

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	1
		ILLINOIS	CONTRACT NO. 62P58	

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED IN THE CITY OF ELMHURST
AND VILLAGE OF ADDISON

TRAFFIC DATA

I-290
ADT: 101,700
POSTED SPEED: 55 MPH
DESIGN DESIGNATION: EXPRESSWAY

APPLY TO SHEETS:
1-44, 69-77

APPLY TO SHEETS:
45-68



Matthew A. Miller

MATTHEW A. MILLER
HNTB
NO.: 062060879
DATE SIGNED: 3/18/2022
EXP. DATE: 11/30/2023



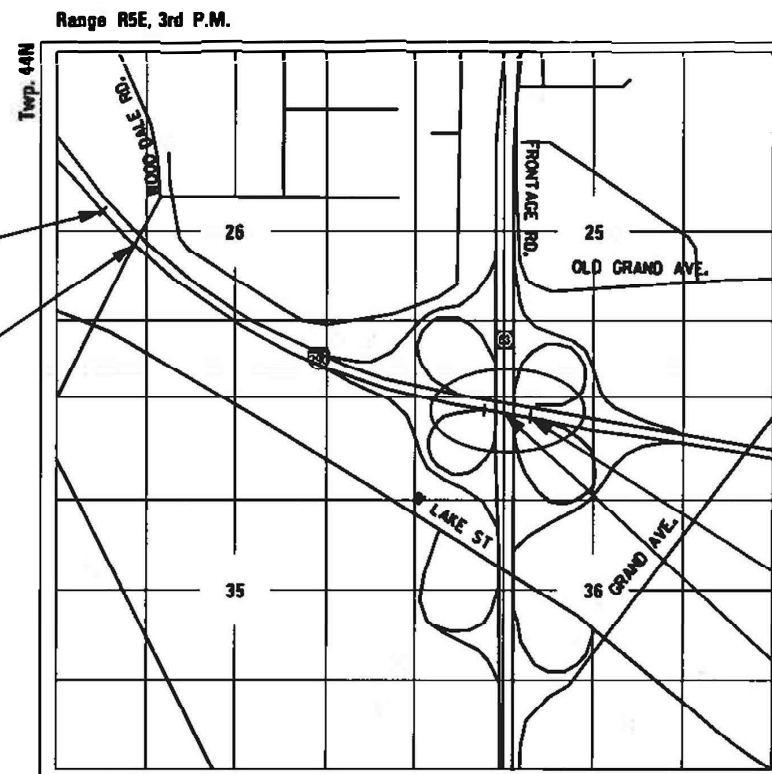
Luke C. Martin

LUKE C. MARTIN
GKE, LLC
NO.: 081007429
DATE SIGNED: 3/18/2022
EXP. DATE: 11/30/2022

PROPOSED HIGHWAY PLANS

(EISENHOWER EXPRESSWAY)
SECTION 2021-140-BR
FAI 290 (1-290) OVER IL 83 AND WOOD DALE RD.
BRIDGE JOINT REPAIR
DUPAGE COUNTY

C-91-014-22

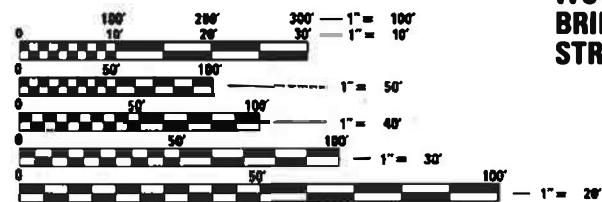


BEGIN IMPROVEMENT
STA. 141 + 55.00

I-290 EB OVER
WOOD DALE ROAD
BRIDGE LOCATION
STRUCTURE NO. 022-0105

END IMPROVEMENT
STA. 215 + 51.00

I-290 EB OVER
IL RT. 83
BRIDGE LOCATION
STRUCTURE NO. 022-0096



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 611

PROJECT ENGINEER: PRAVEEN KAINI (847) 705-4237
PROJECT MANAGER: JEAN ALAIN MIDY (847) 221-3056

PROJECT LENGTH
GROSS LENGTH = 7,396 FT. = 1.40 MILE
NET LENGTH = 7,396 FT. = 1.40 MILE

CONTRACT NO. 62P58



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED March 21, 2022
Jose Rios REGIONAL ENGINEER
May 13, 2022 [Signature]
ENGINEER OF DESIGN AND ENVIRONMENT

May 13, 2022 [Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL JULIE AT (800) 892-0123 OR 811 TO DETERMINE WHICH UTILITIES ARE WITHIN THE AREA. 48 HOUR NOTIFICATION IS REQUIRED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AS REQUIRED.
- PATCHING LIMITS ARE NOT SHOWN ON THE PLANS. LIMITS OF THIS WORK WILL BE DETERMINED BY THE ENGINEER.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJACENT RESIDENTIAL AREAS.
- THE CONTRACTOR SHALL REQUEST AND GAIN APPROVAL FROM THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S EXPRESSWAY TRAFFIC OPERATIONS ENGINEER AT WWW.IDOTLCS.COM TWENTY-FOUR (24) HOURS IN ADVANCE OF ALL DAILY LANE, RAMP AND SHOULDER CLOSURES AND 7 DAYS IN ADVANCE OF ALL PERMANENT AND WEEKEND CLOSURES ON ALL FREEWAYS AND/OR EXPRESSWAYS IN DISTRICT ONE.
- THE ALIGNMENTS AND SUPPORTING DATA SHOWN IN THE PLANS WAS DEVELOPED FROM PREVIOUS PLANIMETRICS AND AERIAL PHOTOGRAPHY FURNISHED BY THE DEPARTMENT AND IS NOT THE RESULT OF A GROUND SURVEY. THEREFORE, THE ALIGNMENTS AND SUPPORTING DATA SHOWN IN THE PLANS IS FOR REFERENCE PURPOSES ONLY. THE RELATIVE ACCURACY OF THE INFORMATION IS UNKNOWN AND CANNOT BE GUARANTEED. THE CONTRACTOR MAY BE REQUIRED TO ADJUST LAYOUT TO MATCH ACTUAL FIELD CONDITIONS AND THE INTENT OF THE PLANS.
- THE CONTRACTOR SHALL SWEEP AND CLEAN THE PAVEMENT SURFACE, PER ARTICLE 107.15 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD (FOR FUTURE REFERENCES), ALL EXISTING PAVEMENT MARKING LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL PROTECT EXISTING UTILITIES AND SHALL BRACE AND SUPPORT THE UTILITIES PROPERLY IN ORDER TO PREVENT SETTLEMENT, DISPLACEMENT, OR DAMAGE TO THE UTILITIES. THE PROTECTION OF THE UTILITIES AS SPECIFIED HEREIN WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT.
- THE CONTRACTOR SHALL TAKE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- TEMPORARY EDGE LINES SHOWN IN THE SUGGESTED MAINTENANCE OF TRAFFIC SHALL BE INSTALLED WITHIN 24 HOURS OF MILLING OR BINDER REPLACEMENT.
- EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH REVISED TRAFFIC PATTERNS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PAVEMENT MARKING REMOVAL.
- THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING DRAINAGE THROUGHOUT THE CONSTRUCTION OF THIS PROJECT. THIS WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.
- NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET.
- VERTICAL BARRICADES WILL REMAIN IN PLACE ALONG THE EDGES OF PAVEMENT AS SHOWN IN THE SUGGESTED MAINTENANCE OF TRAFFIC PLANS UNTIL THE SURFACE COURSE AND PROPOSED PAVEMENT MARKING EDGE LINES HAVE BEEN COMPLETED.
- THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS WHICH WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

INDEX OF SHEETS

1	COVER SHEET
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IDOT STANDARD DRAWINGS

STANDARD NUMBERS	DESCRIPTION
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
642001-03	SHOULDER RUMBLE STRIPS, 16 IN.
643001-02	SAND MODULE IMPACT ATTENUATORS
701101-05	OFF-RD OPERATIONS, MULTILANE 15' (4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
701400-11	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-13	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS>45 MPH
701428-01	TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY
701446-11	TWO LANE CLOSURE, FREEWAY/EXPRESSWAY
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

DISTRICT 1 STANDARD DRAWINGS

STANDARD NUMBERS	DESCRIPTION
TC-08	ENTRANCE AND EXIT RAMP CLOSURE DETAILS
TC-09	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE
TC-12	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS
TC-13	TYPICAL PAVEMENT MARKINGS
TC-17	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES
TC-18	FREEWAY/EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS ON FREEWAYS/EXPRESSWAYS
TC-22	ARTERIAL ROAD INFORMATION SIGN
TC-25	TRAFFIC CONTROL DETAILS FOR FREEWAY CENTER LANE CLOSURE SHOULDER LANE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ NDES	QMP
RUMBLE STRIP REMOVAL		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 2"	4% @ 70 GYR	QC/QA
CLASS D PATCHES (SPECIAL), OF TYPE SPECIFIED, 10"		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 2"	4% @ 70 GYR	QC/QA
(HMA BINDER IL-19.0), 8"	4% @ 70 GYR	QC/QA
CLASS D PATCHES (SPECIAL), OF TYPE SPECIFIED, 14"		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 2"	4% @ 70 GYR	QC/QA
(HMA BINDER IL-19.0), 12"	4% @ 70 GYR	QC/QA
QMP DESIGNATION:QUALITY CONTROL/QUALITY ASSURANCE (QC/QA)		

NOTES:

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES
INDEX OF SHEETS AND STANDARD DRAWINGS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	2
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				100% STATE	
				BRIDGE REHABILITATION	
				0013 URBAN	
				SN. 022-0105	SN. 022-0096
44000100	PAVEMENT REMOVAL	SQ YD	112	48	64
44004250	PAVED SHOULDER REMOVAL	SQ YD	40		40
44200553	CLASS A PATCHES, TYPE II, 10 INCH	SQ YD	12		12
44200559	CLASS A PATCHES, TYPE IV, 10 INCH	SQ YD	28		28
50102400	CONCRETE REMOVAL	CU YD	133.4	27.8	105.6
50300225	CONCRETE STRUCTURES	CU YD	17.3		17.3
50300255	CONCRETE SUPERSTRUCTURE	CU YD	49.4	27.8	21.6
50300260	BRIDGE DECK GROOVING	SQ YD	268	63	205
50300300	PROTECTIVE COAT	SQ YD	286	75	211
50301350	CONCRETE SUPERSTRUCTURE (APPROACH SLAB)	CU YD	66.7		66.7
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	29,390	2,250	27,140
50800515	BAR SPLICERS	EACH	302	24	278
52000110	PREFORMED JOINT STRIP SEAL	FOOT	263	123	140
52200020	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	45		45
58700300	CONCRETE SEALER	SQ FT	311		311
59000200	EPOXY CRACK INJECTION	FOOT	11		11
60262700	INLETS TO BE RECONSTRUCTED	EACH	4	1	3
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	7,626	3,813	3,813
67100100	MOBILIZATION	L SUM	1	0.5	0.5
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	124	62	62
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	36,900	18,450	18,450
70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	5,775	2,888	2,887
70400100	TEMPORARY CONCRETE BARRIER	FOOT	4,013	2,007	2,006
70400125	PINNING TEMPORARY CONCRETE BARRIER	EACH	60	30	30
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	2,713	1,357	1,356

LEGEND

- * SPECIAL PROVISION
- GBSP IDOT GUIDE BRIDGE SPECIAL PROVISION

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PLOT DATE = 3/18/2022	DATE - 12/03/21	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	3
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				100% STATE	
				BRIDGE REHABILITATION	
				0013 URBAN	
				SN. 022-0105	SN. 022-0096
70600250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	1	1
70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	1	1	
70600270	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, WIDE), TEST LEVEL 3	EACH	1	1	
70600350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	1	1
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	11,949	5,975	5,974
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	2,371	1,186	1,185
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	719	360	359
* 78003111	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - STANDARD - LINE 4"	FOOT	63	32	31
* 78003121	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - STANDARD - LINE 5"	FOOT	3,432	1,716	1,716
* 78003141	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -STANDARD - LINE 8"	FOOT	32	16	16
* 78004640	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 8"	FOOT	452	226	226
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	1,303	652	651
* 78008240	POLYUREA PAVEMENT MARKING TYPE I - LINE 8"	FOOT	869	435	434
* 78008250	POLYUREA PAVEMENT MARKING TYPE I - LINE 12"	FOOT	372	186	186
* 78011025	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	12,012	6,006	6,006
* 78011030	GROOVING FOR RECESSED PAVEMENT MARKING 6"	FOOT	3,432	1,716	1,716
* 78011045	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	2,855	1,428	1,427
* 78011065	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	719	360	359
* 78100300	REPLACEMENT REFLECTOR	EACH	383	192	191
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	504	252	252
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	383	192	191
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	8,333	4,167	4,166
* X0322102	TEMPORARY SIDEWALK RAMP	EACH	2	2	
* X0325201	SHOULDER RUMBLE STRIP REMOVAL	SQ YD	3,272	1,636	1,636
* X1700065	CLASS D PATCHES, TYPE IV, 10 INCH (SPECIAL)	SQ YD	64		64
* X1700085	CLASS D PATCHES, TYPE IV, 14 INCH (SPECIAL)	SQ YD	2,708	1,354	1,354
* X4421767	CLASS D PATCHES, TYPE III, 10 INCH (SPECIAL)	SQ YD	48	48	

LEGEND

- SPECIAL PROVISION
- GBSP IDOT GUIDE BRIDGE SPECIAL PROVISION
- * SPECIALTY ITEM

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PLOT DATE = 3/18/2022	DATE - 12/03/21	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	4
CONTRACT NO. 62P58			ILLINOIS FED. AID PROJECT	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				100% STATE	
				BRIDGE REHABILITATION	
				0013 URBAN	
				SN. 022-0105	SN. 022-0096
* X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	6	6
* X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	0.5	0.5
* X7013820	TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS	CAL DA	122	61	61
* X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH	FOOT	22,835	11,418	11,417
* X7030035	WET REFLECTIVE TEMPORARY TAPE TYPE III, 5 INCH	FOOT	2,600	1,300	1,300
* X7030045	WET REFLECTIVE TEMPORARY TAPE TYPE III, 8 INCH	FOOT	10,775	5,388	5,387
* X7030050	WET REFLECTIVE TEMPORARY TAPE TYPE III, 12 INCH	FOOT	1,760	880	880
* X8570000	SMART TRAFFIC MONITORING SYSTEM	L SUM	1	0.5	0.5
GBSP Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	4	4	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	0.5	0.5
GBSP Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	3	3	
GBSP Z0016200	DECK SLAB REPAIR (PARTIAL)	SQ YD	3	3	
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	281	141	140
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500	

LEGEND

- SPECIAL PROVISION
- GBSP IDOT GUIDE BRIDGE SPECIAL PROVISION

PAVEMENT MARKING SCHEDULE										
STATION	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (FOOT)	THERMOPLASTIC PAVEMENT MARKING - LINE 8" (FOOT)	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (FOOT)	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - STANDARD - LINE 4" (FOOT)	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - STANDARD - LINE 5" (FOOT)	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - STANDARD - LINE 8" (FOOT)	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE 8" (FOOT)	POLYUREA PAVEMENT MARKING TYPE I - LINE 4" (FOOT)	POLYUREA PAVEMENT MARKING TYPE I - LINE 8" (FOOT)	POLYUREA PAVEMENT MARKING TYPE I - LINE 12" (FOOT)
141+55 TO 149+00	1490		56		558					
149+00 TO 164+00	3000		85		750					
164+00 TO 179+00	2202		56		551	200		798		144
179+00 TO 194+00	2571	797	105	63	750					
194+00 TO 209+00	1577	1066	228		498	23	252	505	869	228
209+00 TO 215+00	1109	508	189		325	9				
TOTAL	11949	2371	719	63	3432	32	452	1303	869	372

PAY ITEM NUMBER	PAY ITEM NAME		UNIT OF MEASURE
70400100	TEMPORARY CONCRETE BARRIER		FOOT
MOT PHASE	BARRIER LENGTH (FOOT)	ADDITIONAL BARRIER REQUIRED FOR PHASE (FOOT)	QUANTITY (FOOT)
STAGE 1	1950.0	1950.0	1,950.0
STAGE 2	4012.5	2062.5	2,062.5
STAGE 3	762.5	0.0	0.0
			TOTAL 4,012.5

PAY ITEM NUMBER	PAY ITEM NAME		UNIT OF MEASURE
70400200	RELOCATE TEMPORARY CONCRETE BARRIER		FOOT
MOT PHASE	BARRIER LENGTH (FOOT)	RELOCATED BARRIER FROM PREVIOUS PHASE (FOOT)	QUANTITY (FOOT)
STAGE 1	1950.0	0.0	0.0
STAGE 2	4012.5	1950.0	1,950.0
STAGE 3	762.5	762.5	762.5
			TOTAL 2,712.5

PAY ITEM NUMBER	PAY ITEM NAME		UNIT OF MEASURE
X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH		FOOT
MOT PHASE			QUANTITY (FOOT)
STAGE 1			10,985.0
STAGE 2			11,410.0
STAGE 3			440.0
			TOTAL 22,835.0

PAY ITEM NUMBER	PAY ITEM NAME		UNIT OF MEASURE
X7030035	WET REFLECTIVE TEMPORARY TAPE TYPE III, 5 INCH		FOOT
MOT PHASE			QUANTITY (FOOT)
STAGE 1			1,250.0
STAGE 2			1,350.0
STAGE 3			0.0
			TOTAL 2,600.0

PAY ITEM NUMBER	PAY ITEM NAME		UNIT OF MEASURE
X7030045	WET REFLECTIVE TEMPORARY TAPE TYPE III, 8 INCH		FOOT
MOT PHASE			QUANTITY (FOOT)
STAGE 1			2,570.0
STAGE 2			5,760.0
STAGE 3			2,445.0
			TOTAL 10,775.0

PAY ITEM NUMBER	PAY ITEM NAME		UNIT OF MEASURE
X7030050	WET REFLECTIVE TEMPORARY TAPE TYPE III, 12 INCH		FOOT
MOT PHASE			QUANTITY (FOOT)
STAGE 1			195.0
STAGE 2			695.0
STAGE 3			870.0
			TOTAL 1,760.0

PAY ITEM NUMBER	PAY ITEM NAME		UNIT OF MEASURE	
X0325201	RUMBLE STRIP REMOVAL		SQ YD	
BEGIN STA.	END STA.	OFFSET	LENGTH (FOOT)	QUANTITY
164+85.00	173+59.00	RT	874	388.5
166+75.00	173+44.00	LT	669	297.4
175+63.00	203+75.00	LT	2,812	1,249.8
175+81.00	189+50.00	RT	1,369	608.5
195+88.00	201+71.00	RT	583	259.2
207+24.00	213+50.00	LT	626	278.3
210+90.00	215+18.00	RT	428	190.3
				TOTAL 3,272.0

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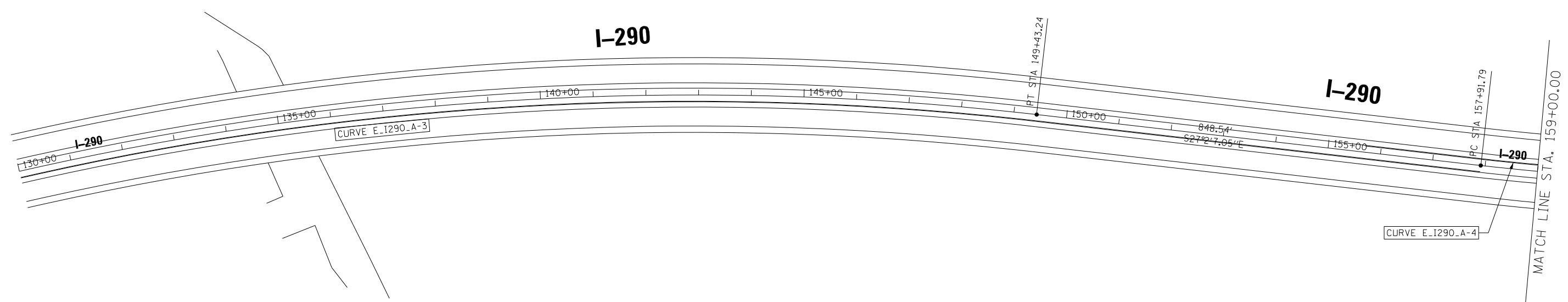
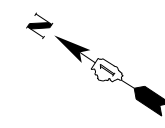
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PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	6
CONTRACT NO. 62P58			ILLINOIS FED. AID PROJECT	



NOTE
 THE ALIGNMENTS AND SUPPORTING DATA SHOWN IN THE PLANS WERE DEVELOPED FROM PREVIOUS PLANIMETRICS AND AERIAL PHOTOGRAPHY FURNISHED BY THE DEPARTMENT AND ARE NOT THE RESULT OF A GROUND SURVEY. THEREFORE, THE ALIGNMENTS AND SUPPORTING DATA SHOWN IN THE PLANS IS FOR REFERENCE PURPOSES ONLY. THE RELATIVE ACCURACY OF THE INFORMATION IS UNKNOWN AND CANNOT BE GUARANTEED. THE CONTRACTOR MAY BE REQUIRED TO ADJUST LAYOUT TO MATCH ACTUAL FIELD CONDITIONS AND THE INTENT OF THE PLANS.

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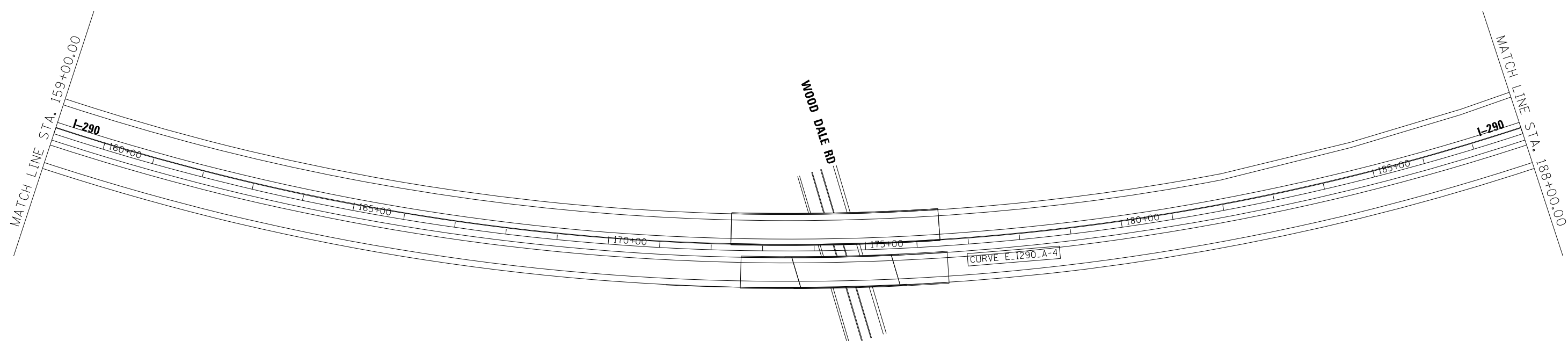
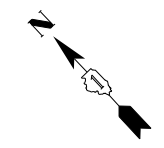
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PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT PLAN
 I-290 MAINLINE**

SCALE: 1"=100' SHEET 1 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	7
CONTRACT NO. 62P58			ILLINOIS FED. AID PROJECT	



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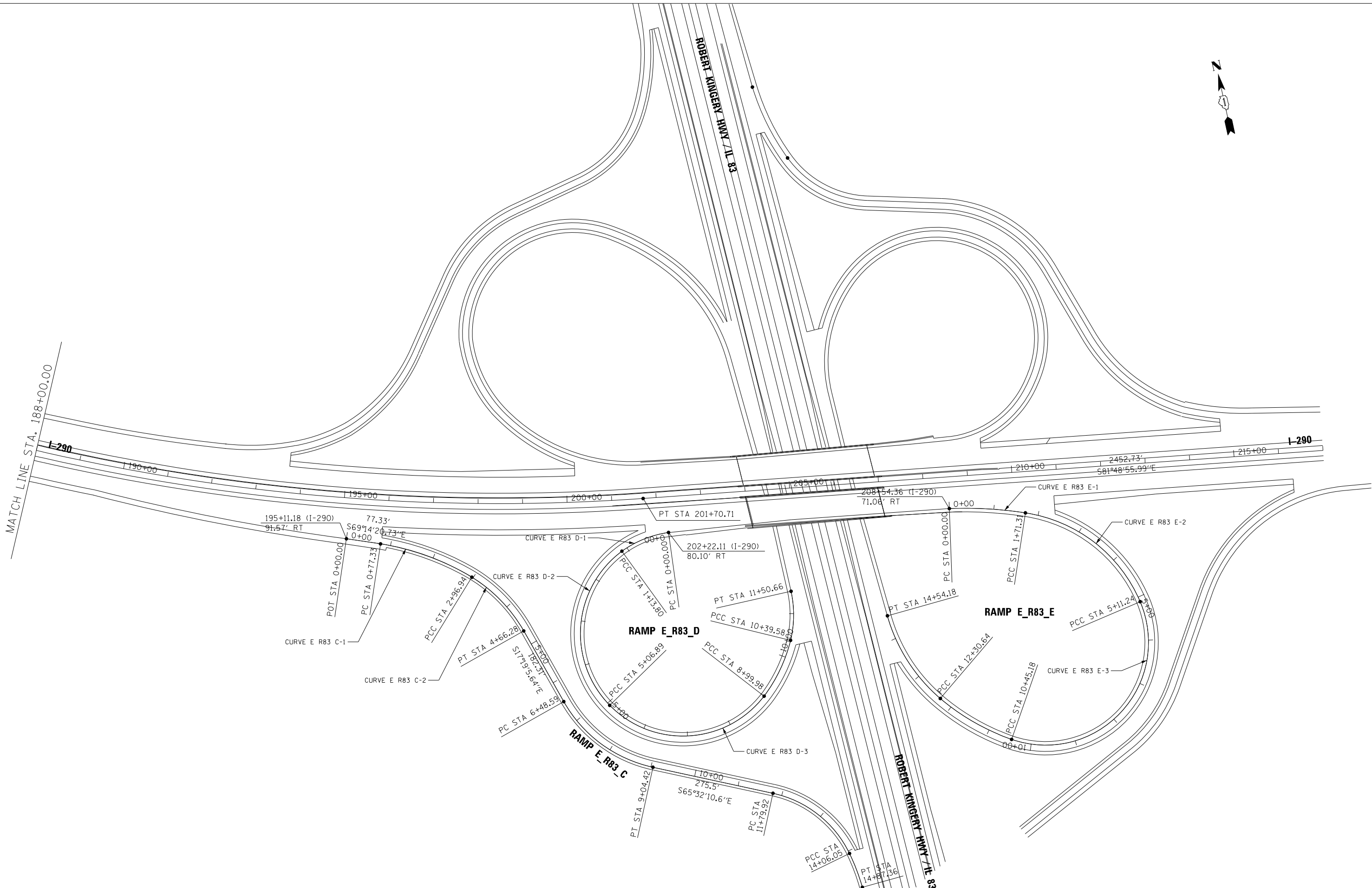
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PLOT SCALE = 200,0182' / in.	CHECKED - MM	REVISED -
PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT PLAN
I-290 MAINLINE

SCALE: 1"=100' SHEET 2 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	8
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	



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USER NAME = amalia,baymundo	DESIGNED - AMB	REVISED -
DRAWN - AMB	REVISIONS	
PLOT SCALE = 200,0182' / in.	CHECKED - MM	REVISED -
PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT PLAN
I-290 MAINLINE**

SCALE: 1"=100' SHEET 3 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	9
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	

RAMP E_R83_C

Point 530 N 1,916,547.6373 E 1,082,959.8713 Sta 0+00.00

Course from 530 to PC E R83 C-1 S 69° 14' 20.73" E Dist 77.3330

Curve Data

Curve E R83 C-1
 P.I. Station = 1+88.64 N 1,916,480.7703 E 1,083,136.2624
 Delta = 23° 02' 40.95" (RT)
 Degree = 10° 29' 37.44"
 Tangent = 111.3068
 Length = 219.6045
 Radius = 546.0000
 External = 11.2299
 Long Chord = 218.1273
 Mid. Ord. = 11.0036
 P.C. Station = 0+77.33 N 1,916,520.2252 E 1,083,032.1830
 P.T. Station = 2+96.94 N 1,916,403.7222 E 1,083,216.5917
 C.C. = N 1,916,009.6782 E 1,082,838.6429
 Back = S 69° 14' 20.73" E
 Ahead = S 46° 11' 39.78" E
 Chord Bear = S 57° 43' 00.26" E

Curve Data

Curve E R83 C-2
 P.I. Station = 3+83.45 N 1,916,343.8401 E 1,083,279.0240
 Delta = 28° 52' 34.14" (RT)
 Degree = 17° 03' 08.34"
 Tangent = 86.5082
 Length = 169.3386
 Radius = 336.0000
 External = 10.9577
 Long Chord = 167.5521
 Mid. Ord. = 10.6117
 P.C. Station = 2+96.94 N 1,916,403.7222 E 1,083,216.5917
 P.T. Station = 4+66.28 N 1,916,261.2536 E 1,083,304.7756
 C.C. = N 1,916,161.2336 E 1,082,984.0078
 Back = S 46° 11' 39.78" E
 Ahead = S 17° 19' 05.64" E
 Chord Bear = S 31° 45' 22.71" E

Course from PT E R83 C-2 to PC E R83 C-3 S 17° 19' 05.64" E Dist 182.3120

RAMP E_R83_D

Curve Data

Curve E R83 D-1
 P.I. Station = 0+58.05 N 1,916,412.7250 E 1,083,611.1727
 Delta = 27° 53' 42.38" (LT)
 Degree = 24° 30' 48.24"
 Tangent = 58.0489
 Length = 113.7953
 Radius = 233.7325
 External = 7.1005
 Long Chord = 112.6748
 Mid. Ord. = 6.8912
 P.C. Station = 0+00.00 N 1,916,408.3813 E 1,083,669.0588
 P.T. Station = 1+13.80 N 1,916,389.4816 E 1,083,557.9805
 C.C. = N 1,916,175.3041 E 1,083,651.5692
 Back = N 85° 42' 31.27" W
 Ahead = S 66° 23' 46.35" W
 Chord Bear = S 80° 20' 37.54" W

Curve Data

Curve E R83 D-2
 P.I. Station = 3+78.02 N 1,916,280.7595 E 1,083,317.1554
 Delta = 97° 55' 25.52" (LT)
 Degree = 24° 54' 40.35"
 Tangent = 264.2295
 Length = 393.0911
 Radius = 230.0000
 External = 120.3102
 Long Chord = 346.9655
 Mid. Ord. = 78.9910
 P.C. Station = 1+13.80 N 1,916,389.4816 E 1,083,557.9805
 P.T. Station = 5+06.89 N 1,916,057.2217 E 1,083,458.0384
 C.C. = N 1,916,179.8541 E 1,083,652.6182
 Back = S 65° 42' 10.38" W
 Ahead = S 32° 13' 15.13" E
 Chord Bear = S 16° 44' 27.62" W

Curve Data

Curve E R83 D-3
 P.I. Station = 7+71.12 N 1,915,833.6838 E 1,083,598.9215
 Delta = 97° 55' 25.52" (LT)
 Degree = 24° 54' 40.35"
 Tangent = 264.2295
 Length = 393.0911
 Radius = 230.0000
 External = 120.3102
 Long Chord = 346.9655
 Mid. Ord. = 78.9910
 P.C. Station = 5+06.89 N 1,916,057.2217 E 1,083,458.0384
 P.T. Station = 8+99.98 N 1,916,004.0376 E 1,083,800.9036
 C.C. = N 1,916,179.8541 E 1,083,652.6182
 Back = S 32° 13' 15.13" E
 Ahead = N 49° 51' 19.35" E
 Chord Bear = S 81° 10' 57.89" E

RAMP E_R83_E

Curve Data

Curve E R83 E-1
 P.I. Station = 0+85.91 N 1,916,312.2934 E 1,084,380.7405
 Delta = 10° 47' 09.81" (RT)
 Degree = 6° 17' 46.46"
 Tangent = 85.9086
 Length = 171.3095
 Radius = 910.0000
 External = 4.0461
 Long Chord = 171.0567
 Mid. Ord. = 4.0282
 P.C. Station = 0+00.00 N 1,916,327.3178 E 1,084,296.1559
 P.T. Station = 1+71.31 N 1,916,281.7050 E 1,084,461.0190
 C.C. = N 1,915,431.3424 E 1,084,137.0075
 Back = S 79° 55' 40.23" E
 Ahead = S 69° 08' 30.43" E
 Chord Bear = S 74° 32' 05.33" E

Curve Data

Curve E R83 E-2
 P.I. Station = 3+57.56 N 1,916,215.3907 E 1,084,635.0598
 Delta = 58° 10' 35.71" (RT)
 Degree = 17° 06' 52.05"
 Tangent = 186.2466
 Length = 339.9266
 Radius = 334.7800
 External = 48.3198
 Long Chord = 325.5112
 Mid. Ord. = 42.2253
 P.C. Station = 1+71.31 N 1,916,281.7050 E 1,084,461.0190
 P.T. Station = 5+11.24 N 1,916,032.5445 E 1,084,670.4863
 C.C. = N 1,915,968.8650 E 1,084,341.8184
 Back = S 69° 08' 30.43" E
 Ahead = S 10° 57' 54.72" E
 Chord Bear = S 40° 03' 12.57" E

Curve Data

Curve E R83 E-3
 P.I. Station = 10+35.44 N 1,915,517.9103 E 1,084,770.1967
 Delta = 132° 26' 12.94" (RT)
 Degree = 24° 48' 12.12"
 Tangent = 524.2046
 Length = 533.9474
 Radius = 231.0000
 External = 341.8450
 Long Chord = 422.7714
 Mid. Ord. = 137.8492
 P.C. Station = 5+11.24 N 1,916,032.5445 E 1,084,670.4863
 P.T. Station = 10+45.18 N 1,915,791.5859 E 1,084,323.1037
 C.C. = N 1,915,988.6053 E 1,084,443.7037
 Back = S 10° 57' 54.72" E
 Ahead = N 58° 31' 41.77" W
 Chord Bear = S 55° 15' 11.76" W

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USER NAME = amalla,baymundo	DESIGNED - AMB	REVISED -
PLOT SCALE = 200,0182' / In.	DRAWN - AMB	REVISED -
PLOT DATE = 3/18/2022	CHECKED - MM	REVISED -
	DATE - 02/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT PLAN
I-290 CURVE DATA**

SCALE: SHEET 4 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	10
CONTRACT NO. 62P58			ILLINOIS FED. AID PROJECT	

I-290

Curve Data

Curve E I290 A-3
 P.I. Station 133+55.93 N 1,921,197.9571 E 1,079,053.1656
 Delta = 32° 43' 06.91" (RT)
 Degree = 1° 00' 06.03"
 Tangent = 1,679.0725
 Length = 3,266.3891
 Radius = 5,720.0000
 External = 241.3492
 Long Chord = 3,222.1883
 Mid. Ord. = 231.5780
 P.C. Station 116+76.86 N 1,922,043.7320 E 1,077,602.6658
 P.T. Station 149+43.24 N 1,919,702.3623 E 1,079,816.3699
 C.C. = N 1,917,102.3979 E 1,074,721.4131
 Back = S 59° 45' 13.96" E
 Ahead = S 27° 02' 07.05" E
 Chord Bear = S 43° 23' 40.50" E

Course from PT E I290 A-3 to PC E I290 A-4 S 27° 02' 07.05" E Dist 848.5422

I-290

Curve Data

Curve E I290 A-4
 P.I. Station 181+64.83 N 1,916,832.8080 E 1,081,280.7080
 Delta = 54° 46' 48.94" (LT)
 Degree = 1° 15' 03.60"
 Tangent = 2,373.0457
 Length = 4,378.9193
 Radius = 4,580.0000
 External = 578.2697
 Long Chord = 4,214.0291
 Mid. Ord. = 513.4425
 P.C. Station 157+91.79 N 1,918,946.5432 E 1,080,202.0656
 P.T. Station 201+70.71 N 1,916,494.9806 E 1,083,629.5840
 C.C. = N 1,921,028.3329 E 1,084,281.5940
 Back = S 27° 02' 07.05" E
 Ahead = S 81° 48' 55.99" E
 Chord Bear = S 54° 25' 31.52" E

Course from PT E I290 A-4 to PC E I290 A-5 S 81° 48' 55.99" E Dist 2,452.7307

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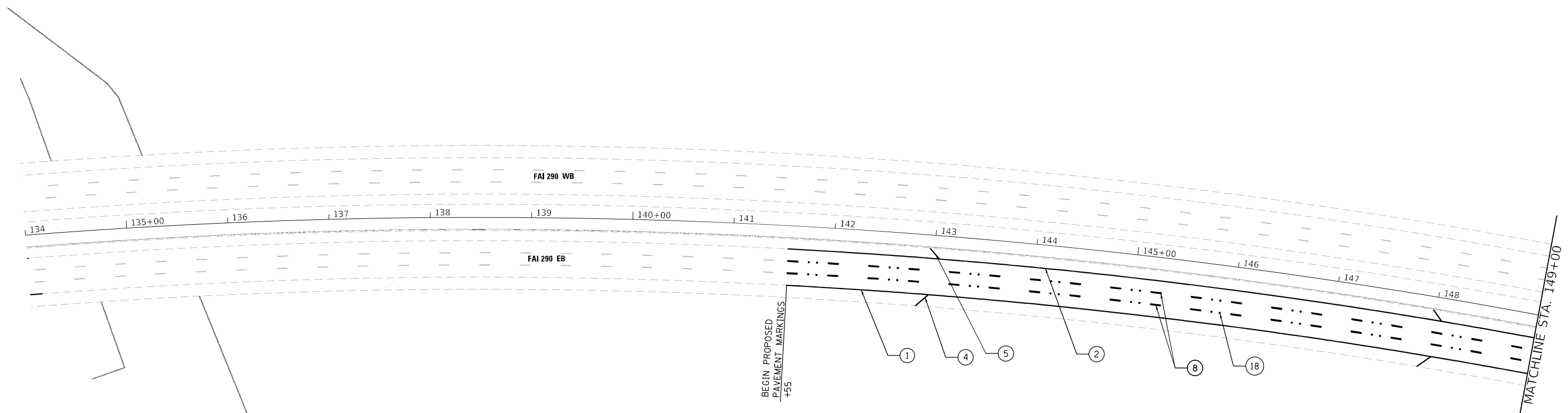
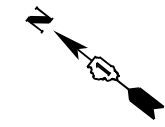
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PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT PLAN
I-290 CURVE DATA**

SCALE: SHEET 5 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	11
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	



LEGEND:

- | | |
|---|---|
| ① THERMOPLASTIC PAVEMENT MARKING LINE 4" (WHITE EDGE LINES) | ⑪ POLYUREA PAVEMENT MARKING TYPE 1, LINE 4" (WHITE EDGE LINE) |
| ② THERMOPLASTIC PAVEMENT MARKING LINE 4" (YELLOW EDGE LINE) | ⑫ POLYUREA PAVEMENT MARKING TYPE 1, LINE 4" (YELLOW EDGE LINE) |
| ③ THERMOPLASTIC PAVEMENT MARKING LINE 8" (WHITE EDGE LINES) | ⑬ POLYUREA PAVEMENT MARKING TYPE 1, LINE 8" (WHITE EDGE LINE) |
| ④ THERMOPLASTIC PAVEMENT MARKING LINE 12" (WHITE DIAGONALS SPACED @ 500' CENTERS ON MAINLINE AND 150' CENTERS ON RAMP) | ⑭ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (YELLOW DIAGONALS SPACED @ 500' CENTERS AND 40' CENTERS ON STRUCTURES) |
| ⑤ THERMOPLASTIC PAVEMENT MARKING LINE 12" (YELLOW DIAGONALS SPACED @ 500' CENTERS ON MAINLINE AND 150' CENTERS ON RAMP) | ⑮ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (WHITE DIAGONALS SPACED @ 500' CENTERS AND 40' CENTERS ON STRUCTURES) |
| ⑥ THERMOPLASTIC PAVEMENT MARKING LINE 12" (WHITE GORE CHEVRONS SPACED @ 30' CENTERS) | ⑯ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (WHITE GORE CHEVRONS SPACED @ 30' CENTERS) |
| ⑦ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 4" (WHITE 2' DASH, 6' SKIP) | ⑰ POLYUREA PAVEMENT MARKING TYPE 1, LINE 8" (WHITE 3' DASH, 9' SKIP) |
| ⑧ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 5" (WHITE 10' DASH, 30' SKIP) | ⑱ REPLACEMENT REFLECTOR MARKER (1-WAY CRYSTAL/OPAQUE) |
| ⑨ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 8" (WHITE 3' DASH, 9' SKIP) | |
| ⑩ PREFORMED PLASTIC PAVEMENT MARKING, TYPE D, LINE 8" (WHITE 10' DASH, 30' SKIP) | |

NOTES

- SEE IDOT DISTRICT 1 STANDARD TC-12 "MULTILANE FREEWAY PAVEMENT MARKING DETAILS" FOR PLACEMENT AND TYPE OF RAISED REFLECTIVE PAVEMENT MARKERS.
- FOR PROPOSED PAVEMENT MARKINGS IN THE PCC SECTIONS USE POLYUREA PAVEMENT MARKINGS ACCORDING TO STANDARD TC-12.
- PAVEMENT MARKINGS (PREFORMED PLASTIC AND THERMOPLASTIC) SHALL BE GROOVED UNLESS PLACED ON THE BRIDGE DECK OR APPROACH SLAB.
- SHOULDER RUMBLE STRIPS SHALL BE PLACED IN LOCATIONS WHERE EXISTING RUMBLE STRIPS WERE REMOVED AND AS DETERMINED BY ENGINEER IN ACCORDANCE WITH STANDARD 642001.

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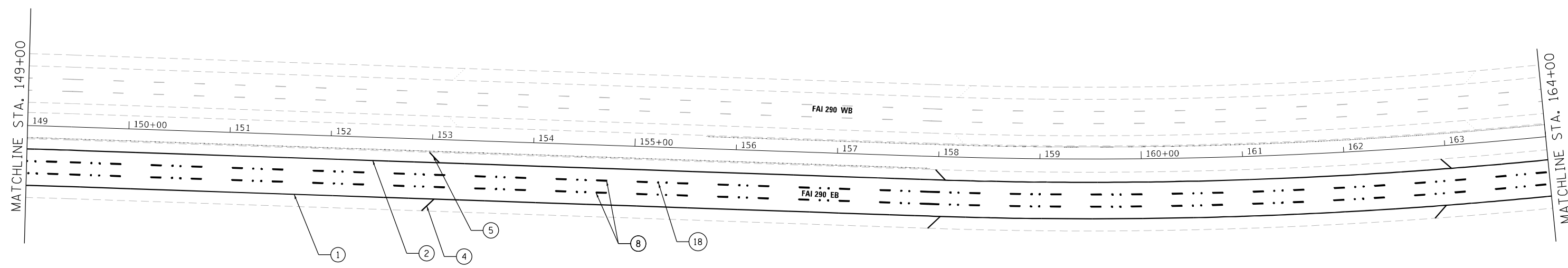
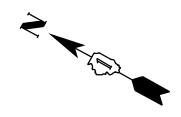
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PLOT SCALE = 100,0091' / in.	CHECKED - MM	REVISED -
PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED PAVEMENT MARKINGS

SCALE: 1" = 50' SHEET 1 OF 6 SHEETS STA. 134+00.00 TO STA. 149+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	12
CONTRACT NO. 62P58			ILLINOIS FED. AID PROJECT	



LEGEND:

- | | |
|---|---|
| ① THERMOPLASTIC PAVEMENT MARKING LINE 4" (WHITE EDGE LINES) | ⑪ POLYUREA PAVEMENT MARKING TYPE 1, LINE 4" (WHITE EDGE LINE) |
| ② THERMOPLASTIC PAVEMENT MARKING LINE 4" (YELLOW EDGE LINE) | ⑫ POLYUREA PAVEMENT MARKING TYPE 1, LINE 4" (YELLOW EDGE LINE) |
| ③ THERMOPLASTIC PAVEMENT MARKING LINE 8" (WHITE EDGE LINES) | ⑬ POLYUREA PAVEMENT MARKING TYPE 1, LINE 8" (WHITE EDGE LINE) |
| ④ THERMOPLASTIC PAVEMENT MARKING LINE 12" (WHITE DIAGONALS SPACED @ 500' CENTERS ON MAINLINE AND 150' CENTERS ON RAMP) | ⑭ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (YELLOW DIAGONALS SPACED @ 500' CENTERS AND 40' CENTERS ON STRUCTURES) |
| ⑤ THERMOPLASTIC PAVEMENT MARKING LINE 12" (YELLOW DIAGONALS SPACED @ 500' CENTERS ON MAINLINE AND 150' CENTERS ON RAMP) | ⑮ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (WHITE DIAGONALS SPACED @ 500' CENTERS AND 40' CENTERS ON STRUCTURES) |
| ⑥ THERMOPLASTIC PAVEMENT MARKING LINE 12" (WHITE GORE CHEVRONS SPACED @ 30' CENTERS) | ⑯ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (WHITE GORE CHEVRONS SPACED @ 30' CENTERS) |
| ⑦ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 4" (WHITE 2' DASH, 6' SKIP) | ⑰ POLYUREA PAVEMENT MARKING TYPE 1, LINE 8" (WHITE 3' DASH, 9' SKIP) |
| ⑧ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 5" (WHITE 10' DASH, 30' SKIP) | ⑱ REPLACEMENT REFLECTOR MARKER (1-WAY CRYSTAL/OPAQUE) |
| ⑨ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 8" (WHITE 3' DASH, 9' SKIP) | |
| ⑩ PREFORMED PLASTIC PAVEMENT MARKING, TYPE D, LINE 8" (WHITE 10' DASH, 30' SKIP) | |

NOTES

- SEE IDOT DISTRICT 1 STANDARD TC-12 "MULTILANE FREEWAY PAVEMENT MARKING DETAILS" FOR PLACEMENT AND TYPE OF RAISED REFLECTIVE PAVEMENT MARKERS.
- FOR PROPOSED PAVEMENT MARKINGS IN THE PCC SECTIONS USE POLYUREA PAVEMENT MARKINGS ACCORDING TO STANDARD TC-12.
- PAVEMENT MARKINGS (PREFORMED PLASTIC AND THERMOPLASTIC) SHALL BE GROOVED UNLESS PLACED ON THE BRIDGE DECK OR APPROACH SLAB.
- SHOULDER RUMBLE STRIPS SHALL BE PLACED IN LOCATIONS WHERE EXISTING RUMBLE STRIPS WERE REMOVED AND AS DETERMINED BY ENGINEER IN ACCORDANCE WITH STANDARD 642001.

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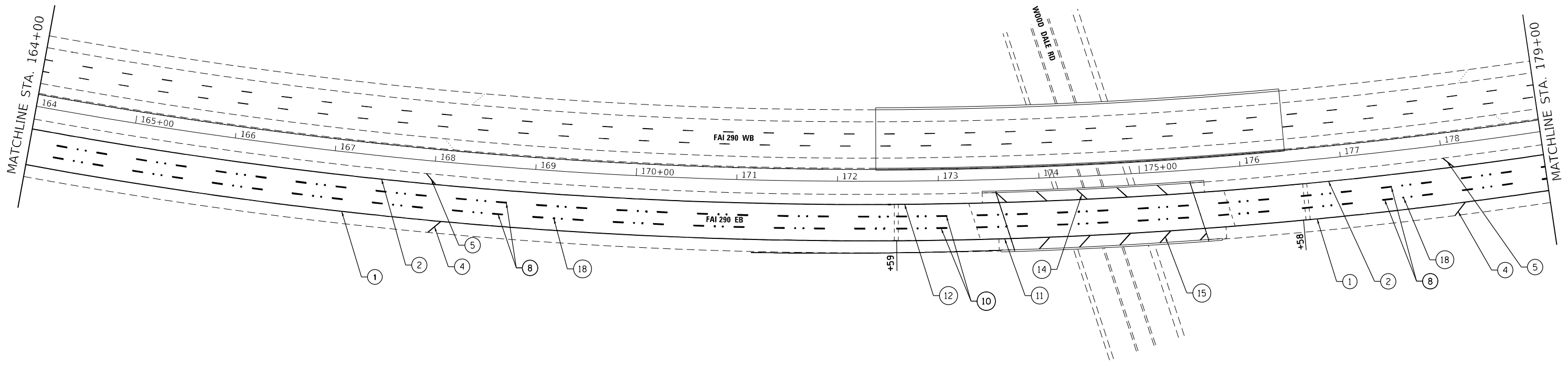
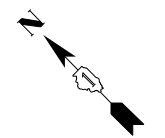
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PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED PAVEMENT MARKINGS

SCALE: 1" = 50' SHEET 2 OF 6 SHEETS STA. 149+00.00 TO STA. 164+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	13
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				



LEGEND:

- | | |
|--|---|
| ① THERMOPLASTIC PAVEMENT MARKING LINE 4" (WHITE EDGE LINES) | ⑪ POLYUREA PAVEMENT MARKING TYPE 1, LINE 4" (WHITE EDGE LINE) |
| ② THERMOPLASTIC PAVEMENT MARKING LINE 4" (YELLOW EDGE LINE) | ⑫ POLYUREA PAVEMENT MARKING TYPE 1, LINE 4" (YELLOW EDGE LINE) |
| ③ THERMOPLASTIC PAVEMENT MARKING LINE 8" (WHITE EDGE LINES) | ⑬ POLYUREA PAVEMENT MARKING TYPE 1, LINE 8" (WHITE EDGE LINE) |
| ④ THERMOPLASTIC PAVEMENT MARKING LINE 12" (WHITE DIAGONALS SPACED @ 500' CENTERS ON MAINLINE AND 150' CENTERS ON RAMPS) | ⑭ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (YELLOW DIAGONALS SPACED @ 500' CENTERS AND 40' CENTERS ON STRUCTURES) |
| ⑤ THERMOPLASTIC PAVEMENT MARKING LINE 12" (YELLOW DIAGONALS SPACED @ 500' CENTERS ON MAINLINE AND 150' CENTERS ON RAMPS) | ⑮ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (WHITE DIAGONALS SPACED @ 500' CENTERS AND 40' CENTERS ON STRUCTURES) |
| ⑥ THERMOPLASTIC PAVEMENT MARKING LINE 12" (WHITE GORE CHEVRONS SPACED @ 30' CENTERS) | ⑯ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (WHITE GORE CHEVRONS SPACED @ 30' CENTERS) |
| ⑦ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 4" (WHITE 2" DASH, 6' SKIP) | ⑰ POLYUREA PAVEMENT MARKING TYPE 1, LINE 8" (WHITE 3' DASH, 9' SKIP) |
| ⑧ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 5" (WHITE 10" DASH, 30' SKIP) | ⑱ REPLACEMENT REFLECTOR MARKER (1-WAY CRYSTAL/OPAQUE) |
| ⑨ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 8" (WHITE 3" DASH, 9' SKIP) | |
| ⑩ PREFORMED PLASTIC PAVEMENT MARKING, TYPE D, LINE 8" (WHITE 10" DASH, 30' SKIP) | |

NOTES

- SEE IDOT DISTRICT 1 STANDARD TC-12 "MULTILANE FREEWAY PAVEMENT MARKING DETAILS" FOR PLACEMENT AND TYPE OF RAISED REFLECTIVE PAVEMENT MARKERS.
- FOR PROPOSED PAVEMENT MARKINGS IN THE PCC SECTIONS USE POLYUREA PAVEMENT MARKINGS ACCORDING TO STANDARD TC-12.
- PAVEMENT MARKINGS (PREFORMED PLASTIC AND THERMOPLASTIC) SHALL BE GROOVED UNLESS PLACED ON THE BRIDGE DECK OR APPROACH SLAB.
- SHOULDER RUMBLE STRIPS SHALL BE PLACED IN LOCATIONS WHERE EXISTING RUMBLE STRIPS WERE REMOVED AND AS DETERMINED BY ENGINEER IN ACCORDANCE WITH STANDARD 642001.

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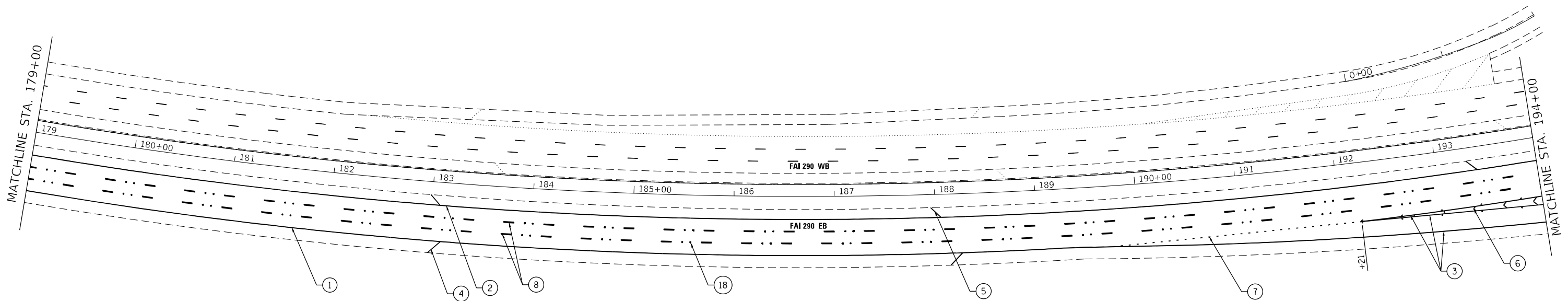
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PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED PAVEMENT MARKINGS

SCALE: 1" = 50' SHEET 3 OF 6 SHEETS STA. 164+00.00 TO STA. 179+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	14
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	



LEGEND:

- | | |
|---|---|
| ① THERMOPLASTIC PAVEMENT MARKING LINE 4" (WHITE EDGE LINES) | ⑪ POLYUREA PAVEMENT MARKING TYPE 1, LINE 4" (WHITE EDGE LINE) |
| ② THERMOPLASTIC PAVEMENT MARKING LINE 4" (YELLOW EDGE LINE) | ⑫ POLYUREA PAVEMENT MARKING TYPE 1, LINE 4" (YELLOW EDGE LINE) |
| ③ THERMOPLASTIC PAVEMENT MARKING LINE 8" (WHITE EDGE LINES) | ⑬ POLYUREA PAVEMENT MARKING TYPE 1, LINE 8" (WHITE EDGE LINE) |
| ④ THERMOPLASTIC PAVEMENT MARKING LINE 12" (WHITE DIAGONALS SPACED @ 500' CENTERS ON MAINLINE AND 150' CENTERS ON RAMP) | ⑭ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (YELLOW DIAGONALS SPACED @ 500' CENTERS AND 40' CENTERS ON STRUCTURES) |
| ⑤ THERMOPLASTIC PAVEMENT MARKING LINE 12" (YELLOW DIAGONALS SPACED @ 500' CENTERS ON MAINLINE AND 150' CENTERS ON RAMP) | ⑮ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (WHITE DIAGONALS SPACED @ 500' CENTERS AND 40' CENTERS ON STRUCTURES) |
| ⑥ THERMOPLASTIC PAVEMENT MARKING LINE 12" (WHITE GORE CHEVRONS SPACED @ 30' CENTERS) | ⑯ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (WHITE GORE CHEVRONS SPACED @ 30' CENTERS) |
| ⑦ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 4" (WHITE 2' DASH, 6' SKIP) | ⑰ POLYUREA PAVEMENT MARKING TYPE 1, LINE 8" (WHITE 3' DASH, 9' SKIP) |
| ⑧ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 5" (WHITE 10' DASH, 30' SKIP) | ⑱ REPLACEMENT REFLECTOR MARKER (1-WAY CRYSTAL/OPAQUE) |
| ⑨ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 8" (WHITE 3' DASH, 9' SKIP) | |
| ⑩ PREFORMED PLASTIC PAVEMENT MARKING, TYPE D, LINE 8" (WHITE 10' DASH, 30' SKIP) | |

NOTES

- SEE IDOT DISTRICT 1 STANDARD TC-12 "MULTILANE FREEWAY PAVEMENT MARKING DETAILS" FOR PLACEMENT AND TYPE OF RAISED REFLECTIVE PAVEMENT MARKERS.
- FOR PROPOSED PAVEMENT MARKINGS IN THE PCC SECTIONS USE POLYUREA PAVEMENT MARKINGS ACCORDING TO STANDARD TC-12.
- PAVEMENT MARKINGS (PREFORMED PLASTIC AND THERMOPLASTIC) SHALL BE GROOVED UNLESS PLACED ON THE BRIDGE DECK OR APPROACH SLAB.
- SHOULDER RUMBLE STRIPS SHALL BE PLACED IN LOCATIONS WHERE EXISTING RUMBLE STRIPS WERE REMOVED AND AS DETERMINED BY ENGINEER IN ACCORDANCE WITH STANDARD 642001.

