

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1541	2016-079RS	COOK	20	1
ILLINOIS			CONTRACT NO. 62D61	

\* 20 + 9 = 29 TOTAL PAGES

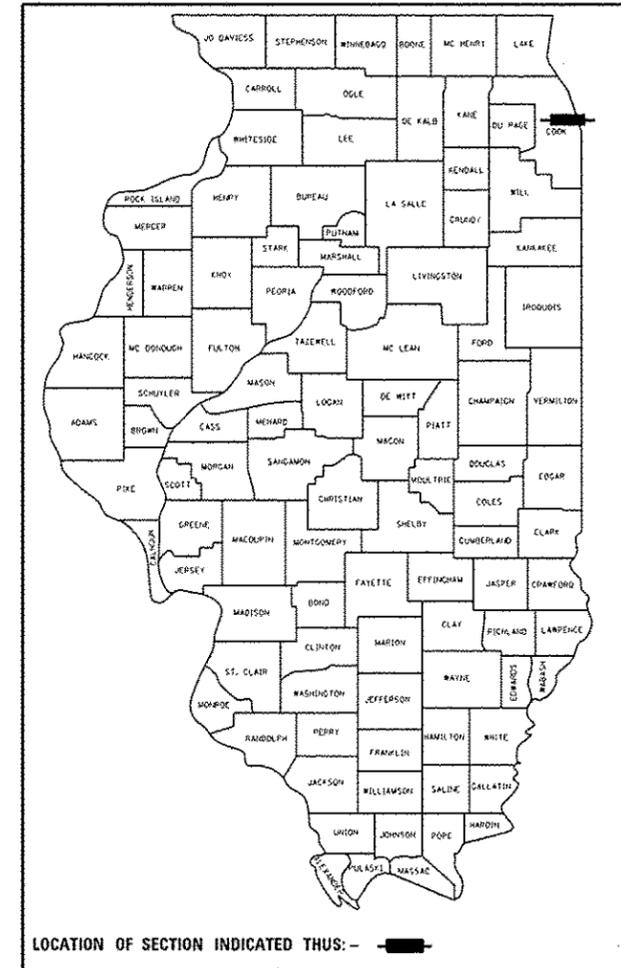
FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE CITY OF CHICAGO

2014 ADT: 9,500  
POSTED SPEED: 30 MPH

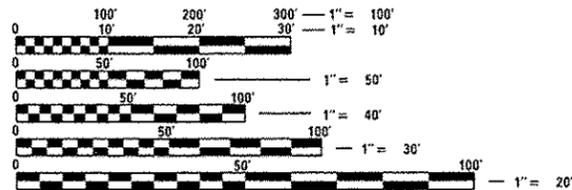
**PROPOSED  
HIGHWAY PLANS**  
F.A.U. ROUTE 1541 (76th STREET)  
ST. LAWRENCE AVE. TO DREXEL AVE.  
SECTION 2016-079RS  
PROJECT STP-1541(002)  
RESURFACING (3P), PEDESTRIAN RAMPS (ADA)  
COOK COUNTY

C-91-070-17



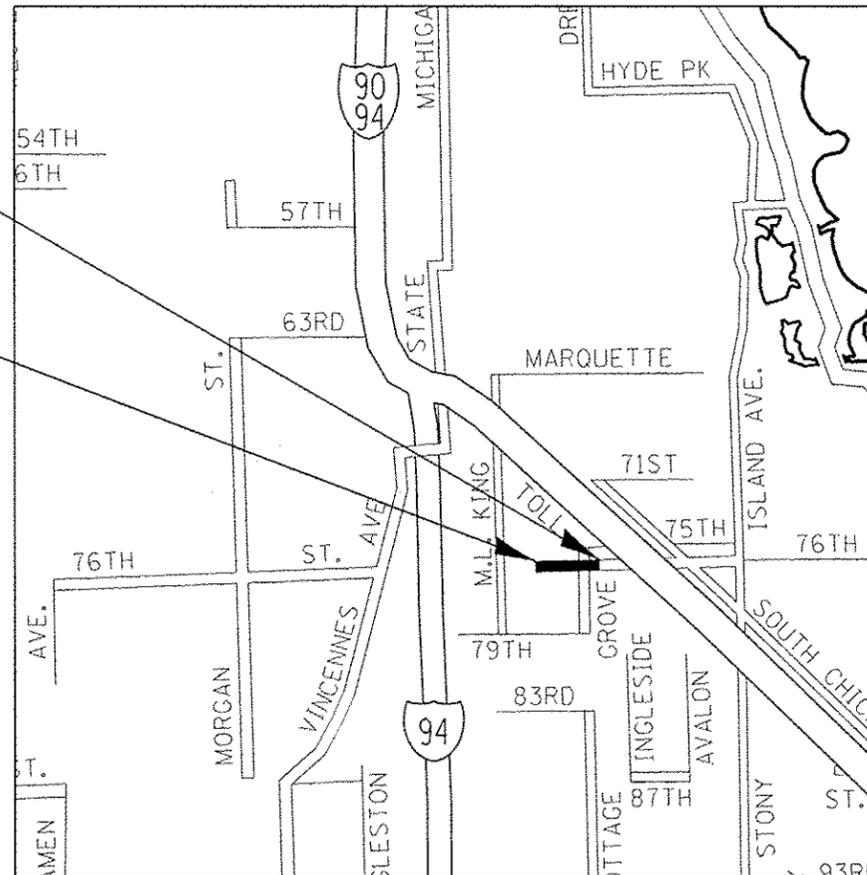
LIMITS OF IMPROVEMENT  
ENDS AT STA. 21+80.96

LIMITS OF IMPROVEMENT  
BEGINS AT STA. 2+42.29



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811



GROSS LENGTH = 1,938.67 FT. = 0.367 MILE  
NET LENGTH = 1,938.67 FT. = 0.367 MILE

PROJECT ENGINEER DAN WILGREEN (847) 705-4240  
PROJECT MANAGER FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62D61

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED March 17 20 17

[Signature]  
REGIONAL ENGINEER

May 12 20 17  
Maureen M. Addis, PE  
ENGINEER OF DESIGN AND ENVIRONMENT

May 12 20 17  
[Signature]  
DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

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  - HIGHWAY STANDARDS

## STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-09	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424016-03	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-03	DEPRESSED CORNER FOR SIDEWALKS
424026-01	ENTERANCE / ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
604001-04	FRAME AND LIDS TYPE 1
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS- DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-06	SIDEWALK, CORNER, OR CROSSWALK CLOSURE
701901-06	TRAFFIC CONTROL DEVICES

## CITY OF CHICAGO NOTES

- ① BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "C.U.A.N." (CHICAGO UTILITY ALERT NETWORK) AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION REQUIRED).
- ② THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO.
- ③ ALL CATCH BASINS IN THE CITY OF CHICAGO MUST MEET THE DEPARTMENT OF SEWERS' STANDARDS.
- ④ PERFORATED LIDS SHALL BE PLACED ON ALL MANHOLES AND CATCH BASINS.
- ⑤ BENCH MONUMENT LOCATIONS WITHIN THE LIMITS OF THE IMPROVEMENT CAN BE OBTAINED IN SUITE 410 - 333 SOUTH STATE STREET, CHICAGO, IL 60604. THE CONTRACTOR IS RESPONSIBLE FOR THE COST OF REPLACEMENT OF ANY BENCH MONUMENT IF DAMAGED OR DESTROYED DURING CONSTRUCTION.
- ⑥ ALL BROKEN, CRACKED, WORN OR OTHERWISE DAMAGED OR BICYCLE UNSAFE FRAMES AND LIDS ON SEWER STRUCTURES, SHALL BE REPLACED WITH NEW DEPARTMENT OF SEWERS' STANDARD FRAMES AND LIDS.
- ⑦ OPEN LID DRAINAGE STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION OF THIS ROADWAY WITHOUT THE WRITTEN PERMISSION FROM THE CITY OF CHICAGO.
- ⑧ CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY OF CHICAGO AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- ⑨ PERMITS FROM THE DEPARTMENT OF SEWERS ARE REQUIRED FOR ALL UNDERGROUND STORM, SANITARY OR COMBINED SEWER SYSTEM CONSTRUCTION, AND FOR RESURFACING WORK INVOLVING ADJUSTMENT OF SEWER STRUCTURES. THE DEPARTMENT OF SEWERS' PERMIT MUST BE OBTAINED BY A LICENSED SEWER DRAIN LAYER PRIOR TO START OF CONSTRUCTION.
- ⑩ THE CONTRACTOR SHALL TAKE EXTRA CARE AND PRECAUTION WHEN REPAIRING THE CONCRETE CURB AND GUTTER AT LOCATIONS WHERE EXISTING TREES ARE PRESENT.

# GENERAL NOTES

- ① BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL DIGGER AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- ② THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. ANY DAMAGE TO THE UNDERGROUND FACILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE DEPARTMENT AT THE CONTRACTOR'S EXPENSE, INCLUDING TEMPORARY REPAIRS WHICH MAY BE REQUIRED TO KEEP THE FACILITY OPERATIONAL WHILE MATERIAL IS BEING OBTAINED TO MAKE PERMANENT REPAIRS.
- ③ THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE CITY OF CHICAGO.
- ④ THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- ⑤ THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- ⑥ THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- ⑦ THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
- ⑧ BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- ⑨ IN ADDITION TO FIELD AND AERIAL SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
- ⑩ THE LOCATIONS OF THOSE BURIED AND ABOVE GROUND UTILITIES SHOWN ARE APPROXIMATE, ARE SHOWN FOR CONTRACTOR INFORMATIONAL USE ONLY, AND ARE NOT TO BE REFERENCED FOR CONSTRUCTION PURPOSES. THE IMPLIED PRESENCE OR ABSENCE OF UTILITIES IS NOT TO BE CONSTRUED BY THE OWNER, ENGINEER, CONTRACTOR, OR SUBCONTRACTORS TO BE AN ACCURATE AND COMPLETE REPRESENTATION OF UTILITIES THAT MAY OR MAY NOT EXIST ON THE CONSTRUCTION SITE. BURIED AND ABOVE GROUND UTILITY LOCATIONS, IDENTIFICATION, AND MARKING ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. REROUTING, DISCONNECTION, PROTECTION, ETC. OF ANY UTILITIES MUST BE COORDINATED BETWEEN CONTRACTOR, UTILITY COMPANY, AND OWNER. SITE SAFETY, INCLUDING THE AVOIDANCE OF HAZARDS ASSOCIATED WITH BURIED AND ABOVE GROUND UTILITIES REMAIN THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
- ⑪ DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- ⑫ THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.
- ⑬ OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED IN THE CONTRACT SPECIFICATIONS.
- ⑭ WHERE SECTION OR SUB-SECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR, OR AGENT HAS WITNESSED THEIR LOCATION.
- ⑮ THE EXACT LOCATIONS AND SIZES OF PAVEMENT PATCHES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
- ⑯ WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 40 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH. WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H) OR A NOTCHED LONGITUDINAL WEDGE IS USED.
- ⑰ ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- ⑱ LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- ⑲ DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- ⑳ EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED
- ㉑ FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAME AND LID UNLESS OTHERWISE SPECIFIED IN THE PLANS.
- ㉒ FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- ㉓ THE NOMINAL THICKNESS FOR HMA BASE AND SURFACE COURSES ARE SHOWN ON THE TYPICAL SECTIONS, STANDARDS, SCHEDULES, OR SPECIAL DETAILS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.
- ㉔ PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.

## COMMITMENTS

NO COMMITMENTS

FILE NAME :	USER NAME : bartonr	DESIGNED - RWB	REVISED - RWB 5/8/2017	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>76TH STREET FROM ST. LAWRENCE AVENUE TO DREXEL AVENUE INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\1084\EBID\INTEG\illinois\gov\PWID\1084\umanta\1007\Offices\District 1\Projects\1084\DRAWING\Design\1084\1017-ast-gennote.dwg	CHECKED -	REVISED -	1541			2016-079R5	COOK	20	2	
Default	PLT SCALE = 100.0000' / 1" =	REVISOR -	CONTRACT NO. 62D61							
	PLT DATE = 5/8/2017	DATE - 3/22/2017	REVISED -			SCALE:	SHEET 1 OF 1 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT	

Ran

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 ROADWAY 80% FEDERAL 20% STATE			
20200100	EARTH EXCAVATION	CU YD	36	36			
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	500	500			
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	7	7			
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	7	7			
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	7	7			
25200110	SODDING, SALT TOLERANT	SO YD	500	500			
25200200	SUPPLEMENTAL WATERING	UNIT	7.5	7.5			
28000400	PERIMETER EROSION BARRIER	FOOT	300	300			
28000510	INLET FILTERS	EACH	34	34			
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	6124	6124			
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	14	14			
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4, 75, N50	TON	383	383			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT BUTT JOINT	SO YD	209	209			
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	765	765			

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 ROADWAY 80% FEDERAL 20% STATE			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	4275	4275			
42400800	DETECTABLE WARNINGS	SO FT	375	375			
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	8864	8864			
44000600	SIDEWALK REMOVAL	SO FT	4611	4611			
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SO YD	317	317			
44201794	CLASS D PATCHES, TYPE III, 12 INCH	SO YD	75	75			
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SO YD	250	250			
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1			
60258100	MANHOLES TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	3	3			
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	10	10			
66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	36	36			
66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1			
66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MD	6	6			
67100100	MOBILIZATION	LSUM	1	1			
	SPECIALTY ITEMS						

FILE NAME	USER NAME = borlanw	DESIGNED - RWB	REVISED -
pn\NLD\BAE\BID\TEG\Itd\supp\WIDOT\Documents\DOC	OF\Plan\Q3\Itd\N\Pro\del\5\Q3\Q3\N\CAD\del\Design\Q3\Itd	REVISED - RWB	REVISED -
	PLOT SCALE = 1000000' = 1"	CHECKED -	REVISED -
	PLOT DATE = 3/23/2017	DATE = 3/22/2017	REVISED -

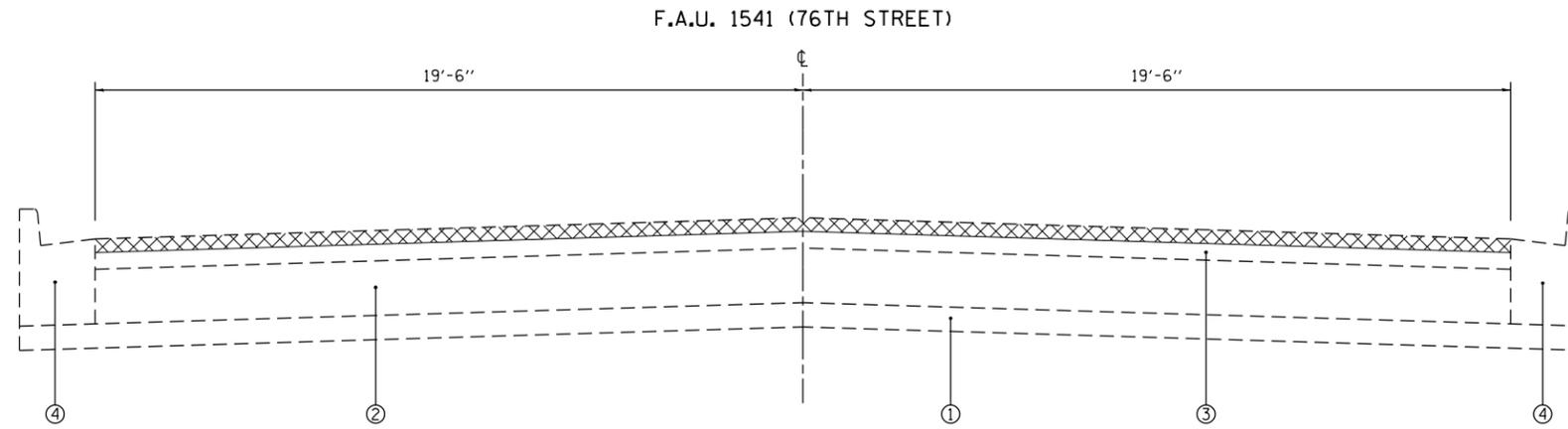
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

76TH STREET FROM ST. LAWRENCE AVENUE TO DREXEL AVENUE  
SUMMARY OF QUANTITIES

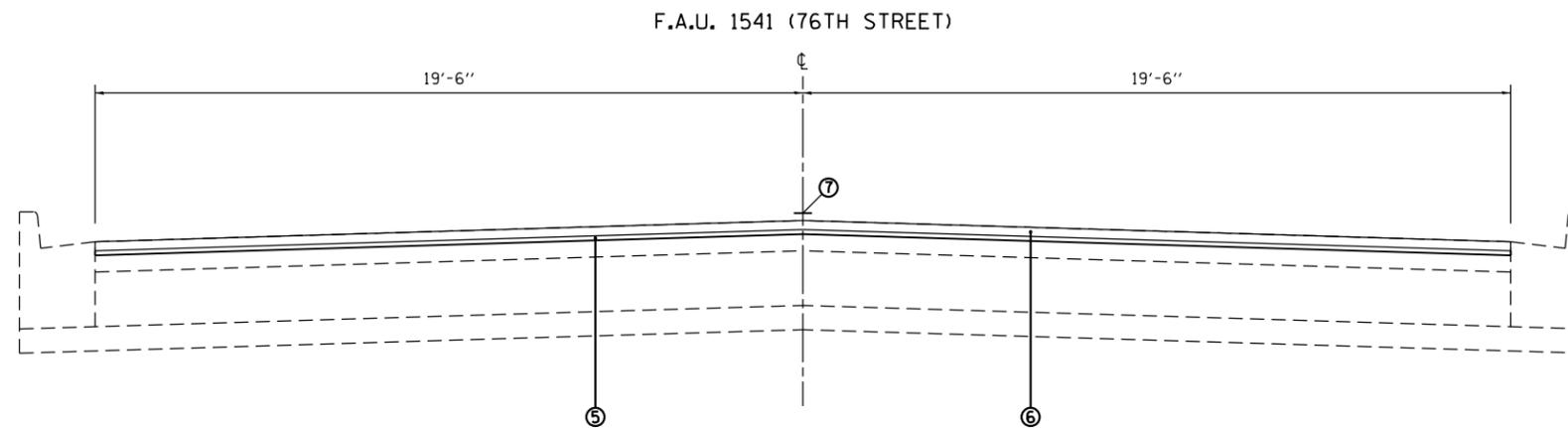
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1541	2016-079RS	COOK	20	3
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 62D61	

SCALE: SHEET NO. 1 OF 2 SHEETS STA. TO STA.





