



License No. 184-000613

CONSULTANTS

APRIL 20, 2018

REHABILITATE T-HANGAR TX PAVEMENT PHASE 1

OWNER



GENERAL WAYNE A. DOWNING PEORIA INTERNATIONAL AIRPORT PEORIA, ILLINOIS

MARK | DATE | DESCRIPTION

AIP PROJ. NO. 3-17-0080-XX
IL PROJ. NO. PIA-4637 CONTRACT NO. PEXXX
CMT PROJECT NO: 17061-06
CAD DWG FILE: 17061-06_GC001.DWG
DESIGNED BY: HWI
DRAWN BY: CMT
CHECKED BY: JMW
APPROVED BY: RLV
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CONSTRUCTION SAFETY PHASING PLAN NOTES

SHEET 3 OF 16 GC-101

GENERAL

1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G (OR LATEST VERSION), AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G (OR LATEST VERSION). NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP IS SPECIALIZED TO AIRPORT OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEANS AND METHODS FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

1. PHASING SHALL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE CONSTRUCTION ACTIVITY PLANS.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE AIRPORT SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN.
3. CONTRACTOR EMPLOYEES SHALL OBTAIN AN AIRPORT IDENTIFICATION BADGE AS REQUIRED BY THE AIRPORT. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
4. ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATIONS AREA (AOA) SHALL ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
5. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA, THE TRUCK DRIVERS MUST BE ESCORTED BY THE CONTRACTOR.
6. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN DRIVING/WORKING IN MOVEMENT AREAS OF THE AIRFIELD. THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS.
7. FLAGMEN TO BE USED DURING THE CONSTRUCTION OF THE PROJECT IN THE LOCATIONS CALLED OUT IN THE PLAN SET SHALL BE FIELD ENDORSED BY THE AIRPORT.
8. THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN ON THE AIRPORT SITE PLAN.
9. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
10. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
11. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
12. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
13. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
14. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
15. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
16. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF NORMAL EMERGENCY ACCESS TO THE AIRPORT. CHANGES SHALL BE MADE TO MAINTAIN EMERGENCY ACCESS TO THE AIRPORT AT ALL TIMES WITH THE APPROVAL OF THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY.

6. WILDLIFE MANAGEMENT

1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL ALWAYS BE MONITORED WITH A GATE GUARD WHEN IN USE.
3. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED AND LOCKED WHEN THE CONTRACTOR IS NOT WORKING.
4. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
5. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

DIRECTOR OF OPERATIONS

DOUG PALMER 309-303-0994

AIRPORT MAINTENANCE

GREG HUSER 309-303-1005

AIRPORT OPERATIONS

OPS CELL 309-303-1001

ENGINEER

WES IOERGER, P.E. - PROJECT ENGINEER 217-787-8050
CMT - RESIDENT ENGINEER 217-787-8050

10. INSPECTION REQUIREMENTS

1. THE CONTRACTOR SHALL INSPECT THE JOB SITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX C OF FAA AC 150/5370-2G (OR LATEST VERSION) MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL REQUEST AN OPERATIONAL INSPECTION WITH THE AIRPORT OPERATIONS THROUGH THE RESIDENT ENGINEER OF EACH PHASE WORK AREA PRIOR TO THE AREA BEING REOPENED FOR AIRCRAFT USE. AIRPORT OPERATIONS SHALL MAKE THE FINAL DETERMINATION OF THE ACCEPTANCE TO REOPEN THIS WORK AREA. THIS WILL NOT BE CONSIDERED AS FINAL OR PARTIAL ACCEPTANCE OF THE WORK IN THIS PHASE.
3. THE CONTRACTOR SHALL SUBMIT TO THE RESIDENT ENGINEER AN OFFICIAL 'SUBSTANTIALLY COMPLETE' LETTER WHEN HE FEELS THE PROJECT IS COMPLETE AND AT THE POINT OF HAVING A FINAL ACCEPTANCE. UPON THE RESIDENT ENGINEERS RECOMMENDATION TO IDA, A FINAL ACCEPTANCE OF THE PROJECT WILL THEN BE SCHEDULED.

11. UNDERGROUND UTILITIES

1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. PENALTIES

1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

1. NO RUNWAY CLOSURES WILL BE REQUIRED FOR THIS PROJECT. HOWEVER, A TAXIWAY CLOSURE IS REQUIRED FOR THIS PROJECT. WHEN THE TAXIWAY CLOSURE IS COORDINATED AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2G (OR LATEST VERSION).
2. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

15. HAZARD MARKING AND LIGHTING

1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G (OR LATEST VERSION) AND 150/5210-5D (OR LATEST VERSION) AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

16. PROTECTION

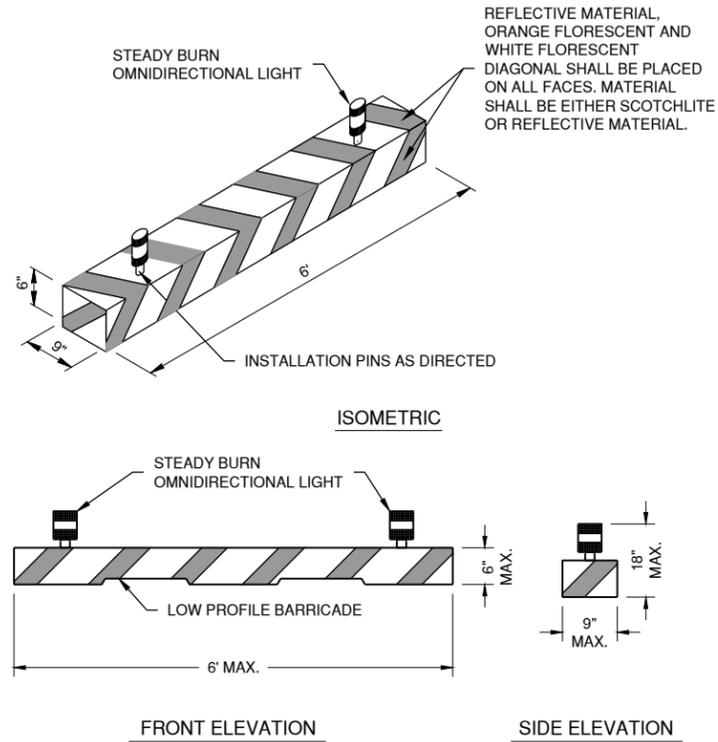
1. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 129.5' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED.
2. ALL WORK REQUIRED ON AN ACTIVE TAXILANE OR INSIDE AN ACTIVE TAXILANE OBJECT FREE AREA, WHICH EXTENDS 129.5' FROM THE TAXILANE CENTERLINE, WILL REQUIRE THE TAXILANE TO BE CLOSED.
3. TAXIWAY/TAXILANE OBJECT FREE AREA DISTANCES MAY BE LESS THAN STATED IN NOTES 1 AND 2 FOR CERTAIN PHASES AND NOTED IN THIS PLAN SET. THOSE OBJECT FREE AREA DISTANCES HAVE BEEN DETERMINED BASED ON SPECIFIC AIRCRAFT THAT TAXI IN THOSE PHASE LOCATIONS.
4. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

17. OTHER LIMITATIONS ON CONSTRUCTION

1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF AT THE LOCATIONS SHOWN ON THE AIRPORT SITE PLAN OR AS DIRECTED BY THE AIRPORT.

