

3/20/2018 10:54:56 AM \$FILES

06-15-2018 LETTING ITEM 167

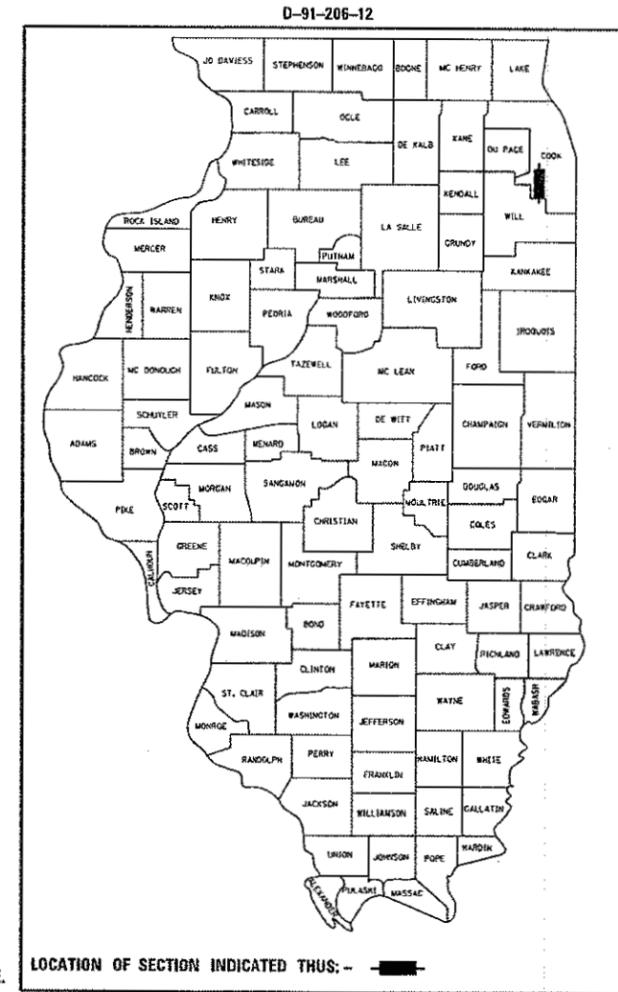
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0202-BR	COOK	27	1
ILLINOIS			CONTRACT NO. 60R73	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

# PROPOSED HIGHWAY PLANS

F.A.P. ROUTE 372 ILLINOIS 171 (FIRST AVENUE)  
OVER SALT CREEK  
SECTION 0202-BR  
PROJECT NHPP BNGA(421)  
BRIDGE DECK OVERLAY & JOINT REPAIR  
COOK COUNTY  
C-91-206-12

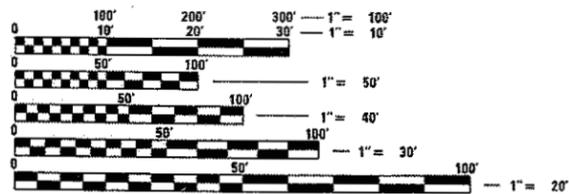


LOCATION OF SECTION INDICATED THUS: - [black rectangle] -

EXISTING ADT = 35,100 (2017)  
POSTED SPEED LIMIT = 40 MPH

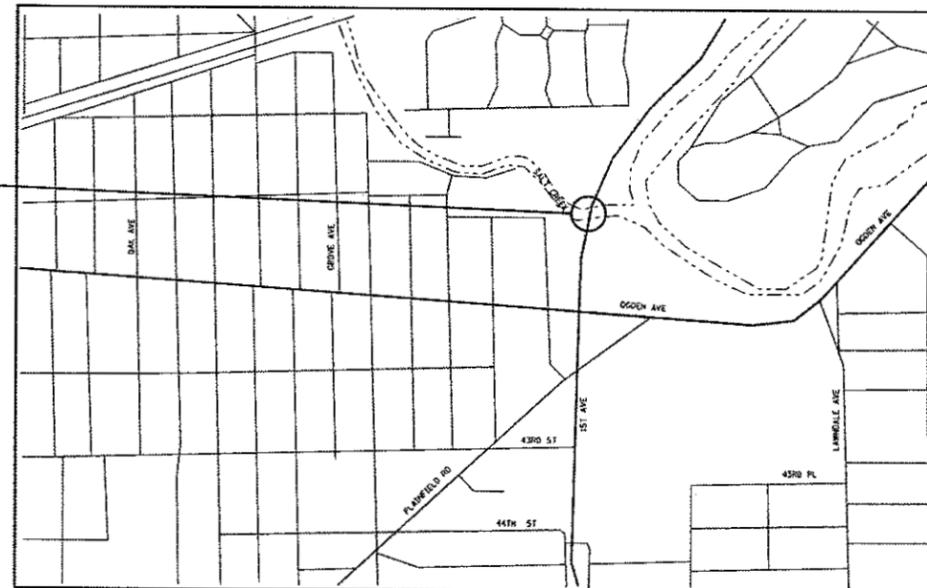
THIS PROJECT IS LOCATED IN THE  
VILLAGE OF LYONS

PROPOSED IMPROVEMENT  
SN 016-0702  
BEGIN STA. = 374+55.00  
END STA. = 376+80.00



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811



LYONS TOWNSHIP

LOCATION MAP  
NOT TO SCALE



*Jason M. Schneider*  
JASON M. SCHNEIDER, S.E., P.E.  
EXP. 11/30/2018  
SHEETS: 8-16



*Zachary J. Tanner*  
ZACHARY J. TANNER, P.E.  
EXP. 11/30/2019  
SHEETS: 4-7

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED *March 22 2018*  
*Robert A. Quigley*  
REGIONAL ENGINEER

*May 11 2018*  
ENGINEER OF DESIGN AND ENVIRONMENT

*May 11 2018*  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

PROJECT ENGINEER: RAGHAD ADEIS-DAHMAN, PE, SE (847) 705-4237  
PROJECT MANAGER: FAWAD AQUEEL, PE, PTOE (847) 705-4247

CONTRACT NO. 60R73

GROSS LENGTH = 202.40 FT. = 0.038 MILE  
NET LENGTH = 202.40 FT. = 0.038 MILE

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3-6	SUMMARY OF QUANTITIES
7	ROADWAY PLAN
8	MAINTENANCE OF TRAFFIC TYPICAL SECTIONS
9	MAINTENANCE OF TRAFFIC STAGE 1
10	MAINTENANCE OF TRAFFIC STAGE 2
11	PAVEMENT MARKING PLAN
12-21	STRUCTURE PLANS
22	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
23	BUTT JOINT AND HMA TAPER (BD-32)
24	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS (TC-10)
25	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)
26	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
27	ARTERIAL ROAD INFORMATION SIGN (TC-22)

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS <= 40 MPH
701611-01	URBAN HALF ROAD CLOSURE MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-07	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
720001-01	SIGN PANEL MOUNTING DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS
728001-01	TELESCOPING STEEL SIGN SUPPORT
729001-01	APPLICATIONS OF TYPE A AND B METAL POSTS (FOR SIGNS AND MARKERS)
782006	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

GENERAL NOTES - ROADWAY

- ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE APRIL 1, 2016 EDITION OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", AND IN ACCORDANCE WITH IDOT'S "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2018. THE ENGINEER WILL RESOLVE ANY CONFLICT WHICH MAY EXIST BETWEEN THESE SPECIFICATIONS.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL J.U.L.I.E. AT (800)892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES (48 HOUR NOTICE IS REQUIRED).
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES WHICH HAVE UTILITIES PRESENT IN THE PROJECT AREA BEFORE ANY WORK IS STARTED. ALL UTILITY COMPANIES, LYONS TOWNSHIP, AND COOK COUNTY SHOULD BE NOTIFIED BY THE CONTRACTOR AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION. ALL UTILITIES MUST BE STAKED PRIOR TO CONSTRUCTION.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- ALL DAMAGE TO EXISTING PAVEMENT MARKING OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTORS EXPENSE.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- 10 FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB AND GUTTER AND MEDIAN ITEMS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED LARGER ITEM OF SPECIFIED WORK.
- WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREA.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD (FOR FUTURE REFERENCES), ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM DISTRICT 1 BRIDGE INSPECTORS.
- SAW CUTTING OF PAVEMENTS, SHOULDERS, ETC., SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN, STRAIGHT EDGE ON THE PORTION REMAINING. THE COST OF SAW CUTTING REMOVAL ITEMS SHALL BE INCLUDED IN THE UNIT PRICES OF THESE ITEMS.
- PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATION. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION OR ORDERING MATERIALS. SUCH VARIATION SHALL NOT CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR QUANTITY ACTUALLY FURNISHED BASED AT THE UNIT PRICE BID FOR THE WORK.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

GENERAL NOTES - MAINTENANCE OF TRAFFIC

- SEE SPECIAL PROVISIONS TITLED TRAFFIC CONTROL AND PROTECTION (ARTERIALS).
- THE CONTRACTOR SHALL REMOVE AND SAFELY STORE (FREE FROM THEFT OR DAMAGE) OR COVER ALL CONFLICTING EXISTING SIGNS FOR THE DURATION OF THE CONSTRUCTION. ALL SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AT THE END OF CONSTRUCTION.
- THE FOLLOWING APPLY TO CONSTRUCTION SIGNS:
  - A) THE CONTRACTOR SHALL FURNISH ALL SIGNS.
  - B) THE CONTRACTOR SHALL BE RESPONSIBLE FOR AND REPLACE ANY SIGNS THAT ARE SUPPLIED BY OTHERS AND DAMAGED BY THE CONTRACTOR'S WORK FORCE OR SUBCONTRACTORS DURING RELOCATION OR CONSTRUCTION OPERATIONS.
  - C) ALL SIGNS AND ASSEMBLIES SHALL BE CERTIFIED BY THE CONTRACTOR AS MEETING THE APPLICABLE REQUIREMENTS OF NCHRP REPORT 350. TEST LEVEL 2.
  - D) ALL SIGNS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE TRAFFIC CONTROL AND PROTECTION (SPECIAL) PAY ITEM, EXCEPT FOR TEMPORARY INFORMATIONAL SIGNING AS NOTED ON THE PLANS.
- ANY RAISED REFLECTIVE PAVEMENT MARKERS THAT CONFLICT WITH THE TEMPORARY TRAFFIC LANES MUST HAVE THE REFLECTIVE LENSES REMOVED AS DIRECTED BY THE ENGINEER.
- ALL TEMPORARY PAVEMENT MARKINGS DURING STAGED CONSTRUCTION SHALL BE PAVEMENT MARKING TAPE, TYPE IV OF THE WIDTH AND COLOR SPECIFIED ON THE PLAN SHEETS.
- THE CONTRACTOR SHALL MAINTAIN DRAINAGE AND EROSION CONTROL DURING CONSTRUCTION FOR THE DURATION OF THE CONTRACT.
- THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ACCESS TO ALL COMMERCIAL AND RESIDENTIAL ENTRANCES FOR THE ENTIRE DURATION OF THE PROJECT UNLESS OTHERWISE NOTED ON THE PLANS.
- SIDE ROAD, INTERSECTIONS, AND DRIVEWAY TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE TYPICAL ENTRANCE SIGNING DETAIL, DISTRICT DETAILS TC-10 AND TC-26, AND AS SHOWN ON THE PLANS.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS (%) AT N <sub>DES</sub>	QUALITY MANAGEMENT PROGRAM (QMP)
BRIDGE APPROACH OVERLAY AND BUTT JOINT		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL-9.5mm), 1-3/4"	4.0% AT 70 GYR.	QC/OA
LEVELING BINDER (HM), N70, (IL-9.5mm)	4.0% AT 70 GYR.	QC/OA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA)		

NOTES:

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ. YD./IN.
- THE "AC TYPE" FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
- FOR USE OF RECYCLED MATEIRALS SEE DISTRICT ONE SPECIAL PROVISIONS.
- QUALITY MANAGEMENT PROGRAM IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

FILE NAME = I:\18303 P1B 1B2 0A\18303\17-IL 171 over Sat\CAD\ADD\CADD\_Sheets\18303\17-11-171.dwg



USER NAME = ztanner	DESIGNED - ZJT	REVISED -
123 North Wacker Drive Suite 900 Chicago, IL 60606 (312) 704-9300 www.collinsengr.com	DRAWN - ZJT	REVISED -
PLOT SCALE = 2.0000' / 1in.	CHECKED - KVH	REVISED -
PLOT DATE = 3/23/2018	DATE - 03/23/2018	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES  
ILLINOIS 171 (FIRST AVENUE)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0202-BR	COOK	27	2
CONTRACT NO. 6073				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				ROADWAY	BRIDGE
				0047 URBAN	0047 016-0702
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	20	20	
25200110	SODDING, SALT TOLERANT	SQ YD	20	20	
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	300	300	
40600535	LEVELING BINDER (HAND METHOD), N70	TON	3	3	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	320	320	
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	71	71	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	300	300	
44000600	SIDEWALK REMOVAL	SQ FT	300	300	
50102400	CONCRETE REMOVAL	CU YD	35.6	35.6	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	38.8	38.8	
50300260	BRIDGE DECK GROOVING	SQ YD	577	577	
50300300	PROTECTIVE COAT	SQ YD	236	236	
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	20	20	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	6770	6770	

\* SPECIALTY ITEM

FILE NAME: I:\18303 PTB 182 04\18303.17-IL 171 over- Salt\ROAD\CADD\_Sheets\18303-182-900.dgn



USER NAME = zstanner	DESIGNED - ZJT	REVISED -
PLLOT SCALE = 0.1667' / in.	CHECKED - KVH	REVISED -
PLLOT DATE = 3/23/2018	DATE - 03/23/2018	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
ILLINOIS 171 (FIRST AVENUE)			
SCALE: NONE	SHEET 1	OF 4 SHEETS	STA. TO STA.

F.A.I. RTE. 372	SECTION 0202-BR	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 3
CONTRACT NO. 60RT3				ILLINOIS FED. AID PROJECT

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				ROADWAY	BRIDGE
				URBAN	016-0702
				URBAN	80/20 (FED/STATE)
50800515	BAR SPLICERS	EACH	36	36	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	135	135	
58700300	CONCRETE SEALER	SQ FT	294	294	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3	
67100100	MOBILIZATION	L SUM	1	1	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	56	56	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	19	19	
70300904	PAVEMENT MARKING TAPE, TYPE IV 4"	FOOT	9972	9972	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	500	500	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	500	500	
70600250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
70600350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4506	4506	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	65	65	

\* SPECIALTY ITEM

FILE NAME = J:\181803 P1B.182\_04\181803.DWG - 171 over - Sat 11/23/2018 10:00:00 AM - Sheets\181803.dwg

**COLLINS ENGINEERS**  
 123 North Wacker Drive  
 Suite 900  
 Chicago, IL 60606  
 (312) 794-9300  
 www.collinsengr.com

USER NAME = ztanner	DESIGNED - ZJT	REVISED -
PLOT SCALE = 0.1667' / 1"	DRAWN - ZJT	REVISED -
PLOT DATE = 3/23/2018	CHECKED - KVH	REVISED -
	DATE - 03/23/2018	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES  
 ILLINOIS 171 (FIRST AVENUE)

SCALE: SHEET 2 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0202-BR	COOK	27	4
			CONTRACT NO. 60R73	
ILLINOIS FED. AID PROJECT				

FILE NAME = J:\18233 PTB 182 84\18233\17-IL 171 over-SaltACADD\GOOD\_Sheet\A\0168R73-sh1-500.dgn

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				80/20 (FED / STATE)	
				ROADWAY 0047 URBAN	BRIDGE 0047 016-0702
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	474	474	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	20	20	
* 78100300	REPLACEMENT REFLECTOR	EACH	50	50	
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	80	80	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	20	20	
X0322194	POLYMER MODIFIED PORTLAND CEMENT MORTAR	SQ FT	5	5	
X0327638	STREAM GAUGE	EACH	1	1	
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	2050	2050	
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	3325	3325	
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	120	120	
Z0006014	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/2 INCHES	SQ YD	548	548	
Z0010400	CLEANING BRIDGE SEATS	SQ FT	294	294	
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	548	548	

\* SPECIALTY ITEM

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Suite 900  
Chicago, IL 60606  
(312) 754-9300  
www.collinsengr.com

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PLOT SCALE = 0.1667' / 1"	DRAWN - ZJT	REVISED -
PLOT DATE = 3/23/2018	CHECKED - KVH	REVISED -
	DATE - 03/23/2018	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES  
ILLINOIS 171 (FIRST AVENUE)

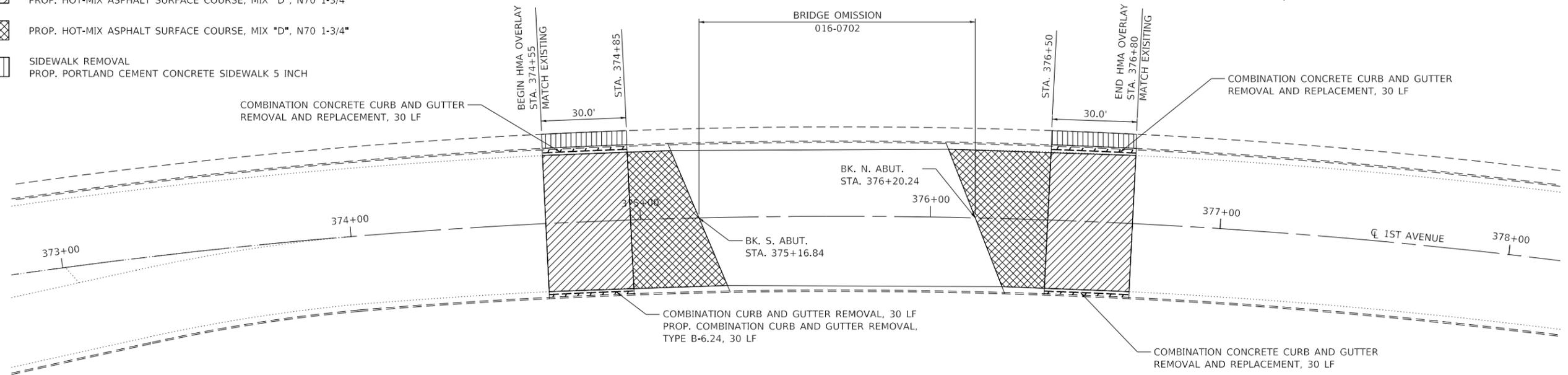
SCALE: SHEET 3 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0202-BR	COOK	27	5
CONTRACT NO. 60R73				
ILLINOIS FED. AID PROJECT				



**LEGEND**

-  COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
-  HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT  
PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1-3/4"
-  PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1-3/4"
-  SIDEWALK REMOVAL  
PROP. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH



**BITUMINOUS MATERIAL (TACK COAT)**  
 STA. 374+55 TO STA. 375+16.84 = 150 POUNDS  
 STA. 376+20.24 TO STA. 376+80 = 150 POUNDS  
 TOTAL = 300 POUNDS

**HOT-MIX SURFACE COUSE MIX "D" N 70**  
 STA. 374+55 TO STA. 375+16.84 = 35 TON  
 STA. 376+20.24 TO STA. 376+80 = 36 TON  
 TOTAL = 71 TON

**HMA SURFACE REMOVAL - BUTT JOINT**  
 STA. 374+55 TO STA. 374+85 = 160 SQ. YD.  
 STA. 376+50 TO STA. 376+80 = 160 SQ. YD.  
 TOTAL = 320 SQ. YD.