(1) STA. 2+42.29 TO STA. 21+80.96 (1,938.67')  
(EXISTING)



(2) STA. 2+42.29 TO STA. 21+80.96 (1,938.67')  
(PROPOSED)

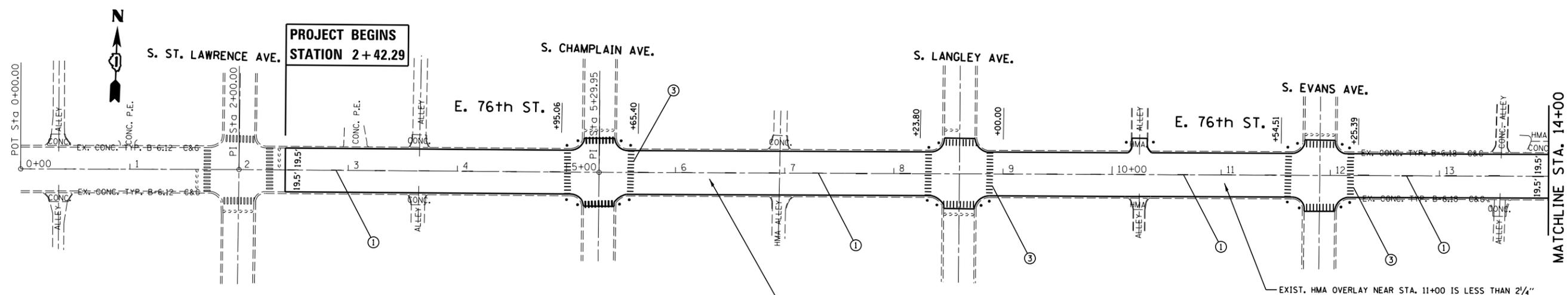
**LEGEND**

- ① EXIST. SUBBASE GRANULAR MATERIAL, TYPE A 4"
- ② EXIST. PCC BASE COURSE 9"
- ③ EXIST. 2 3/4" HMA OVERLAY
- ④ EXIST. COMBINATION CURB AND GUTTER TYPE B-6.18
- ⑤ PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑥ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING - LINE, 4"

PROP. HOT-MIX ASPHALT SURFACE REMOVAL 2 1/4"

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (OMP)
MIXTURE TYPE	AIR VOIDS @ Ndes	
RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm)	4% @ 70 GYR.	QC/OA
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% @ 50 GYR.	QC/OA
PATCHING		
CLASS D PATCHING (HMA BINDER, IL-19.0)	4% @ 70 GYR.	QC/OA
OMP DESIGNATION: QUALITY CONTROL/ QUALITY ASSURANCE (QC/OA)		

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
- FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS
- QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE
- THE SEQUENCE OF CONSTRUCTION SHALL BE PATCHING FIRST THEN MILLING



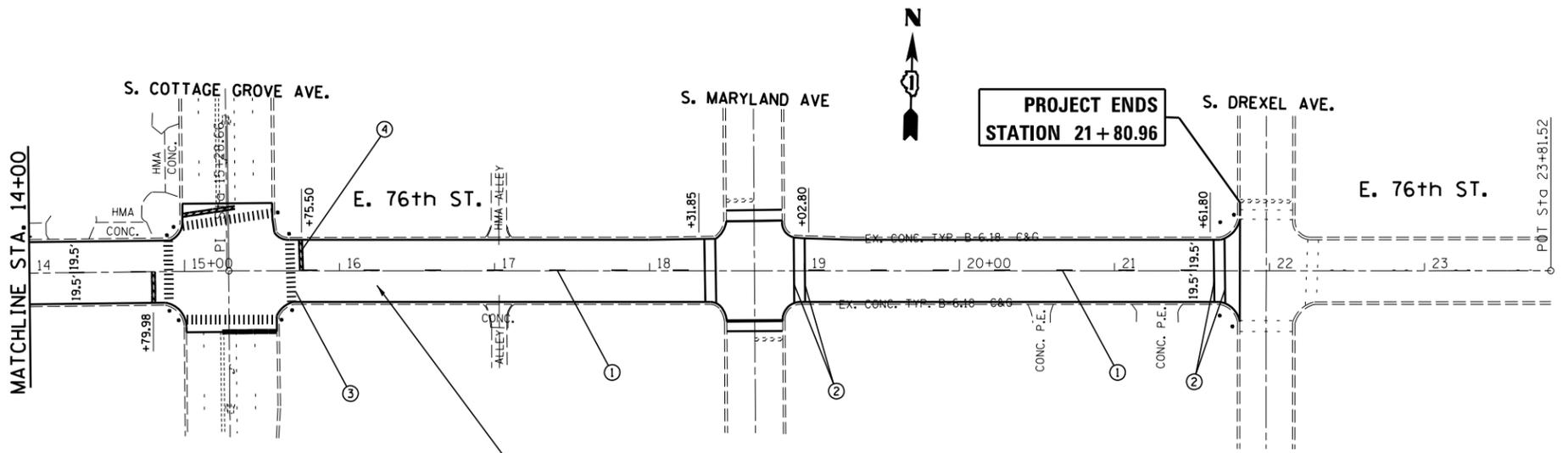
**LEGEND**

- ① PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4" SKIP DASH YELLOW LINE, 10' LINE 30' SKIP
- ② PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6" SOLID WHITE CROSSWALK, 6' C-C
- ③ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 24" SOLID WHITE CROSSWALK, 6' LENGTH, 4' C-C
- ④ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 24" SOLID WHITE STOPBAR

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"  
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

• ADA RAMP TO BE RECONSTRUCTED

EXIST. HMA OVERLAY NEAR STA. 11+00 IS LESS THAN 2 1/4"



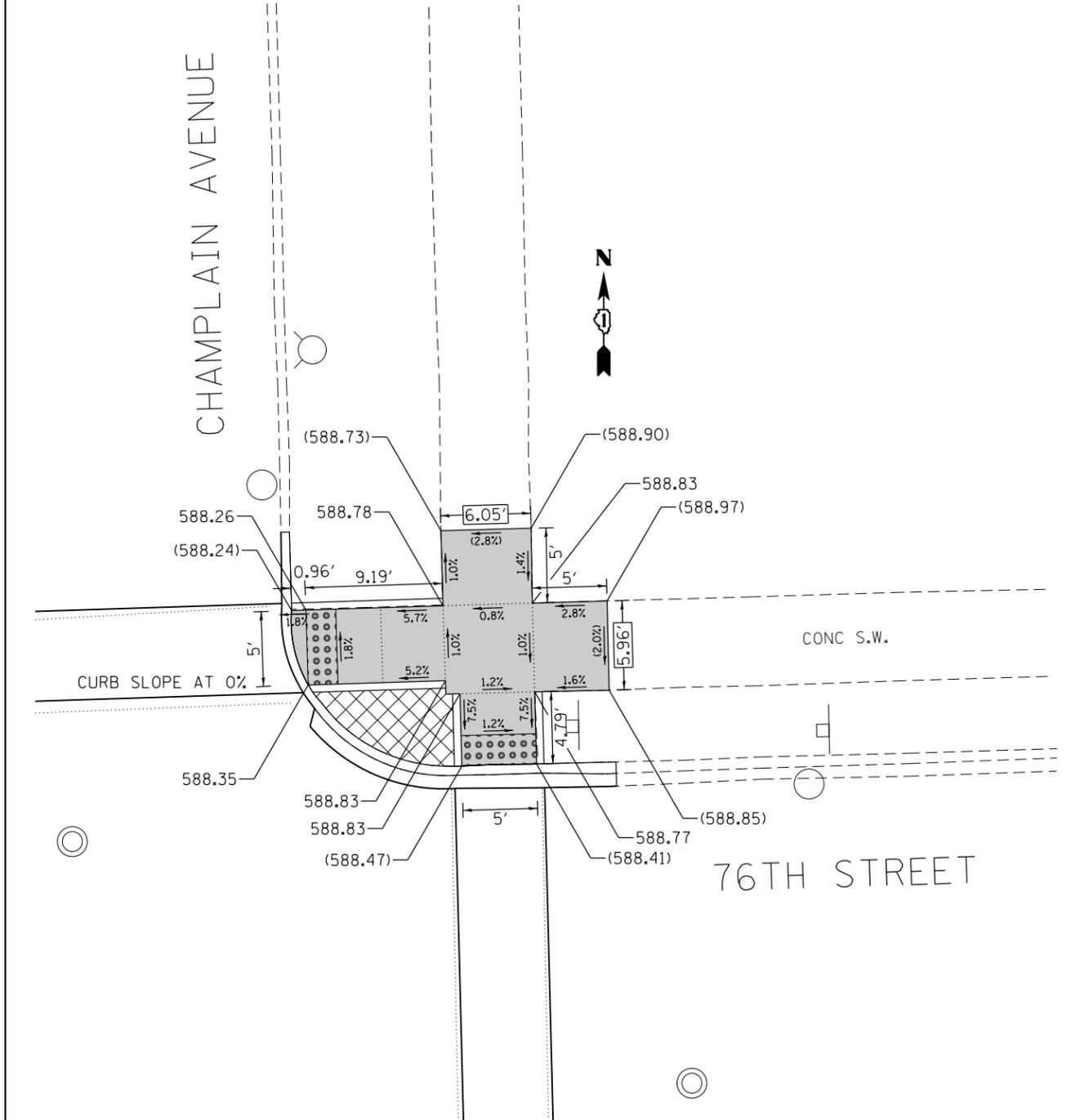
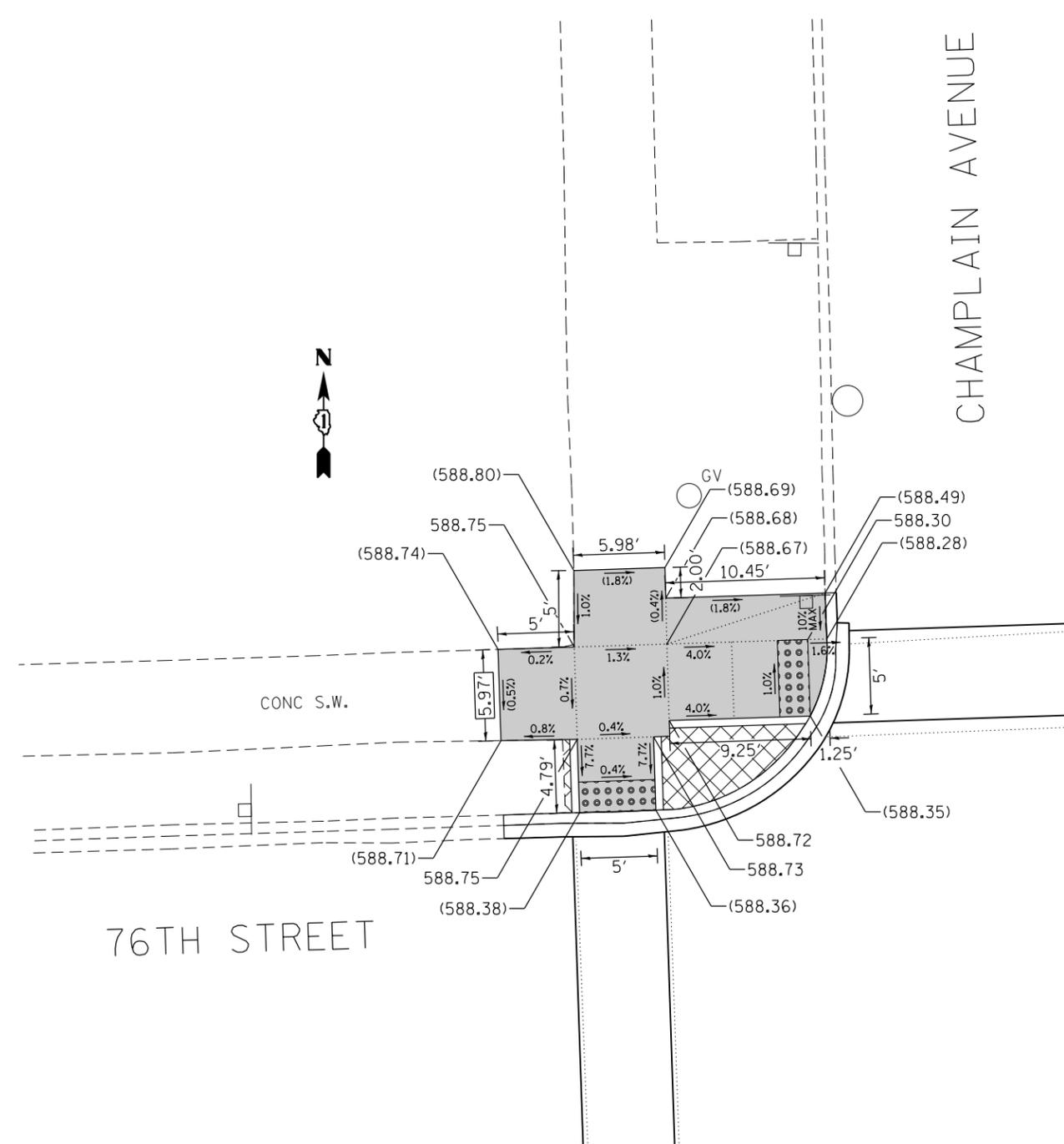
PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"  
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

• ADA RAMP TO BE RECONSTRUCTED

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>76TH STREET FROM ST. LAWRENCE AVENUE TO DREXEL AVENUE ROADWAY AND PAVEMENT MARKING PLAN</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBID\INTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01070\Drawings\Design\017-sht-plen.dgn	PLotted SCALE = 100.0000' / in.	CHECKED -	REVISED -			1541	2016-079RS	COOK	20	6	
Default	PLOT DATE = 3/27/2017	DATE - 3/27/2017	REVISED -			CONTRACT NO. 62D61					
						ILLINOIS FED. AID PROJECT					

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FILE NAME =	USER NAME = bartonw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>76TH STREET FROM ST. LAWRENCE AVENUE TO DREXEL AVENUE</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI07017\Drawings\Design\DI07017-sht-details.dgn		CHECKED -	REVISED -				1541	2016-079RS	COOK	20	7
Default	PLOT SCALE = 100.0000' / 1in.	DATE -	REVISED -				CONTRACT NO. 62D61		ILLINOIS FED. AID PROJECT		
	PLOT DATE = 3/23/2017	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.