BEAM BARRICADE NOTES

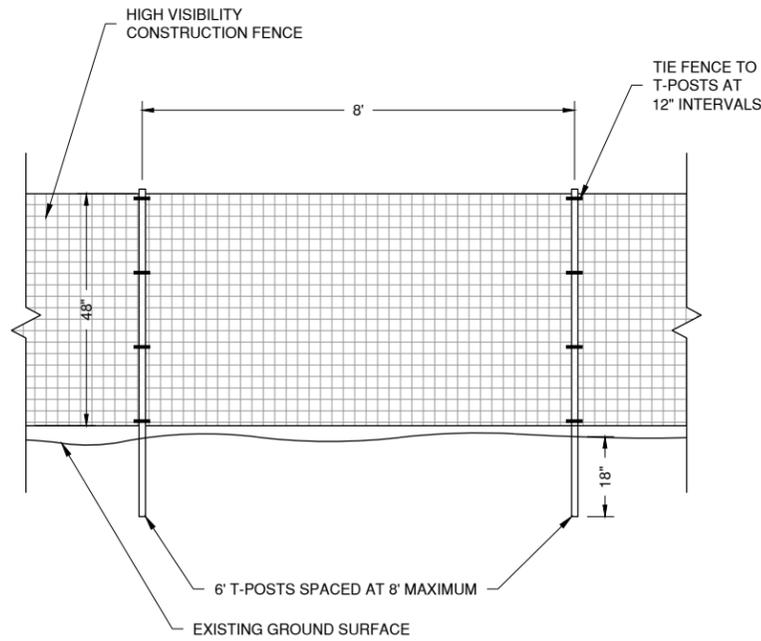
1. BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
2. BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
3. PLACE BARRICADES END TO END AROUND CONSTRUCTION PHASE LIMITS UNLESS NOTED IN THE PLANS TO ALLOW ACCESS FOR ARFF VEHICLES AND GROUND CREW OPERATIONS.
4. BARRICADES CLOSING TAXIWAYS AND CONSTRUCTION ACCESS ROUTES SHALL BE PLACED AT 15 FOOT INTERVALS.
5. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.



LOW PROFILE - BEAM BARRICADE DETAILS
N.T.S.

SECURITY NOTES

1. MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
2. AT NO TIME ARE UNSUPERVISED GATE OPENINGS PERMITTED AT THE AIRPORT. IF THE PROPOSED WORK REQUIRES GATE OPENING, THE CONTRACTOR SHALL PROVIDE A GUARD TO PREVENT UNAUTHORIZED ACCESS. THE CONTRACTOR SHALL SECURE THE GATE AT THE COMPLETION OF THE WORK DAY.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES DURING THE COURSE OF THE WORK.
4. FINES CAN BE LEVIED AGAINST THE CONTRACTOR BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) FOR NEGLIGENCE IF THE AIRPORT SECURITY IS COMPROMISED AND THE AIRPORT PERIMETER FENCE LINE IS NOT MAINTAINED AS SPECIFIED ABOVE. FINES CAN ALSO BE LEVIED AGAINST THE CONTRACTOR FOR FAILURE TO COOPERATE WITH THE AIRPORT MANAGEMENT AS REQUIRED TO MAINTAIN AIRPORT SECURITY.



CONSTRUCTION SAFETY FENCE DETAILS
N.T.S.

CONSTRUCTION ACTIVITY PLAN GENERAL NOTES

1. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
 2. THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
 3. IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT IS THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
 4. WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT.
 5. DELAYS, CHANGES IN SCHEDULING OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
 6. ALL PERIMETER ROADS INSIDE THE FENCE SHALL REMAIN ACCESSIBLE TO THE AIRPORT.
 7. THE CONTRACTOR SHALL BE AWARE THAT DAILY OPERATION OF THE AIRPORT WILL CONTINUE THROUGHOUT THIS PROJECT. AS A RESULT, DAILY COORDINATION WILL BE NECESSARY TO LIMIT DISRUPTION TO AIRPORT/TENANT AND CONTRACTOR OPERATIONS.
 8. CONTRACTOR'S STAGING, STORAGE, AND PARKING WILL BE AS SHOWN ON THE AIRPORT SITE PLAN.
 9. THE CONTRACTOR SHALL PLACE ALL BARRICADES, CONSTRUCTION SETBACK LINES, AND EROSION CONTROL ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
 10. CONSTRUCTION PHASING IS OF CRITICAL IMPORTANCE TO THE AIRPORT FOR THIS PROJECT. LIQUIDATED DAMAGES MAY BE SOUGHT FOR EXCEEDING THE CALENDAR DAYS ALLOWED FOR EACH PHASE.
 11. CONSTRUCTION RELATED ITEMS REQUIRING THE CLOSURE OF RUNWAYS AND/OR TAXIWAYS SHALL REQUIRE A MINIMUM OF 72 HOURS NOTICE TO COORDINATE WITH THE AIRPORT AND FAA. NO EXTENSION TO CONTRACT TIME WILL BE GIVEN FOR DELAYS CAUSED BY LACK OF ADEQUATE COORDINATION.
 12. CONTRACTOR SHALL SUPPLY RAMPS (RUBBER OR OTHER SUITABLE MATERIAL) TO BE USED IF AN AIRCRAFT NEEDS TO BE MOVED OVER MILLED SURFACE. THE RAMP SHOULD BE 2' ON ONE SIDE AND 0' ON THE OTHER.
12. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
- A. THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - B. THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE AIRPORT OPERATIONS BY THE CONTRACTOR.
 - C. THE CONTRACTOR SHALL USE EXISTING OR NEW GATE(S) AS CALLED OUT IN THE PLANS, FOR ACCESS TO THE AIRFIELD.
 - D. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - E. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - F. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED AND LOCKED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
 - G. THE CONTRACTOR SHALL CLOSE AND SECURE THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - H. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - I. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
 - J. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE USED BY HIS VEHICLES AND CONSTRUCTION EQUIPMENT ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE OR CONSTRUCTION EQUIPMENT.
 - K. THE CONTRACTOR SHALL EVALUATE AND PRESERVE THE CONDITION OF THE EXISTING AIRPORT PERIMETER ROAD AND ALL OTHER ACCESS ROUTES UTILIZED BY THE CONTRACTOR FOR ACCESS TO THE SITE. THE CONTRACTOR SHALL DOCUMENT THE CONDITION OF THE EXISTING PAVEMENT IN THE PRESENCE OF THE AIRPORT AND RESIDENT ENGINEER PRIOR TO THE INITIATION OF CONSTRUCTION. AT THE COMPLETION OF CONSTRUCTION, ANY PAVEMENT USED BY THE CONTRACTOR THAT IS SAID TO BE DAMAGED BY HIS OPERATIONS SHALL BE REPAIRED TO ITS ORIGINAL CONDITION AND/OR TO THE SATISFACTION OF THE AIRPORT AT THE CONTRACTOR'S EXPENSE.