**LEVELING BINDER (HAND METHOD), N70**  
 TOTAL = 3 TON

**SIDEWALK REMOVAL**  
 STA. 374+55 TO STA. 374+85 = 150 SQ. FT.  
 STA. 376+50 TO STA. 376+80 = 150 SQ. FT.  
 TOTAL = 300 SQ. FT.

**PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH**  
 STA. 374+55 TO STA. 374+85 = 150 SQ. FT.  
 STA. 376+50 TO STA. 376+80 = 150 SQ. FT.  
 TOTAL = 300 SQ. FT.

**NOTES:**

1. THE BASE FOR ALL PROPOSED HMA SURFACE COURSE SHALL BE PREPARED, TACKED, AND LEVELED ACCORDING TO ART. 406.05. A NOMINAL QTY OF 3 TONS OF LEVELING BINDER (HAND METHOD), N70 HAS BEEN PROVIDED FOR ALL DEPRESSIONS OF 1 INCH OR MORE IN THE SURFACE OF THE EXISTING PAVEMENT. EXACT LOCATIONS OF DEPRESSIONS SHALL BE DETERMINED BY THE ENGINEER PRIOR TO PLACEMENT OF PROPOSED HMA SURFACE COURSE.
2. REMOVAL AND REATTACHMENT OF GUARDRAIL, TRAFFIC BARRIER TERMINAL, ETC. REQUIRED FOR REMOVAL AND REPLACEMENT OF COMBINATION CURB AND GUTTER SHALL BE INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
3. A NOMINAL QTY OF 20 SQ YD OF SODDING, SALT TOLERANT AND 20 SQ YD OF TOPSOIL FURNISH AND PLACE, 4" HAS BEEN PROVIDED TO RESTORE ALL EXISTING GROUND DISTURBED DURING THE REMOVAL OF CURB AND GUTTER AND SIDEWALK.
4. COMBINATION CONCRETE CURB AND GUTTER REMOVAL SHALL BE IN ACCORDANCE WITH THE DISTRICT DETAIL BD-24.

FILE NAME = I:\10203 PTB 182 04\1020317-IL 171 over-Sat1\CADD\CADD\_Sheets\0160R73-shr-pln.dgn



USER NAME = ztaner  
 DESIGNED - ZJT  
 DRAWN - ZJT  
 PLOT SCALE = 0.1667' / 1" = 1/6"  
 PLOT DATE = 4/12/2018

DESIGNED - ZJT  
 DRAWN - ZJT  
 CHECKED - KVH  
 DATE - 03/23/2018

REVISED -  
 REVISED -  
 REVISED -  
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

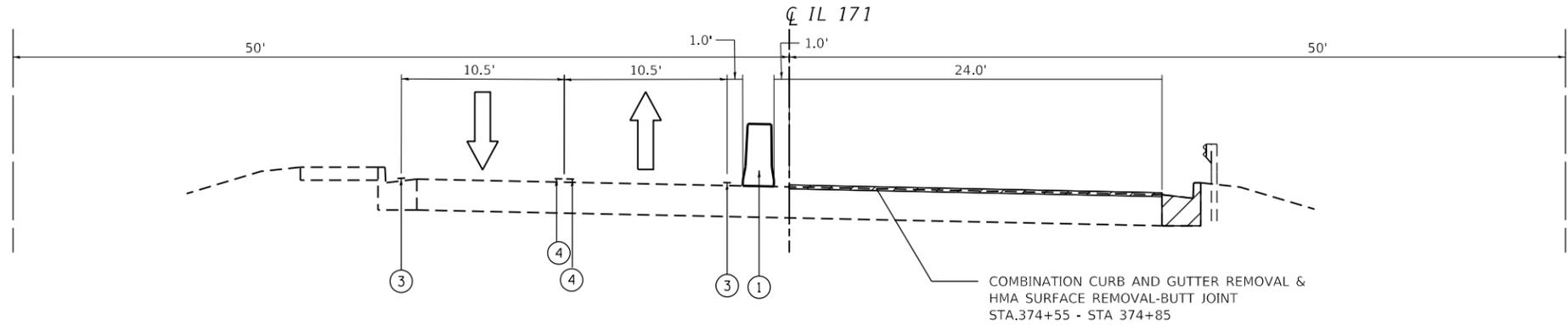
**ROADWAY PLAN  
 ILLINOIS 171 (FRIST AVENUE)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0202-BR	COOK	27	7
CONTRACT NO. 60R73				
ILLINOIS FED. AID PROJECT				

**STAGE I NOTES:**

1. ERECT TEMPORARY CONSTRUCTION SIGNS FOR STAGE 1 MOT.
2. INSTALL TEMPORARY MOT FOR STAGE 1 REPAIR WORK ON BRIDGE.
3. CONSTRUCTION FOR STAGE 1: BRIDGE DECK HYDRO-SCARIFICATION, BRIDGE DECK CONCRETE OVERLAY, JOINT RECONSTRUCTION, PARTIAL DEPTH AND FULL DEPTH PAVEMENT AND SUBSTRUCTURE REPAIRS.

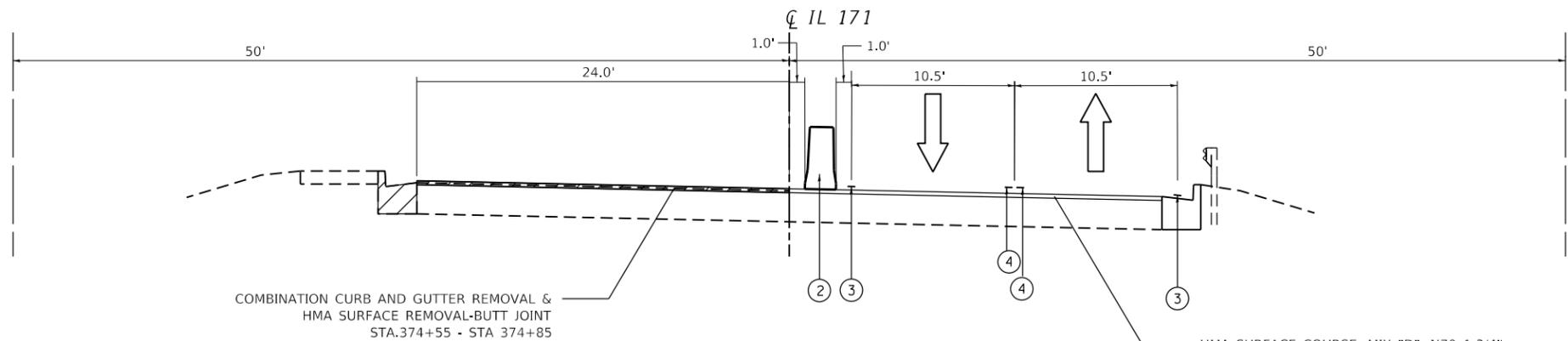


**PROPOSED TYPICAL SECTION  
IL 171 - STAGE I**

STA. 372+50.00 TO STA. 378+10.00  
CONCRETE BARRIER FROM STA. 373+20 - STA. 378+10

**STAGE II NOTES:**

1. ERECT TEMPORARY CONSTRUCTION SIGNS FOR STAGE 2 MOT.
2. INSTALL TEMPORARY MOT FOR STAGE 2 REPAIR WORK ON BRIDGE.
3. CONSTRUCTION FOR STAGE 2: BRIDGE DECK HYDRO-SCARIFICATION, BRIDGE DECK CONCRETE OVERLAY, JOINT RECONSTRUCTION, PARTIAL DEPTH AND FULL DEPTH PAVEMENT AND SUBSTRUCTURE REPAIRS.

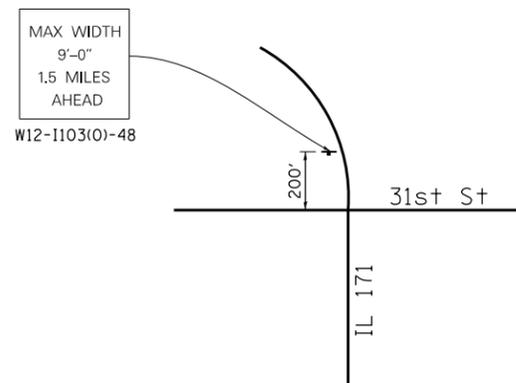


**PROPOSED TYPICAL SECTION  
IL 171 - STAGE II**

STA. 373+70 TO STA. 378+54  
CONCRETE BARRIER FROM STA. 373+20 - STA. 378+10

HMA SURFACE COURSE, MIX "D", N70 1-3/4"  
STA. 374+55 - 375+16.84 &  
STA. 376+20.24 - 376++50

**WIDTH RESTRICTION SIGNING IL 171  
SOUTHBOUND TRAFFIC FOR STAGE I & II**



**PROPOSED LEGEND**

- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY CONCRETE BARRIER, RELOCATE
- ③ PAVEMENT MARKING TAPE, TYPE IV 4" WHITE
- ④ PAVEMENT MARKING TAPE, TYPE IV 4" YELLOW

FILE NAME = I:\102803\_PTB\_182\_04\102803\17-IL\_171\_ever\_Sat1\CADD\CADD\_Sheets\0160R73-shr+siging-1.dgn

**COLLINS ENGINEERS**  
123 North Wacker Drive  
Suite 900  
Chicago, IL 60606  
(312) 704-9300  
www.collinsengr.com

USER NAME = ztaner	DESIGNED - ZJT	REVISED -
PLOT SCALE = 100.0000' / 1in.	DRAWN - ZJT	REVISED -
PLOT DATE = 4/12/2018	CHECKED - KVH	REVISED -
	DATE - 03/23/2018	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

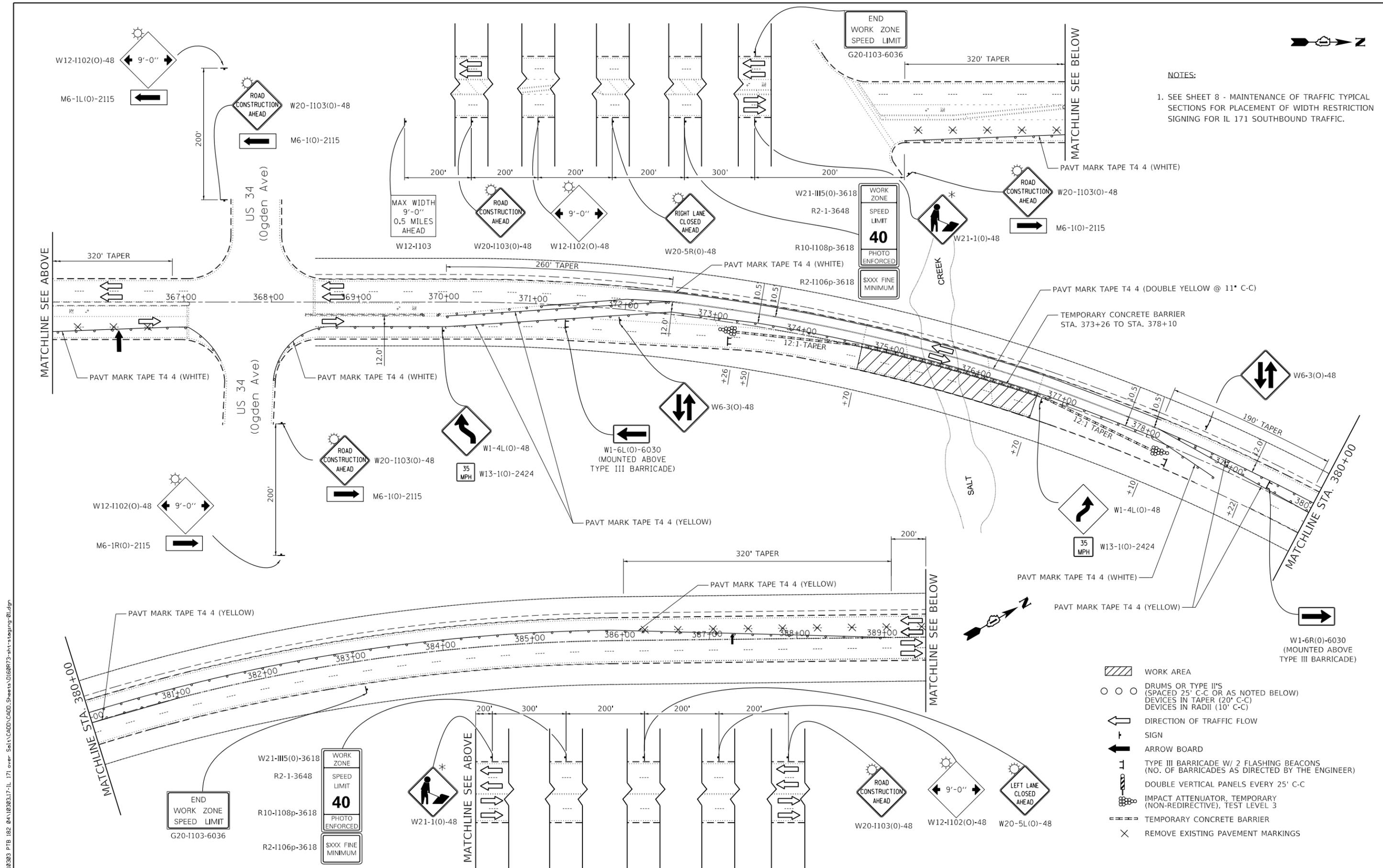
**MAINTENANCE OF TRAFFIC - TYPICAL SECTIONS  
ILLINOIS 171 (FIRST AVENUE)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0202-BR	COOK	27	8
CONTRACT NO. 60R73				
ILLINOIS FED. AID PROJECT				



NOTES:  
 1. SEE SHEET 8 - MAINTENANCE OF TRAFFIC TYPICAL SECTIONS FOR PLACEMENT OF WIDTH RESTRICTION SIGNING FOR IL 171 SOUTHBOUND TRAFFIC.



- WORK AREA
- DRUMS OR TYPE II'S (SPACED 25' C-C OR AS NOTED BELOW)  
 DEVICES IN TAPER (20' C-C)  
 DEVICES IN RADII (10' C-C)
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- TYPE III BARRICADE W/ 2 FLASHING BEACONS (NO. OF BARRICADES AS DIRECTED BY THE ENGINEER)
- DOUBLE VERTICAL PANELS EVERY 25' C-C
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
- TEMPORARY CONCRETE BARRIER
- REMOVE EXISTING PAVEMENT MARKINGS

FILE NAME = I:\102803\_PTB\_182\_04\102803-17-IL\_171\_ever\_Salt\_Salt\_Creek\0202-0166073-sh1-t-loging-01.dgn

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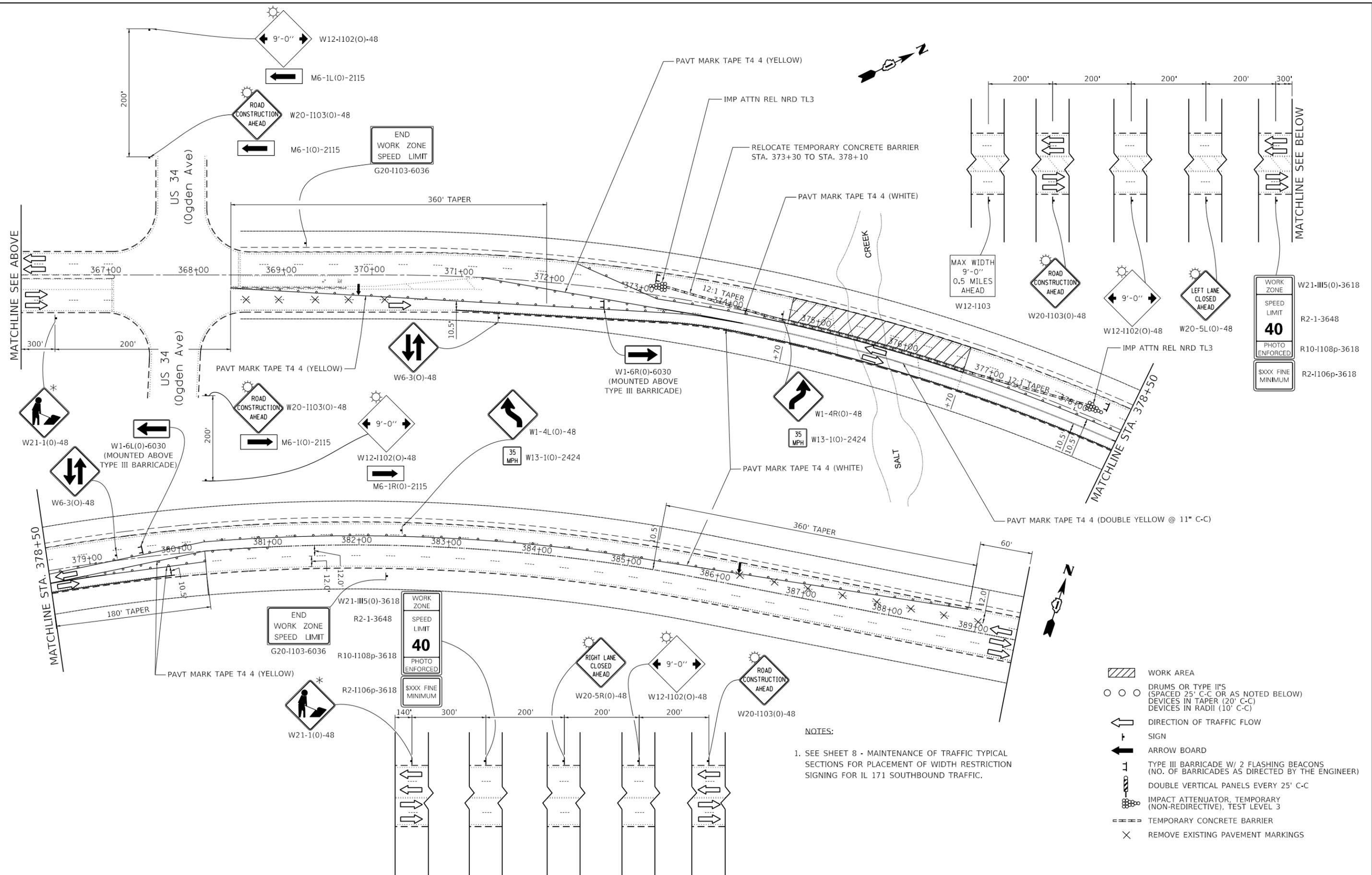
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PLOT DATE = 4/12/2018	CHECKED - KVH	REVISED -
	DATE - 03/23/2018	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>MAINTENANCE OF TRAFFIC - STAGE 1</b>	
<b>ILLINOIS 171 (FIRST AVENUE)</b>	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0202-BR	COOK	27	9
CONTRACT NO. 60R73				
ILLINOIS FED. AID PROJECT				

FILE NAME = I:\102803\_PTB\_182\_04\102803\17-IL\_171\_ever\_Salt\_CADD\CADD\_Sheets\0160R73-sh1+stage-02.dgn



MAX WIDTH  
9'-0"  
0.5 MILES  
AHEAD  
W12-1103

WORK ZONE	W21-1115(0)-3618
SPEED LIMIT	R2-1-3648
40	
PHOTO ENFORCED	R10-1108p-3618
\$XXX FINE MINIMUM	R2-1106p-3618

NOTES:  
1. SEE SHEET 8 - MAINTENANCE OF TRAFFIC TYPICAL SECTIONS FOR PLACEMENT OF WIDTH RESTRICTION SIGNING FOR IL 171 SOUTHBOUND TRAFFIC.