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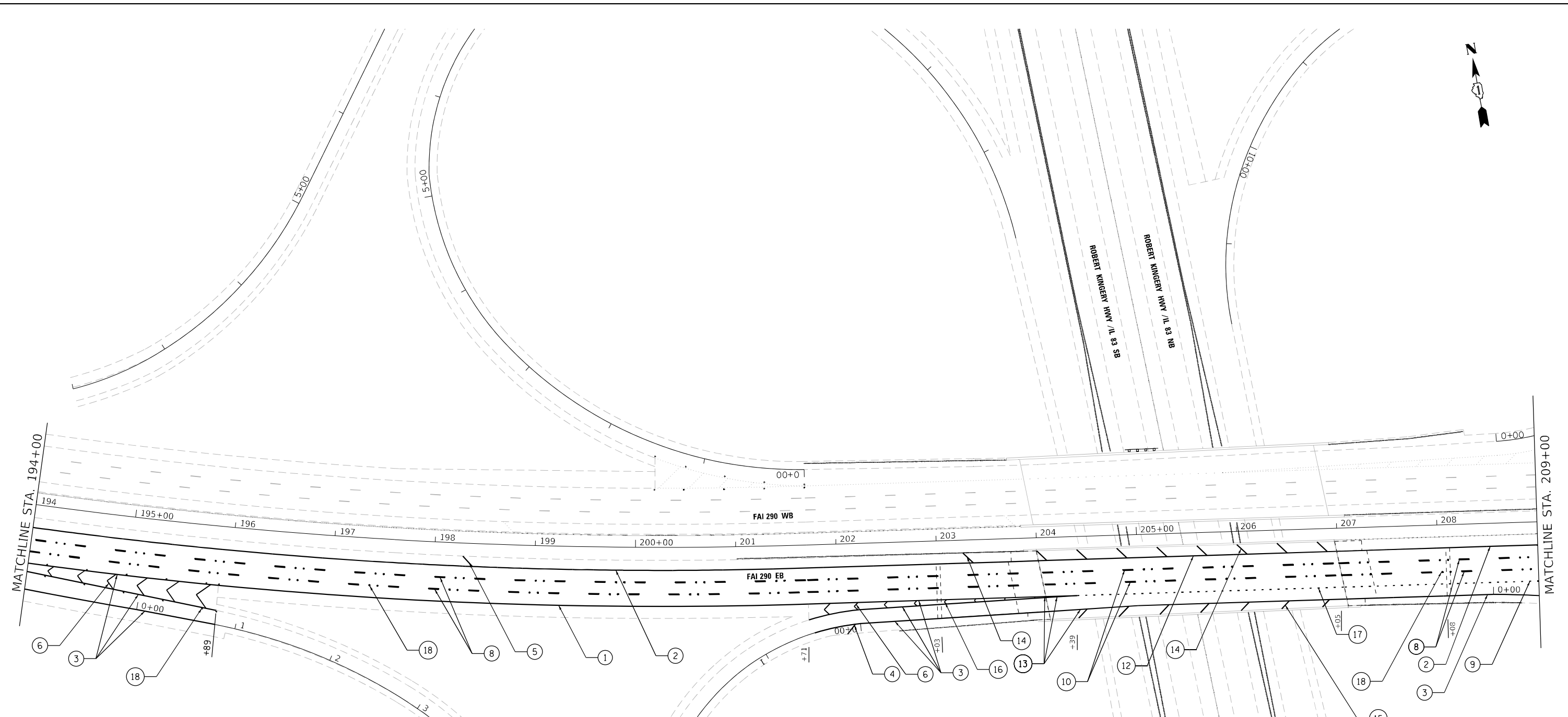
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PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED PAVEMENT MARKINGS

SCALE: 1" = 50' SHEET 4 OF 6 SHEETS STA. 179+00.00 TO STA. 194+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	15
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				



LEGEND:

- | | |
|--|---|
| ① THERMOPLASTIC PAVEMENT MARKING LINE 4" (WHITE EDGE LINES) | ⑪ POLYUREA PAVEMENT MARKING TYPE 1, LINE 4" (WHITE EDGE LINE) |
| ② THERMOPLASTIC PAVEMENT MARKING LINE 4" (YELLOW EDGE LINE) | ⑫ POLYUREA PAVEMENT MARKING TYPE 1, LINE 4" (YELLOW EDGE LINE) |
| ③ THERMOPLASTIC PAVEMENT MARKING LINE 8" (WHITE EDGE LINES) | ⑬ POLYUREA PAVEMENT MARKING TYPE 1, LINE 8" (WHITE EDGE LINE) |
| ④ THERMOPLASTIC PAVEMENT MARKING LINE 12" (WHITE DIAGONALS SPACED @ 500' CENTERS ON MAINLINE AND 150' CENTERS ON RAMPS) | ⑭ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (YELLOW DIAGONALS SPACED @ 500' CENTERS AND 40' CENTERS ON STRUCTURES) |
| ⑤ THERMOPLASTIC PAVEMENT MARKING LINE 12" (YELLOW DIAGONALS SPACED @ 500' CENTERS ON MAINLINE AND 150' CENTERS ON RAMPS) | ⑮ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (WHITE DIAGONALS SPACED @ 500' CENTERS AND 40' CENTERS ON STRUCTURES) |
| ⑥ THERMOPLASTIC PAVEMENT MARKING LINE 12" (WHITE GORE CHEVRONS SPACED @ 30' CENTERS) | ⑯ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (WHITE GORE CHEVRONS SPACED @ 30' CENTERS) |
| ⑦ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 4" (WHITE 2' DASH, 6' SKIP) | ⑰ POLYUREA PAVEMENT MARKING TYPE 1, LINE 8" (WHITE 3' DASH, 9' SKIP) |
| ⑧ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 5" (WHITE 10' DASH, 30' SKIP) | ⑱ REPLACEMENT REFLECTOR MARKER (1-WAY CRYSTAL/OPAQUE) |
| ⑨ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 8" (WHITE 3' DASH, 9' SKIP) | |
| ⑩ PREFORMED PLASTIC PAVEMENT MARKING, TYPE D, LINE 8" (WHITE 10' DASH, 30' SKIP) | |

NOTES

- SEE IDOT DISTRICT 1 STANDARD TC-12 "MULTILANE FREEWAY PAVEMENT MARKING DETAILS" FOR PLACEMENT AND TYPE OF RAISED REFLECTIVE PAVEMENT MARKERS.
- FOR PROPOSED PAVEMENT MARKINGS IN THE PCC SECTIONS USE POLYUREA PAVEMENT MARKINGS ACCORDING TO STANDARD TC-12.
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- SHOULDER RUMBLE STRIPS SHALL BE PLACED IN LOCATIONS WHERE EXISTING RUMBLE STRIPS WERE REMOVED AND AS DETERMINED BY ENGINEER IN ACCORDANCE WITH STANDARD 642001.

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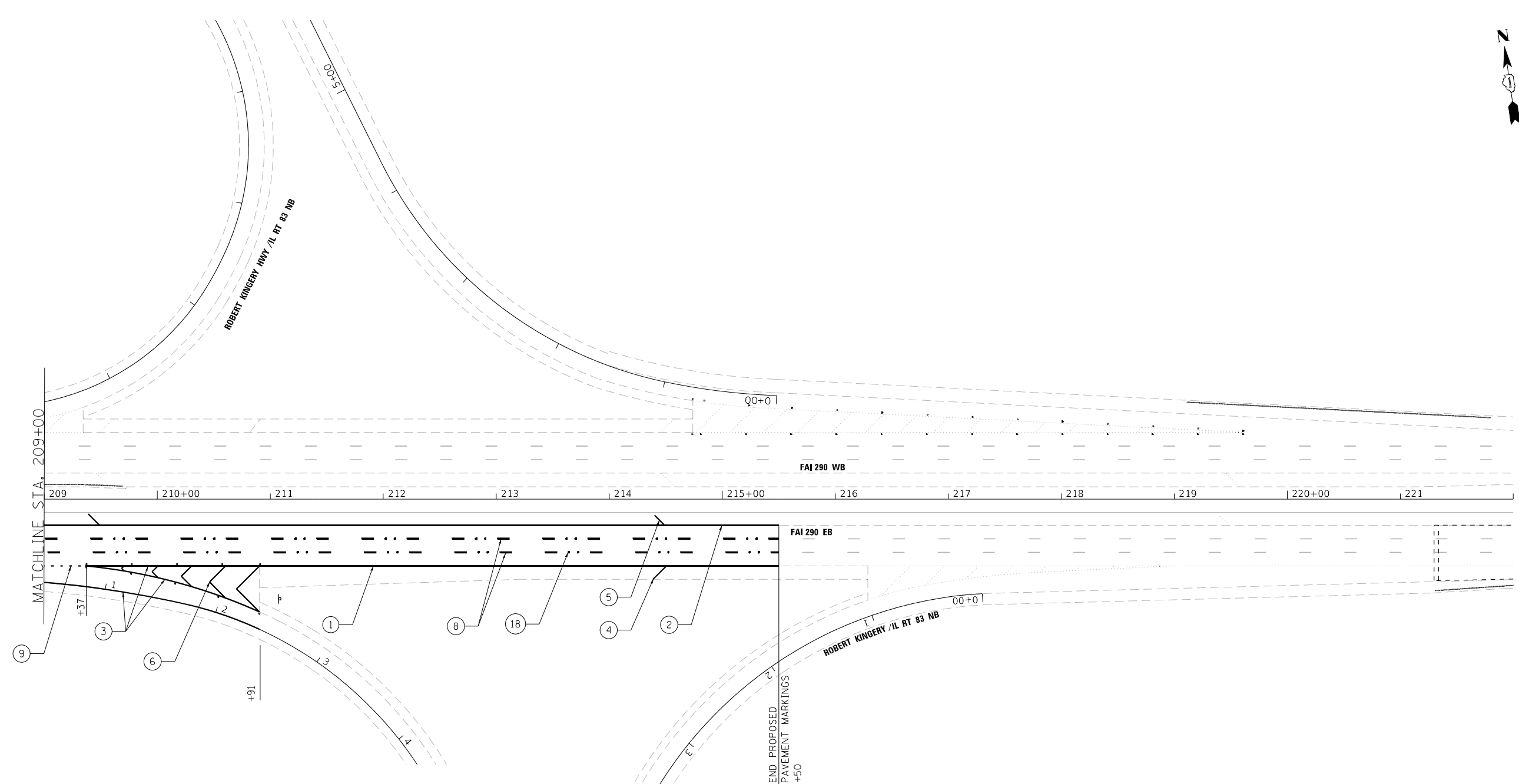
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PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED PAVEMENT MARKING

SCALE: 1" = 50' SHEET 5 OF 6 SHEETS STA. 209+00.00 TO STA. 194+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	16
CONTRACT NO. 62P58			ILLINOIS FED. AID PROJECT	



LEGEND:

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| <ul style="list-style-type: none"> ① THERMOPLASTIC PAVEMENT MARKING LINE 4" (WHITE EDGE LINES) ② THERMOPLASTIC PAVEMENT MARKING LINE 4" (YELLOW EDGE LINE) ③ THERMOPLASTIC PAVEMENT MARKING LINE 8" (WHITE EDGE LINES) ④ THERMOPLASTIC PAVEMENT MARKING LINE 12" (WHITE DIAGONALS SPACED @ 500' CENTERS ON MAINLINE AND 150' CENTERS ON RAMP) ⑤ THERMOPLASTIC PAVEMENT MARKING LINE 12" (YELLOW DIAGONALS SPACED @ 500' CENTERS ON MAINLINE AND 150' CENTERS ON RAMP) ⑥ THERMOPLASTIC PAVEMENT MARKING LINE 12" (WHITE GORE CHEVRONS SPACED @ 30' CENTERS) ⑦ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 4" (WHITE 2' DASH, 6' SKIP) ⑧ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 5" (WHITE 10' DASH, 30' SKIP) ⑨ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, STANDARD, LINE 8" (WHITE 3' DASH, 9' SKIP) ⑩ PREFORMED PLASTIC PAVEMENT MARKING, TYPE D, LINE 8" (WHITE 10' DASH, 30' SKIP) | <ul style="list-style-type: none"> ⑪ POLYUREA PAVEMENT MARKING TYPE 1, LINE 4" (WHITE EDGE LINE) ⑫ POLYUREA PAVEMENT MARKING TYPE 1, LINE 4" (YELLOW EDGE LINE) ⑬ POLYUREA PAVEMENT MARKING TYPE 1, LINE 8" (WHITE EDGE LINE) ⑭ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (YELLOW DIAGONALS SPACED @ 500' CENTERS AND 40' CENTERS ON STRUCTURES) ⑮ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (WHITE DIAGONALS SPACED @ 500' CENTERS AND 40' CENTERS ON STRUCTURES) ⑯ POLYUREA PAVEMENT MARKING TYPE 1, LINE 12" (WHITE GORE CHEVRONS SPACED @ 30' CENTERS) ⑰ POLYUREA PAVEMENT MARKING TYPE 1, LINE 8" (WHITE 3' DASH, 9' SKIP) ⑱ REPLACEMENT REFLECTOR MARKER (1-WAY CRYSTAL/OPAQUE) |
|--|---|

NOTES

1. SEE IDOT DISTRICT 1 STANDARD TC-12 "MULTILANE FREEWAY PAVEMENT MARKING DETAILS" FOR PLACEMENT AND TYPE OF RAISED REFLECTIVE PAVEMENT MARKERS.
2. FOR PROPOSED PAVEMENT MARKINGS IN THE PCC SECTIONS USE POLYUREA PAVEMENT MARKINGS ACCORDING TO STANDARD TC-12.
3. PAVEMENT MARKINGS (PREFORMED PLASTIC AND THERMOPLASTIC) SHALL BE GROOVED UNLESS PLACED ON THE BRIDGE DECK OR APPROACH SLAB.
4. SHOULDER RUMBLE STRIPS SHALL BE PLACED IN LOCATIONS WHERE EXISTING RUMBLE STRIPS WERE REMOVED AND AS DETERMINED BY ENGINEER IN ACCORDANCE WITH STANDARD 642001.

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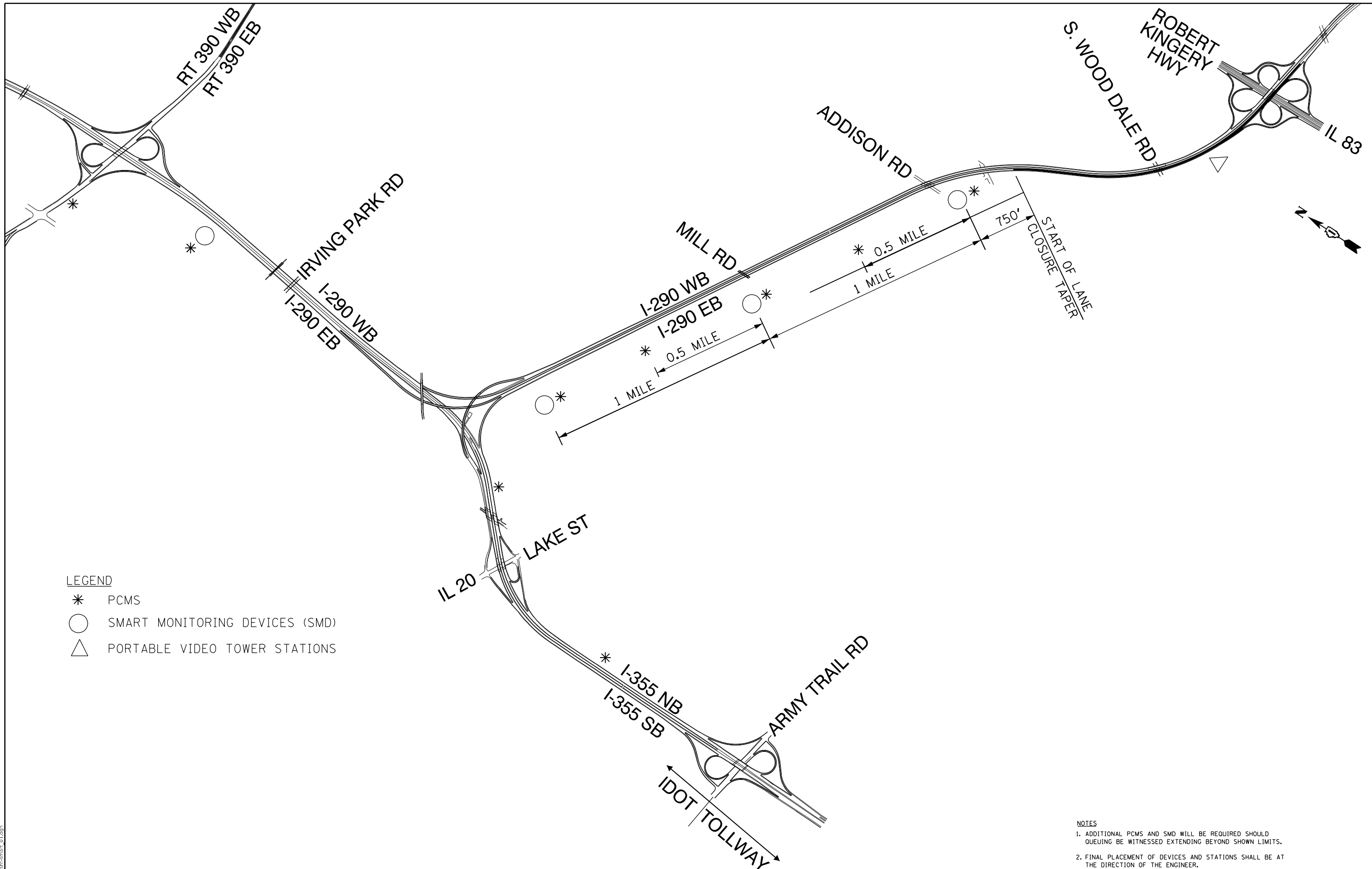
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PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED PAVEMENT MARKING PLAN

SCALE: 1" = 50' SHEET 6 OF 6 SHEETS STA. 209+00.00 TO STA. 222+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	17
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				



LEGEND

- * PCMS
- SMART MONITORING DEVICES (SMD)
- △ PORTABLE VIDEO TOWER STATIONS

NOTES

1. ADDITIONAL PCMS AND SMD WILL BE REQUIRED SHOULD QUEUING BE WITNESSED EXTENDING BEYOND SHOWN LIMITS.
2. FINAL PLACEMENT OF DEVICES AND STATIONS SHALL BE AT THE DIRECTION OF THE ENGINEER.

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PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SMART TRAFFIC MONITORING SYSTEM
TYPICAL LAYOUT**

SCALE: 1"=1000' SHEET OF SHEETS STA. TO STA.

F.A. I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	18
CONTRACT NO. 62P58			ILLINOIS FED. AID PROJECT	

MAINTENANCE OF TRAFFIC GENERAL NOTES

- ALL LANES (11' MINIMUM) SHALL BE KEPT OPEN TO TRAFFIC DURING ALL HOURS EXCEPT HOURS AS SPECIFIED IN THE KEEPING THE EXPRESSWAYS OPEN TO TRAFFIC SPECIAL PROVISIONS.
- WHERE WORK AREAS ARE NOT SEPARATED FROM TRAFFIC BY TEMPORARY CONCRETE BARRIER WALL, WORK ON BOTH SIDES OF THE ROADWAY IN THE SAME DIRECTION, DURING THE SAME TIME PERIOD WILL NOT BE ALLOWED.
- ALL TRAFFIC CONTROL SIGNS AND DEVICES, WORK ZONE PAVEMENT MARKING, TEMPORARY CONCRETE BARRIERS AND ATTENUATORS NECESSARY FOR THE MAINTENANCE OF TRAFFIC, AS SHOWN ON THE PLANS, SHALL BE IN PLACE BEFORE THE START OF ANY CONSTRUCTION STAGE.
- ALL CONSTRUCTION WARNING SIGNS SHALL BE BLACK LEGEND ON ORANGE BACKGROUND. ALL CONSTRUCTION WARNING SIGNS SHALL BE 48" X 48".
- DROP OFF (BETWEEN TRAVEL LANES) ALONG ARTERIAL OR LOCAL ROADWAYS WITHIN THE PROJECT LIMITS THAT ARE GREATER THAN 1" AND LESS THAN OR EQUAL TO 2 1/2" LIFT DIFFERENCE OR GREATER THAN 1 1/2" AND LESS THAN OR EQUAL TO 4" VERTICAL MILLED FACE SHALL REQUIRE UNEVEN LANE SIGNS PLACED ONE HALF MILE APART AS DIRECTED BY THE ENGINEER.
- BARRIER WALL MARKERS SHALL BE INSTALLED ON BOTH THE FACE OF THE TEMPORARY CONCRETE BARRIER WALL NEXT TO TRAFFIC, AND THE TOP OF SECTIONS OF THE TEMPORARY CONCRETE BARRIER WALL AS SHOWN IN STANDARD 704001. THE COLOR OF THESE REFLECTORS SHALL MATCH THE COLOR OF THE EDGELINES (YELLOW ON THE LEFT AND CRYSTAL OR WHITE ON THE RIGHT). IF THE BASE OF THE TEMPORARY CONCRETE BARRIER WALL IS 12 INCHES OR LESS FROM THE TRAVEL LANE, THEN THE LOWER SLOPE OF THE WALL SHALL ALSO HAVE A 6 INCH WIDE TEMPORARY PAVEMENT MARKING EDGELINE (YELLOW ON THE LEFT AND WHITE ON THE RIGHT). TEMPORARY PAVEMENT MARKING ON THE LOWER SLOPE OF THE TEMPORARY CONCRETE BARRIER WALL WILL BE MEASURED AND PAID FOR AS TEMPORARY PAVEMENT MARKING - LINE 6" PAINT AS SHOWN ON THE PLANS.
- ALL "ROAD CONSTRUCTION AHEAD" WARNING SIGNS SHALL BE EQUIPPED WITH TYPE A MONO-DIRECTIONAL FLASHING LIGHT.
- THE CONTRACTOR SHALL PLACE CHANGEABLE MESSAGE SIGN(S) TO INFORM MOTORISTS OF UPCOMING CONSTRUCTION ACTIVITIES. THE LOCATION SHALL BE AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER. THE MESSAGE SIGN SHALL BE IN PLACE TWO WEEKS PRIOR TO START OF CONSTRUCTION ACTIVITIES. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR DAYS FOR "CHANGEABLE MESSAGE SIGN."
- THE WORK REQUIRED TO COMPLY WITH THE MAINTENANCE OF TRAFFIC GENERAL NOTES SHALL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS), EXCEPT FOR PAY ITEMS WHICH HAVE BEEN ESTABLISHED IN THE CONTRACT AND ARE SHOWN ON THE PLANS.
- EXISTING PAVEMENT MARKINGS AND RAISED REFLECTIVE MARKERS THAT CONFLICT WITH TEMPORARY MARKINGS SHALL BE REMOVED. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE REPLACED DURING THE PLACEMENT OF THE FINAL PAVEMENT MARKINGS.
- WORKERS SIGN SHALL BE USED ONLY WHEN WORKERS ARE PRESENT. FLAGGER SIGN SHALL BE USED ONLY WHEN FLAGGERS ARE PRESENT.
- THE APPROVAL OF THE ENGINEER SHALL BE REQUIRED FOR ALL TEMPORARY TRAFFIC BARRIER WALL OPENINGS NOT SHOWN ON THE PLANS. THE APPROACH END OF TEMPORARY TRAFFIC BARRIER SHALL BE PROTECTED WITH AN NCHRP 350 APPROVED END TREATMENT. SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS SHALL REQUIRE ADHERANCE TO DISTRICT 1 DETAIL TC-18. THE WORK REQUIRED TO PROVIDE ADDITIONAL TEMPORARY TRAFFIC BARRIER OPENINGS FOR CONTRACTOR'S ACCESS TO WORK AREAS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR TRAFFIC CONTROL AND PROTECTION (EXPRESSWAY).
- THE CONTRACTOR SHALL REQUEST AND GAIN APPROVAL FROM THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S EXPRESSWAY TRAFFIC OPERATIONS ENGINEER AT WWW.IDOTLCS.COM TWENTY-FOUR (24) HOURS IN ADVANCE OF ALL DAILY LANE, RAMP AND SHOULDER CLOSURES AND 7 DAYS IN ADVANCE OF ALL PERMANENT AND WEEKEND CLOSURES ON ALL FREEWAYS AND/OR EXPRESSWAYS IN DISTRICT ONE.
- UNLESS SPECIFIED IN THE CONTRACT PLANS, ALL TRAFFIC LANES SHALL BE KEPT OPEN TO TRAFFIC DURING PEAK TRAFFIC VOLUME HOURS AS DEFINED IN THE CONTRACT SPECIAL PROVISION FOR KEEPING THE EXPRESSWAY OPEN TO TRAFFIC.
- THE CONTRACTOR WILL BE ALLOWED TO IMPLEMENT TEMPORARY LANE CLOSURES, TEMPORARY SHOULDER CLOSURES, TEMPORARY RAMP CLOSURES (FULL OR PARTIAL) ACCORDING TO THE APPLICABLE IDOT HIGHWAY STANDARDS AND DISTRICT ONE DETAILS, AND PER THE ALLOWABLE HOURS IN THE CONTRACT SPECIAL PROVISION FOR KEEPING THE EXPRESSWAY OPEN TO TRAFFIC. TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY LANE AND SHOULDER CLOSURES SHALL BE IN ACCORDANCE WITH THE APPLICABLE HIGHWAY STANDARD OR DISTRICT DETAIL FOR THE PROPOSED WORK.
- A MAINTENANCE OF TRAFFIC PLAN SHALL BE SUBMITTED TO THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISORS 14 DAYS IN ADVANCE OF ANY STAGES OR FULL EXPRESSWAY CLOSURES. THE MAINTNANCE OF TRAFFIC PLAN SHALL INCLUDE, BUT NOT LIMITED TO, LANE AND RAMP CLOSURES, EXISTING GEOMETRICS, AND EQUIPMENT AND MATERIAL LOCATION.

SUGGESTED STAGING AND TRAFFIC CONTROL

PRE STAGE

EB I-290

TRAFFIC CONTROL:
HIGHWAY STANDARDS 701400-11, 701401-13, AND TC-17 SHALL BE UTILIZED FOR RUMBLE STRIP REMOVAL ON SHOULDERS. CLOSURES SHALL BE LIMITED TO THE HOURS SHOWN IN KEEPING THE EXPRESSWAYS OPEN TO TRAFFIC SPECIAL PROVISION.

CONSTRUCTION:

COMPLETE RUMBLE STRIPS REMOVAL FOR THE LOCATIONS SHOWN ON THE STAGE 1 MAINTENANCE OF TRAFFIC PLANS. THE ENGINEER SHALL FIELD VERIFY THE SHOULDER CONDITION AND DETERMINE THE LOCATION OF CLASS D PATCHES, TYPE IV, 14 INCH (SPECIAL) AS NECESSARY. NECESSARY PATCHING SHALL BE COMPLETE PRIOR TO SHIFTING TRAFFIC ONTO THE SHOULDERS.

STAGE 1

INSTALL TEMPORARY SIGNING, STRIPING AND OTHER TRAFFIC CONTROL DEVICES NECESSARY FOR THE STAGE 1 TRAFFIC CONFIGURATION, INCLUDING ANY NECESSARY MODIFICATIONS TO THE EXISTING GROUND MOUNTED AND OVERHEAD SIGNS.

EB I-290

TRAFFIC CONTROL:
1. TRAFFIC WILL BE REDUCED TO TWO EASTBOUND LANES.
2. TWO EASTBOUND LANES WILL BE MAINTAINED THROUGHOUT THE CONTRACT LIMITS WITH SHIFTS TO THE OUTSIDE SHOULDER OCCURRING AT THE BRIDGE STRUCTURES TO ACCOMMODATE THE WORK ZONES.

WOOD DALE RD

TRAFFIC CONTROL:
1. THE RIGHT LANE IN EACH DIRECTION OF WOOD DALE RD SHALL BE CLOSED UTILIZING HIGHWAY STANDARD 701601-09.
2. THE SIDEWALK ON THE NORTHBOUND SIDE OF WOOD DALE RD SHALL BE DIVERTED IN ACCORDANCE WITH HIGHWAY STANDARD 701801-06 AND THE SPECIAL PROVISION TEMPORARY SIDEWALK RAMP.

CONSTRUCTION

WOOD DALE RD

- REPAIR BRIDGE DECK.
- RECONSTRUCT DECK JOINTS AT EACH ABUTMENT WITH PREFORMED JOINT STRIP SEAL.
- RECONSTRUCT DECK JOINT AT PIN CONNECTIONS.

IL 83

- RECONSTRUCT THE EAST ABUTMENT BACKWALL.
- REMOVE AND REPLACE EAST APPROACH SLAB.
- RECONSTRUCT THE PREFORMED JOINT STRIP SEAL DECK JOINTS AT EACH ABUTMENT.
- APPLY CONCRETE SEALER TO DECK SURFACE AND PARAPETS AND NEWLY CONSTRUCTED PORTION OF APPROACH SLAB AND BACKWALL.
- PLACE RUMBLE STRIPS ON SHOULDERS.

STAGE 2

INSTALL TEMPORARY SIGNING, STRIPING AND OTHER TRAFFIC CONTROL DEVICES NECESSARY FOR THE STAGE 2 TRAFFIC CONFIGURATION, INCLUDING ANY NECESSARY MODIFICATIONS TO THE EXISTING GROUND MOUNTED AND OVERHEAD SIGNS.

EB I-290

TRAFFIC CONTROL:
1. TRAFFIC WILL REMAIN AS TWO EASTBOUND LANES.
2. TWO EASTBOUND LANES WILL BE MAINTAINED WITH SHIFTS TO THE INSIDE SHOULDER OCCURRING AT THE BRIDGE STRUCTURES TO ACCOMMODATE THE WORK ZONES. A THIRD LANE IS ADDED PRIOR TO THE IL 83 EXIT RAMP.

WOOD DALE RD

TRAFFIC CONTROL:
1. THE RIGHT LANE IN EACH DIRECTION OF WOOD DALE RD SHALL BE CLOSED UTILIZING HIGHWAY STANDARD 701601-09.
2. THE SIDEWALK ON THE NORTHBOUND SIDE OF WOOD DALE RD SHALL BE DIVERTED IN ACCORDANCE WITH HIGHWAY STANDARD 701801-06 AND THE SPECIAL PROVISION TEMPORARY SIDEWALK RAMP.

CONSTRUCTION

WOOD DALE RD

- REPAIR BRIDGE DECK.
- RECONSTRUCT DECK JOINTS AT EACH ABUTMENT WITH PREFORMED JOINT STRIP SEAL.
- RECONSTRUCT DECK JOINT AT PIN CONNECTIONS.

IL 83

- RECONSTRUCT THE EAST ABUTMENT BACKWALL.
- REMOVE AND REPLACE EAST APPROACH SLAB.
- RECONSTRUCT THE PREFORMED JOINT STRIP SEAL DECK JOINTS AT EACH ABUTMENT.
- APPLY CONCRETE SEALER TO DECK SURFACE AND PARAPETS AND NEWLY CONSTRUCTED PORTION OF APPROACH SLAB AND BACKWALL.

STAGE 3

INSTALL TEMPORARY SIGNING, STRIPING AND OTHER TRAFFIC CONTROL DEVICES NECESSARY FOR THE STAGE 3 TRAFFIC CONFIGURATION, INCLUDING ANY NECESSARY MODIFICATIONS TO THE EXISTING GROUND MOUNTED AND OVERHEAD SIGNS.

EB I-290

TRAFFIC CONTROL:
1. TRAFFIC WILL REMAIN AS TWO EASTBOUND LANES.
2. TWO EASTBOUND LANES WILL BE MAINTAINED THROUGHOUT THE CONTRACT LIMITS WITH SHIFTS TO THE INSIDE SHOULDER OCCURRING AT THE WOOD DALE ROAD BRIDGE TO ACCOMMODATE THE WORK ZONES.

CONSTRUCTION

IL 83

- RECONSTRUCT THE EAST ABUTMENT BACKWALL.
- REMOVE AND REPLACE EAST APPROACH SLAB.
- RECONSTRUCT THE PREFORMED JOINT STRIP SEAL DECK JOINTS AT EACH ABUTMENT.
- APPLY CONCRETE SEALER TO DECK SURFACE AND PARAPETS AND NEWLY CONSTRUCTED PORTION OF APPROACH SLAB AND BACKWALL.
- PLACE RUMBLE STRIPS ON SHOULDERS.
- PLACE PERMANENT PAVEMENT MARKINGS.

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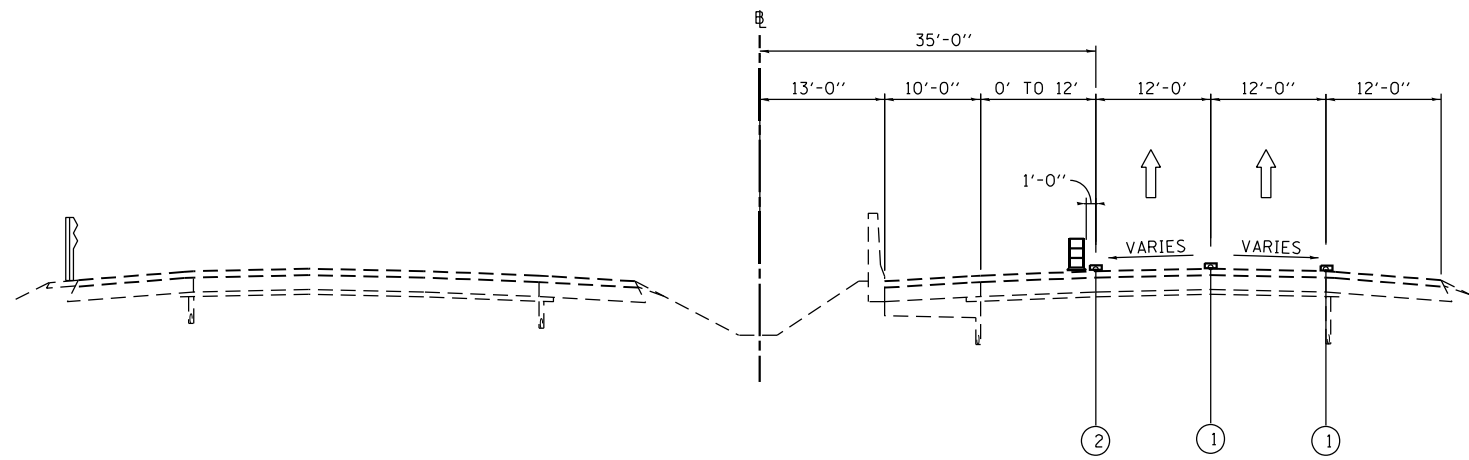
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PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFICE
GENERAL NOTES AND SEQUENCE OF STAGING**

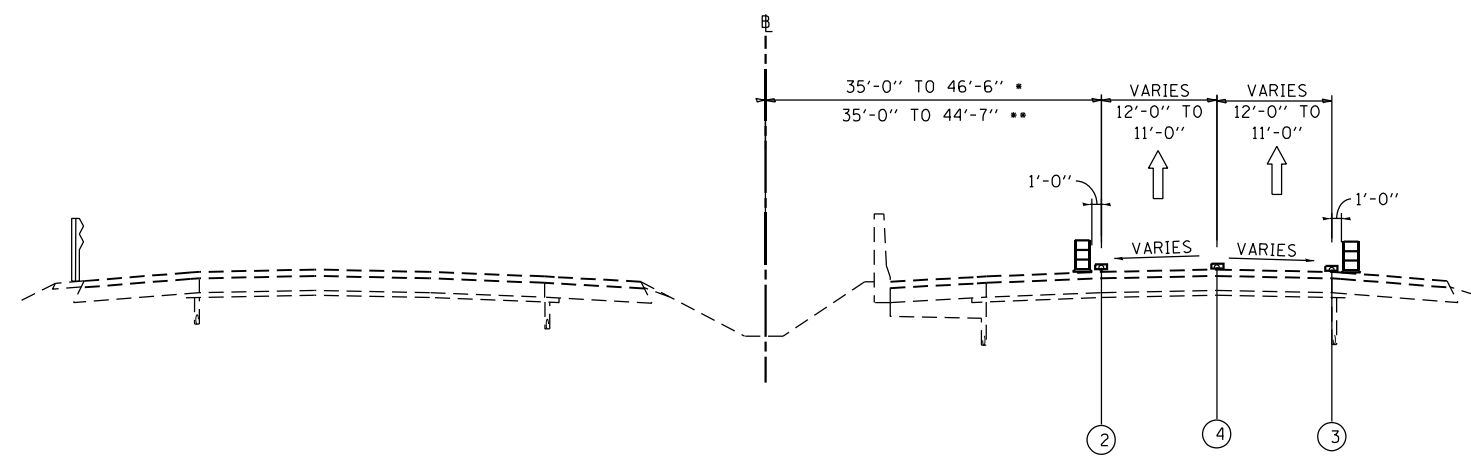
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				



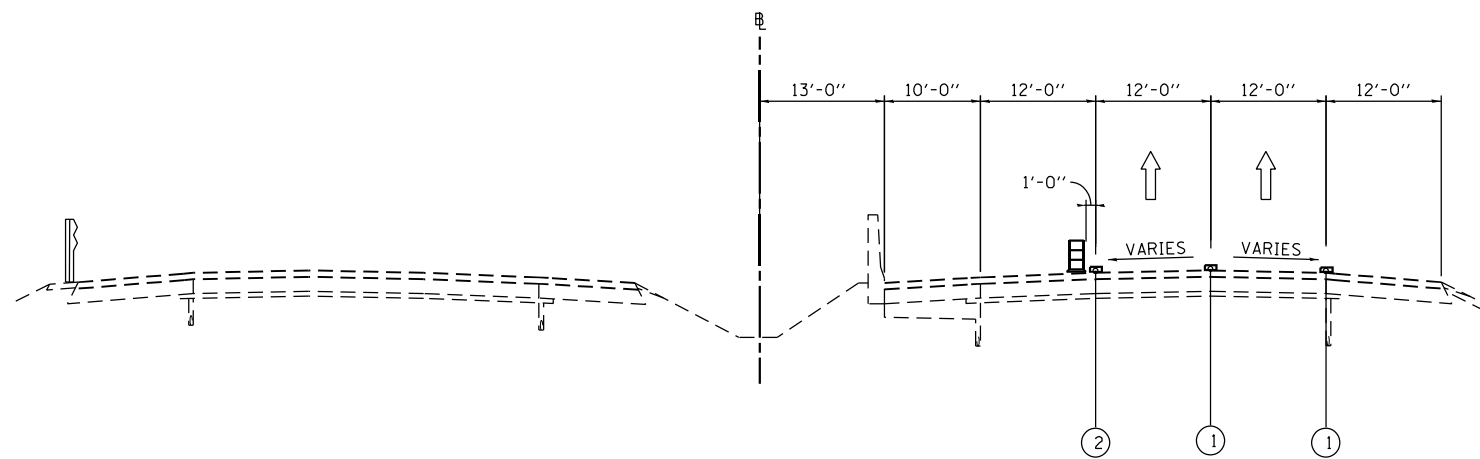
**I-290 MAINTENANCE OF TRAFFIC
TYPICAL SECTION
STAGE 1**

EB 290
STA 141+55 TO STA 151+55
STA 214+32 TO STA 215+50



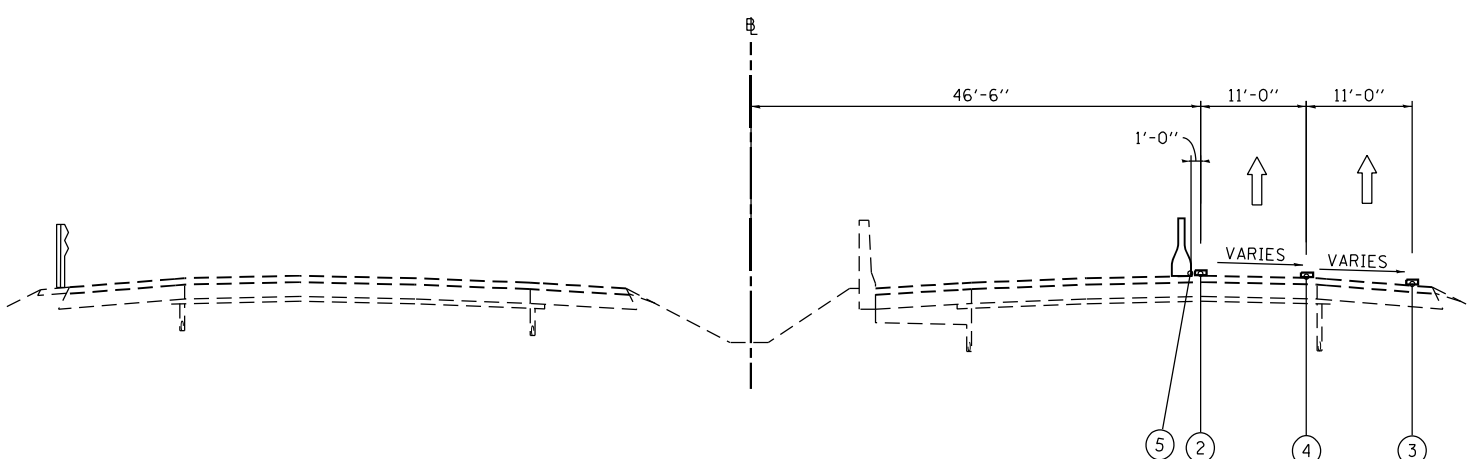
**I-290 MAINTENANCE OF TRAFFIC
TYPICAL SECTION
STAGE 1**

EB 290
STA 163+96 TO STA 169+10 •
STA 210+90 TO STA 214+32 ••



**I-290 MAINTENANCE OF TRAFFIC
TYPICAL SECTION
STAGE 1**

EB 290
STA 151+55 TO STA 163+96



**I-290 MAINTENANCE OF TRAFFIC
TYPICAL SECTION
STAGE 1**

EB 290
STA 169+10 TO STA 180+00
BRIDGE OMISSION STA 172+56 TO STA 176+65

LEGEND

- ① EXISTING PAVEMENT MARKING
- ② WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 4 INCH (YELLOW)
- ③ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 4 INCH (WHITE)
- ④ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 5 INCH (WHITE)
- ⑤ TEMPORARY PAVEMENT MARKING-LINE 6"-PAINT (YELLOW) (ON BARRIER)
- ⑥ TEMPORARY PAVEMENT MARKING-LINE 6"-PAINT (WHITE) (ON BARRIER)
- ⑦ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 8 INCH (WHITE)
- TEMPORARY CONCRETE BARRIER
- TYPE II BARRIER, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT

NOTE

SEE MAINTENANCE OF TRAFFIC STAGING PLANS FOR LIMITS OF TEMPORARY CONCRETE BARRIER.

MODEL: D:\p1\101422-210-1\101422-210-1.dgn



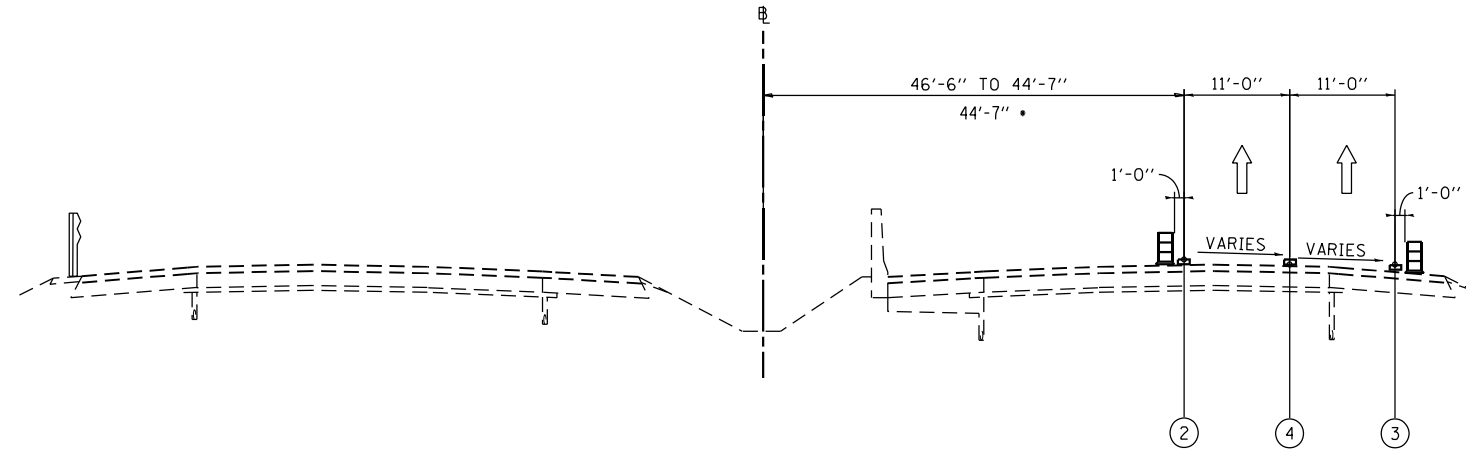
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	DRAWN - AMB	REVISED -
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PLOT DATE = 3/18/2022	DATE - 02/11/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
TYPICAL SECTION - STAGE 1**

SCALE: N.T.S. SHEET 1 OF 2 SHEETS STA. TO STA.

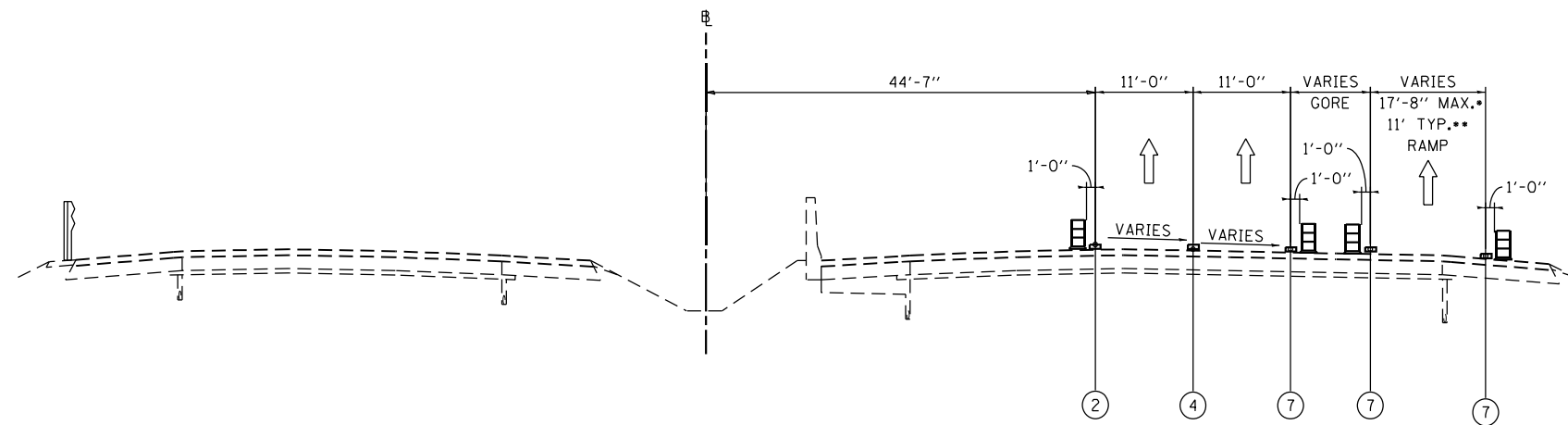
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	20
			CONTRACT NO. 62P58	
ILLINOIS FED. AID PROJECT				



**I-290 MAINTENANCE OF TRAFFIC
TYPICAL SECTION
STAGE 1**

EB 290

STA 180+00 TO 180+85
 STA 180+85 TO STA 190+80 •
 STA 195+88 TO STA 201+55 •



**I-290 MAINTENANCE OF TRAFFIC
TYPICAL SECTION
STAGE 1**

EB 290

STA 190+80 TO 195+88 •
 STA 201+55 TO 210+90 ••
 BRIDGE OMISSION STA 203+00 TO STA 208+12

LEGEND

- ① EXISTING PAVEMENT MARKING
- ② WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 4 INCH (YELLOW)
- ③ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 4 INCH (WHITE)
- ④ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 5 INCH (WHITE)
- ⑤ TEMPORARY PAVEMENT MARKING-LINE 6"-PAINT (YELLOW) (ON BARRIER)
- ⑥ TEMPORARY PAVMENT MARKING-LINE 6"-PAINT (WHITE) (ON BARRIER)
- ⑦ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 8 INCH (WHITE)
- ☐ TEMPORARY CONCRETE BARRIER
- ☐ TYPE II BARRIER, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT

NOTE

SEE MAINTENANCE OF TRAFFIC STAGING PLANS FOR LIMITS OF TEMPORARY CONCRETE BARRIER.

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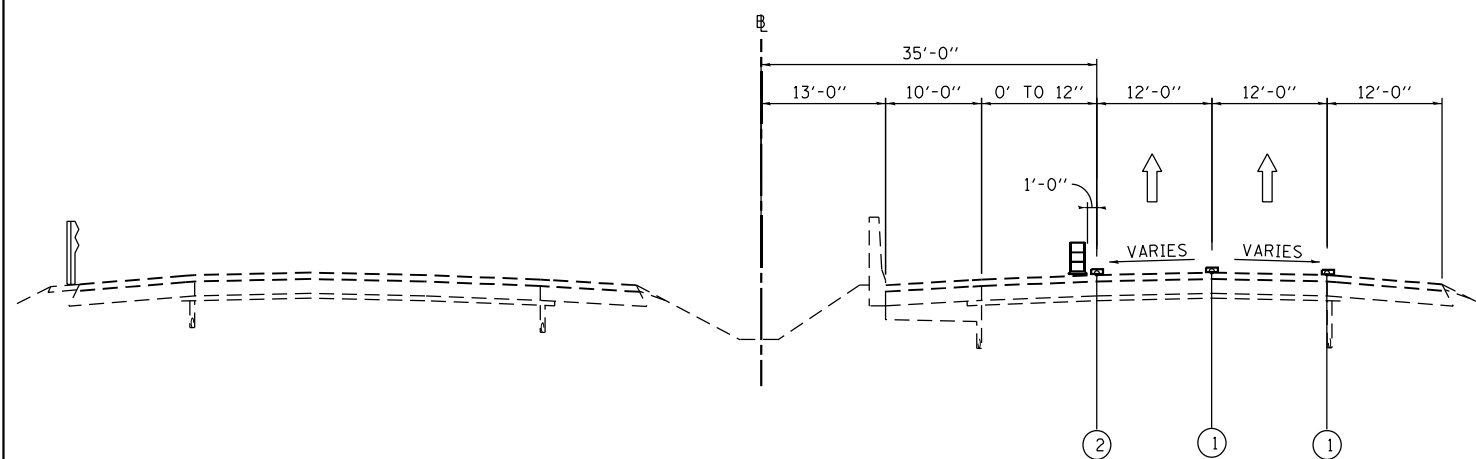
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PLOT SCALE = 20,0018' / in.	CHECKED - JJT	REVISED -
PLOT DATE = 3/18/2022	DATE - 02/18/2021	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

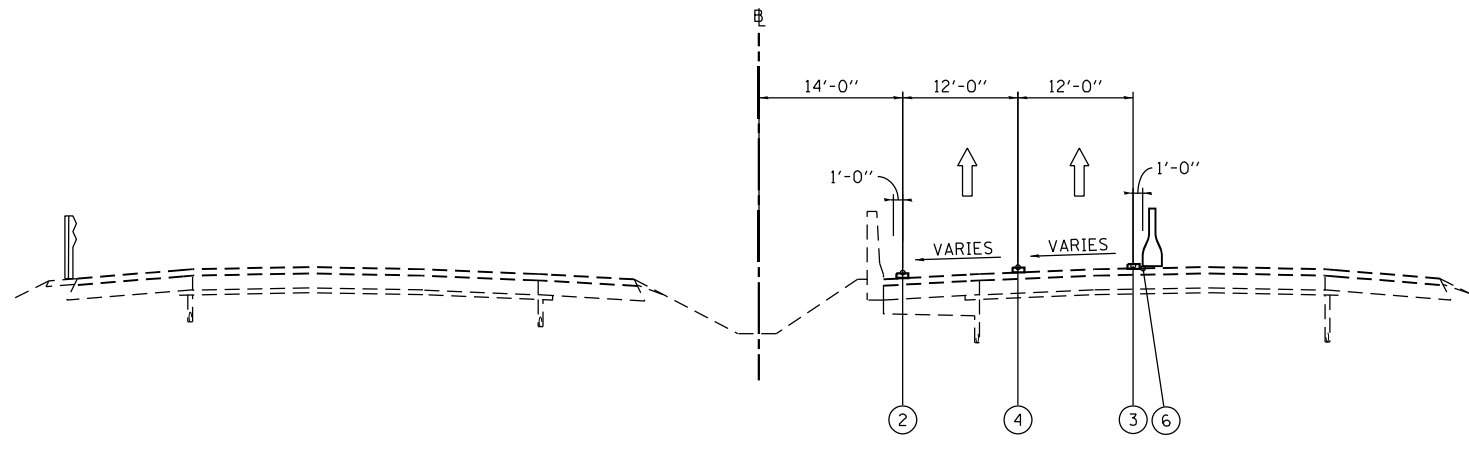
**MAINTENANCE OF TRAFFIC
TYPICAL SECTION - STAGE 1**

SCALE: N.T.S. SHEET 2 OF 2 SHEETS STA. TO STA.