REFERENCE BENCHMARK ELEV 590.09  
 BENCHMARK : CUT "X" ON CHAIN BOLT OF FIRE HYDRANT  
 LOCATION : ON NORTH EAST CORNER OF 76TH STREET AND CHAMPLAIN AVENUE

**LEGEND**

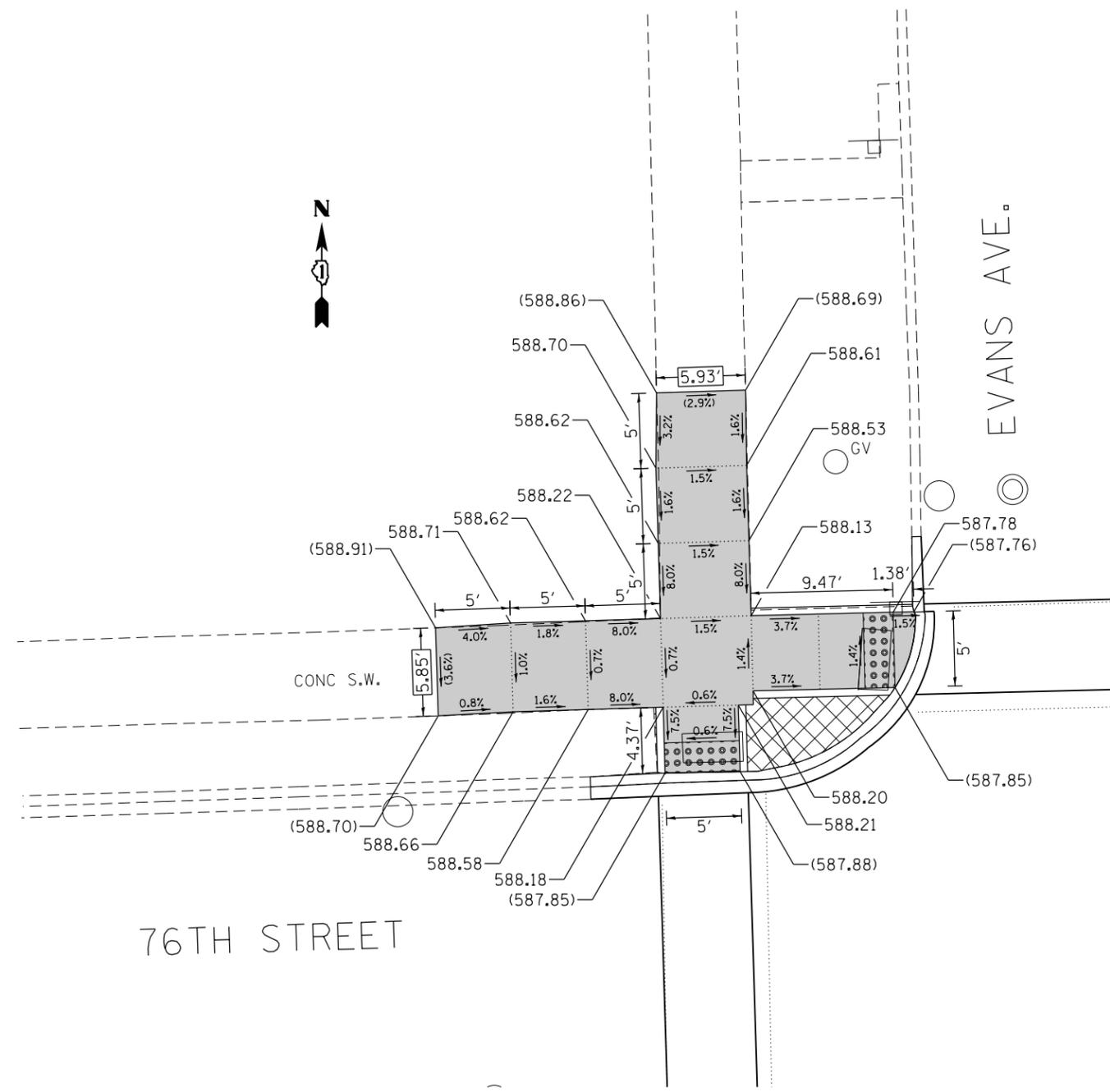
<span style="border: 1px solid black; padding: 2px;">xx.xx'</span>	EXISTING LENGTH		PROPOSED SIDEWALK
	PROPOSED SIDE CURB		DETECTABLE WARNINGS
( )	EXISTING ELEVATION/SLOPE		SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 590.09  
 BENCHMARK : CUT "X" ON CHAIN BOLT OF FIRE HYDRANT  
 LOCATION : ON NORTH EAST CORNER OF 76TH STREET AND CHAMPLAIN AVENUE



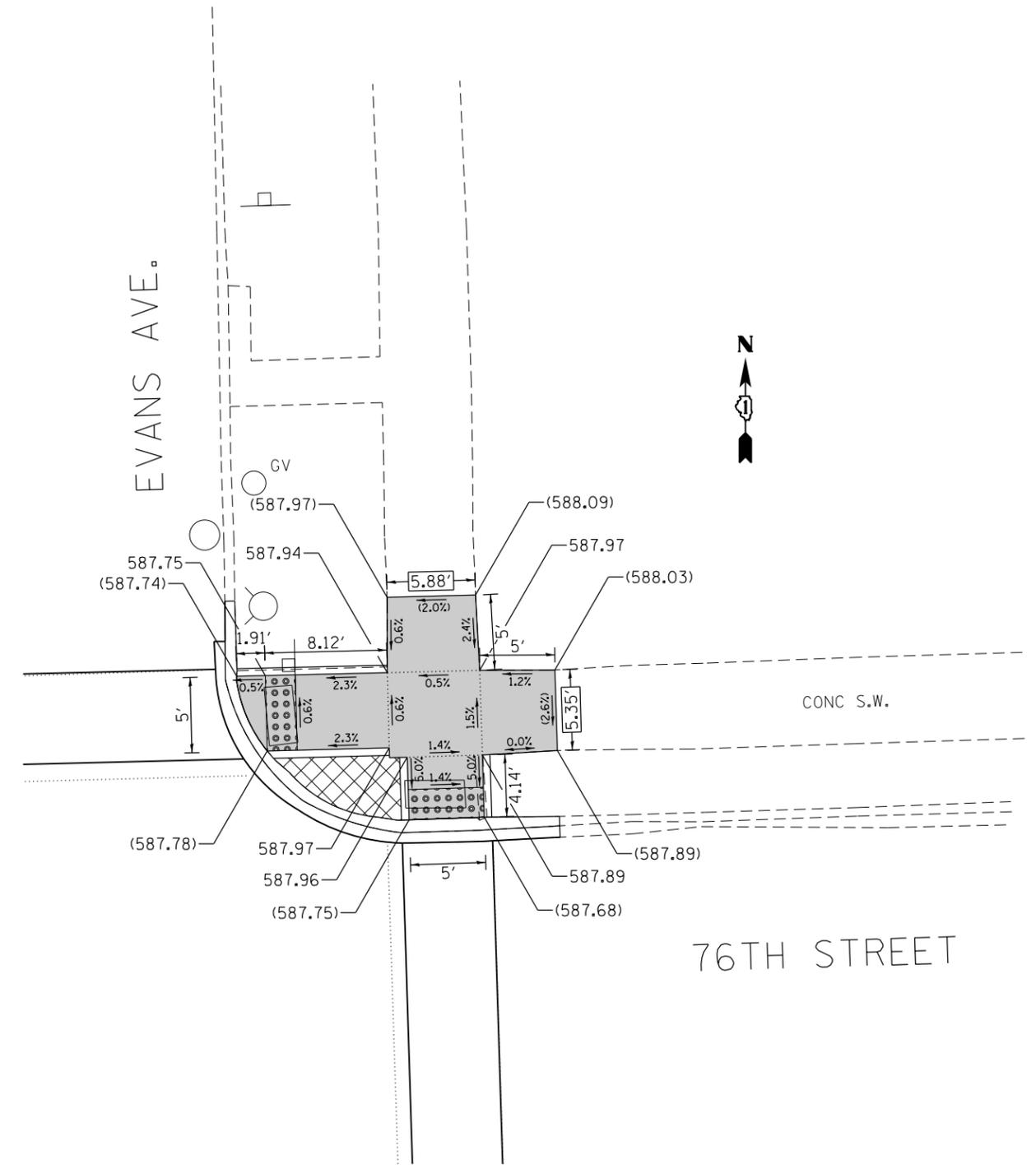






76TH STREET

EVANS AVE.



76TH STREET

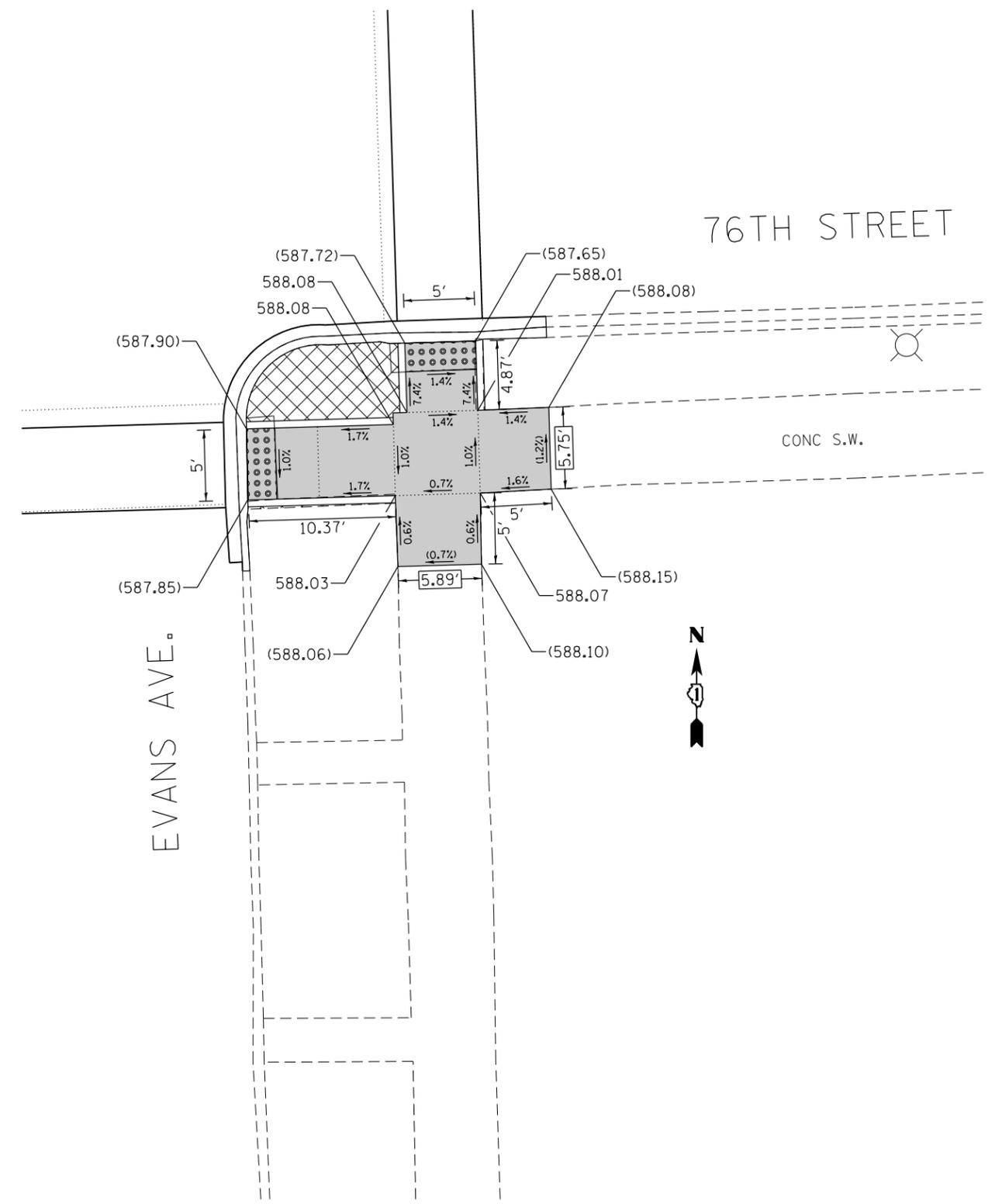
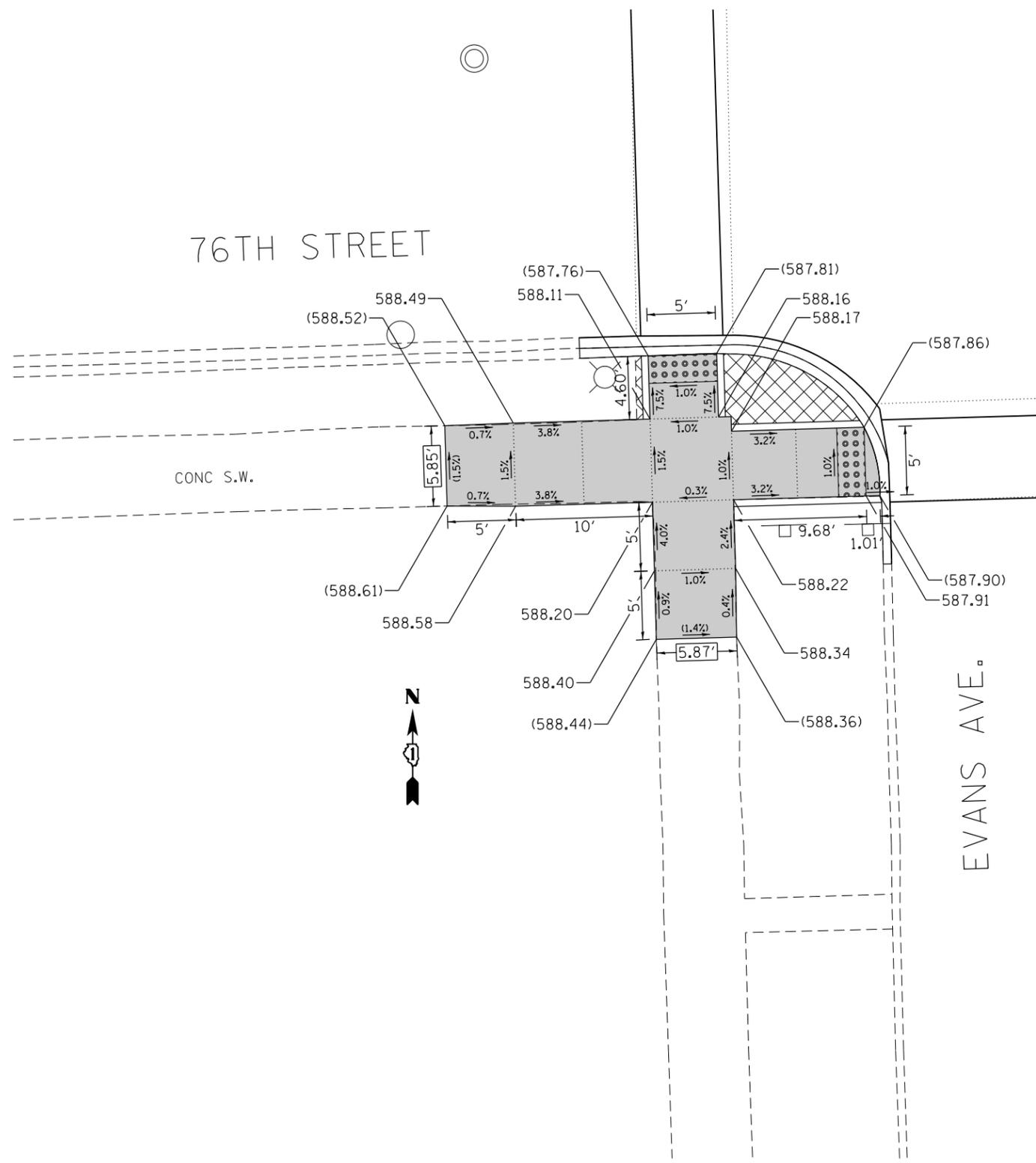
EVANS AVE.