- WORK AREA
- DRUMS OR TYPE III'S (SPACED 25' C-C OR AS NOTED BELOW)  
DEVICES IN TAPER (20' C-C)  
DEVICES IN RADII (10' C-C)
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- TYPE III BARRICADE W/ 2 FLASHING BEACONS (NO. OF BARRICADES AS DIRECTED BY THE ENGINEER)
- DOUBLE VERTICAL PANELS EVERY 25' C-C
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
- TEMPORARY CONCRETE BARRIER
- REMOVE EXISTING PAVEMENT MARKINGS

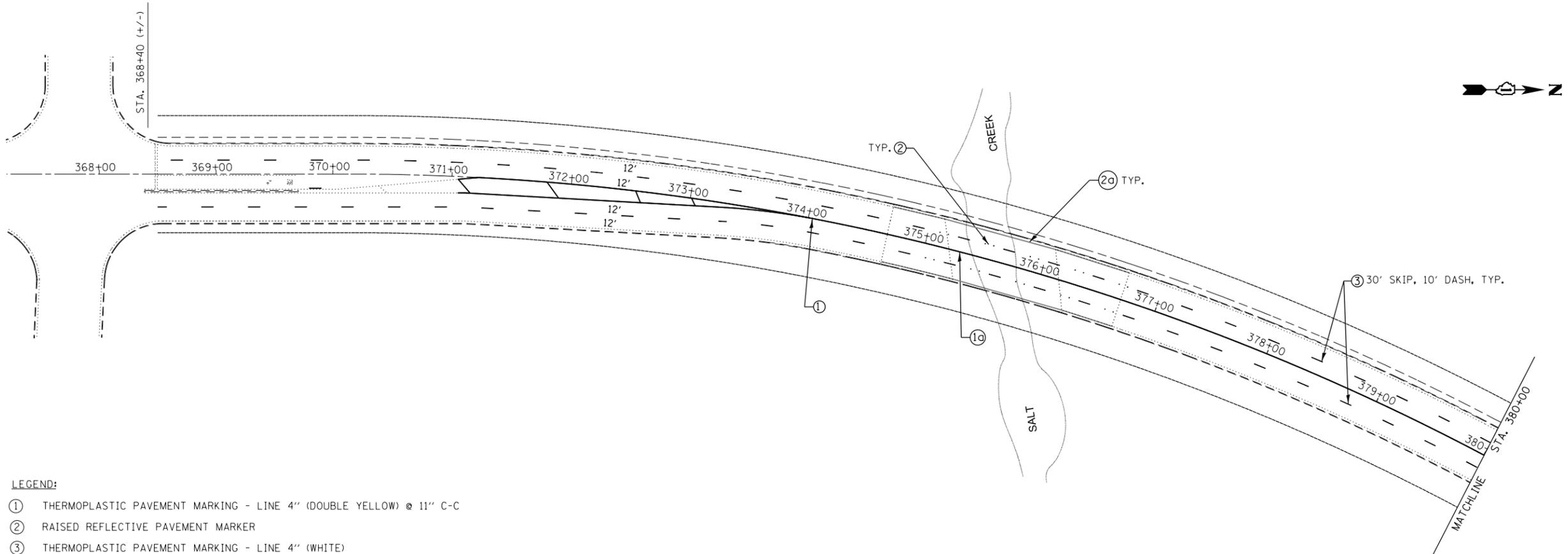
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PLOT SCALE = 100.0000' / 1"	DRAWN - ZJT	REVISED -
PLOT DATE = 4/12/2018	CHECKED - KVH	REVISED -
	DATE - 03/23/2018	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCALE:		SHEET OF SHEETS		STA. TO STA.	
MAINTENANCE OF TRAFFIC - STAGE 2		ILLINOIS 171 (FIRST AVENUE)			

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0202-BR	COOK	27	10
CONTRACT NO. 60R73				
ILLINOIS FED. AID PROJECT				

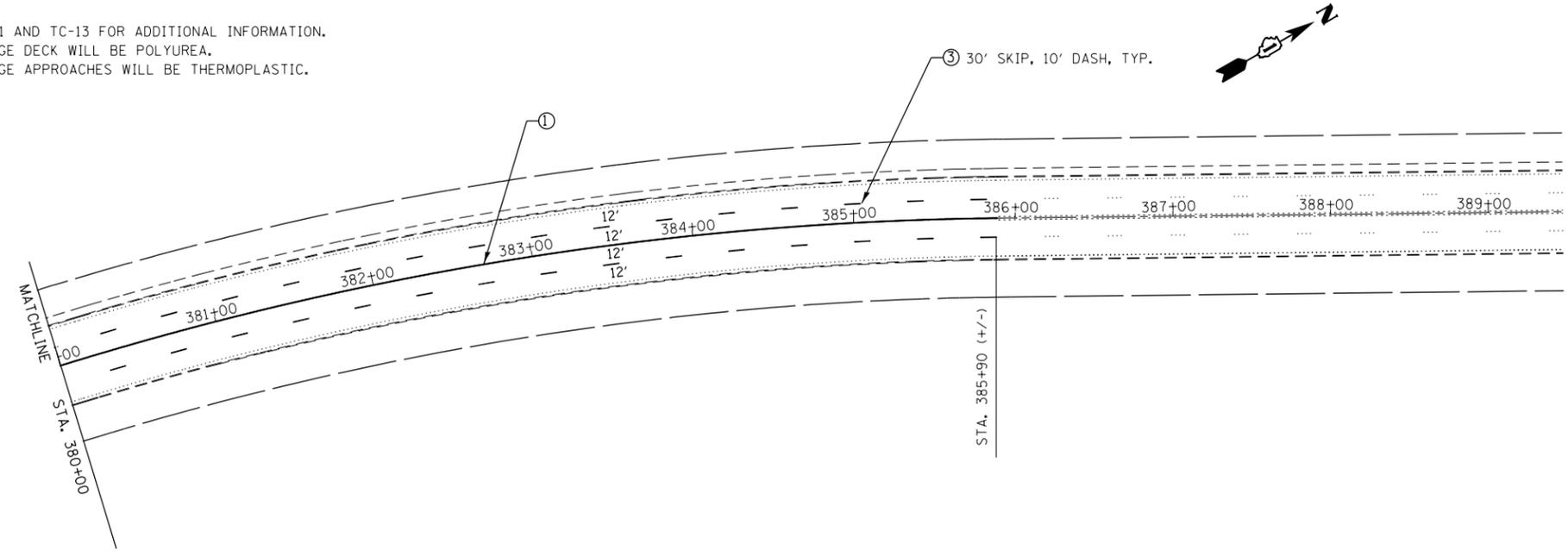


**LEGEND:**

- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW) @ 11" C-C
- ② RAISED REFLECTIVE PAVEMENT MARKER
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE)
- ①a POLYUREA PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW) @ 11" C-C
- ②a POLYUREA PAVEMENT MARKING - LINE 4" (WHITE)

**NOTES:**

1. REFER TO DISTRICT ONE DETAILS TC-11 AND TC-13 FOR ADDITIONAL INFORMATION.
2. ALL PAVEMENT MARKINGS ON THE BRIDGE DECK WILL BE POLYUREA.
3. ALL PAVEMENT MARKINGS ON THE BRIDGE APPROACHES WILL BE THERMOPLASTIC.



FILE NAME = I:\102803\_PTB\_182\_04\102803\17-IL\_171\_ever\_Salt\CADD\CADD\_Sheets\0160R73-shr.pmk.dgn

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USER NAME = ztanner	DESIGNED - ZJT	REVISED -
DRAWN - ZJT	REVISED -	
PLOT SCALE = 100.0000' / 1" =	CHECKED - KVH	REVISED -
PLOT DATE = 4/12/2018	DATE - 03/23/2018	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN  
 ILLINOIS 171 (FIRST AVENUE)**

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0202-BR	COOK	27	11
CONTRACT NO. 60R73				
ILLINOIS FED. AID PROJECT				

Existing Structure No. 016-0702 was built in 1931 as SA 33 Section 0202.  
 In 1984, under FAU 2746 Section 0204, the superstructure was replaced.  
 Existing Structure is a two span reinforced concrete slab on reinforced concrete beams between closed abutments, 107'-2<sup>3</sup>/<sub>8</sub>" bk. to bk. abutments, 59'-9" out to out deck.

Traffic to be maintained via staged construction.

Salvage: None

**INDEX OF SHEETS**

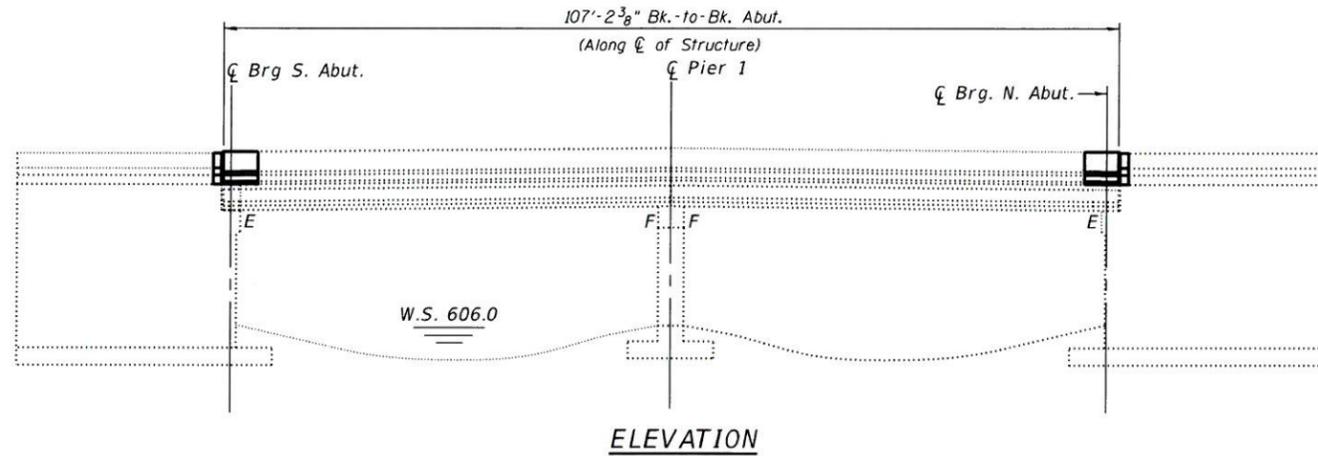
- S1 General Plan
- S2 Stage Construction Details
- S3 Deck Plan, Stage 1
- S4 Deck Plan, Stage 2
- S5-S6 Expansion Joint Repair
- S7 Preformed Joint Strip Seal
- S8 Bar Splicer Assembly
- S9 Substructure Repairs
- S10 Stream Gauge

**SCOPE OF WORK**

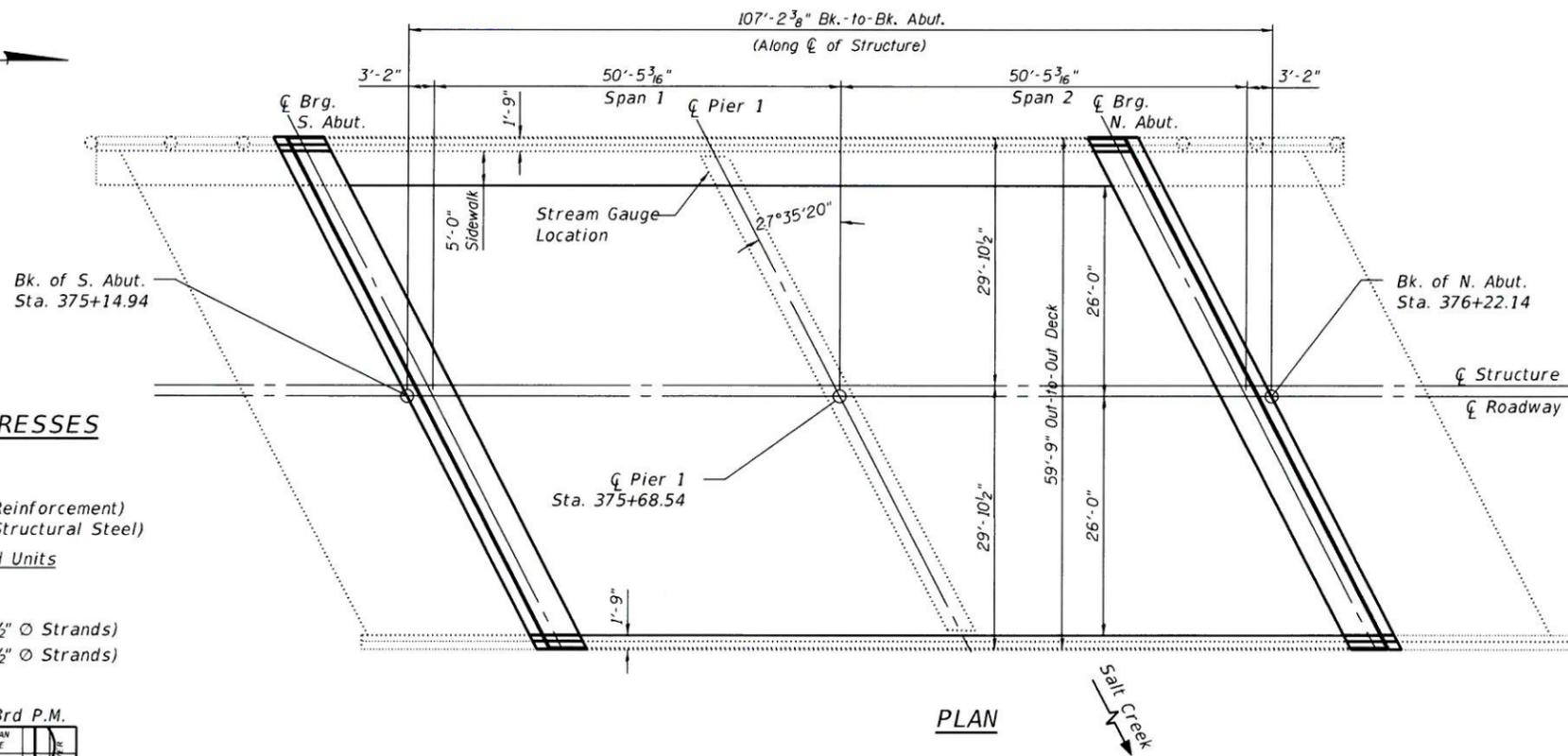
1. Remove and Replace Expansion Joints.
2. Scarify and Repair Deck.
3. Place Concrete Overlay on Deck.
4. Clean and Seal Bridge Seats.
5. Substructure Repairs
6. Apply Protective Coat.
7. Finish and install Stream Gauge.

**TOTAL BILL OF MATERIAL**

Item	Unit	Total
Concrete Removal	Cu. Yd.	35.6
Concrete Superstructure	Cu. Yd.	38.8
Bridge Deck Grooving	Sq. Yd.	577
Protective Coat	Sq. Yd.	236
Furnishing and Erecting Structural Steel	Pound	20
Reinforcement Bars, Epoxy Coated	Pound	6770
Bar Splicers	Each	36
Preformed Joint Strip Seal	Foot	135
Concrete Sealer	Sq. Ft.	294
Polymer Modified Portland Cement Mortar	Sq. Ft.	5
Stream Gauge	Ea.	1
Bridge Deck Latex Concrete Overlay 2 <sup>1</sup> / <sub>2</sub> "	Sq. Yd.	548
Cleaning Bridge Seats	Sq. Ft.	294
Bridge Deck Scarification <sup>3</sup> / <sub>4</sub> "	Sq. Yd.	548
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	149
Structural Repair of Concrete (Depth Greater than 5 inches)	Sq. Ft.	277
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	4



**ELEVATION**



**PLAN**

**DESIGN STRESSES**

Field Units

- f'c = 3,500 psi
- f<sub>y</sub> = 60,000 psi (Reinforcement)
- f<sub>y</sub> = 36,000 psi (Structural Steel)

Precast Prestressed Units

- f'c = 6,000 psi
- f'ci = 4,300 psi
- f<sub>s</sub> = 270,000 psi (1/2" Ø Strands)
- f<sub>si</sub> = 189,000 psi (1/2" Ø Strands)

**Range 12 E, 3rd P.M.**



**LOCATION SKETCH**

**GENERAL NOTES**

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.