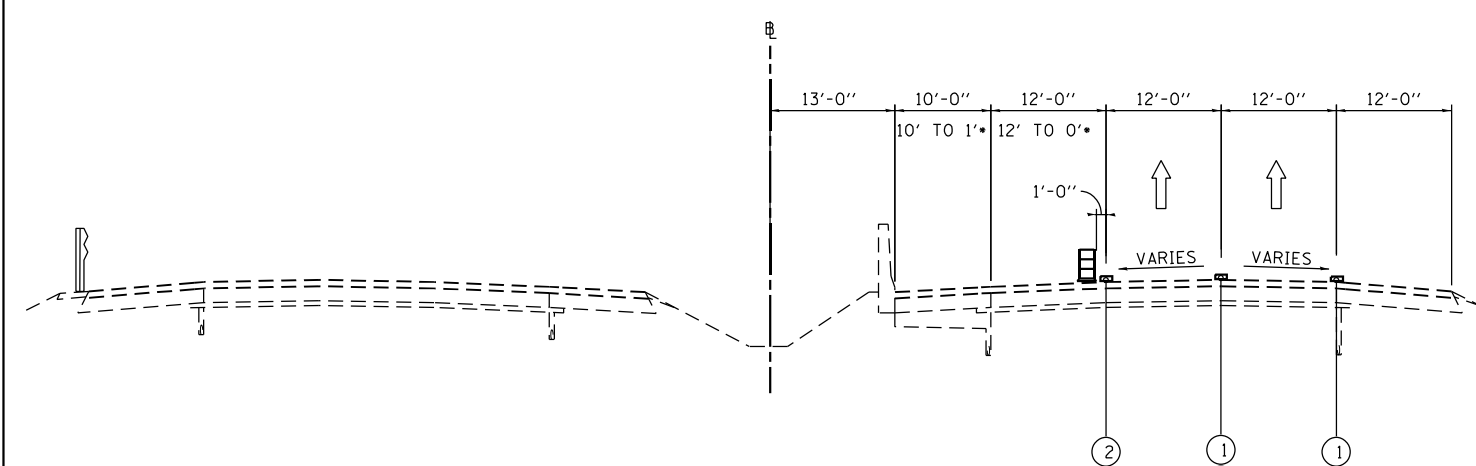
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	21
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	



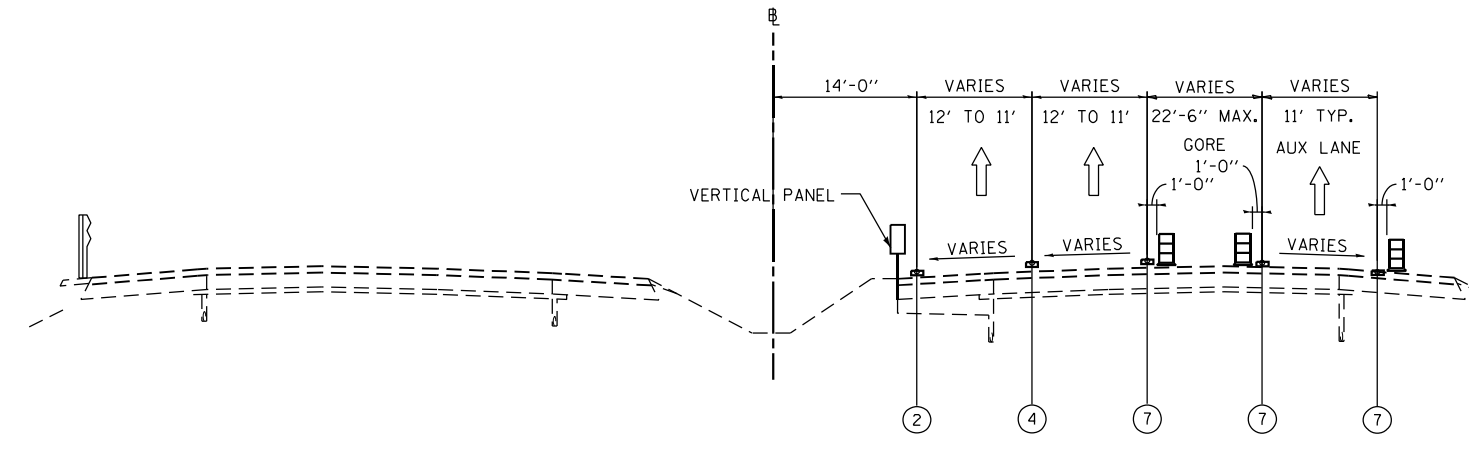
STAGE 2
I-290 MAINTENANCE OF TRAFFIC
TYPICAL SECTION
EB 290
STA 141+55 TO STA 151+55



STAGE 2
I-290 MAINTENANCE OF TRAFFIC
TYPICAL SECTION
EB 290
STA 171+00 TO STA 177+60
BRIDGE OMISSION
STA 172+56 TO STA 176+65



STAGE 2
I-290 MAINTENANCE OF TRAFFIC
TYPICAL SECTION
EB 290
STA 151+55 TO STA 161+55
STA 161+55 TO STA 171+00 •



STAGE 2
I-290 MAINTENANCE OF TRAFFIC
TYPICAL SECTION
EB 290
STA 177+60 TO STA 191+50

LEGEND

- ① EXISTING PAVEMENT MARKING
- ② WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 4 INCH (YELLOW)
- ③ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 4 INCH (WHITE)
- ④ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 5 INCH (WHITE)
- ⑤ TEMPORARY PAVEMENT MARKING-LINE 6"-PAINT (YELLOW) (ON BARRIER)
- ⑥ TEMPORARY PAVEMENT MARKING-LINE 6"-PAINT (WHITE) (ON BARRIER)
- ⑦ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 8 INCH (WHITE)
- TEMPORARY CONCRETE BARRIER
- TYPE II BARRIER, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT

NOTE
SEE MAINTENANCE OF TRAFFIC STAGING PLANS FOR LIMITS OF TEMPORARY CONCRETE BARRIER.

MODEL: D:\p1\11\1101422-211-1101422-01.dgn



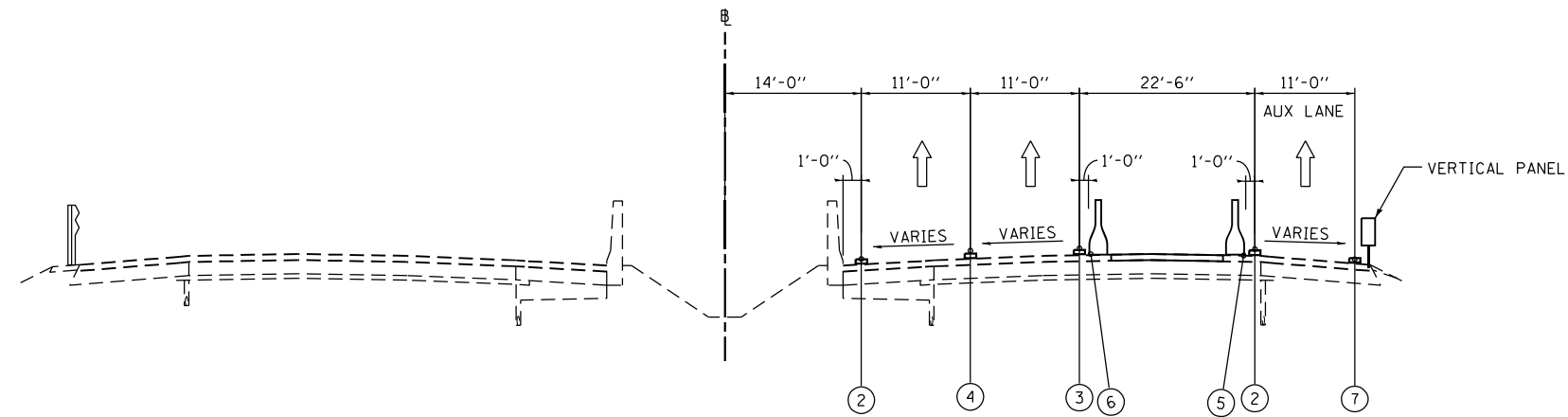
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	DRAWN - AMB	REVISED -
PLOT SCALE = 20,0018' / in.	CHECKED - MM	REVISED -
PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
TYPICAL SECTION - STAGE 2

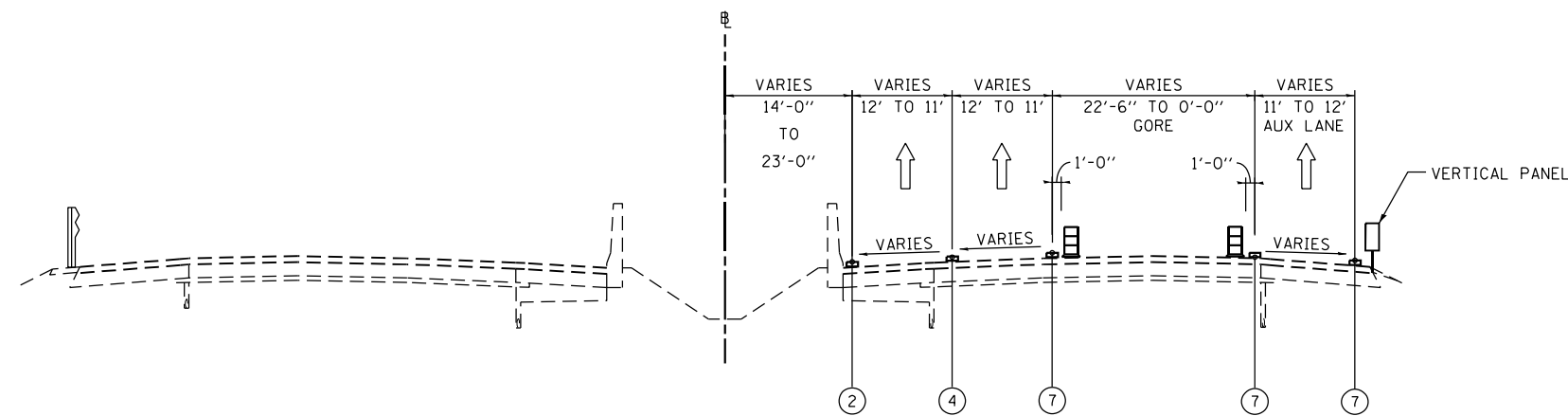
SCALE: N.T.S. SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	22
			CONTRACT NO. 62P58	
ILLINOIS FED. AID PROJECT				



STAGE 2
I-290 MAINTENANCE OF TRAFFIC
TYPICAL SECTION

EB 290
 STA 191+50 TO STA 209+15
 BRIDGE OMISSION STA 203+03 TO STA 208+12



STAGE 2
I-290 MAINTENANCE OF TRAFFIC
TYPICAL SECTION

EB 290
 STA 209+15 TO STA 215+18

LEGEND

- ① EXISTING PAVEMENT MARKING
- ② WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 4 INCH (YELLOW)
- ③ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 4 INCH (WHITE)
- ④ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 5 INCH (WHITE)
- ⑤ TEMPORARY PAVEMENT MARKING-LINE 6''-PAINT (YELLOW) (ON BARRIER)
- ⑥ TEMPORARY PAVEMENT MARKING-LINE 6''-PAINT (WHITE) (ON BARRIER)
- ⑦ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 8 INCH (WHITE)
- TEMPORARY CONCRETE BARRIER
- TYPE II BARRIER, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT

NOTE

SEE MAINTENANCE OF TRAFFIC STAGING PLANS FOR LIMITS OF TEMPORARY CONCRETE BARRIER.

MODEL: D:\p1\1422-215-1\1422-215-1.dwg



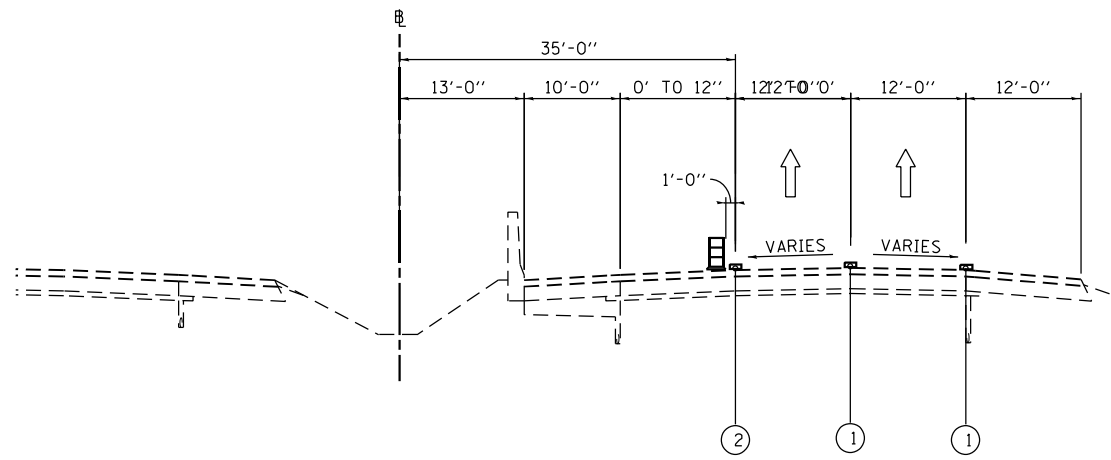
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PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
TYPICAL SECTION - STAGE 2

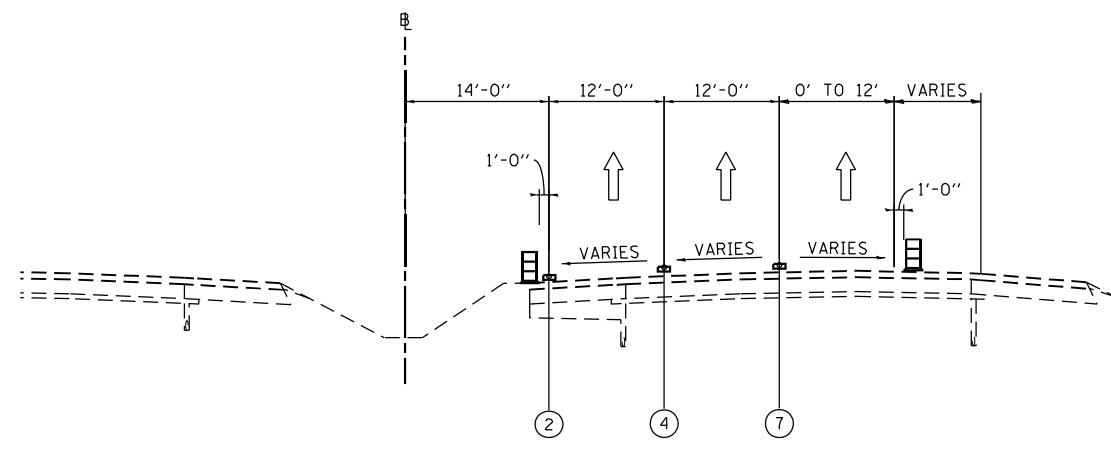
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	23
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				



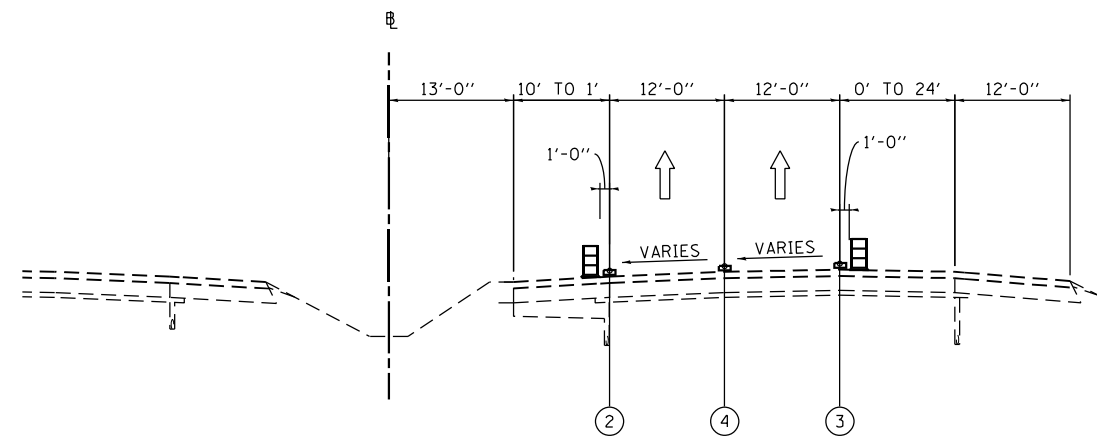
STAGE 3
I-290 MAINTENANCE OF TRAFFIC
TYPICAL SECTION

EB 290
STA 141+55 TO STA 161+55



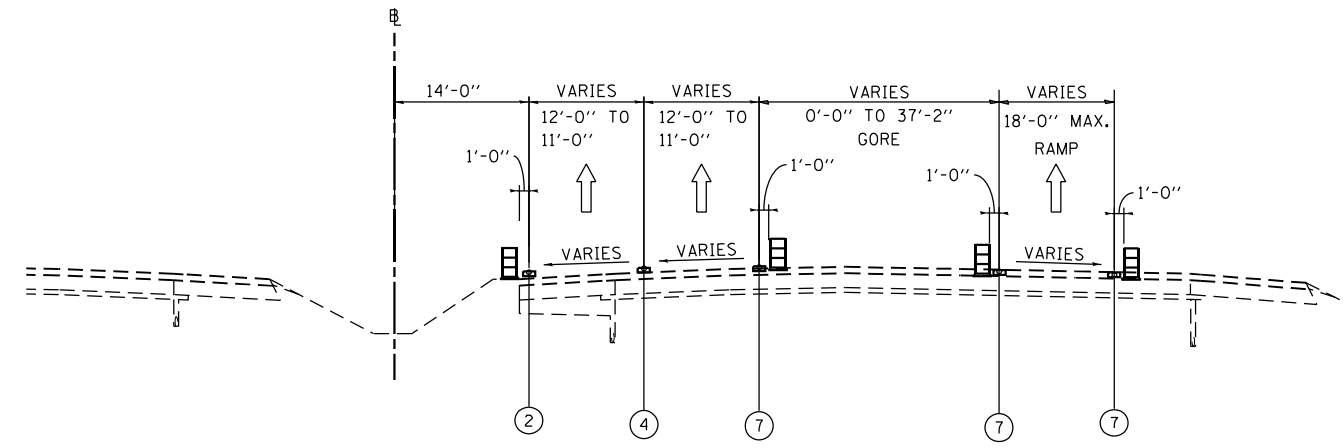
STAGE 3
MAINTENANCE OF TRAFFIC
TYPICAL SECTION

EB 290
STA 177+60 TO STA 185+35



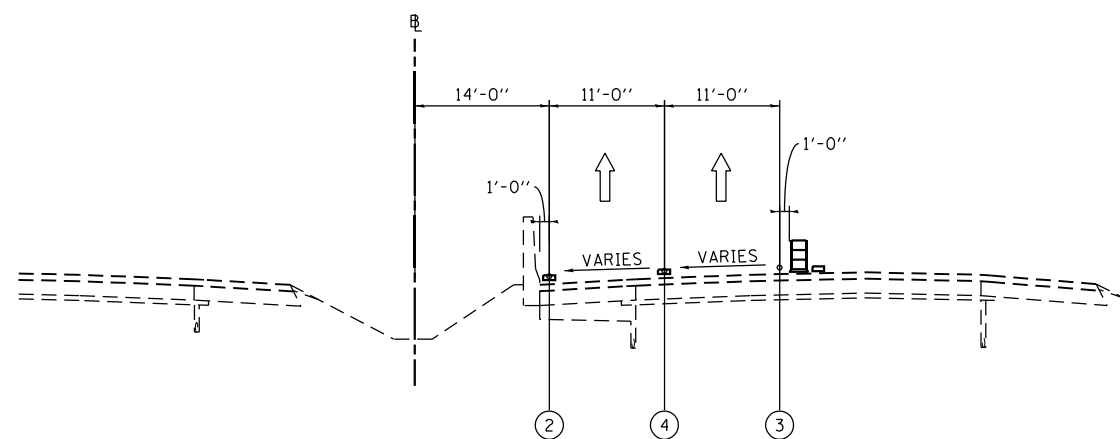
STAGE 3
MAINTENANCE OF TRAFFIC
TYPICAL SECTION

EB 290
STA 161+55 TO STA 171+00



STAGE 3
I-290 MAINTENANCE OF TRAFFIC
TYPICAL SECTION

EB 290
STA 185+35 TO STA 195+88



STAGE 3
MAINTENANCE OF TRAFFIC
TYPICAL SECTION

EB 290
STA 171+00 TO STA 177+60
STA 195+88 TO STA 202+00

LEGEND

- ① EXISTING PAVEMENT MARKING
- ② WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 4 INCH (YELLOW)
- ③ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 4 INCH (WHITE)
- ④ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 5 INCH (WHITE)
- ⑤ TEMPORARY PAVEMENT MARKING-LINE 6''-PAINT (YELLOW) (ON BARRIER)
- ⑥ TEMPORARY PAVEMENT MARKING-LINE 6''-PAINT (WHITE) (ON BARRIER)
- ⑦ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 8 INCH (WHITE)
- TEMPORARY CONCRETE BARRIER
- TYPE II BARRIER, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT

NOTE

SEE MAINTENANCE OF TRAFFIC STAGING PLANS FOR LIMITS OF TEMPORARY CONCRETE BARRIER.

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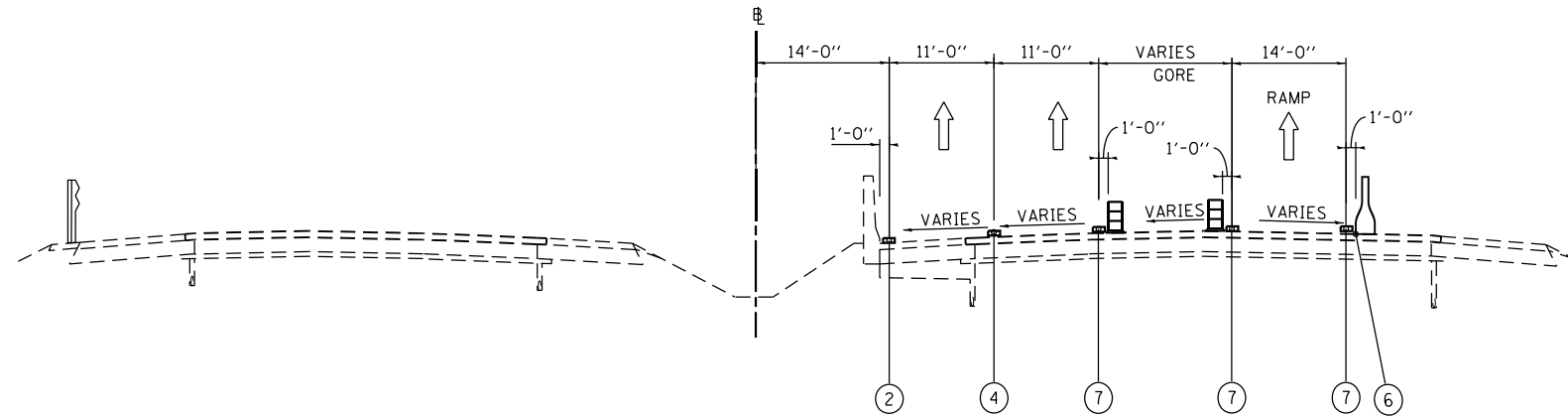
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PLOT DATE = 3/18/2022	CHECKED - MM	REVISED -
	DATE - 02/18/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
TYPICAL SECTIONS - STAGE 3

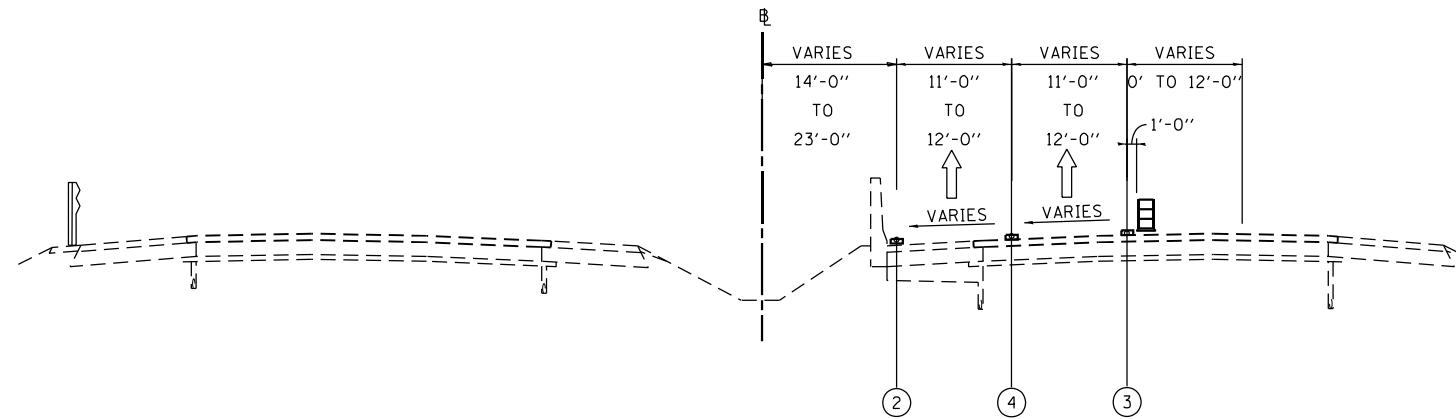
SCALE: N.T.S. SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	24
				CONTRACT NO. 62P58
ILLINOIS FED. AID PROJECT				



STAGE 3
MAINTENANCE OF TRAFFIC
TYPICAL SECTION

EB 290
STA 202+00 TO STA 211+00
BRIDGE OMISSION FROM STA 203+03 TO STA 208+12



STAGE 3
MAINTENANCE OF TRAFFIC
TYPICAL SECTION

EB 290
STA 211+00 TO STA 215+17

LEGEND

- ① EXISTING PAVEMENT MARKING
- ② WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 4 INCH (YELLOW)
- ③ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 4 INCH (WHITE)
- ④ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 5 INCH (YELLOW)
- ⑤ TEMPORARY PAVEMENT MARKING-LINE 6''-PAINT (YELLOW) (ON BARRIER)
- ⑥ TEMPORARY PAVEMENT MARKING-LINE 6''-PAINT (WHITE) (ON BARRIER)
- ⑦ WET REFLECTIVE TEMPORARY TAPE, - TYPE III, 8 INCH (WHITE)
- TEMPORARY CONCRETE BARRIER
- TYPE II BARRIER, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT

NOTE

SEE MAINTENANCE OF TRAFFIC STAGING PLANS FOR LIMITS OF TEMPORARY CONCRETE BARRIER.

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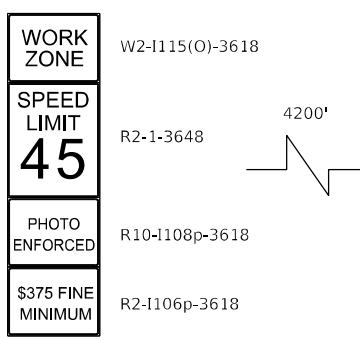
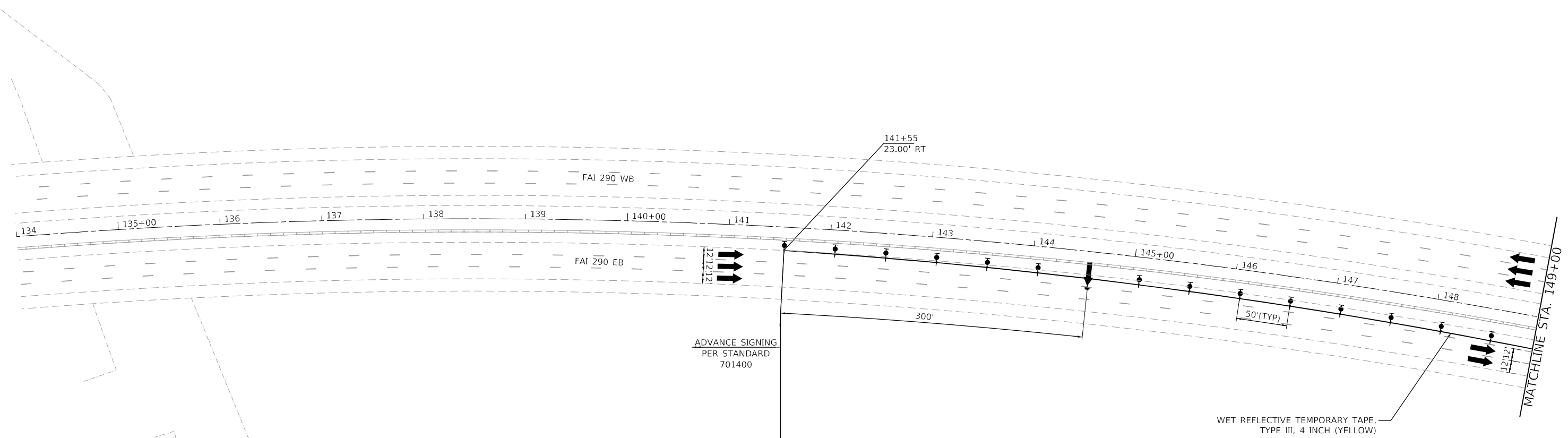
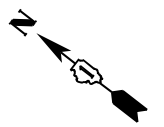
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PLOT DATE = 3/18/2022	DATE - 02/18/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
TYPICAL SECTIONS - STAGE 3

SCALE: N.T.S. SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	25
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				



LEGEND:

- SHOULDER RUMBLE STRIP REMOVAL ENGINEER TO FIELD VERIFY SHOULDER CONDITION AND DETERMINE LOCATION OF CLASS D PATCHING AS NECESSARY
- TEMPORARY SIGN PANELS
- WORK ZONE
- TRAFFIC ARROW
- FLASHING ARROW BOARD
- TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TEMPORARY IMPACT ATTENUATORS
- TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION

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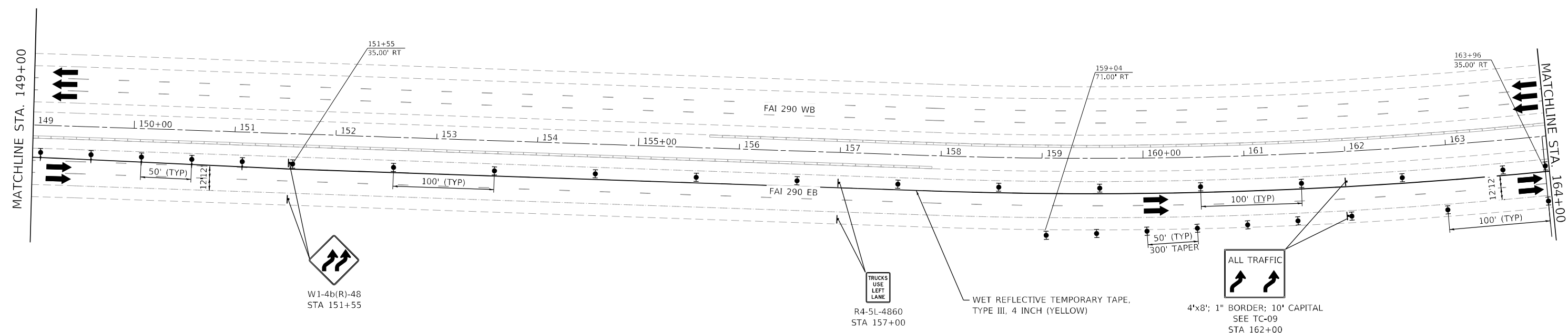
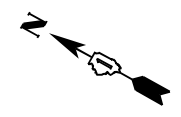


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	DRAWN - JSCHAEFER	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

MAINTENANCE OF TRAFFIC PLAN - STAGE 1			
SCALE: 1" = 50'	SHEET 1	OF 16 SHEETS	STA. 134+00.00 TO STA. 149+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	26
CONTRACT NO. 62P58			ILLINOIS FED. AID PROJECT	



LEGEND:

	SHOULDER RUMBLE STRIP REMOVAL ENGINEER TO FIELD VERIFY SHOULDER CONDITION AND DETERMINE LOCATION OF CLASS D PATCHING AS NECESSARY
	TEMPORARY SIGN PANELS
	WORK ZONE
	TRAFFIC ARROW
	FLASHING ARROW BOARD
	TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
	DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATORS
	TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION

MODEL Path.mxd
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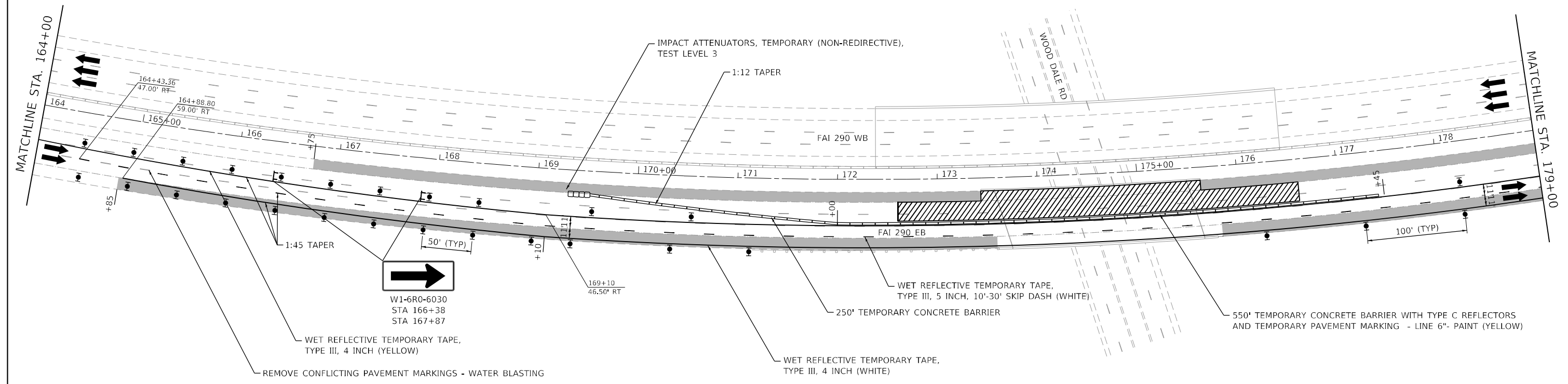
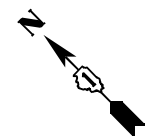
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	DRAWN - JSCHAEFER	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
 PLAN - STAGE 1**

SCALE: 1" = 50' SHEET 2 OF 16 SHEETS STA. 149+00.00 TO STA. 164+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	27
CONTRACT NO. 62P58			ILLINOIS FED. AID PROJECT	



LEGEND:

	SHOULDER RUMBLE STRIP REMOVAL ENGINEER TO FIELD VERIFY SHOULDER CONDITION AND DETERMINE LOCATION OF CLASS D PATCHING AS NECESSARY
	TEMPORARY SIGN PANELS
	WORK ZONE
	TRAFFIC ARROW
	FLASHING ARROW BOARD
	TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
	DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATORS
	TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION

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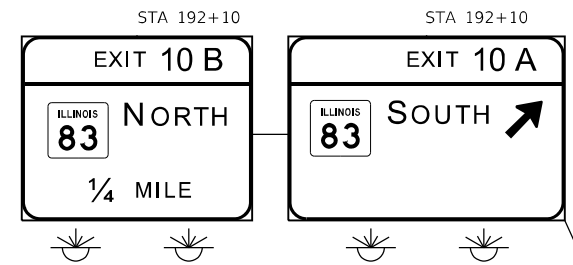
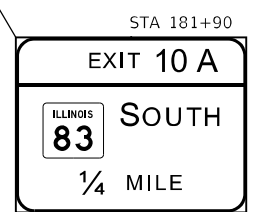
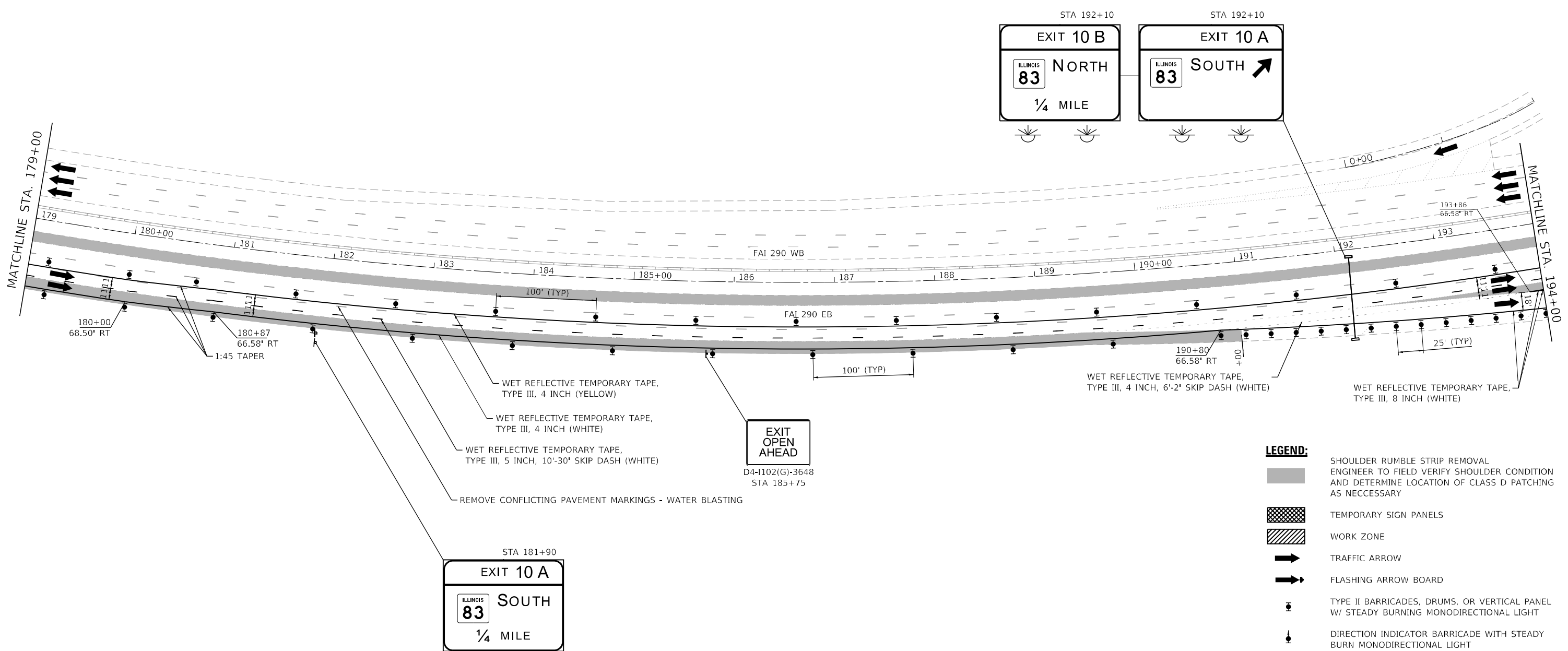
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	DRAWN - JSCHAEFER	REVISED -
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PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
 PLAN - STAGE 1**

SCALE: 1" = 50' SHEET 3 OF 16 SHEETS STA. 164+00.00 TO STA. 179+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	28
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	



- LEGEND:**
- SHOULDER RUMBLE STRIP REMOVAL ENGINEER TO FIELD VERIFY SHOULDER CONDITION AND DETERMINE LOCATION OF CLASS D PATCHING AS NECESSARY
 - TEMPORARY SIGN PANELS
 - WORK ZONE
 - TRAFFIC ARROW
 - FLASHING ARROW BOARD
 - TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
 - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
 - TEMPORARY IMPACT ATTENUATORS
 - TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION

USER NAME = amalla,baymundo	DESIGNED - SLASHOCK	REVISED -
	DRAWN - JSCHAEFER	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

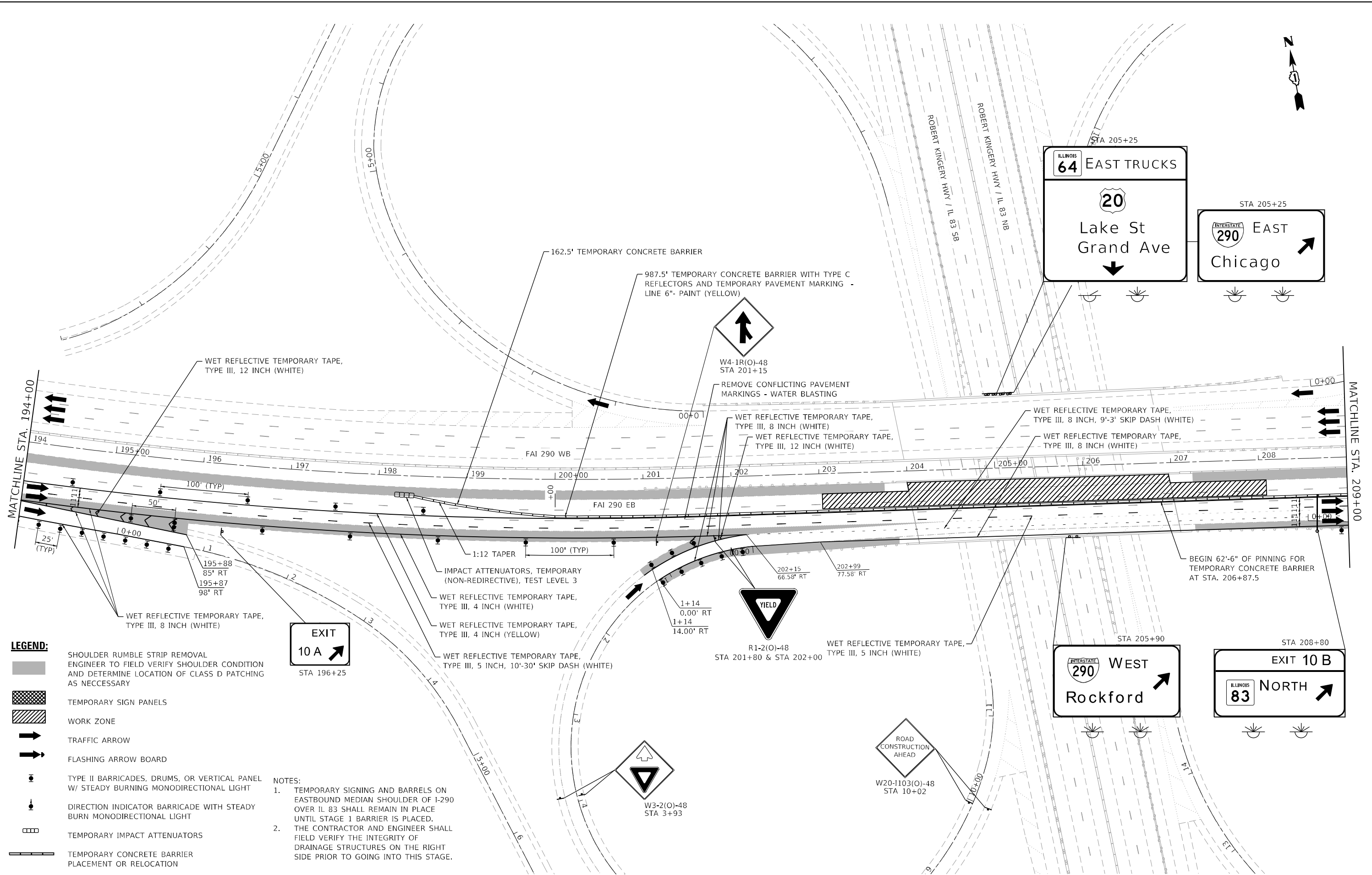
**MAINTENANCE OF TRAFFIC
PLAN - STAGE 1**

SCALE: 1" = 50' SHEET 4 OF 16 SHEETS STA. 179+00.00 TO STA. 194+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	29
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				



MODEL: D:\46411
FILE NAME: D:\10_1422-2h-rtb3\m01_03.dgn



- LEGEND:**
- SHOULDER RUMBLE STRIP REMOVAL ENGINEER TO FIELD VERIFY SHOULDER CONDITION AND DETERMINE LOCATION OF CLASS D PATCHING AS NECESSARY
 - TEMPORARY SIGN PANELS
 - WORK ZONE
 - TRAFFIC ARROW
 - FLASHING ARROW BOARD
 - TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
 - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
 - TEMPORARY IMPACT ATTENUATORS
 - TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION

- NOTES:**
1. TEMPORARY SIGNING AND BARRELS ON EASTBOUND MEDIAN SHOULDER OF I-290 OVER IL 83 SHALL REMAIN IN PLACE UNTIL STAGE 1 BARRIER IS PLACED.
 2. THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY THE INTEGRITY OF DRAINAGE STRUCTURES ON THE RIGHT SIDE PRIOR TO GOING INTO THIS STAGE.



USER NAME = amalla,baymundo	DESIGNED - SLASHOCK	REVISED -
PLOT SCALE = 100,000' / in.	DRAWN - JSCHAEFER	REVISED -
PLOT DATE = 3/18/2022	CHECKED - HREGAN	REVISED -
	DATE - 3/18/2022	REVISED -

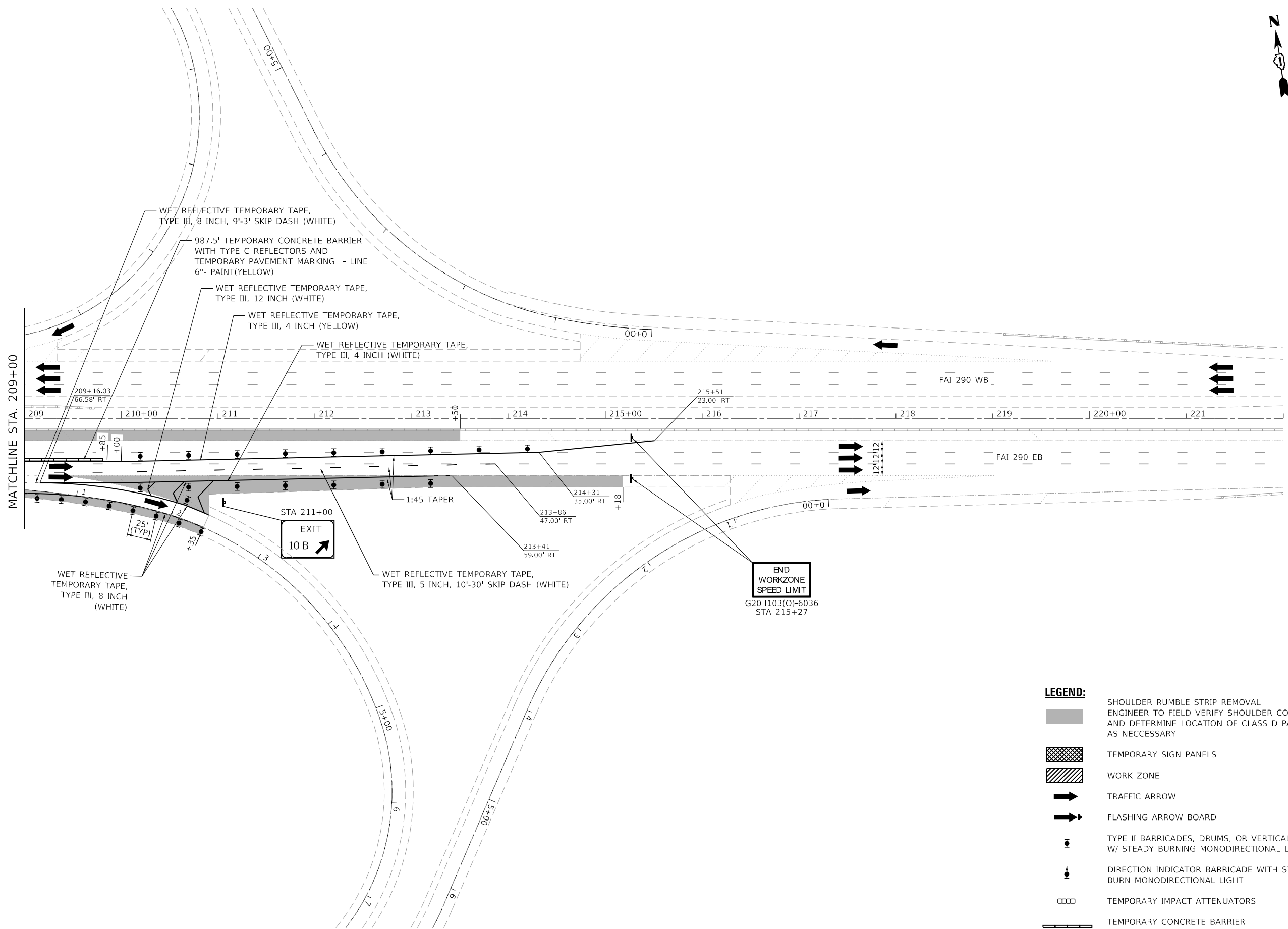
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
PLAN - STAGE 1**










SCALE: 1" = 50' SHEET 5 OF 16 SHEETS STA. 194+00.00 TO STA. 209+00.00

F.A.I. RTE. 290	SECTION 2021-140-BR	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 30
CONTRACT NO. 62P58			ILLINOIS FED. AID PROJECT	

MODEL: D:\ref\14
FILE NAME: D:\10_1422-21-2022-2021-140-BR\10_1422-21-2022-2021-140-BR.dwg



LEGEND:

	SHOULDER RUMBLE STRIP REMOVAL ENGINEER TO FIELD VERIFY SHOULDER CONDITION AND DETERMINE LOCATION OF CLASS D PATCHING AS NECESSARY
	TEMPORARY SIGN PANELS
	WORK ZONE
	TRAFFIC ARROW
	FLASHING ARROW BOARD
	TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
	DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATORS
	TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION

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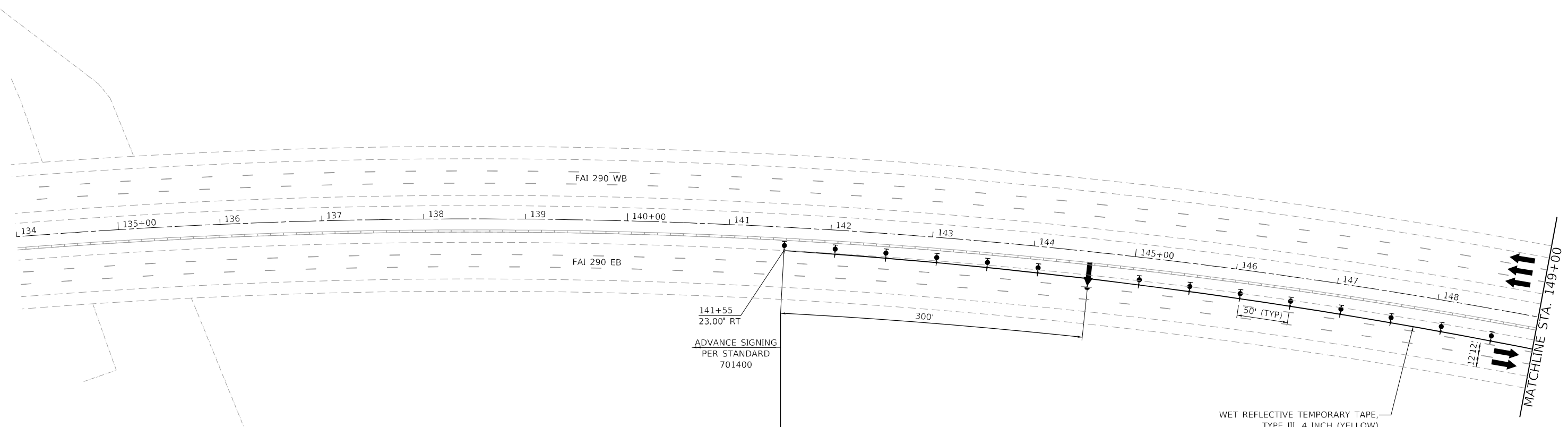
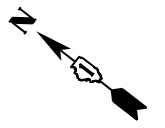


USER NAME = amalla,baymundo	DESIGNED - SLASHOCK	REVISED -
	DRAWN - JSCHAEFER	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

MAINTENANCE OF TRAFFIC PLAN - STAGE 1			
SCALE: 1" = 50'	SHEET 6	OF 16 SHEETS	STA. 209+00.00 TO STA. 222+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	31
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	



WORK ZONE	W2-I115(O)-3618
SPEED LIMIT 45	R2-1-3648
PHOTO ENFORCED	R10-I108p-3618
\$375 FINE MINIMUM	R2-I106p-3618

LEGEND:

	TEMPORARY SIGN PANELS
	WORK ZONE
	TRAFFIC ARROW
	FLASHING ARROW BOARD
	TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
	DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATORS
	TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION

NOTE: THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY THE INTEGRITY ON DRAINAGE STRUCTURES ON THE LEFT SIDE PRIOR TO GOING INTO THIS STAGE.

