

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	D-RS-7	WILL	38	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO.	60N60	

* 38 + 2 = 40

D-91-330-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED HIGHWAY PLANS

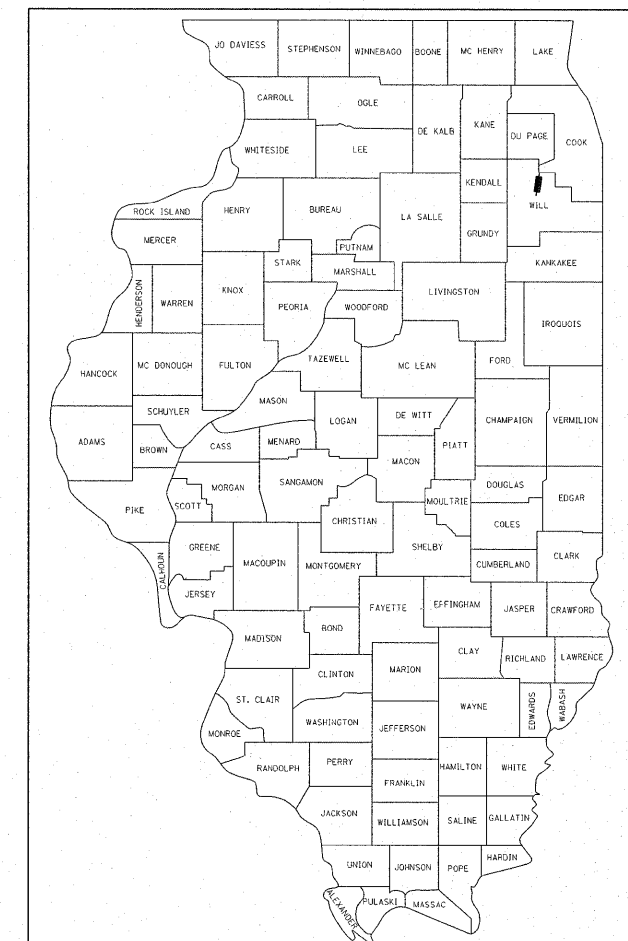
FOR INDEX OF SHEETS, SEE SHEET NO. 2

FAP ROUTE 577: ILL 171
CLAY ST TO S. OF BOULA AVE.
SECTION: D-RS-7
RESURFACING
WILL COUNTY
C-91-330-11

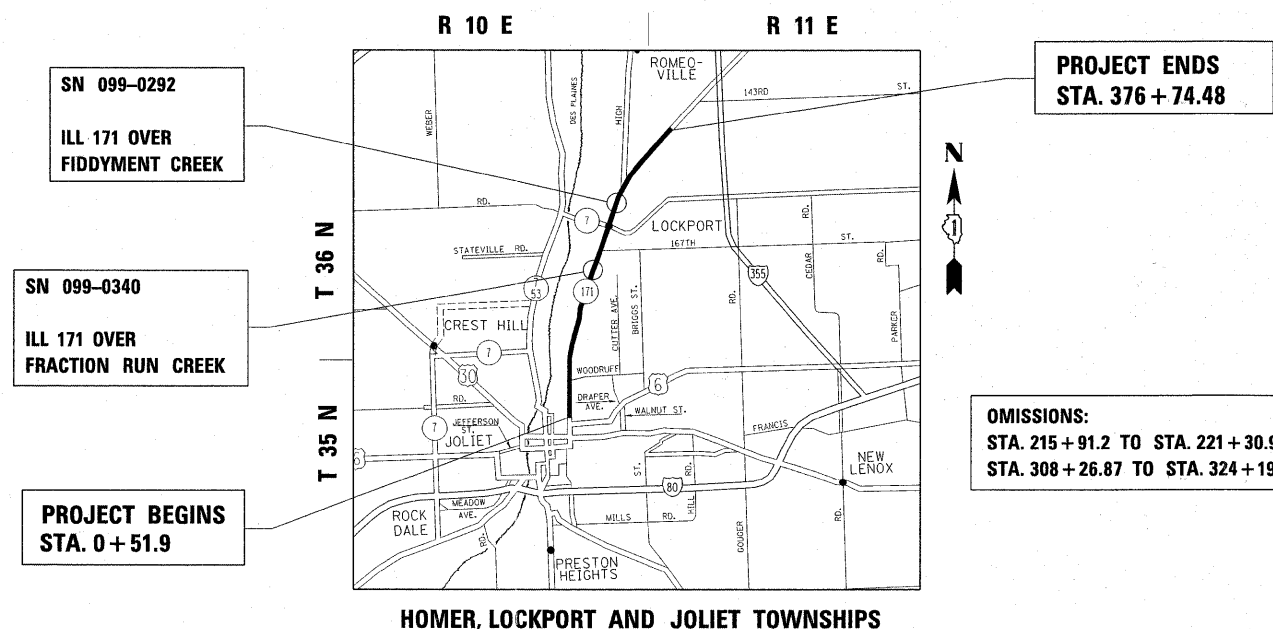
THE PROJECT IS LOCATED IN THE CITIES
OF LOCKPORT AND JOLIET

TRAFFIC DATA

IL 171: 2009 ADT = 14,300
POSTED SPEED LIMIT = 35 - 45 MPH



LOCATION OF SECTION INDICATED THUS: - [rectangle] -



J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER ROBERT BORO (847)705-4178
PROJECT MANAGER ISSAM RAYYAN

CONTRACT NO. 60N60

GROSS LENGTH OF IMPROVEMENT = 37,616.58 FEET = 7.12 MILES
NET LENGTH OF IMPROVEMENT = 35,158.2 FEET = 6.65 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED MARCH 28 20 11

Diane O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 13 20 11
Scott E. Stett P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

May 13 20 11
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES.
3-4	SUMMARY OF QUANTITIES
5-7	TYPICAL SECTIONS
8-20	ROADWAY AND PAVEMENT MARKING PLANS
21-26	DETECTOR LOOP REPLACEMENT PLANS
27-28	BRIDGE PLAN DETAILS
29	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8)
30	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
31	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
32	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
32A	DETAILS FOR DEPRESSED CURB & GUTTER AND SHOULDER TREATMENT AT TBT TY 1 SPL. (BD-34)
33	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)
34	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (TC-11)
35	DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13)
35A.	TRAFFIC CONTROL DETAIL (TC-14)
36	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)
37	ARTERIAL ROAD INFORMATION SIGN (TC-22)
38	DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING (TS-07)

STANDARD NO.	DESCRIPTION
442201-03	CLASS C AND D PATCHES
630001-09	STEEL PLATE BEAM GUARDRAIL
630301-05	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINAL
701201-04	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS > 45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATION
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701601-07	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701606-07	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-07	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES
701306-03	
701336-06	
701502-04	
701602-05	
701006-03	
701011-02	
701427	

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITIES OF LOCKPORT AND JOLIET.

THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION FROM THE DEPARTMENT.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT

THE ENGINEER SHALL CONTACT LAWRENCE HILL, AREA TRAFFIC FIELD ENGINEER, AT (815) 485-6475 A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF FINAL PAVEMENT MARKINGS.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H)

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 FOR ARTERIALS A MINIMUM OF 72 HOURS IN ADVANCE PRIOR TO BEGINNING WORK.

THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OF BASE ON WHICH THE HMA MIXTURE IS PLACED.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINE IN THE FIELD BE THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE CONTRACTOR SHALL VERIFY THE EXISTING TYPE/ HEIGHT OF EXISTING GUARDRAIL BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION SHALL MATCH THE HEIGHT OF THE EXISTING GUARDRAIL.

THE REMOVAL OF GUARDRAIL TERMINAL SECTIONS SHALL BE INCLUDED IN THE UNIT PRICE PER FOOT FOR "GUARDRAIL REMOVAL."

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS				F.A.P.	SECTION	COUNTY	TOTAL	SHEET
ct:\pw\work\pwsdot\midyja\d0248692\013301	sht-plan.dgn	DRAWN -	REVISED -		STATE STANDARDS, GENERAL NOTES				577	D-RS-7	WILL	38	2
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO	STA.	CONTRACT NO. 60N60	
	PLOT DATE = 3/28/2011	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

Rev.

SUMMARY OF QUANTITIES				URBAN 100% STATE				CONSTRUCTION TYPE CODE: 0005				SUMMARY OF QUANTITIES				URBAN 100% STATE				CONSTRUCTION TYPE CODE: 0005				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES					CODE NO	ITEM	UNIT	TOTAL QUANTITIES					CODE NO	ITEM	UNIT	TOTAL QUANTITIES					
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	149	149				58700300	CONCRETE SEALER	SO FT	11248	11248												
20400800	FURNISHED EXCAVATION	CU YD	480	480				* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	8	8												
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	848	848				63200310	GUARDRAIL REMOVAL	FOOT	400	400												
25000210	SEEDING, CLASS 2A	ACRE	0.18	0.18				67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6												
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	16	16				67100100	MOBILIZATION	L SUM	1	1												
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	16	16				70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1												
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	16	16				70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1												
25100630	EROSION CONTROL BLANKET	SO YD	848	848				70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1												
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	141	141				70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1												
40600300	AGGREGATE (PRIME COAT)	TON	706	706				70300100	SHORT TERM PAVEMENT MARKING	FOOT	27123	27123												
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	265	265				70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	763.2	763.2												
40600895	CONSTRUCTING TEST STRIP	EACH	2	2				70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	92616	92616												
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	1201	1201				70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	6146	6146												
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	190	190				70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	740	740												
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	14820	14820				70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1555	1555												
42001300	PROTECTIVE COAT	SO YD	556	556				70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	1020	1020												
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SO YD	1255	1255				70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	9041	9041												
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	176430	176430				* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	763.2	763.2												
44002213	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3 1/4"	SO YD	1043	1043				* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	92616	92616												
44201749	CLASS D PATCHES, TYPE I, 9 INCH	SO YD	17	17				* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	6146	6146												
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SO YD	422	422				* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	740	740												
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SO YD	293	293				* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1555	1555												
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SO YD	261	261				* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	1020	1020												
44201785	CLASS D PATCHES, TYPE I, 12 INCH	SO YD	54	54				* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	764	764												
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SO YD	471	471				* 78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	46	46												
44201794	CLASS D PATCHES, TYPE III, 12 INCH	SO YD	335	335				* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	2775	2775												
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SO YD	626	626				* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	8	8												
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SO YD	52	52				78300100	PAVEMENT MARKING REMOVAL	SO FT	161	161												
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SO YD	40	40																				
44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SO YD	1451	1451																				
48101620	AGGREGATE SHOULDERS, TYPE B 10"	SO YD	120	120																				
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	325	325																				

FILE NAME =	USER NAME = mdy/ja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				ILL 171 SUMMARY OF QUANTITIES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2:\work\p\rd\mdy\ja\0248692\01330H-stt-plandp	DRAWN -	REVISED -	577									D-RS-7	WILL	38	3	
PLOT SCALE = 507684' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60N60													
PLOT DATE = 3/29/2011	DATE -	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT													

* SPECIALTY ITEMS

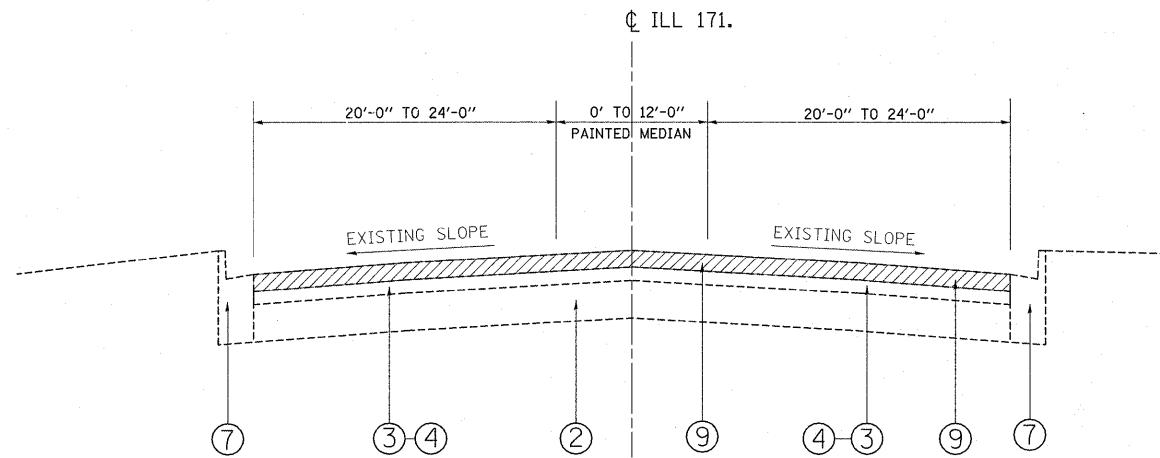
SUMMARY OF QUANTITIES			URBAN 100% STATE	CONSTRUCTION TYPE CODE: 0005					SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE: 0005					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES						CODE NO	ITEM	UNIT	TOTAL QUANTITIES					
				0005													
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	2220	2220													
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	2210	2210													
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	160	160													
X4060826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	7225	7225													
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	143	143													
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	2500	2500													
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	92	92													
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	102.8	102.8													
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1													
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1													
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1													
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1													

FILE NAME =	USER NAME = mly/ja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILL. 171 SUMMARY OF QUANTITIES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwr\work\pwr\dot\mly\ja\0248692\0133011-sM-plan.dgn	DRAWN -	REVISED -	577			D-RS-7	WILL	38	4	
PLOT SCALE = 507684' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60N60							
PLOT DATE = 3/28/2011	DATE -	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

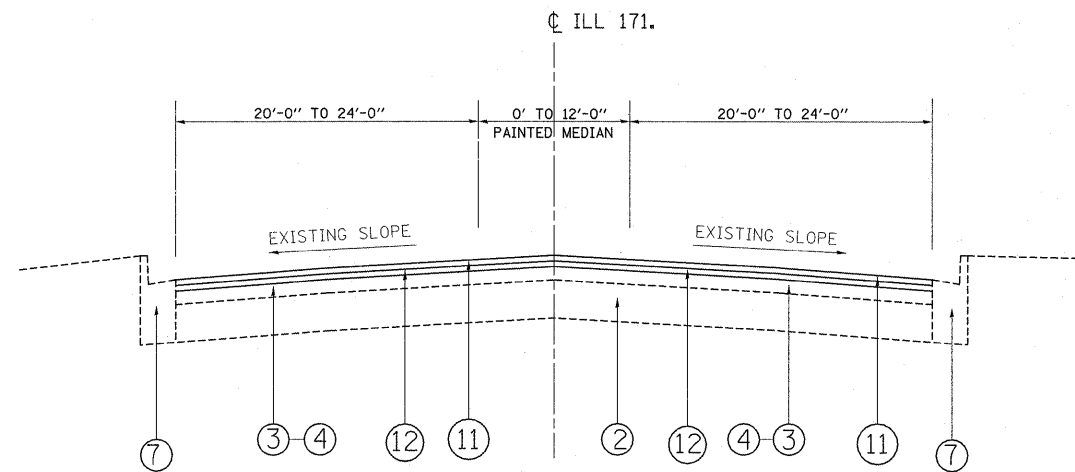
* SPECIALTY ITEMS

LEGEND

- ① EXISTING P.C.C. PAVEMENT, ±10"
- ② EXISTING P.C.C. PAVEMENT, ±9"
- ③ EXISTING HOT-MIX ASPHALT AFTER MILLING, ±5 1/4"
- ④ EXISTING HOT-MIX ASPHALT AFTER MILLING, ±3 1/4" *
- ⑤ EXISTING HOT-MIX ASPHALT SHOULDER
- ⑥ EXISTING AGGREGATE SHOULDER
- ⑦ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 & B-6.24
- ⑧ EXISTING MEDIAN
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑩ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑫ PROPOSED POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑬ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑭ PROPOSED GRADING AND SHAPING SHOULDER
- ⑮ PROPOSED SAFETY EDGE WHEN HMA SHOULDER < 3 FT (30°, 5" DEPTH MAX)



EXISTING TYPICAL SECTION
ILL 171
STA. 0+51.9 TO STA. 206+00
*(STA. 0+51.9 TO STA. 163+00)



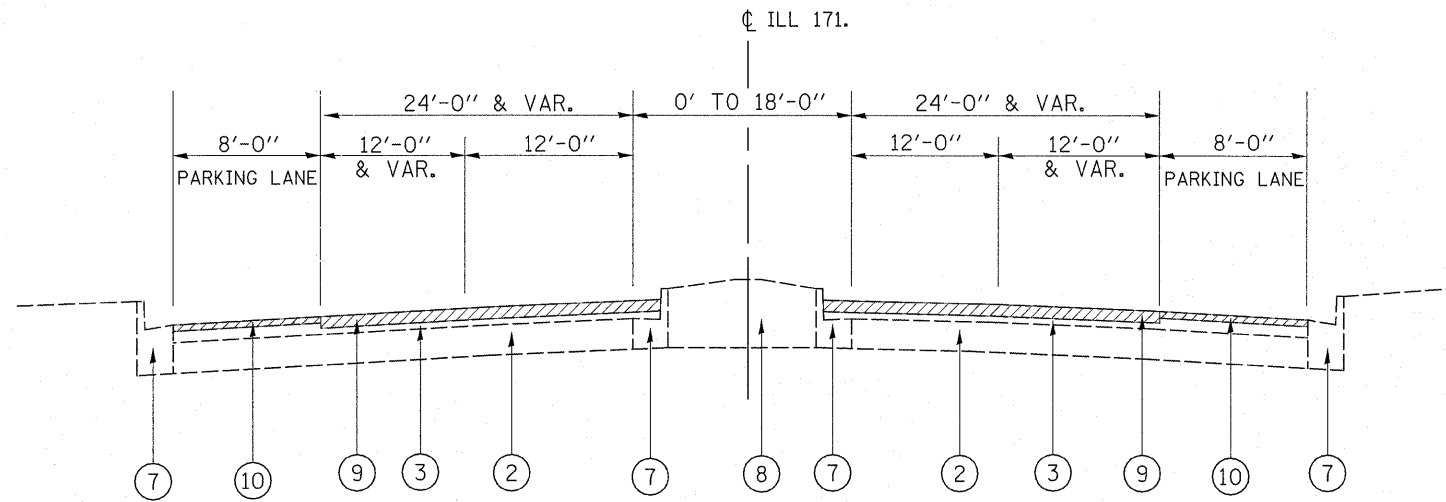
PROPOSED TYPICAL SECTION
ILL 171
STA. 0+51.9 TO STA. 206+00
*(STA. 0+51.9 TO STA. 163+00)

**THE CONTRACTOR SHALL MILL FIRST
BEFORE PATCHING (EXCEPT AS NOTED BELOW)**

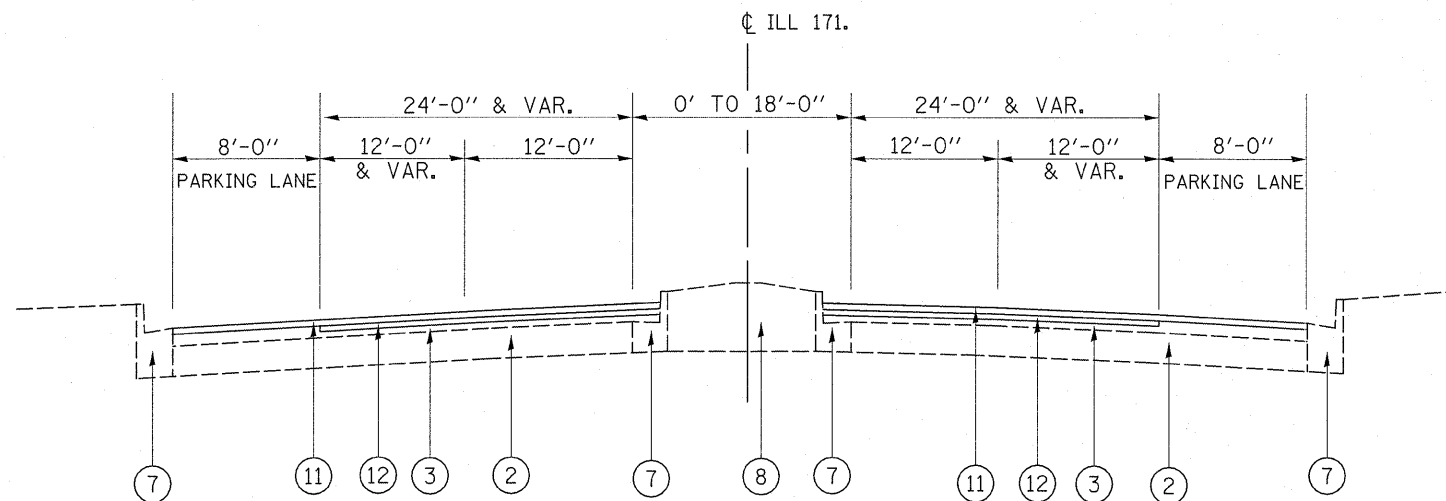
**THE CONTRACTOR SHALL PATCH FIRST
BEFORE MILLING BETWEEN STA 0+51.9 TO STA 163+00**

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ NODES
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm)	4% @ 70 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES, (HMA BINDER IL-19 mm)	4% @ 70 GYR
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR

- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ.YD./IN
- THE AC TYPE FOR POLYMERIZED HMA MIXES SHALL BE SBS/SBR PG 70 -22 AND FOR NON-POLYMERIZED HMA THE AC TYPE SHALL BE PG 64 -22 UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.