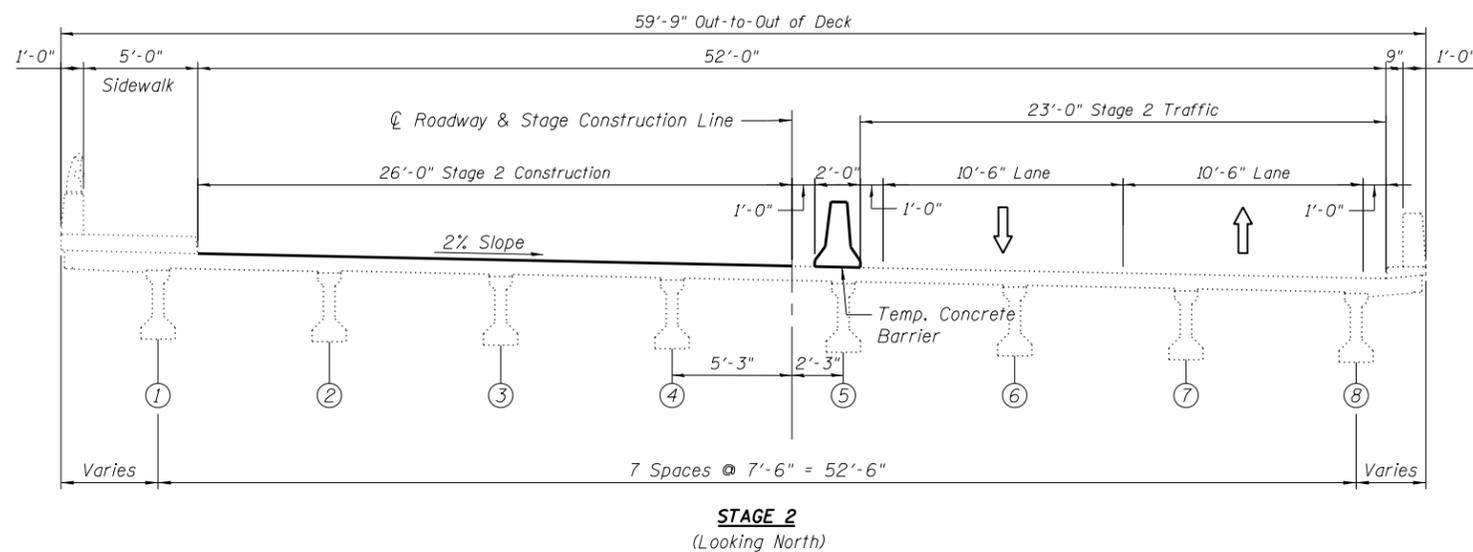
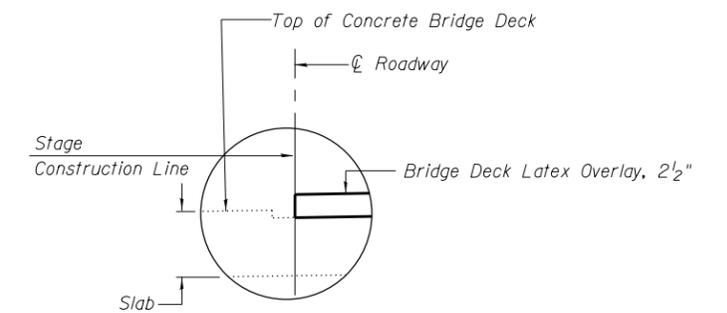
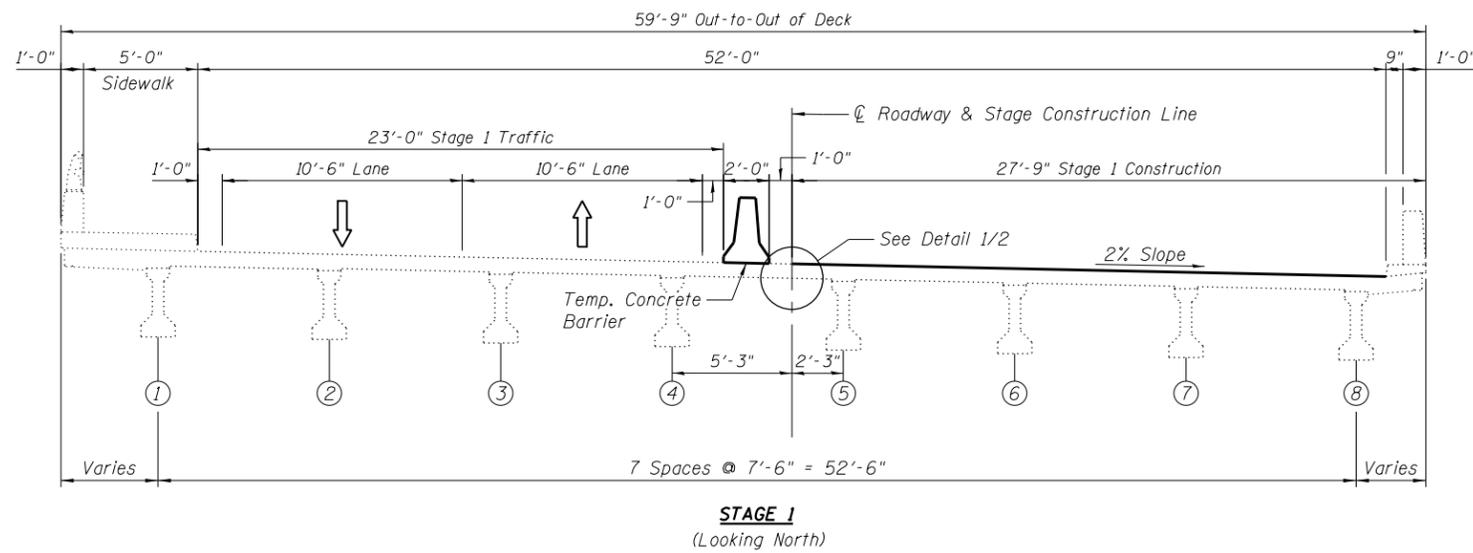
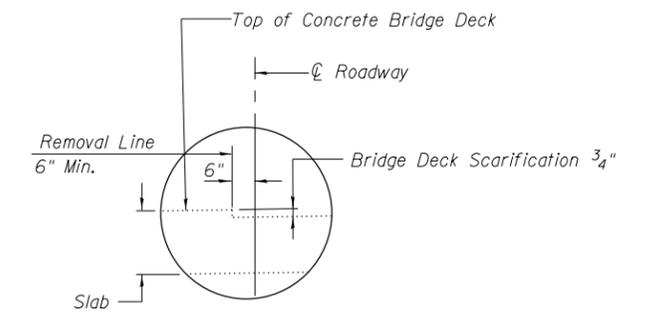
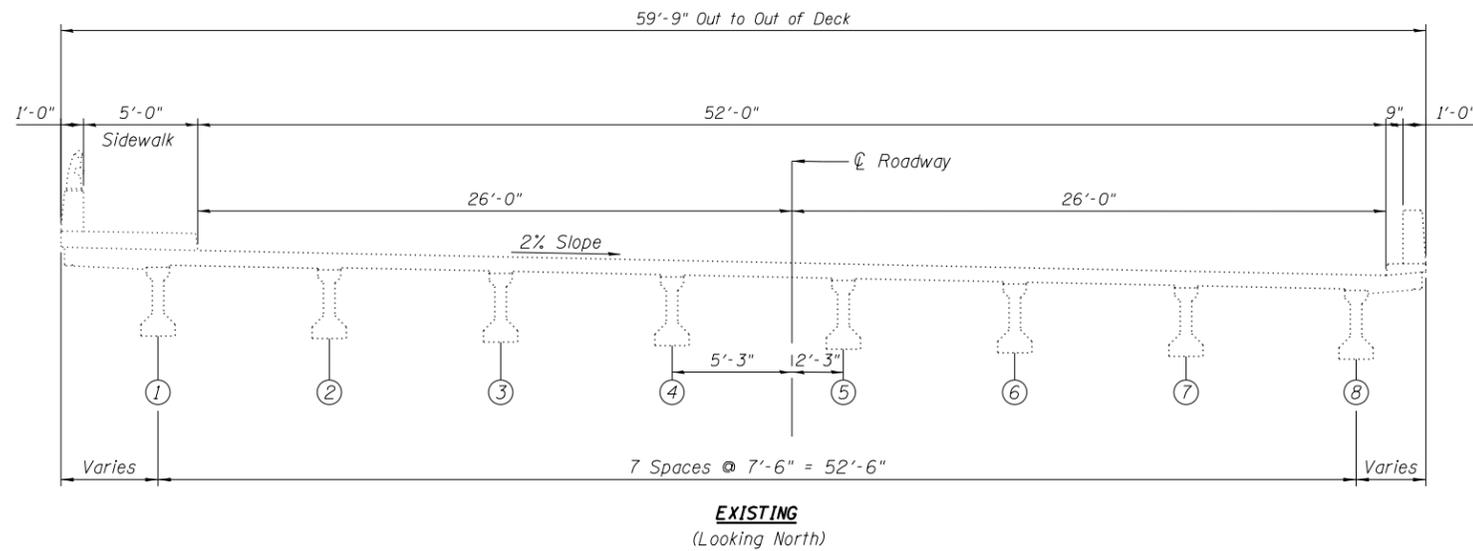
Reinforcement bars designated (E) shall be epoxy coated.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

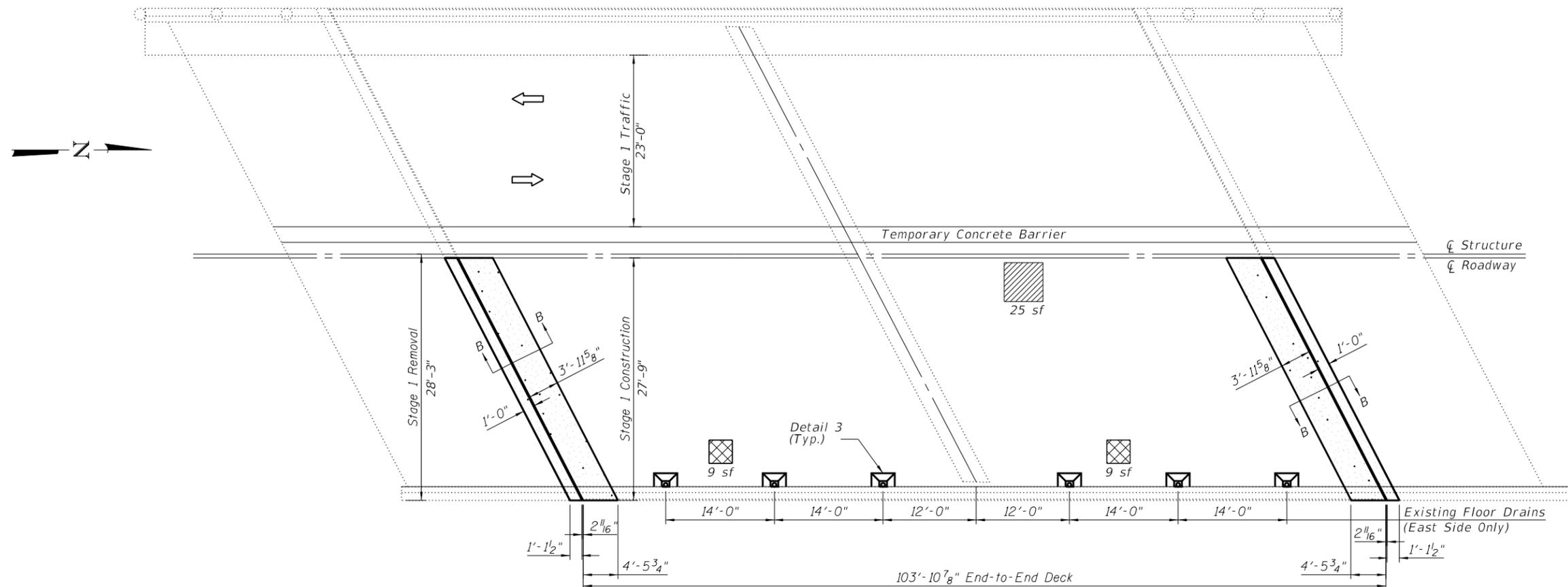
Areas of deck repairs shown are estimated. The Engineer shall show actual locations of deck repairs on As-built Plans.



**JASON M. SCHNEIDER, S.E., P.E.**  
 EXP. 11/30/2018



**NOTE:**  
Temporary Concrete Barrier is paid for as Roadway Item. See Standard 704001-07.



**DECK PLAN - STAGE 1**

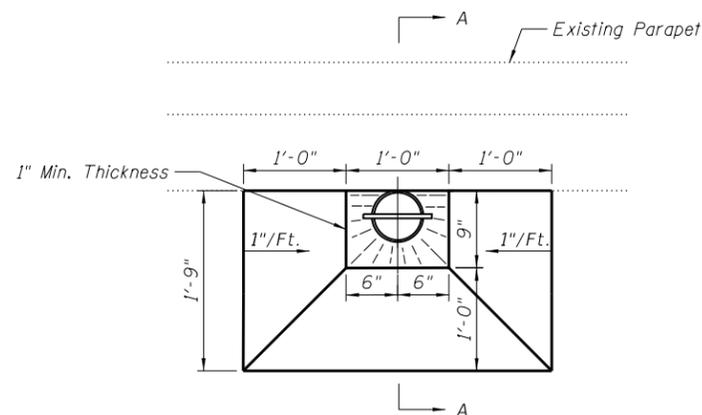
**NOTES:**

- Deck Slab Repair areas estimated based on visual inspection completed in June 2017 and November 2017. Actual repair areas and locations shall be determined by the Engineer and shown on the as-built plans.
- For Section B-B and Expansion Joint Reconstruction Details See Sheet S5.
- Bridge Deck Scarification, <sup>3</sup>/<sub>4</sub>" and Bridge Deck Latex Concrete Overlay, 2<sup>1</sup>/<sub>2</sub>" shall be performed over the limits of the bridge deck, excluding the transverse joint reconstruction areas.
- Bridge Deck Grooving shall be performed over the limits of the bridge deck.
- Protective coat shall be applied to the top and inside faces of the bridge deck parapets, curbs, sidewalks, and the transverse joint reconstruction areas.
- Synthetic fibers shall be added to the Bridge Deck Latex Concrete Overlay, see Special Provisions.
- The Contractor is responsible for preventing debris from falling into the waterway.

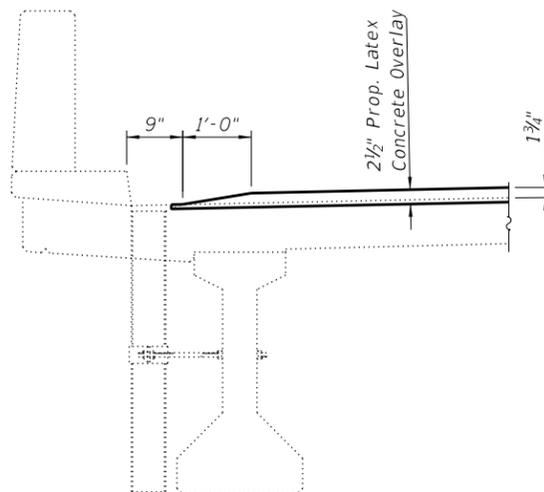
**BILL OF MATERIAL**

Legend	Item	Unit	Total
	Deck Slab Repair (Partial Depth)*	Sq. Yd.	3
	Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	2
	Concrete Removal and Replacement		
	Bridge Deck Scarification <sup>3</sup> / <sub>4</sub> "	Sq. Yd.	274
	Bridge Deck Grooving	Sq. Yd.	289
	Bridge Deck Latex Concrete Overlay, 2 <sup>1</sup> / <sub>2</sub> "	Sq. Yd.	274
	Protective Coat	Sq. Yd.	118

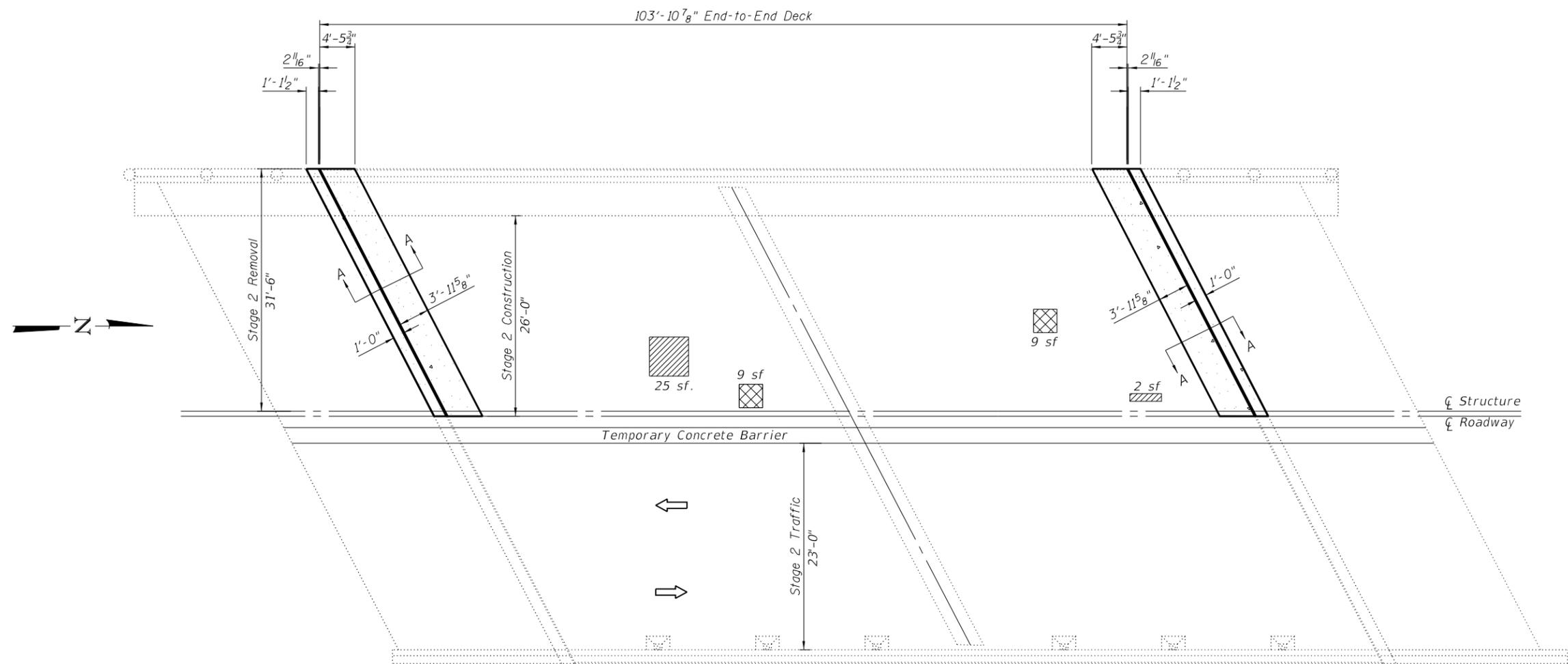
\* For Information Only to assist the Contractor in bidding. See Special Provision for "Bridge Deck Latex Concrete Overlay".



**DETAIL 3**  
SHOWING OVERLAY TAPER AT FLOOR DRAINS



**SECTION A-A**



Note:  
For Sections B-B and Expansion Joint Reconstruction Details See Sheet S5.