MODEL: D:\64\h... FILE NAME: D:\10\1422-2h-103\m02_00.dgn



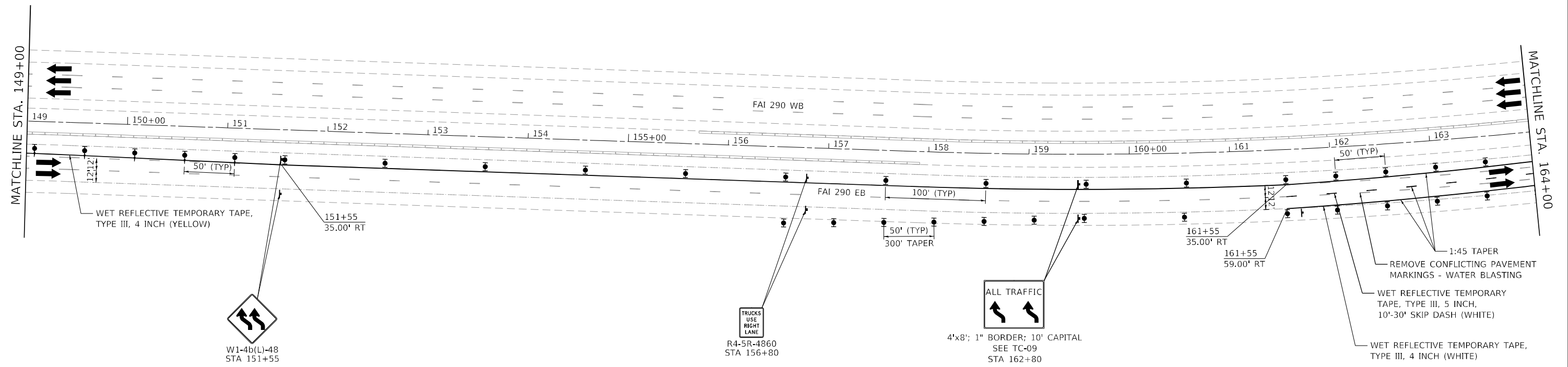
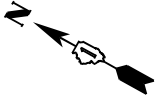
USER NAME = amalla.baymundo	DESIGNED - SLASHOCK	REVISED -
	DRAWN - JSCHAEFER	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
PLAN - STAGE 2**

SCALE: 1" = 50' SHEET 7 OF 16 SHEETS STA. 134+00.00 TO STA. 149+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	32
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	



LEGEND:

	TEMPORARY SIGN PANELS
	WORK ZONE
	TRAFFIC ARROW
	FLASHING ARROW BOARD
	TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
	DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATORS
	TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION

NOTE: THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY THE INTEGRITY ON DRAINAGE STRUCTURES ON THE LEFT SIDE PRIOR TO GOING INTO THIS STAGE.

MODEL: D:\6461\101422-21-103\m02_01.dgn

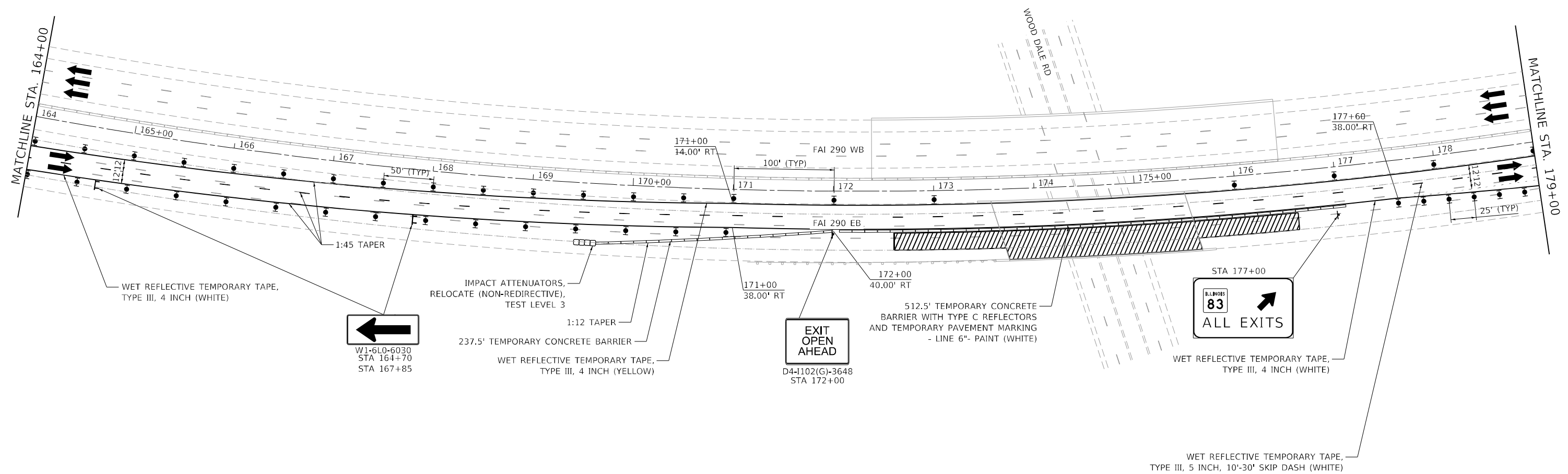
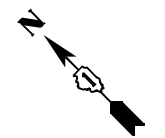


USER NAME = amalla,baymundo	DESIGNED - SLASHOCK	REVISED -
	DRAWN - JSCHAEFER	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MAINTENANCE OF TRAFFIC PLAN - STAGE 2			
SCALE: 1" = 50'	SHEET 8	OF 16 SHEETS	STA. 149+00.00 TO STA. 164+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	33
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	



WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4 INCH (WHITE)

W1-6L0-6030
STA 164+70
STA 167+85

IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3

237.5' TEMPORARY CONCRETE BARRIER

WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4 INCH (YELLOW)

EXIT OPEN AHEAD
D4-1102(G)-3648
STA 172+00

172+00
40.00' RT









512.5' TEMPORARY CONCRETE BARRIER WITH TYPE C REFLECTORS AND TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT (WHITE)

STA 177+00
ILLINOIS 83
ALL EXITS

WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4 INCH (WHITE)

WET REFLECTIVE TEMPORARY TAPE, TYPE III, 5 INCH, 10'-30' SKIP DASH (WHITE)

LEGEND:

-  TEMPORARY SIGN PANELS
-  WORK ZONE
-  TRAFFIC ARROW
-  FLASHING ARROW BOARD
-  TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
-  DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  TEMPORARY IMPACT ATTENUATORS
-  TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION

NOTE: THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY THE INTEGRITY ON DRAINAGE STRUCTURES ON THE LEFT SIDE PRIOR TO GOING INTO THIS STAGE.



USER NAME = amalla,baymundo	DESIGNED - SLASHOCK	REVISED -
	DRAWN - JSCHAEFER	REVISED -
PLOT SCALE = 100,000' / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

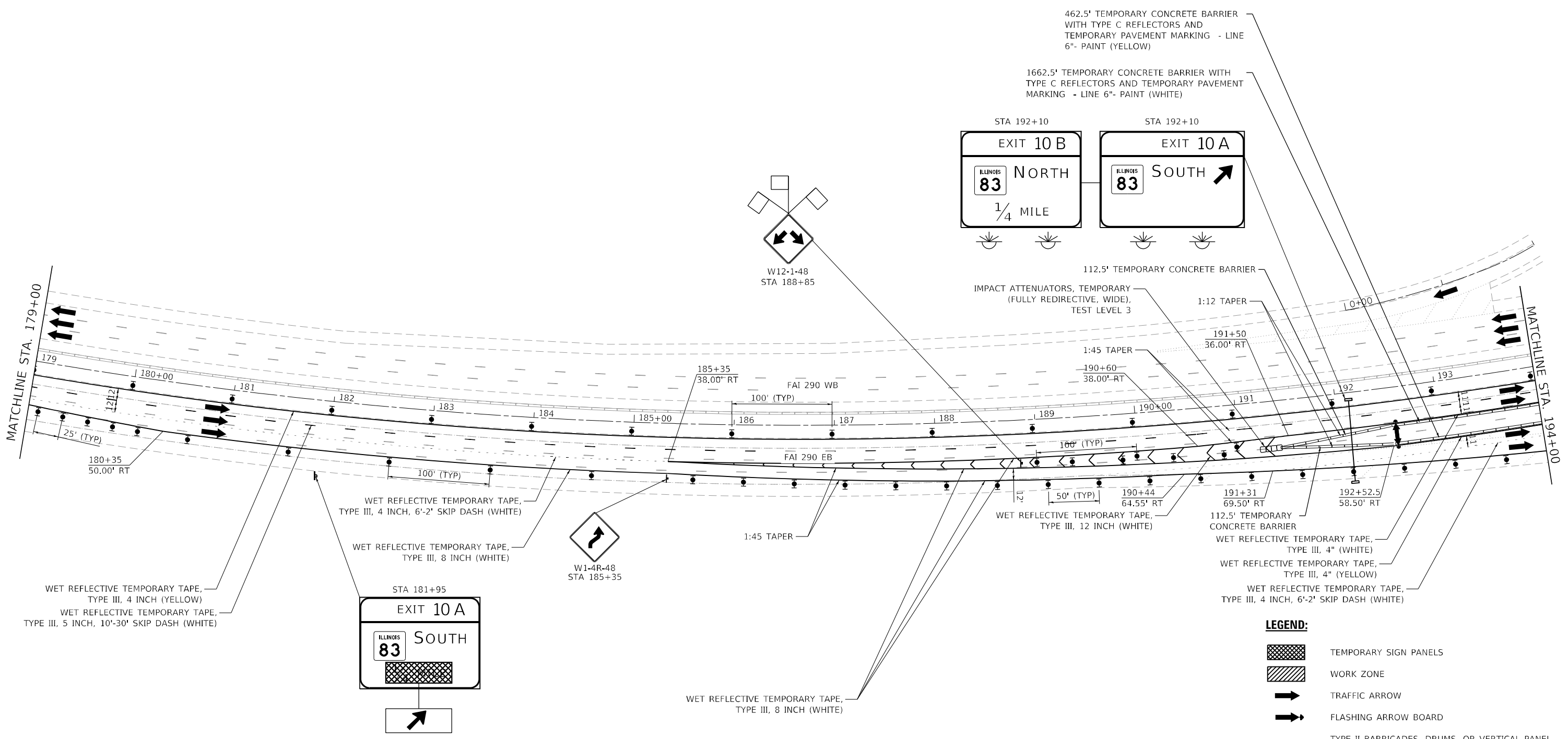
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
PLAN - STAGE 2**

SCALE: 1" = 50' SHEET 9 OF 16 SHEETS STA. 164+00.00 TO STA. 179+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	34
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	

MODEL: D:\64\h... FILE NAME: D:\0_1422-2h-103\m02_02.dgn



NOTE: THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY THE INTEGRITY ON DRAINAGE STRUCTURES ON THE LEFT SIDE PRIOR TO GOING INTO THIS STAGE.

MODEL: D:\64bit\... FILE NAME: D:\10_1422-2b-103\m02_03.dgn











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PLOT SCALE = 100,0000' / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

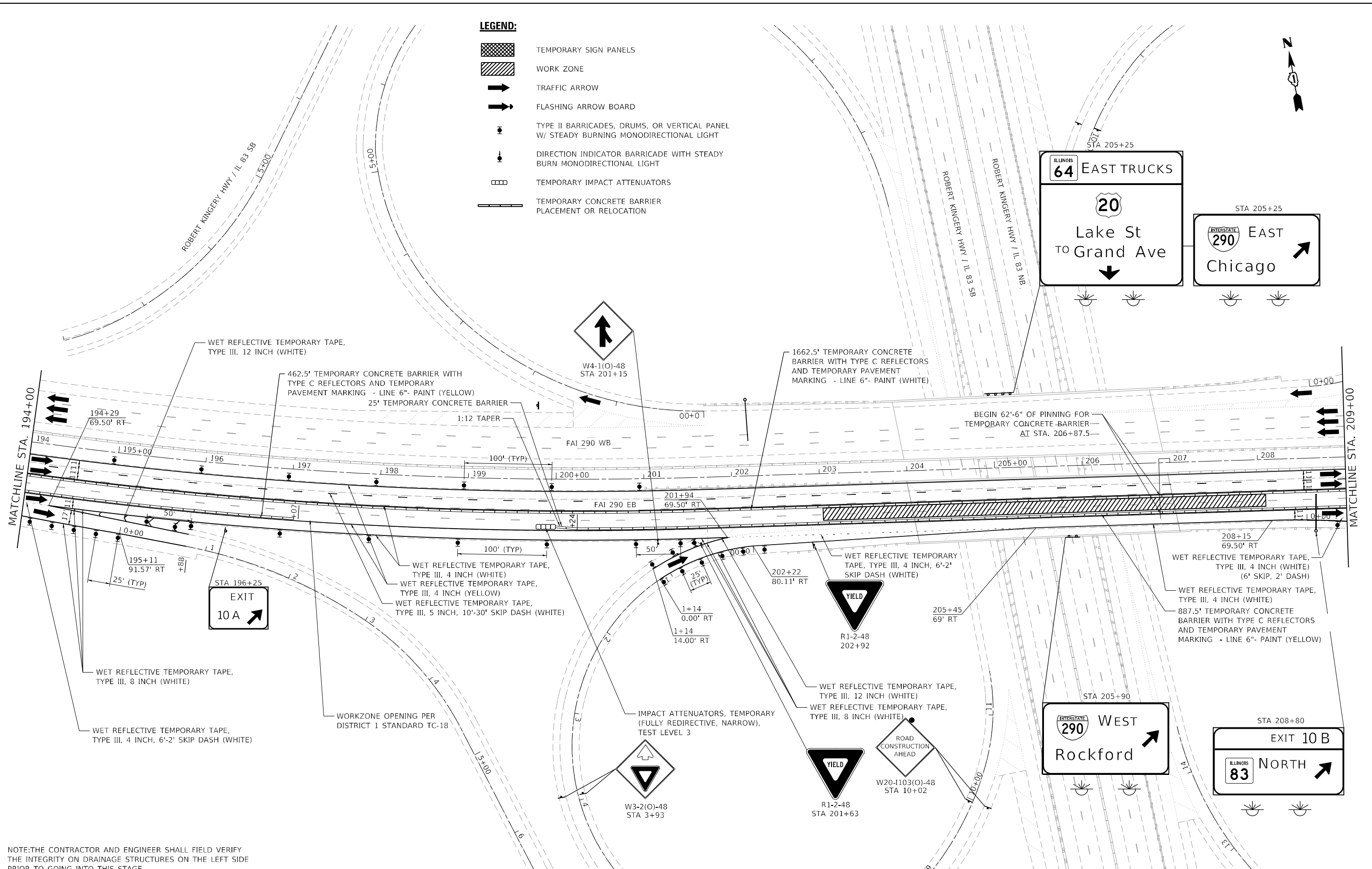
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
PLAN - STAGE 2**

SCALE: 1" = 50' SHEET 10 OF 16 SHEETS STA. 179+00.00 TO STA. 194+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	35
CONTRACT NO. 62P58			ILLINOIS FED. AID PROJECT	

- LEGEND:**
-  TEMPORARY SIGN PANELS
 -  WORK ZONE
 -  TRAFFIC ARROW
 -  FLASHING ARROW BOARD
 -  TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
 -  DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
 -  TEMPORARY IMPACT ATTENUATORS
 -  TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION



NOTE: THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY THE INTEGRITY ON DRAINAGE STRUCTURES ON THE LEFT SIDE PRIOR TO GOING INTO THIS STAGE.

MODEL: D:\64\hntb\1422-2h-1422\m02_04.dgn



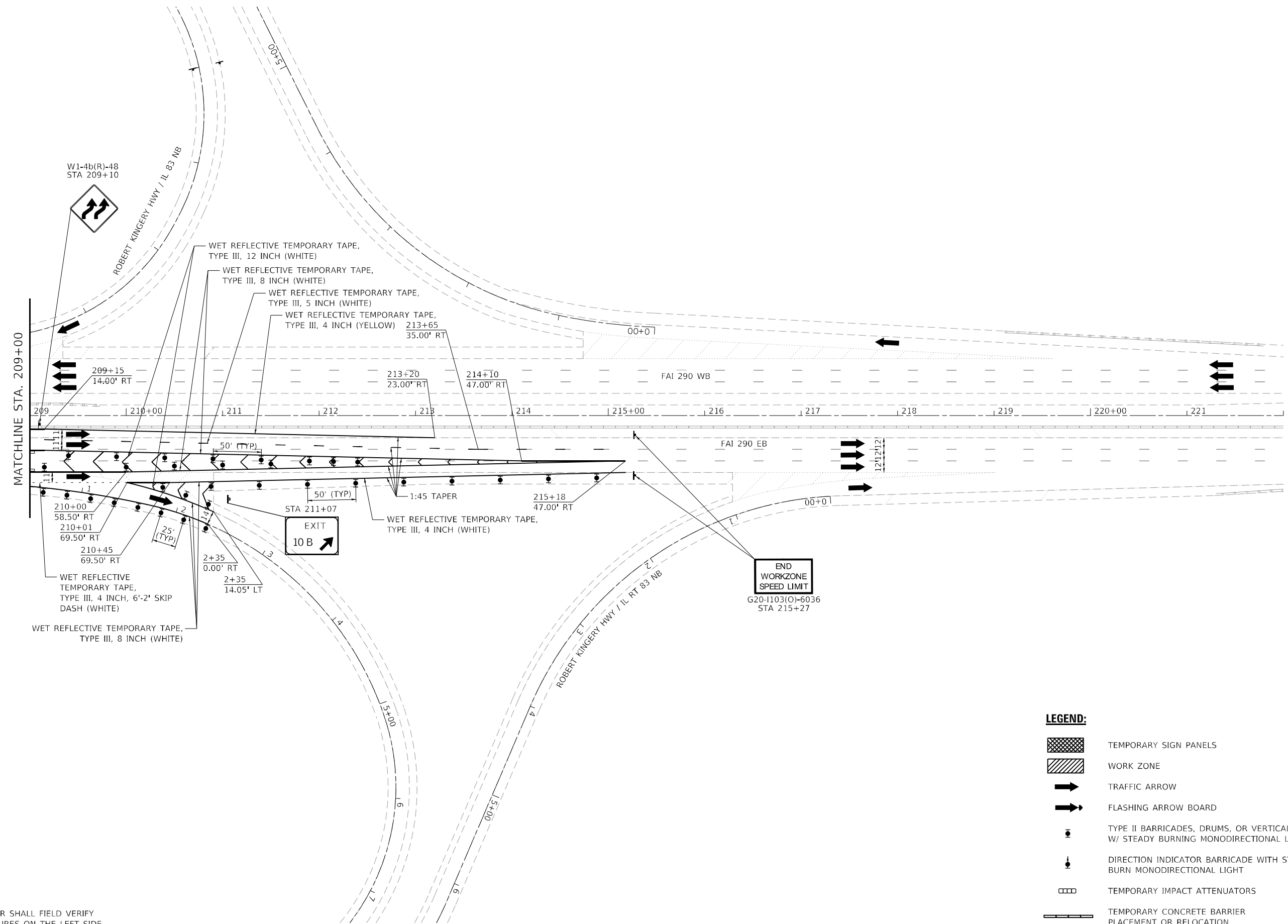
USER NAME = amalla,baymundo	DESIGNED - SLASHOCK	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN - JSCHAEFER	REVISED -
PLOT DATE = 3/18/2022	CHECKED - HREGAN	REVISED -
	DATE - 3/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
PLAN - STAGE 2**

SCALE: 1" = 50' SHEET 11 OF 16 SHEETS STA. 194+00.00 TO STA. 209+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	36
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				



LEGEND:

	TEMPORARY SIGN PANELS
	WORK ZONE
	TRAFFIC ARROW
	FLASHING ARROW BOARD
	TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
	DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATORS
	TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION

NOTE: THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY THE INTEGRITY ON DRAINAGE STRUCTURES ON THE LEFT SIDE PRIOR TO GOING INTO THIS STAGE.

MODEL: D:\64bit... FILE NAME: D:\01422-2b-rt-rt-rt-rt-rt-rt-05.dgn



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	DRAWN - JSCHAEFER	REVISED -
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PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

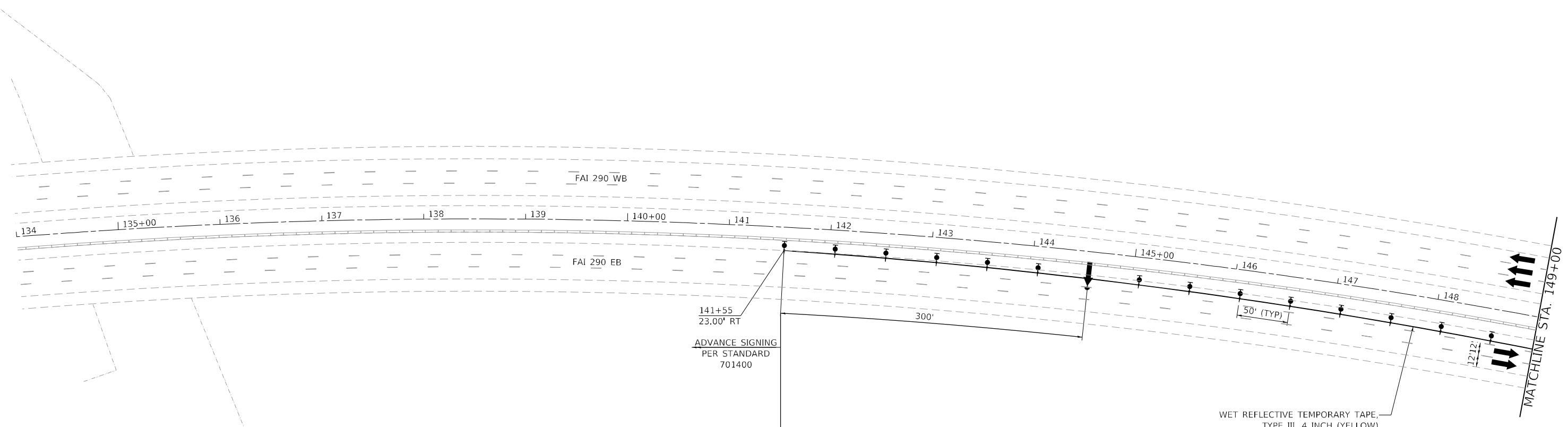
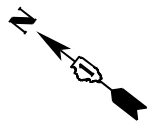
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
PLAN - STAGE 2**

SCALE: 1" = 50' SHEET 12 OF 16 SHEETS STA. 209+00.00 TO STA. 222+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	37
CONTRACT NO. 62P58				

ILLINOIS FED. AID PROJECT



WORK ZONE	W2-I115(O)-3618
SPEED LIMIT 45	R2-1-3648
PHOTO ENFORCED	R10-I108p-3618
\$375 FINE MINIMUM	R2-I106p-3618

LEGEND:

	TEMPORARY SIGN PANELS
	WORK ZONE
	TRAFFIC ARROW
	FLASHING ARROW BOARD
	TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
	DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATORS
	TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION

NOTE: THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY THE INTEGRITY ON DRAINAGE STRUCTURES ON THE LEFT SIDE PRIOR TO GOING INTO THIS STAGE.

MODEL: D:\64\h... FILE NAME: D:\0_1422-2h-103\093-00



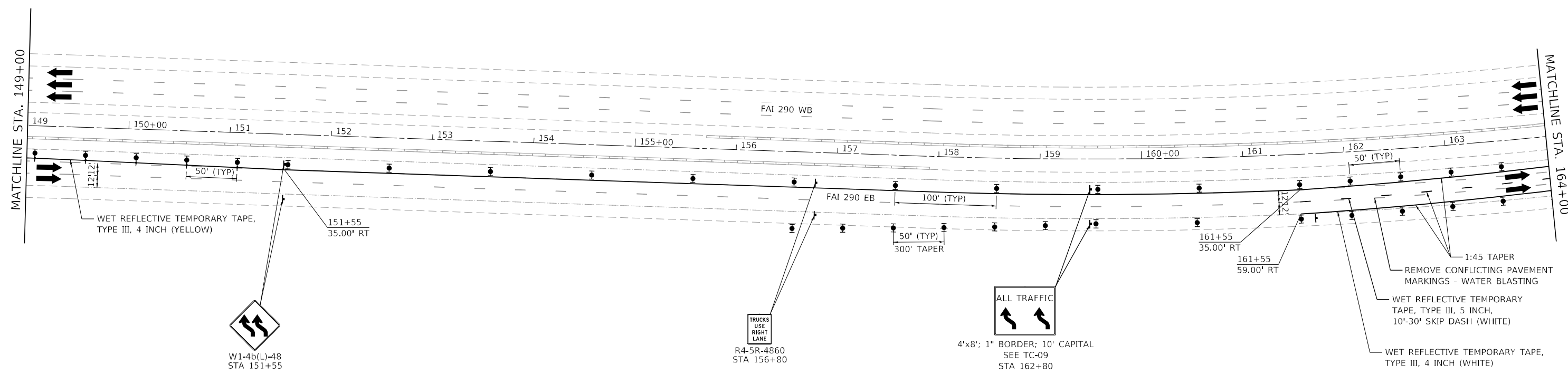
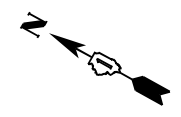
USER NAME = amalle.baymundo	DESIGNED - SLASHOCK	REVISED -
	DRAWN - JSCHAEFER	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
PLAN - STAGE 3**

SCALE: 1" = 50' SHEET 7 OF 16 SHEETS STA. 134+00.00 TO STA. 149+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	38
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	



WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4 INCH (YELLOW)



W1-4b(L)-48
STA 151+55











R4-5R-4860
STA 156+80



ALL TRAFFIC
4'x8'; 1" BORDER; 10' CAPITAL
SEE TC-09
STA 162+80

LEGEND:

-  TEMPORARY SIGN PANELS
-  WORK ZONE
-  TRAFFIC ARROW
-  FLASHING ARROW BOARD
-  TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
-  DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  TEMPORARY IMPACT ATTENUATORS
-  TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION

NOTE: THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY THE INTEGRITY ON DRAINAGE STRUCTURES ON THE LEFT SIDE PRIOR TO GOING INTO THIS STAGE.

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FILE NAME: D:\0\1422-2h-103\093-01



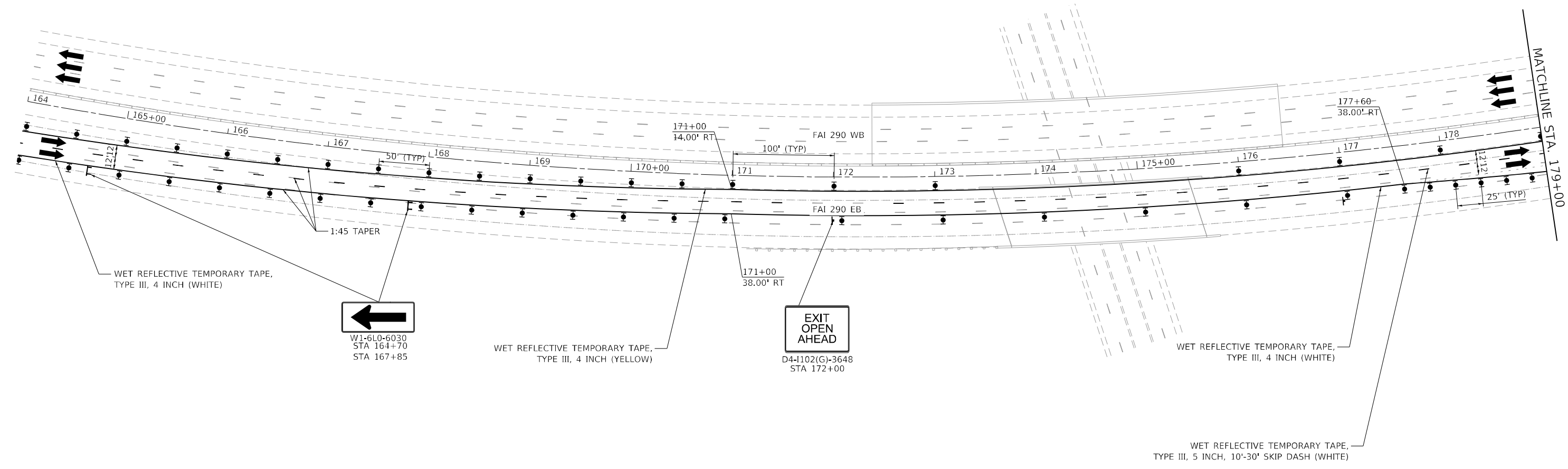
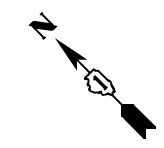
USER NAME = amalla,baymundo	DESIGNED - SLASHOCK	REVISED -
	DRAWN - JSCHAEFER	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
PLAN - STAGE 3**

SCALE: 1" = 50' SHEET 8 OF 16 SHEETS STA. 149+00.00 TO STA. 164+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	39
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	



WET REFLECTIVE TEMPORARY TAPE,
TYPE III, 4 INCH (WHITE)

←
W1-6L0-6030
STA 164+70
STA 167+85









WET REFLECTIVE TEMPORARY TAPE,
TYPE III, 4 INCH (YELLOW)

EXIT
OPEN
AHEAD
D4-1102(G)-3648
STA 172+00

WET REFLECTIVE TEMPORARY TAPE,
TYPE III, 4 INCH (WHITE)

WET REFLECTIVE TEMPORARY TAPE,
TYPE III, 5 INCH, 10'-30' SKIP DASH (WHITE)

LEGEND:

-  TEMPORARY SIGN PANELS
-  WORK ZONE
-  TRAFFIC ARROW
-  FLASHING ARROW BOARD
-  TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL
W/ STEADY BURNING MONODIRECTIONAL LIGHT
-  DIRECTION INDICATOR BARRICADE WITH STEADY
BURN MONODIRECTIONAL LIGHT
-  TEMPORARY IMPACT ATTENUATORS
-  TEMPORARY CONCRETE BARRIER
PLACEMENT OR RELOCATION

NOTE: THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY
THE INTEGRITY ON DRAINAGE STRUCTURES ON THE LEFT SIDE
PRIOR TO GOING INTO THIS STAGE.

MODEL: D:\64\h...
FILE NAME: D:\0\1422-2h-103\0903_02.dgn



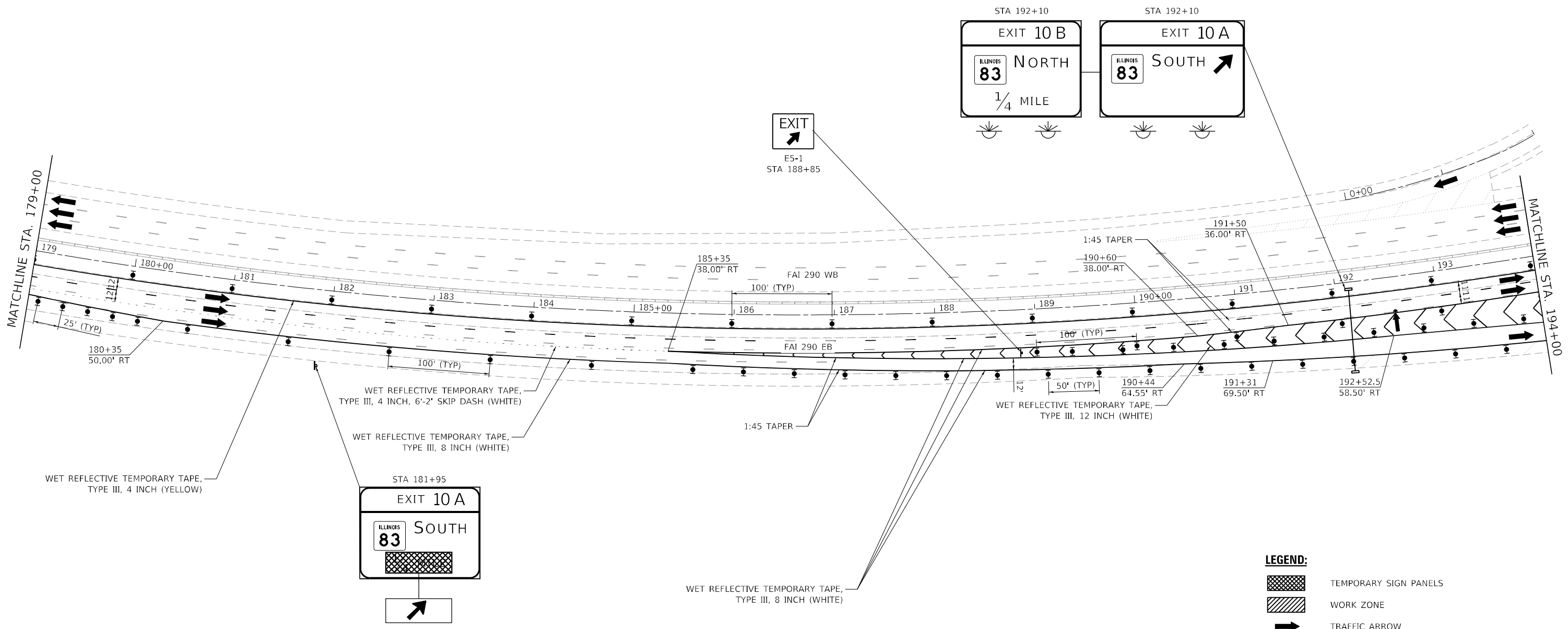
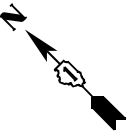
USER NAME = amalla,baymundo	DESIGNED - SLASHOCK	REVISED -
	DRAWN - JSCHAEFER	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
PLAN - STAGE 3**

SCALE: 1" = 50' SHEET 13 OF 16 SHEETS STA. 164+00.00 TO STA. 179+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	40
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	



LEGEND:









	TEMPORARY SIGN PANELS
	WORK ZONE
	TRAFFIC ARROW
	FLASHING ARROW BOARD
	TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATORS
	TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION

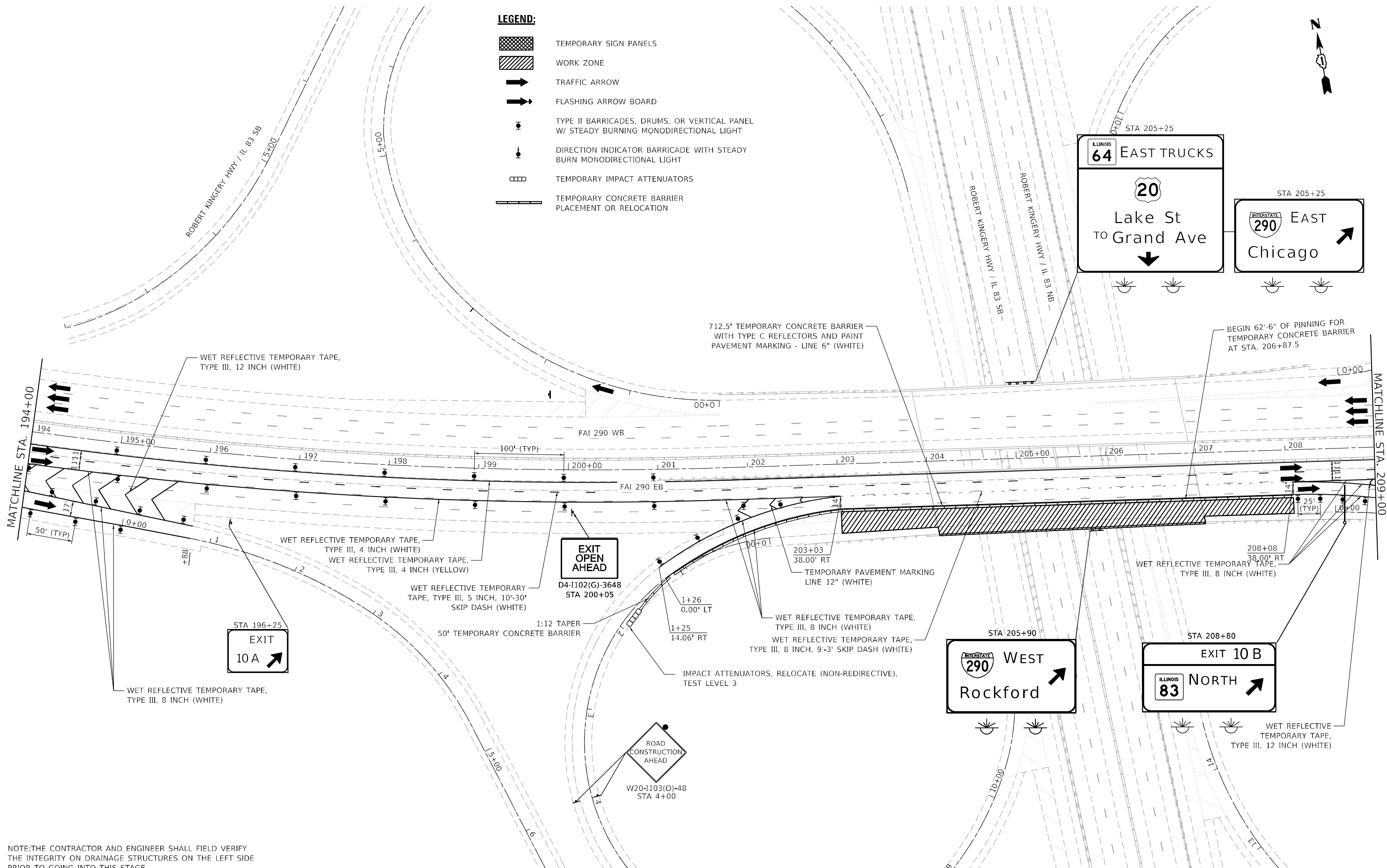
NOTE: THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY THE INTEGRITY ON DRAINAGE STRUCTURES ON THE LEFT SIDE PRIOR TO GOING INTO THIS STAGE.

MODEL: D:\64\hntb\1422-2b-140-140-03.dgn

	USER NAME = amalla,baymundo	DESIGNED - SLASHOCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MAINTENANCE OF TRAFFIC PLAN - STAGE 3			F.A.I. RTE. 290	SECTION 2021-140-BR	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 41
	PLOT SCALE = 100,0000' / in.	CHECKED - HREGAN	REVISED -					CONTRACT NO. 62P58				
	PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -					ILLINOIS FED. AID PROJECT				

SCALE: 1" = 50' SHEET 14 OF 16 SHEETS STA. 179+00.00 TO STA. 194+00.00

- LEGEND:**
-  TEMPORARY SIGN PANELS
 -  WORK ZONE
 -  TRAFFIC ARROW
 -  FLASHING ARROW BOARD
 -  TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
 -  DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
 -  TEMPORARY IMPACT ATTENUATORS
 -  TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION



712.5' TEMPORARY CONCRETE BARRIER WITH TYPE C REFLECTORS AND PAINT PAVEMENT MARKING - LINE 6" (WHITE)

BEGIN 62'-6" OF PINNING FOR TEMPORARY CONCRETE BARRIER AT STA. 206+87.5

EXIT OPEN AHEAD
D4-I102(G)-3648
STA 200+05

WEST
Rockford

EXIT 10 B
NORTH

ROAD CONSTRUCTION AHEAD
W20-I103(O)-48
STA 4+00

NOTE: THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY THE INTEGRITY ON DRAINAGE STRUCTURES ON THE LEFT SIDE PRIOR TO GOING INTO THIS STAGE.

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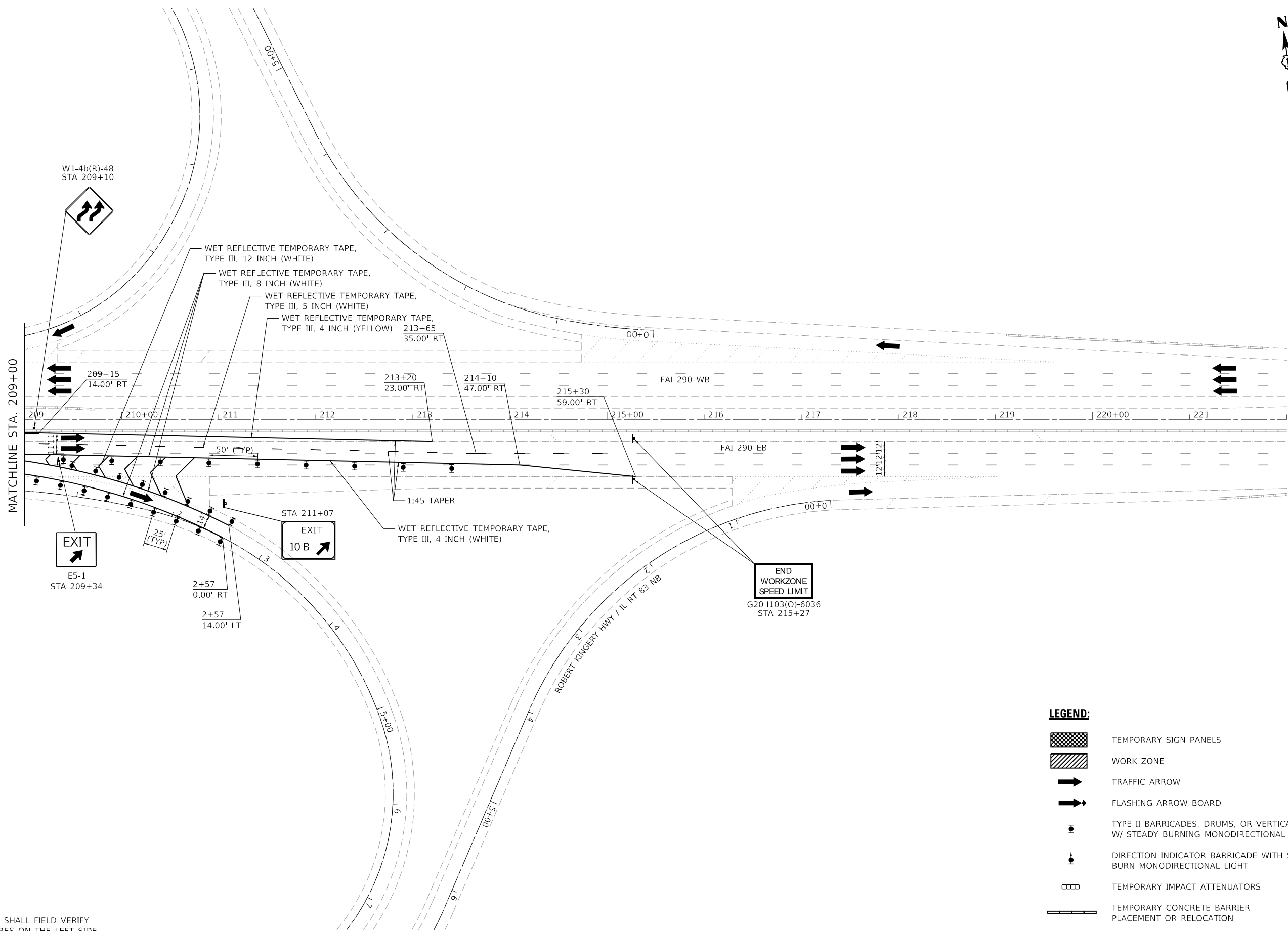
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	DRAWN - JSCHAEFER	REVISED -
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PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
PLAN - STAGE 3**

SCALE: 1" = 50' SHEET 15 OF 16 SHEETS STA. 194+00.00 TO STA. 209+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	42
CONTRACT NO. 62P58			ILLINOIS FED. AID PROJECT	



LEGEND:

	TEMPORARY SIGN PANELS
	WORK ZONE
	TRAFFIC ARROW
	FLASHING ARROW BOARD
	TYPE II BARRICADES, DRUMS, OR VERTICAL PANEL W/ STEADY BURNING MONODIRECTIONAL LIGHT
	DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATORS
	TEMPORARY CONCRETE BARRIER PLACEMENT OR RELOCATION

NOTE: THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY THE INTEGRITY ON DRAINAGE STRUCTURES ON THE LEFT SIDE PRIOR TO GOING INTO THIS STAGE.

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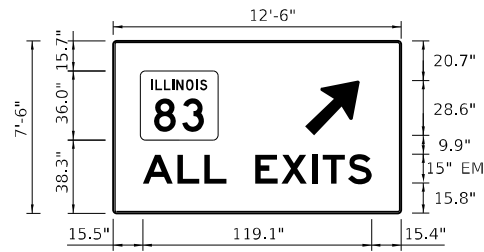


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DRAWN - JSCHAEFER	REVISIONS -	
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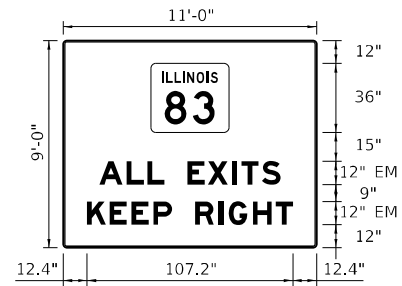
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MAINTENANCE OF TRAFFIC PLAN - STAGE 3			
SCALE: 1" = 50'	SHEET 16	OF 16 SHEETS	STA. 209+00.00 TO STA. 222+00.00

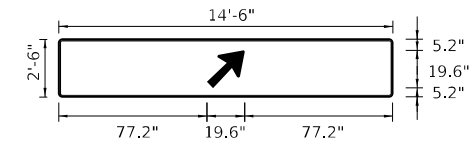
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	43
CONTRACT NO. 62P58			ILLINOIS FED. AID PROJECT	



R=12" STAGE 2
 TH=1.5" BLACK LETTERS ON ORANGE BACKGROUND
 IN=0.5" STA 177+00



R=12" STAGE 2
 TH=1.5" BLACK LETTERS ON ORANGE BACKGROUND
 IN=0.5" STA 161+70



R=1.5" STAGE 2
 TH=0.63" BLACK ARROW ON ORANGE BACKGROUND
 IN=0.47" STA 181+95

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PLOT DATE = 3/18/2022

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DRAWN - JSCHAEFER	REVISD -
CHECKED - HREGAN	REVISD -
DATE - 3/18/2022	REVISD -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
 TEMPORARY INFORMATION SIGNING**

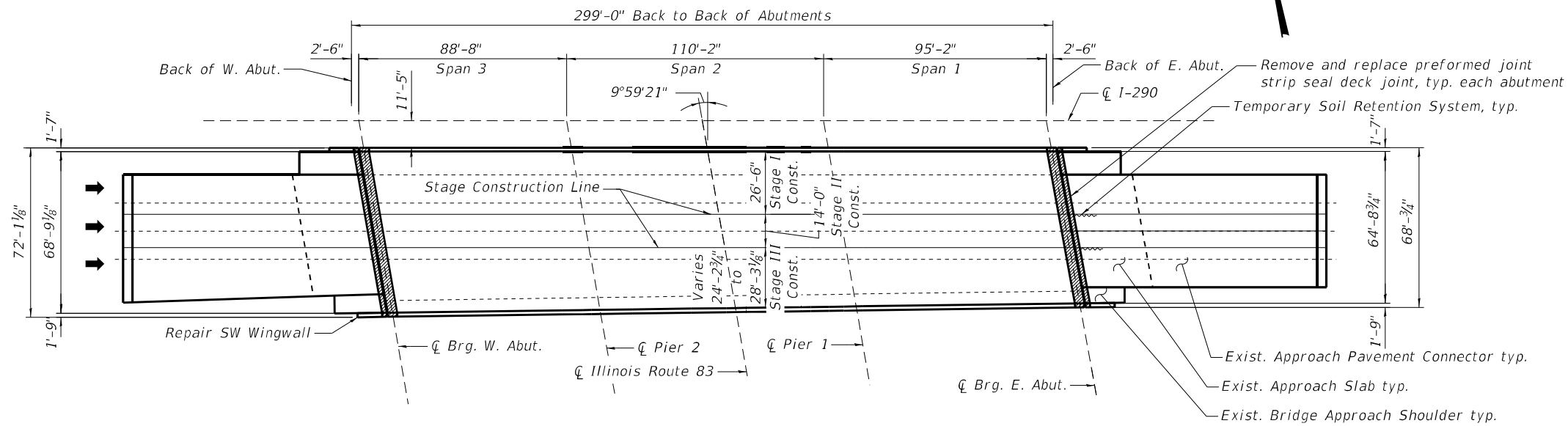
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	44
CONTRACT NO. 62P58			ILLINOIS FED. AID PROJECT	

Existing Structure:
 The bridge is a three-span continuous, composite plate girder bridge with a 7¾-inch reinforced concrete deck and a 2-inch concrete overlay. The original structure was built in 1971. In 1985, the structure was widened and overlaid, the expansion joints were reconstructed and the bearings were replaced. In 1998, the expansion joints were reconstructed and partial depth repairs were made on the approaches. In 2002, the bridge was cleaned and painted. In 2010, the concrete overlay and the deck joints were replaced.

Stage construction shall be utilized to maintain traffic during construction.

No salvage



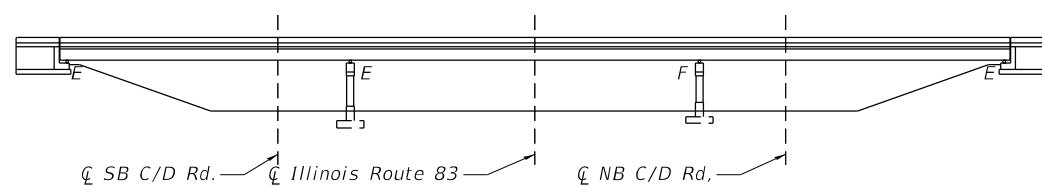
PLAN

DESIGN SPECIFICATIONS

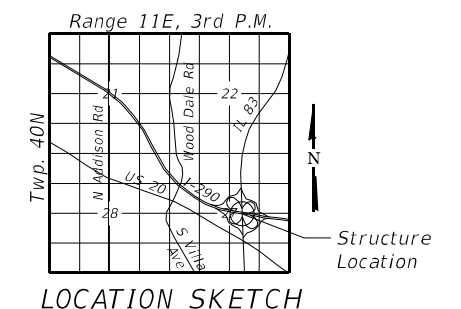
2002 AASHTO Standard Specifications for Highway Bridges, 17th Edition

SCOPE OF WORK

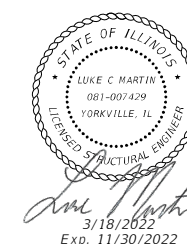
1. Reconstruct the East Abutment Backwall
2. Remove and replace East Approach Slab
3. Reconstruct the preformed joint strip seal deck joints at each abutment
4. Apply concrete sealer to deck surface and parapets, and to the newly constructed portions of the approach slab and backwall.
5. Repair cracks in Southwest wingwall.
6. Replace Bridge Approach Shoulders at East Abutment
7. Patch Approach Pavement Connector



ELEVATION



LOCATION SKETCH



GENERAL PLAN AND ELEVATION
I-290 EB OVER ILLINOIS ROUTE 83
DUPAGE COUNTY
STATION 205+44
STRUCTURE NO. 022-0096

MODEL: Default
 FILE NAME: D:\10422-5\11-022-0096-01-GPE



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PLOT DATE =	CHECKED - LM	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION
STRUCTURE NO. 022-0096

SHEET 1 OF 12 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	45
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

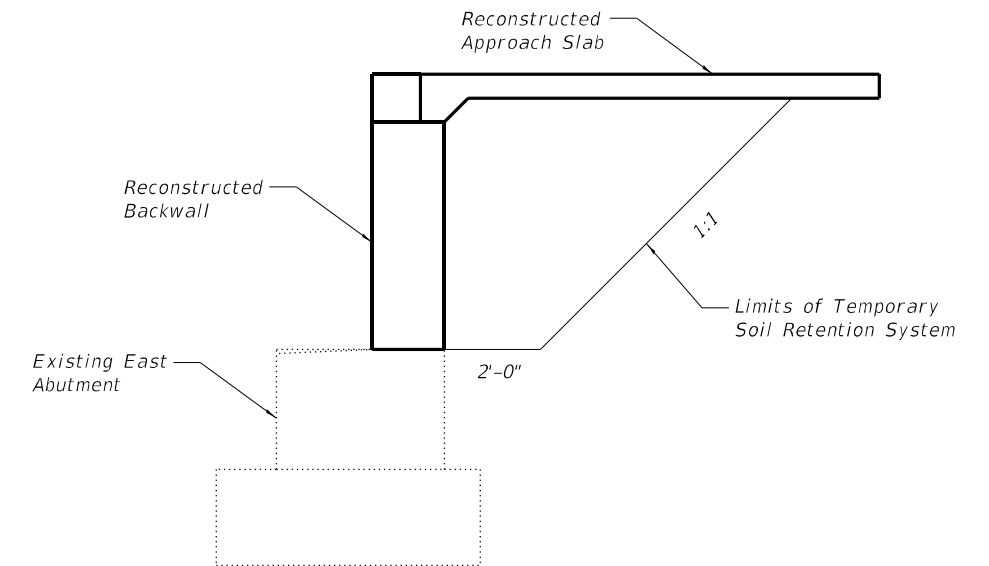
As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that cannot be removed by grinding 1/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.
3. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
4. Concrete Sealer shall be applied to the East abutment backwall. All surfaces to be sealed shall be cleaned thoroughly prior to sealer application. Cost included with Concrete Sealer.
5. The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
6. A cantilevered sheet piling design does not appear feasible and additional members or other retention systems may be necessary. The Contractor shall submit a temporary soil retention system design including plan details and calculations for review and acceptance by the Engineer.
7. Stage construction shall be utilized to maintain traffic during construction.
8. The Contractor shall exercise care during removal of existing joints to ensure that the slab, beams and diaphragms' integrity will not be detrimentally impacted. The Contractor shall repair any damage(s) to the slab, beams and diaphragms caused by his operation as directed by the Engineer at no additional cost to the Department.
9. Protective Coat shall be applied to the new top surface of the deck, the new East approach slab, and the top and inside vertical surfaces of the new parapet areas only per Section 503.19 of the Standard Specifications.
10. Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50°F.
11. Class D Patching has been included in the contract for replacing a six foot long segment of pavement at the end of the Existing Approach Pavement Connector. Removal of the Existing Approach Pavement Connector will be paid as Pavement Removal.

INDEX OF SHEETS

1. General Plan and Elevation
2. General Data
3. Stage Construction Details
4. Temporary Concrete Barrier for Stage Construction
5. Abutment Expansion Joint Repair Plan
6. Abutment Expansion Joint Repair Details
7. East Abutment Back Wall
8. Southwest Wingwall Repair
9. East Approach Slab Details (1 of 2)
10. East Approach Slab Details (2 of 2)
11. Preformed Joint Strip Seal
12. Bar Splicer Assembly and Mechanical Splicer Details

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Pavement Removal	Sq. Yd.	64		64
Paved Shoulder Removal	Sq. Yd.	40		40
Class A Patches, Type II, 10 Inch	Sq. Yd.	12		12
Class A Patches, Type IV, 10 Inch	Sq. Yd.	28		28
Concrete Removal	Cu. Yd.	88.3	17.3	105.6
Concrete Structures	Cu. Yd.		17.3	17.3
Concrete Superstructure	Cu. Yd.	21.6		21.6
Bridge Deck Grooving	Sq. Yd.	205		205
Protective Coat	Sq. Yd.	211		211
Concrete Superstructure (Approach Slab)	Cu. Yd.	66.7		66.7
Reinforcement Bars, Epoxy Coated	Pound	25,840	1300	27,140
Bar Splicers	Each	258	20	278
Preformed Joint Strip Seal	Foot	140		140
Temporary Soil Retention System	Sq. Ft.	45		45
Concrete Sealer	Sq. Ft.		311	311
Epoxy Crack Injection	Foot	11		11
Class D Patches, Type IV, 10 Inch (Special)	Sq. Yd.	64		64



TEMPORARY SOIL RETENTION SYSTEM

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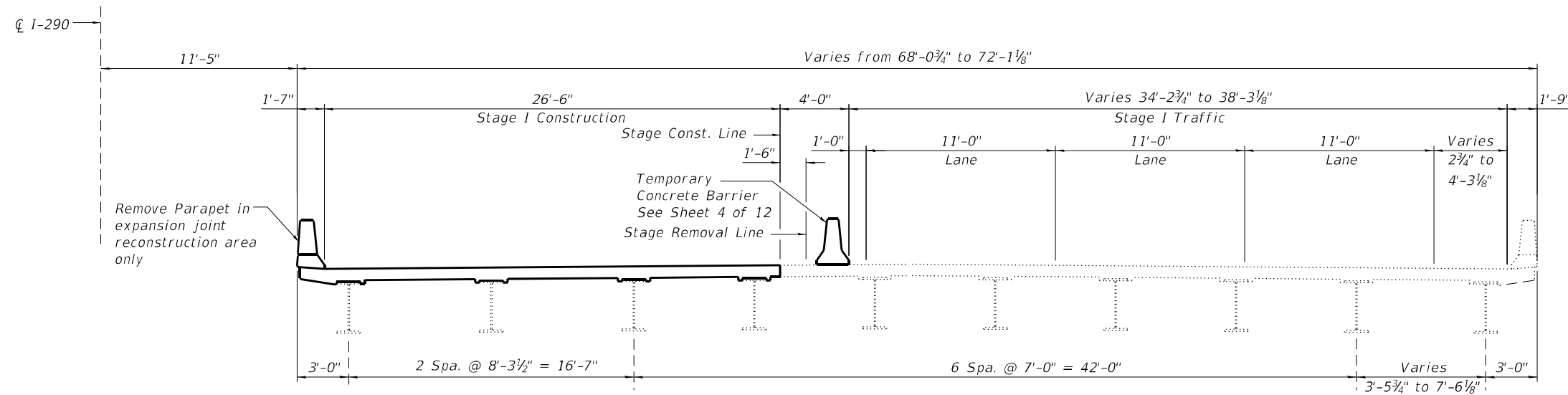
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

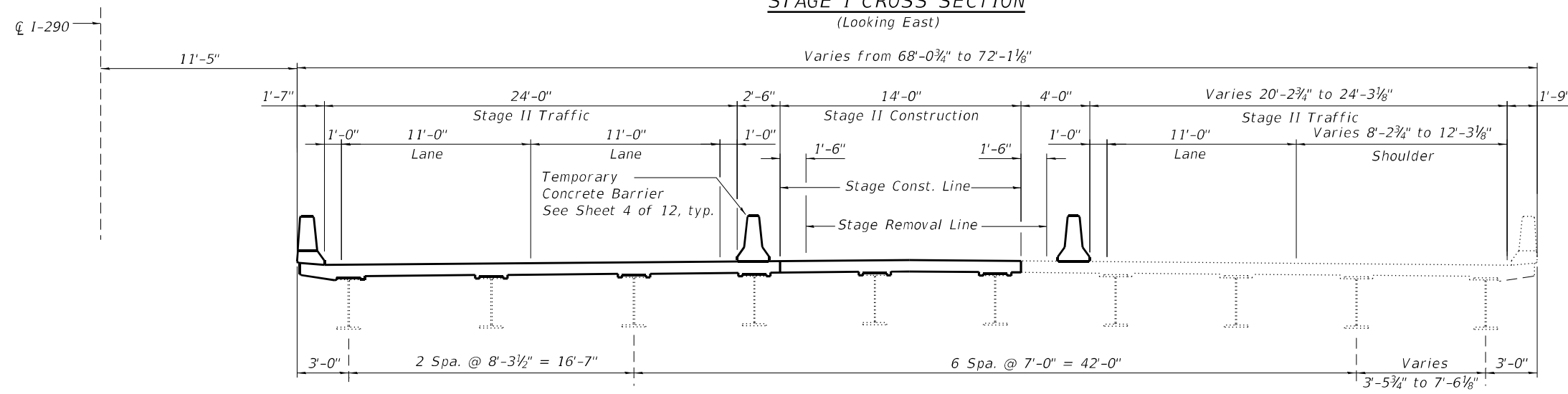
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STRUCTURE NO. 022-0096**

SHEET 2 OF 12 SHEETS

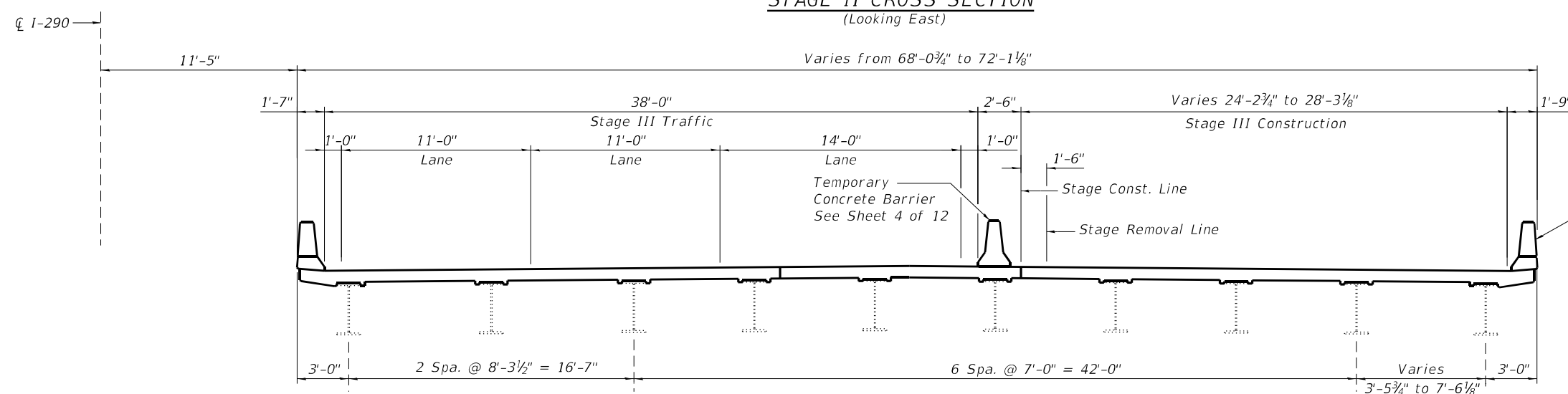
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290	2021-140-BR	DUPAGE	77	46
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				



STAGE I CROSS SECTION
(Looking East)



STAGE II CROSS SECTION
(Looking East)



STAGE III CROSS SECTION
(Looking East)

Remove Parapet in expansion joint reconstruction area only

NOTE:
For Quantity of Temporary Concrete Barrier, See Roadway Plans.

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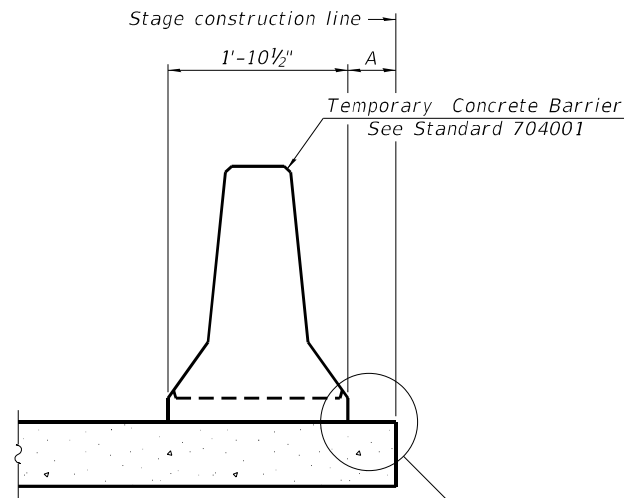
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 022-0096

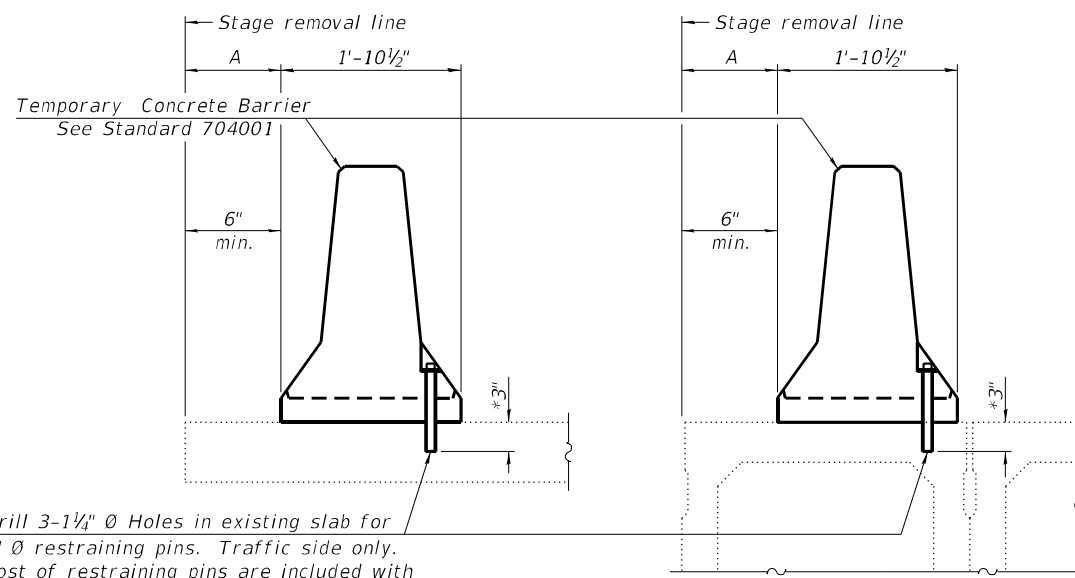
SHEET 3 OF 12 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	47
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM

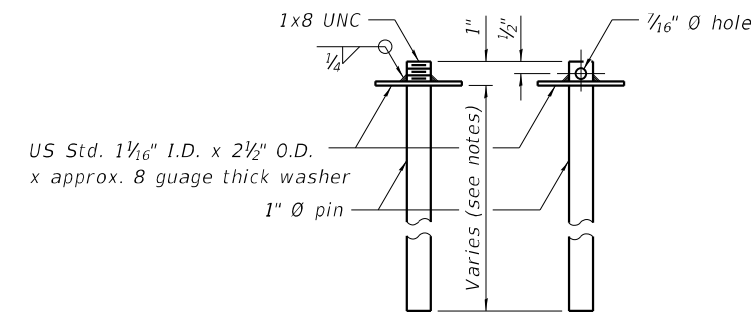


Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

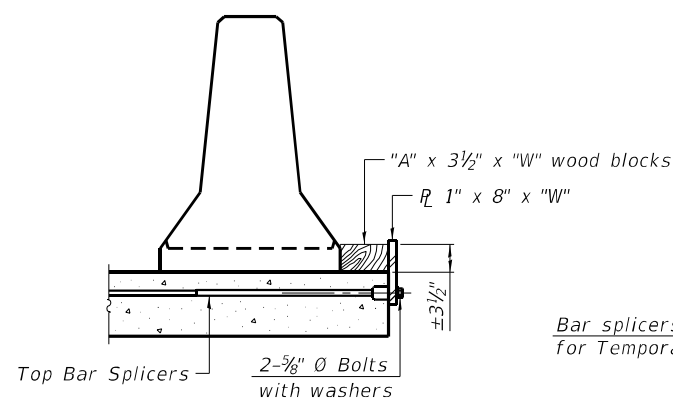
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM



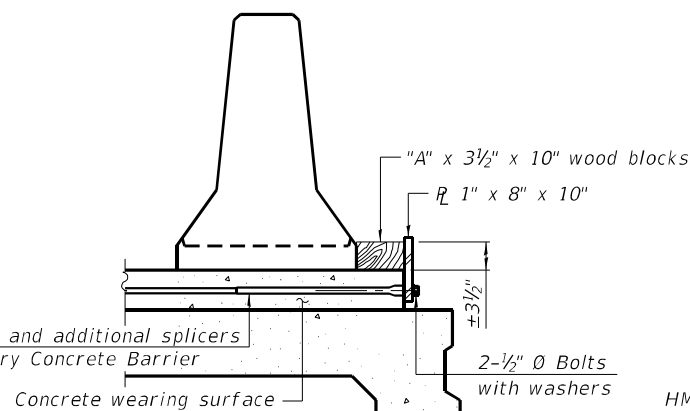
RESTRAINING PIN

SECTIONS THRU SLAB OR DECK BEAM

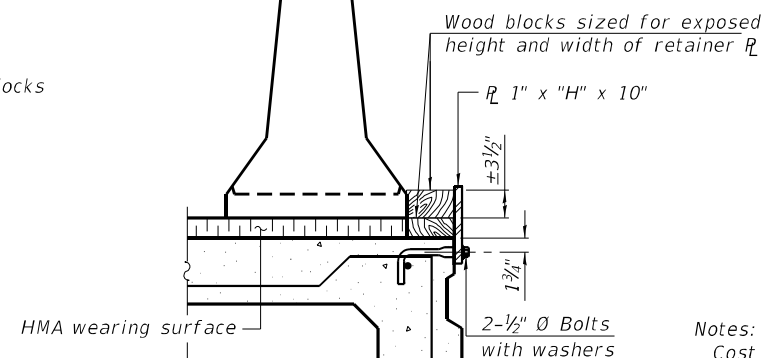


DETAIL I

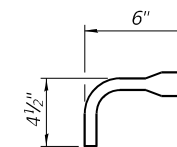
Bar splicers and additional splicers for Temporary Concrete Barrier



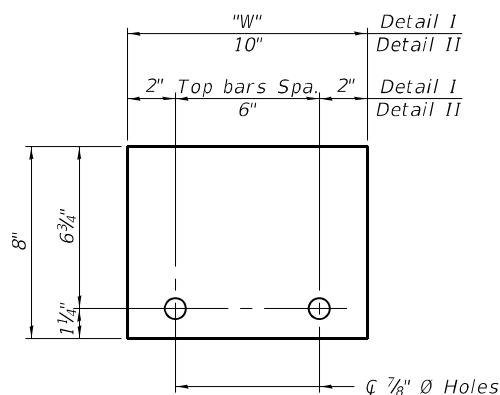
DETAIL II



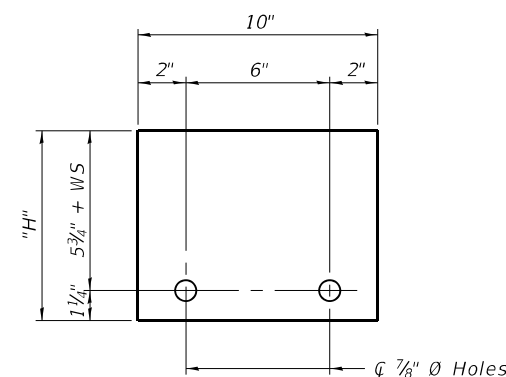
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER R 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate $\frac{1}{2}$ of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

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R-27 2-17-2017



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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

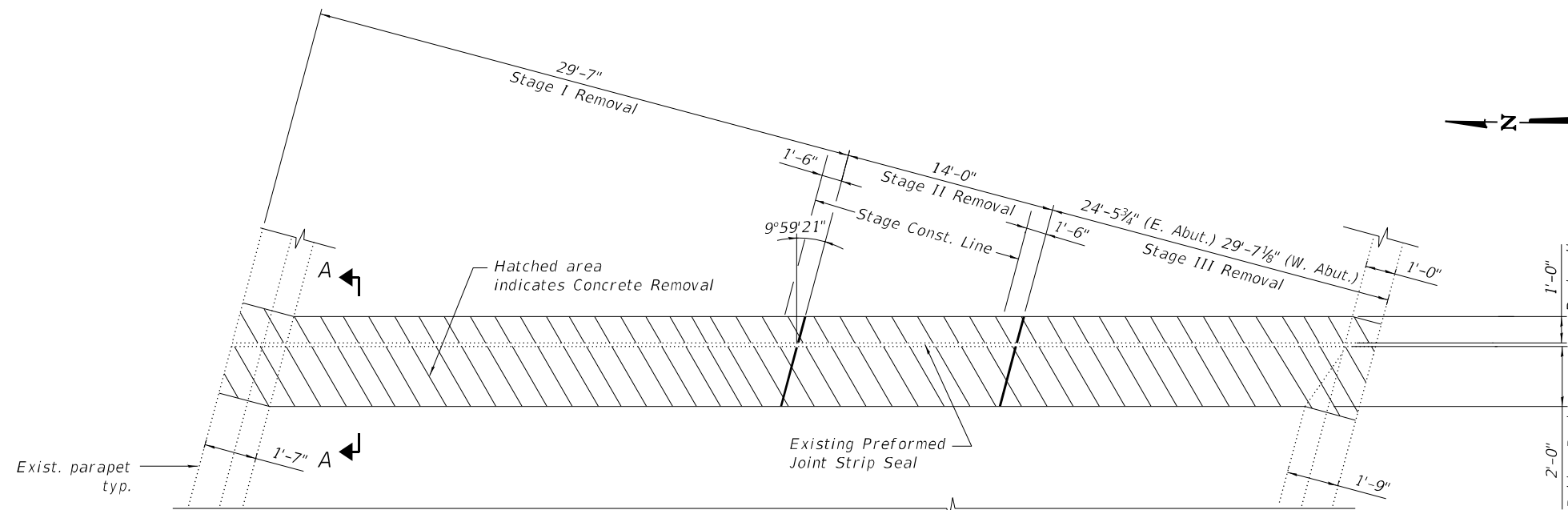
TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
 STRUCTURE NO. 022-0096

SHEET 4 OF 12 SHEETS

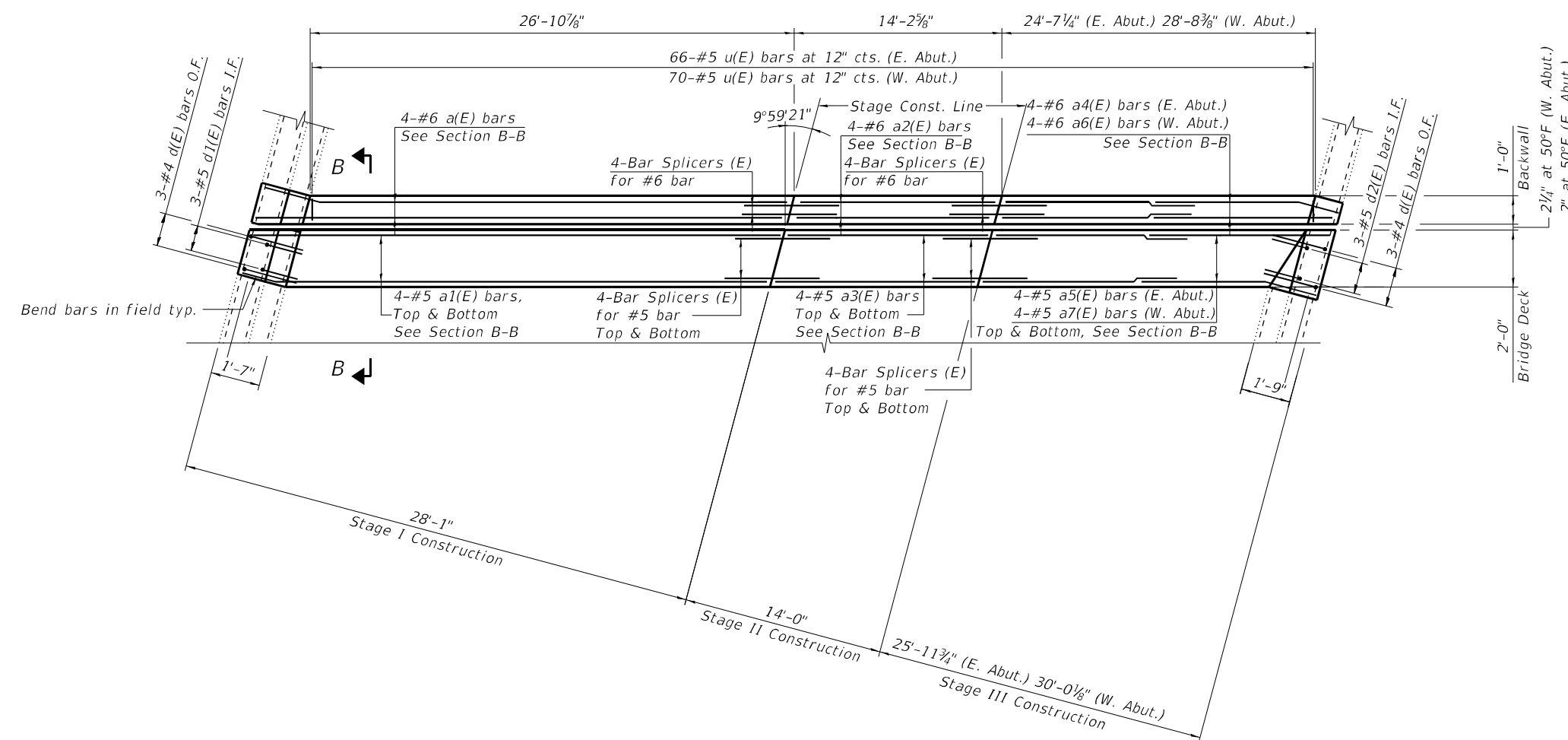
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290	2021-140-BR	DUPAGE	77	48
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				

BILL OF MATERIAL

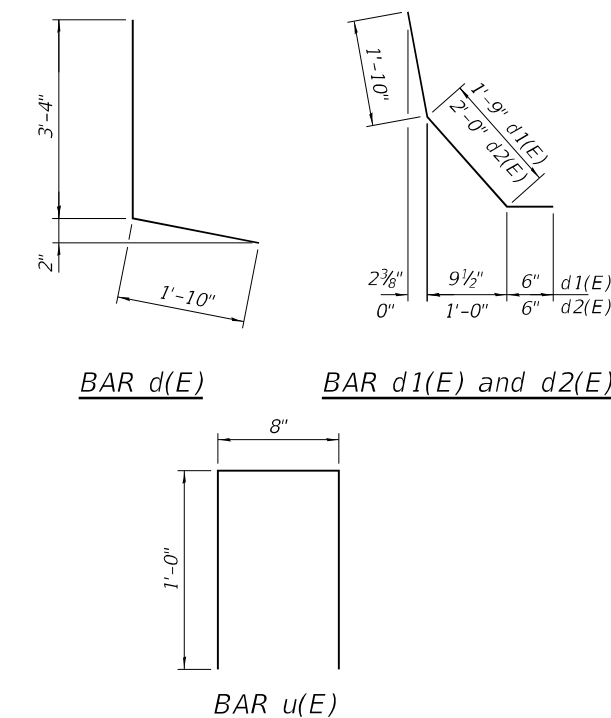
Bar	No.	Size	Length	Shape
a(E)	8	#6	28'-2"	—
a1(E)	16	#5	28'-2"	—
a2(E)	8	#6	13'-10"	—
a3(E)	16	#5	13'-10"	—
a4(E)	4	#6	26'-0"	—
a5(E)	8	#5	26'-0"	—
a6(E)	4	#6	30'-1"	—
a7(E)	8	#5	30'-1"	—
d(E)	12	#4	5'-2"	┌
d1(E)	6	#5	4'-1"	└
d2(E)	6	#5	4'-4"	└
u(E)	136	#5	2'-8"	▢
Item	Unit	Total		
Concrete Removal	Cu. Yd.	21.6		
Concrete Superstructure	Cu. Yd.	21.6		
Bridge Deck Grooving	Sq. Yd.	45		
Protective Coat	Sq. Yd.	51		
Reinforcement Bars, Epoxy Coated	Pound	2,490		



EXISTING PARTIAL PLAN AT EAST ABUTMENT
(Opposite Hand for West Abutment)



PROPOSED PARTIAL PLAN AT EAST ABUTMENT
(Opposite Hand for West Abutment)



NOTES:

1. I.F. denotes Inside Face.
O.F. denotes Outside Face.
2. See Sheet 6 of 12 for Sections A-A and B-B.

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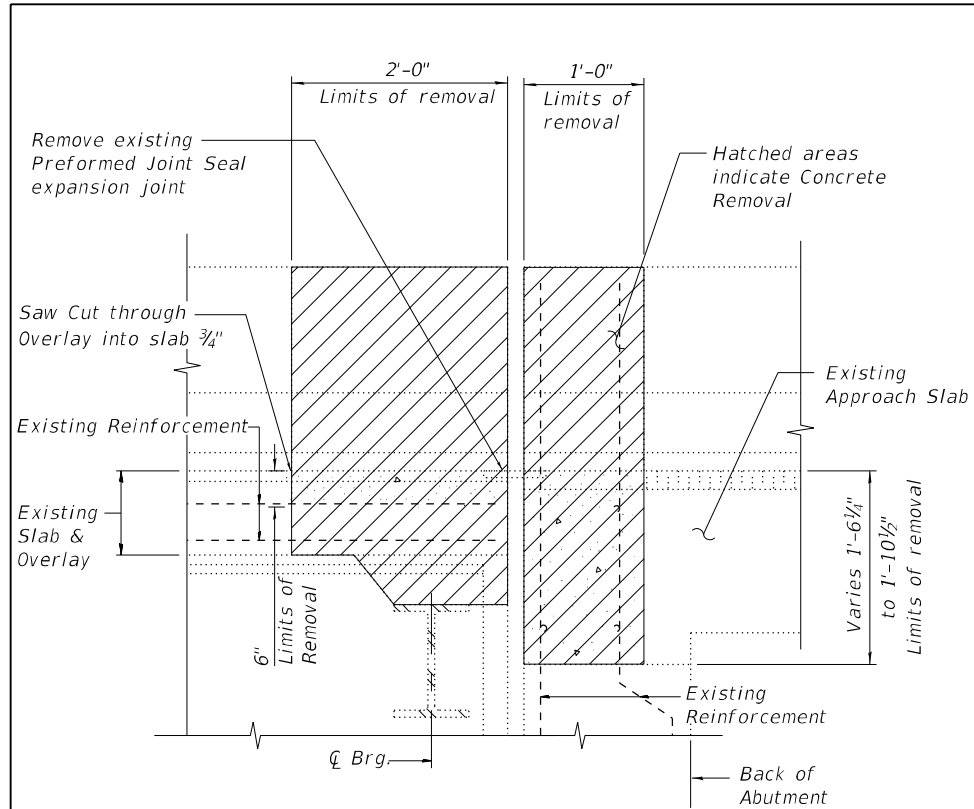
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

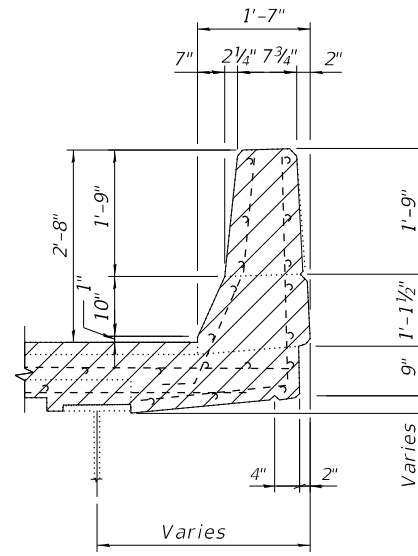
**ABUTMENT EXPANSION JOINT REPAIR PLAN
STRUCTURE NO. 022-0096**

SHEET 5 OF 12 SHEETS

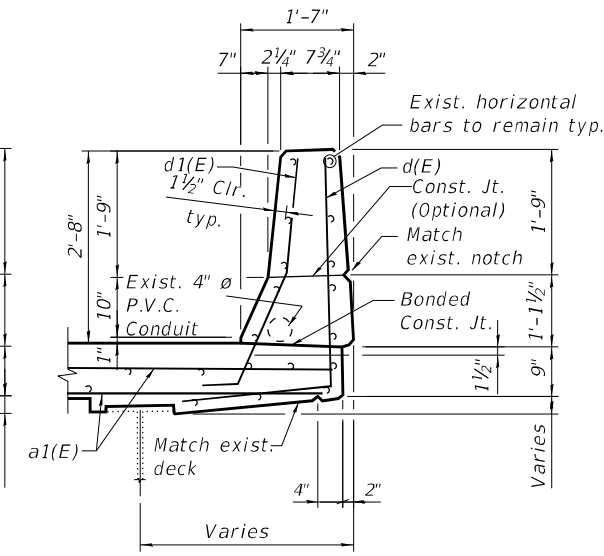
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290	2021-140-BR	DUPAGE	77	49
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				



SECTION A-A



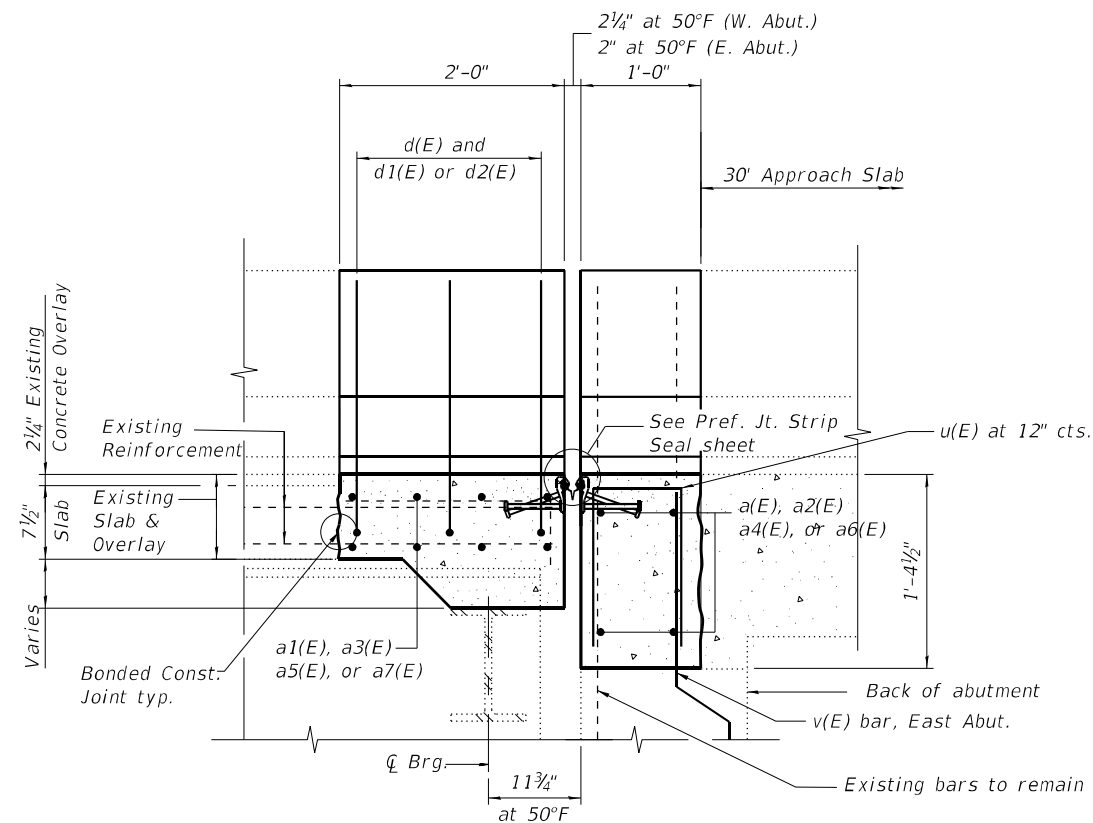
EXISTING INSIDE PARAPET SECTION



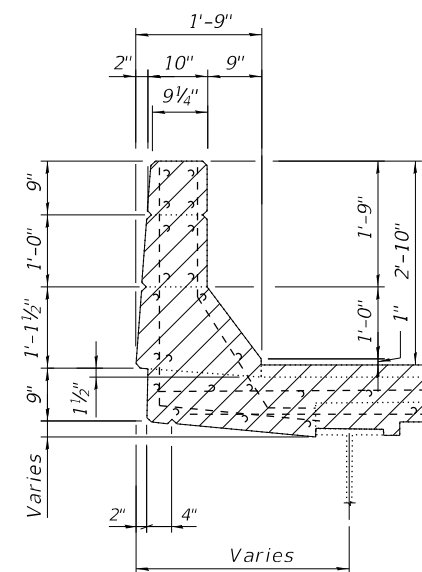
PROPOSED INSIDE PARAPET SECTION

NOTES:

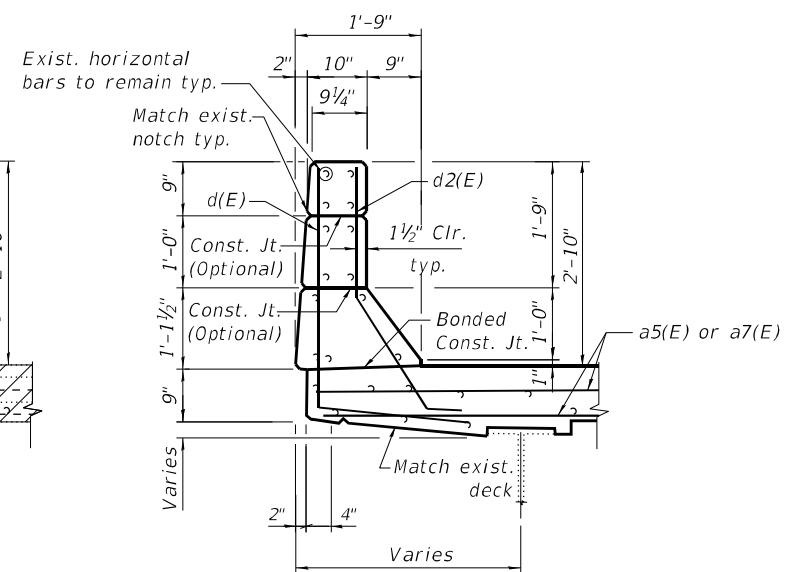
- Existing reinforcement bars extending into the concrete removal area shall be cleaned, straightened and incorporated into the new construction. Cost included with Concrete Removal. Any reinforcement bars that are damaged during concrete removal shall be repaired or replaced with an approved bar splicer or anchorage system at no additional cost to the Department.
- Existing reinforcement bars in the concrete removal area parallel to the expansion joints shall be removed.
- Removal and disposal of the existing expansion joints will not be paid for separately, but shall be included with the cost of Concrete Removal.
- If existing name plate falls within the limits of Concrete Removal, it shall be removed and reinstalled in its original location in accordance with IDOT Std. 515001. Cost included with Concrete Superstructure.
- If existing guardrail and/or end shoe fall within the limits of Concrete Removal, they shall be removed and reinstalled in their original location in accordance with District 1 Std. BM-21. Cost included with Concrete Superstructure.
- The Contractor shall exercise extreme care with the existing conduits in sections of the parapet to be removed and to protect and support the conduit to the satisfaction of the Engineer. No splicing will be allowed to any cable damage resulting from this work, instead the Contractor will be required to repair the entire span of any damaged cable at no additional cost to the Department.
- See Sheet 5 of 12 for Bar Bending Details and Bill of Material.



SECTION B-B



EXISTING OUTSIDE PARAPET SECTION



PROPOSED OUTSIDE PARAPET SECTION

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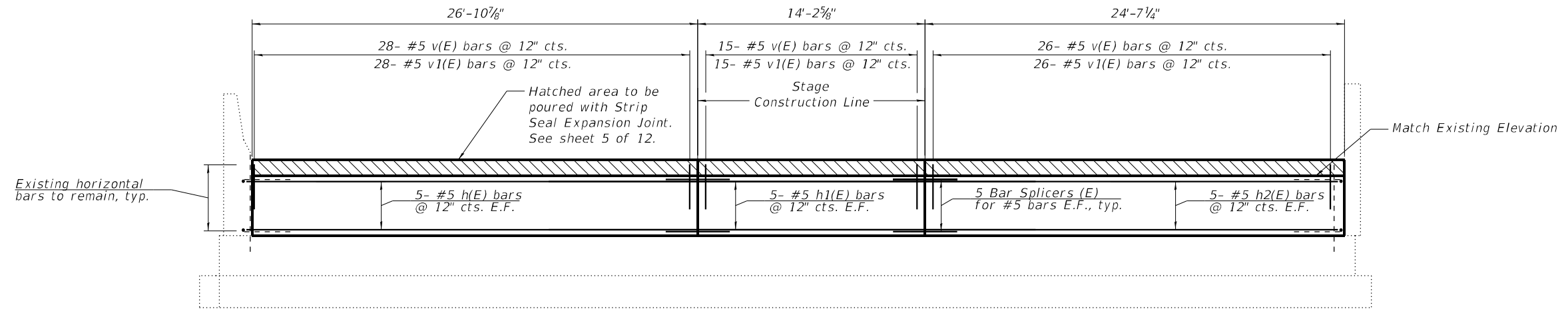
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ABUTMENT EXPANSION JOINT REPAIR DETAILS
STRUCTURE NO. 022-0096

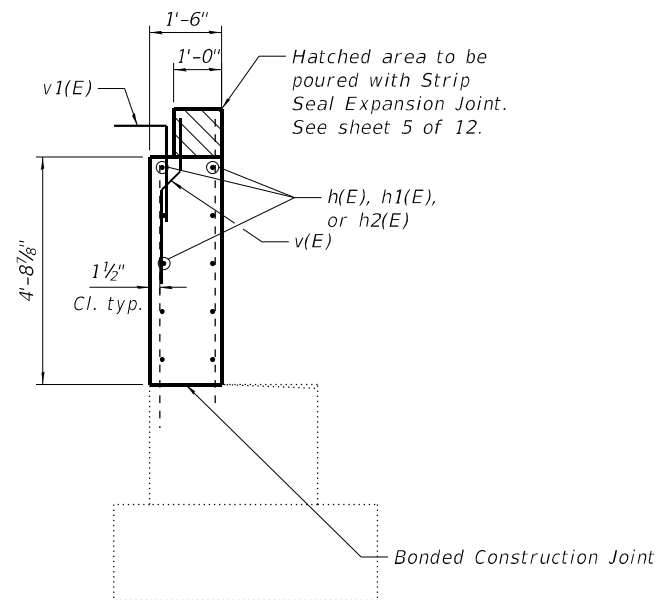
SHEET 6 OF 12 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	50
CONTRACT NO. 62P58				

ILLINOIS FED. AID PROJECT



EAST ABUTMENT REPAIR
(Looking East)



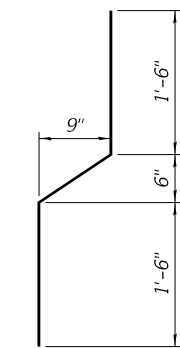
SECTION THRU EAST ABUTMENT

NOTES:

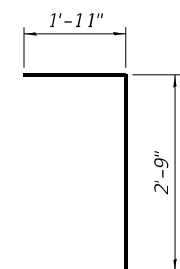
- The Contractor shall exercise extreme care with the existing conduits located near the repair areas. The Contractor will be required to repair any damage done to the conduit to the satisfaction of the Engineer, at no additional cost to the Department. No splicing will be allowed to any cable damage resulting from this work, instead the Contractor will be required to repair the entire span of any damaged cable at no additional cost to the Department.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	10	#5	26'-6"	—
h1(E)	10	#5	13'-10"	—
h2(E)	10	#5	24'-3"	—
v(E)	69	#5	3'-11"	⌋
v1(E)	69	#5	4'-8"	⌋
Item	Unit	Total		
Concrete Removal	Cu. Yd.	17.3		
Concrete Structures	Cu. Yd.	17.3		
Reinforcement Bars, Epoxy Coated	Pound	1300		
Concrete Sealer	Sq. Ft.	311		



BAR v(E)



BAR v1(E)

MODEL: Default
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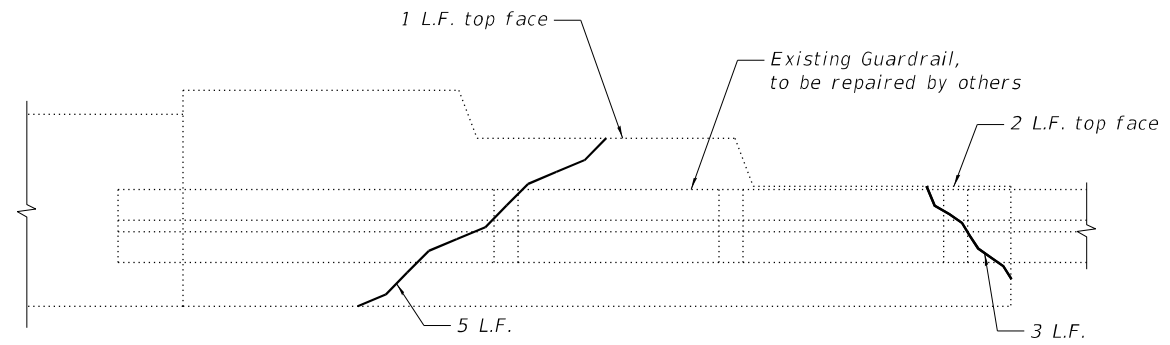
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EAST ABUTMENT BACK WALL
STRUCTURE NO. 022-0096**

SHEET 7 OF 12 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	51
CONTRACT NO. 62P58				

ILLINOIS FED. AID PROJECT



SOUTHWEST WINGWALL REPAIR
(Looking South)

BILL OF MATERIAL

SYMBOL	ITEM	UNIT	QUANTITY
~~~~~	Epoxy Crack Injection	Foot	11

MODEL: Default  
FILE NAME: D:\101422-shl-022-0096-08-5WingwallRepair



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

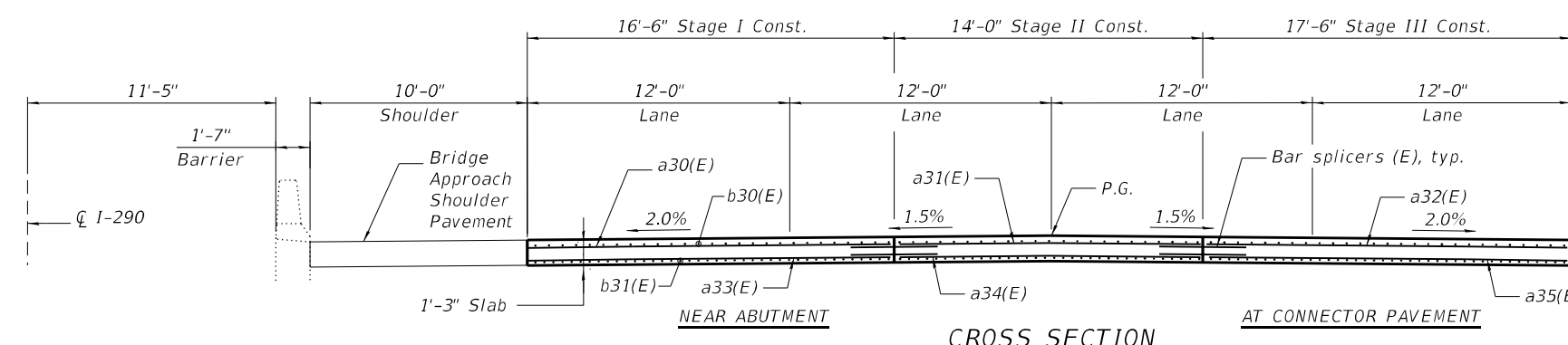
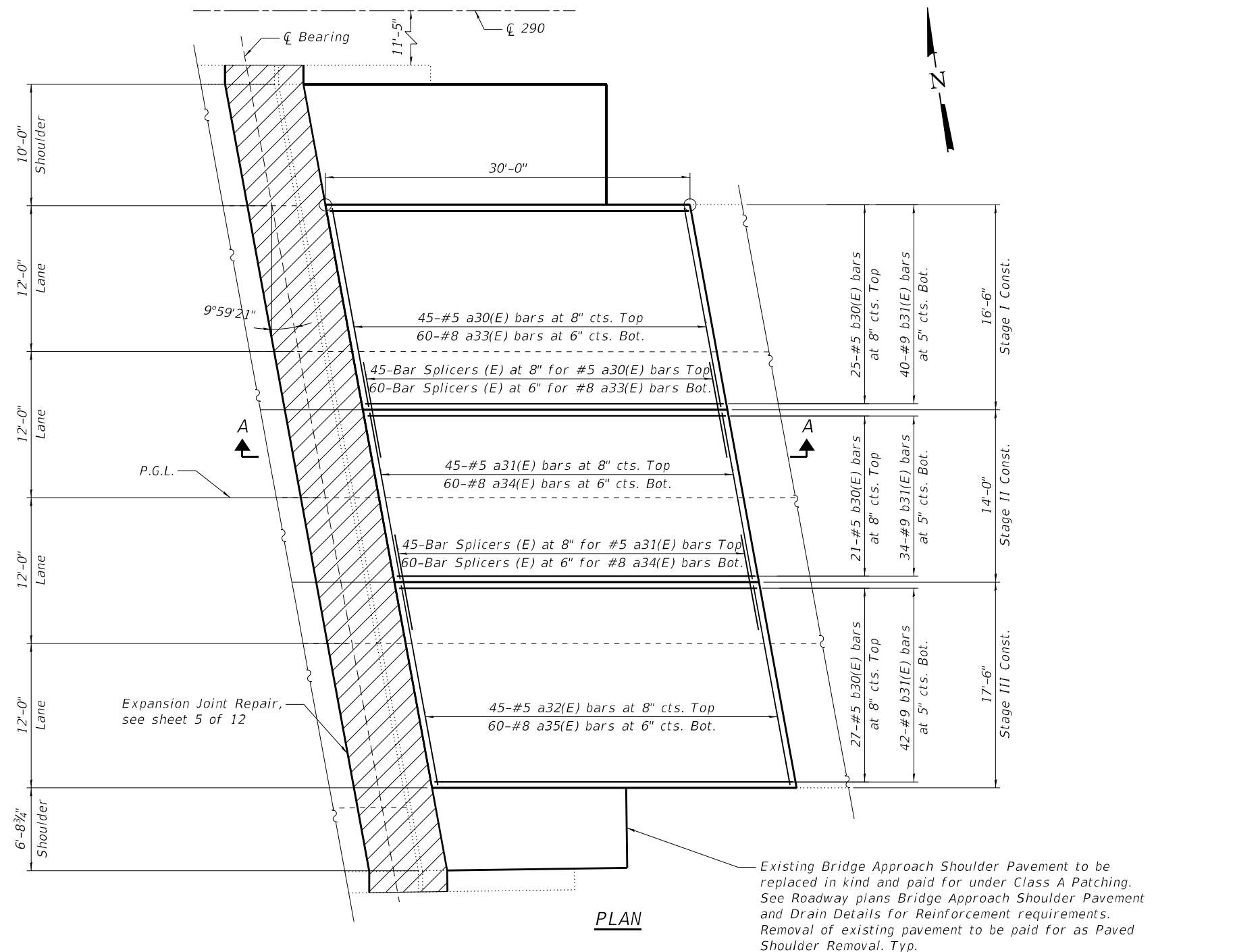
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STRUCTURE NO. 022-0096**

SHEET 8 OF 12 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	52
			CONTRACT NO. 62P58	
		ILLINOIS FED. AID PROJECT		

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a30(E)	45	#5	16'-4"	—
a31(E)	45	#5	13'-10"	—
a32(E)	45	#5	17'-5"	—
a33(E)	60	#8	16'-4"	—
a34(E)	60	#8	13'-10"	—
a35(E)	60	#8	17'-5"	—
b30(E)	73	#5	28'-8"	—
b31(E)	116	#9	28'-8"	—
Item	Unit	Total		
Paved Shoulder Removal	Sq. Yd.	40		
Class A Patches, Type II, 10 Inch	Sq. Yd.	12		
Class A Patches, Type IV, 10 Inch	Sq. Yd.	28		
Concrete Removal	Cu. Yd.	66.7		
Concrete Superstructure (Approach Slab)	Cu. Yd.	66.7		
Bridge Deck Grooving	Sq. Yd.	160		
Protective Coat	Sq. Yd.	160		
Reinforcement Bars, Epoxy Coated	Pound	23,350		



(Sheet 1 of 2)

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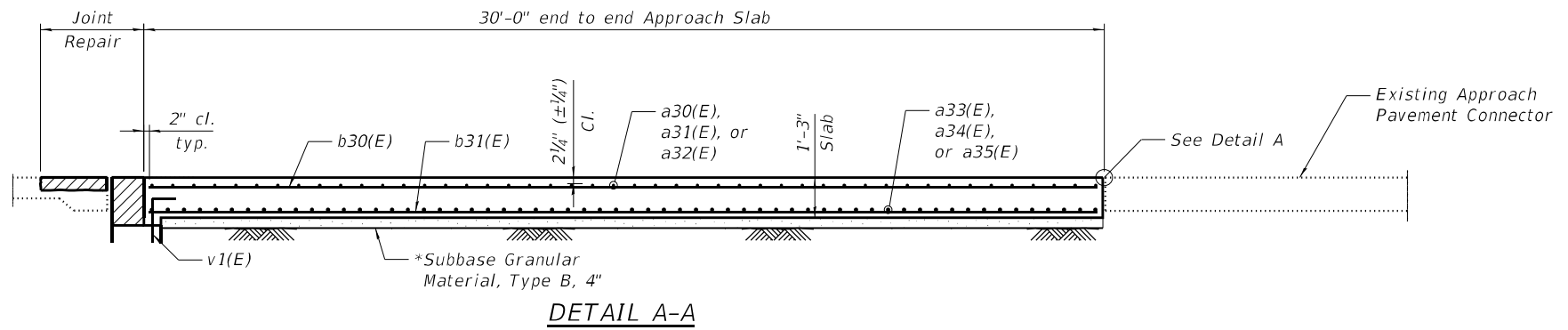
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EAST APPROACH SLAB DETAILS  
STRUCTURE NO. 022-0096**

SHEET 9 OF 12 SHEETS

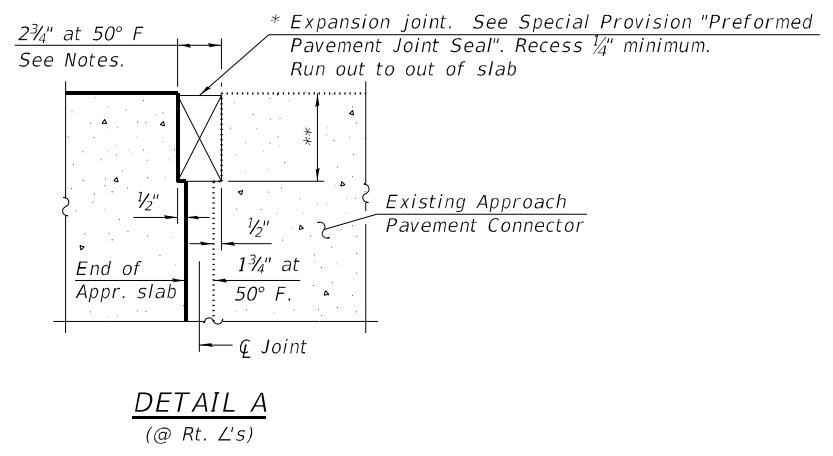
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290	2021-140-BR	DUPAGE	77	53
CONTRACT NO. 62P58				

ILLINOIS FED. AID PROJECT



**NOTES:**

1. Approach slab shall be paid for as Concrete Superstructure (Approach Slab).
2. See sheet 5 of 12 for hatched block details.
3. The given dimensions and cross slopes come from existing plans and should be verified in the field by the Contractor. The top of slab elevations shall be set to match the existing conditions.



* Cost included with Concrete Superstructure (Approach Slab).  
 ** Per manufacturer recommendations

(Sheet 2 of 2)

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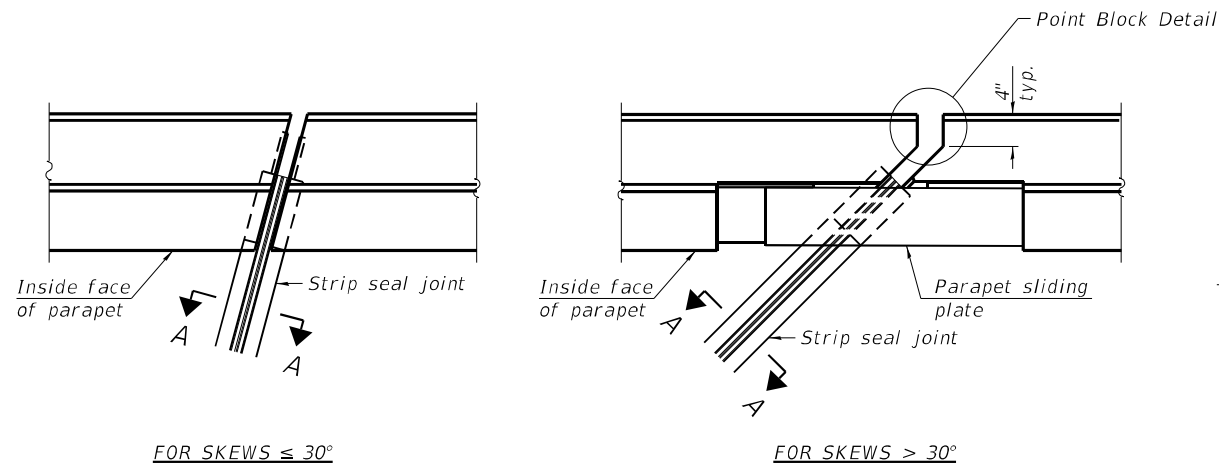
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

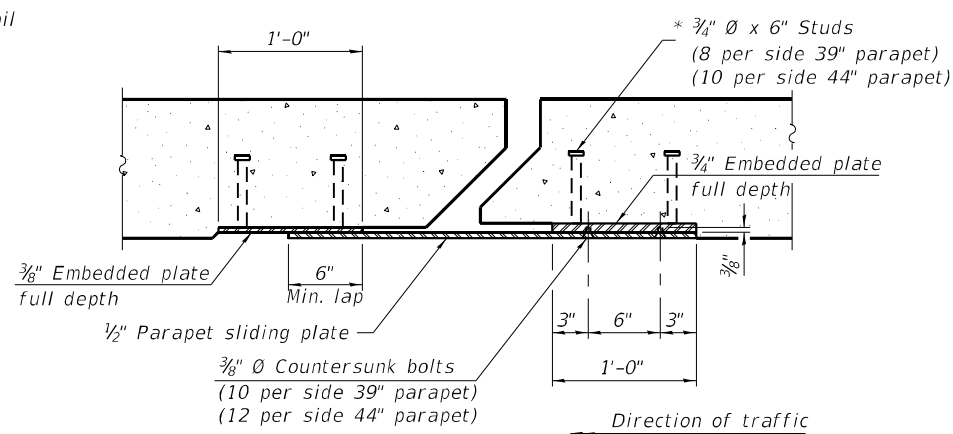
**EAST APPROACH SLAB DETAILS  
STRUCTURE NO. 022-0096**

SHEET 10 OF 12 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 62P58	
		ILLINOIS	FED. AID PROJECT	

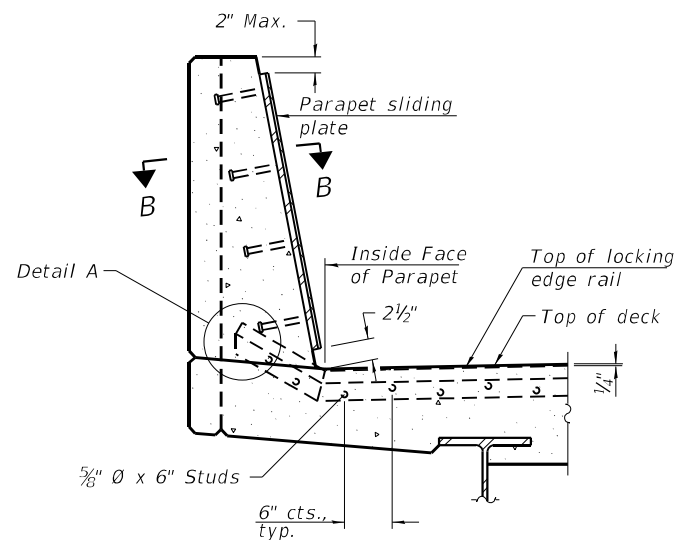


PLAN AT PARAPET



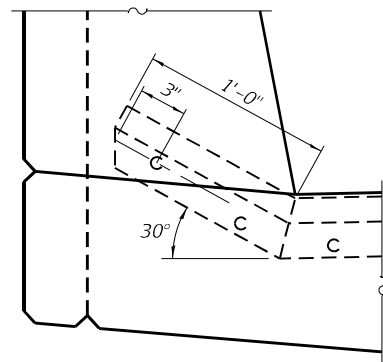
SECTION B-B

Notes:  
The strip seal shall be made continuous and shall have a minimum thickness of  $\frac{1}{4}$ ". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.  
The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the  $4\frac{1}{2}$ " maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.  
The manufacturer's recommended installation methods shall be followed.  
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.  
The Maximum space between locking edge rail segments shall be  $\frac{3}{16}$ " and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.  
Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.  
39" constant slope barrier shown, 44" constant slope barrier similar as noted.  
The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

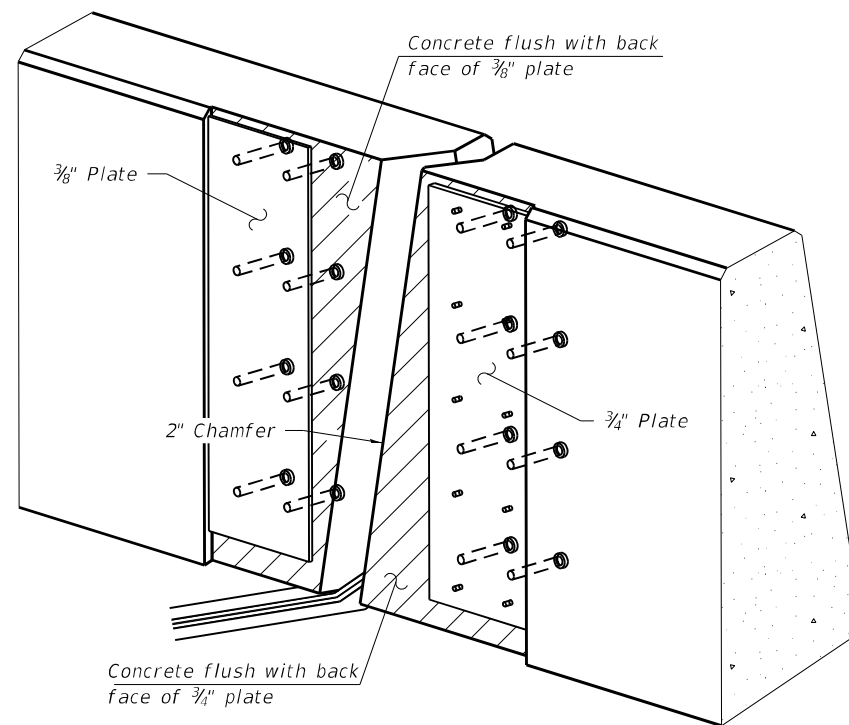


SECTION AT PARAPET

(Skews  $> 30^\circ$  shown. Skews  $\leq 30^\circ$  similar except as shown in plan view.)

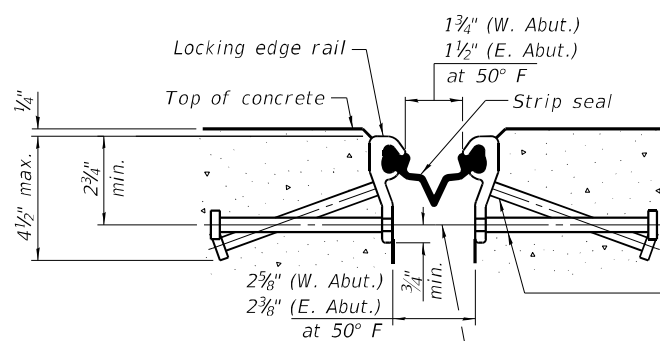


DETAIL A



TRIMETRIC VIEW

(Showing embedded plates only)



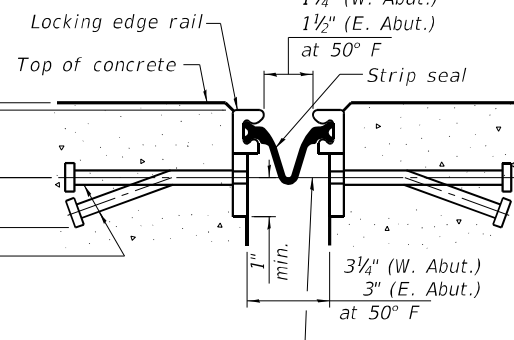
SHOWING ROLLED RAIL JOINT

*  $\frac{3}{8}$ "  $\emptyset$  x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

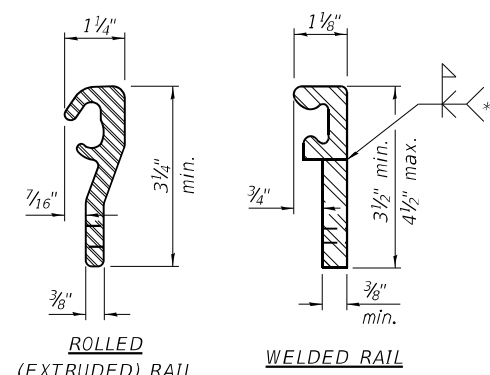
$\frac{3}{8}$ "  $\emptyset$  threaded rods in  $\frac{1}{8}$ "  $\emptyset$  holes at  $\pm 4$ -0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

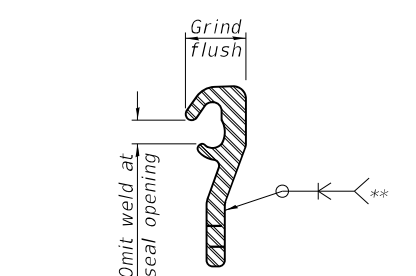


SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	140

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1-1-2020



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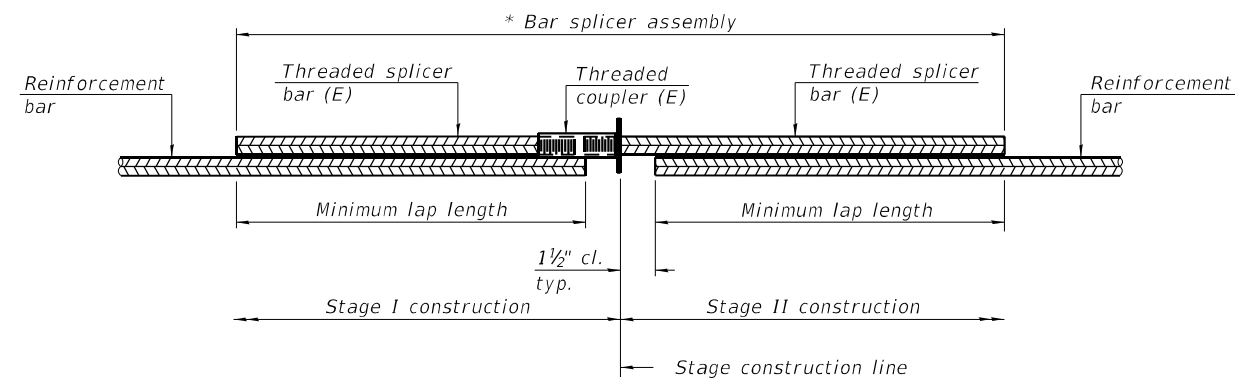
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL  
STRUCTURE NO. 022-0096

SHEET 11 OF 12 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	55
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				

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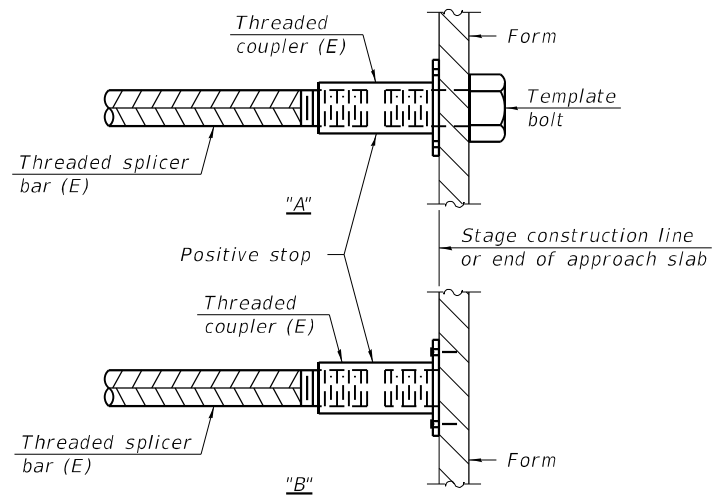


**STANDARD BAR SPLICER ASSEMBLY PLAN**  
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Expansion Joints	#5	32	3'-6"
Expansion Joints	#6	16	4'-0"
Backwall	#5	20	3'-6"
Approach Slab	#5	90	3'-8"
Approach Slab	#8	120	7'-8"

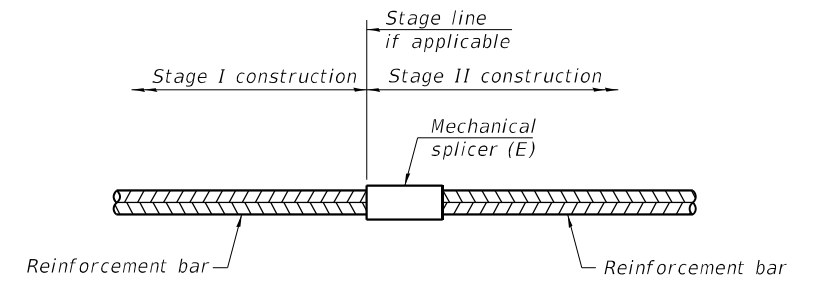


**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required

**Notes:**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
STRUCTURE NO. 022-0096

SHEET 12 OF 12 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	56
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				

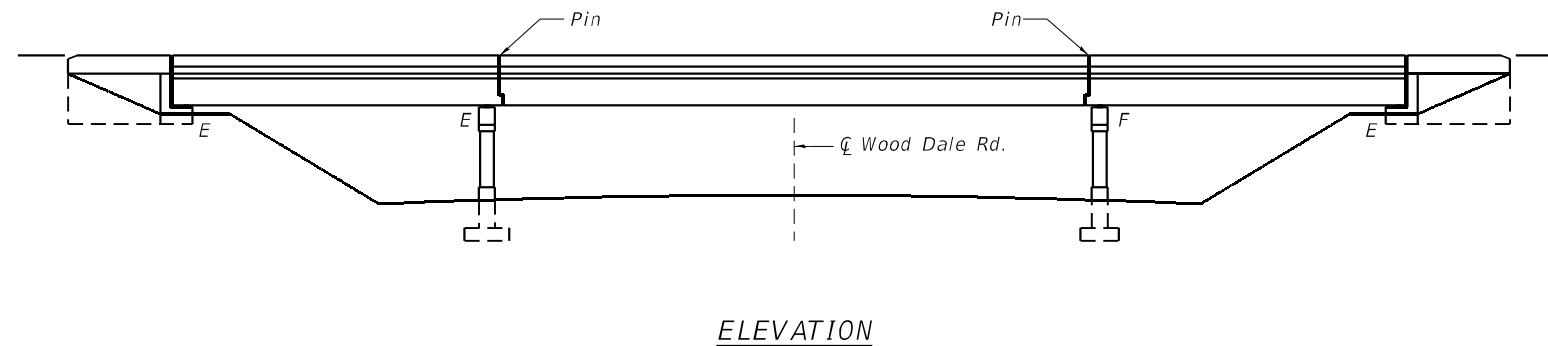
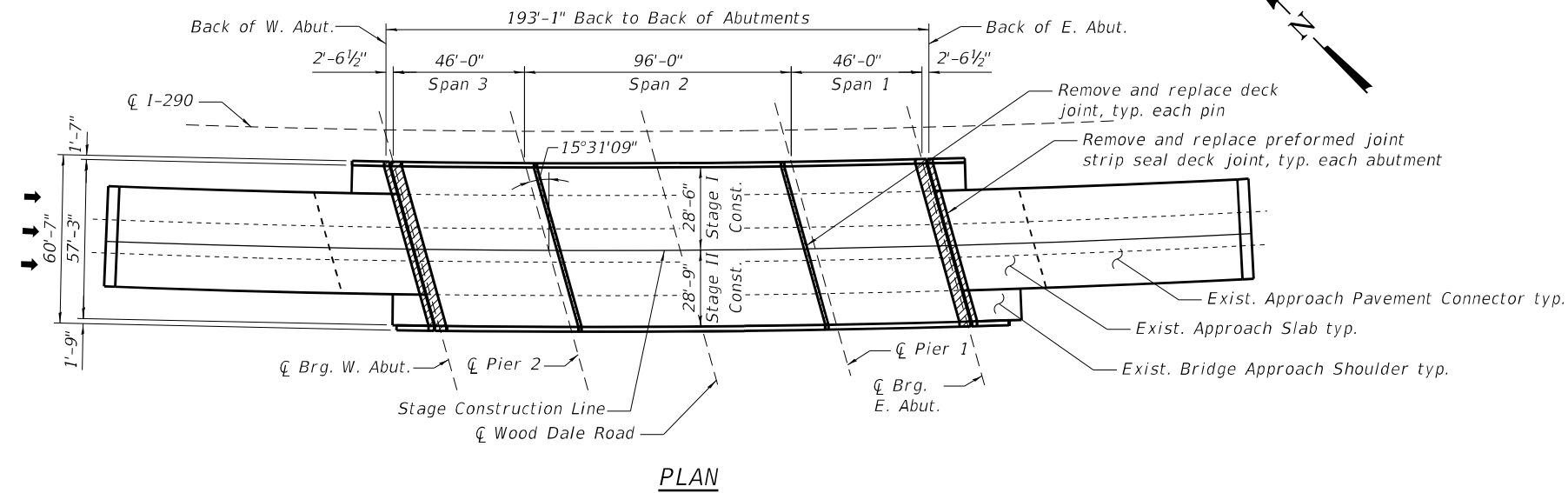


**Existing Structure:**

The bridge is a three-span continuous, plate girder bridge with a 8-inch reinforced concrete deck and a 2-inch concrete overlay. The original structure was built in 1971 as FAI-290, and the structure is in Section 1984-079-BW. In 1985, the structure was widened, patched and overlaid, the expansion joints were reconstructed, and the superstructure was cleaned and painted. In 1998, the deck and approaches were patched, the expansion joints were replaced, and the parapets were repaired. In 2010, the concrete overlay and the deck joints were replaced.

Stage construction shall be utilized to maintain traffic during construction.

No salvage.

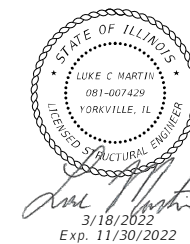
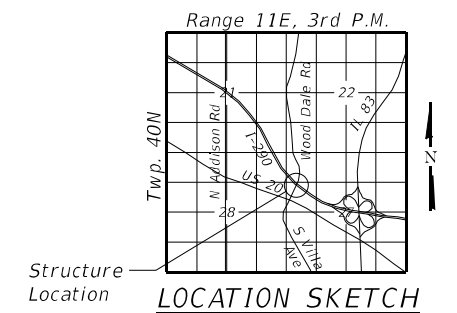


**DESIGN SPECIFICATIONS**

2002 AASHTO Standard Specifications for Highway Bridges, 17th Edition

**SCOPE OF WORK**

1. Repair bridge deck.
2. Reconstruct the preformed joint strip seal deck joints at each abutment.
3. Reconstruct the deck joints at pin connections.
4. Repair Backwall.
5. Apply Protective Coat to deck surface and parapets.
6. Patch Approach Pavement Connector



**GENERAL PLAN AND ELEVATION  
I-290 EB OVER WOOD DALE ROAD  
DuPAGE COUNTY  
STATION 174+61  
STRUCTURE NO. 022-0105**

MODEL: Default  
FILE NAME: D:\10422-5\11-022-0105-01-GPE



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN AND ELEVATION  
STRUCTURE NO. 022-0105**

SHEET 1 OF 10 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	57
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				

**GENERAL NOTES**

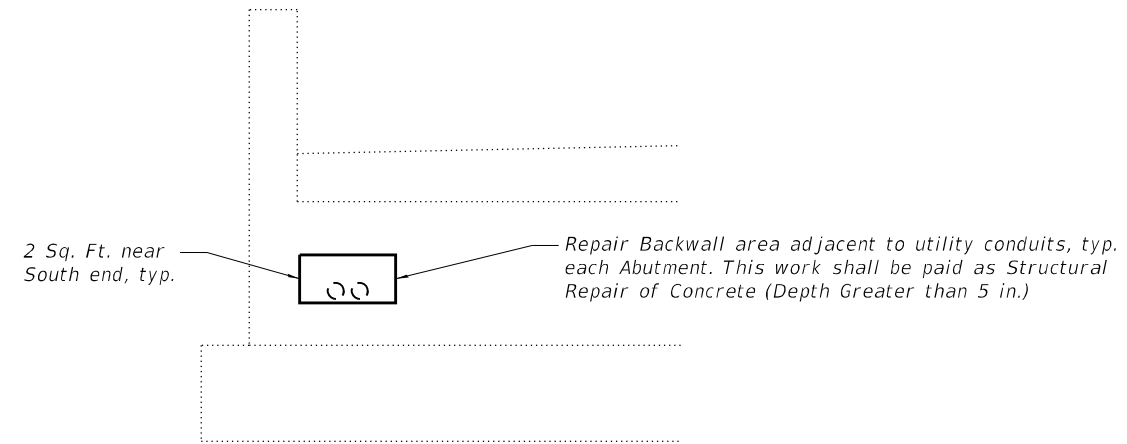
- Reinforcement bars designated (E) shall be epoxy coated.
- Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.  
  
As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that cannot be removed by grinding 1/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.
- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
- Stage construction shall be utilized to maintain traffic during construction.
- The Contractor shall exercise care during removal of existing joints to ensure that the slab, beams and diaphragms' integrity will not be detrimentally impacted. The Contractor shall repair any damage(s) to the slab, beams and diaphragms caused by his operation as directed by the Engineer at no additional cost to the Department.
- Protective Coat shall be applied to the new top surface of the deck and the top and inside vertical surfaces of the new parapet areas only per Section 503.19 of the Standard Specifications.
- Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50°F.
- The lanes of traffic and sidewalks along Wood Dale Road shall be temporarily closed while joint and deck repair work is occurring overhead. See Roadway plans for details.
- Class D Patching has been included in the contract for replacing a six foot long segment of pavement at the end of the Existing Approach Pavement Connector. Removal of the Existing Approach Pavement Connector will be paid as Pavement Removal.

**INDEX OF SHEETS**

- General Plan and Elevation
- General Data
- Stage Construction Details
- Temporary Concrete Barrier for Stage Construction
- Deck Repair Plan
- Abutment Expansion Joint Repair Plan
- Abutment Expansion Joint Repair Details
- Pin Joint Repair Plan and Details
- Preformed Joint Strip Seal
- Bar Splicer Assembly and Mechanical Splicer Details

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	SUPER	SUB	TOTAL
Pavement Removal	Sq. Yd.	48		48
Concrete Removal	Cu. Yd.	27.8		27.8
Concrete Superstructure	Cu. Yd.	27.8		27.8
Bridge Deck Grooving	Sq. Yd.	63		63
Protective Coat	Sq. Yd.	75		75
Reinforcement Bars, Epoxy Coated	Pound	2,250		2,250
Bar Splicers	Each	24		24
Preformed Joint Strip Seal	Foot	123		123
Class D Patches, Type III, 10 Inch (Special)	Sq. Yd.	48		48
Structural Repair of Concrete (Depth Greater than 5 in.)	Sq. Ft.		4	4
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	3		3
Deck Slab Repair (Partial)	Sq. Yd.	3		3



**ABUTMENT BACKWALL REPAIR**  
(West Abutment shown Looking West, East Abutment similar)

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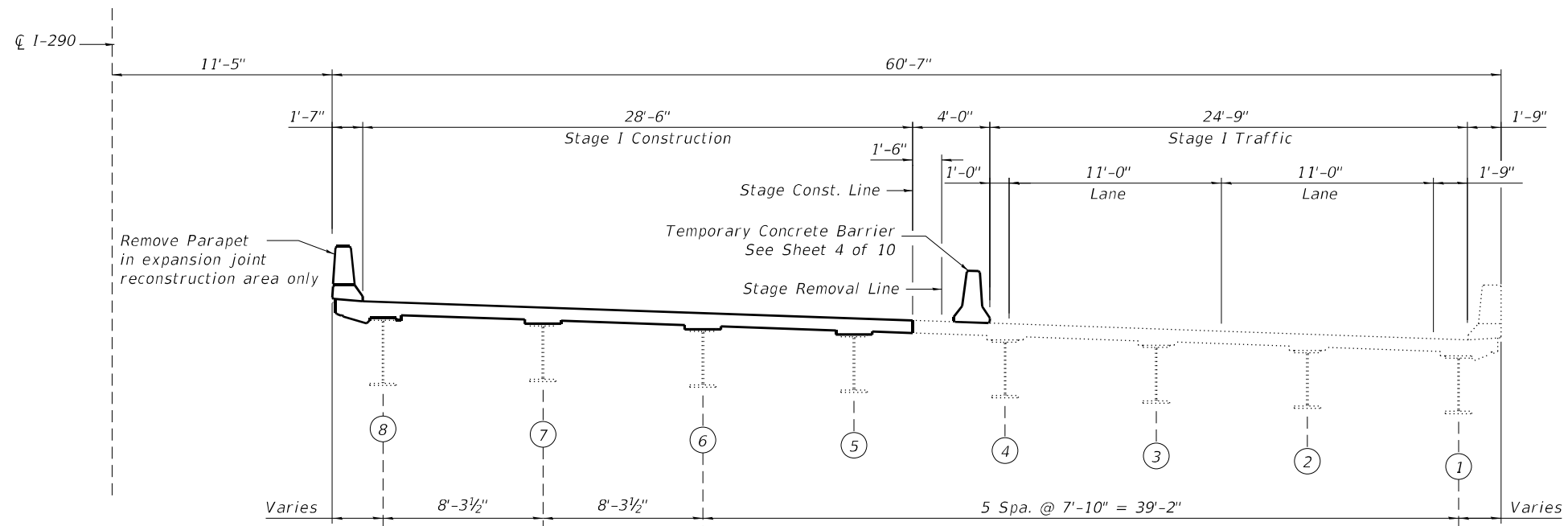
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

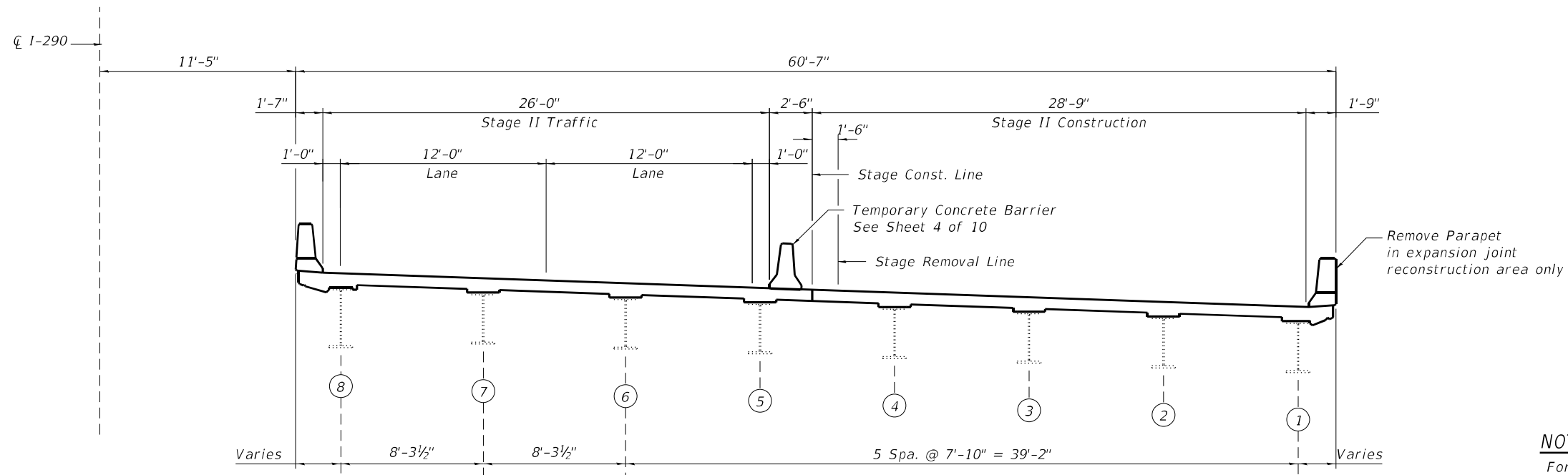
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STRUCTURE NO. 022-0105**

SHEET 2 OF 10 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	58
CONTRACT NO. 62P58				
ILLINOIS		FED. AID PROJECT		



**STAGE I CROSS SECTION**  
(Looking East)



**STAGE II CROSS SECTION**  
(Looking East)

**NOTE:**  
For Quantity of Temporary Concrete Barrier,  
See Roadway Plans

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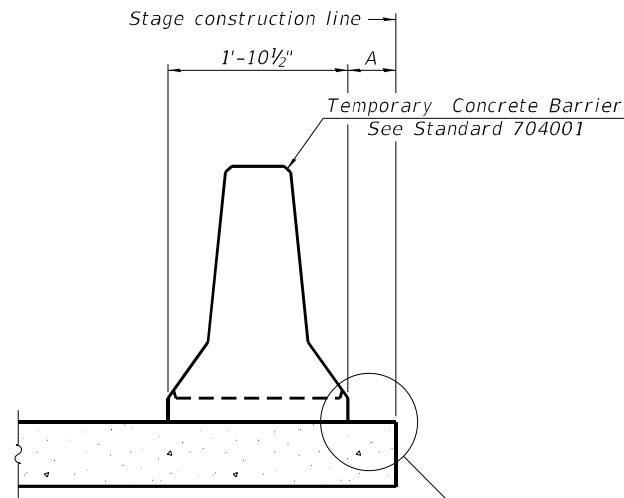
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PLOT SCALE =	DRAWN - JJM	REVISED -
PLOT DATE =	CHECKED - LM	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION STAGING DETAILS  
STRUCTURE NO. 022-0105

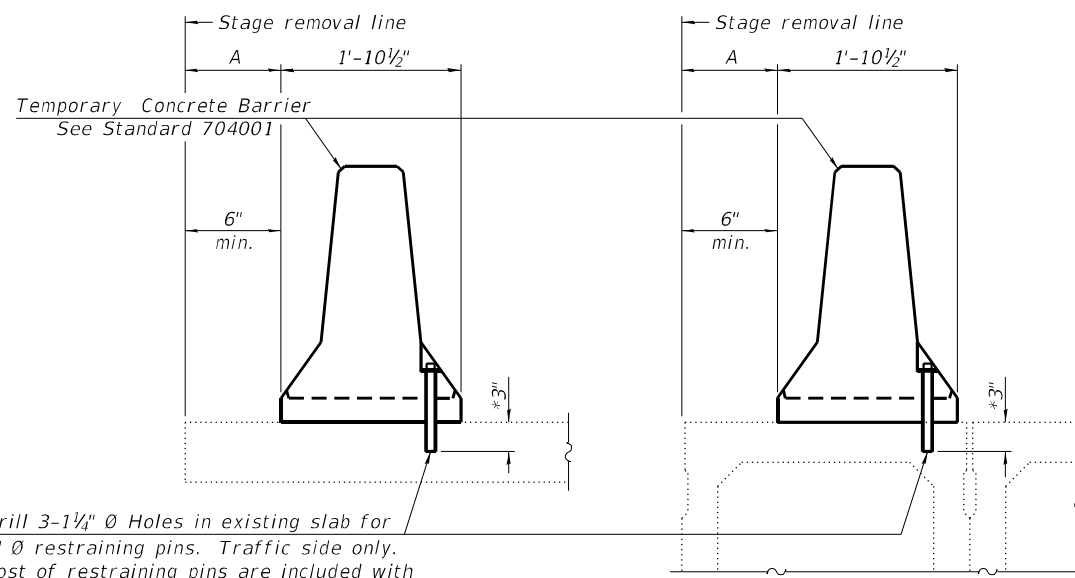
SHEET 3 OF 10 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	59
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

**NEW SLAB OR NEW DECK BEAM**

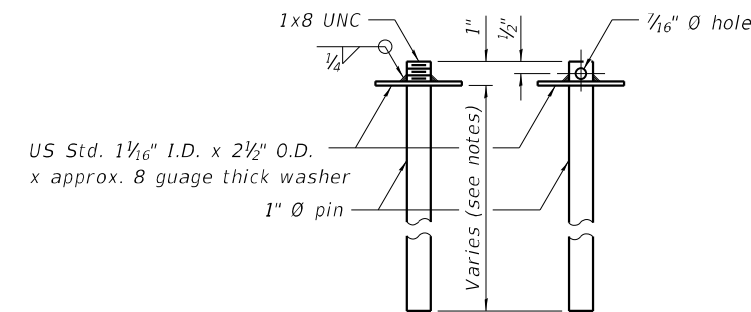


Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

**EXISTING SLAB**

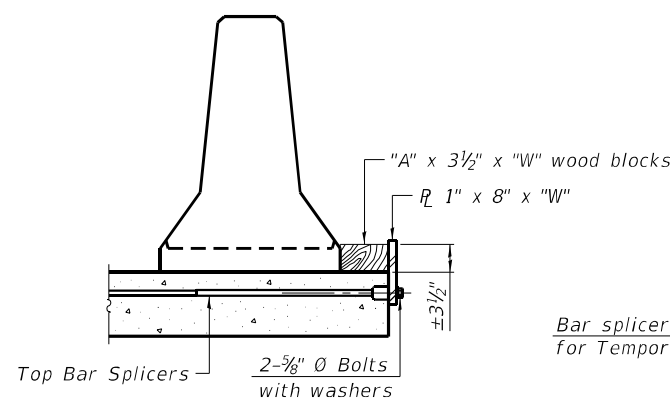
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

**EXISTING DECK BEAM**



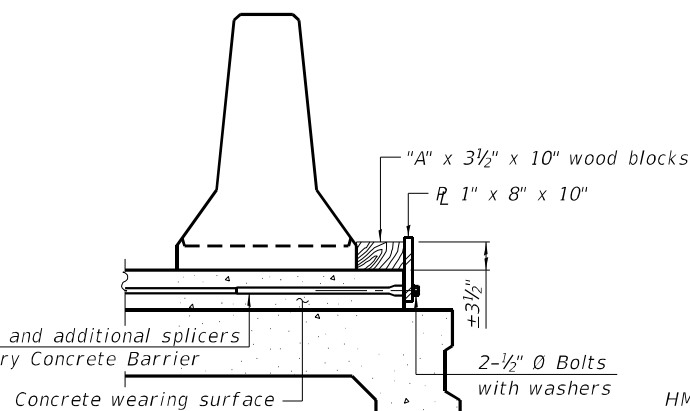
**RESTRAINING PIN**

**SECTIONS THRU SLAB OR DECK BEAM**

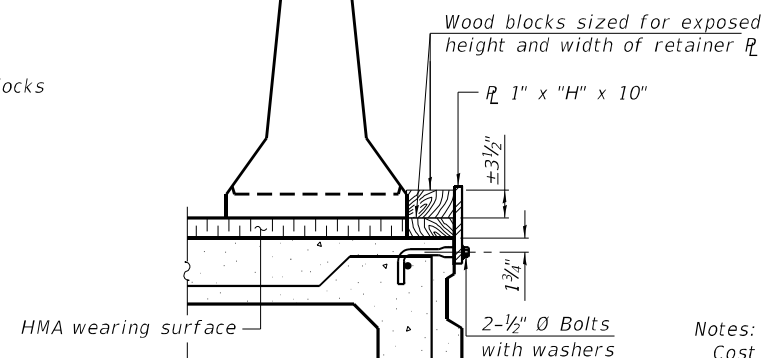


**DETAIL I**

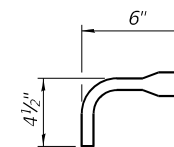
Bar splicers and additional splicers for Temporary Concrete Barrier



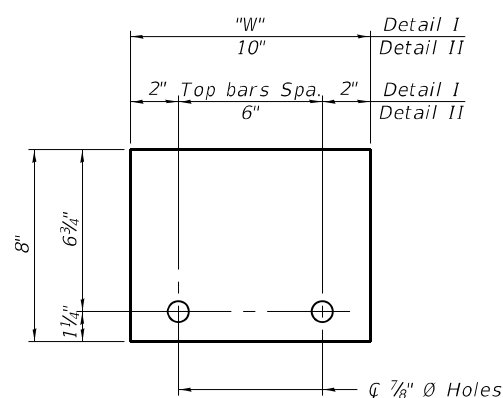
**DETAIL II**



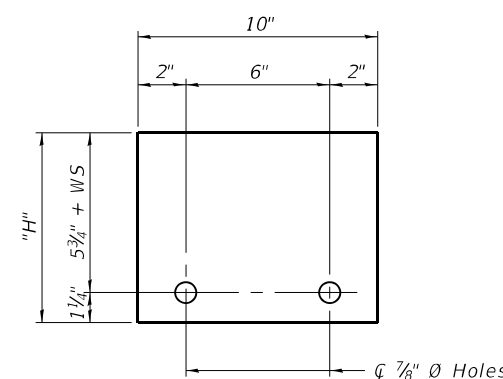
**DETAIL III**



**BAR SPLICER FOR #4 BAR - DETAIL III**



**STEEL RETAINER 1" x 8" x "W"**  
(Detail I and II)



**STEEL RETAINER 1" x "H" x 10"**  
(Detail III)

**Notes:**

Cost of retainer assembly is included with Temporary Concrete Barrier. A retainer assembly shall be located at the approximate center of each temporary concrete barrier.

The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.

When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6' to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.

Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.

Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

MODEL: Default  
FILE NAME: D:\10422-spl-022-0105-04-TempConcBarrierStd.dgn

R-27

2-17-2017



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PLOT DATE =	DRAWN - JJM	REVISED -
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

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

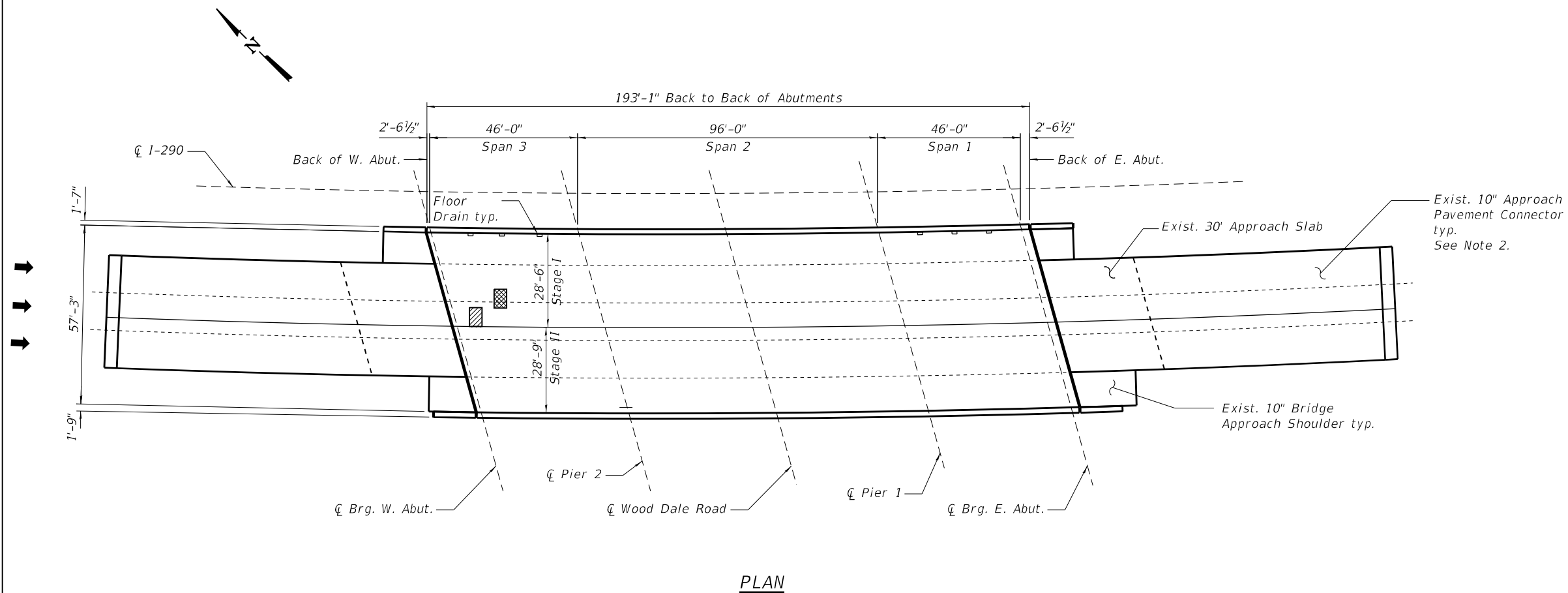
TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION  
STRUCTURE NO. 022-0105

SHEET 4 OF 10 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	60
CONTRACT NO. 62P58				
ILLINOIS		FED. AID PROJECT		

**BILL OF MATERIAL**

SYMBOL	ITEM	UNIT	QUANTITY
	Protective Coat	Sq. Yd.	6
	Deck Slab Repair (Full Depth - Type II)	Sq. Yd.	3
	Deck Slab Repair (Partial)	Sq. Yd.	3



**PLAN**

**NOTES:**

- Deck slab repair areas are estimated based on an inspection conducted by IDOT and the visual inspection conducted in November of 2021. Actual repair areas and locations shall be determined by the Engineer and shown on As-Built plans.
- The Engineer shall determine the type and quantity of Class D patching. Estimated quantities are included in the overall Summary of Quantities in the Roadway Plans.

MODEL: Default  
FILE NAME: D:\104225\104225-022-0105-05-Deck Repair



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	CHECKED - LM	REVISED -
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PLOT DATE =	CHECKED - LM	REVISED -

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DEPARTMENT OF TRANSPORTATION**

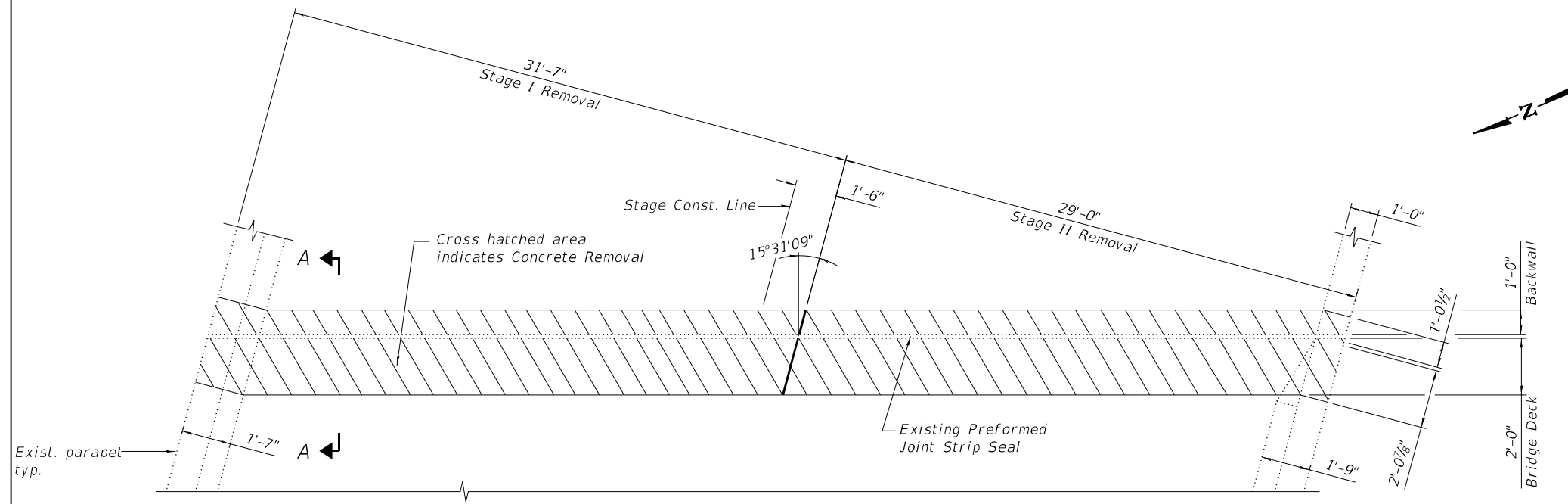
**DECK REPAIR PLAN  
STRUCTURE NO. 022-0105**

SHEET 5 OF 10 SHEETS

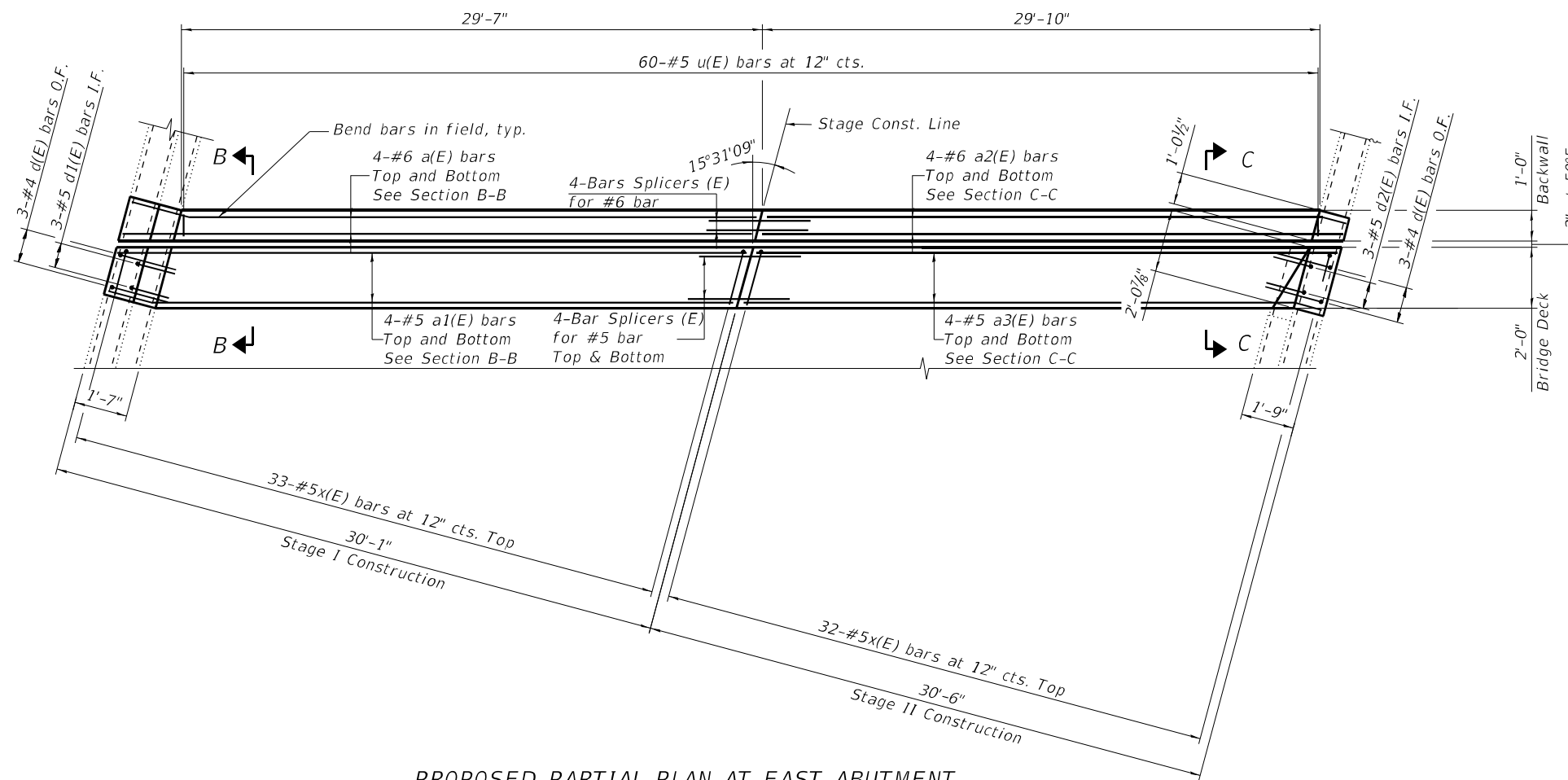
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	61
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				

**BILL OF MATERIAL**

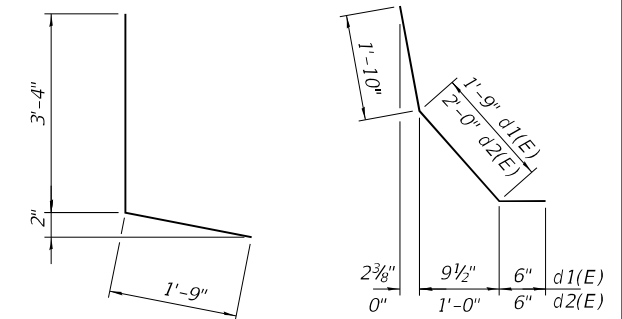
Bar	No.	Size	Length	Shape
a(E)	8	#6	31'-11"	—
a1(E)	16	#5	31'-11"	—
a2(E)	8	#6	31'-3"	—
a3(E)	16	#5	31'-3"	—
d(E)	12	#4	5'-1"	┌
d1(E)	6	#5	4'-1"	└
d2(E)	6	#5	4'-4"	└
u(E)	120	#5	2'-8"	□
Item	Unit	Total		
Concrete Removal	Cu. Yd.	19.5		
Concrete Superstructure	Cu. Yd.	19.5		
Bridge Deck Grooving	Sq. Yd.	40		
Protective Coat	Sq. Yd.	46		
Reinforcement Bars, Epoxy Coated	Pound	2,250		



**EXISTING PARTIAL PLAN AT EAST ABUTMENT**  
(Opposite for West Abutment)

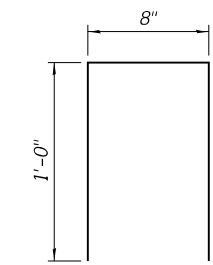


**PROPOSED PARTIAL PLAN AT EAST ABUTMENT**  
(Opposite for West Abutment)



**BAR d(E)**

**BAR d1(E) and d2(E)**



**BAR u(E)**

**NOTES:**

- I.F. denotes Inside Face.  
O.F. denotes Outside Face.
- See Sheet 7 of 10 for Sections A-A, B-B, and C-C.

MODEL: Default  
FILE NAME: D:\104225\104225-022-0105-06-Exp\104225-022-0105-06-Exp\104225-022-0105-06-Exp.dwg



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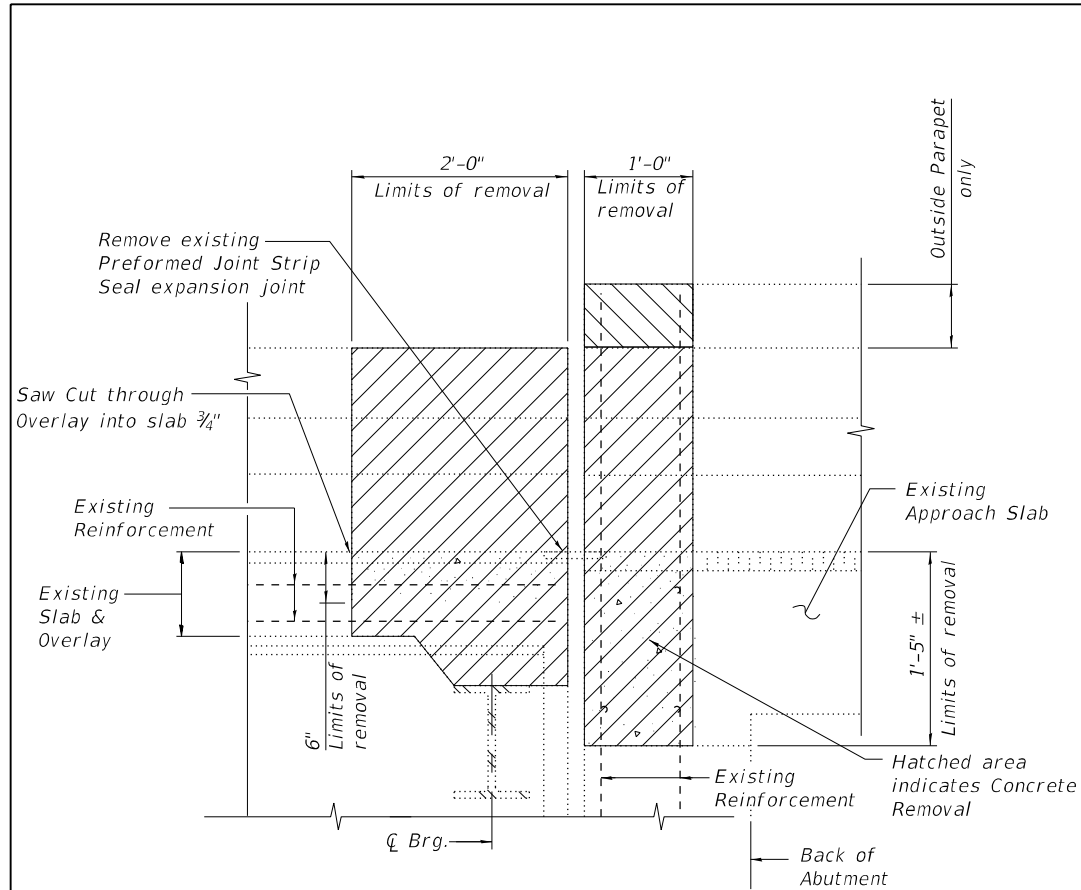
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ABUTMENT EXPANSION JOINT REPAIR PLAN  
STRUCTURE NO. 022-0105**

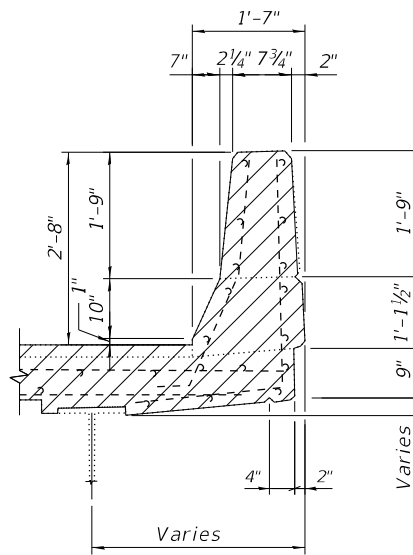
SHEET 6 OF 10 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	62
CONTRACT NO. 62P58				

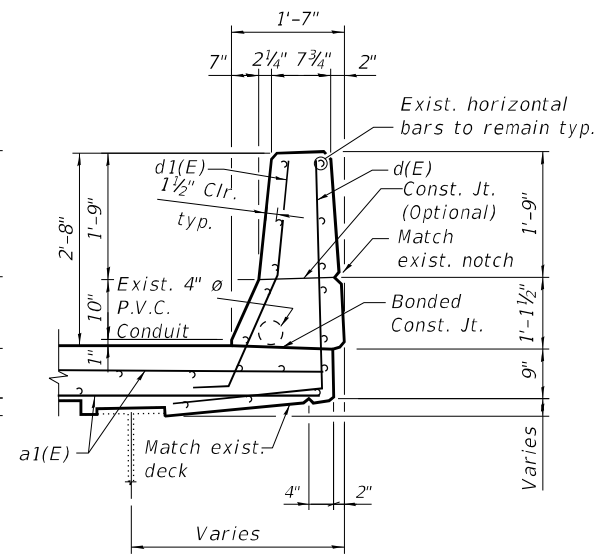
ILLINOIS FED. AID PROJECT



SECTION A-A



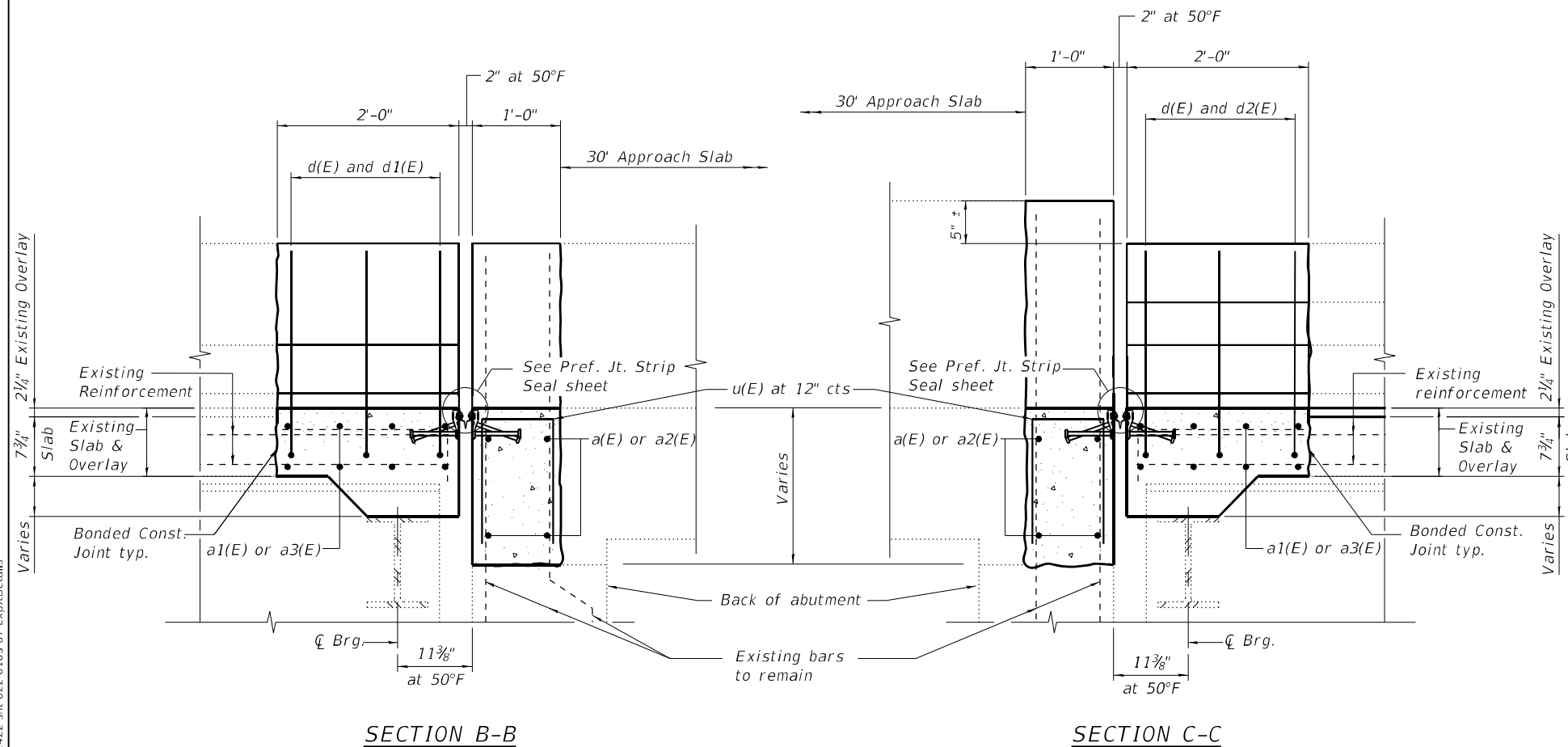
EXISTING INSIDE PARAPET SECTION



PROPOSED INSIDE PARAPET SECTION

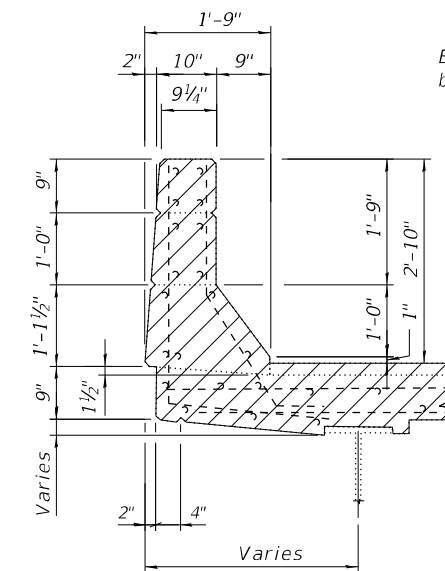
NOTES:

- Existing reinforcement bars extending into the concrete removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- Existing reinforcement bars in the concrete removal area parallel to the expansion joints shall be removed.
- Removal and disposal of the existing expansion joints will not be paid for separately, but shall be included with the cost of Concrete Removal.
- If existing name plate falls within the limits of Concrete Removal, it shall be removed and reinstalled in its original location in accordance with IDOT Std. 515001. Cost included with Concrete Superstructure.
- If existing guardrail and/or end shoe fall within the limits of Concrete Removal, they shall be removed and reinstalled in their original locations in accordance with District 1 Std. BM-21. Cost included with Concrete Superstructure.
- The Contractor shall exercise extreme care with the existing conduits in sections of the parapet to be removed and to protect and support the conduit. The Contractor will be required to repair any damage done to the conduit to the satisfaction of the Engineer. No splicing will be allowed to any cable damage resulting from this work, instead the Contractor will be required to repair the entire span of any damaged cable at no additional cost to the Department.
- See Sheet 6 of 10 for Bar Bending Details and Bill of Material.

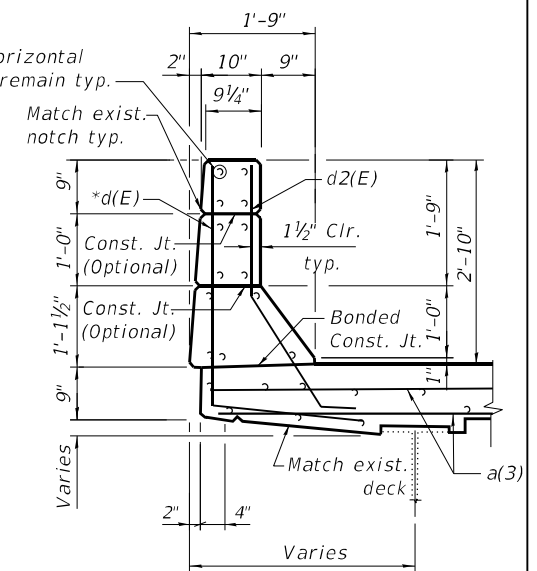


SECTION B-B

SECTION C-C



EXISTING OUTSIDE PARAPET SECTION



PROPOSED OUTSIDE PARAPET SECTION

* Cut off end of d(E) bar as needed in field

MODEL: Default  
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ABUTMENT EXPANSION JOINT REPAIR DETAILS  
STRUCTURE NO. 022-0105

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	63
CONTRACT NO. 62P58				

MODEL: Default  
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STATE OF ILLINOIS  
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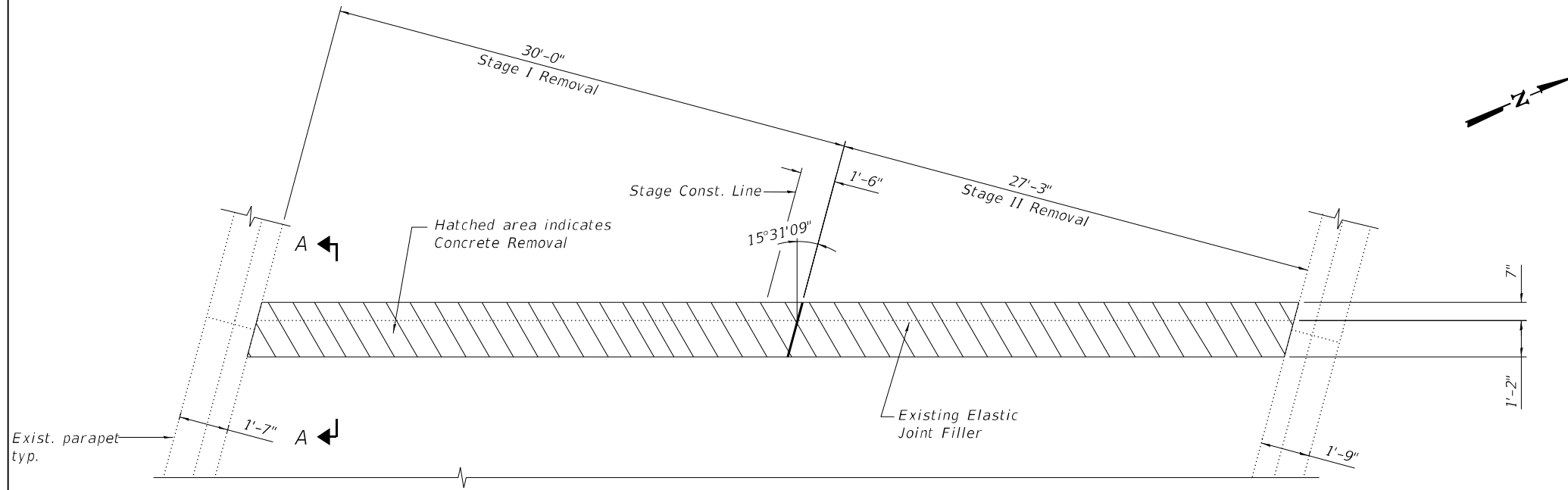
PIN JOINT REPAIR PLAN AND DETAILS  
 STRUCTURE NO. 022-0105

SHEET 8 OF 10 SHEETS

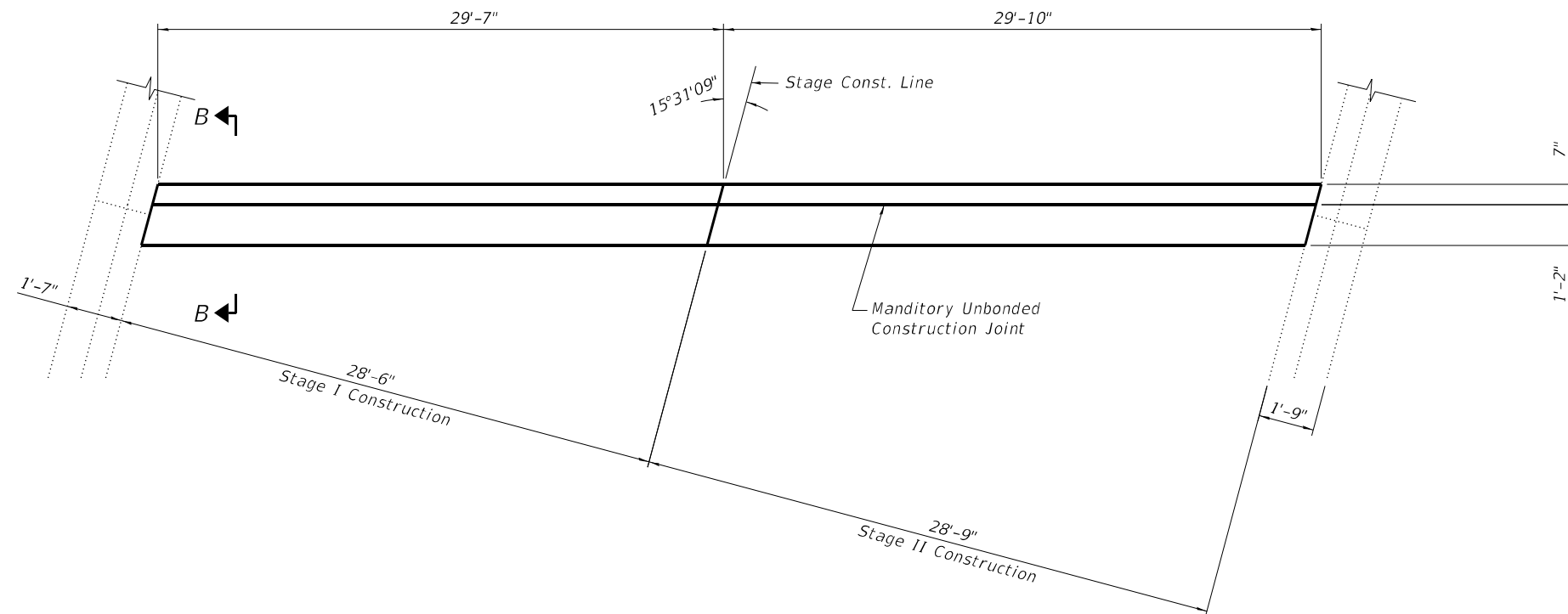
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	64
CONTRACT NO. 62P58				
ILLINOIS		FED. AID PROJECT		

**BILL OF MATERIAL**

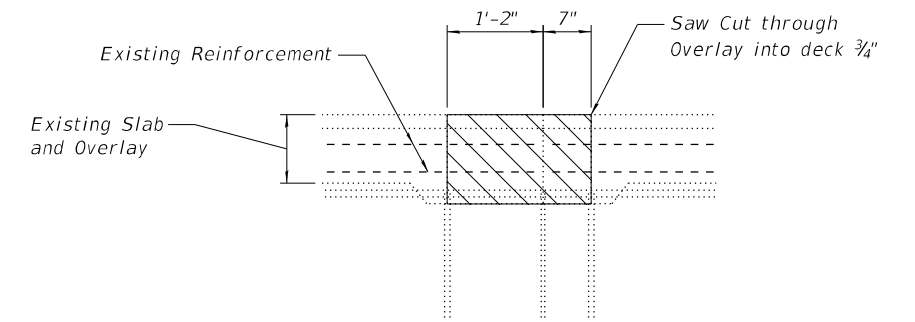
Item	Unit	Total
Concrete Removal	Cu. Yd.	8.3
Concrete Superstructure	Cu. Yd.	8.3
Bridge Deck Grooving	Sq. Yd.	23
Protective Coat	Sq. Yd.	23



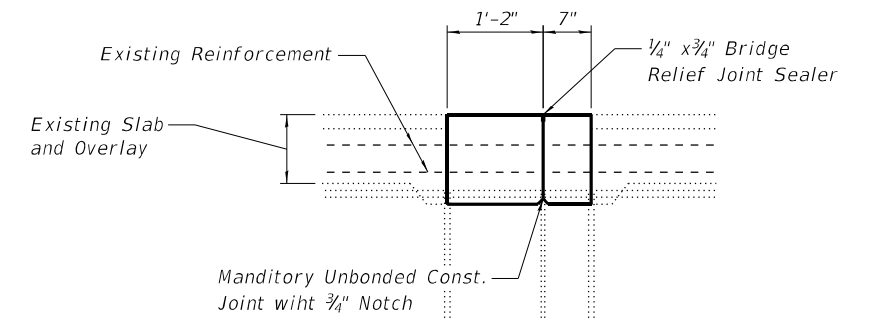
**EXISTING PARTIAL PLAN AT WEST PIN JOINT**  
 (Opposite for East Pin Joint)



**PROPOSED PARTIAL PLAN AT WEST PIN JOINT**  
 (Opposite for East Pin Joint)



**SECTION A-A**

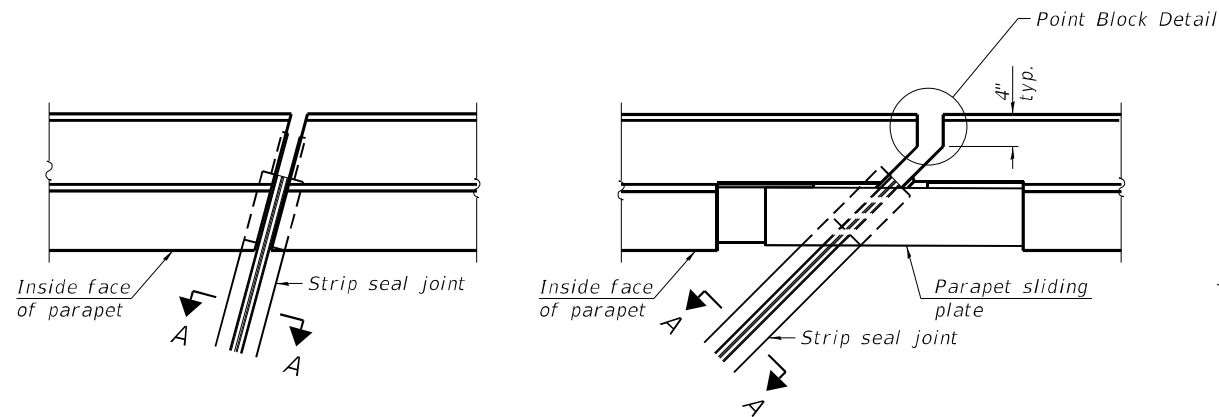


**SECTION B-B**

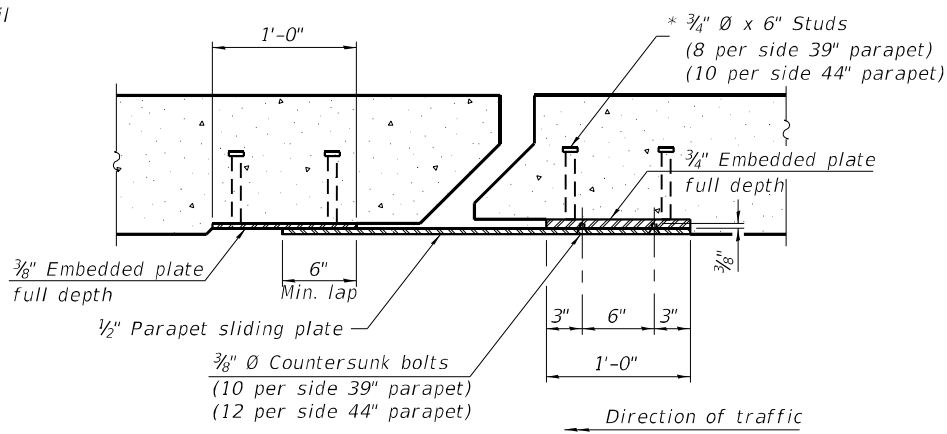
**NOTES:**

1. Bridge Relief Joint Sealer shall be according to Section 588 of the Standard Specifications. The cost of the Bridge Relief Joint Sealer shall be included with the cost of Concrete Superstructure.





PLAN AT PARAPET



SECTION B-B

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

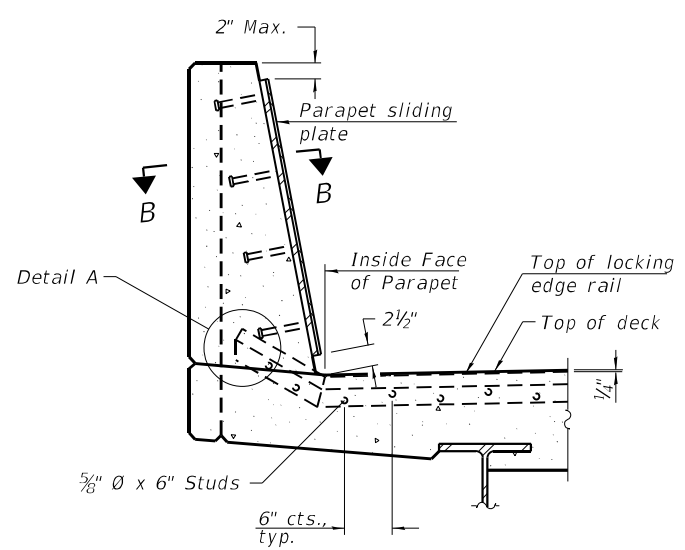
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

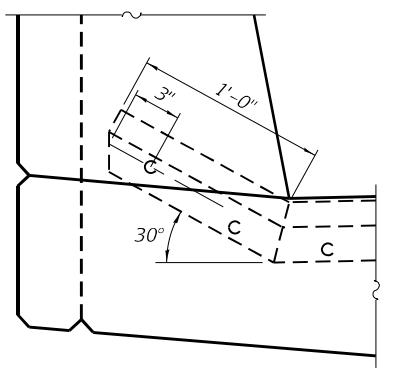
39" constant slope barrier shown, 44" constant slope barrier similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

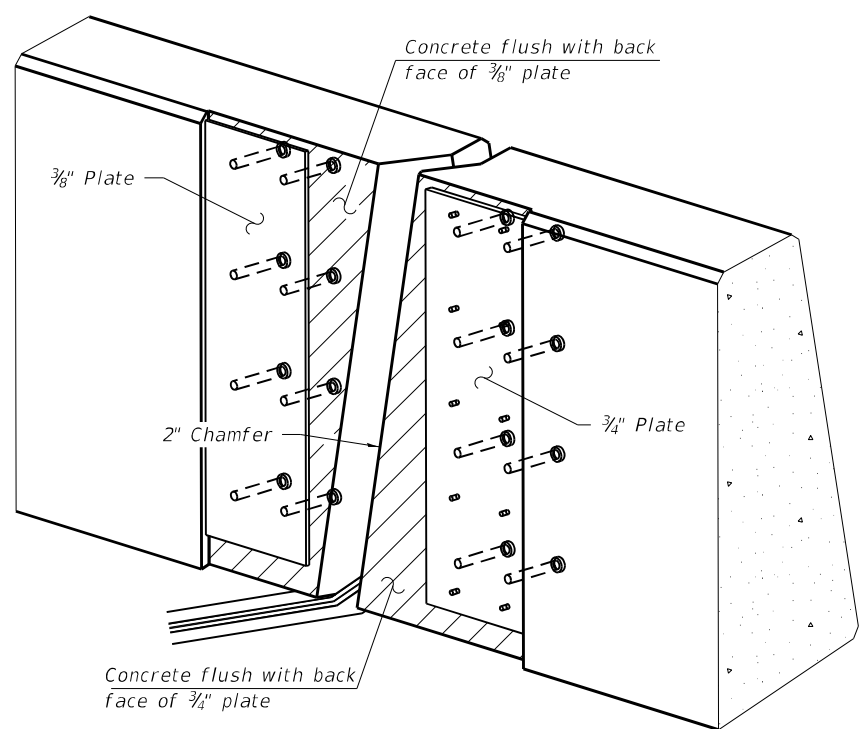


SECTION A-A

(Skews  $> 30^\circ$  shown. Skews  $\leq 30^\circ$  similar except as shown in plan view.)

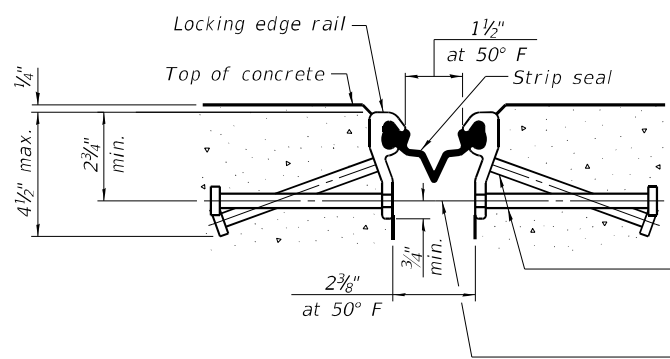


DETAIL A



TRIMETRIC VIEW

(Showing embedded plates only)



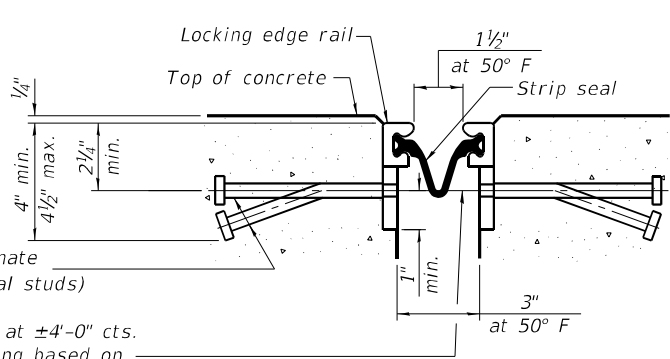
SHOWING ROLLED RAIL JOINT

* 3/8"  $\phi$  x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

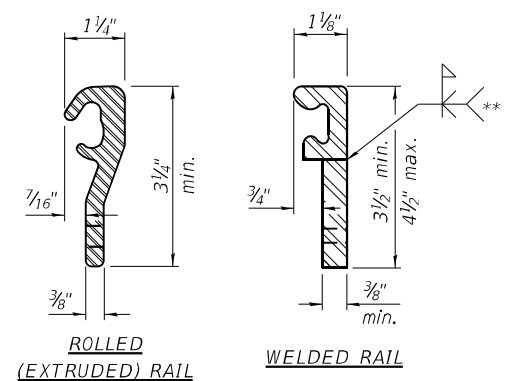
3/8"  $\phi$  threaded rods in 1/16"  $\phi$  holes at  $\pm 4$ -0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

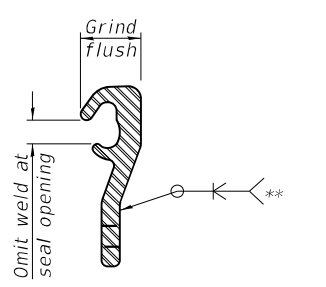


SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	123

MODEL: Default  
FILE NAME: D:\10422-5\1-022-0105-09-Prof\StripSeal_1

EJ-SS

1-1-2020



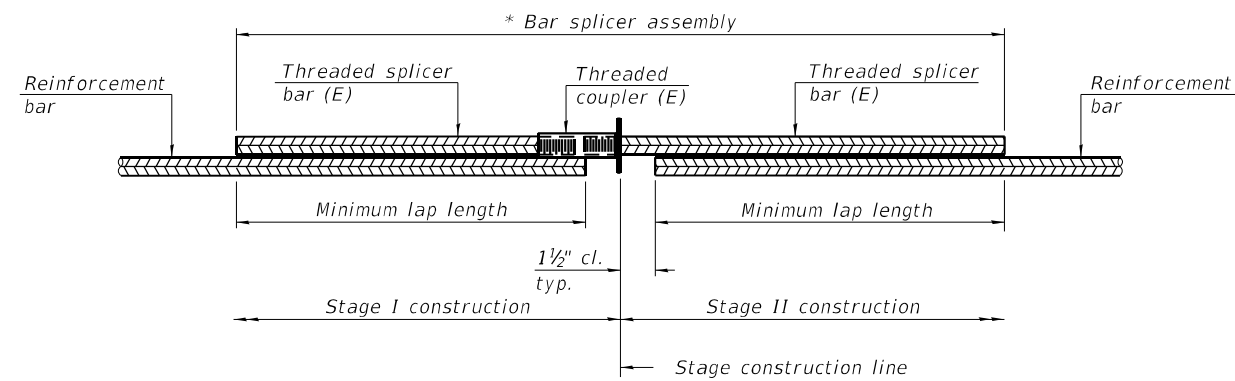
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PREFORMED JOINT STRIP SEAL  
STRUCTURE NO. 022-0105

SHEET 9 OF 10 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	65
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				

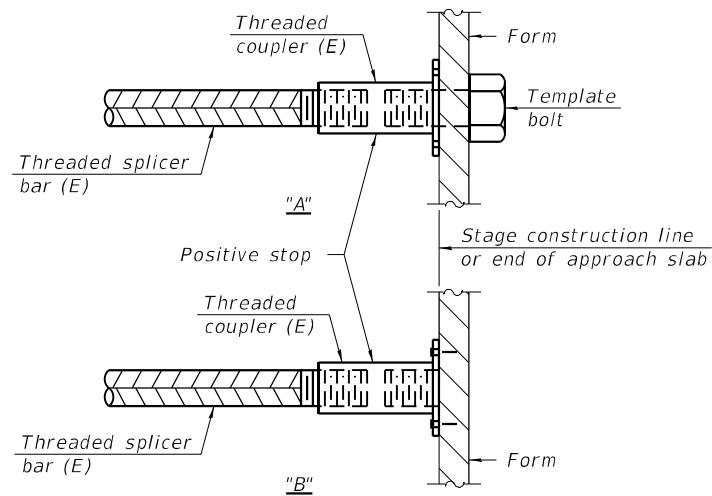


**STANDARD BAR SPLICER ASSEMBLY PLAN**  
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
W. Abut Joint	#5	8	3'-6"
W. Abut Joint	#6	4	4'-0"
E. Abut Joint	#5	8	3'-6"
E. Abut Joint	#6	4	4'-0"

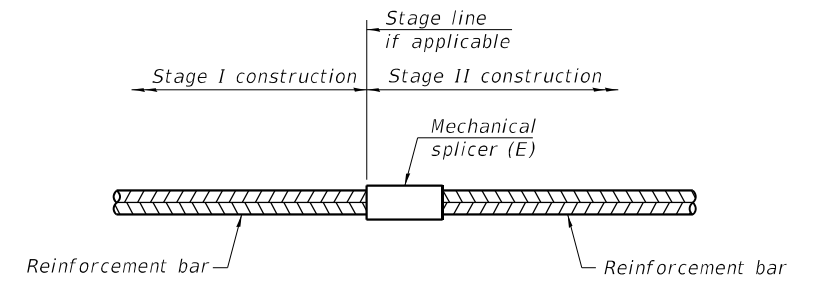


**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required

**Notes:**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: Default  
FILE NAME: D:\101422-5\10-022-0105-10-BarSplicer

BSD-1

1-1-2020



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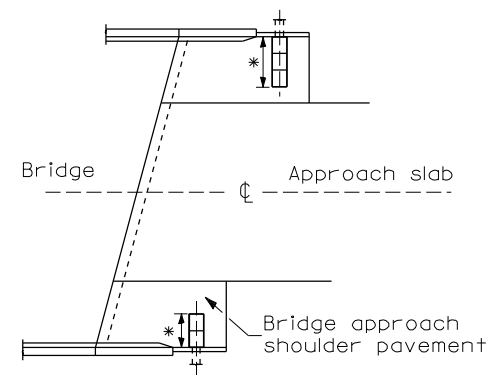
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BAR SPICER ASSEMBLY AND MECHANICAL SPICER DETAILS  
STRUCTURE NO. 022-0105

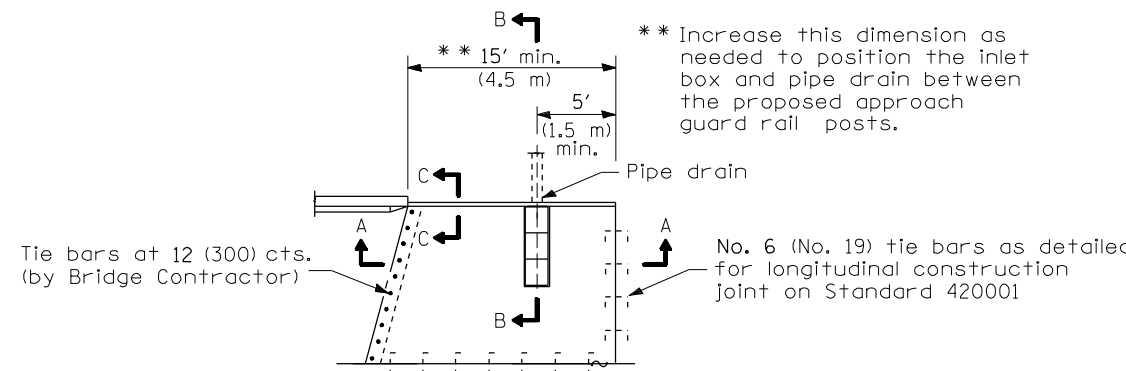
SHEET 10 OF 10 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	66
ILLINOIS			FED. AID PROJECT	

* Type B, C, or D inlet box as required.

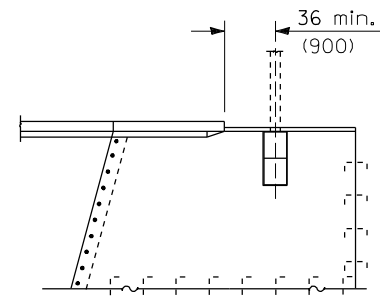


GENERAL PLAN



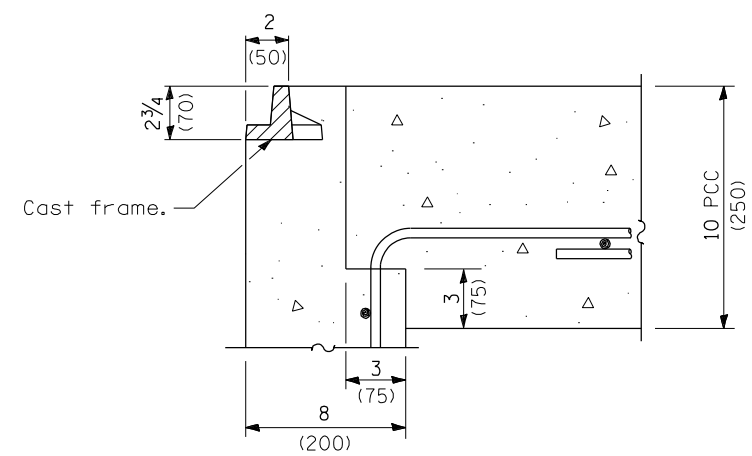
TYPICAL DETAIL PLAN

(W/O Wingwall)

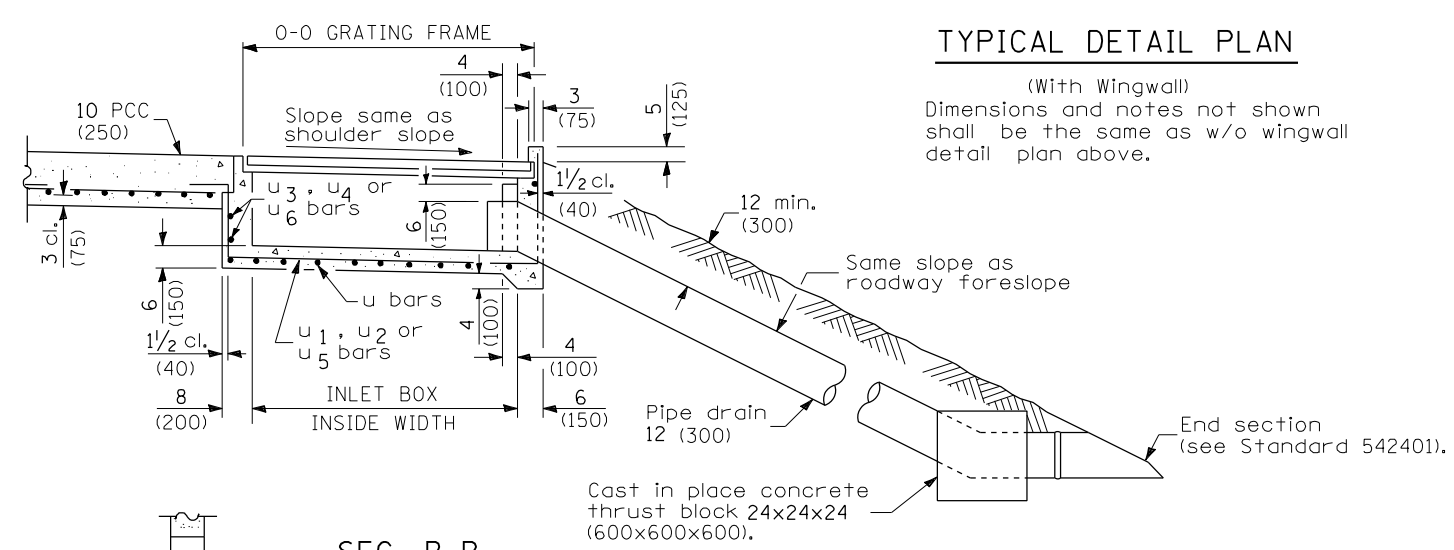


TYPICAL DETAIL PLAN

(With Wingwall)  
Dimensions and notes not shown shall be the same as w/o wingwall detail plan above.



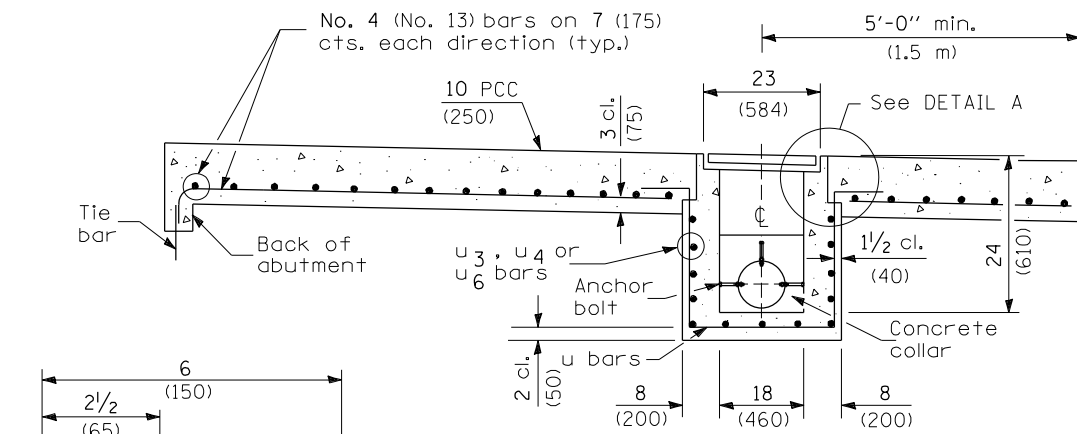
DETAIL A



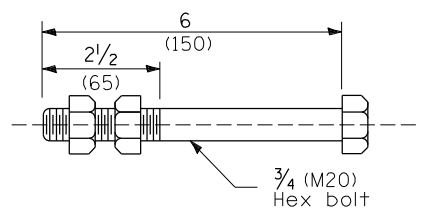
SEC. B-B

BOX OUTLET WHEN PRECAST

INLET TYPE	SHOULDER WIDTH	0-0 GRATING FRAME	INLET BOX INSIDE WIDTH	INLET BOX INSIDE LENGTH
Type B	Less than 5' (1.5 m)	2'-3" (0.690 m)	1'-10" (0.560 m)	18 (460)
Type C	5' - 6' (1.5 m) (1.8 m)	4'-4" (1.325 m)	3'-11" (1.195 m)	18 (460)
Type D	Greater than 6' (1.8 m)	6'-5" (1.960 m)	6'-0" (1.830 m)	18 (460)

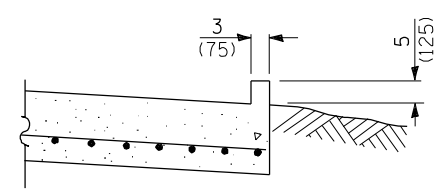


SEC. A-A



ANCHOR BOLT

(Used to tie pipe to concrete collar)



SEC. C-C

GENERAL NOTES

All exposed edges of the inlet, except the upper perimeter, shall be beveled 3/4 (20).  
For placement of approach shoulder pavement on existing construction substitute expansion anchor ties for bars. Omit tie bars for flexible approaches or bridge approach shoulder pavement constructed monolithically with shoulder pavement.

**FOR REFERENCE ONLY**

MODEL Path: \\... FILE NAME: 0101422-dht-border_01.dgn



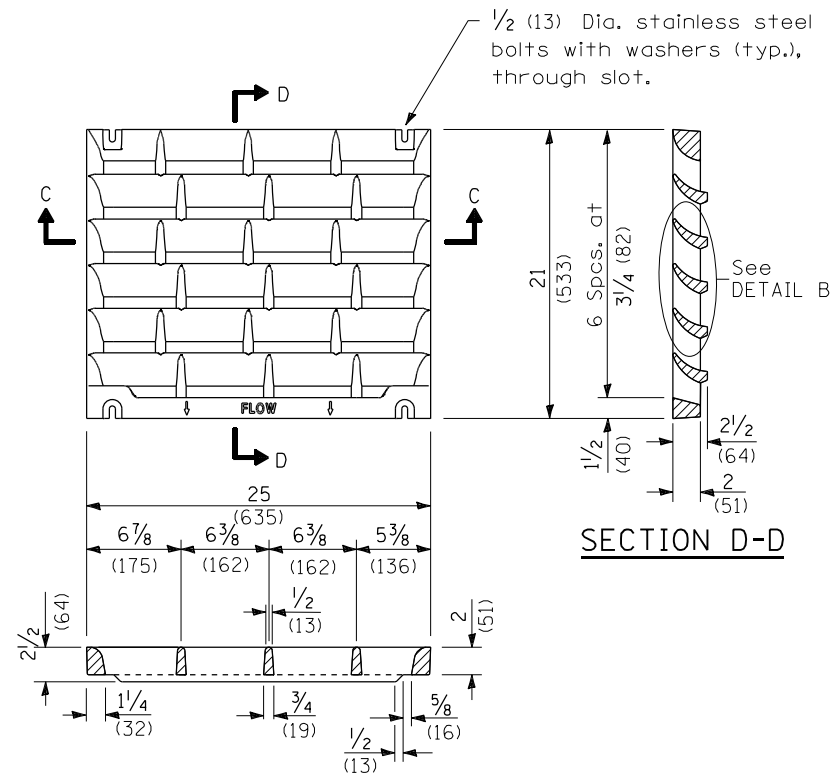
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

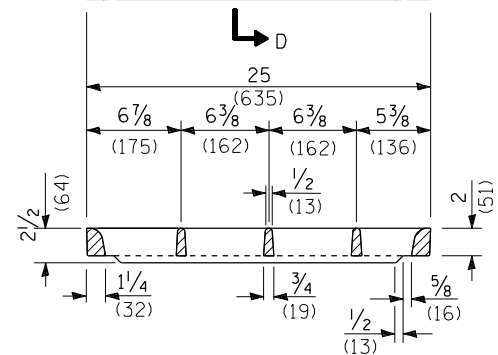
BRIDGE APPROACH SHOULDER  
PAVEMENT AND DRAIN DETAILS

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	67
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P58	



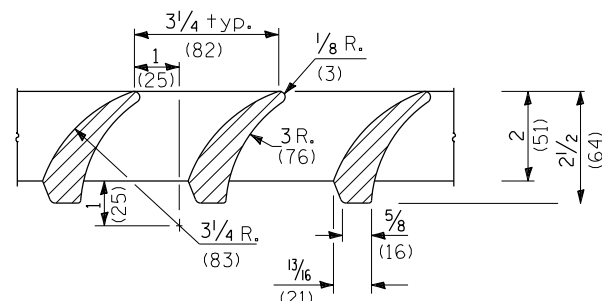
SECTION D-D



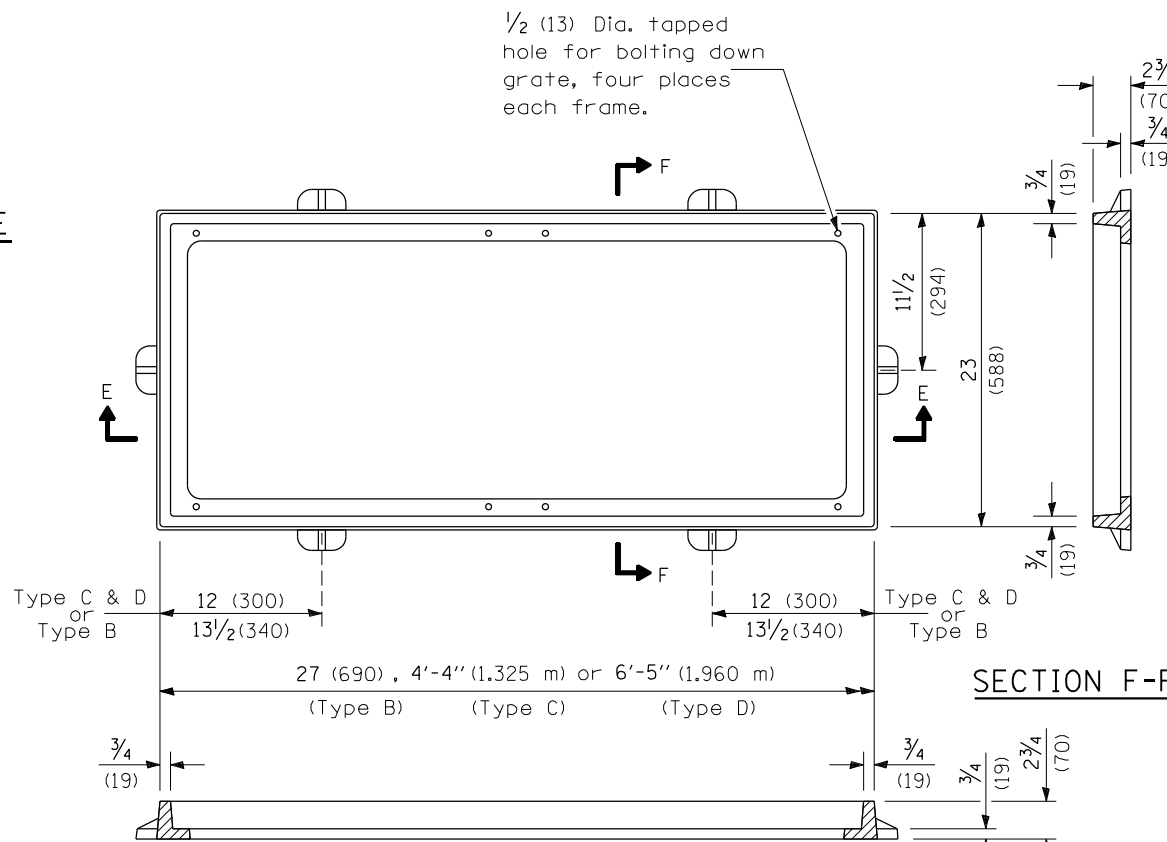
SECTION C-C

**DETAIL OF CAST GRATE**

Type B requires 1 grate  
 Type C requires 2 grates  
 Type D requires 3 grates



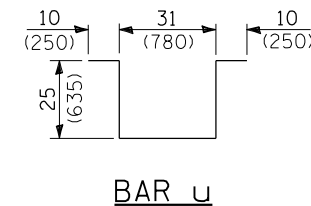
DETAIL B



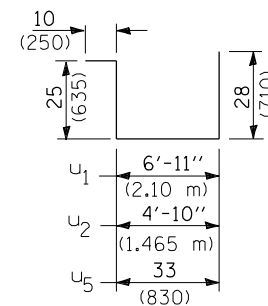
SECTION E-E

**DETAIL OF CAST FRAME**

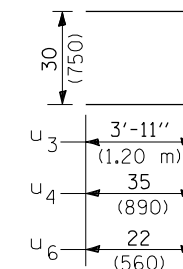
(Type C shown)



BAR U



BARS U₁, U₂ & U₅



BARS U₃, U₄ & U₆

**INLET BOX**

REQUIRED MATERIAL			
TYPE B			
Bar	Qty.	Size	Length
u	4	No. 4 (No.13)	8'-5" (2,550 m)
u ₅	3	No. 4 (No.13)	8'-0" (2,425 m)
u ₆	4	No. 4 (No.13)	6'-2" (1,870 m)
Concrete	cu. yds. (m ³ )		0.5 (0.4)
Reinf. bars	lbs. (kg)		55.0 (25.0)
Grating	sq. ft. (m ² )		3.6 (0.34)
TYPE C			
Bar	Qty.	Size	Length
u	6	No. 4 (No.13)	8'-5" (2,550 m)
u ₂	3	No. 4 (No.13)	10'-1" (3,060 m)
u ₄	4	No. 4 (No.13)	8'-4" (2,530 m)
Concrete	cu. yds. (m ³ )		0.8 (0.6)
Reinf. bars	lbs. (kg)		76 (34.5)
Grating	sq. ft. (m ² )		7.3 (0.68)
TYPE D			
Bar	Qty.	Size	Length
u	8	No. 4 (No.13)	8'-5" (2,550 m)
u ₁	3	No. 4 (No.13)	12'-2" (3,695 m)
u ₃	4	No. 4 (No.13)	10'-4" (3,150 m)
Concrete	cu. yds. (m ³ )		1.1 (0.8)
Reinf. bars	lbs. (kg)		97.0 (44.0)
Grating	sq. ft. (m ² )		10.9 (1.02)

**FOR REFERENCE ONLY**

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 PLOT DATE = 3/18/2022

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 DRAWN - AMB  
 CHECKED - MM  
 DATE - 02/18/2022

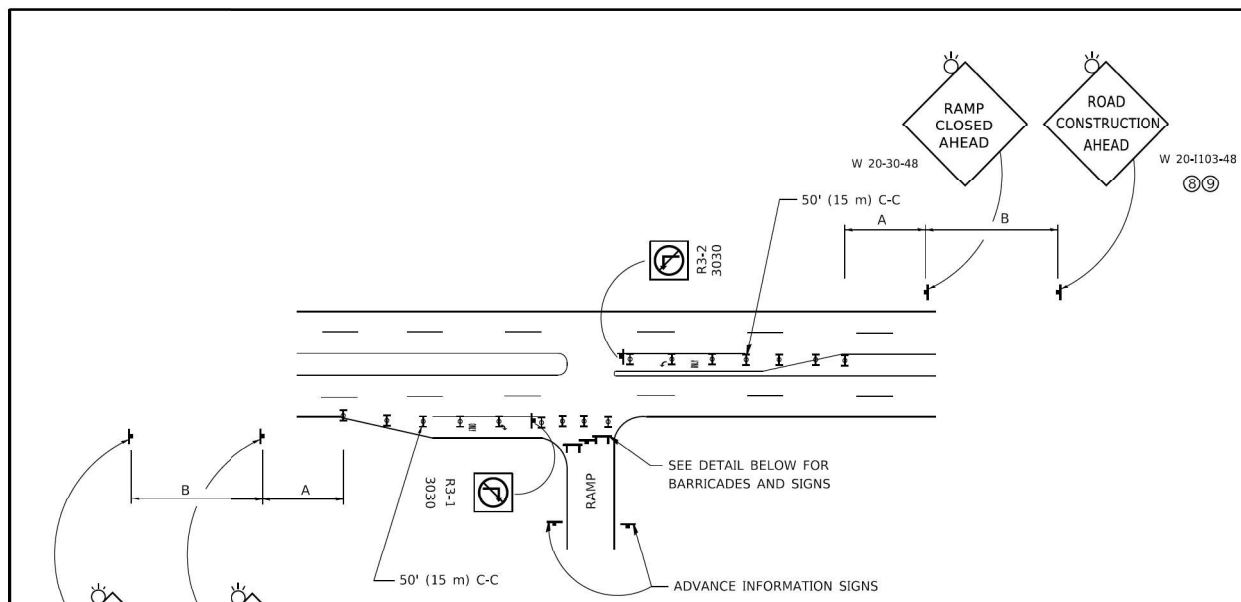
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 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BRIDGE APPROACH SHOULDER  
 PAVEMENT AND DRAIN DETAILS

SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	68
			CONTRACT NO. 62P58	
		ILLINOIS FED. AID PROJECT		

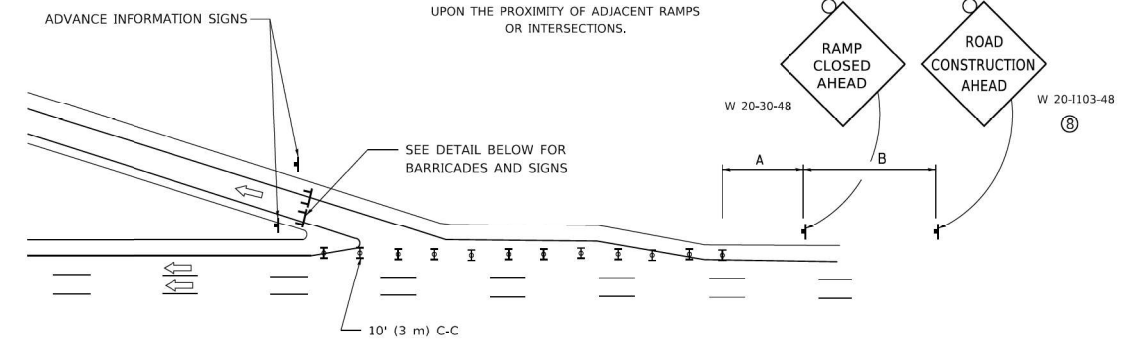
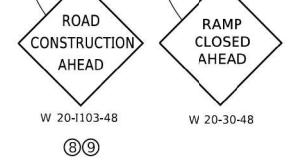


**ENTRANCE RAMP CLOSURE**

**SIGN SPACING TABLE**

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

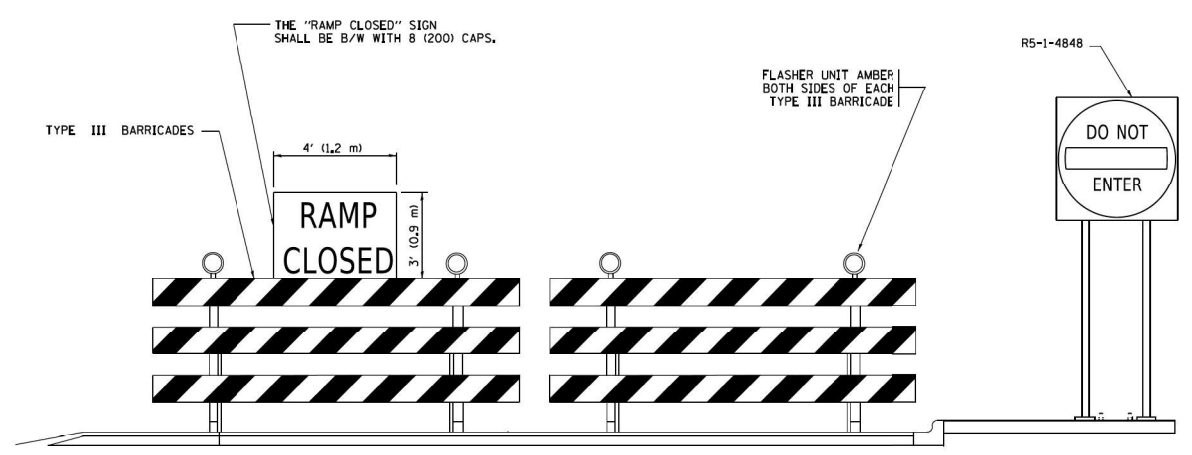
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



**EXIT RAMP CLOSURE**

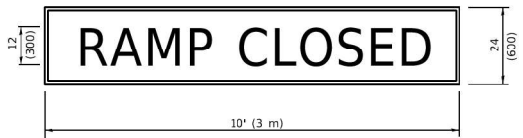
**SYMBOLS**

- ▬ TYPE II BARRICADE OR DRUM
- ⌈ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



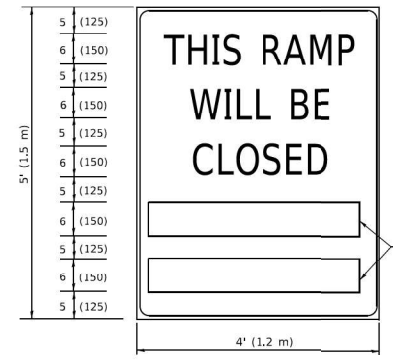
**DETAIL FOR REQUIRED BARRICADES & SIGNS**

**RAMP CLOSURE ADVANCE WARNING SIGN**



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY  
E MOD FONT  
1 (25) BORDER  
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

**RAMP CLOSURE ADVANCE INFORMATION SIGN**



BLACK LEGEND ON WHITE BACKGROUND  
1/2 (12) BORDER  
E MOD FONT  
THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.  
THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

**GENERAL NOTES:**

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS, CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	DRAWN -	REVISED - S.P.B._12-09
PLOT SCALE = 30,0000' / in.	CHECKED -	REVISED - M.D._06-13
PLOT DATE = 3/4/2019	DATE - 02-83	REVISED - M.D._01-18

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP  
CLOSURE DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-08			
				CONTRACT NO.

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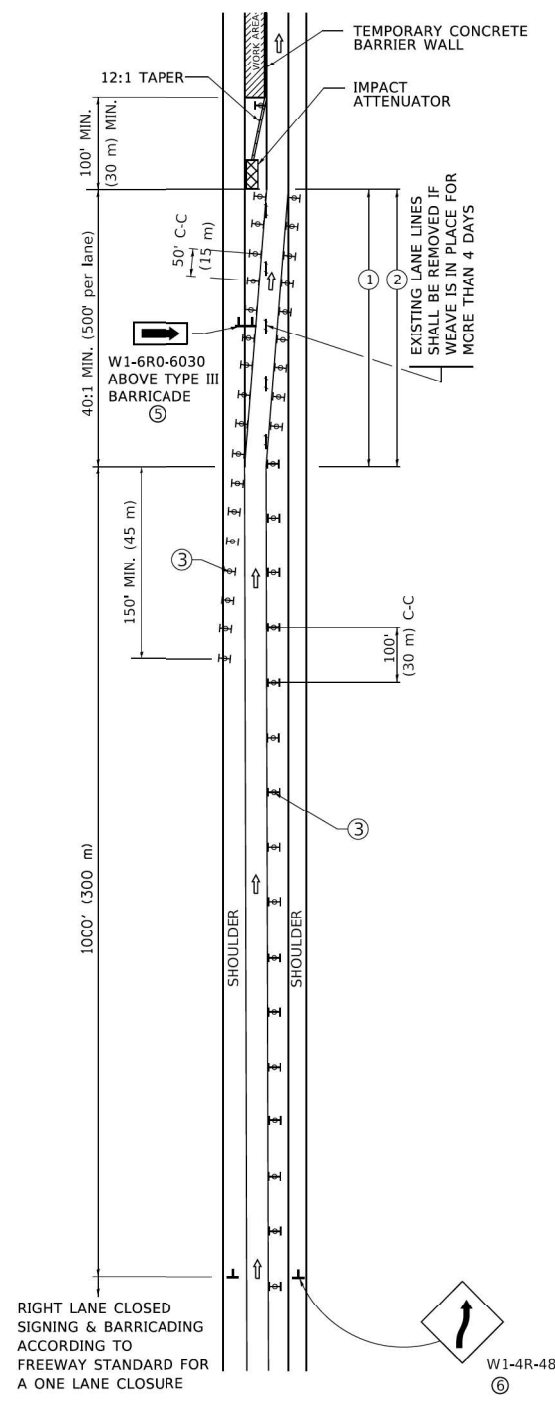
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP  
CLOSURE DETAILS**

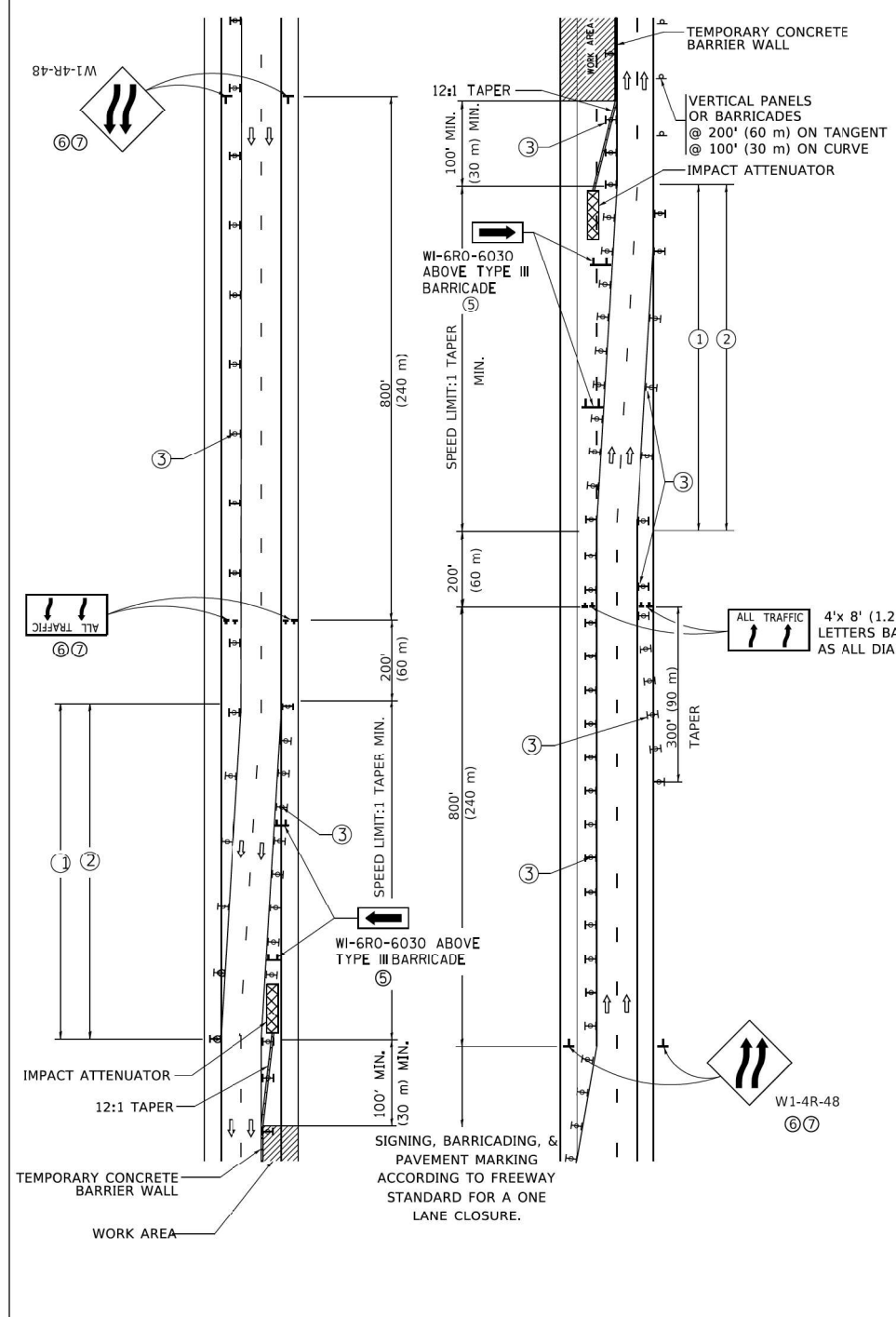
SCALE: SHEET 1 OF 9 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	69
				CONTRACT NO. 62P58

### SINGLE LANE WEAVE



### MULTI-LANE WEAVE



#### GENERAL NOTES:

- EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

#### SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48 (6)
- W24-1-48 (7)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

USER NAME = footejm	DESIGNED - D.W.S.	REVISED - J.A.F. 02-06
DRAWN -	REVISOR - S.P.B. 01-07	
PLOT SCALE = 30,0000' / in.	CHECKED -	REVISED - S.P.B. 12-09
PLOT DATE = 3/4/2019	DATE - 02-87	REVISED - M.D. 06-13

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR  
FREEWAY SINGLE & MULTI-LANE WEAVE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	290	2021-140-BR	77	70
SCALE: NONE		SHEET 1 OF 1 SHEETS STA.		TO STA.
ILLINOIS		FED. AID PROJECT		

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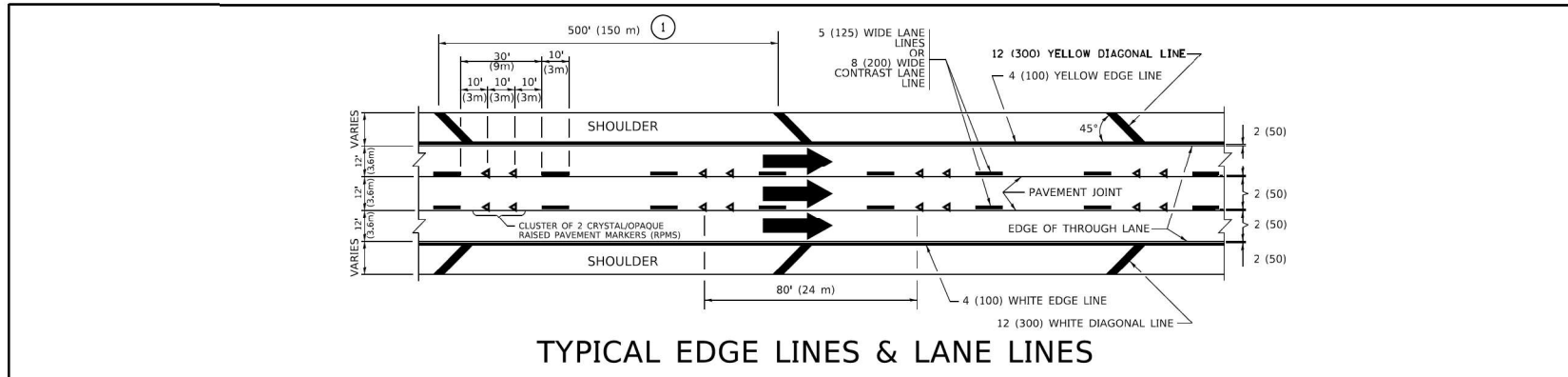


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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

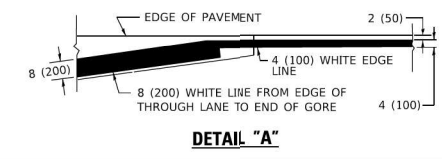
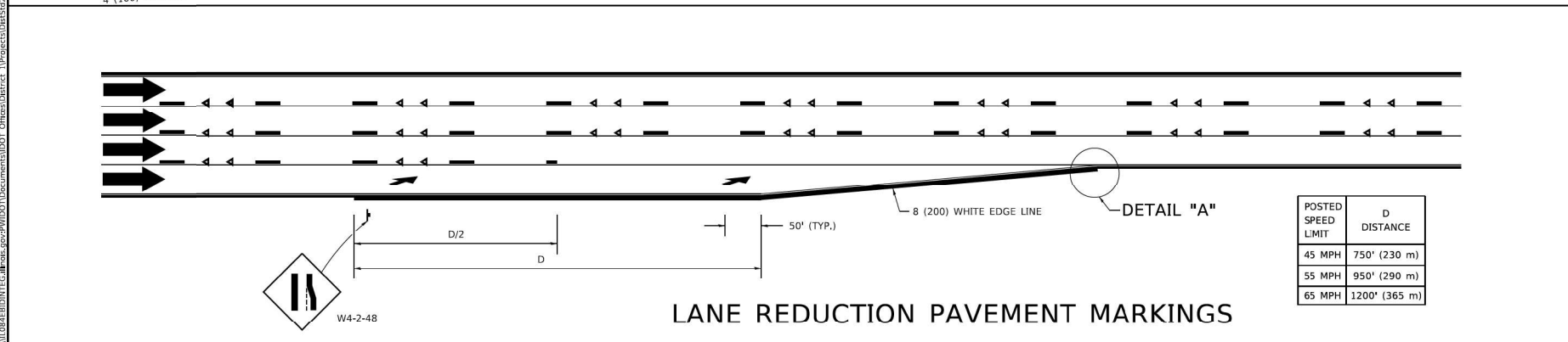
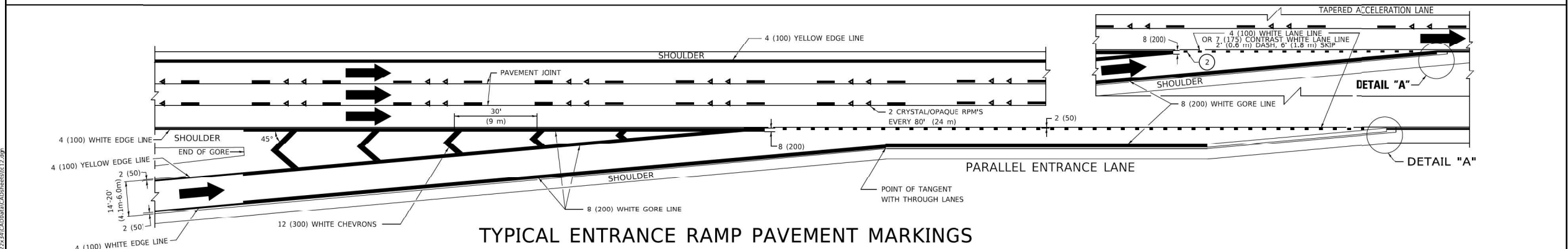
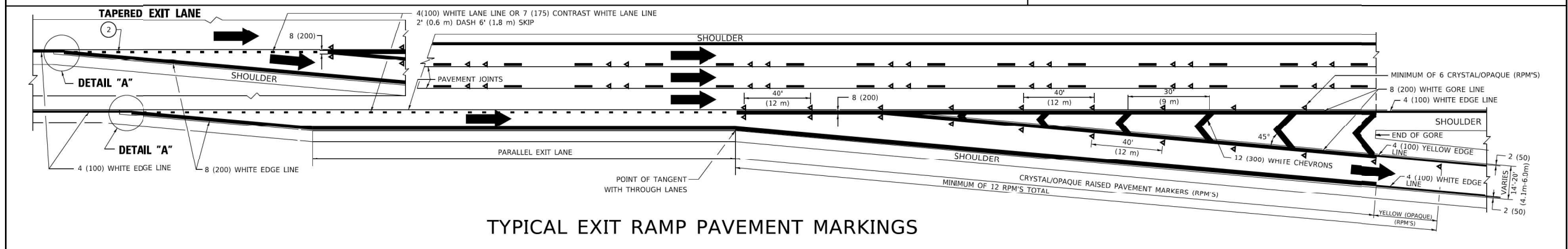
TRAFFIC CONTROL DETAILS FOR  
FREEWAY SINGLE & MULTI-LANE WEAVE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	290	2021-140-BR	77	70
SCALE:		SHEET 2 OF 9 SHEETS STA.		TO STA.
ILLINOIS		FED. AID PROJECT		



**PAVEMENT MARKING MATERIALS**

1. THERMOPLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON HMA PAVEMENTS.
2. POLYUREA OR MODIFIED URETHANE PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON PCC PAVEMENTS.
3. PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, INLAID OR GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENTS.
4. CONTRAST PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON PCC PAVEMENT.



- NOTES:**
- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
  - ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)

USER NAME = footemj	DESIGNED - D.W.S.	REVISED - S.P.B. 01-07
DRAWN -	CHECKED -	REVISED - S.P.B. 01-10
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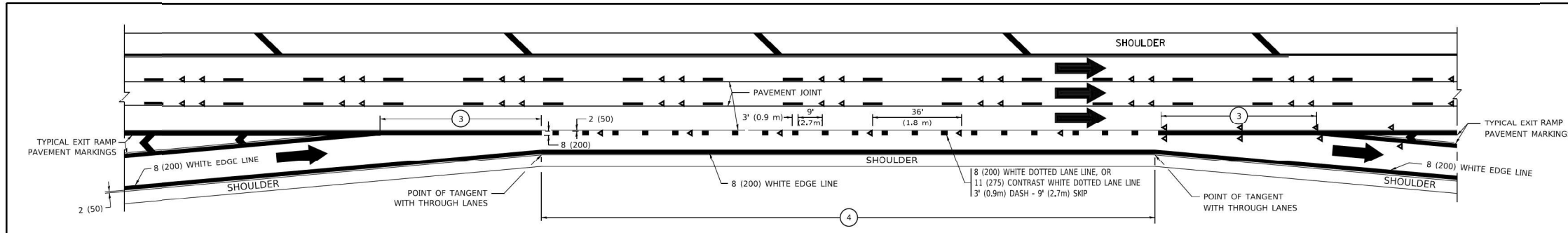
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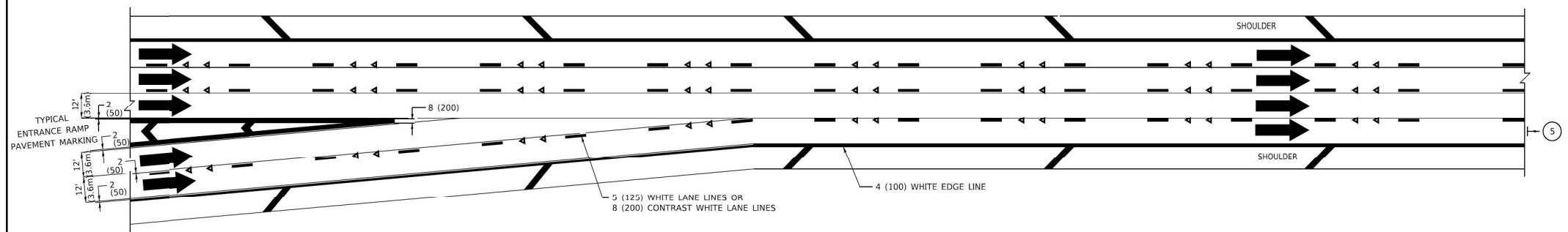
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		SCALE:	SHEET 3 OF 9 SHEETS STA.	TO STA.	290	2021-140-BR	DUPAGE	77 71
				ILLINOIS		FED. AID PROJECT		

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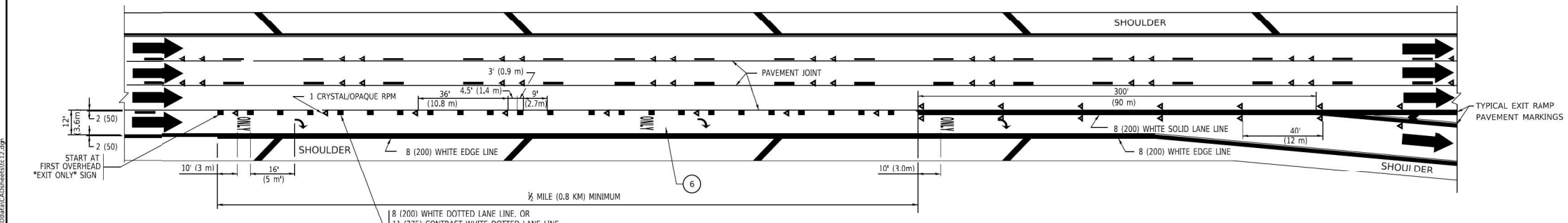




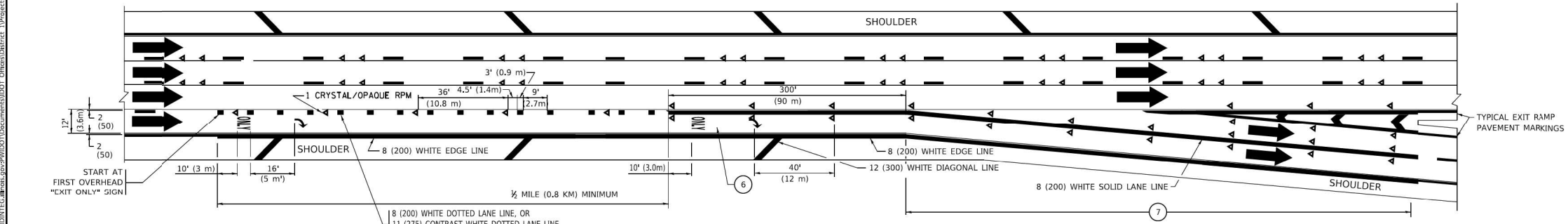
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

- NOTES:
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
  - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
  - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
  - ⑥ ONLY AND ARROWS EQUALLY SPACED. 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED..
  - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

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PLOT DATE = 3/4/2019	DATE - 01-90	REVISED - S.P.B. 01-10
		REVISED - M.D. 09-17

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS	
SCALE: NONE	SHEET 1 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-12			
ILLINOIS		FED. AID PROJECT		

MODEL: Default  
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**GARZA KARHOFF**  
 ENGINEERING, LLC

USER NAME = amalia.baymundo	DESIGNED - AMB	REVISED -
PLOT SCALE = 0.2000' / in.	CHECKED - MM	REVISED -
PLOT DATE = 3/18/2022	DATE - 02/18/2023	REVISED -

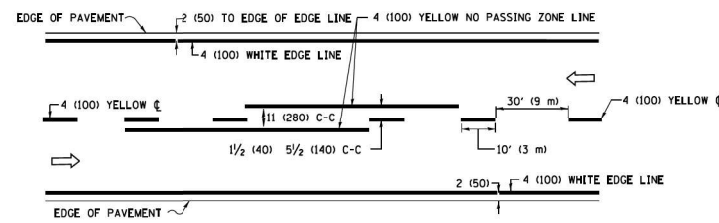
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS	
SCALE:	SHEET 4 OF 9 SHEETS STA. TO STA.

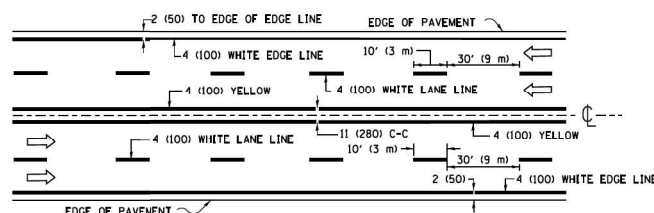
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	290	2021-140-BR	77	72
ILLINOIS		FED. AID PROJECT		

CONTRACT NO. 62P58

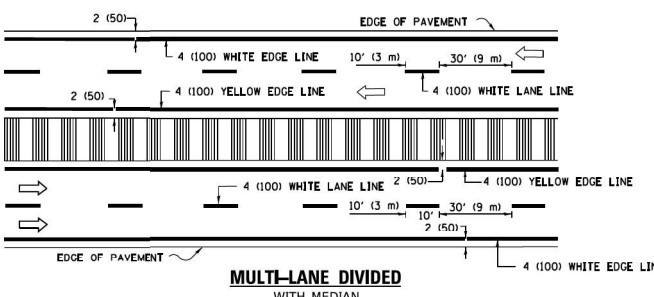




**2-LANE ROADWAY**

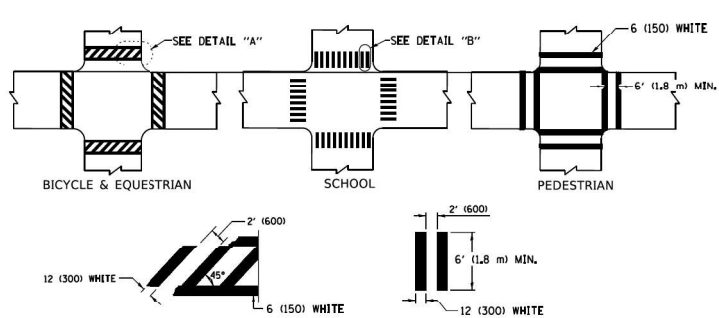


**MULTI-LANE UNDIVIDED**



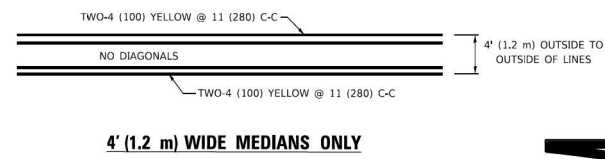
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

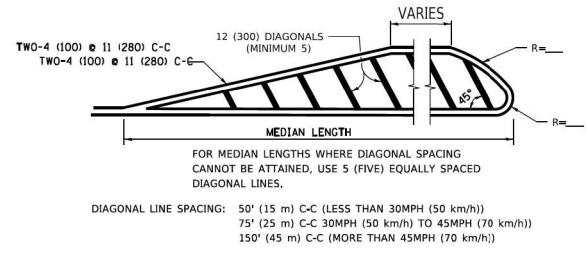


**TYPICAL CROSSWALK MARKING**

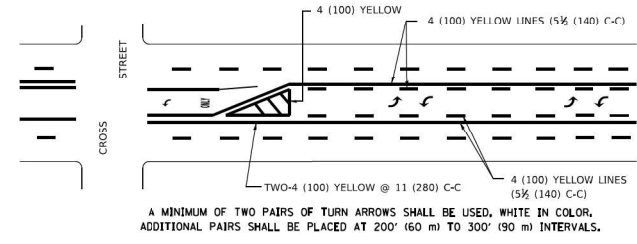
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



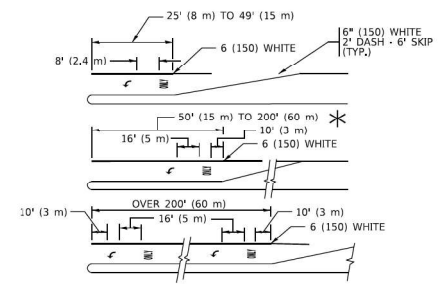
**4' (1.2 m) WIDE MEDIANS ONLY**



**MEDIANS OVER 4' (1.2 m) WIDE**

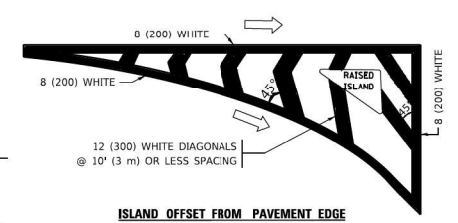


**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**

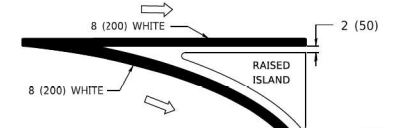


**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

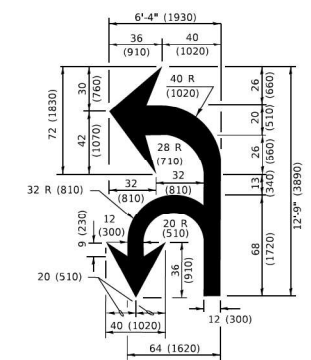


**ISLAND OFFSET FROM PAVEMENT EDGE**

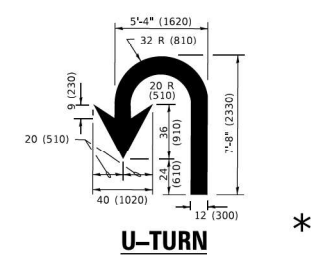


**ISLAND AT PAVEMENT EDGE**

**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**  
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30 MPH (50 km/h)) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m ² ) EACH *X*=54.0 SQ. FT. (5.0 m ² )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h)) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footem	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
PLOT SCALE = 30,0000' / in.	CHECKED -	REVISED - C. JUCIUS 07-01-13
PLOT DATE = 3/18/2019	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-13			
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

USER NAME = amalia.baymundo	DESIGNED - AMB	REVISED -
PLOT SCALE = 0.2000' / in.	CHECKED - MM	REVISED -
PLOT DATE = 3/18/2022	DATE - 02/18/2023	REVISED -

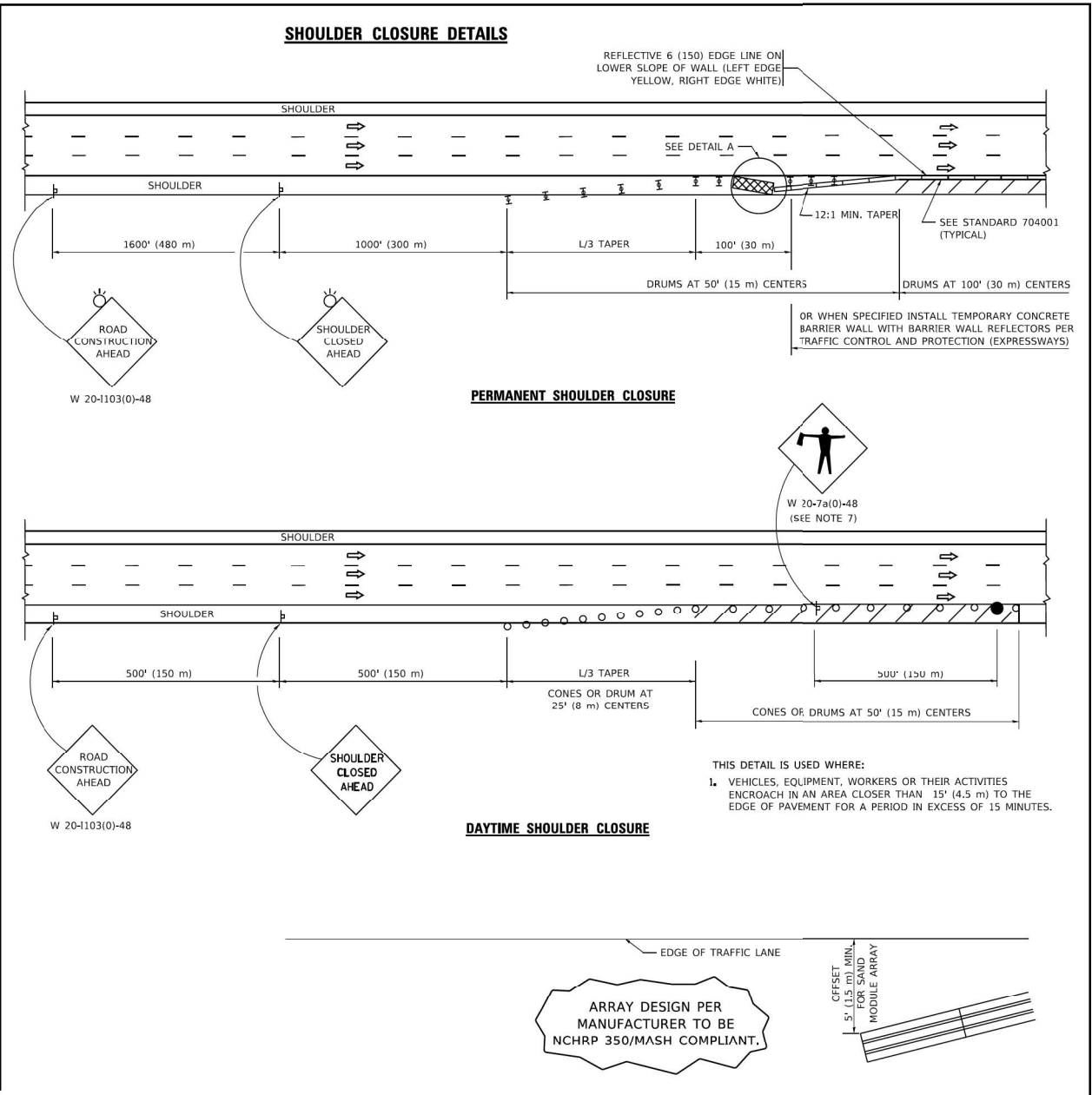
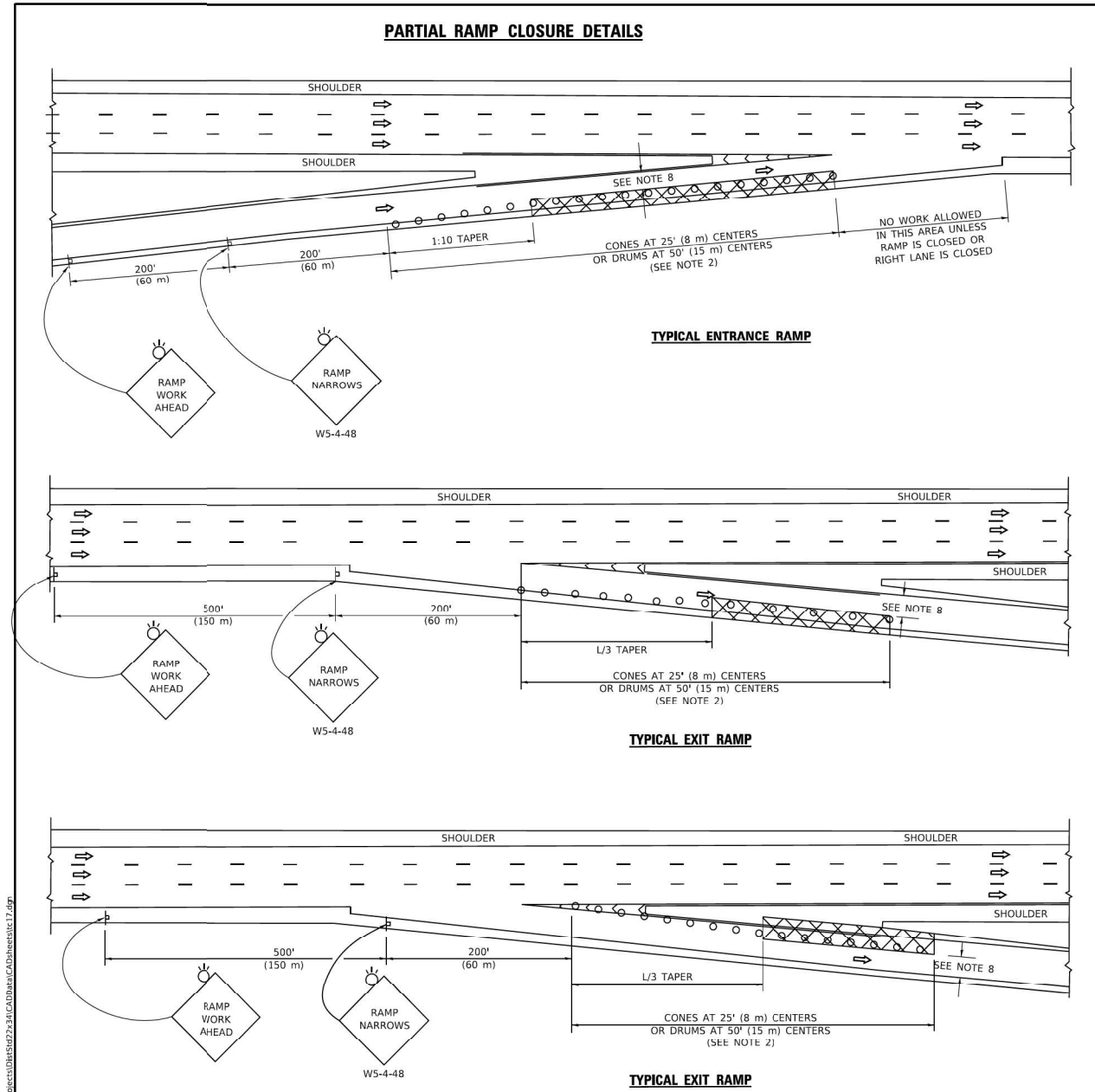
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	290	2021-140-BR	DUPAGE	77 73
CONTRACT NO. 62P58				
ILLINOIS FED. AID PROJECT				

MODEL: D:\p\11...  
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**SYMBOLS**

ACTIVE WORK AREA

SIGN ON PORTABLE OR PERMANENT SUPPORT

FLAGGER WITH CONTROL SIGN

TYPE II BARRICADE OR DRUM

CONE, DRUM OR BARRICADE

IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

**GENERAL NOTES:**

1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER	METRIC: $L = 0.65(WXS)$ ENGLISH: $L = (WXS)$
	W = WIDTH OF OFFSET IN FEET (METERS) S = NORMAL POSTED SPEED MPH (KM/H)

2. TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.

3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.

4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.

6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.

7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:

- FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
- THE WORK ACTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.

THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

8. 12' MIN. WIDTH TANGENT SECTION  
16' MIN. WIDTH CURVE SECTION.

**DETAIL "A"**  
IMPACT ATTENUATOR, TEMPORARY (SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = footejm	DESIGNED -	REVISED - S.P.B. 01-07
	DRAWN - U.W.S.	REVISED - S.P.B. 12-09
PLOT SCALE = 30,000' / in.	CHECKED -	REVISED - M.D. 06-13
PLOT DATE = 3/4/2019	DATE - 11-96	REVISED - M.D. 01-18

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		SCALE: NONE		TC-17		ILLINOIS		FED. AID PROJECT
		SHEET 1 OF 1 SHEETS STA. TO STA.		CONTRACT NO.				

USER NAME = amalia.baymundo	DESIGNED - AMB	REVISED -
	DRAWN - AMB	REVISED -
PLOT SCALE = 0,2000' / in.	CHECKED - MM	REVISED -
PLOT DATE = 3/18/2022	DATE - 02/18/2023	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES	
		SCALE: SHEET 6 OF 9 SHEETS STA. TO STA.	

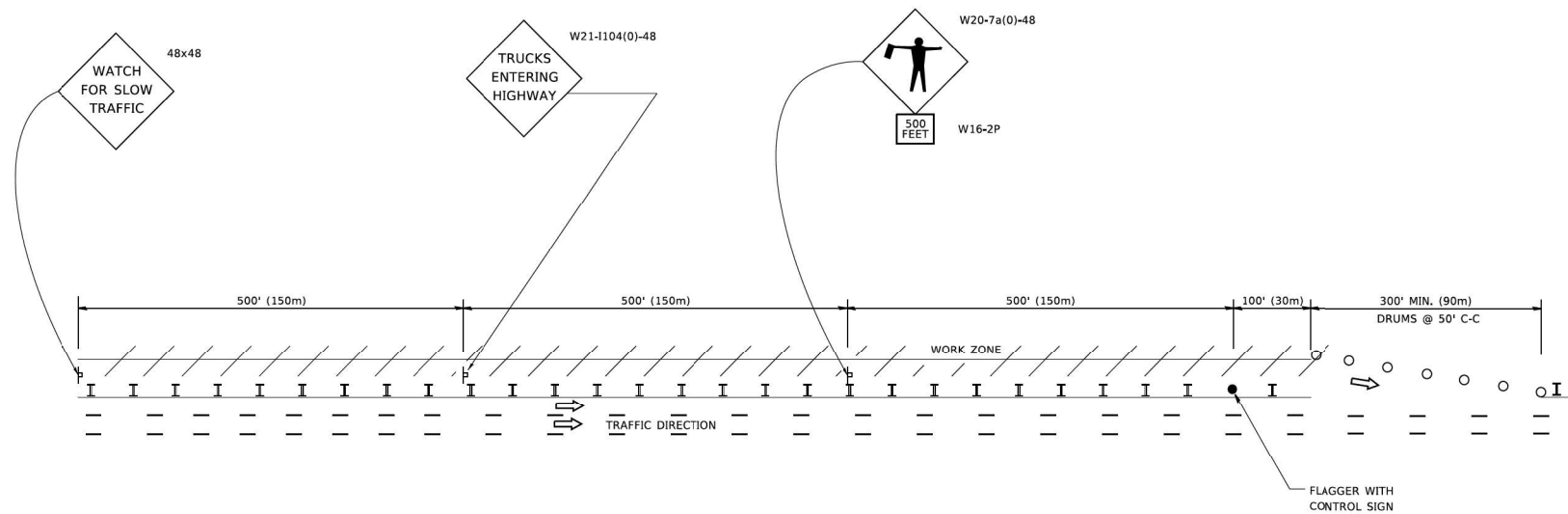
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290	2021-140-BR	DUPAGE	77	74
		CONTRACT NO. 62P58		
		ILLINOIS		FED. AID PROJECT

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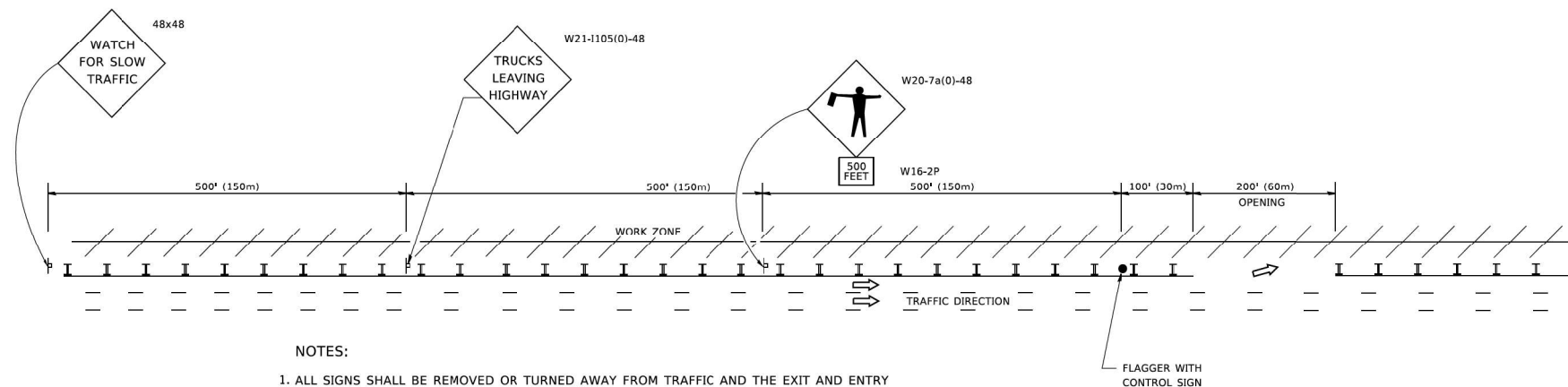


SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. ALL SIGNS SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE OPENINGS SHALL BE A MINIMUM OF ONE HALF MILE APART AND A MINIMUM OF ONE QUARTER MILE FROM ALL ENTRANCE AND EXIT RAMPS.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS
5. FLAGGERS SHALL NOT STOP TRAFFIC OR DIRECT TRAFFIC INTO AN ADJACENT LANE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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FILE NAME: ...

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PLOT SCALE = 30,0000 ' / in.	CHECKED -	REVISED - S.P.B. 01-07
PLOT DATE = 3/4/2019	DATE -	REVISED - S.P.B. 12-09
		REVISED - M.D.06-13

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

FREeway /EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS  
AT WORK ZONE OPENINGS ON FREEWAYS /EXPRESSWAYS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-18			
ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

MODEL: D:\p\h\...  
FILE NAME: ...

**garza karhoff**  
ENGINEERING, LLC

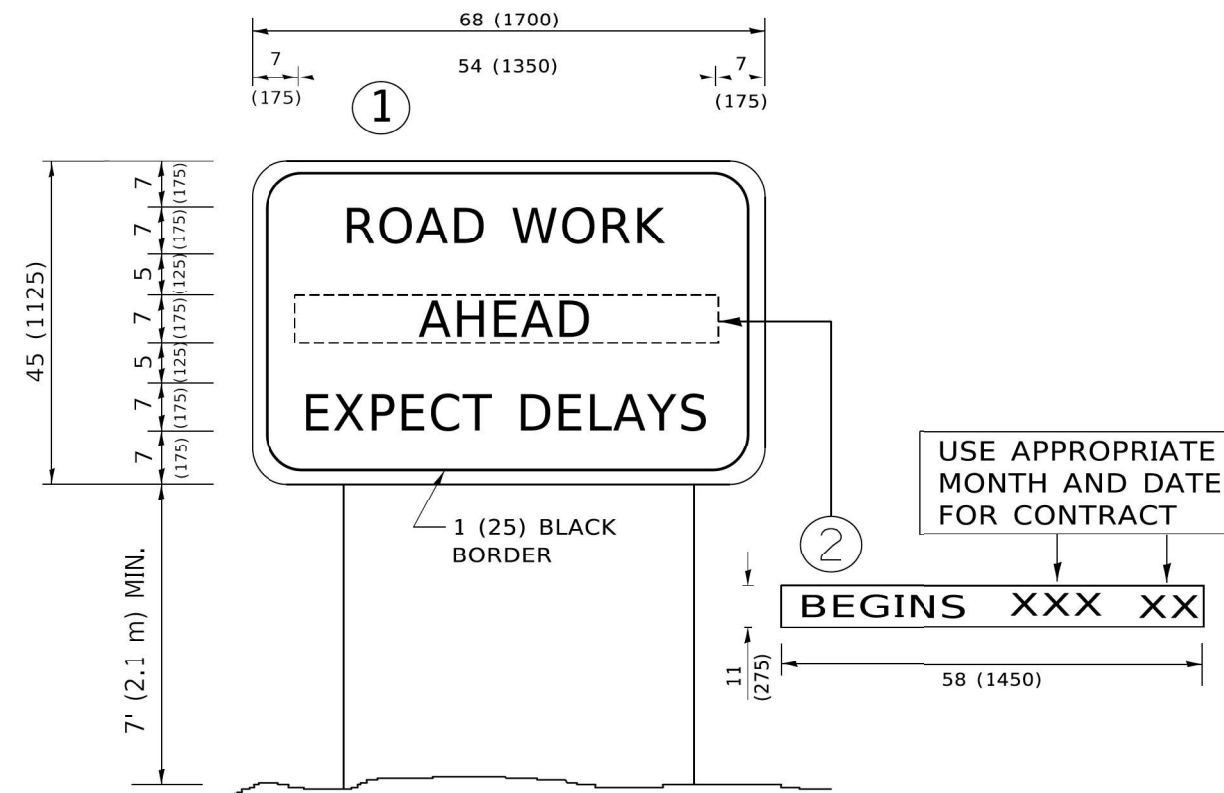
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PLOT SCALE = 0.2000 ' / in.	CHECKED - MM	REVISED -
PLOT DATE = 3/18/2022	DATE - 02/18/2023	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

FREeway/EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS  
AT WORK ZONE OPENINGS ON FREEWAYS/EXPRESSWAYS

SCALE: SHEET 7 OF 9 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	75
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = foitemj	DESIGNED -	REVISED - R, MIRS 09-15-97
DRAWN -	CHECKED -	REVISED - R, MIRS 12-11-97
PLOT SCALE = 30,0000 ' / in.	DATE -	REVISED - T, RAMMACHER 02-02-99
PLOT DATE = 3/4/2019		REVISED - C, JUCIUS 01-31-07

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		ARTERIAL ROAD INFORMATION SIGN		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	TC-22	CONTRACT NO.		
				ILLINOIS FED. AID PROJECT				

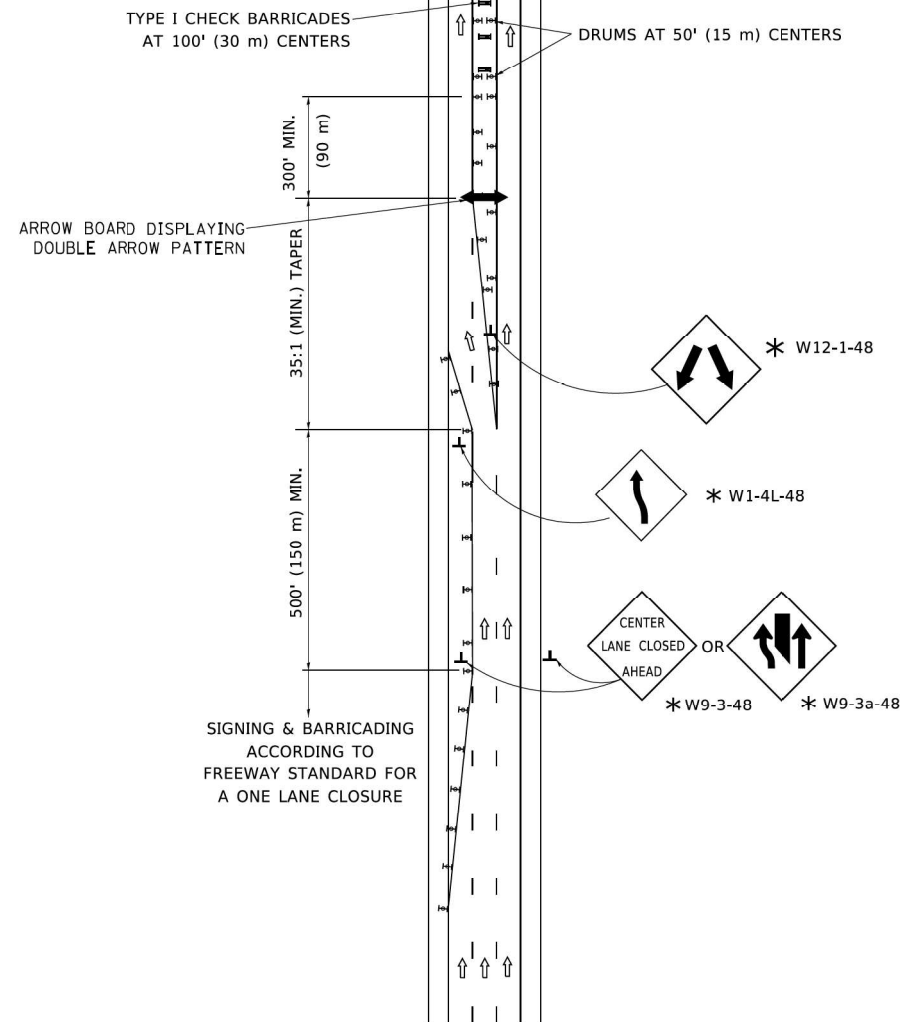
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PLOT DATE = 3/18/2022		

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		ARTERIAL ROAD INFORMATION SIGN		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		SCALE:	SHEET 8 OF 9 SHEETS	STA. TO STA.	290	2021-140-BR	DUPAGE	77
				ILLINOIS FED. AID PROJECT		CONTRACT NO. 62P58		

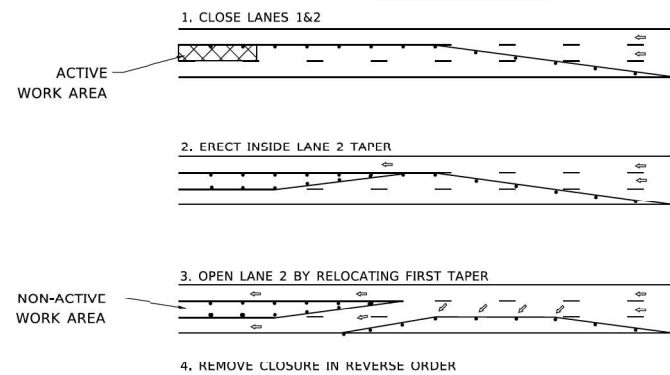
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### CENTER LANE CLOSURE



#### INSTALLATION SEQUENCE

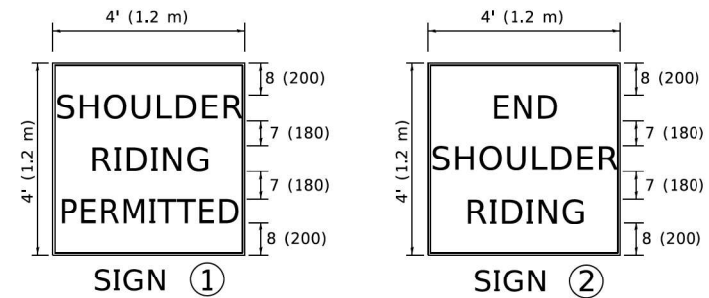
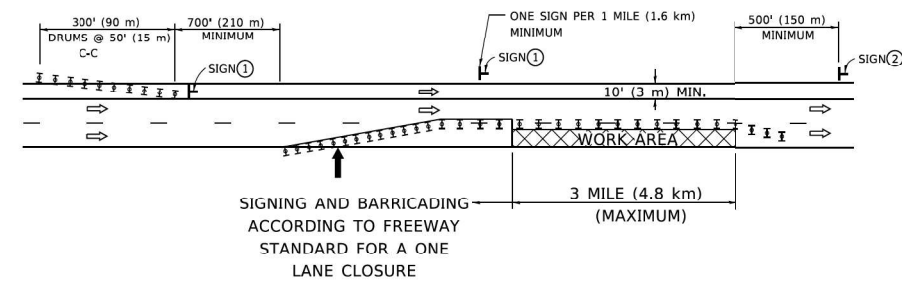
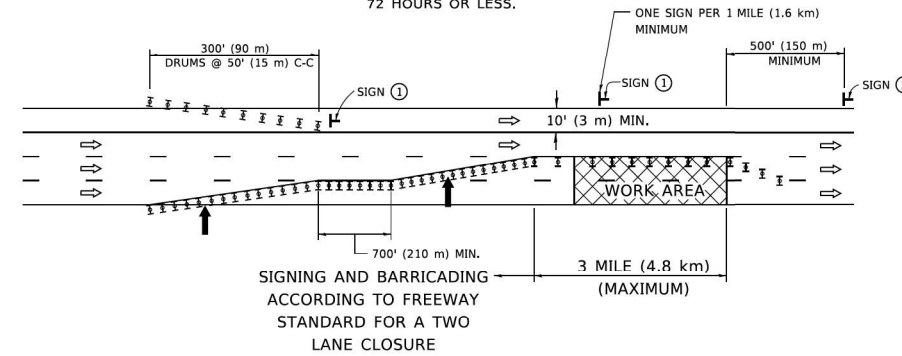


#### NOTES:

1. DRUMS WITH STEADY BURN LIGHTS SHALL BE USED AT 50' (15 m) CENTERS ON ALL TAPERS AND TANGENTS IN ADVANCE OF WORK AREA.
2. CLOSURE SHALL BE USED ONLY FOR OPERATIONS LASTING 72 HOURS OR LESS.
3. CENTER LANE CLOSURE CONFIGURATION IS NOT TO BE USED WITH WORKERS PRESENT.

### SHOULDER LANE

NOTE:  
CLOSURE SHALL BE USED ONLY FOR OPERATIONS LASTING 72 HOURS OR LESS.



6 (150) SERIES "C" LEGEND  
BLACK LEGEND  
WHITE REFLECT. BACKGROUND  
1 (25) BORDER

#### SYMBOLS

- ↑ DIRECTION OF TRAFFIC
- ➔ ARROWBOARD
- ▣ ACTIVE WORK AREA
- ✱ SIGN ON PORTABLE OR PERMANENT SUPPORT ✱
- ⊥ TYPE II BARRICADE, OR DRUM WITH MONO-DIRECTIONAL STEADY BURN LIGHT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

✱ ALL SIGNS SHALL BE MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).

USER NAME = footemj	DESIGNED -	REVISED - J.A.F. 04-03
	DRAWN -	REVISED - S.P.B. 01-07
PLOT SCALE = 30,0000' / in.	CHECKED -	REVISED - S.P.B. 12-09
PLOT DATE = 3/18/2019	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY  
CENTER LANE CLOSURE SHOULDER LANE

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-25			
ILLINOIS		FED. AID PROJECT		

MODEL: Default  
FILE NAME: D:\01422-2ht-d\1222-2ht-d\1222-2ht-d.dwg

**garza karhoff**  
ENGINEERING, LLC

USER NAME = amalia.baymundo	DESIGNED - AMB	REVISED -
	DRAWN - AMB	REVISED -
PLOT SCALE = 0.2000' / in.	CHECKED - MM	REVISED -
PLOT DATE = 3/18/2022	DATE - 02/18/2023	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY  
CENTER LANE CLOSURE SHOULDER LANE

SCALE: SHEET 9 OF 9 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2021-140-BR	DUPAGE	77	77
ILLINOIS		FED. AID PROJECT		