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APRIL 20, 2018

REHABILITATE T-HANGAR TXY
PAVEMENT PHASE 1

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SHEET TITLE
**CONSTRUCTION
ACTIVITY PLAN
NOTES AND DETAILS**

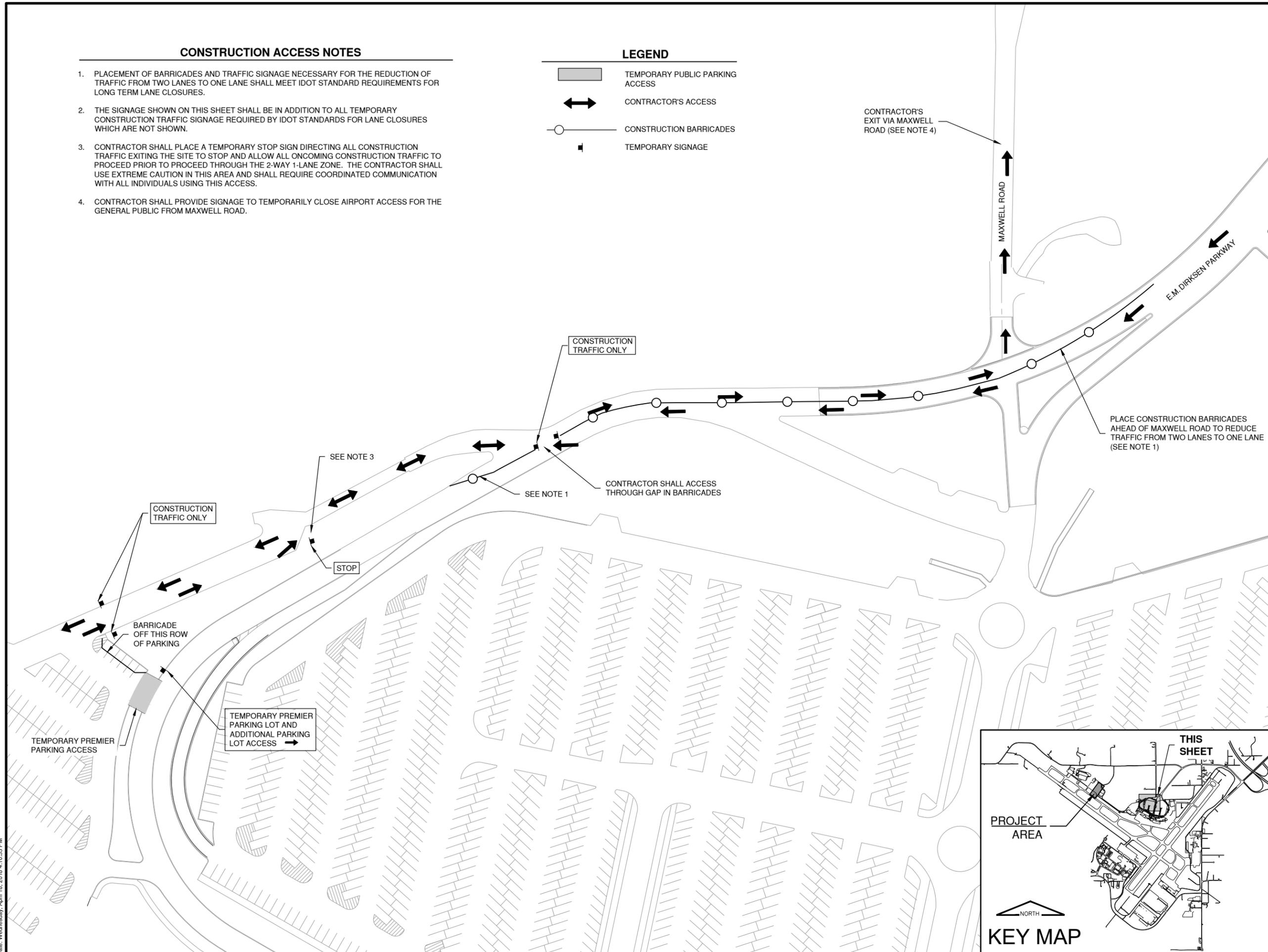
GC-102
SHEET 4 OF 16

CONSTRUCTION ACCESS NOTES

1. PLACEMENT OF BARRICADES AND TRAFFIC SIGNAGE NECESSARY FOR THE REDUCTION OF TRAFFIC FROM TWO LANES TO ONE LANE SHALL MEET IDOT STANDARD REQUIREMENTS FOR LONG TERM LANE CLOSURES.
2. THE SIGNAGE SHOWN ON THIS SHEET SHALL BE IN ADDITION TO ALL TEMPORARY CONSTRUCTION TRAFFIC SIGNAGE REQUIRED BY IDOT STANDARDS FOR LANE CLOSURES WHICH ARE NOT SHOWN.
3. CONTRACTOR SHALL PLACE A TEMPORARY STOP SIGN DIRECTING ALL CONSTRUCTION TRAFFIC EXITING THE SITE TO STOP AND ALLOW ALL ONCOMING CONSTRUCTION TRAFFIC TO PROCEED PRIOR TO PROCEED THROUGH THE 2-WAY 1-LANE ZONE. THE CONTRACTOR SHALL USE EXTREME CAUTION IN THIS AREA AND SHALL REQUIRE COORDINATED COMMUNICATION WITH ALL INDIVIDUALS USING THIS ACCESS.
4. CONTRACTOR SHALL PROVIDE SIGNAGE TO TEMPORARILY CLOSE AIRPORT ACCESS FOR THE GENERAL PUBLIC FROM MAXWELL ROAD.

LEGEND

-  TEMPORARY PUBLIC PARKING ACCESS
-  CONTRACTOR'S ACCESS
-  CONSTRUCTION BARRICADES
-  TEMPORARY SIGNAGE



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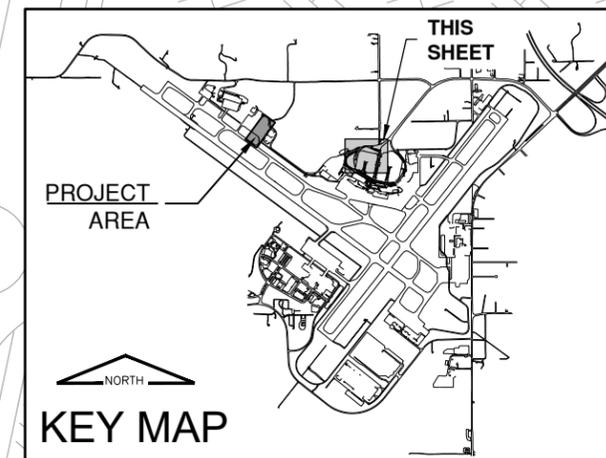
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SHEET 5 OF 16 GC-103

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PEORIA, ILLINOIS

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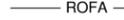
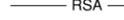
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CONSTRUCTION ACTIVITY PLAN 1