**DECK PLAN - STAGE 2**

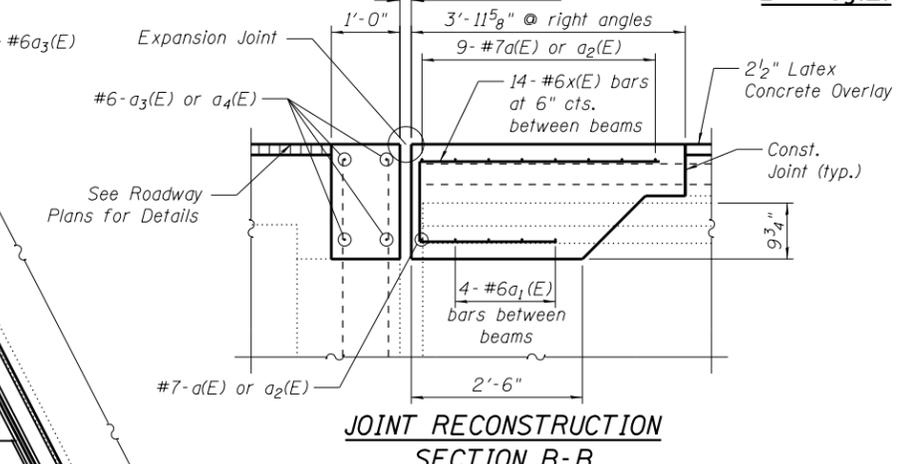
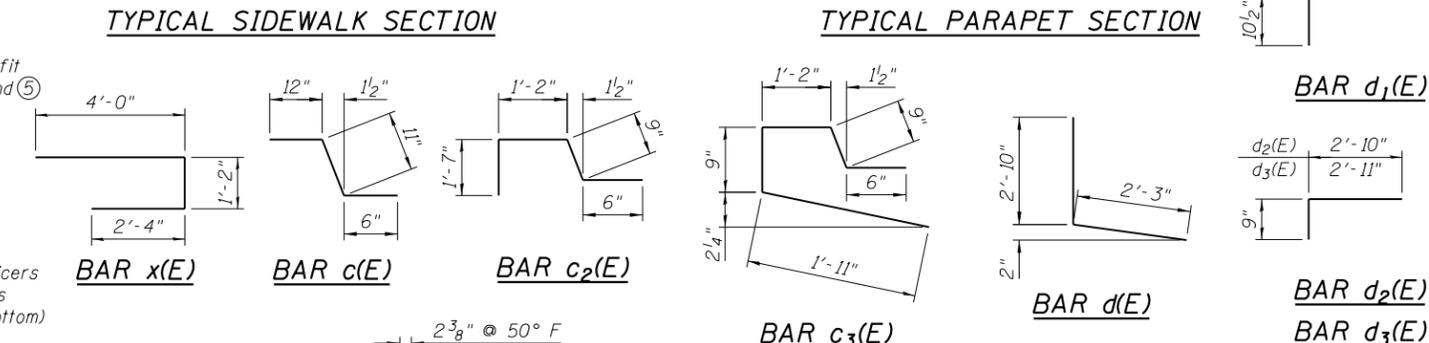
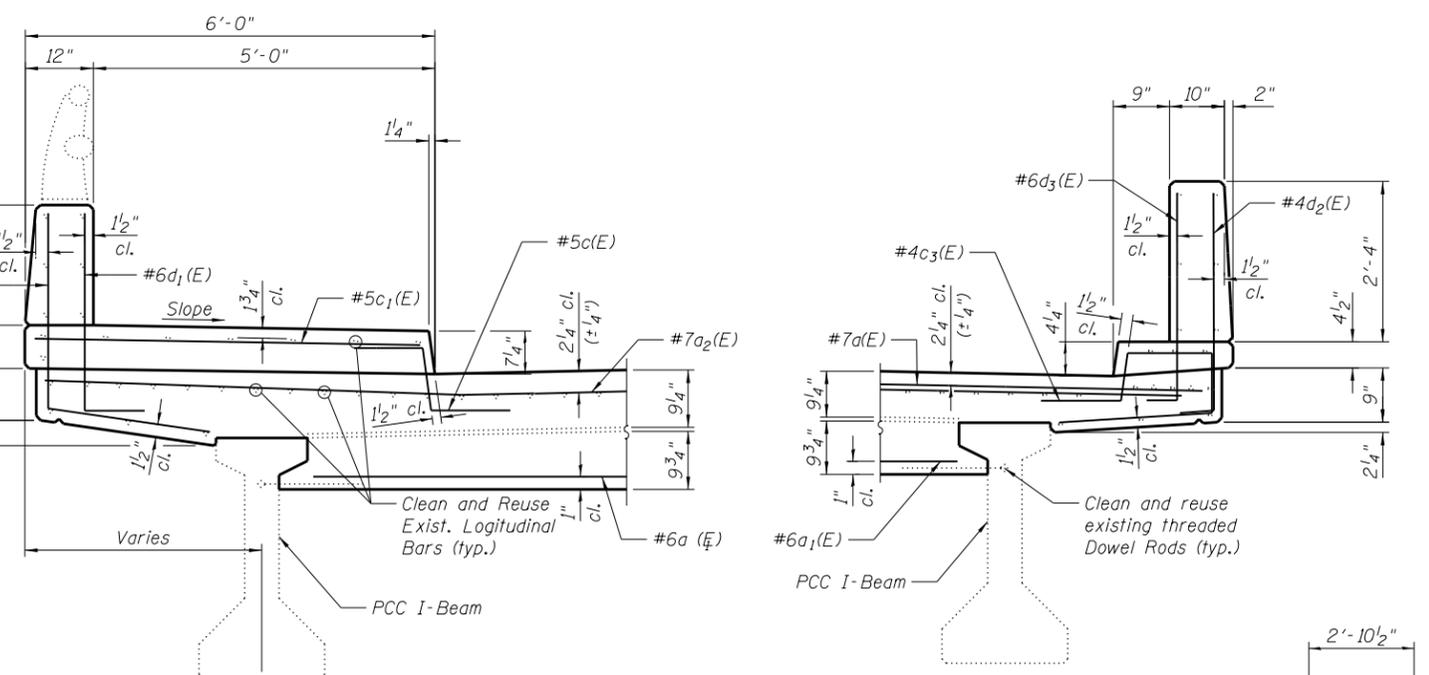
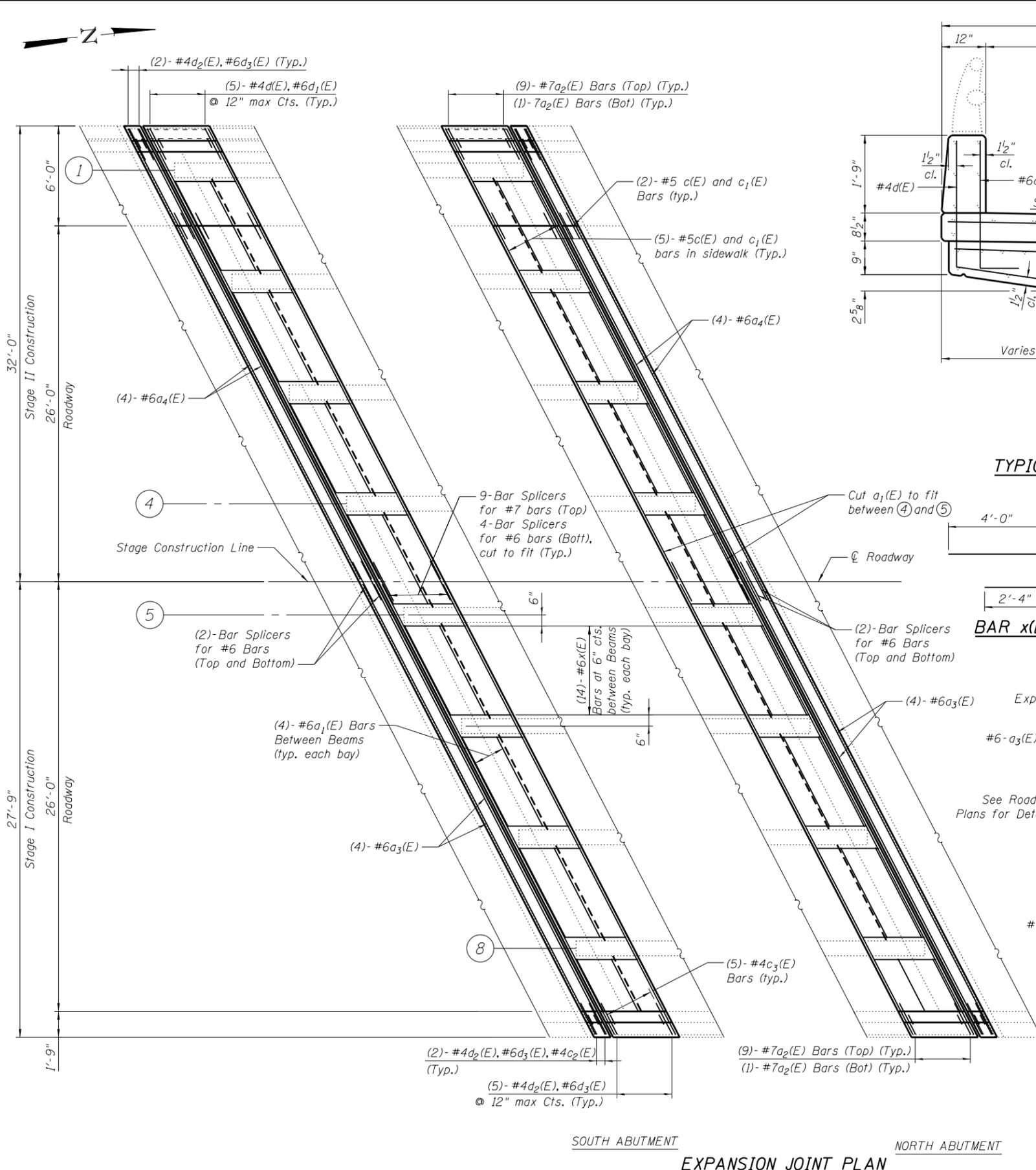
**NOTES:**

1. Deck Slab Repair areas estimated based on visual inspection completed in June 2017 and November 2017. Actual repair areas and locations shall be determined by the Engineer and shown on the as-built plans.
2. For Section B-B and Expansion Joint Reconstruction Details See Sheet S5.
3. Bridge Deck Scarification, 3/4" and Bridge Deck Latex Concrete Overlay, 2 1/2" shall be performed over the limits of the bridge deck, excluding the transverse joint reconstruction areas.
4. Bridge Deck Grooving shall be performed over the limits of the bridge deck.
5. Protective coat shall be applied to the top and inside faces of the bridge deck parapets, curbs, sidewalks, and the transverse joint reconstruction areas.
6. Synthetic fibers shall be added to the Bridge Deck Latex Concrete Overlay, see Special Provisions.
7. The Contractor is responsible for preventing debris from falling into the waterway.

**BILL OF MATERIAL**

Legend	Item	Unit	Total
	Deck Slab Repair (Partial Depth)*	Sq. Yd.	3
	Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	2
	Concrete Removal and Replacement		
	Bridge Deck Scarification 3/4"	Sq. Yd.	274
	Bridge Deck Grooving	Sq. Yd.	289
	Bridge Deck Latex Concrete Overlay, 2 1/2 "	Sq. Yd.	274
	Protective Coat	Sq. Yd.	118

\* For Information Only to assist the Contractor in bidding. See Special Provision for "Bridge Deck Latex Concrete Overlay".

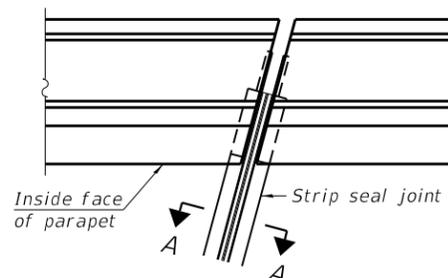


**NOTES**  
 For details of Preformed Joint Strip Seal Expansion Joints, see Sheet No. S6.  
 Existing longitudinal deck, sidewalk and parapet reinforcement shall remain in place. The contractor shall exercise care during removal of existing joints to ensure that the slab, beams and diaphragms integrity will not be detrimentally impacted. The contractor shall repair any damage(s) to the slabs, beams, and diaphragms caused by his operation as directed by the engineer at no additional cost to the department.  
 Placement of longitudinal reinforcement shall be parallel to roadway. Placement of traverse reinforcement shall be parallel to Abutment.  
 The removal and reattachment of guardrail, handrail, traffic barrier terminal, etc. required for the traverse joint replacement shall be included in the pay item "Concrete Removal."

**BILL OF MATERIAL**

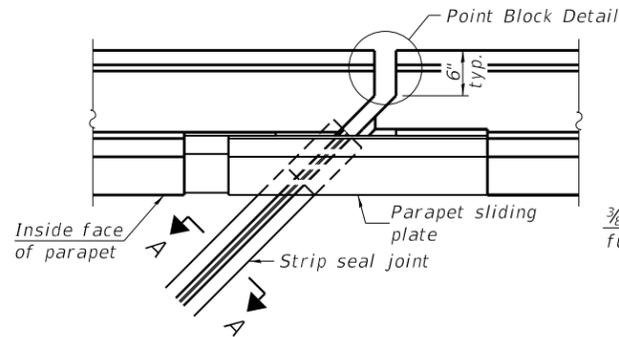
Bar	No.	Size	Length	Shape
a(E)	20	#7	31'-0"	—
a <sub>1</sub> (E)	56	#6	7'-7"	—
a <sub>2</sub> (E)	20	#7	35'-9"	—
a <sub>3</sub> (E)	8	#6	31'-0"	—
a <sub>4</sub> (E)	8	#6	35'-9"	—
c(E)	14	#5	2'-5"	—
c <sub>1</sub> (E)	14	#5	5'-8"	—
c <sub>2</sub> (E)	4	#4	4'-0"	—
c <sub>3</sub> (E)	10	#4	5'-1"	—
d(E)	10	#4	5'-1"	—
d <sub>1</sub> (E)	10	#6	3'-9"	—
d <sub>2</sub> (E)	18	#4	3'-7"	—
d <sub>3</sub> (E)	18	#6	3'-8"	—
x(E)	196	#6	7'-6"	—
Concrete Removal			Cu. Yd.	35.6
Concrete Superstructure			Cu. Yd.	38.8
Reinforcement Bars, Epoxy Coated			Pound	6770

See Sheet S8 for Bar Splicers.

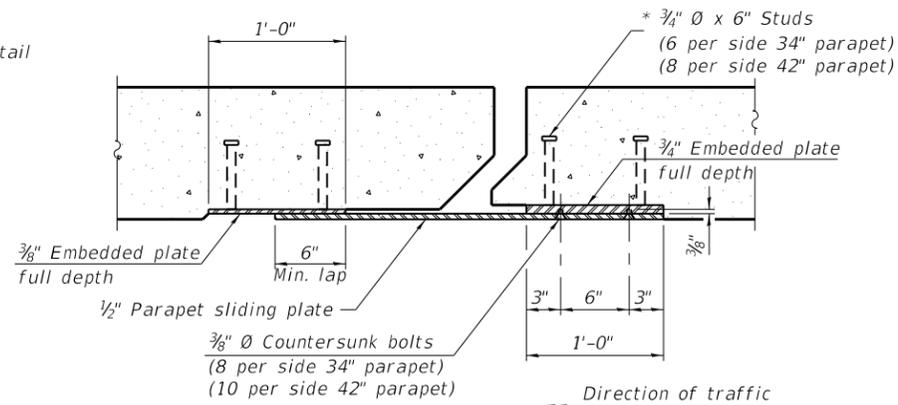


FOR SKEWS  $\leq 30^\circ$

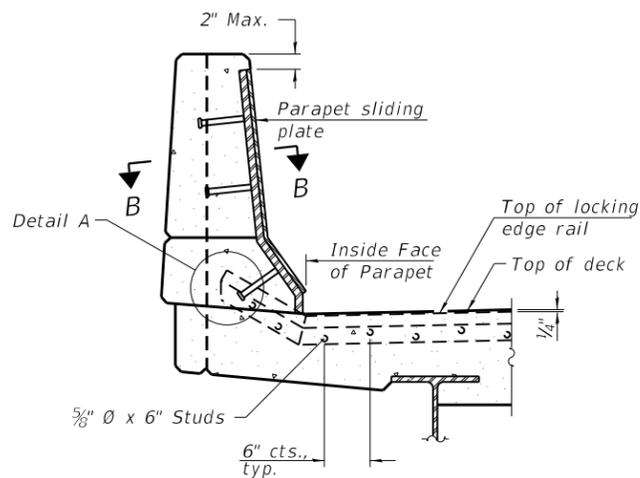
PLAN AT PARAPET



FOR SKEWS  $> 30^\circ$

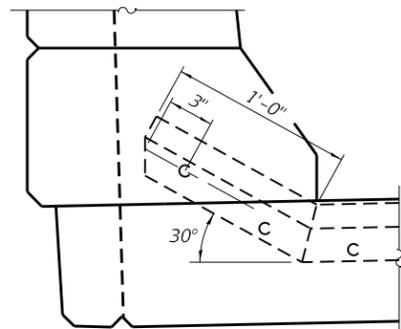


SECTION B-B

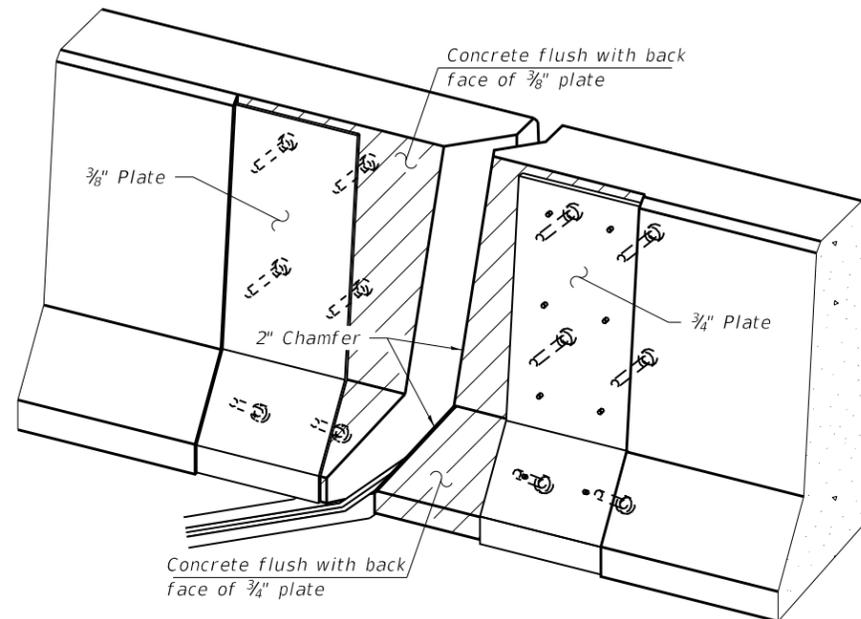


ELEVATION AT PARAPET

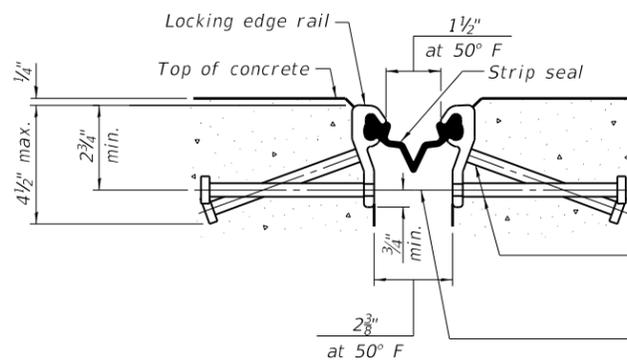
(Skews  $> 30^\circ$  shown. Skews  $\leq 30^\circ$  similar except as shown in plan view.)



DETAIL A



TRIMETRIC VIEW  
(Showing embedded plates only)



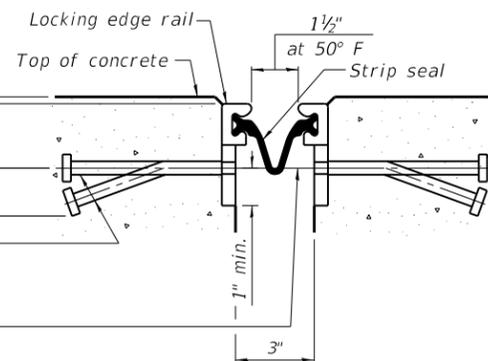
SHOWING ROLLED RAIL JOINT

\*  $5/8"$   $\phi$  x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

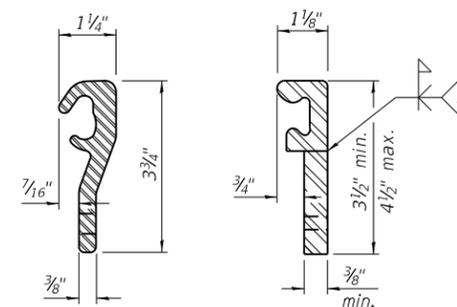
$3/8"$   $\phi$  threaded rods in  $7/16"$   $\phi$  holes at  $\pm 4"-0"$  cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

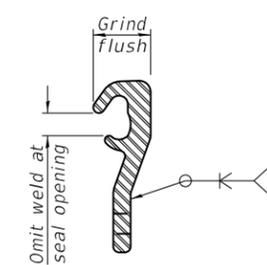
\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS  
\*\* Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	135

EJ-SS-S

8-11-17

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123 North Wacker Drive  
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Chicago, IL 60606  
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CHECKED - JMS  
DRAWN - KVH  
CHECKED - JMS

REVISED -  
REVISED -  
REVISED -  
REVISED -

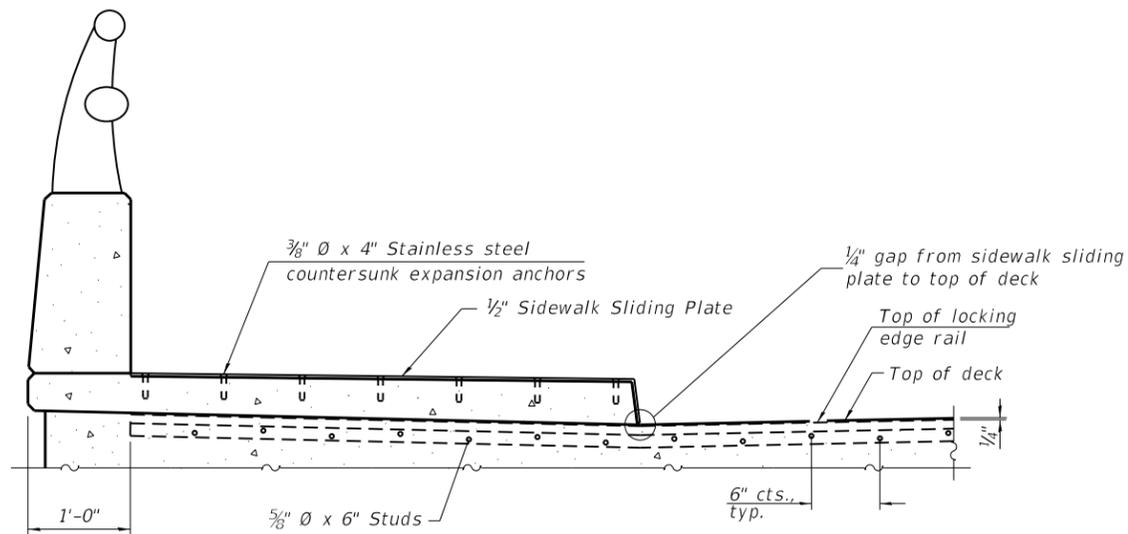
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL - SIDEWALK  
STRUCTURE NO.