REFERENCE BENCHMARK ELEV 589.45  
 BENCHMARK : CUT "X" ON CHAIN BOLT OF FIRE HYDRANT  
 LOCATION : ON NORTH EAST CORNER OF 76TH STREET AND EVANS AVENUE

**LEGEND**

<span style="border: 1px solid black; padding: 2px;">xx.xx'</span>	EXISTING LENGTH		PROPOSED SIDEWALK
	PROPOSED SIDE CURB		DETECTABLE WARNINGS
( )	EXISTING ELEVATION/SLOPE		SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 589.45  
 BENCHMARK : CUT "X" ON CHAIN BOLT OF FIRE HYDRANT  
 LOCATION : ON NORTH EAST CORNER OF 76TH STREET AND EVANS AVENUE

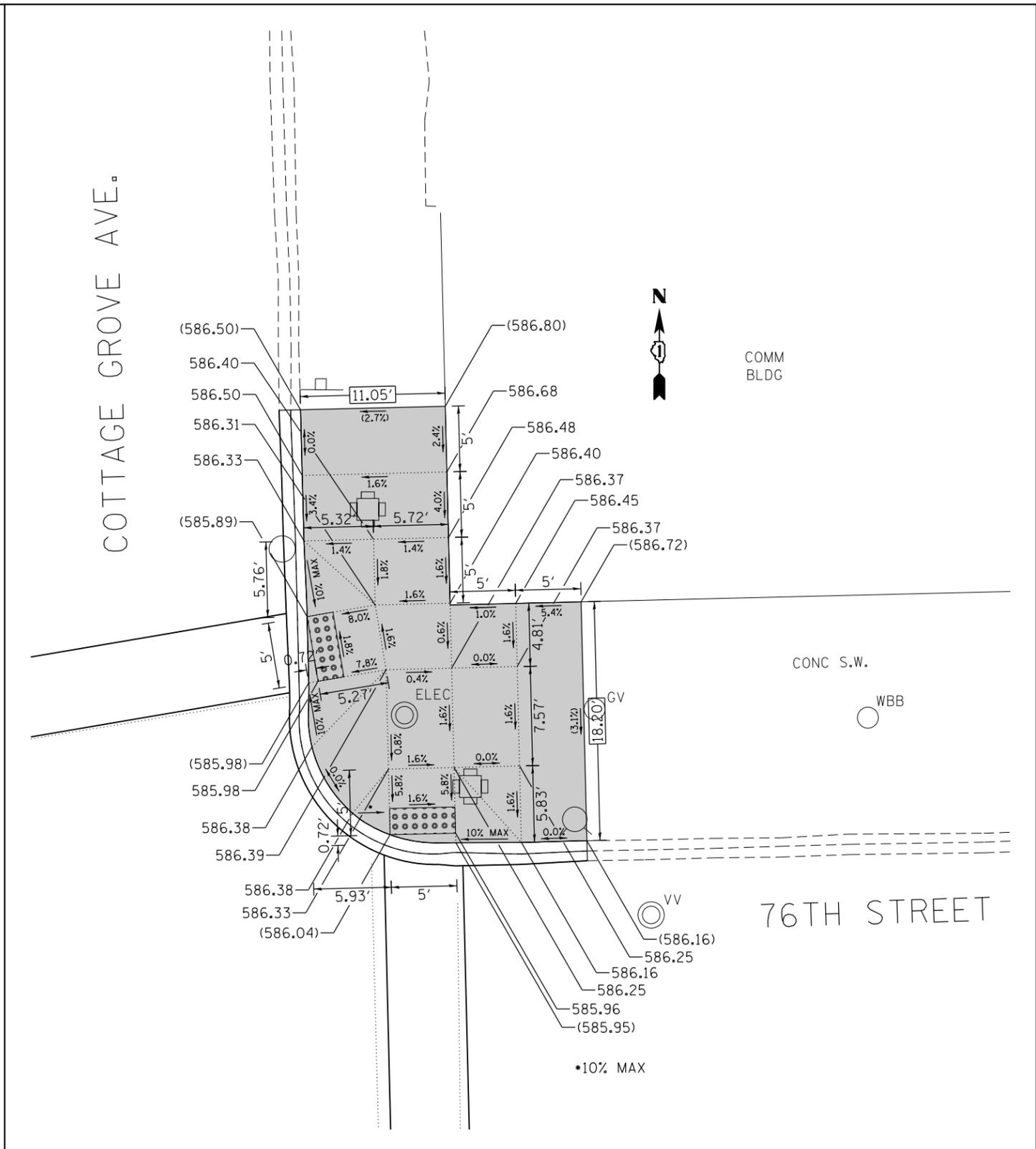
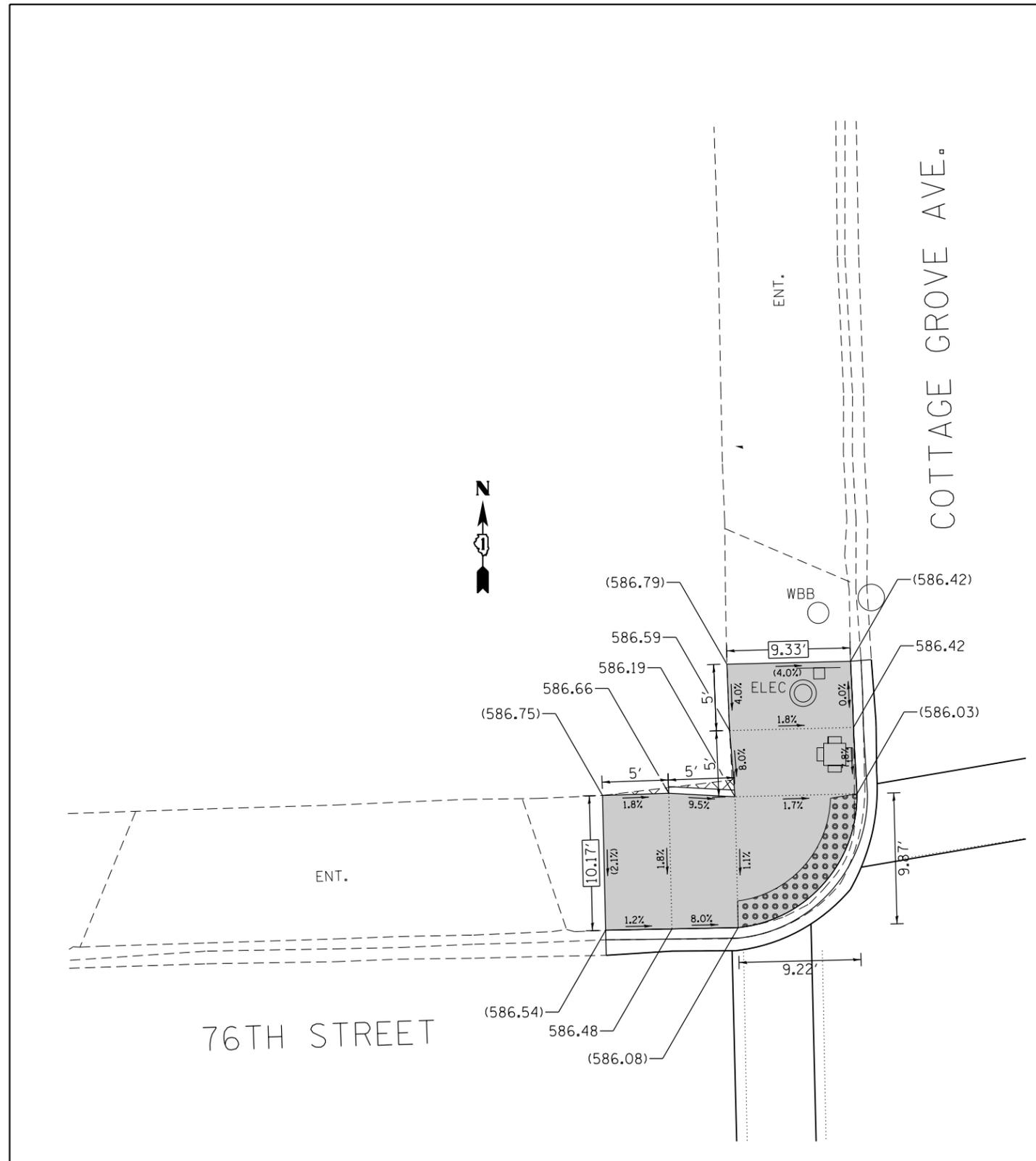


REFERENCE BENCHMARK ELEV 589.45  
 BENCHMARK : CUT "X" ON CHAIN BOLT OF FIRE HYDRANT  
 LOCATION : ON NORTH EAST CORNER OF 76TH STREET AND EVANS AVENUE

**LEGEND**

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- ( ) EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 889.45  
 BENCHMARK : CUT "X" ON CHAIN BOLT OF FIRE HYDRANT  
 LOCATION : ON NORTH EAST CORNER OF 76TH STREET AND EVANS AVENUE



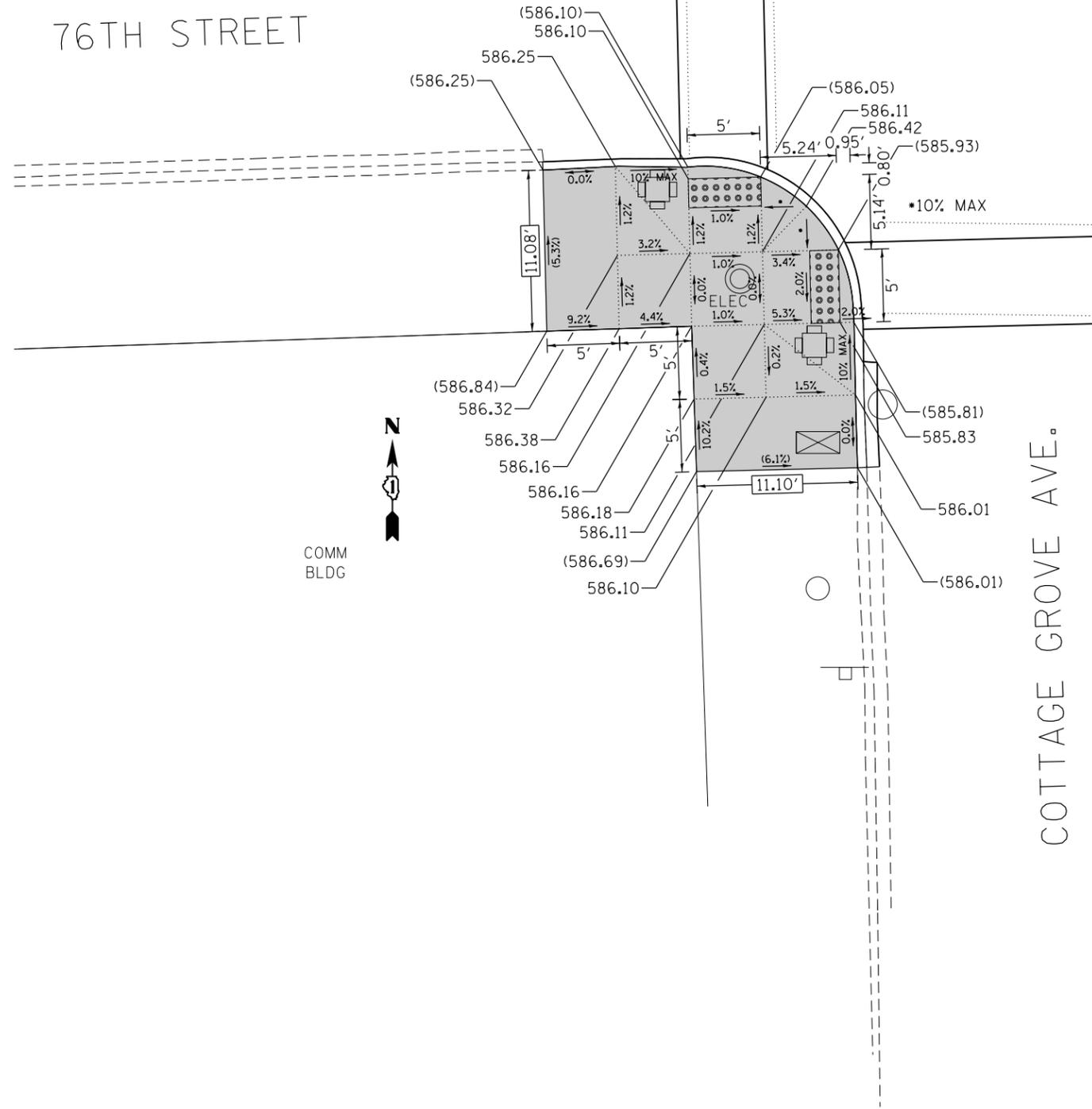
REFERENCE BENCHMARK ELEV 587.72  
 BENCHMARK : CUT "X" ON CHAIN BOLT OF FIRE HYDRANT  
 LOCATION : ON NORTH EAST CORNER OF 76TH ST. AND COTTAGE GROVE AVE.

**LEGEND**

<span style="border: 1px solid black; padding: 2px;">xx.xx'</span>	EXISTING LENGTH	<span style="display: inline-block; width: 15px; height: 15px; background-color: #cccccc; border: 1px solid black;"></span>	PROPOSED SIDEWALK
<span style="border-bottom: 2px solid black; width: 20px; display: inline-block;"></span>	PROPOSED SIDE CURB	<span style="display: inline-block; width: 15px; height: 15px; background: repeating-linear-gradient(45deg, transparent, transparent 2px, black 2px, black 4px); border: 1px solid black;"></span>	DETECTABLE WARNINGS
( )	EXISTING ELEVATION/SLOPE	<span style="display: inline-block; width: 15px; height: 15px; border: 1px solid black; background: repeating-linear-gradient(-45deg, transparent, transparent 2px, black 2px, black 4px);"></span>	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

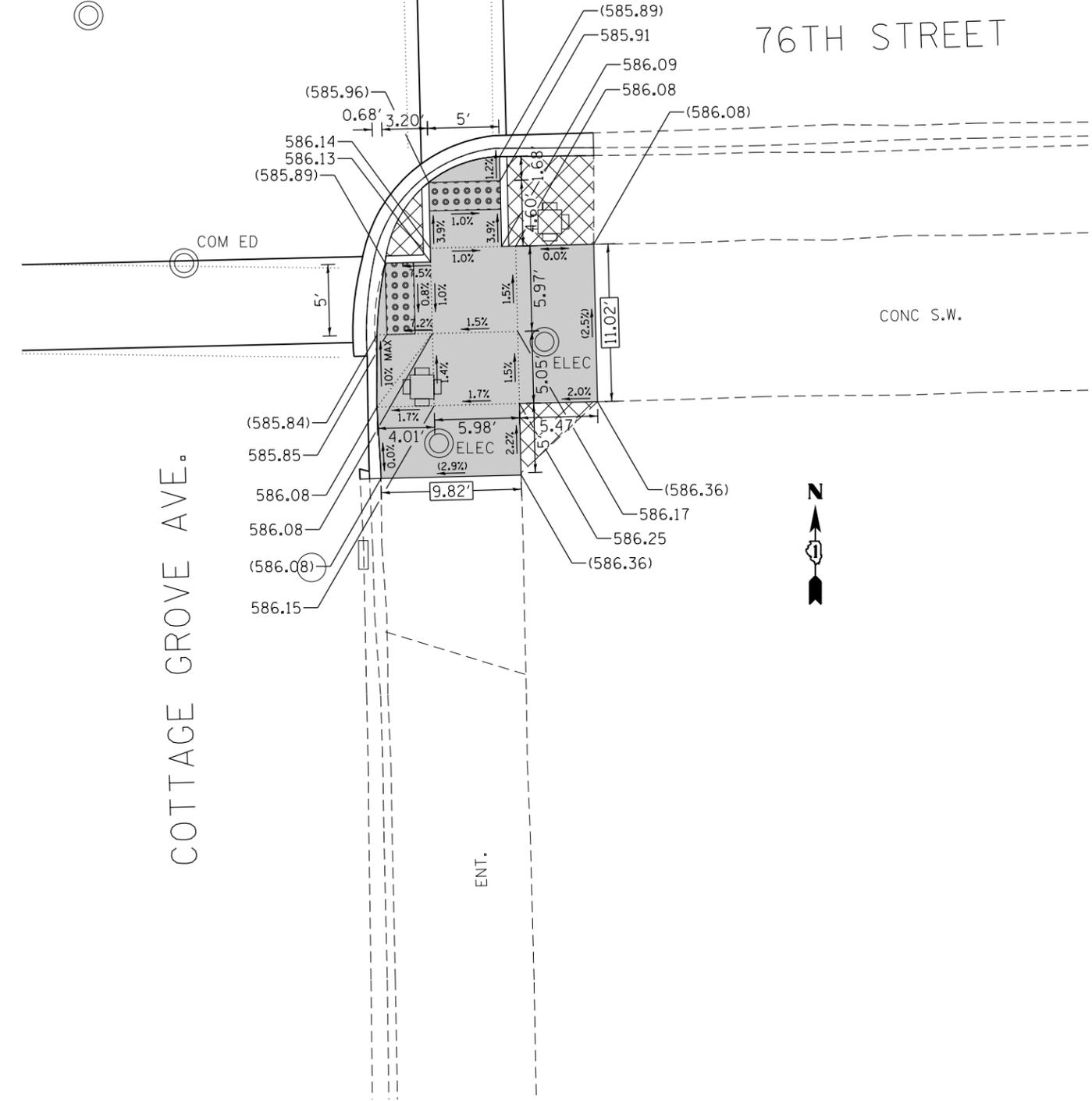
REFERENCE BENCHMARK ELEV 587.72  
 BENCHMARK : CUT "X" ON CHAIN BOLT OF FIRE HYDRANT  
 LOCATION : ON NORTH EAST CORNER OF 76TH ST. AND COTTAGE GROVE AVE.

76TH STREET



COTTAGE GROVE AVE.

76TH STREET



COTTAGE GROVE AVE.

REFERENCE BENCHMARK ELEV 587.72  
 BENCHMARK : CUT "X" ON CHAIN BOLT OF FIRE HYDRANT  
 LOCATION : ON NORTH EAST CORNER OF 76TH ST. AND COTTAGE GROVE AVE.

**LEGEND**

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- ( ) EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 587.72  
 BENCHMARK : CUT "X" ON CHAIN BOLT OF FIRE HYDRANT  
 LOCATION : ON NORTH EAST CORNER OF 76TH ST. AND COTTAGE GROVE AVE.

