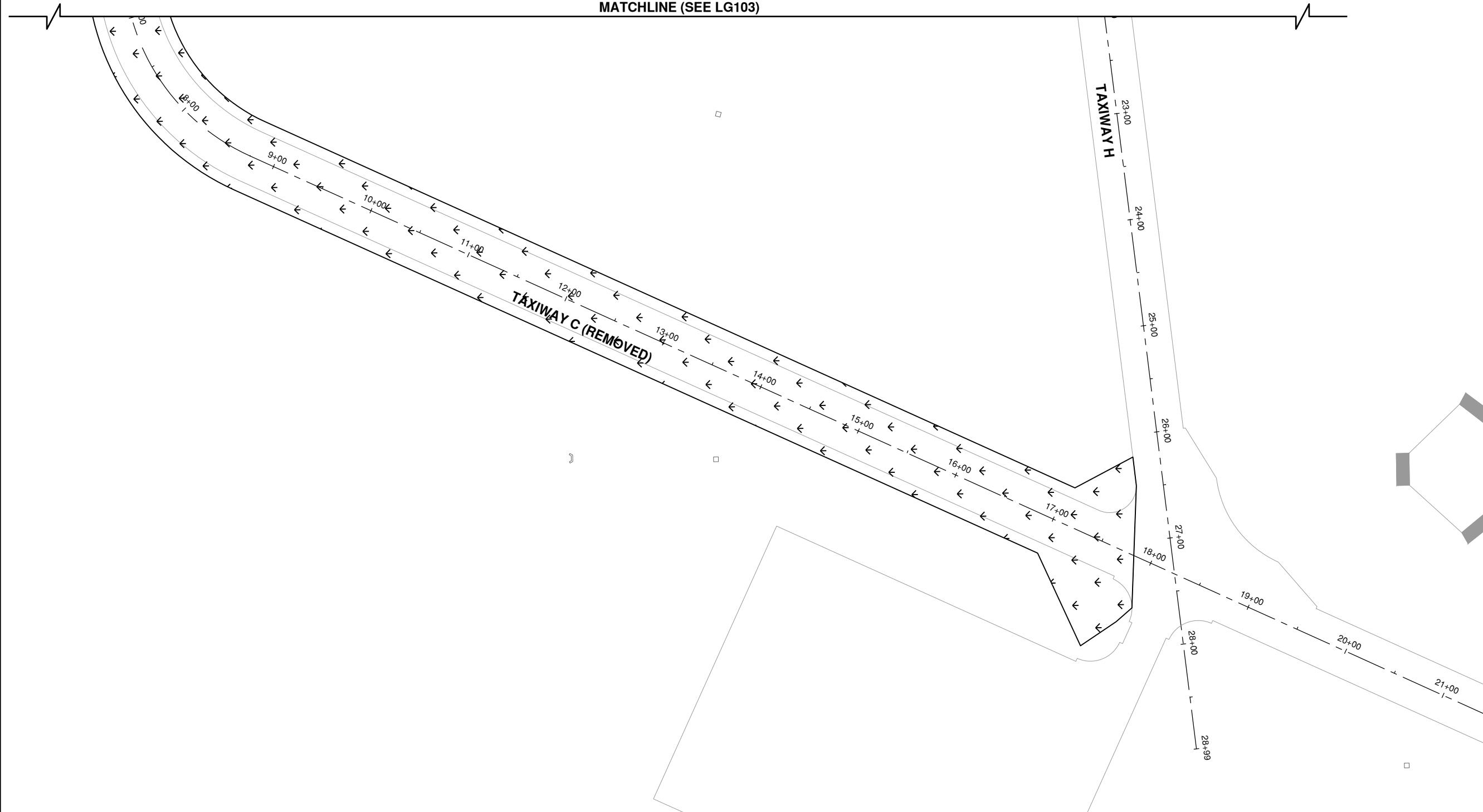
RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

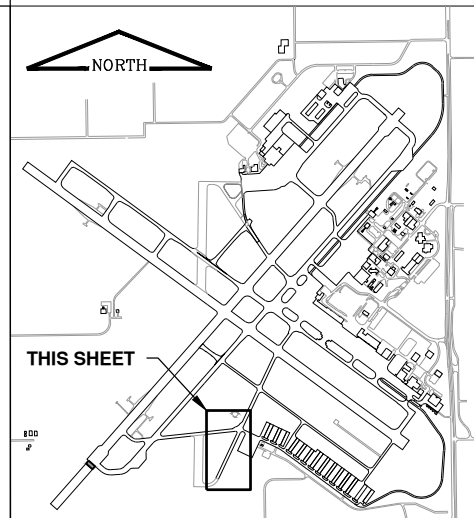
Path: K:\Springfield\18035-05_Rvw_18-36\Draw\Sheets\PHASE 3\18003505-PH3-LG100.dwg
Date: Tuesday, April 26, 2022 12:50:54 PM



LEGEND

	NEW SEEDING & MULCHING
	NEW SOD 5' WIDTH
	PROPOSED STORM SEWER
	EXISTING STORM SEWER
	NEW INLET PROTECTION
	SILT FENCE

KEYMAP



MARK	DATE	DESCRIPTION	
		AIP PROJ. NO: 3-17-0096-XX	CA022
		IL PROJ. NO: SPI-4943	
		CMT PROJECT NO: 180035-05	
		CAD DWG FILE: 18003505-PH3-LG100.DWG	
		DESIGNED BY: HWI	
		DRAWN BY: DPA	
		CHECKED BY: CHK	
		APPROVED BY: APR	
		COPYRIGHT:	

EROSION CONTROL & TURFING PLANS 4

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO:	3-17-0096-XX	CA022
---------------	--------------	-------

IL PROJ. NO:	SPI-4943
--------------	----------

CMT PROJECT NO:	180035-05
-----------------	-----------

CAD DWG FILE:	18003505-PH3-LG500.DWG
---------------	------------------------

DESIGNED BY:	HWI
--------------	-----

DRAWN BY:	DPA
-----------	-----

CHECKED BY:	CHK
-------------	-----

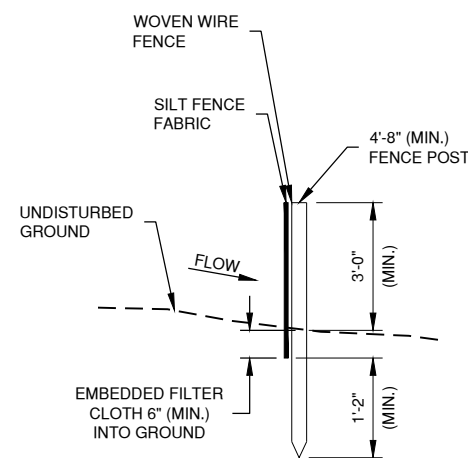
APPROVED BY:	APR
--------------	-----

COPYRIGHT:

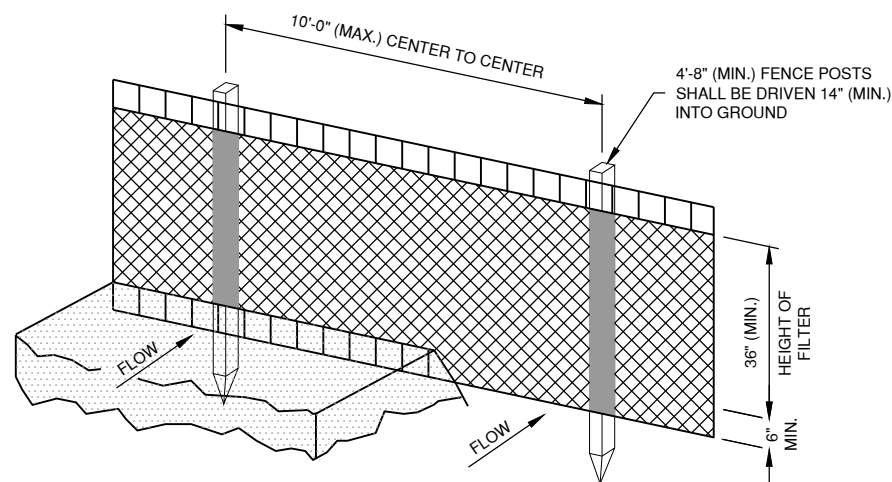
SHEET TITLE

**EROSION CONTROL
DETAILS**

LG501
SHEET 74 OF 91



SECTION



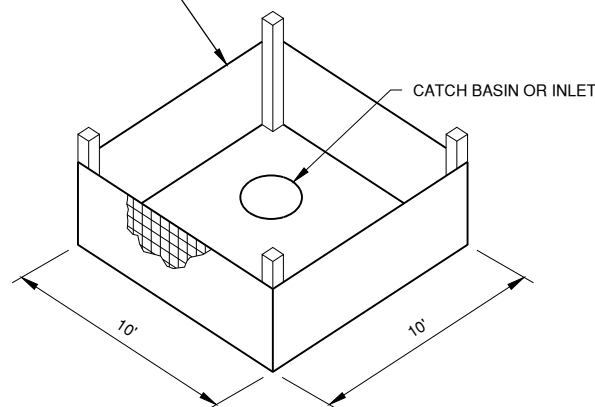
PERSPECTIVE VIEW

1 SILT FENCE
N.T.S.

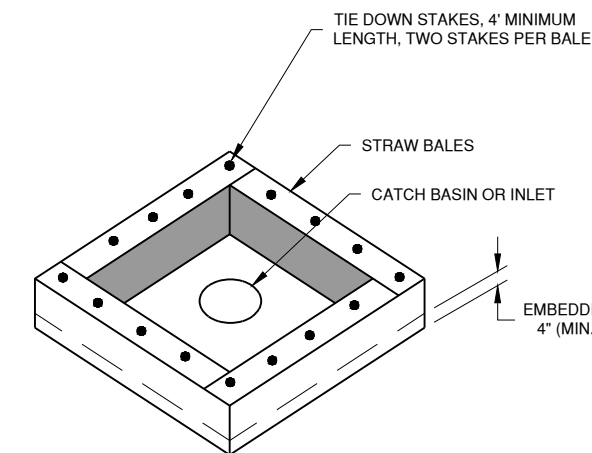
SILT FENCE NOTES

- WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
- FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
- WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
- MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.

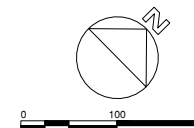
SILT FENCE PER DETAIL 1
THIS SHEET



2 INLET PROTECTION WITH SILT FENCE
N.T.S.



3 INLET PROTECTION WITH STRAW BALES
N.T.S.



BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX CA022

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-CG600.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

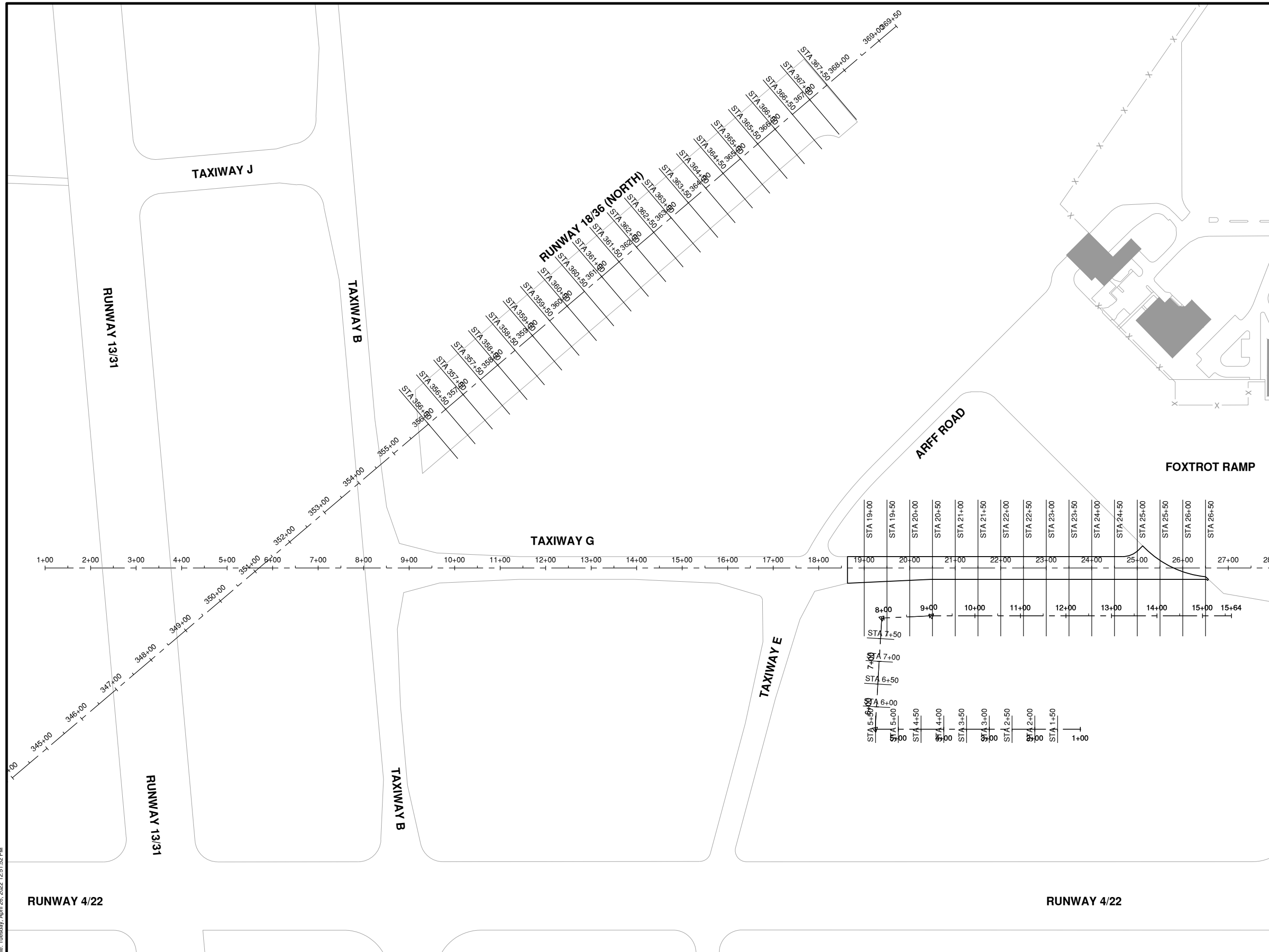
CHECKED BY: CHK

APPROVED BY: APR

COPYRIGHT:

