

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	1
ILLINOIS			CONTRACT NO. 62B90	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE CITY OF ST. CHARLES, VILLAGE OF WAYNE AND UNINCORPORATED ST. CHARLES TOWNSHIP.

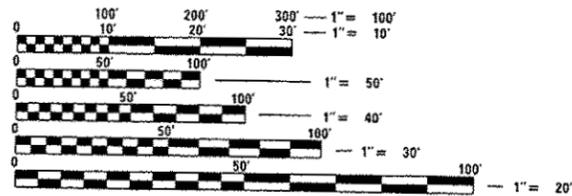
PROPOSED
HIGHWAY PLANS

FAU ROUTE 2503: IL 25
JOHNOR AVE TO SOUTH OF BREWSTER CREEK BRIDGE
SECTION: (38, 43 & 49) RS-7
PROJECT: STP-2503(020)
RESURFACING (3P), PEDESTRIAN RAMPS
KANE COUNTY

C-91-175-16

TRAFFIC DATA:

IL 25:
PROJECT BEGIN TO ARMY TRAIL RD:
ADT (2016) = 9,400
SPEED LIMIT = 35, 45 MPH
ARMY TRAIL RD TO PROJECT END:
ADT (2016) = 8,200
SPEED LIMIT = 45 MPH

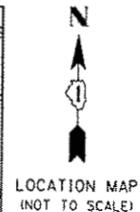
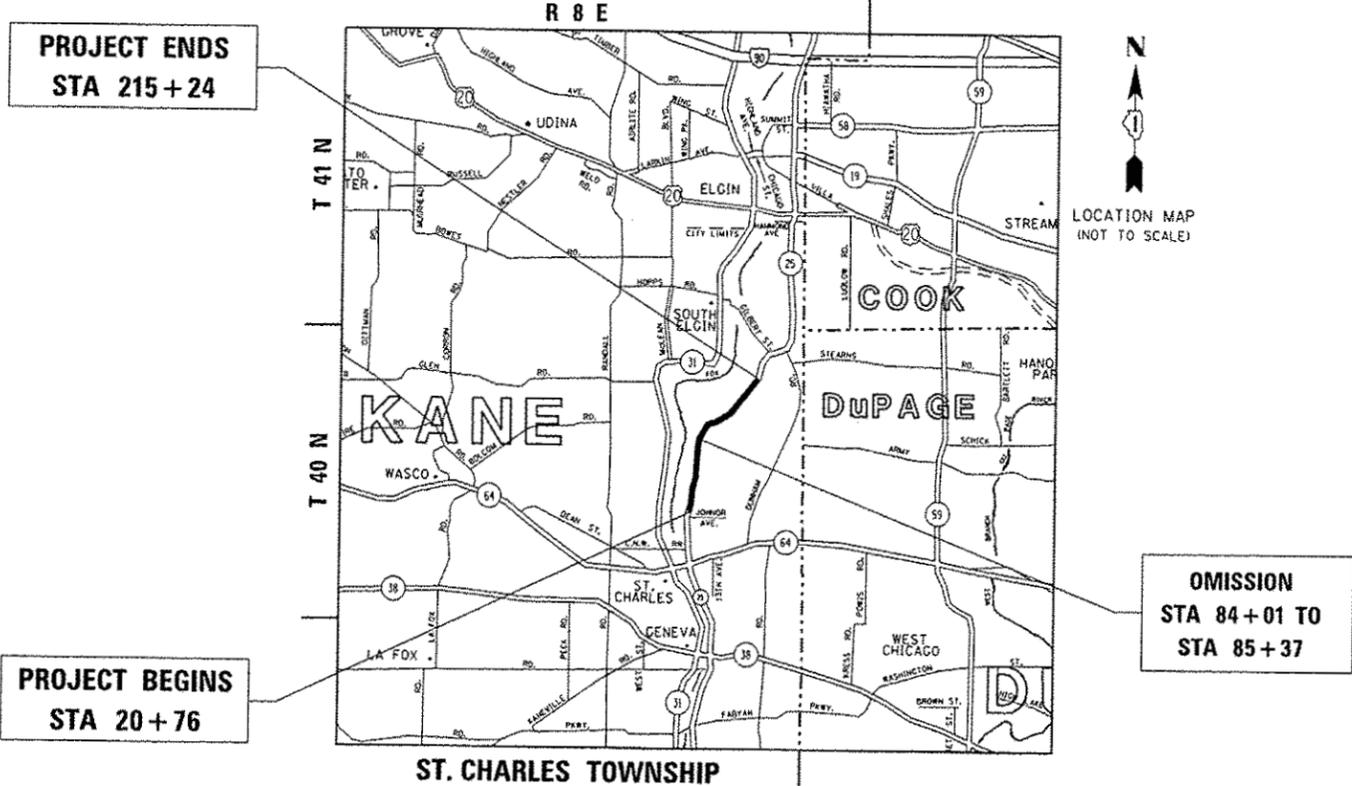


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: KARI SMITH (847) 705-4437
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62B90



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
SUBMITTED April 3 2017
Regional Engineer
May 12 2017
Engineer of Design and Environment
May 12 2017
Director of Program Development

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

GROSS LENGTH = 19,448 FT. = 3.683 MILES
NET LENGTH = 19,312 FT. = 3.657 MILES

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

SHEET NO.	DESCRIPTION	STANDARD NO.	DESCRIPTION
1	COVER SHEET	000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
2-3	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	424001-09	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
4-7	SUMMARY OF QUANTITIES	424011-03	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
8-9	TYPICAL SECTIONS	424016-03	MID-BLOCK CURB RAMPS FOR SIDEWALKS
10	SCHEDULE OF QUANTITIES	424021-03	DEPRESSED CORNER FOR SIDEWALKS
11-17	ROADWAY AND PAVEMENT MARKING PLAN	442201-03	CLASS C AND D PATCHES
18-19	CURB RAMPS IMPROVEMENT PLANS	482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
20-21	DETECTOR LOOP REPLACEMENT PLAN	542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
22	BD-01: DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB OR EDGE OF SHOULDER GREATER THAN OR EQUAL TO 15' (4.5 M)	606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
23	BD-02: DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB IS LESS THAN 15' (4.5 M)	630001-11	STEEL PLATE BEAM GUARDRAIL
24	BD-08: DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	630101-10	STRONG POST GUARDRAIL ATTACHED TO CULVERT
25	BD-22: PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	630301-07	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
26	BD-24: CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	725001-01	OBJECT AND TERMINAL MARKERS
27	BD-32: BUTT JOINT AND HMA TAPER DETAILS	782006	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS
28	BD-51: BENCHING DETAIL FOR EMBANKMENT WIDENING	642006	SHOULDER RUMBLE STRIPS, 8 IN.
29	TC-10: TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS	701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
30	TC-11: TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLow RESISTANT)	701011-04	OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY
31	TC-13: DISTRICT ONE TYPICAL PAVEMENT MARKINGS	701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
32	TC-14: TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
33	TC-16: PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING	701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
34	TC-22: ARTERIAL ROAD INFORMATION SIGN	701502-07	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
35	TS-05: DISTRICT 1 - STANDARD TRAFFIC SIGNAL DESIGN DETAILS (SHEET 2 OF 7)	701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
36	TS-07: DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
		701901-06	TRAFFIC CONTROL DEVICES
		814001-03	HANDHOLES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF ST. CHARLES, VILLAGE OF WAYNE AND ST. CHARLES TOWNSHIP.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- THE CONTRACTOR SHALL CONTACT DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE RESIDENT ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ADJUTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.
- LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.
- CATCH BASINS, MANHOLES, INLETS, DRAINAGE STRUCTURES AND VALVE VAULTS ADJUSTMENT AND/OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER / TECHNICIAN.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS, UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER, VIA E-MAIL AT DON.CHIARUGI@ILLINOIS.GOV, A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.

SEE SHEET 3 FOR CONTINUATION

FILE NAME :	USER NAME : Veliokovv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 25 - JOHNSON AVE TO SOUTH OF BREWSTER CREEK BRIDGE INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	Documents\IDOT Offices\District 1\Projects\0117516\Design\0117516-shr-gennote.dgn	CHECKED -	REVISED -			2503	138, 43 & 491 RS-7	KANE	36	2	
	PLOT SCALE = 100.0000' / 1"	DATE -	REVISED -			CONTRACT NO. 62890					
	PLOT DATE = 3/30/2017					ILLINOIS FED. AID PROJECT					

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE			
42400800	DETECTABLE WARNINGS	SO FT	34	34			
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	72855	72855			
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	46	46			
44000600	SIDEWALK REMOVAL	SO FT	600	600			
44201823	CLASS D PATCHES, TYPE I, 15 INCH	SO YD	80	80			
44201827	CLASS D PATCHES, TYPE II, 15 INCH	SO YD	3200	3200			
44201831	CLASS D PATCHES, TYPE III, 15 INCH	SO YD	700	700			
44201833	CLASS D PATCHES, TYPE IV, 15 INCH	SO YD	1180	1180			
48101498	AGGREGATE SHOULDERS, TYPE B 4"	SO YD	304	304			
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	256	256			
50105220	PIPE CULVERT REMOVAL	FOOT	170	170			
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	8	8			
542A0217	PIPE CULVERTS, CLASS A, TYPE I 12"	FOOT	160	160			
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	1	1			

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE			
* 63000003	STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS	FOOT	787.5	787.5			
* 63000030	STRONG POST GUARDRAIL ATTACHED TO CULVERT	FOOT	50	50			
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	11	11			
63200310	GUARDRAIL REMOVAL	FOOT	1250	1250			
64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	2560	2560			
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	73	73			
* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1			
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6			
67100100	MOBILIZATION	LSUM	1	1			
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1	1			
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	LSUM	1	1			
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1			

* = SPECIALTY ITEMS

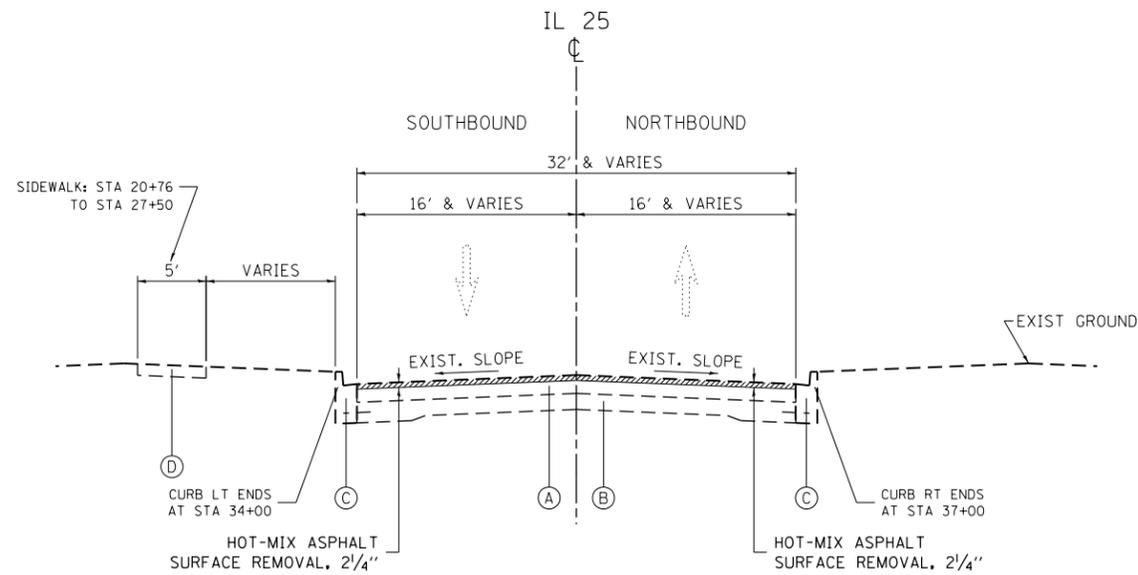
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PLOT SCALE :	CHECKED :	REVISOR :	REVISOR :
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PLOT DATE :	DATE :	REVISION :	REVISION :
3/31/2017			

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

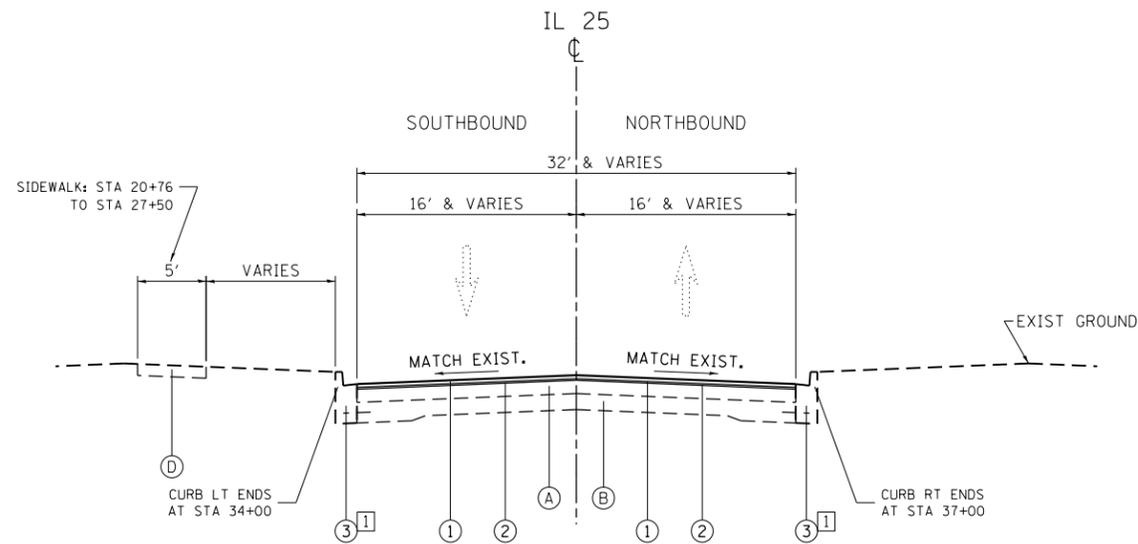
**IL 25 - JOHNSON AVE TO SOUTH OF BREWSTER CREEK BRIDGE
SUMMARY OF QUANTITIES**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	5
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

SCALE: SHEET NO. OF SHEETS STA. TO STA.



EXISTING TYPICAL SECTION
STA 20+76 (PROJECT BEGIN) TO STA 37+00



PROPOSED TYPICAL SECTION
STA 20+76 (PROJECT BEGIN) TO STA 37+00

[1] = LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.

LEGEND - EXISTING:

- (A) HMA SURFACE (8" AND VARIES)
- (B) PCC PAVEMENT (9" AND VARIES)
- (C) COMB. CONC. CURB & GUTTER, TYPE B-6.12
- (D) PCC SIDEWALK
- (E) HMA SHOULDER
- (F) AGGREGATE SHOULDER, TYPE B
- (G) FULL DEPTH HMA SURFACE
- (H) AGGREGATE SUBGRADE IMPROVEMENT
- (I) SHOULDER RUMBLE STRIPS (STA 156+20 TO STA 169+00)

NOTES:

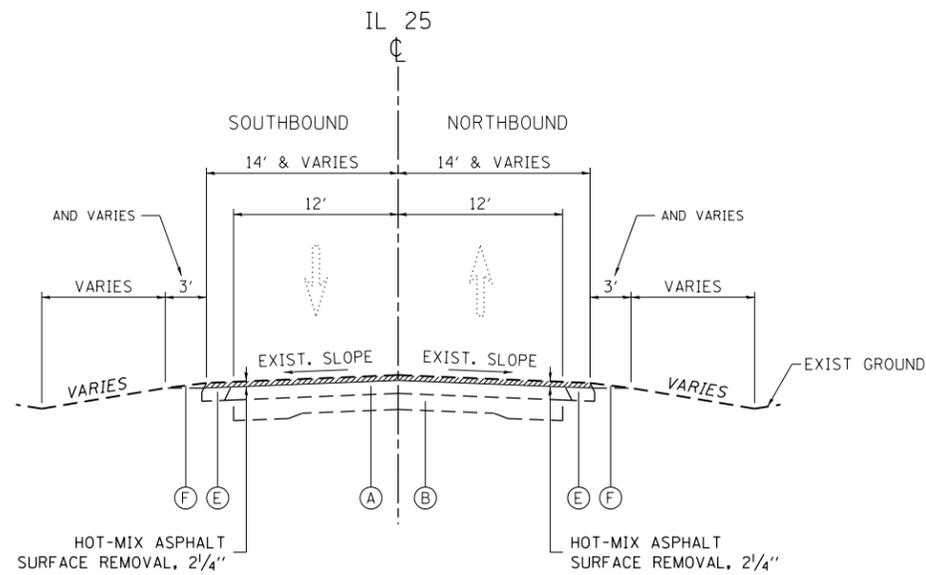
THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING

LEGEND - PROPOSED

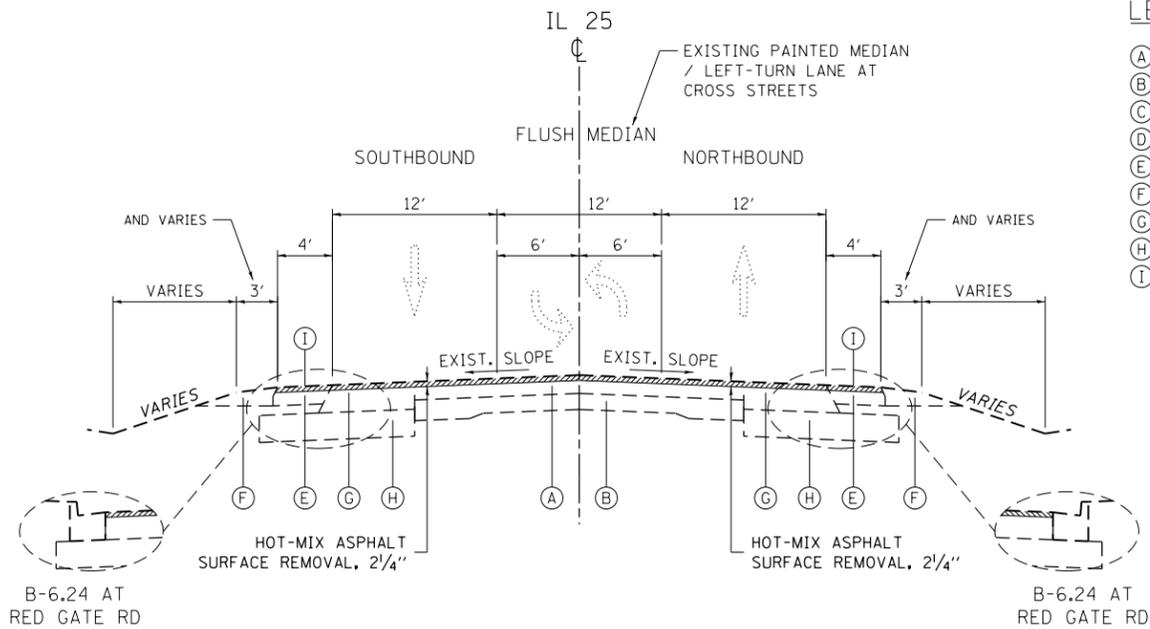
- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- (3) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- (4) GRADING AND SHAPING SHOULDERS
- (5) AGGREGATE SHOULDERS, TYPE B
- (6) SHOULDER RUMBLE STRIPS, 8 INCH (STA 156+20 TO STA 169+00)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS @ Ndes	
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	4% AT 70 GYR.	QCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% AT 50 GYR.	QCP
DRIVEWAYS		
HMA SURFACE COURSE, MIX D, N 50 (IL 9.5 mm); 2"	4% AT 50 GYR.	QC/OA
HMA BASE COURSE (HMA BINDER IL-19 mm); PE -6", CE - 8"	4% AT 50 GYR.	QC/OA
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/OA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (QCP)		

- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
- NOTE 3: QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.