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GC-104

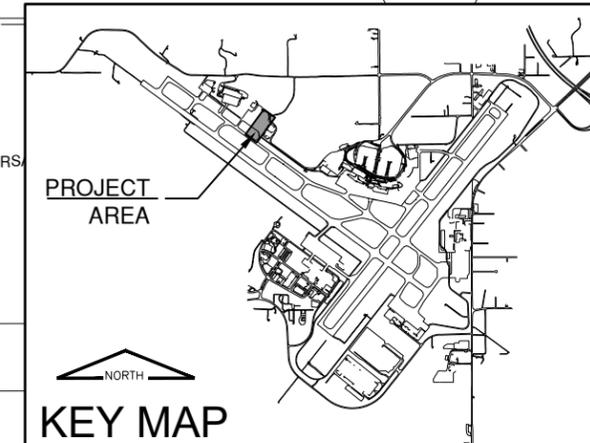
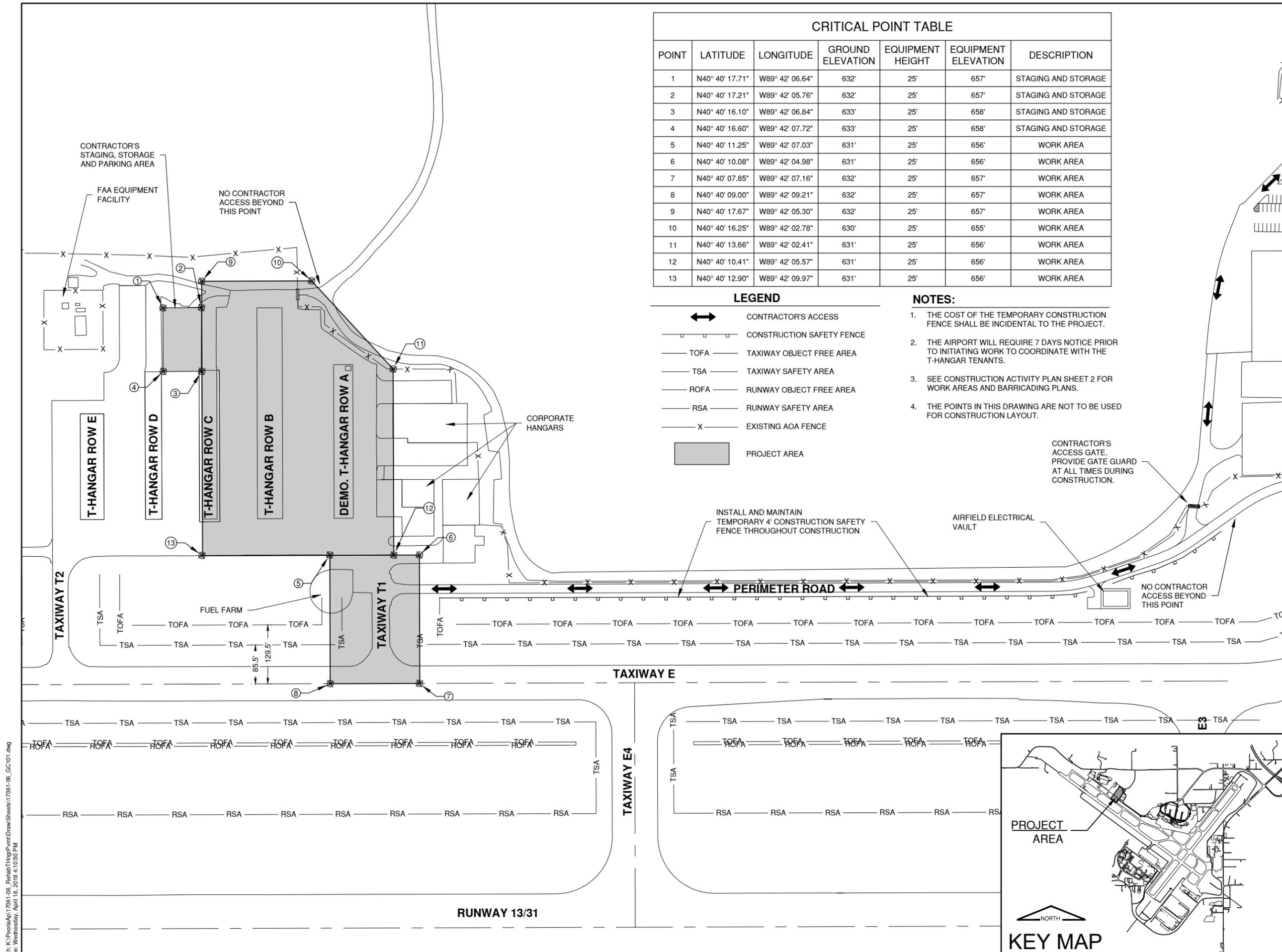
CRITICAL POINT TABLE						
POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	EQUIPMENT HEIGHT	EQUIPMENT ELEVATION	DESCRIPTION
1	N40° 40' 17.71"	W89° 42' 06.64"	632'	25'	657'	STAGING AND STORAGE
2	N40° 40' 17.21"	W89° 42' 05.76"	632'	25'	657'	STAGING AND STORAGE
3	N40° 40' 16.10"	W89° 42' 06.84"	633'	25'	658'	STAGING AND STORAGE
4	N40° 40' 16.60"	W89° 42' 07.72"	633'	25'	658'	STAGING AND STORAGE
5	N40° 40' 11.25"	W89° 42' 07.03"	631'	25'	656'	WORK AREA
6	N40° 40' 10.08"	W89° 42' 04.98"	631'	25'	656'	WORK AREA
7	N40° 40' 07.85"	W89° 42' 07.16"	632'	25'	657'	WORK AREA
8	N40° 40' 09.00"	W89° 42' 09.21"	632'	25'	657'	WORK AREA
9	N40° 40' 17.67"	W89° 42' 05.30"	632'	25'	657'	WORK AREA
10	N40° 40' 16.25"	W89° 42' 02.78"	630'	25'	655'	WORK AREA
11	N40° 40' 13.66"	W89° 42' 02.41"	631'	25'	656'	WORK AREA
12	N40° 40' 10.41"	W89° 42' 05.57"	631'	25'	656'	WORK AREA
13	N40° 40' 12.90"	W89° 42' 09.97"	631'	25'	656'	WORK AREA

LEGEND

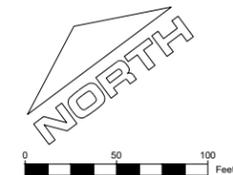
-  CONTRACTOR'S ACCESS
-  CONSTRUCTION SAFETY FENCE
-  TOFA — TAXIWAY OBJECT FREE AREA
-  TSA — TAXIWAY SAFETY AREA
-  ROFA — RUNWAY OBJECT FREE AREA
-  RSA — RUNWAY SAFETY AREA
-  X — EXISTING AOA FENCE
-  PROJECT AREA

NOTES:

- THE COST OF THE TEMPORARY CONSTRUCTION FENCE SHALL BE INCIDENTAL TO THE PROJECT.
- THE AIRPORT WILL REQUIRE 7 DAYS NOTICE PRIOR TO INITIATING WORK TO COORDINATE WITH THE T-HANGAR TENANTS.
- SEE CONSTRUCTION ACTIVITY PLAN SHEET 2 FOR WORK AREAS AND BARRICADING PLANS.
- THE POINTS IN THIS DRAWING ARE NOT TO BE USED FOR CONSTRUCTION LAYOUT.



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APRIL 20, 2018

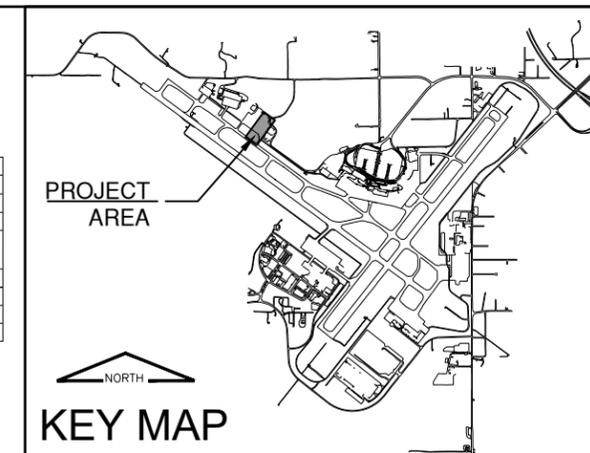
REHABILITATE T-HANGAR TX
PAVEMENT PHASE 1



GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL
AIRPORT
PEORIA, ILLINOIS

MARK	DATE	DESCRIPTION
		AIP PROJ. NO. 3-17-0080-XX
		IL PROJ. NO. PIA-4637 CONTRACT NO. PEXXX
		CMT PROJECT NO: 17061-06
		CAD DWG FILE: 17061-06_GC102.DWG
		DESIGNED BY: HWI
		DRAWN BY: CMT
		CHECKED BY: JMW
		APPROVED BY: RLV
		COPYRIGHT:

SHEET TITLE
**CONSTRUCTION
ACTIVITY PLAN 2**

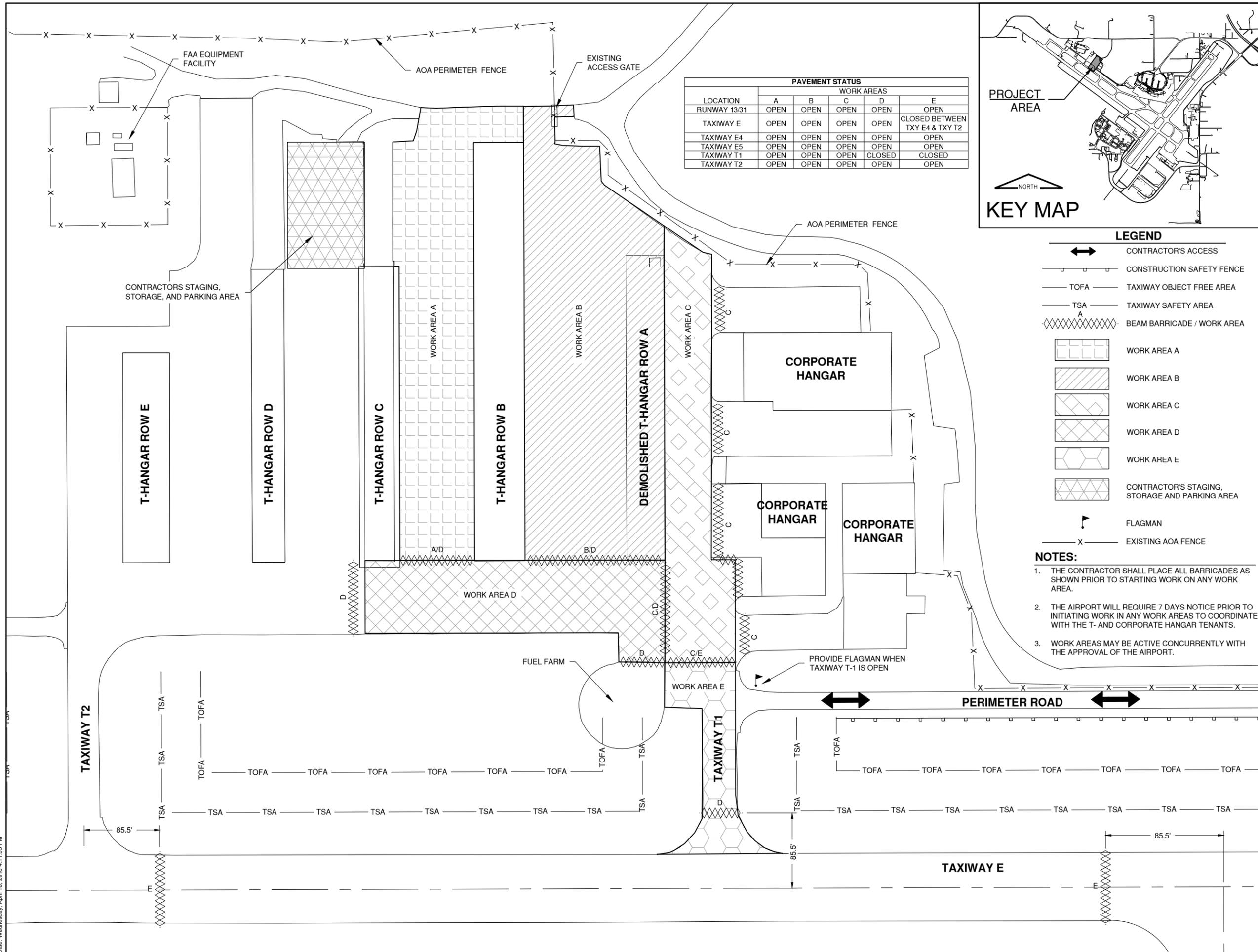


LOCATION	PAVEMENT STATUS				
	A	B	C	D	E
RUNWAY 13/31	OPEN	OPEN	OPEN	OPEN	OPEN
TAXIWAY E	OPEN	OPEN	OPEN	OPEN	CLOSED BETWEEN TXY E4 & TXY T2
TAXIWAY E4	OPEN	OPEN	OPEN	OPEN	OPEN
TAXIWAY E5	OPEN	OPEN	OPEN	OPEN	OPEN
TAXIWAY T1	OPEN	OPEN	OPEN	CLOSED	CLOSED
TAXIWAY T2	OPEN	OPEN	OPEN	OPEN	OPEN

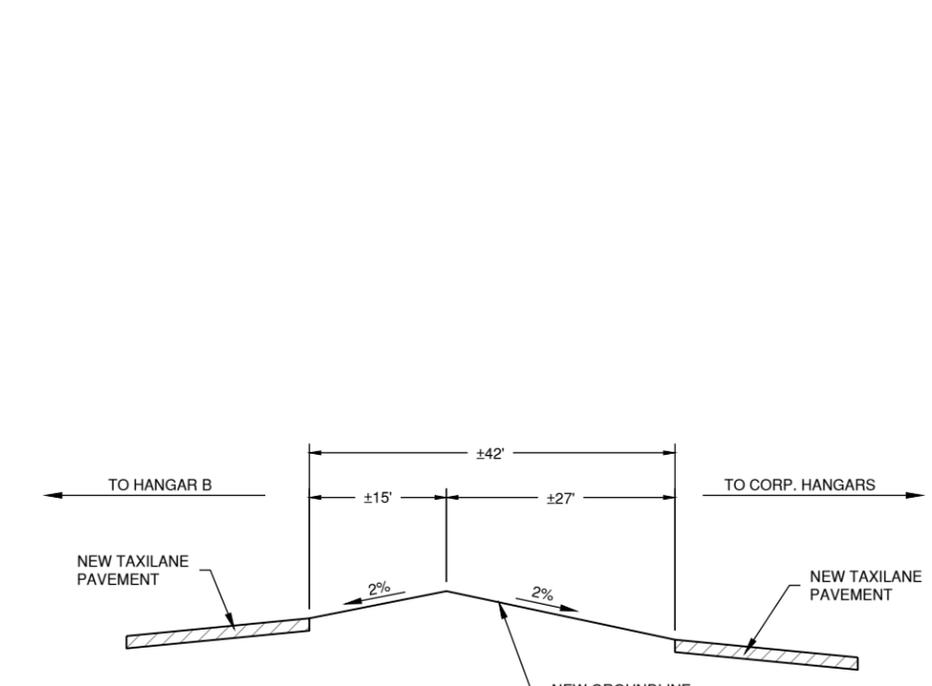
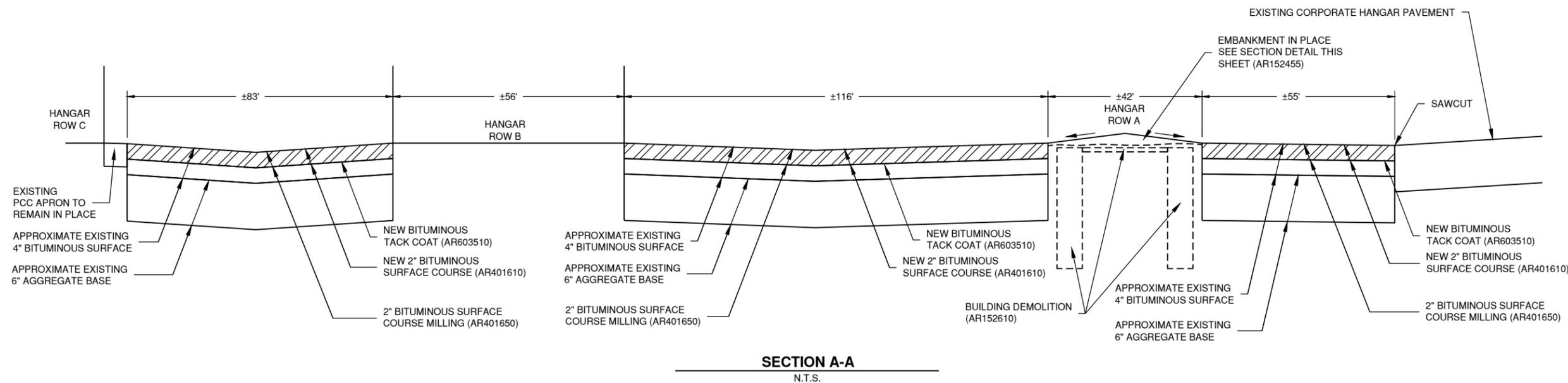
LEGEND

- CONTRACTOR'S ACCESS
- CONSTRUCTION SAFETY FENCE
- TOFA — TAXIWAY OBJECT FREE AREA
- TSA — TAXIWAY SAFETY AREA
- BEAM BARRICADE / WORK AREA
- WORK AREA A
- WORK AREA B
- WORK AREA C
- WORK AREA D
- WORK AREA E
- CONTRACTOR'S STAGING, STORAGE AND PARKING AREA
- FLAGMAN
- EXISTING AOA FENCE

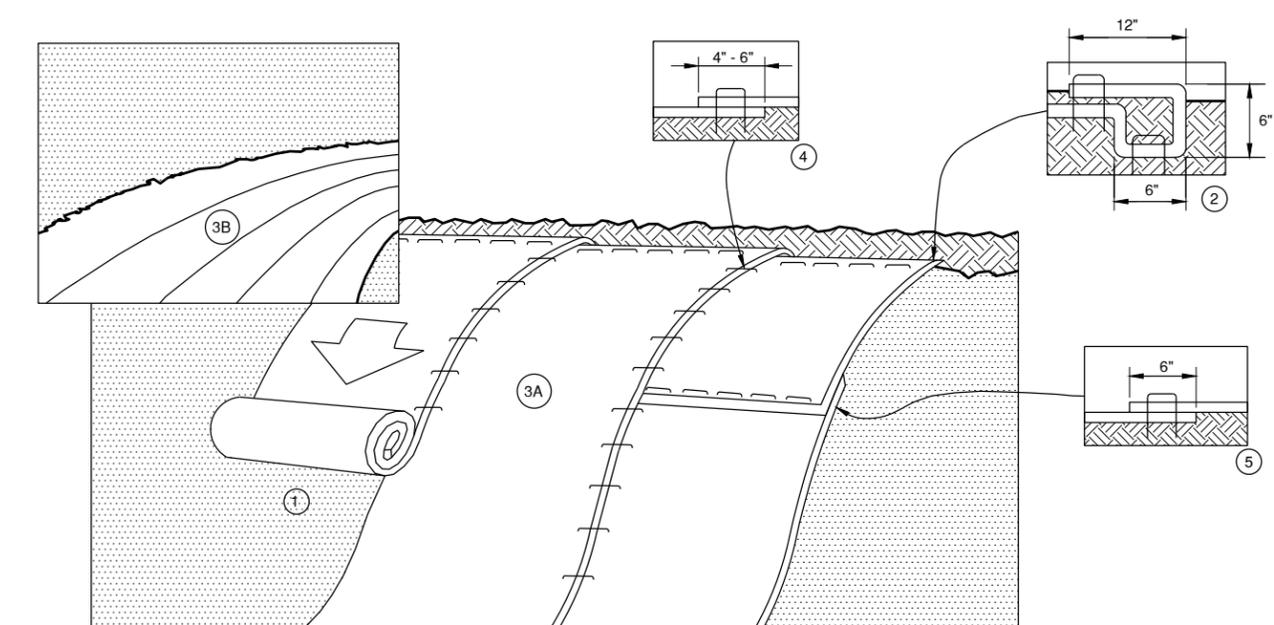
- NOTES:**
- THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON ANY WORK AREA.
 - THE AIRPORT WILL REQUIRE 7 DAYS NOTICE PRIOR TO INITIATING WORK IN ANY WORK AREAS TO COORDINATE WITH THE T- AND CORPORATE HANGAR TENANTS.
 - WORK AREAS MAY BE ACTIVE CONCURRENTLY WITH THE APPROVAL OF THE AIRPORT.



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NOTE:
FINISHED GRADE SHALL BE SEEDED AND COVERED WITH EXCELSIOR BLANKET (AR901510, AR908520)



1. PREPARE SOIL BEFORE INSTALLING BLANKETS, INCLUDING ANY NECESSARY APPLICATION OF LIME, FERTILIZER, AND SEED.
2. BEGIN AT THE TOP OF THE SLOPE BY ANCHORING THE BLANKET IN A 6" DEEP X 6" WIDE TRENCH WITH APPROXIMATELY 12" OF BLANKET EXTENDED BEYOND THE UP-SLOPE PORTION OF THE TRENCH. ANCHOR THE BLANKET WITH A ROW OF STAPLES / STAKES APPROXIMATELY 12" APART IN THE BOTTOM OF THE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING. APPLY SEED TO COMPACTED SOIL AND FOLD REMAINING 12" PORTION OF BLANKET BACK OVER SEED AND COMPACTED SOIL. SECURE BLANKET OVER COMPACTED SOIL WITH A ROW OF STAPLES / STAKES SPACED APPROXIMATELY 12" APART ACROSS THE WIDTH OF THE BLANKET.
3. ROLL THE BLANKETS (A.) DOWN OR (B.) HORIZONTALLY ACROSS THE SLOPE. BLANKETS WILL UNROLL WITH APPROPRIATE SIDE AGAINST THE SOIL SURFACE. ALL BLANKETS MUST BE SECURELY FASTENED TO SOIL SURFACE BY PLACING STAPLES / STAKES IN APPROPRIATE LOCATIONS PER MANUFACTURER'S RECOMMENDATION.
4. THE EDGES OF PARALLEL BLANKETS MUST BE STAPLED WITH MINIMUM 6" OVERLAP. TO ENSURE PROPER SEAM ALIGNMENT, PLACE THE EDGE OF THE OVERLAPPING BLANKET (BLANKET BEING INSTALLED ON TOP) EVEN WITH THE SEAM STITCH ON THE PREVIOUSLY INSTALLED BLANKET.
5. CONSECUTIVE BLANKETS SPLICED DOWN THE SLOPE MUST BE PLACED END OVER END (SHINGLE STYLE) WITH AN APPROXIMATELY 6" OVERLAP. STAPLE THROUGH OVERLAPPED AREA, APPROXIMATELY 12" APART ACROSS ENTIRE BLANKET WIDTH.
6. PLACE STAPLES / STAKES PER MANUFACTURER'S RECOMMENDATION FOR THE APPROPRIATE SLOPE BEING APPLIED. IN LOOSE SOIL CONDITIONS, THE USE OF STAPLE / STAKE LENGTHS GREATER THAN 6" MAY BE NECESSARY TO PROPERLY SECURE THE BLANKETS.