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -
pw:\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI07017\Drawings\Design\17-sht-details.dgn		REVISIONS	REVISED -
Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/5/2017	DATE - 5/3/2017	REVISED -

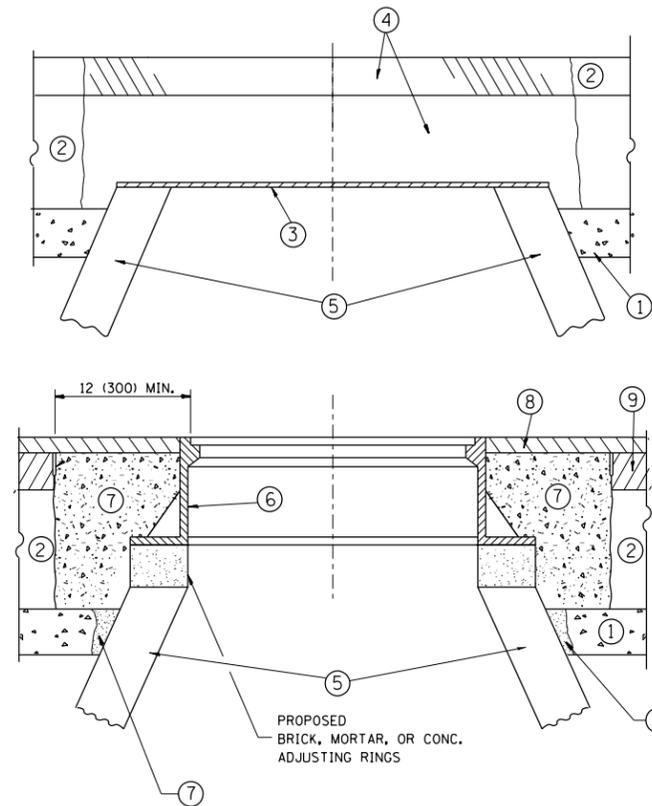
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

76TH STREET FROM ST. LAWRENCE AVENUE TO DREXEL AVENUE  
 PEDESTRIAN RAMP DETAILS

SCALE: 1" = 5' SHEET 8 OF 9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1541	2016-079R5	COOK	20	7H
CONTRACT NO. 62D61				
ILLINOIS FED. AID PROJECT				





**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

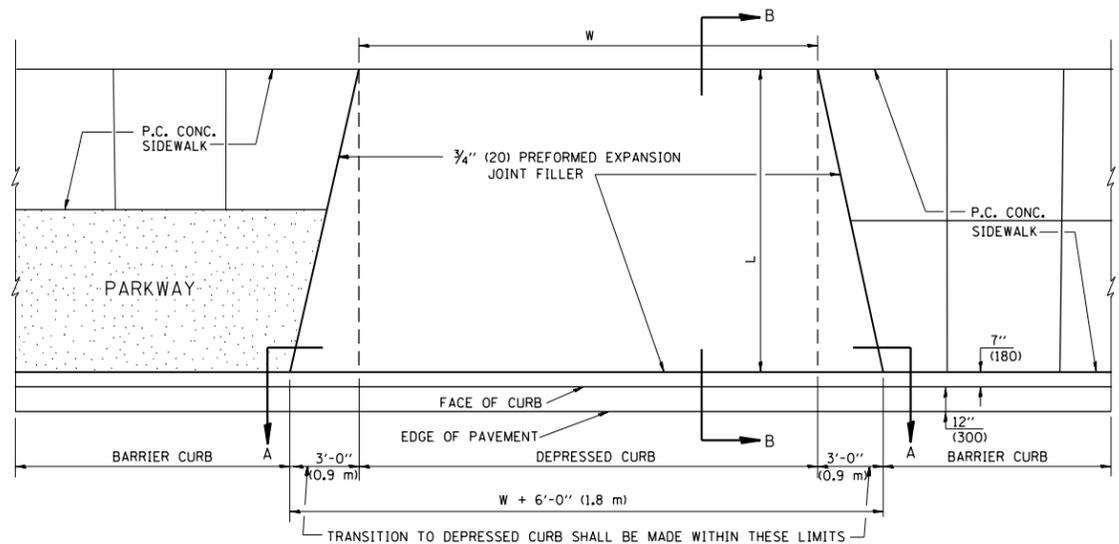
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bartonrw	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI0701\Drawings\Design\Diststd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 3/23/2017	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

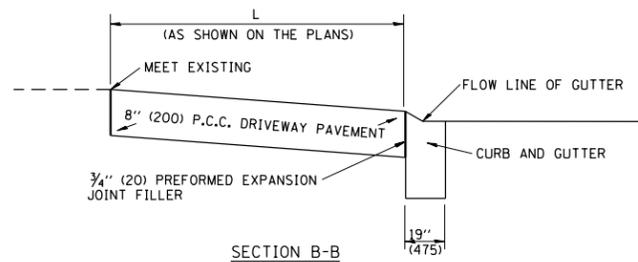
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1541	2016-079R5	COOK	20	8
<b>BD600-03 (BD-8)</b>		<b>CONTRACT NO. 62D61</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



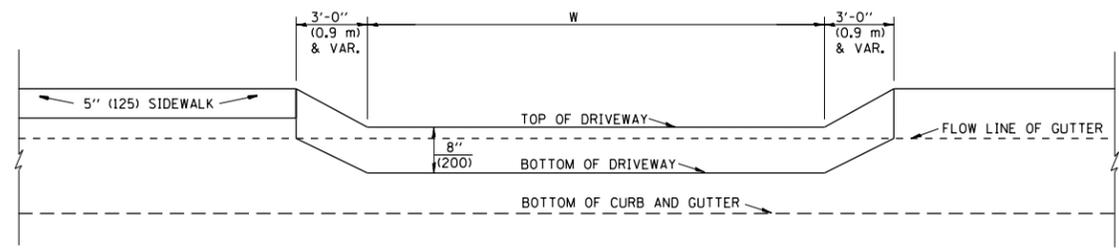
PLAN VIEW

NOTES:

1. EXPANSION JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE DETAILS FOR P.C.C. SIDEWALK.
2. THE CURB BETWEEN ADJACENT DRIVEWAYS SHALL BE FULL HEIGHT FOR A DISTANCE OF AT LEAST FOUR FEET (1.2 METERS)
3. P.C. CONCRETE DRIVEWAYS SHALL BE CONSTRUCTED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
4. 3/4" (20) PREFORMED EXPANSION JOINTS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO P.C.C. DRIVEWAY PAVEMENT 8" (200).
5. COMBINATION CONC. CURB AND GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE TRANSITION CURB AND GUTTER.

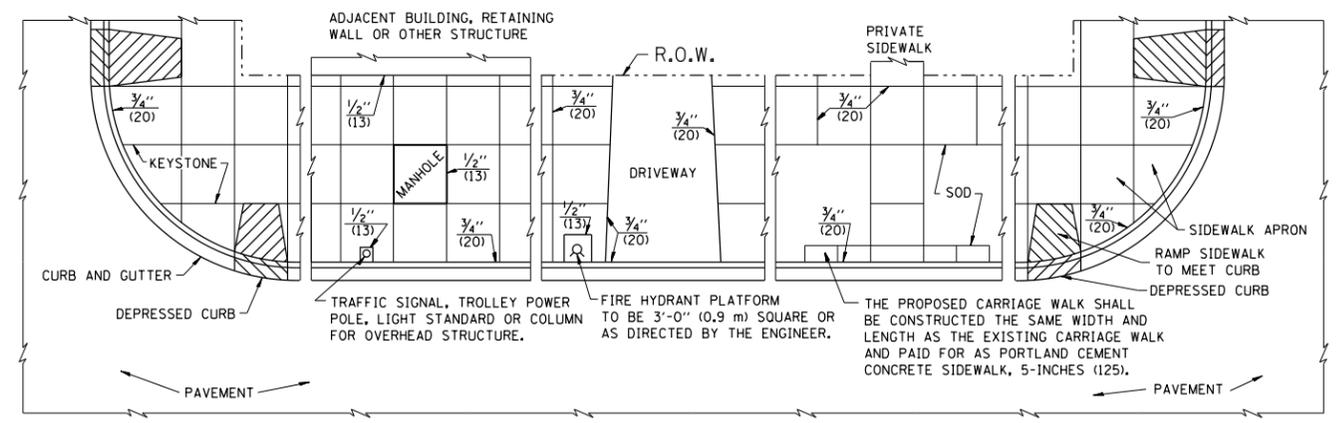


SECTION B-B



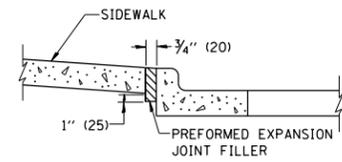
SECTION A-A

P.C.C. DRIVEWAY PAVEMENT DETAIL



NOTES:

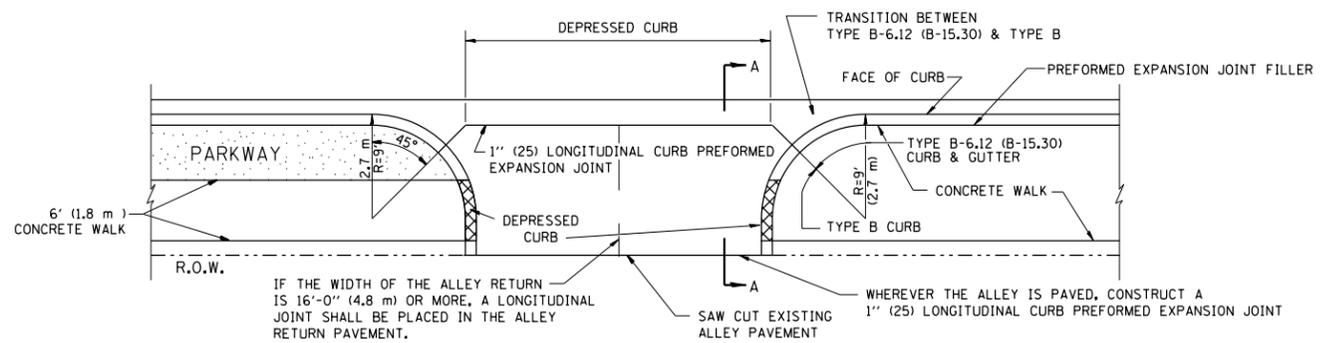
1. ONE-HALF INCH THICK EXPANSION JOINTS SHALL BE PLACED BETWEEN THE SIDEWALK AND ALL STRUCTURES SUCH AS LIGHT STANDARDS, TRAFFIC LIGHT STANDARDS, MANHOLES, WHICH EXTEND THROUGH THE SIDEWALK.
2. 3/4" (20) THICK EXPANSION JOINTS SHALL BE PLACED AT INTERVALS OF NOT MORE THAN 100 FEET (30 METERS) IN THE SIDEWALK, WHERE THE SIDEWALK IS CONSTRUCTED ADJACENT TO PAVEMENT OR CURB HAVING EXPANSION JOINTS, THE EXPANSION JOINTS IN THE SIDEWALK SHALL BE PLACED OPPOSITE THE EXISTING EXPANSION JOINTS AS NEARLY AS PRACTICABLE. EXPANSION JOINTS SHALL ALSO BE PLACED WHERE THE SIDEWALK ABUTS EXISTING SIDEWALKS, BETWEEN DRIVEWAY PAVEMENT AND SIDEWALK, AND BETWEEN SIDEWALK AND CURBS WHERE THE SIDEWALK ABUTS A CURB.



SLOPE FOR SIDEWALK  
1" (25) IN 3'-0" (0.9 m) IN CHICAGO

PORTLAND CEMENT CONCRETE SIDEWALK DETAILS

NOTES: NO EXTRA COMPENSATION SHALL BE ALLOWED FOR THE GUTTER FLARE



SECTION A-A

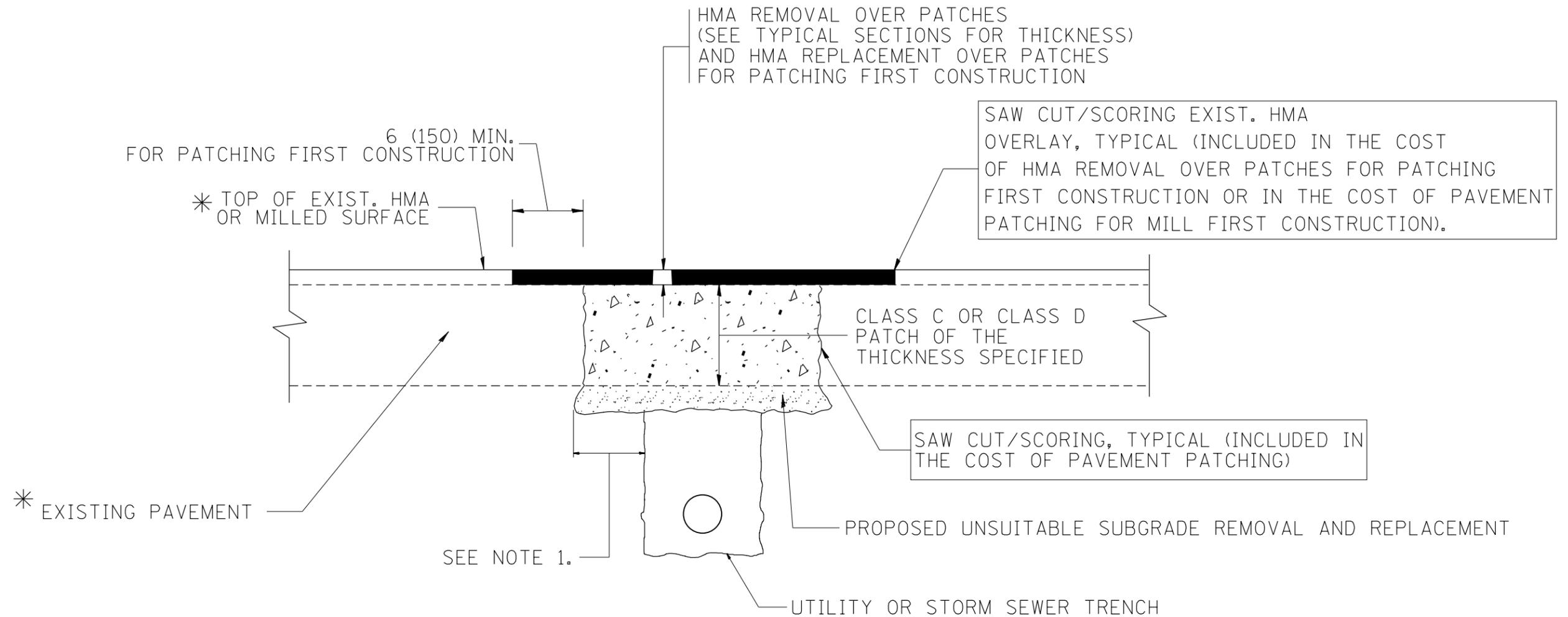
ALLEY RETURN DETAIL

FILE NAME =	USER NAME = bartonrw	DESIGNED - M. DE YONG	REVISED -
p:\1\084EBID\INTEG.illinois.gov\PIWID\Documents\DOT Offices\District 1\Projects\DI070\Drawings\Design\Diststd.dgn		CHECKED -	REVISED -
		DATE - 06-13-90	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO  
DETAILS FOR P.C. CONCRETE DRIVEWAY, ALLEY RETURN AND SIDEWALK  
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1541	2016-079R5	COOK	20	9
BD400-03 (BD-17)		CONTRACT NO. 62D61		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

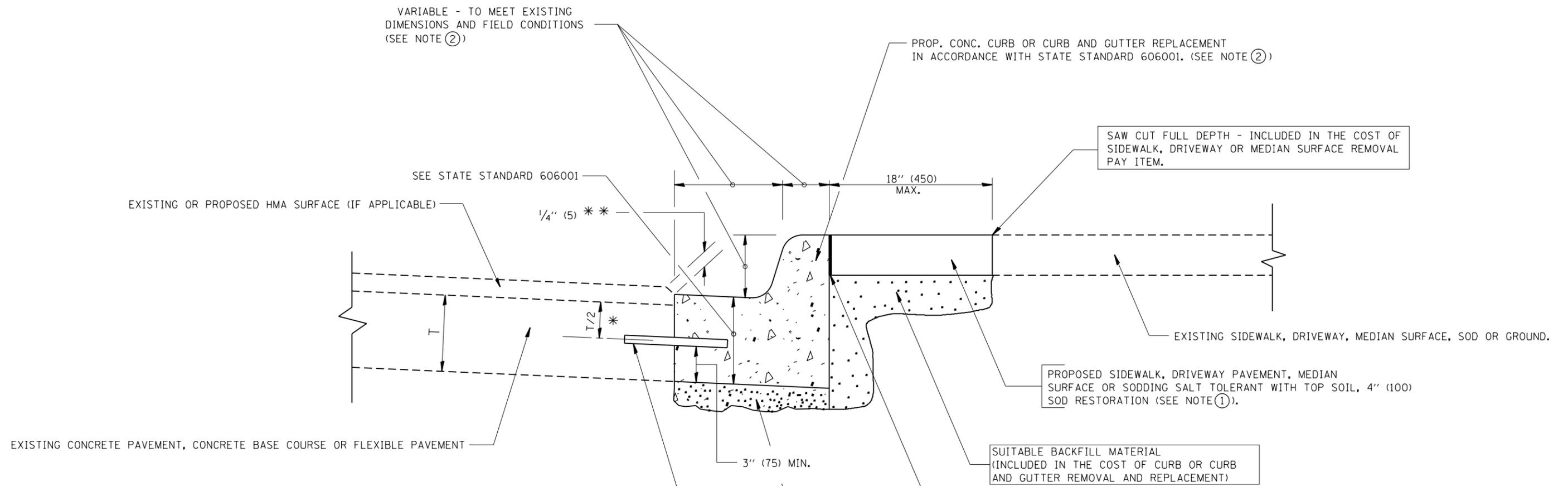
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = bartonw	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI070\Drawings\Design\Diststd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					1541	2016-079R5	COOK	20	10
	PLOT DATE = 3/23/2017	DATE - 10-25-94	REVISED - R. BORO 09-04-07		<b>BD400-04 (BD-22)</b>			<b>CONTRACT NO. 62D61</b>				
			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT			



