06-17-2022 LETTING ITEM 004

THIS PROJECT IS LOCATED IN THE VILLAGE OF CRETE

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	1
	ILLINOIS	CONTRACT NO. 62N50		

D-91-100-21

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED
HIGHWAY PLANS

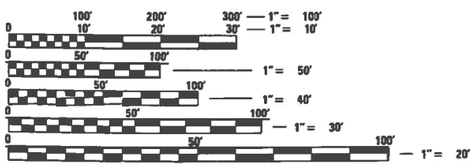
F.A.P. ROUTE 876: IL–1 (MAIN STREET)
UNION AVENUE TO BURVILLE ROAD
SECTION 2021–036–RS
PROJECT NHPP–LSCM(170)
SMART OVERLAY AND ADA IMPROVEMENTS
WILL COUNTY
C–91–123–21

FOR INDEX OF SHEETS AND STANDARDS, SEE SHEET NO. 2

DESIGN DESIGNATION

IL–1 (MAIN STREET) = OTHER PRINCIPAL ARTERIAL
2019 AADT =
10,300 (UNION AVE TO W EXCHANGE ST)
6,800 (W EXCHANGE ST TO W BURVILLE RD)

POSTED SPEED LIMIT =
40 MPH (UNION AVE TO 5TH ST)
35 MPH (5TH ST TO 1ST ST)
30 MPH (1ST ST TO CASS ST)
35 MPH (CASS ST TO BURVILLE RD)

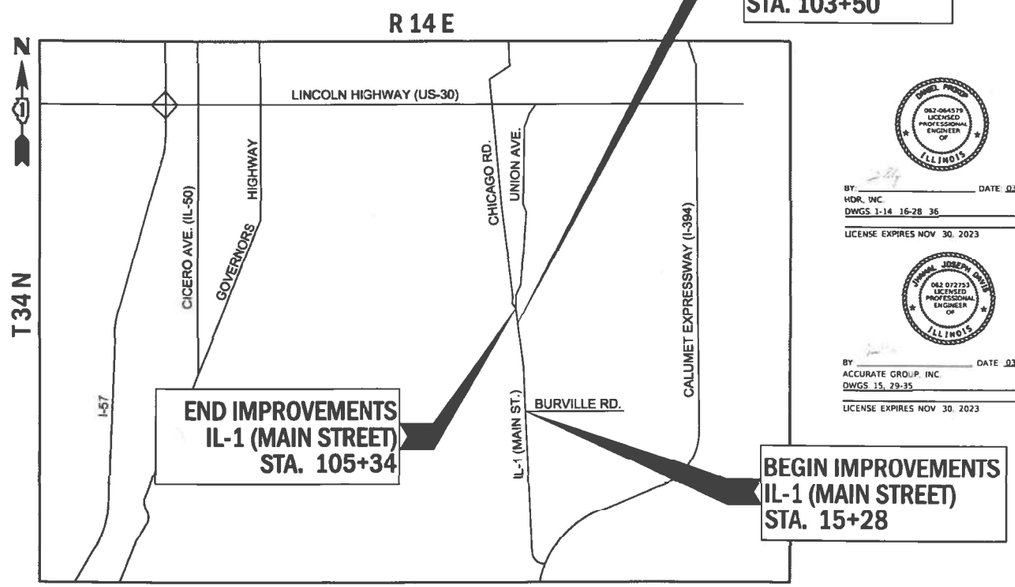


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1–800–892–0123
OR 811

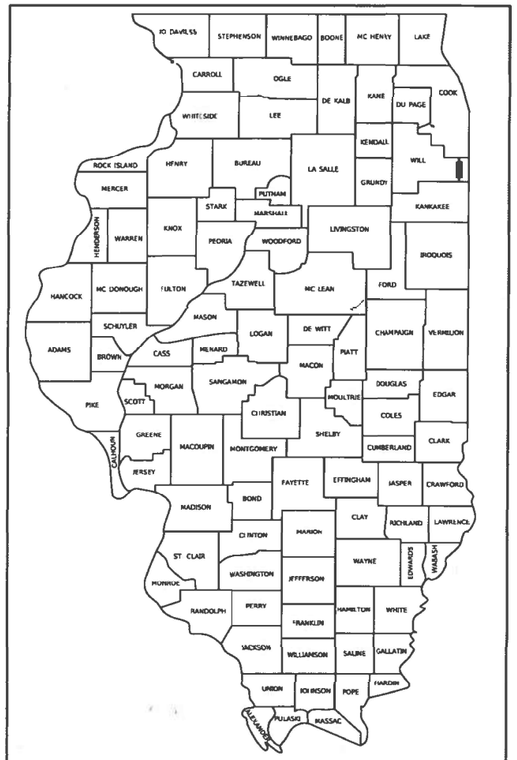
PROJECT ENGINEER: VESELIN VELICHKOV, PE (847) 705–4432
PROJECT MANAGER: FAWAD AQUEEL, PE, PTOE

CONTRACT NO. 62N50



LOCATION MAP
SCALE = N.T.S.

PROJECT LENGTH (GROSS/NET) = 9,006/8,606 FT (1.71/1.63 MILES)



LOCATION OF SECTION INDICATED THUS: –

DANIEL PROKOP
062-064579
LICENSED PROFESSIONAL ENGINEER OF
ILLINOIS

BY: _____ DATE: 03/09/2022
HDR, INC.
DWGS. 1-14 16-28 36
LICENSE EXPIRES NOV 30, 2023

JOSEPH DEITZ
062 072733
LICENSED PROFESSIONAL ENGINEER OF
ILLINOIS

BY _____ DATE 03/09/2022
ACCURATE GROUP, INC.
DWGS. 15, 29-35
LICENSE EXPIRES NOV 30, 2023

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED March 11 20 22
Jose Rios / DB
REGIONAL ENGINEER

May 13, 2022
ENGINEER OF DESIGN AND ENVIRONMENT

May 13, 2022
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

HDR
9450 W. Bryn Mawr Ave., Suite 400
Chicago, IL 60018
773-380-7900
HDR, Inc.
DESIGN FIRM REGISTRATION NUMBER 184.001070

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
000001-08	STANDARD SYMBOL, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424021-06	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-05	FRAMES AND LIDS TYPE 1
604051-04	FRAME AND GRATE, TYPE 11
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5m) AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) to 24" (600 mm) FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' (4.5m) AWAY
701301-04	LANE CLOSURE 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L,2W MOVING OPERATIONS-DAY ONLY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS ≤ 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS

GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2022 AND THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2022; THE "DETAILS" IN THE PLANS, AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS. ANY REFERENCE TO "STANDARDS" THROUGHOUT THE PLANS OR SPECIAL PROVISON SHALL BE INTERPRETED AS THE LASTEST IDOT STANDARD. SHOULD A REVISED STANDARD EXIST THAT SUPERSEDES STANDARDS REFERENCED IN THE CONTRACT DOCUMENTS, THE CONTRACTOR IS RESPONSIBLE FOR SEEKING CLARIFICATION FROM THE ENGINEER BEFORE PROCEEDING WITH THE ORDERING OF MATERIALS, SCHEDULING OF PERSONNEL, PERFORMING THE WORK OR ANY OTHER ACTIVITY RELATED TO THE WORK. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING THE CORRECT STARNDARD BEFORE PREFORMING WORK.
2. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOURS NOTIFICATION REQUIRED)
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF CRETE.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
5. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
6. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
7. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
8. ALL PAVEMENT PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, STRUCTURE FRAME REPLACEMENTS, STRUCTURE ADJUSTMENTS, AND STRUCTURE/STORM SEWER TO BE CLEANED, WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
9. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONTRUCTION OF THIS PROJECT.
10. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
11. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
12. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 40 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 40 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
13. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR FOR ARTERIALS AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
14. DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.
15. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE REMOVAL OF PAVEMENT MARKING TAPE, TYPE III SHALL BE PAID FOR AS SHORT TERM PAVEMENT MARKING REMOVAL.
16. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
17. UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
18. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
19. PROPOSED SIDEWALK RAMPS SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
20. THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION
21. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
22. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
23. THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY OR ALL EXISTING ITEMS THAT WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE TO THE SATIFACTION OF ENGINEER.
24. IDOT FACILITIES ARE NOT LOCATED BY JULIE OR DIGGER. IDOT ELECTRICAL FACILITIES INCLUDING ROADWAY LIGHTING, FIBER OPTIC, ITS EQUIPMENT, TRAFFIC SIGNAL AND PUMP STATION FACILITIES ARE LOCATED BY THE DEPARTMENT'S ELECTRICAL MAINTENANCE CONTRACTOR. AS OF THE LETTING DATE, CONTACT THE MEADE ELECTRIC COMPANY AT 773-287-7672.
25. THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.
26. QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL VERIFY FINAL PATCH LOCATIONS IN THE FIELD, PRIOR TO REMOVAL.
27. THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD ENGINEER, ERIC CAMPOS, AT ERIC.CAMPOS@ILLINOIS.GOV TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
28. THE CONTRACTOR SHALL USE 3 CHANGEABLE MESSAGE SIGNS AT LOCATIONS TO BE DETERMINED BY THE ENGINEER FOR A PERIOD FROM ONE WEEK PRIOR TO THE START OF CONSTRUCTION TO THE CONCLUSION OF THE PROJECT.
29. INLET FILTERS SHALL BE USED ON ALL OPEN GRATE DRAINAGE STRUCTURES WITHIN THE PROJECT LIMITS. THE QUANTITIES IN THE PLANS REFLECTS THIS.
30. ALL LOOSE MATERIAL DEPOSITED IN THE FLOWLINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF INLET FILTERS.
31. ALL RAISED REFELCTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (TC-11)" STANDARD DETAIL.
32. ALL PAVEMENT MARKINGS SHALL BE INSTALLED IN ACCORDANCE WITH DISTRICT ONE TYPICAL PAVEMENT MARKING DETAIL TC-13.
33. ALL MILLED SURFACES SHALL BE AT A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.
34. ONLY CORNERS WITH CURB RAMP IMPROVEMENT SYMBOLS WILL BE RECONSTRUCTED UNDER THIS CONTRACT.

HDR
9450 W. BRYN MAWR AVE.
ROSEMONT, IL 60018

USER NAME = RAZEVEDO	DESIGNED - MSM	REVISED -
	DRAWN - HL	REVISED -
PLOT SCALE = 200.0000 ' / in.	CHECKED - JRY	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, HIGHWAY STANDARDS, AND GENERAL NOTES
IL-1 – MAIN ST. (UNION AVE. TO BURVILLE RD.)

SCALE: N.T.S. | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	2
		CONTRACT NO. 62N50		
		ILLINOIS	FED. AID PROJECT	

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY URBAN	0005 80% FED 20% STATE	0005 100% STATE	0021 80% FED 20% STATE
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	72	72		
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	58	58		
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	9	9		
20200100	EARTH EXCAVATION	CU YD	56	56		
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	12	12		
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	290	290		
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	80	80		
25200110	SODDING, SALT TOLERANT	SQ YD	80	80		
25200200	SUPPLEMENTAL WATERING	UNIT	1.2	1.2		
28000510	INLET FILTERS	EACH	155	155		
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	12	12		
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	48	48		
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	17,444	17,444		
40600370	LONGITUDINAL JOINT SEALANT	FOOT	21,035	21,035		
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	39	39		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	394	394		
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	215	215		
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	3,799	3,799		
42001300	PROTECTIVE COAT	SQ YD	257	257		
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2,136	2,136		
42400800	DETECTABLE WARNINGS	SQ FT	437	437		
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"	SQ YD	38,374	38,374		
44000600	SIDEWALK REMOVAL	SQ FT	2,136	2,136		
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SQ YD	1,275	1,275		
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	469	469		
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	270	270		

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY URBAN	0005 80% FED 20% STATE	0005 100% STATE	0021 80% FED 20% STATE
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	420	420		
60250200	CATCH BASINS TO BE ADJUSTED	EACH	5	5		
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1		
60255500	MANHOLES TO BE ADJUSTED	EACH	4	4		
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1	1		
60260100	INLETS TO BE ADJUSTED	EACH	8	8		
60262700	INLETS TO BE RECONSTRUCTED	EACH	1	1		
60266600	VALVE BOXES TO BE ADJUSTED	EACH	2	2		
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	3	3		
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	3	3		
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	25	25		
63200310	GUARDRAIL REMOVAL	FOOT	25	25		
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	56	56		
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	10	10		
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1		
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1		
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	10	10		
67100100	MOBILIZATION	L SUM	1	1		
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1		
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1		
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1		
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1		
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	6	6		
70300100	SHORT TERM PAVEMENT MARKING	FOOT	21,019	21,019		
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	9,187	9,187		
70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	299	299		

LEGEND

* - DENOTES SPECIALTY ITEM

MODEL: 2D MODEL
FILE NAME: pw:\\pwhdnusce01:nDR_US_Central_01\Documents\IL DOT\IDOT Pbs_IT Serv_Various_TO_4_199-15\IDOT PTB_199-15_T06_IL1_Main\6.0_CAD_BIM\6.2_WIP\6.2.3_CADD_Sheet\Roadway\Summary of Quantities

HDR ENGINEERING, INC.
9450 W. BRYN MAWR AVE.
ROSEMONT, IL 60018

USER NAME = RAZEVEDO	DESIGNED - MSM	REVISED -
	DRAWN - HL	REVISED -
PLOT SCALE = 200.0000 ' / in.	CHECKED - JRY	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL-1 – MAIN ST. (UNION AVE. TO BURVILLE RD.)

SCALE: N.T.S. | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	3
		CONTRACT NO. 62N50		
		ILLINOIS	FED. AID PROJECT	

REV-SEP

	CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY URBAN	0005 80% FED 20% STATE	0005 100% STATE	0021 80% FED 20% STATE
	70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	17,501	17,501		
	70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	776	776		
	70300251	TEMPORARY PAVEMENT MARKING - LINE 8"- PAINT	FOOT	185	185		
	70300261	TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	2,274	2,274		
	70300281	TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	284	284		
	70306120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE III TAPE	FOOT	10,510	10,510		
*	72000100	SIGN PANEL - TYPE 1	SQ FT	33	33		
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	299	299		
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	17,501	17,501		
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	776	776		
*	78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	185	185		
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	2,274	2,274		
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	284	284		
*	78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	40	40		
*	78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	5	5		
*	78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	39	39		
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	506	506		
*	78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	5	5		
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	506	506		
	78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	9,187	9,187		
*	81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	40			40
*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1			1
*	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1,274			1,274
*	87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	60			60
*	87900200	DRILL EXISTING HANDHOLE	EACH	3			3
*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	1,326			1,326
Ø	Z0076600	TRAINEES	HOURS	500	500		
Ø	Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500		

	CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY URBAN	0005 80% FED 20% STATE	0005 100% STATE	0021 80% FED 20% STATE
*	89500400	RELOCATE EXISTING PEDESTRIAN PUSH-BUTTON	EACH	3			3
*	89502200	MODIFY EXISTING CONTROLLER	EACH	1			1
*	89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	691			691
*	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1			1
*	89502376	REBUILD EXISTING HANDHOLE	EACH	1			1
	X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1		
	X0327611	REMOVE AND REINSTALL BRICK PAVER	SQ FT	488	488		
	X0326681	REMOVE AND RE-ERECT BOULDERS	L SUM	1	1		
*	X1400378	PEDESTRIAN SIGNAL POST, 5 FT.	EACH	3			3
	X2010100	TREE LIMB REMOVAL (4 TO 10 INCHES DIAMETER)	EACH	2	2		
	X2010350	TREE REMOVAL, ACRES (SPECIAL)	ACRE	0.25	0.25		
	X2100002	PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE	UNIT	2	2		
	X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	312	312		
	X5538000	STORM SEWERS TO BE CLEANED 18"	FOOT	850		850	
	X5538200	STORM SEWERS TO BE CLEANED 24"	FOOT	500		500	
	X5538600	STORM SEWERS TO BE CLEANED 36"	FOOT	300		300	
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	46	46		
	X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12		
*	X8760200	ACCESSIBLE PEDESTRIAN SIGNALS	EACH	8			8
*	X8780012	CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	12			12
	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1,245	1,245		
	Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	87		87	
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	115	115		
*	Z0033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1			1

LEGEND

* - DENOTES SPECIALTY ITEM

ø 0042

MODEL: 2D MODEL
FILE NAME: pw:\\pwhdrusceen01-hdr.US Central 01\Documents\IL DOT\IDOT Pns II Serv Various TO s 199-15\IDOT PTB 199-15 TO6 IL1 Main\6.0 CAD BIM\6.2 WIP\6.2.3 CADD Sheet\RoadwaySummary of Quantities

HDR ENGINEERING, INC.
9450 W. BRYN MAWR AVE.
ROSEMONT, IL 60018

USER NAME = RAZEVEDO	DESIGNED - MSM	REVISED -
	DRAWN - HL	REVISED -
PLOT SCALE = 200.0000 ' / in.	CHECKED - JRY	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

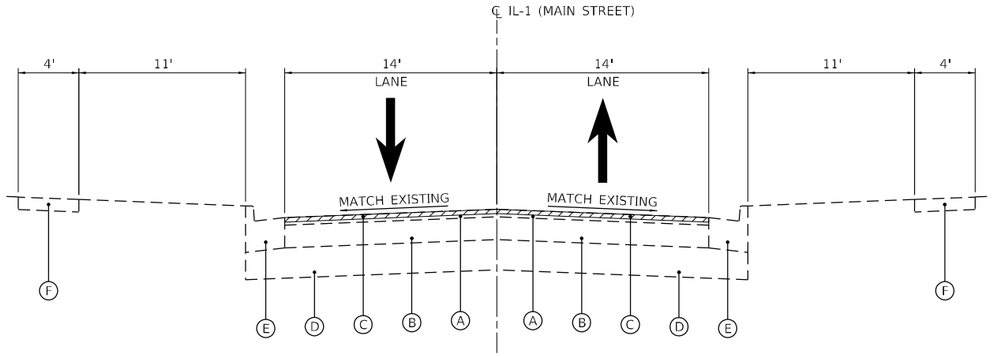
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL-1 – MAIN ST. (UNION AVE. TO BURVILLE RD.)

SCALE: N.T.S. | SHEET 2 OF 2 SHEETS | STA. | TO STA.

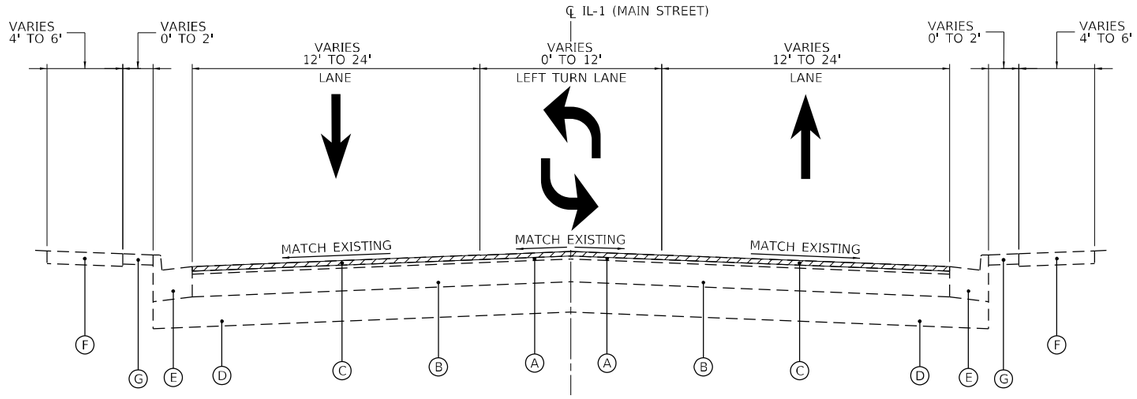
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	4
		CONTRACT NO. 62N50		
	ILLINOIS	FED. AID PROJECT		

REV-SEP



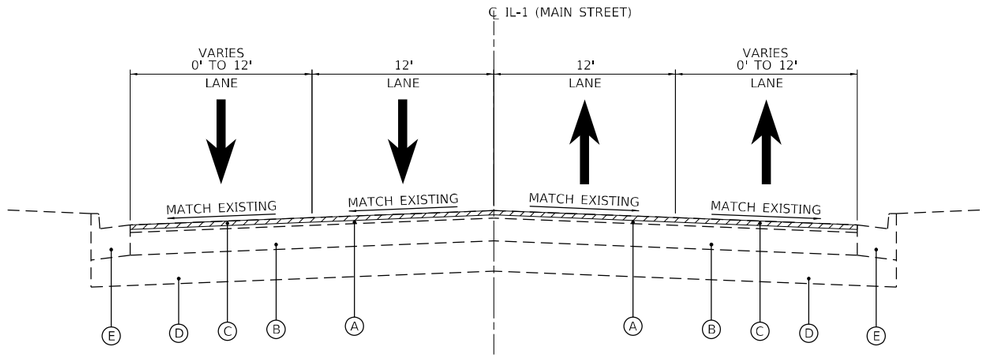
EXISTING TYPICAL SECTION

STA. 15+28 TO STA. 41+74



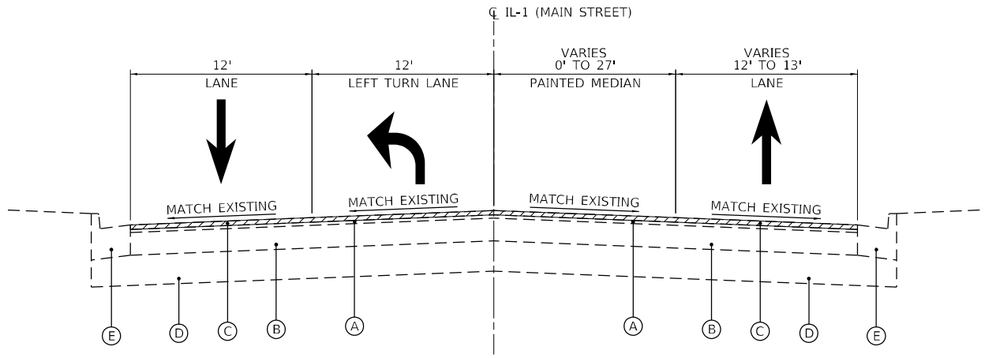
EXISTING TYPICAL SECTION

STA. 41+74 TO STA. 71+50



EXISTING TYPICAL SECTION

STA. 71+50 TO STA. 96+38



EXISTING TYPICAL SECTION

STA. 96+38 TO STA. 105+34

*BRIDGE OMISSION STA. 99+50 TO STA. 103+50

EXISTING LEGEND

- (A) EXISTING HOT-MIX ASPHALT SURFACE COURSE, 3"
- (B) EXISTING PCC PAVEMENT, 9"
- (C) HOT-MIX ASPHALT SURFACE REMOVAL, 1.75"
- (D) EXISTING SUBBASE GRANULAR MATERIAL, TYPE B
- (E) EXISTING CONCRETE CURB & GUTTER
- (F) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (G) EXISTING BRICK PAVERS

HDR ENGINEERING, INC.
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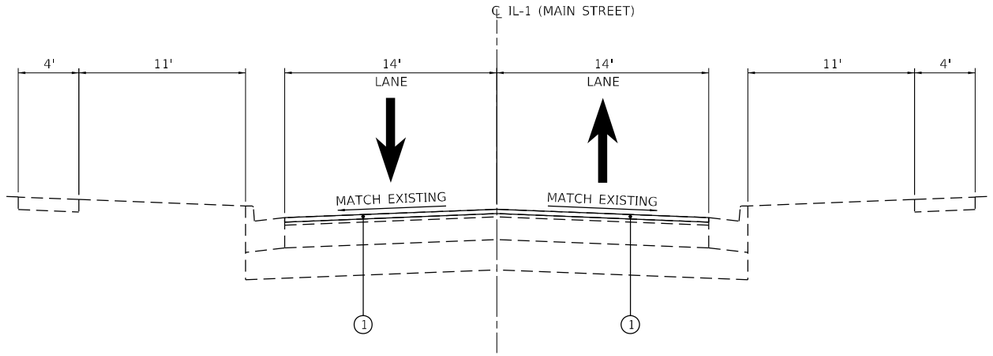
PLOT DATE = 3/18/2022	DATE - 11/12/2021	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING TYPICAL SECTIONS
IL-1 – MAIN ST. (UNION AVE. TO BURVILLE RD.)

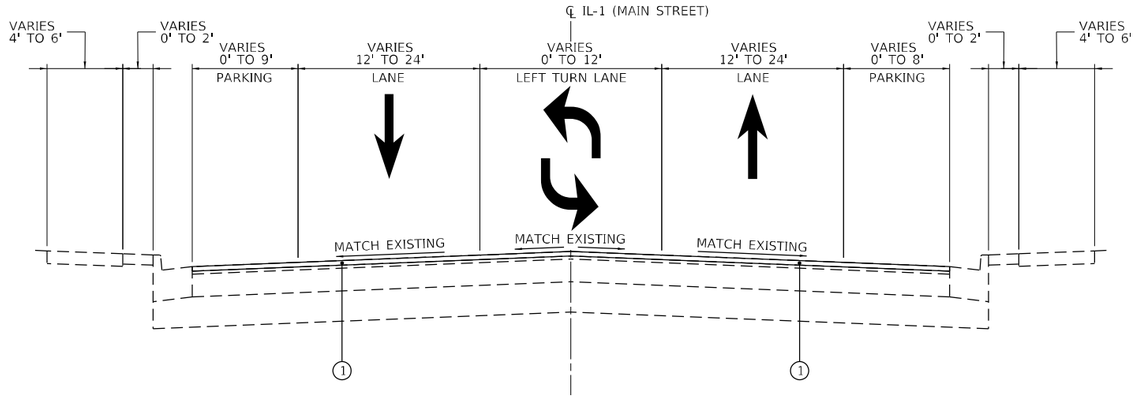
SCALE: N.T.S. | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	5
		CONTRACT NO. 62N50		
		ILLINOIS	FED. AID PROJECT	



PROPOSED TYPICAL SECTION

STA. 15+28 TO STA. 41+74



PROPOSED TYPICAL SECTION

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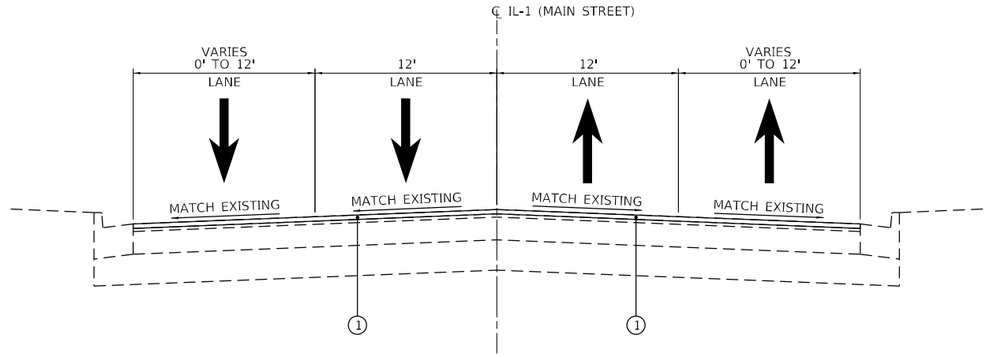
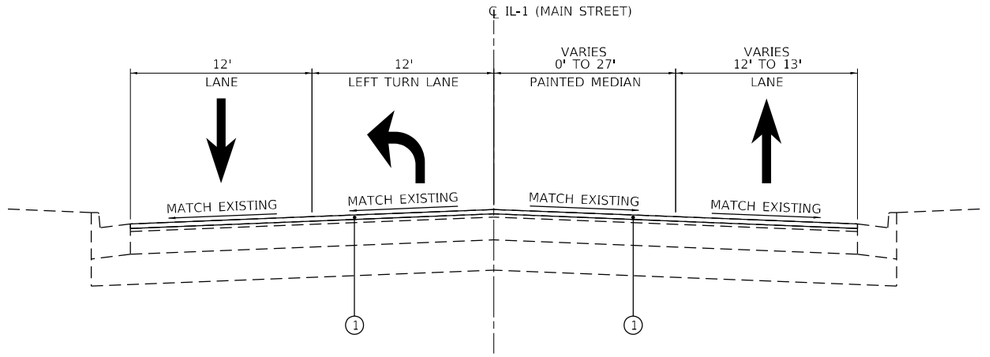


PROPOSED TYPICAL SECTION

STA. 71+50 TO STA. 96+38



PROPOSED TYPICAL SECTION

STA. 96+38 TO STA. 105+34

*BRIDGE OMISSION STA. 99+50 TO STA. 103+50

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ NDES	QMP
PAVEMENT RESURFACING (SMART)		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70; 1.75"	4% @ 70 GYR.	QCP
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19mm)	4% @ 70 GYR.	QC/QA
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (HMA BINDER IL-19mm)	4% @ 70 GYR.	QC/QA
QMP DESIGNATIONS: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

NOTES FOR HMA MIXTURE REQUIREMENTS:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.
3. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILLED SURFACE.
4. FOR THE EXISTING HMA SURFACE, THE CONTRACTOR SHALL DO PAVEMENT PATCHING FIRST, THEN PAVEMENT MILLING PER BD-22 DETAIL.

PROPOSED LEGEND

(1) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70; 1.75"

HDR ENGINEERING, INC.
9450 W. BRYN MAWR AVE.
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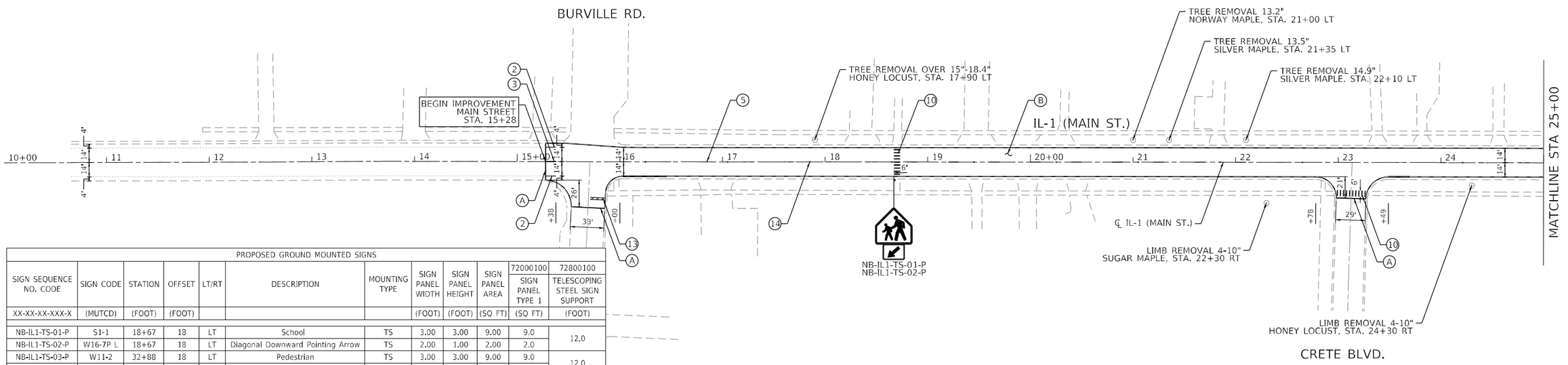
PLOT SCALE = 10.0000 ' / in.	CHECKED - JRY	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
IL-1 – MAIN ST. (UNION AVE. TO BURVILLE RD.)

SCALE: N.T.S. | SHEET 2 OF 2 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	6
		CONTRACT NO. 62N50		
		ILLINOIS	FED. AID PROJECT	



PROPOSED GROUND MOUNTED SIGNS											
SIGN SEQUENCE NO. CODE	SIGN CODE	STATION	OFFSET	LT/RT	DESCRIPTION	MOUNTING TYPE	SIGN PANEL WIDTH	SIGN PANEL HEIGHT	SIGN PANEL AREA	72000100 SIGN PANEL TYPE 1	72800100 TELESCOPING STEEL SIGN SUPPORT
XX-XX-XX-XXX-X	(MUTCD)	(FOOT)	(FOOT)				(FOOT)	(FOOT)	(SQ FT)	(SQ FT)	(FOOT)
NB-IL1-TS-01-P	S1-1	18+67	18	LT	School	TS	3.00	3.00	9.00	9.0	12.0
NB-IL1-TS-02-P	W16-7P L	18+67	18	LT	Diagonal Downward Pointing Arrow	TS	2.00	1.00	2.00	2.0	
NB-IL1-TS-03-P	W11-2	32+88	18	LT	Pedestrian	TS	3.00	3.00	9.00	9.0	12.0
NB-IL1-TS-04-P	W16-7P L	32+88	18	LT	Diagonal Downward Pointing Arrow	TS	2.00	1.00	2.00	2.0	
SB-IL1-TS-01-P	W11-2	33+48	20	RT	Pedestrian	TS	3.00	3.00	9.00	9.0	12.0
SB-IL1-TS-02-P	W16-7P L	33+48	20	RT	Diagonal Downward Pointing Arrow	TS	2.00	1.00	2.00	2.0	
TOTALS										33	36

NOTES

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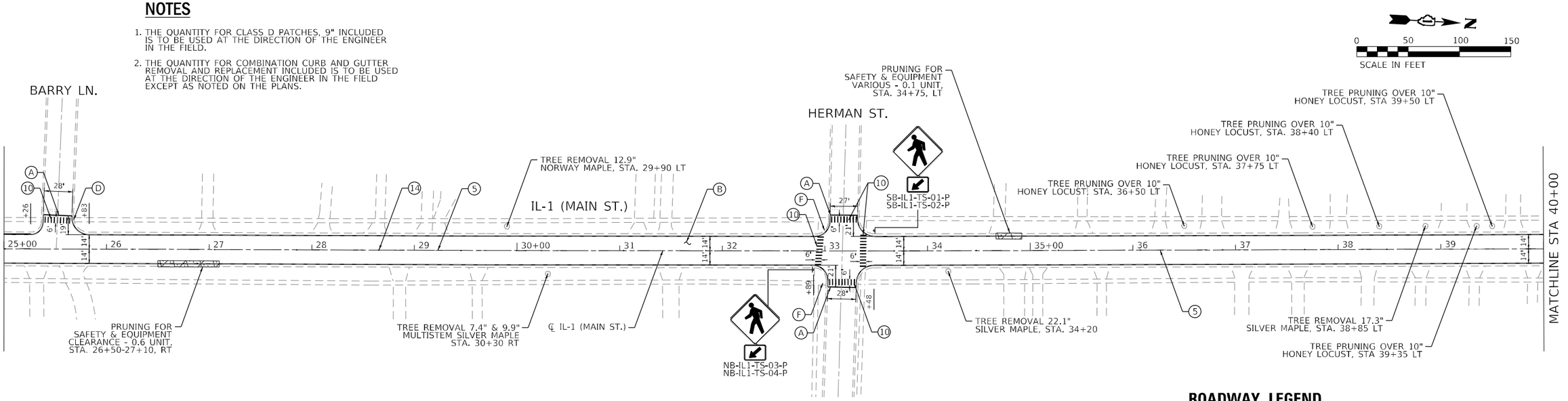

PAVEMENT MARKING LEGEND

1. THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
2. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SOLID WHITE)
3. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE SOLID YELLOW)
4. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (10' DASH, 30' SKIP, WHITE)
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12. THERMOPLASTIC PAVEMENT MARKING - LINE 12" (DIAGONAL & CHEVRON, WHITE)
13. THERMOPLASTIC PAVEMENT MARKING - LINE 24" (SOLID WHITE)
14. RAISED REFLECTIVE PAVEMENT MARKER
15. MODIFIED URETHENE PAVEMENT MARKING - LINE 4" (DOUBLE SOLID YELLOW)
16. MODIFIED URETHENE PAVEMENT MARKING- LINE 24" (SOLID WHITE)

ROADWAY LEGEND

- (A) HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT, 4.5'
- (B) HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 1 3/4"
- (C) PROP. ADA RAMP DETAILS, SEE DETAIL DESIGN ON SHEETS 11-14
- (D) PROP. ADA RAMP DETAILS, SEE PD-01B
- (E) PROP. ADA RAMP DETAILS, SEE PD-01C
- (F) PROP. ADA RAMP DETAILS, SEE PD-03

HDR ENGINEERING, INC.
9450 W. BRYN MAWR AVE.
ROSEMONT, IL 60018

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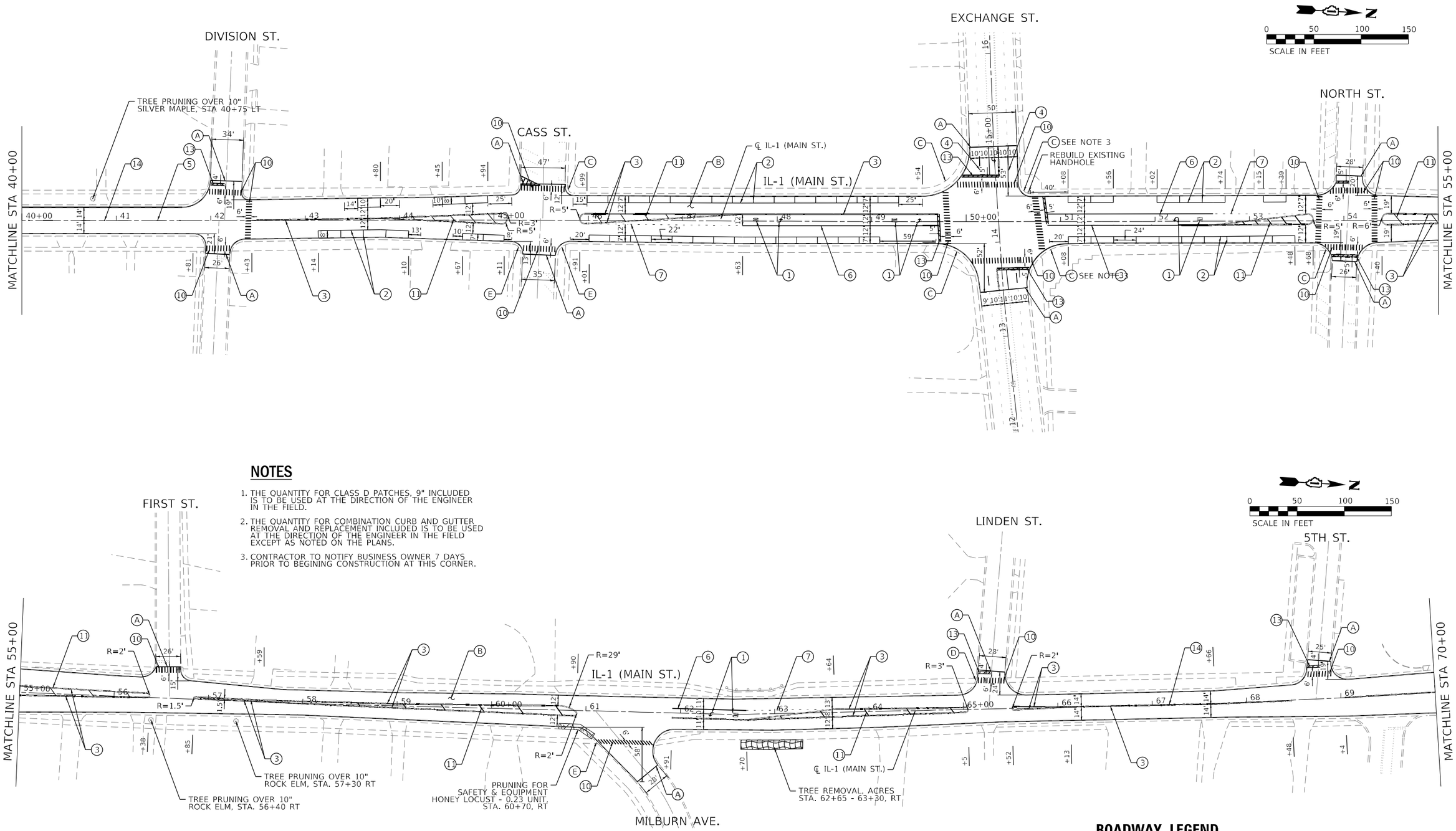
PLOT SCALE = 100.0000 ' / in.	CHECKED - JRY	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS
IL-1 – MAIN ST. (UNION AVE. TO BURVILLE RD.)

SCALE: 1"-50' | SHEET 1 OF 4 SHEETS | STA. 15+28 | TO STA. 40+00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	7
		CONTRACT NO. 62N50		
		ILLINOIS	FED. AID PROJECT	



NOTES

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2. THE QUANTITY FOR COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT INCLUDED IS TO BE USED AT THE DIRECTION OF THE ENGINEER IN THE FIELD EXCEPT AS NOTED ON THE PLANS.
3. CONTRACTOR TO NOTIFY BUSINESS OWNER 7 DAYS PRIOR TO BEGINING CONSTRUCTION AT THIS CORNER.

PAVEMENT MARKING LEGEND

1. THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
2. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SOLID WHITE)
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ROADWAY LEGEND

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS
IL-1 – MAIN ST. (UNION AVE. TO BURVILLE RD.)

SCALE: 1"-50' | SHEET 2 OF 4 SHEETS | STA. 40+00 | TO STA. 70+00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	8
		CONTRACT NO. 62N50		
	ILLINOIS	FED. AID PROJECT		

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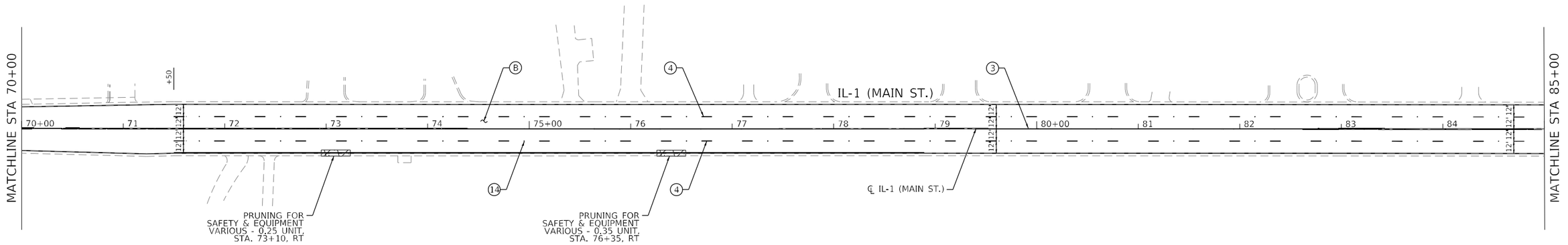

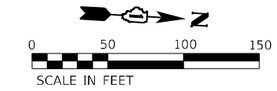

ROADWAY LEGEND

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NOTES

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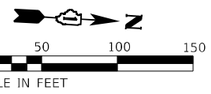

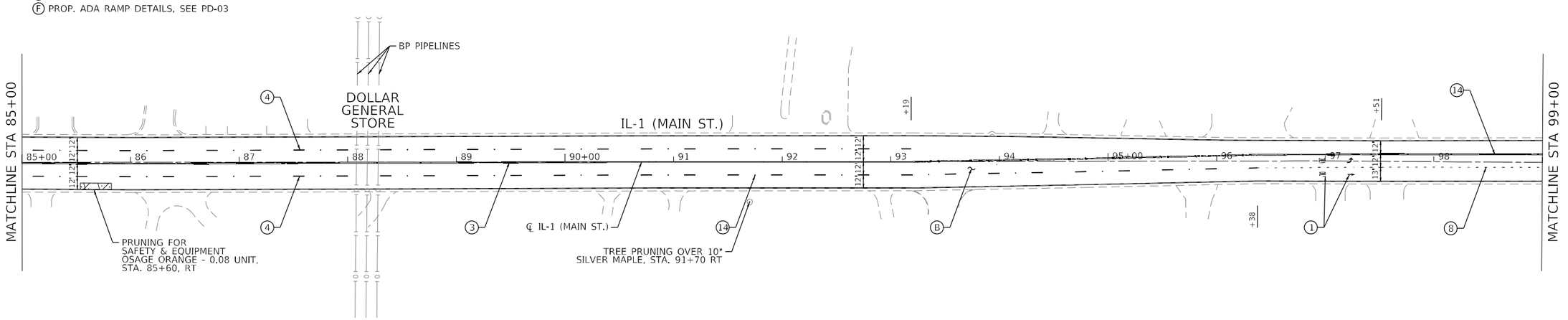

PAVEMENT MARKING LEGEND

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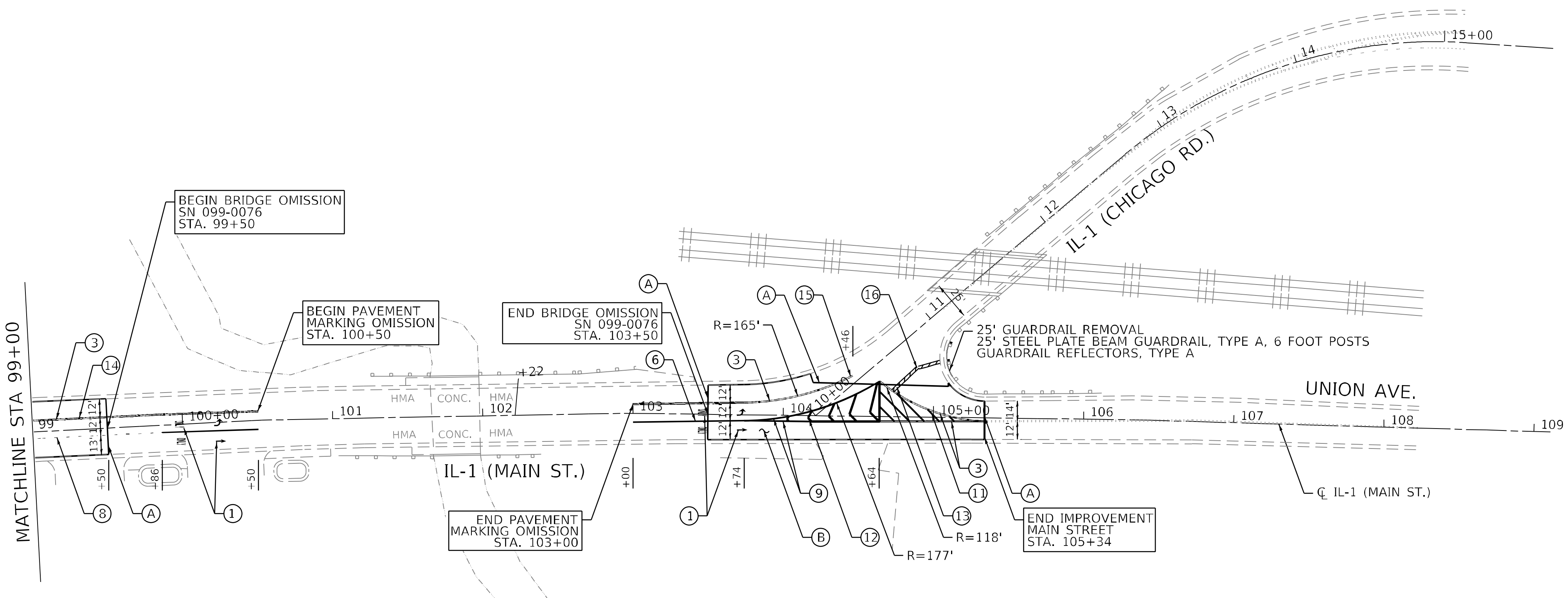
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS
IL-1 – MAIN ST. (UNION AVE. TO BURVILLE RD.)

SCALE: 1"-50' | SHEET 3 OF 4 SHEETS | STA. 70+00 | TO STA. 99+00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	9
		CONTRACT NO. 62N50		
	ILLINOIS	FED. AID PROJECT		



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1. THE QUANTITY FOR CLASS D PATCHES, 9" INCLUDED IS TO BE USED AT THE DIRECTION OF THE ENGINEER IN THE FIELD.
2. THE QUANTITY FOR COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT INCLUDED IS TO BE USED AT THE DIRECTION OF THE ENGINEER IN THE FIELD EXCEPT AS NOTED ON THE PLANS.
3. MODIFIED URETHENE PAVEMENT MARKING SHALL BE USED IN PLACE OF THERMOPLASTIC FOR ALL PAVEMENT MARKINGS ON EXPOSED CONCRETE PAVEMENT.

ROADWAY LEGEND

- (A) HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT, 4.5"
- (B) HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 1 3/4"
- (C) PROP. ADA RAMP DETAILS, SEE DETAIL DESIGN ON SHEETS 11-14
- (D) PROP. ADA RAMP DETAILS, SEE PD-01B
- (E) PROP. ADA RAMP DETAILS, SEE PD-01C
- (F) PROP. ADA RAMP DETAILS, SEE PD-03

N

0 50 100 150

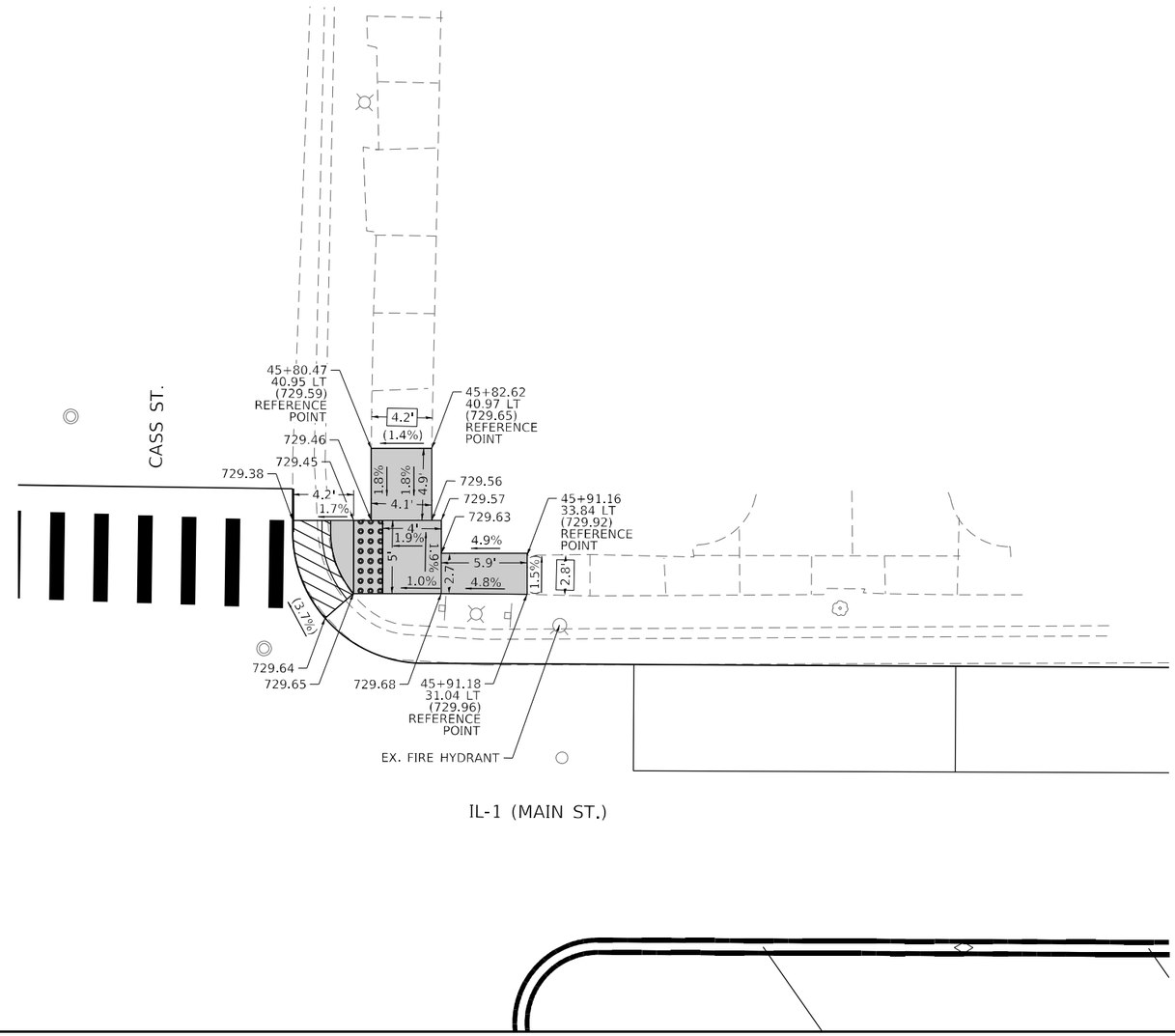
SCALE IN FEET

HDR ENGINEERING, INC. 9450 W. BRYN MAWR AVE. ROSEMONT, IL 60018	USER NAME = RAZEVEDO	DESIGNED - MSM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
		DRAWN - YJP	REVISED -	
	PLOT SCALE = 100.0000 ' / in.	CHECKED - JRY	REVISED -	
	PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -	

ROADWAY AND PAVEMENT MARKING PLANS
IL-1 – MAIN ST. (UNION AVE. TO BURVILLE RD.)

SCALE: 1"-50' | SHEET 4 OF 4 SHEETS | STA. 99+00 TO STA. 105+34

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	10
		CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			



IL-1 (MAIN ST.)

LEGEND

- XX.XX' EXISTING LENGTH
- ( ) EXISTING ELEVATION/SLOPE
- PROPOSED SIDE CURB
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- DEPRESSED CURB & GUTTER

0 5 10 15
SCALE IN FEET

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HDR ENGINEERING, INC. 9450 W. BRYN MAWR AVE. ROSEMONT, IL 60018	USER NAME = RAZEVEDO	DESIGNED - MSM	REVISED -
		DRAWN - YJP	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED - JRY	REVISED -
	PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
IL-1 – MAIN ST. (UNION AVE. TO BURVILLE RD.)

SCALE: 1"=5' | SHEET 1 OF 4 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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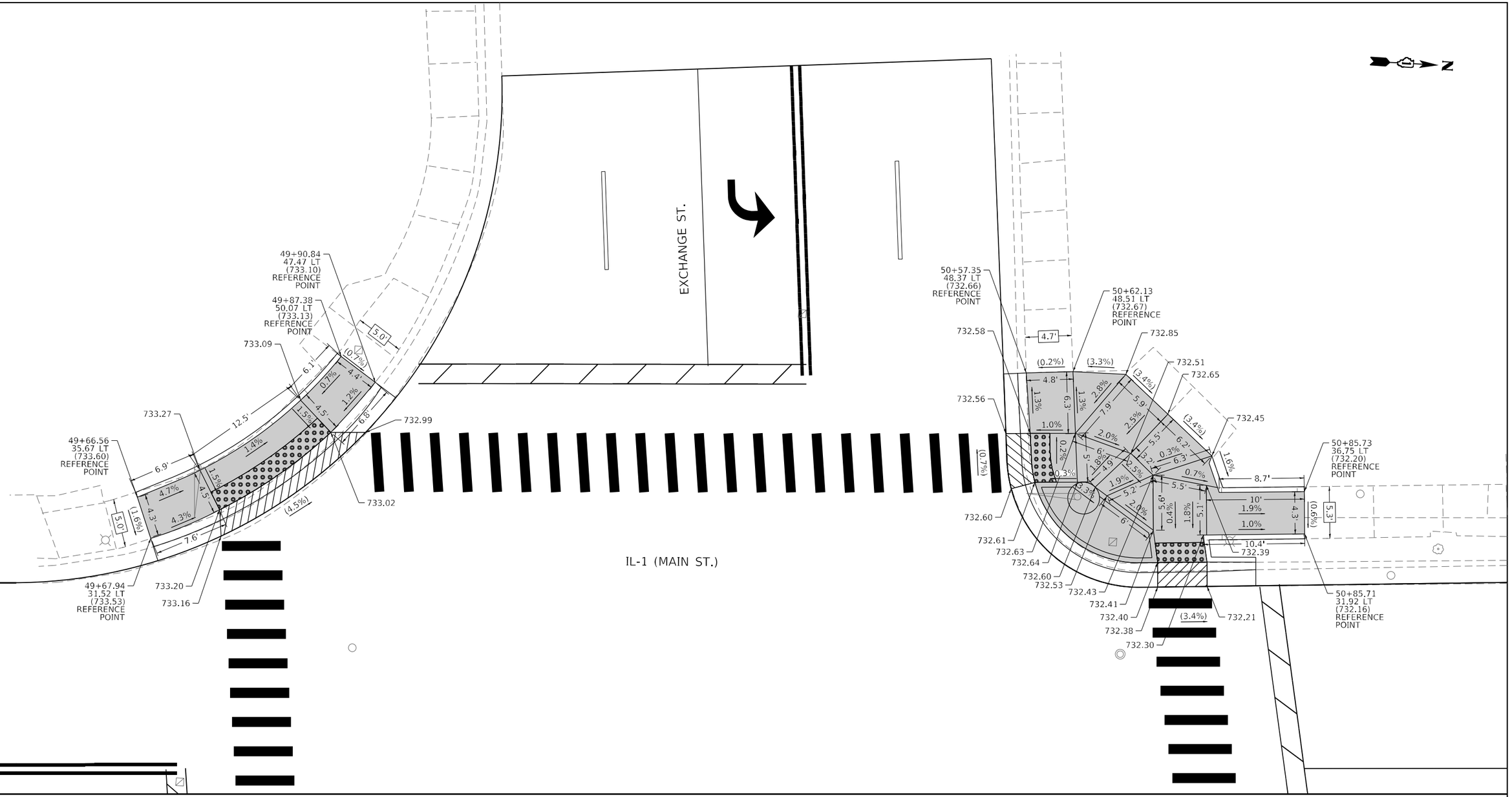
		CONTRACT NO. 62N50		
		ILLINOIS	FED. AID PROJECT	



LEGEND

- XX.XX' EXISTING LENGTH
- ( ) EXISTING ELEVATION/SLOPE
- PROPOSED SIDE CURB
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- DEPRESSED CURB & GUTTER

0 5 10 15
SCALE IN FEET

HDR
9450 W. BRYN MAWR AVE.
ROSEMONT, IL 60018

USER NAME = RAZEVEDO	DESIGNED - MSM	REVISED -
	DRAWN - YJP	REVISED -
PLOT SCALE = 10.0000 ' / in.	CHECKED - JRY	REVISED -
PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
IL-1 – MAIN ST. (UNION AVE. TO BURVILLE RD.)

SCALE: 1"=5' | SHEET 2 OF 4 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	12
		CONTRACT NO. 62N50		
	ILLINOIS	FED. AID PROJECT		

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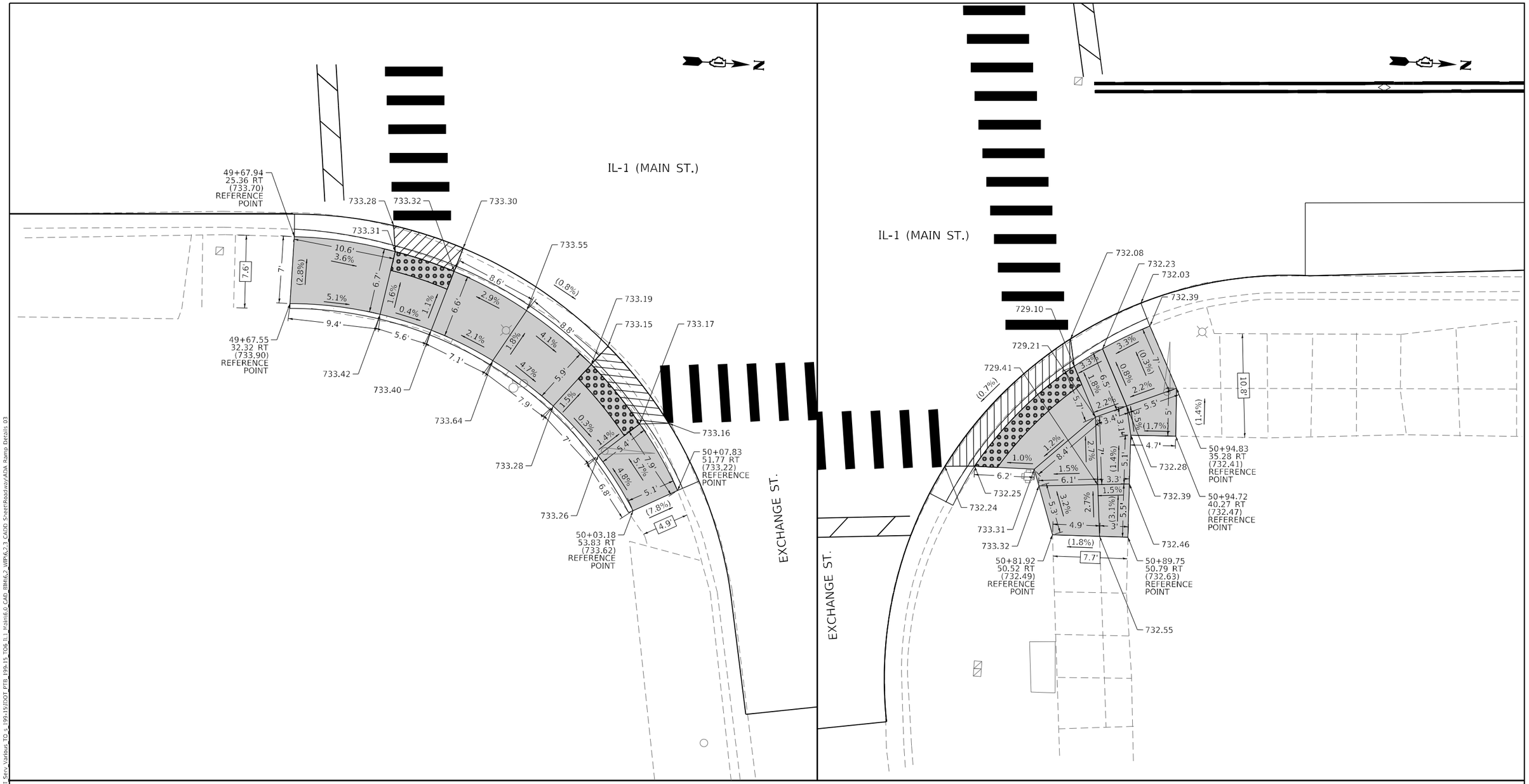

LEGEND

- XX.XX' — EXISTING LENGTH
- ( ) — EXISTING ELEVATION/SLOPE
- PROPOSED SIDE CURB
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- DEPRESSED CURB & GUTTER

0 5 10 15
SCALE IN FEET

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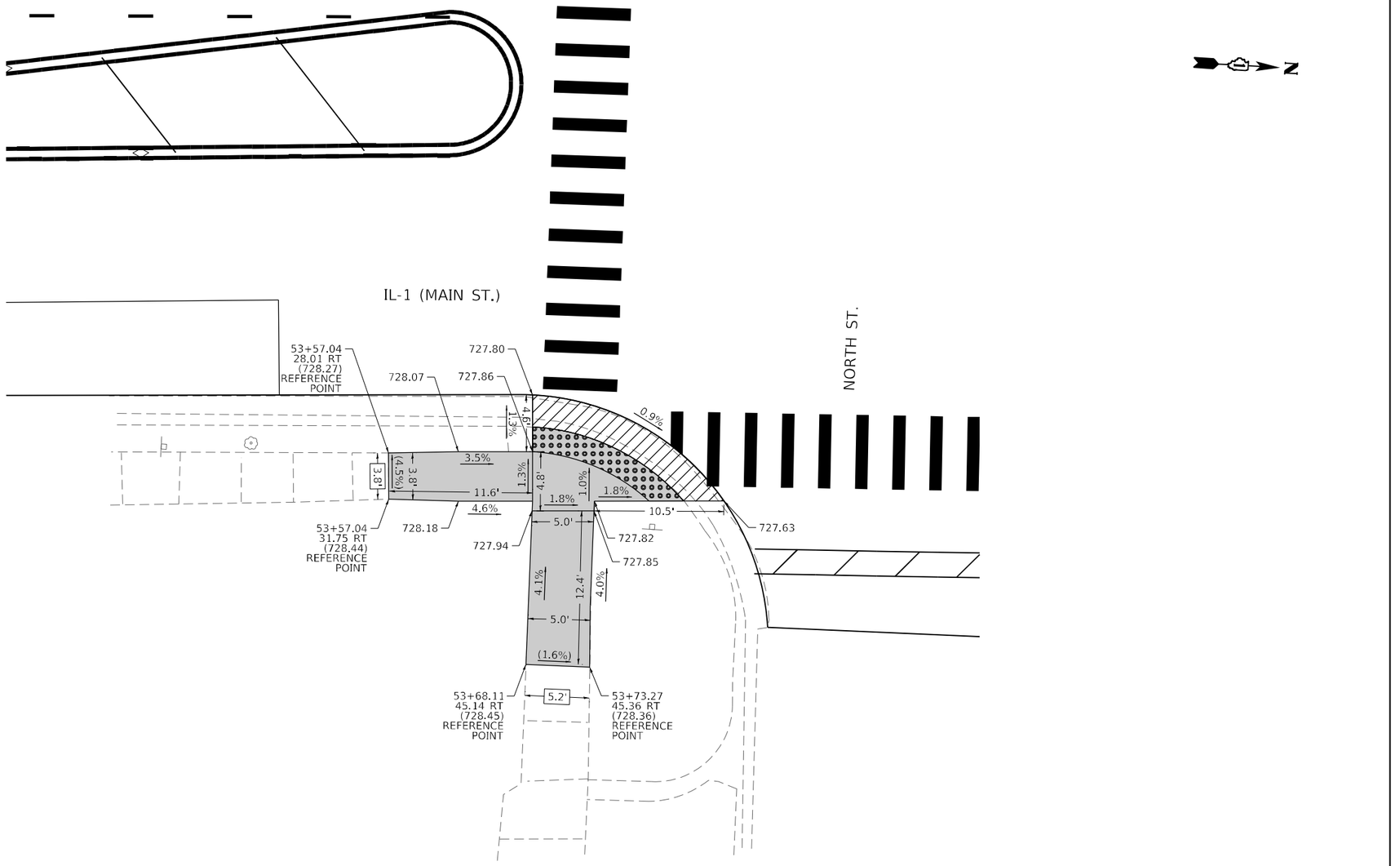
HDR 9450 W. BRYN MAWR AVE. ROSEMONT, IL 60018	USER NAME = RAZEVEDO	DESIGNED - MSM	REVISED -
		DRAWN - YJP	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED - JRY	REVISED -
	PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
IL-1 – MAIN ST. (UNION AVE. TO BURVILLE RD.)

SCALE: 1"=5' | SHEET 3 OF 4 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	13
		CONTRACT NO. 62N50		
	ILLINOIS	FED. AID PROJECT		



LEGEND

- XX.XX' — EXISTING LENGTH
- ( ) — EXISTING ELEVATION/SLOPE
- PROPOSED SIDE CURB
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- DEPRESSED CURB & GUTTER

0 5 10 15
SCALE IN FEET

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HDR 9450 W. BRYN MAWR AVE. ROSEMONT, IL 60018	USER NAME = RAZEVEDO	DESIGNED - MSM	REVISED -
		DRAWN - YJP	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED - JRY	REVISED -
	PLOT DATE = 3/18/2022	DATE - 3/18/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

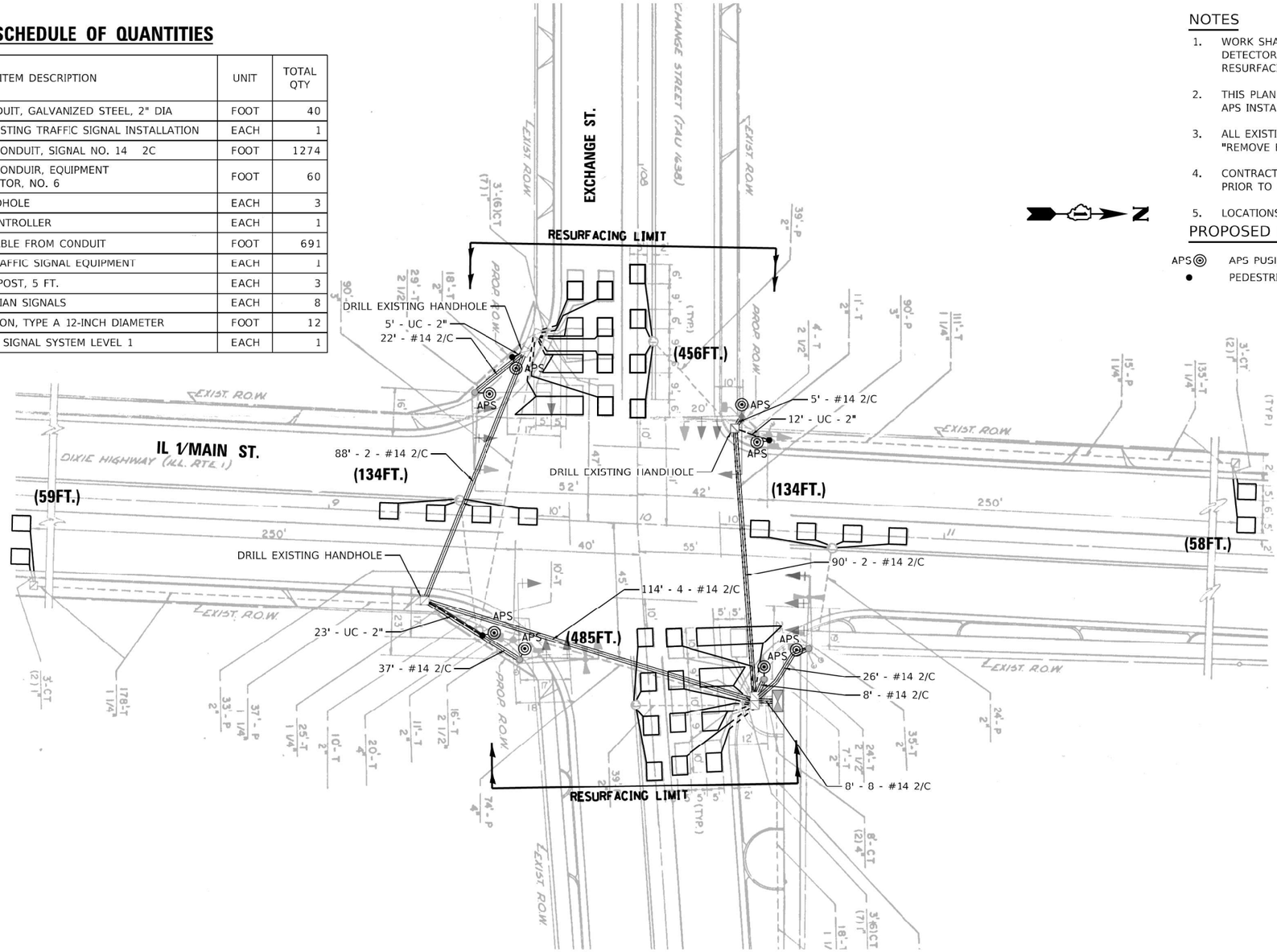
ADA RAMP DETAILS
IL-1 – MAIN ST. (UNION AVE. TO BURVILLE RD.)

SCALE: 1"=5' | SHEET 4 OF 4 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	14
		CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNIT	TOTAL QTY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA	FOOT	40
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1274
ELECTRIC CABLE IN CONDUIR, EQUIPMENT GROUNDING CONDUCTOR, NO. 6	FOOT	60
DRILL EXISTING HANDHOLE	EACH	3
MODIFY EXISTING CONTROLLER	EACH	1
REMOVE EXISTING CABLE FROM CONDUIT	FOOT	691
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	3
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	8
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	12
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1



NOTES

1. WORK SHALL MEET REQUIREMENTS OF THE SPECIAL PROVISION, DETECTOR LOOP REPLACEMENT AND/ OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT AND APS INSTALLATION
3. ALL EXISTING PUSH BUTTONS SHALL BE REMOVED AND PAID FOR UNDER "REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT."
4. CONTRACTOR SHALL FIELD VERIFY ALL EXISTING TRAFFIC SIGNAL EQUIPMENT PRIOR TO ORDERING MATERIAL AND STARTING ANY WORK.
5. LOCATIONS OF PROPOSED TRAFFIC SIGNAL EQUIPMENT ARE APPROXIMATE.

PROPOSED LEGEND

APS◎ APS PUSH-BUTTON

● PEDESTRIAN SIGNAL POST, 5 FT.

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	1326	FOOT

TS#7500

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Accurate GROUP, INC.

USER NAME = jdavis	DESIGNED - JLS	REVISED -	
	DRAWN - JLS	REVISED -	
PLOT SCALE = 40.000 ' / in	CHECKED - JJD	REVISED -	
PLOT DATE = 3/11/2022	DATE - 3/18/2022	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

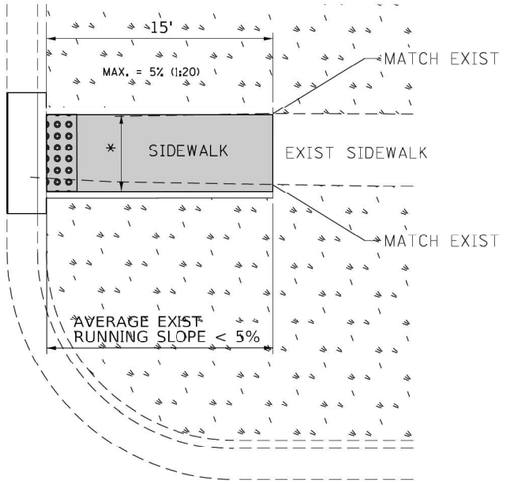
ACCESSIBLE PEDESTRIAN SIGNAL INSTALLATION PLAN
IL 1/MAIN ST. AT W EXCHANGE ST.

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

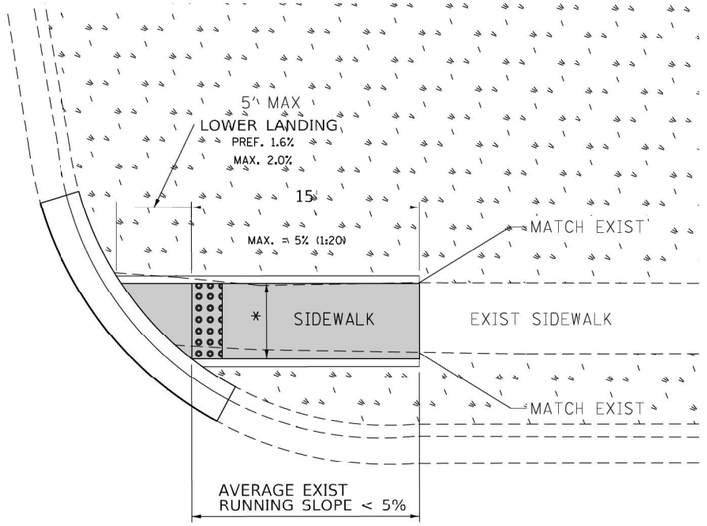
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	15
		CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

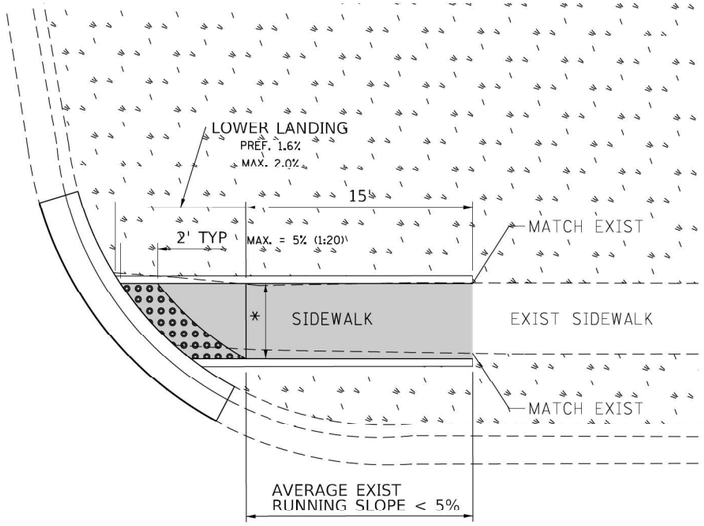
PD-01A



PD-01B



PD-01C



DESIGNER NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

PROPOSED SIDE CURB

EXIST. GRASS

PROPOSED SIDEWALK

DETECTABLE WARNINGS

CONSTRUCTION NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

FILE NAME = S:\WP\PLANPREP\SQUAD_1\Des_RL\Typical ADA details\Typical-ADA-sht-plan.dgn	USER NAME = ledezmarm	DESIGNED - --- --/--/----	REVISED -
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Default	PLOT DATE = 12/17/2019	CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

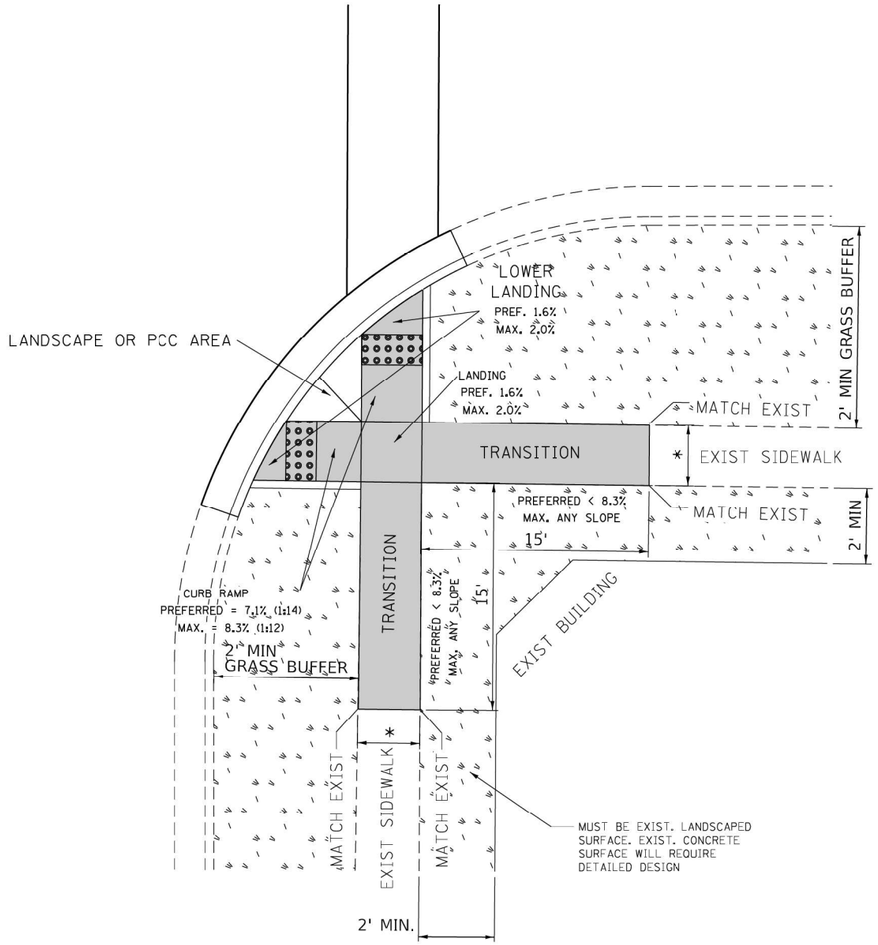
PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-01)

SCALE: | SHEET 1 OF 2 SHEETS | STA. | TO STA.

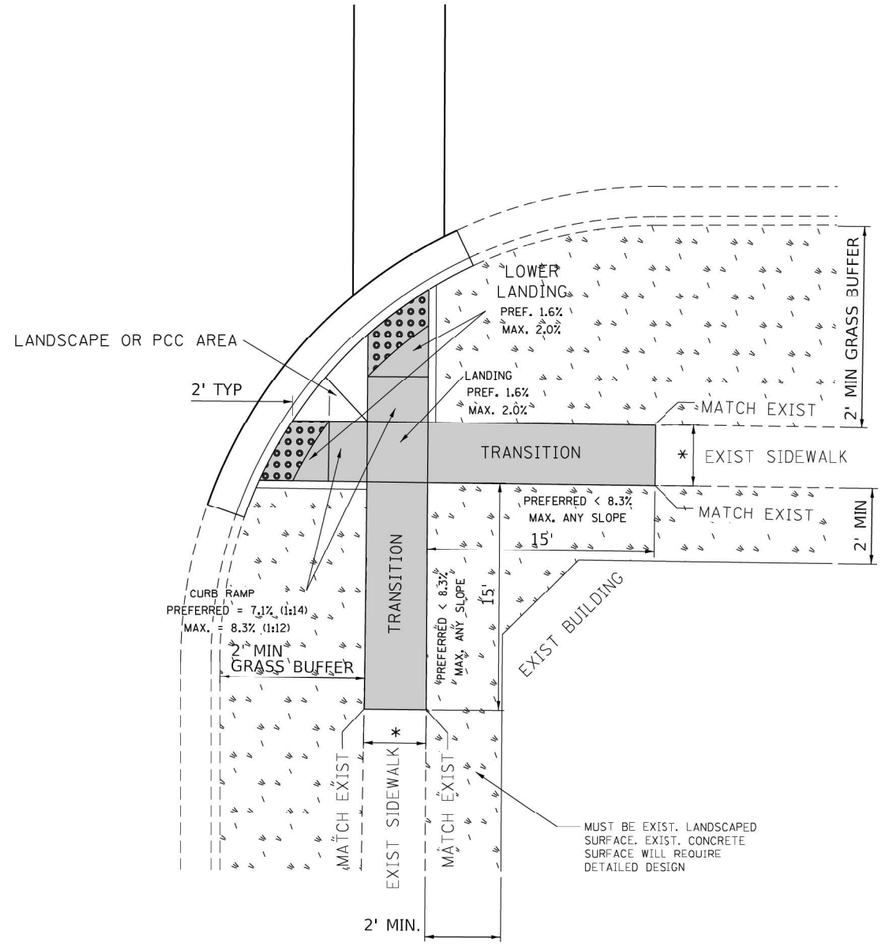
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	16
	PD-01	CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			

ADA DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS

PD-03A



PD-03B



DESIGNER NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

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Default	PLOT DATE = 12/17/2019	DATE -		

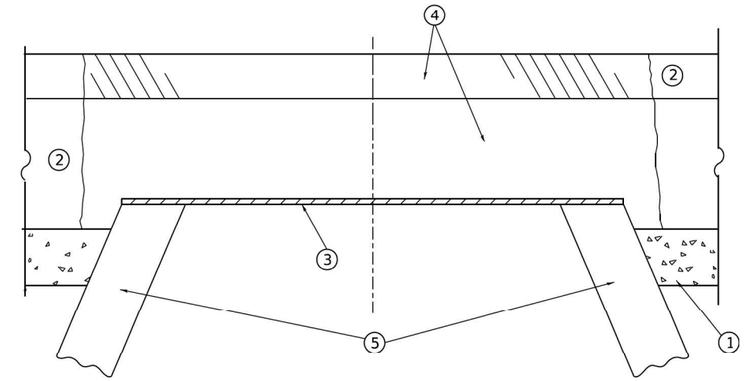
REVISED -		
REVISED -		
REVISED -		
REVISED -		

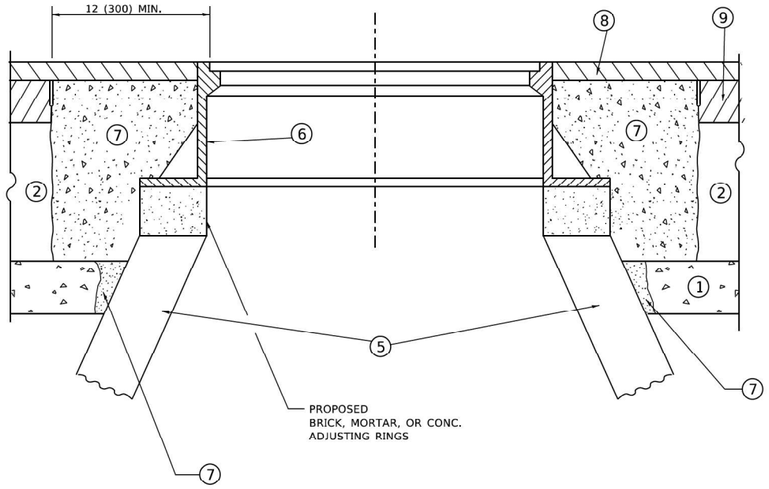
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS
(PD-03)

SCALE: | SHEET 2 OF 2 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	17
	PD-03	CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			





DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

NOTES

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.

B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.

C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.

D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 1 1/2 (40) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.

B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.

C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS*PP-1 CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS*PP-1 CONCRETE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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	DRAWN -	REVISED - R. BORO 03-09-11	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 12-06-11	
PLOT DATE = 2/2/2022	DATE - 10-25-94	REVISED - K. SMITH 02-01-22	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

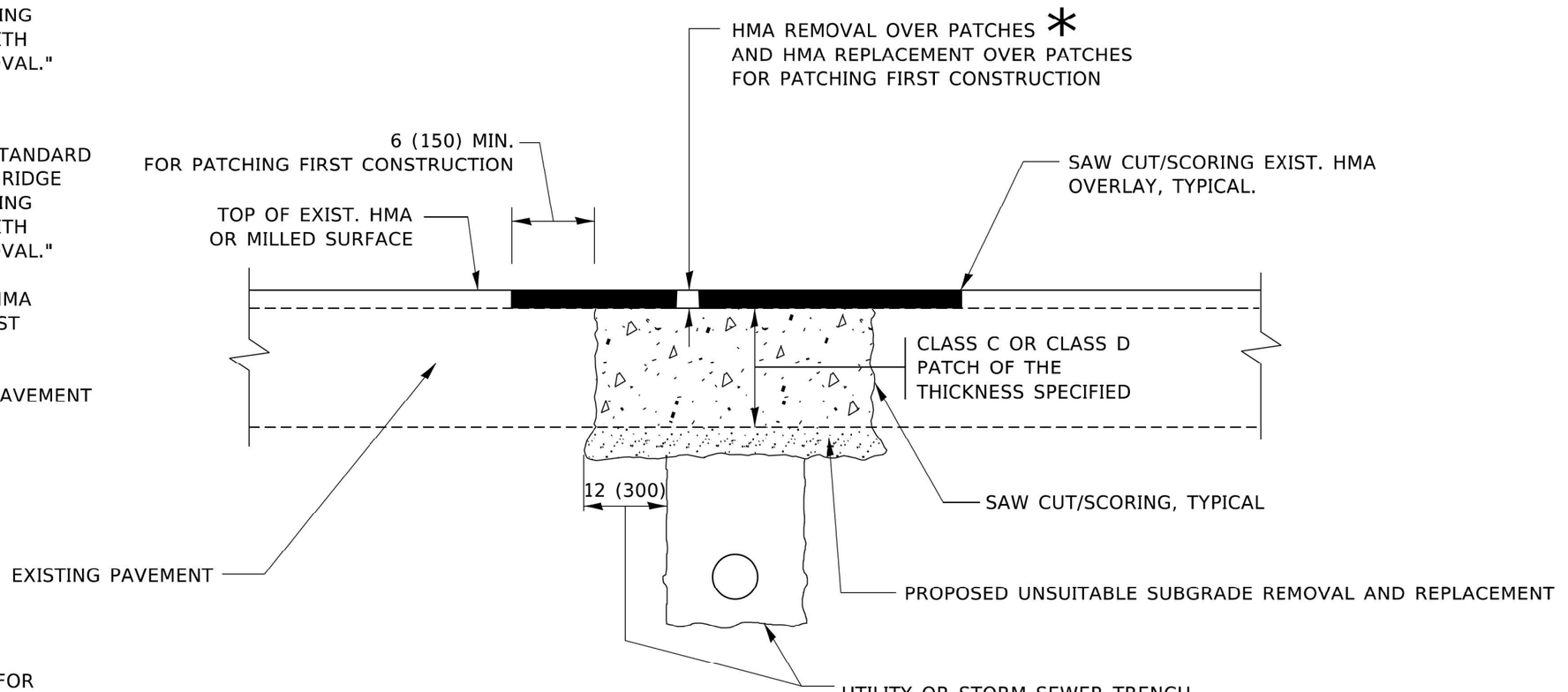
F.A. P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	18
	BD600-03 (BD-08)	CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

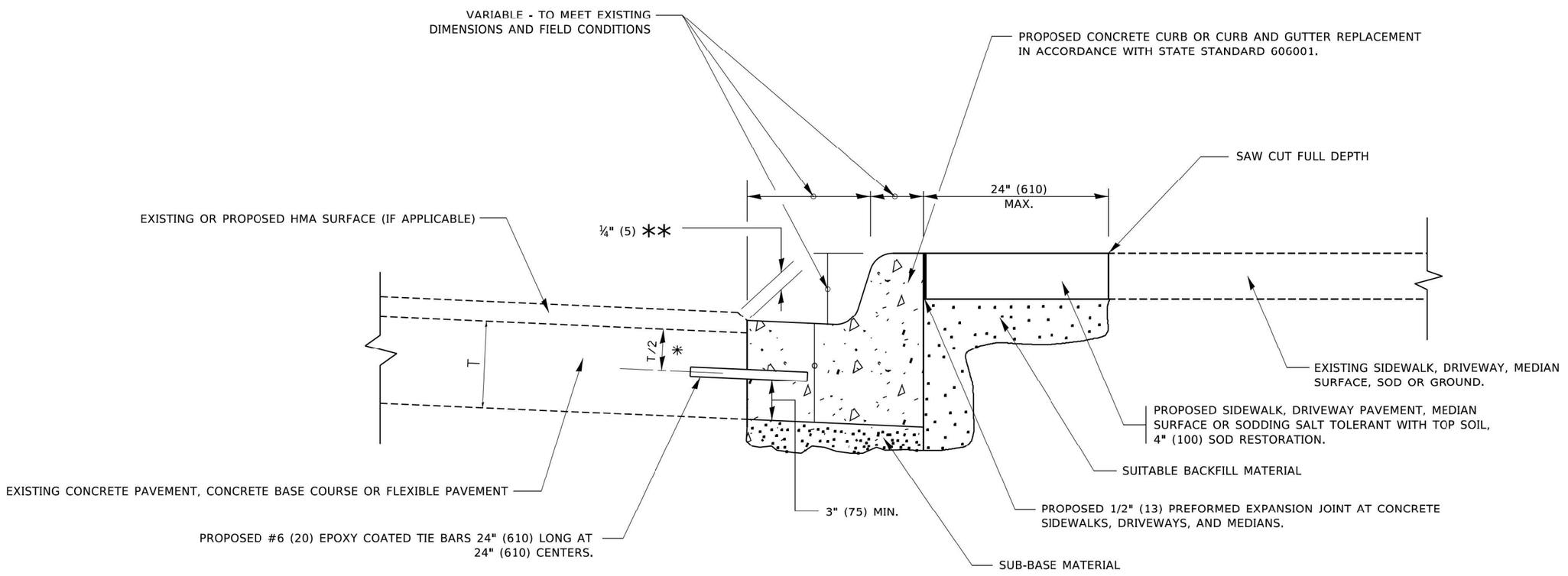
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
FILE NAME: W:\district\d22x34\bd22.dgn

USER NAME = demanchelt	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT**		F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN -	REVISED - R. BORO 09-04-07				876	2021-036-RS	WILL	36	19
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - K. ENG 10-27-08				**BD400-04 (BD-22)**		CONTRACT NO. 62N50		
PLOT DATE = 2/2/2022	DATE - 10-25-94	REVISED - K. SMITH 02-01-22		SCALE: NONE	SHEET 1 OF 1 SHEETS STA. TO STA.		ILLINOIS	FED. AID PROJECT		



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODE.: Default
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USER NAME = footemj	DESIGNED - A. HOUSEH	REVISED - A. ABBAS 03-21-97	
	DRAWN -	REVISED - M. GOMEZ 01-22-01	
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - R. BORO 12-15-09	
PLOT DATE = 7/11/2019	DATE - 03-11-94	REVISED - K. SMITH 07-11-19	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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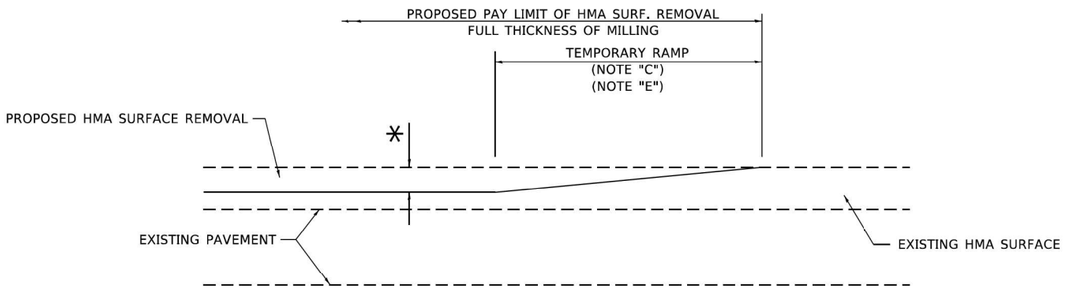
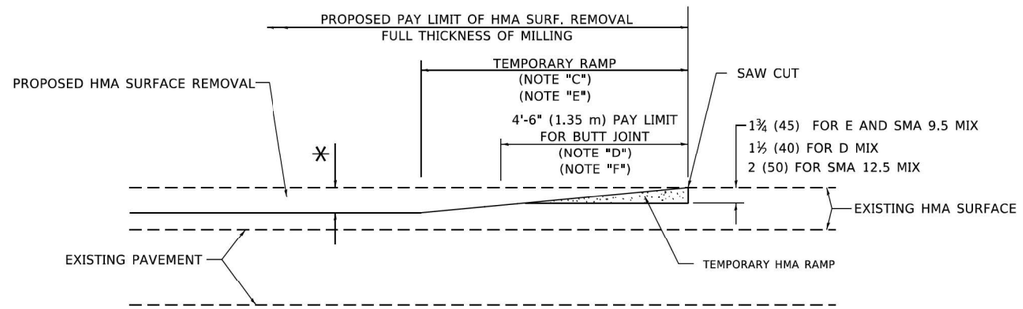
	BD600-06 (BD-24)	CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

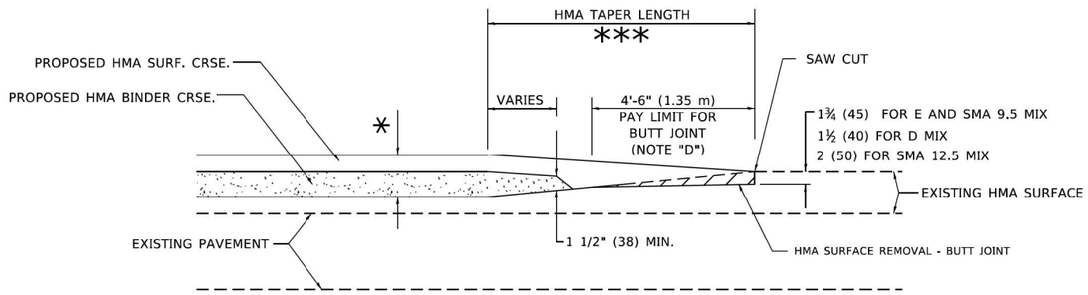


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

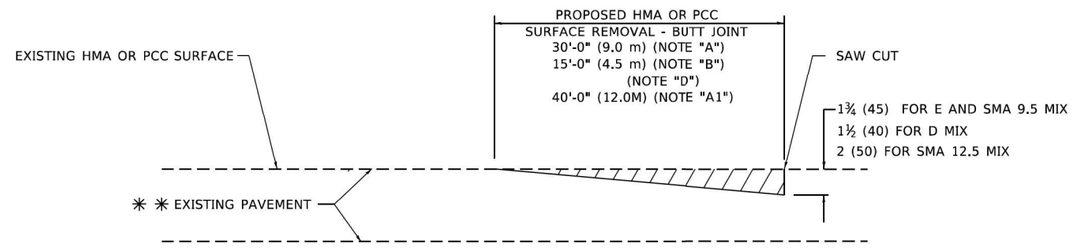
OPTION 2

TYPICAL TEMPORARY RAMP

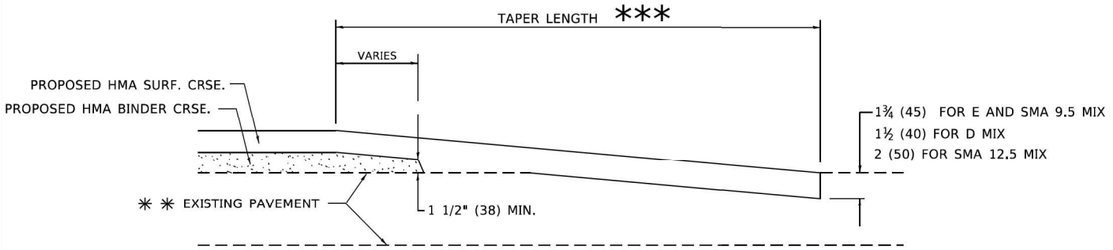


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.

A1. INTERSTATES

B. MINOR SIDE ROADS.

C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
FILE NAME: W:\dist\std\22x34\bd32.dgn

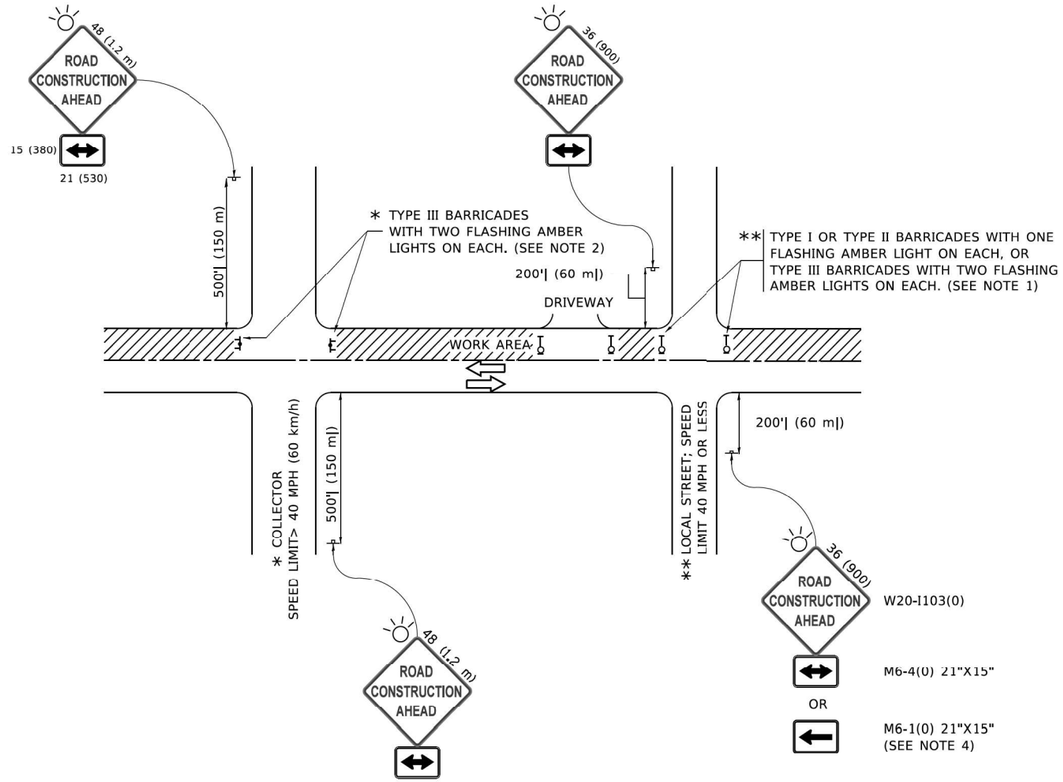
USER NAME = demanchelt	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97	
	DRAWN -	REVISED - M. GOMEZ 04-06-01	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07	
PLOT DATE = 2/2/2022	DATE - 06-13-90	REVISED - K. SMITH 02-01-22	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	21
	BD400-05 BD-32	CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 50.0000 ' / In.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	22
	TC-10	CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			

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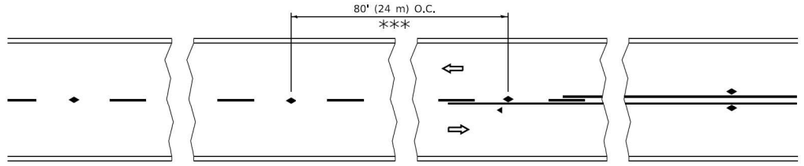
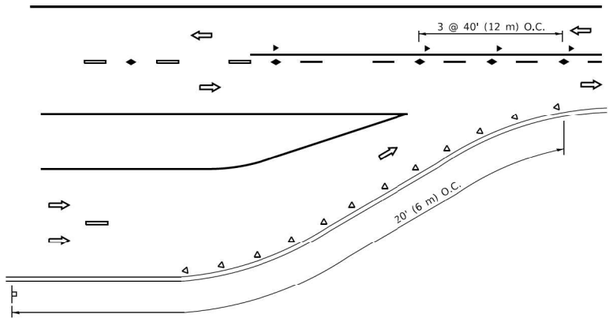
tc10.dgn 3/4/2019 10:27:07 AM User:footemj



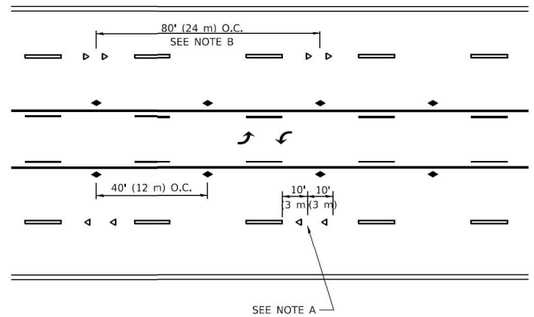
*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO–LANE/TWO–WAY

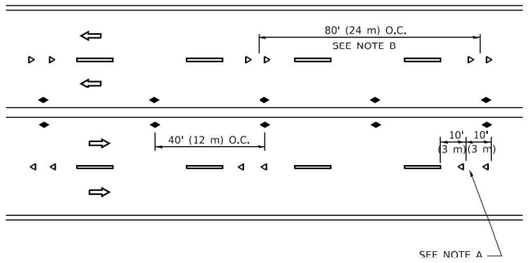
SEE FIGURE 3B-14 MUTCD



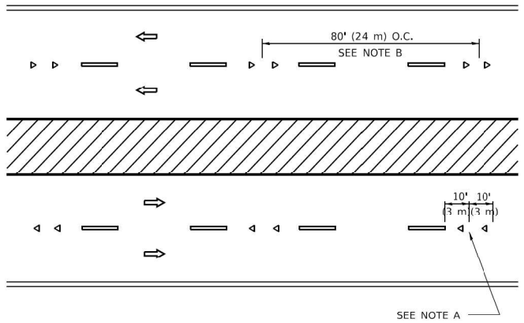
LANE REDUCTION TRANSITION



TWO–WAY LEFT TURN



MULTI–LANE/UNDIVIDED



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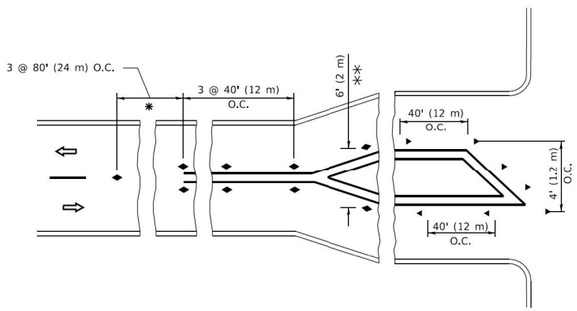

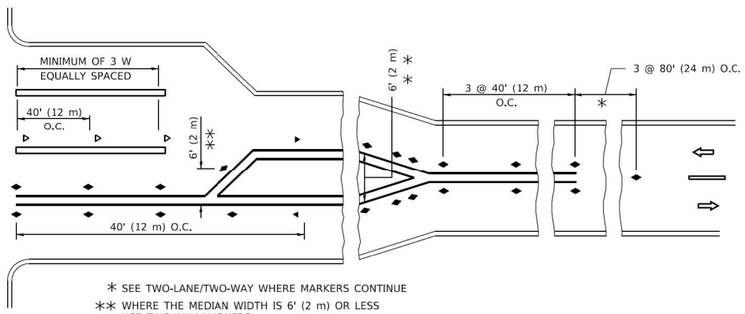

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.

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	DRAWN -	REVISED - T. RAMMACHER 01-06-00	
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09	
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 07-01-13	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW–PLOW RESISTANT)

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A. P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	23
	TC-11	CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			

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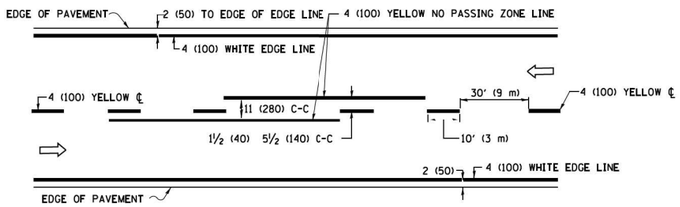
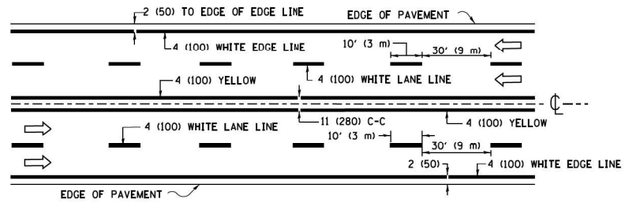
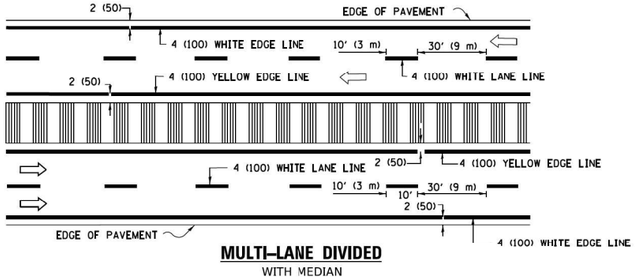


2–LANE ROADWAY

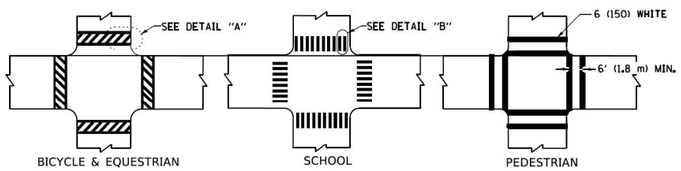


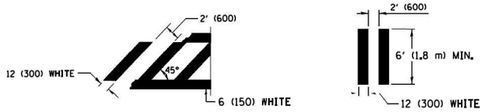
MULTI–LANE UNDIVIDED



MULTI–LANE DIVIDED
WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

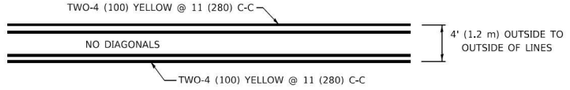




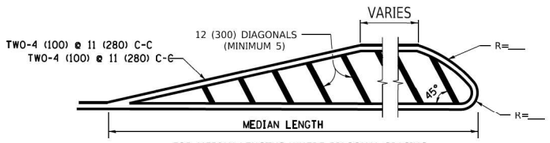
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

✱ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



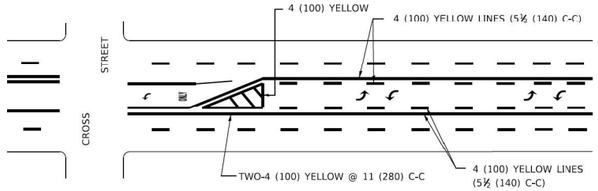
4' (1.2 m) WIDE MEDIANS ONLY



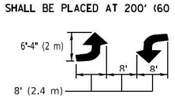
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

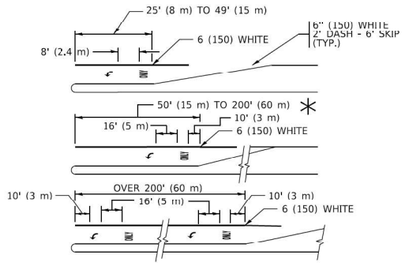


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

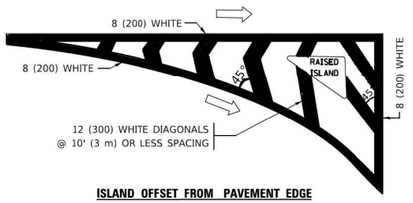


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) **ONLY** AREA = 20.8 SQ. FT. (1.9 m²)

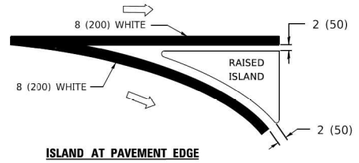
✱ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

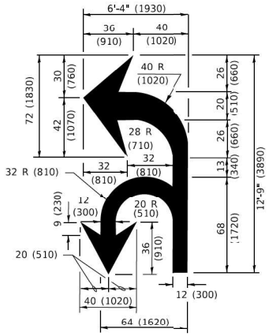


ISLAND OFFSET FROM PAVEMENT EDGE

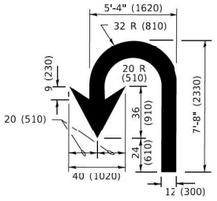


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

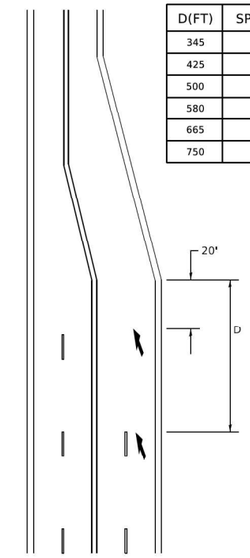


COMBINATION LEFT AND U–TURN



U–TURN

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55



LANE REDUCTION TRANSITION

✱ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m² EACH "X"=54.0 SQ. FT. (5.0 m²
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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	DRAWN -	REVISED - C. JUCIUS 07-01-13	
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15	
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A. P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	24
	TC–13	CONTRACT NO. 62N50		
		ILLINOIS FED. AID PROJECT		

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

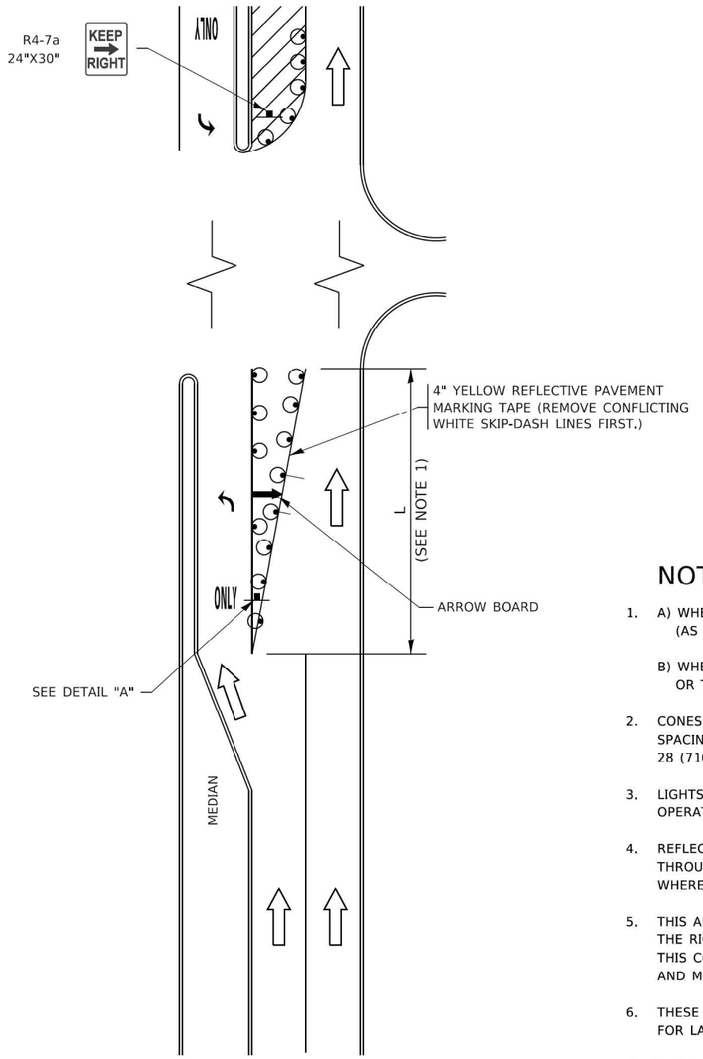


FIGURE 1

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.

   B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

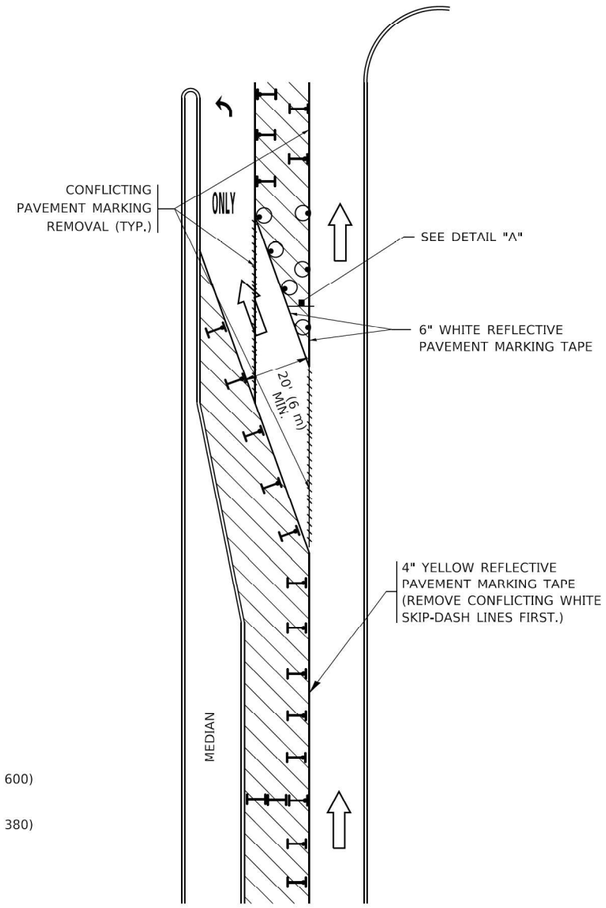
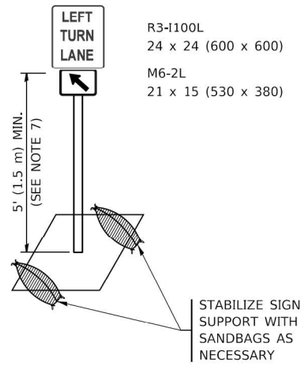


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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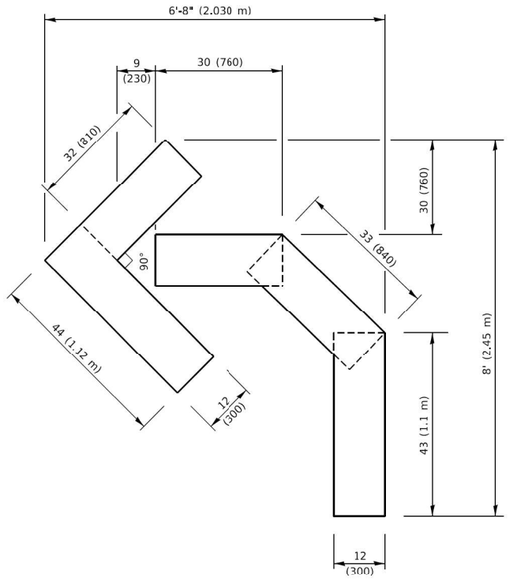
USER NAME = footemj	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	
PLOT SCALE = 50.0000 ' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16	
PLOT DATE = 3/4/2019	DATE - T. RAMMACHER 01-06-00	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)

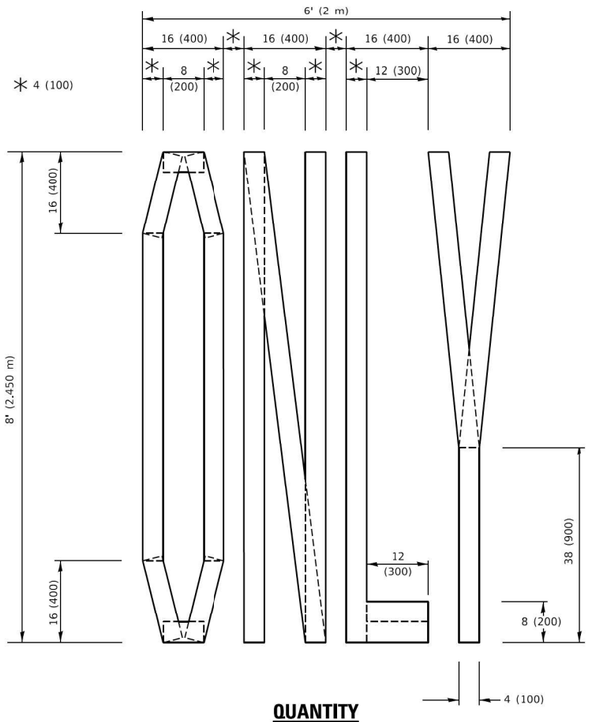
SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A. P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	25
	TC-14	CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			



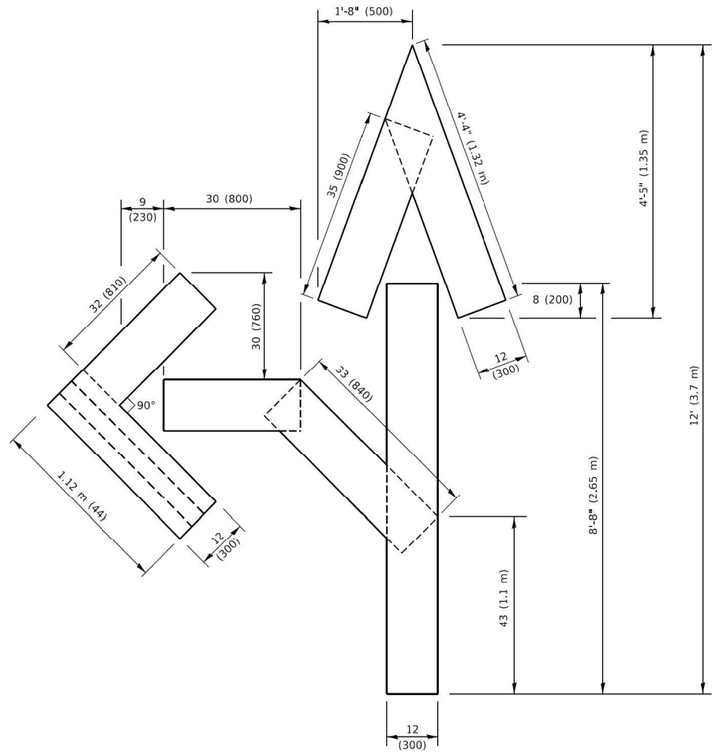
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

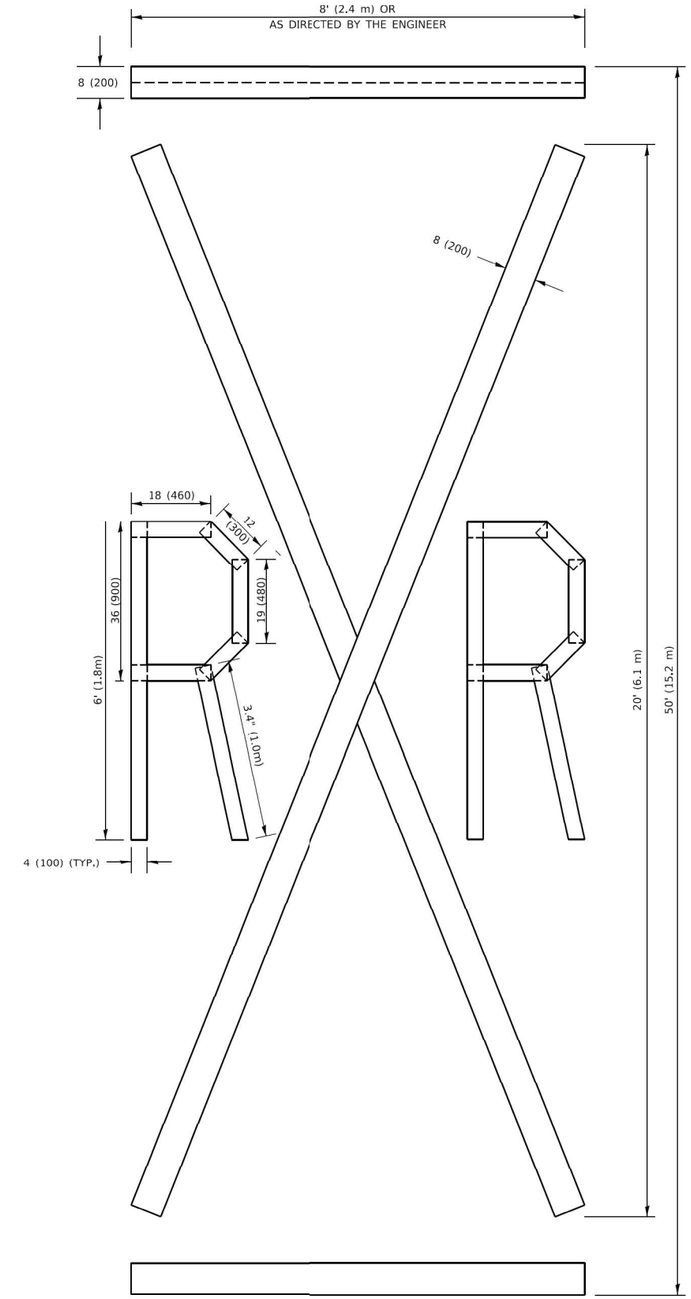


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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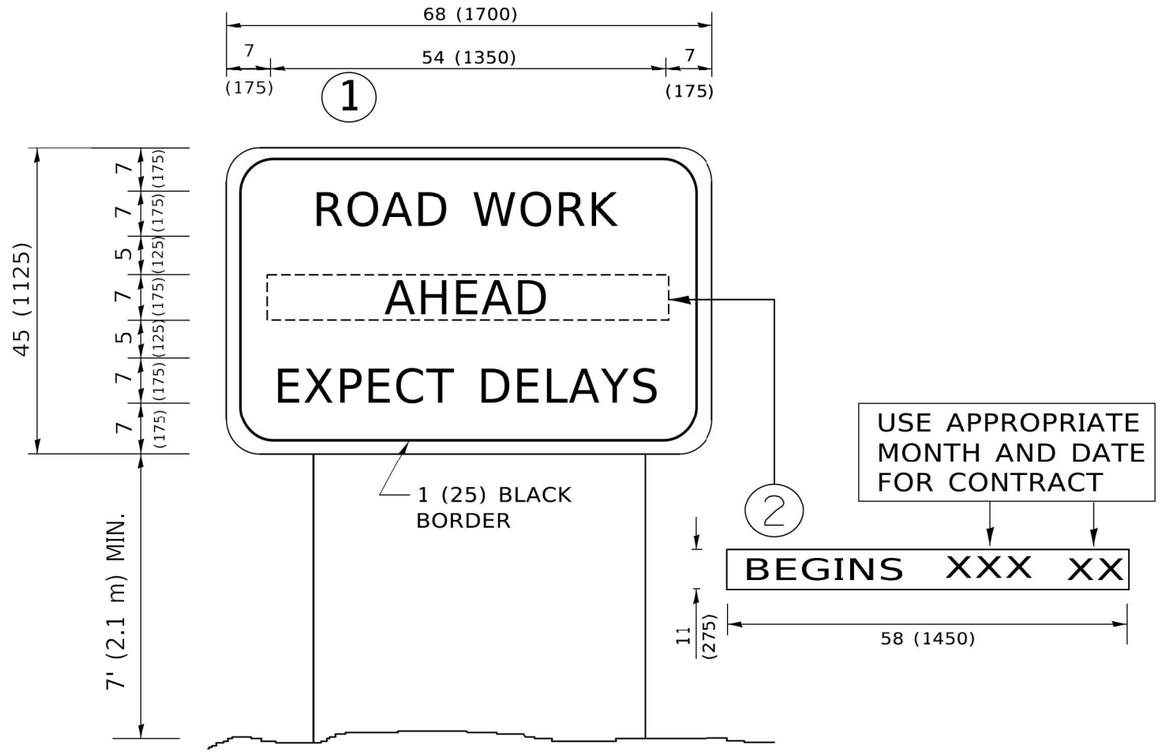
USER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 03-02-98	
	DRAWN -	REVISED - E. GOMEZ 08-28-00	
PLOT SCALE = 50.0068 ' / In.	CHECKED -	REVISED - E. GOMEZ 08-28-00	
PLOT DATE = 3/4/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	26
	TC-16	CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

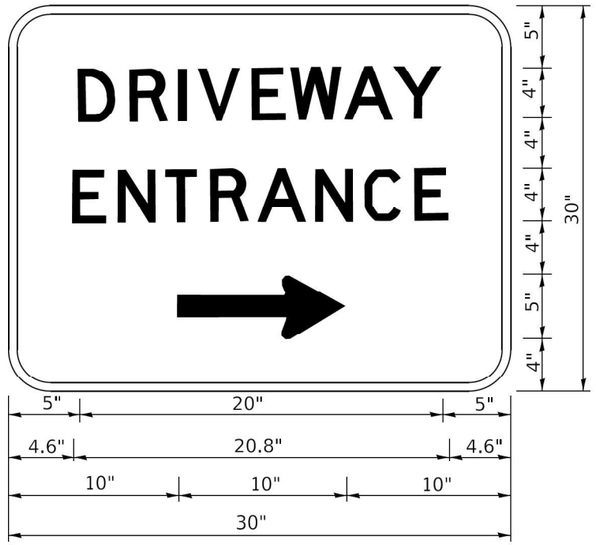
USER NAME = footemj	DESIGNED -	REVISED -	R. MIRS 09-15-97
	DRAWN -	REVISED -	R. MIRS 12-11-97
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -	T. RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED -	C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A. P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	27
	TC-22	CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

USER NAME = leysa	DESIGNED -	REVISED - C. JUCIUS 02-15-07	
	DRAWN -	REVISED -	
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 8/6/2021	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A. P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	28
	TC-26	CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			

TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

ITEM	EXISTING	PROPOSED
CONTROLLER CABINET		
COMMUNICATION CABINET	CC	CC
MASTER CONTROLLER	MC	MC
MASTER MASTER CONTROLLER	MMC	MMC
UNINTERRUPTABLE POWER SUPPLY		
SERVICE INSTALLATION -(P) POLE MOUNTED	P	P
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED	G GM	G GM
TELEPHONE CONNECTION	ET	T
STEEL MAST ARM ASSEMBLY AND POLE		
ALUMINUM MAST ARM ASSEMBLY AND POLE		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY		BM
WOOD POLE		
GUY WIRE		
SIGNAL HEAD		
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD OPTICALLY PROGRAMMED	P P	P P
FLASHER INSTALLATION -(FS) SOLAR POWERED	F FS F FS	F FS F FS
PEDESTRIAN SIGNAL HEAD		
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON	APS	APS
RADAR DETECTION SENSOR	R	R
VIDEO DETECTION CAMERA	V	V
RADAR/VIDEO DETECTION ZONE		
PAN, TILT, ZOOM (PTZ) CAMERA	PTZ	PTZ
EMERGENCY VEHICLE LIGHT DETECTOR		
CONFIMATION BEACON		
WIRELESS INTERCONNECT		
WIRELESS INTERCONNECT RADIO REPEATER	RR	RR

ITEM	EXISTING	PROPOSED
HANDHOLE -SQUARE -ROUND		
HEAVY DUTY HANDHOLE -SQUARE -ROUND	H	H
DOUBLE HANDHOLE		
JUNCTION BOX	J	J
RAILROAD CANTILEVER MAST ARM		
RAILROAD FLASHING SIGNAL		
RAILROAD CROSSING GATE		
RAILROAD CROSSBUCK		
RAILROAD CONTROLLER CABINET	RC	RC
UNDERGROUND CONDUIT (UC), GALVANIZED STEEL		
TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE		
SYSTEM ITEM	S	SP
INTERSECTION ITEM	I	IP
REMOVE ITEM		R
RELOCATE ITEM		RL
ABANDON ITEM		A
CONTROLLER CABINET AND FOUNDATION TO BE REMOVED		RCF
MAST ARM POLE AND FOUNDATION TO BE REMOVED		RMF
SIGNAL POST AND FOUNDATION TO BE REMOVED		RPF
DETECTOR LOOP, TYPE I		
PREFORMED DETECTOR LOOP	P P	P P
SAMPLING (SYSTEM) DETECTOR	S S	S S
INTERSECTION AND SAMPLING (SYSTEM) DETECTOR	IS IS	IS IS
QUEUE AND SAMPLING (SYSTEM) DETECTOR	QS QS	QS QS
WIRELESS DETECTOR SENSOR	W	W
WIRELESS ACCESS POINT		

ITEM	EXISTING	PROPOSED
SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD	P	P
SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE	P RB	P RB
PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER	C D	C D
ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"		
NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED	5	5
GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)	1#6	1#6
ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C	1	1
COAXIAL CABLE	C	C
VENDOR CABLE	V	V
COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED	6#18	6#18
FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F	12F 24F 36F	12F 24F 36F
GROUND ROD -(C) CONTROLLER -(M) MAST ARM -(P) POST -(S) SERVICE	C M P S	C M P S

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	DRAWN - IP	REVISED -	
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PLOT DATE = 3/4/2019	DATE - 9/29/2016	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 1 OF 7 SHEETS | STA. | TO STA.

F.A. P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	29
	TS-05	CONTRACT NO. 62N50		
		ILLINOIS	FED. AID PROJECT	

ts05.dgn 3/4/2019 11:21:59 AM User:footemj

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

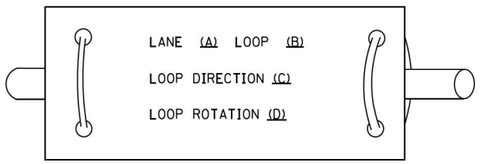
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

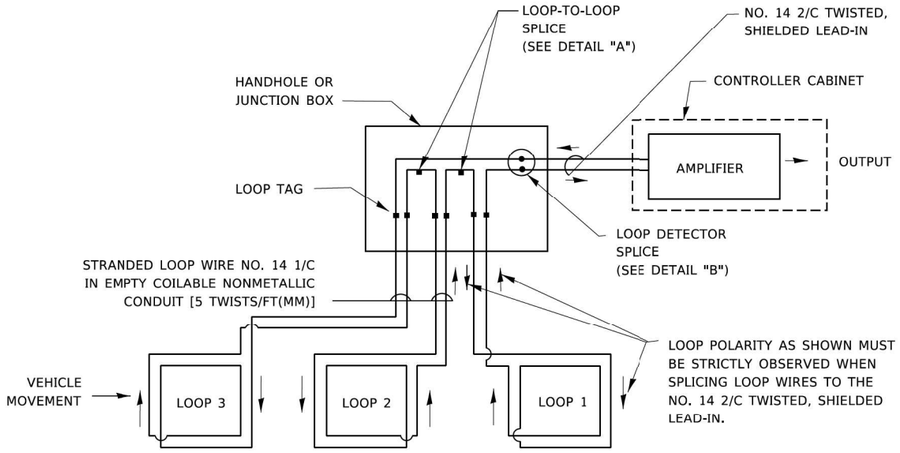
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD–IN CABLE TAG

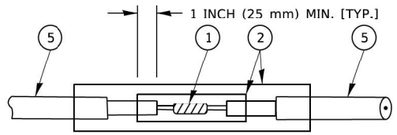


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

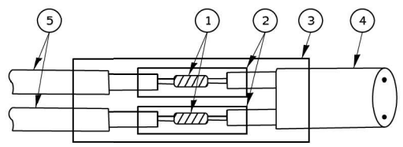


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

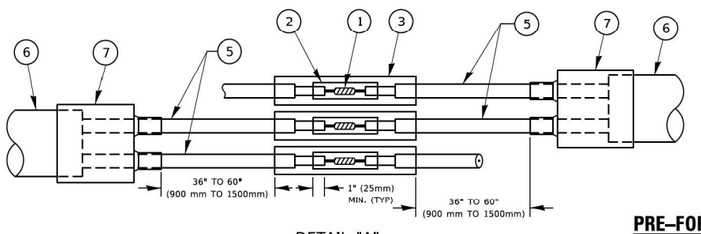


DETAIL "A"
LOOP-TO-LOOP SPLICE



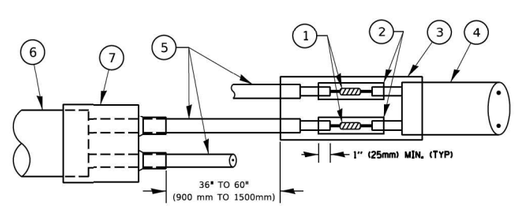
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

PRE–FORMED LOOP



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PRE-FORMED LOOP

(6) XL POLYOLEFIN 2 CONDUCTOR

(7) BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 2 OF 7 SHEETS | STA. | TO STA.

F.A. P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	TS–05	CONTRACT NO. 62N50		
	ILLINOIS	FED. AID PROJECT		

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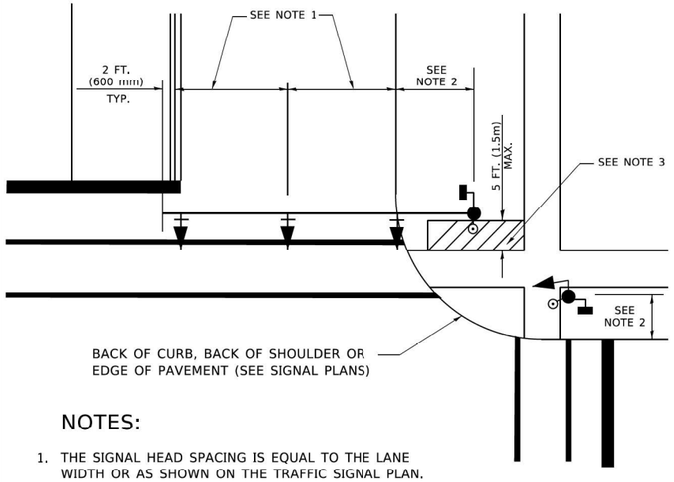
TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

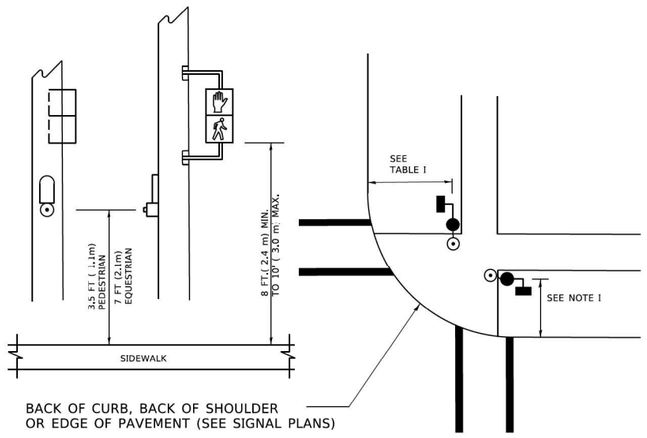
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

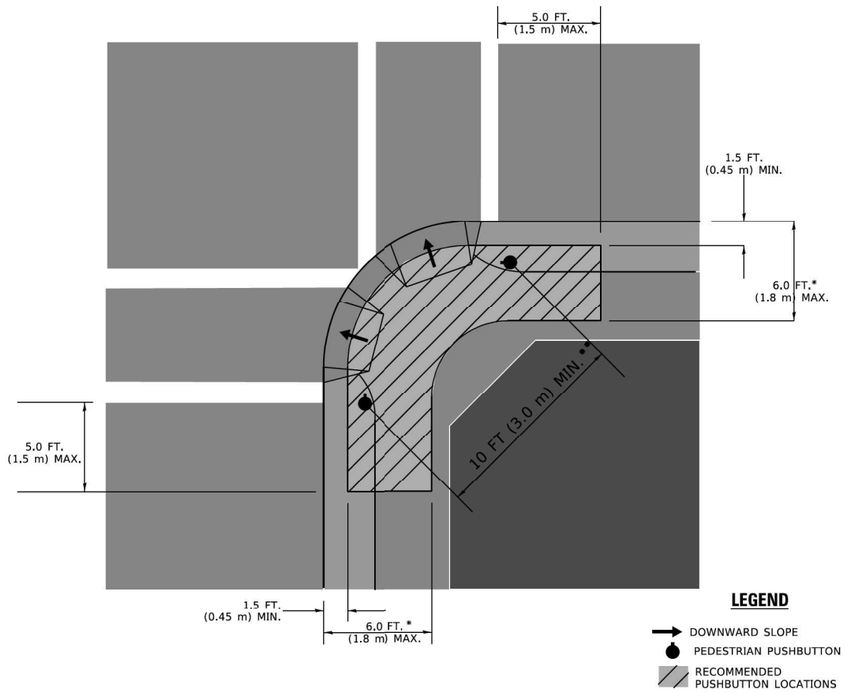
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



* WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.

** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.5m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.5m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.5m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.5m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.5m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

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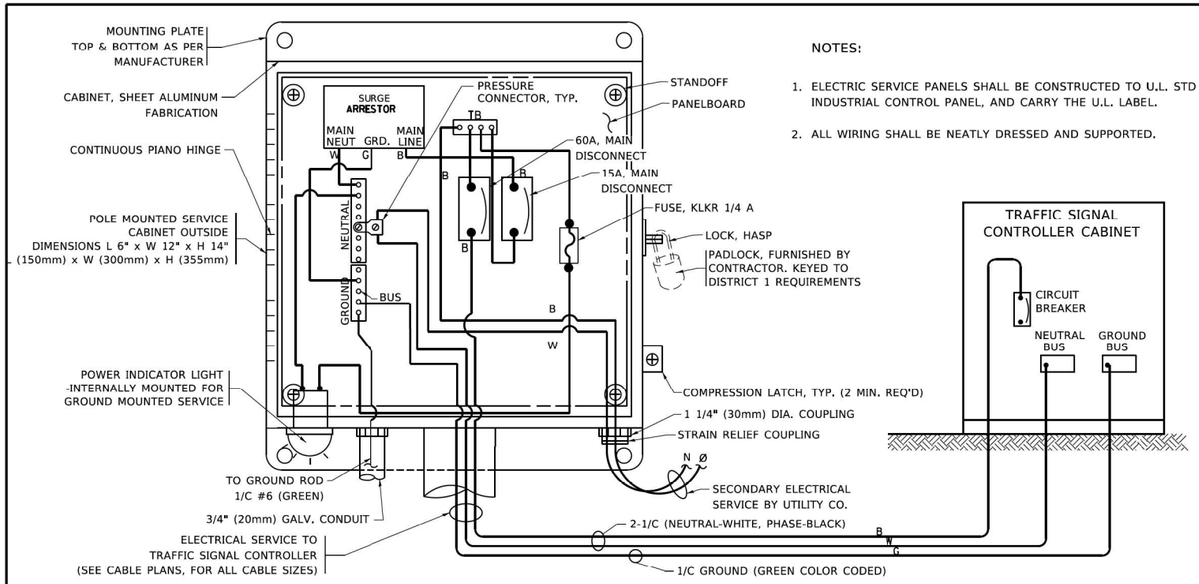


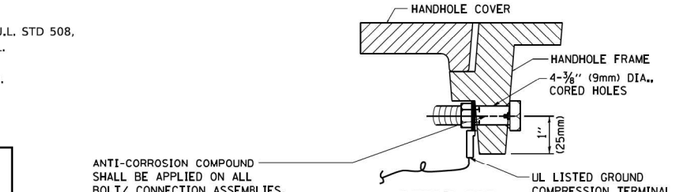
NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

SERVICE INSTALLATION POLE MOUNT (SHOWN)

(NOT TO SCALE)



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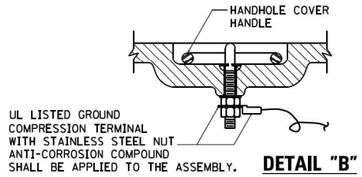
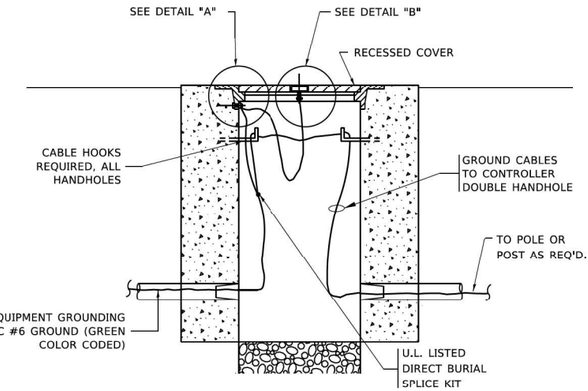


DETAIL "B"

NOTES:

GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC,). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINCIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



HANDHOLE COVER & FRAME – GROUNDING DETAIL

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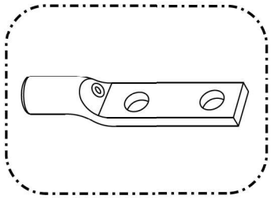
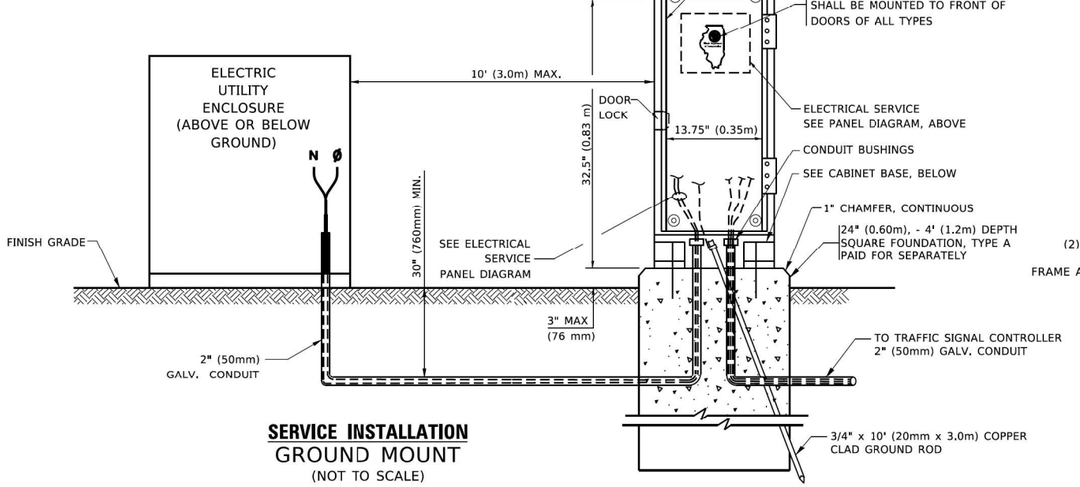


HEAVY-DUTY COMPRESSION TERMINAL (BURNDY TYPE YGHA OR APPROVED EQUAL)

¾" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EQUAL)

NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
  6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
  13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
  5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



SERVICE INSTALLATION

GROUND MOUNT

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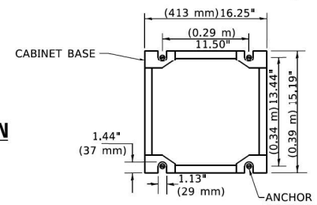

CABINET – BASE BOLT PATTERN

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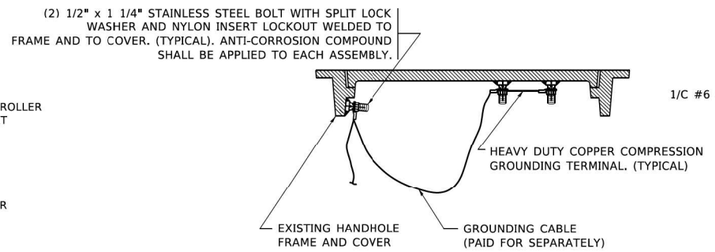
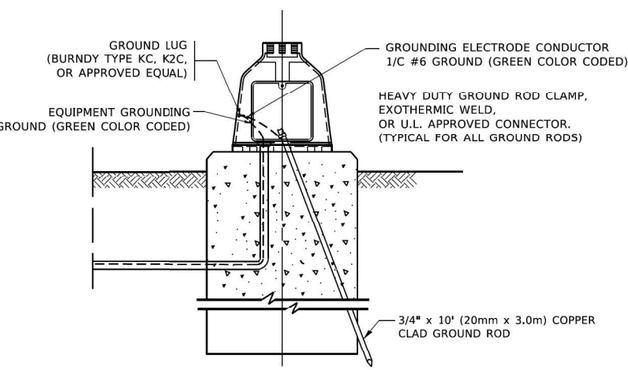


EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)



MAST ARM POLE /POST–GROUNDING DETAIL

(NOT TO SCALE)

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

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STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 4 OF 7 SHEETS | STA. | TO STA.

F.A. P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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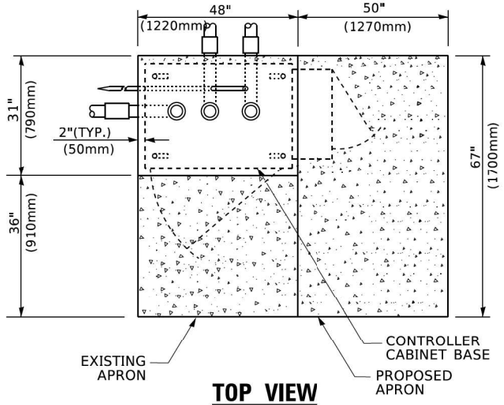
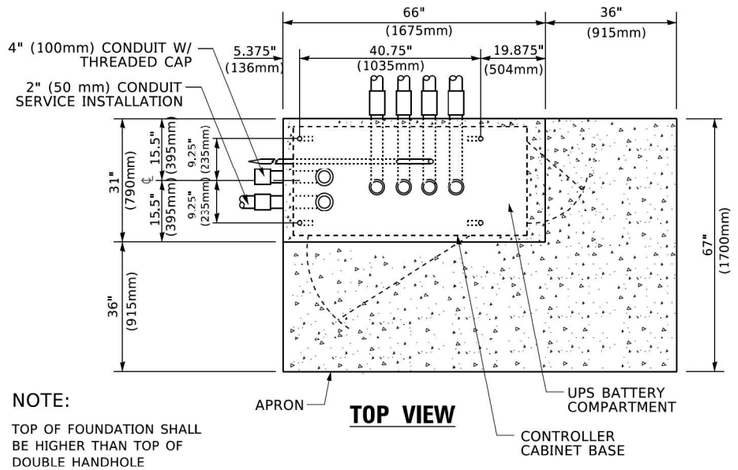


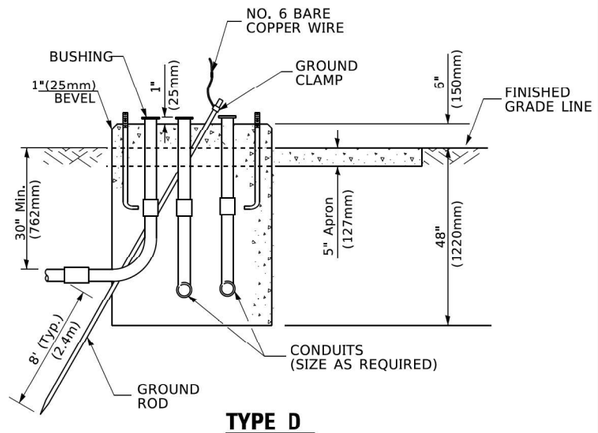
TOP VIEW



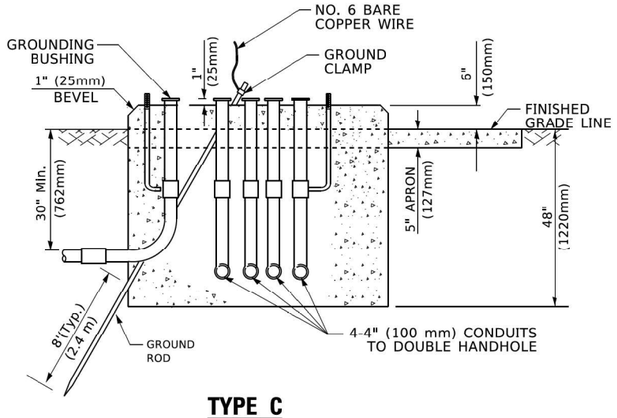
NOTE:

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE

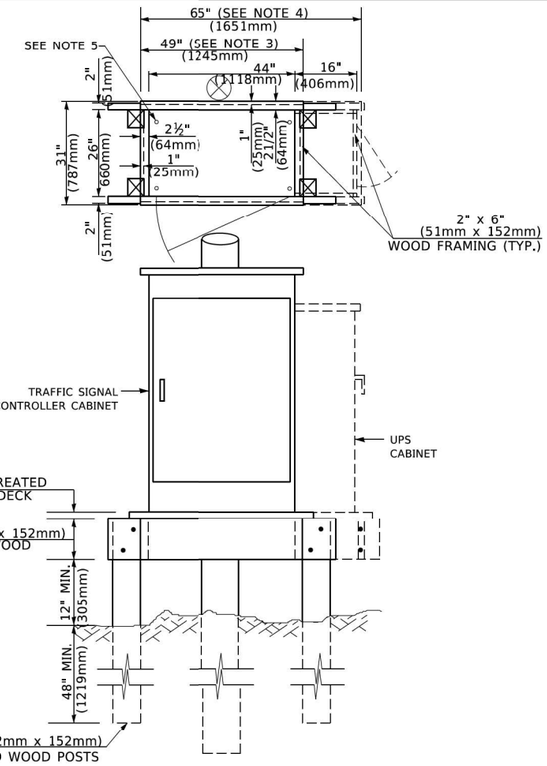
TOP VIEW



TYPE D
FOR GROUND MOUNTED CONTROLLER CABINET AND UPS BATTERY CABINET



TYPE C
FOR GROUND MOUNTED SUPER P (TYPE IV) AND SUPER R (TYPE V) CONTROLLER CABINETS



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

TEMPORARY SIGNAL CONTROLLER WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
4. For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 5 OF 7 SHEETS | STA. | TO STA.

F.A. P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	33
	TS-05	CONTRACT NO. 62N50		
		ILLINOIS	FED. AID PROJECT	

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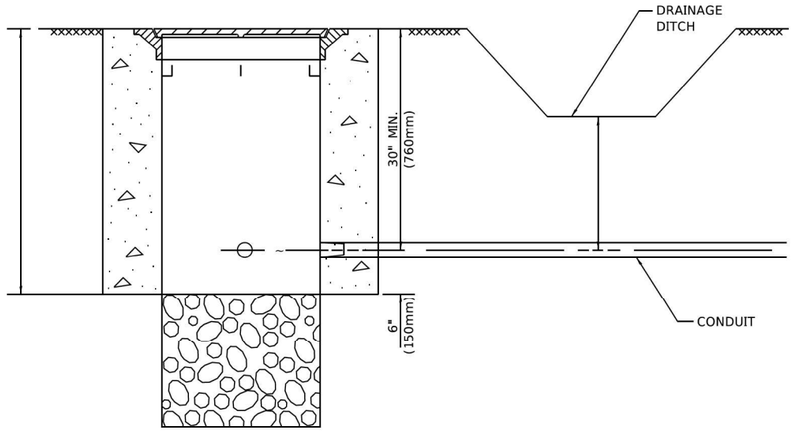
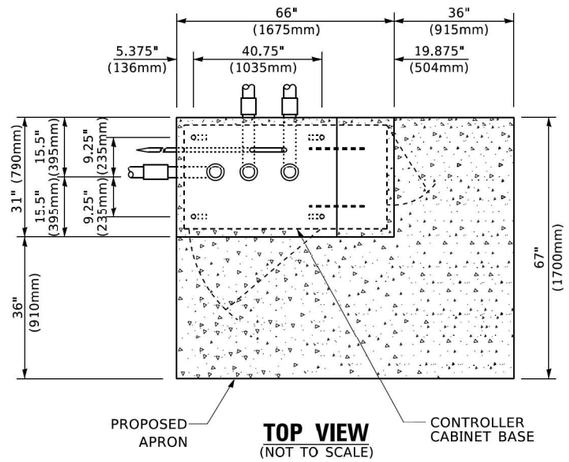


NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

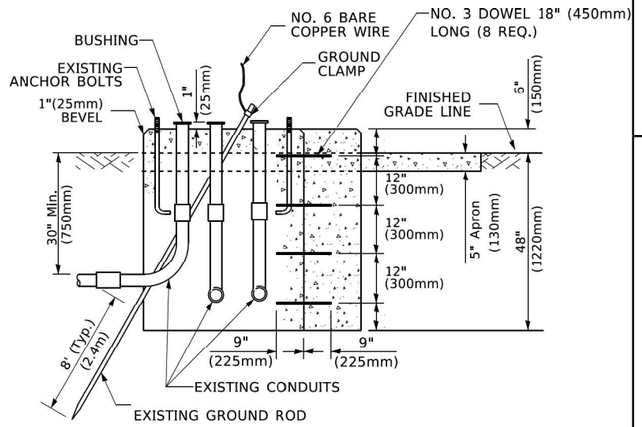
HANDHOLE WITH MINIMUM CONDUIT DEPTH

(NOT TO SCALE)



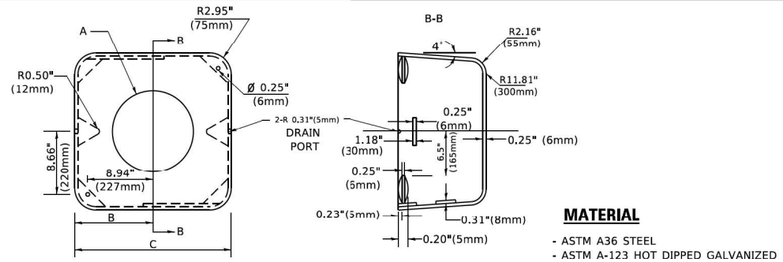
TOP VIEW

(NOT TO SCALE)



MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION

(NOT TO SCALE)



MATERIAL

- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

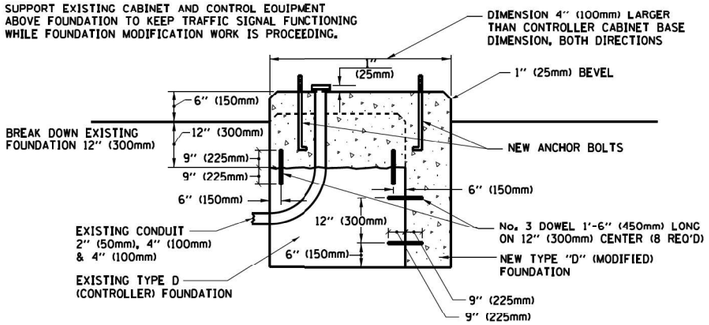
SHROUD

NOTES:

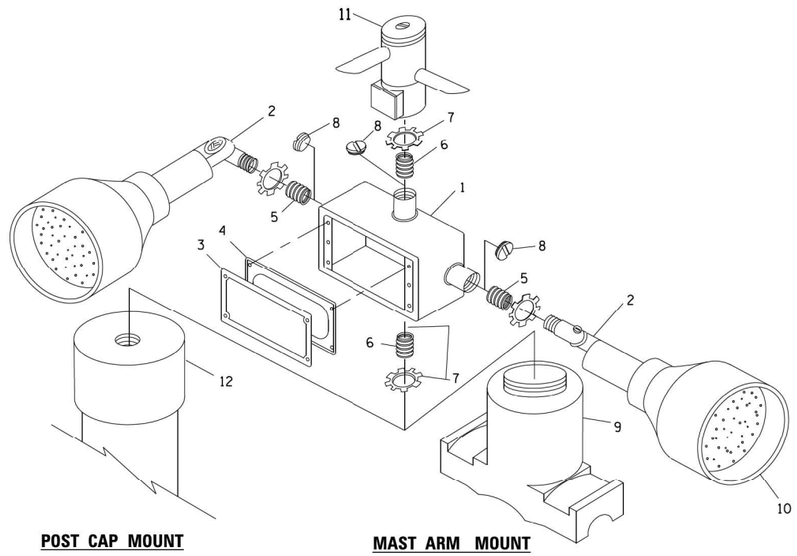
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



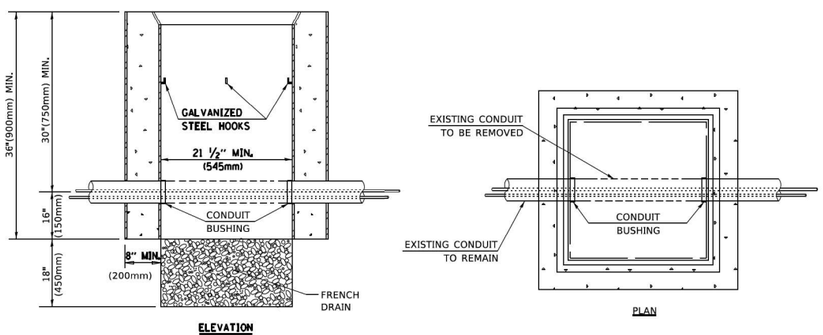
POST CAP MOUNT **MAST ARM MOUNT**

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	¾"(19 mm) CLOSE NIPPLE
7	¾"(19 mm) LOCKNUT
8	¾"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
   ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
   ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 "(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



ELEVATION PLAN

NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 6 OF 7 SHEETS | STA. | TO STA.

F.A. P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	34
	TS-05	CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			

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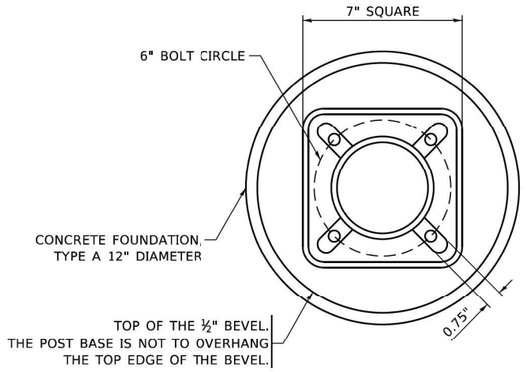
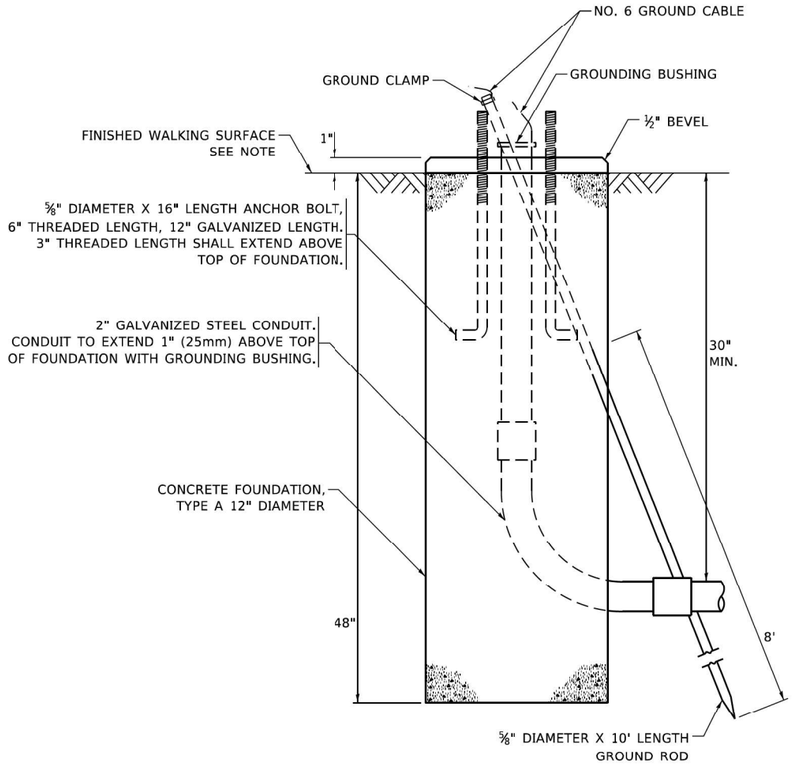


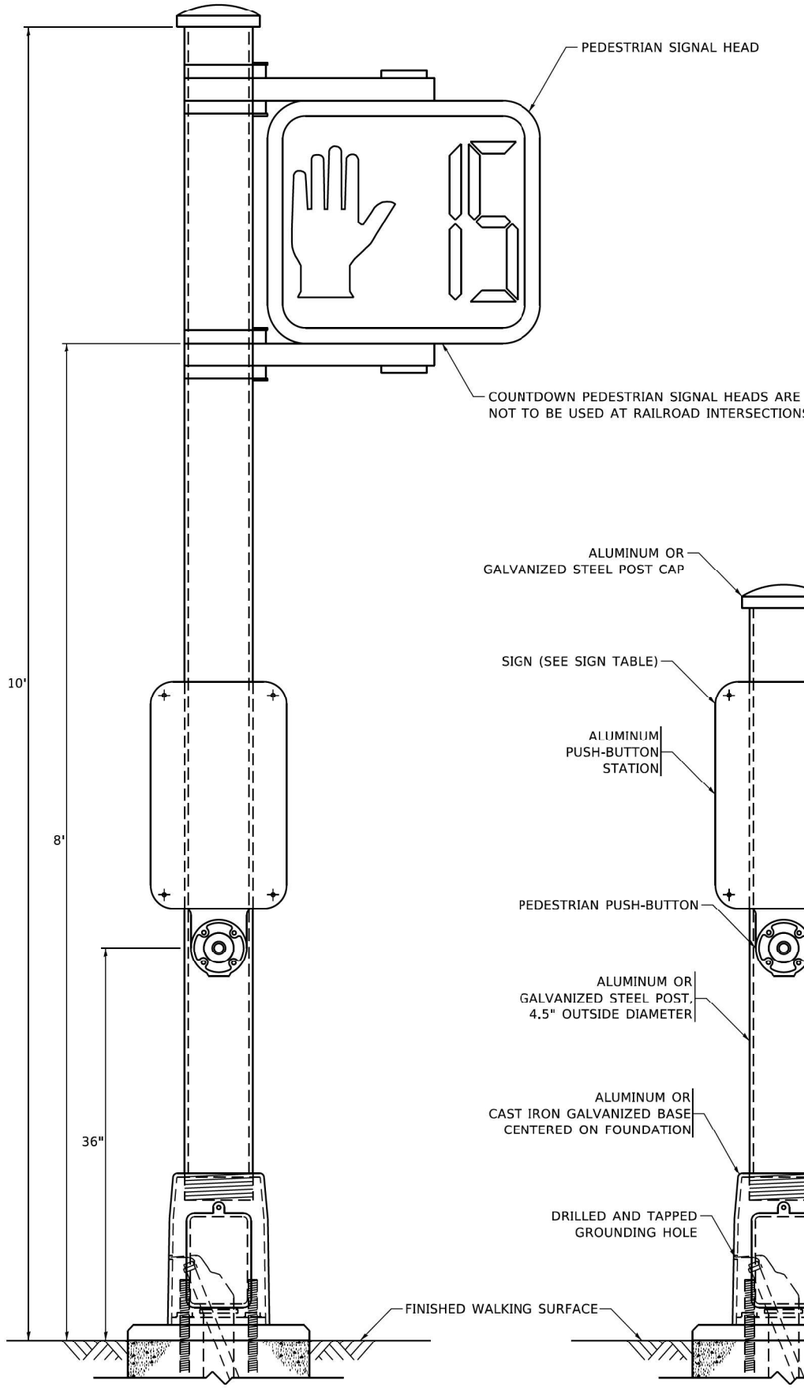
BOLT PATTERN

NOTE:

1. IF THE PEDESTRIAN SIGNAL POST FOUNDATION IS INSTALLED WITHIN OR BEHIND A BARRIER CURB, THE TOP OF THE FOUNDATION SHALL BE INSTALLED FLUSH WITH THE TOP OF THE BARRIER CURB.



CONCRETE FOUNDATION, TYPE A 12–INCH DIAMETER

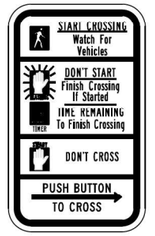


PEDESTRIAN SIGNAL POST, 10 FT.

PEDESTRIAN SIGNAL POST, 5 FT.

R10–3b

R10–3d



R10–3e

SIGN TABLE

SIGN	DIMENSIONS
R10-3b (RAILROAD ONLY)	9" X 12"
R10-3d (RAILROAD ONLY)	9" X 12"
R10-3e	9" X 12"

NOTES:

1. THE SIGN PANELS SHALL BE TYPE AP SHEETING.
2. THE ARROW ON SIGNS FOR PUSH-BUTTONS SERVING TWO DIRECTIONS ON THE SAME PHASE SHALL BE BI-DIRECTIONAL.
3. THE SIGN FOR DUAL-CALL PUSH-BUTTONS SHALL HAVE NO ARROW.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

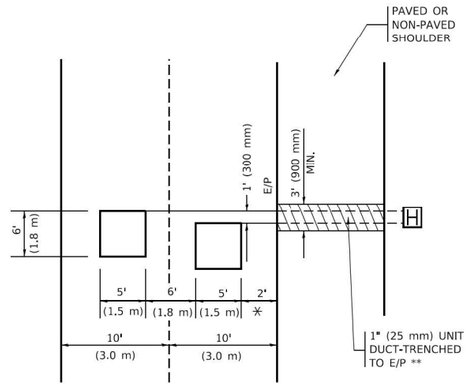
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 7 OF 7 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2021-036-RS	WILL	36	35
	TS–05	CONTRACT NO. 62N50		
	ILLINOIS FED. AID PROJECT			

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



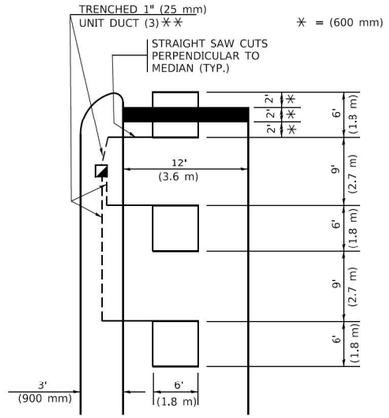
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



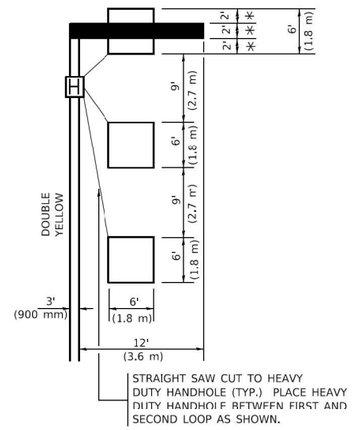
** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS

VOLUME DENSITY (''FAR OUT'' DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)

* = (600 mm)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

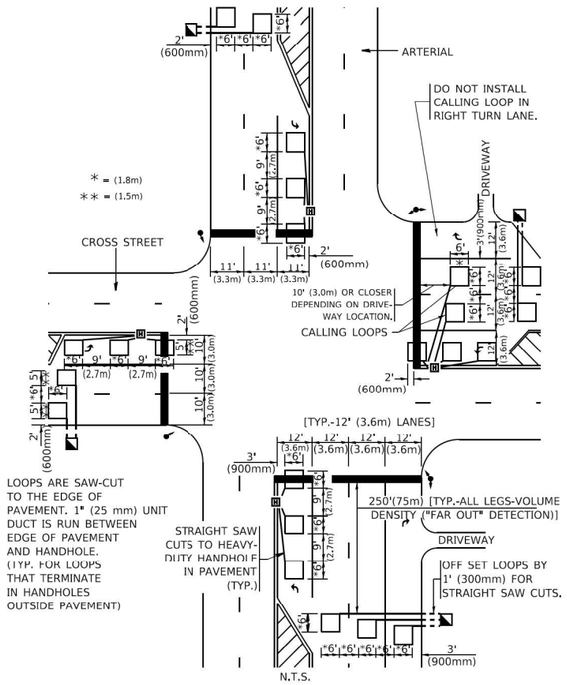
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

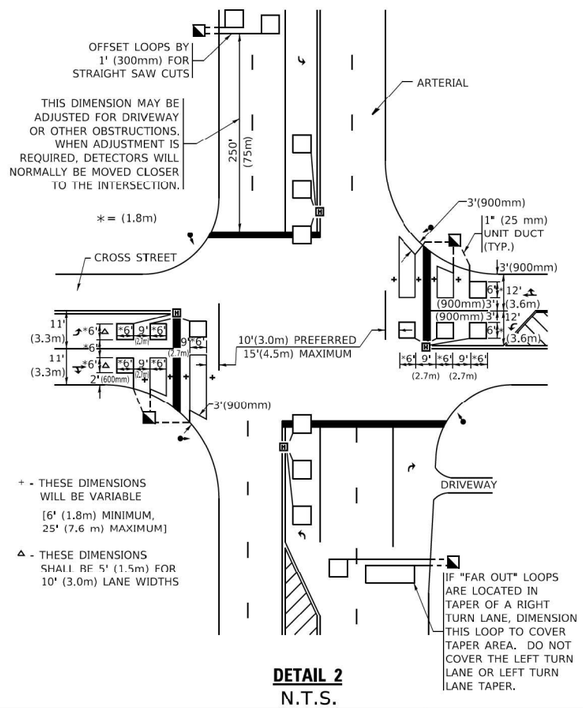
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A. P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	TS-07	CONTRACT NO. 62N50		
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