EXISTING TYPICAL SECTION
STA STA 37+00 TO STA 215+24 (PROJECT END)



EXISTING TYPICAL SECTION
PAINTED MEDIAN / LEFT-TURN LANE
CARL LEE RD
PEARSON DR / ARMY TRAIL RD
RED GATE RD (SOUTH LEG)
COUNTRY CLUB RD

LEGEND - EXISTING:

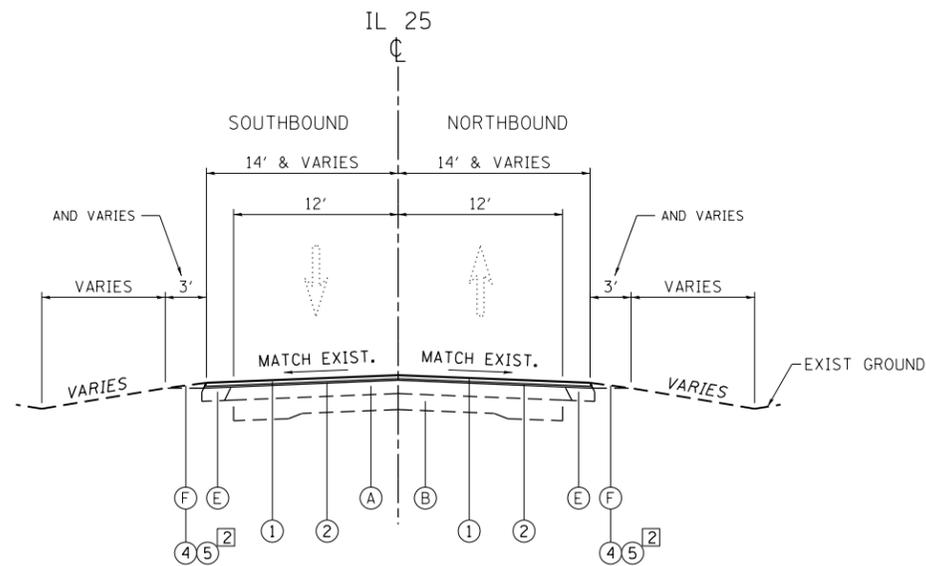
- (A) HMA SURFACE (8" AND VARIES)
- (B) PCC PAVEMENT (9" AND VARIES)
- (C) COMB. CONC. CURB & GUTTER, TYPE B-6.12
- (D) PCC SIDEWALK
- (E) HMA SHOULDER
- (F) AGGREGATE SHOULDER, TYPE B
- (G) FULL DEPTH HMA SURFACE
- (H) AGGREGATE SUBGRADE IMPROVEMENT
- (I) SHOULDER RUMBLE STRIPS (STA 156+20 TO STA 169+00)

LEGEND - PROPOSED

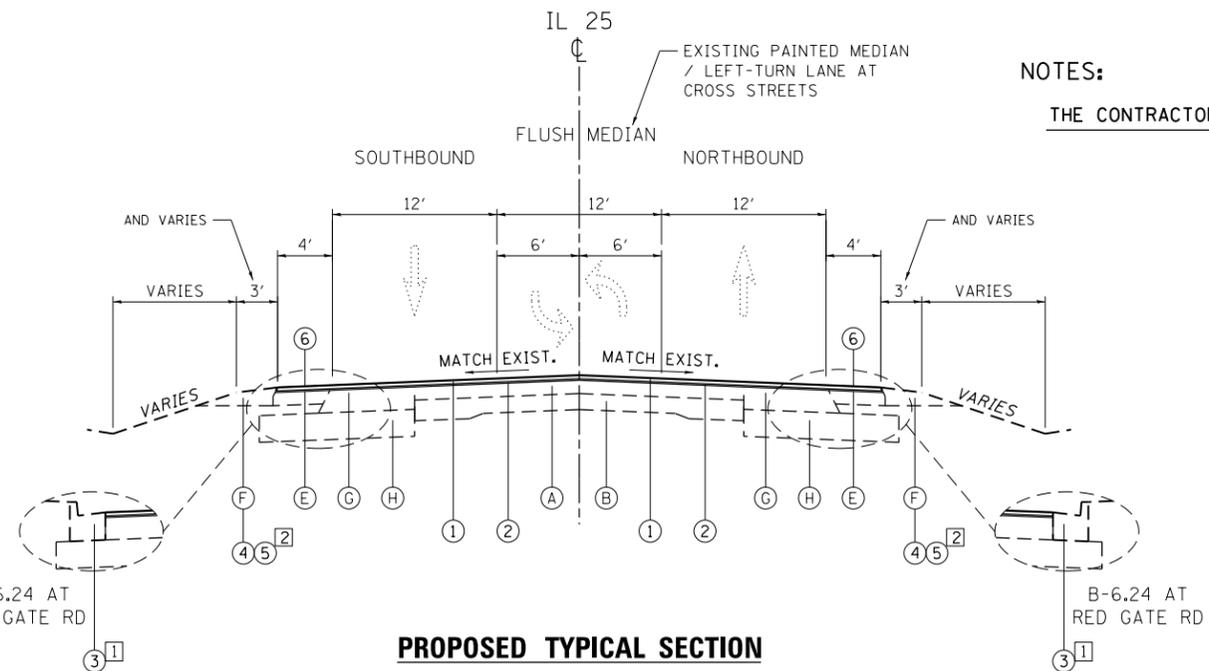
- ① HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ② POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ③ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- ④ GRADING AND SHAPING SHOULDERS
- ⑤ AGGREGATE SHOULDERS, TYPE B
- ⑥ SHOULDER RUMBLE STRIPS, 8 INCH (STA 156+20 TO STA 169+00)

NOTES:

THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING



PROPOSED TYPICAL SECTION
STA STA 37+00 TO STA 215+24 (PROJECT END)

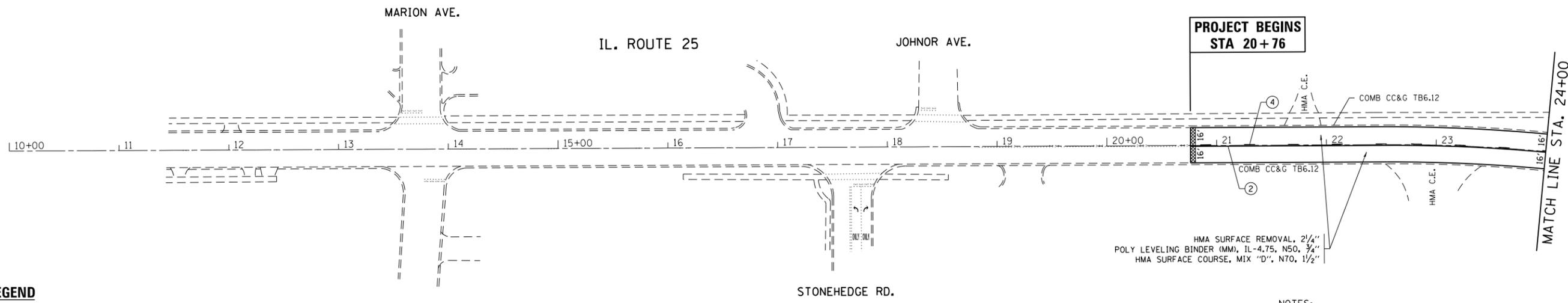
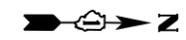


PROPOSED TYPICAL SECTION
PAINTED MEDIAN / LEFT-TURN LANE
CARL LEE RD
PEARSON DR / ARMY TRAIL RD
RED GATE RD (SOUTH LEG)
COUNTRY CLUB RD

① = LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.

② = LOCATIONS OF GRADING AND SHAPING SHOULDERS AND AGGREGATE SHOULDER, TYPE B WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 25 - JOHNOR AVE TO SOUTH OF BREWSTER CREEK BRIDGE TYPICAL SECTIONS	F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			2503	(38, 43 & 49) RS-7	KANE	36	9	
	PLOT DATE = 3/30/2017	DATE -	REVISED -			CONTRACT NO. 62B90					
						ILLINOIS FED. AID PROJECT					



ROADWAY LEGEND

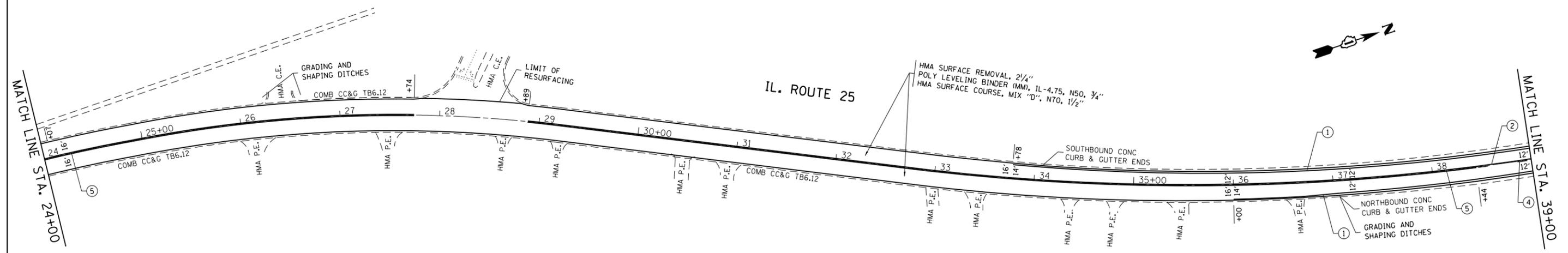
☒ HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

PROPOSED PAVEMENT MARKINGS LEGEND

- ① THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)
- ③ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, WHITE (TYP.)
- ④ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, YELLOW (TYP.)
- ⑤ THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ⑥ THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)
- ⑦ THERMOPLASTIC PVMT. MARKING 6", 2' LINE - 6' SPACE, WHITE (TYP.)
- ⑧ THERMOPLASTIC PVMT. MARKING 6", CROSSWALK-PEDESTRIAN, WHITE (TYP.)
- ⑨ THERMOPLASTIC PVMT. MARKING 8", LINE, WHITE (TYP.)
- ⑩ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING
- ⑪ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING
- ⑫ THERMOPLASTIC PVMT. MARKING 12", CROSSWALK-SCHOOL, WHITE (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING
- ⑬ THERMOPLASTIC PVMT. MARKING 24", STOP LINE, SOLID, WHITE (TYP.)
- ⑭ THERMOPLASTIC PVMT. MARKING, LETTERS & SYMBOLS, WHITE (TYP.)
- ⑮ RAISED REFLECTIVE PVMT. MARKER (TYP.)

NOTES:

- 1) LIMIT OF RESURFACING ON THE SIDE-STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
- 2) ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
- 3) ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
- 4) ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.



FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -
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Default	PLOT DATE = 3/30/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 25 - JOHNOR AVE TO SOUTH OF BREWSTER CREEK BRIDGE
ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	11
CONTRACT NO. 62B90				
ILLINOIS FED. AID PROJECT				

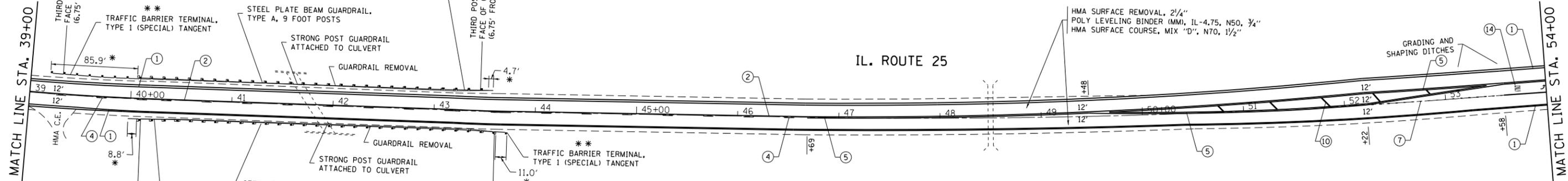
SOUTHBOUND GUARDRAIL REMOVAL AND REPLACEMENT:

- GUARDRAIL REMOVAL = 350 LF
- STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS = 312.5 LF
- STRONG POST GUARDRAIL ATTACHED TO CULVERT = 25 LF
- TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT = 2 EA
- TERMINAL MARKER - DIRECT APPLIED = 2 EA
- GUARDRAIL REFLECTORS, TYPE A = 4 EA

SET FACE OF GUARDRAIL 6.0 FT FROM EDGE-OF-PAVEMENT

* = DISTANCE TO EXISTING GUARDRAIL

** = SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS PAID AS:
 - AGGREGATE SHOULDERS, TYPE B 4"
 - EXCAVATING AND GRADING EXISTING SHOULDER



NORTHBOUND GUARDRAIL REMOVAL AND REPLACEMENT:

- GUARDRAIL REMOVAL = 360 LF
- STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS = 250 LF
- STRONG POST GUARDRAIL ATTACHED TO CULVERT = 25 LF
- TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT = 2 EA
- TERMINAL MARKER - DIRECT APPLIED = 2 EA
- GUARDRAIL REFLECTORS, TYPE A = 4 EA

SET FACE OF GUARDRAIL 6.0 FT FROM EDGE-OF-PAVEMENT

* = DISTANCE TO EXISTING GUARDRAIL

ROADWAY LEGEND

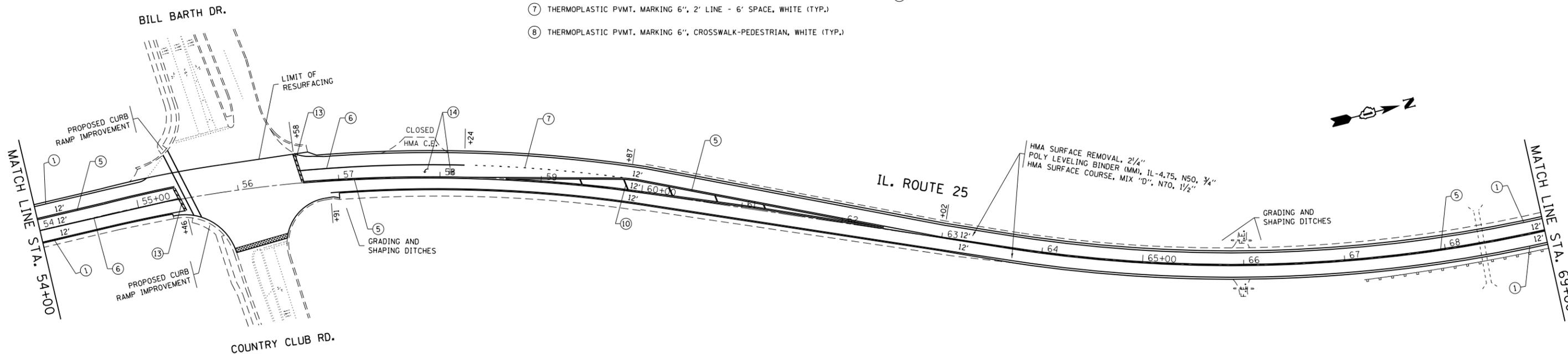
☒ HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

PROPOSED PAVEMENT MARKINGS LEGEND

- ① THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)
- ③ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, WHITE (TYP.)
- ④ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, YELLOW (TYP.)
- ⑤ THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ⑥ THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)
- ⑦ THERMOPLASTIC PVMT. MARKING 6", 2' LINE - 6' SPACE, WHITE (TYP.)
- ⑧ THERMOPLASTIC PVMT. MARKING 6", CROSSWALK-PEDESTRIAN, WHITE (TYP.)
- ⑨ THERMOPLASTIC PVMT. MARKING 8", LINE, WHITE (TYP.)
- ⑩ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.) - SEE DISTRICT DETAIL TC-13 FOR SPACING
- ⑪ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.) - SEE DISTRICT DETAIL TC-13 FOR SPACING
- ⑫ THERMOPLASTIC PVMT. MARKING 12", CROSSWALK-SCHOOL, WHITE (TYP.) - SEE DISTRICT DETAIL TC-13 FOR SPACING
- ⑬ THERMOPLASTIC PVMT. MARKING 24", STOP LINE, SOLID, WHITE (TYP.)
- ⑭ THERMOPLASTIC PVMT. MARKING, LETTERS & SYMBOLS, WHITE (TYP.)
- ⑮ RAISED REFLECTIVE PVMT. MARKER (TYP.)

NOTES:

- LIMIT OF RESURFACING ON THE SIDE-STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
- ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
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- ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.