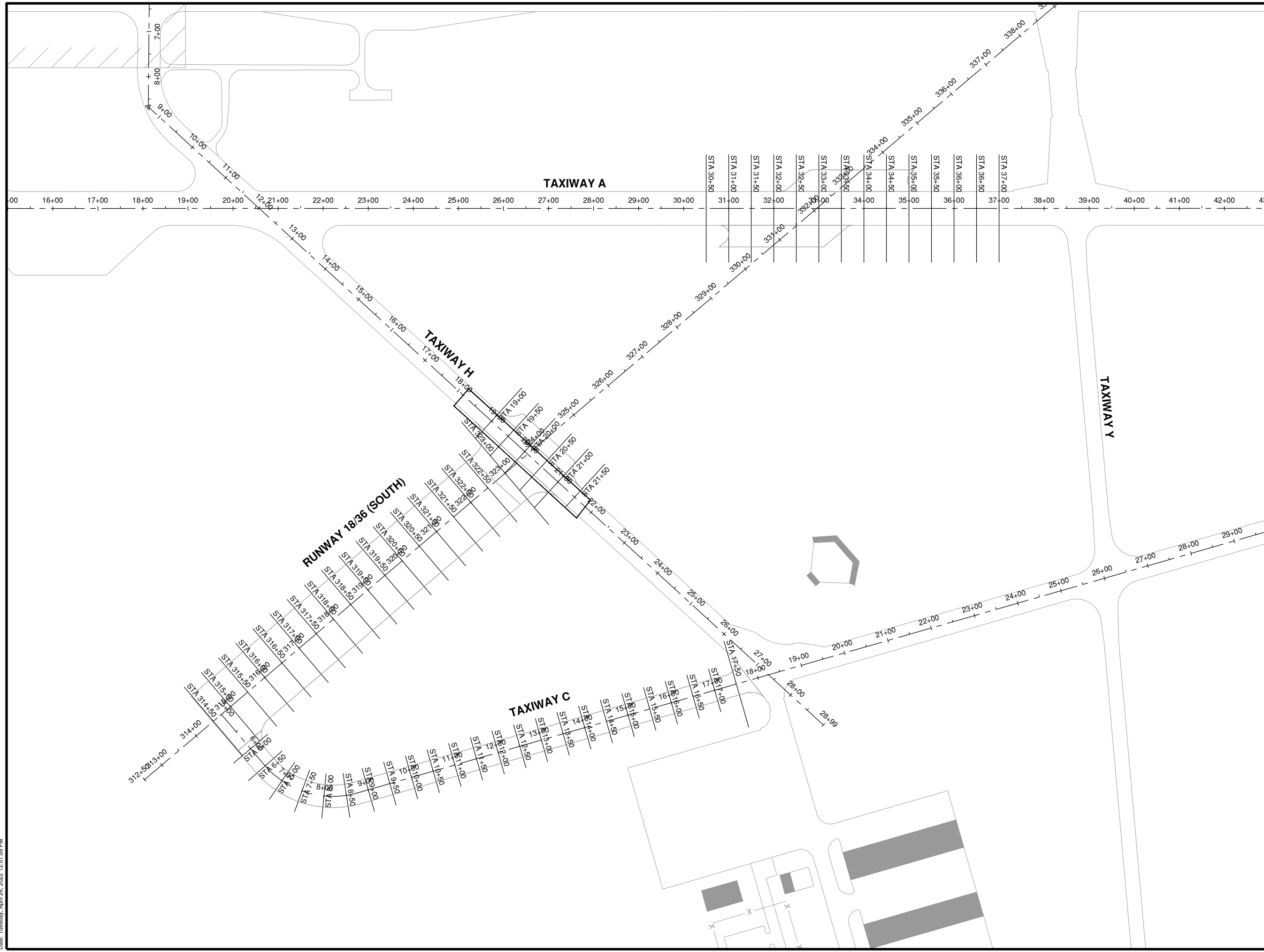
SHEET TITLE

CROSS SECTION
INDEX 1

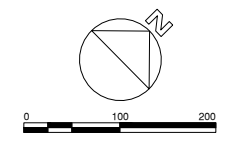


Path: K:\Springfield\0180035-05_Rwy_18-36\Draw\Sheets\PH3-CG600.dwg
Date: Tuesday, April 26, 2022 12:51:52 PM

Path: K:\Springfield\18035-05_Rwy_18-36\Draw\Sheets\PH3-CG600.dwg
 Date: Tuesday, April 26, 2022 12:51:59 PM



License No. 184-000613
 CONSULTANTS



BID SUBMITTAL
 APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
 ASSOCIATED IMPROVEMENTS,
 PHASE 3

OWNER



SPRINGFIELD AIRPORT
 AUTHORITY
 ABRAHAM LINCOLN CAPITAL
 AIRPORT
 SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

SHEET TITLE

CROSS SECTION INDEX 2

CG601

SHEET 76 OF 91

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

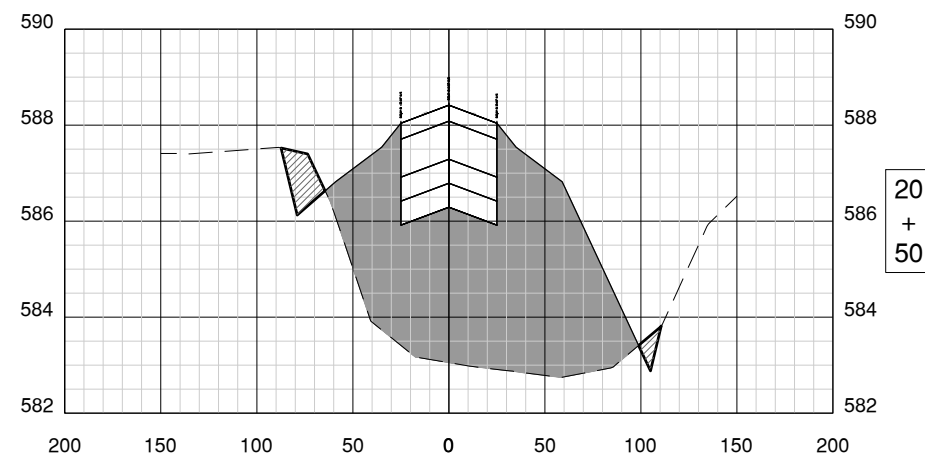
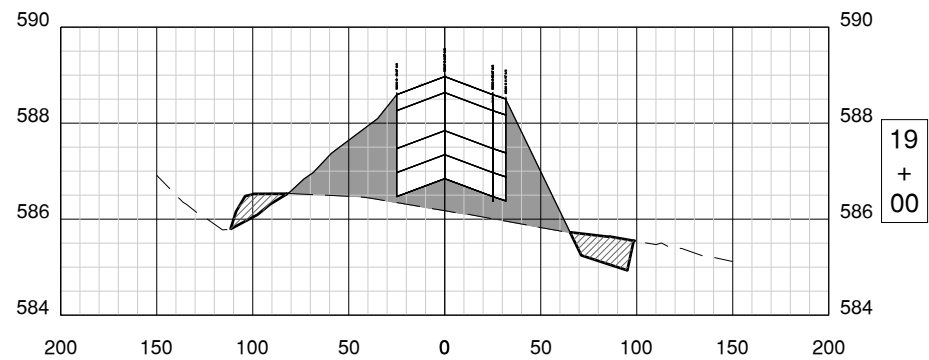
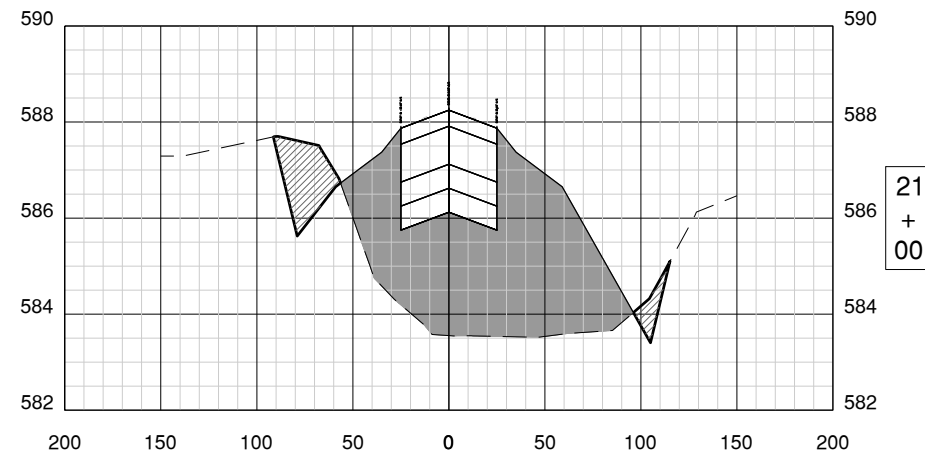
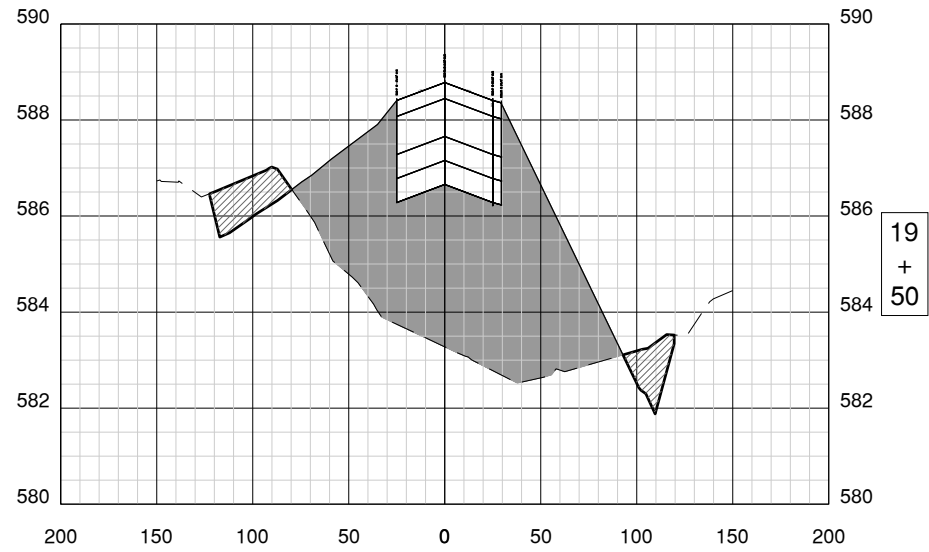
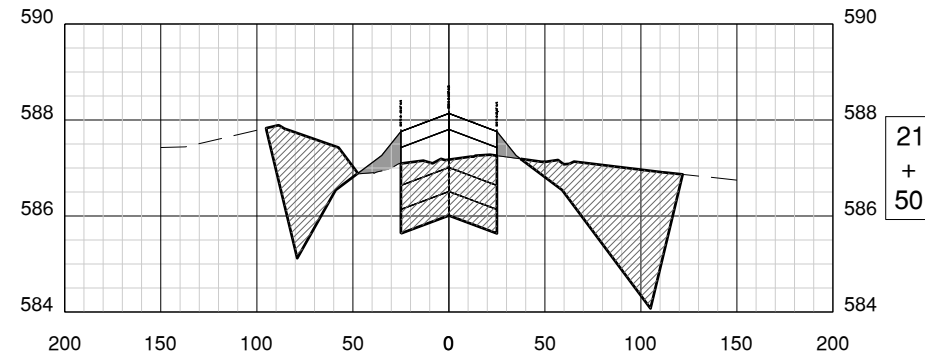
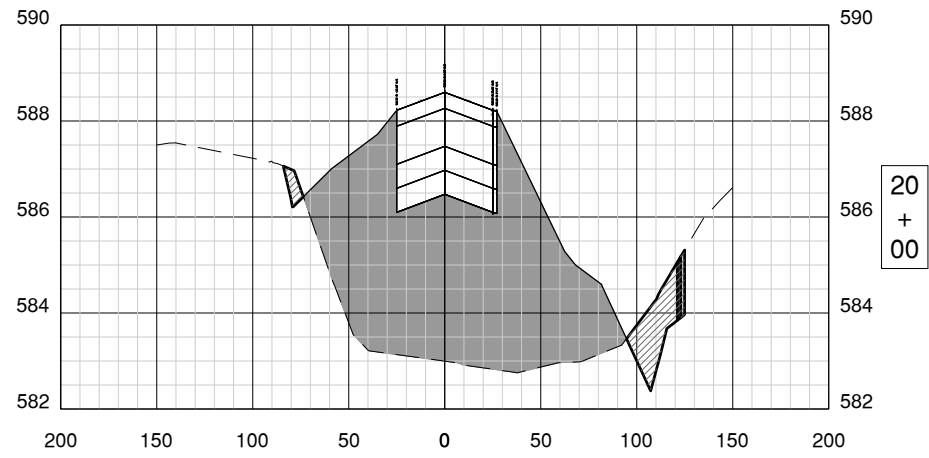
OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION
AIP PROJ. NO:	3-17-0096-XX	CA022
IL PROJ. NO:	SPI-4943	
CMT PROJECT NO:	180035-05	
CAD DWG FILE:	18003505-PH3-CG600.DWG	
DESIGNED BY:	HWI	
DRAWN BY:	DPA	
CHECKED BY:	CHK	
APPROVED BY:	APR	
COPYRIGHT:		

SHEET TITLE
**TAXIWAY G CROSS
SECTIONS 1**





License No. 184-000613

CONSULTANTS

BID SUBMITTAL
APRIL 22, 2022

**RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3**

OWNER



**SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL**

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX **CA022**

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-CG600.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