APRIL 20, 2018

REHABILITATE T-HANGAR TXY PAVEMENT PHASE 1

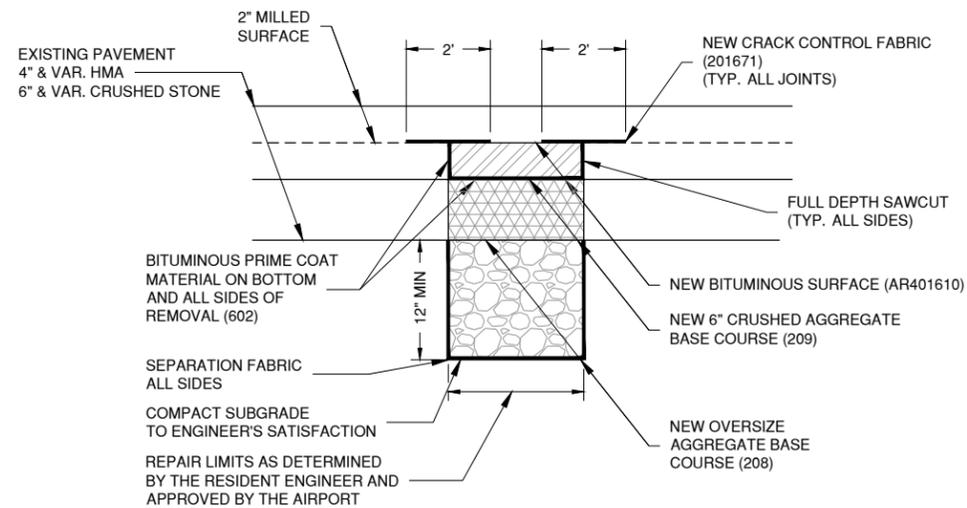
OWNER

GENERAL WAYNE A. DOWNING PEORIA INTERNATIONAL AIRPORT
GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS

MARK	DATE	DESCRIPTION

TYPICAL SECTIONS AND DETAILS

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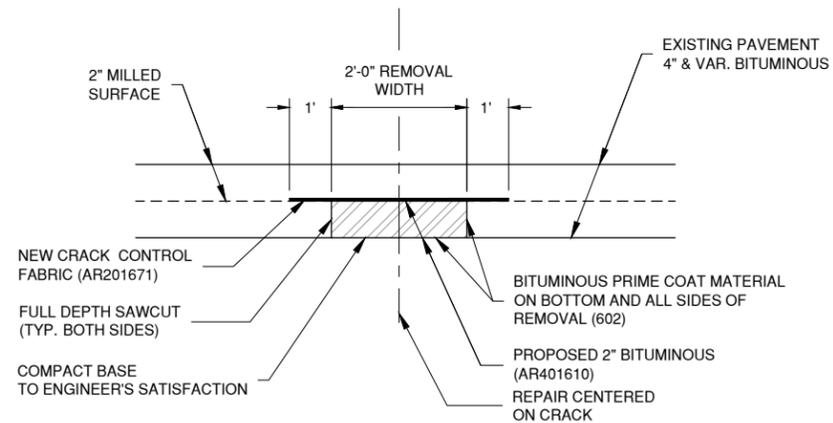


SUBGRADE UNDERCUT (AR152515 - C.Y.)

N.T.S

SUBGRADE UNDERCUT NOTES:

1. THE REMOVAL AREA SHALL BE DETERMINED AT THE TIME OF CONSTRUCTION BY THE RESIDENT ENGINEER.
2. THE FOLLOWING SHALL BE THE STEPS TAKEN TO REPAIR THE SUBGRADE:
 - 2.1. SAWCUT AND REMOVE THE HMA PAVEMENT REMAINING AFTER MILLING.
 - 2.2. REMOVE BITUMINOUS PAVEMENT, AGGREGATE BASE AND SOIL SUBGRADE TO A MINIMUM DEPTH OF 12" BELOW BOTTOM OF AGGREGATE BASE OR AS DIRECTED BY THE RESIDENT ENGINEER.
 - 2.3. COMPACT THE SUBGRADE TO THE SATISFACTION OF THE RESIDENT ENGINEER.
 - 2.4. PLACE SEPARATION FABRIC ITEM ON TOP OF THE COMPACTED SUBGRADE AND UP THE SIDES OF THE EXCAVATION.
 - 2.5. PLACE AND CONSOLIDATE OVERSIZED AGGREGATE ITEM 208 TO THE BOTTOM OF THE EXISTING AGGREGATE BASE COURSE.
 - 2.6. PLACE AND COMPACT CRUSHED AGGREGATE ITEM 209 TO THE BOTTOM OF THE EXISTING BITUMINOUS PAVEMENT.
 - 2.7. APPLY PRIME COAT ITEM 602510 TO THE BOTTOM AND SIDES OF THE EXCAVATED AREA.
 - 2.8. PLACE NEW BITUMINOUS PAVEMENT ITEM 401610 TO THE MILLED SURFACE OF THE REMAINING PAVEMENT.
 - 2.9. PLACE CRACK CONTROL FABRIC ITEM 201671 AS SHOWN ON THE DETAIL.
3. OVERSIZE AGGREGATE SHALL BE IDOT GRADATION CA-01 OR OTHER SUITABLE MATERIAL APPROVED BY THE RESIDENT ENGINEER.
4. THE FOLLOWING SHALL BE INCIDENTAL TO THE SUBGRADE UNDERCUT PAY ITEM (AR152515):
 - 4.1. FULL DEPTH SAWCUT
 - 4.2. UNCLASSIFIED EXCAVATION
 - 4.3. SUBGRADE PREPARATION
 - 4.4. SEPARATION FABRIC
 - 4.5. OVERSIZE AGGREGATE
 - 4.6. AGGREGATE BASE COURSE
 - 4.7. BITUMINOUS PRIME COAT
5. BITUMINOUS REPLACEMENT AND OVERLAY AND CRACK CONTROL FABRIC WILL BE PAID SEPARATELY.

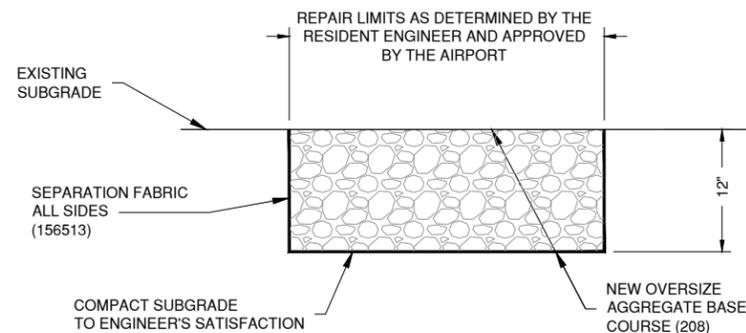


BITUMINOUS CRACK REPAIR (AR201660 - L.F.)

N.T.S

BITUMINOUS CRACK REPAIR NOTES:

1. THE REMOVAL WIDTH SHALL BE 2'-0" WIDE OR AS DIRECTED BY THE RESIDENT ENGINEER.
2. THE FOLLOWING SHALL BE THE STEPS TAKEN TO REPAIR THE CRACK:
 - 2.1. SAWCUT AND REMOVE THE FULL DEPTH OF BITUMINOUS PAVEMENT TO A WIDTH OF 24" CENTERED ON THE CRACK.
 - 2.2. COMPACT THE AGGREGATE BASE COURSE TO THE SATISFACTION OF THE RESIDENT ENGINEER.
 - 2.3. APPLY PRIME COAT ITEM 602510 TO THE AGGREGATE BASE COURSE AND THE SIDES OF THE AREA TO BE PATCHED.
 - 2.4. PLACE BITUMINOUS PAVEMENT ITEM 401610 TO THE TOP OF THE MILLED SURFACE.
 - 2.5. PLACE CRACK CONTROL FABRIC ITEM 201671 AS SHOWN IN THE PLANS.
3. THE FOLLOWING SHALL BE INCIDENTAL TO THE BITUMINOUS CRACK REPAIR PAY ITEM (AR201660):
 - 3.1. FULL DEPTH SAWCUT
 - 3.2. PAVEMENT REMOVAL
 - 3.3. BASE PREPARATION
 - 3.4. BITUMINOUS PRIME COAT
4. BITUMINOUS REPLACEMENT AND OVERLAY AND CRACK CONTROL FABRIC WILL BE PAID SEPARATELY.



SUBGRADE REPAIR (ADDITIVE ALTERNATE 1 - AS152511 - S.Y.)

N.T.S

SUBGRADE REPAIR NOTES:

1. THE REMOVAL AREA SHALL BE DETERMINED AT THE TIME OF CONSTRUCTION BY THE RESIDENT ENGINEER.
2. THE FOLLOWING SHALL BE THE STEPS TAKEN TO REPAIR THE SUBGRADE:
 - 2.1. REMOVE SOIL SUBGRADE TO A DEPTH OF 12" OR AS DIRECTED BY THE RESIDENT ENGINEER.
 - 2.2. COMPACT THE SUBGRADE TO THE SATISFACTION OF THE RESIDENT ENGINEER.
 - 2.3. PLACE SEPARATION FABRIC ITEM 156513 ON TOP OF THE COMPACTED SUBGRADE AND UP THE SIDES OF THE EXCAVATION.
 - 2.4. PLACE AND CONSOLIDATE OVERSIZED AGGREGATE ITEM 208 TO THE SATISFACTION OF THE RESIDENT ENGINEER.
3. OVERSIZE AGGREGATE SHALL BE IDOT GRADATION CA-01 OR OTHER SUITABLE MATERIAL APPROVED BY THE RESIDENT ENGINEER.
4. THE FOLLOWING SHALL BE INCIDENTAL TO THE SUBGRADE UNDERCUT PAY ITEM (AR152515):
 - 4.1. UNCLASSIFIED EXCAVATION
 - 4.2. SUBGRADE PREPARATION
 - 4.3. SEPARATION FABRIC
 - 4.4. OVERSIZE AGGREGATE



License No. 184-000613

CONSULTANTS

APRIL 20, 2018

REHABILITATE T-HANGAR TX
PAVEMENT PHASE 1



GENERAL WAYNE A. DOWNING
AIRPORT
PEORIA, ILLINOIS

MARK	DATE	DESCRIPTION

AIP PROJ. NO. 3-17-0080-XX
IL PROJ. NO. PIA-4637 CONTRACT NO. PEXXX
CMT PROJECT NO: 17061-06
CAD DWG FILE: 17061-06_CP501.DWG
DESIGNED BY: HWI
DRAWN BY: CMT
CHECKED BY: JMW
APPROVED BY: RLV
COPYRIGHT:

SHEET TITLE
**PAVING &
MISCELLANEOUS
DETAILS**

SHEET **13** OF **16**
CP-104



APRIL 20, 2018

REHABILITATE T-HANGAR TXV
PAVEMENT PHASE 1



GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL
AIRPORT
PEORIA, ILLINOIS

MARK	DATE	DESCRIPTION
AIP PROJ. NO. 3-17-0080-XX		
IL PROJ. NO. PIA-4637		CONTRACT NO. PEXXX
CMT PROJECT NO: 17061-06		
CAD DWG FILE: 17061-06_CP102.DWG		
DESIGNED BY: HWI		
DRAWN BY: CMT		
CHECKED BY: JMW		
APPROVED BY: RLV		
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SHEET TITLE
**ADDITIVE ALTERNATE
1**

EXISTING SHED
TO REMAIN

T-HANGAR ROW B

DEMOLISHED T-HANGAR ROW A

CORPORATE
HANGAR

CORPORATE
HANGAR

T-HANGAR ROW B

DEMOLISHED T-HANGAR ROW A

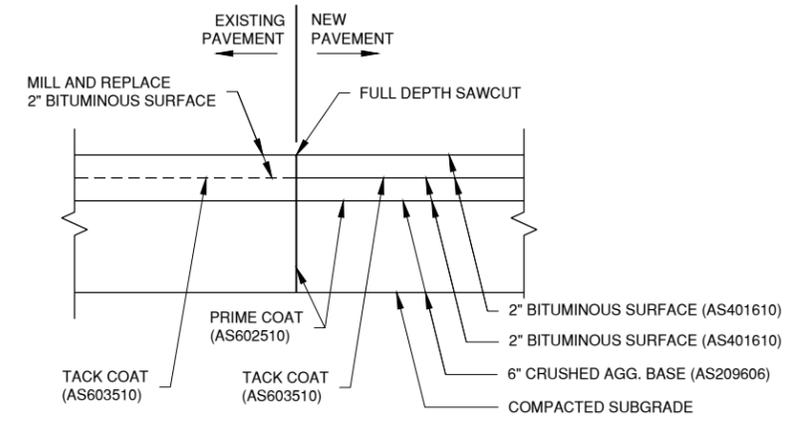
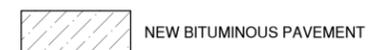
CORPORATE
HANGAR

CORPORATE
HANGAR

ADDITIVE ALTERNATE - PAVING PLAN

ADDITIVE ALTERNATE - STAKING PLAN

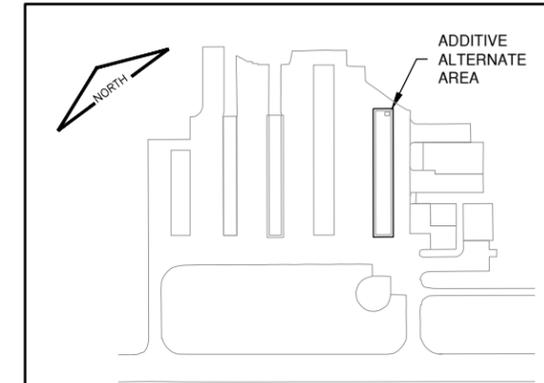
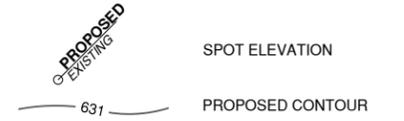
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HANGAR ROW A PAVEMENT SECTION

N.T.S.

LEGEND



KEY MAP