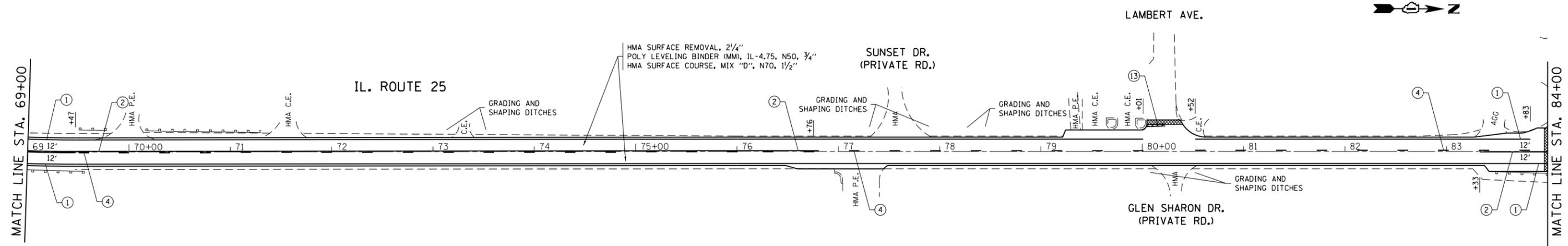
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 25 - JOHNOR AVE TO SOUTH OF BREWSTER CREEK BRIDGE
ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	12
CONTRACT NO. 62B90				
ILLINOIS FED. AID PROJECT				



ROADWAY LEGEND

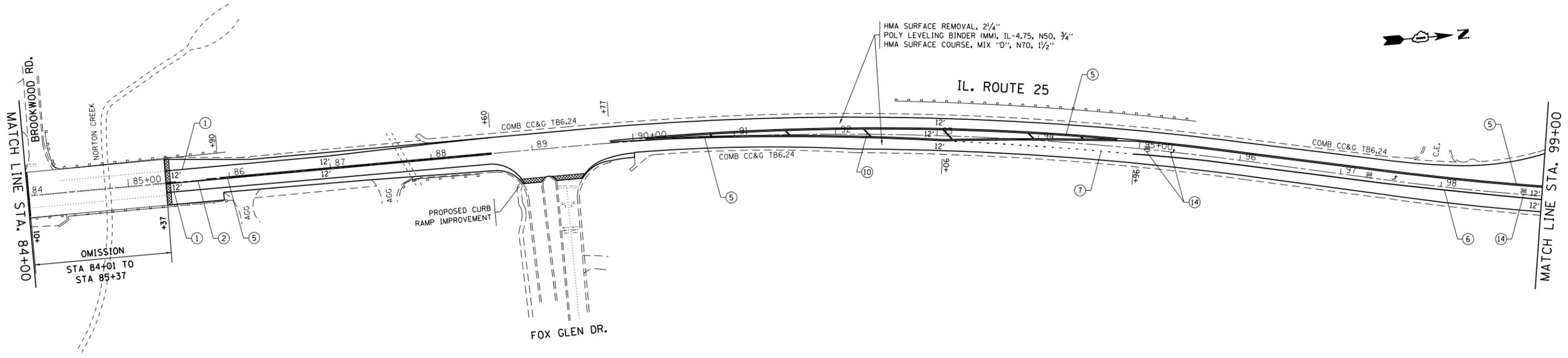
☒ HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

PROPOSED PAVEMENT MARKINGS LEGEND

- | | | |
|---|--|--|
| ① THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.) | ⑥ THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.) | ⑪ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING |
| ② THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.) | ⑦ THERMOPLASTIC PVMT. MARKING 6", 2' LINE - 6' SPACE, WHITE (TYP.) | ⑫ THERMOPLASTIC PVMT. MARKING 12", CROSSWALK-SCHOOL, WHITE (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING |
| ③ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, WHITE (TYP.) | ⑧ THERMOPLASTIC PVMT. MARKING 6", CROSSWALK-PEDESTRIAN, WHITE (TYP.) | ⑬ THERMOPLASTIC PVMT. MARKING 24", STOP LINE, SOLID, WHITE (TYP.) |
| ④ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, YELLOW (TYP.) | ⑨ THERMOPLASTIC PVMT. MARKING 8", LINE, WHITE (TYP.) | ⑭ THERMOPLASTIC PVMT. MARKING, LETTERS & SYMBOLS, WHITE (TYP.) |
| ⑤ THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.) | ⑩ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING | ⑮ RAISED REFLECTIVE PVMT. MARKER (TYP.) |

NOTES:

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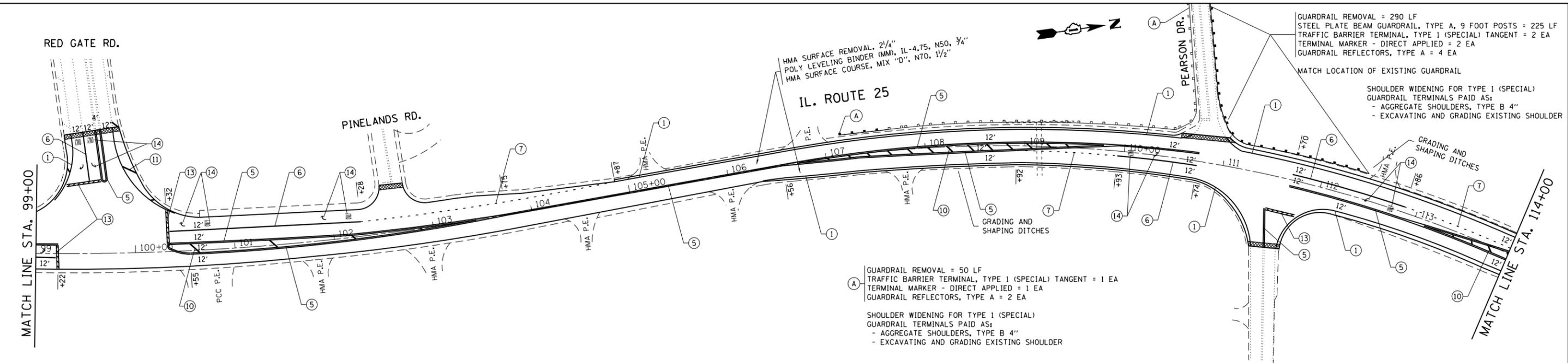
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 25 - JOHNOR AVE TO SOUTH OF BREWSTER CREEK BRIDGE
ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	13
CONTRACT NO. 62B90				
ILLINOIS FED. AID PROJECT				



GUARDRAIL REMOVAL = 290 LF
 STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS = 225 LF
 TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT = 2 EA
 TERMINAL MARKER - DIRECT APPLIED = 2 EA
 GUARDRAIL REFLECTORS, TYPE A = 4 EA

MATCH LOCATION OF EXISTING GUARDRAIL

SHOULDER WIDENING FOR TYPE 1 (SPECIAL)
 GUARDRAIL TERMINALS PAID AS:
 - AGGREGATE SHOULDERS, TYPE B 4"
 - EXCAVATING AND GRADING EXISTING SHOULDER

A GUARDRAIL REMOVAL = 50 LF
 TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT = 1 EA
 TERMINAL MARKER - DIRECT APPLIED = 1 EA
 GUARDRAIL REFLECTORS, TYPE A = 2 EA

SHOULDER WIDENING FOR TYPE 1 (SPECIAL)
 GUARDRAIL TERMINALS PAID AS:
 - AGGREGATE SHOULDERS, TYPE B 4"
 - EXCAVATING AND GRADING EXISTING SHOULDER

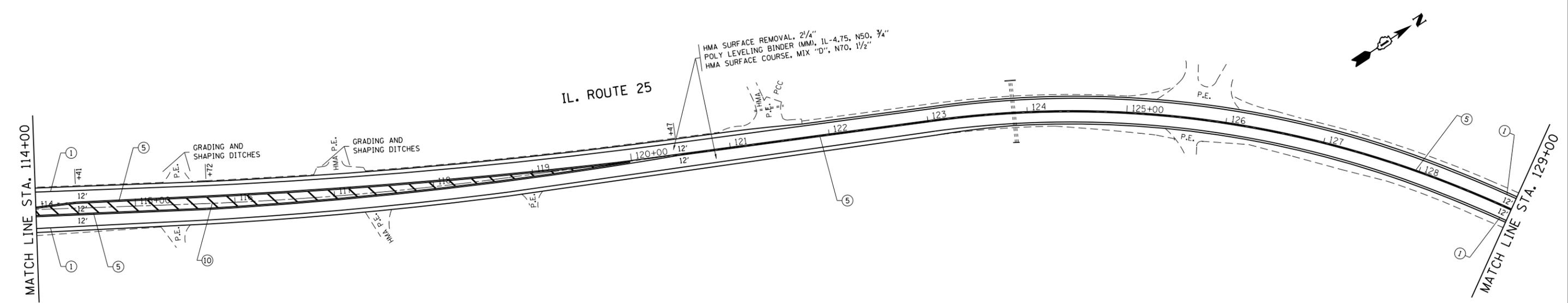
ROADWAY LEGEND

☒ HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

PROPOSED PAVEMENT MARKINGS LEGEND

- ① THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)
- ③ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, WHITE (TYP.)
- ④ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, YELLOW (TYP.)
- ⑤ THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ⑥ THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)
- ⑦ THERMOPLASTIC PVMT. MARKING 6", 2' LINE - 6' SPACE, WHITE (TYP.)
- ⑧ THERMOPLASTIC PVMT. MARKING 6", CROSSWALK-PEDESTRIAN, WHITE (TYP.)
- ⑨ THERMOPLASTIC PVMT. MARKING 8", LINE, WHITE (TYP.)
- ⑩ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
 - SEE DISTRICT DETAIL TC-13 FOR SPACING
- ⑪ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
 - SEE DISTRICT DETAIL TC-13 FOR SPACING
- ⑫ THERMOPLASTIC PVMT. MARKING 12", CROSSWALK-SCHOOL, WHITE (TYP.)
 - SEE DISTRICT DETAIL TC-13 FOR SPACING
- ⑬ THERMOPLASTIC PVMT. MARKING 24", STOP LINE, SOLID, WHITE (TYP.)
- ⑭ THERMOPLASTIC PVMT. MARKING, LETTERS & SYMBOLS, WHITE (TYP.)
- ⑮ RAISED REFLECTIVE PVMT. MARKER (TYP.)

- NOTES:**
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FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -
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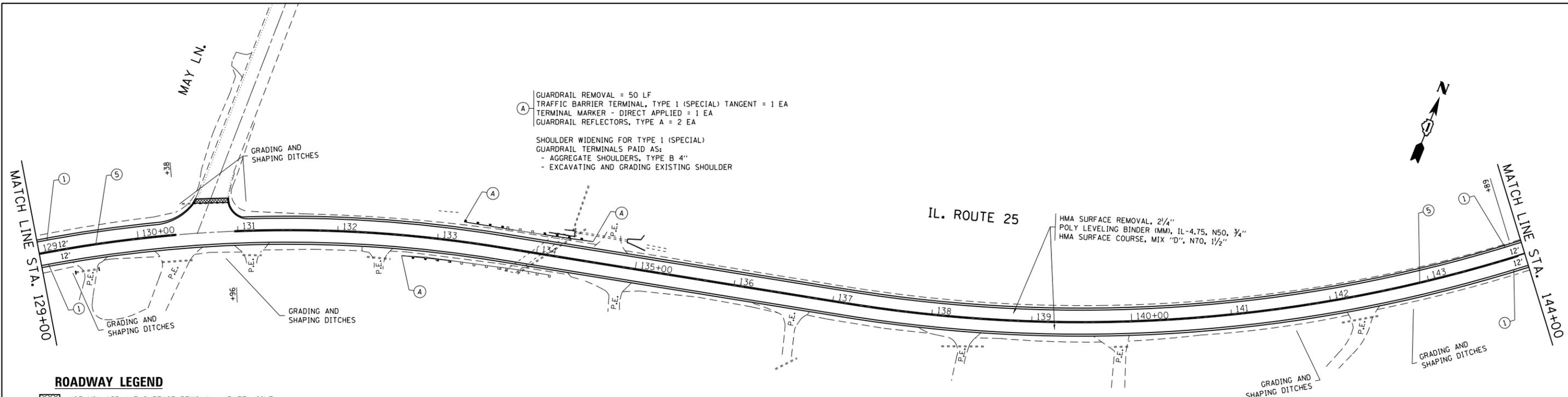
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL 25 - JOHNOR AVE TO SOUTH OF BREWSTER CREEK BRIDGE
 ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: 1" = 50'

SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	14
CONTRACT NO. 62B90				
ILLINOIS FED. AID PROJECT				



(A) GUARDRAIL REMOVAL = 50 LF
 TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT = 1 EA
 TERMINAL MARKER - DIRECT APPLIED = 1 EA
 GUARDRAIL REFLECTORS, TYPE A = 2 EA

 SHOULDER WIDENING FOR TYPE 1 (SPECIAL)
 GUARDRAIL TERMINALS PAID AS:
 - AGGREGATE SHOULDERS, TYPE B 4"
 - EXCAVATING AND GRADING EXISTING SHOULDER

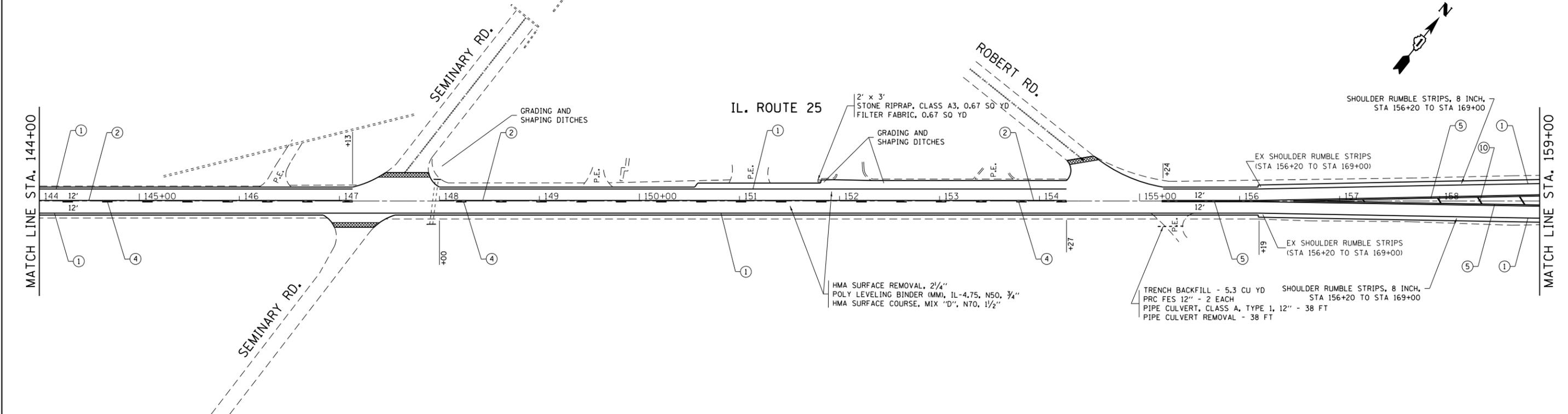
ROADWAY LEGEND

HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

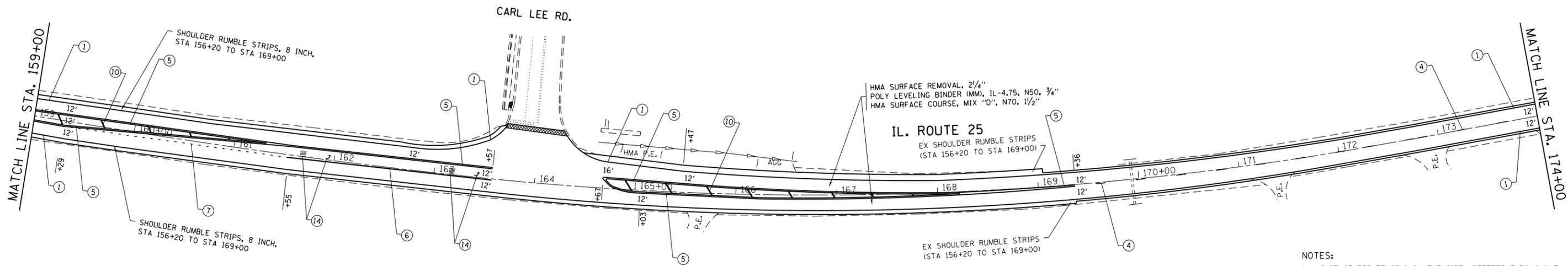
PROPOSED PAVEMENT MARKINGS LEGEND

- | | | |
|---|--|--|
| ① THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.) | ⑥ THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.) | ⑪ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING |
| ② THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.) | ⑦ THERMOPLASTIC PVMT. MARKING 6", 2' LINE - 6' SPACE, WHITE (TYP.) | ⑫ THERMOPLASTIC PVMT. MARKING 12", CROSSWALK-SCHOOL, WHITE (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING |
| ③ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, WHITE (TYP.) | ⑧ THERMOPLASTIC PVMT. MARKING 6", CROSSWALK-PEDESTRIAN, WHITE (TYP.) | ⑬ THERMOPLASTIC PVMT. MARKING 24", STOP LINE, SOLID, WHITE (TYP.) |
| ④ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, YELLOW (TYP.) | ⑨ THERMOPLASTIC PVMT. MARKING 8", LINE, WHITE (TYP.) | ⑭ THERMOPLASTIC PVMT. MARKING, LETTERS & SYMBOLS, WHITE (TYP.) |
| ⑤ THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.) | ⑩ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING | ⑮ RAISED REFLECTIVE PVMT. MARKER (TYP.) |

- NOTES:**
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FILE NAME =	USER NAME = Vajchkovv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 25 - JOHNOR AVE TO SOUTH OF BREWSTER CREEK BRIDGE ROADWAY AND PAVEMENT MARKING PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\IL084EBIDINTEG\Illinois.gov\PWIDOT\Documents\IDOT\Offices\District 1\Projects\DI17516\Drawings\Design\DI17516-shr-plan.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			2503	(38, 43 & 49) RS-7	KANE	36	15	
Default	PLOT DATE = 3/31/2017	DATE -	REVISED -			CONTRACT NO. 62B90					
						ILLINOIS FED. AID PROJECT					



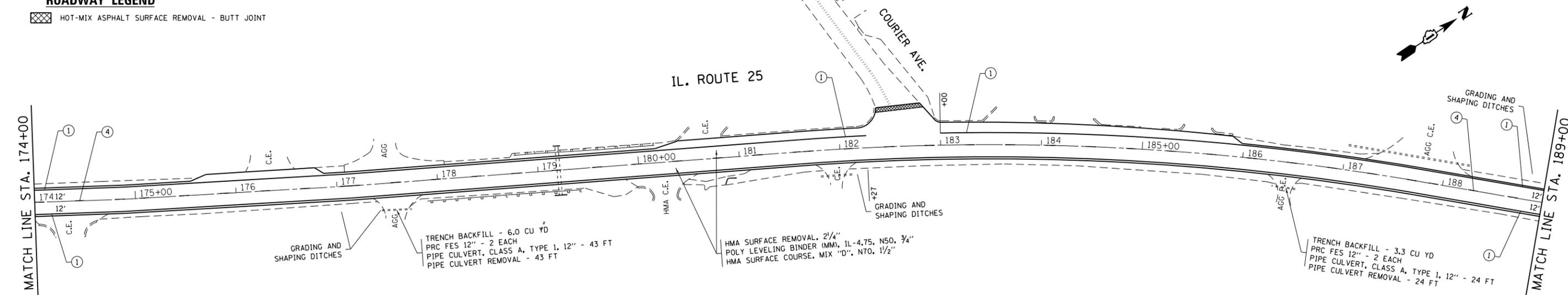
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PROPOSED PAVEMENT MARKINGS LEGEND

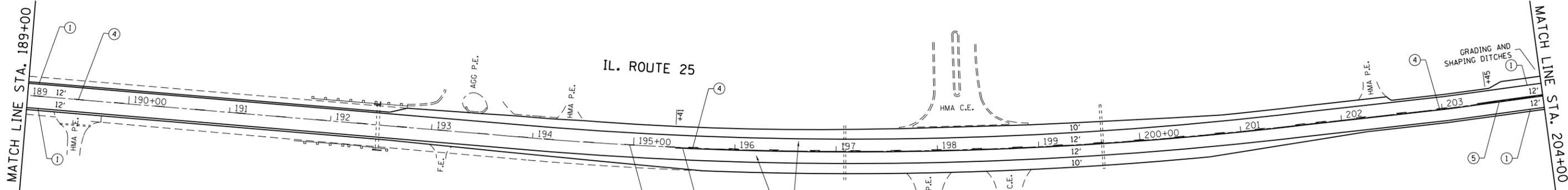
- | | | |
|---|--|--|
| ① THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.) | ⑥ THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.) | ⑪ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING |
| ② THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.) | ⑦ THERMOPLASTIC PVMT. MARKING 6", 2' LINE - 6' SPACE, WHITE (TYP.) | ⑫ THERMOPLASTIC PVMT. MARKING 12", CROSSWALK-SCHOOL, WHITE (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING |
| ③ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, WHITE (TYP.) | ⑧ THERMOPLASTIC PVMT. MARKING 6", CROSSWALK-PEDESTRIAN, WHITE (TYP.) | ⑬ THERMOPLASTIC PVMT. MARKING 24", STOP LINE, SOLID, WHITE (TYP.) |
| ④ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, YELLOW (TYP.) | ⑨ THERMOPLASTIC PVMT. MARKING 8", LINE, WHITE (TYP.) | ⑭ THERMOPLASTIC PVMT. MARKING, LETTERS & SYMBOLS, WHITE (TYP.) |
| ⑤ THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.) | ⑩ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING | ⑮ RAISED REFLECTIVE PVMT. MARKER (TYP.) |

ROADWAY LEGEND

☒ HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT



FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 25 - JOHNOR AVE TO SOUTH OF BREWSTER CREEK BRIDGE ROADWAY AND PAVEMENT MARKING PLAN	F.A.U. RT.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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Default	PLOT DATE = 3/30/2017	DATE -	REVISED -			CONTRACT NO. 62B90					
						ILLINOIS FED. AID PROJECT					