APPROVED BY: APR

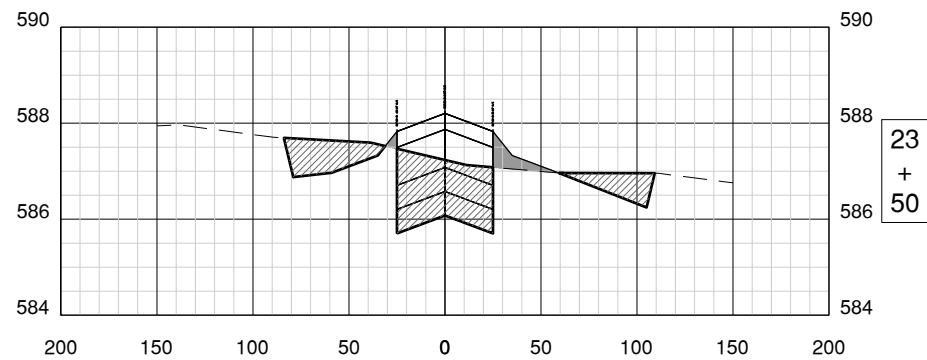
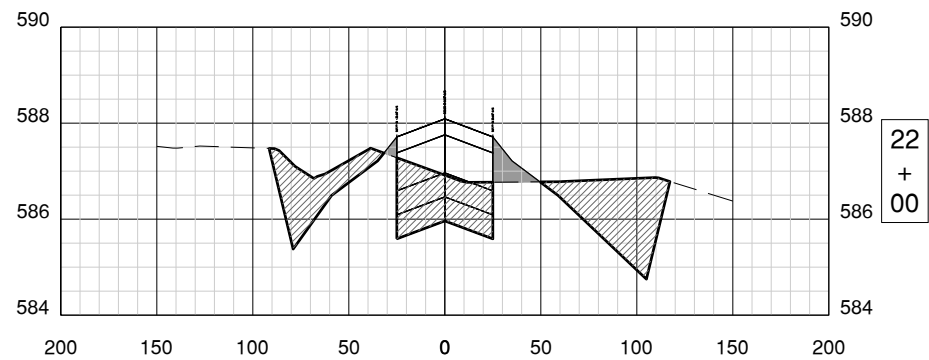
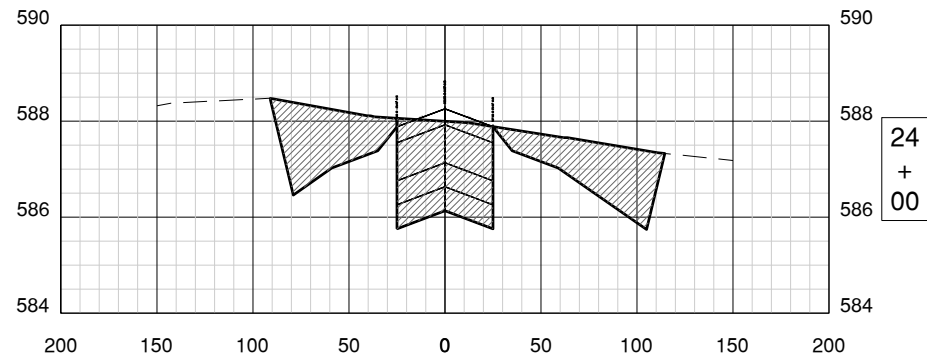
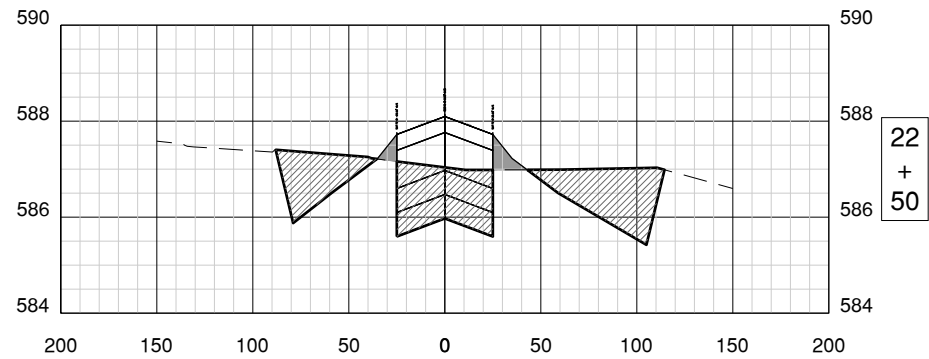
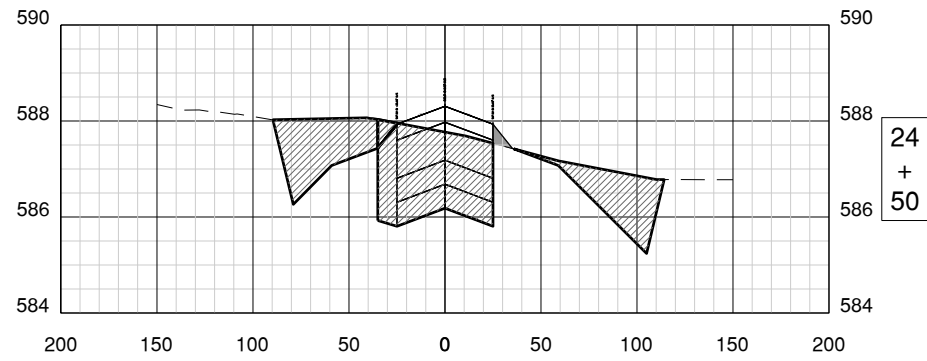
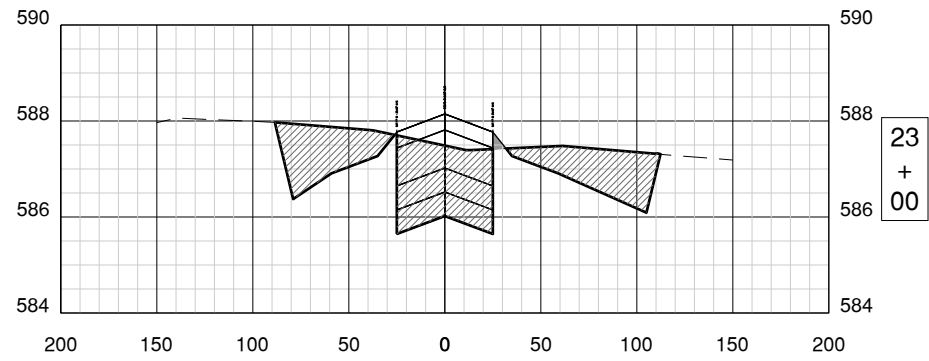
COPYRIGHT:

SHEET TITLE

**TAXIWAY G CROSS
SECTIONS 2**

CG603

SHEET **78** OF **91**



Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PH3\CG600.dwg
Date: Tuesday, April 26, 2022 12:52:12 PM



License No. 184-000613

CONSULTANTS

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX **CA022**

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-CG600.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

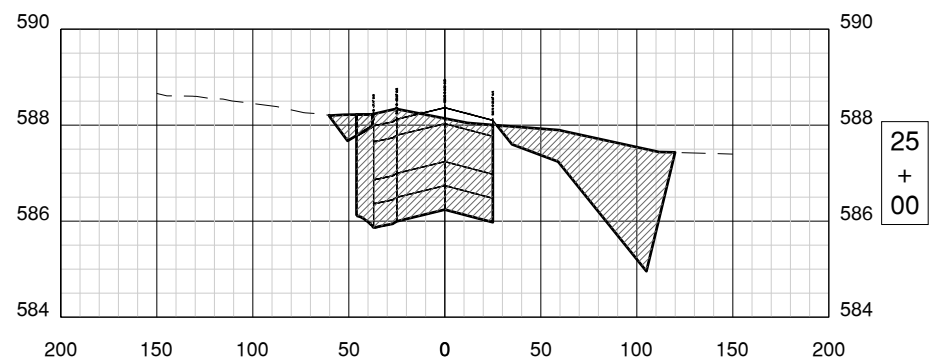
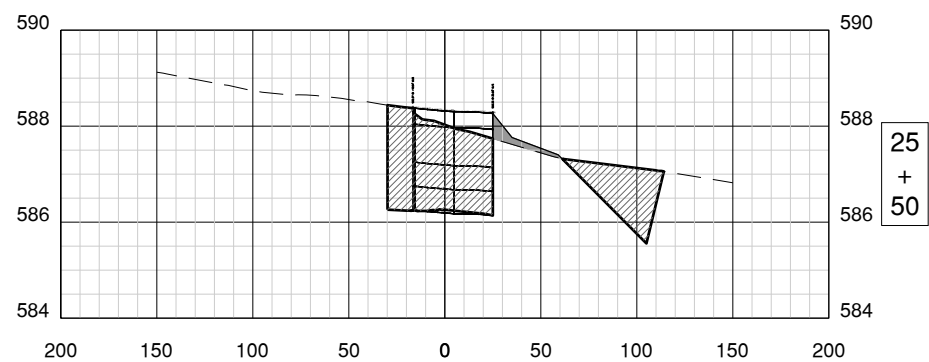
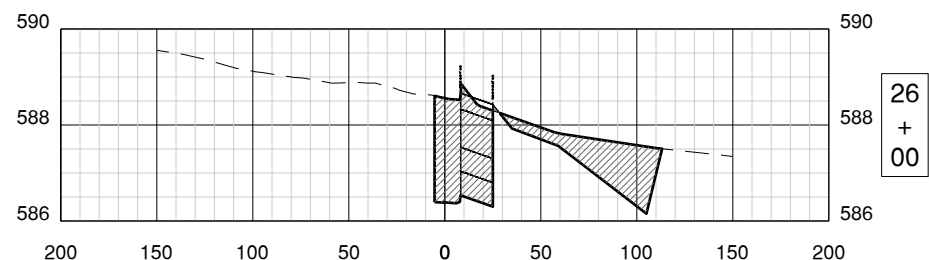
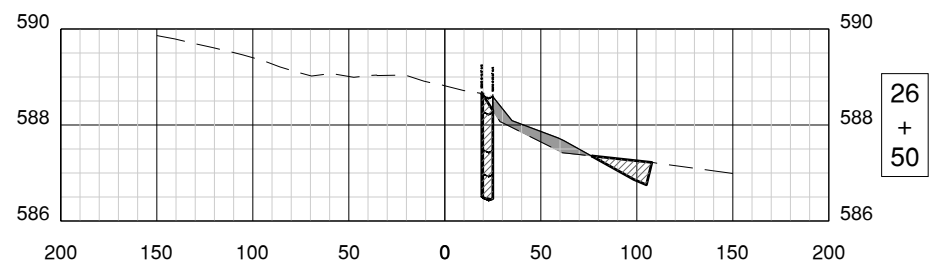
APPROVED BY: APR

COPYRIGHT:

SHEET TITLE
**TAXIWAY G CROSS
SECTIONS 3**

CG604
SHEET 79 OF 91

TAXY G STAT ON	FILL			CUT (IN-SITU)		
	AREA [SF]	VOL [CY]	CUM VOL [CY]	AREA [SF]	VOL [CY]	CUM VOL [CY]
19+00.00	184.2	342	342	8.9	17	17
19+50.00	753.7	1396	1738	48.5	90	107
20+00.00	574.1	1064	2802	30.5	57	164
20+50.00	509.6	944	3746	22.1	41	205
21+00.00	374.6	594	4440	48.9	91	296
21+50.00	10.4	20	4460	248.2	460	756
22+00.00	11.9	23	4483	176.2	327	1083
22+50.00	8.9	17	4500	167.3	310	1393
23+00.00	1.3	3	4503	197.6	366	1759
23+50.00	10	19	4522	117.1	217	1976
24+00.00	0	0	4503	256.6	476	2235
24+50.00	2.1	4	4526	222.3	412	2388
25+00.00	0.1	1	4527	272	504	2892
25+50.00	6.1	12	4539	147	273	3165
26+00.00	0.2	1	4540	118.2	219	3384
26+50.00	10.4	20	4560	20.2	38	3422



BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

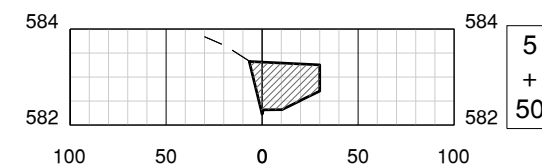
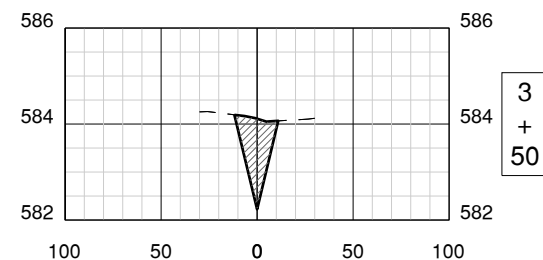
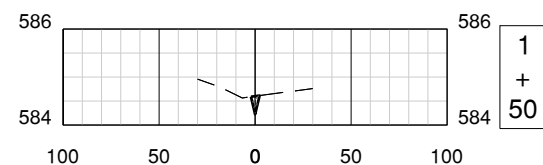
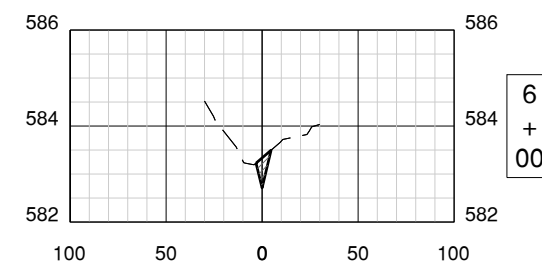
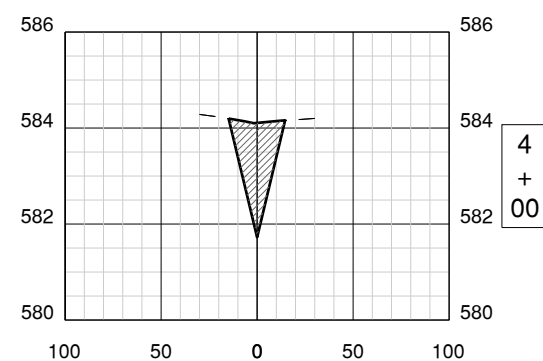
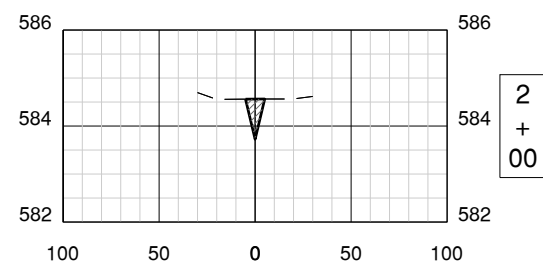
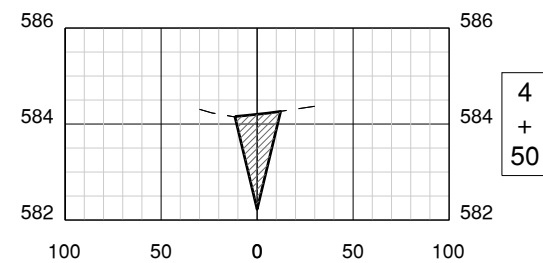
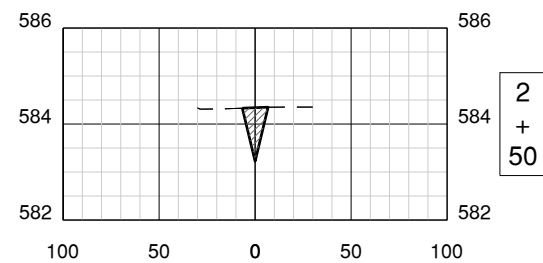
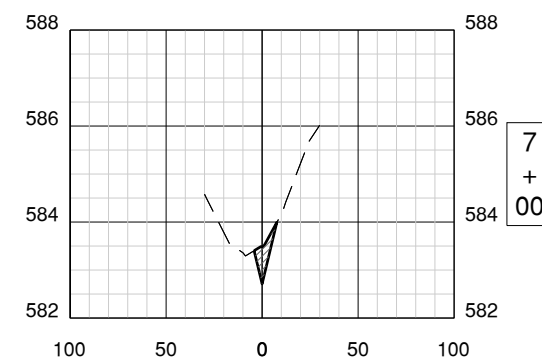
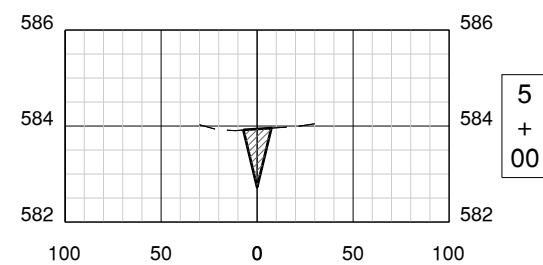
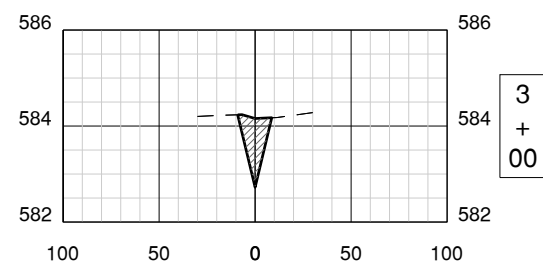
OWNER



