

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE. 3732	SECTION 3348-RS	COUNTY COOK	TOTAL SHEETS 38	SHEET NO. 1
ILLINOIS CONTRACT NO. 60M17				

FOR INDEX OF SHEETS, SEE SHEET NO. 2

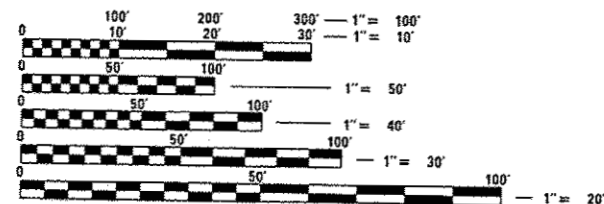
PROJECT LOCATED IN THE CITY OF EVANSTON

TRAFFIC DATA:

ASBURY AVE.  
2016 - ADT = 9650  
SPEED LIMIT = 30 MPH

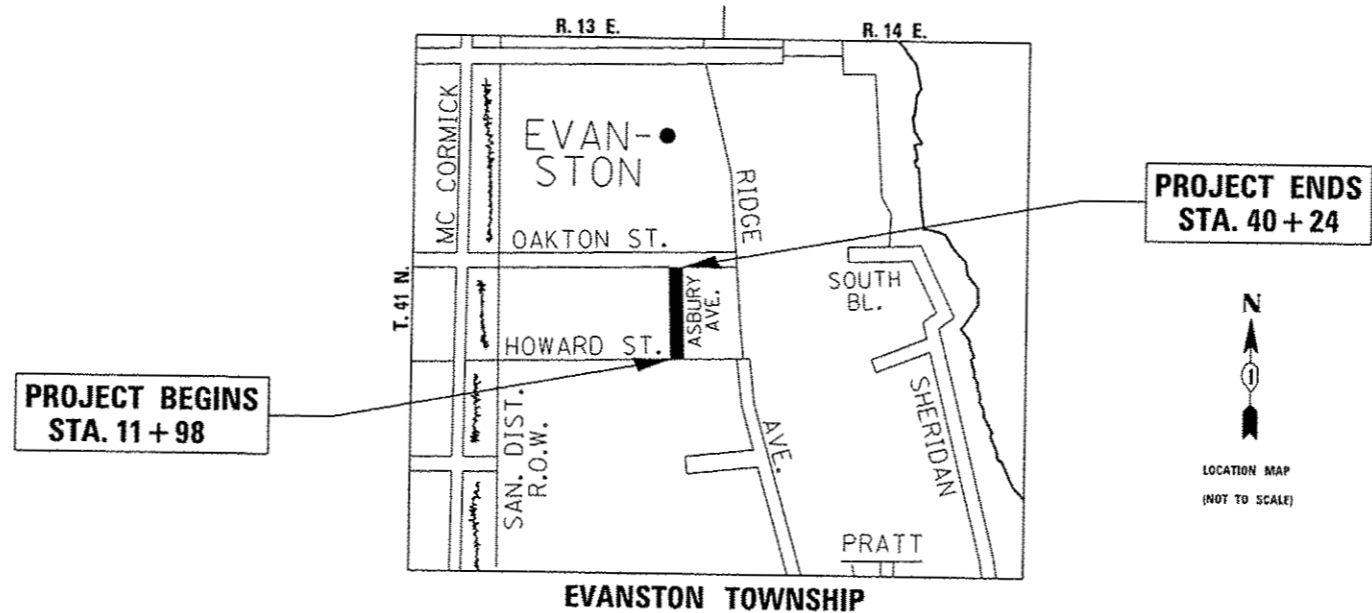
PROPOSED  
HIGHWAY PLANS

F.A.U. 3732 / ASBURY AVE.  
OAKTON ST. TO HOWARD ST.  
SECTION: 3348-RS  
PROJECT: STP-3732(001)  
RESURFACING (3P) AND PEDESTRIAN RAMPS  
COOK COUNTY  
C-91-084-11



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811



PROJECT ENGINEER: DAN WILGREEN (847) 705-4240  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 60M17

GROSS LENGTH = 2826 FT. = 0.54 MILE  
NET LENGTH = 2701 FT. = 0.51 MILE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED March 23, 2017  
Andy J. [Signature]  
REGIONAL ENGINEER

May 12, 2017  
Matthew M. [Signature]  
ENGINEER OF DESIGN AND ENVIRONMENT

May 12, 2017  
[Signature]  
DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

SHEET NO.	DESCRIPTION
1	COVER SHEET
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11	DRIVEWAY DETAILS- DISTANCE BETWEEN R.O.W. AND FACE OF CURB AND EDGE OF SHOULDER >= 15' (4.5 m) (BD-01)
12	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB < 15' (4.5 m) (BD-02)
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17	TRAFFIC CONTROL AND PROTECTION FOR SIDE READS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
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19	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
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21	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)
22	ARTERIAL ROAD INFORMATION SIGN (TC-22)
23	DRIVEWAY ENTRANCE SIGNING (TC-26)
24	STANDARD TRAFFIC SIGNAL DESIGN DETAILS (TS-05, SHEET 2 OF 7)
25	DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)

STANDARD NO.	DESCRIPTION
000001-06	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-09	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-02	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-03	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021-03	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-04	FRAME AND LIDS, TYPE 1
604091-03	FRAME AND GRATE, TYPE 24
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701101-05	OFF-RO OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT, OR MOVING OPERATION FOR SPEEDS < 40 MPH
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER, OR CROSSWALK CLOSURE
701901-06	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
886001-01	DETECTOR LOOP INSTALLATIONS

- BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR "CUAN" (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC.
- TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURBS AND GUTTERS IN THE FIELD, UNLESS OTHERWISE SHOWN, THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE CITY OF EVANSTON.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION OF THE DEPARTMENT.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS WILL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- ALL PAVEMENT PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL, P.C.C. SIDEWALK 5", AND DRAINAGE ADJUSTMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING OF WORK.
- THE RESIDENT ENGINEER SHALL CONTACT CORY JUCIUS, ARTERIAL TRAFFIC FIELD ENGINEER, AT CORY.JUCIUS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES WHERE THE SPEED LIMIT IS 40 MPH OR LESS. WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS
- PROPOSED SIDEWALK RAMPS SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
- THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
- ALL DETECTOR LOOPS IMPACTED BY MILLING AND RESURFACING OPERATIONS SHALL BE REPLACED IN KIND.

FILE NAME	USER NAME = PEROTIARR	DESIGNED -	REVISED - RP 5/12/2017	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES			F.A.U. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
D:\IL\04E8IDINTEG\Illinois.gov\PWIDOT\Documents\1007 Offices\District 1\Projects\01084\DRAWINGS\Design\0108411-ah1-plan.dgn					REVISED -	F.A.U. 3732 (ASBURY AVE.)-OAKTON ST. TO HOWARD ST.			3732	3348-RS	COOK	17	1
PLDT SCALE = 100.0000' / in.	CHECKED -	REVISED -	REVISED -		SCALE:			SHEET NO. 2 OF	SHEETS	STA.	TO STA.	CONTRACT NO. 60M17	
PLOT DATE = 5/12/2017	DATE -	REVISED -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								

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SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005				
20101200	TREE ROOT PRUNING	EACH	12	12				
20200100	EARTH EXCAVATION	CU YD	85	85				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	406	406				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	6	6				
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	6	6				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	6	6				
25200110	SODDING, SALT TOLERANT	SQ YD	406	406				
25200200	SUPPLEMENTAL WATERING	UNIT	4.1	4.1				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	9583	9583				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	22.4	22.4				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	431	431				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	290	290				
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	79	79				

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1256	1256				
42001300	PROTECTIVE COAT	SQ YD	1254	1254				
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	75	75				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	6160	6160				
42400800	DETECTABLE WARNINGS	SQ FT	654	654				
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	2250	2250				
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	12697	12697				
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	75	75				
44000600	SIDEWALK REMOVAL	SQ FT	6562	6562				
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SQ YD	470	470				
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	314	314				
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	34	34				
			* SPECIALTY ITEMS					

FILE NAME :	USER NAME = PEROTTHR	DESIGNED -	REVISED -
OFFICE: District 1 Projects/DOBAIN/CAO/Design/008/11/2007		REVISIONS	
PLOT SCALE = 100,000 1" = 100'	CHECKED -	REVISIONS	
PLOT DATE = 4/1/2007	DATE -	REVISIONS	

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**  
F.A.U. 3732 (ASBURY AVE.)--OAKTON ST. TO HOWARD ST.  
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE. 3732	SECTION 3348-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 3
CONTRACT NO. 60M17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005					
44201759	CLASS D PATCHES, TYPE IV. 9 INCH	SQ YD	60	60					
56108200	ADJUSTING WATER VALVES 6"	EACH	14	14					
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	3	3					
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	2	2					
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	11	11					
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	12	12					
60404950	FRAMES AND GRATES, TYPE 24	EACH	10	10					
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	10	10					
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	38	38					
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	85	85					
* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1					
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					
67100100	MOBILIZATION	L SUM	1	1					
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005					
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1					
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					
70300100	SHORT TERM PAVEMENT MARKING	FOOT	1656	1656					
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	552	552					
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	104	104					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	4163	4163					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1866	1866					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	347	347					
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	828	828					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	104	104					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4163	4163					
* SPECIALTY ITEMS									

FILE NAME :	USER NAME : PEROTIAR	DESIGNED -	REVISED -
OFFICE :	PROJECT :	CHECKED -	REVISED -
PLOT SCALE : 1/8" = 1'-0"	DATE :	DATE :	DATE :

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**  
F.A.U. 3732 (ASBURY AVE.) - OAKTON ST. TO HOWARD ST.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	25	4
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT CONTRACT NO. 60M17				

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SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005				
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1866	1866				
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	347	347				
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	246	246				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	98	98				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	98	98				
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1150	1150				
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	LSUM	1	1				
X0327980	PAVEMENT MARKING REMOVAL- WATER BLASTING	SO FT	82	82				
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	520	520				
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	51	51				
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	3119	3119				
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1901	1901				
* SPECIALTY ITEMS								

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES					
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	21	21				
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4				

FILE NAME :	USER NAME : PEROTIAR	DESIGNED -	REVISED -
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		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

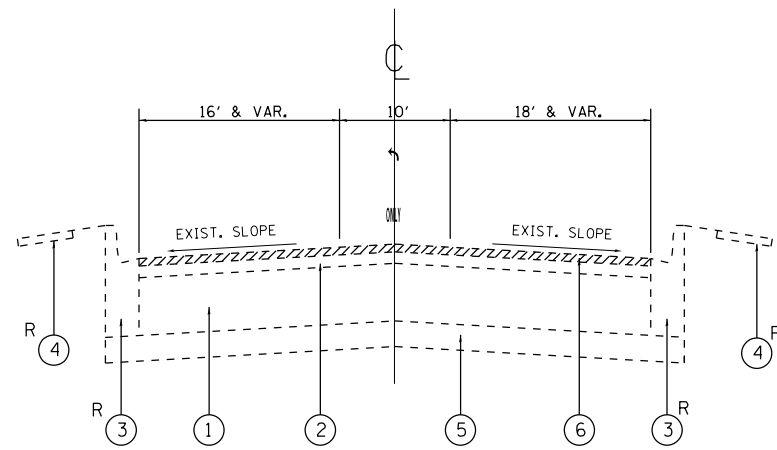
**SUMMARY OF QUANTITIES**  
F.A.U. 3732 (ASBURY AVE.)-OAKTON ST. TO HOWARD ST.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	25	5
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

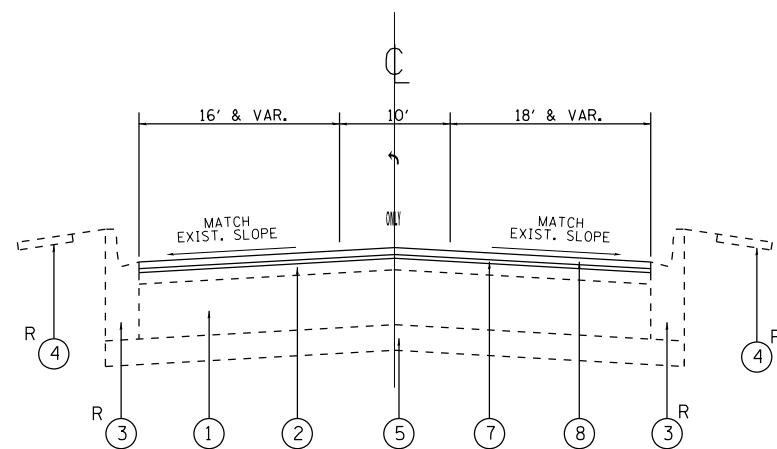
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NON PARTICIPATING

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**EXISTING TYPICAL SECTION**  
 STA. 11+97 TO STA. 15+81  
 STA. 37+00 TO STA. 40+24



**PROPOSED TYPICAL SECTION**  
 STA. 11+97 TO STA. 15+81  
 STA. 37+00 TO STA. 40+24

**LEGEND**

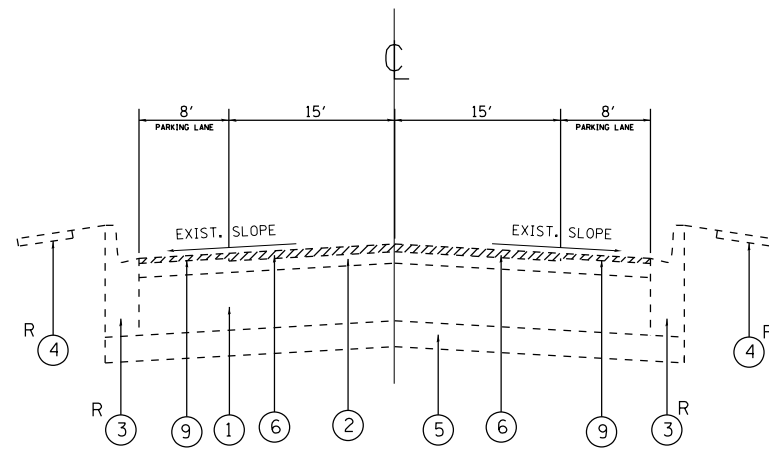
- ① EXIST. P.C.C. BASE COURSE, ±9"
  - ② EXIST. HMA SURFACE, ±3"
  - ③ EXIST. COMB. CONC. CURB AND GUTTER, TYPE B-6.18
  - ④ EXIST. P.C.C. SIDEWALK
  - ⑤ EXIST. STABILIZED SUB-BASE
  - ⑥ PROP. HMA SURFACE REMOVAL, 2 1/4"
  - ⑦ PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
  - ⑧ PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
- R SIDEWALK, CURB AND GUTTER REMOVAL AND REPLACEMENT  
 (LOCATION AS DIRECTED BY THE ENGINEER)

**NOTES**

1. THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING
  2. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE QUANTITIES IS 112 LBS/SQ YD/IN
  3. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS
- FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS  
 QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS			
MIXTURE USES	MIXTURE USE	DESIGN AIR VOIDS	QUALITY MANAGEMENT PROGRAM (QMP)
PATCHING	CLASS "D" PATCHES, 9" HMA BINDER COURSE, IL-19 mm	4% @ 70	QC/QA
	HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 70	QC/QA
MAINLINE RESURFACING	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"	3.5% @ 50	QC/QA
	HMA SURFACE COURSE MIX "D", N70, IL-9.5 mm; 1 1/2"	4% @ 70	OCP
PARKING LANE RESURFACING	HMA SURFACE COURSE MIX "D", N70, IL-9.5 mm; 1 1/2"	4% @ 70	OCP

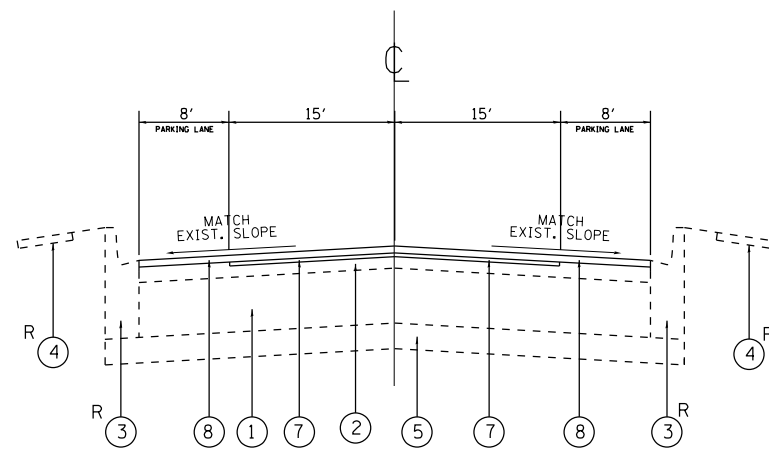
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (OCP)



**EXISTING TYPICAL SECTION**  
 STA. 15+81 TO STA. 21+61  
 STA. 22+84 TO STA. 37+00

**LEGEND**

- ① EXIST. P.C.C. BASE COURSE, ±9"
  - ② EXIST. HMA SURFACE, ±3"
  - ③ EXIST. COMB. CONC. CURB AND GUTTER, TYPE B-6.18
  - ④ EXIST. P.C.C. SIDEWALK
  - ⑤ EXIST. STABILIZED SUB-BASE
  - ⑥ PROP. HMA SURFACE REMOVAL, 2¼"
  - ⑦ PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
  - ⑧ PROP. HMA SURFACE COURSE, MIX "D", N70, 1½"
  - ⑨ PROP. HMA SURFACE REMOVAL 1½" (AT PARKING LANE LOCATIONS ONLY)
- R SIDEWALK, CURB AND GUTTER REMOVAL AND REPLACEMENT  
 (LOCATION AS DIRECTED BY THE ENGINEER)



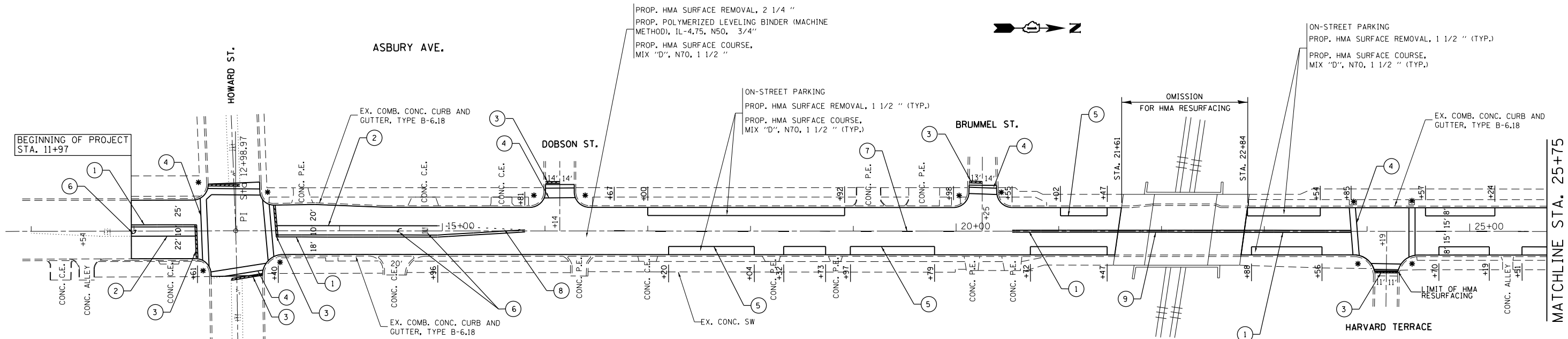
**PROPOSED TYPICAL SECTION**  
 STA. 15+81 TO STA. 21+61  
 STA. 22+84 TO STA. 37+00

FILE NAME =	USER NAME = PEROTTIRR	DESIGNED -	REVISED -
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PLOT SCALE = 100.0004' / 1in.	CHECKED -	REVISED -	REVISED -
PLOT DATE = 4/4/2017	DATE -	REVISED -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED TYPICAL SECTIONS			
F.A.U. 3732 (ASBURY AVE.)--OAKTON ST. TO HOWARD ST.			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	25	7
CONTRACT NO. 60M17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



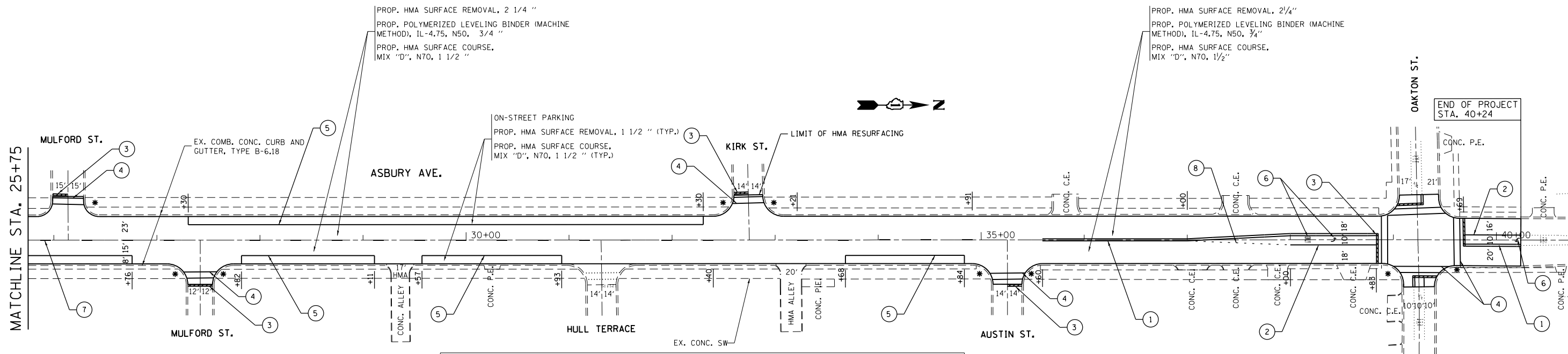
**PAVEMENT MARKING LEGEND**

- |   |  |
|---|--|
| ① THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 11" C-C (TYP.)                   | ⑤ THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE PARKING LINE (TYP.)                         |
| ② THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE (TYP.)   | ⑥ THERMOPLASTIC PAVEMENT MARKING, LETTERS & SYMBOLS, SOLID WHITE (TYP.)                      |
| ③ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)   | ⑦ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LANE LINE, 10' DASH - 30' SKIP (TYP.)  |
| ④ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP. UNLESS OTHERWISE NOTED) | ⑧ THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE TURN LANE, 2' LINE WITH 6' SPACE (TYP.) |
|   | ⑨ POLYUREA PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 11" C-C (TYP.)           |

**NOTE:**

1. RE-STRIPE ALL STOP BAR PAVEMENT MARKINGS LOCATED BEHIND THE LIMIT OF HMA RESURFACING

\* ADA CURB RAMP IMPROVEMENT



**PAVEMENT MARKING LEGEND**

- |   |  |
|---|--|
| ① THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 11" C-C (TYP.)                   | ⑤ THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE PARKING LINE (TYP.)                         |
| ② THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE (TYP.)   | ⑥ THERMOPLASTIC PAVEMENT MARKING, LETTERS & SYMBOLS, SOLID WHITE (TYP.)                      |
| ③ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)   | ⑦ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LANE LINE, 10' DASH - 30' SKIP (TYP.)  |
| ④ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP. UNLESS OTHERWISE NOTED) | ⑧ THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE TURN LANE, 2' LINE WITH 6' SPACE (TYP.) |
|   | ⑨ POLYUREA PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 11" C-C (TYP.)           |

**NOTE:**

1. RE-STRIPE ALL STOP BAR PAVEMENT MARKINGS LOCATED BEHIND THE LIMIT OF HMA RESURFACING

\* ADA CURB RAMP IMPROVEMENT

FILE NAME =	USER NAME = PEROTTIRR	DESIGNED -	REVISED -
p:\11\084EBIDINTEG\11\inois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI084EBIDINTEG\Design\DI08411-sht-plan.dgn		REVISED -	REVISED -
PLOT SCALE = 99.9998' / in.	CHECKED -	REVISED -	REVISED -
PLOT DATE = 4/4/2017	DATE -	REVISED -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

ROADWAY AND PAVEMENT MARKING PLANS  
F.A.U. 3732 (ASBURY AVE.)--OAKTON ST. TO HOWARD ST.

SCALE: 1"=50' SHEET OF SHEETS STA. 11+97.3 TO STA. 40+23.6

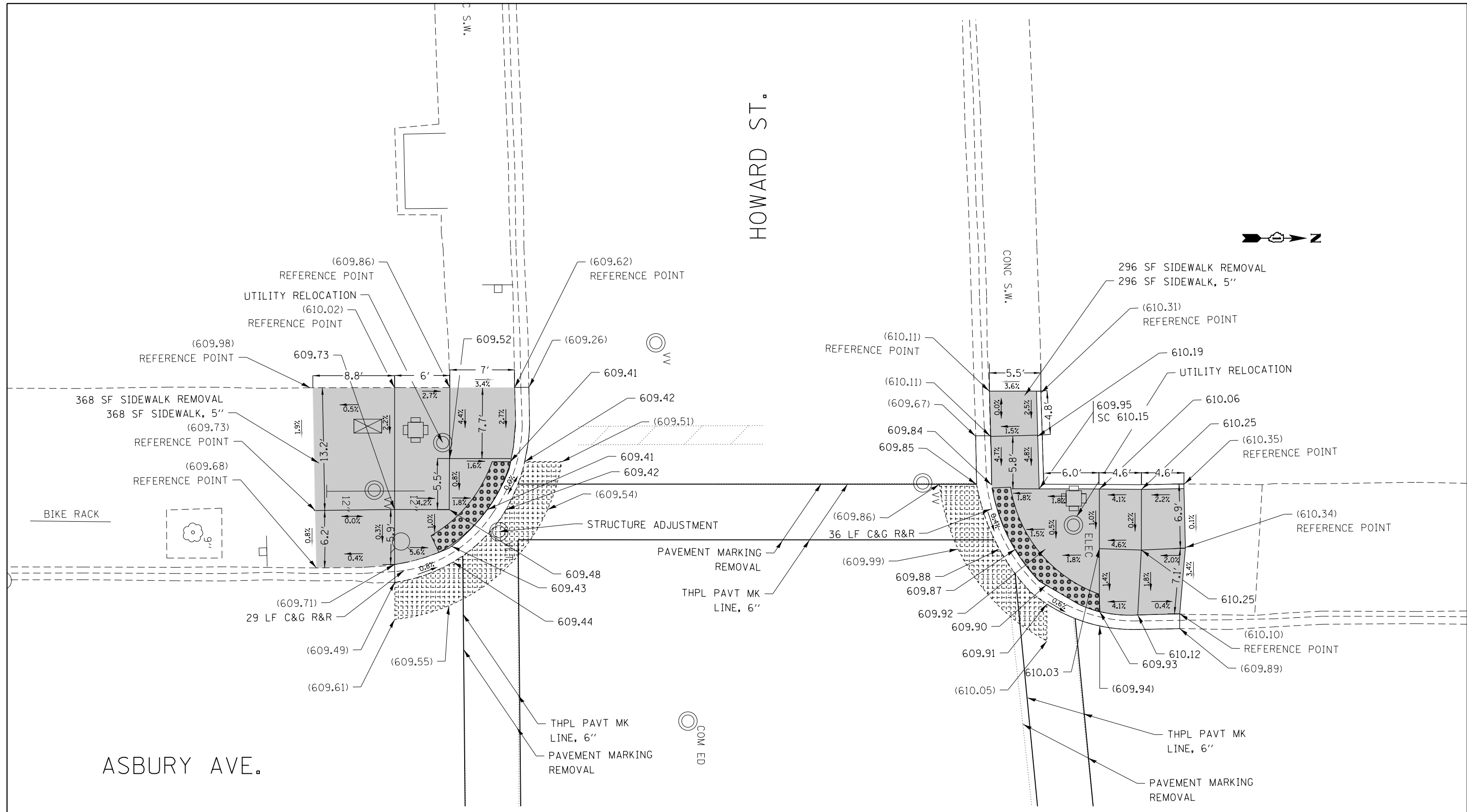
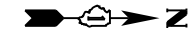
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	25	8
CONTRACT NO. 60M17				
ILLINOIS FED. AID PROJECT				



THIS SHEET IS INTENTIONALLY LEFT BLANK

FILE NAME =	USER NAME = PEROTTIR	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	F.A.U. 3732 (ASBURY AVE.)--OAKTON ST. TO HOWARD ST.		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI084EBID\Drawings\Design\DI08411-shr-plan.dgn		CHECKED -	REVISED -				3732	3348-RS	COOK	25	9
	PLOT SCALE = 100.0000' / 1in.	DATE -	REVISED -		SCALE:      SHEET NO.    OF    SHEETS    STA.                    TO STA.		CONTRACT NO. 60M17				
	PLOT DATE = 4/4/2017	DATE -	REVISED -		FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT						

HOWARD ST.



**LEGEND**

xx.xx'

EXISTING LENGTH

( )

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB

SC

TOP OF PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 610.474

BENCHMARK: "X" CUT ON NE BOLT OF TRAFFIC SIGNAL/LIGHT (#11391)

LOCATION: SE CORNER OF ASBURY AVE. AND HOWARD ST.

FILE NAME =	USER NAME = PEROTTIR	DESIGNED -	REVISED - RP 5/12/2017
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI08411\Drawings\Design\DI08411-shd-details.dgn		CHECKED -	REVISED -
Default	PLOT DATE = 5/12/2017	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN  
ASBURY AVE. AT HOWARD ST.**

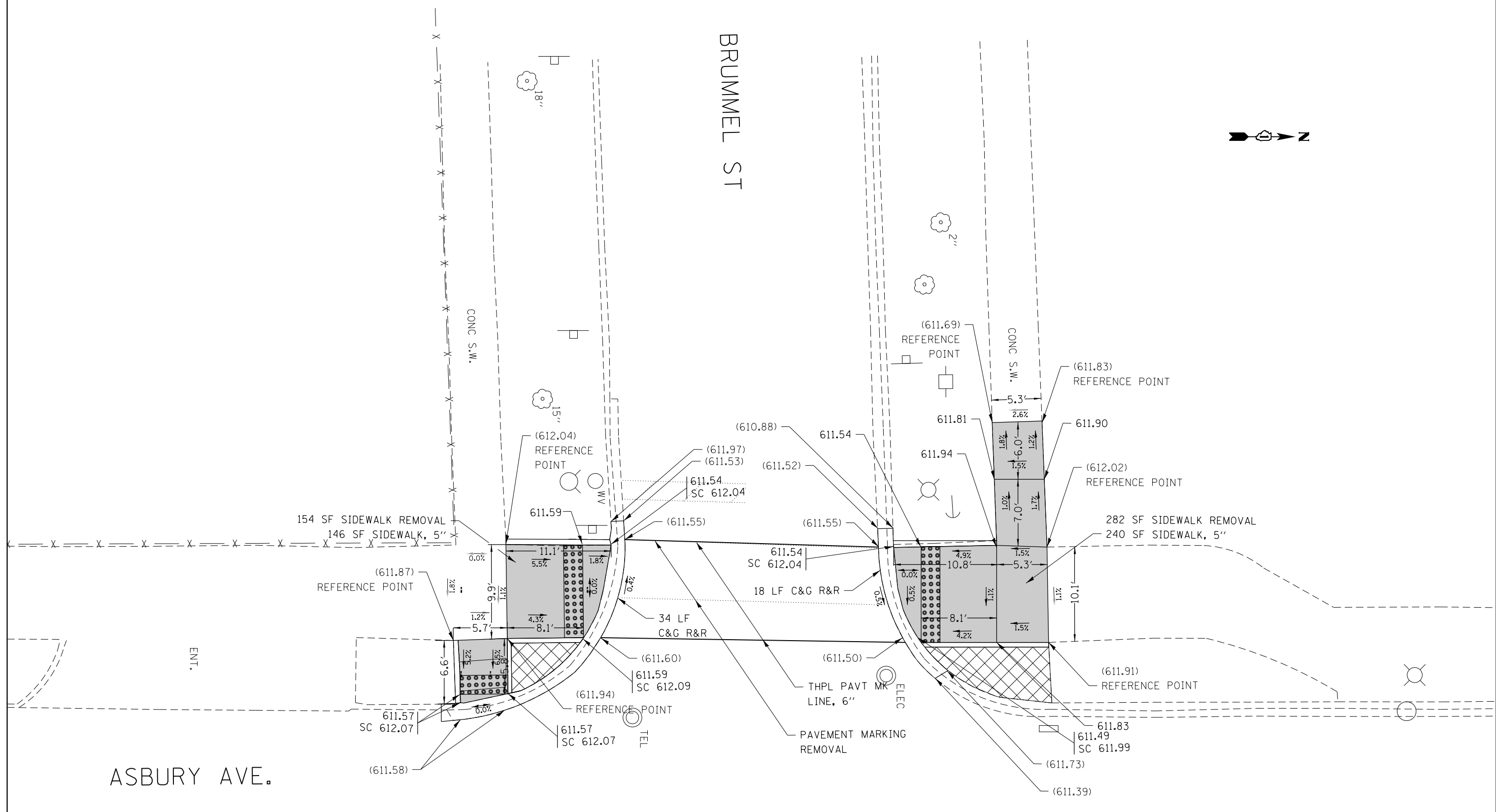
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	9A	25
CONTRACT NO. 60M17				
ILLINOIS FED. AID PROJECT				





BRUMMEL ST



**LEGEND**

xx.xx'

EXISTING LENGTH

( )

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB

SC

TOP OF PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 613.271

BENCHMARK: "X" CUT ON SE BOLT OF FIRE HYDRANT (BM #5253)

LOCATION: SW CORNER OF ASBURY AVE. AND BRUMMEL ST.

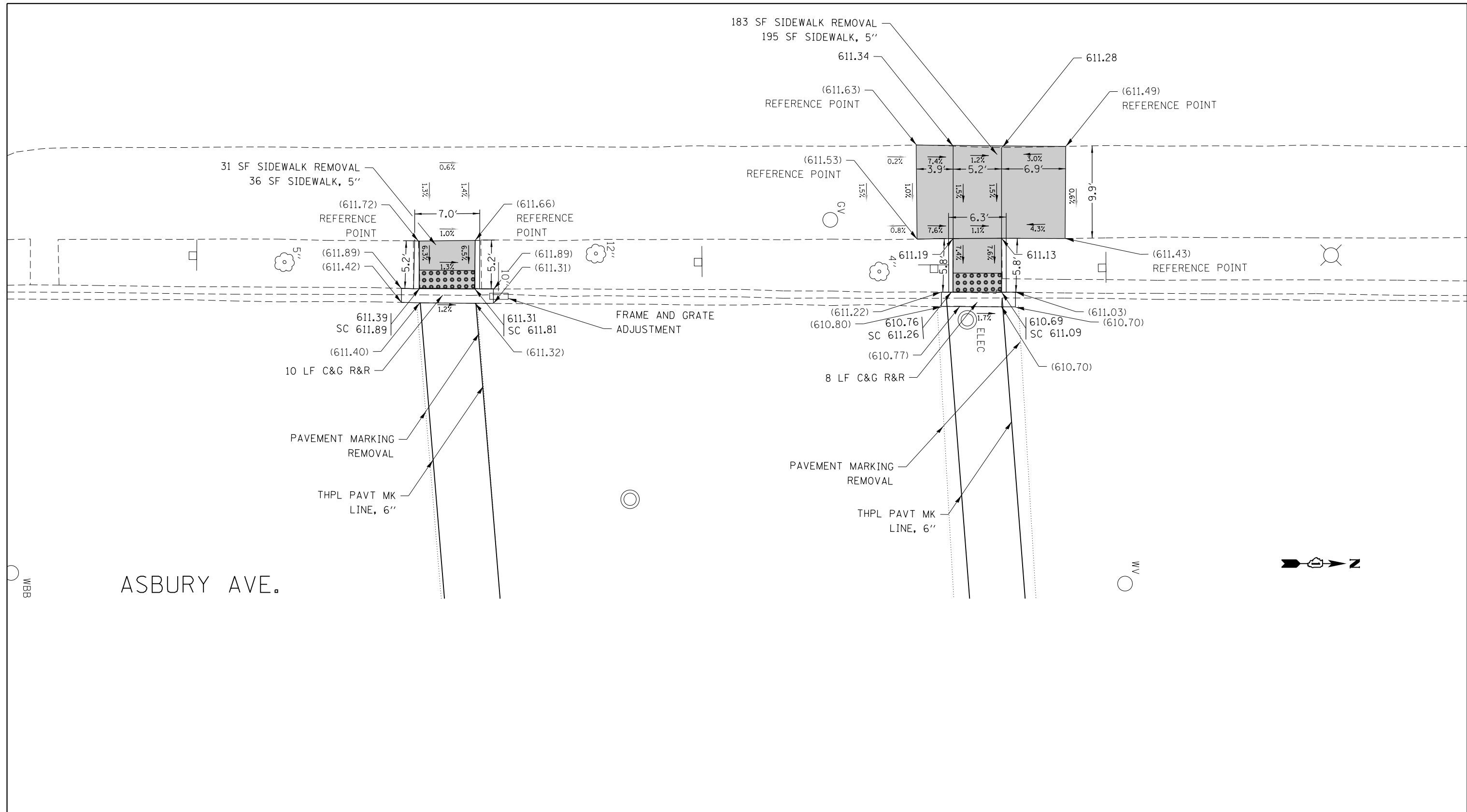
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Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/12/2017	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN  
ASBURY AVE. AT BRUMMEL ST.**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	9D	25
CONTRACT NO. 60M17				
ILLINOIS FED. AID PROJECT				



**LEGEND**

xx.xx'

EXISTING LENGTH

( )

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB

SC

TOP OF PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 613.668

BENCHMARK: "X" CUT ON NW BOLT OF FIRE HYDRANT (\*12161)

LOCATION: NE CORNER OF ASBURY AVE. AND HARVARD TERRACE

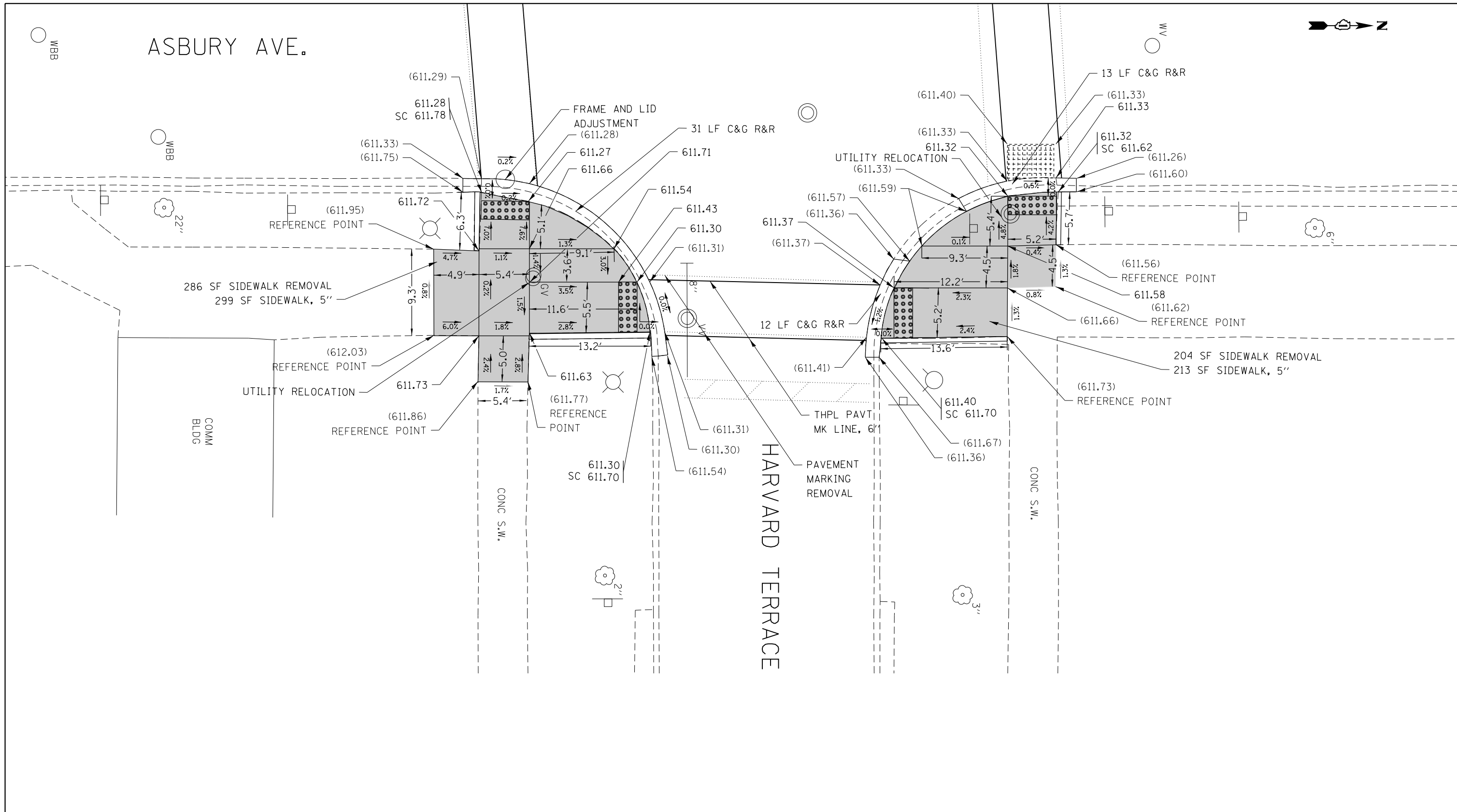
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Default	PLOT DATE = 5/12/2017	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN  
ASBURY AVE. AT HARVARD TERRACE**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	9E	25
CONTRACT NO. 60M17				
ILLINOIS FED. AID PROJECT				



**LEGEND**

xx.xx'

EXISTING LENGTH

( )

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB

SC

TOP OF PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 613.668

BENCHMARK: "X" CUT ON NW BOLT OF FIRE HYDRANT (\*12161)

LOCATION: NE CORNER OF ASBURY AVE. AND HARVARD TERRACE

FILE NAME =	USER NAME = PEROTTIR	DESIGNED -	REVISED - RP 5/12/2017
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	PLOT DATE = 5/12/2017	DATE -	REVISED -

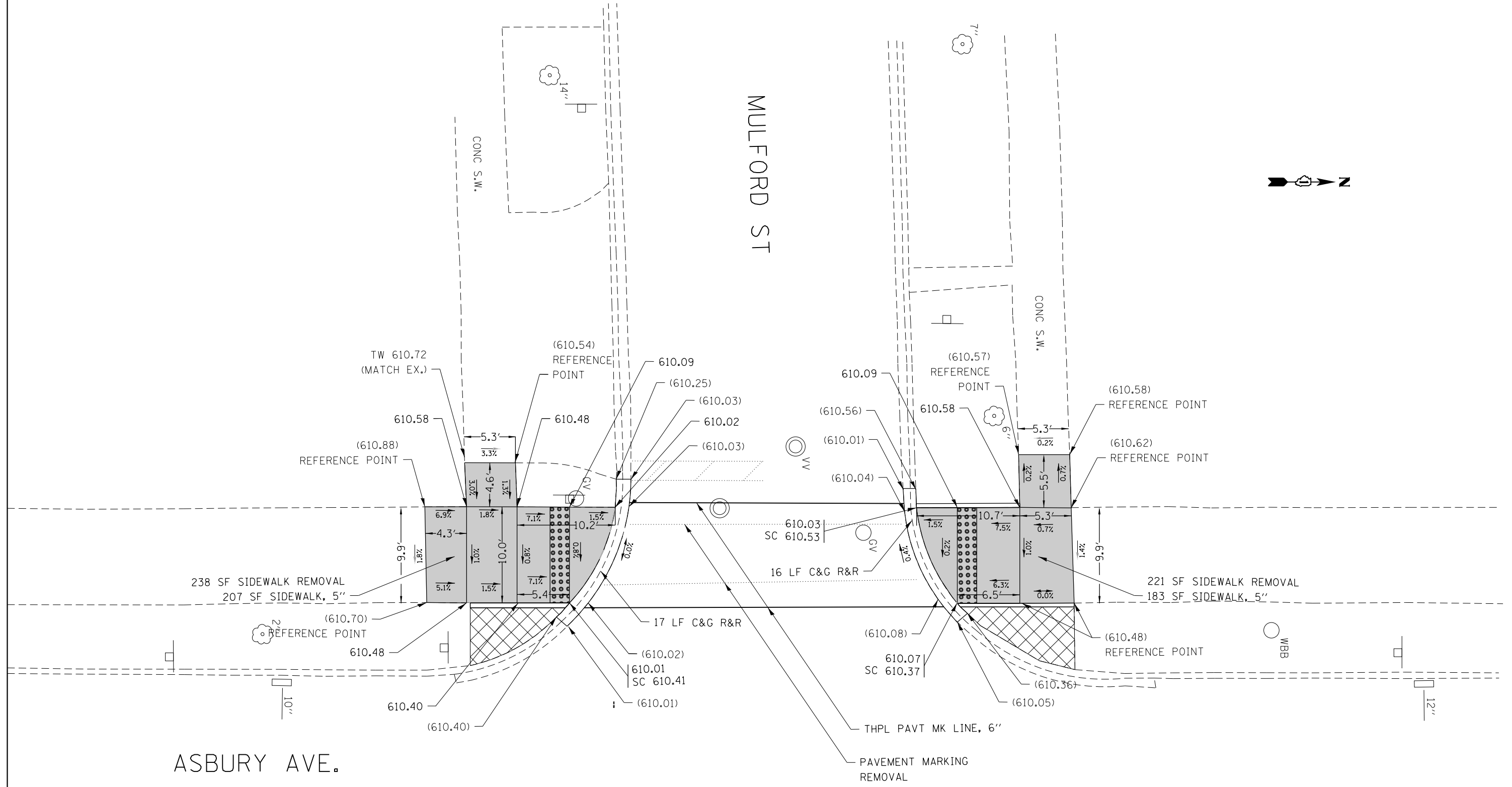
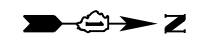
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN  
ASBURY AVE. AT HARVARD TERRACE**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	9F	25
CONTRACT NO. 60M17				
ILLINOIS FED. AID PROJECT				

MULFORD ST



ASBURY AVE.

**LEGEND**

xx.xx'

EXISTING LENGTH

( )

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB

SC

TOP OF PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 612.508

BENCHMARK: "X" CUT ON SW BOLT OF FIRE HYDRANT (\*12675)

LOCATION: NE CORNER OF ASBURY AVE. AND E. MULFORD ST.

FILE NAME =	USER NAME = PEROTTIRR	DESIGNED -	REVISED - RP 5/12/2017
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Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/12/2017	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN  
ASBURY AVE. AT MULFORD ST.**

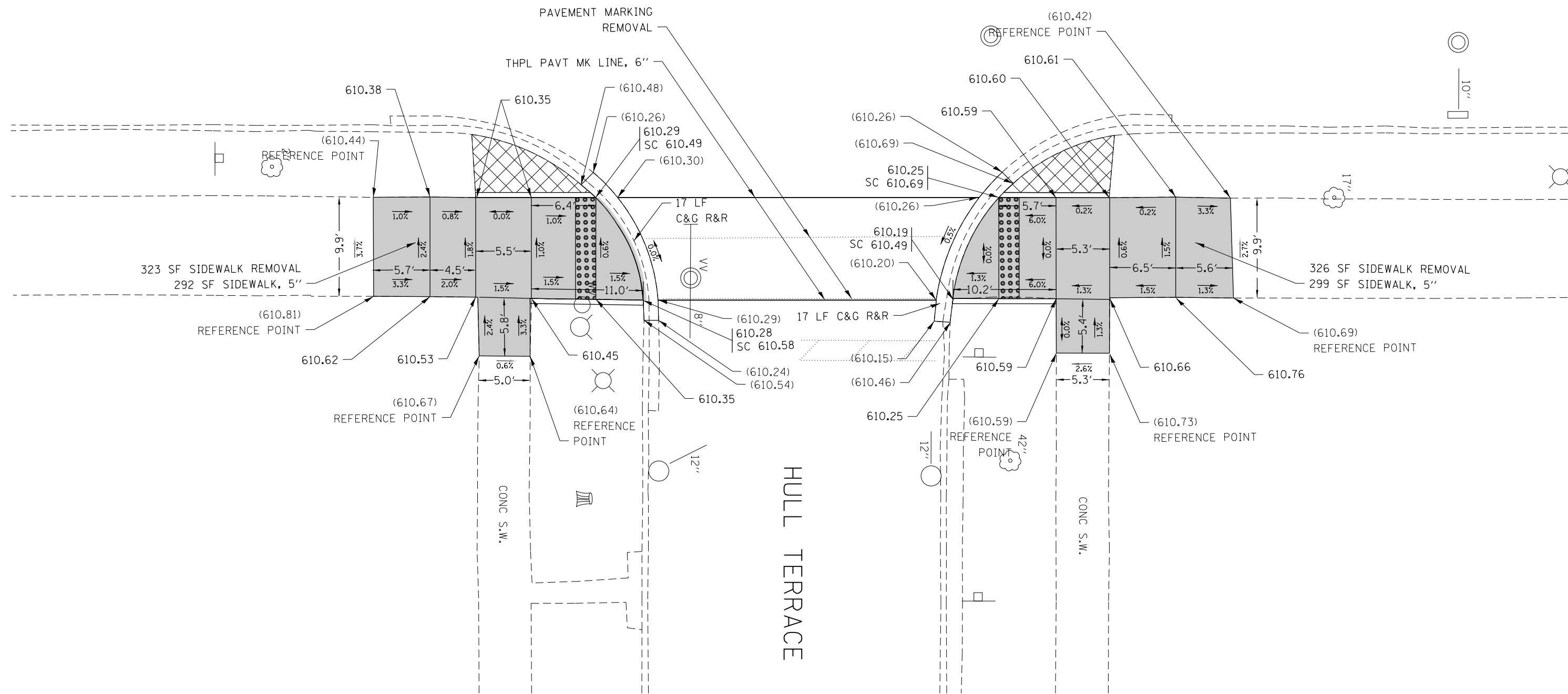
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	96	25
CONTRACT NO. 60M17				
ILLINOIS FED. AID PROJECT				





ASBURY AVE.



**LEGEND**

xx.xx'

EXISTING LENGTH

( )

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB

SC

TOP OF PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 612.392

BENCHMARK: "X" CUT ON NW BOLT OF FIRE HYDRANT (\*13799)

LOCATION: SE CORNER OF ASBURY AVE. AND HULL TERRACE

FILE NAME =	USER NAME = PEROTTIR	DESIGNED -	REVISED - RP 5/12/2017
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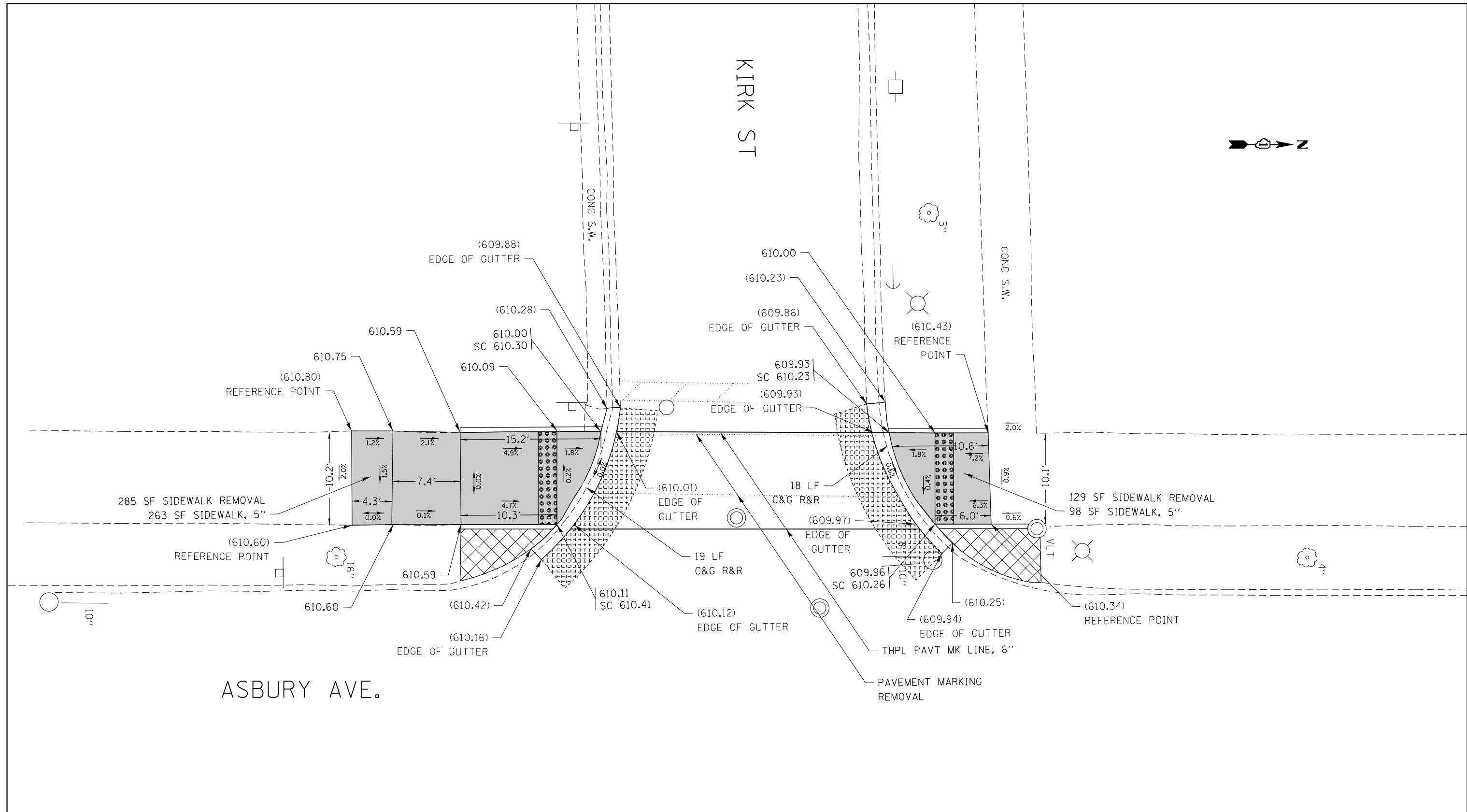
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**SIDEWALK DETAIL PLAN  
ASBURY AVE. AT HULL TERRACE**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	91	25
CONTRACT NO. 60M17				
ILLINOIS FED. AID PROJECT				

KIRK ST



ASBURY AVE.

### LEGEND

[xx.xx']

EXISTING LENGTH

( )

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB

SC

TOP OF PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 610.582

BENCHMARK: "□" CUT ON EAST SIDE OF LIGHT POLE BASE (#13456)

LOCATION: NW CORNER OF ASBURY AVE. AND KIRK ST.

FILE NAME =	USER NAME = PEROTTIR	DESIGNED -	REVISED - RP 5/12/2017
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Default	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/12/2017	DATE -	REVISED -

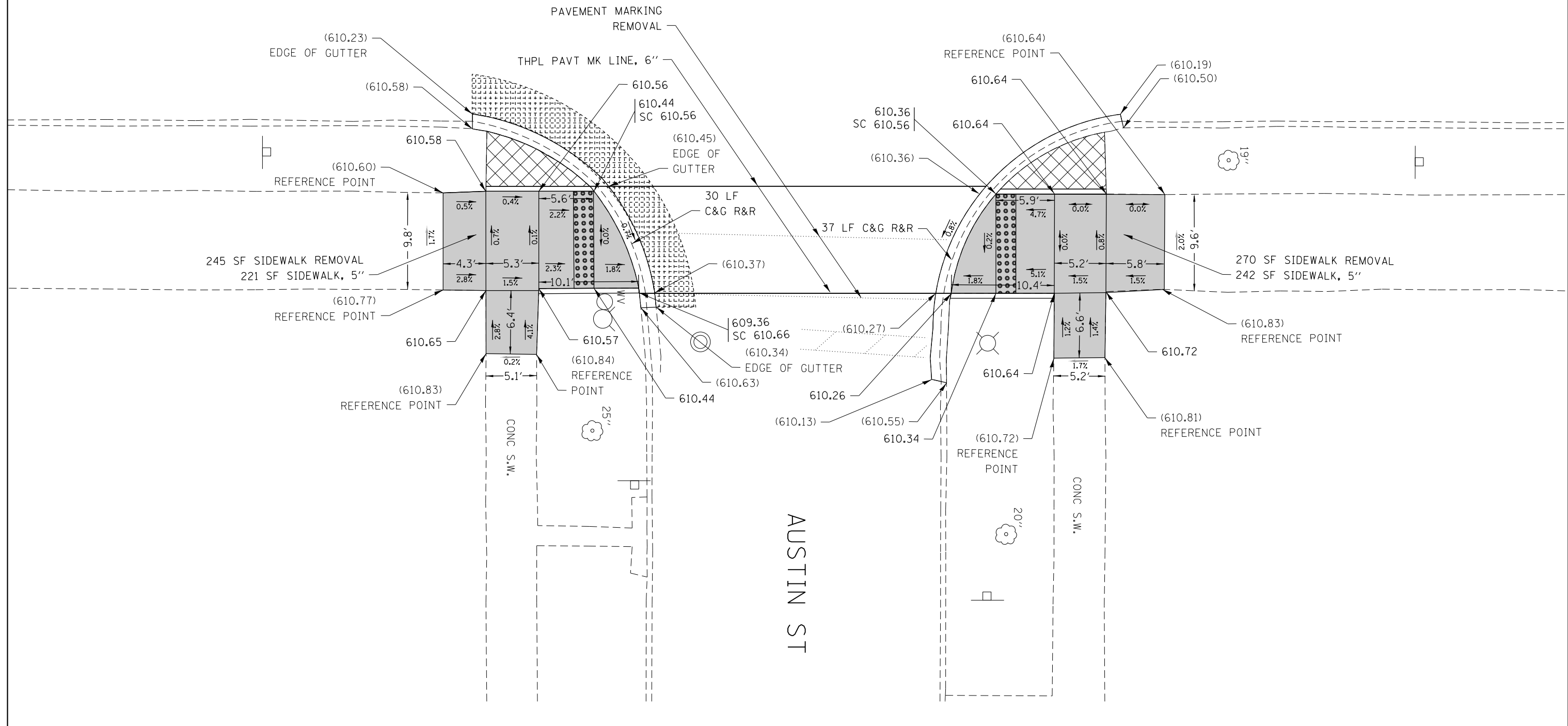
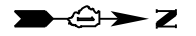
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

### SIDEWALK DETAIL PLAN ASBURY AVE. AT KIRK ST.

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	9J	25
CONTRACT NO. 60M17				
ILLINOIS FED. AID PROJECT				

# ASBURY AVE.



## LEGEND

xx.xx'

EXISTING LENGTH

( )

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB

SC

TOP OF PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 612.649

BENCHMARK: "X" CUT ON SW BOLT OF FIRE HYDRANT (\*13453)

LOCATION: SE CORNER OF ASBURY AVE. AND AUSTIN ST.

FILE NAME =	USER NAME = PEROTTIR	DESIGNED -	REVISED - RP 5/12/2017
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI0844\Drawings\Design\DI08411-shd-details.dgn		DRAWN -	REVISED -
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	PLOT DATE = 5/12/2017	DATE -	REVISED -

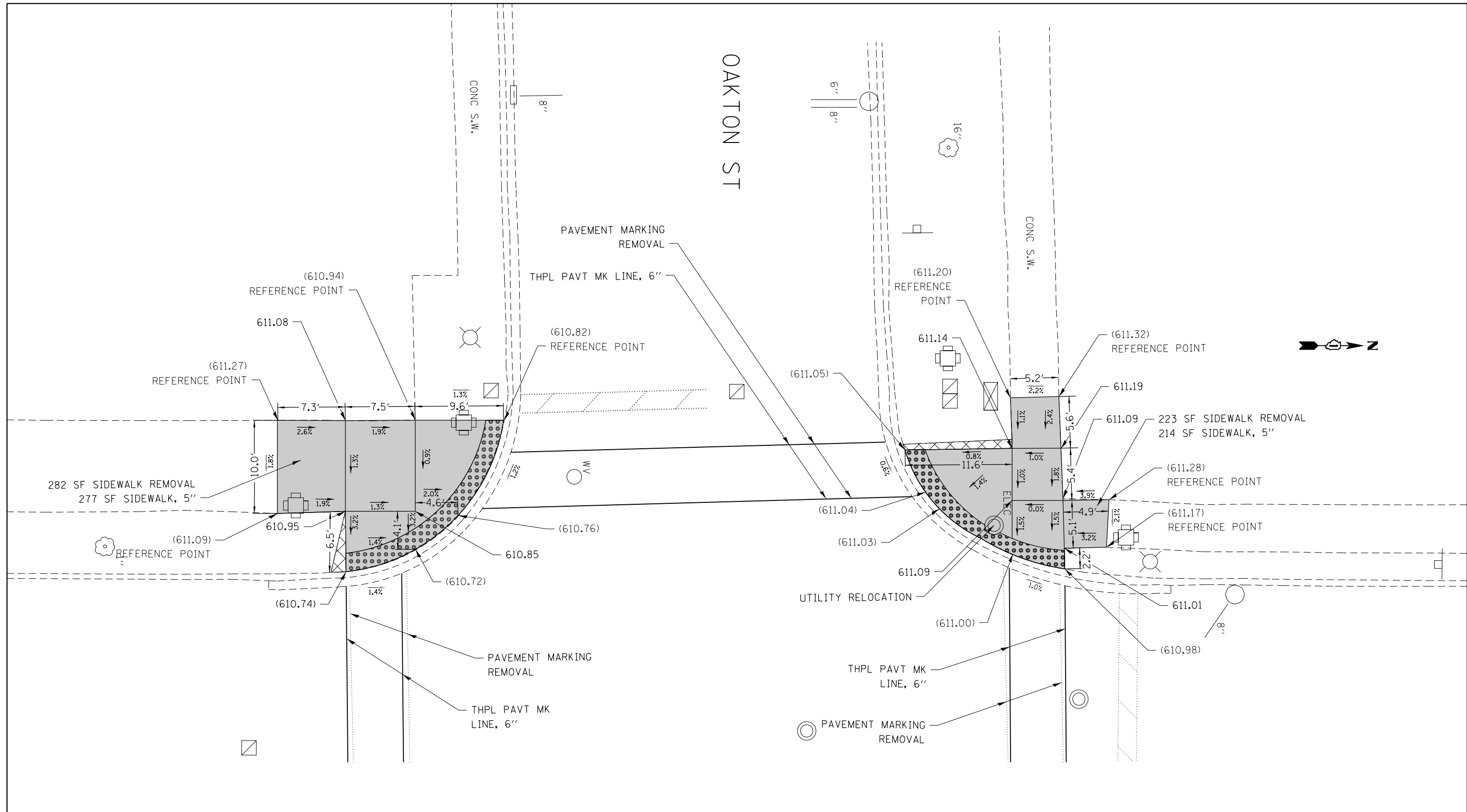
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

### SIDEWALK DETAIL PLAN ASBURY AVE. AT AUSTIN ST.

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	9K	25
CONTRACT NO. 60M17				
ILLINOIS FED. AID PROJECT				

OAKTON ST



**LEGEND**

[xx.xx']

EXISTING LENGTH

( )

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB

SC

TOP OF PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 610.901

BENCHMARK: "X" CUT ON NW BOLT OF TRAFFIC SIGNAL/LIGHT (#12679)

LOCATION: NE CORNER OF ASBURY AVE. AND OAKTON ST.

FILE NAME =	USER NAME = PEROTTIRR	DESIGNED -	REVISED - RP 5/12/2017
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Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

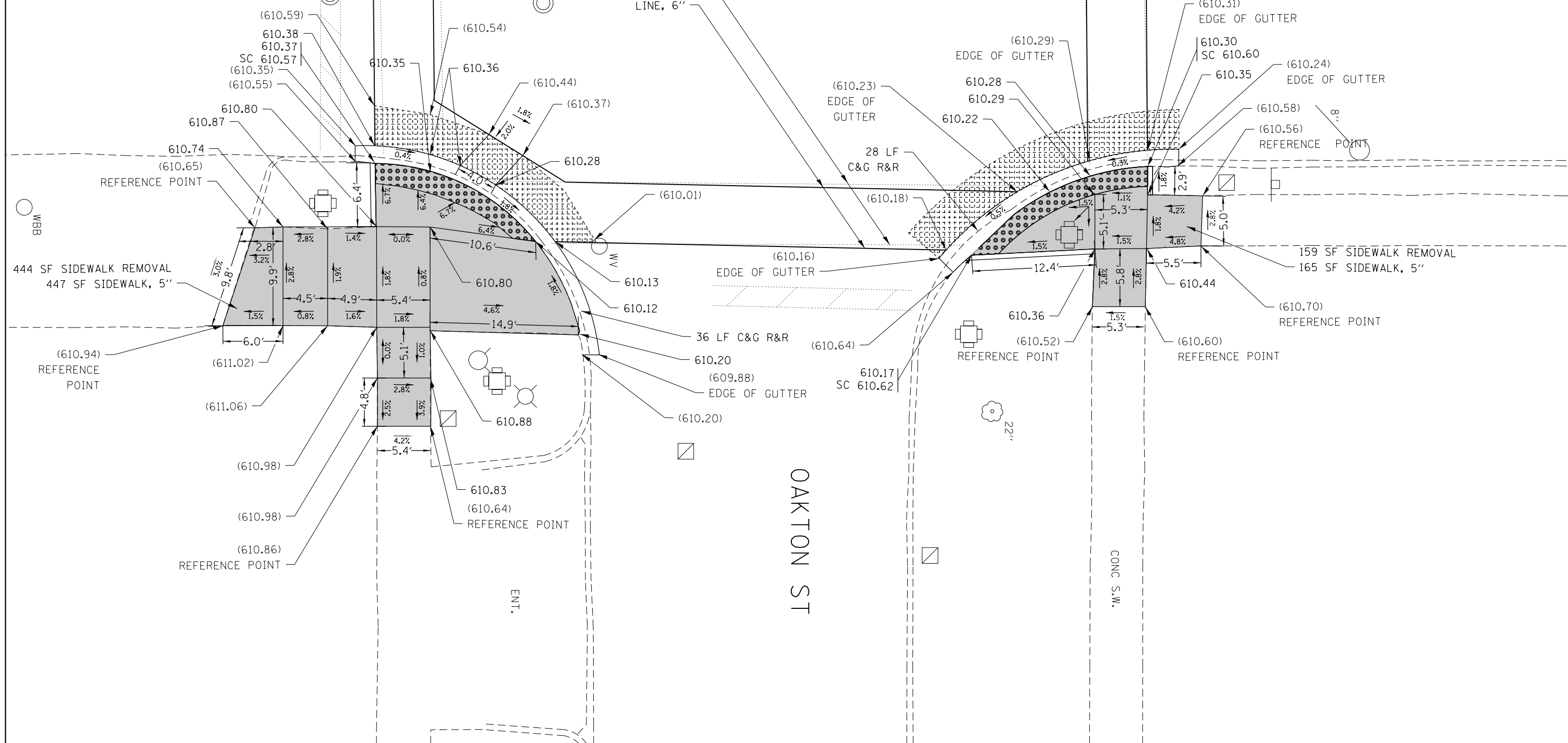
**SIDEWALK DETAIL PLAN  
ASBURY AVE. AT OAKTON ST.**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	9L	25
CONTRACT NO. 60M17				

ILLINOIS FED. AID PROJECT

ASBURY AVE.



**LEGEND**

xx.xx'

EXISTING LENGTH

( )

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB

SC

TOP OF PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 610.901

BENCHMARK: "X" CUT ON NW BOLT OF TRAFFIC SIGNAL/LIGHT (\*12679)

LOCATION: NE CORNER OF ASBURY AVE. AND OAKTON ST.

FILE NAME =	USER NAME = PEROTTIR	DESIGNED -	REVISED - RP 5/12/2017
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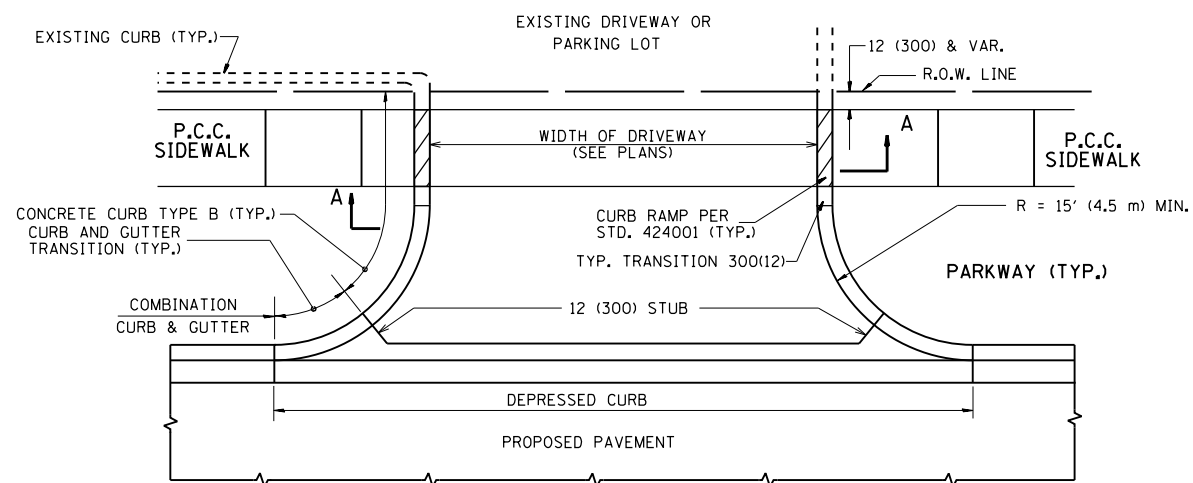
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN  
ASBURY AVE. AT OAKTON ST.**

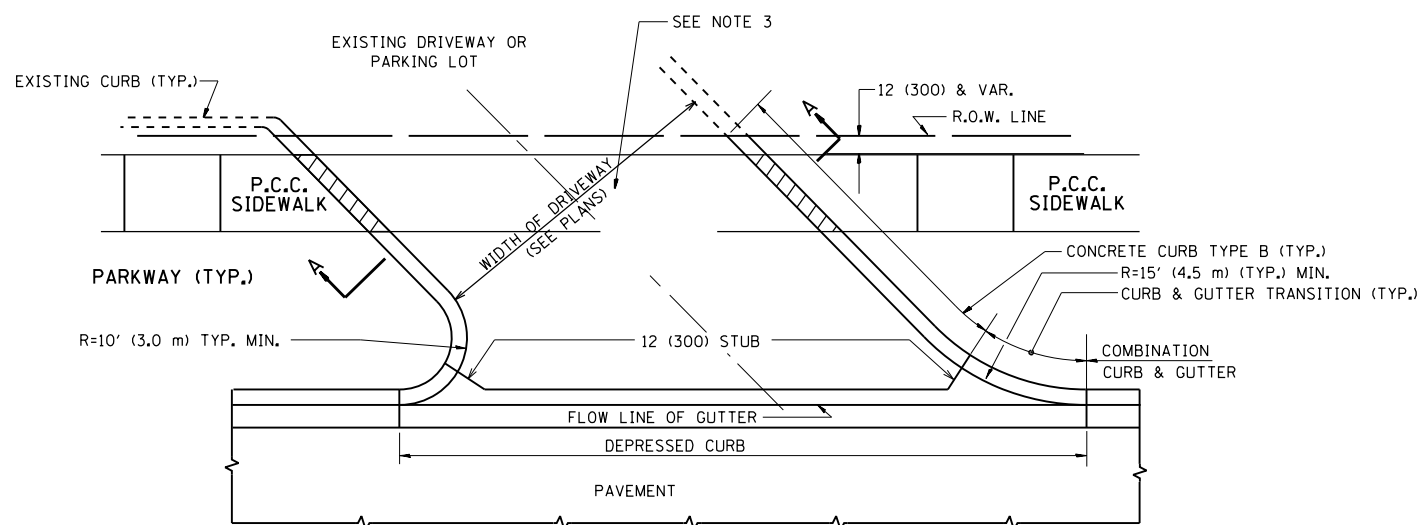
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	9M	25
CONTRACT NO. 60M17				
ILLINOIS FED. AID PROJECT				

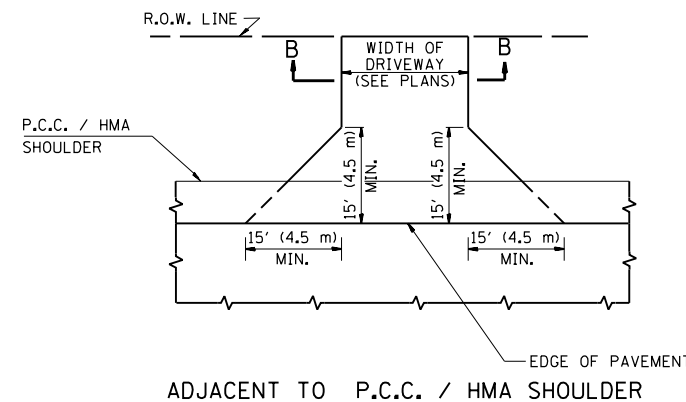
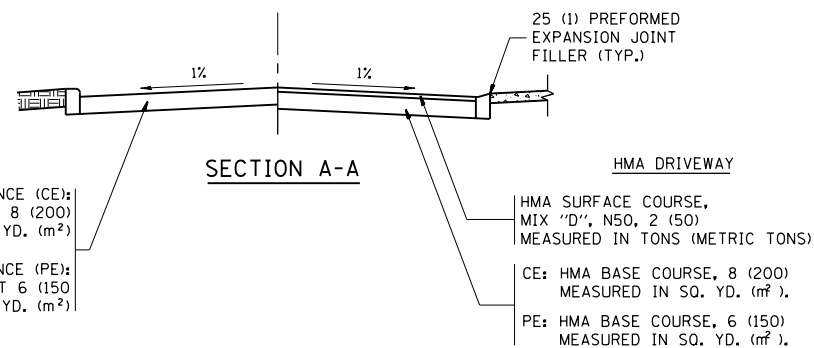




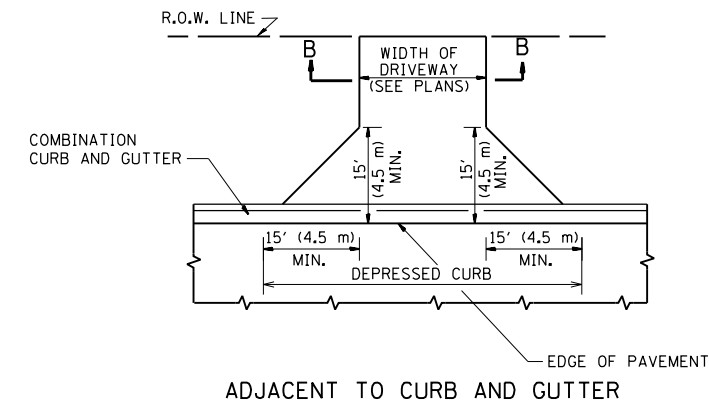
WITH CONCRETE CURB, TYPE B



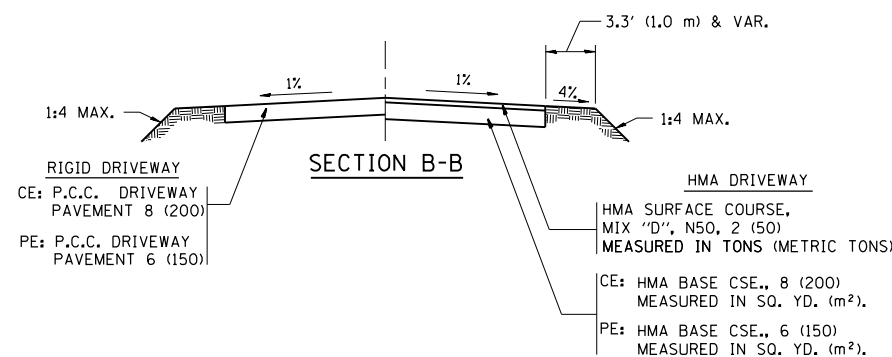
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m²).

**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

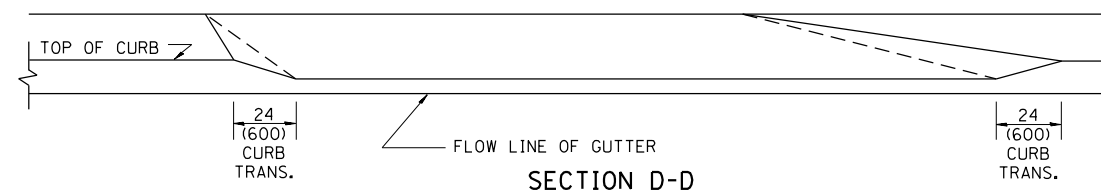
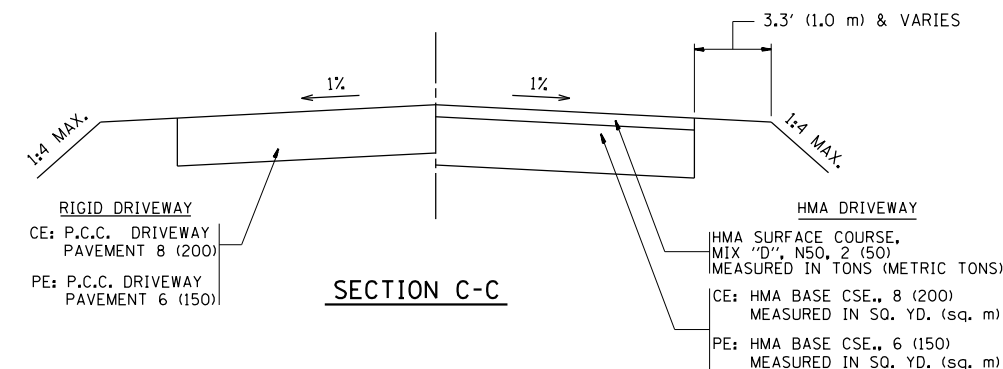
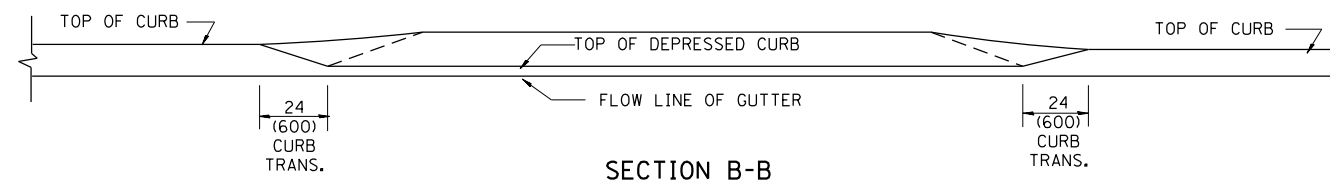
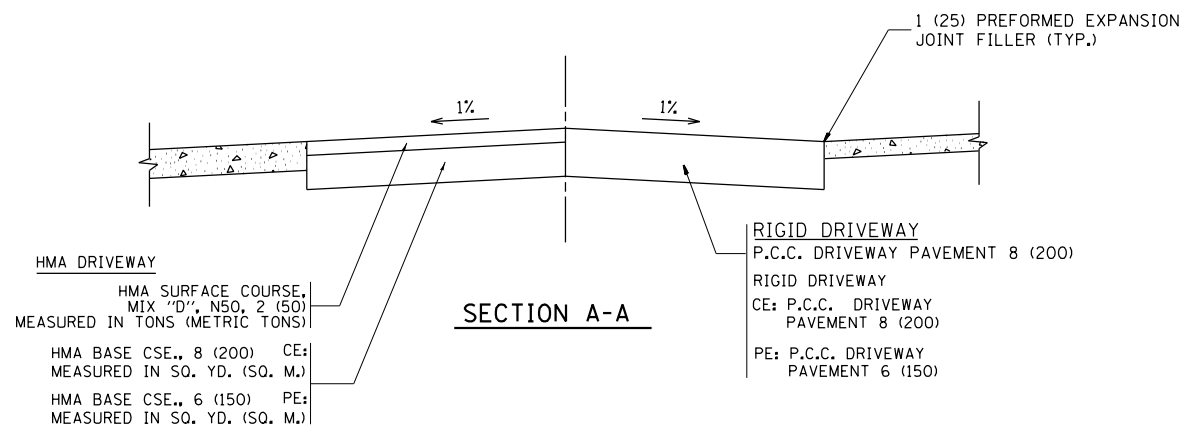
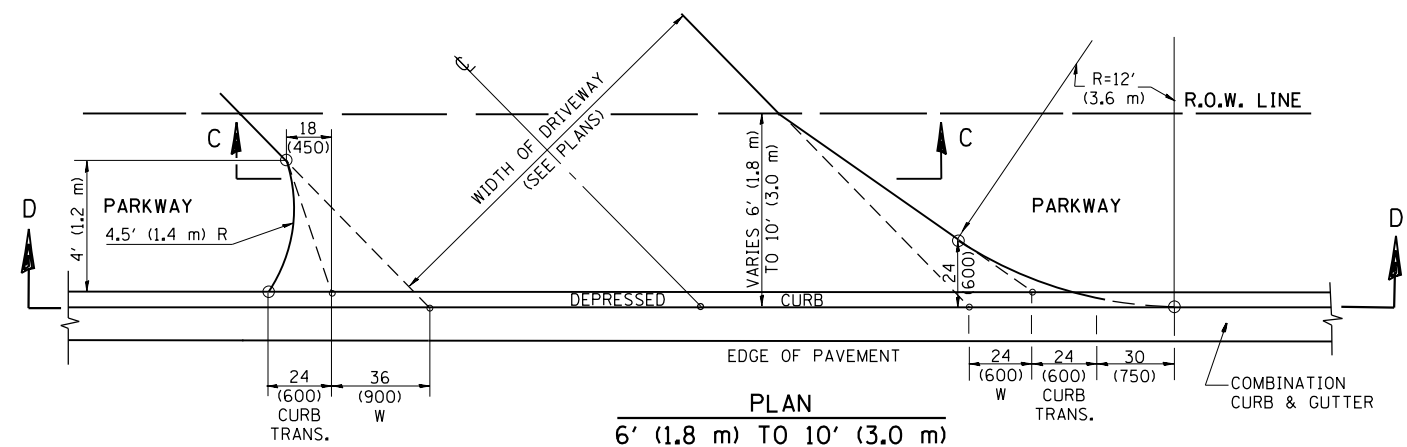
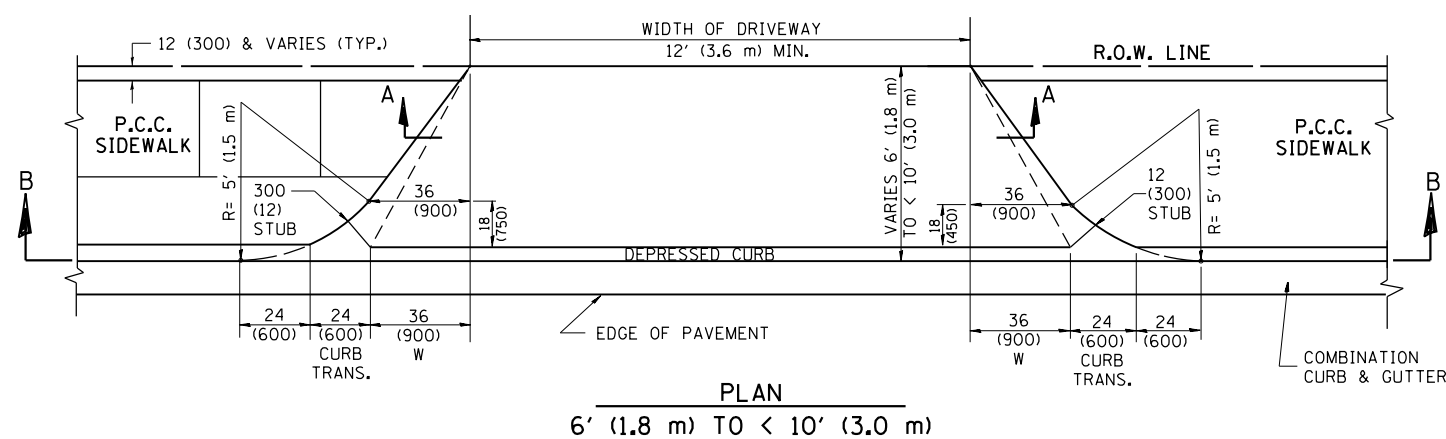
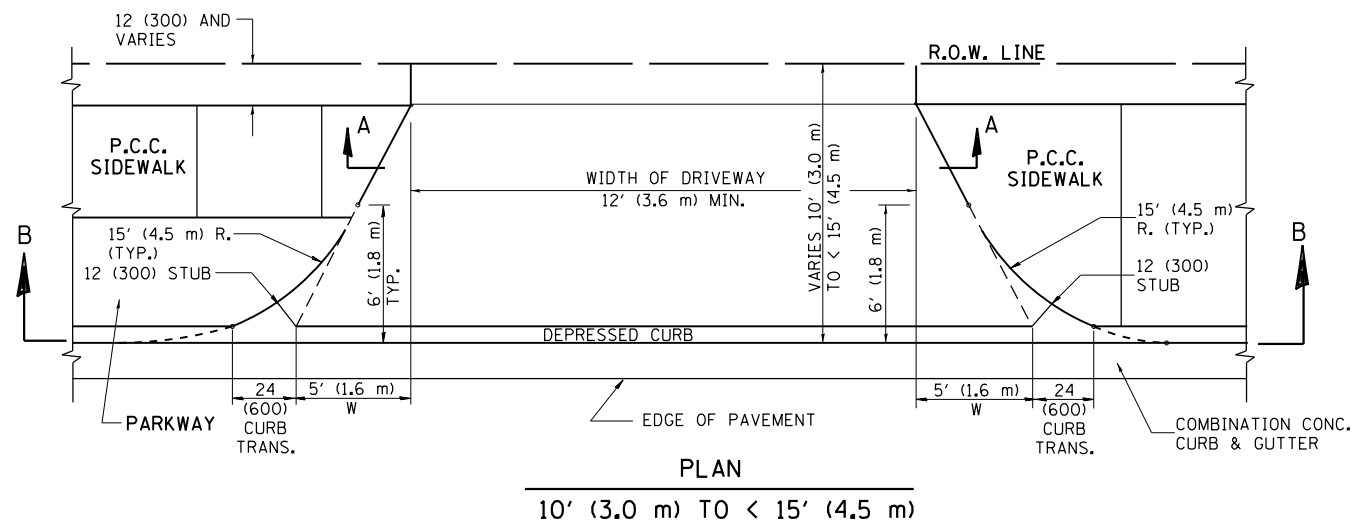
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		DATE - 11-04-95	REVISED - R. BORO 06-11-08
			REVISED - R. BORO 09-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB &amp; EDGE OF SHOULDER &gt;= 15' (4.5 m)</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	25	11
<b>BD0156-07 (BD-01)</b>		<b>CONTRACT NO. 60M17</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**GENERAL NOTES**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

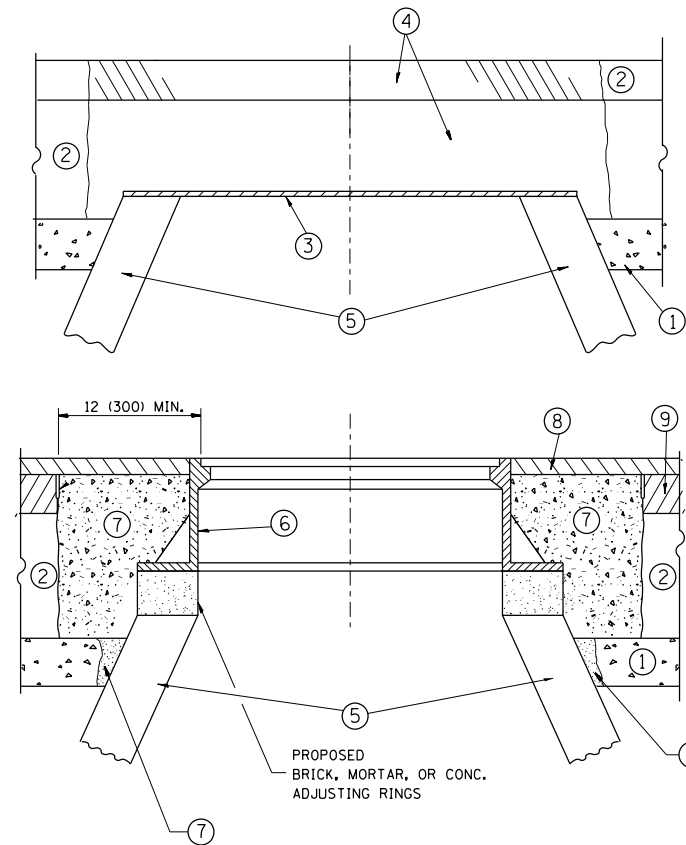
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

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PLOT SCALE = 100.0000' / 1"		DATE - 11-06-95	REVISED - R. BORO 01-01-07
PLOT DATE = 4/4/2017			REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS			
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-R5	COOK	25	12
BD400-02 (BD-02)			CONTRACT NO. 60M17	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

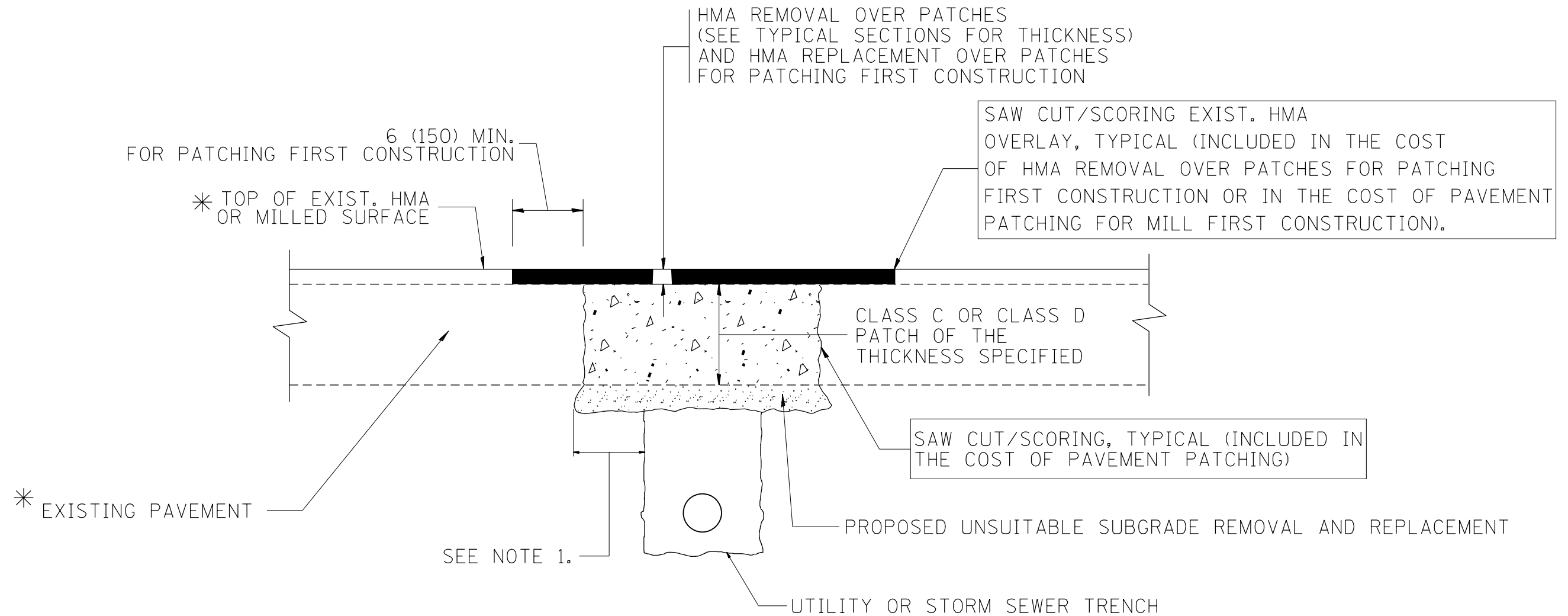
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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	PLOT DATE = 4/4/2017	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	25	13
<b>BD600-03 (BD-8)</b>		<b>CONTRACT NO. 60M17</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = PEROTTIR	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI084\DRAWING\Design\DistStd.dgn		REVISOR - R. BORO 01-01-07	REVISED - R. BORO 01-01-07					3732	3348-RS	COOK	25	14
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISOR - R. BORO 09-04-07	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)		CONTRACT NO. 60M17					
PLOT DATE = 4/4/2017	DATE - 10-25-94	REVISED - K. ENG 10-27-08	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

T/2 \*

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

3" (75) MIN.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

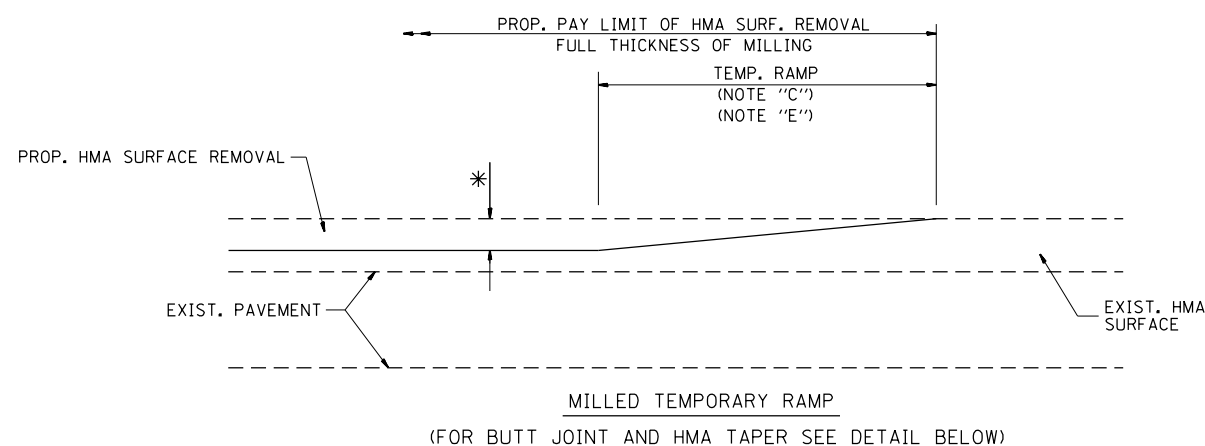
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

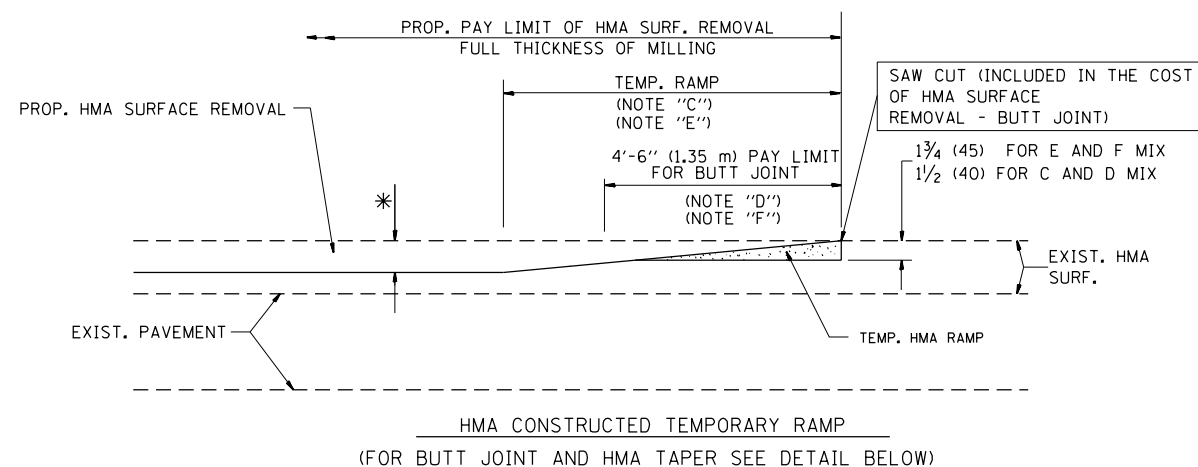
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = PEROTTIR	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.U. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI084\Drawings\Design\DistStd.dgn		REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			3732	3348-R5	COOK	25	15
PLOT SCALE = 100.0000' / 1".	CHECKED -	REVISED - R. BORO 12-15-09				BD600-06 (BD-24)		CONTRACT NO. 60M17		
PLOT DATE = 4/4/2017	DATE - 03-11-94					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT

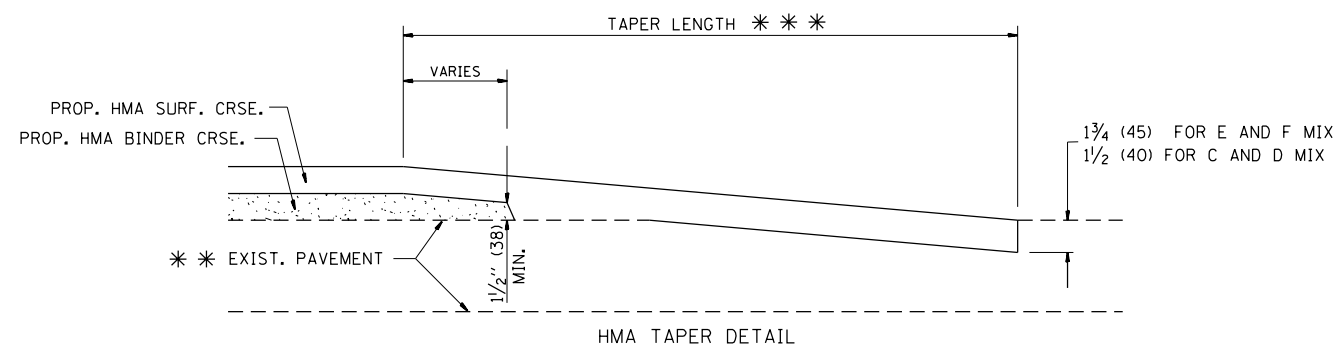
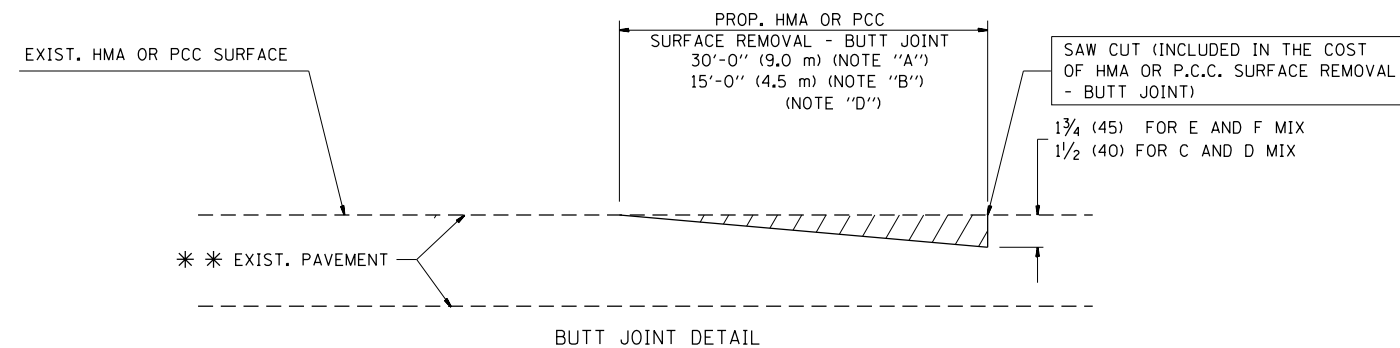


**OPTION 1**



**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

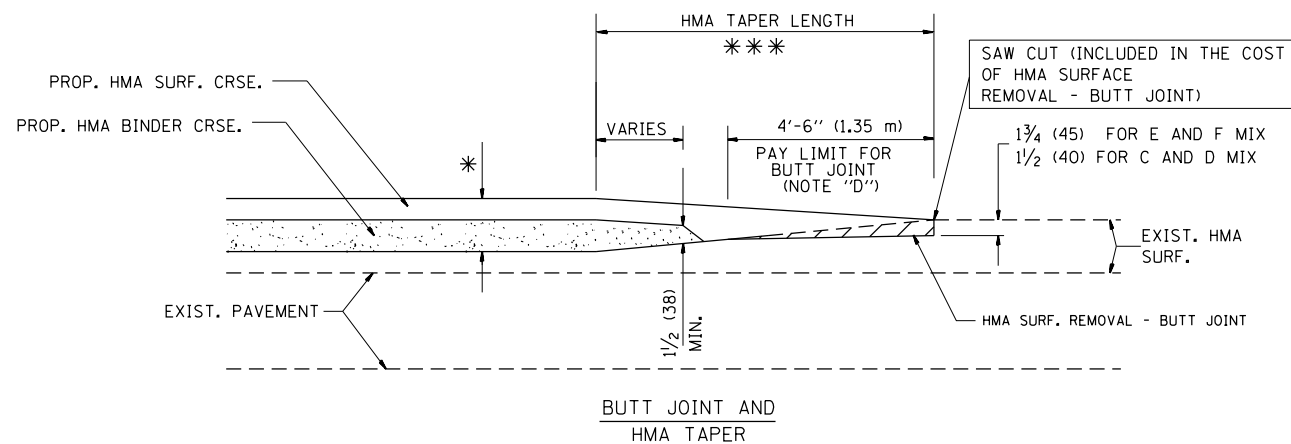
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

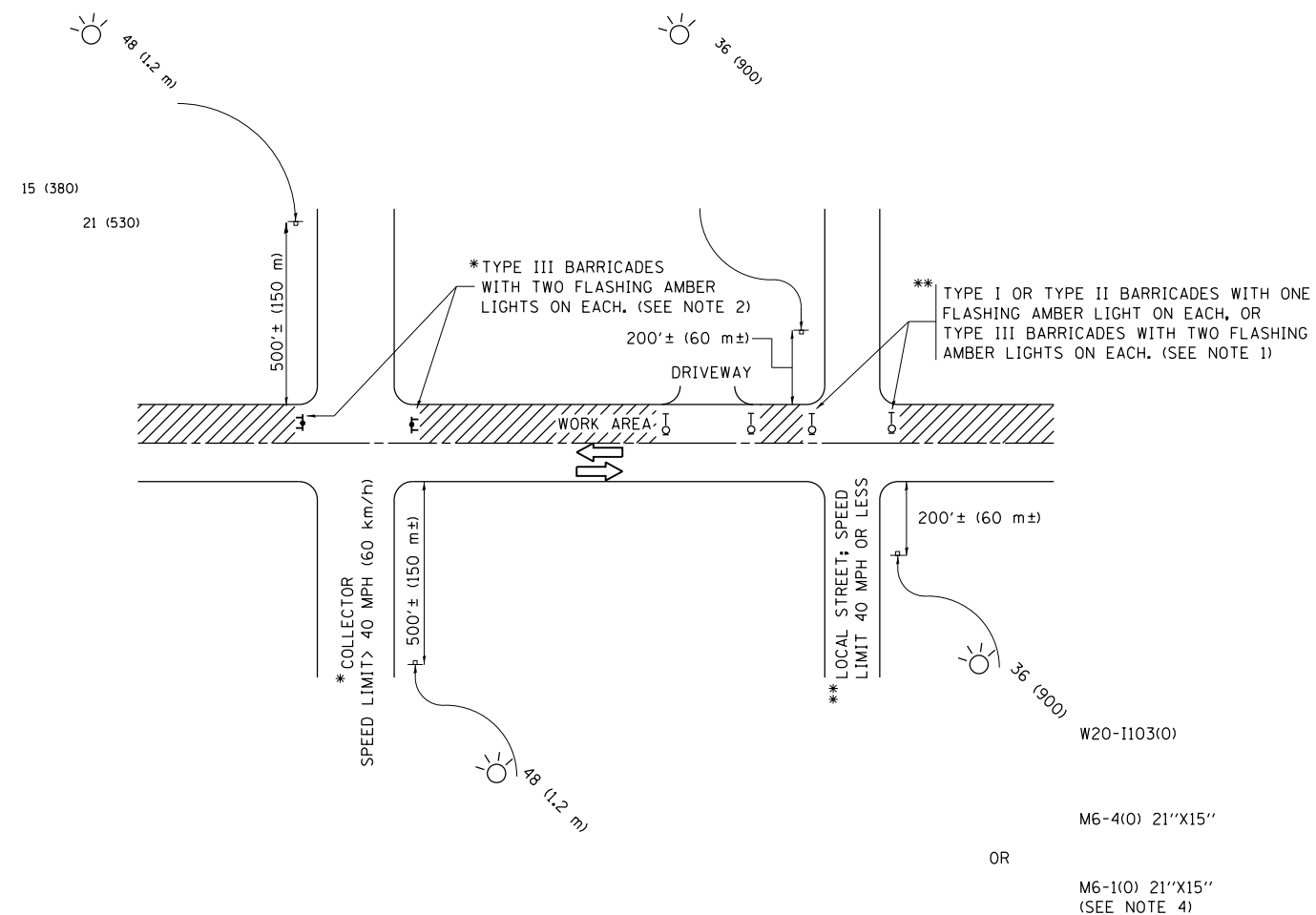
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	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 4/4/2017	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-R5	COOK	25	16
BD400-05 BD32		CONTRACT NO. 60M17		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

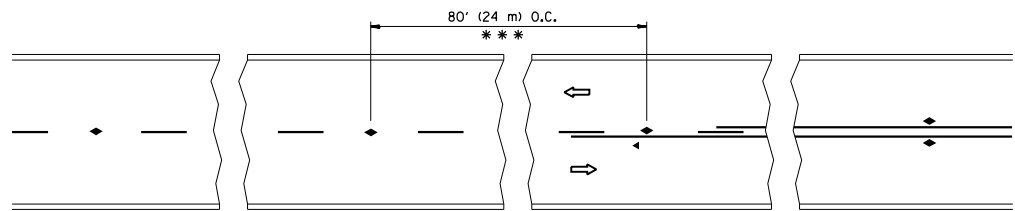
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	PLOT DATE = 4/4/2017	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

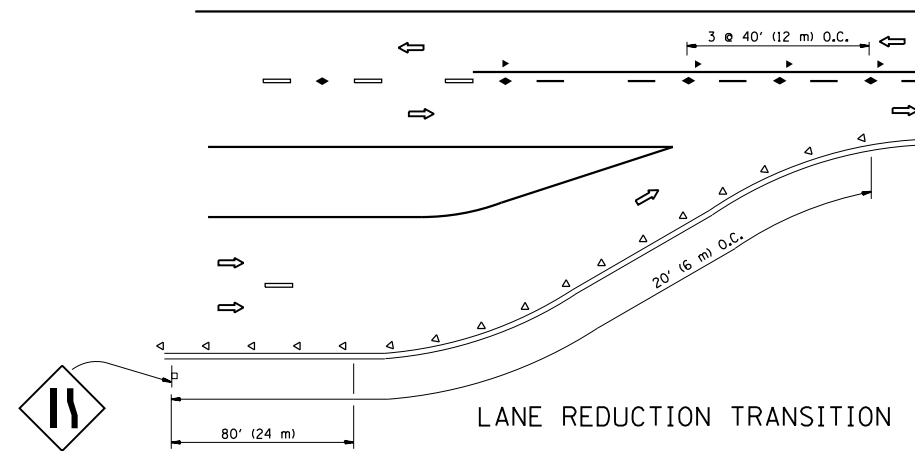
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	25	17
<b>TC-10</b>			<b>CONTRACT NO. 60M17</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

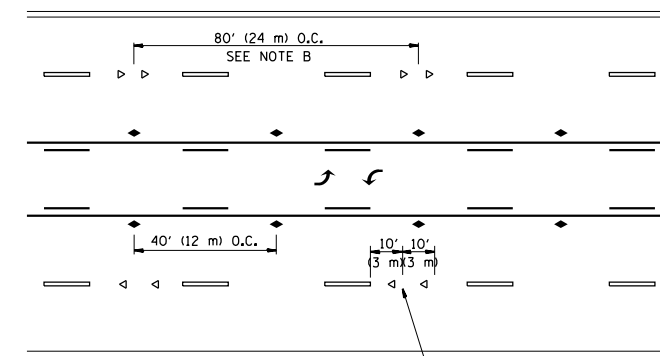


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

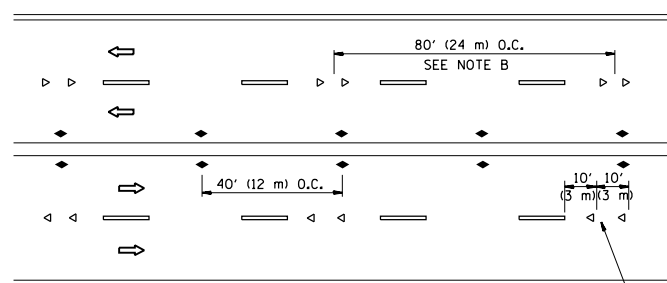
TWO-LANE/TWO-WAY



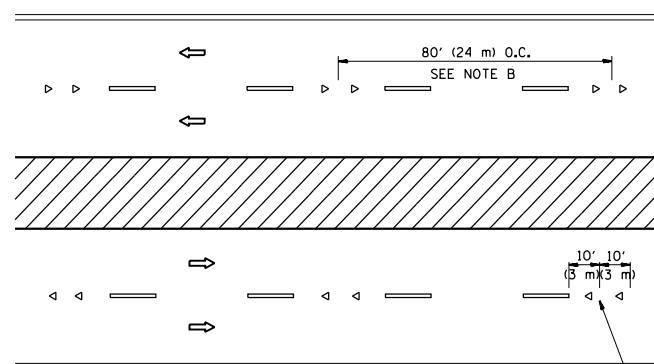
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

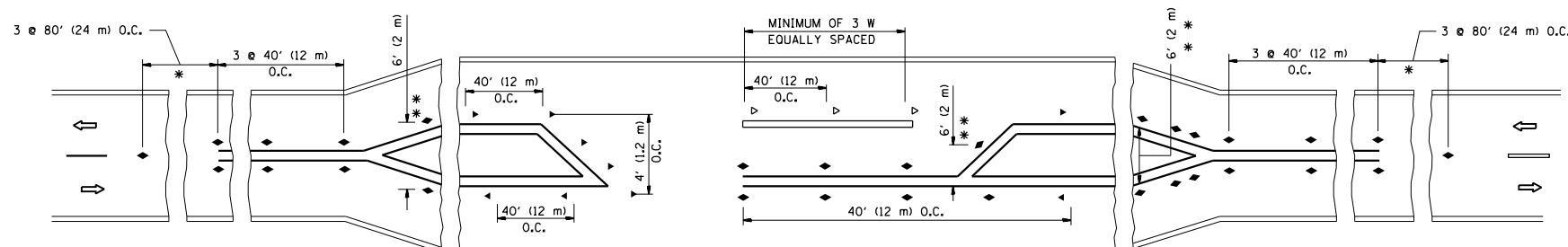
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

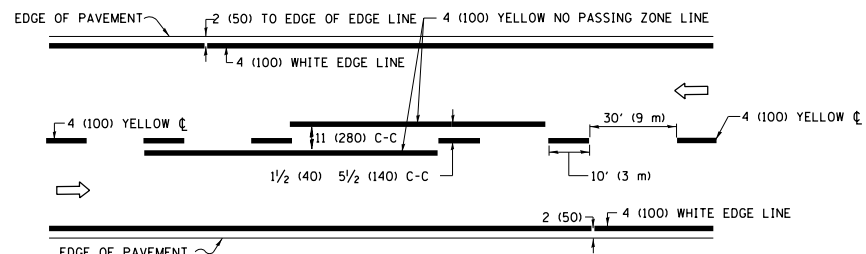
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = PEROTTIR	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI084\Drawings\Design\DistStd.dgn			REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 4/4/2017	DATE -	REVISED - C. JUCIUS 09-09-09

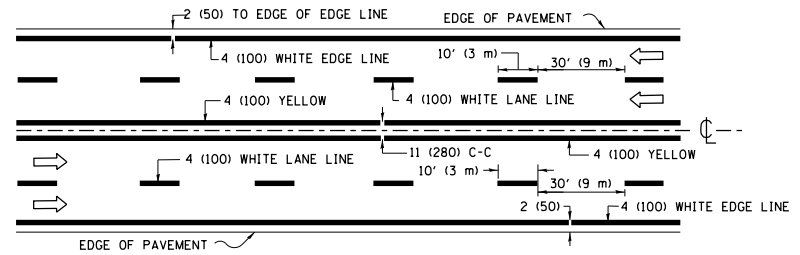
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

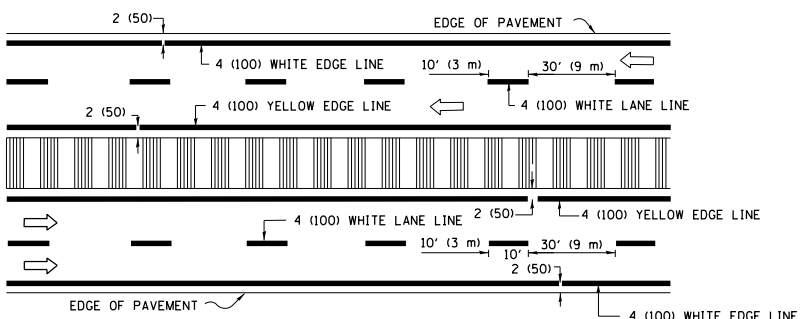
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-R5	COOK	25	18
TC-11			CONTRACT NO. 60M17	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**2-LANE ROADWAY**

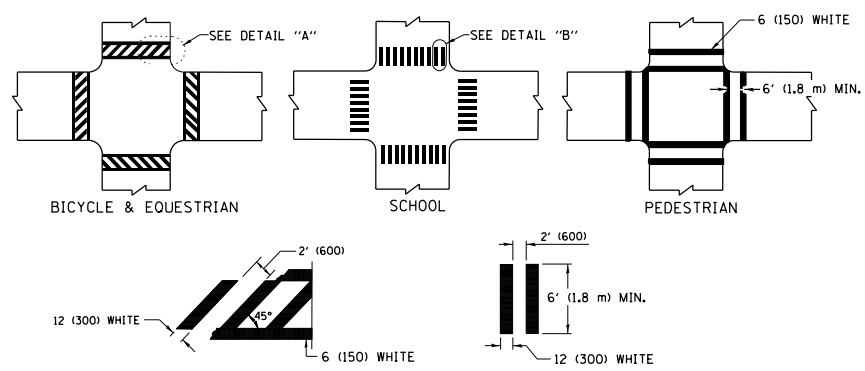


**MULTI-LANE UNDIVIDED**



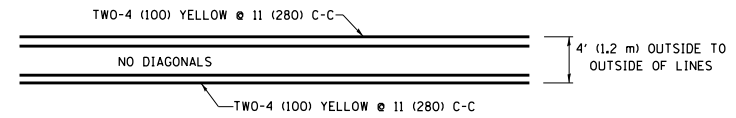
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

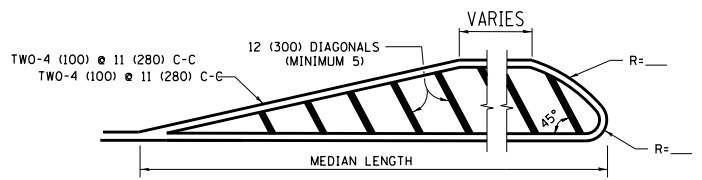


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

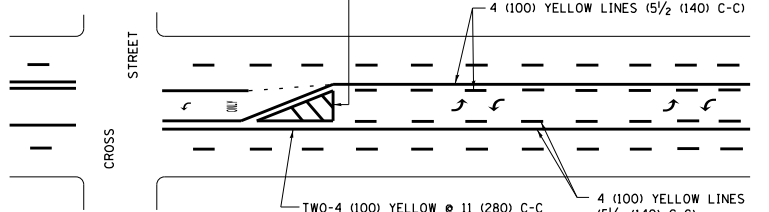


**4' (1.2 m) WIDE MEDIANS ONLY**



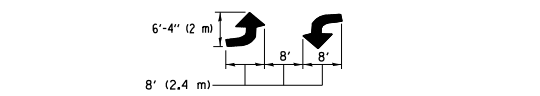
**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



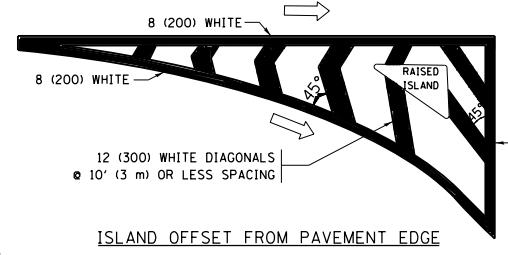
**MEDIAN WITH TWO-WAY LEFT TURN LANE  
TYPICAL PAINTED MEDIAN MARKING**

A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

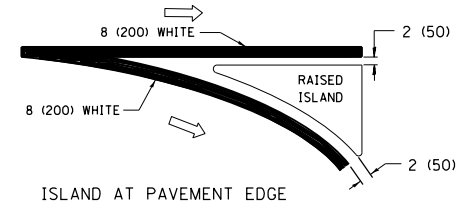


**TYPICAL LEFT (OR RIGHT) TURN LANE  
TYPICAL TURN LANE MARKING**

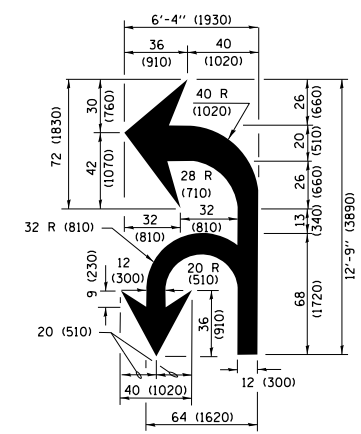
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



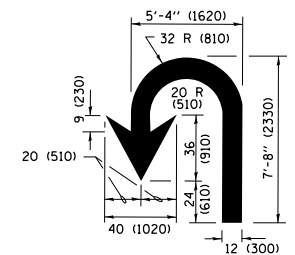
**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE  
TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = PEROTTIR	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\11\084EBIDINTEG\11\inois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01084\Drawings\Design\DistStd.dgn			REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 4/4/2017	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

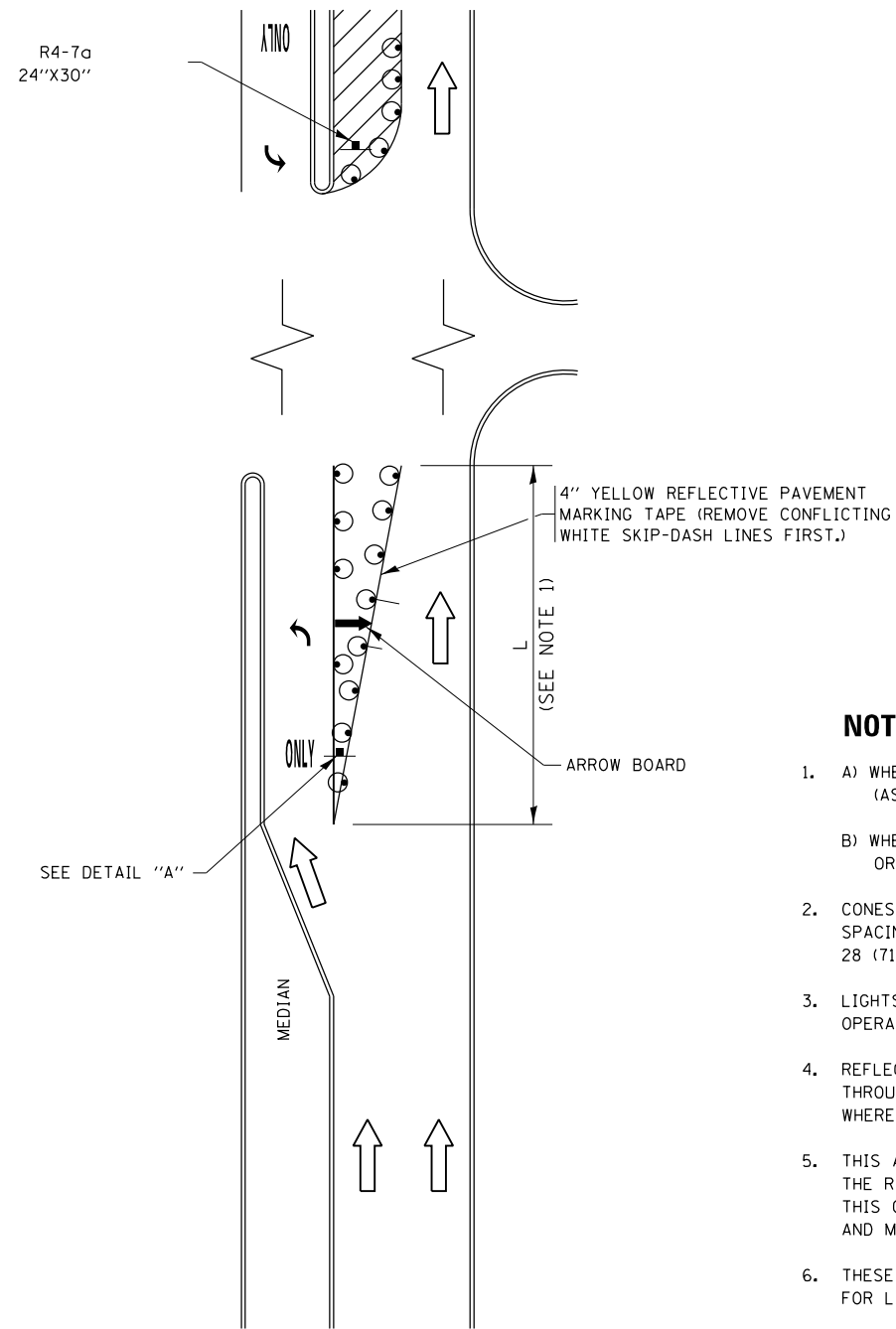
**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.
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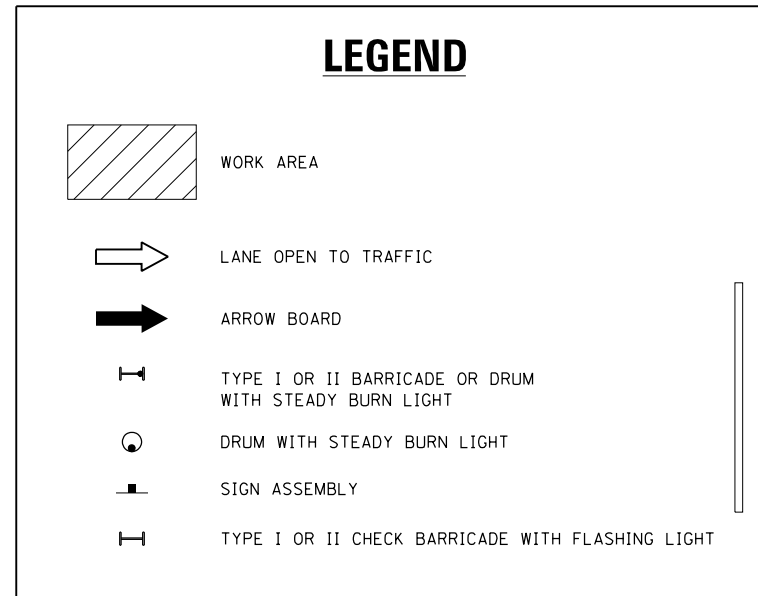
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	25	19
<b>TC-13</b>		<b>CONTRACT NO. 60M17</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



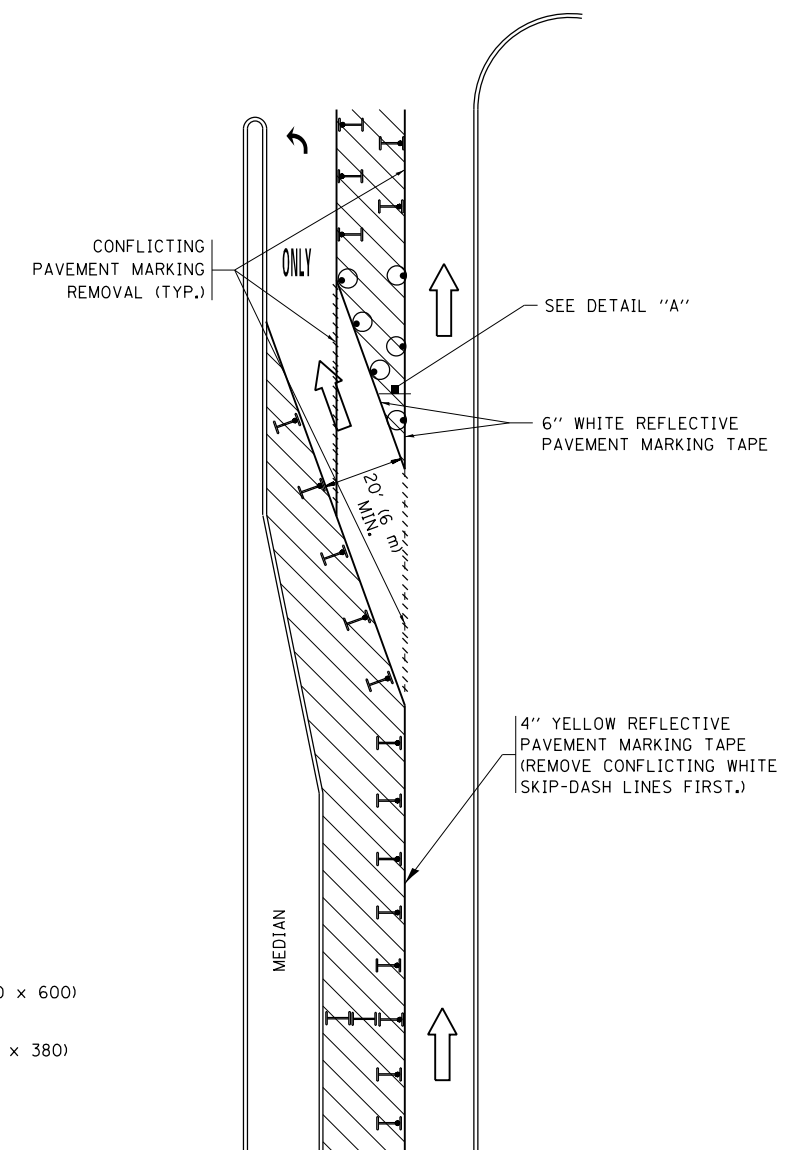
**FIGURE 1**



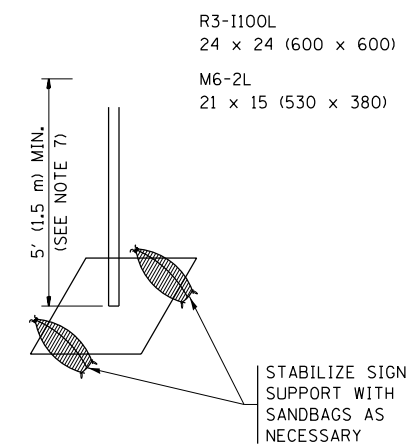
### NOTES:

1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



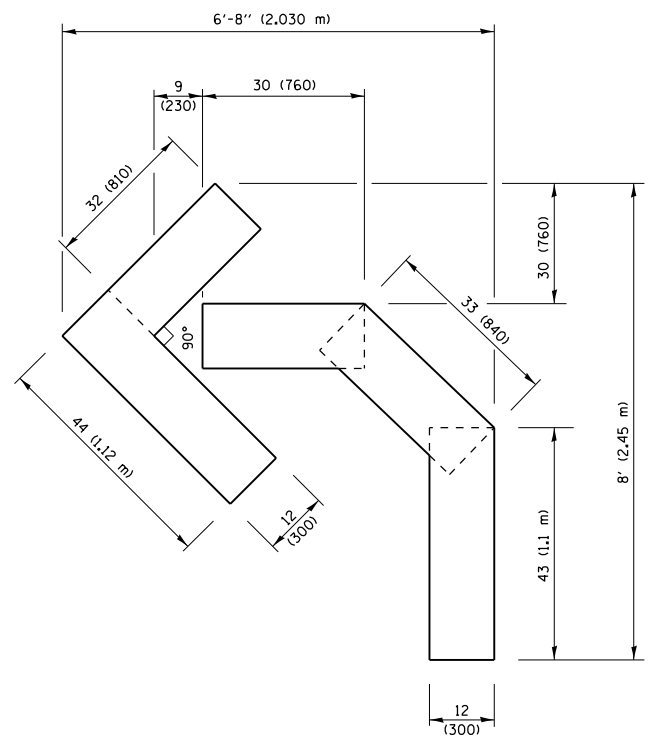
**FIGURE 2**



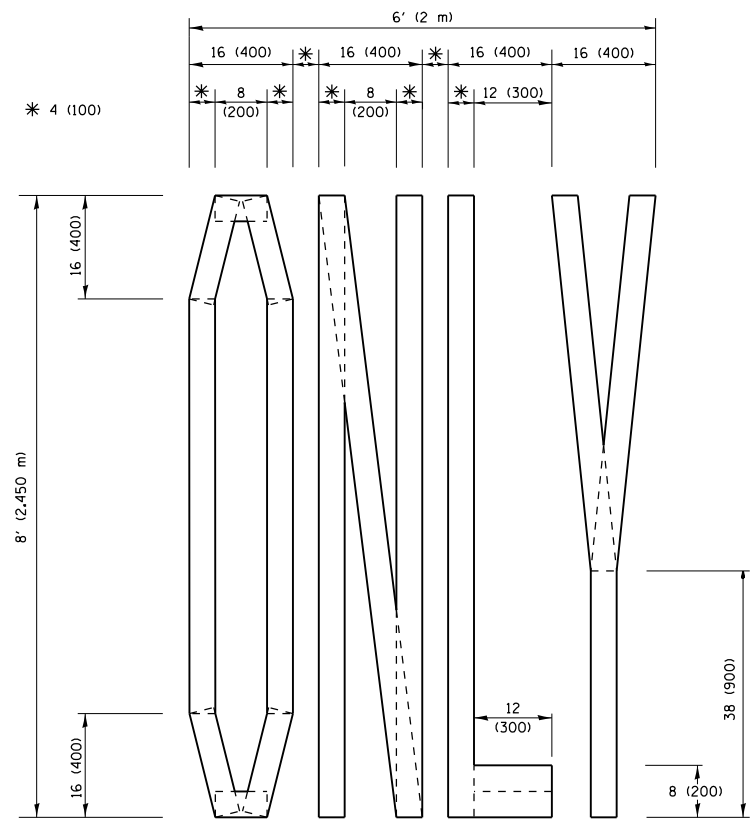
**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

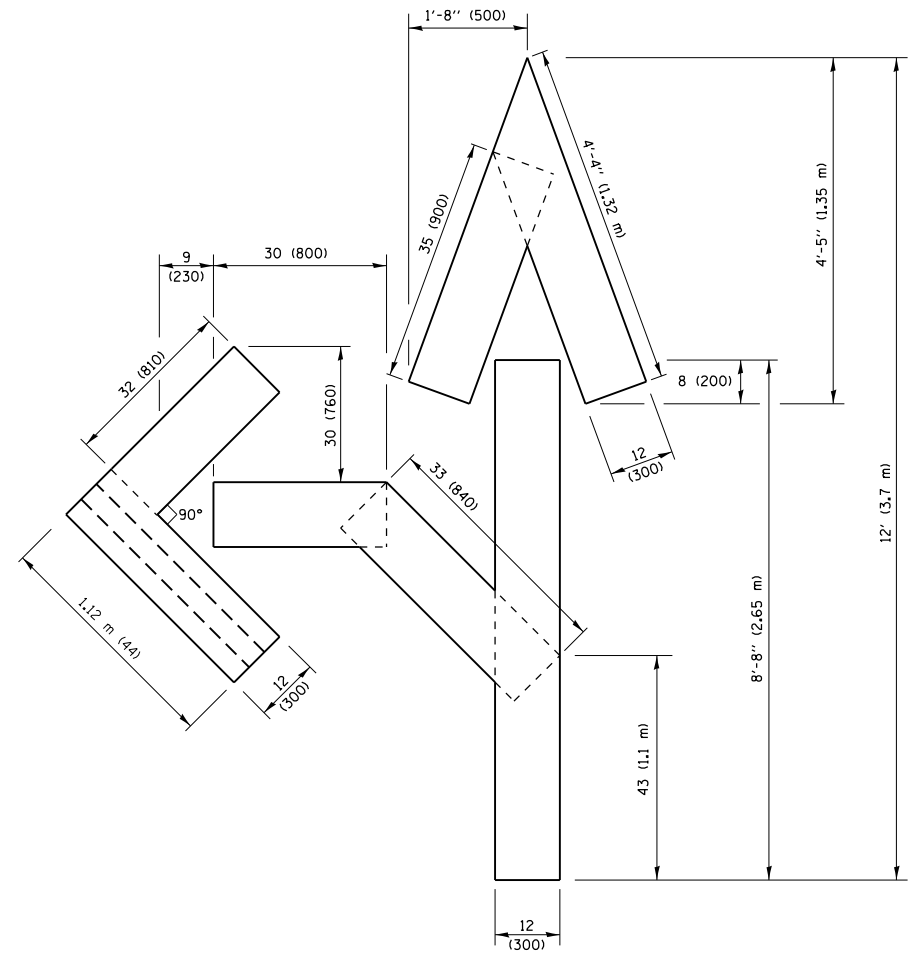
FILE NAME =	USER NAME = PEROTTIR	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>			F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default		REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16					3732	3348-R5	COOK	25	20	
	PLOT SCALE = 100.0000' / in.	REVISED - T. RAMMACHER 01-06-00	REVISED -		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	<b>TC-14</b>			CONTRACT NO. 60M17
	PLOT DATE = 4/4/2017							FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

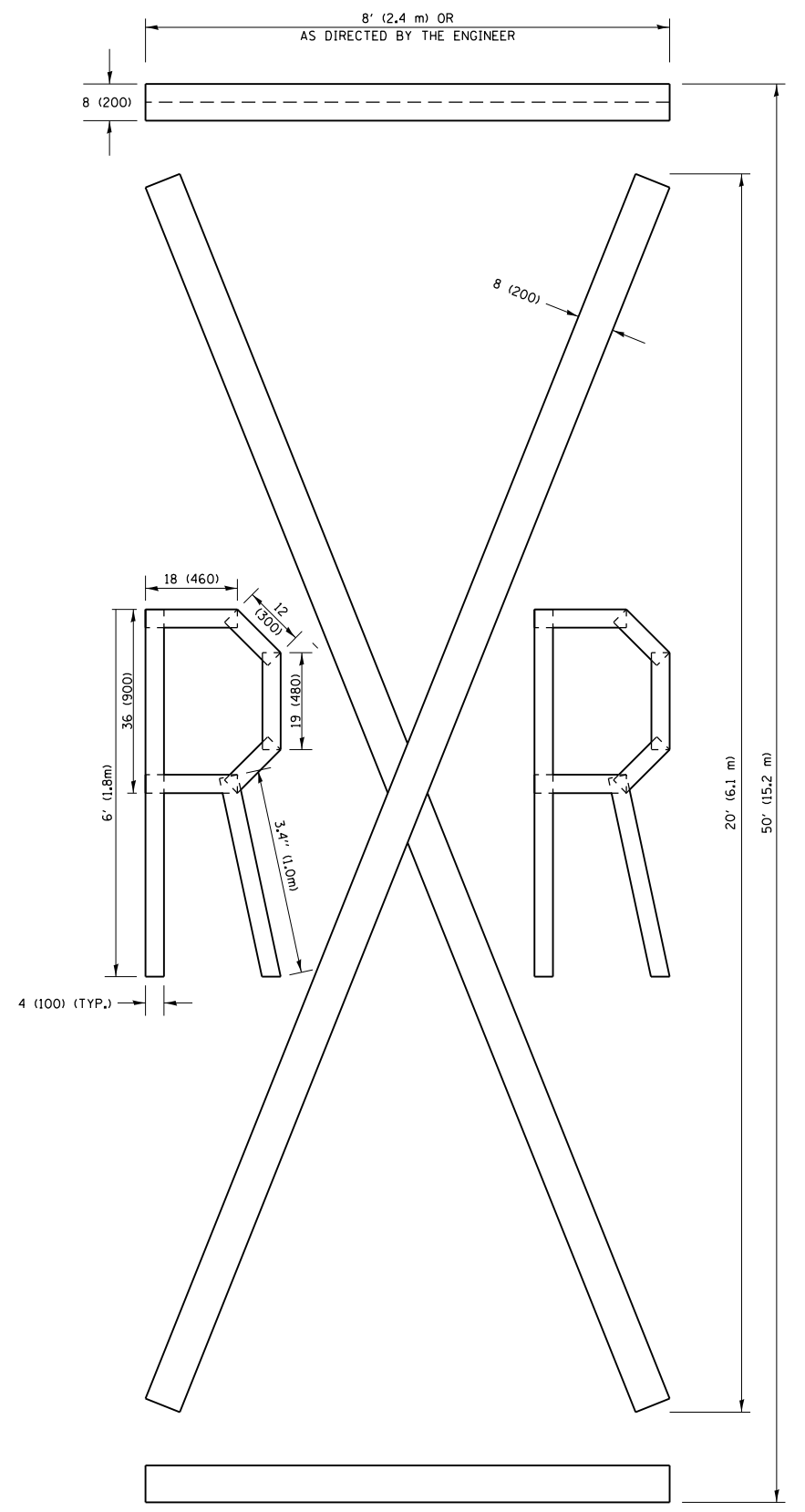


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

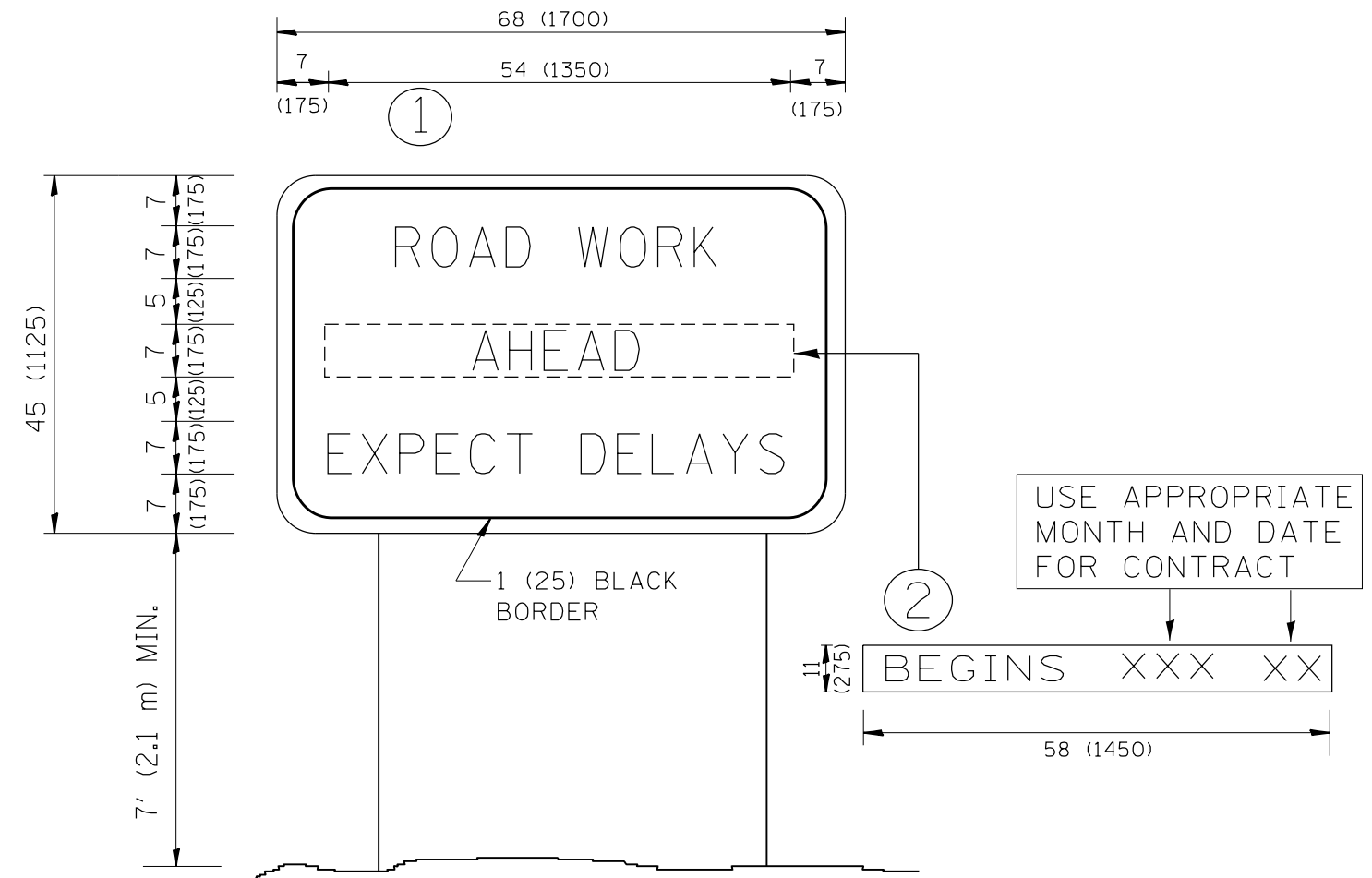
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = PEROTTIR	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
pw\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI184\Drawings\Design\DistStd.dgn			REVISED - E. GOMEZ 08-28-00
		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-RS	COOK	25	21
<b>TC-16</b>		<b>CONTRACT NO. 60M17</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

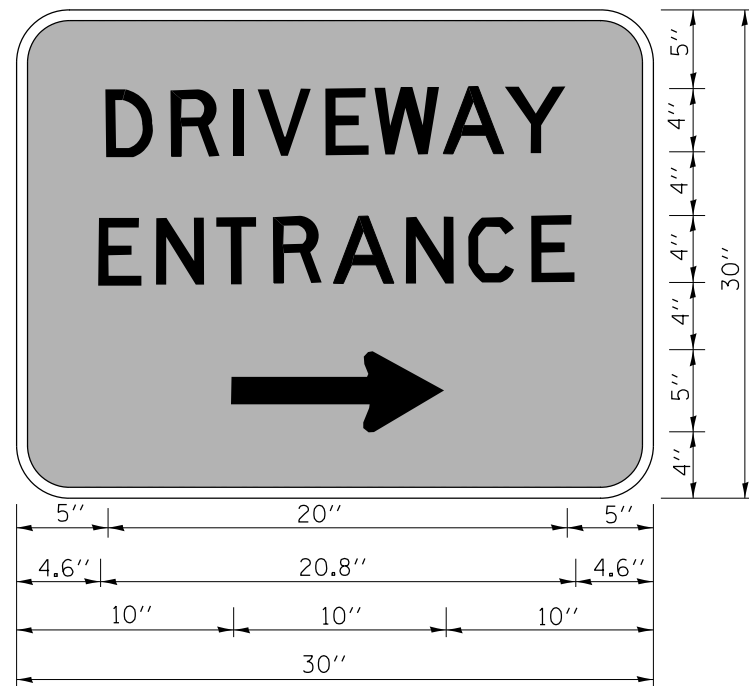


**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = PEROTTIR	DESIGNED -	REVISED - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI084\Drawings\Design\DistStd.dgn			REVISED - R. MIRS 12-11-97			3732	3348-R5	COOK	25	22
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99				<b>TC-22</b>		<b>CONTRACT NO. 60M17</b>		
PLOT DATE = 4/4/2017	DATE -	REVISED - C. JUCIUS 01-31-07	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE  
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)  
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY  
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE  
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = PEROTTIRR	DESIGNED -	REVISED - C. JUCIUS 02-15-07
p:\11\084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI084\Drawings\Design\DistStd.dgn		DRAWN -	REVISED -
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	PLOT DATE = 4/4/2017	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY ENTRANCE SIGNING**

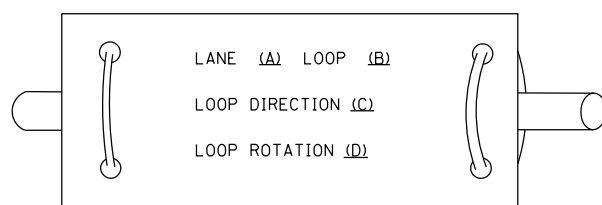
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3732	3348-R5	COOK	25	23
<b>TC-26</b>			<b>CONTRACT NO. 60M17</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

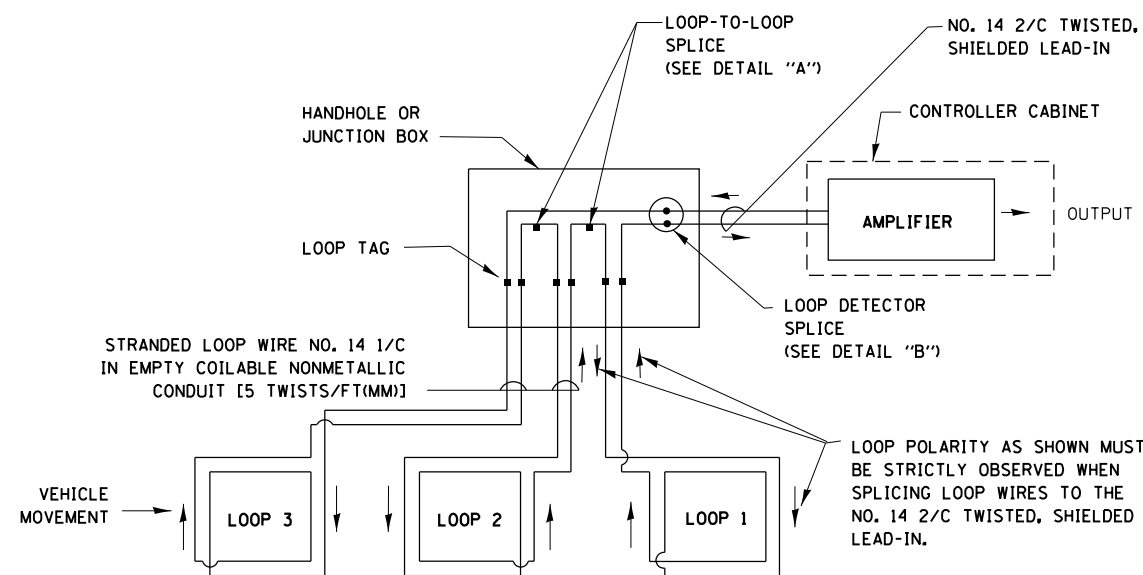
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

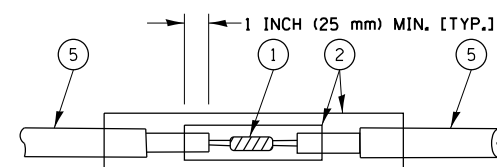


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

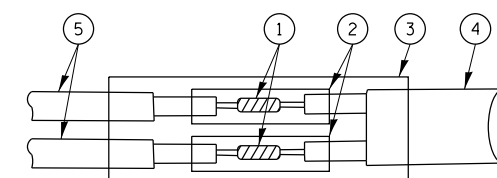


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

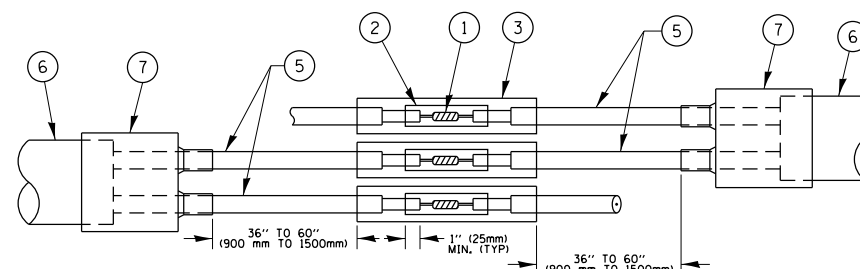


DETAIL "A"  
LOOP-TO-LOOP SPLICE

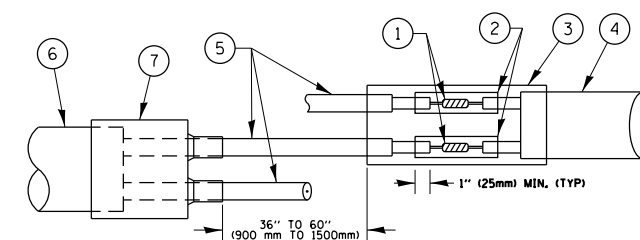


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**TYPE I LOOP**



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**PRE-FORMED LOOP**

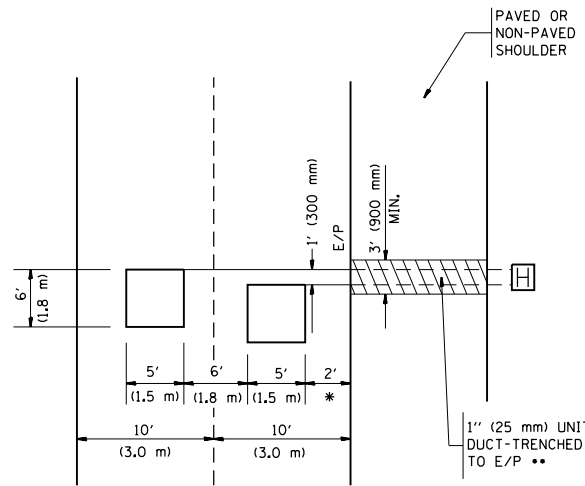
**LOOP DETECTOR SPLICE**

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH, THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PRE-FORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = PEROTTIRR	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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Default	PLOT DATE = 4/4/2017	DATE -	REVISED -			<b>TS-05</b>		<b>CONTRACT NO. 60M17</b>		<b>ILLINOIS FED. AID PROJECT</b>	
						SCALE: NONE	SHEET 2 OF 7 SHEETS	STA. TO STA.			

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



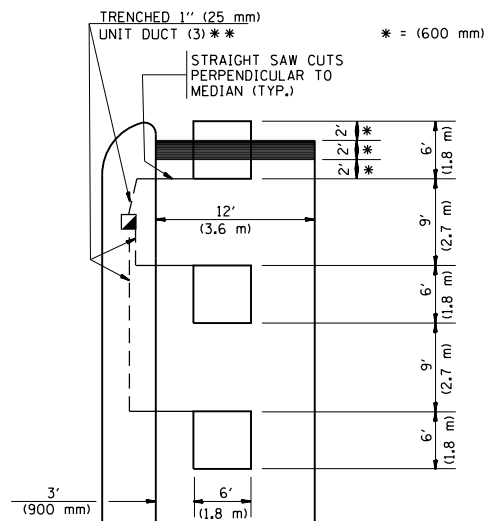
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

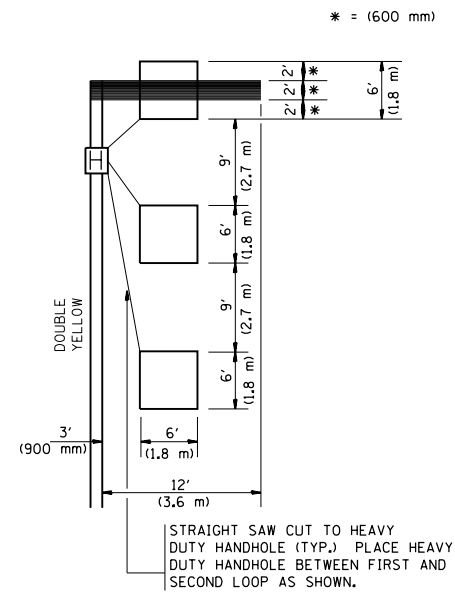


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

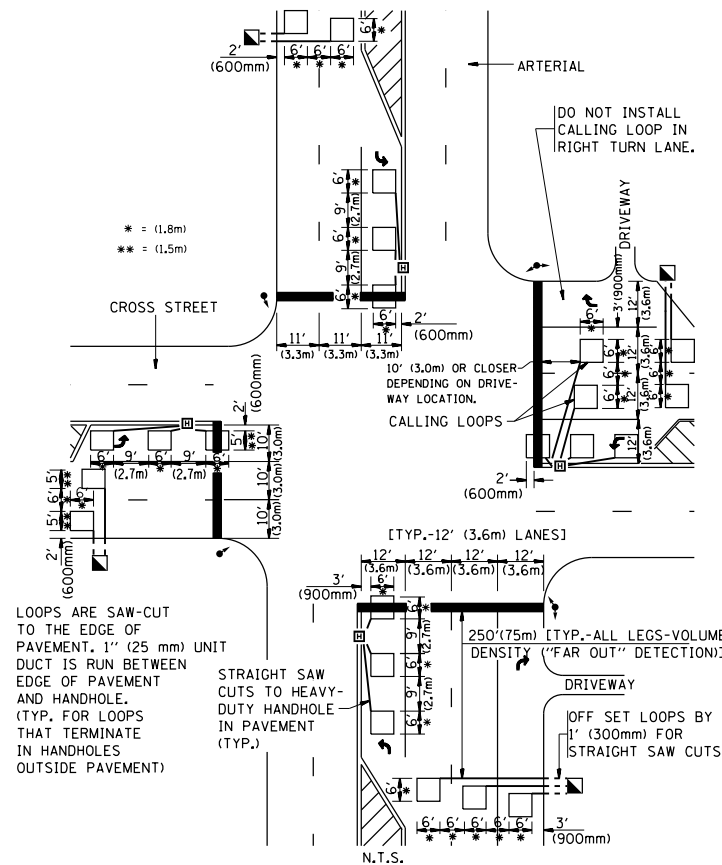
LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



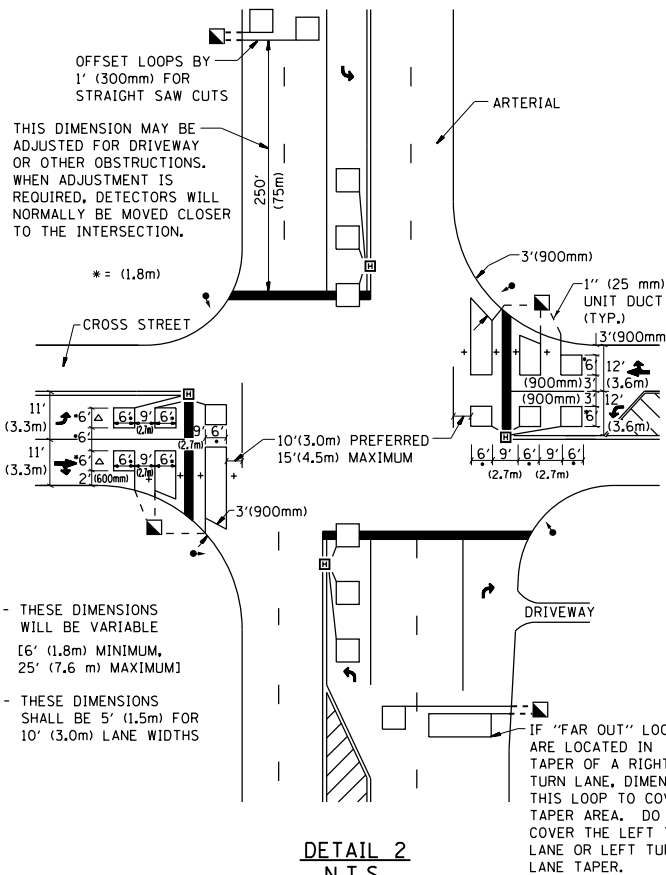
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1  
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2  
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = PEROTTIR	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>			F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI084\Drawings\Design\DistStd.dgn		CHECKED - R.K.F.	REVISED -		3732	3348-RS	COOK	25	25			
PLOT SCALE = 100.0000' / 1in.		DATE -	REVISED -		<b>TS-07</b>			<b>CONTRACT NO. 60M17</b>				
PLOT DATE = 4/4/2017			REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			