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
  - \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

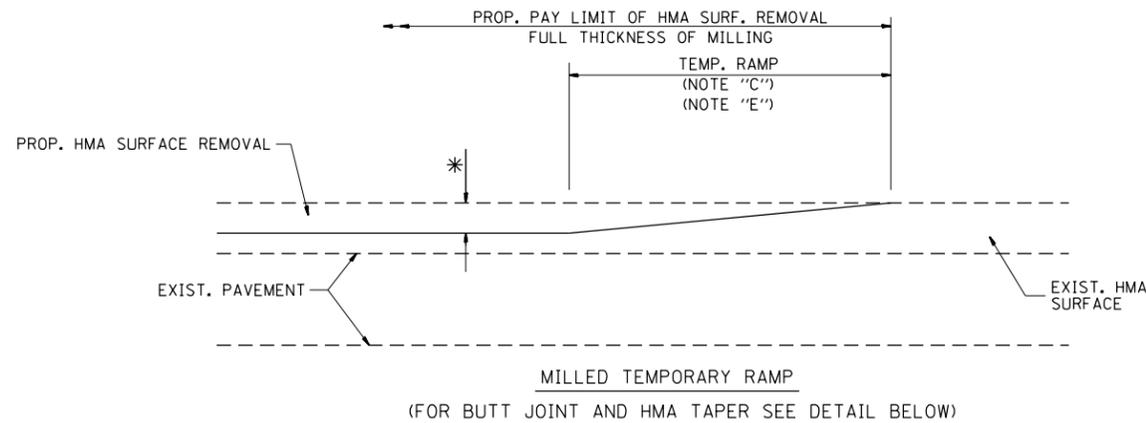
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

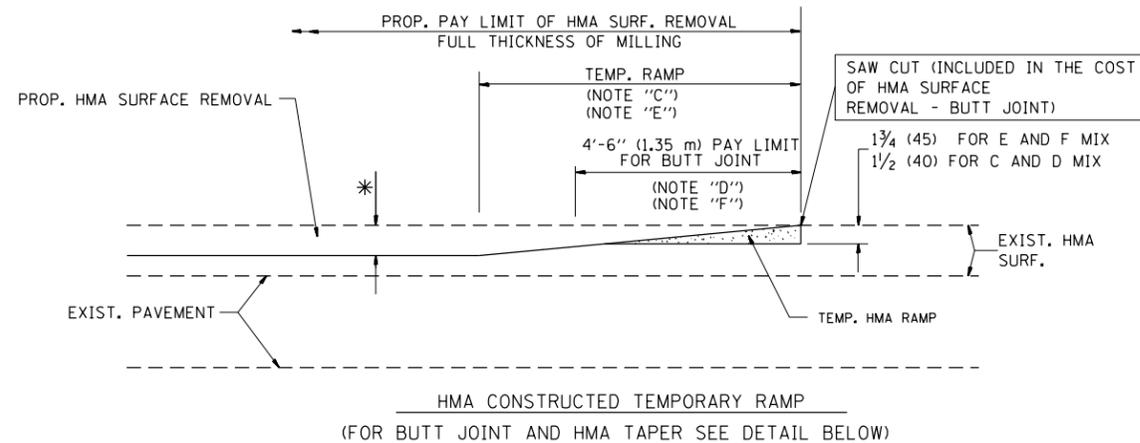
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = bartonrw	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.U. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
			REVISED - A. ABBAS 03-21-97			1541	2016-079RS	COOK	20	11	
			REVISED - M. GOMEZ 01-22-01			<b>BD600-06 (BD-24)</b>		<b>CONTRACT NO. 62D61</b>			
			REVISED - R. BORO 12-15-09			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		
PLOT SCALE = 100.0000' / 1in.		CHECKED -	DATE - 03-11-94	FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT							
PLOT DATE = 3/23/2017											

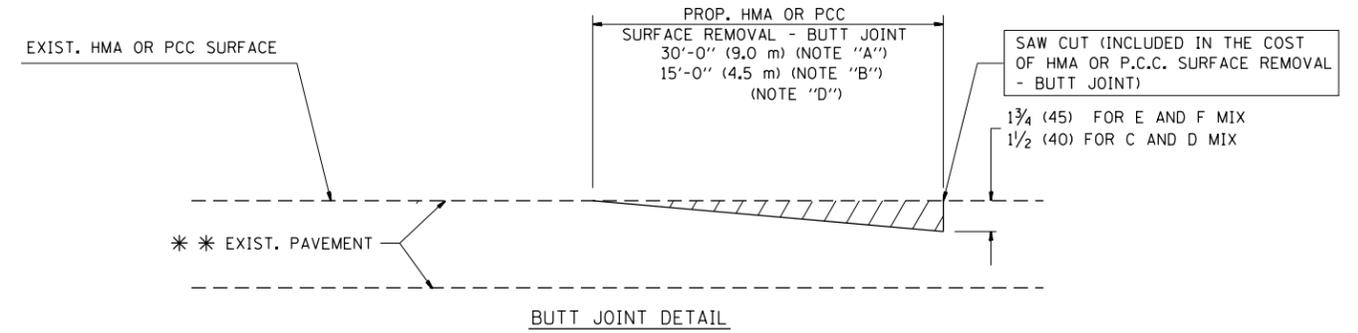


**OPTION 1**

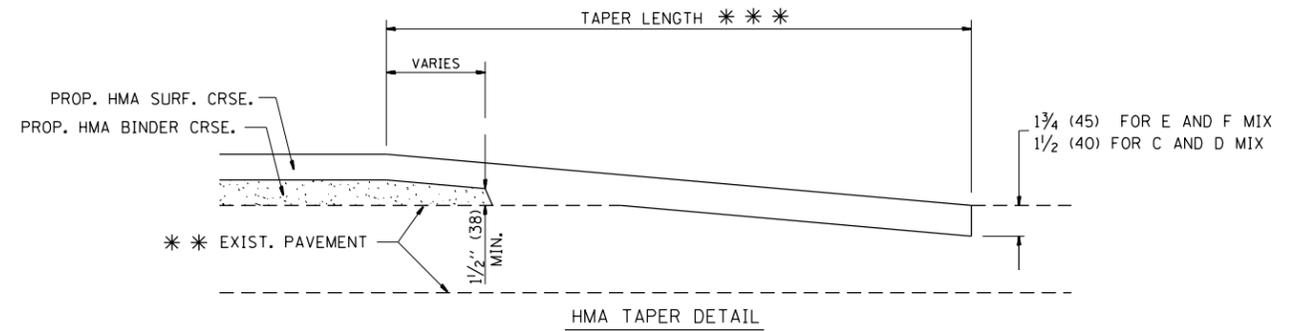


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

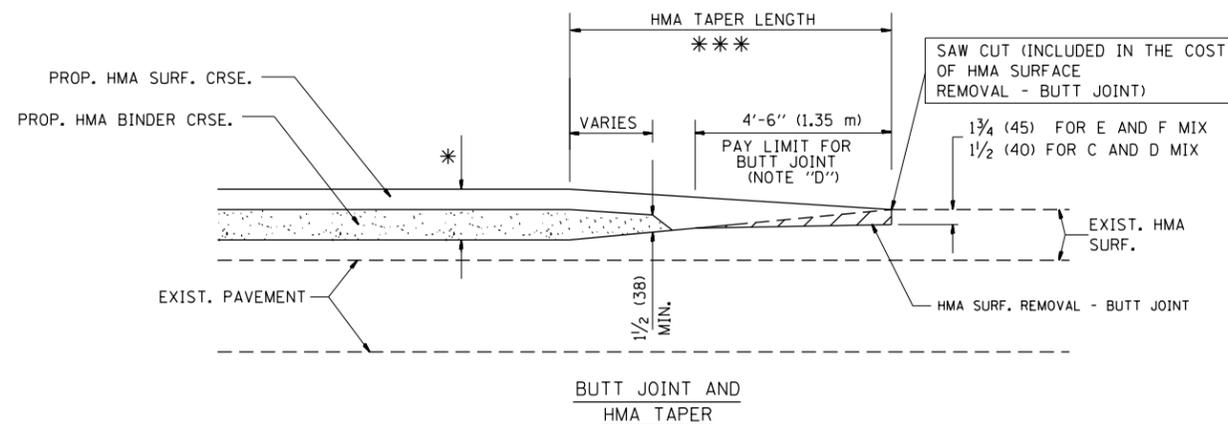
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

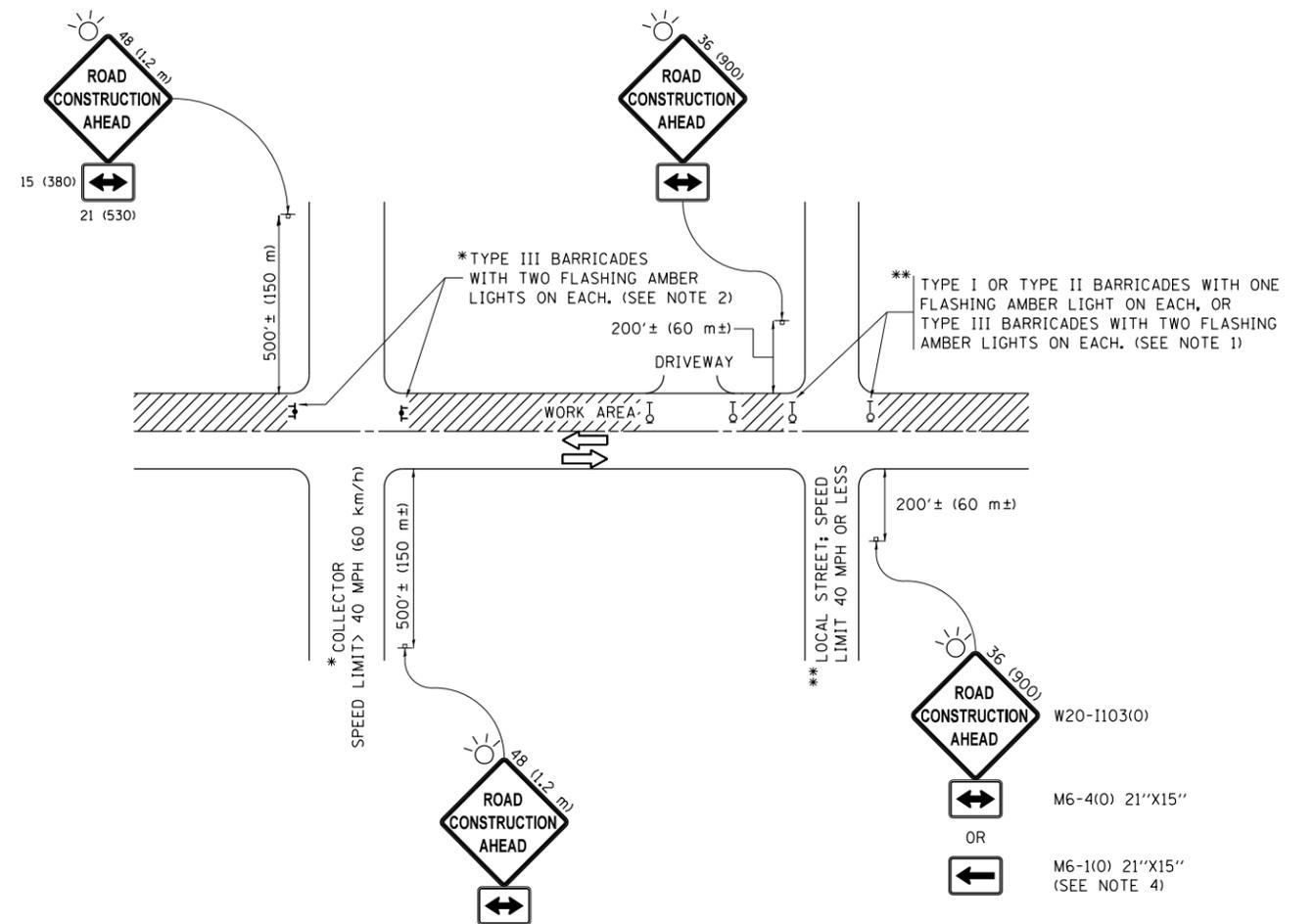
FILE NAME =	USER NAME = bartonw	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
p:\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI070\Drawings\Design\Diststd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 3/23/2017	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1541	2016-079R5	COOK	20	12
<b>BD400-05 BD32</b>		<b>CONTRACT NO. 62D61</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

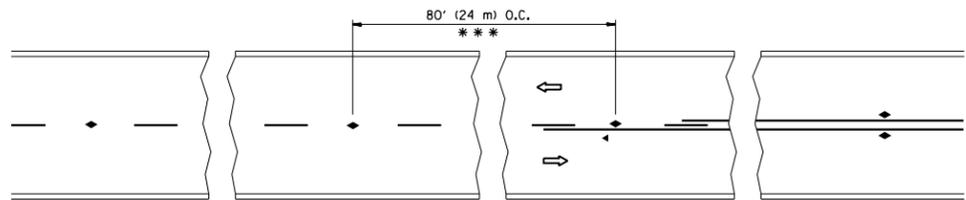
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	PLOT DATE = 3/23/2017	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

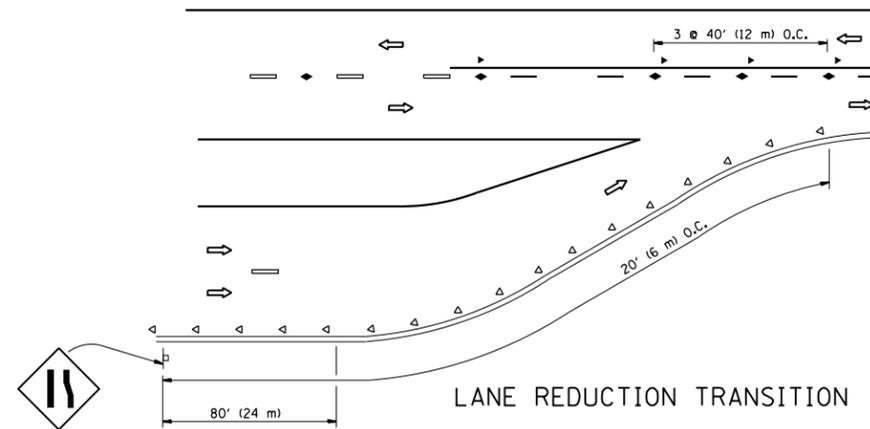
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ILLINOIS FED. AID PROJECT				

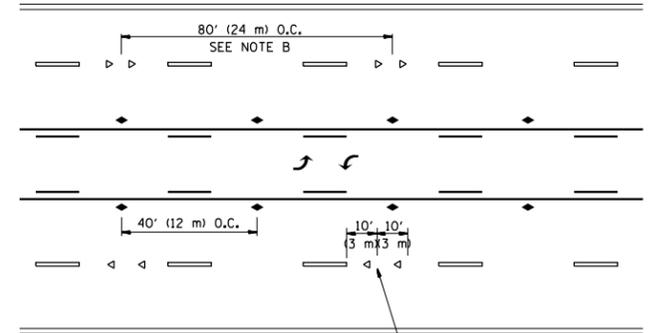


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

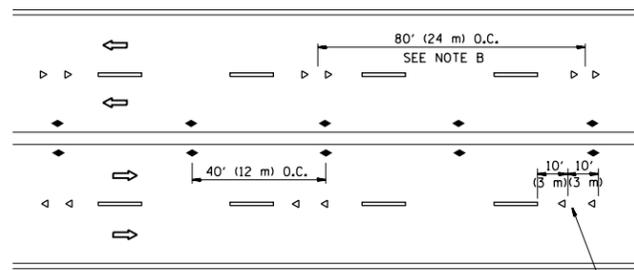
TWO-LANE/TWO-WAY



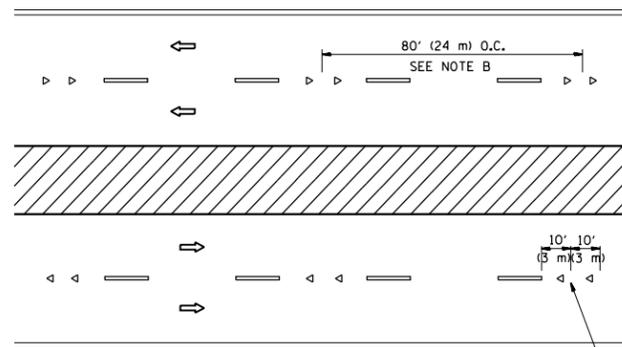
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

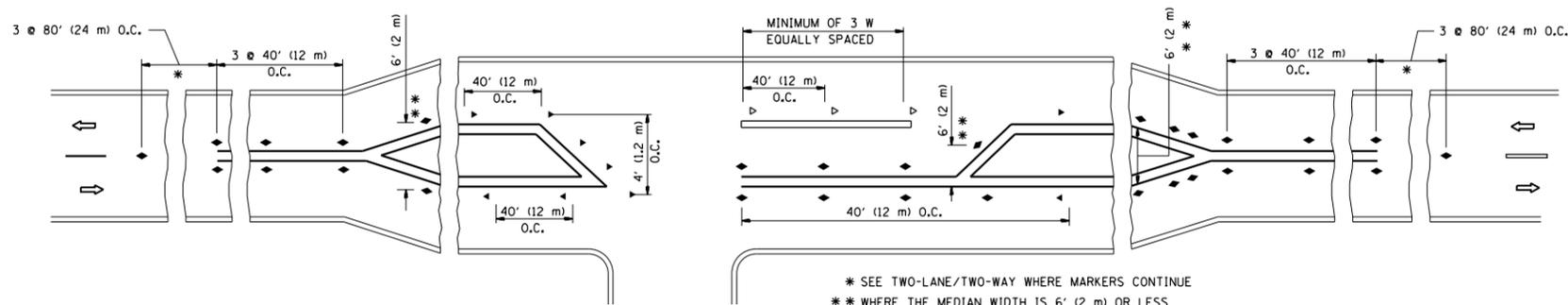
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

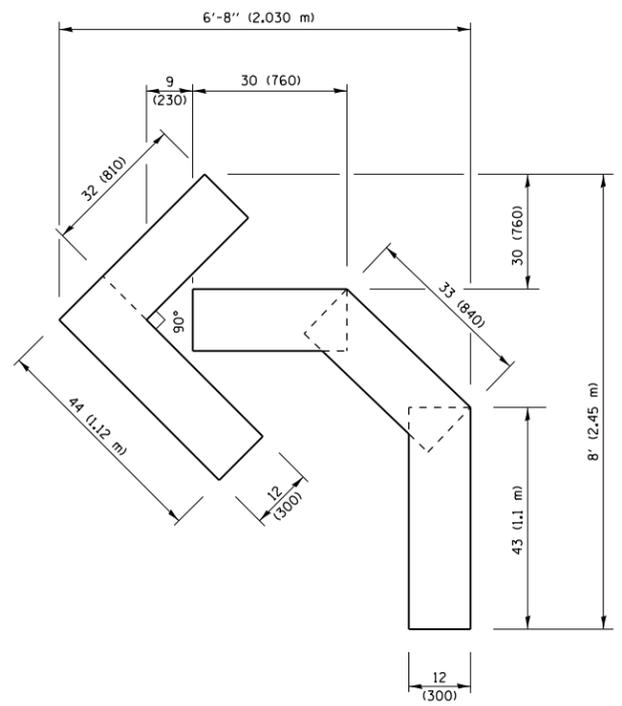
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = bartonrw	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
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		DATE -	REVISED - T. RAMMACHER 01-06-00
			REVISED - C. JUCIUS 09-09-09

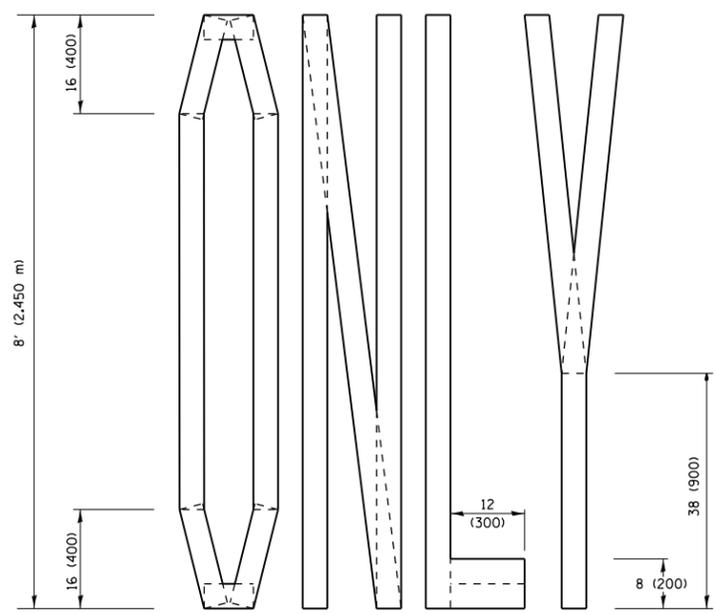
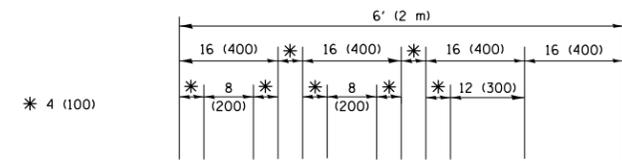
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

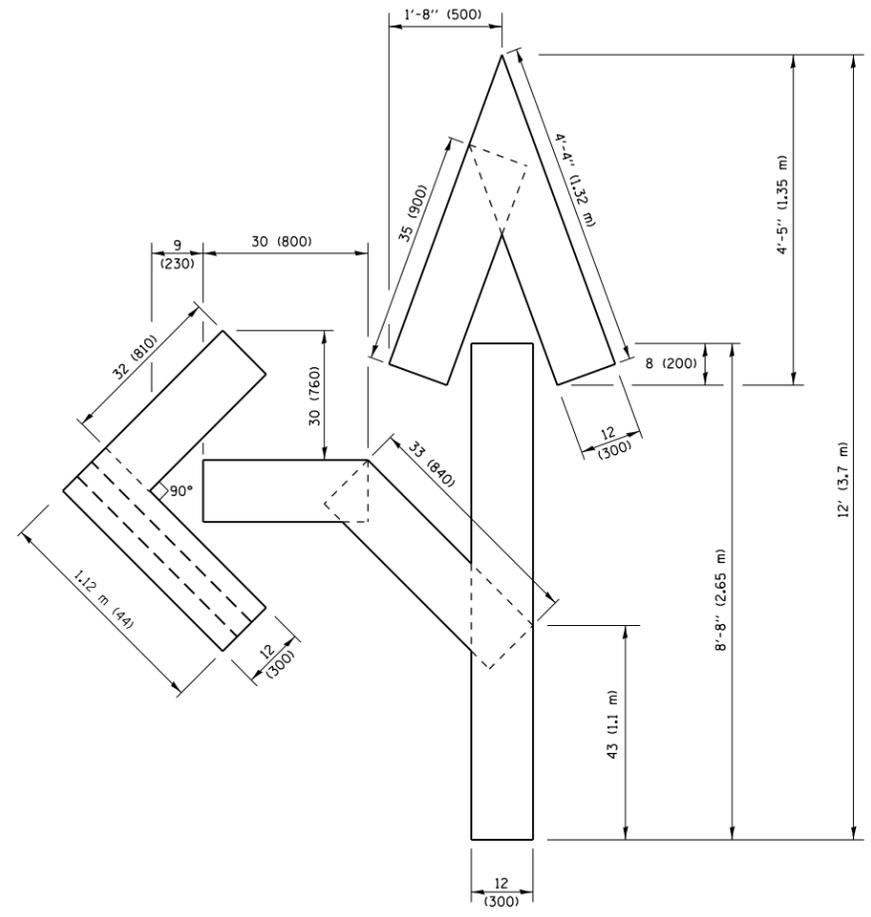
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1541	2016-079RS	COOK	20	15
TC-11			CONTRACT NO. 62D61	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

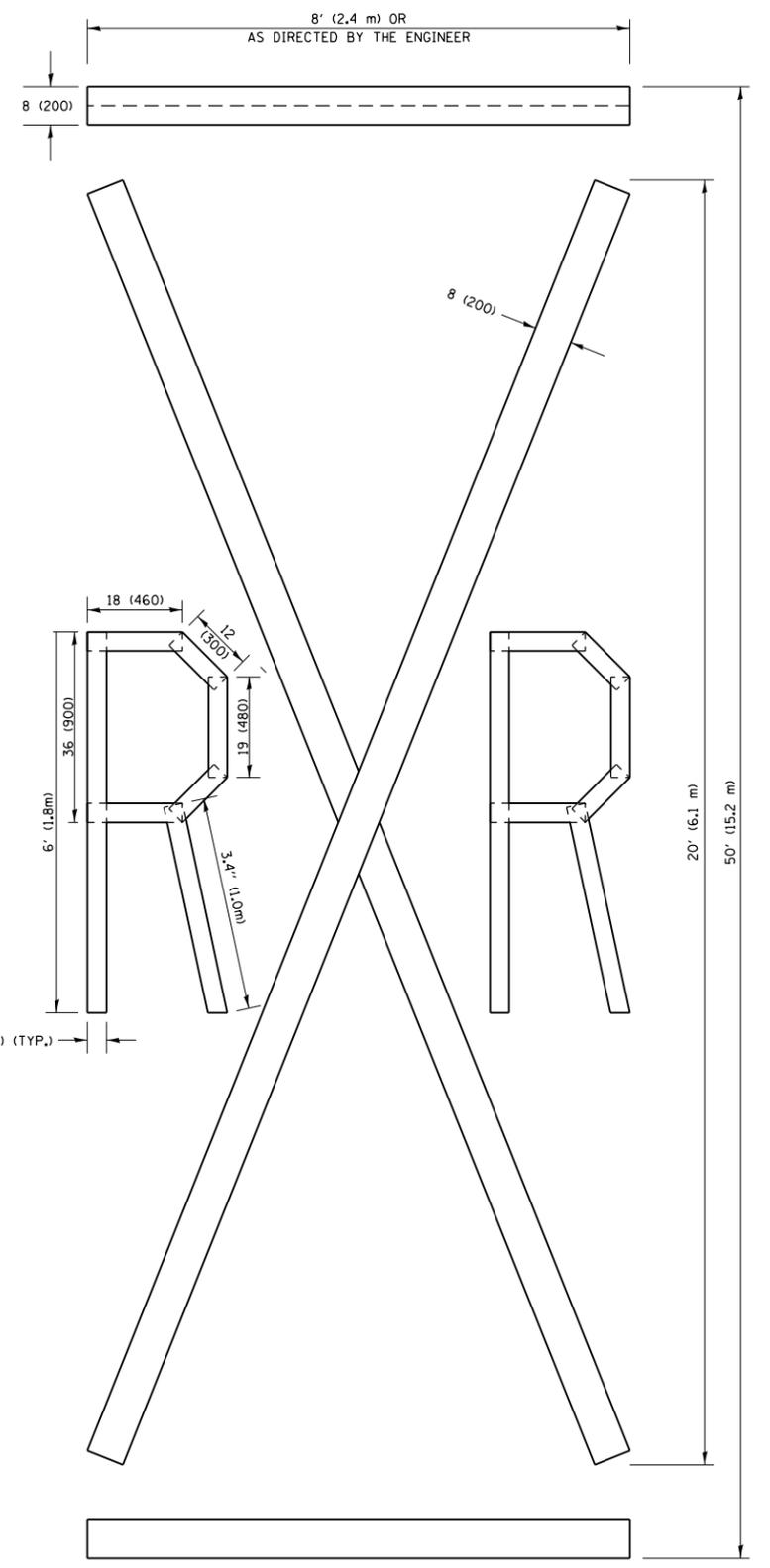


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

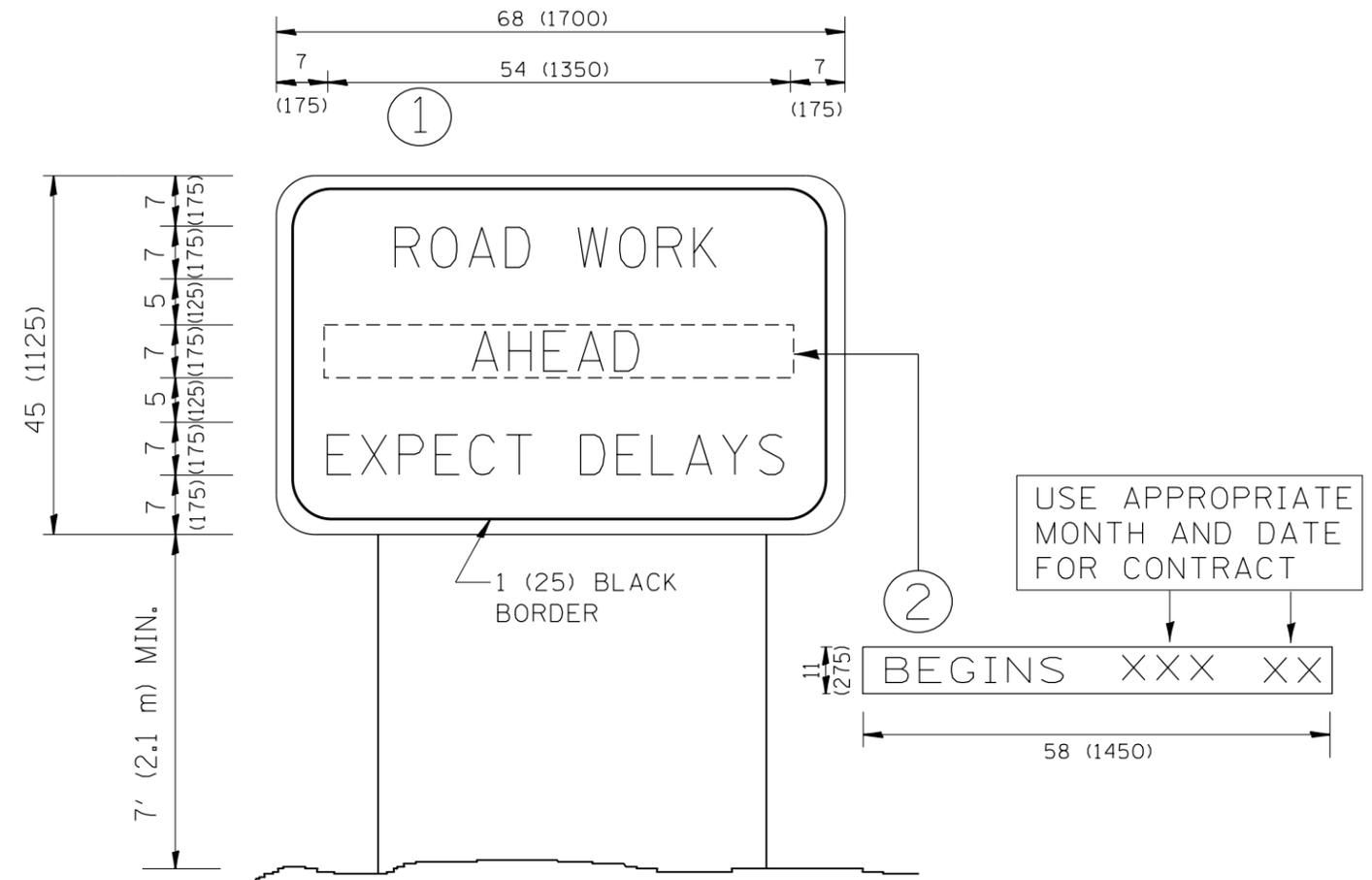
All dimensions are in inches (millimeters) unless otherwise shown.

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		DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1541	2016-079RS	COOK	20	16
<b>TC-16</b>			<b>CONTRACT NO. 62D61</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

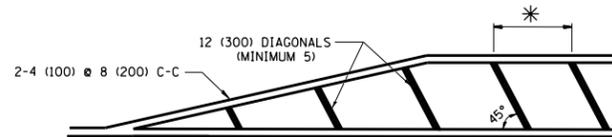
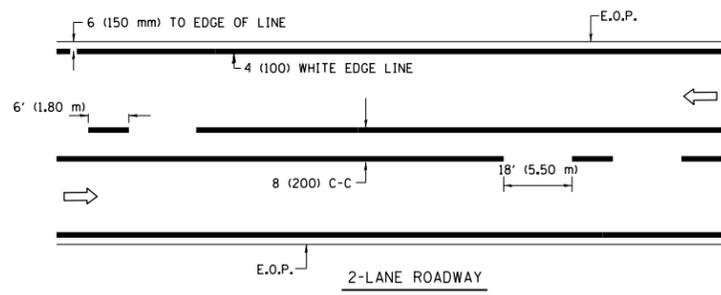


**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

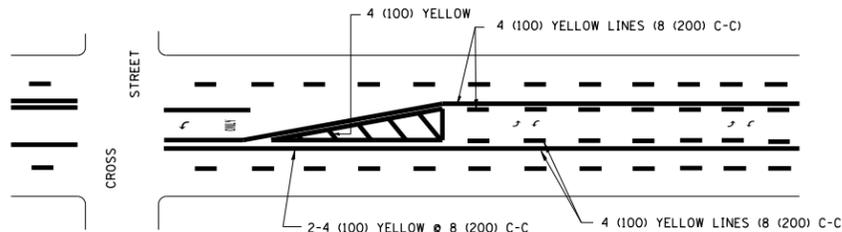
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		PLOT SCALE = 100.0000' / 1in.	REVISED - T. RAMMACHER 02-02-99			<b>TC-22</b>		<b>CONTRACT NO. 62D61</b>			
		PLOT DATE = 3/23/2017	REVISED - C. JUCIUS 01-31-07			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT

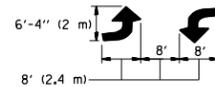


\* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
 \* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

**PAINTED MEDIANS**

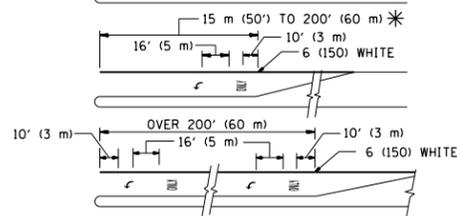
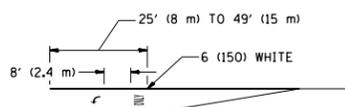
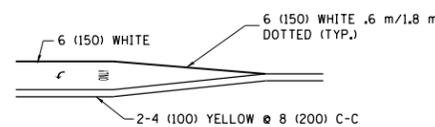


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

**TYPICAL PAINTED MEDIAN MARKING**

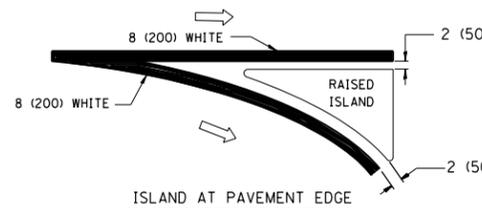
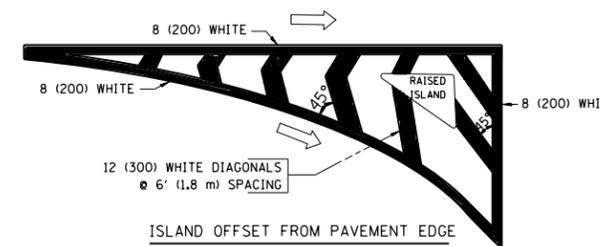


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.8 SQ. FT. (1.47 m<sup>2</sup>) ONLY AREA = 22.9 SQ. FT. (2.13 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

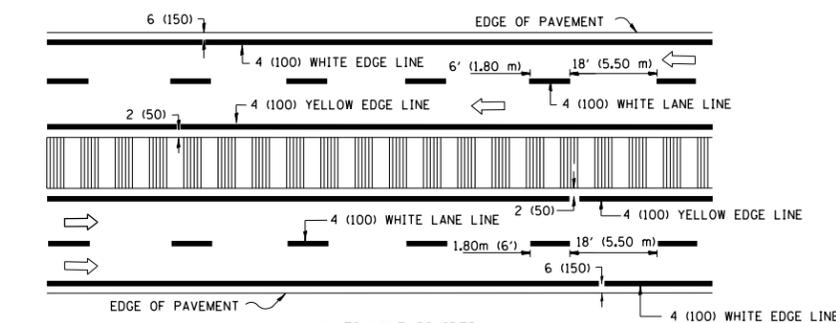
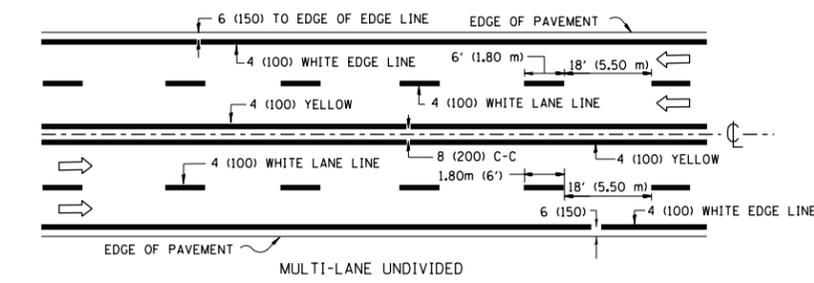


**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )

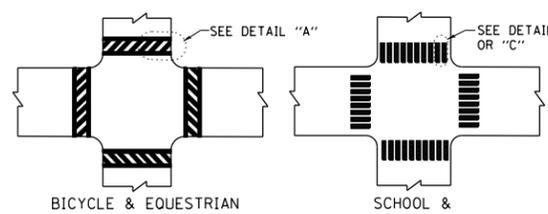
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

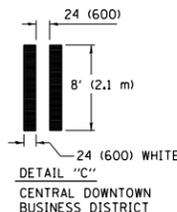


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

**TYPICAL LANE AND EDGE LINE MARKING**



**TYPICAL CROSSWALK MARKING**

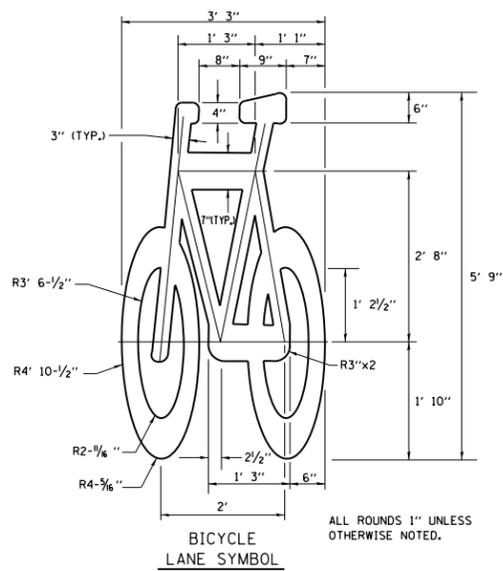
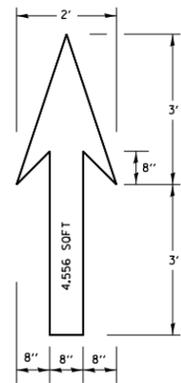


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		DATE -	REVISED -
			REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

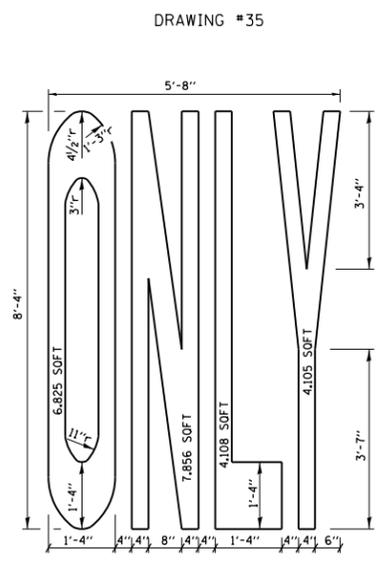
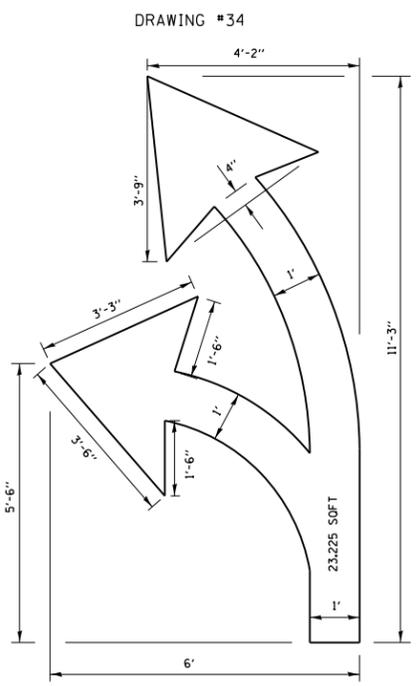
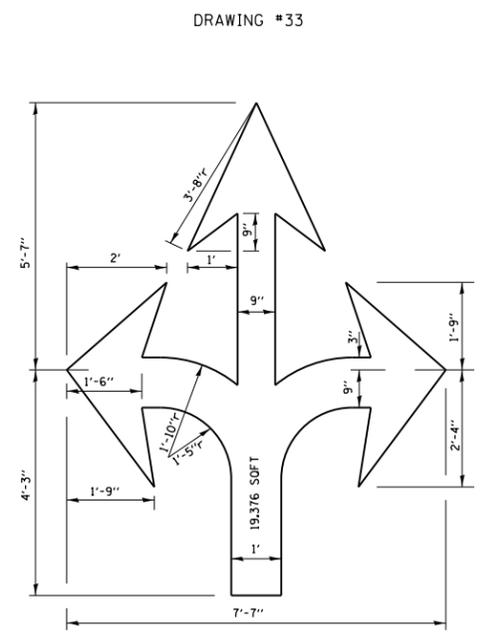
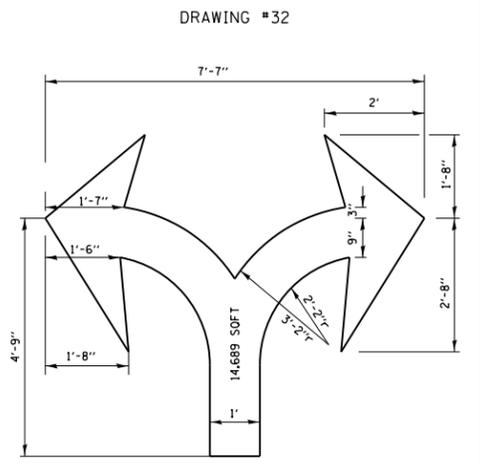
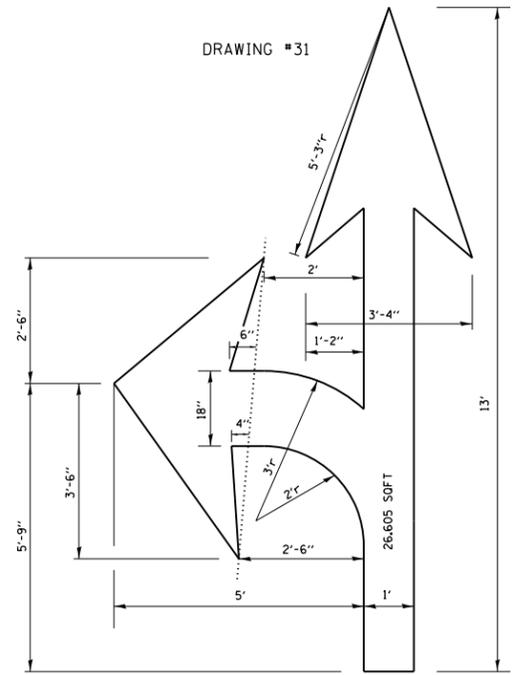
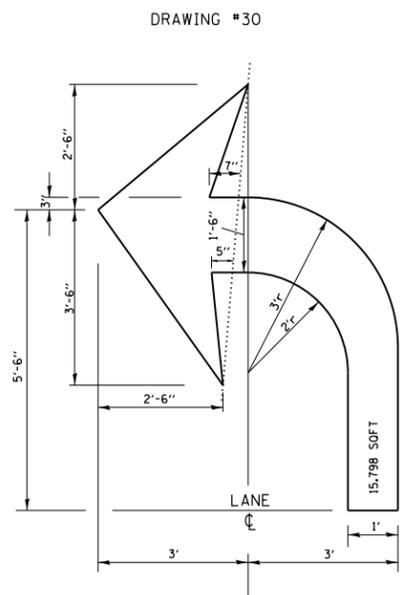
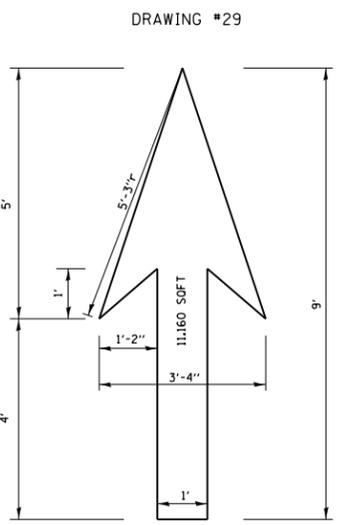
CITY OF CHICAGO			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 3 SHEETS	STA. TO STA.	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1541	2016-079RS	COOK	20	18
TC-24		CONTRACT NO. 62D61		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTE:**  
 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.  
 2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

**TYPICAL BIKE LANE SYMBOLS**  
 DRAWING #28



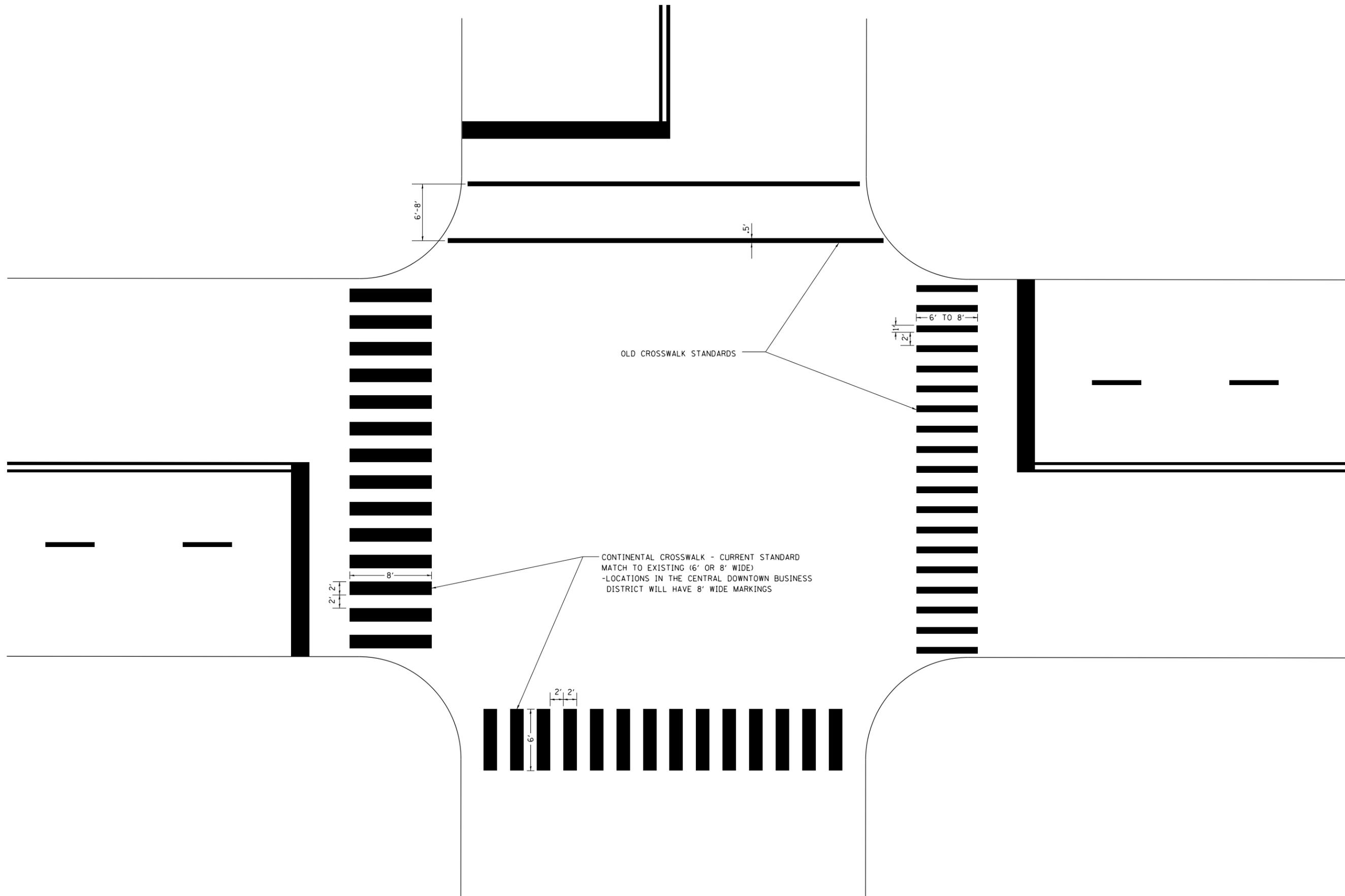
**NOTE:**  
 ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

FILE NAME =	USER NAME = bartonw	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI070\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - K. ENG 01-12-12
		DATE -	REVISED -
			REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>CITY OF CHICAGO</b>	
<b>TYPICAL PAVEMENT MARKINGS</b>	
SCALE: NONE	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1541	2016-079R5	COOK	20	19
<b>TC-24</b>		<b>CONTRACT NO. 62D61</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



OLD CROSSWALK STANDARDS

CONTINENTAL CROSSWALK - CURRENT STANDARD  
 MATCH TO EXISTING (6' OR 8' WIDE)  
 -LOCATIONS IN THE CENTRAL DOWNTOWN BUSINESS  
 DISTRICT WILL HAVE 8' WIDE MARKINGS

FILE NAME =	USER NAME = bartonw	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
p:\11\084EBIDINTEG.illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI0702\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - K. ENG 01-12-12
	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -
	PLOT DATE = 3/23/2017		REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>CITY OF CHICAGO</b>			
<b>TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET NO. 3 OF 3 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1541	2016-079RS	COOK	20	20
<b>TC-24</b>			<b>CONTRACT NO. 62D61</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				