ROADWAY LEGEND

☒ HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

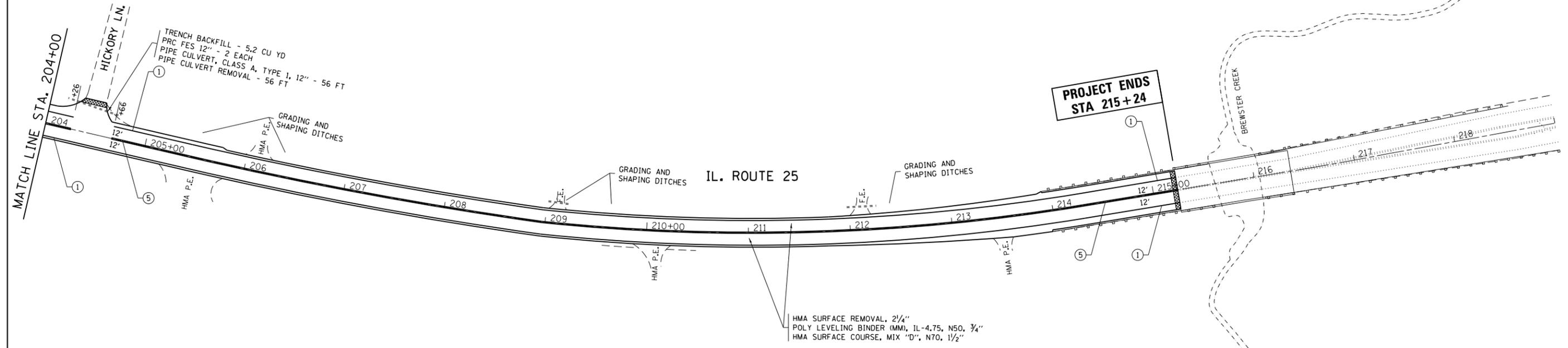
PROPOSED PAVEMENT MARKINGS LEGEND

- | | | |
|---|--|--|
| ① THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.) | ⑥ THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.) | ⑪ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
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- SEE DISTRICT DETAIL TC-13 FOR SPACING | ⑮ RAISED REFLECTIVE PVMT. MARKER (TYP.) |

NOTES:

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HMA SURFACE REMOVAL, 2 1/4"
POLY LEVELING BINDER (MM), IL-4.75, N50, 3/4"
HMA SURFACE COURSE, MIX "D", N70, 1 1/2"

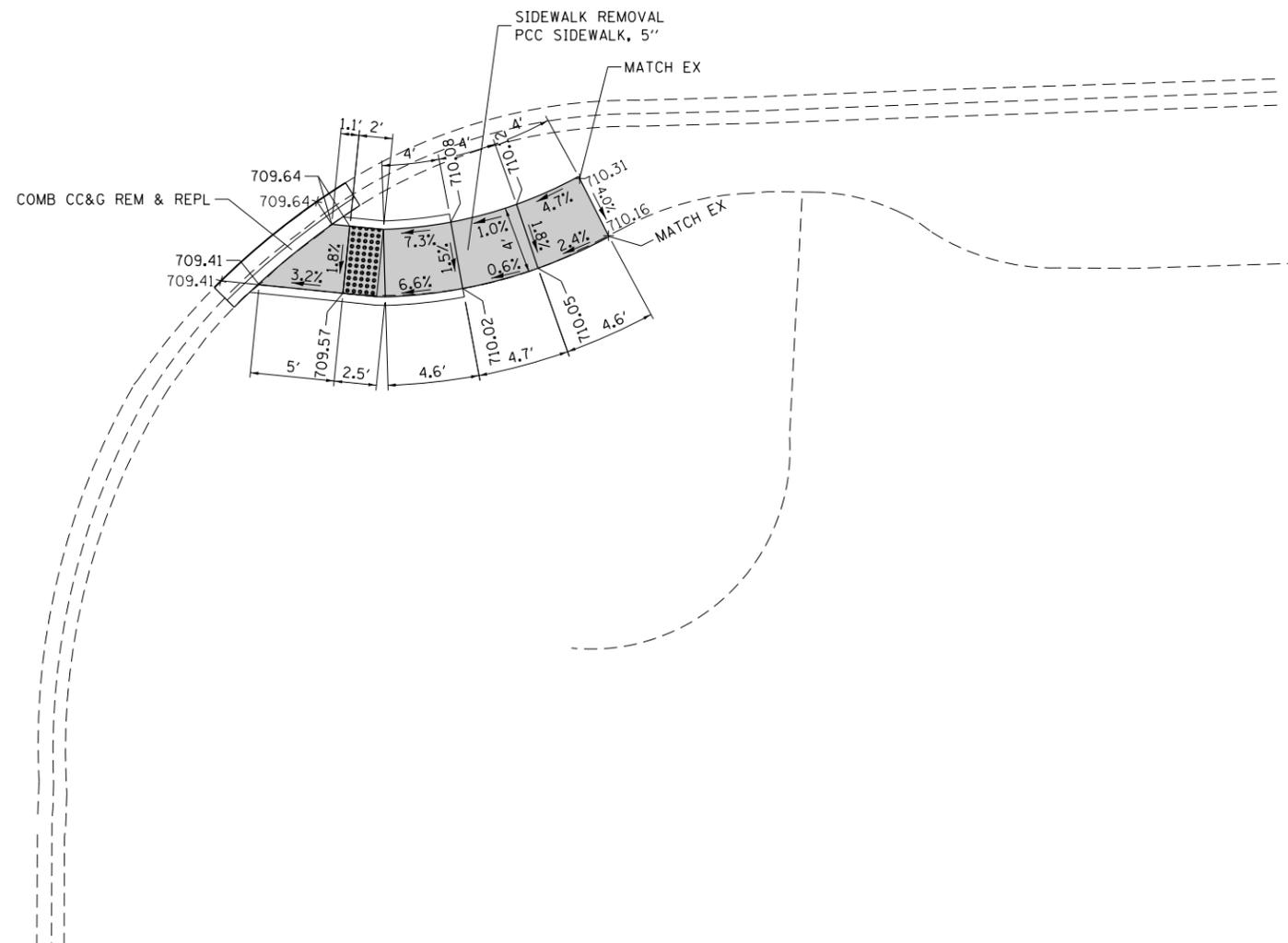


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pw\IL084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI17516\Drawings\Design\DI17516-shr-plan.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			2503	(38, 43 & 49) RS-7	KANE	36	17
Default	PLOT DATE = 3/30/2017	DATE -	REVISED -			CONTRACT NO. 62B90				
						SCALE: 1" = 50'	SHEET	OF	SHEETS	STA.



FOX GLEN DRIVE

IL. RTE. 25



SITE BENCHMARK
 CHISLED SQUARE ON NORTH
 END OF 25-FT HEADWALL
 EAST SIDE OF IL 25
 APPROX 100-FT SOUTH OF
 FOX GLEN DR
 ELEVATION = 702.252
 VERT DATUM: NAVD 88

LEGEND

- ×701.41 EXISTING ELEVATION
- PROPOSED SIDEWALK
- PROPOSED DETECTABLE WARNING
- PROPOSED SIDEWALK REMOVAL
REPLACE WITH TOPSOIL & SOD
- PROPOSED SIDE CURB
- 799.38 PROPOSED ELEVATION

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -
pw\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI17516\Drawings\Design\DI17516-shd-details.dgn		CHECKED -	REVISED -
Default	PLOT DATE = 3/31/2017	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL 25 - JOHNOR AVE TO SOUTH OF BREWSTER CREEK BRIDGE
 CURB RAMPS FOR SIDEWALKS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	18
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B90	

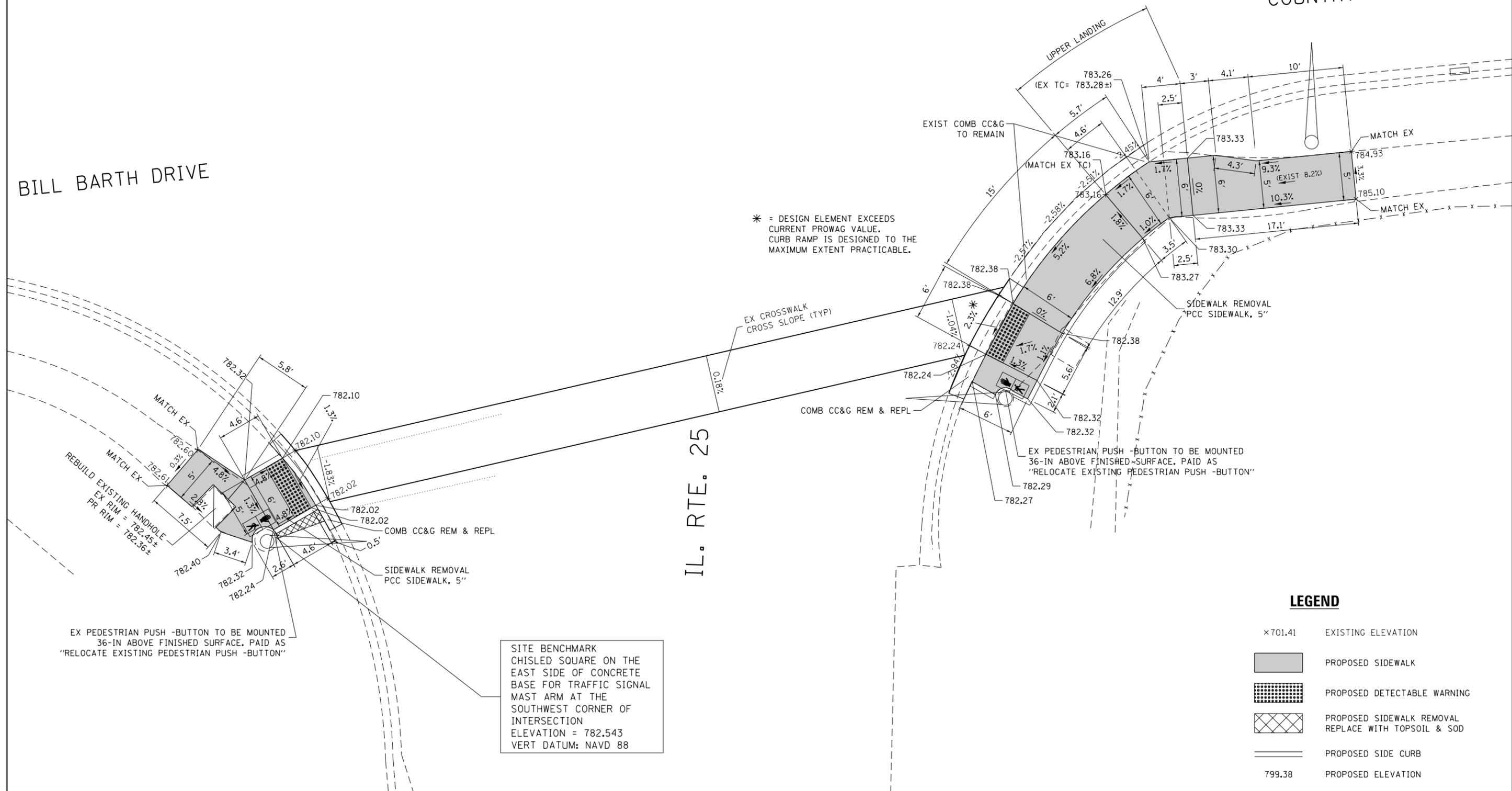


COUNTRY CLUB ROAD

BILL BARTH DRIVE

IL. RTE. 25

* = DESIGN ELEMENT EXCEEDS CURRENT PROWAG VALUE. CURB RAMP IS DESIGNED TO THE MAXIMUM EXTENT PRACTICABLE.



LEGEND

- × 701.41 EXISTING ELEVATION
-  PROPOSED SIDEWALK
-  PROPOSED DETECTABLE WARNING
-  PROPOSED SIDEWALK REMOVAL
REPLACE WITH TOPSOIL & SOD
-  PROPOSED SIDE CURB
- 799.38 PROPOSED ELEVATION

EX PEDESTRIAN PUSH -BUTTON TO BE MOUNTED 36-IN ABOVE FINISHED SURFACE. PAID AS "RELOCATE EXISTING PEDESTRIAN PUSH -BUTTON"

SITE BENCHMARK
CHISLED SQUARE ON THE
EAST SIDE OF CONCRETE
BASE FOR TRAFFIC SIGNAL
MAST ARM AT THE
SOUTHWEST CORNER OF
INTERSECTION
ELEVATION = 782.543
VERT DATUM: NAVD 88

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -
p:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\D117516\Drawings\Design\117516-shd-details.dgn		REVISOR -	REVISION -
Default	PLOT DATE = 4/3/2017	CHECKED -	REVISOR -
		DATE -	REVISION -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

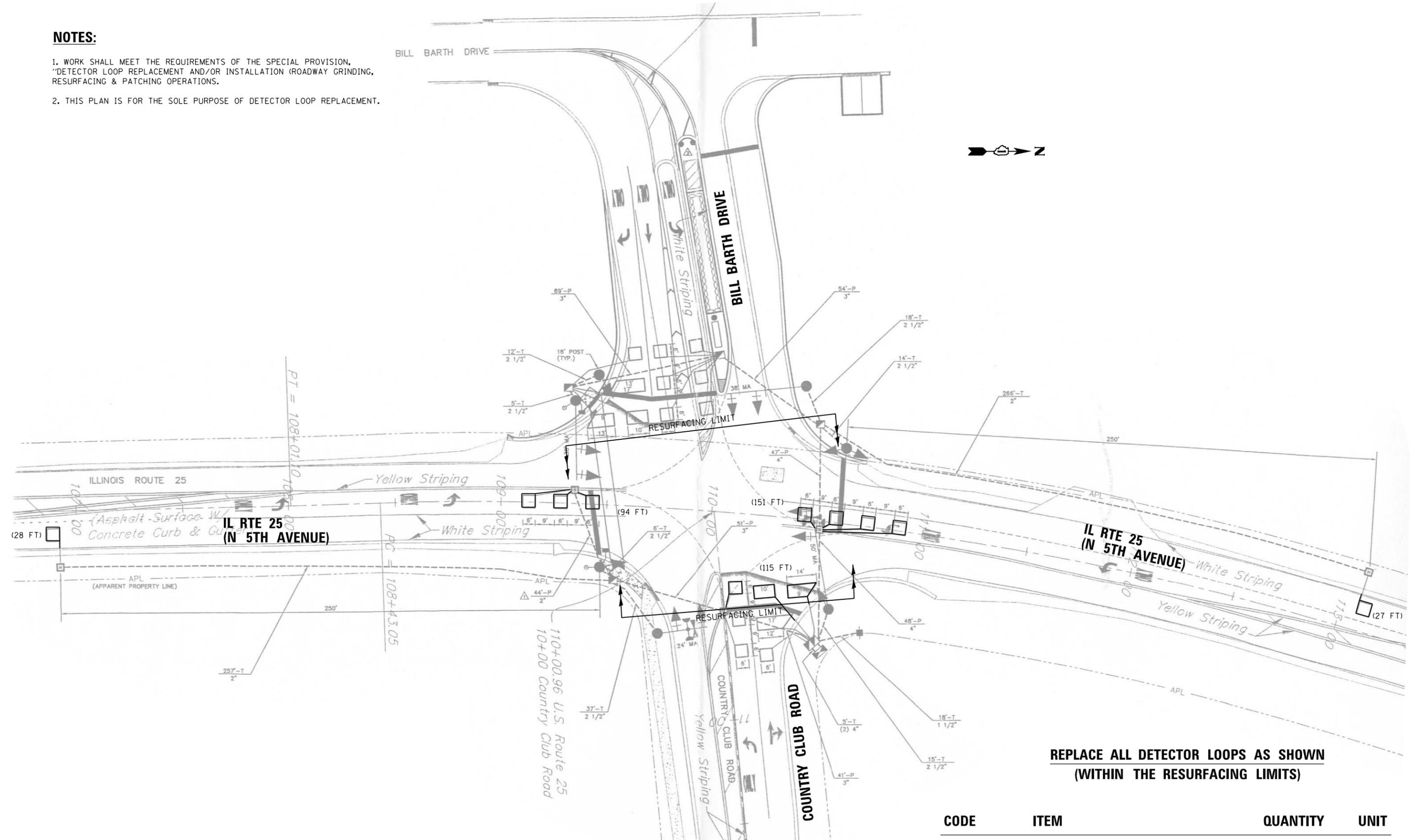
**IL 25 - JOHNOR AVE TO SOUTH OF BREWSTER CREEK BRIDGE
CURB RAMPS FOR SIDEWALKS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	19
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B90	

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	415	FOOT

FILE NAME =	USER NAME = javanmardizg	DESIGNED - ZGJ	REVISED -
TS 21996 ILL RTE 25 & COUNTRY CLUB ROAD.dgn	0.12222016.dgn	DRAWN - ZGJ	REVISED -
Default	PLOT SCALE = 40.0000' / in.	CHECKED - LP	REVISED -
	PLOT DATE = 12/22/2016	DATE - 12/22/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

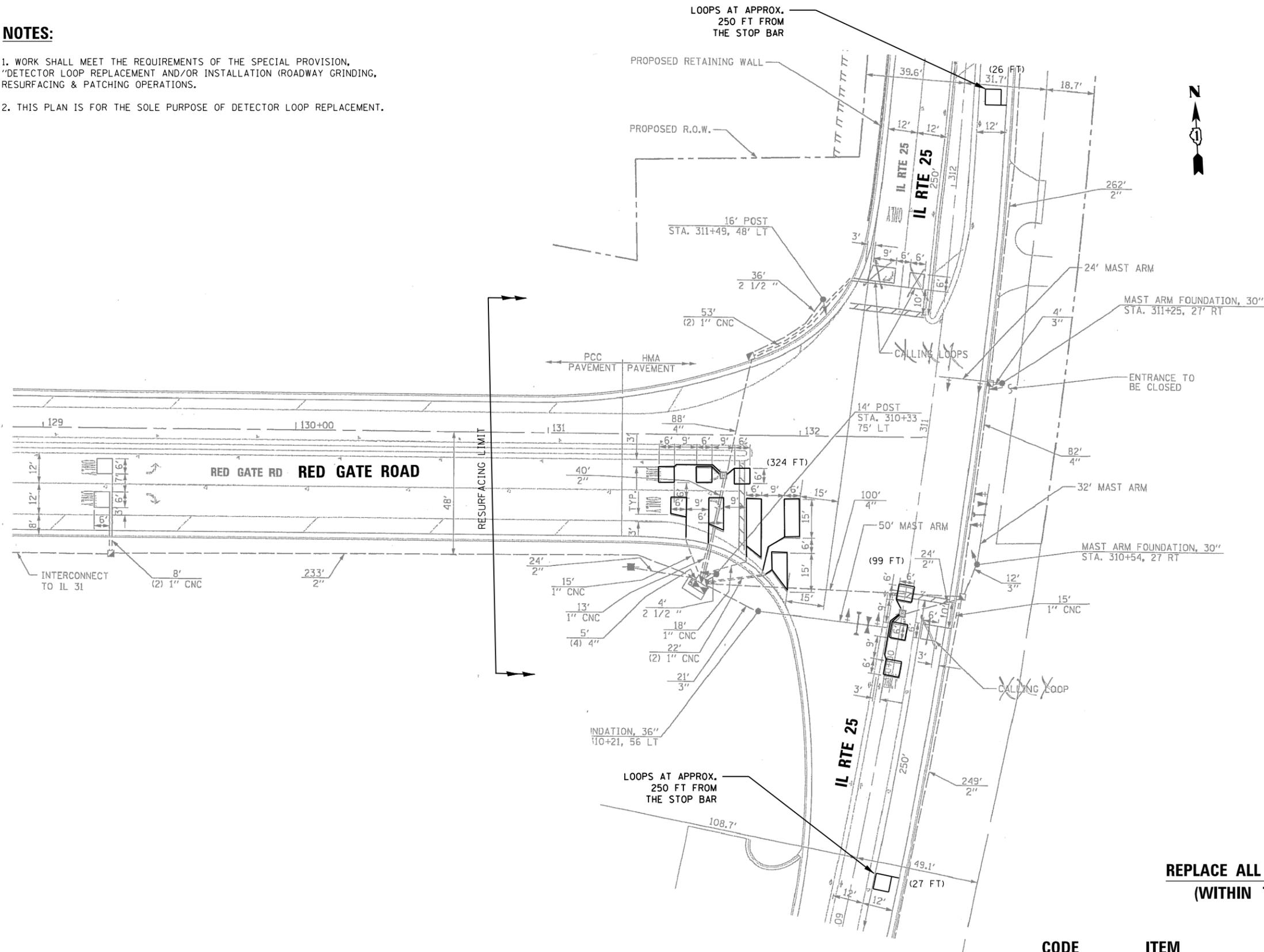
**DETECTOR LOOP REPLACEMENT PLAN
ILL RTE 25 (NORTH 5 AVE) AT COUNTRY CLUB ROAD (BILL BARTH DR)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	20
CONTRACT NO. 62B90				
ILLINOIS FED. AID PROJECT				