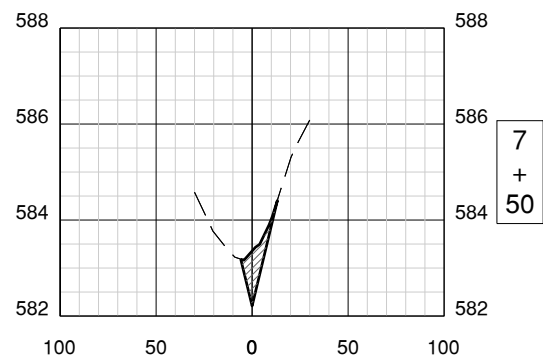
SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA022
IL PROJ. NO: SPI-4943		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH3-CG600.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: CHK		
APPROVED BY: APR		
COPYRIGHT:		

SHEET TITLE
IN-FIELD DITCH
CROSS SECTIONS 1



Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CG600.dwg
 Date: Tuesday, April 26, 2022 12:52:31 PM



STATION	IN-FIELD DITCH EARTHWORK					
	FILL			CUT (14:51%)		
	AREA (SF)	VOL (CY)	CUM VOL (CY)	AREA (SF)	VOL (CY)	CUM VOL (CY)
1+50.00	0	0	0	0.9	2	2
2+00.00	0	0	0	4.3	8	10
2+50.00	0	0	0	7.7	15	25
3+00.00	0	0	0	13.2	25	50
3+50.00	0	0	0	22.1	41	91
4+00.00	0	0	0	36.1	67	158
4+50.00	0	0	0	74.3	45	203
5+00.00	0	0	0	9.1	17	220
5+50.00	0	0	0	29.6	55	275
6+00.00	0	0	0	2.5	5	280
6+50.00	0	0	0	0	0	280
7+00.00	0	0	0	4.6	9	289
7+50.00	0	0	0	9.4	18	307



License No. 184-000613

CONSULTANTS

BID SUBMITTAL
 APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
 ASSOCIATED IMPROVEMENTS,
 PHASE 3

OWNER



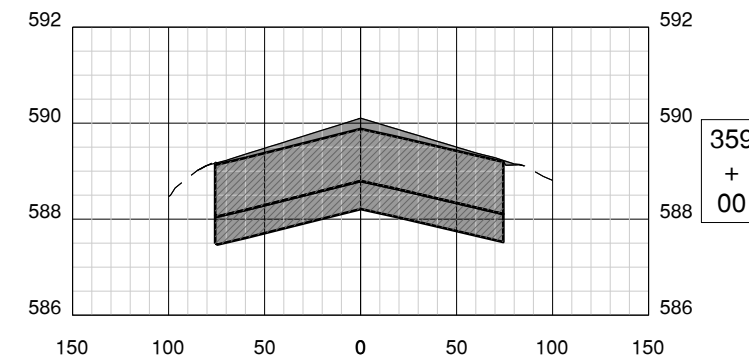
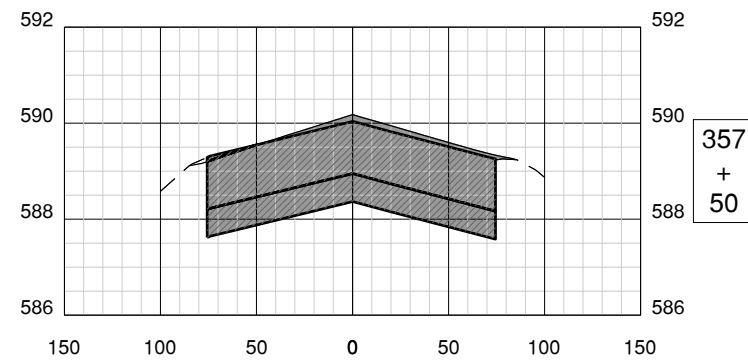
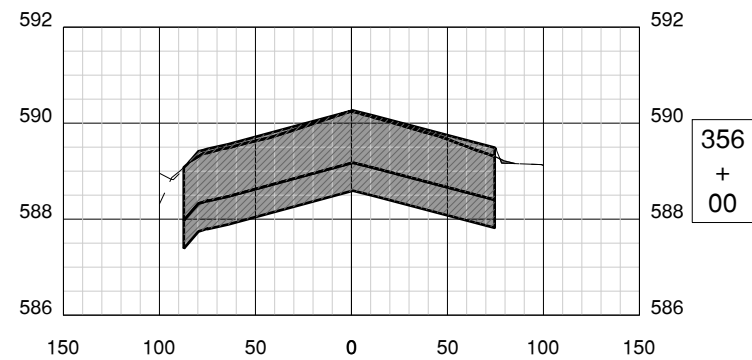
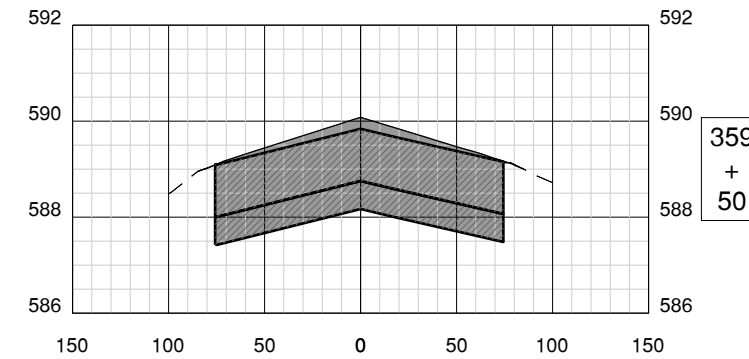
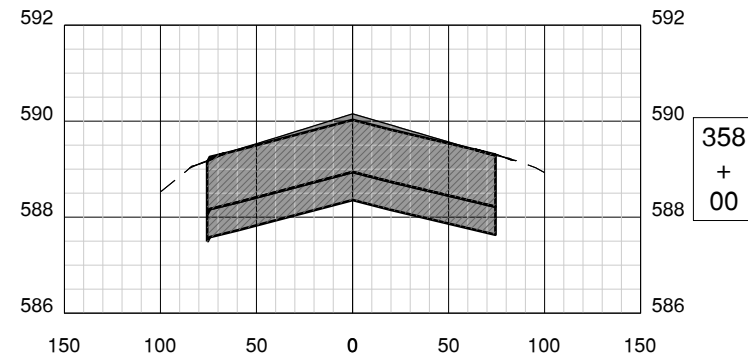
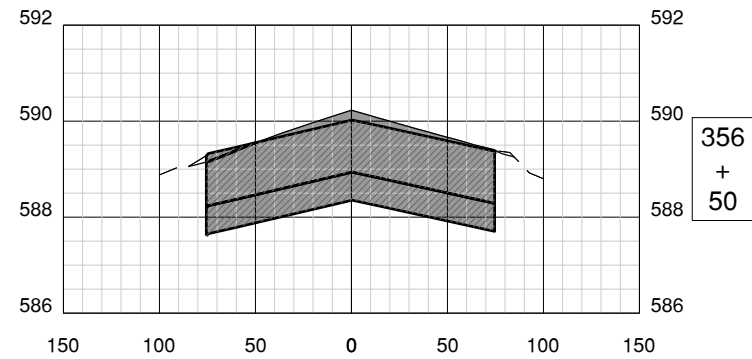
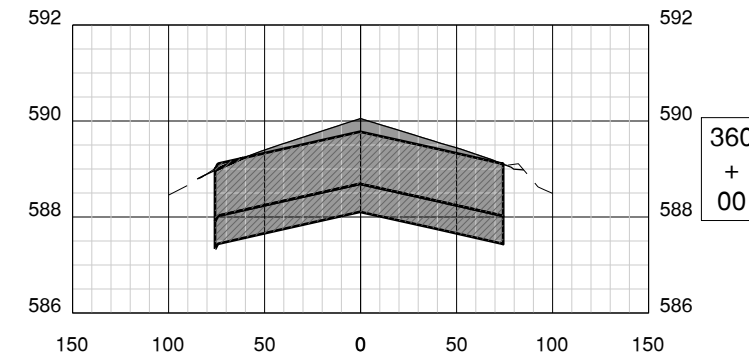
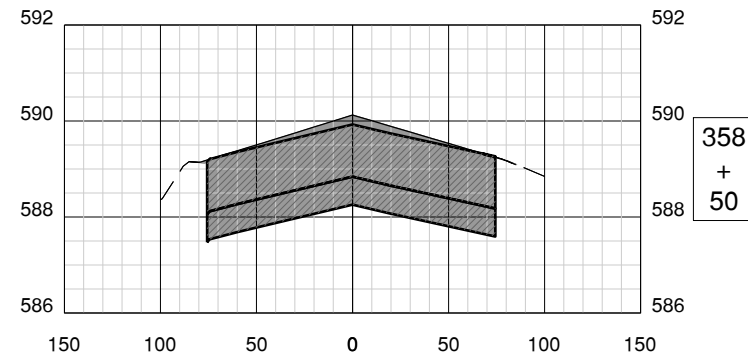
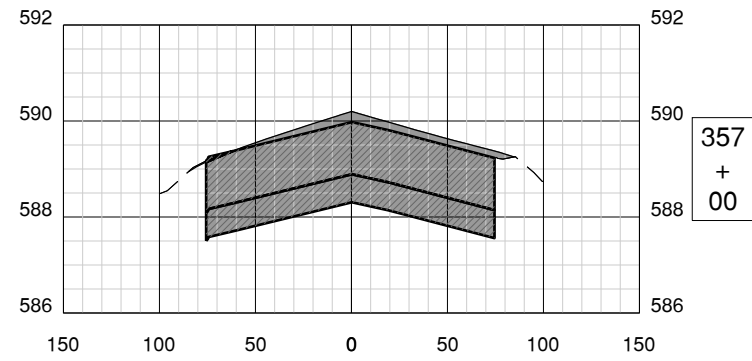
SPRINGFIELD AIRPORT
 AUTHORITY
 ABRAHAM LINCOLN CAPITAL
 AIRPORT
 SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CG600.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

SHEET TITLE
**IN-FIELD DITCH
 CROSS SECTIONS 2**

CG606
 SHEET 81 OF 91



BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CG600.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

SHEET TITLE
**RWY 18-36 NORTH
CROSS SECTIONS 1**

Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CG600.dwg
Date: Tuesday, April 26, 2022 12:52:37 PM



License No. 184-000613
CONSULTANTS

BID SUBMITTAL
APRIL 22, 2022

**RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3**

OWNER

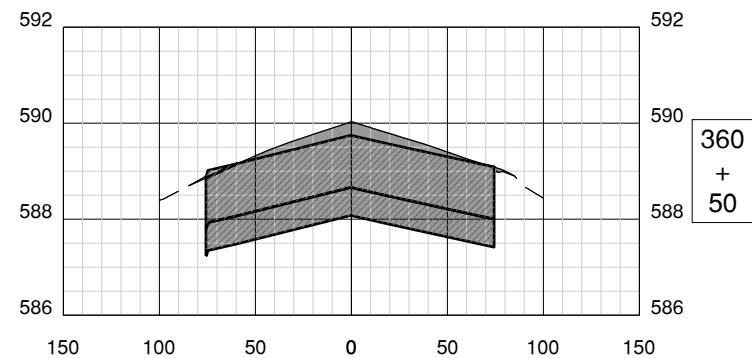
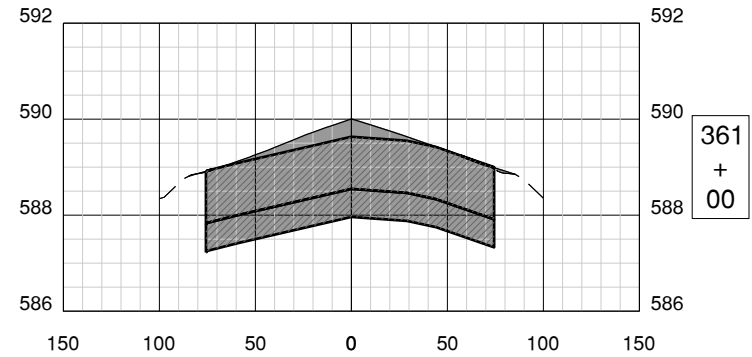
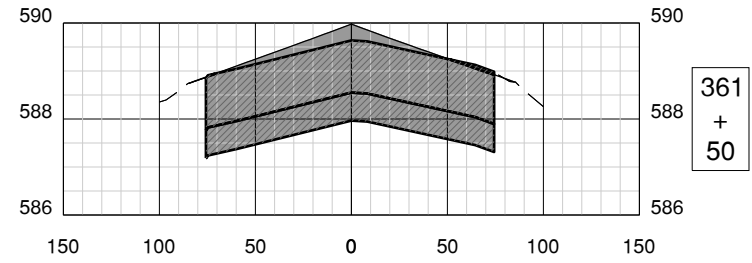
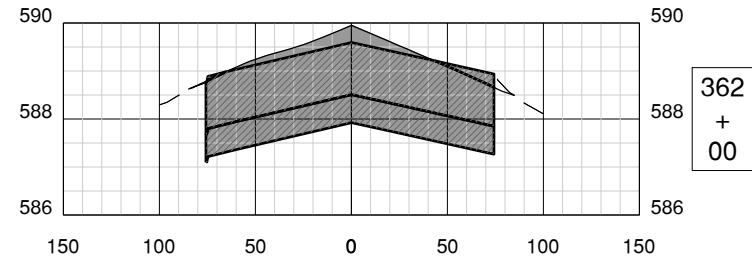
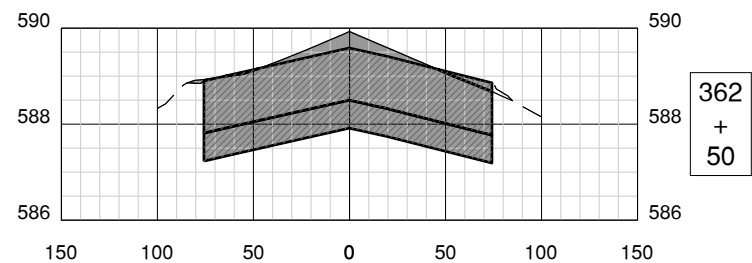
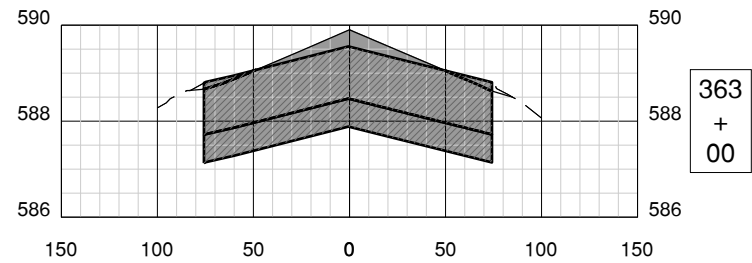
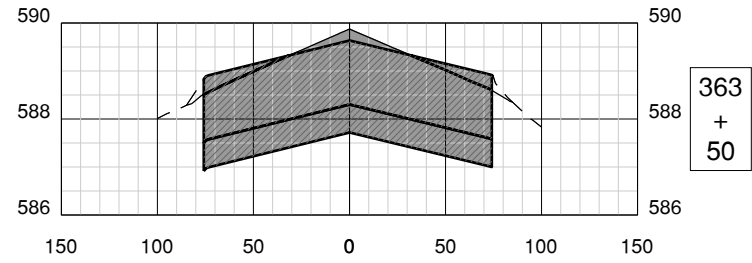
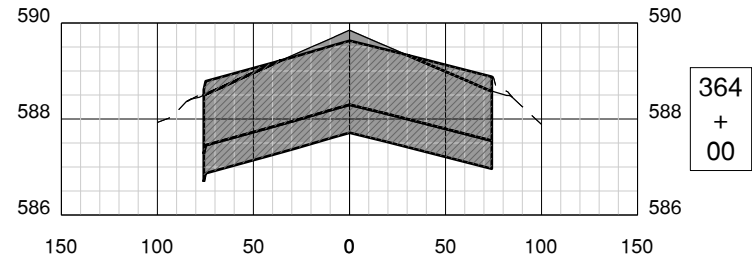
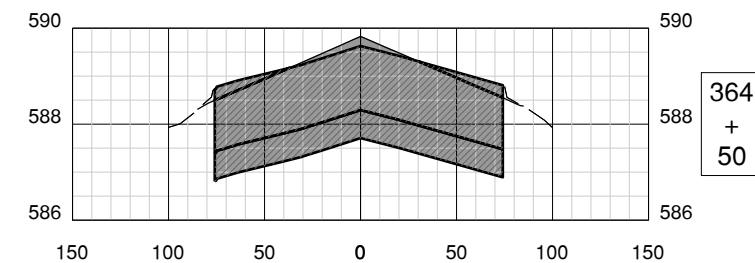
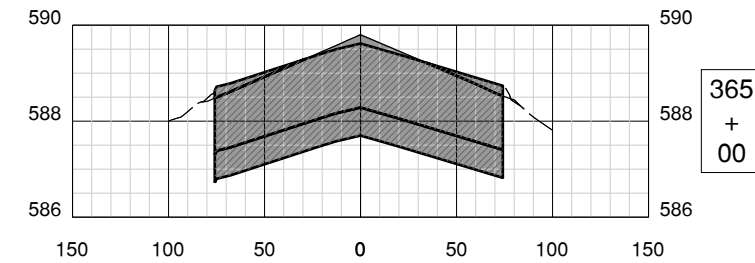
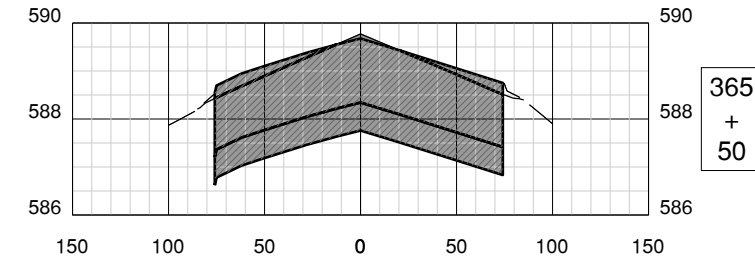
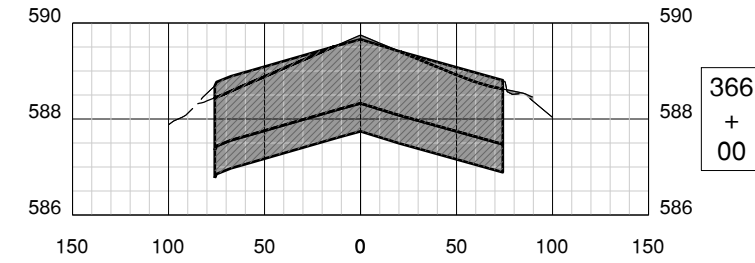


**SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL**

MARK	DATE	DESCRIPTION

AIP PROJ. NO:	3-17-0096-XX	CA022
IL PROJ. NO:	SPI-4943	
CMT PROJECT NO:	180035-05	
CAD DWG FILE:	18003505-PH3-CG600.DWG	
DESIGNED BY:	HWI	
DRAWN BY:	DPA	
CHECKED BY:	CHK	
APPROVED BY:	APR	
COPYRIGHT:		

SHEET TITLE
**RWY 18-36 NORTH
CROSS SECTIONS 2**



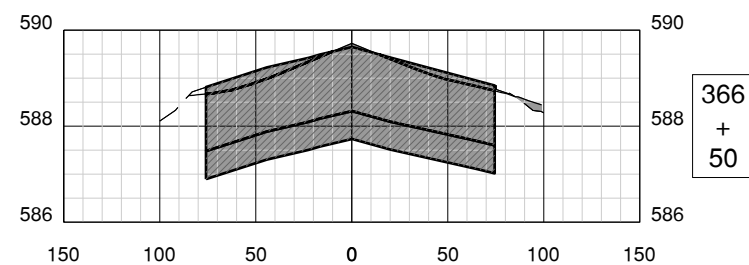
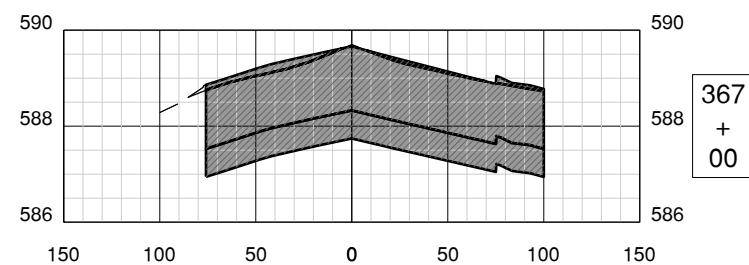
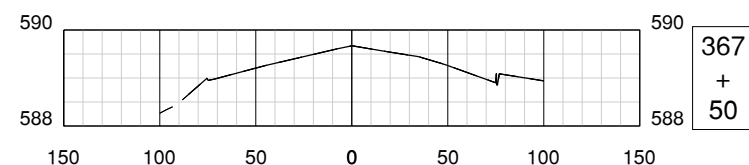
Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CG600.dwg
Date: Tuesday, April 26, 2022 12:52:44 PM



License No. 184-000613

CONSULTANTS

RWY 1836 STATION	FILL			REMOVE BASE (IN-SITU)			REMOVE PVMT (IN-SITU)		
	AREA (SF)	VOL (CY)	CUM VOL (CY)	AREA (SF)	VOL (CY)	CUM VOL (CY)	AREA (SF)	VOL (CY)	CUM VOL (CY)
356+00.00	278.7	517	517	97.2	180	180	181.3	336	336
356+50.00	271.4	503	1020	90.2	168	348	168.1	312	648
357+00.00	277.8	515	1535	90.2	168	516	168.1	312	960
357+50.00	267.7	496	2031	90.2	168	684	168.1	312	1272
358+00.00	263.6	489	2520	90.1	167	851	168.1	312	1584
358+50.00	267.6	496	3016	90.1	167	1018	168.1	312	1896
359+00.00	270.2	501	3517	90.1	167	1185	168.1	312	2208
359+50.00	271.5	503	4020	90.1	167	1352	168	312	2520
360+00.00	272.9	506	4526	90.1	167	1519	168	312	2832
360+50.00	275.1	510	5036	90.1	167	1686	168	312	3144
361+00.00	277.8	515	5551	90.1	167	1853	168	312	3456
361+50.00	277.7	515	6066	90.1	167	2020	168	312	3768
362+00.00	280.9	521	6587	90.1	167	2187	168	312	4080
362+50.00	278.1	515	7102	90.1	167	2354	168	312	4392
363+00.00	281.6	522	7624	90.1	167	2521	168	312	4704
363+50.00	306.4	568	8192	90.1	167	2688	206.1	382	5086
364+00.00	307	569	8761	90.1	167	2855	206.1	382	5468
364+50.00	306.1	567	9328	90.1	167	3022	206.1	382	5850
365+00.00	303.9	563	9891	90.1	167	3189	206.1	382	6232
365+50.00	298.3	553	10444	90.1	167	3356	206.1	382	6614
366+00.00	298.7	554	10998	90.1	167	3523	206.1	382	6996
366+50.00	296.9	550	11548	90.3	168	3691	203.9	378	7374
367+00.00	347.1	634	12182	105.6	196	3887	236.1	438	7812
367+50.00	0	0	12182	0	0	3887	0	0	7812



BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK | DATE | DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CG600.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

SHEET TITLE
RWY 18-36 NORTH
CROSS SECTIONS 3

CG609
SHEET 84 OF 91



License No. 184-000613

CONSULTANTS

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX CA022

IL PROJ. NO: SPI-4943

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH3-CG600.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

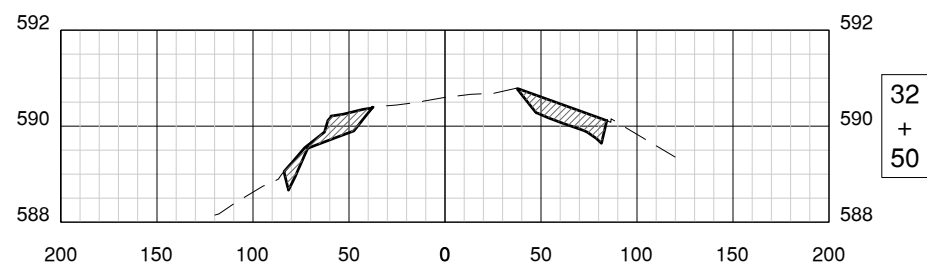
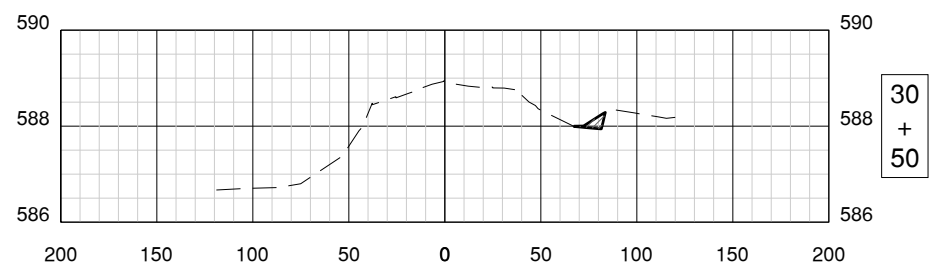
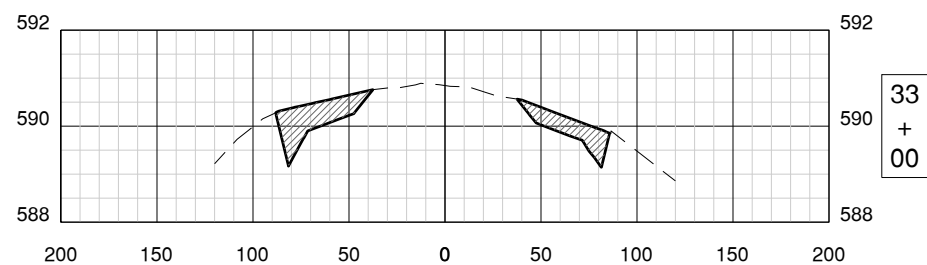
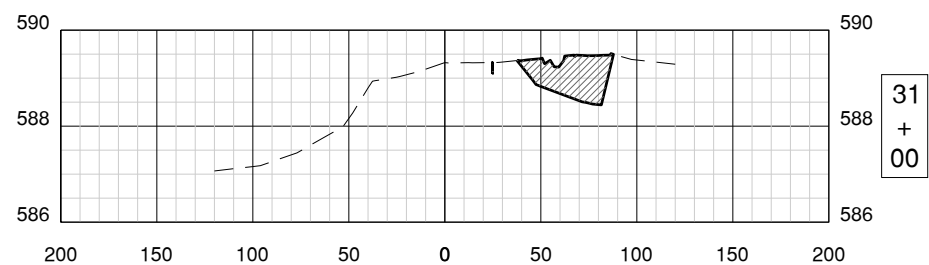
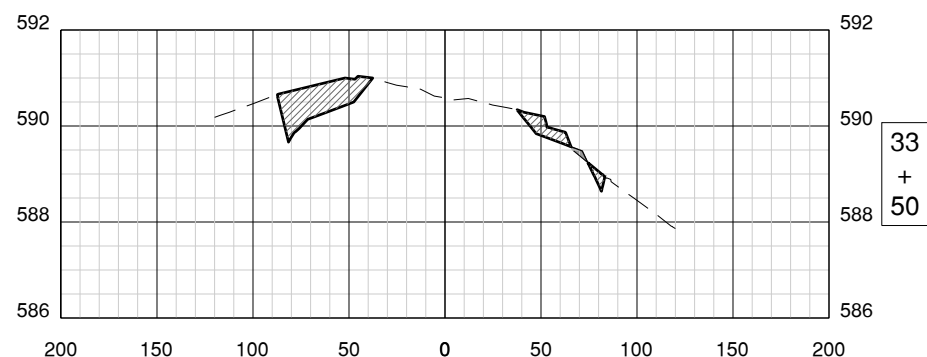
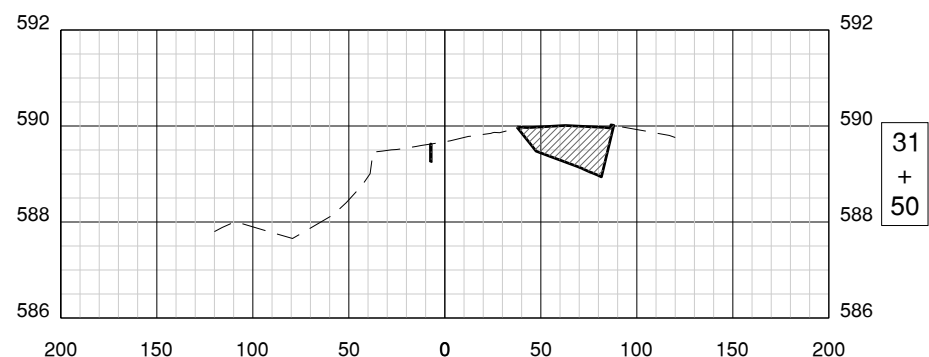
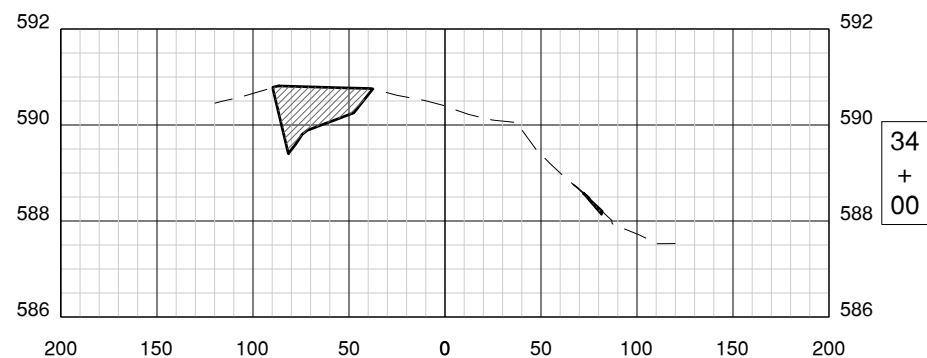
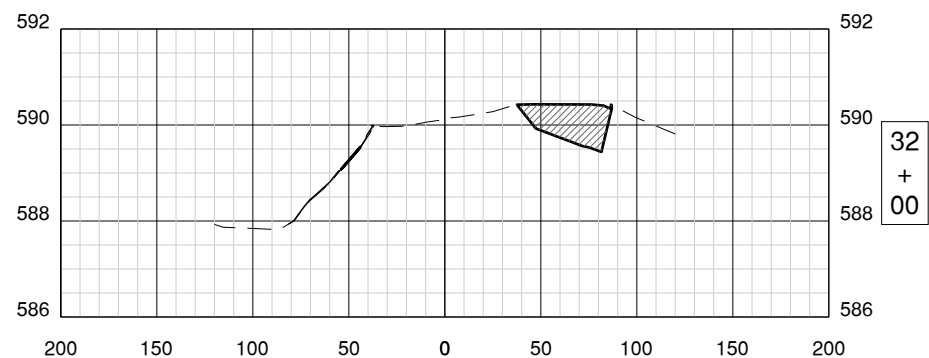
APPROVED BY: APR

COPYRIGHT:

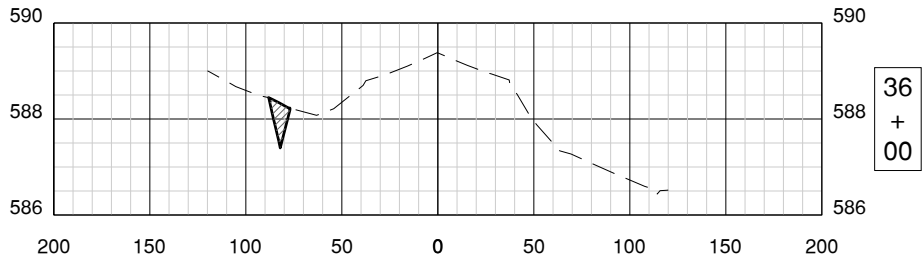
SHEET TITLE

TAXIWAY A CROSS
SECTIONS 1

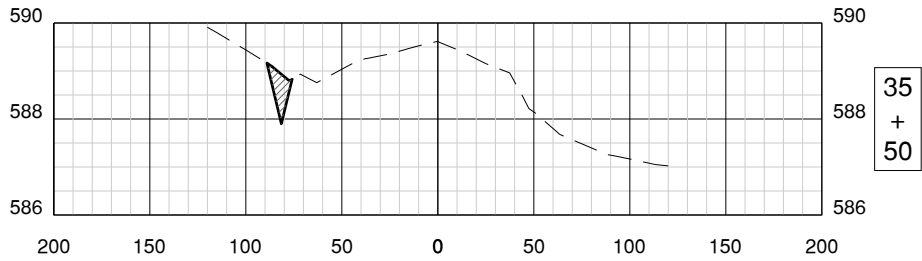
CG610
SHEET 85 OF 91



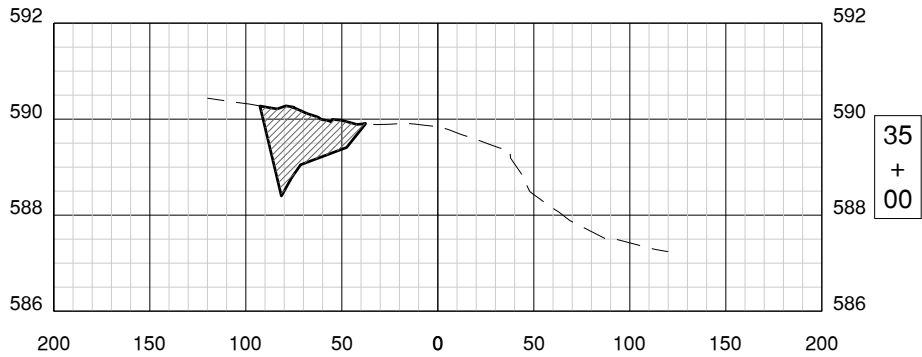
Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PH3\CG600.dwg
Date: Tuesday, April 26, 2022 12:52:56 PM



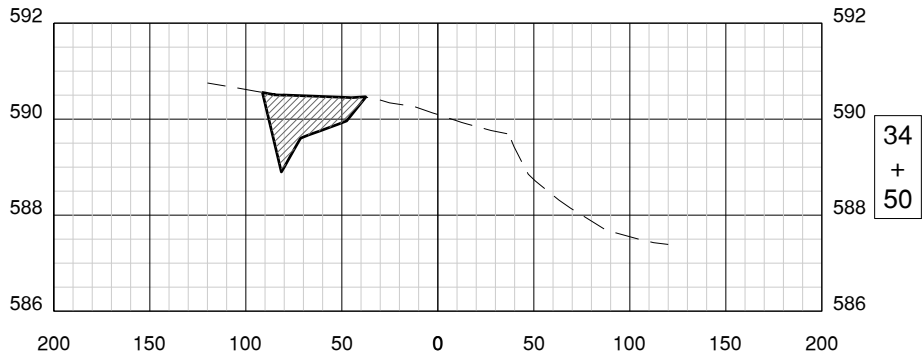
36
+
00



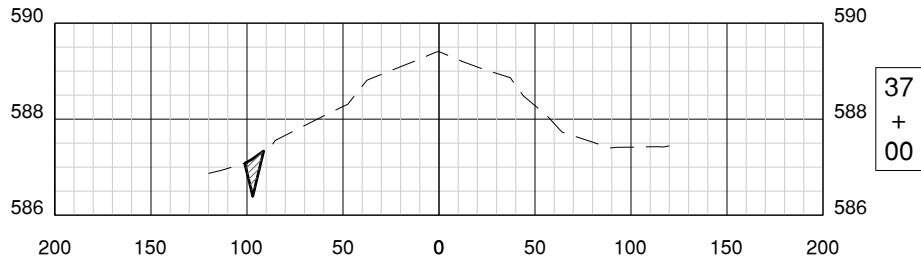
35
+
50



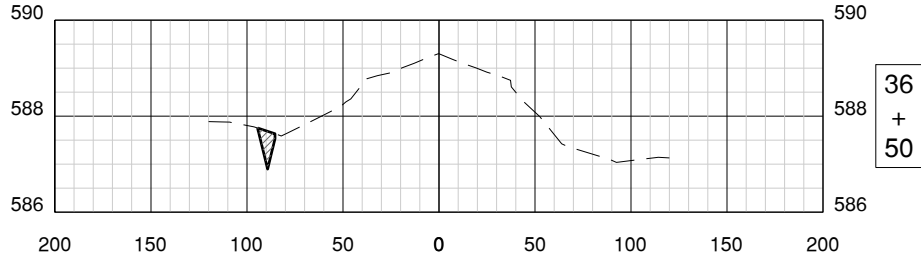
35
+
00



34
+
50



37
+
00



36
+
50

TXY A STATION	TAXIWAY A EARTHWORK					
	FILL			CUT (IN-SITU)		
	AREA (SF)	VOL (CY)	CUM VOL (CY)	AREA (SF)	VOL (CY)	CUM VOL (CY)
30+50.00	0	0	0	2	4	4
31+00.00	0	0	0	33.6	63	67
31+50.00	0	0	0	33	67	179
32+00.00	0.6	2	2	31.8	59	188
32+50.00	0	0	2	30	56	244
33+00.00	0	0	2	45.8	85	329
33+50.00	0.8	2	4	38.1	71	400
34+00.00	0	0	4	38.4	72	472
34+50.00	0	0	4	40.4	75	547
35+00.00	0	0	4	48.1	90	637
35+50.00	0	0	4	6.9	13	650
36+00.00	0	0	4	5.4	10	660
36+50.00	0	0	4	3.9	8	668
37+00.00	0	0	4	4	8	675



License No. 184-000613

CONSULTANTS

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

SHEET TITLE
TAXIWAY A CROSS
SECTIONS 2

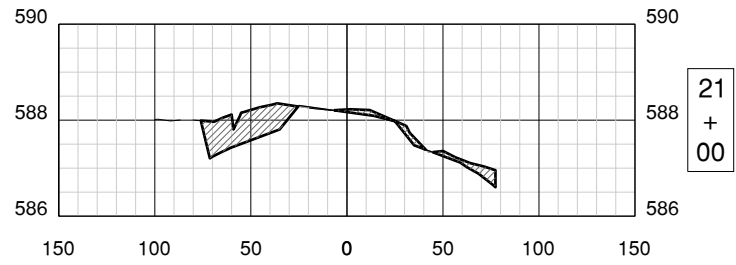
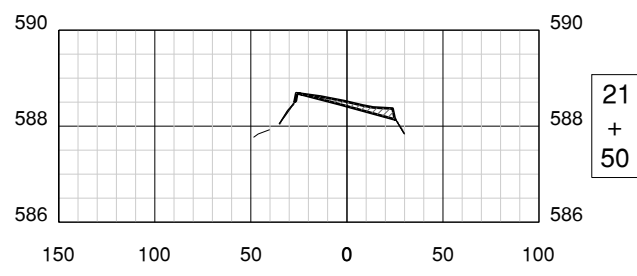
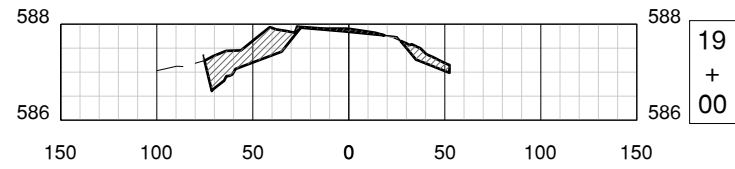
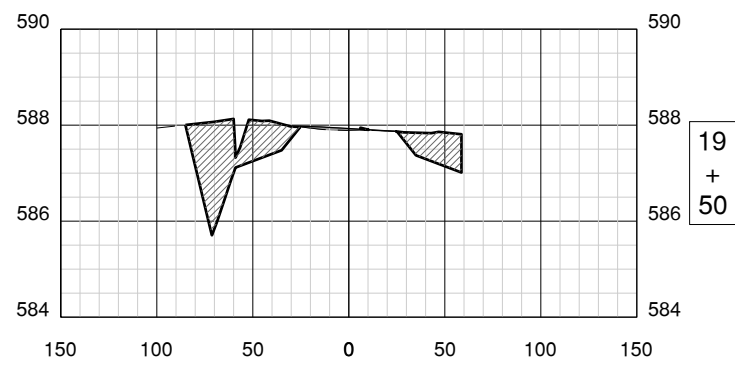
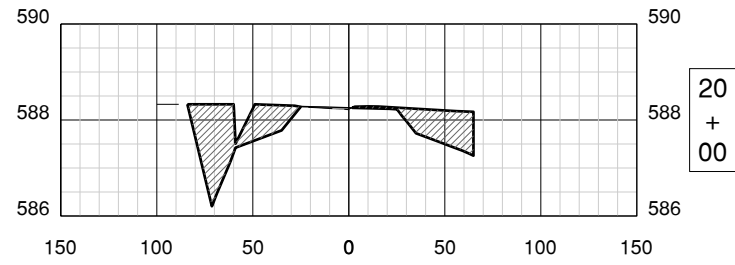
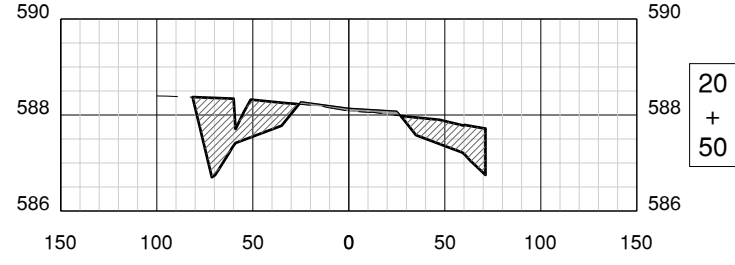
CG611
SHEET 86 OF 91

TXY #	TAXIWAY H EARTHWORK					
	FILL			CUT (IN-SITU)		
	AREA (SF)	VOL (CY)	CUMVOL (CY)	AREA (SF)	VOL (CY)	CUMVOL (CY)
STAT ON						
19+00.00	0.3	1	1	28.8	54	54
19+50.00	1.3	3	4	75.4	140	194
20+00.00	0.6	2	6	74.3	138	332
20+50.00	3	6	12	65.2	121	453
21+00.00	0.4	1	13	36.3	68	521
21+50.00	0.2	1	14	5	10	531



License No. 184-000613

CONSULTANTS



BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

TAXIWAY H CROSS SECTIONS 1

Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CG600.dwg
Date: Tuesday, April 26, 2022 12:53:09 PM



License No. 184-000613

CONSULTANTS

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 3

OWNER

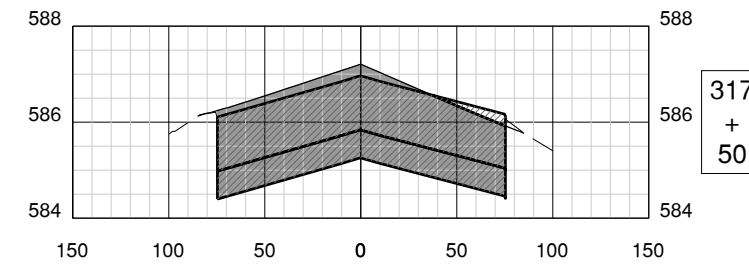
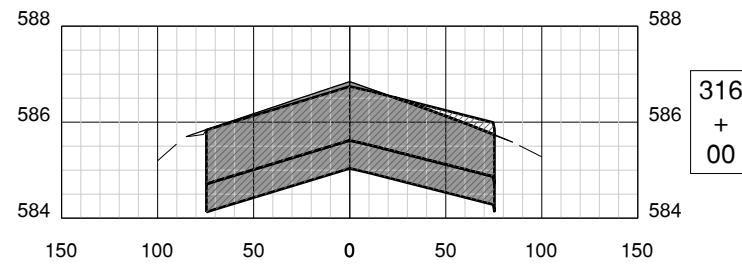
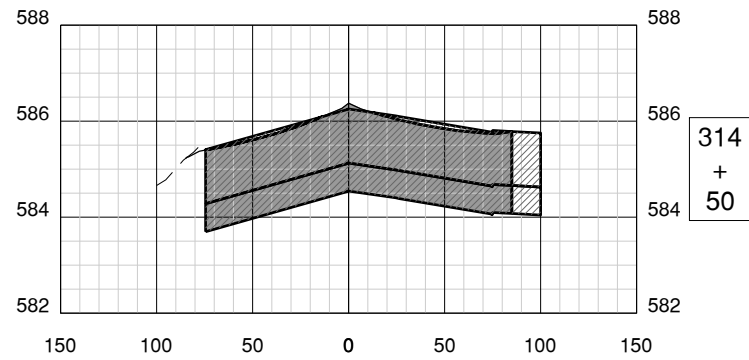
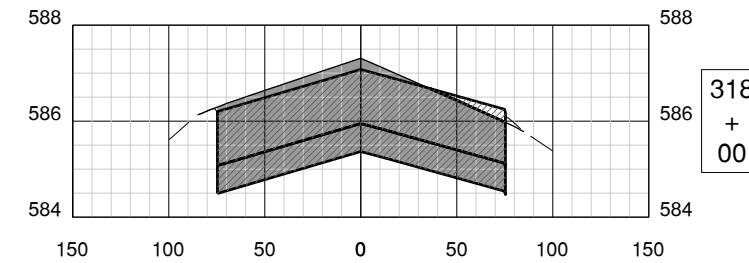
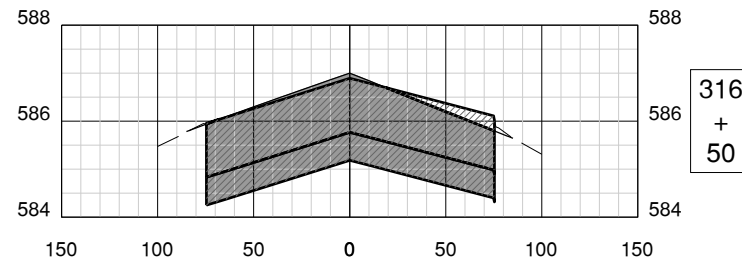
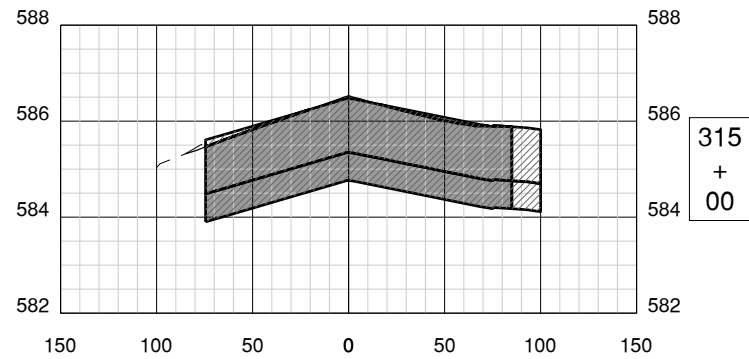
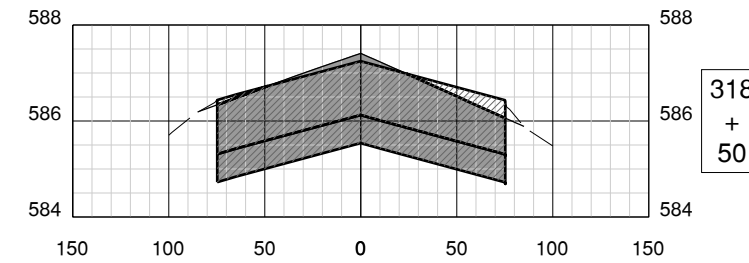
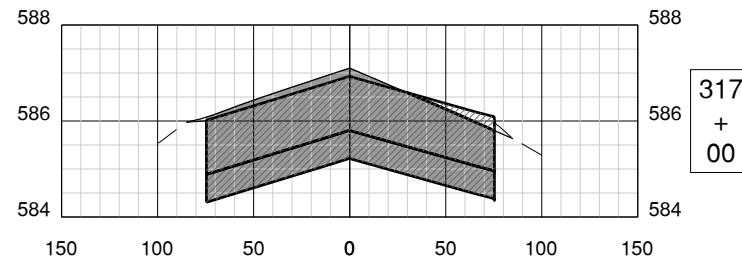
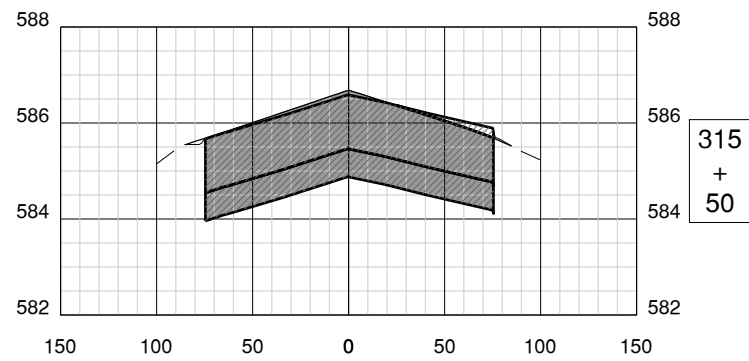


SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

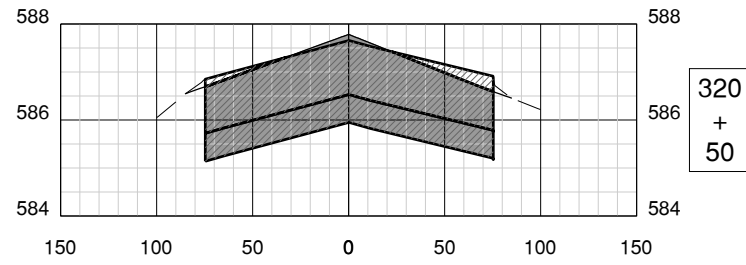
MARK	DATE	DESCRIPTION
AIP PROJ. NO:	3-17-0096-XX	CA022
IL PROJ. NO:	SPI-4943	
CMT PROJECT NO:	180035-05	
CAD DWG FILE:	18003505-PH3-CG600.DWG	
DESIGNED BY:	HWI	
DRAWN BY:	DPA	
CHECKED BY:	CHK	
APPROVED BY:	APR	
COPYRIGHT:		

SHEET TITLE
**RUNWAY 18-36 SOUTH
CROSS SECTIONS 1**

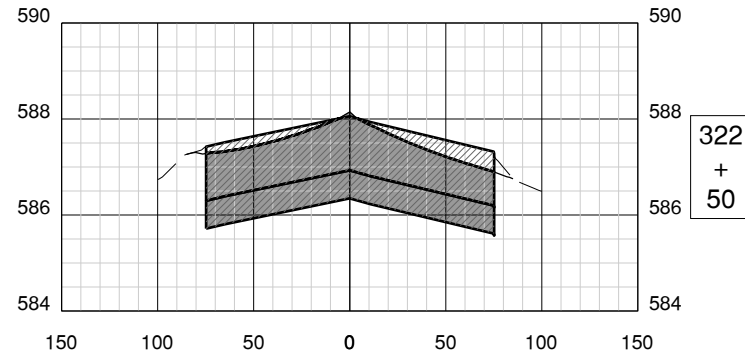
CG613
SHEET 88 OF 91



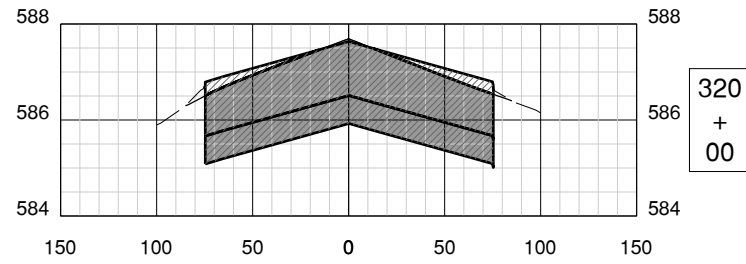
Path: K:\Springfield\18035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CG600.dwg
 Date: Tuesday, April 26, 2022 12:53:22 PM



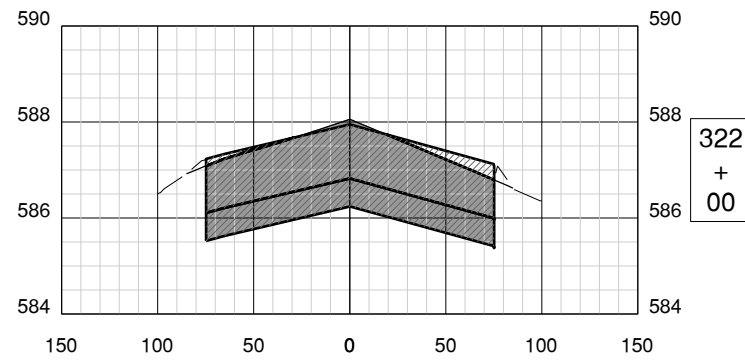
320
+
50



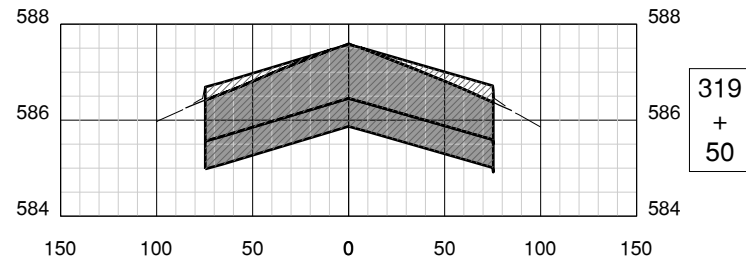
322
+
50



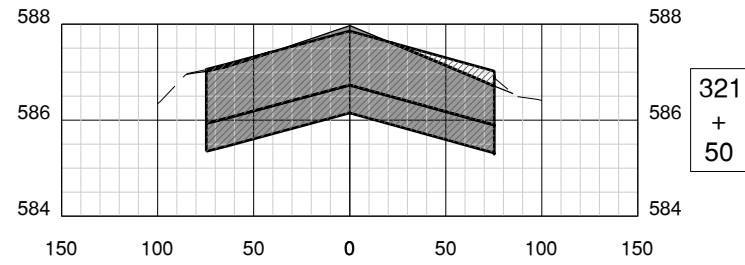
320
+
00



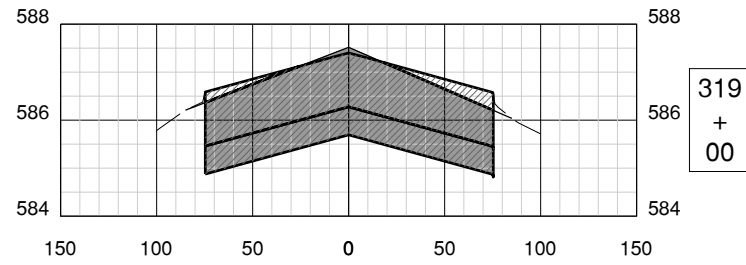
322
+
00



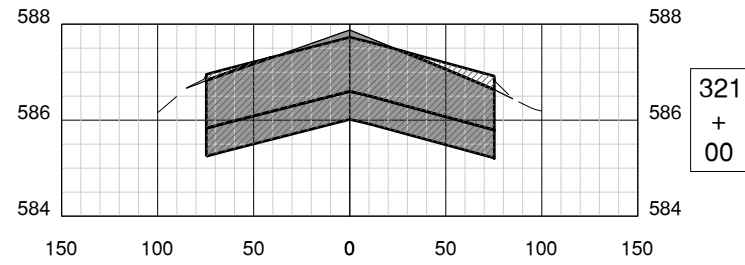
319
+
50



321
+
50



319
+
00



321
+
00

RWY 1836 STATION	18/36'S EARTHWORK								
	FILL			REMOVE BASE (IN-SITU)			REMOVE PAVEMENT (IN-SITU)		
	AREA (SF)	VOL (CY)	CUM VOL (CY)	AREA (SF)	VOL (CY)	CUM VOL (CY)	AREA (SF)	VOL (CY)	CUM VOL (CY)
314-50.00	273.8	908	508	104.8	195	195	202.3	375	375
315-00.00	275.4	510	1018	104.8	195	390	202.3	375	750
315-50.00	264.9	491	1509	90.1	167	557	173.8	322	1072
316-00.00	263	488	1997	90.1	167	724	173.8	322	1394
316-50.00	259.9	482	2479	90.1	167	891	173.8	322	1716
317-00.00	270.1	501	2980	90.1	167	1058	173.8	322	2038
317-50.00	277.8	515	3495	90.1	167	1225	173.8	322	2360
318-00.00	276	512	4007	90.1	167	1392	173.8	322	2682
318-50.00	258.8	480	4487	90.1	167	1559	173.8	322	3004
319-00.00	250	463	4950	90.1	167	1726	173.8	322	3326
319-50.00	244	452	5402	90.1	167	1893	173.8	322	3648
320-00.00	248.3	460	5862	90.1	167	2060	173.8	322	3970
320-50.00	255.2	473	6335	90.1	167	2227	173.8	322	4292
321-00.00	259.5	481	6816	90.1	167	2394	173.8	322	4614
321-50.00	257.8	478	7294	90.1	167	2561	173.8	322	4936
322-00.00	254	471	7765	90.1	167	2728	173.8	322	5258
322-50.00	234.2	434	8199	90.1	167	2895	173.8	322	5580
323-00.00	125.1	232	8431	90.1	167	3062	173.8	322	5902



License No. 184-000613

CONSULTANTS

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA022
IL PROJ. NO: SPI-4943	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH3-CG600.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: CHK	
APPROVED BY: APR	
COPYRIGHT:	

SHEET TITLE
RUNWAY 18-36 SOUTH
CROSS SECTIONS 2

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

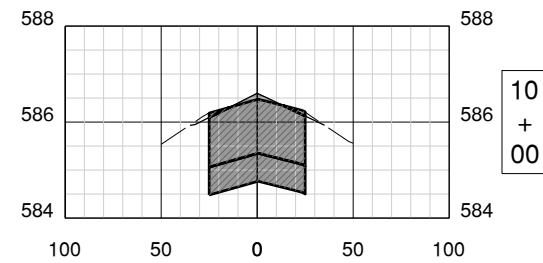
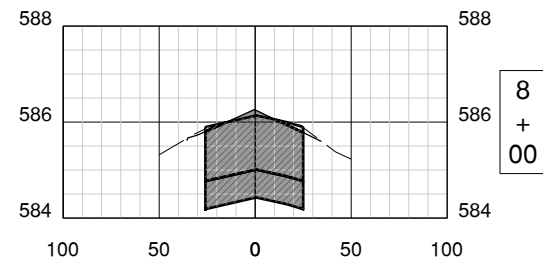
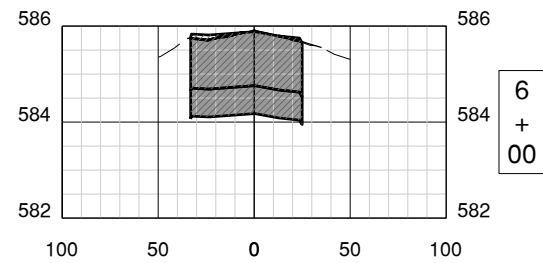
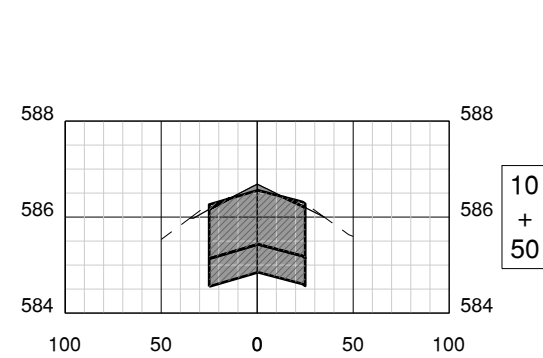
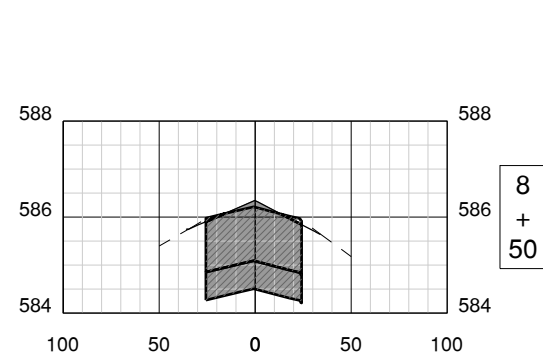
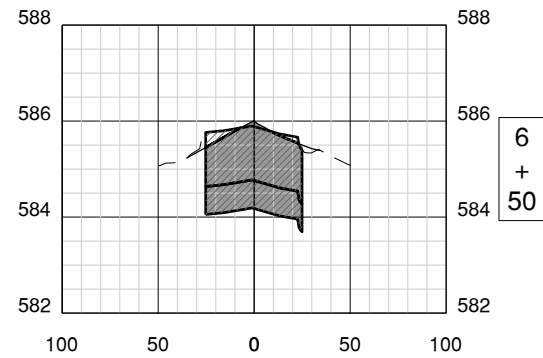
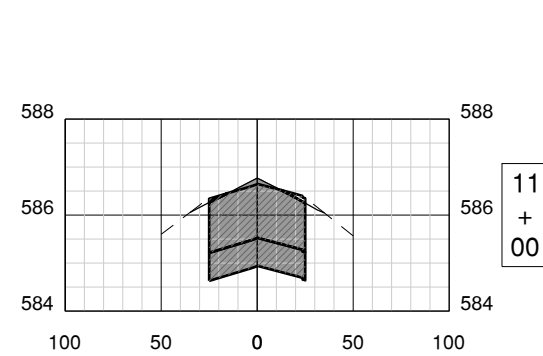
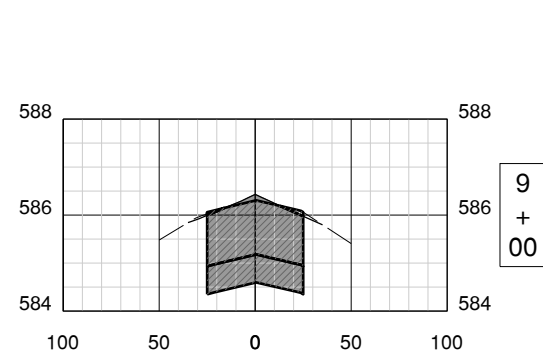
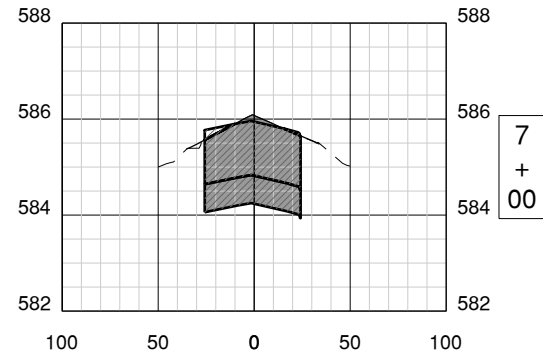
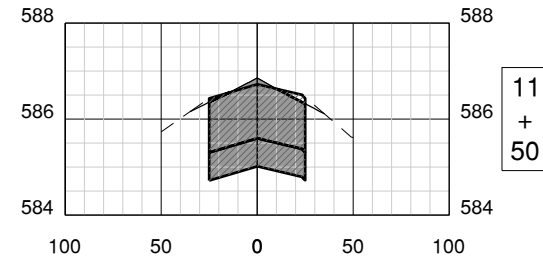
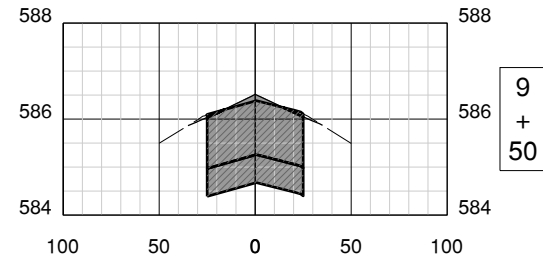
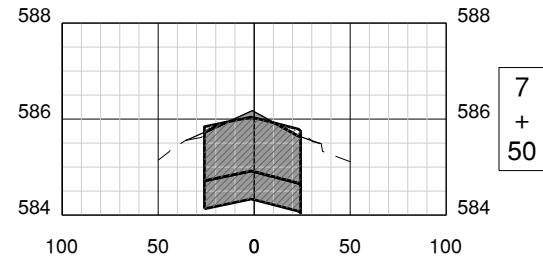
OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION
AIP PROJ. NO:	3-17-0096-XX	CA022
IL PROJ. NO:	SPI-4943	
CMT PROJECT NO:	180035-05	
CAD DWG FILE:	18003505-PH3-CG600.DWG	
DESIGNED BY:	HWI	
DRAWN BY:	DPA	
CHECKED BY:	CHK	
APPROVED BY:	APR	
COPYRIGHT:		

SHEET TITLE
**TAXIWAY C CROSS
SECTIONS 1**





License No. 184-000613

CONSULTANTS

BID SUBMITTAL
APRIL 22, 2022

RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 3

OWNER



SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK DATE DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX CA022

IL PROJ. NO: SPI-4943
CMT PROJECT NO: 180035-05
CAD DWG FILE: 18003505-PH3-CG600.DWG
DESIGNED BY: HWI
DRAWN BY: DPA
CHECKED BY: CHK
APPROVED BY: APR
COPYRIGHT:

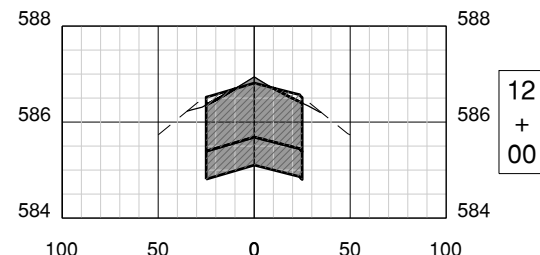
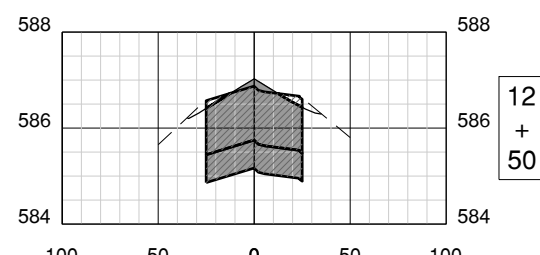
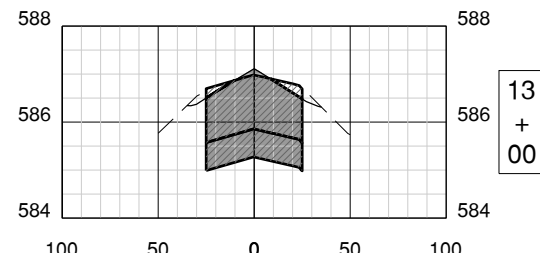
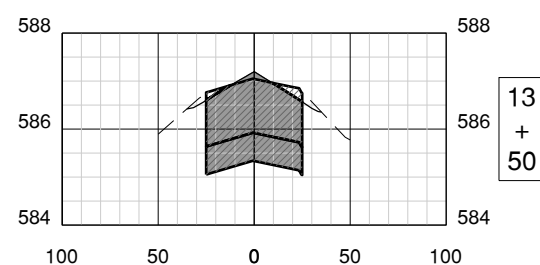
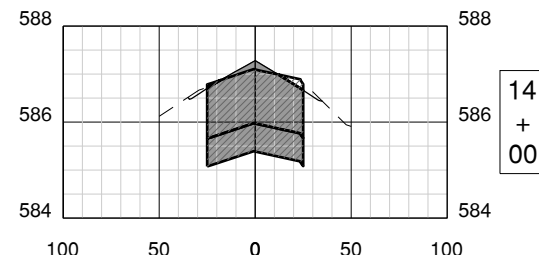
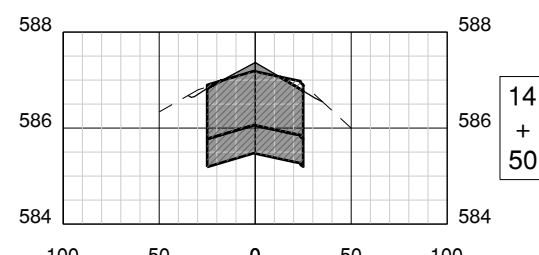
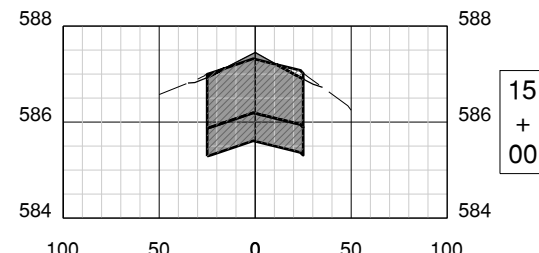
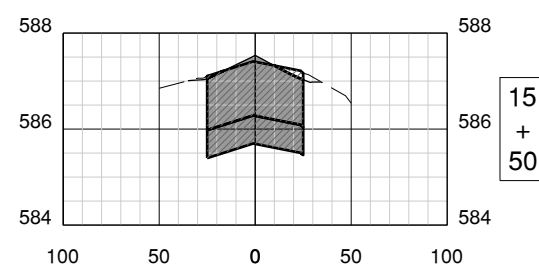
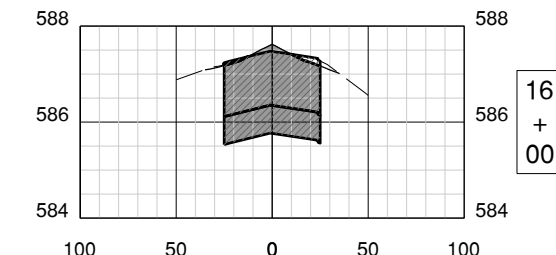
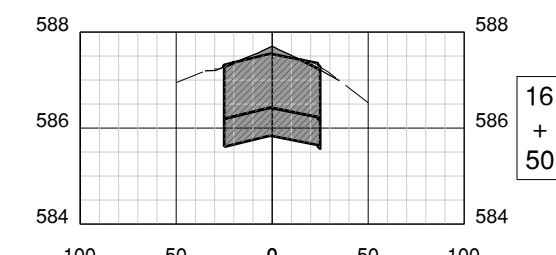
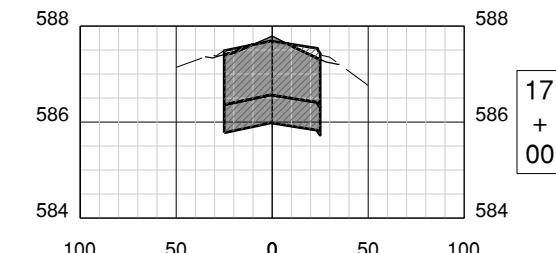
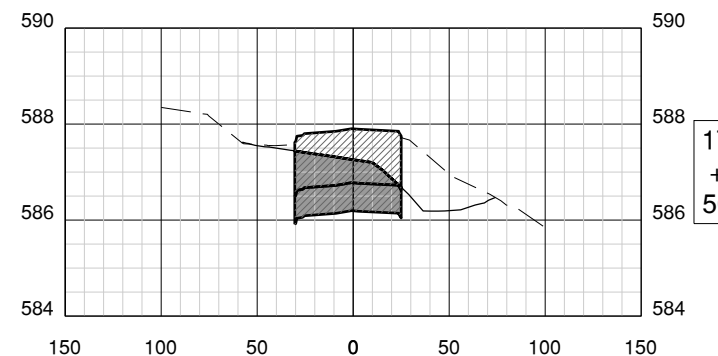
SHEET TITLE
TAXIWAY C CROSS
SECTIONS 2

CG616
SHEET 91 OF 91

TAXIWAY C EARTHWORK			
TAXIWAY C STATION	FILL		
	AREA (SF)	VOL (CY)	CUM VOL (CY)
6+00.00	100.2	185	185
6+50.00	85.7	159	345
7+00.00	87.7	163	508
7+50.00	88.1	164	672
8+00.00	89.9	167	839
8+50.00	88.7	165	1004
9+00.00	88.7	165	1169
9+50.00	88.7	165	1334
10+00.00	87.7	163	1497
10+50.00	87.5	163	1660
11+00.00	87.2	162	1822
11+50.00	86.7	161	1983
12+00.00	86.6	161	2144
12+50.00	88.2	164	2308
13+00.00	85.8	159	2467
13+50.00	86.5	161	2628
14+00.00	88.7	165	2793
14+50.00	88.5	164	2957
15+00.00	87.4	162	3119
15+50.00	87	162	3281
16+00.00	87.3	162	3443
16+50.00	89.5	165	3609
17+00.00	86.1	160	3769
17+50.00	61.8	115	3884

TAXIWAY C EARTHWORK			
TAXIWAY C STATION	BASE REMOVAL (IN-SITU)		
	AREA (SF)	VOL (CY)	CUM VOL (CY)
6+00.00	34.9	65	65
6+50.00	30.2	56	121
7+00.00	29.9	56	177
7+50.00	30.1	56	233
8+00.00	30.6	57	290
8+50.00	29.9	56	346
9+00.00	30	56	402
9+50.00	30	56	458
10+00.00	30	56	514
10+50.00	30	56	570
11+00.00	30	56	626
11+50.00	30	56	682
12+00.00	30	56	738
12+50.00	30	56	794
13+00.00	30	56	850
13+50.00	30	56	906
14+00.00	30	56	962
14+50.00	30	56	1018
15+00.00	30	56	1074
15+50.00	30	56	1130
16+00.00	30	56	1186
16+50.00	30	56	1242
17+00.00	30	56	1298
17+50.00	33.3	62	1360

TAXIWAY C EARTHWORK			
TAXIWAY C STATION	PAVEMENT REMOVAL (IN-SITU)		
	AREA (SF)	VOL (CY)	CUM VOL (CY)
6+00.00	67.3	125	125
6+50.00	58.2	108	233
7+00.00	57.8	108	341
7+50.00	58	108	449
8+00.00	59	110	559
8+50.00	57.8	108	667
9+00.00	57.9	108	775
9+50.00	57.9	108	883
10+00.00	57.9	108	991
10+50.00	57.9	108	1099
11+00.00	57.9	108	1207
11+50.00	57.9	108	1315
12+00.00	57.9	108	1423
12+50.00	57.9	108	1531
13+00.00	57.9	108	1639
13+50.00	57.9	108	1747
14+00.00	57.9	108	1855
14+50.00	57.9	108	1963
15+00.00	57.9	108	2071
15+50.00	57.9	108	2179
16+00.00	57.9	108	2287
16+50.00	57.9	108	2395
17+00.00	57.9	108	2503
17+50.00	64.2	119	2622



Path: K:\Springfield\180035-05_Rwy_18-36\Draw\Sheets\PHASE 3\18003505-PH3-CG600.dwg
Date: Tuesday, April 26, 2022 12:53:35 PM