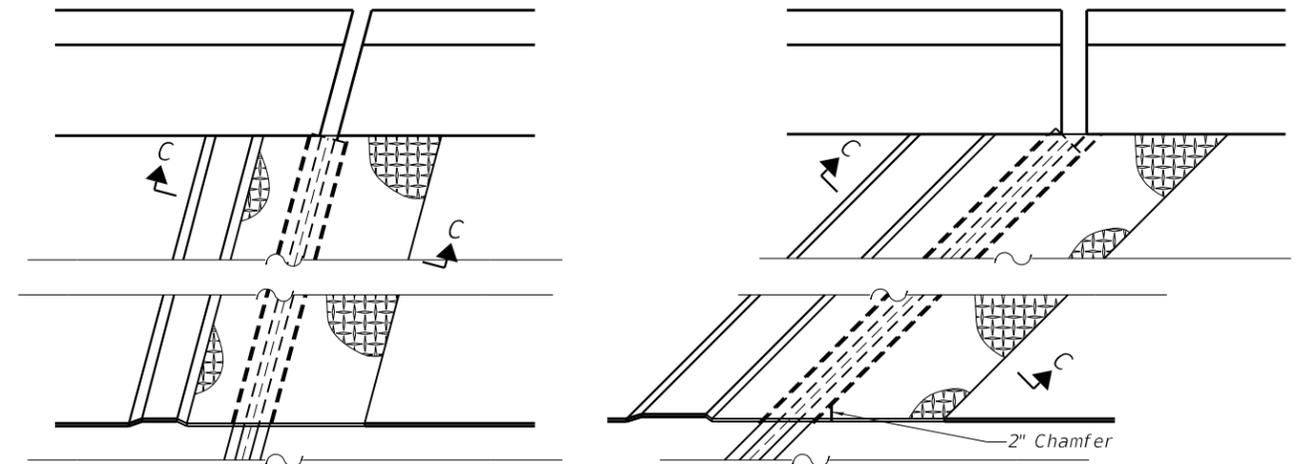
SHEET NO. 56 OF 510 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA. TO STA.		CONTRACT NO. 60R73		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

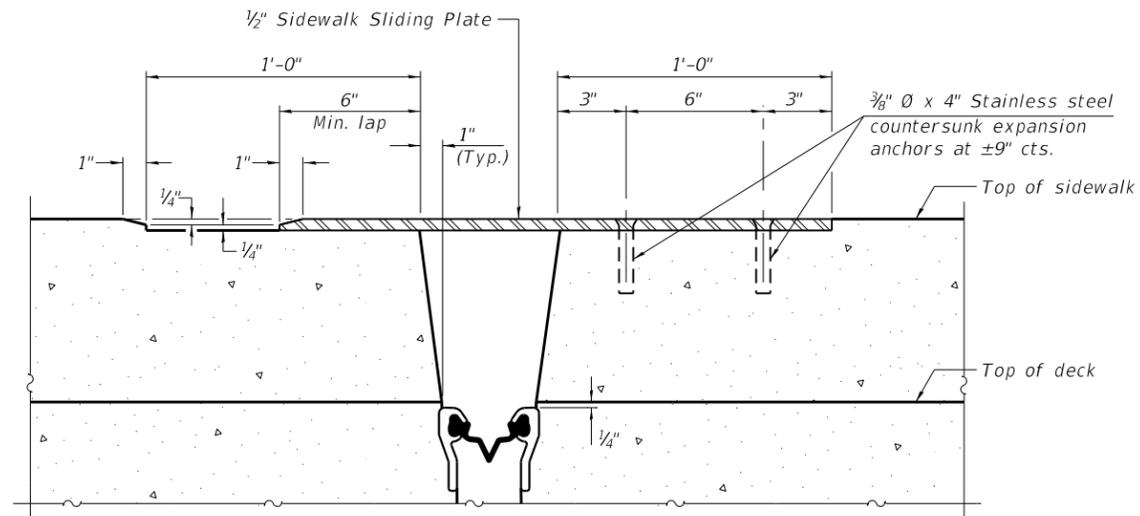
(Sheet 1 of 2)



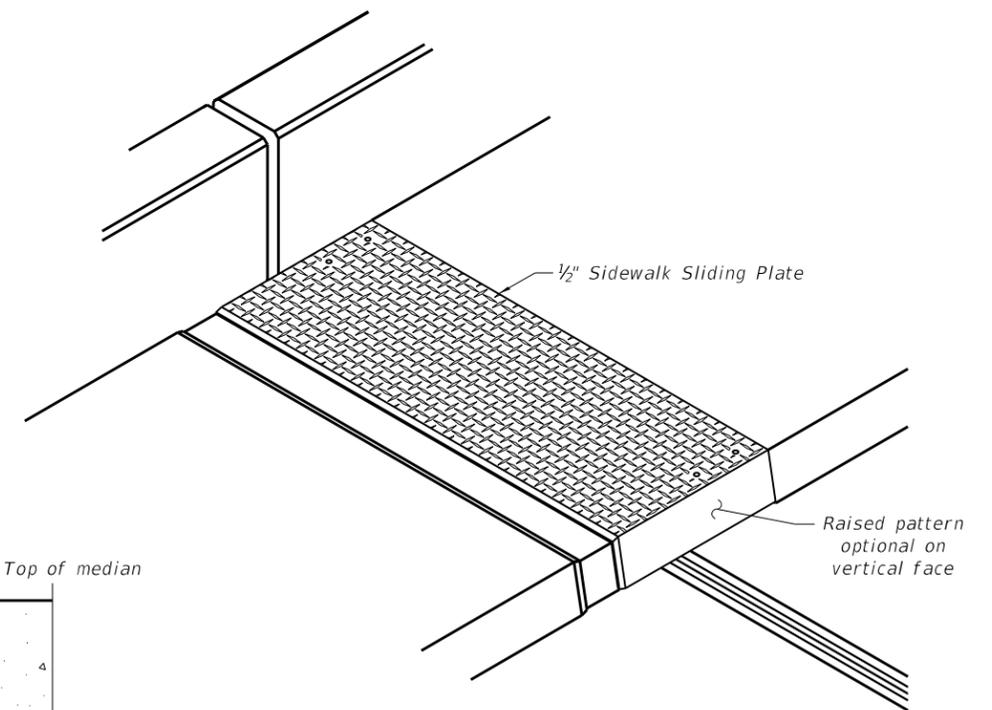
ELEVATION AT RAISED SIDEWALK



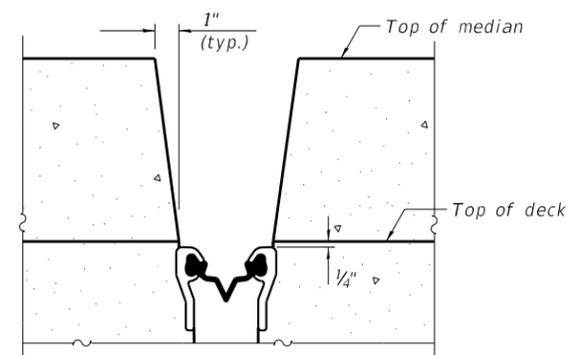
PLAN AT RAISED SIDEWALK



SECTION C-C



TRIMETRIC VIEW



SECTION D-D  
(at Rt. L's)

(Sheet 2 of 2)

EJ-SS-S

8-11-17

**COLLINS ENGINEERS**  
 123 North Wacker Drive  
 Suite 900  
 Chicago, IL 60606  
 (312) 704-9300  
 www.collinsengr.com

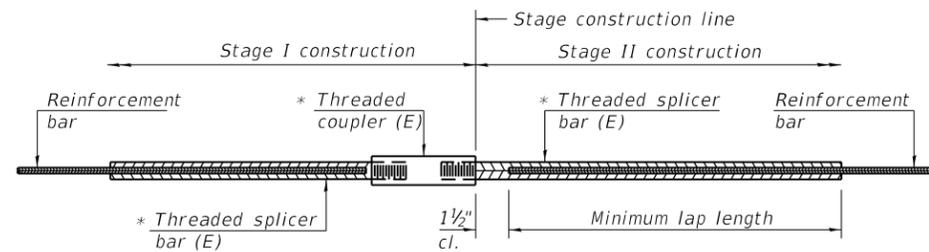
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CHECKED	- JMS	REVISED	-
DRAWN	- KVH	REVISED	-
CHECKED	- JMS	REVISED	-

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL - SIDEWALK  
 STRUCTURE NO.

SHEET NO. 57 OF 510 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0202-BR	COOK	27	18
STA. TO STA.	CONTRACT NO. 60R73			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

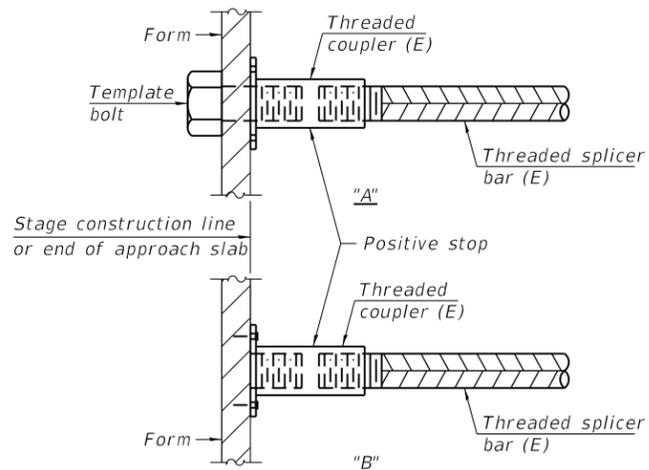


**STANDARD BAR SPLICER ASSEMBLY**

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
South Abutment	7	10	4'-8"
South Abutment	6	8	3'-7"
North Abutment	7	10	4'-8"
North Abutment	6	8	3'-7"

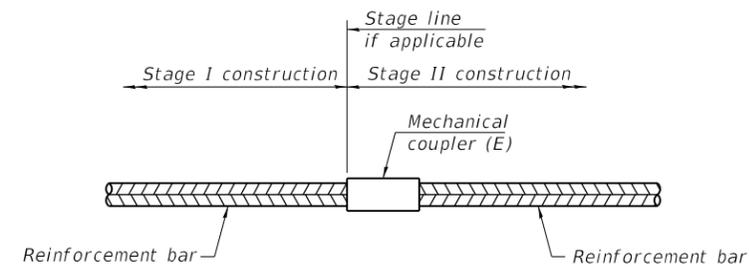


**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.

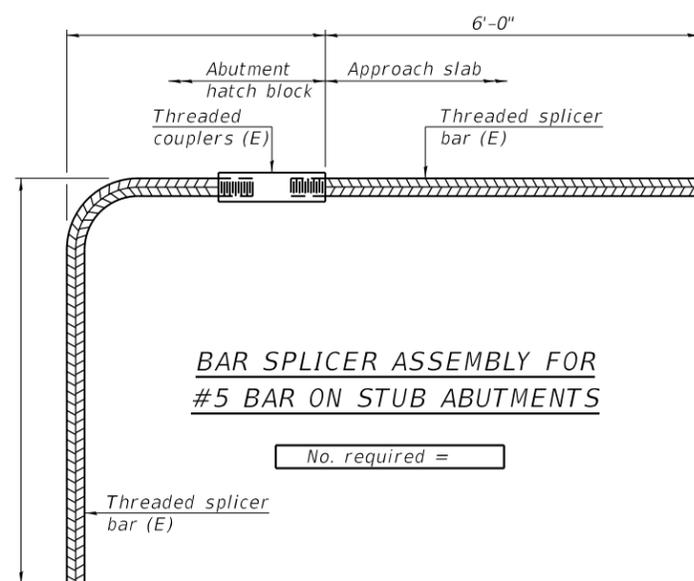
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required =

**NOTES**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars. Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 2-17-2017

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 123 North Wacker Drive  
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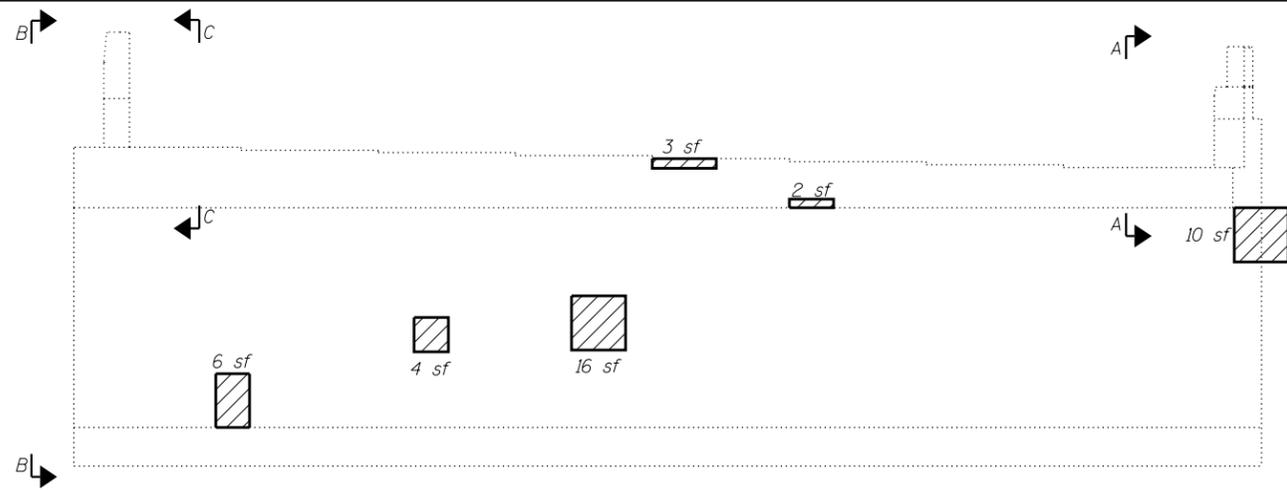
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CHECKED - JMS	REVISED -
DRAWN - KVH	REVISED -
CHECKED - JMS	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

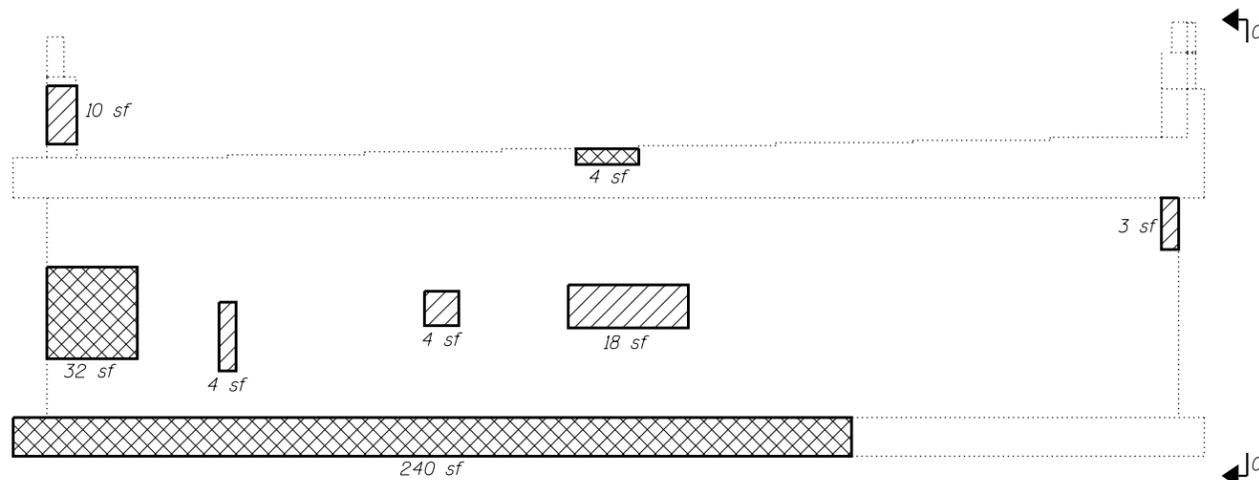
**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS**  
**STRUCTURE NO.**

SHEET NO. 58 OF 510 SHEETS

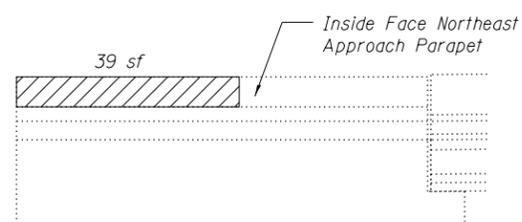
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0202-BR	COOK	27	19
STA. TO STA.	CONTRACT NO. 60R73			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