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	476	FOOT

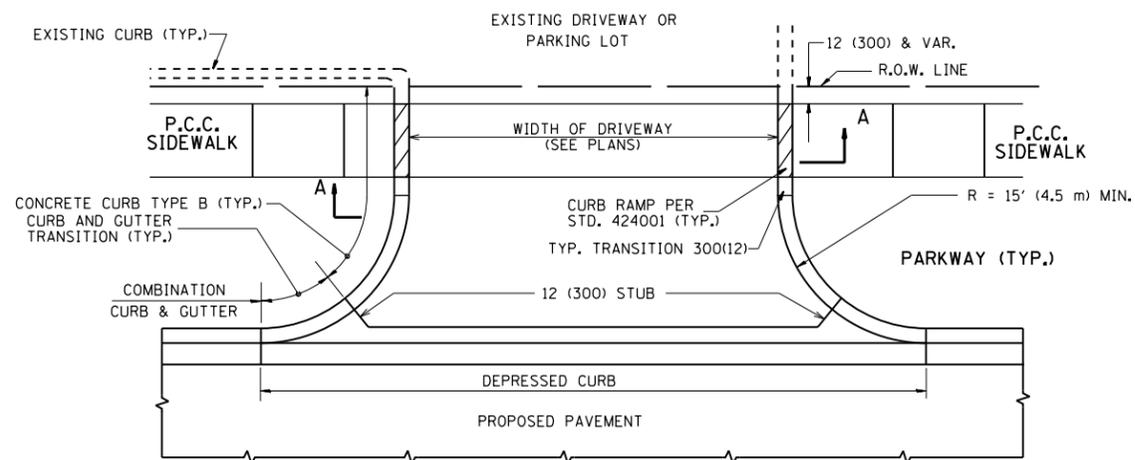
FILE NAME =	USER NAME = javanmardizg	DESIGNED - ZGJ	REVISED -
TS 21997 IL 25 & Red gate Road.03282017.dgn		DRAWN - ZGJ	REVISED -
Default	PLOT SCALE = 40.0000' / in.	CHECKED - LP	REVISED -
	PLOT DATE = 3/28/2017	DATE - 03/28/2017	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

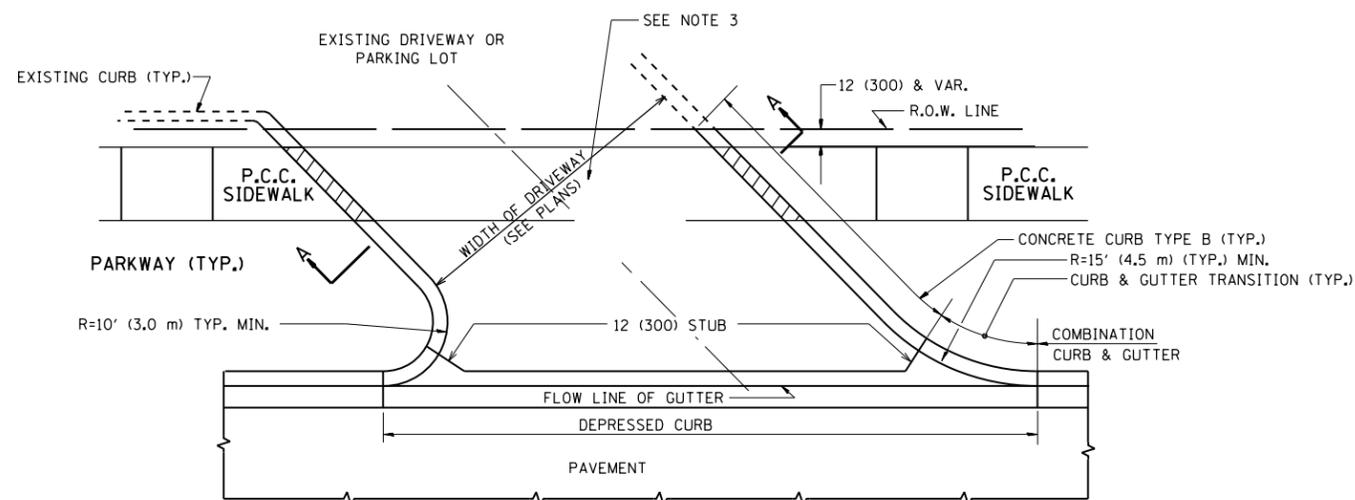
**DETECTOR LOOP REPLACEMENT PLAN
IL RTE 25 AT RED GATE ROAD**

SCALE: SHEET OF SHEETS STA. TO STA.

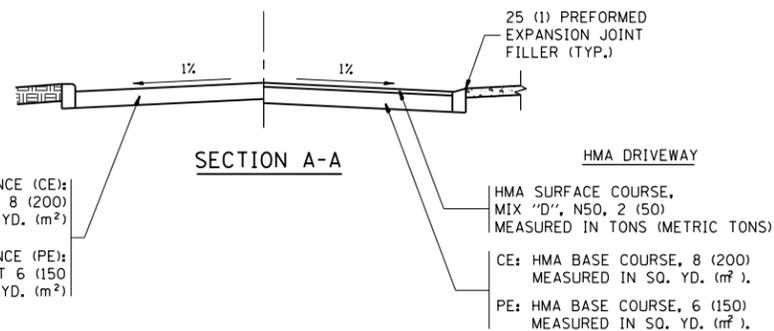
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	21
				CONTRACT NO. 62B90
ILLINOIS FED. AID PROJECT				



WITH CONCRETE CURB, TYPE B

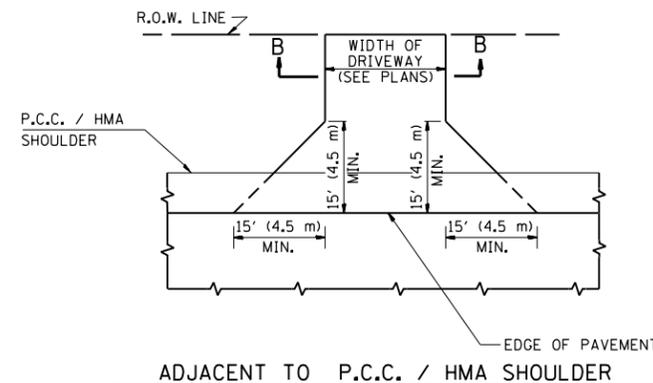


WITH CONCRETE CURB, TYPE B

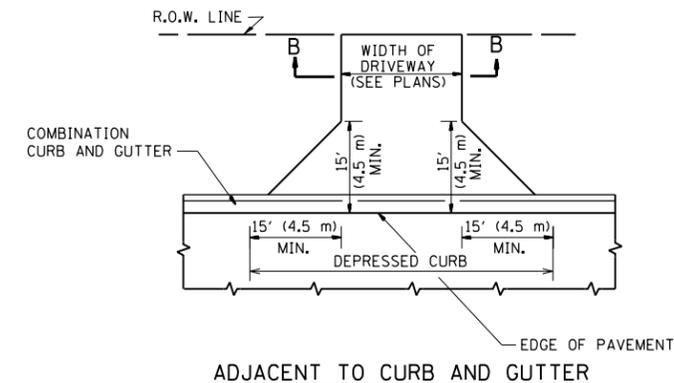


RIGID DRIVEWAY
 COMMERCIAL ENTRANCE (CE):
 P.C.C. DRIVEWAY PAVEMENT 8 (200)
 MEASURED IN SQ. YD. (m²)
 NON-COMMERCIAL ENTRANCE (PE):
 P.C.C. DRIVEWAY PAVEMENT 6 (150)
 MEASURED IN SQ. YD. (m²)

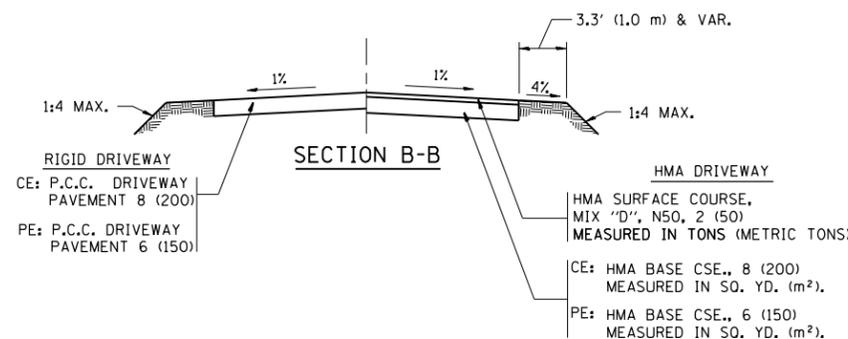
HMA DRIVEWAY
 HMA SURFACE COURSE,
 MIX "D", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 CE: HMA BASE COURSE, 8 (200)
 MEASURED IN SQ. YD. (m²),
 PE: HMA BASE COURSE, 6 (150)
 MEASURED IN SQ. YD. (m²).



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

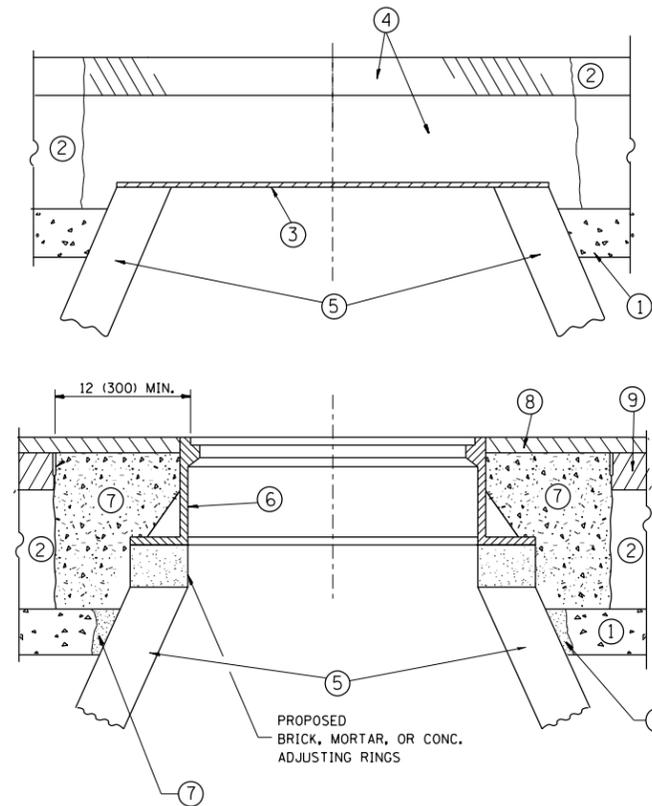
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME =	USER NAME = Velichkovv	DESIGNED - R. SHAH	REVISED - P. LaFLUER 04-15-03
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI1751\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - R. BORO 01-01-07
PLOT SCALE = 100.0000' / 1"		DATE - 11-04-95	REVISED - R. BORO 06-11-08
PLOT DATE = 3/30/2017			REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	22
BD0156-07 (BD-01)		CONTRACT NO. 62B90		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

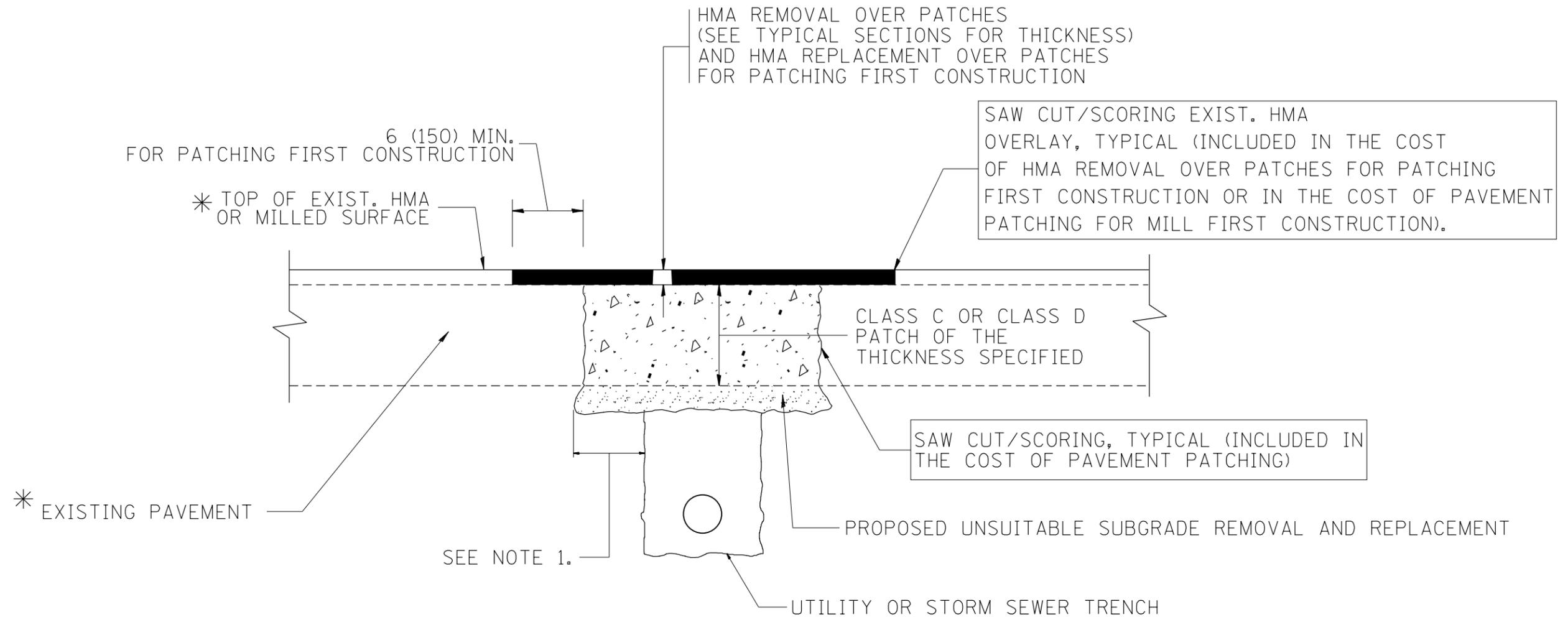
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = Velichkovv	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI1751\Drawings\Design\Diststd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 3/30/2017	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	24
BD600-03 (BD-8)		CONTRACT NO. 62B90		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

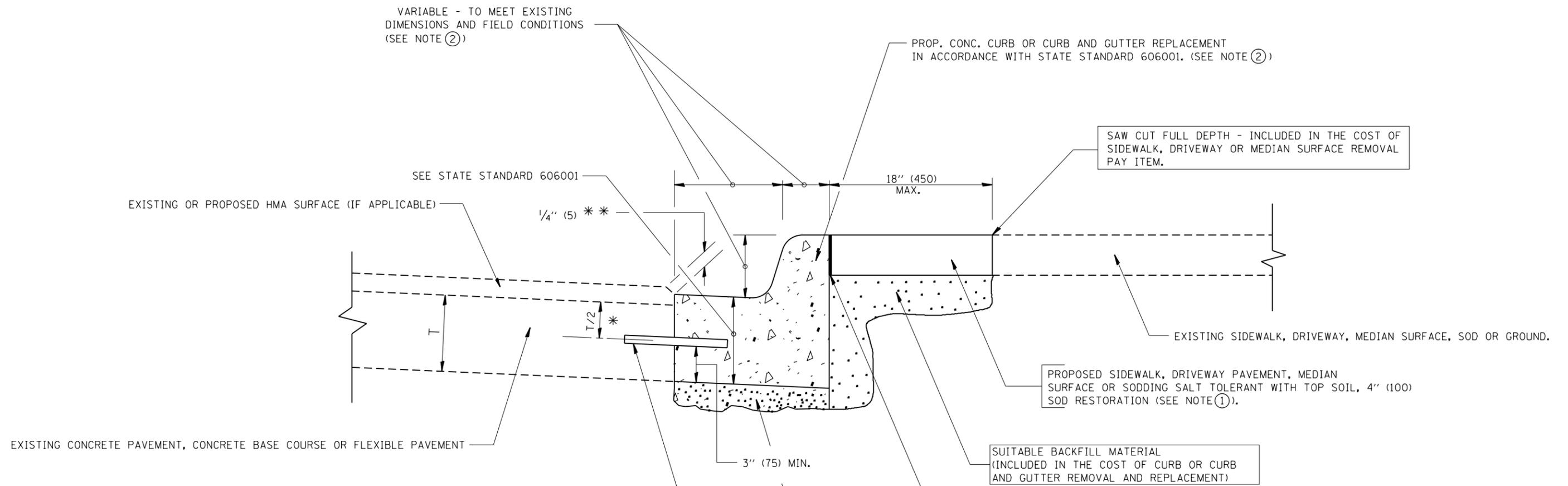
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Velichkovv	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI1751\Drawings\Design\Diststd.dgn	PLotted SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					2503	(38, 43 & 49) RS-7	KANE	36	25
	PLOT DATE = 3/30/2017	DATE - 10-25-94	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 62B90				
			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 - ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
 - ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
 - ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
 - ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
 - ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

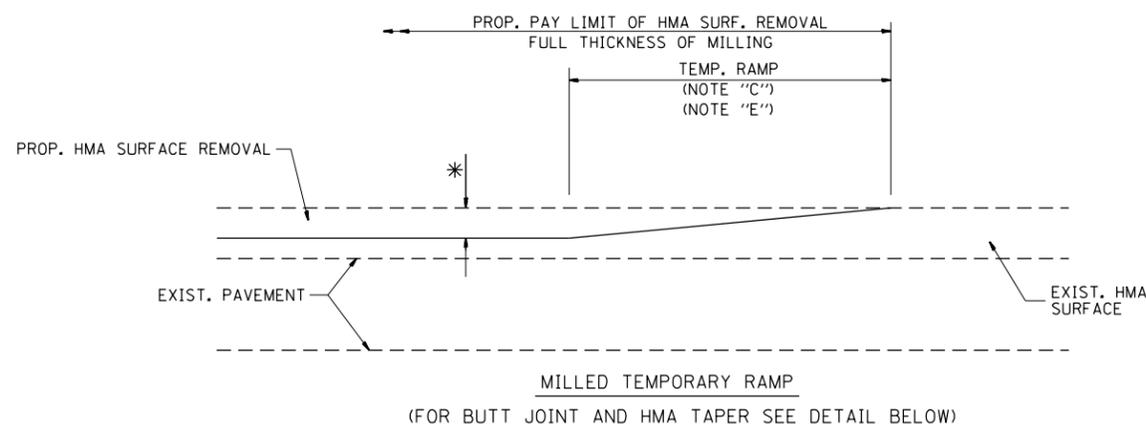
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

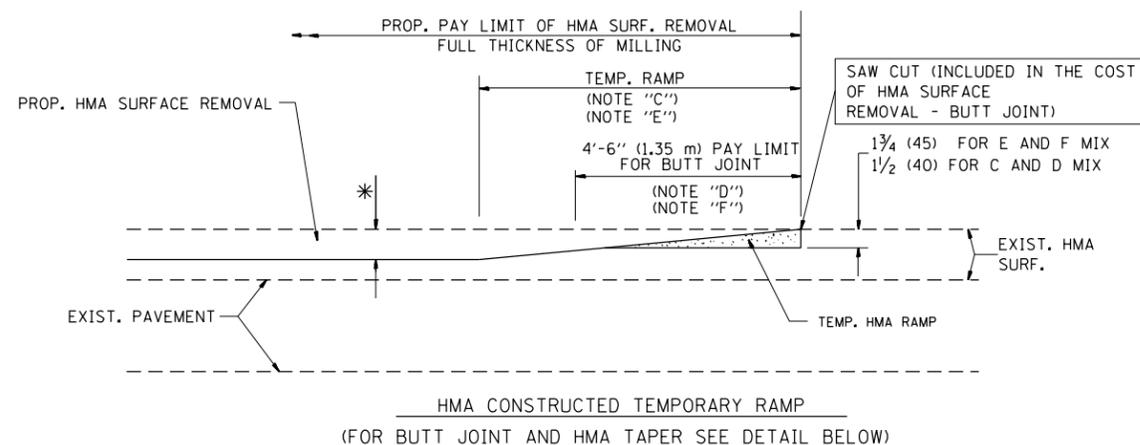
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Velichkovv	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
			REVISED - A. ABBAS 03-21-97			2503	(38, 43 & 49) RS-7	KANE	36	26	
			REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24)		CONTRACT NO. 62B90			
			REVISED - R. BORO 12-15-09			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
PLOT SCALE = 100.0000' / 1in.		CHECKED -	DATE - 03-11-94	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.				

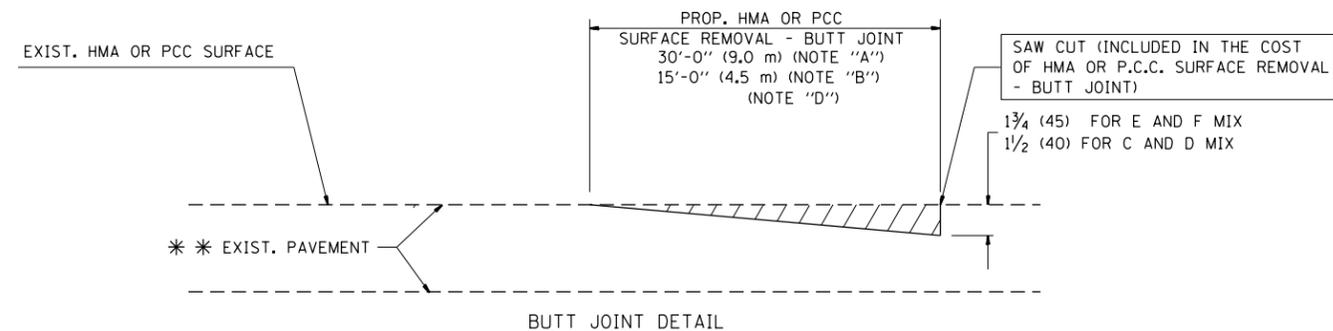


OPTION 1

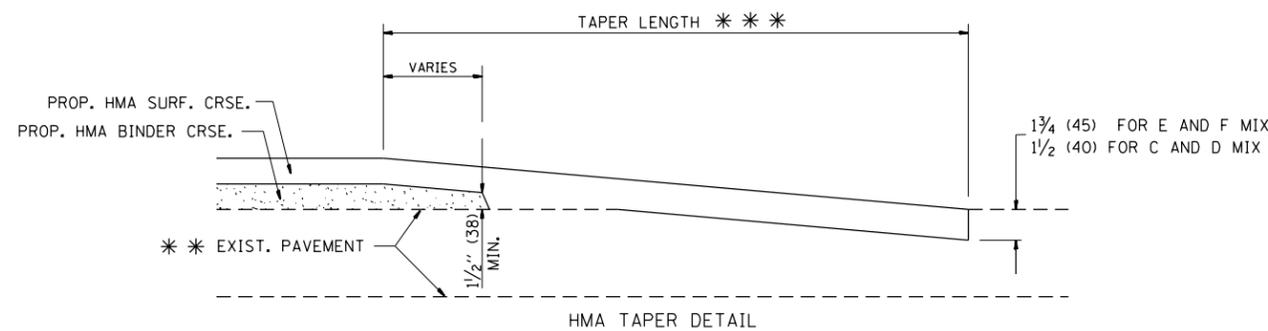


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

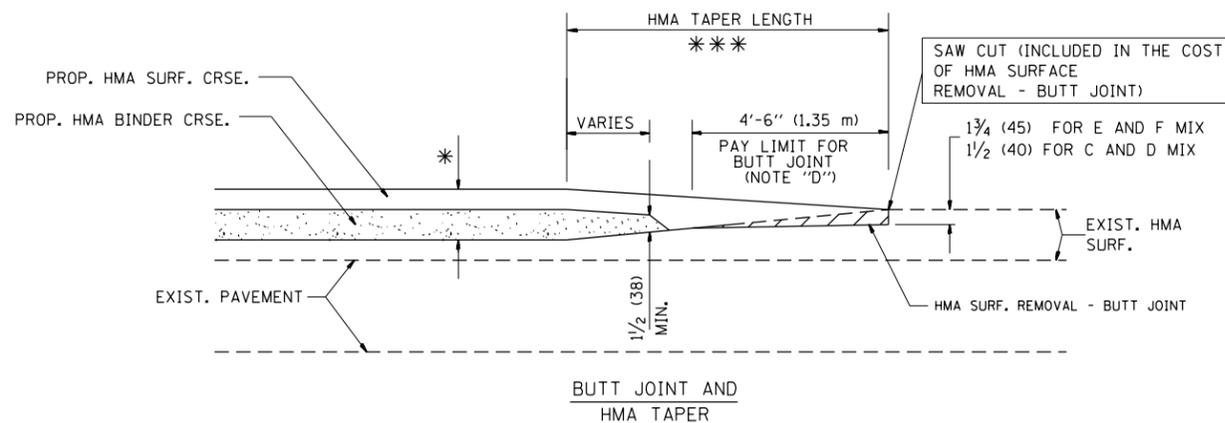
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

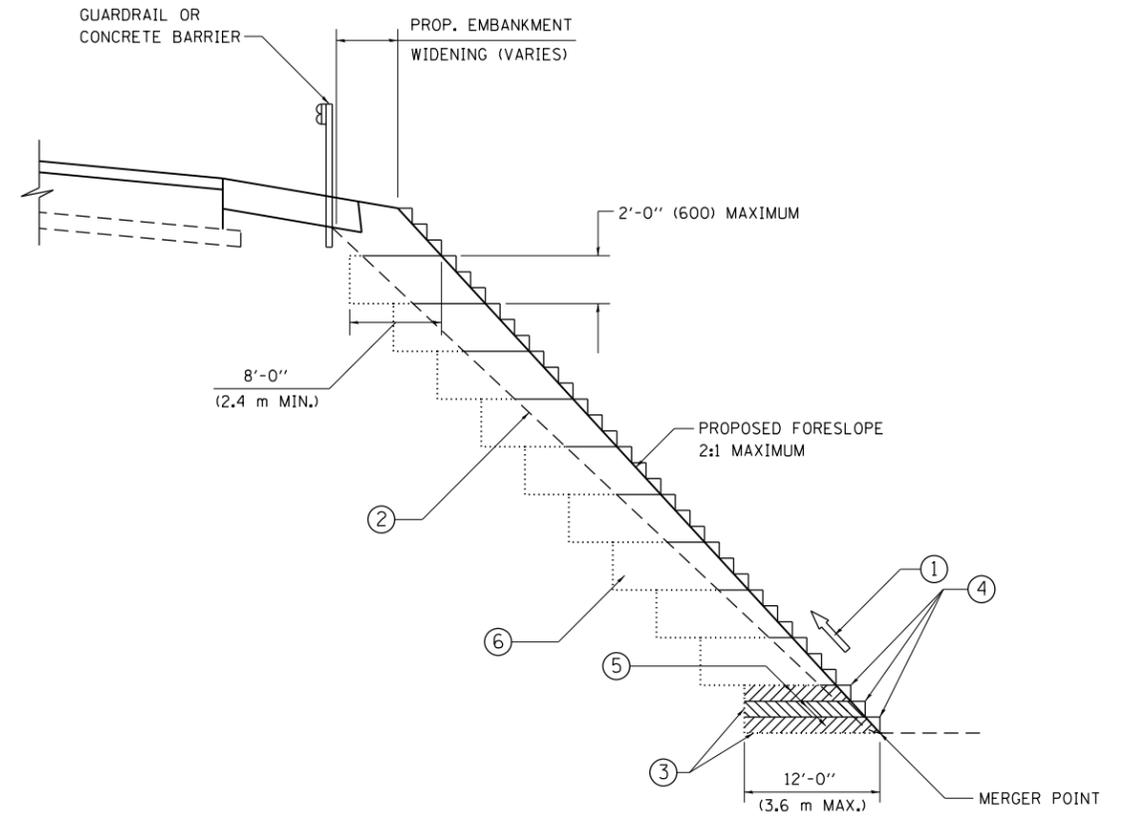
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	PLOT DATE = 3/30/2017	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	27
BD400-05 BD32		CONTRACT NO. 62B90		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

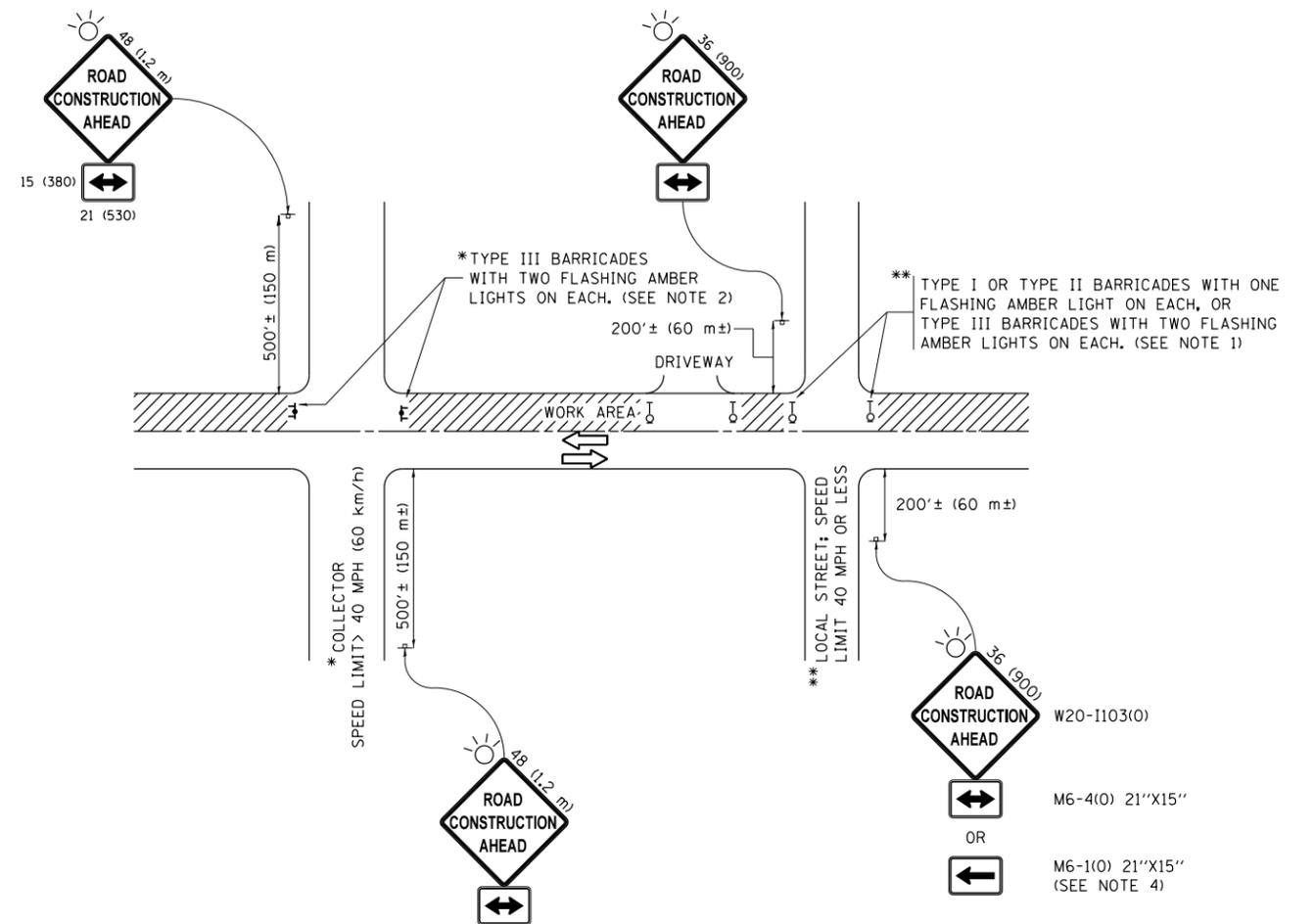
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	PLOT DATE = 3/30/2017	DATE - 06-16-04	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BENCHING DETAIL
FOR EMBANKMENT WIDENING**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	28
BD-51			CONTRACT NO. 62B90	
ILLINOIS FED. AID PROJECT				



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

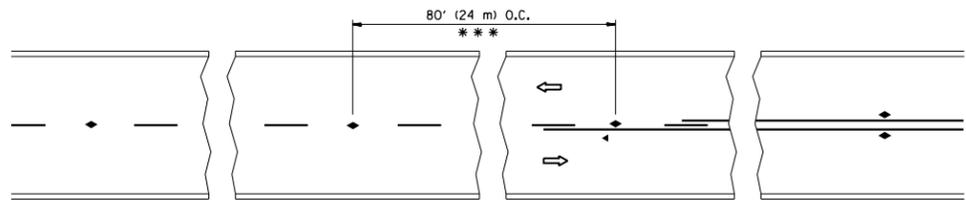
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 3/30/2017	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

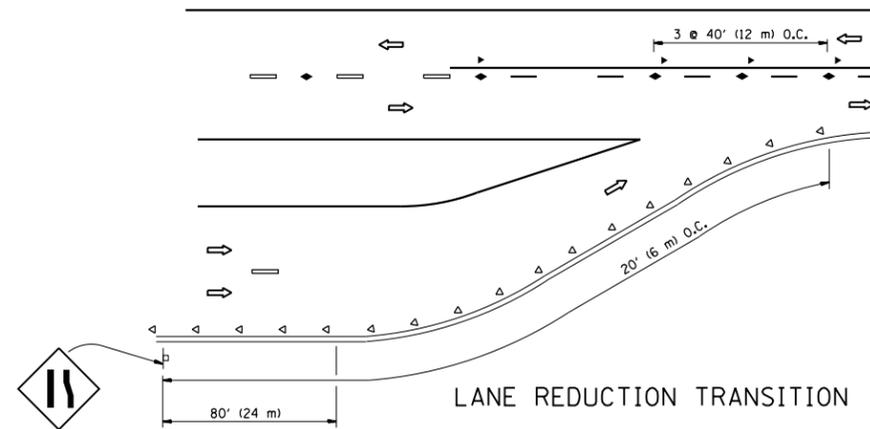
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F.A.U R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 62B90	
ILLINOIS FED. AID PROJECT				

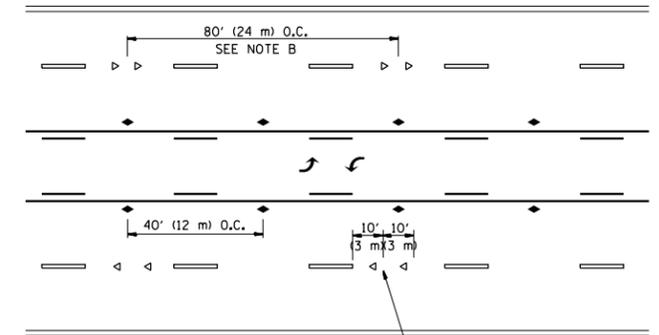


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

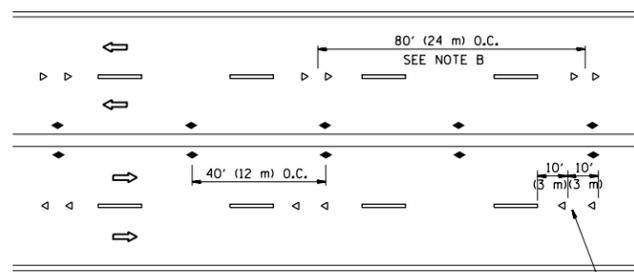
TWO-LANE/TWO-WAY



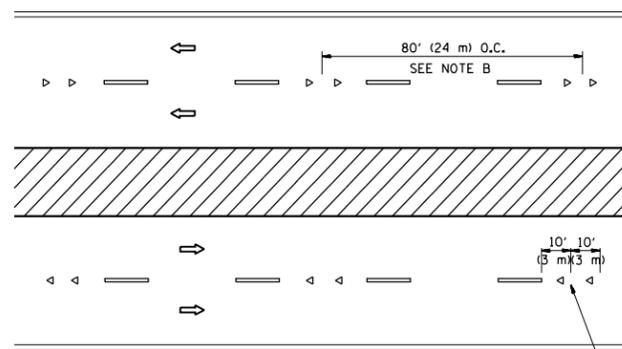
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

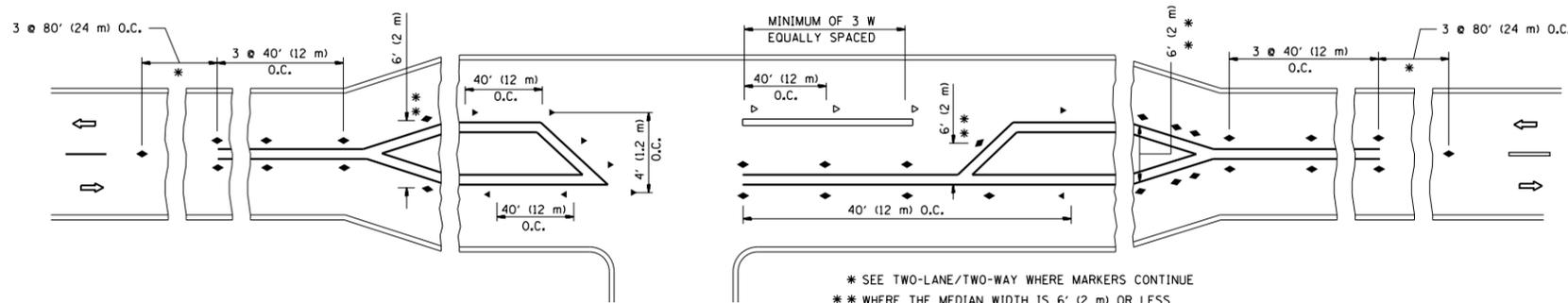
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

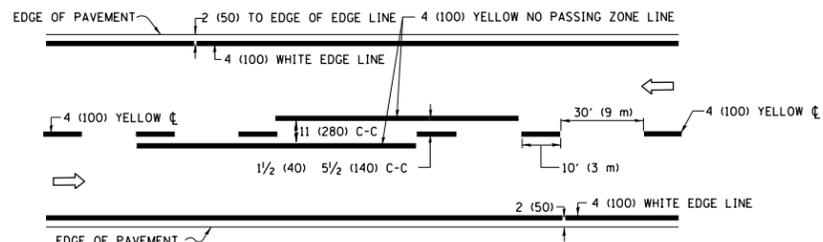
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 3/30/2017	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

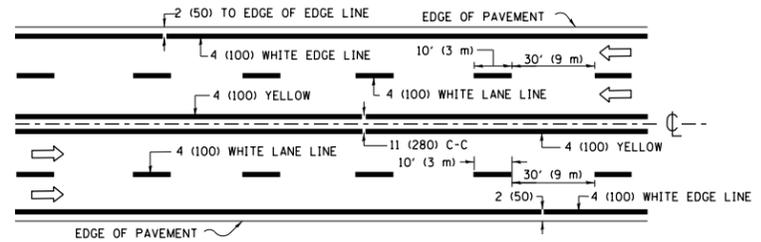
TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

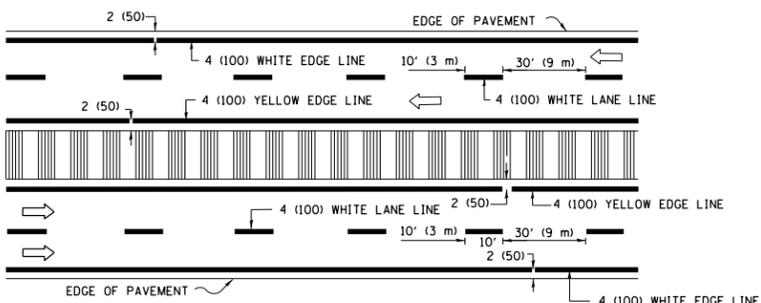
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	30
TC-11		CONTRACT NO. 62B90		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

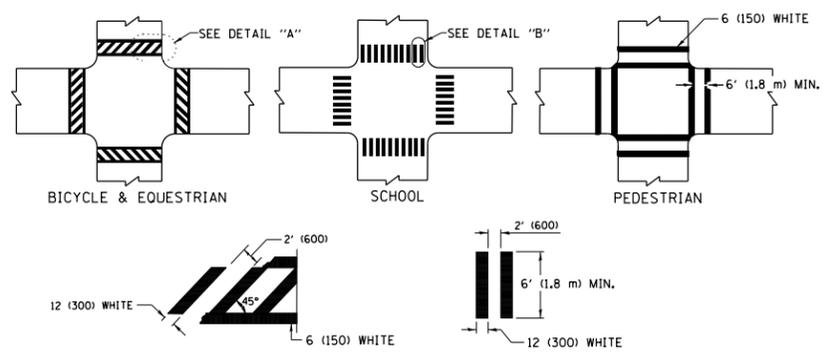


MULTI-LANE UNDIVIDED



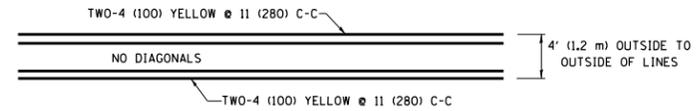
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

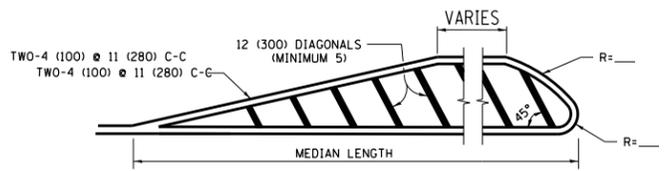


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

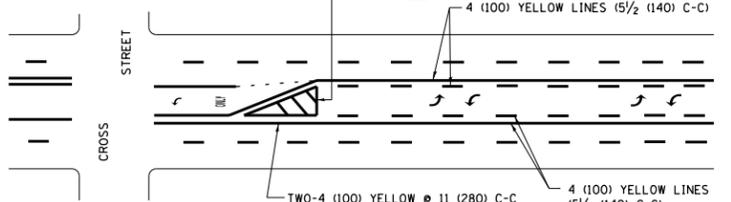


4' (1.2 m) WIDE MEDIANS ONLY



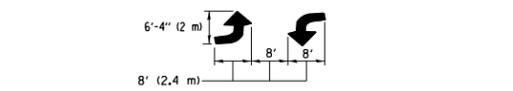
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

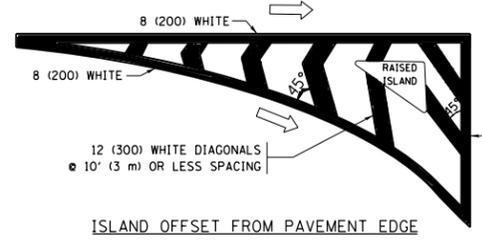
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



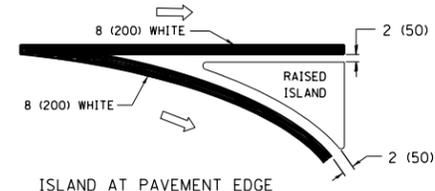
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

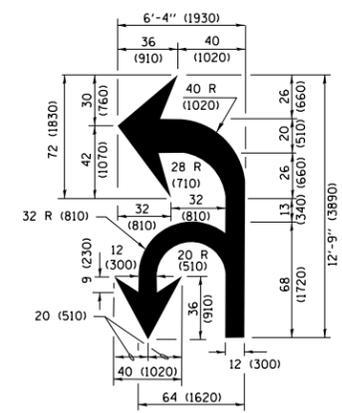


ISLAND OFFSET FROM PAVEMENT EDGE

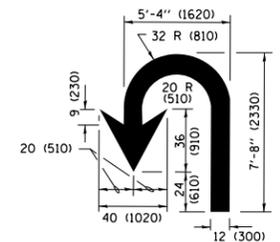


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = Velichkovv	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
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Default	PLOT SCALE = 100.0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 3/30/2017		REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	31
TC-13		CONTRACT NO. 62B90		
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

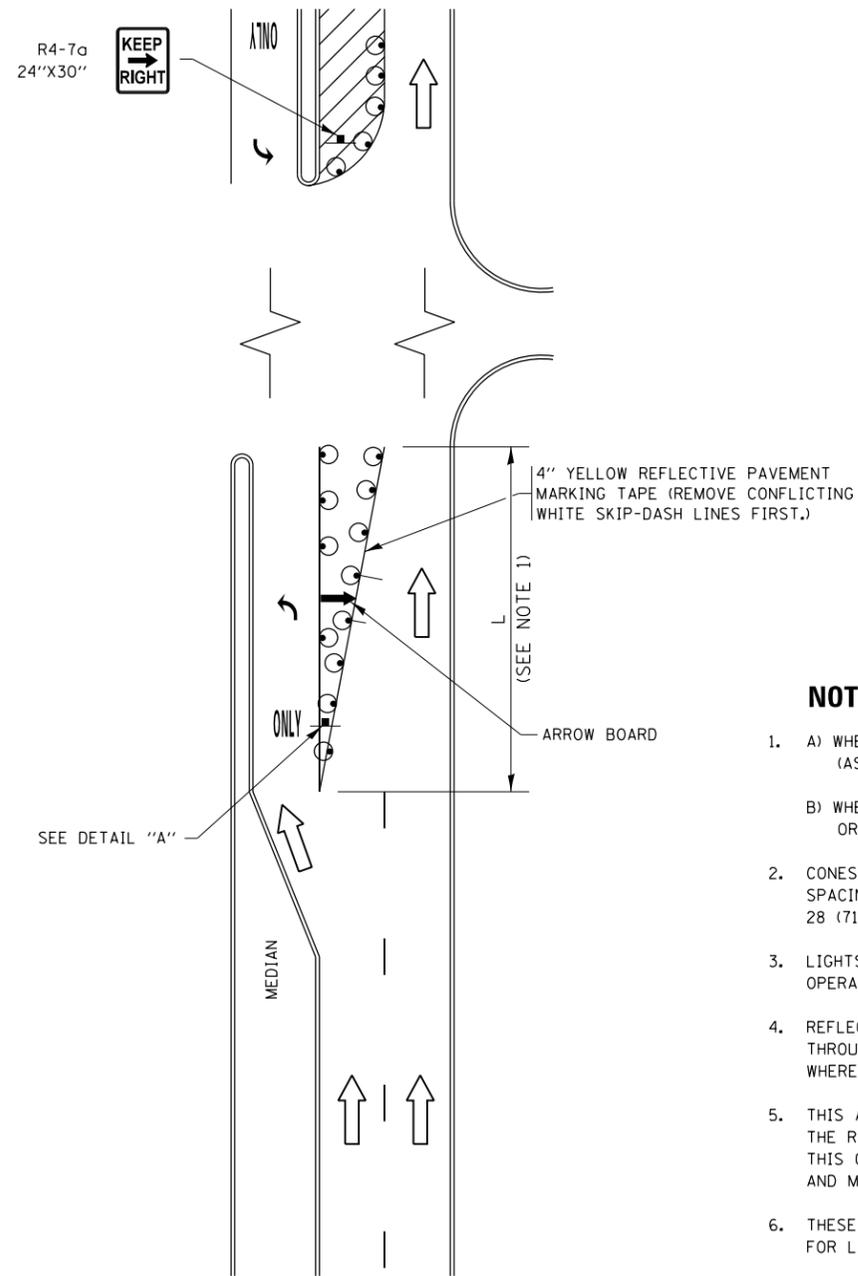


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

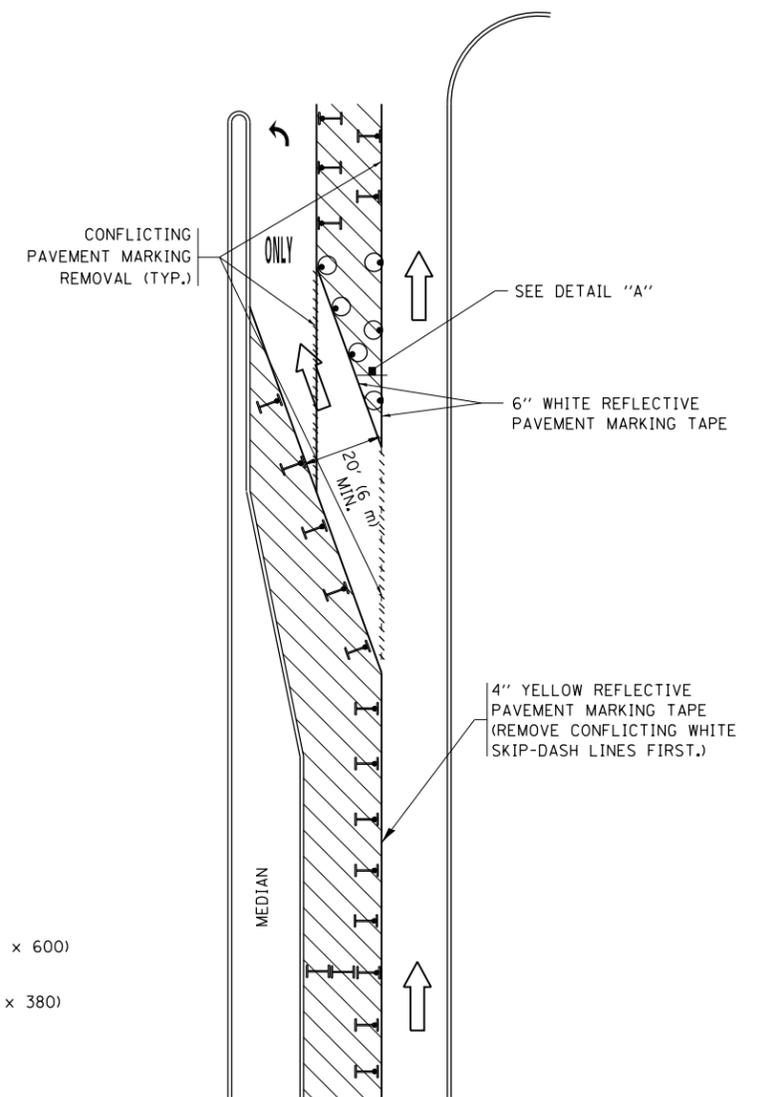


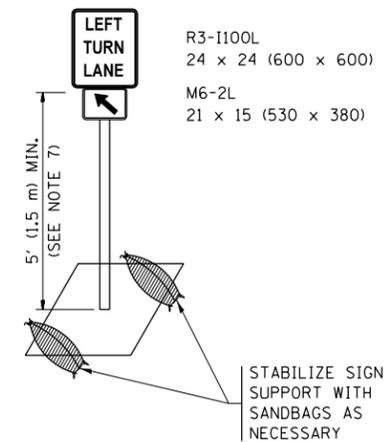
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

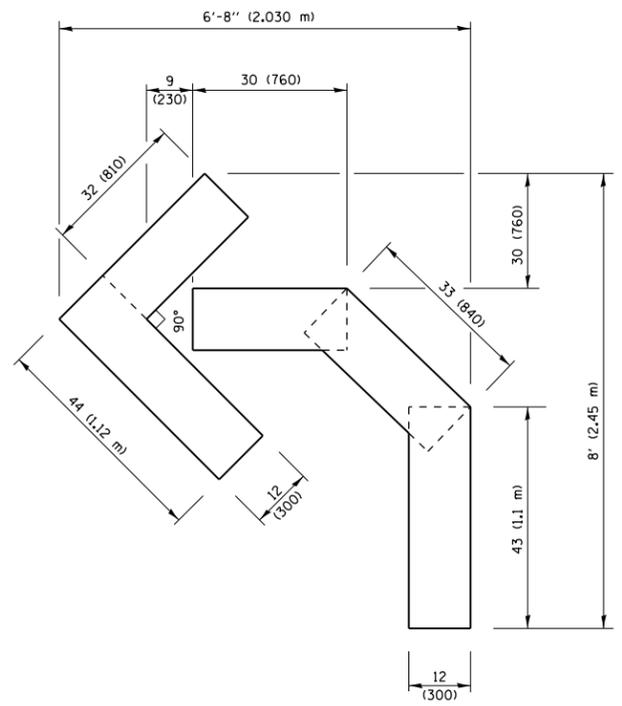
1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



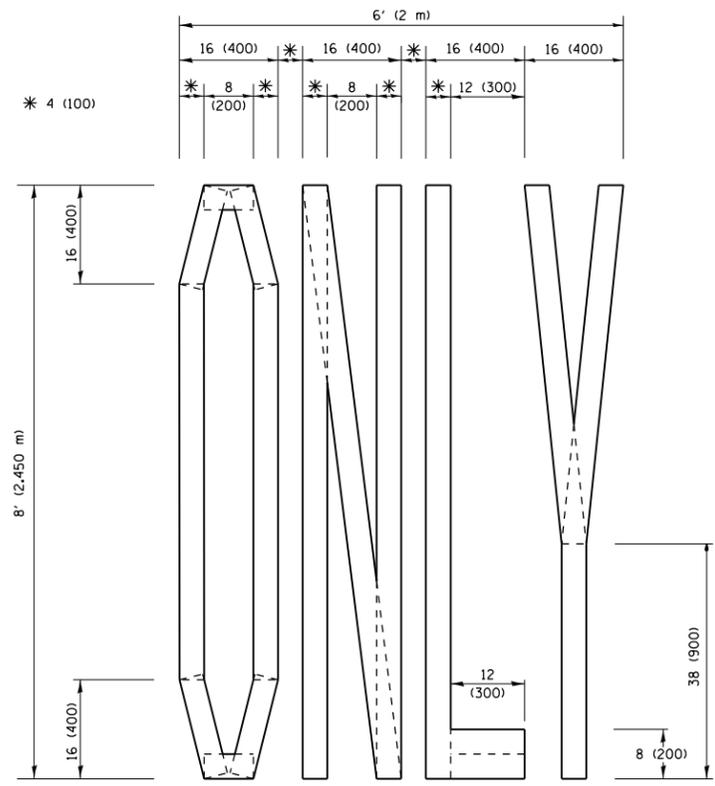
DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

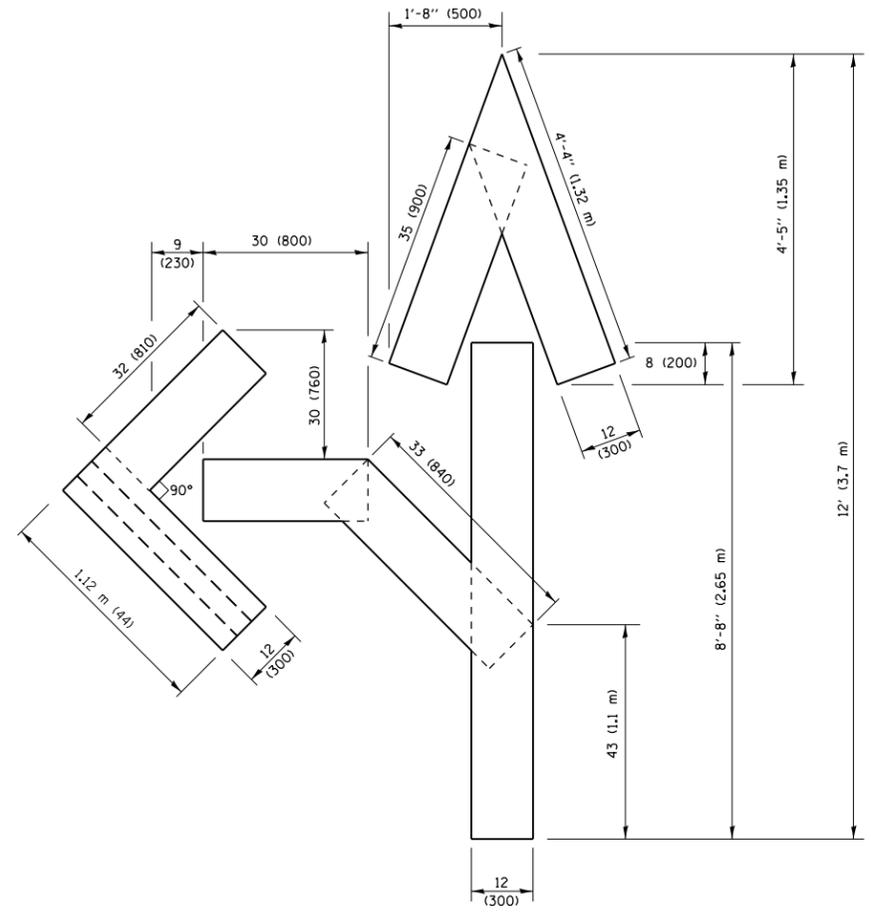
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Default		REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13			2503	(38, 43 & 49) RS-7	KANE	36	32	
	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			TC-14		CONTRACT NO. 62B90		ILLINOIS FED. AID PROJECT	
	PLOT DATE = 3/30/2017	REVISED - T. RAMMACHER 01-06-00	REVISED -			SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.		



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

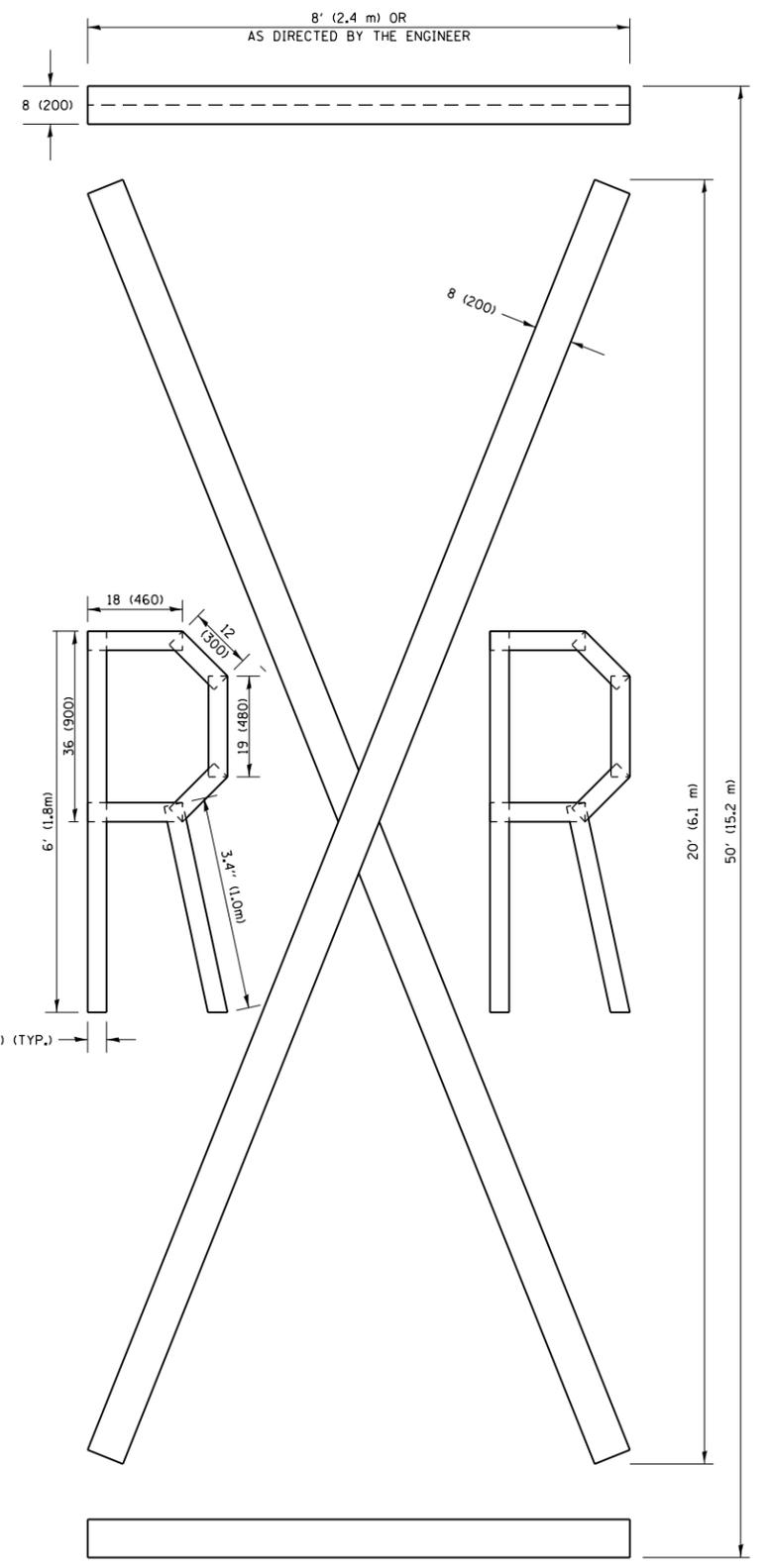


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

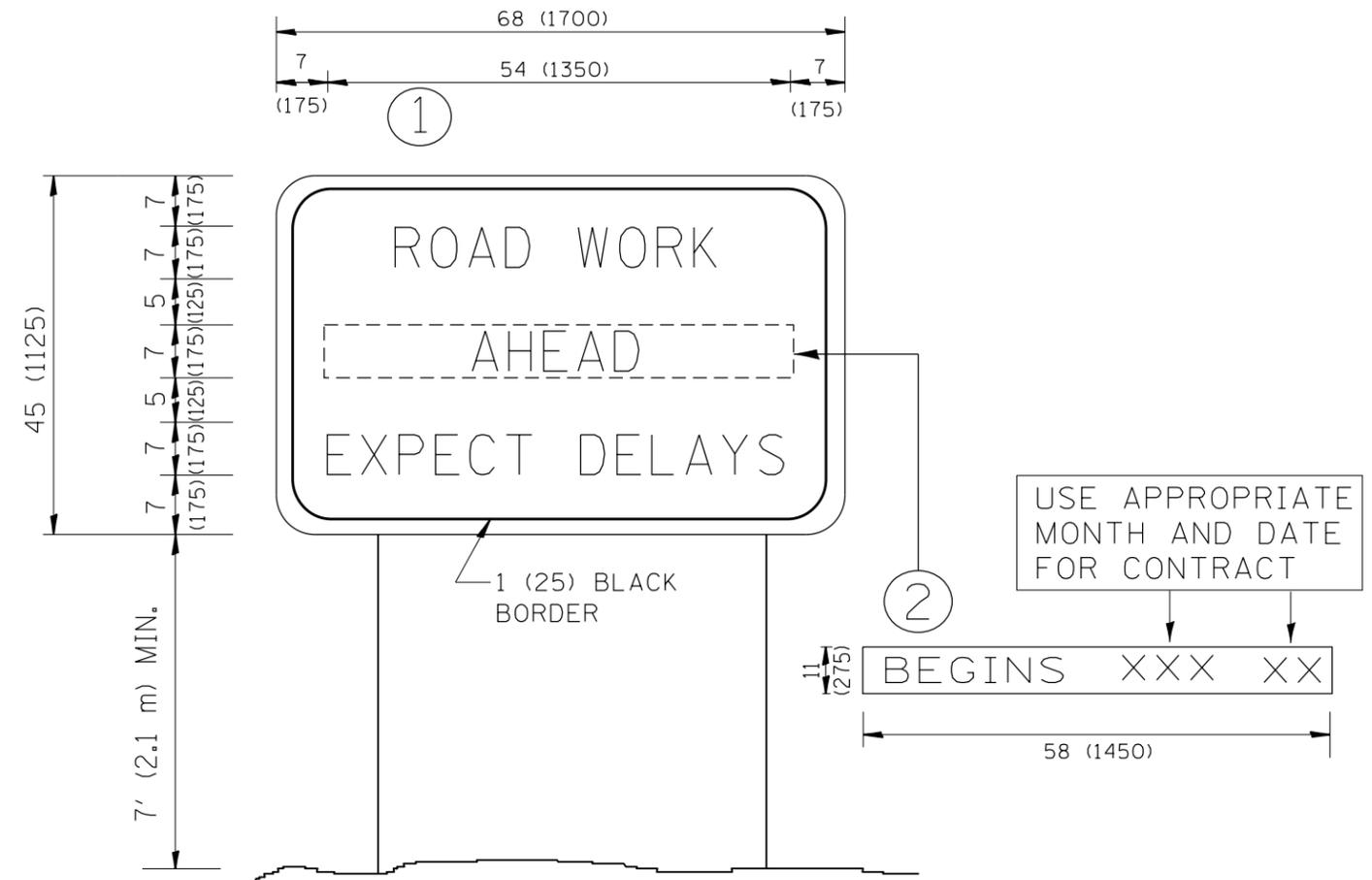
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI1751\Drawings\Design\Diststd.dgn		CHECKED -	REVISED -E. GOMEZ 08-28-00
		DATE -	REVISED -E. GOMEZ 08-28-00
			REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	33
TC-16		CONTRACT NO. 62B90		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

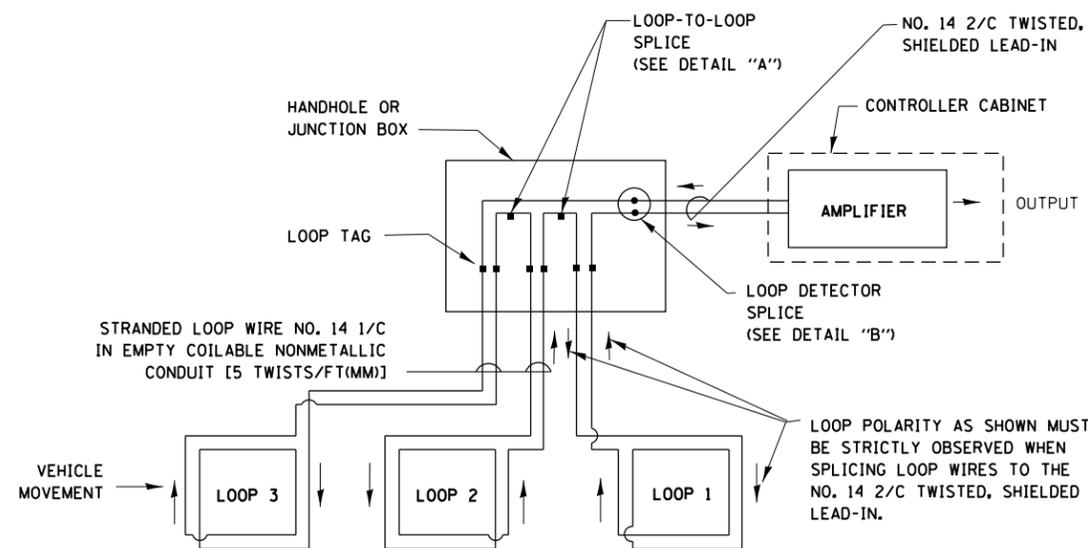
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI1751\Drawings\Design\Diststd.dgn			REVISED - R. MIRS 12-11-97			2503	(38, 43 & 49) RS-7	KANE	36	34
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99				TC-22		CONTRACT NO. 62B90		
PLOT DATE = 3/30/2017	DATE -	REVISED - C. JUCIUS 01-31-07	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

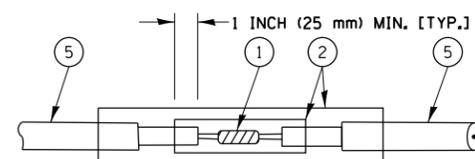
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

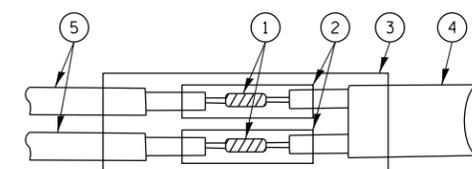


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



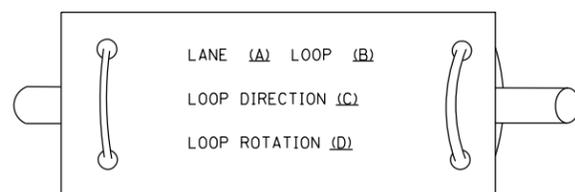
DETAIL "A"
LOOP-TO-LOOP SPLICE



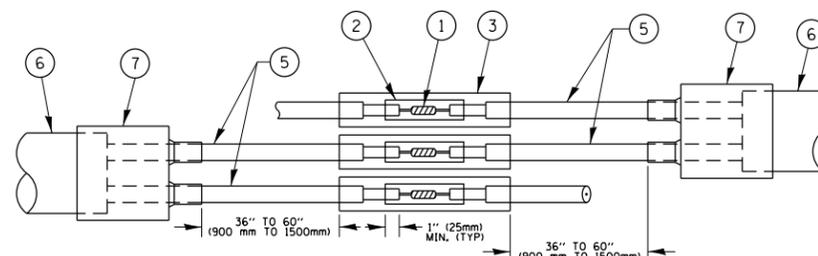
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP

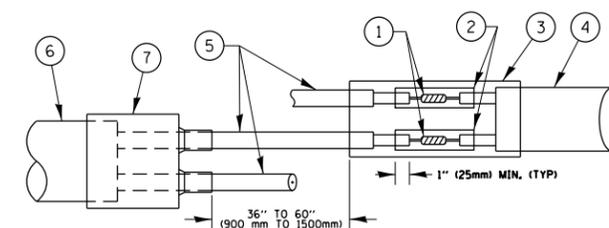
LOOP LEAD-IN CABLE TAG



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH, THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PRE-FORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -
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Default	PLOT DATE = 3/30/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

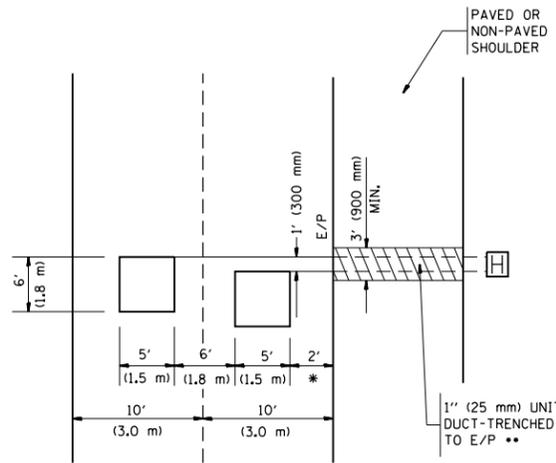
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	(38, 43 & 49) RS-7	KANE	36	35
TS-05		CONTRACT NO. 62B90		
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



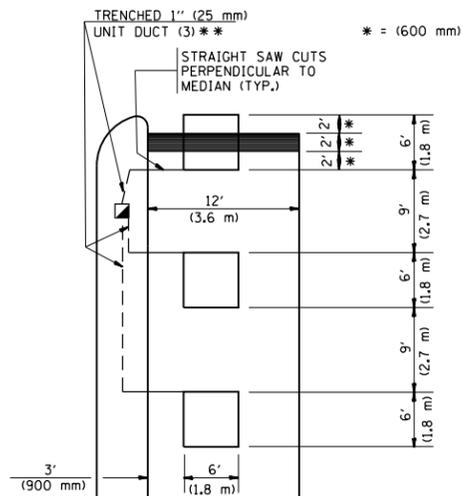
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

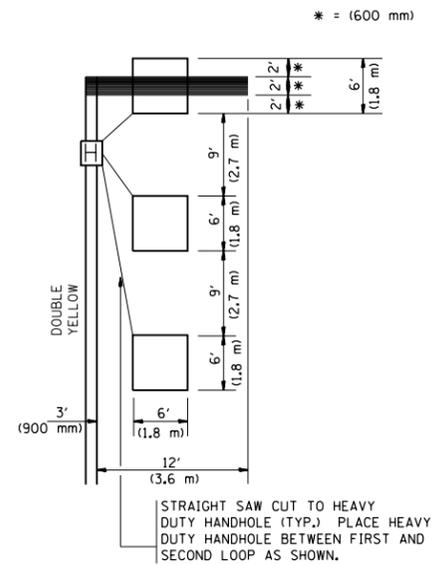


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

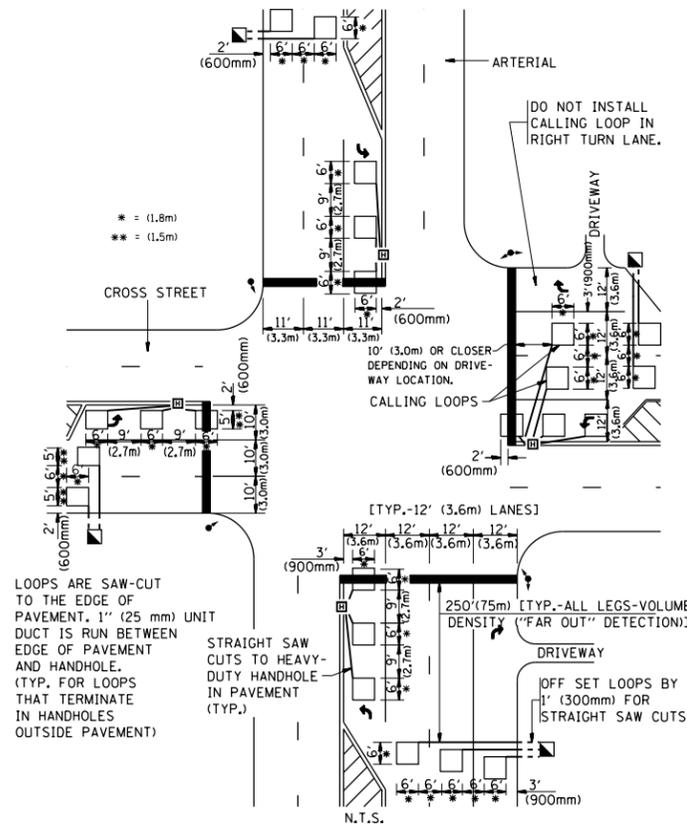
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



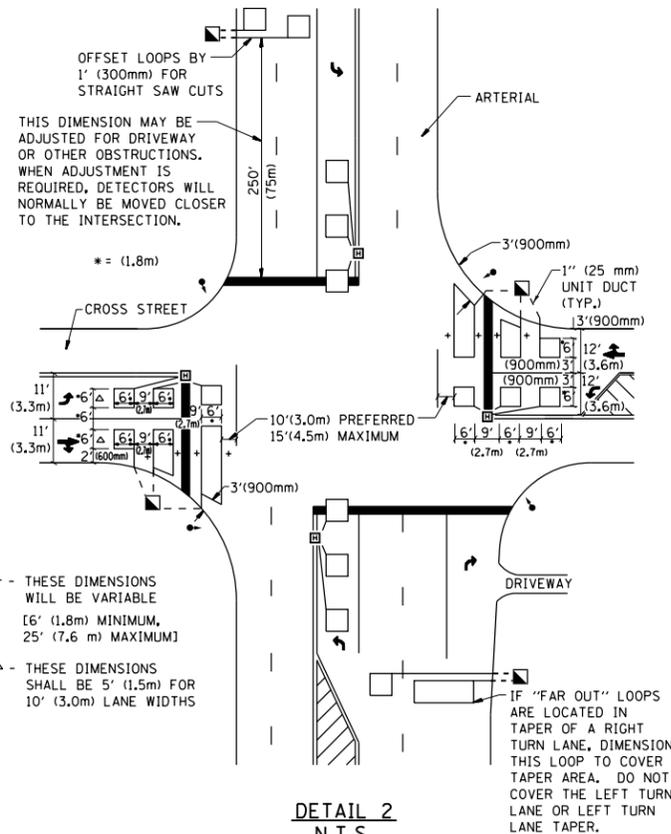
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBID\INTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI175\Drawings\Design\Diststd.dgn		CHECKED - R.K.F.	REVISED -			2503	(38, 43 & 49) RS-7	KANE	36	36
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -			TS-07		CONTRACT NO. 62B90		
PLOT DATE = 3/30/2017			REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	