EXISTING TYPICAL SECTION
ILL 171
STA. 206+00 TO STA. 264+00

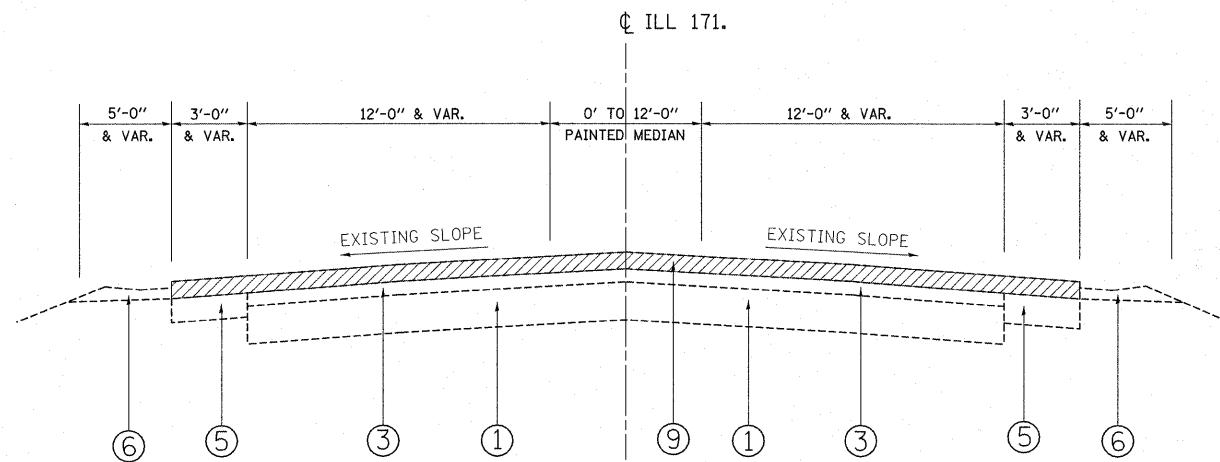


PROPOSED TYPICAL SECTION
ILL 171
STA. 206+00 TO STA. 264+00

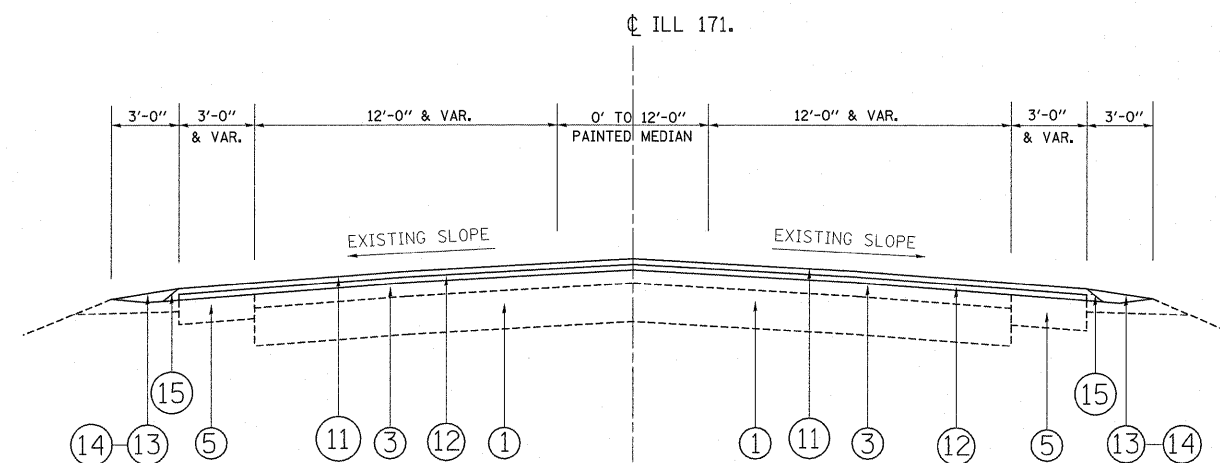
LEGEND

- ① EXISTING P.C.C. PAVEMENT, ±10"
- ② EXISTING P.C.C. PAVEMENT, ±9"
- ③ EXISTING HOT-MIX ASPHALT AFTER MILLING, ±5/4"
- ④ EXISTING HOT-MIX ASPHALT AFTER MILLING, ±3/4" *
- ⑤ EXISTING HOT-MIX ASPHALT SHOULDER
- ⑥ EXISTING AGGREGATE SHOULDER
- ⑦ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 & B-6.24
- ⑧ EXISTING MEDIAN
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑩ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑫ PROPOSED POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑬ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑭ PROPOSED GRADING AND SHAPING SHOULDER
- ⑮ PROPOSED SAFETY EDGE WHEN HMA SHOULDER < 3 FT (30°, 5" DEPTH MAX)

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILL 171 EXISTING & PROPOSED TYPICAL SECTIONS		F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 6	
c:\pw_work\pwidot\midyja\d0248692\013301-sht-plan.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO. 1 OF 1 SHEETS	STA. +	TO STA. +	CONTRACT NO. 60N60			
		CHECKED -	REVISED -		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT							
		DATE -	REVISED -									



EXISTING TYPICAL SECTION
ILL 171
STA. 264+00 TO STA. 376+74.48

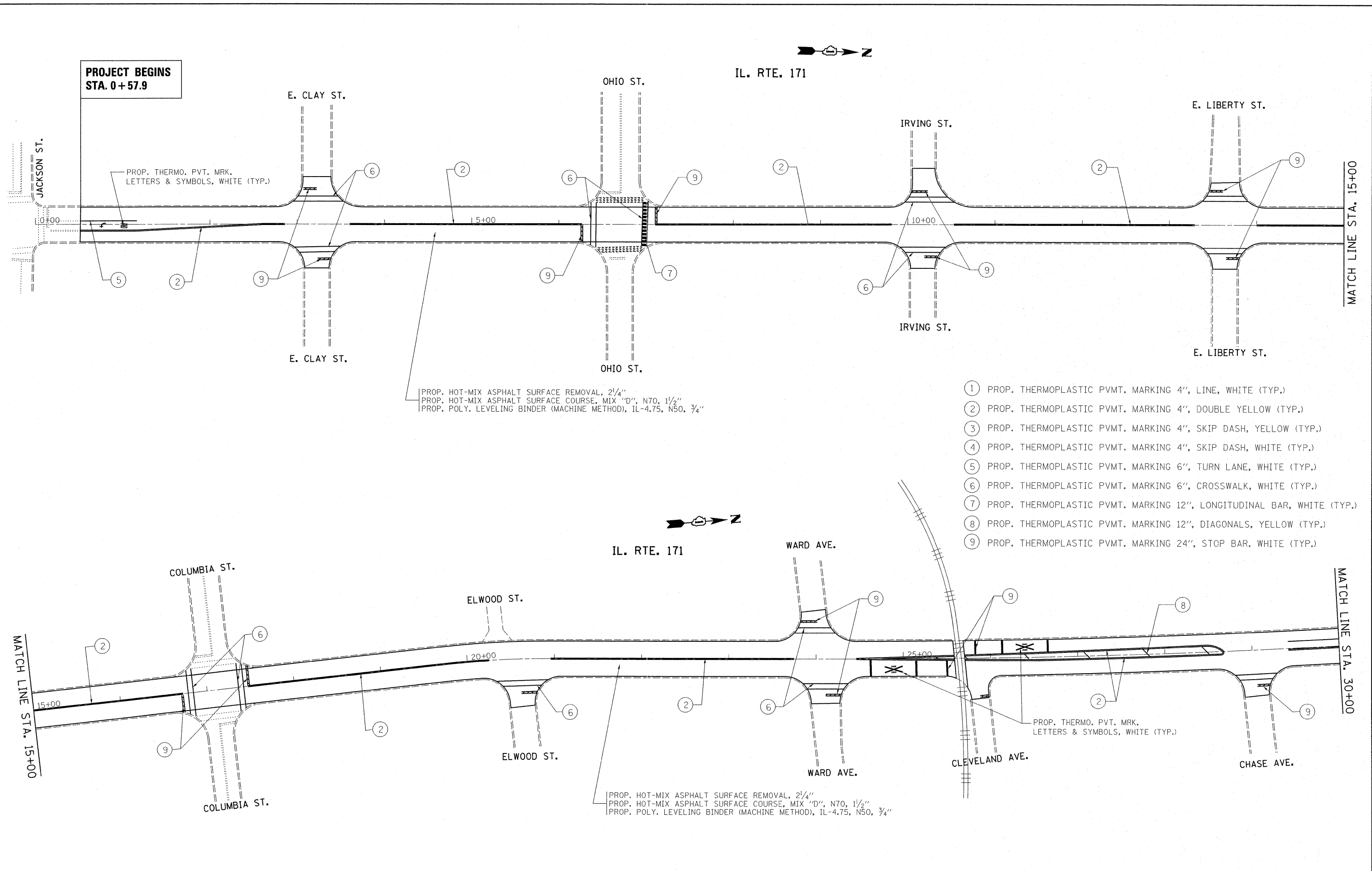


PROPOSED TYPICAL SECTION
ILL 171
STA. 264+00 TO STA. 376+74.48

LEGEND

- ① EXISTING P.C.C. PAVEMENT, ±10"
- ② EXISTING P.C.C. PAVEMENT, ±9"
- ③ EXISTING HOT-MIX ASPHALT AFTER MILLING, ±5/4"
- ④ EXISTING HOT-MIX ASPHALT AFTER MILLING, ±3/4" *
- ⑤ EXISTING HOT-MIX ASPHALT SHOULDER
- ⑥ EXISTING AGGREGATE SHOULDER
- ⑦ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 & B-6.24
- ⑧ EXISTING MEDIAN
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑩ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑫ PROPOSED POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑬ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑭ PROPOSED GRADING AND SHAPING SHOULDER
- ⑮ PROPOSED SAFETY EDGE WHEN HMA SHOULDER < 3 FT (30°, 5" DEPTH MAX)

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILL 171 EXISTING & PROPOSED TYPICAL SECTIONS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw\work\pwwdot\midyja\0248692\013301\sh-t-pln.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO. 1 OF 1 SHEETS	STA. +	577	D-RS-7	WILL	38	7
		CHECKED -	REVISED -				TO STA. +					
		DATE -	REVISED -					CONTRACT NO. 60N60				
							FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT					



PROJECT BEGINS
STA. 0+57.9

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", TURN LANE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", CROSSWALK, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", LONGITUDINAL BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)

FILE NAME =
c:\pwork\pwsdot\mdyja\d0248692\013301-sh1-plen.dgn

USER NAME = mdyja
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 3/28/2011

DESIGNED -
DRAWN -
CHECKED -
DATE -

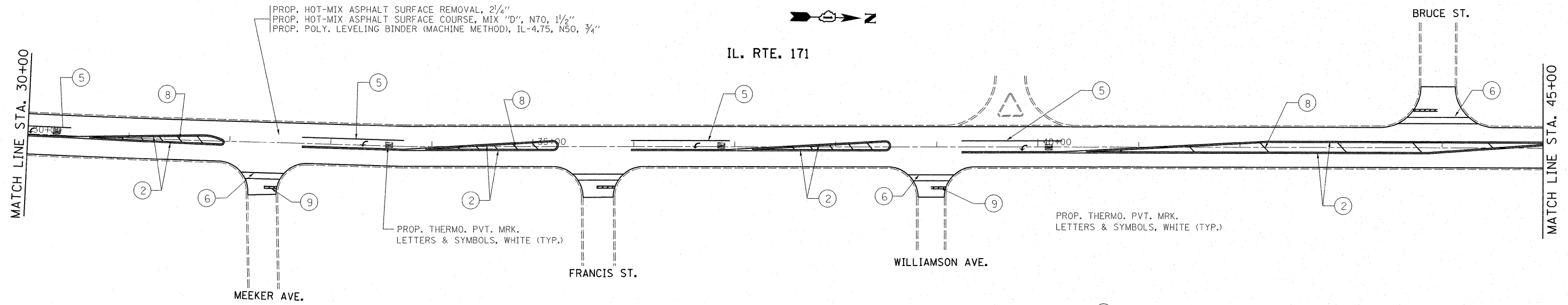
REVISED -
REVISED -
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REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

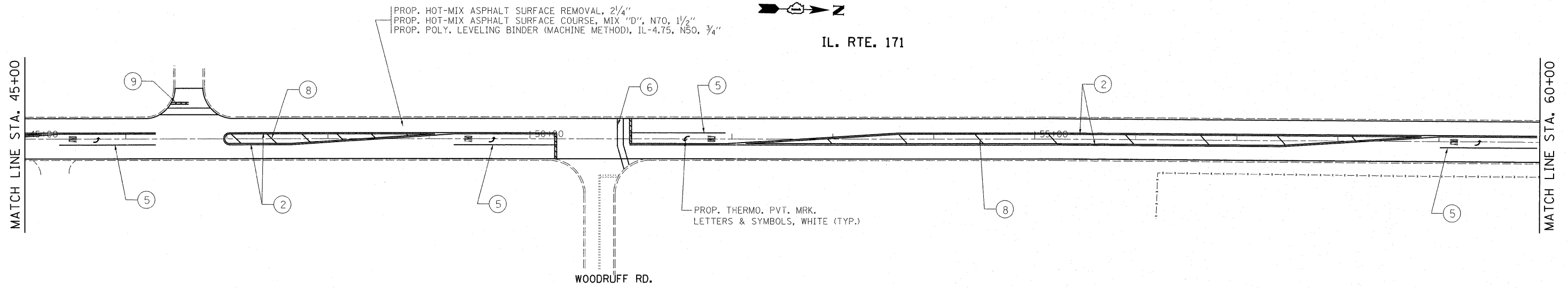
ILL 171
ROADWAY AND PAVEMENT MARKING PLAN

SCALE: SHEET NO. OF SHEETS STA. TO STA.

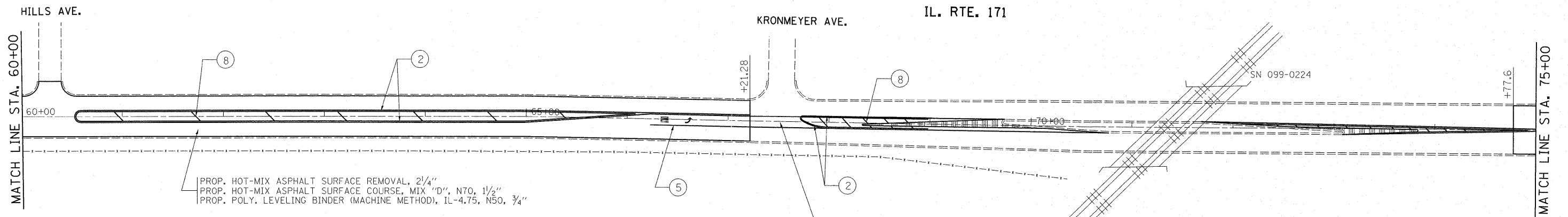
F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 8
CONTRACT NO. 60N60				
ILLINOIS FED. AID PROJECT				



- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", TURN LANE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", CROSSWALK, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", LONGITUDINAL BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)



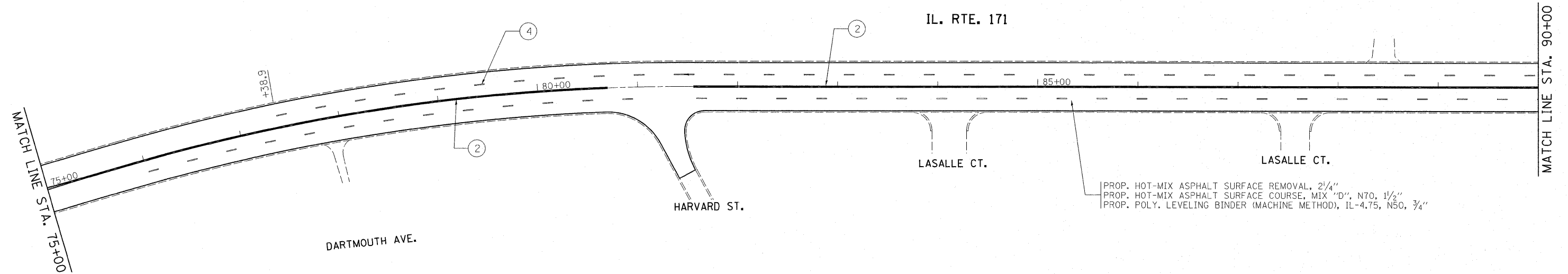
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	PLOT DATE = 3/26/2011	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT								
		DATE -	REVISED -										



PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

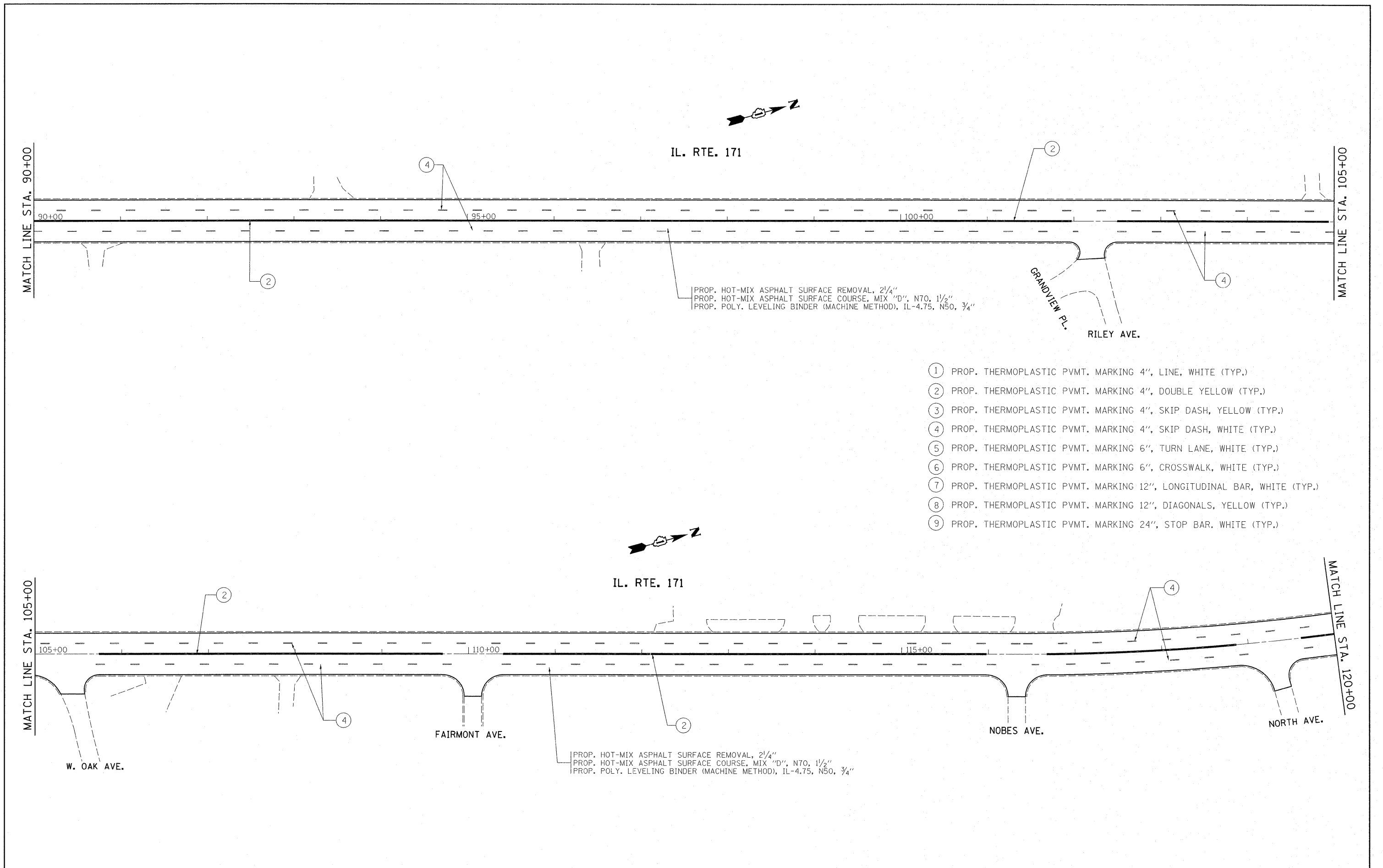
STA 67+21.28 TO STA 74+77.60 (MEDIAN SECTION ONLY)
 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", TURN LANE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", CROSSWALK, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", LONGITUDINAL BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)

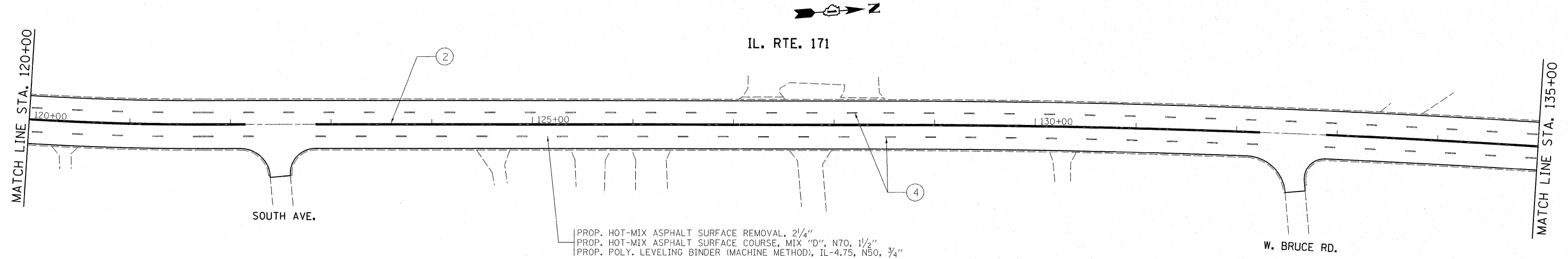


PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

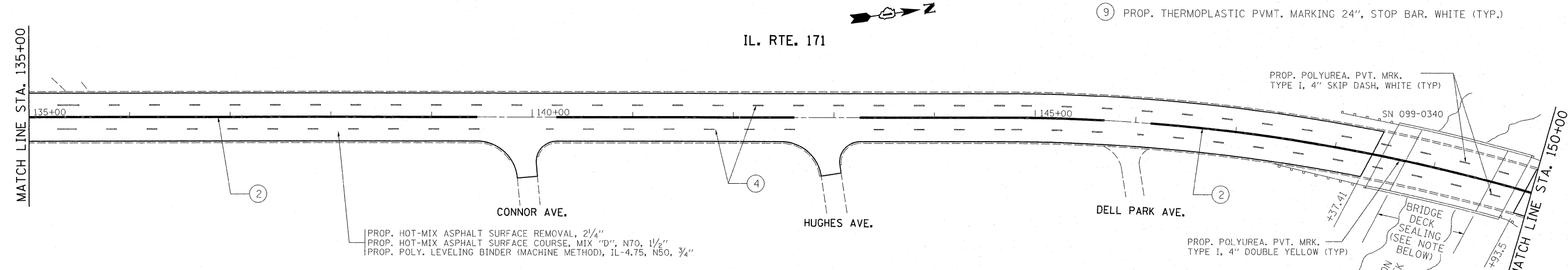
FILE NAME =	USER NAME = midjje	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILL 171 ROADWAY AND PAVEMENT MARKING PLAN				F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 10
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	PLT DATE = 3/29/2011	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT								
		DATE -	REVISED -										



FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILL 171 ROADWAY AND PAVEMENT MARKING PLAN			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
c:\px_work\pwi\dot\midyja\0248692\013301-sh1-plan.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	577	D-RS-7	WILL	38	11
		CHECKED -	REVISED -		CONTRACT NO. 60N60										
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT										
PLOT SCALE = 50.0000' / IN.		PLOT DATE = 3/29/2011													



- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", TURN LANE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", CROSSWALK, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", LONGITUDINAL BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)



NOTE: THE CONTRACTOR SHALL SEAL THE EXISTING DECK, SIDEWALK AND PARAPET WITH CONCRETE SEALER

THE CONTRACTOR SHALL CLEAN THE DECK PRIOR TO THE APPLICATION OF THE CONCRETE SEALER TO THE SATISFACTION OF THE ENGINEER.

COST OF CLEANING IS INCLUDED IN THE COST OF CONCRETE SEALER.

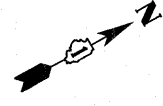
SEE BRIDGE PLAN SHEET #27

FILE NAME =	USER NAME = mdy.ja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILL 171 ROADWAY AND PAVEMENT MARKING PLAN				F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 12
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT								
	PLOT DATE = 3/29/2011	DATE -	REVISED -										

MATCH LINE STA. 150+00

MATCH LINE STA. 165+00

IL. RTE. 171



150+00

155+00

160+00

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

E. WOODS LN.

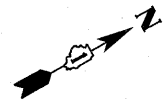
PARKVIEW LN.

- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", TURN LANE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", CROSSWALK, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", LONGITUDINAL BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)

MATCH LINE STA. 165+00

MATCH LINE STA. 180+00

IL. RTE. 171



165+00

170+00

175+00

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

DAGGETT ST.

E. 19th ST.

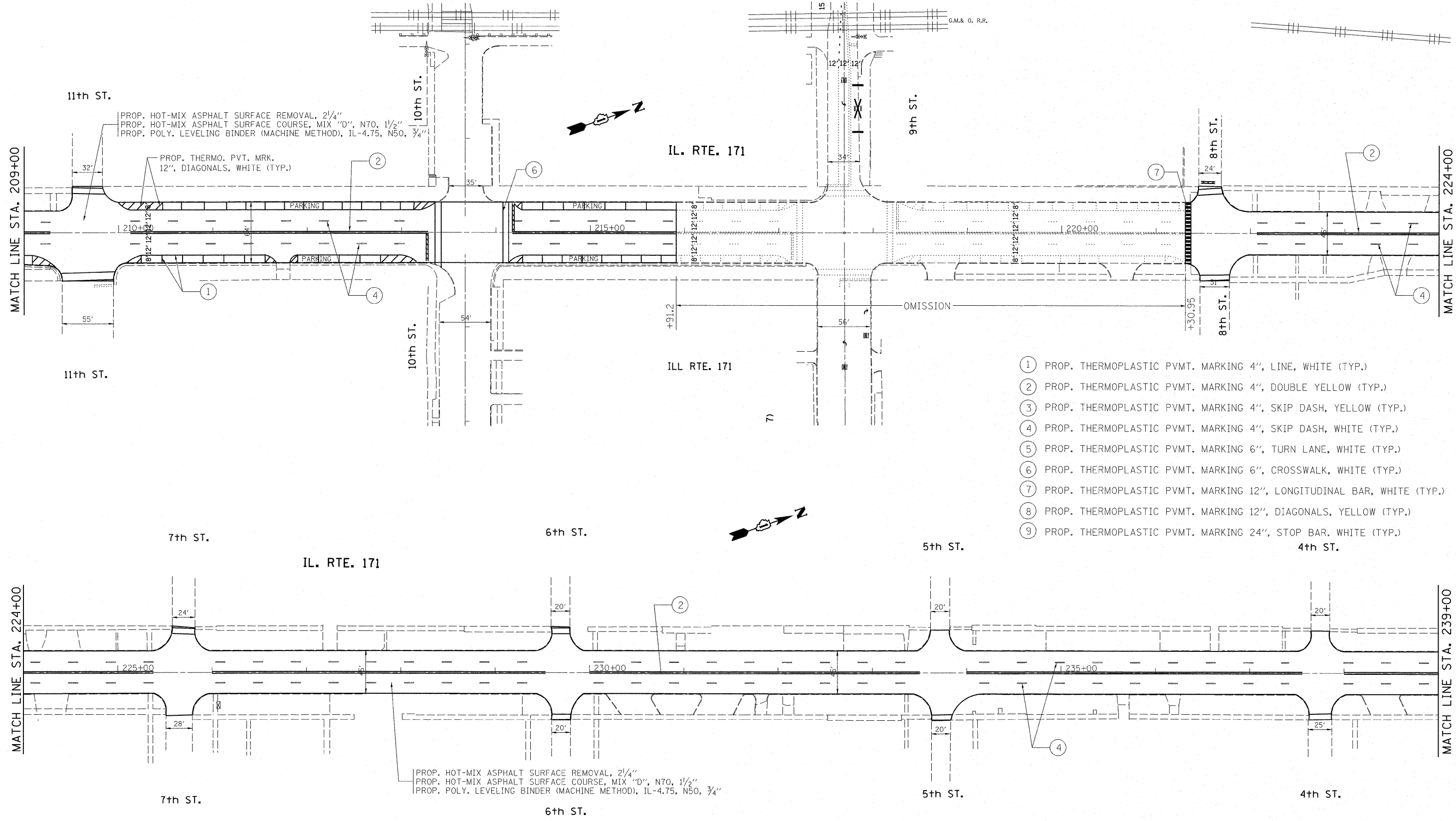
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PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = 3/29/2011		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ILL 171
 ROADWAY AND PAVEMENT MARKING PLAN**

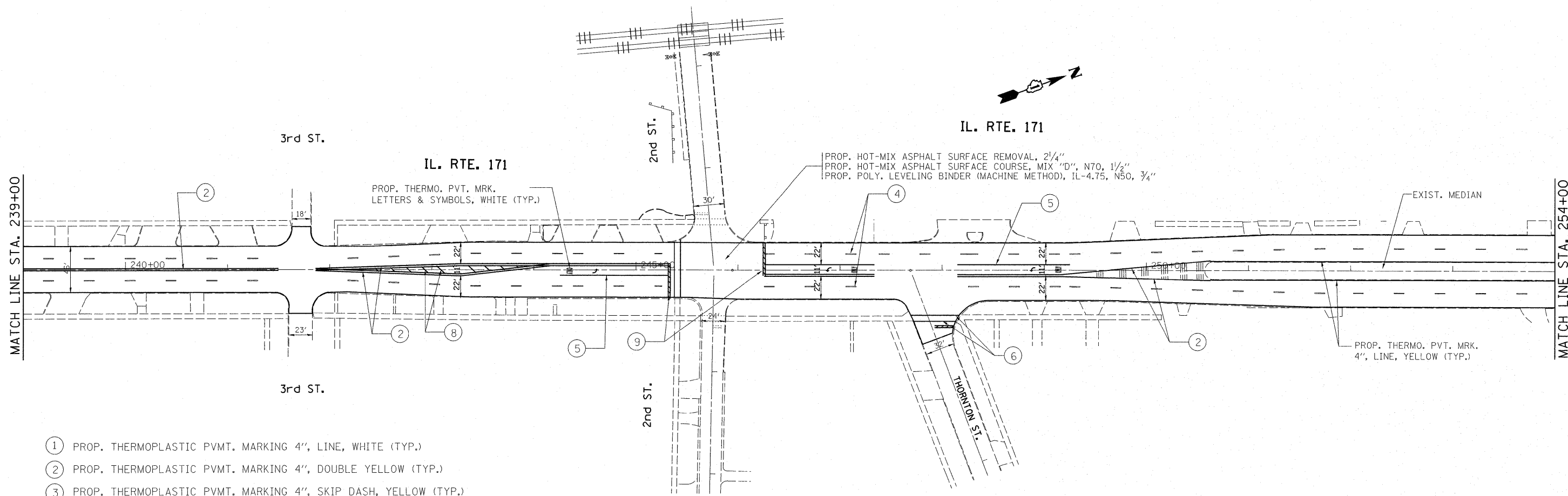
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	D-RS-7	WILL	38	13
CONTRACT NO. 60N60				
ILLINOIS FED. AID PROJECT				

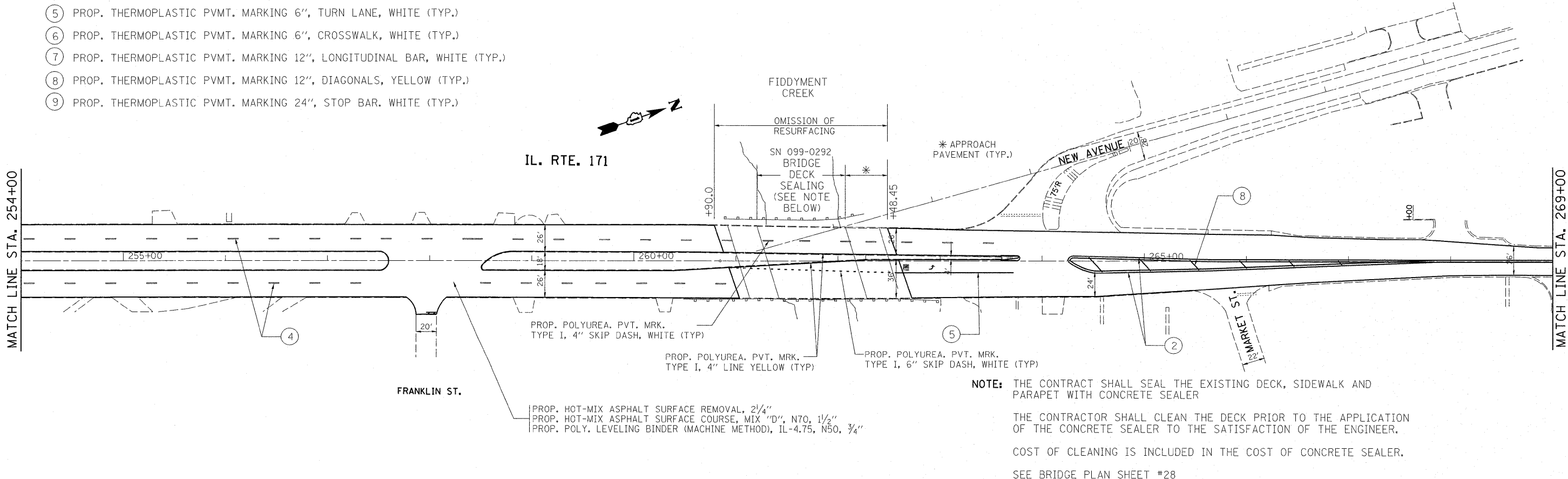


- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", TURN LANE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", CROSSWALK, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", LONGITUDINAL BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILL 171 ROADWAY AND PAVEMENT MARKING PLAN			F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 15
c:\pw_work\pwsdot\midyja\d0248692\013301	-sht-plan.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 60N60	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -								ILLINOIS FED. AID PROJECT	
	PLOT DATE = 3/29/2011	DATE -	REVISED -									



- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", TURN LANE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", CROSSWALK, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", LONGITUDINAL BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)



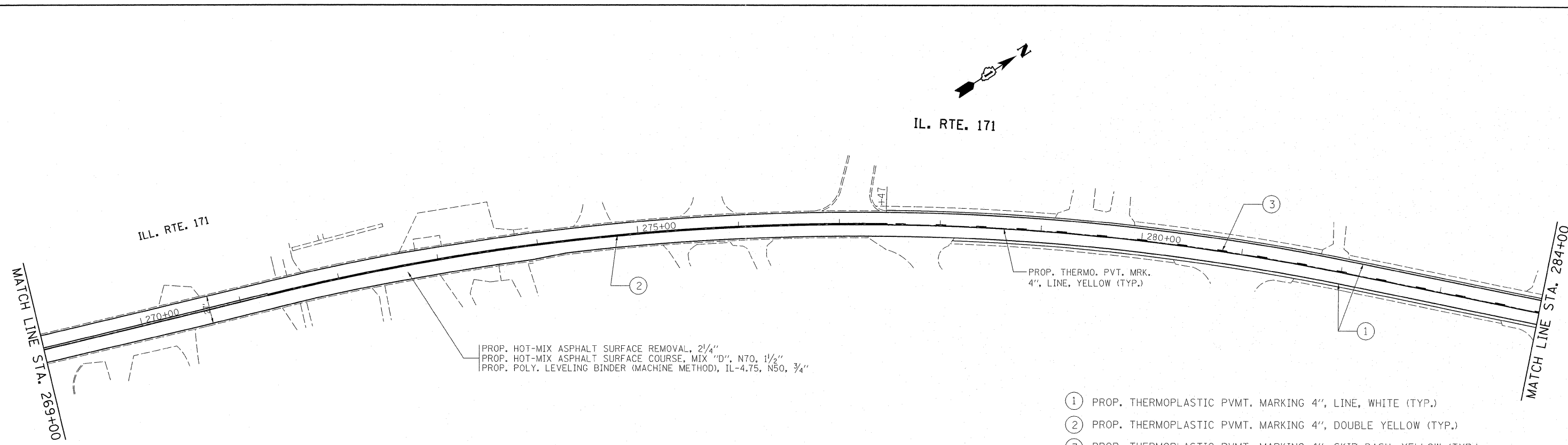
NOTE: THE CONTRACTOR SHALL SEAL THE EXISTING DECK, SIDEWALK AND PARAPET WITH CONCRETE SEALER

THE CONTRACTOR SHALL CLEAN THE DECK PRIOR TO THE APPLICATION OF THE CONCRETE SEALER TO THE SATISFACTION OF THE ENGINEER.

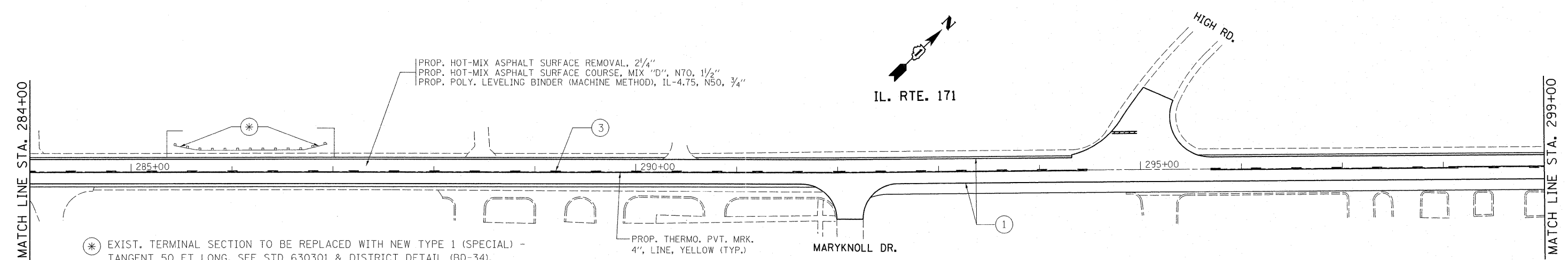
COST OF CLEANING IS INCLUDED IN THE COST OF CONCRETE SEALER.

SEE BRIDGE PLAN SHEET #28

FILE NAME =	USER NAME = mdyjs	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILL 171 ROADWAY AND PAVEMENT MARKING PLAN				F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 16
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PLOT SCALE = 50.0000 / / IN.		CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT								
PLOT DATE = 3/29/2011		DATE -	REVISED -										



- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", TURN LANE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", CROSSWALK, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", LONGITUDINAL BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)

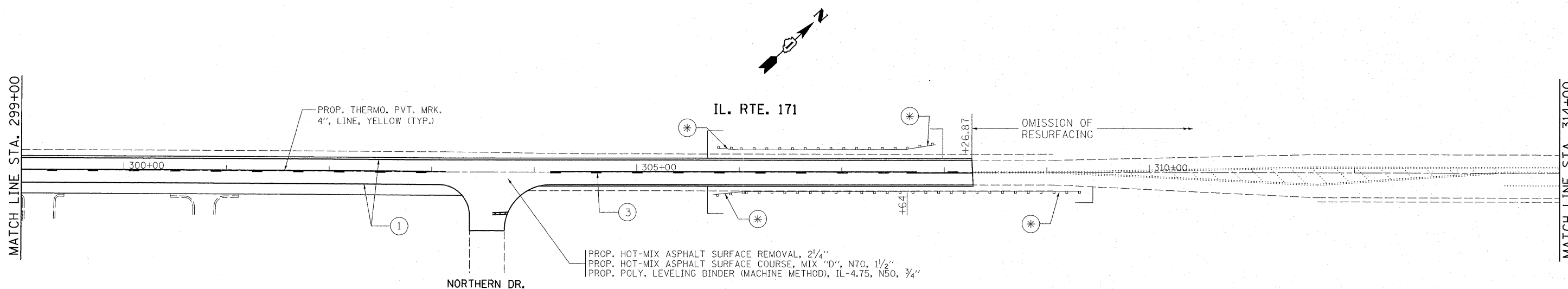


⊛ EXIST. TERMINAL SECTION TO BE REPLACED WITH NEW TYPE 1 (SPECIAL) - TANGENT 50 FT LONG, SEE STD 630301 & DISTRICT DETAIL (BD-34). OTHER PAY ITEMS WITH QUANTITIES ESTIMATED FOR THIS AREA INCLUDE:
 REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL;
 FURNISHED EXCAVATION;
 TOP SOIL FURNISH AND PLACE, 4";
 SEEDING, CLASS 2A;
 NITROGEN FERTILIZER NUTRIENT;
 PHOSPHORUS FERTILIZER NUTRIENT;
 POTASSIUM FERTILIZER NUTRIENT;
 EROSION CONTROL BLANKET;
 AGGREGATE SHOULDER, TYPE B, 10";
 GUARDRAIL REMOVAL;

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILL 171 ROADWAY AND PAVEMENT MARKING PLAN				F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 17
et\pv_work\pwsdot\midyja\d0248692\013301-sht-plan.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 60N60			
		CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT								
		DATE -	REVISED -										

MATCH LINE STA. 299+00

MATCH LINE STA. 314+00

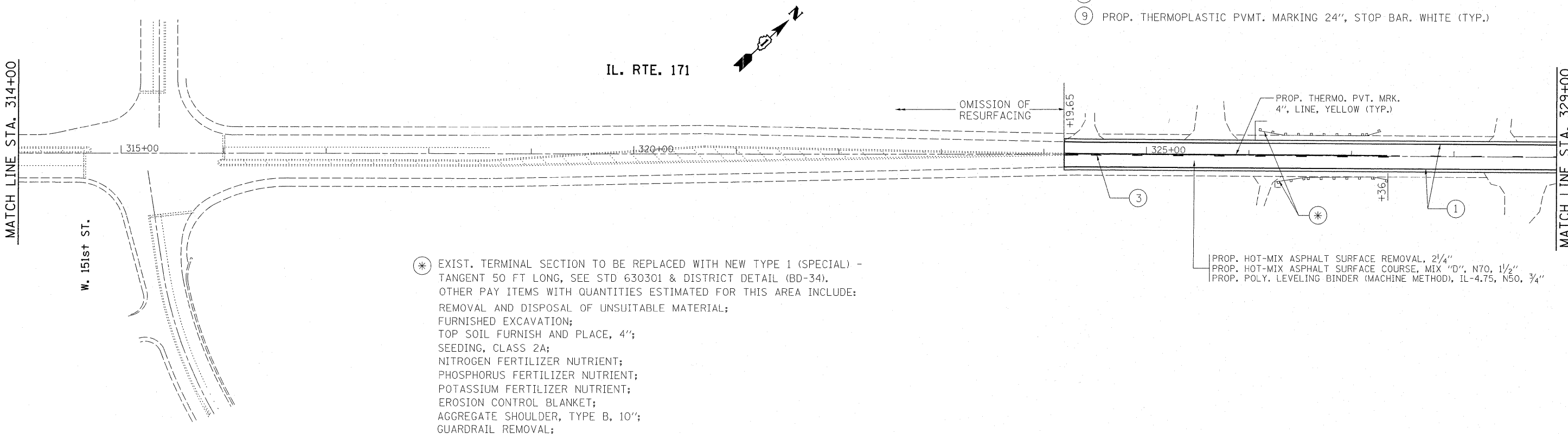


PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", TURN LANE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", CROSSWALK, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", LONGITUDINAL BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)

MATCH LINE STA. 314+00

MATCH LINE STA. 329+00



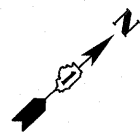
* EXIST. TERMINAL SECTION TO BE REPLACED WITH NEW TYPE 1 (SPECIAL) - TANGENT 50 FT LONG, SEE STD 630301 & DISTRICT DETAIL (BD-34). OTHER PAY ITEMS WITH QUANTITIES ESTIMATED FOR THIS AREA INCLUDE:
 REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL;
 FURNISHED EXCAVATION;
 TOP SOIL FURNISH AND PLACE, 4";
 SEEDING, CLASS 2A;
 NITROGEN FERTILIZER NUTRIENT;
 PHOSPHORUS FERTILIZER NUTRIENT;
 POTASSIUM FERTILIZER NUTRIENT;
 EROSION CONTROL BLANKET;
 AGGREGATE SHOULDER, TYPE B, 10";
 GUARDRAIL REMOVAL;

FILE NAME =	USER NAME = midyjo	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILL 171 ROADWAY AND PAVEMENT MARKING PLAN				F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 18
cr:\pw_work\pwsdot\midyjo\d0248692\013301-shr-plan.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 60N60		
PLOT SCALE = 50,0000' / IN.		CHECKED -	REVISED -										
PLOT DATE = 3/26/2011		DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

MATCH LINE STA. 329+00

MATCH LINE STA. 344+00

IL. RTE. 171



330+00

335+00

340+00

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

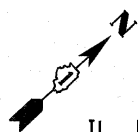
- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", TURN LANE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", CROSSWALK, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", LONGITUDINAL BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)

MATCH LINE STA. 344+00

MATCH LINE STA. 359+00

147th ST.

IL. RTE. 171



345+00

350+00

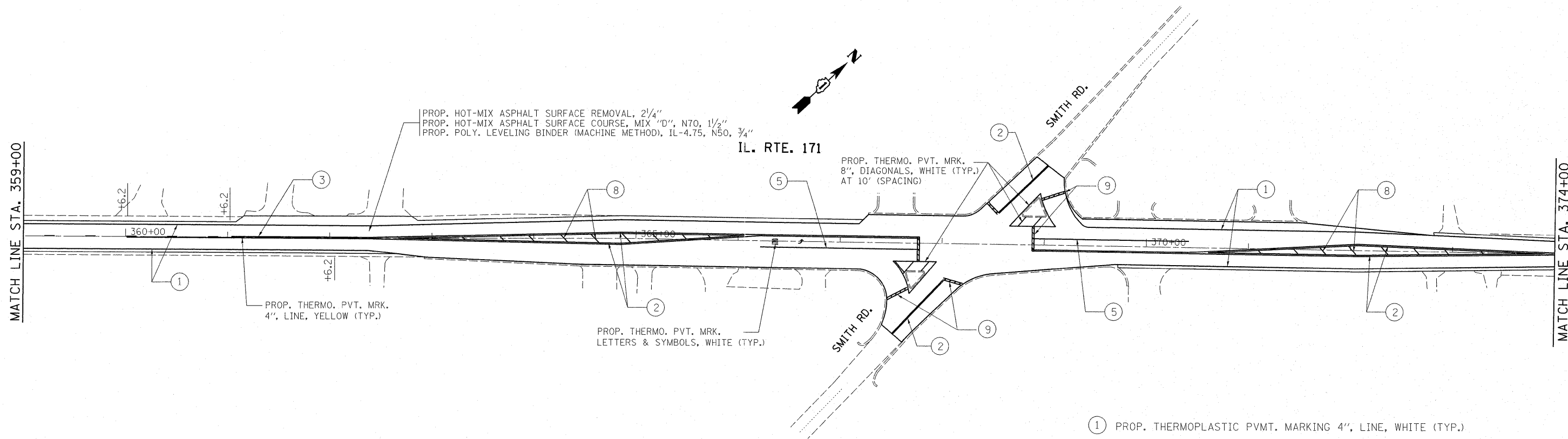
355+00

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

FILE NAME =	USER NAME = mdyjs	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILL 171 ROADWAY AND PAVEMENT MARKING PLAN				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pwork\pwork\mdyjs\0248692\013301-sht-plan.dgn		DRAWN -	REVISED -		577	D-RS-7	WILL	38	19				
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.				CONTRACT NO. 60N60				
PLOT DATE = 3/29/2011		DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

MATCH LINE STA. 359+00

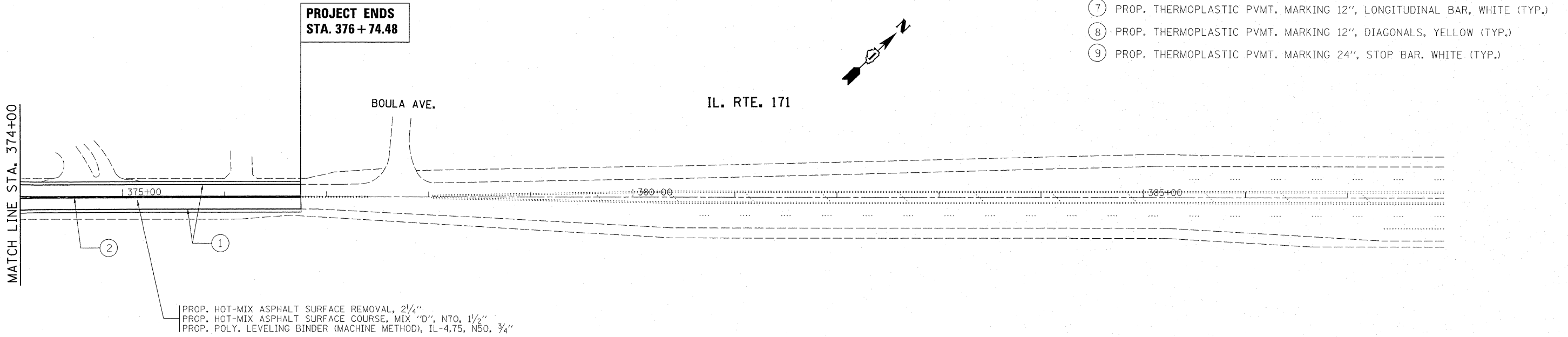
MATCH LINE STA. 374+00



- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", TURN LANE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", CROSSWALK, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", LONGITUDINAL BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)

MATCH LINE STA. 374+00

**PROJECT ENDS
STA. 376 + 74.48**



FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -
c:\pwork\pwork\midyja\0248692\013301-shit-plan.dgn		DRAWN -	REVISED -
PLOT SCALE = 50.0000 / / IN.		CHECKED -	REVISED -
PLOT DATE = 3/29/2011		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ILL 171
ROADWAY AND PAVEMENT MARKING PLAN**

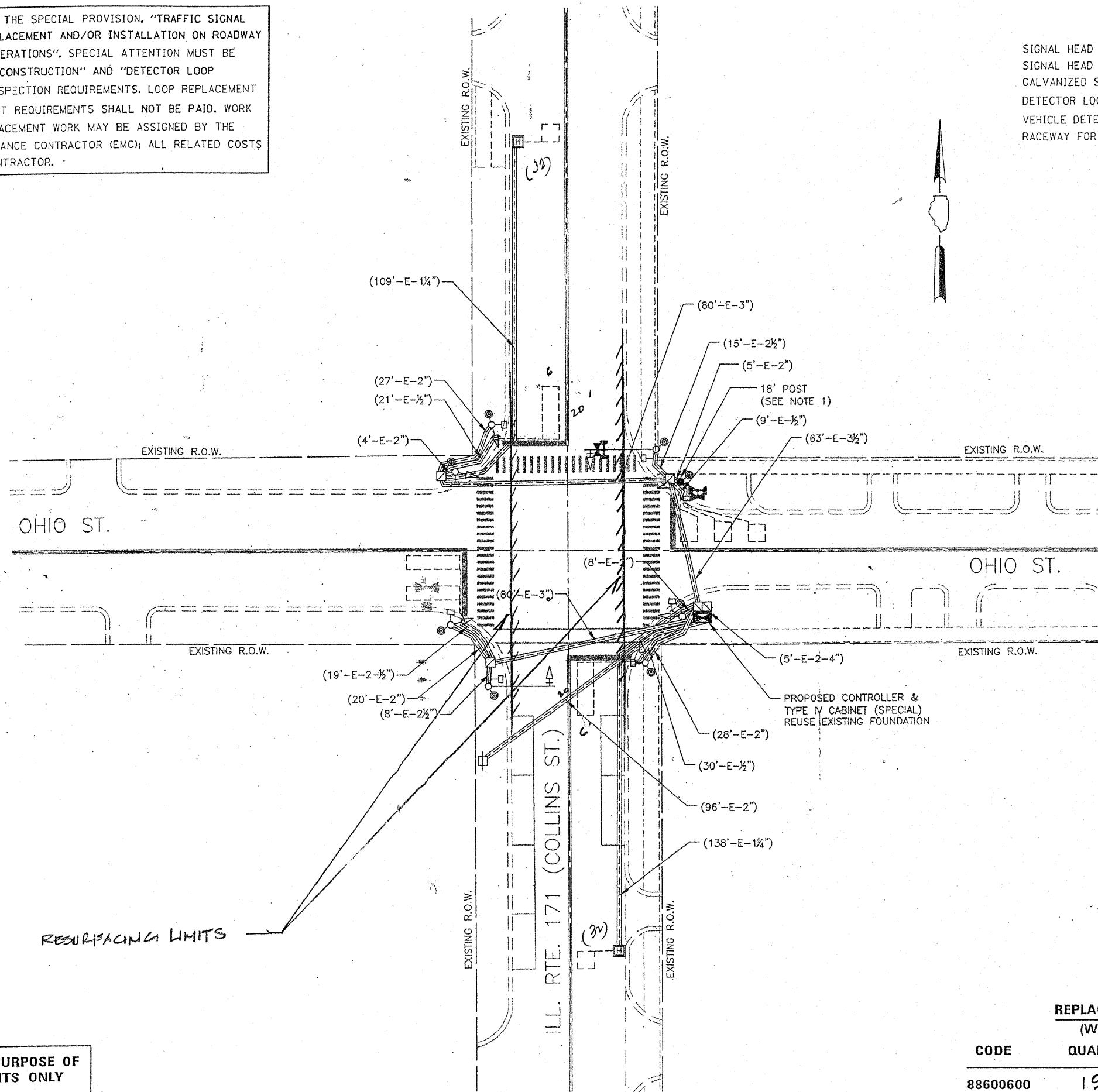
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 20
CONTRACT NO. 60N60				
ILLINOIS FED. AID PROJECT				

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE	
SIGNAL HEAD	
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	
DETECTOR LOOP	
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	192	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME *	USER NAME * nguyenm	DESIGNED -	REVISED -
et:\pr-work\p1\dot\nguyenm\0112618\1\101er.dgn		DRAWN -	REVISED -
PLOT SCALE = 100.0000 ' / IN.		CHECKED -	REVISED -
PLOT DATE = 12/1/2010		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. ROUTE 171 @ OHIO STREET

SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	F.A.P. RTE. 577	SECTION D-R5-7	COUNTY WILL.	TOTAL SHEETS 38	SHEET NO. 21
			FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

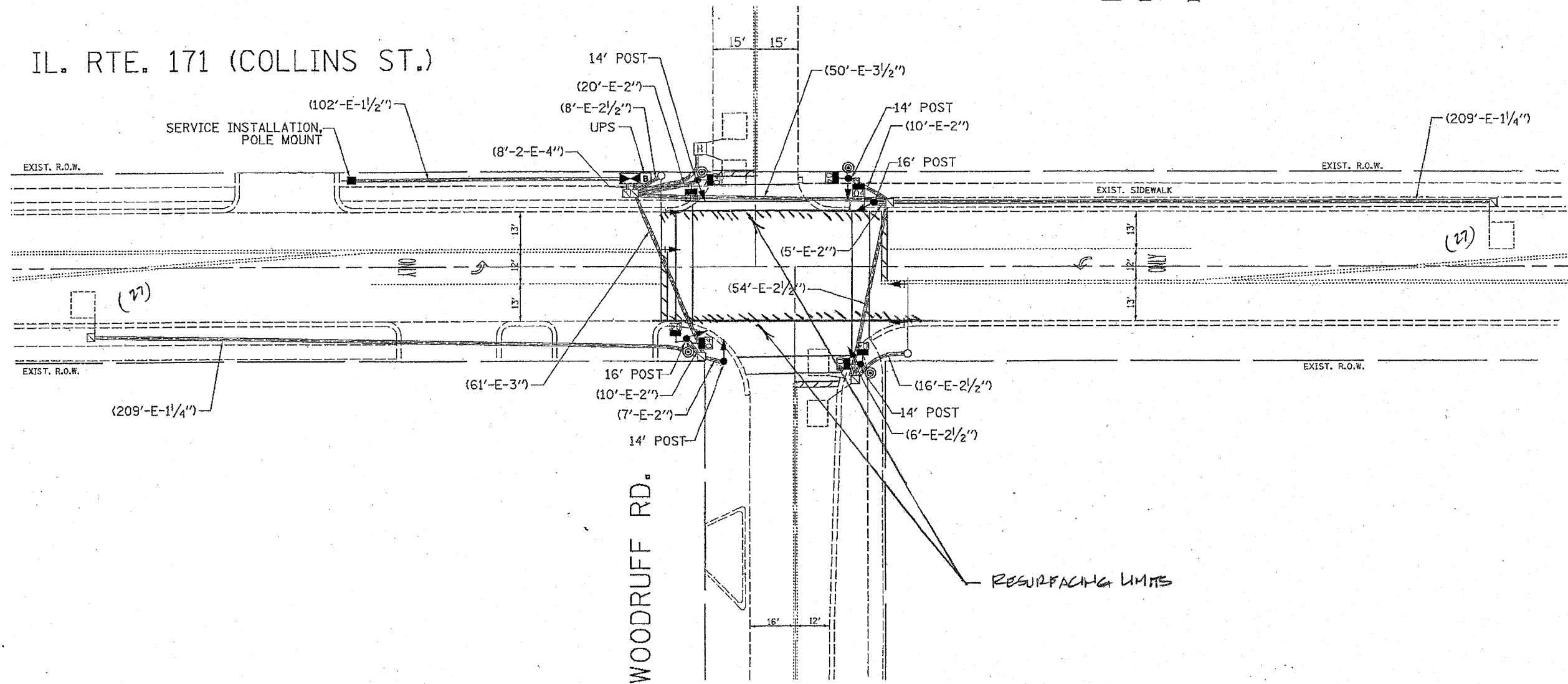
CONTRACT NO. 60N60

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE	➔	➔
SIGNAL HEAD	➔	➔
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	---	---
DETECTOR LOOP	□	□
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	⊙	⊙
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	—E—	—E—

IL. RTE. 171 (COLLINS ST.)



**THIS PLAN IS FOR THE SOLE PURPOSE OF
DETECTOR LOOP REPLACEMENTS ONLY**

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

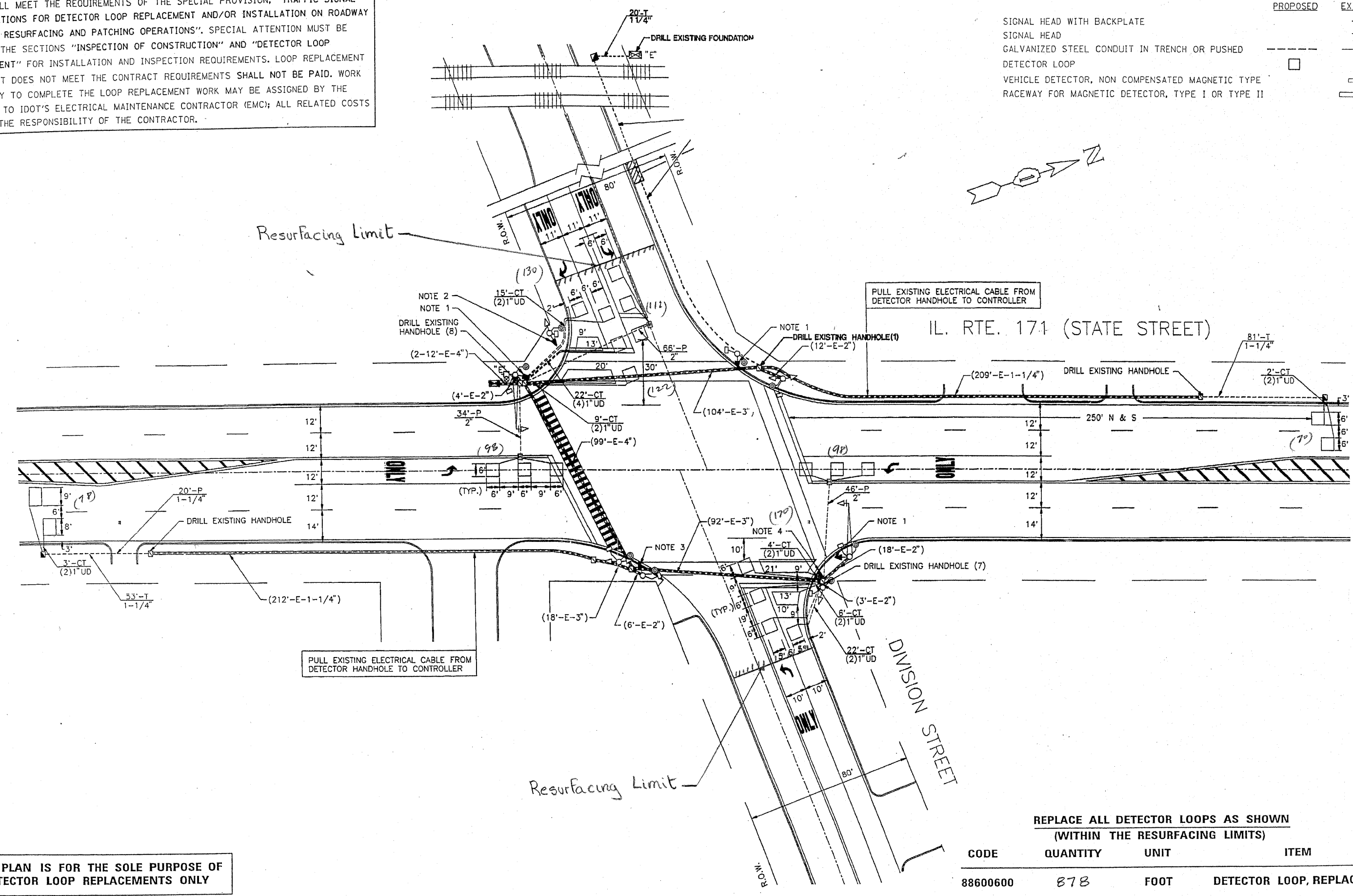
CODE	QUANTITY	UNIT	ITEM
88600600	54	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME *	USER NAME = nguyenm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT ILL. ROUTE 171 @ WOODRUFF ROAD	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw\work\p\idot\nguyenm\d0112618\1\laser.dgn	DRAWN -	REVISED -	677			D-RS-7	WILL	38	22	
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60N60							
PLOT DATE = 12/1/2010	DATE -	REVISED -	SCALE:			SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE	⊕	⊕
SIGNAL HEAD	→	→
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	---	---
DETECTOR LOOP	□	□
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	—	—
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	—	—



PULL EXISTING ELECTRICAL CABLE FROM DETECTOR HANDHOLE TO CONTROLLER

PULL EXISTING ELECTRICAL CABLE FROM DETECTOR HANDHOLE TO CONTROLLER

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

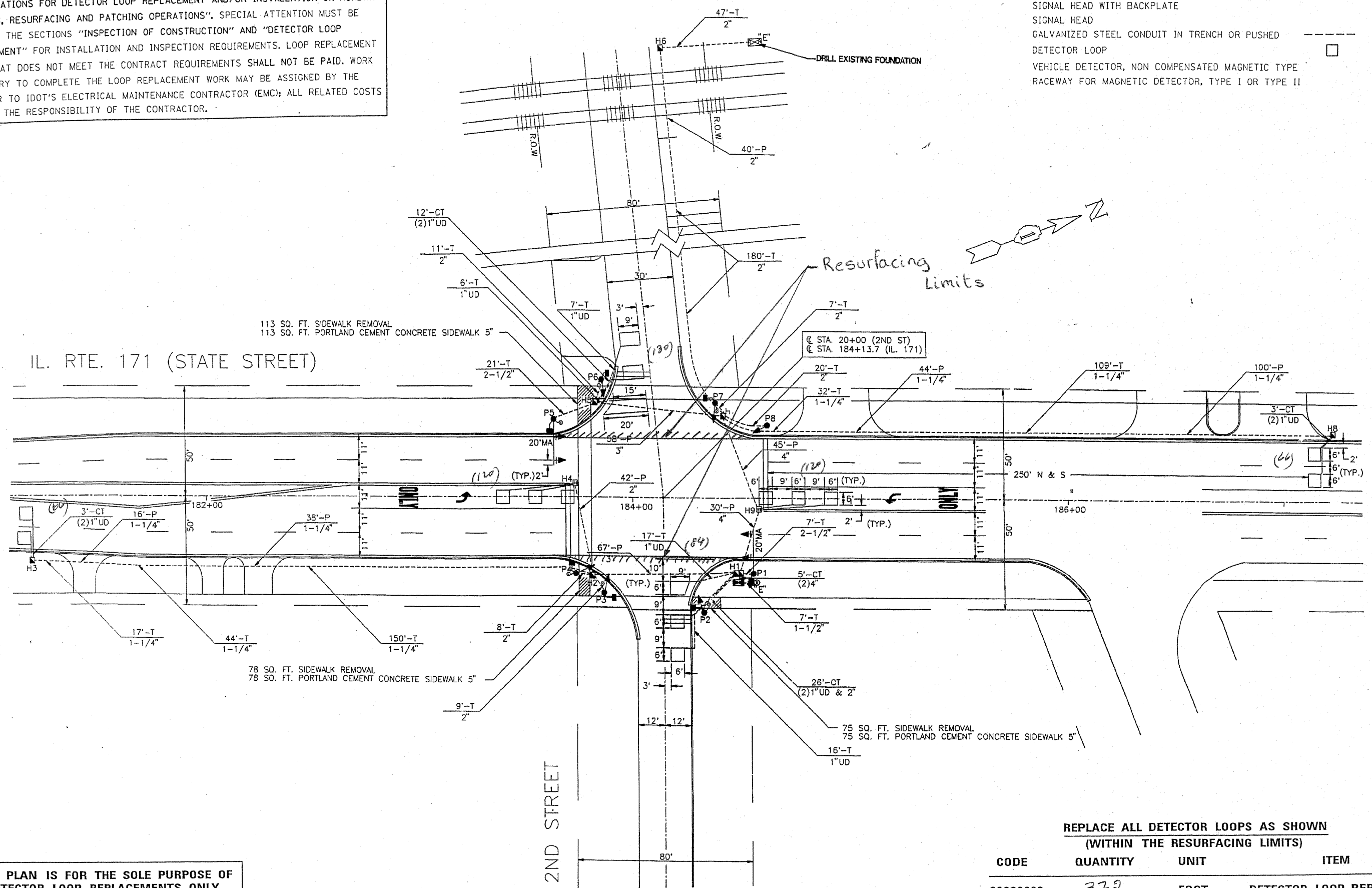
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)			
CODE	QUANTITY	UNIT	ITEM
88600600	878	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME: c:\pwork\pwork\nguyenm\08112618\F105\er.dgn	USER NAME: nguyenm	DESIGNED: -	REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT ILL. ROUTE 171 @ DIVISION STREET	F.A.R. RTE. 577	SECTION D-RS-7	COUNTY Will	TOTAL SHEETS 38	SHEET NO. 23
PLOT SCALE: 100.0000' / IN.	DRAWN: -	REVISIONS: -	SCALE: SHEET NO. OF SHEETS STA. TO STA.			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
PLOT DATE: 12/1/2010	CHECKED: -	REVISIONS: -								
	DATE: -	REVISIONS: -								

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE	→
SIGNAL HEAD	→
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	---
DETECTOR LOOP	□
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	⊞
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	—E—



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

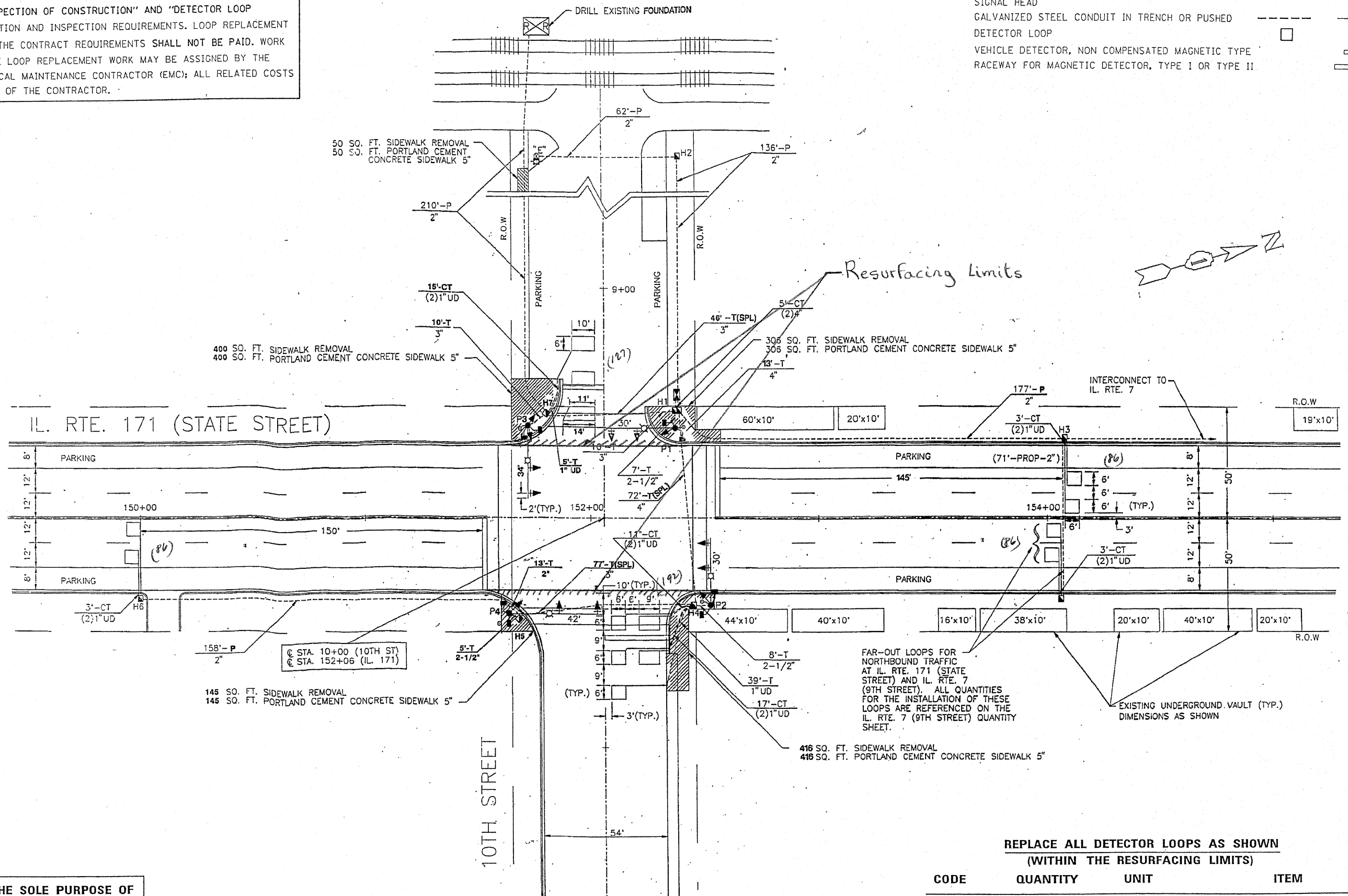
CODE	QUANTITY	UNIT	ITEM
88600600	372	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = nguyensm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT ILL. ROUTE 171 @ 2ND STREET	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pwork\p\dot\nguyensm\d0112618\1\l...er.dgn		DRAWN -	REVISED -			577	D-R3-7	Will	38	24
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -			FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
PLOT DATE = 12/1/2010		DATE -	REVISED -							

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

FAR-OUT LOOPS FOR NORTHBOUND TRAFFIC AT IL. RTE. 171 (STATE STREET) AND IL. RTE. 7 (9TH STREET). ALL QUANTITIES FOR THE INSTALLATION OF THESE LOOPS ARE REFERENCED ON THE IL. RTE. 7 (9TH STREET) QUANTITY SHEET.

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

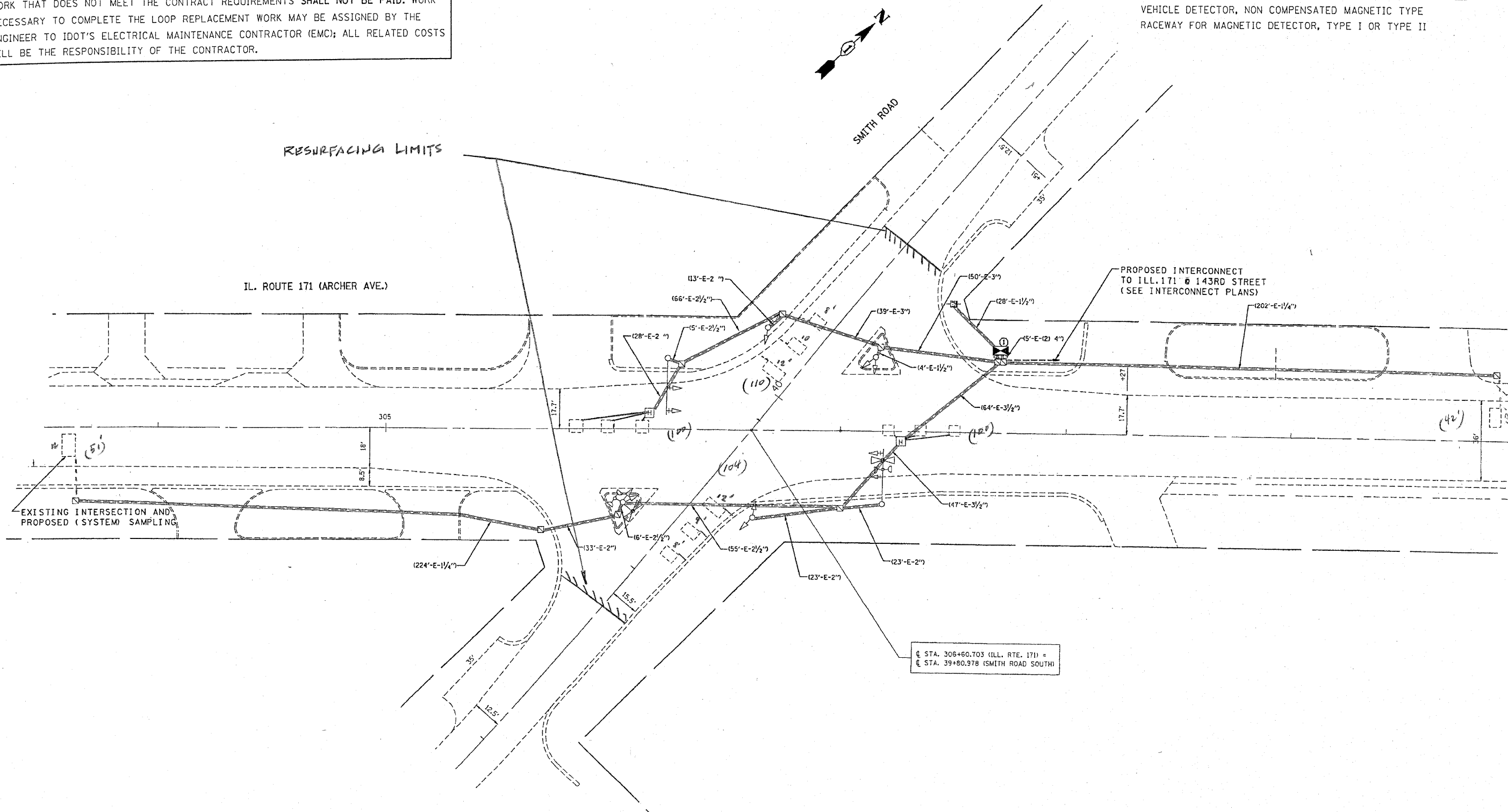
CODE	QUANTITY	UNIT	ITEM
88600600	258	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = nguyensm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT ILL. ROUTE 171 @ 10TH STREET	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ci:\work\pwidot\nguyensm\d0112618\files\ar.dgn		DRAWN -	REVISED -			597	D-RS-7	WILL	38	25	
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 60N60					
PLOT DATE = 12/1/2010		DATE -	REVISED -			SCALE: SHEET NO. OF SHEETS STA. TO STA. FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		

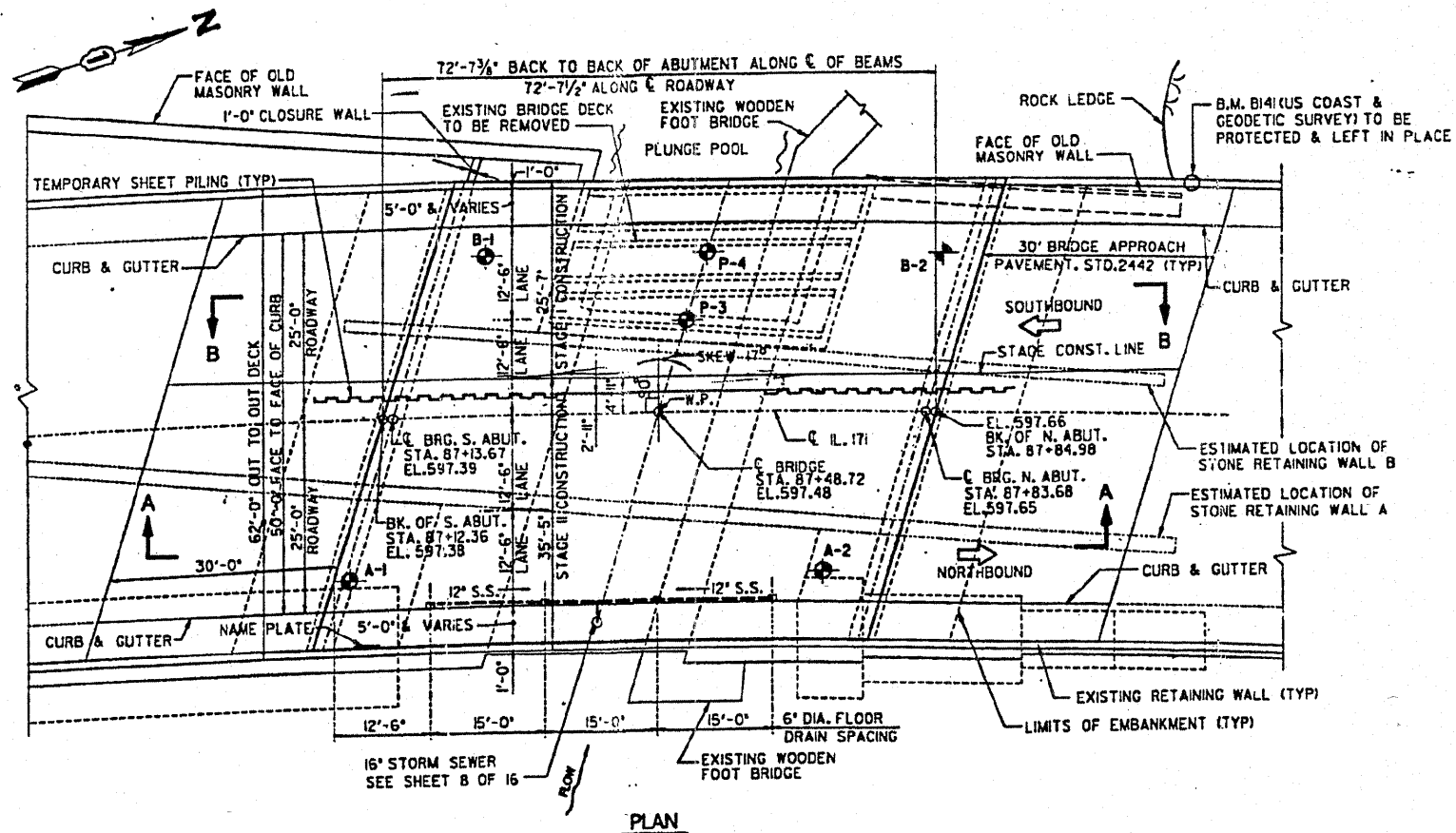


THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

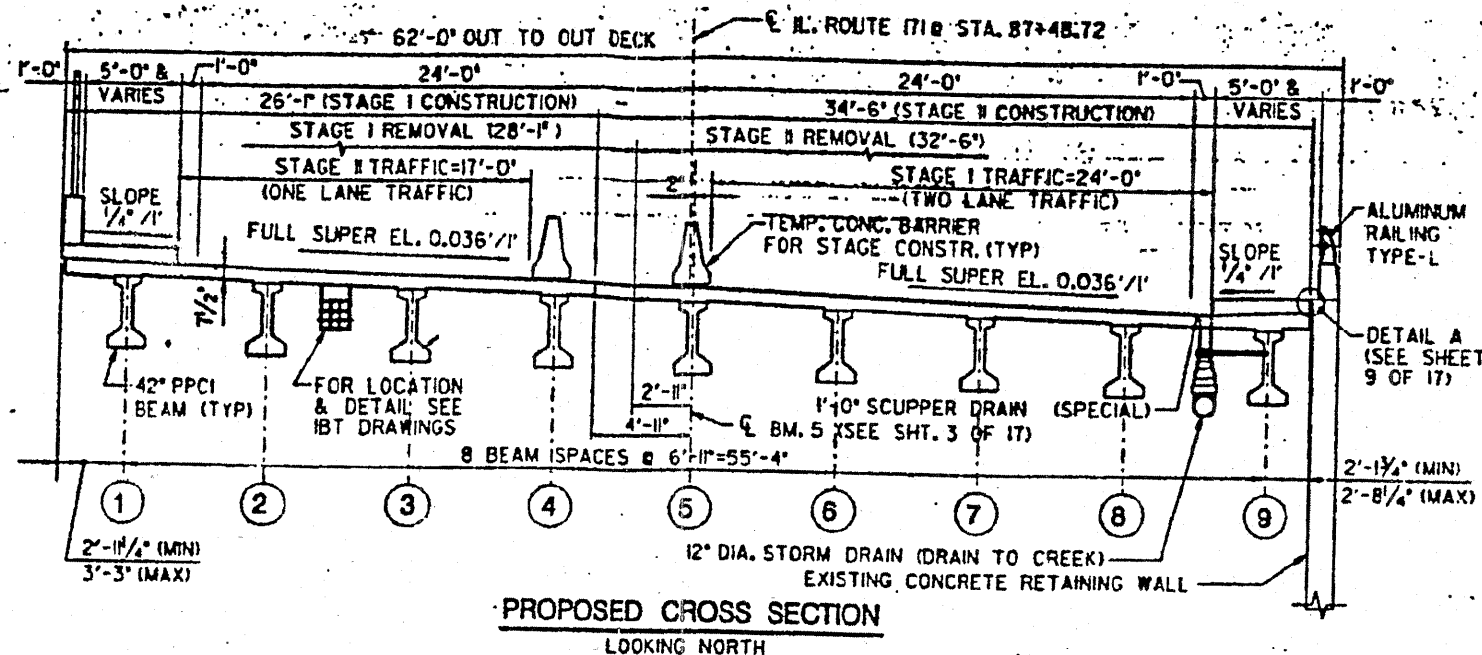
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	456	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME * es:\p\work\p\idot\nguyensm\08112618\11011011.dgn	USER NAME * nguyensm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT ILL. ROUTE 171 @ SMITH ROAD	F.A.P. RTE. 577	SECTION D-R5-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 26	
PLOT SCALE * 100.0000' / IN.	CHECKED -	REVISED -	REVISED -			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 60N60
PLOT DATE * 11/19/2010	DATE -	REVISED -	REVISED -								



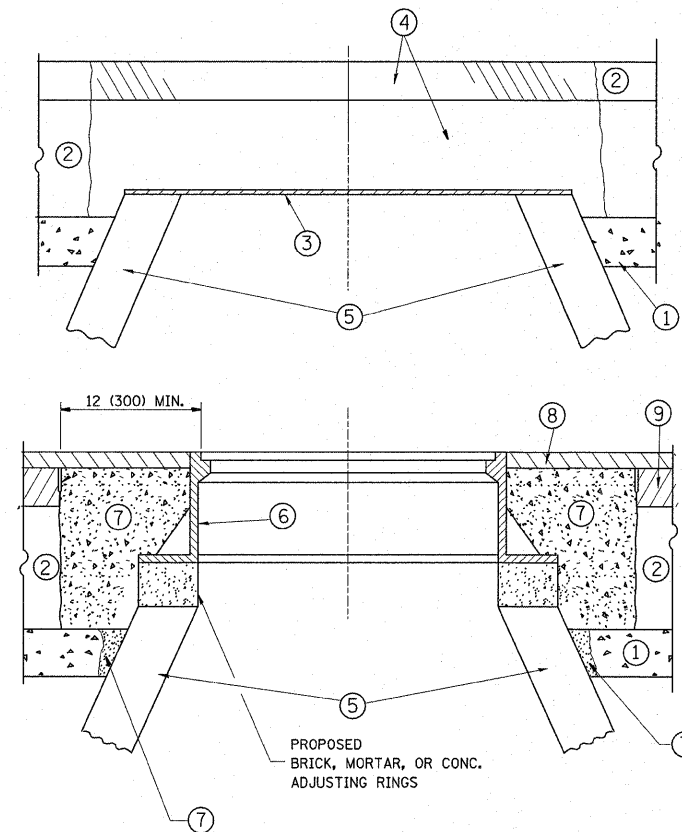
PLAN



PROPOSED CROSS SECTION
LOOKING NORTH

FOR INFORMATION ONLY

FILE NAME = c:\pwwork\pwwidot\midjje\d0248692\01330	USER NAME = midjje -sht-pln.dgn	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SN 099-0340 BRIDGE PLAN DETAILS		F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 27	
PLOT SCALE = 1/8" = 1' IN.	CHECKED -	REVISED -	REVISED -		SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO. 60N60 ILLINOIS FED. AID PROJECT				
PLOT DATE = 1/7/2011	DATE -	REVISED -	REVISED -									



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ⑥ FRAME AND LID (SEE NOTES)
- ② EXISTING PAVEMENT
- ⑦ CLASS PP-1* CONCRETE
- ③ 36 (900) DIAMETER METAL PLATE
- ⑧ PROPOSED HMA SURFACE COURSE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑨ PROPOSED HMA BINDER COURSE
- ⑤ EXISTING STRUCTURE

LOCATION OF STRUCTURES:

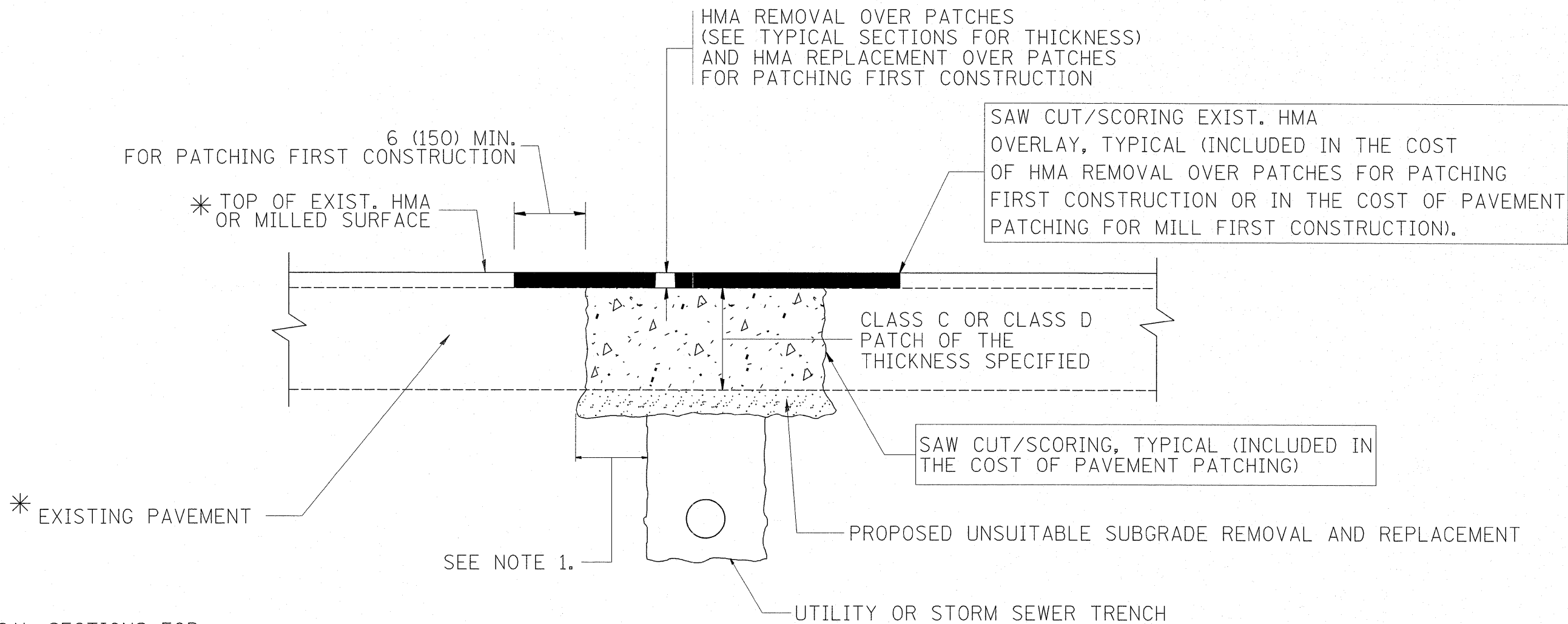
THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = msdyjs	DESIGNED - R. SHAH	REVISED - A. ABBAS 03-21-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ci:\pwr_work\pwr\dot\msdyjs\d0248692\DistS	dgn	DRAWN -	REVISED - R. WIEDEMAN 05-14-04			577	D-RS-7	WILL	38	29	
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. BORO 01-01-07				BD600-03 (BD-8)		CONTRACT NO. 60N60			
PLOT DATE = 3/26/2011	DATE - 10-25-94	REVISED - R. BORO 03-09-11				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = mdy.je	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pwork\pwork\mdy\je\08248692\DistSched.dgn		DRAWN -	REVISED - R. BORO 01-01-07					577	D-RS-7	WILL	38	30
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - R. BORO 09-04-07		SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		BD400-04 (BD-22) CONTRACT NO. 60N60	
PLOT DATE = 3/26/2011		DATE - 10-25-94	REVISED - K. ENG 10-27-08		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

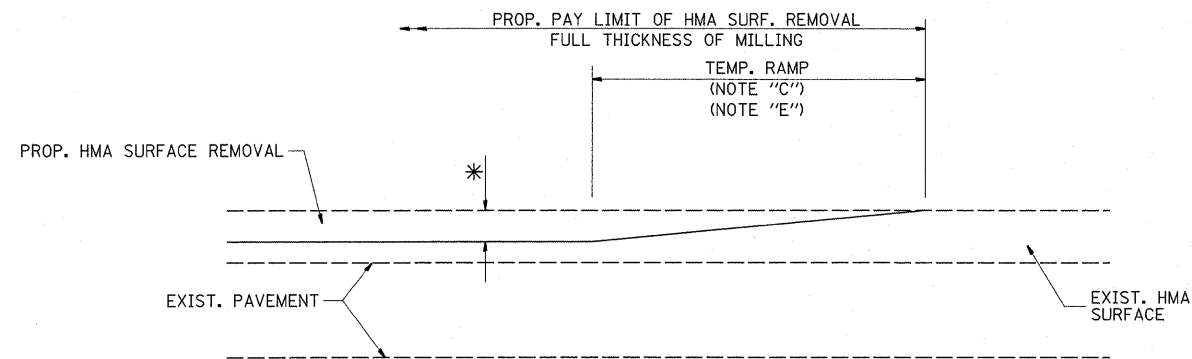
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

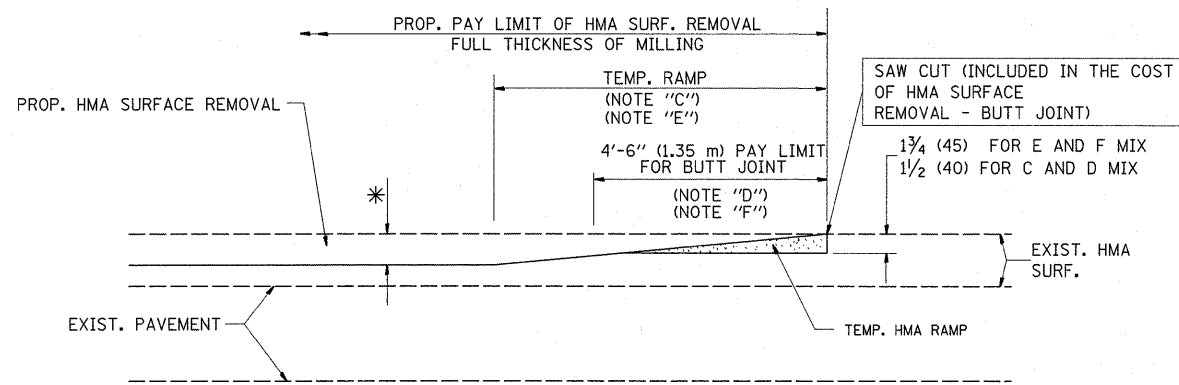
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = mdyj	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 31	
ci:\pwork\pwork\mdyj\48248692\DistS	Plotted	DRAWN -	REVISED - A. ABBAS 03-21-97			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD600-06 (BD-24)		CONTRACT NO. 60N60
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			
	PLOT DATE = 3/26/2011	DATE - 03-11-94	REVISED - R. BORO 12-15-09								



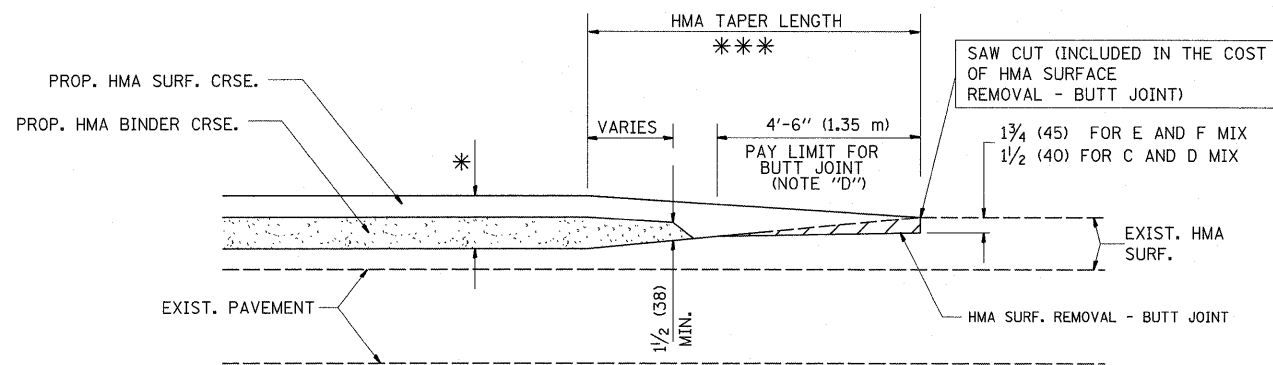
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1



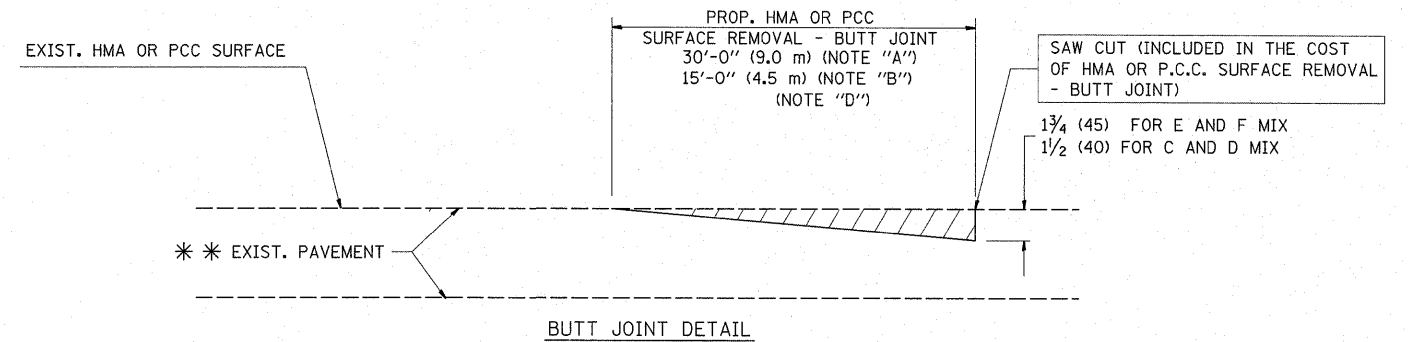
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP

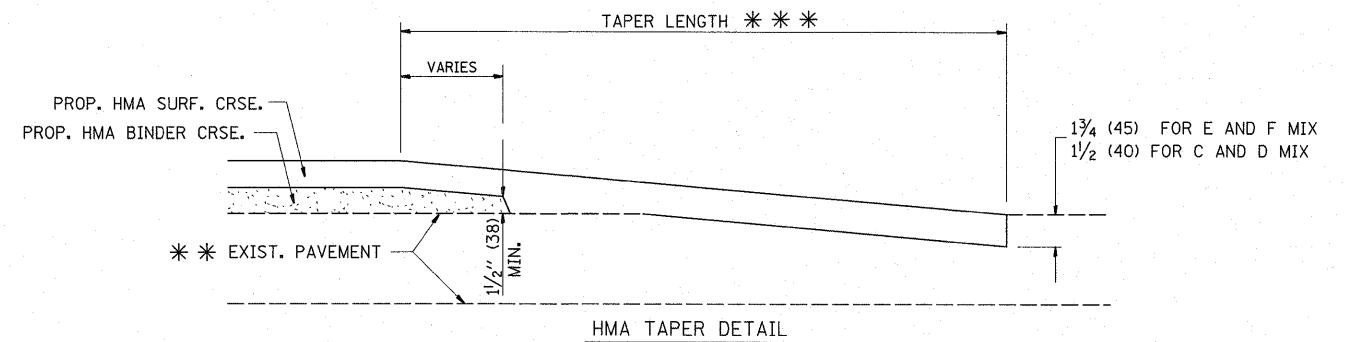


BUTT JOINT AND
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

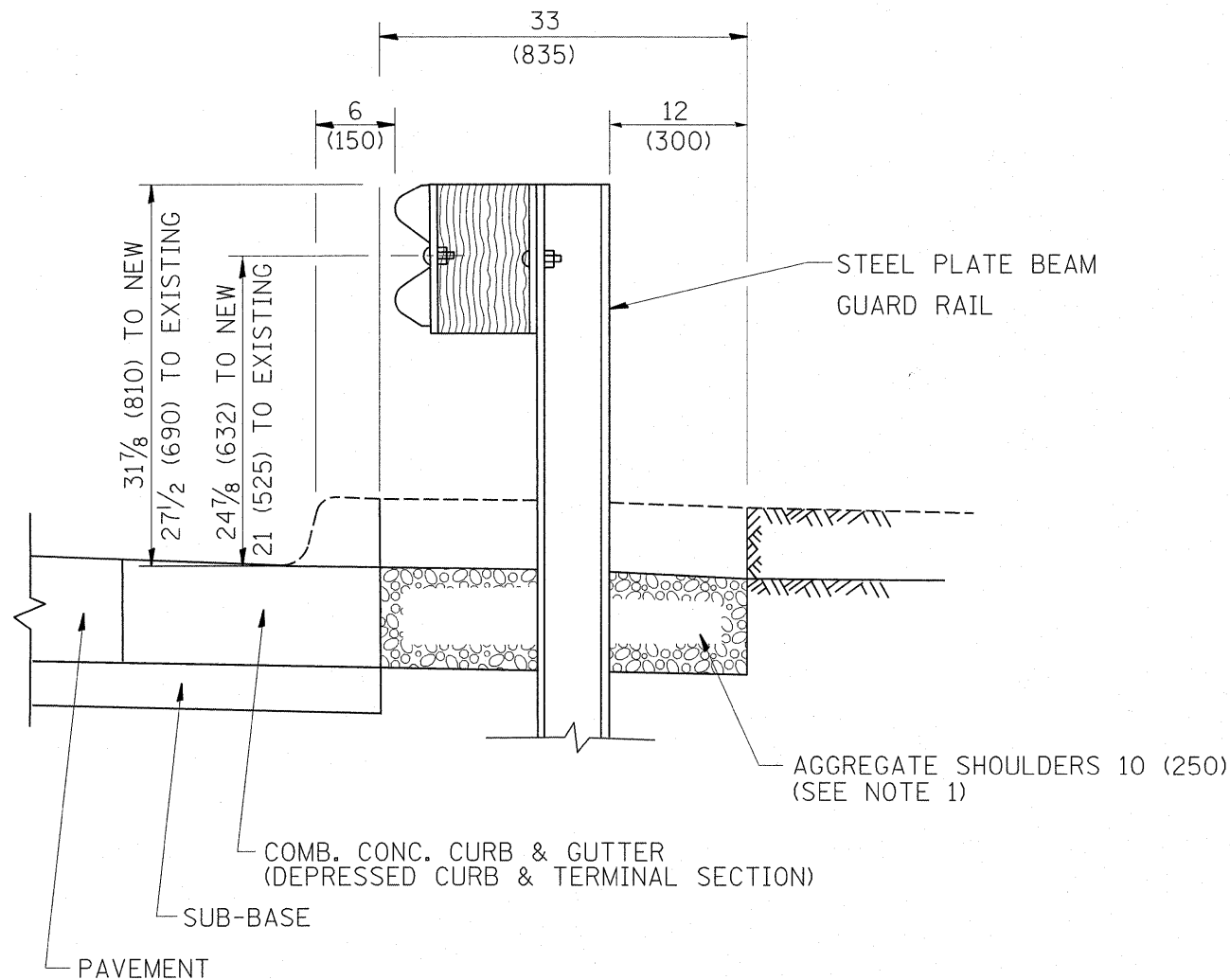
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	PLOT DATE = 3/26/2011	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

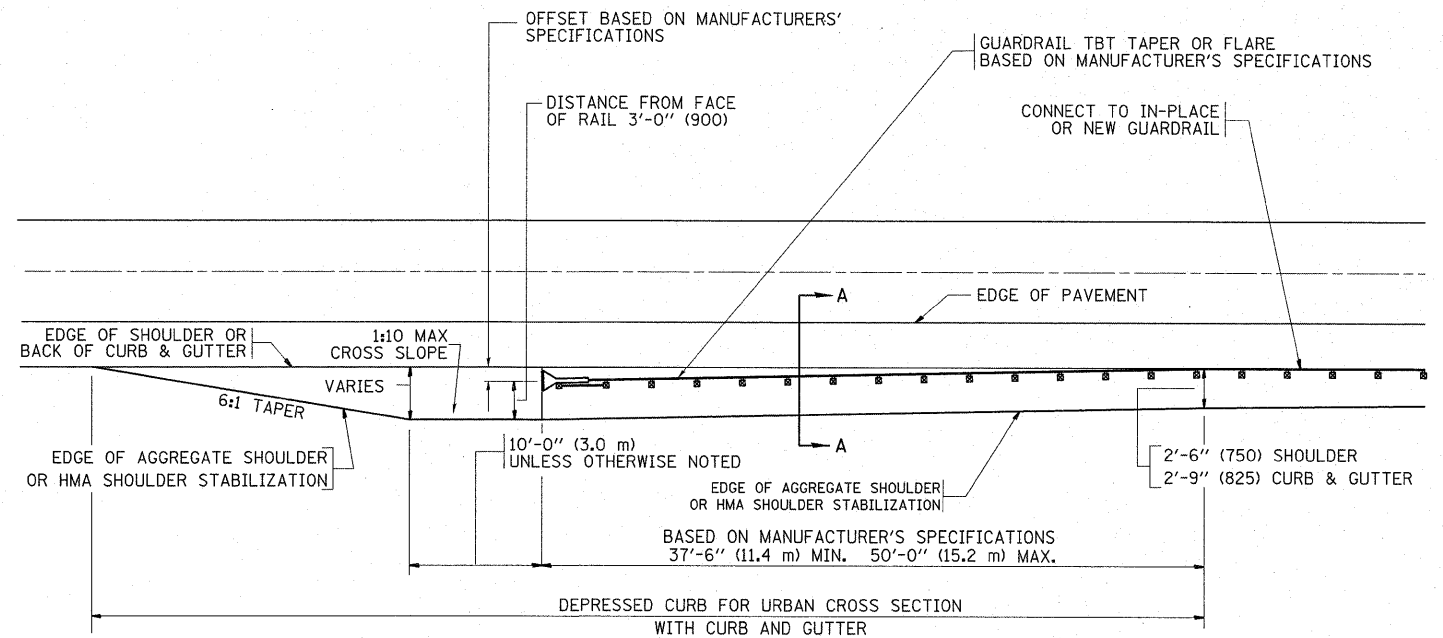
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	D-RS-7	WILL	38	32
BD400-05 BD32		CONTRACT NO. 60N60		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SECTION A-A

- NOTES: 1. THE AGGREGATE SHOULDER, 10" OR HMA SHOULDER, 6" (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

**DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER
[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]**



**DEPRESSED CURB AND GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

BASIS OF PAYMENT: HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDERS 6" (150 mm)".

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

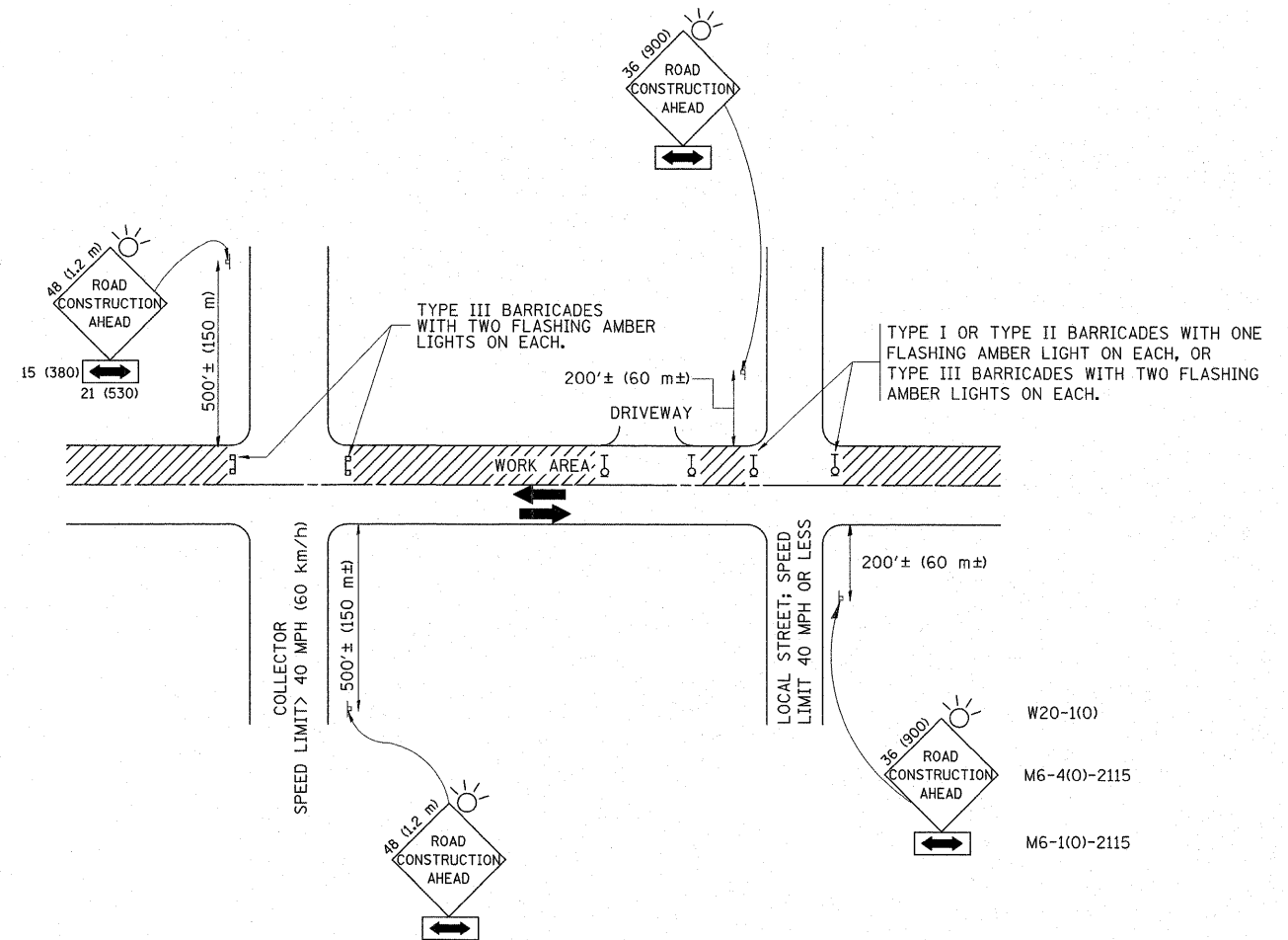
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	PLOT DATE = 3/28/2011	DATE - 09-22-90	REVISED - R. BORO 09-14-2009

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR DEPRESSED CURB & GUTTER AND
SHOULDER TREATMENT AT TBT TY 1 SPL.**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 32A
BD600-10 (BD 34)			CONTRACT NO. 60N60	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (Inches) unless otherwise shown.

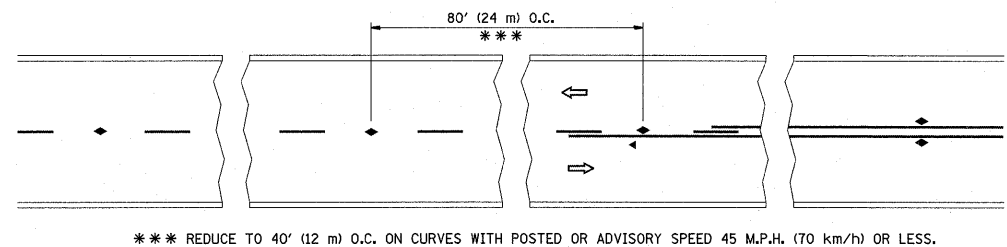
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

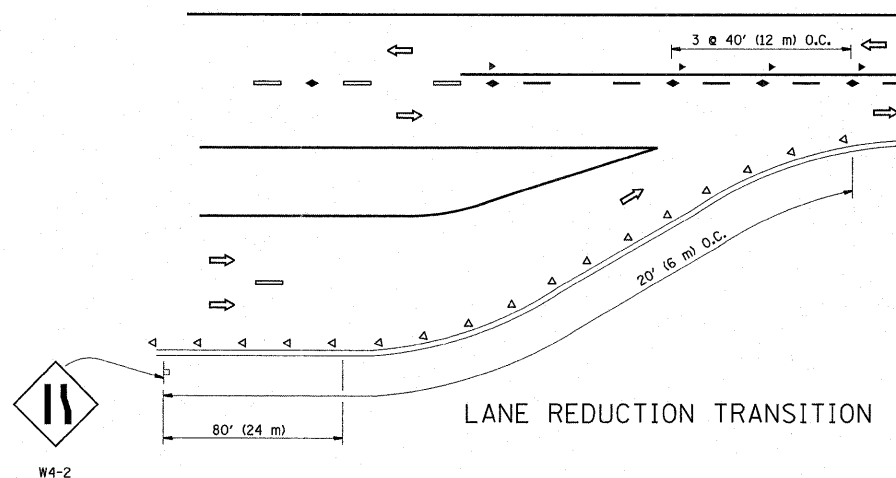
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 33
TC-10			CONTRACT NO. 60N60	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

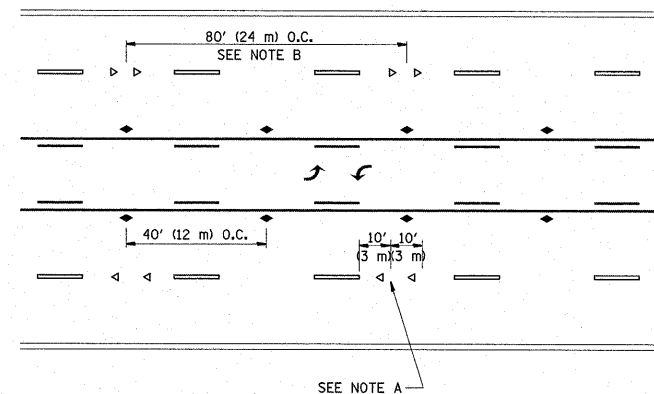
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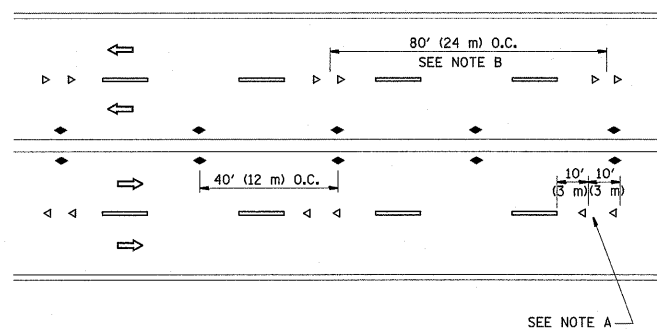
TWO-LANE/TWO-WAY



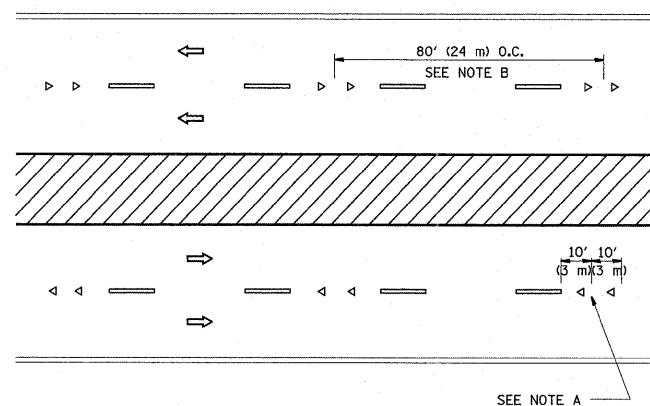
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

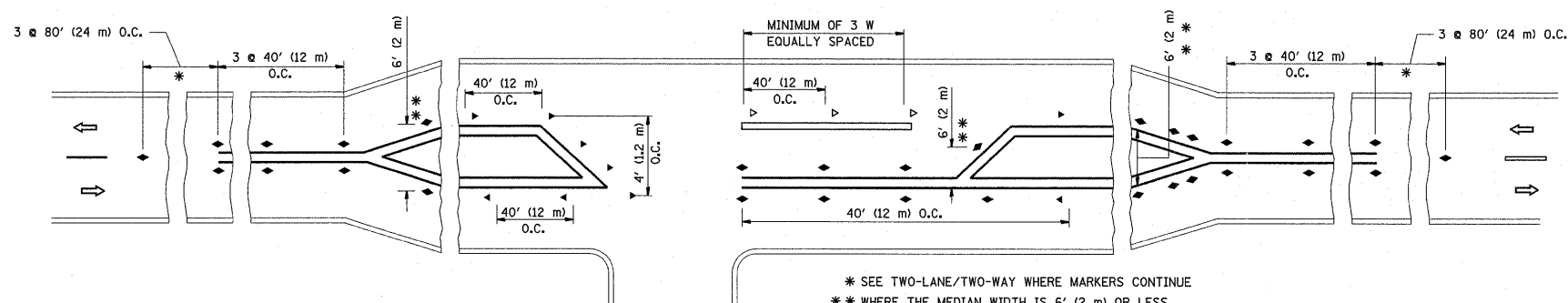
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

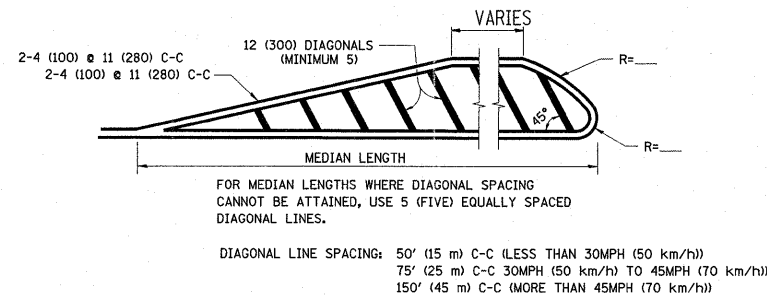
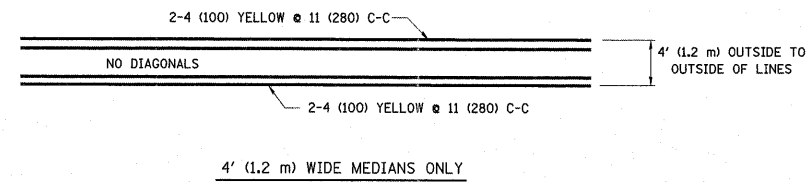
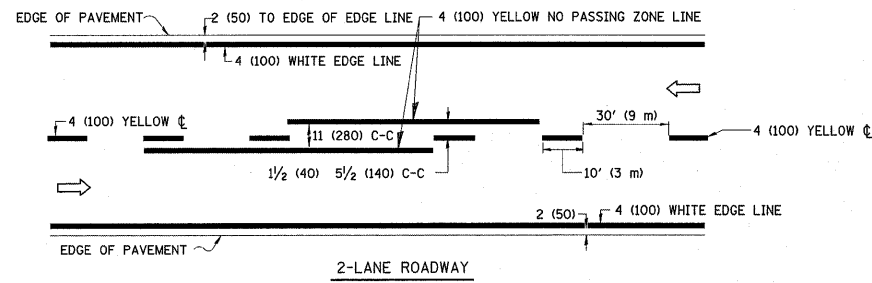
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

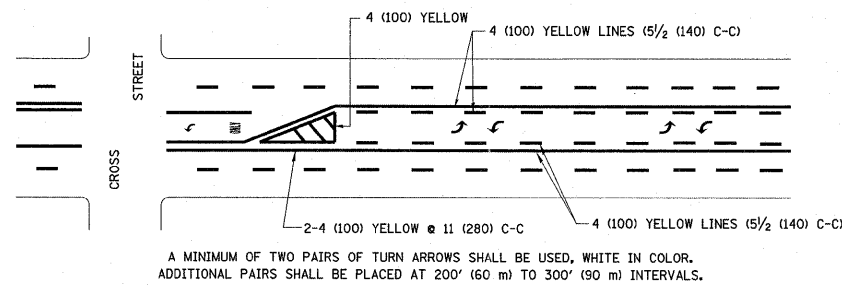
TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	D-RS-7	WILL	38	34
TC-11			CONTRACT NO. 60NGO	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

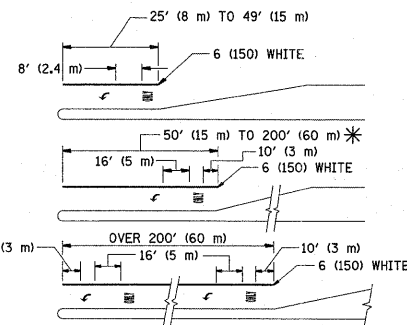


MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

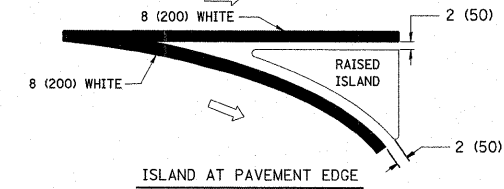
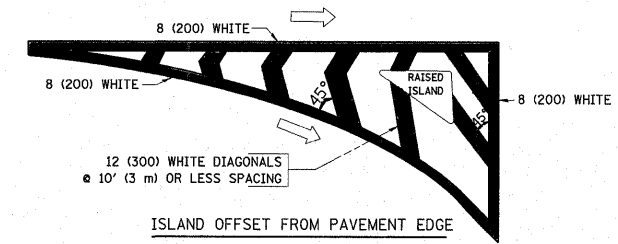


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

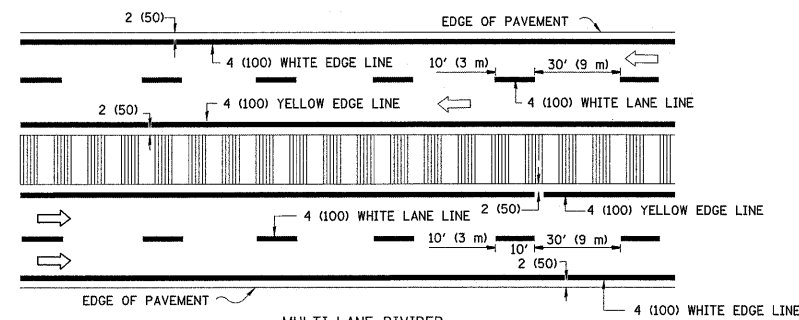
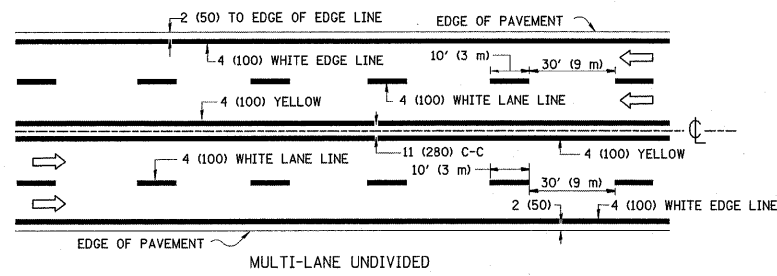


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

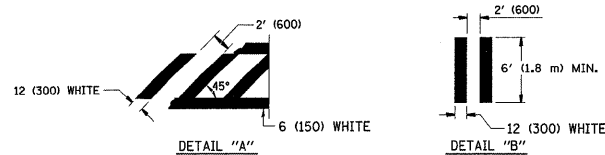
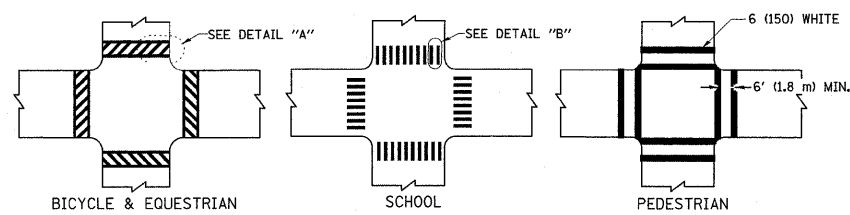
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

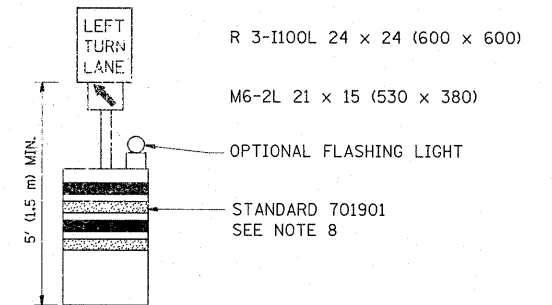
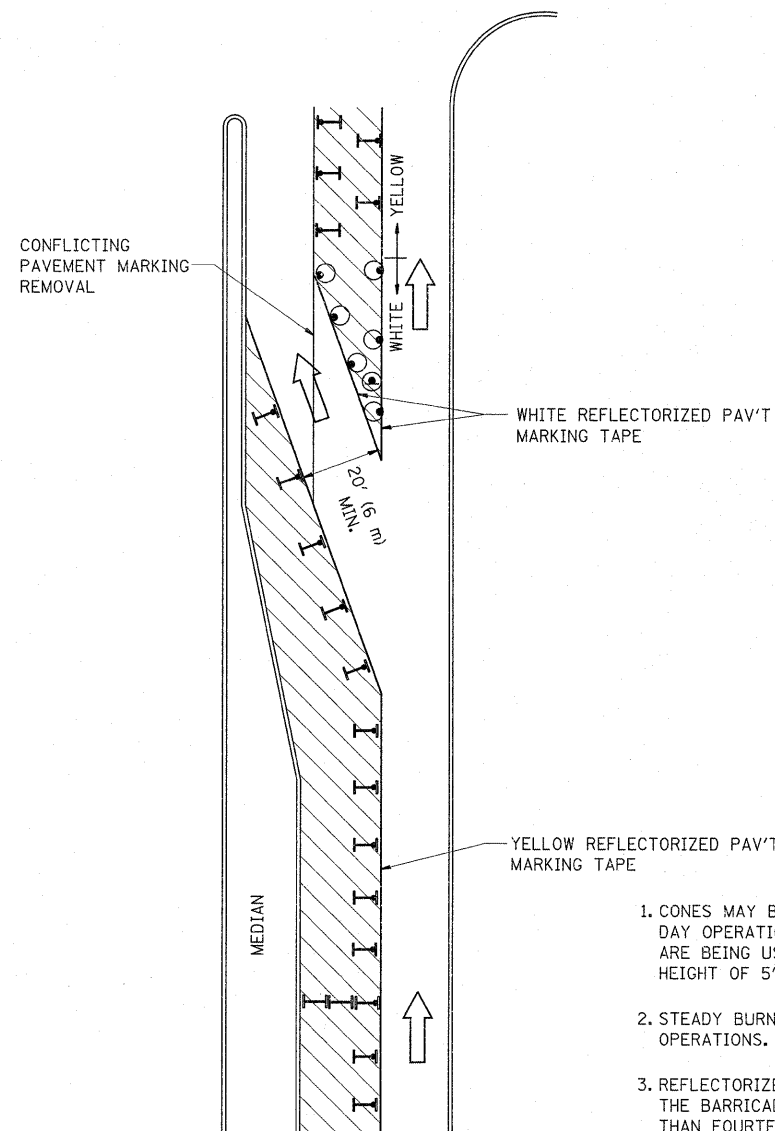
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PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = 3/26/2011		DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 35
TC-13			CONTRACT NO. 60N60	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

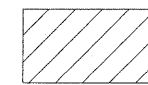







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

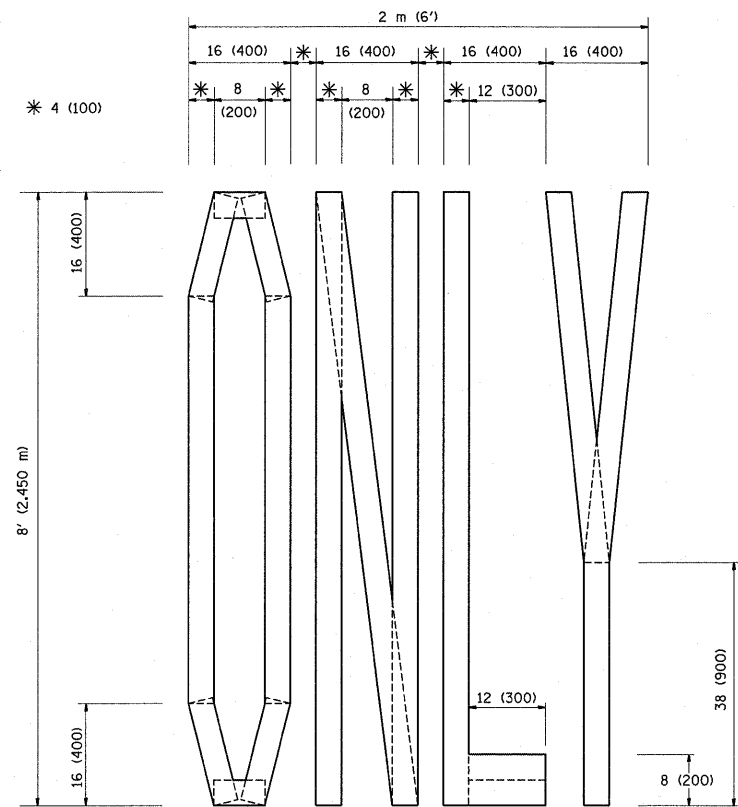
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	PLOT DATE = 5/11/2011	REVISED - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

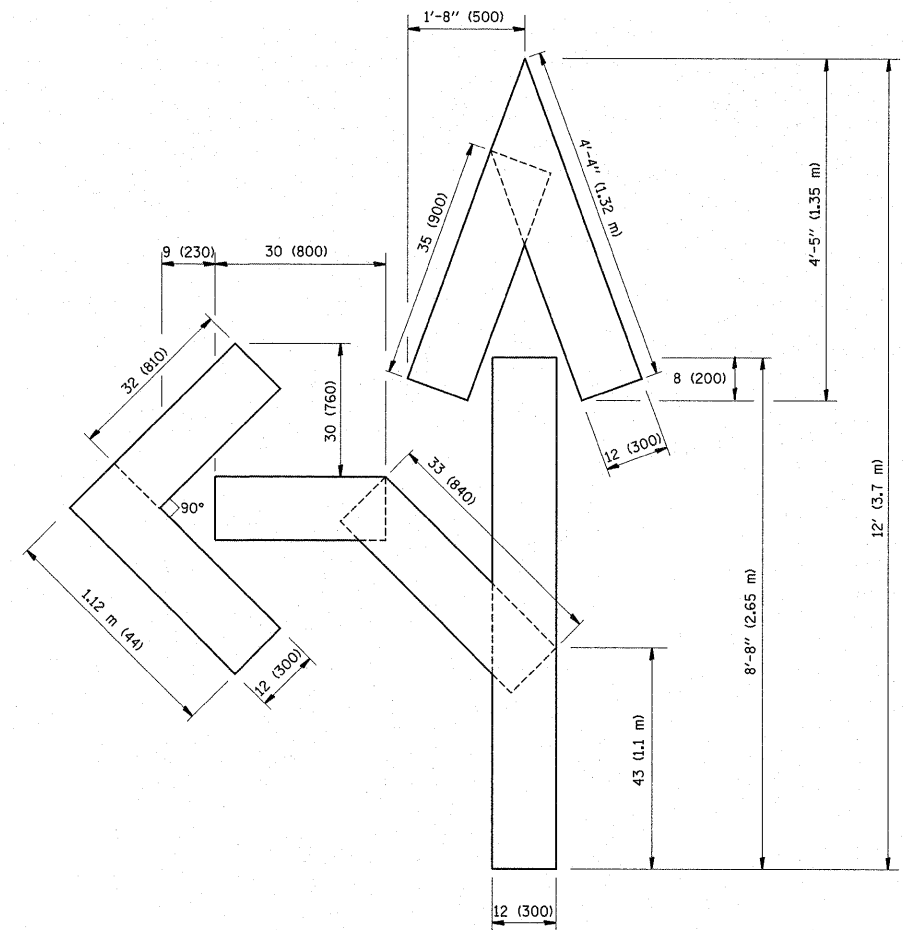
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

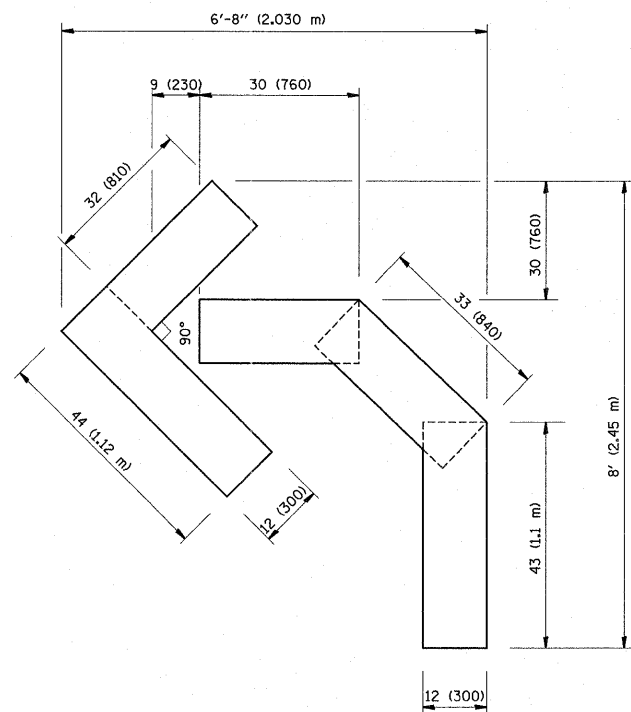
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TC-14			CONTRACT NO. 60N60	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in Inches (millimeters) unless otherwise shown.

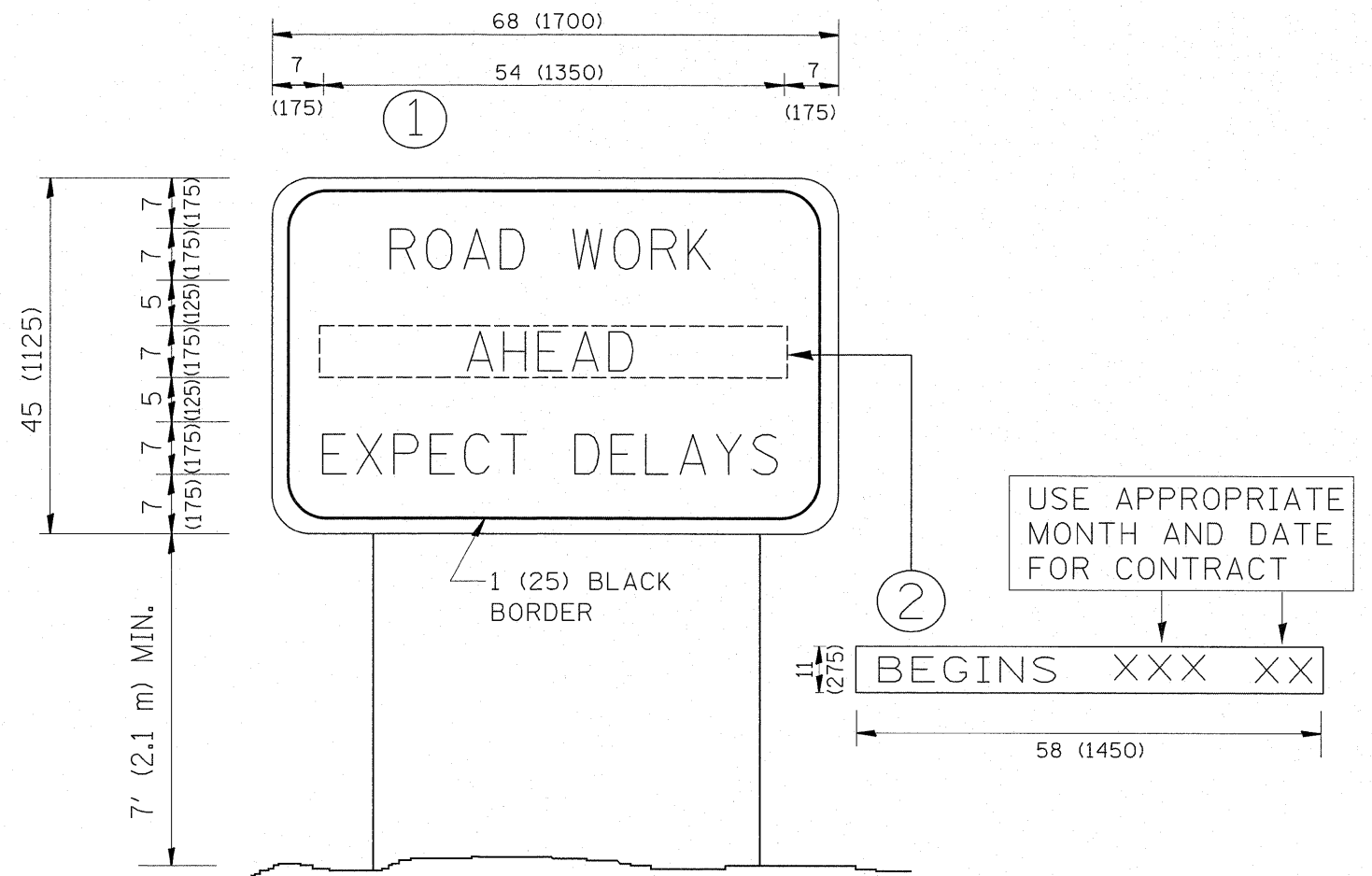
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PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -T. RAMMACHER 03-02-98
PLOT DATE = 3/26/2011		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 36
TC-16			CONTRACT NO. 60N60	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

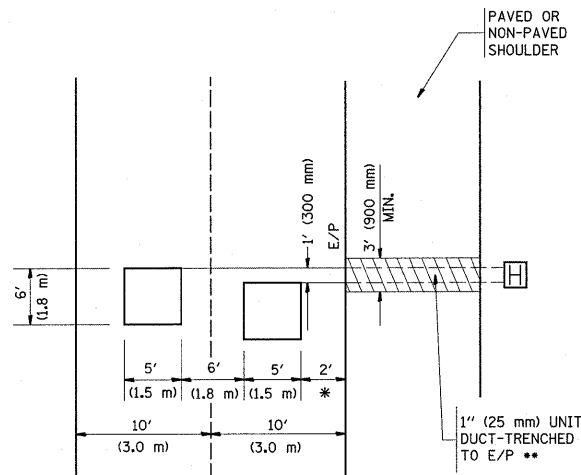
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = mdyjo	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN			F.A.P. RTE. 577	SECTION D-RS-7	COUNTY WILL	TOTAL SHEETS 38	SHEET NO. 37
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		CHECKED -	REVISED - T. RAMMACHER 02-02-99		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE -	REVISED - C. JUCIUS 01-31-07									

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.



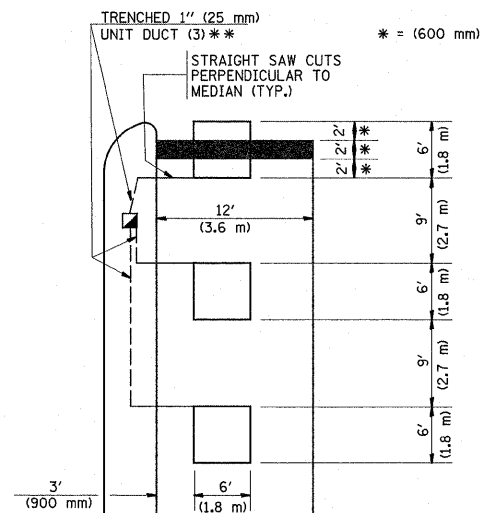
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.

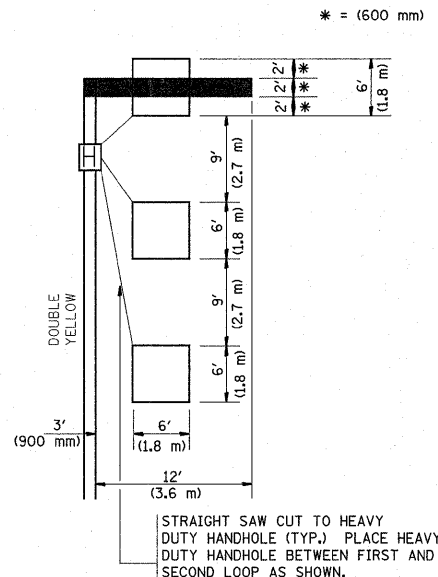


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

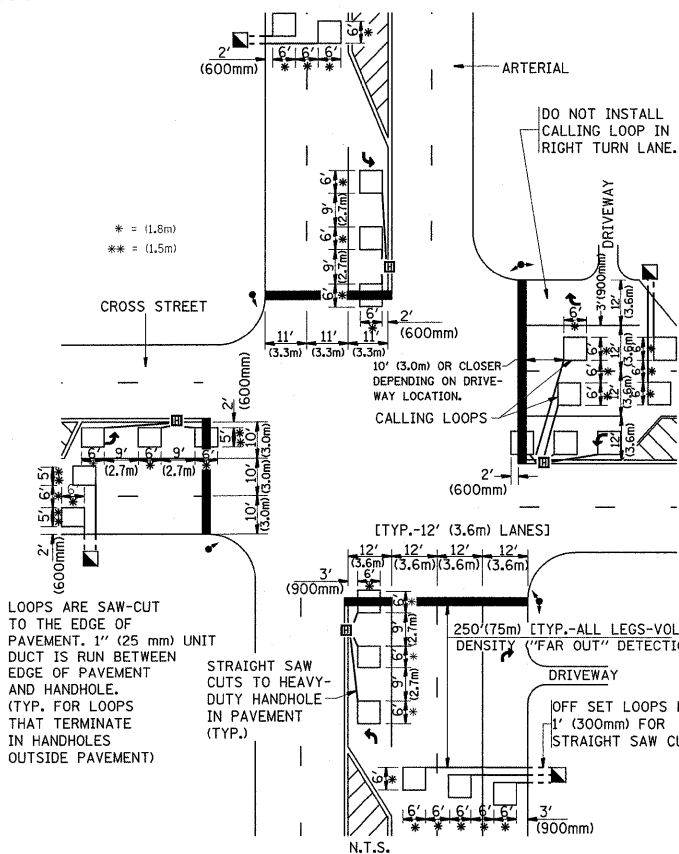
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



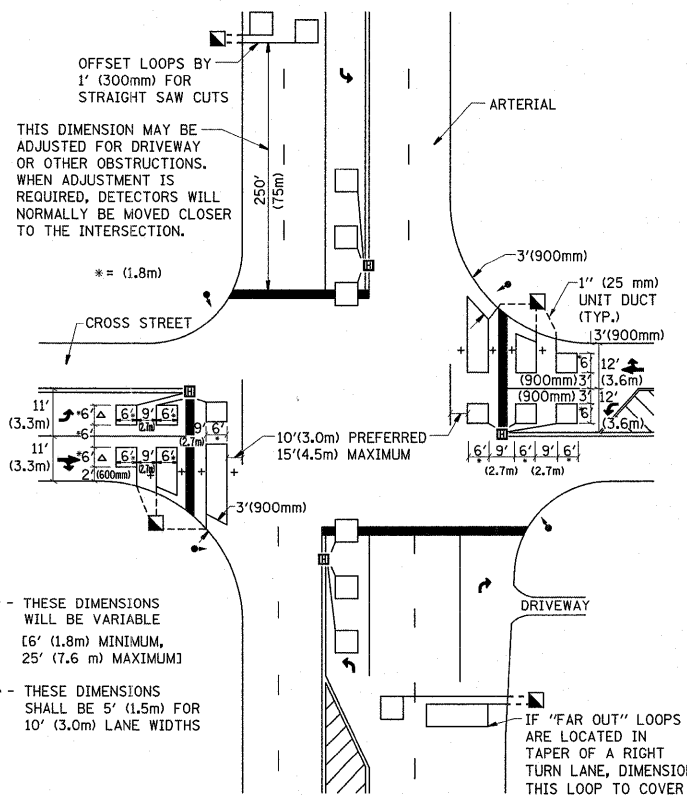
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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	PLOT SCALE = 50.0000' / IN.	CHECKED - R.K.F.	REVISED -		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
	PLOT DATE = 3/26/2011	DATE -	REVISED -		CONTRACT NO. 60NG6							
					TS-07							