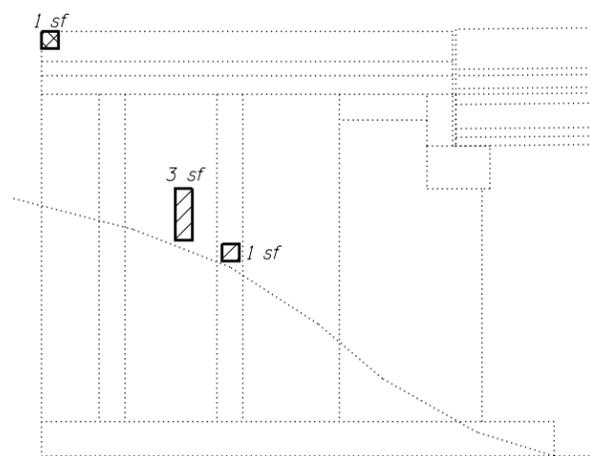
**NORTH ABUTMENT ELEVATION**



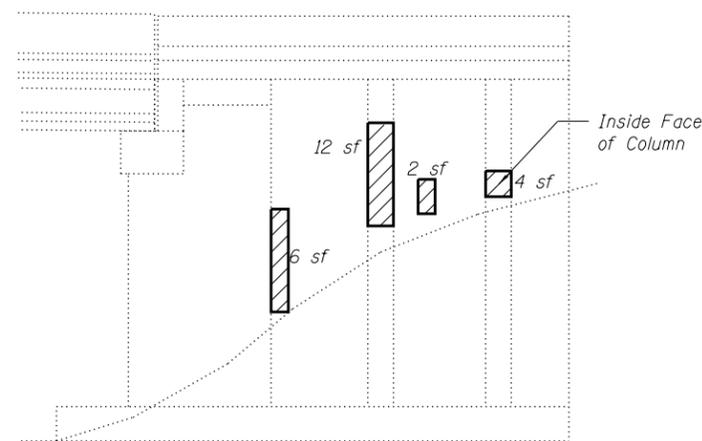
**SOUTH ABUTMENT ELEVATION**



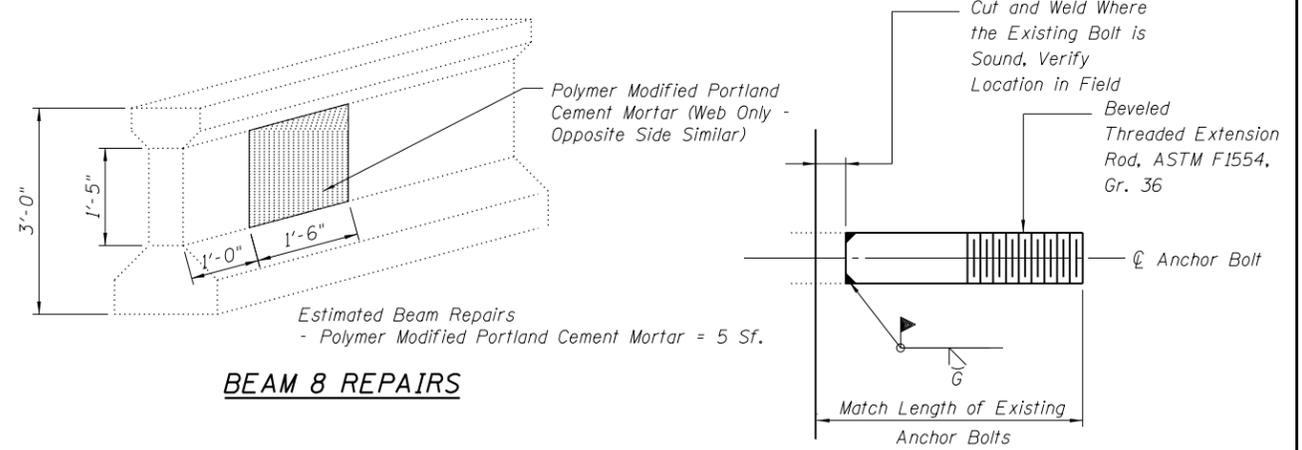
**SECTION A-A**



**SECTION B-B**

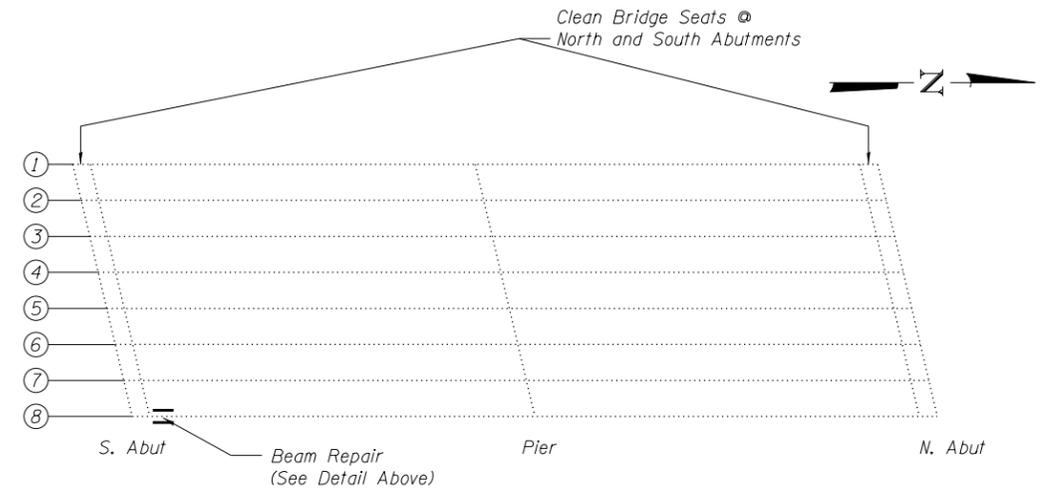


**SECTION C-C**

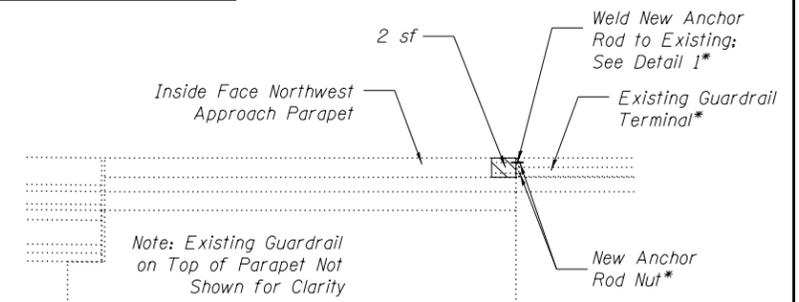


**BEAM 8 REPAIRS**

**DETAIL 1**



**EXISTING FRAMING PLAN**

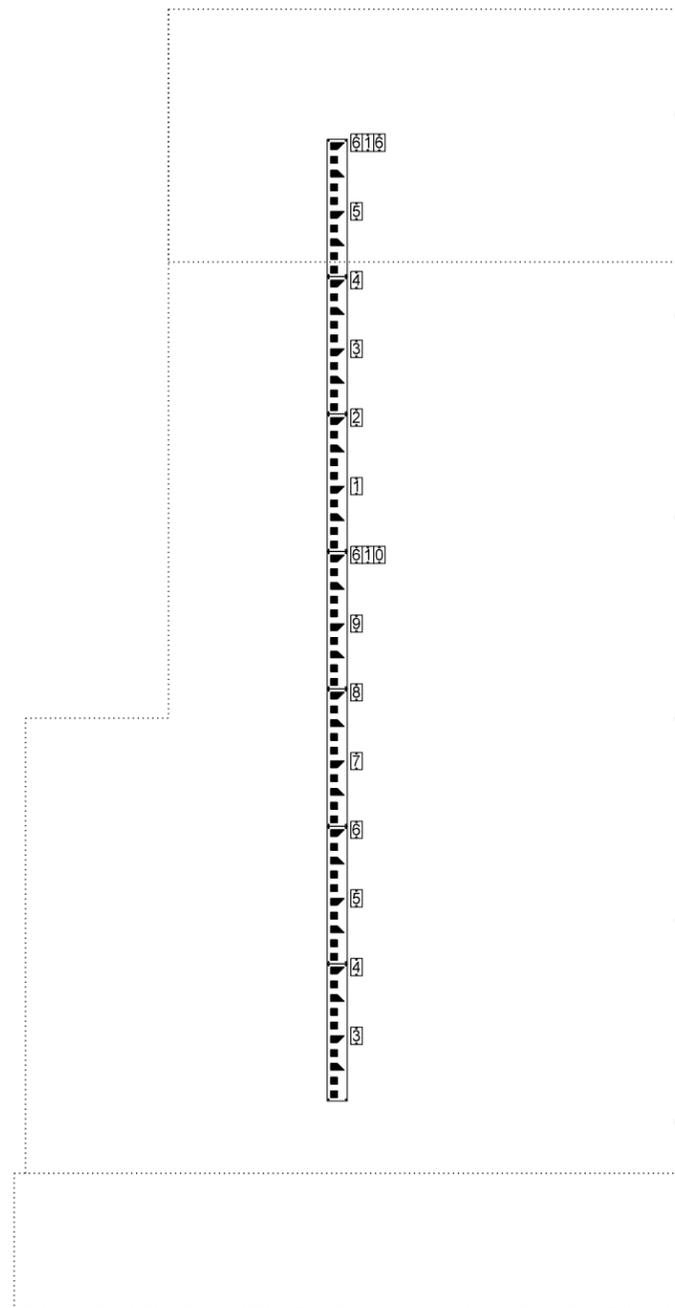


**SECTION D-D**

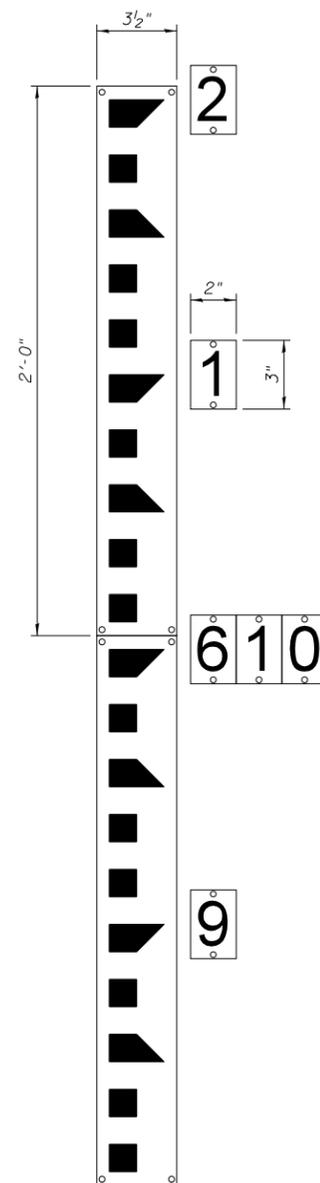
\*Cost Included in Pay Item "Furnish and Erect Structural Steel"

**BILL OF MATERIAL**

Legend	Item	Unit	Total
	Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	147
	Structural Repair of Concrete (Depth Greater than 5 inches)	Sq. Ft.	277
	Cleaning Bridge Seats	Sq. Ft.	294
	Polymer Modified Portland Cement Mortar	Sq. Ft.	5
	Concrete Sealer	Sq. Ft.	294
	Furnishing and Erecting Structural Steel	Pound	20



**PARTIAL ELEVATION OF PIER**  
(West end of South face)



**STREAM GAUGE DETAIL**

**BILL OF MATERIAL**

Legend	Item	Unit	Total
	Stream Gauge	Ea.	1

**NOTES**

- The gauge plates shall be porcelain enameled graduated in feet and tenths, unnumbered, and 3 1/2" wide. Gauge plates shall be "WaterMark " Style "E" or approved equivalent.
- Each individual number plate should a black numeral on a 2"x3" white porcelain enameled iron plate. Number plates shall be "WaterMark " Style "E" or approved equivalent.
- The gauge plates and number plates are to be fastened directly to the pier/abutment with 1/4 inch diameter, 1 1/4 inch long masonry screws with a hex washer head.
- Three digit elevations to be installed at the top of the gauge and at every elevation ending with 0. At all of the other whole elevations, place the last digit as shown.
- Cost included in Pay Item "Stream Gauge".

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

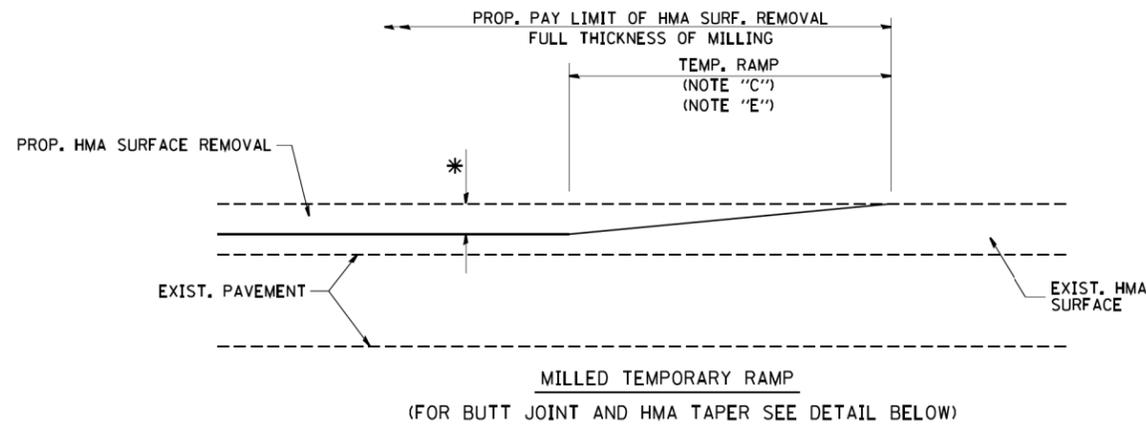
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

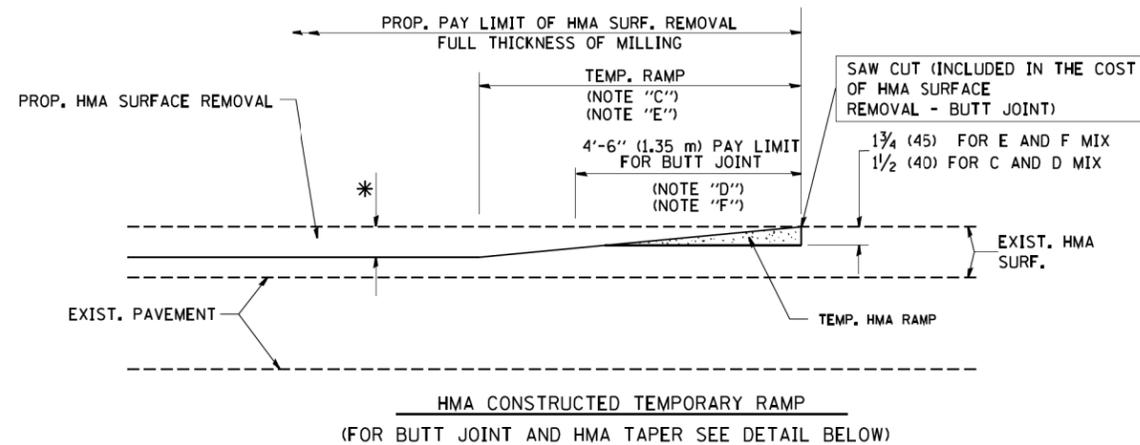
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivkosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01			<b>BD600-06 (BD-24)</b>		CONTRACT NO. 60R73			
	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		

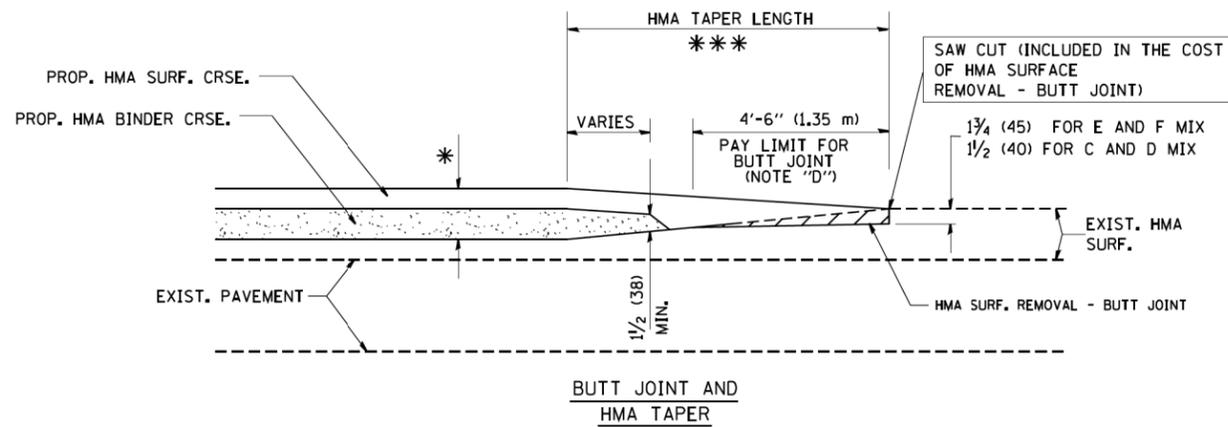


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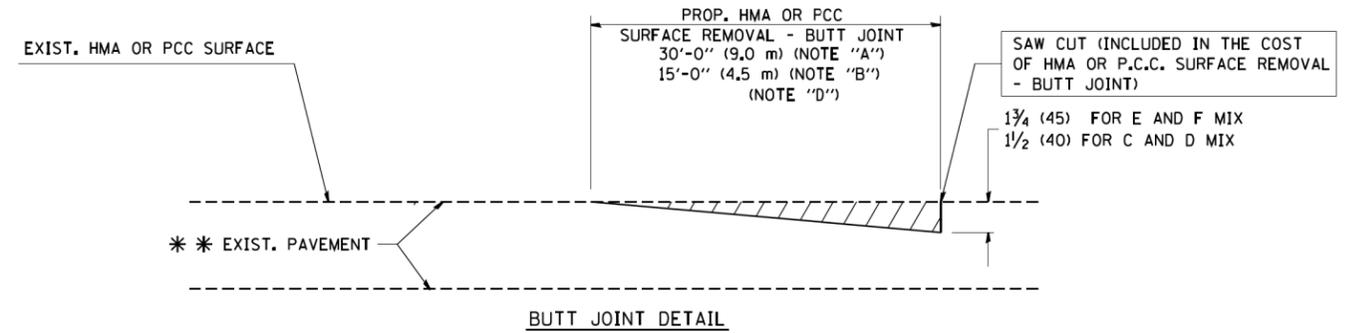


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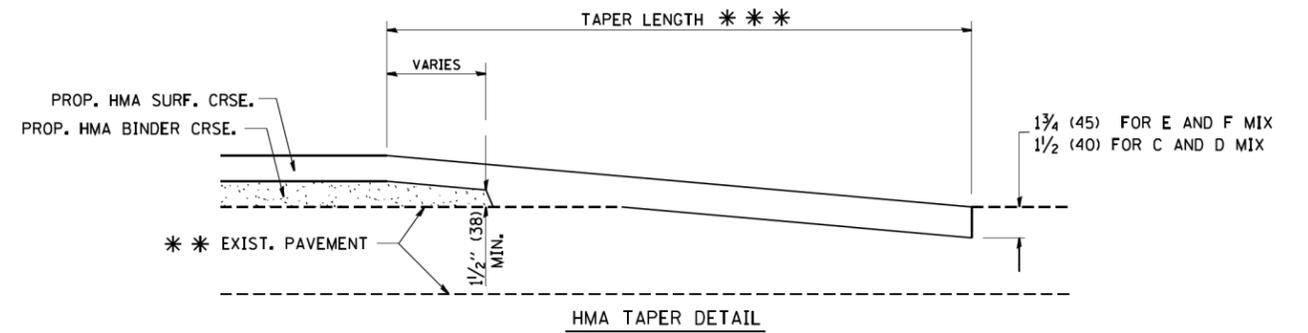
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

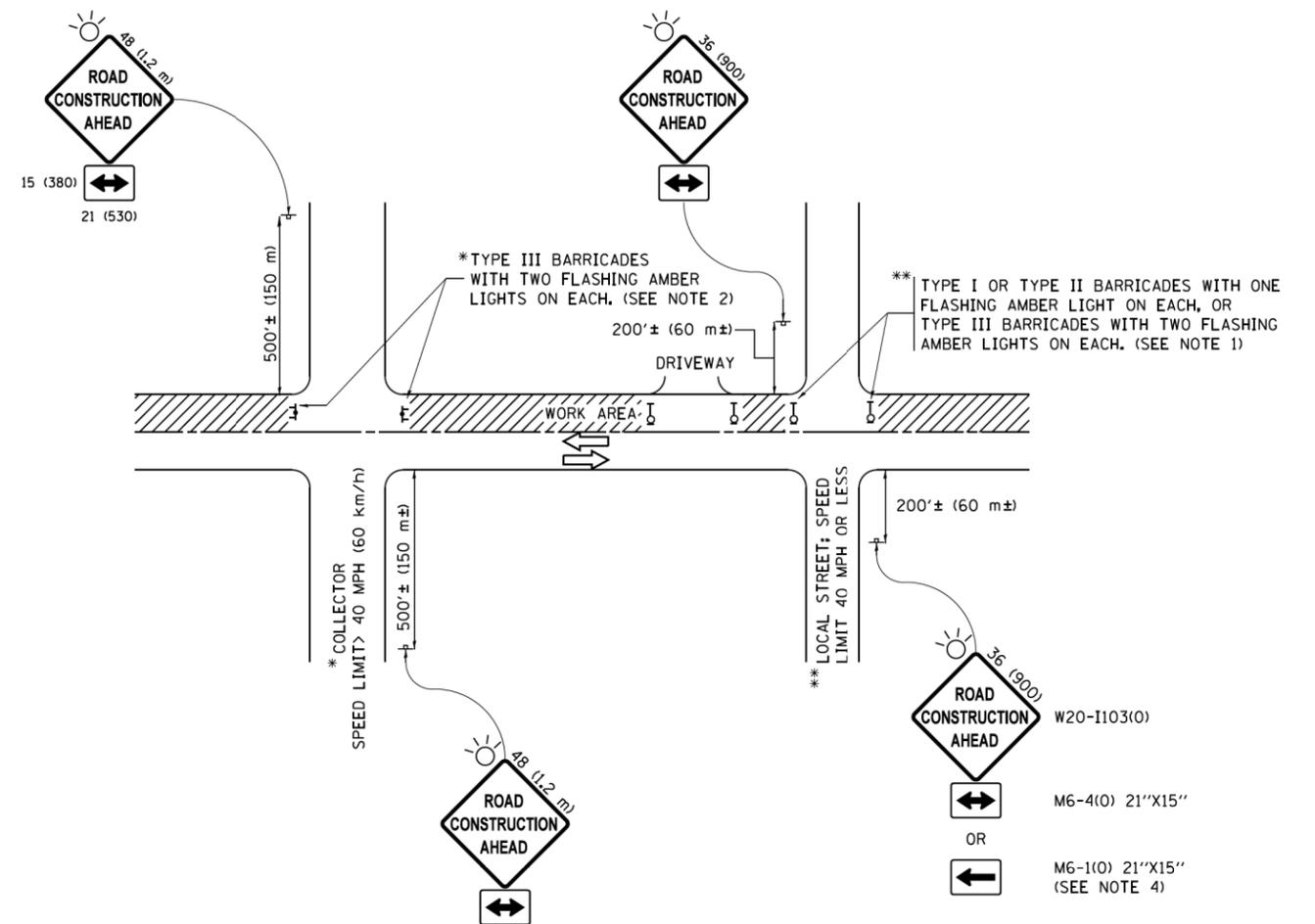
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0202-BR	COOK	27	23
BD400-05 BD32			CONTRACT NO. 60R73	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

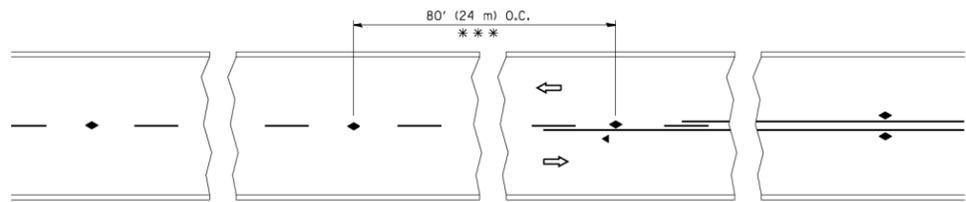
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	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

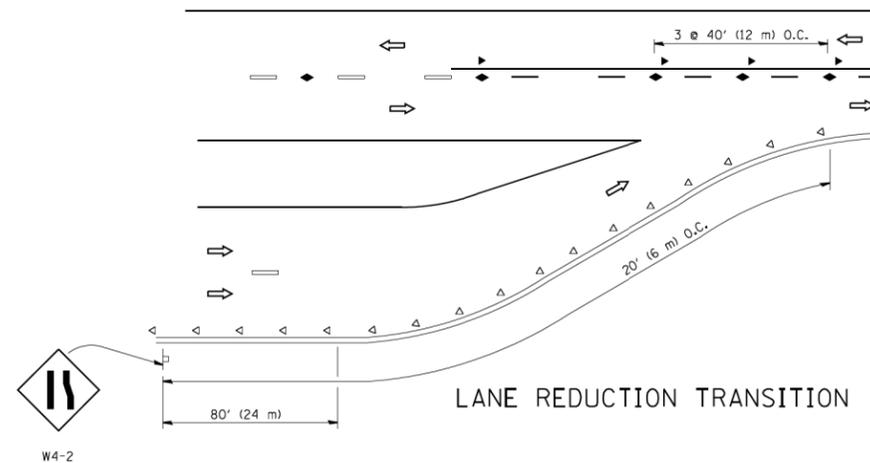
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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<b>TC-10</b>			<b>CONTRACT NO. 60R73</b>	
ILLINOIS FED. AID PROJECT				

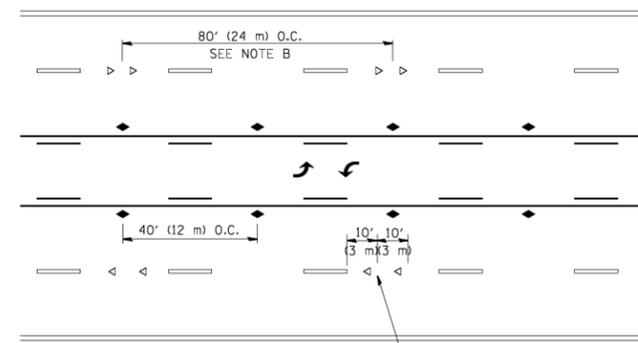


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

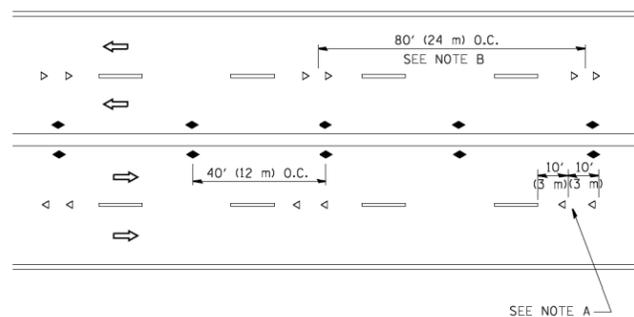
TWO-LANE/TWO-WAY



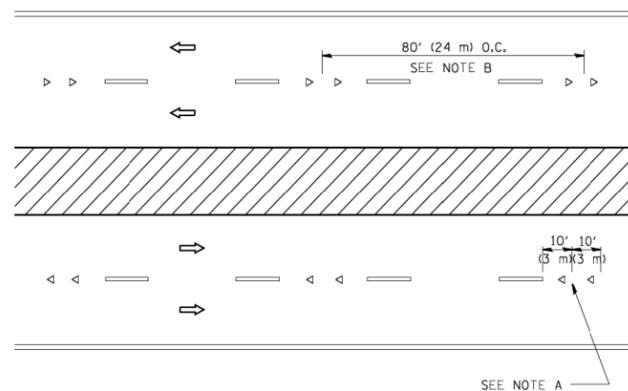
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

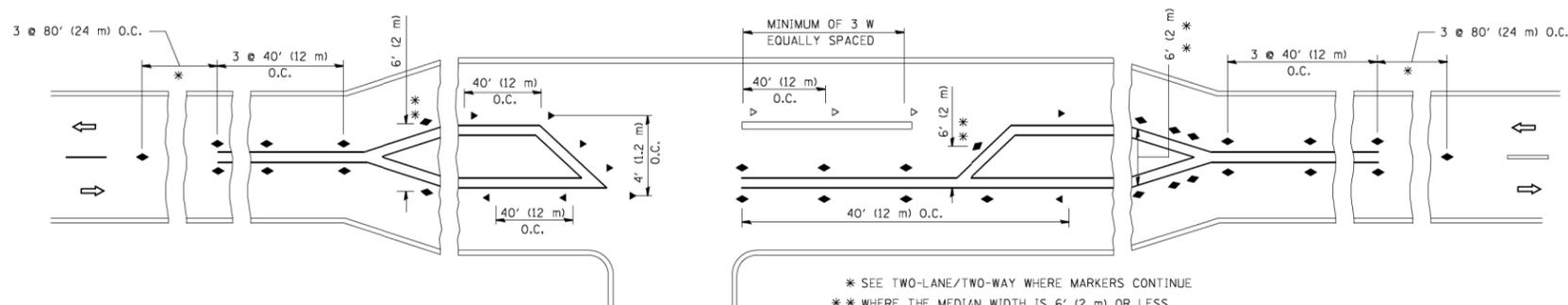
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

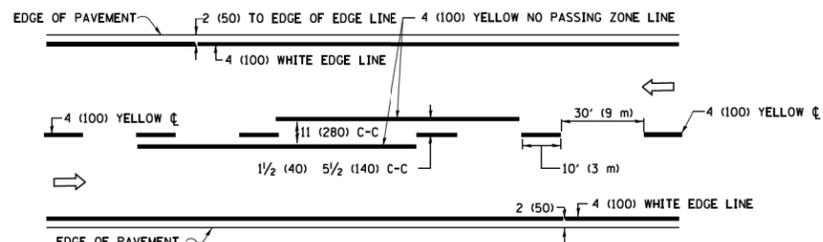
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT DATE = 3/2/2011	DATE -	REVISED - C. JUCIUS 09-09-09

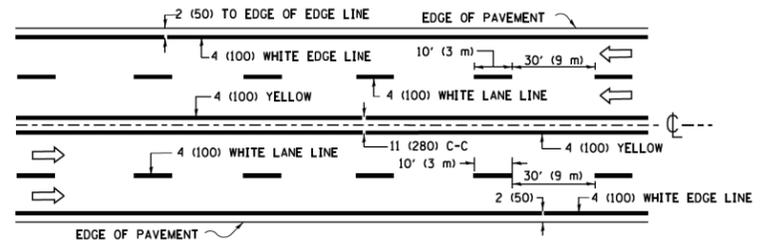
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS	
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

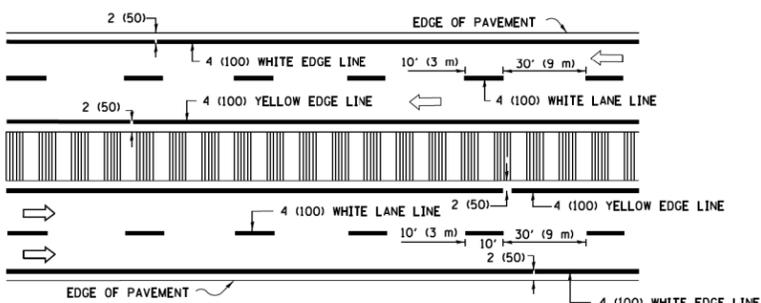
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0202-BR	COOK	27	25
TC-11		CONTRACT NO. 60R73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**2-LANE ROADWAY**

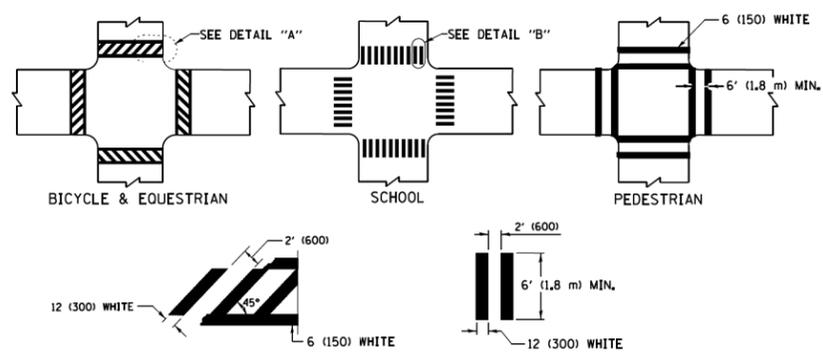


**MULTI-LANE UNDIVIDED**



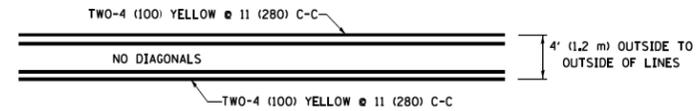
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

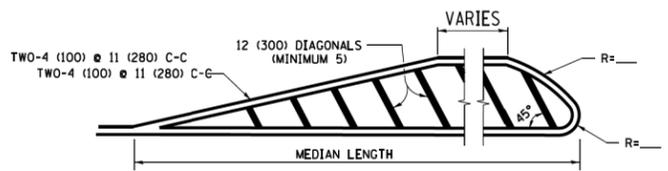


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

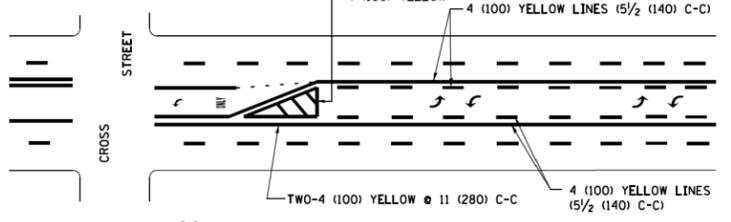


**4' (1.2 m) WIDE MEDIANS ONLY**



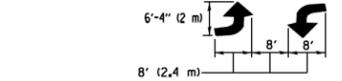
**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



**MEDIAN WITH TWO-WAY LEFT TURN LANE  
TYPICAL PAINTED MEDIAN MARKING**

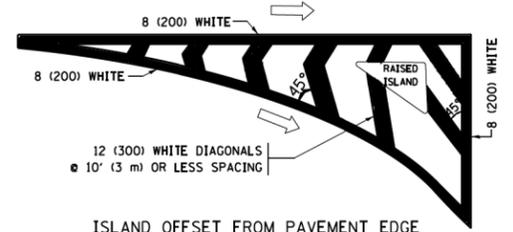
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



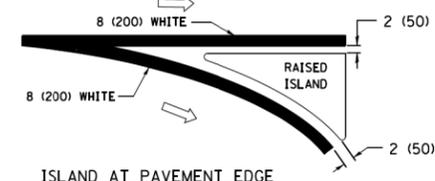
**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

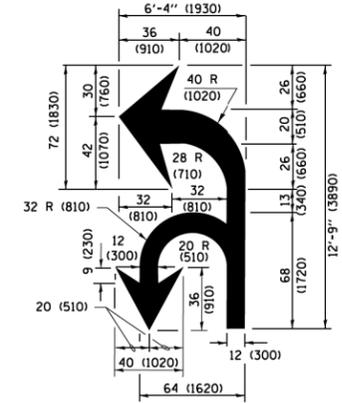
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



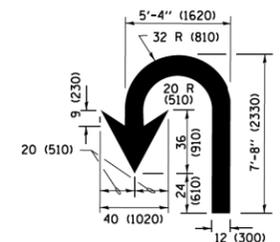
**ISLAND OFFSET FROM PAVEMENT EDGE**



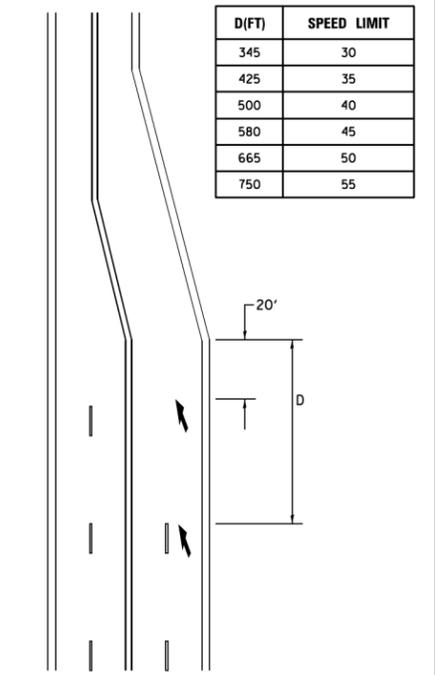
**ISLAND AT PAVEMENT EDGE  
TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**



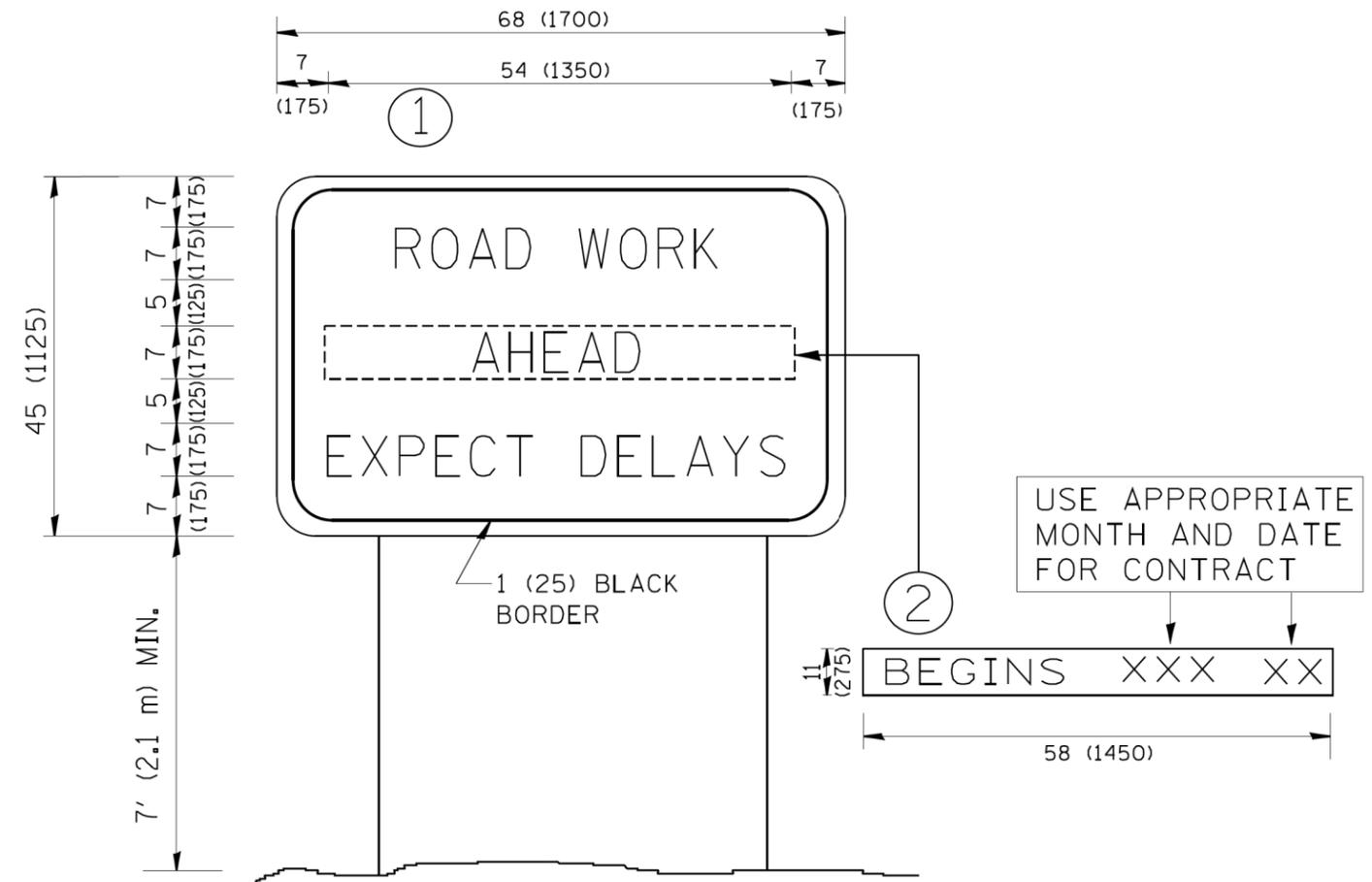
**LANE REDUCTION TRANSITION**  
\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "RR" = 3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X" = 54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gegl1enobt	DESIGNED - DRAWN -	REVISED - R. MIRS 09-15-97 REVISED - R. MIRS 12-11-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>		F.A.U. RTE. 372	SECTION 0202-BR	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 27	
PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	REVISED - C. JUCIUS 01-31-07		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	CONTRACT NO. 60R73	
PLOT DATE = 1/4/2008	DATE -				FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT							