

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-1-3	COOK	32	1
ILLINOIS		CONTRACT NO. 60M93		

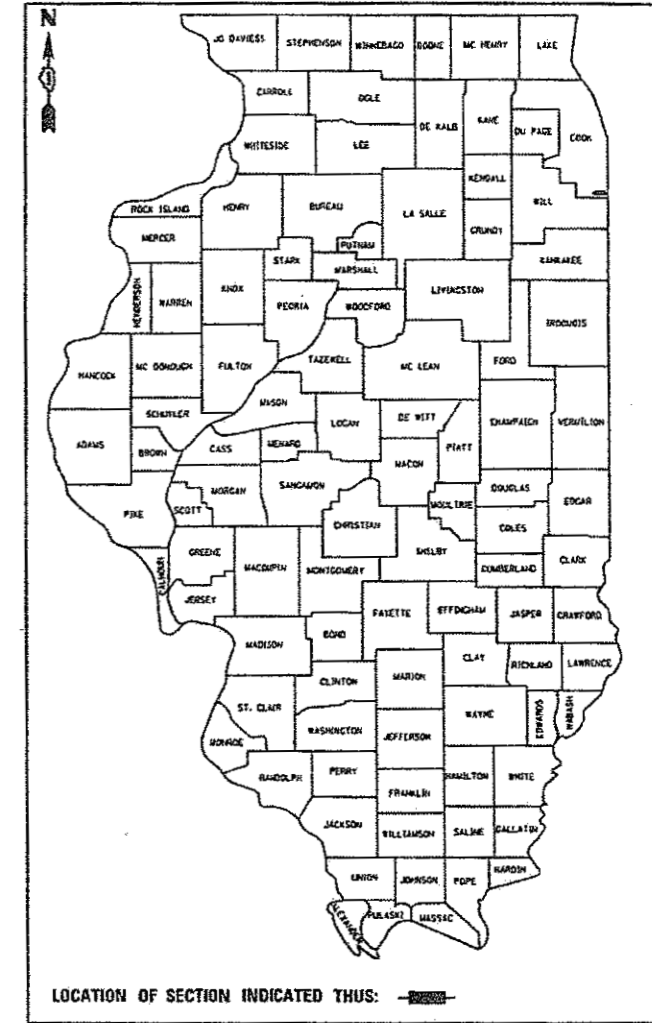
*32 + 2 = 34
D-91-199-11

FOR INDEX OF SHEETS, SEE SHEET NO. 2

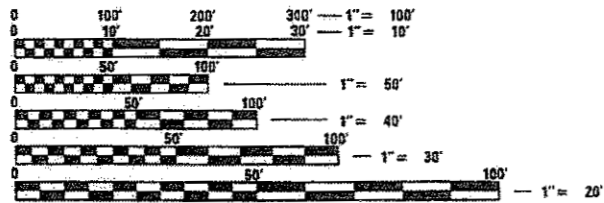
IMPROVEMENT IS LOCATED IN
THE VILLAGE OF FORD HEIGHTS

**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 353: US RTE. 30
OVER DEER CREEK
SECTION 11-B-1-3
FEDERAL PROJECT NUMBER: NHPP-GQ2P(886)
BRIDGE DECK OVERLAY, MEDIAN REPLACEMENT
SN: 016-0276
COOK COUNTY
C-91-199-11



DESIGN DESIGNATION: OTHER PRINCIPAL
FAP RTE. 353 (US RTE. 30):
2017 ADT=19,000
POSTED SPEED=40 MPH
DESIGN SPEED=45 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

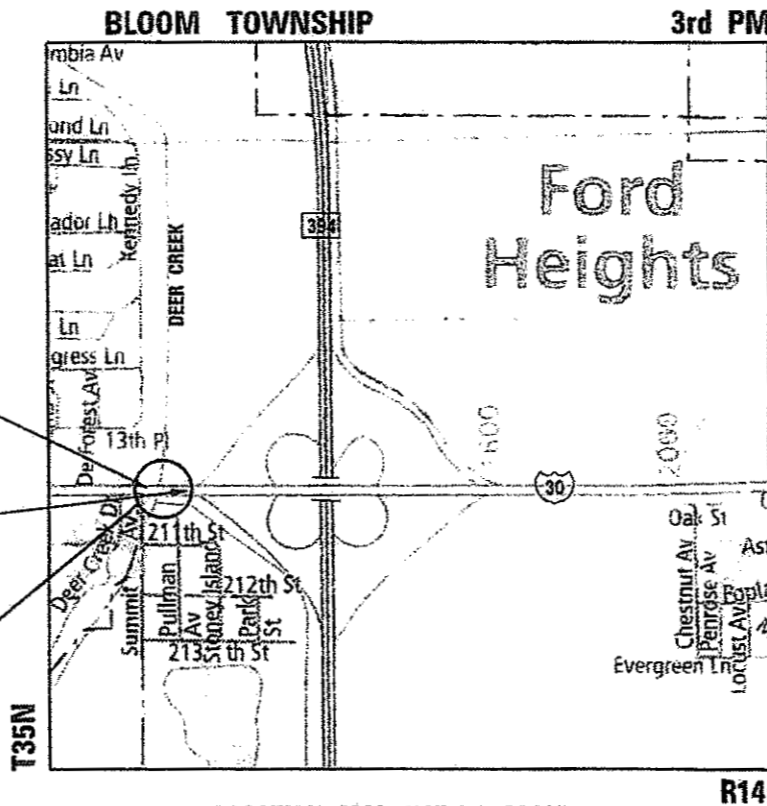
PROJECT ENGINEER: MS. RAGHAD ADEIS-DAHMAN, PE, SE (847) 705-4237
PROJECT MANAGER: MR. FAWAD AQUEEL, PE, PTOE (847) 705-4247

CONTRACT NO. 60M93

BEGIN
IMPROVEMENT
STA 263 + 49.50

END
IMPROVEMENT
STA 272 + 70

IMPROVEMENT LOCATION
US RTE. 30 OVER DEER CREEK
STRUCTURE NO. 016-0276



LOCATION MAP NOT TO SCALE
GROSS LENGTH = 920.5 FT = 0.17 MILES
NET LENGTH = 786.5 FT = 0.15 MILES

Accurate
GROUP, INC.
WWW.ACCGI.COM
101 SCHELTER RD., SUITE B-200
LINCOLNSHIRE, ILLINOIS 60069
T (847) 613-1100 F (847) 613-1110
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184.0020 3



DATE SIGNED: 03/08/2018
EXP. DATE: 11/30/2018



DATE SIGNED: 3/8/2018
EXP. DATE: 11/30/2018

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED March 22 20 18
[Signature] REGIONAL ENGINEER
May 11 20 18
[Signature] ENGINEER OF DESIGN AND ENVIRONMENT
May 11 20 18
[Signature] DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

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1	COVER SHEET
2	INDEX OF SHEETS, LIST OF STATE STANDARDS AND GENERAL NOTES
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7	TYPICAL SECTIONS
8	MEDIAN REMOVAL AND REPLACEMENT PLANS
9	MAINTENANCE OF TRAFFIC NOTES
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11-16	MAINTENANCE OF TRAFFIC PLANS
17-19	EROSION CONTROLS PLANS
20	PAVEMENT MARKING & LANDSCAPING PLANS
21-26A	STRUCTURAL DRAWINGS - SN 016-0276
27	BD-24 CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
28	TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
29	TC-11 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
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LIST OF ILLINOIS DOT HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
606306-04	CORRUGATED PC CONCRETE MEDIANS
701101-05	OFF ROAD OPERATIONS, MULTILANE, 15' (4.5M) TO 24" (600 MM) FROM PAVEMENT EDGE
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS- DAY ONLY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS \geq 45 MPH
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS \leq 40 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-07	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
782006	GUARDRAIL AND BARRIER WALL REFLECTORS MOUNTING DETAILS

COMMITMENTS

NONE

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "I.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES.
- MEADE ELECTRIC COMPANY, THE IDOT DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR, LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES. CALL 773-287-7672 FOR THE INITIAL LOCATE. REQUEST FOR LOCATES OF PREVIOUSLY MARKED FACILITIES MAY BE AT THE CONTRACTOR'S EXPENSE.
- TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE VILLAGE OF FORD HEIGHTS, OTHER LOCAL GOVERNMENT AGENCIES, AND IDOT.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO BEGINNING CONSTRUCTION AND ORDERING MATERIALS.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.
- THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) AND USE/WASTE REVIEW (BDE 2290) SUBMITTALS, THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION 11.G.1. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICES AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- PRIOR TO BEGINNING ANY WORK, THE CONTRACTOR SHALL RECORD AND RETAIN FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKINGS (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THE MARKINGS CAN BE RE-ESTABLISHED. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847)-705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS AREA TRAFFIC FIELD TECHNICIAN VIA EMAIL AT PATRICE.HARRIS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACING OF PERMANENT PAVEMENT MARKINGS.
- THE DEPARTMENT HAS DETERMINED THAT IN STREAM WORK IS NOT REQUIRED FOR THE WORK SPECIFIED IN THIS CONTRACT. THE DEPARTMENT HAS NOT OBTAINED A 404 PERMIT. IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING AN USACE 404 PERMIT, IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER USACE PERMITS.

FILE NAME = D:\Engineering\Live\Projects\13834 IDOT DLR Chas1\01\CADD Sheets\Civil\ID01B0M03-Sht-Index-DN-Hwy-Stds.dgn



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	DRAWN - JMT	REVISED -
PLOT SCALE = 2,000' = 1" = 1/4"	CHECKED - TGM	REVISED -
PLOT DATE = 4/11/2018	DATE - 03/16/2018	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, LIST OF STATE STANDARDS AND GENERAL NOTES
US RTE. 30 OVER DEER CREEK

SCALE:	SHEET OF SHEETS	STA. TO STA.	F.A.P. RTE. 353	SECTION 11-B-1-3	COUNTY COOK	TOTAL SHEETS 32	SHEET NO. 2
ILLINOIS FED. AID PROJECT							CONTRACT NO. 60M93

REV

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				URBAN	80/80 (FED/STATE) BRIDGE 0047
20101000	TEMPORARY FENCE	FOOT	164		164
20101700	SUPPLEMENTAL WATERING	UNIT	3		3
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	60		60
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	1		1
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	1		1
25200110	SODDING, SALT TOLERANT	SQ YD	60		60
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	1.2		1.2
28000400	PERIMETER EROSION BARRIER	FOOT	164		164
28000510	INLET FILTERS	EACH	6		6
28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	60		60
35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD	32		32
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	26		26
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	13		13
44000100	PAVEMENT REMOVAL	SQ YD	291		291

* SPECIALTY ITEM

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PLOT DATE = 3/20/2018	CHECKED - TGM	REVISED -
	DATE - 03/16/2018	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES - I
US RTE. 30 OVER DEER CREEK

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-I-3	COOK	32	3
			CONTRACT NO. 60M93	
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				URBAN	80/20 (FED STATE) BRIDGE 0047
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	116		116
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	80		80
44003100	MEDIAN REMOVAL	SQ FT	2280		2280
50102400	CONCRETE REMOVAL	CU YD	30.8		30.8
50300255	CONCRETE SUPERSTRUCTURE	CU YD	28.4		28.4
50300300	PROTECTIVE COAT	SQ YD	54		54
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	5420		5420
50800530	MECHANICAL SPLICERS	EACH	240		240
60260100	INLETS TO BE ADJUSTED	EACH	1		1
60605500	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (VARIABLE WIDTH GUTTER FLAG)	FOOT	80		80
60624300	CONCRETE BARRIER MEDIAN, 4 FT. (DOWELLED)	SQ FT	2280		2280
60624610	CORRUGATED MEDIAN (DOWELLED)	SQ FT	332		332
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6		6
67100100	MOBILIZATION	L SUM	1		1
70300904	PAVEMENT MARKING TAPE, TYPE IV 4"	FOOT	18170		18170
70300908	PAVEMENT MARKING TAPE, TYPE IV 8"	FOOT	2386		2386

* SPECIALTY ITEM

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	DATE - 03/16/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES-II
US RTE. 30 OVER DEER CREEK**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 11-B-I-3	COUNTY COOK	TOTAL SHEETS 32	SHEET NO. 4
CONTRACT NO. 60M93				ILLINOIS FED. AID PROJECT

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				URBAN	30/20 (FEN/STATE) BRIDGE 0047
70300912	PAVEMENT MARKING TAPE, TYPE IV 12"	FOOT	524		524
70400100	TEMPORARY CONCRETE BARRIER	FOOT	250		250
70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	2		2
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	5800		5800
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	387		387
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	98		98
* 78004110	PREFORMED PLASTIC PAVEMENT MARKING, TYPE C - LINE 4"	FOOT	60		60
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	424		424
* 78100300	REPLACEMENT REFLECTOR	EACH	134		134
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	20		20
X0326766	CLEAN & RESEAL RELIEF JOINT	FOOT	117		117
X0326806	WASHOUT BASIN	L SUM	1		1
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	2352		2352
X6060500	CORRUGATED MEDIAN REMOVAL	SQ FT	332		332
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1		1
X701015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1		1

* SPECIALTY ITEM

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	DATE - 03/16/2018	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES-III
US RTE. 30 OVER DEER CREEK

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-1-3	COOK	32	5
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

REV

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				URBAN	80/30 (FED/STATE) BRIDGE 0047
X7015005	CHANGEABLE MESSAGE SIGN	CAL DAY	42		42
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	3286		3286
X7040125	PINNING TEMPORARY CONCRETE BARRIER	EACH	54		54
X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	134		134
* X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	484		484
Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	6		6
Z0012102	CONCRETE BRIDGE DECK SCARIFICATION 3/8 INCH	SQ YD	597		597
Z0012193	BRIDGE DECK THIN POLYMER OVERLAY 3/8"	SQ YD	597		597
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	19		19
Z0013798	CONSTRUCTION LAYOUT	L SUM	1		1
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE 11)	SQ YD	50		50
Z0016200	DECK SLAB REPAIR (PARTIAL)	SQ YD	22		22
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	113.9		113.9
Z0062456	TEMPORARY PAVEMENT	SQ YD	291		291

14

* SPECIALTY ITEM

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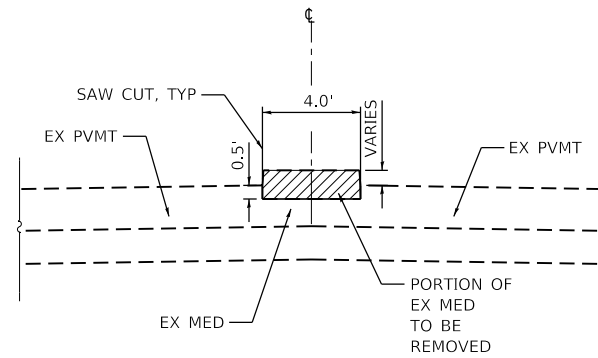


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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES-IV
US RTE. 30 OVER DEER CREEK

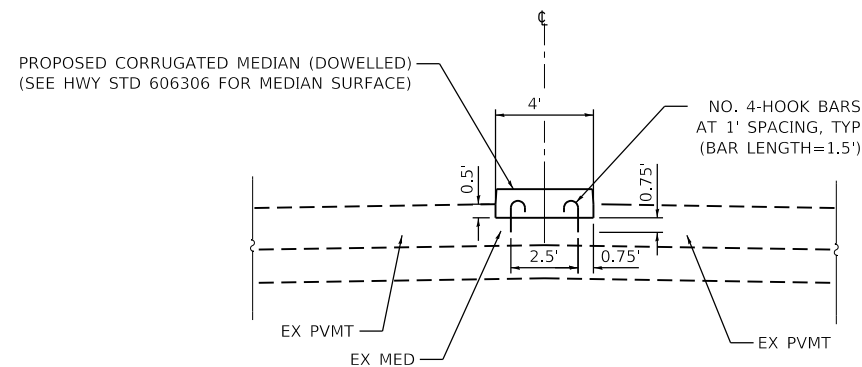
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-1-3	COOK	32	6
SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 60M93	
ILLINOIS FED. AID PROJECT				



**EXISTING US RTE. 30
MEDIAN REMOVAL**

STA 263+49.50 TO STA 264+60 (CORRUGATED MEDIAN)
STA 266+25 TO STA 268+25
STA 269+00 TO STA 272+70

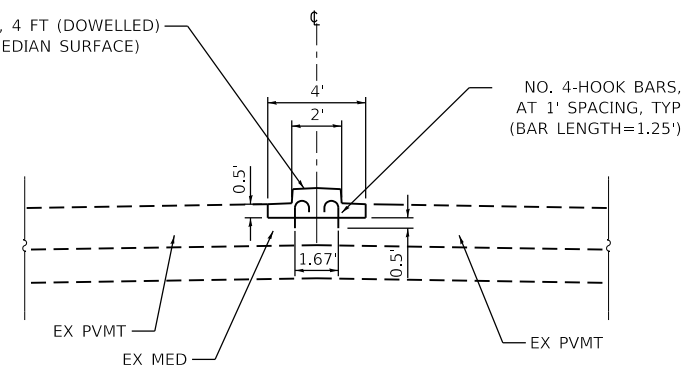
NOTE:
CONTRACTOR SHALL SAW CUT MEDIAN TO A DEPTH OF 6", AS DIRECTED BY THE ENGINEER.
THE SAW CUT SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF MEDIAN REMOVAL.
REFER TO HIGHWAY STANDARDS 606301 AND 606306 FOR MEDIAN DETAILS.



**PROPOSED US RTE. 30
CORRUGATED MEDIAN REPLACEMENT**

STA 263+49.50 TO STA 264+60 (CORRUGATED MEDIAN)

PROPOSED CONCRETE BARRIER MEDIAN, 4 FT (DOWELLED)
(SB-6.12) (SEE HWY STD 606301 FOR MEDIAN SURFACE)



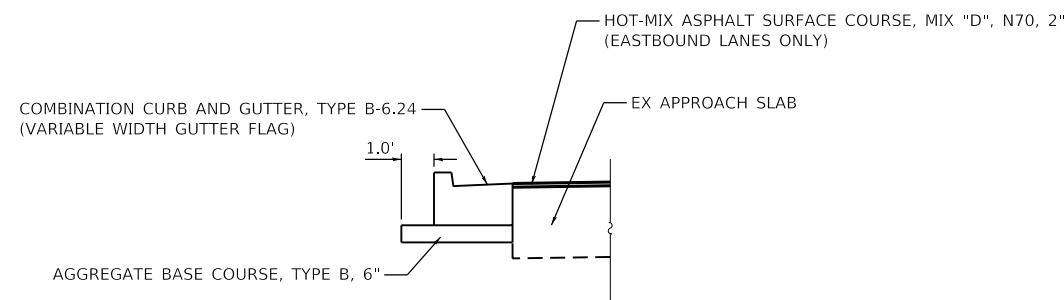
**PROPOSED US RTE. 30
MEDIAN REPLACEMENT**

STA 266+25 TO STA 268+25
STA 269+00 TO STA 272+70

HMA MIXTURE REQUIREMENTS CHART

OPERATION	MIXTURE TYPE	AIR VOIDS (%) @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
TEMPORARY PAVEMENT	HMA SURFACE COURSE, MIX "D", N70 (IL 9.5 mm), 2" HMA BINDER IL-19.0, 4"	4% @ 70 GYR 4% @ 70 GYR	QC/QA QC/QA
RESURFACING ON APPR. SLABS	HMA SURFACE COURSE, MIX "D", N70 (IL 9.5 mm), 2"	4% @ 70 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA)			

- NOTES:
- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN
 - THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR 76-22"; AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
 - FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
 - QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.
 - THE PCC TEMPORARY PAVEMENT OPTION SHALL CONSIST OF CLASS PV CONCRETE MEETING THE REQUIREMENTS OF SECTION 1020 OF THE STANDARD SPECIFICATIONS; PAVEMENT THICKNESS SHALL BE 6". TEMPORARY PCC PAVEMENT DOES NOT REQUIRE DOWEL BARS. A BOND BREAKER WILL BE REQUIRED IF THE PCC TEMPORARY PAVEMENT OPTION IS USED. THE COST OF THE BOND BREAKER IS INCLUDED WITH THE COST OF THE TEMPORARY PAVEMENT.



**CURB AND GUTTER DETAIL
AT APPROACH SLABS**

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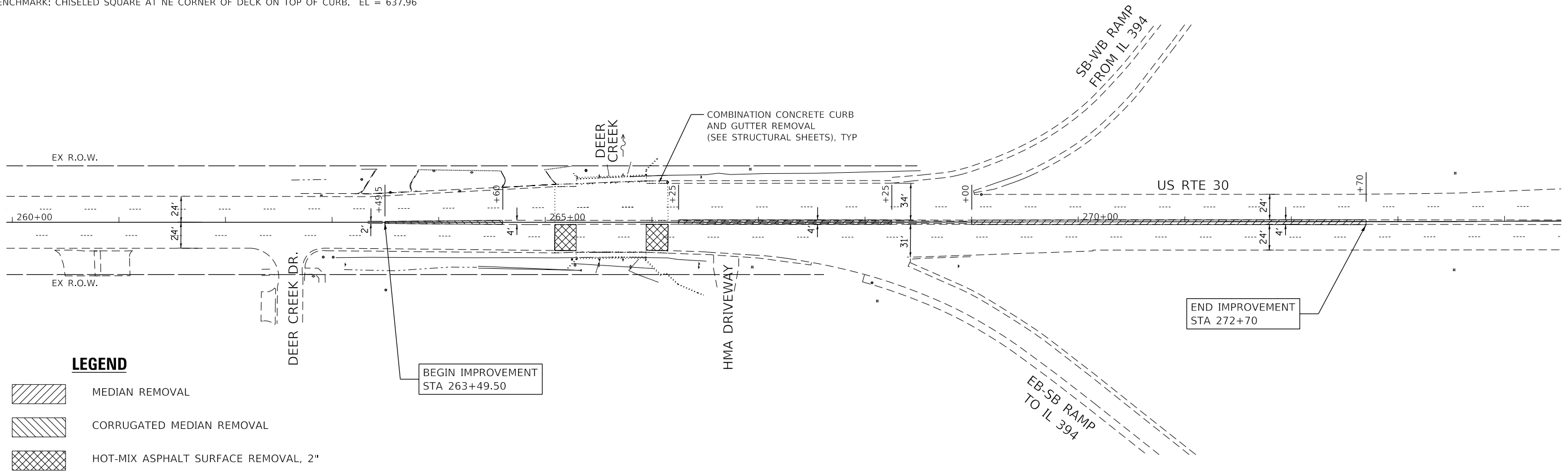
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	DRAWN - JMT	REVISED -
PLOT SCALE = 2.0000' / in.	CHECKED - TGM	REVISED -
PLOT DATE = 3/20/2018	DATE - 03/16/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS US RTE. 30 OVER DEER CREEK			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

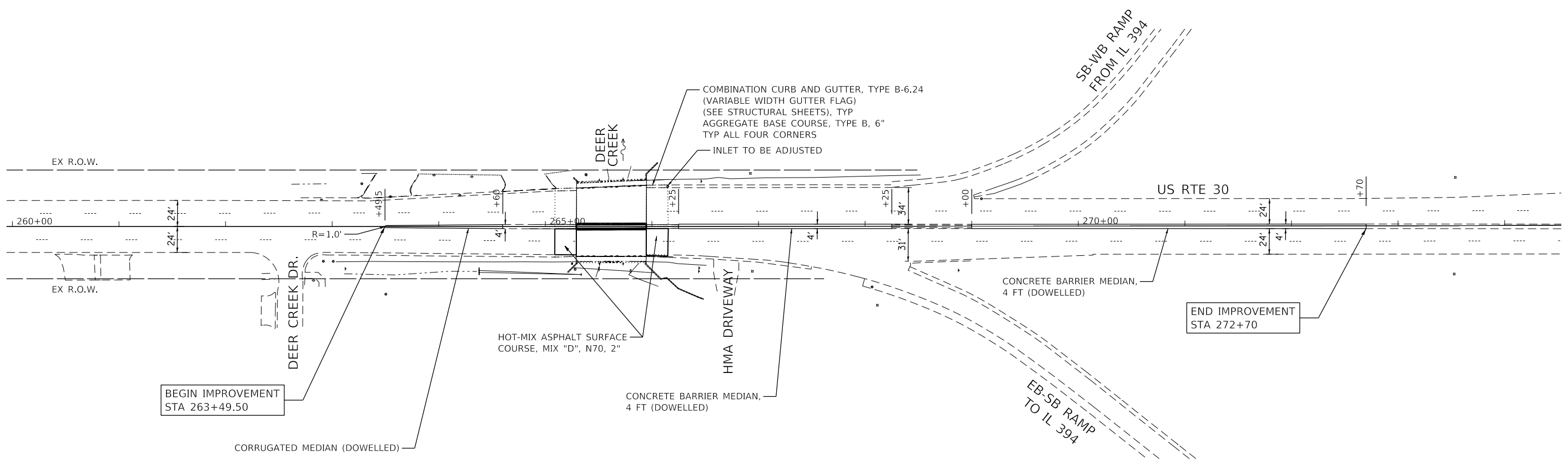
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-I-3	COOK	32	7
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

BENCHMARK: CHISELED SQUARE AT NE CORNER OF DECK ON TOP OF CURB. EL = 637.96



LEGEND

- MEDIAN REMOVAL
- CORRUGATED MEDIAN REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL, 2"



FILE NAME = D:\Engineering\LiveProjects\13034 IDOT DUR Chest\NO 9\CADD\CADD Sheets\Civil\DR160M93-Sht-Rem&Prop.dgn



USER NAME = jent	DESIGNED - JMT	REVISED -
	DRAWN - JMT	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED - TGM	REVISED -
PLOT DATE = 3/20/2018	DATE - 03/16/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MEDIAN REMOVAL AND REPLACEMENT PLANS			
US RTE. 30 OVER DEER CREEK			
SCALE: 1"=50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-I-3	COOK	32	8
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES:

1. THE PERMANENT TRAFFIC CONTROL DEPICTED HERE IN IS THE MINIMUM REQUIREMENT. ADDITIONAL TRAFFIC CONTROL DEVICES AS SPECIFIED IN THE HIGHWAY STANDARDS AS SHOWN IN THE INDEX OF SHEETS AND THE SPECIAL PROVISIONS SHALL BE PLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER. ALL TRAFFIC CONTROL DEVICES SHALL BE CONSIDERED INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL) UNLESS OTHERWISE INDICATED WITHIN THESE GENERAL NOTES, PLANS OR SPECIAL PROVISIONS.
2. TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL PROMPTLY RESPOND AT THE TIME OF NOTIFICATION BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC CONTROL DEVICES.
3. TYPE II BARRICADES, DRUMS OR VERTICAL BARRICADES SHALL BE PROVIDED AS SHOWN IN THE PLANS AND SPACED 50 FEET CENTER TO CENTER IN TANGENTS, 20 FEET CENTER TO CENTER IN TAPERS, AND 10 FEET CENTER TO CENTER IN RADII IN THE CONSTRUCTION WORK ZONE.
4. ALL EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL PLAN SHALL BE COVERED OR REMOVED IN ACCORDANCE WITH ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.
5. THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE ALL SIGNS AND SIGN SUPPORTS REQUIRED FOR TRAFFIC CONTROL AND PROTECTION.
6. ALL TEMPORARY PAVEMENT MARKINGS SHOWING DETERIORATION AFTER 7 DAYS OF SERVICE SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. SUFFICIENT QUANTITIES FOR THE INITIAL PLACEMENT AND A ONE-TIME REPLACEMENT HAVE BEEN PROVIDED FOR EACH STAGE. ALL MARKINGS THAT REQUIRE REPLACEMENT PRIOR TO 7 DAYS OF SERVICE OR REPLACEMENT AFTER THE INITIAL REPLACEMENT SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING LABOR, SIGNS AND TRAFFIC CONTROL DEVICES NECESSARY FOR THE MAINTENANCE OF TRAFFIC UNLESS NOTED OTHERWISE IN THE SPECIAL PROVISIONS.
8. ALL TRAFFIC CONTROL DEVICES SHALL BE REMOVED, COVERED OR TURNED AWAY FROM THE TRAFFIC IMMEDIATELY WHEN THEY ARE NO LONGER NECESSARY. WHEN A SIGN IS COVERED, ITS POST SHALL HAVE A REFLECTIVE 3" x 6" DELINEATOR INSTALLED. COST OF THE DELINEATOR IS INCLUDED IN TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
9. WORK ZONE SPEED LIMIT SHALL BE 35 MPH ON US ROUTE 30.
10. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN TRAFFIC IN ACCORDANCE WITH THE TRAFFIC CONTROL PLANS, SPECIAL PROVISIONS, APPLICABLE STATE STANDARDS, AND AS DIRECTED BY THE ENGINEER. ANY CHANGES TO THE TRAFFIC CONTROL SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO IMPLEMENTING ANY CHANGES.
11. THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY PROPOSED CHANGE TO THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN.
12. THE CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY DRAINAGE AND EROSION CONTROL PROTECTION DURING ALL PHASES OF CONSTRUCTION.
13. THE CONTRACTOR SHALL PLACE ONE (1) CHANGEABLE MESSAGE SIGN AT EACH END OF THE PROJECT AND/OR AS DIRECTED BY THE ENGINEER TO INFORM MOTORISTS OF UPCOMING CONSTRUCTION ACTIVITIES. THE MESSAGE SIGNS WITH APPROPRIATE INFORMATION SHALL BE PLACED TWO WEEKS BEFORE START OF CONSTRUCTION ACTIVITY. THIS WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR DAY, "CHANGEABLE MESSAGE SIGN".
14. THE CONTRACTOR SHALL MAINTAIN A MINIMUM OF ONE 12' THRU-LANE IN EACH DIRECTION FOR TWO-WAY TRAFFIC FLOW UNLESS OTHERWISE SPECIFIED.
15. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
16. PLACE TEMPORARY DRIVEWAY ENTRANCE SIGNING IN ACCORDANCE WITH IDOT DISTRICT ONE HIGHWAY STANDARD TC-26 - DRIVEWAY ENTRANCE SIGNING.
17. THE CONTRACTOR SHALL BE REQUIRED TO REMOVE ALL EXISTING PAVEMENT MARKINGS WHICH CONFLICT WITH THE DESIGNATED TRAFFIC CONTROL PLAN.
18. TEMPORARY CONCRETE BARRIER AND TEMPORARY IMPACT ATTENUATORS SHALL BE PLACED AS INDICATED IN THE PLANS. FURNISHING, INSTALLING AND RELOCATING TEMPORARY CONCRETE BARRIER AND TEMPORARY IMPACT ATTENUATORS SHALL BE IN ACCORDANCE WITH IDOT SPECIAL PROVISIONS, IDOT HIGHWAY STANDARDS, STANDARD SPECIFICATIONS, AND AS DIRECTED BY THE ENGINEER.
19. IMMEDIATELY AFTER THE COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL RESTORE ALL PERMANENT PAVEMENT MARKINGS, SIGNS, AND OTHER TRAFFIC CONTROL DEVICES THAT WERE COVERED, REMOVED, DAMAGED, OR OTHERWISE AFFECTED BY CONSTRUCTION.

SUGGESTED SEQUENCE OF CONSTRUCTION AND MAINTENANCE OF TRAFFIC

THE FOLLOWING SEQUENCE OF CONSTRUCTION AND MAINTENANCE OF TRAFFIC IS SUGGESTED. VARIATIONS MAY BE MADE WITH THE APPROVAL OF THE ENGINEER.

FOR EACH STAGE OF CONSTRUCTION, PROVIDE TRAFFIC CONTROL AS SHOWN ON THE SUGGESTED MAINTENANCE OF TRAFFIC PLANS. COORDINATE INSTALLATION OF TEMPORARY PAVEMENT MARKINGS AND OTHER TRAFFIC CONTROL DEVICES WITH THE EXISTING TRAFFIC PATTERNS AT THE ENDS OF THE PROJECT.

PRE-STAGE I

1. INSTALL EROSION CONTROL DEVICES AS PER EROSION CONTROL PLANS.
2. USING DAILY LANE CLOSURES, REMOVE THE EXISTING CORRUGATED MEDIAN WEST OF THE BRIDGE AND THE EXISTING CONCRETE MEDIAN EAST OF THE BRIDGE.
3. USING DAILY LANE CLOSURES, CONSTRUCT TEMPORARY PAVEMENT AT THE LOCATIONS OF THE MEDIAN REMOVAL.

STAGE I

1. INSTALL STAGE I TRAFFIC CONTROL ON US ROUTE 30 INCLUDING WIDTH RESTRICTION SIGNAGE AND IL 394 CONSTRUCTION SIGNAGE.
2. SHIFT TRAFFIC TO THE NORTH SIDE OF THE EXISTING ROADWAY UTILIZING THE WESTBOUND LANES AND TEMPORARY PAVEMENT ON US ROUTE 30 (MINIMUM ONE 12' THRU LANE IN EACH DIRECTION).
3. REMOVE AND REPLACE THE MEDIAN ON THE BRIDGE, CLOSING THE 1" LONGITUDINAL JOINT.
4. PERFORM THE BRIDGE DECK REPAIRS, SCARIFICATION AND OVERLAY OF THE EASTBOUND LANES INCLUDING THE APPROACH SLABS.

STAGE II

1. REMOVE STAGE I TRAFFIC CONTROL AND PLACE STAGE II TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENT MARKINGS PER MAINTENANCE OF TRAFFIC PLANS.
2. SHIFT TRAFFIC TO THE SOUTH ON THE EASTBOUND LANES OF US ROUTE 30 (MINIMUM ONE 12' THRU LANE IN EACH DIRECTION).
3. PERFORM THE BRIDGE DECK REPAIRS, SCARIFICATION AND OVERLAY OF THE WESTBOUND LANES INCLUDING THE APPROACH SLABS.

STAGE III

1. REMOVE STAGE II TRAFFIC CONTROL AND FULLY REOPEN THE ROAD TO TRAFFIC. USE TEMPORARY PAVEMENT MARKING AS REQUIRED.
2. USING DAILY LANE CLOSURES, REMOVE THE TEMPORARY PAVEMENT CONSTRUCTED IN PRE-STAGE.
3. USING DAILY LANE CLOSURES, RECONSTRUCT THE CORRUGATED MEDIAN WEST OF THE BRIDGE AND THE CONCRETE MEDIAN EAST OF THE BRIDGE.
4. INSTALL PERMANENT PAVEMENT MARKING AND REMOVE EROSION CONTROL DEVICES AND REMAINING TRAFFIC CONTROL DEVICES.

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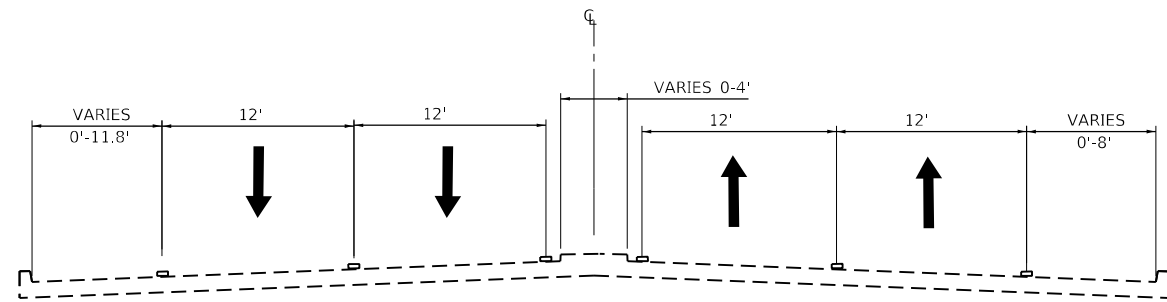
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PLOT DATE = 3/20/2018	DATE - 03/16/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE II
US RTE. 30 OVER DEER CREEK**

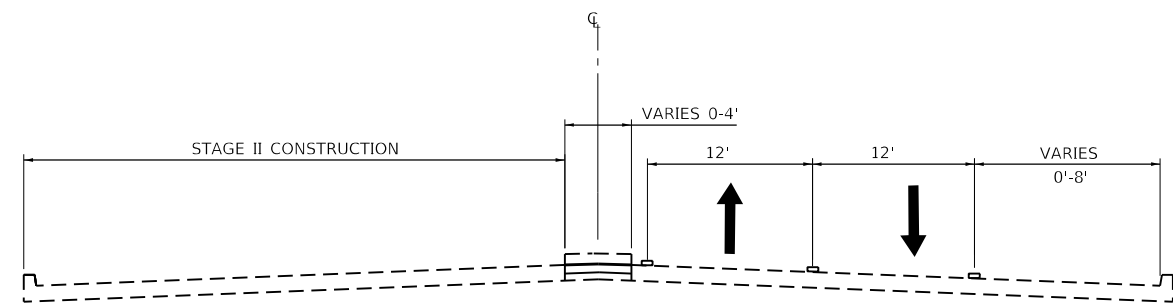
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-I-3	COOK	32	9
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				



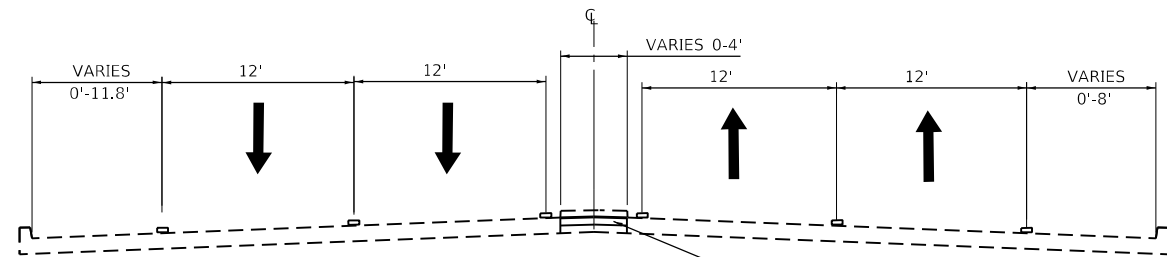
US ROUTE 30 EXISTING TYPICAL SECTION

STA 257+00 TO STA 275+20



US ROUTE 30 - STAGE II

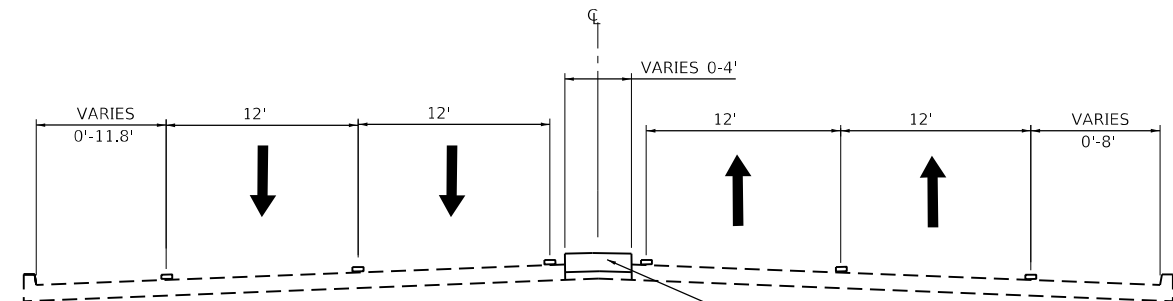
STA 258+80 TO STA 275+20



US ROUTE 30 - PRE STAGE

STA 263+49.50 TO STA 275+20

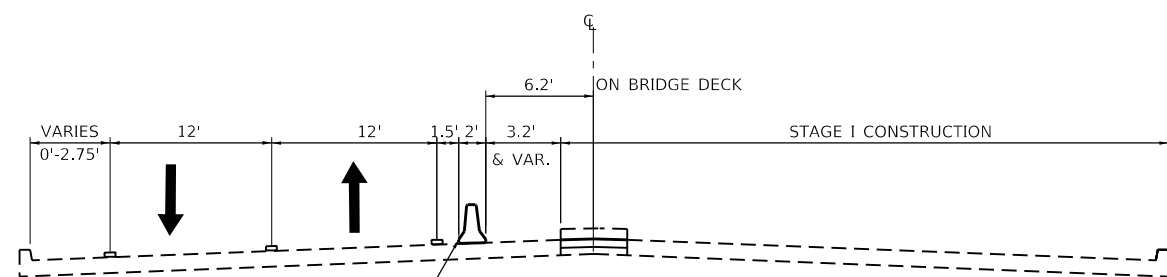
TEMPORARY PAVEMENT
(HMA SURFACE COURSE, MIX "D", N70 (IL 9.5 MM), 2"
HMA BINDER IL-19.0, 4")
STA 263+50 TO STA 264+60
STA 266+25 TO STA 268+25
STA 269+00 TO STA 272+70



US ROUTE 30 - STAGE III

STA 263+49.50 TO STA 275+20

CORRUGATED MEDIAN
STA 263+50 TO STA 264+60
CONCRETE BARRIER MEDIAN
4 FT (DOWELLED)
STA 266+25 TO STA 268+25
STA 269+00 TO STA 272+70



US ROUTE 30 - STAGE I

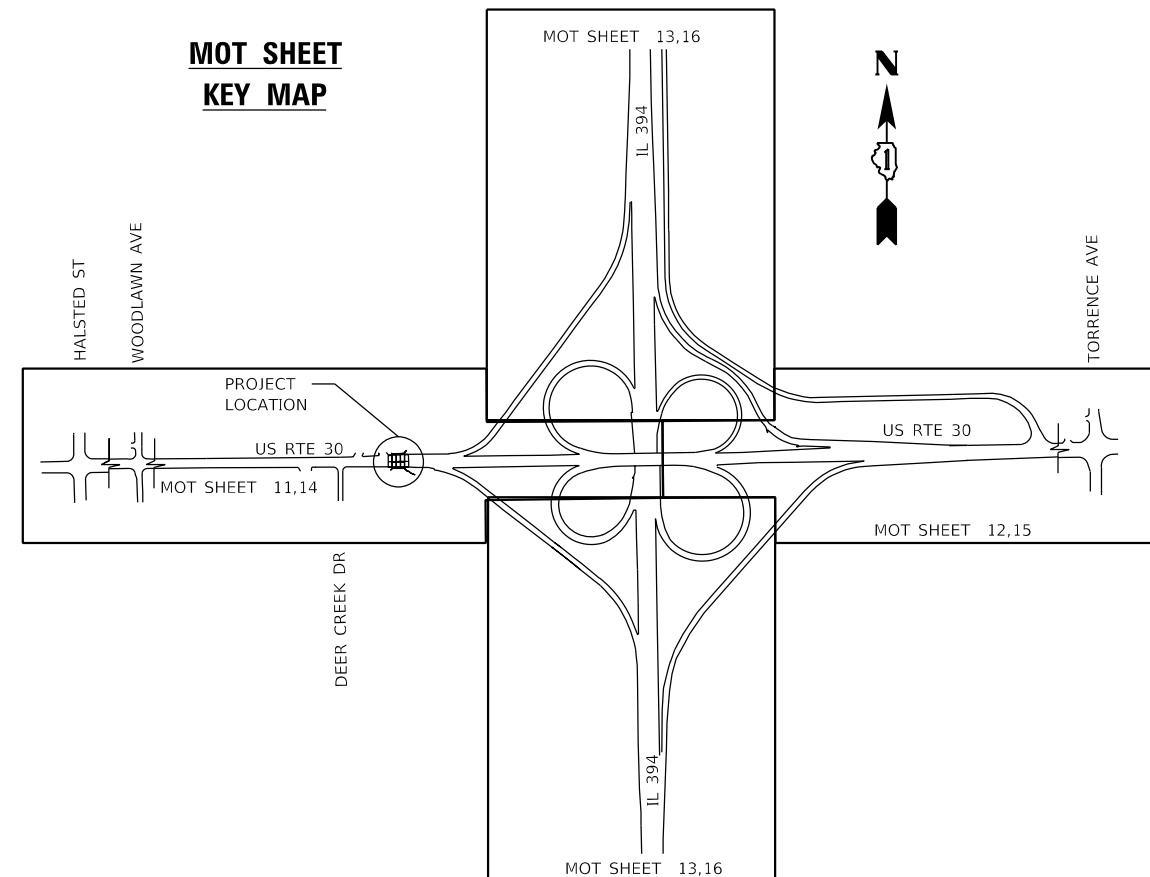
STA 256+90 TO STA 275+20

TEMPORARY CONCRETE BARRIER
STA 263+50 TO STA 266+00
CONTINUOUSLY PINNED TO PAVEMENT
OMIT PINNING ON BRIDGE DECK AND
APPROACH SLAB

LEGEND

- TEMPORARY PAVEMENT MARKING LINE
- TEMPORARY CONCRETE BARRIER WALL WITH BARRIER WALL REFLECTORS, TYPE C
- DIRECTION OF TRAFFIC

**MOT SHEET
KEY MAP**



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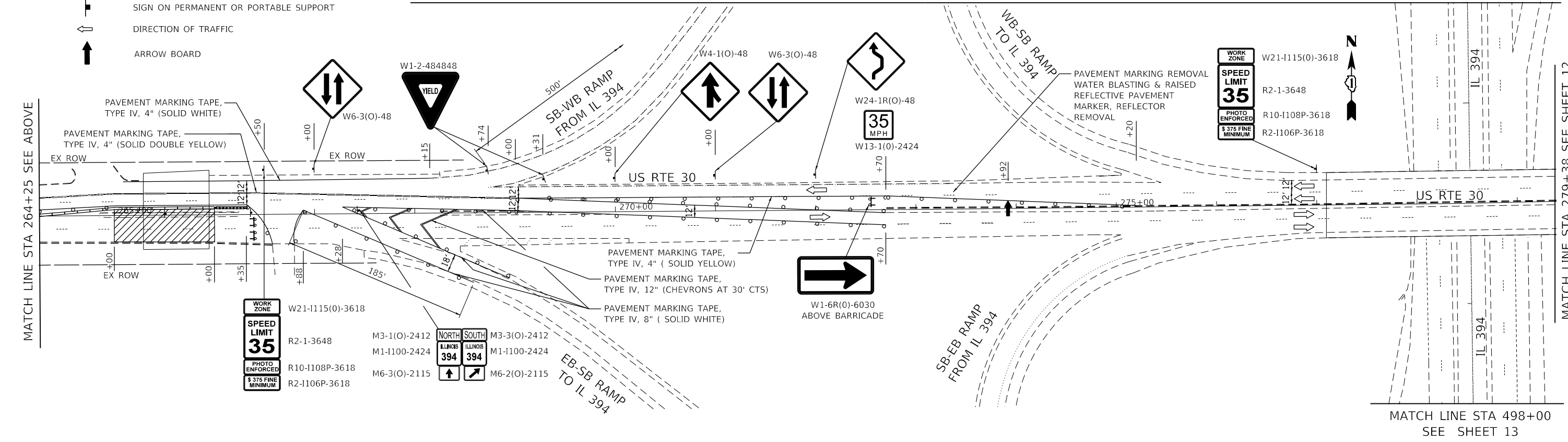
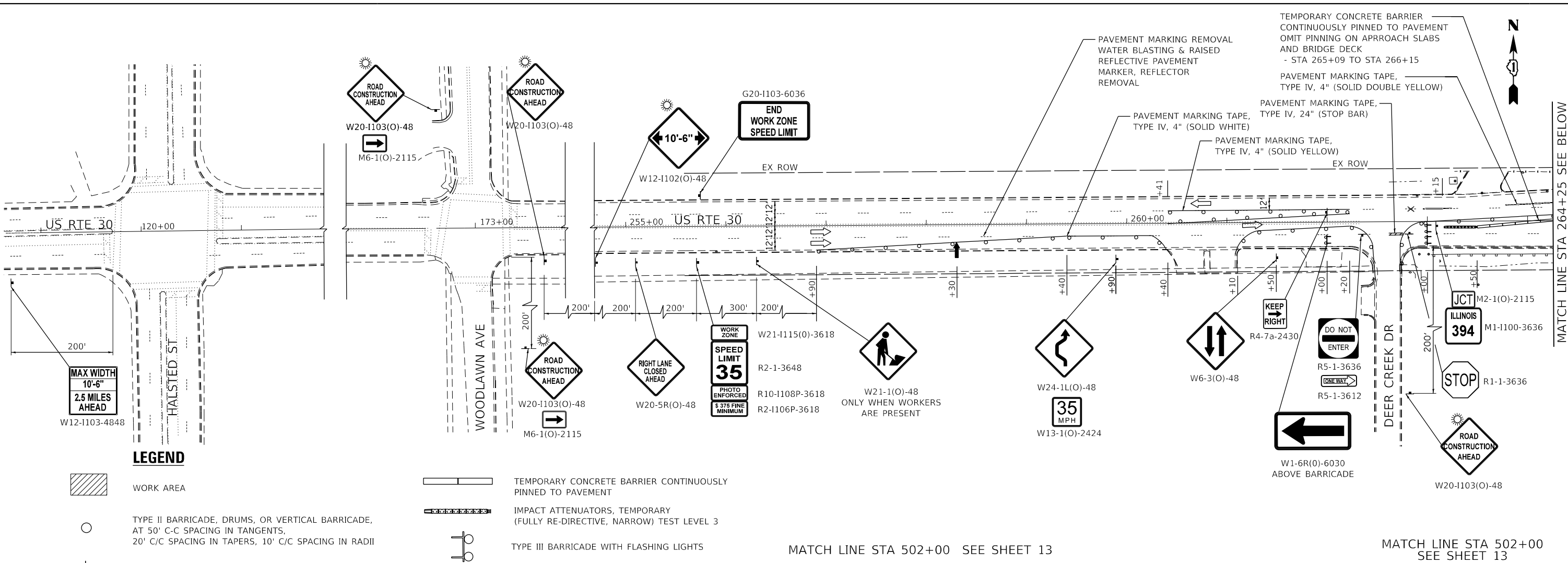
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC TYPICAL SECTIONS
US RTE. 30 OVER DEER CREEK**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-I-3	COOK	32	10
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

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LEGEND

- WORK AREA
- TYPE II BARRICADE, DRUMS, OR VERTICAL BARRICADE, AT 50' C-C SPACING IN TANGENTS, 20' C/C SPACING IN TAPERS, 10' C/C SPACING IN RADII
- SIGN ON PERMANENT OR PORTABLE SUPPORT
- DIRECTION OF TRAFFIC
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER CONTINUOUSLY PINNED TO PAVEMENT
- IMPACT ATTENUATORS, TEMPORARY (FULLY RE-DIRECTIVE, NARROW) TEST LEVEL 3
- TYPE III BARRICADE WITH FLASHING LIGHTS



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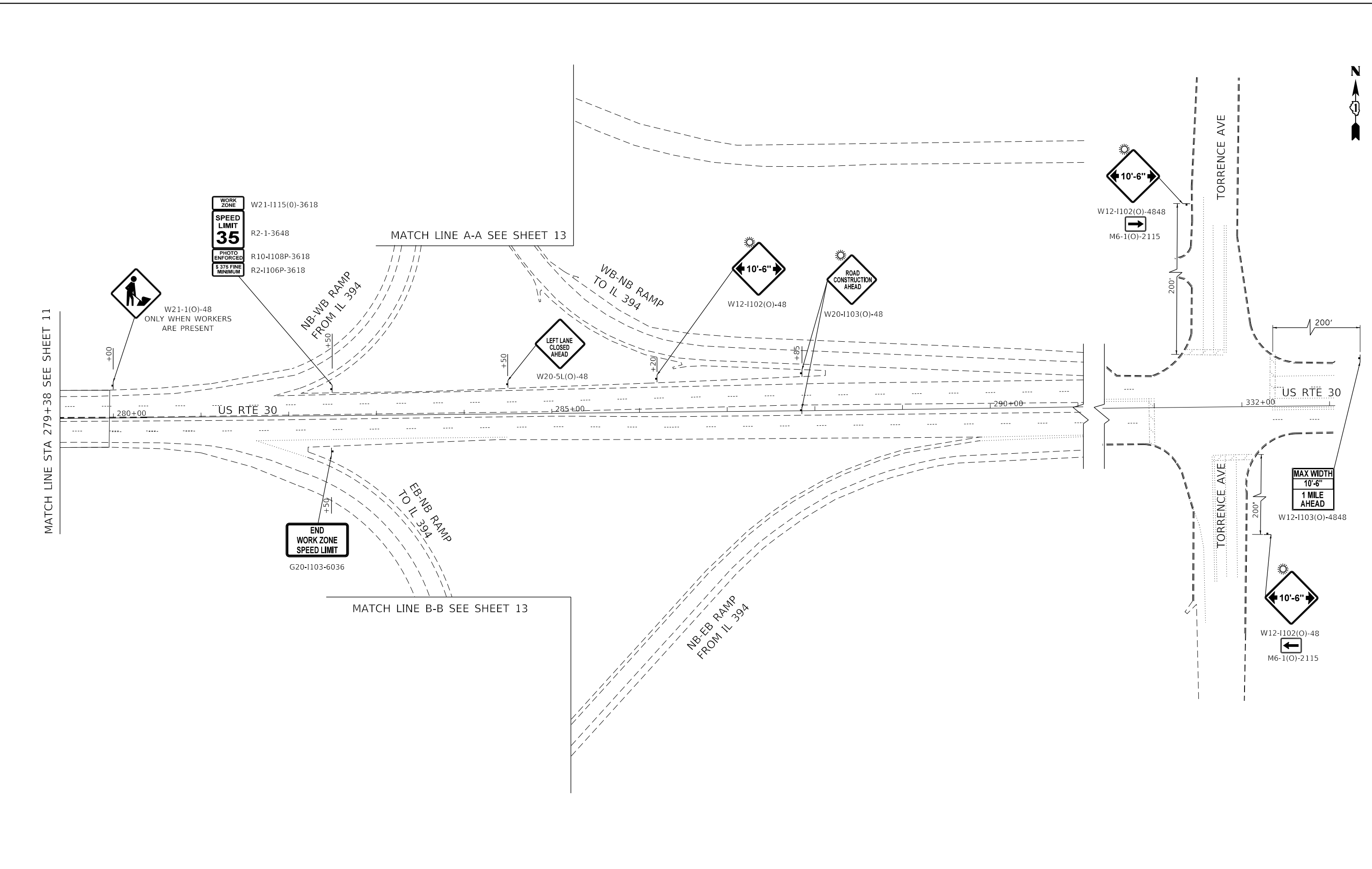
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DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE I
US RTE. 30 OVER DEER CREEK**

SCALE: 1" = 50' SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 11-B-I-3	COUNTY COOK	TOTAL SHEETS 31	SHEET NO. 11
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

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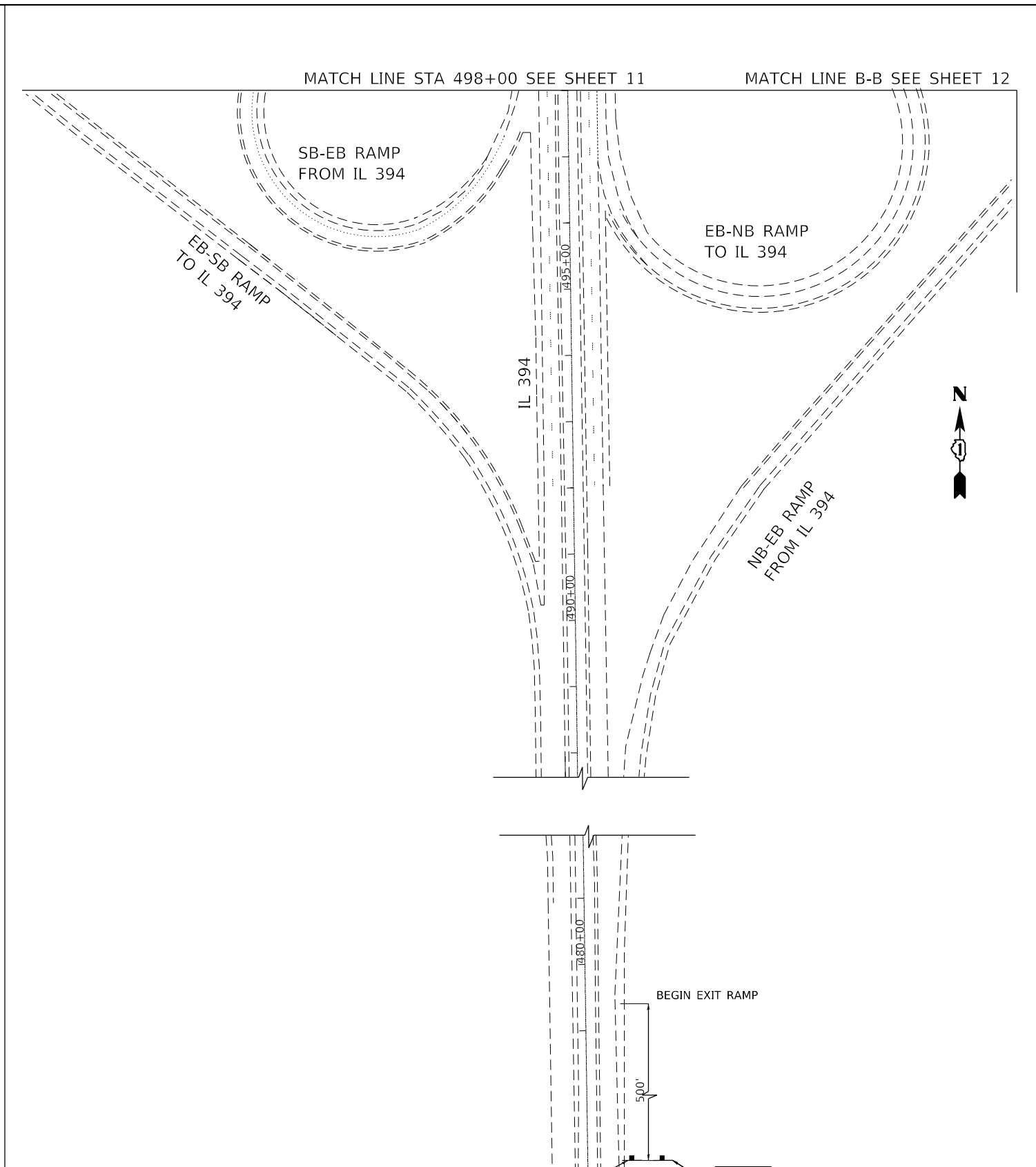
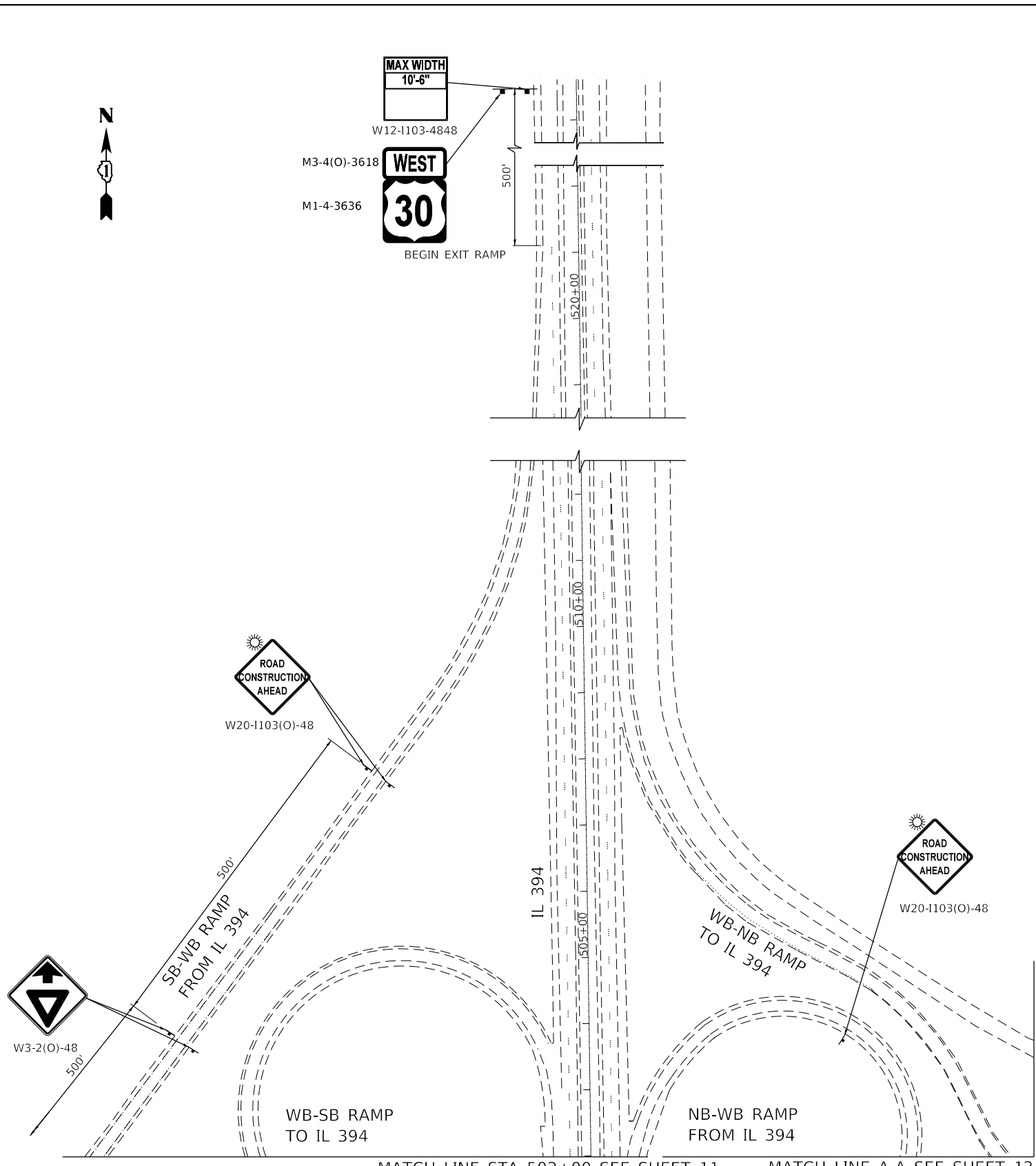
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE I
US RTE. 30 OVER DEER CREEK**

SCALE: 1"=50' SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 11-B-I-3	COUNTY COOK	TOTAL SHEETS 32	SHEET NO. 12
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

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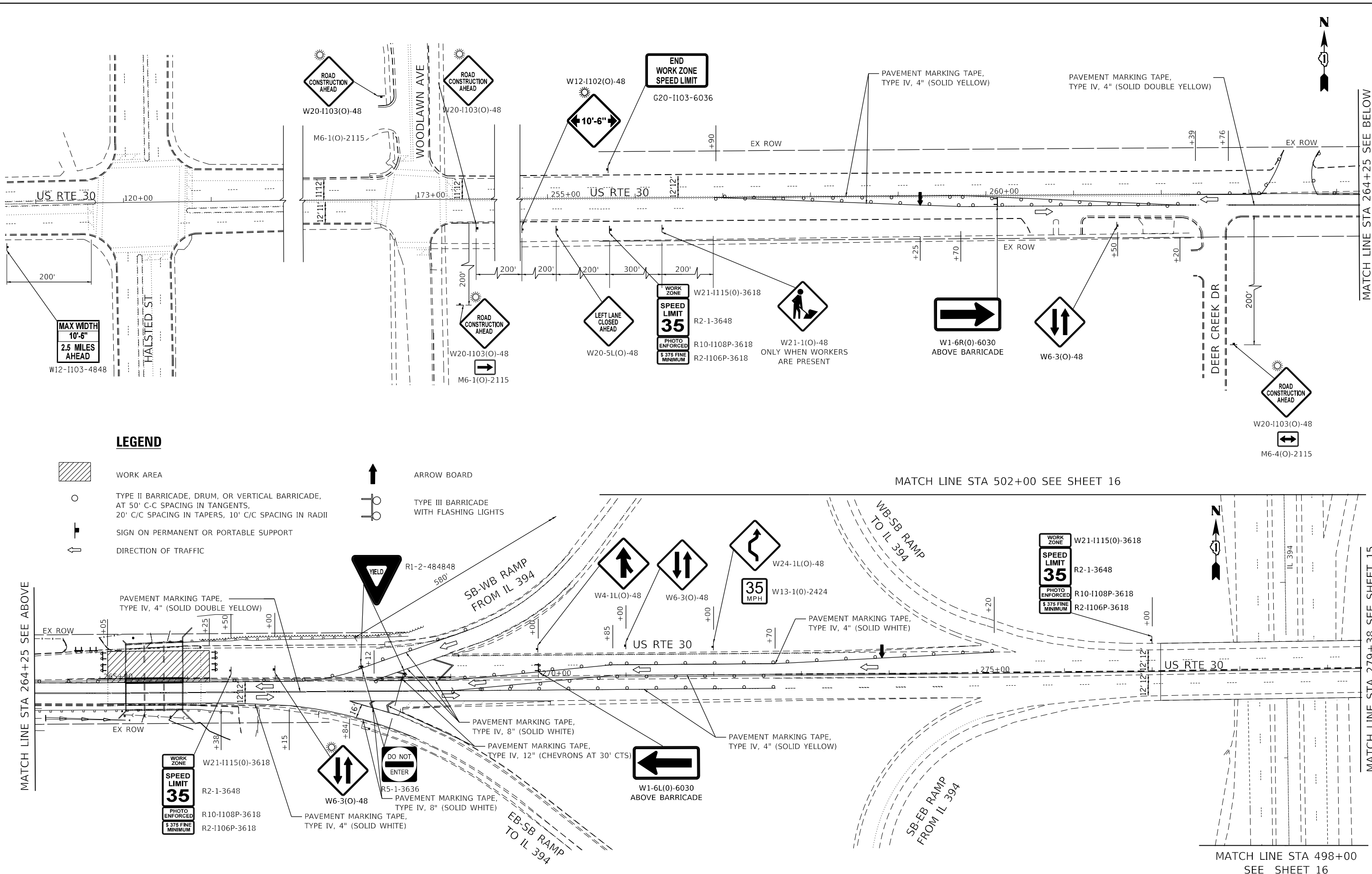


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PLOT DATE = 4/11/2018	DATE - 03/16/2018	REVISOR -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MAINTENANCE OF TRAFFIC - STAGE I	
US RTE. 30 OVER DEER CREEK	
SCALE: 1" = 50'	SHEET 3 OF 3 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-I-3	COOK	32	13
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				



LEGEND

- WORK AREA
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE, AT 50' C-C SPACING IN TANGENTS, 20' C/C SPACING IN TAPERS, 10' C/C SPACING IN RADII
- SIGN ON PERMANENT OR PORTABLE SUPPORT
- DIRECTION OF TRAFFIC
- ARROW BOARD
- TYPE III BARRICADE WITH FLASHING LIGHTS

MATCH LINE STA 264+25 SEE ABOVE

MATCH LINE STA 502+00 SEE SHEET 16

MATCH LINE STA 279+38 SEE SHEET 15

MATCH LINE STA 498+00 SEE SHEET 16



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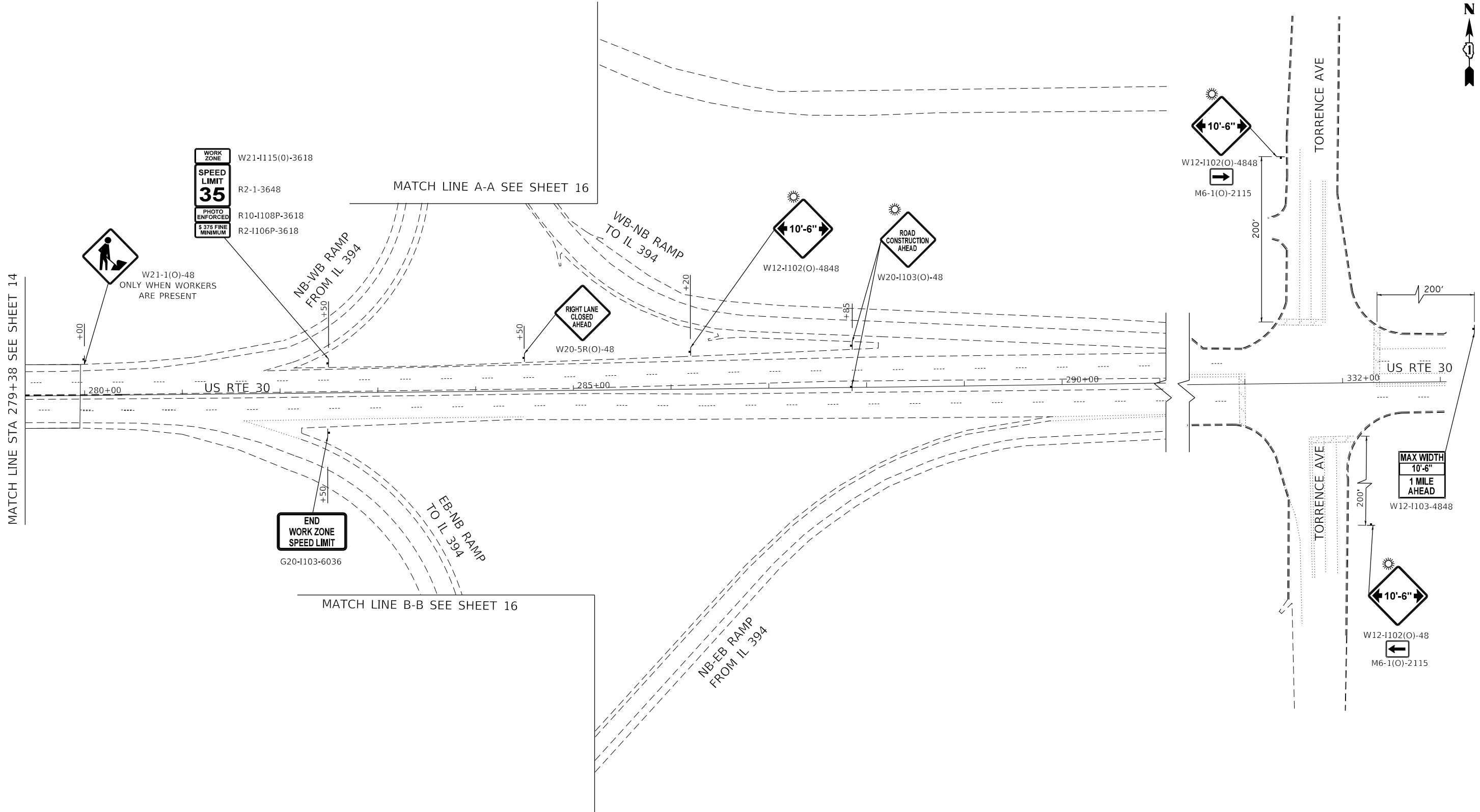
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE II
US RTE. 30 OVER DEER CREEK**

SCALE: 1"=50' SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-I-3	COOK	32	14
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

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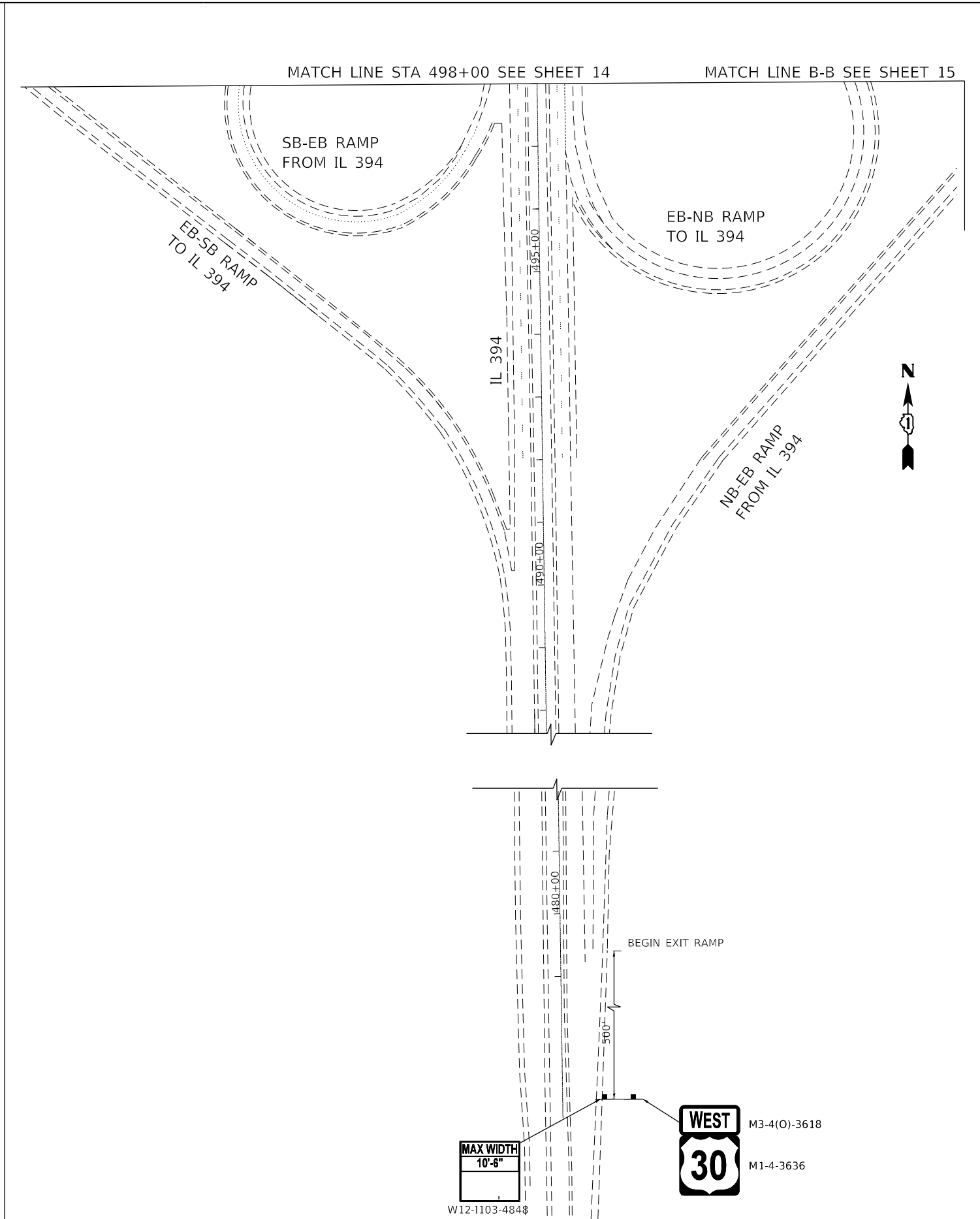
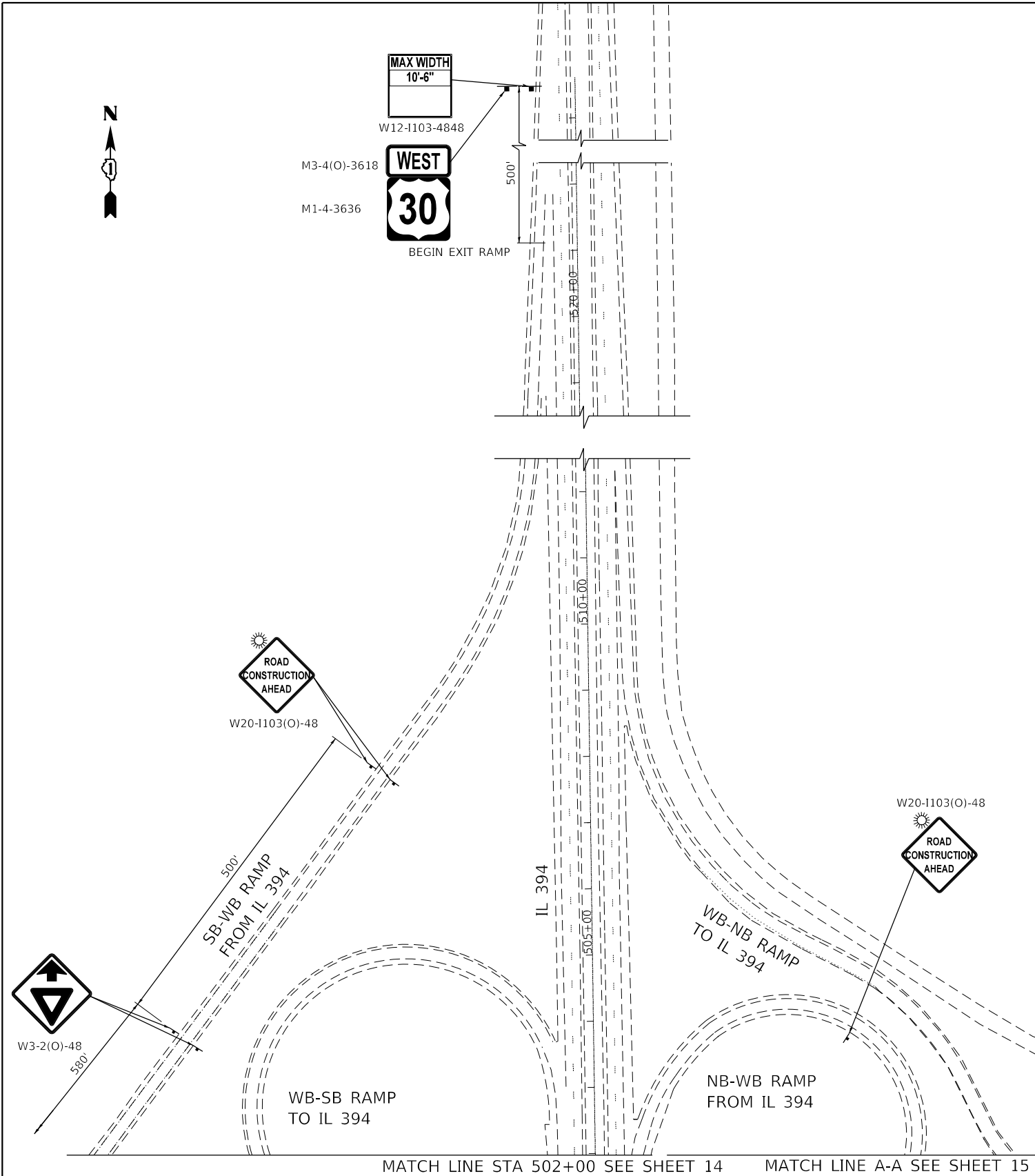
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE II
US RTE. 30 OVER DEER CREEK**

SCALE: 1"=50' SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-I-3	COOK	32	15
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MAINTENANCE OF TRAFFIC – STAGE II	
US RTE. 30 OVER DEER CREEK	
SCALE: 1"=100'	SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-I-3	COOK	32	16
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

EROSION AND SEDIMENT CONTROL NOTES:

1. ALL CONTROL MEASURES NECESSARY MUST MEET THE MINIMUM REQUIREMENTS AS DESCRIBED IN THE LATEST EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION. ADDITIONAL DETAILS AND BMPs ARE ALSO AVAILABLE AND CAN BE UTILIZED AS SHOWN IN THE ILLINOIS URBAN MANUAL, REVISED TO LATEST VERSION AS AMENDED. ALL ESC MEASURES WILL BE MAINTAINED IN ACCORDANCE WITH THE IDOT EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION AND IDOT'S BEST MANAGEMENT PRACTICES - MAINTENANCE GUIDE; (HTTP://WWW.IDOT.ILLINOIS.GOV/TRANSPORTATION-SYSTEM/ENVIRONMENT/EROSION-AND-SEDIMENT-CONTROL).
2. ALL THE SOIL EROSION AND SEDIMENT CONTROL FEATURES MUST BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF UPLAND DISTURBANCE. SOIL DISTURBANCE MUST BE PHASED OR ENACTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES MUST CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY AND/OR PERMANENT MEASURES.
3. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT SEDIMENT TRANSPORT OFF THE SITE IS REDUCED BY A COMBINATION OF MINIMIZATION OF EROSION AT THE SOURCE AND THE INSTALLATION OF SPECIFIC MEASURES TO CONTROL OR REDUCE THE TRANSPORT OF SEDIMENT. A COPY OF THE EROSION AND SEDIMENT CONTROL SCHEDULE BEING IMPLEMENTED BY THE CONTRACTOR MUST BE APPROVED BY THE ENGINEER, WILL BE ON THE CONSTRUCTION SITE AT ALL TIMES.
4. ALL RUNOFF ORIGINATING ON DISTURBED AREAS ASSOCIATED WITH THIS PROJECT WILL PASS THROUGH ONE OR MORE MEASURES THAT WILL MINIMIZE THE OFF-SITE SEDIMENT IMPACTS OF THE CONSTRUCTION ACTIVITIES.
5. DISTURBED AREAS ARE TO BE PROTECTED FROM EROSION IN A TIMELY MANNER. UPON COMPLETION OF GRADING OR CONSTRUCTION ACTIVITY, THE AREA WILL BE STABILIZED (USING PERMANENT MEASURES WHEN POSSIBLE) WITHIN ONE (1) CALENDAR DAY.
6. THE CONTRACTOR MUST CLEAN UP, GRADE THE WORK AREA AS THE PROJECT PROGRESSES AND INSTALL EROSION PROTECTION TO ELIMINATE THE CONCENTRATION OF RUNOFF, OR MUST INSTALL APPROPRIATE SEDIMENT CONTROL DEVICES TO TRAP SEDIMENT. PAVEMENT MUST BE CLEANED DAILY OR AS NECESSARY TO REMOVE EARTHEN MATERIAL TO THE SATISFACTION OF THE ENGINEER OR AUTHORIZED IDOT PERSONNEL.
7. STABILIZATION OF CUT OR FILL SLOPES WITH TEMPORARY OR PERMANENT EROSION CONTROL MEASURES IS REQUIRED WHENEVER THE CUT OR FILL ACTIVITY REACHES 10-FT VERTICALLY OR THE FINISHED SLOPE EQUALS 30-FT, WHICHEVER IS MORE RESTRICTIVE. ONCE THE STABILIZATION MEASURES ARE INSTALLED, THE PLACEMENT OF FILL EXCAVATION ACTIVITIES ARE ALLOWED TO PROCEED.
8. THE CONTRACTOR WILL ASSUME RESPONSIBILITY FOR MAINTENANCE OF ALL SOIL EROSION CONTROL DURING CONSTRUCTION. THE CONTRACTOR SHALL DESIGNATE ONE OF HIS EMPLOYEES TO BE RESPONSIBLE FOR IMPLEMENTATION OF THE EROSION AND SEDIMENT CONTROL PLAN ON ALL DISTURBED AREAS THROUGHOUT THE PROJECT.
9. THE CONTRACTOR'S REPRESENTATIVE HAS TO BE KNOWLEDGEABLE ABOUT INSTALLATION AND MAINTENANCE OF THE REQUIRED MEASURES AND HAVE TAKEN AN ILLINOIS DEPARTMENT OF TRANSPORTATION OR APPROVED EQUAL EROSION AND SEDIMENT CONTROL COURSE. THIS PERSON SHALL HAVE THE AUTHORITY TO CARRY OUT THE IMPLEMENTATION OF ANY INSTRUCTION CONCERNING THE EROSION AND SEDIMENT CONTROL PLAN PROVIDED BY THE ENGINEER. THIS INDIVIDUAL AND THE ENGINEER MUST MAKE INSPECTIONS A MINIMUM OF ONCE EVERY SEVEN DAYS OF THE FOLLOWING:
 - A. DISTURBED AREAS OF THE PROJECT SITE THAT HAVE NOT BEEN FULLY STABILIZED.
 - B. STRUCTURAL CONTROL MEASURES (SUCH AS PERIMETER EROSION BARRIER, ETC.)
 - C. LOCATIONS WHERE VEHICLES ENTER OR EXIT THE PROJECT SITE.
 - D. AN ADDITIONAL INSPECTION OF THE ITEMS LISTED ABOVE MUST BE MADE WITHIN 24-HOURS AFTER A 24-HOUR RAINFALL OR EQUIVALENT SNOWFALL EVENT GREATER THAN 0.5-INCH. DURING WINTER MONTHS, ALL MEASURES MUST BE CHECKED BY THE CONTRACTOR AFTER EACH SIGNIFICANT SNOWMELT.
10. ALL THE EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED DURING THE CONSTRUCTION SEASON, AS WELL AS OVER THE WINTER SHUTDOWN PERIOD AND OTHER DAYS WHEN THE PROJECT IS CLOSED DOWN FOR A LONGER DURATION. ANY CONTROL MEASURES FILLED MORE THAN 75% MUST BE CLEANED AND RESET AND THESE SPOILS REMOVED TO AN APPROVED SITE.
11. SALVAGED TOPSOIL SHALL BE PLACED ON WELL DRAINED LAND AWAY FROM INTERMITTENT AND ACTIVE DRAINAGE PATHS WITH THE APPROPRIATE RUNOFF CONTROL AND SEDIMENT CONTROL MEASURES INSTALLED AROUND THE STORAGE SITE. IMMEDIATELY AFTER THE FINAL SHAPING OF THE STOCKPILE, THE TOPSOIL WILL BE STABILIZED IN ACCORDANCE WITH THE METHOD APPROVED BY IDOT. THE CONTRACTOR WILL PROVIDE ADEQUATE QUANTITY OF SILT FENCE TO CONTROL THE PERIMETER OF THE STOCKPILE.
12. EXCAVATION TO BE USED FOR EMBANKMENTS SHALL NOT BE STOCKPILED UNLESS PERIMETER CONTROLS ARE UTILIZED. WHEN THIS MATERIAL IS STOCKPILED FOR THE CONVENIENCE OF THE CONTRACTOR, THE COST OF THE CONTROLS WILL BE BORNE BY THE CONTRACTOR. IF THE MATERIAL IS STOCKPILED AT THE DIRECTION OF THE ENGINEER, THE DEPARTMENT WILL ASSUME THE COST OF INSTALLING AND MAINTAINING THE CONTROLS.
13. IF AND/OR WHEN THE CONTRACTOR REQUESTS CHANGE TO POSTPONE COMPLETION OF THE EXCAVATION OF A SPECIFIC AREA AS A CONTINUOUS OPERATION AND PLACING THE TOPSOIL AS DEFINED IN THE STANDARD SPECIFICATIONS, THE ENGINEER MAY ALLOW THE CONTRACTOR TO STABILIZE THE AREA USING TEMPORARY STABILIZATION WITH STRAW MULCH 25 FEET AWAY FROM THE SHOULDER OF THE ROAD PROVIDED THE FOLLOWING CONDITIONS ARE MET:
 - A. ALL AREAS BEING STABILIZED ARE 1:3 SLOPES OR FLATTER
 - B. THE CONTRACTOR BEARS THE COST OF PREPARING THE SEED BED AND STABILIZING THE AREA WITH TEMPORARY STABILIZATION WITH MULCH METHOD 3.
 - C. ALL REQUIRED SEDIMENT CONTROL MEASURES FOR THE SECTION OF ROAD IN QUESTION HAVE BEEN INSTALLED AND ARE BEING MAINTAINED.
14. TOPSOIL PLACEMENT: TOPSOIL WILL BE PLACED ON FINAL SLOPES WHICH WILL NOT BE DISTURBED BY FUTURE CONSTRUCTION. TOPSOIL WILL NOT BE PLACED ON SURFACES WHICH WILL BE PAVED IN THE FUTURE NOR ON TEMPORARY STEEP SLOPES.
15. IN AREAS WHERE A PERMANENT VEGETATIVE COVER IS PRACTICABLE AND INCLUDED IN THE CONTRACT DOCUMENTS, A SPECIAL EFFORT SHOULD BE MADE TO ESTABLISH A COVER AS SOON AS A DISTURBED AREA IS BROUGHT TO FINAL GRADE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG FINAL GRADING AND SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
16. THE CONTRACTOR'S REPRESENTATIVE AND THE ENGINEER MUST KEEP A WRITTEN REPORT SUMMARIZING THE REQUIRED INSPECTIONS. THE REPORTS MUST BE KEPT AT THE SITE DURING CONSTRUCTION. THE REPORT MUST ALSO BE RETAINED FOR THREE YEARS FROM THE DATE THE SITE IS FINALLY STABILIZED.
17. ANY SEDIMENT LADEN DEWATERING DISCHARGE MUST BE DIRECTED TO AN APPROVED SEDIMENT TRAPPING CONTROL MEASURE PRIOR TO RELEASE FROM THE PROJECT SITE.
18. NO WORK IS ALLOWED BEYOND THE PERMITTED AREA. ANY WORK WITHIN A CREEK OR DITCH CAPABLE OF CONVEYING WATER MUST BE CONDUCTED IN THE DRY. PROVISIONS MUST BE MADE TO BYPASS PUMP OR DEWATER ANY AREAS IN WHICH WORK WILL BE CONDUCTED. IN HIGH FLOW CHANNELS WHERE DEWATERING IS NOT POSSIBLE OR PRACTICAL, SILT FENCE OR SEDIMENT CURTAINS MAY BE INSTALLED PARALLEL TO THE STREAM BANK. IN NO CASE WILL THE CURTAINS BE INSTALLED PERPENDICULAR TO THE FLOW. DEWATERING MUST BE DISCHARGED TO A STABLE, NON-ERODIBLE SURFACE AND IN-STREAM WORK BARRIERS MUST BE COMPOSED OF NON-ERODIBLE MATERIAL.
19. SEEDING USAGE

SODDING, SALT TOLERANT:
USED ON FINAL DISTURBED CONSTRUCTION AREAS INDICATED ON THE PLANS.

TEMPORARY EROSION CONTROL SEEDING:
USED IN AREAS REQUIRING SHORT TERM TEMPORARY SEEDING DURING CONSTRUCTION.
20. THE CONTRACTOR MUST COOPERATE WITH THE ENGINEER AND HIS/HER REPRESENTATIVE WHO WILL MAKE SITE VISITS TO REVIEW THE COMPLIANCE OF THE PLANS IN THE FIELD AND AUDIT IF NECESSARY. THE CONTRACTOR MUST PREPARE THE LOGS AND RECORDS WHEN REQUIRED AND SUBMIT TO IDOT AND/OR APPROPRIATE AGENCIES.
21. THE INSTALLATION, MAINTENANCE, REMOVAL AND RESTORATION OF THE AREA DISTURBED BY THE PLACEMENT OF THE PERIMETER EROSION BARRIER ARE INCLUDED IN THE CONTRACT UNIT PRICE FOR PERIMETER EROSION BARRIER. AFTER ALL PERIMETER EROSION BARRIER IS REMOVED, THE AREAS DAMAGED BY THE PERIMETER EROSION CONTROL BARRIER MUST BE RESTORED TO THEIR ORIGINAL CONDITION.
22. STABILIZATION MEASURES SHALL BE INITIATED IMMEDIATELY WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN ONE (1) DAY AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED ON ALL DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION WILL NOT OCCUR FOR A PERIOD OF FOURTEEN (14) OR MORE CALENDAR DAYS.

SOIL PROTECTION SCHEDULE:

STABILIZATION TYPE	JAN.	FEB.	MAR.	APR.	MAY	JUN.	JUL.	AUG.	SEP.	OCT.	NOV.	DEC.
PERMANENT SEEDING						→					→	
DORMANT SEEDING		→								→		→
TEMPORARY SEEDING										→		
EROSION BLANKET / HYDROMULCH											→	

SOIL EROSION AND SEDIMENT CONTROL STRATEGY:

1. ERECT PERIMETER EROSION BARRIERS AS SHOWN ON THE PLANS.
2. ESTABLISH STABILIZED CONSTRUCTION ENTRANCES.
3. INSPECT AND MAINTAIN ALL EROSION AND SEDIMENT CONTROL MEASURES FOR THE DURATION OF CONSTRUCTION.
4. TEMPORARY STABILIZATION OF EACH STAGE SHOULD BE COMPLETED BEFORE WORK BEGINS ON SUBSEQUENT STAGES.
5. STABILIZE DISTURBED AREAS WITH TEMPORARY EROSION CONTROL MEASURES. USE THE PERMANENT SEEDING WITH EROSION CONTROL BLANKET AS SHOWN ON THE PLANS FOR PERMANENT STABILIZATION.
6. WHEN THE PERMANENT STABILIZATION IS ESTABLISHED, REMOVE ALL TEMPORARY EROSION CONTROL MEASURES.

HIGHWAY STANDARD

STD. NO.	TITLE
280001	TEMPORARY EROSION CONTROL SYSTEMS

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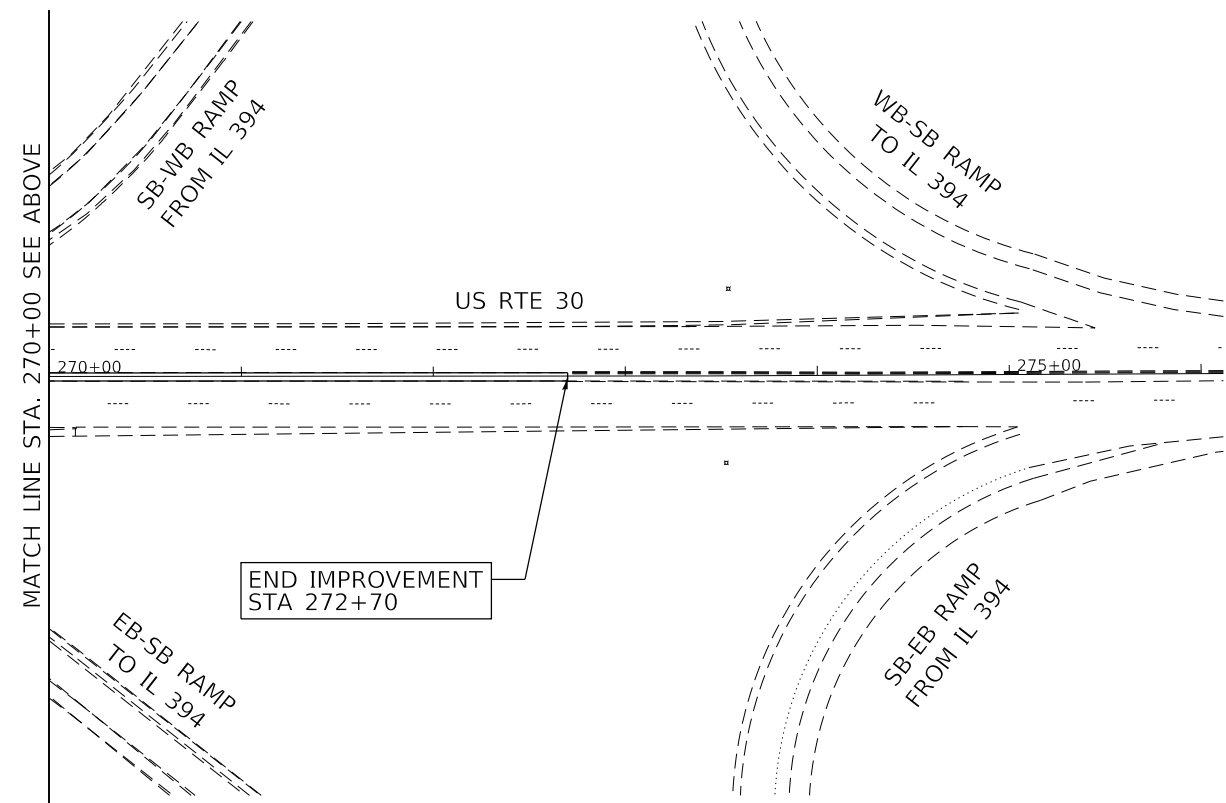
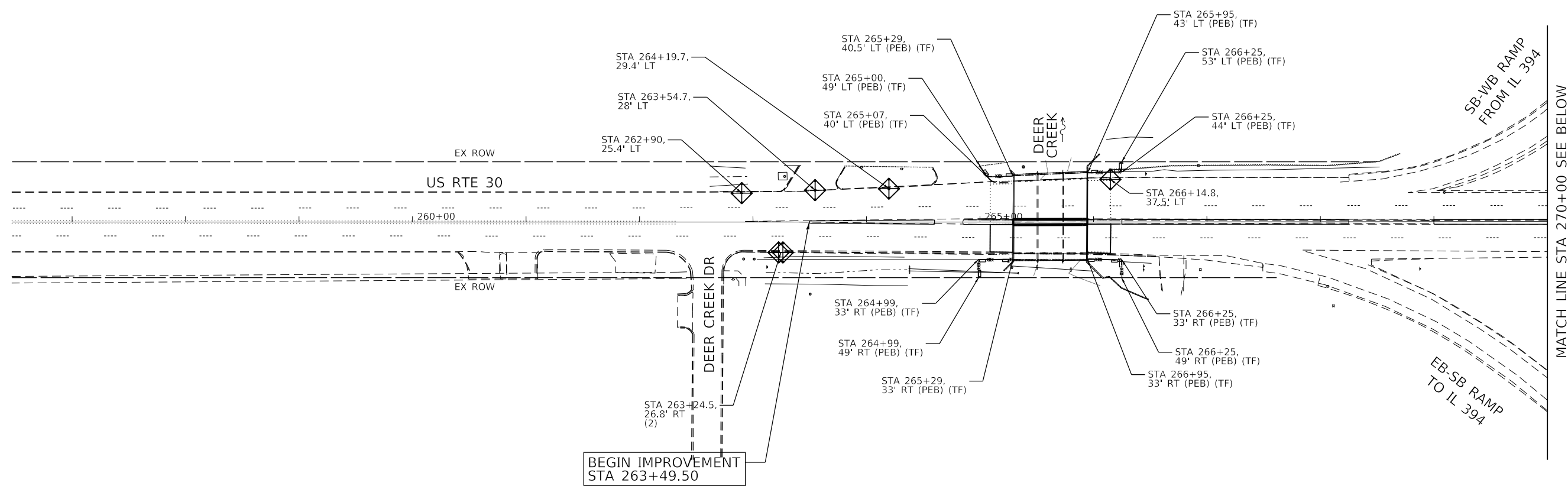
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**


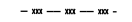
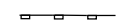
**EROSION AND SEDIMENT CONTROL NOTES
US RTE. 30 OVER DEER CREEK**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-I-3	COOK	32	17
			CONTRACT NO. 60M93	
ILLINOIS FED. AID PROJECT				



LEGEND

-  INLET FILTER
-  TEMPORARY FENCE (TF)
-  PERIMETER EROSION BARRIER (PEB)

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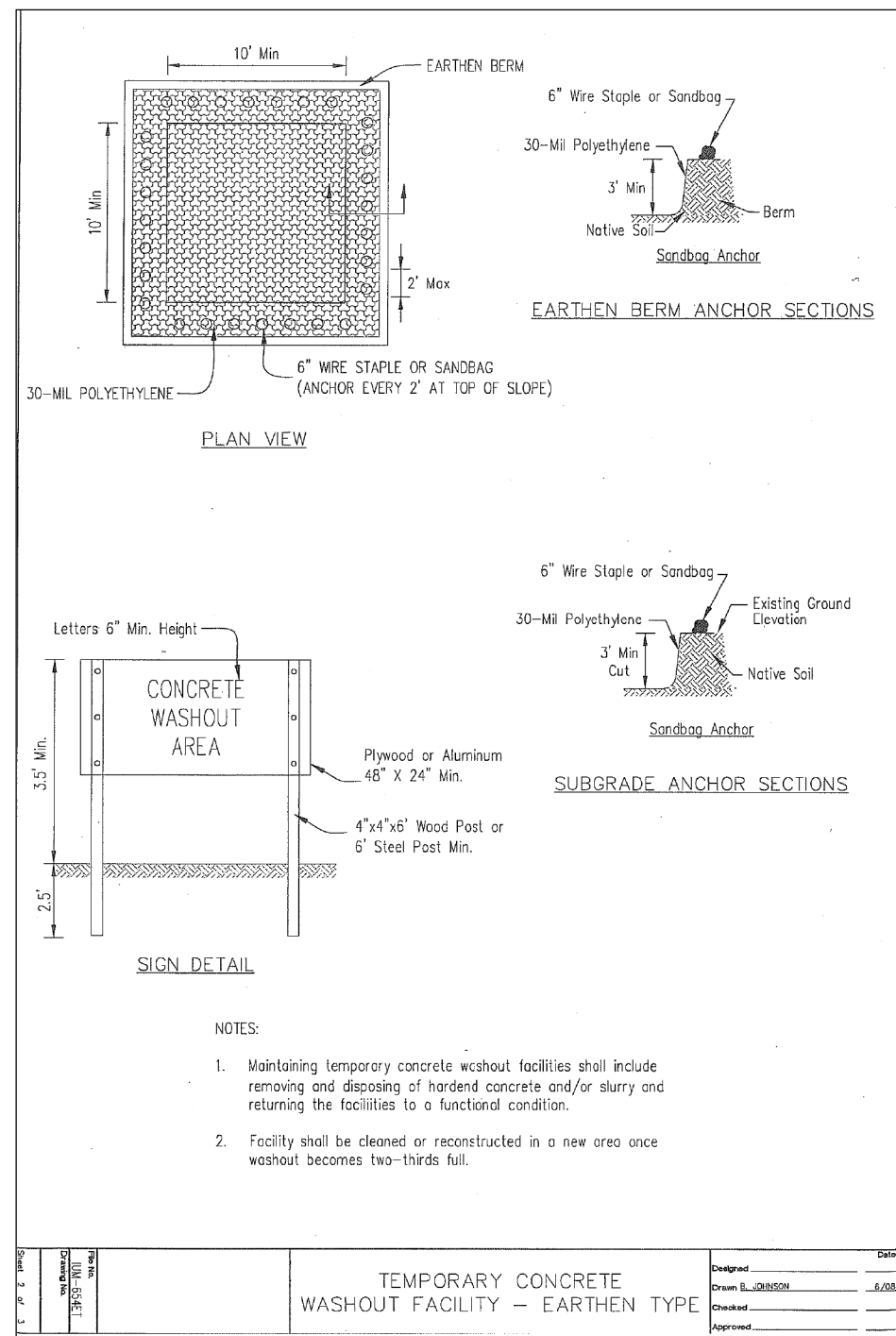
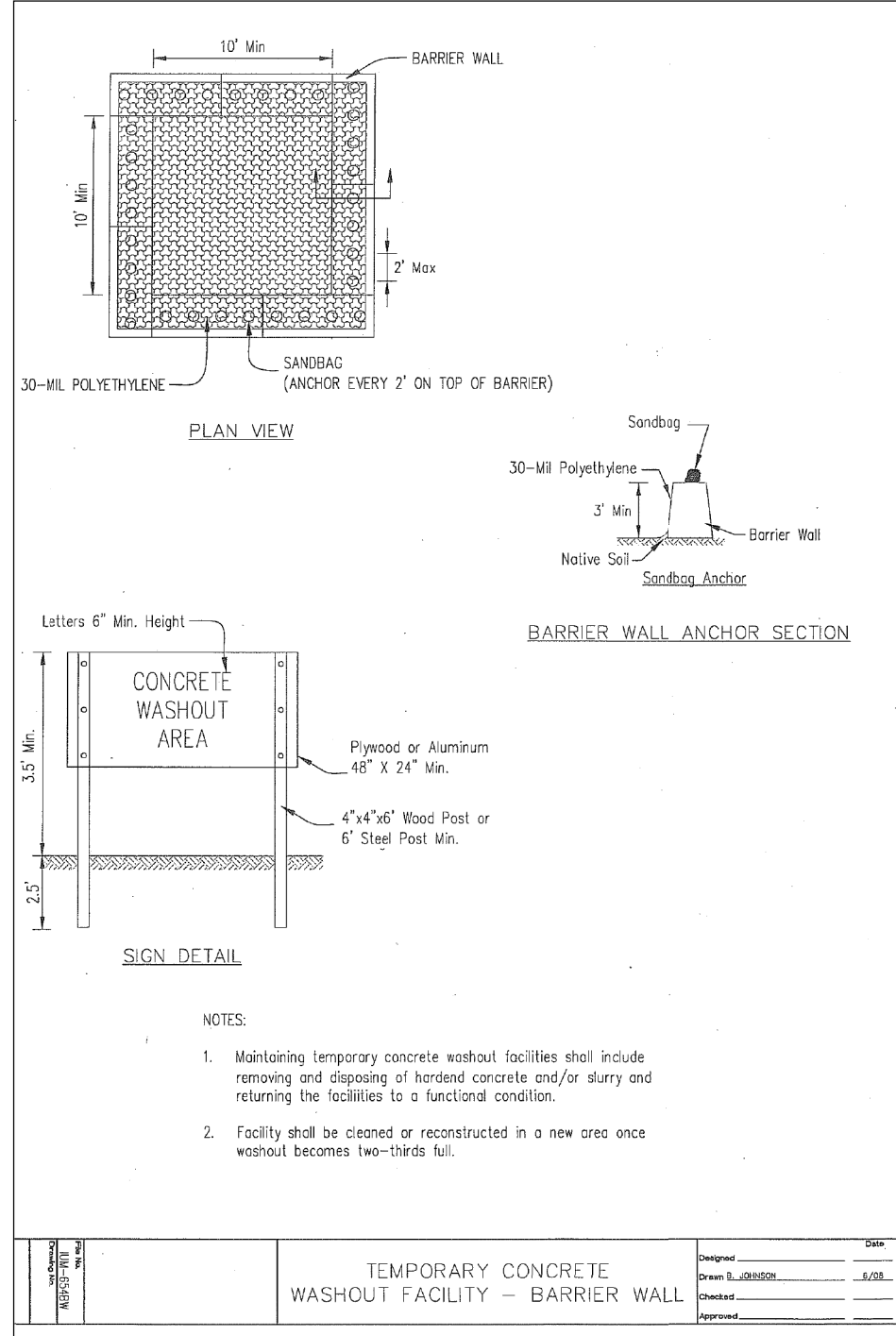
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EROSION CONTROL PLANS
US RTE. 30 OVER DEER CREEK**

SCALE: SHEET OF SHEETS STA. TO STA.

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353	11-B-I-3	COOK	32	18
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

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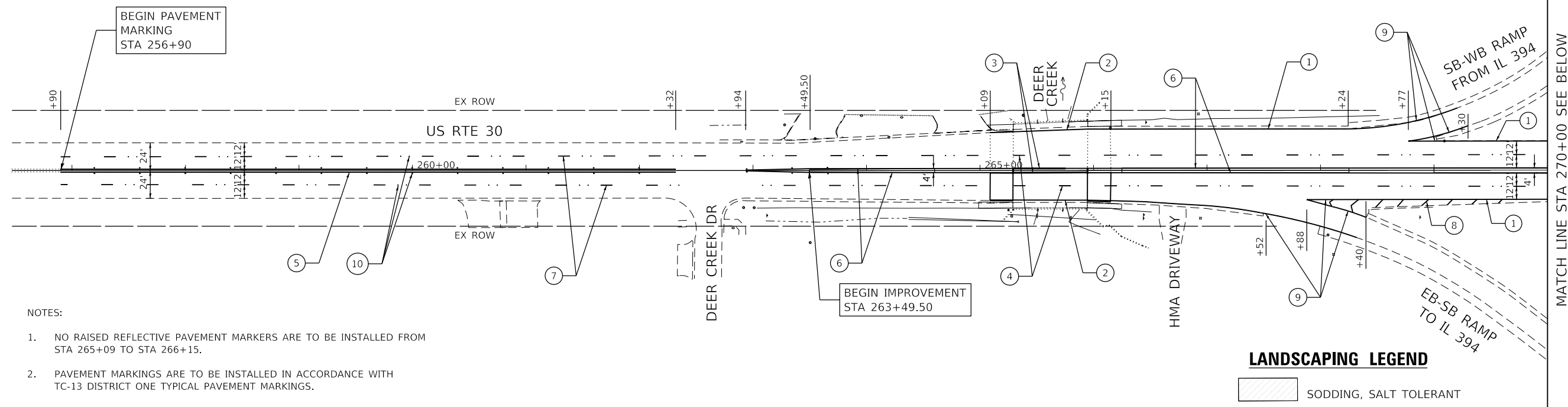


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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EROSION CONTROL DETAILS			
US RTE. 30 OVER DEER CREEK			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

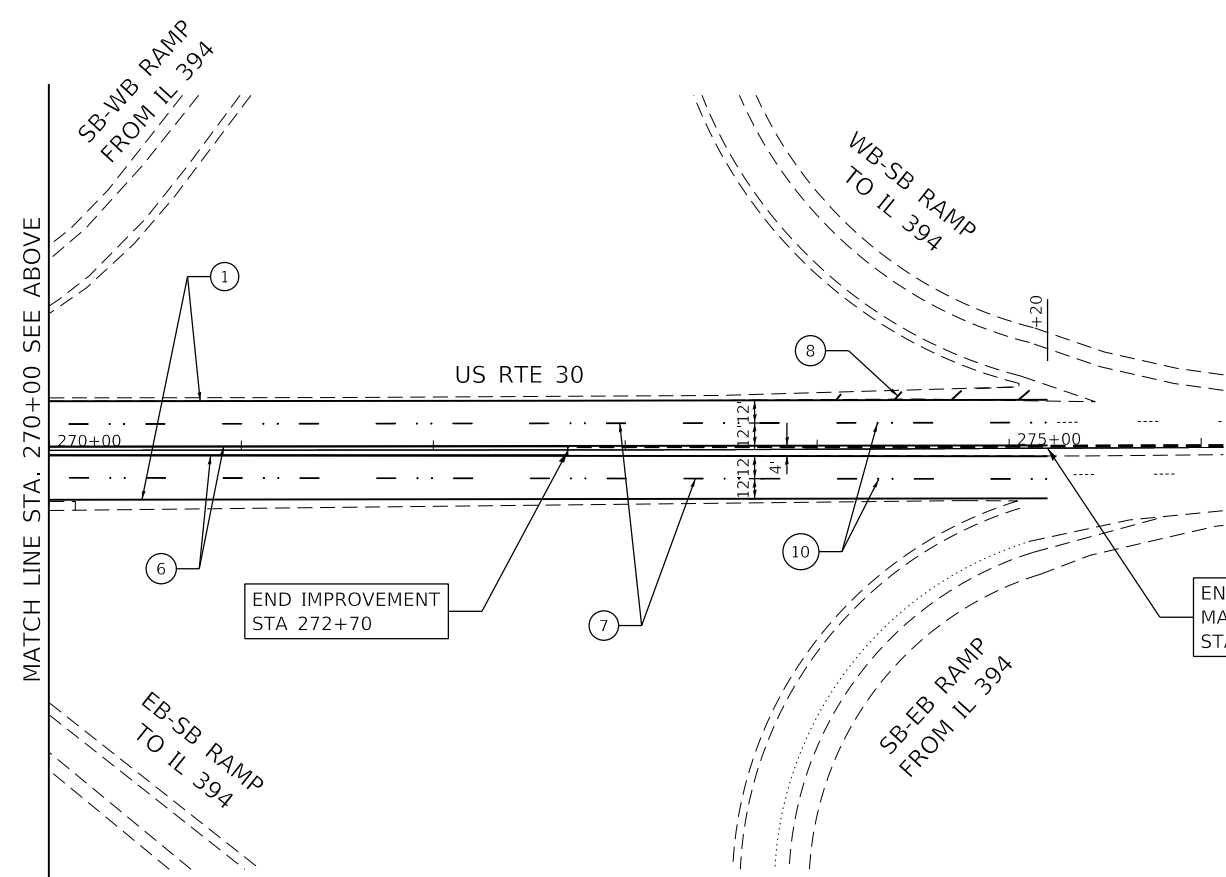


NOTES:

1. NO RAISED REFLECTIVE PAVEMENT MARKERS ARE TO BE INSTALLED FROM STA 265+09 TO STA 266+15.
2. PAVEMENT MARKINGS ARE TO BE INSTALLED IN ACCORDANCE WITH TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS.
3. RAISED REFLECTIVE PAVEMENT MARKERS AND REPLACEMENT REFLECTORS ARE TO BE INSTALLED IN ACCORDANCE WITH TC-11 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT).

LANDSCAPING LEGEND

SODDING, SALT TOLERANT



PAVEMENT MARKING LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SOLID WHITE)
- ② RECESSED POLYUREA PAVEMENT MARKING TYPE I - LINE 4" (SOLID WHITE)
- ③ RECESSED POLYUREA PAVEMENT MARKING TYPE I - LINE 4" (SOLID YELLOW)
- ④ RECESSED PREFORMED PLASTIC PAVEMENT MARKING TYPE C - LINE 4" (WHITE - 10' LINE, 30' SPACE)
- ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW) @ 11" C-C
- ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SOLID YELLOW)
- ⑦ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE - 10' LINE, 30' SPACE)
- ⑧ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE DIAGONAL @ 45°, 20' C-C SPACING) - 5 MINIMUM
- ⑨ THERMOPLASTIC PAVEMENT MARKING - LINE 8" (SOLID WHITE)
- ⑩ REPLACEMENT REFLECTOR

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING & LANDSCAPING PLANS			
US RTE. 30 OVER DEER CREEK			
SCALE: 1"=50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

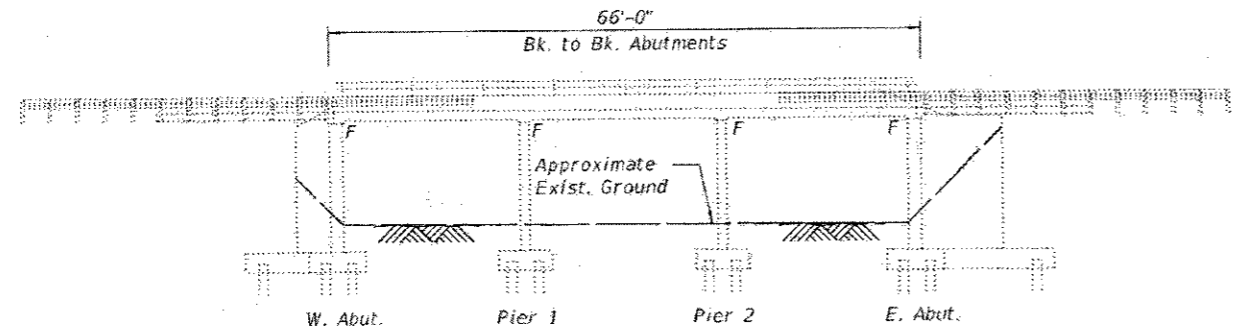
Existing Structure:
 SN 016-0276 built in 1926, Section 11-5. Structure consists of 3 spans.
 Existing bridge is a slab bridge supported on stub abutments and piers.
 overall length of the bridge is 66'-0" back to back abutments and overall
 width of the bridge varies from 75'-2" to 77'-10".
 In 1978 the structure was rehabilitated. Repairs included deck
 reconstruction, partial reconstruction of piers and abutments.

Traffic will be maintained utilizing staged construction with crossover.

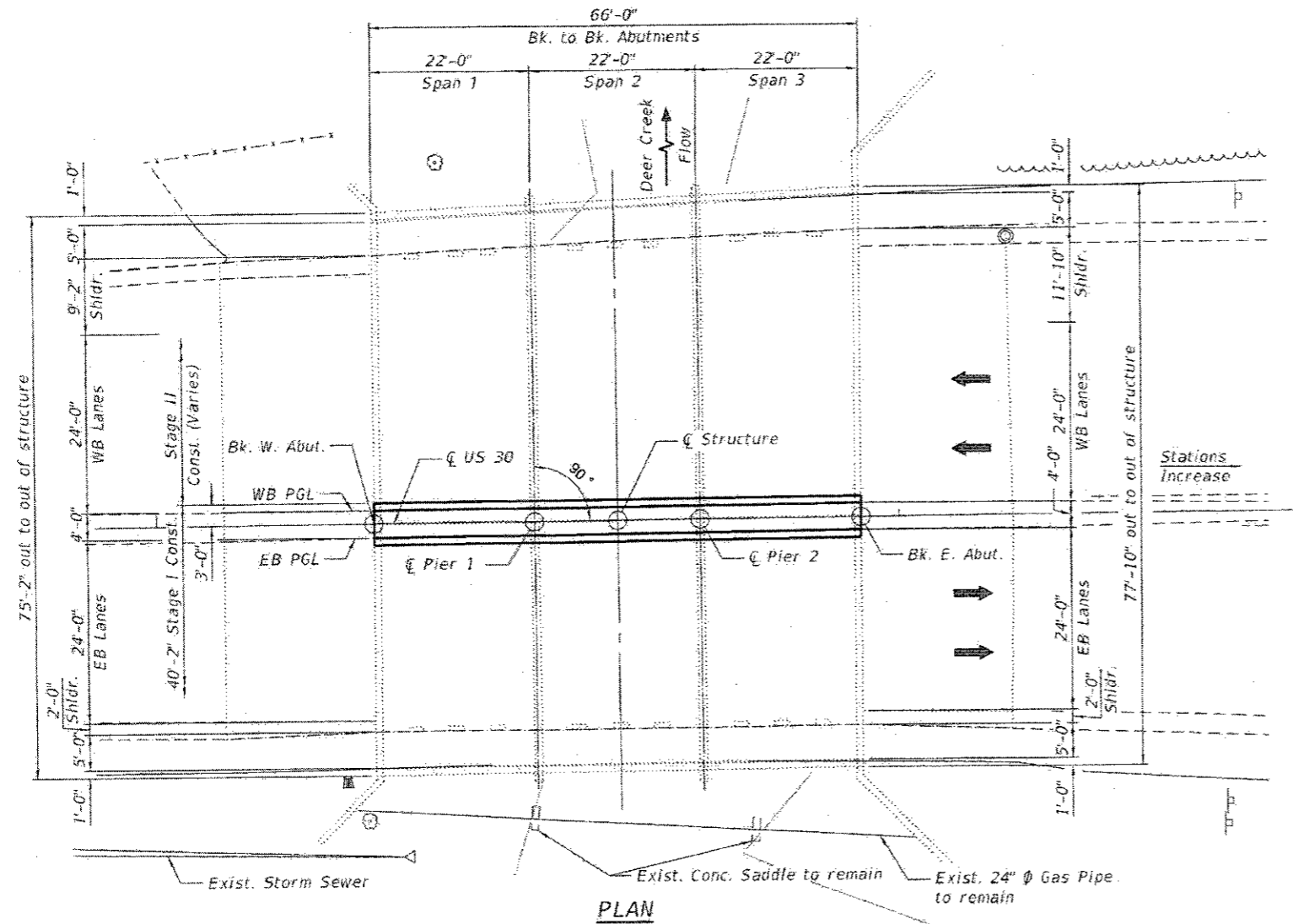
No Salvage.

SCOPE OF WORK

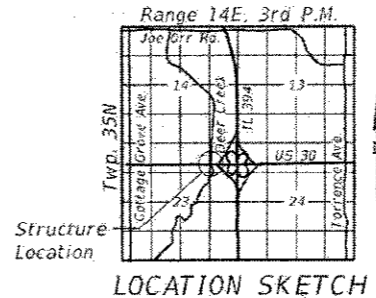
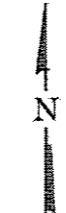
1. Remove and replace existing raised median and portion of slab to close the open joint along bridge centerline.
2. Scarify Bridge Deck and approach slabs.
3. Perform concrete deck repairs (Partial Depth and Full Depth).
4. Eliminate outer floor drains in each span.
5. Clean and Reseal Pavement Relief Joints.
6. Construct Bridge Deck Thin Polymer Overlay.



ELEVATION

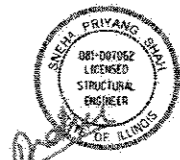


PLAN



LOCATION SKETCH

DESIGN SPECIFICATIONS
 2002 AASHTO Standard
 Specifications, 17th Edition



DATE SIGNED: 04/04/2018
 EXP. DATE: 11/30/2018

GENERAL PLAN AND ELEVATION
U.S. ROUTE 30 AT DEER CREEK
 F.A.P. RTE. 353
 SECTION 11-B-1-3
 COOK COUNTY
 STRUCTURE NO. 016-0276

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET 1 OF 6A SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-1-3	COOK	32	21
			CONTRACT NO. 60M93	
ILLINOIS FED. AID PROJECT				

INDEX OF SHEETS

1. General Plan and Elevation
2. General Data and Stage Construction Details
3. Temporary Concrete Barrier for Stage Construction
4. Deck Repair Details
5. Miscellaneous Repair Details
6. Superstructure Details
- 6A. Bar Splicer Assembly and Mechanical Splicer Details

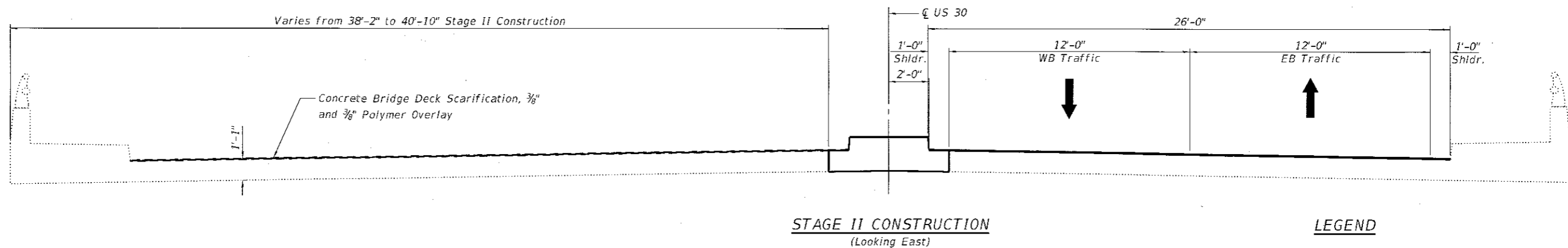
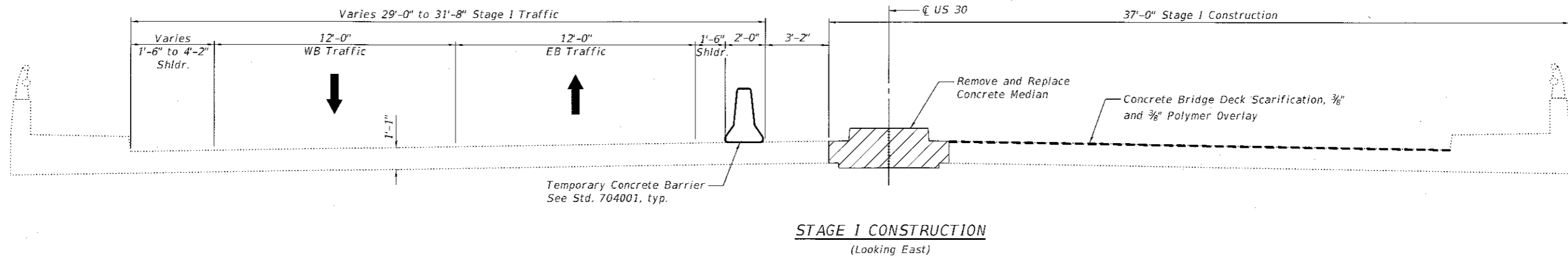
GENERAL NOTES

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
3. Areas of deck repairs repairs are estimated. Actual type, location and dimensions are to be determined by the Engineer during construction and documented on as-built plans.
4. The Department has determined that in-stream work is not required for the work specified in this Contract. The Department has not obtained a 404 Permit. If the Contractor chooses to use activities requiring an USACE 404 Permit, it is the Contractor's responsibility to secure the proper USACE Permits.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Hot-Mix Asphalt Surface Course, Mix "D", N70	Ton	13		13
Hot-Mix Asphalt Surface Removal, 2 Inch	Sq. Yd.	116		116
Combination Curb and Gutter Removal	Foot	80		80
Concrete Removal	Cu. Yd.	30.8		30.8
Concrete Superstructure	Cu. Yd.	28.4		28.4
Protective Coat	Sq. Yd.	54		54
Reinforcement Bars, Epoxy Coated	Pound	5420		5420
Mechanical Splicers	Each	240		240
Combination Concrete Curb and Gutter, Type B-6.24 (Variable Width Gutter Flag)	Foot	80		80
Clean and Reseal Relief Joint	Foot	117		117
Approach Slab Repair (Partial Depth)	Sq. Yd.	6		6
Concrete Bridge Deck Scarification, 3/8"	Sq. Yd.	597		597
Bridge Deck Thin Polymer Overlay, 3/8"	Sq. Yd.	597		597
Structural Repair of Concrete (Depth Equal to or less than 5 Inches)	Sq. Ft.		19	19
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	50		50
Deck Slab Repair (Partial)	Sq. Yd.	22		22

* Top and vertical surfaces of reconstructed median only.



LEGEND

Concrete Removal

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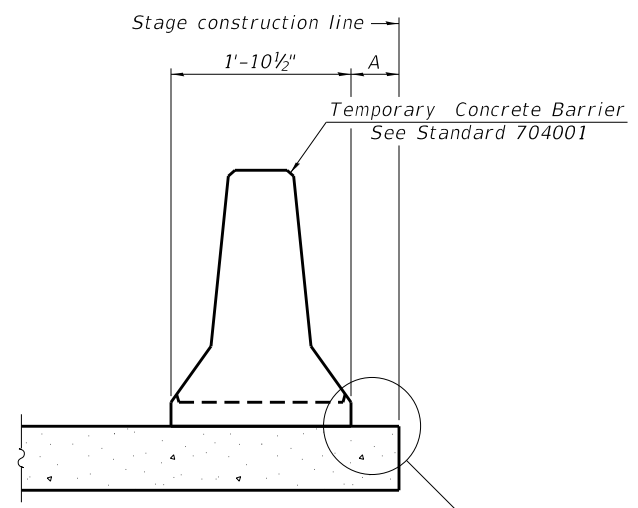
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL DATA AND STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 016-0276**

SHEET 2 OF 6A SHEETS

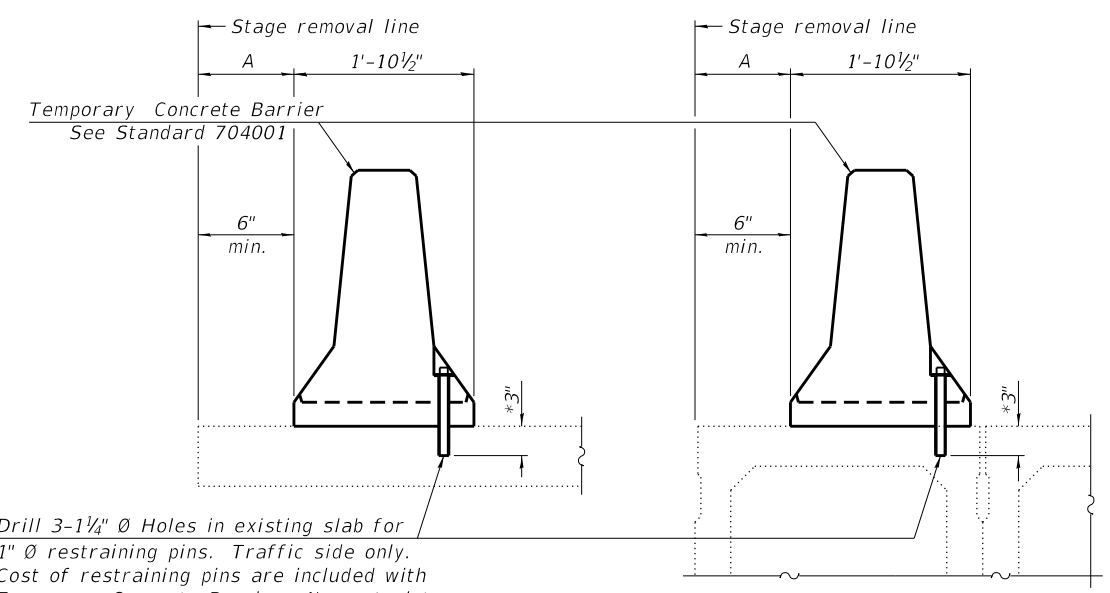
F.A.P. RTE. 353	SECTION 11-B-13	COUNTY COOK	TOTAL SHEETS 32	SHEET NO. 22
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

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When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



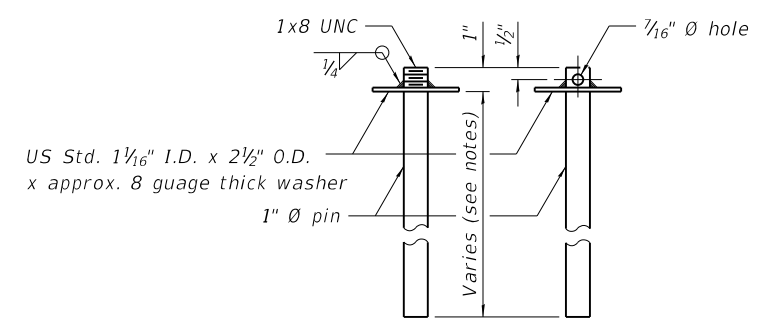
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

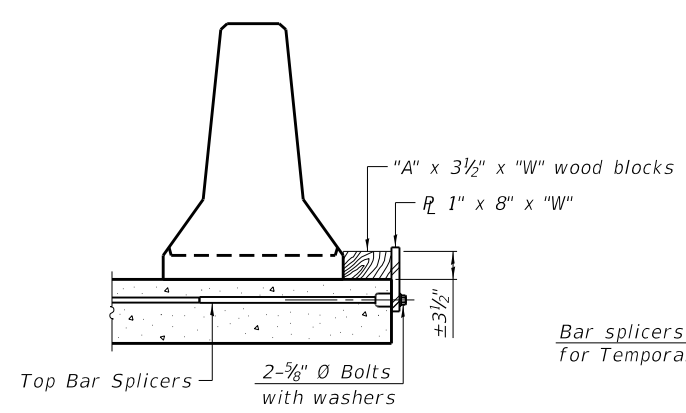
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

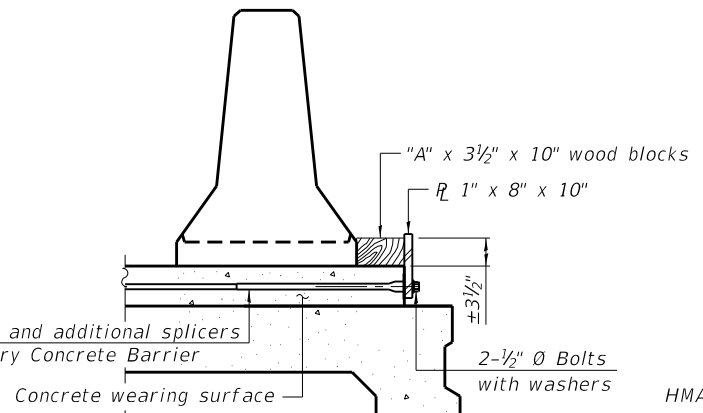
SECTIONS THRU SLAB OR DECK BEAM



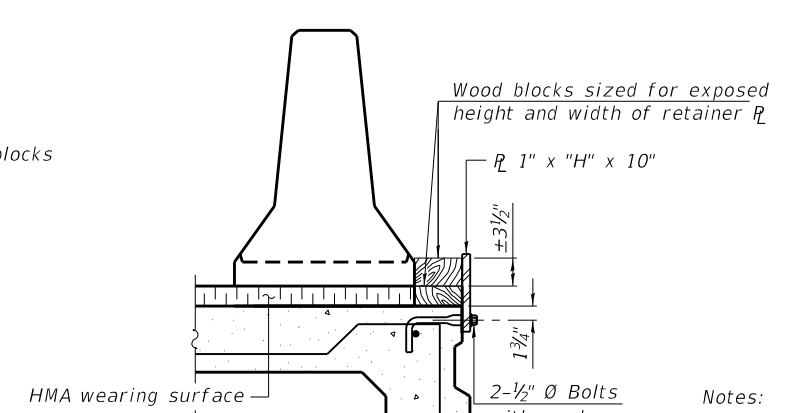
RESTRAINING PIN



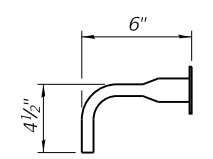
DETAIL I



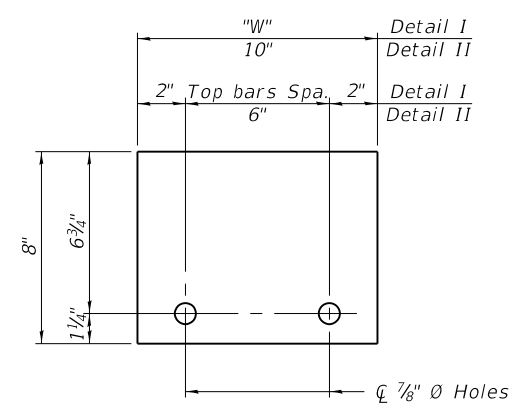
DETAIL II



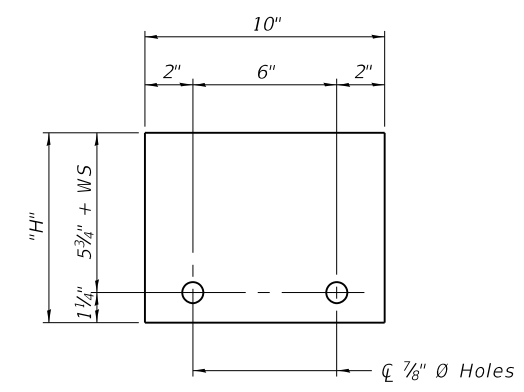
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate center of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.
Detail I - Installation for a new bridge deck or bridge slab.
Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

R-27

8-11-2017



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

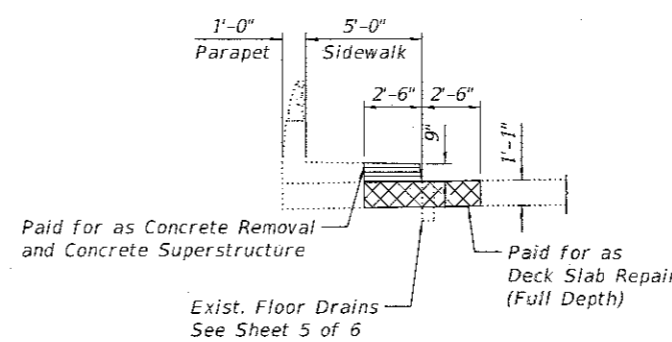
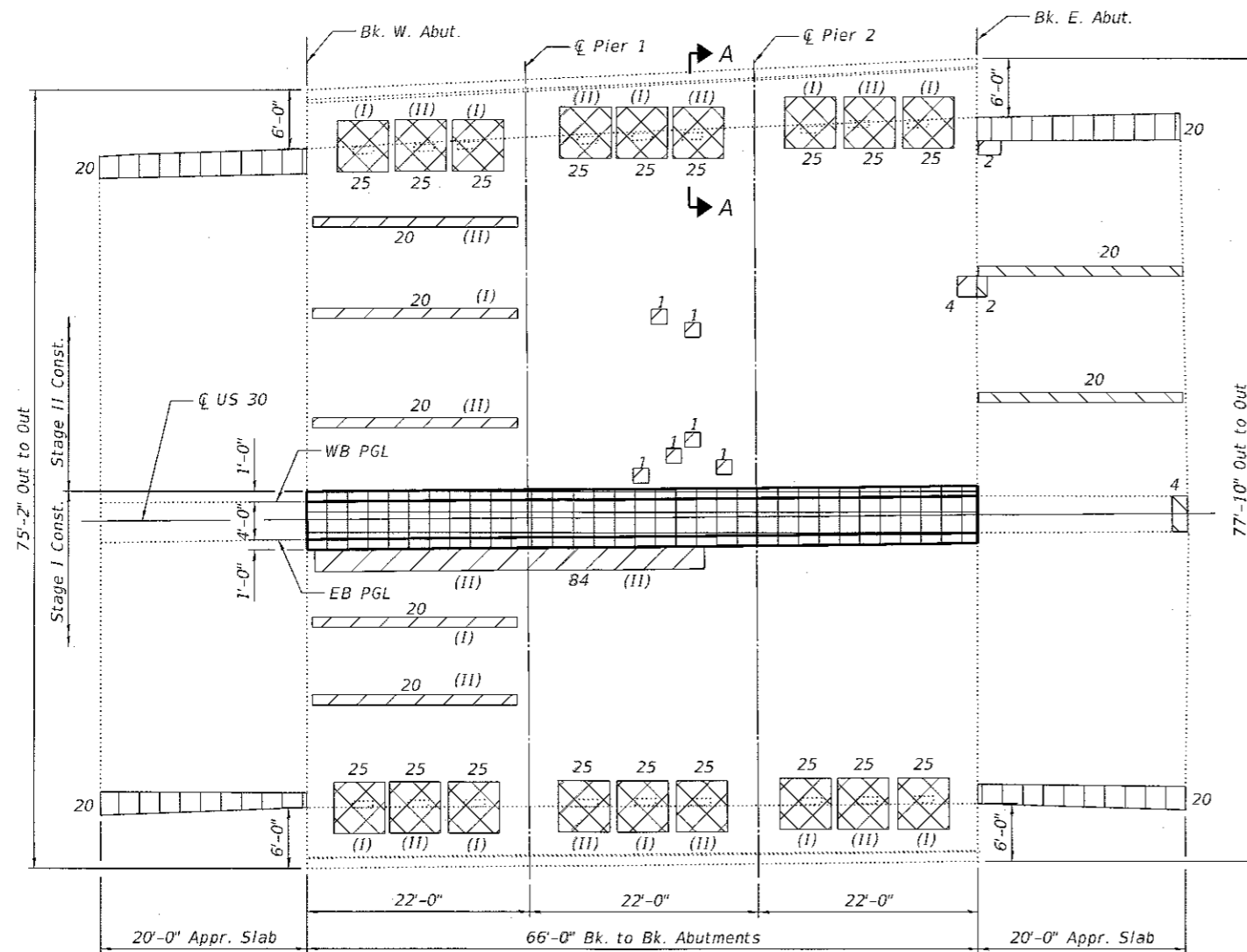
**TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
STRUCTURE NO. 016-0276**

SHEET 3 OF 6 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-4-3	COOK	32	23
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

NOTES

- Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- Longitudinal joint removal cost included with Concrete Removal.
- See Sheet 6 of 6 for Reinforcement Details in the Median.
- Protective Coat shall be applied on the proposed median.
- Construction of the Deck Slab Repair (Full Depth and Partial Depth) shall be performed in sequence I and II in each stage. Sequence II removal areas shall not be started until the new concrete in sequence I removal areas has reached a minimum of 650 psi modulus of rupture.



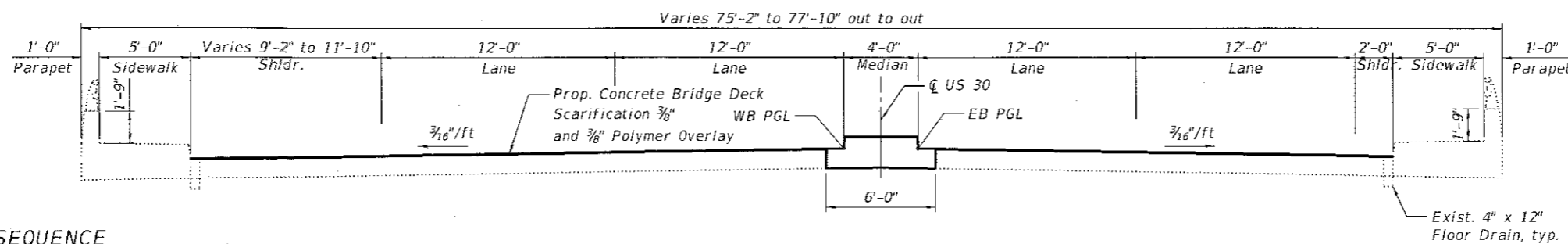
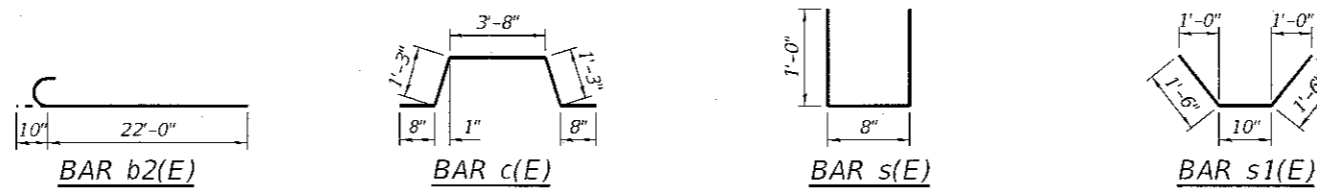
SECTION A-A

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	240	#5	4'-6"	—
b(E)	30	#5	18'-11"	—
b1(E)	15	#7	38'-6"	—
b2(E)	22	#7	22'-10"	—
b3(E)	11	#7	32'-4"	—
b4(E)	10	#5	34'-8"	—
c(E)	67	#4	7'-6"	—
s(E)	14	#4	2'-8"	—
s1(E)	14	#4	3'-10"	—
HMA Surface Course Mix D, N70		Ton	13	
HMA Surface Removal, 2"		Sq. Yd.	116	
Combination Curb and Gutter Removal		Foot	80	
Concrete Removal		Cu. Yd.	30.8	
Concrete Superstructure		Cu. Yd.	28.4	
Protective Coat		Sq. yd.	54	
Reinforcement Bars, Epoxy Coated		Pound	5420	
Mechanical Splicers		Each	240	
Combination Curb and Gutter, Type B-6.24 (Variable width Gutter Flag)		Foot	80	
Clean and Reseal Relief Joint		Foot	117	
Approach Slab Repair (Partial Depth)		Sq. Yd.	6	
Concrete Bridge Deck Scarification 3/8"		Sq. Yd.	597	
Bridge Deck Thin Polymer Overlay, 3/8"		Sq. Yd.	597	
Structural Repair of Concrete (Depth Equal to or less than 5")		Sq. Ft.	19	
Deck Slab Repair (Full Depth, Type II)		Sq. Yd.	50	
Deck Slab Repair (Partial)		Sq. Yd.	22	

LEGEND

- Partial Depth Patching (Quantity shown in Sq. Ft.)
- Full Depth Patching (Quantity shown in Sq. Ft.)
- Removal and Replacement of Curb and Gutter (Quantity shown in Feet)
- Approach Slab Repair (Partial Depth) (Quantity shown in Sq. Ft.)
- Median Removal and Replacement



CROSS SECTION
(Looking East)

DECK SLAB PATCHING SEQUENCE

- (I) = Sequence I
- (II) = Sequence II

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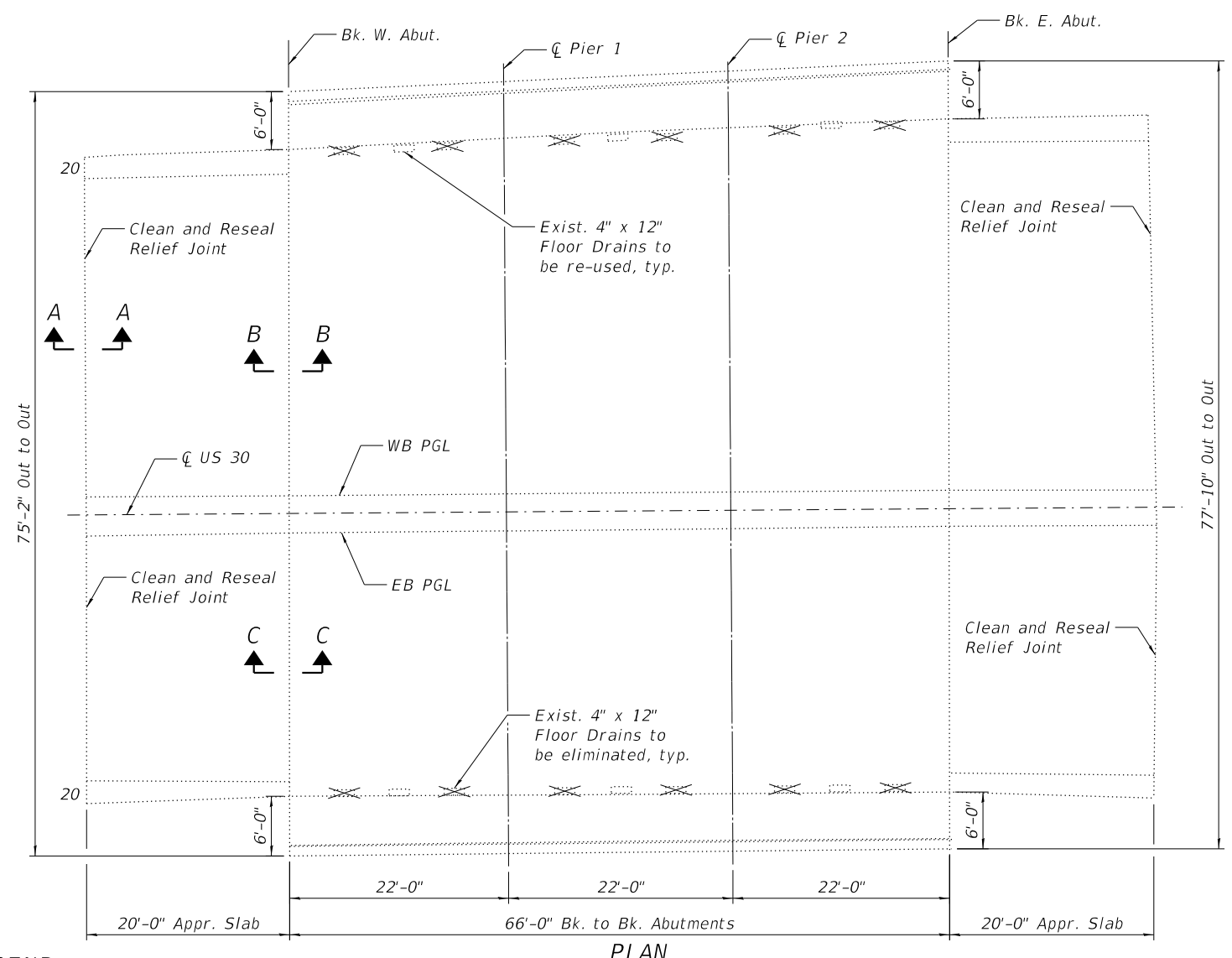
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK REPAIR DETAILS
STRUCTURE NO. 016-0276

SHEET 4 OF 6A SHEETS

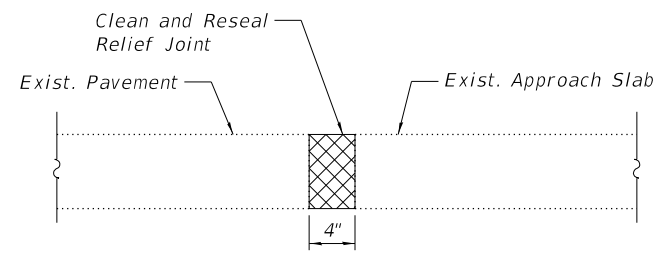
F.A.P. RTE. 353	SECTION 11-B-13	COUNTY COOK	TOTAL SHEETS 32	SHEET NO. 24
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60M93	

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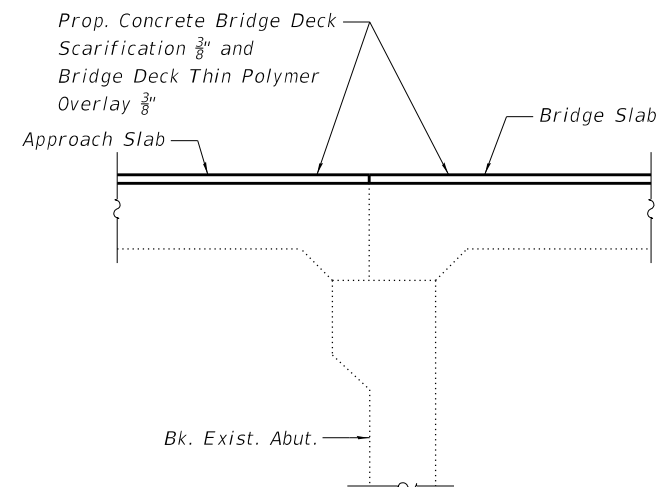
LEGEND

- Floor Drains to be Eliminated
- Structural Repair of Concrete (Depth equal to or less than 5")

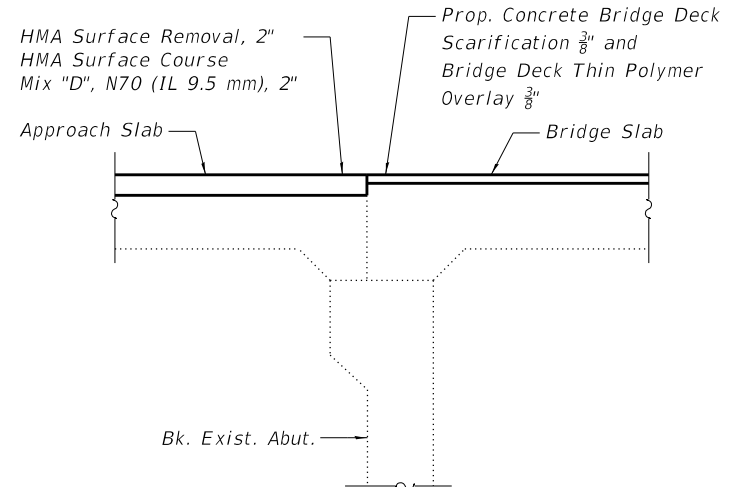


SECTION A-A

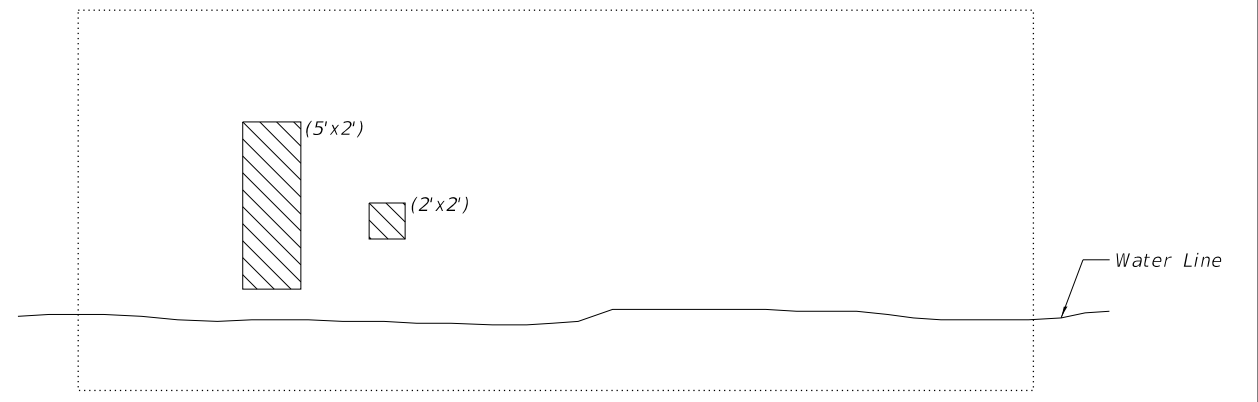
Note: Eastbound Approach Slabs are overlaid with \pm 2" of HMA.



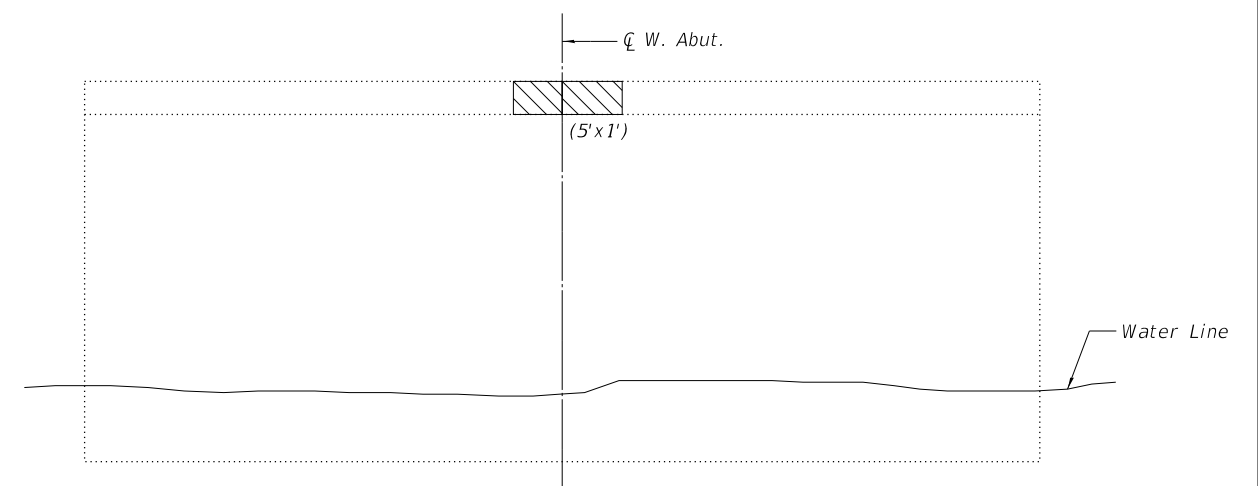
SECTION B-B



SECTION C-C



PIER 2
(Looking West)



WEST ABUTMENT
(Looking West)

NOTES

1. Areas shown for Structural Repair of Concrete are estimated. Actual area should be determined by the Engineer at the time of repair.
2. Removal of existing HMA pavement over the pavement relief joint at the end of the approach slabs shall not be paid separately. The cost shall be included with the cost of Relief Joint, 4".
3. Cost of removing the floor drains and re-using the floor drains is included with the cost of Deck Slab Repair (Full Depth, Type II).
4. See Sheet 4 of 6 for Bill of Materials.



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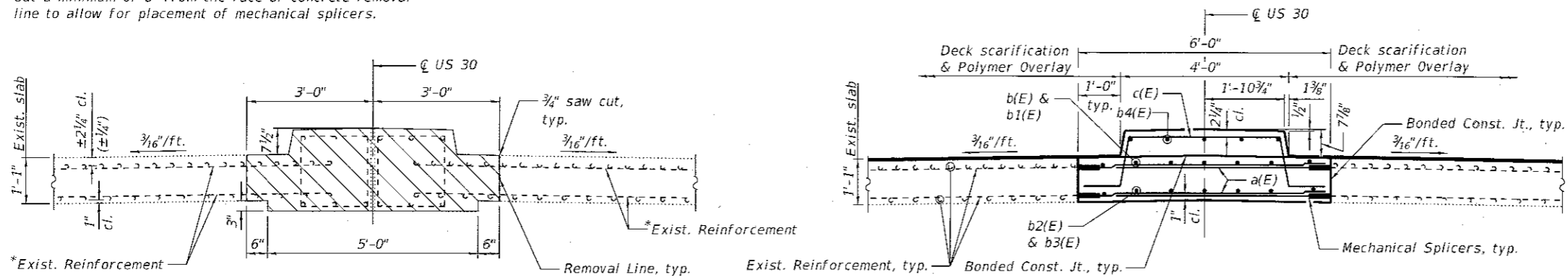
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS REPAIR DETAILS
STRUCTURE NO. 016-0276

SHEET 5 OF 6 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B4-3	COOK	32	25
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

* Cut a minimum of 6" from the face of concrete removal line to allow for placement of mechanical splicers.

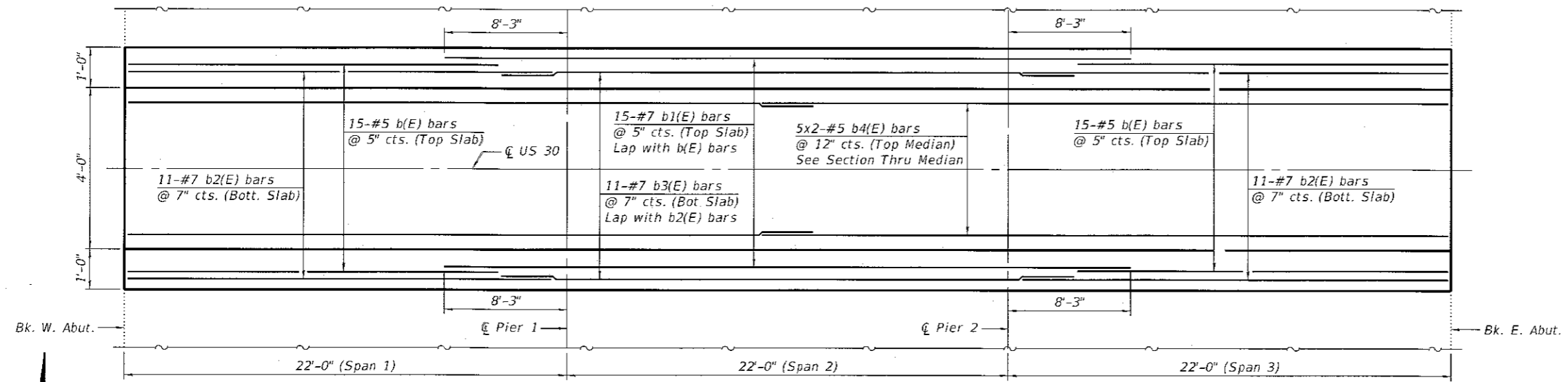


SECTION THRU EXISTING LONGITUDINAL JOINT & MEDIAN

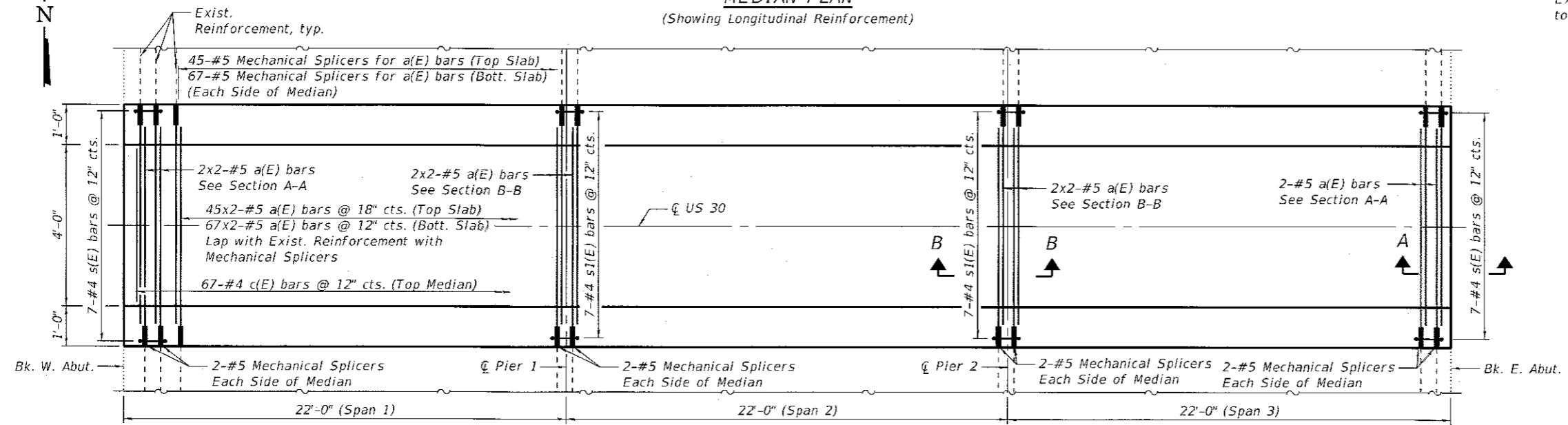
SECTION THRU MEDIAN

NOTES

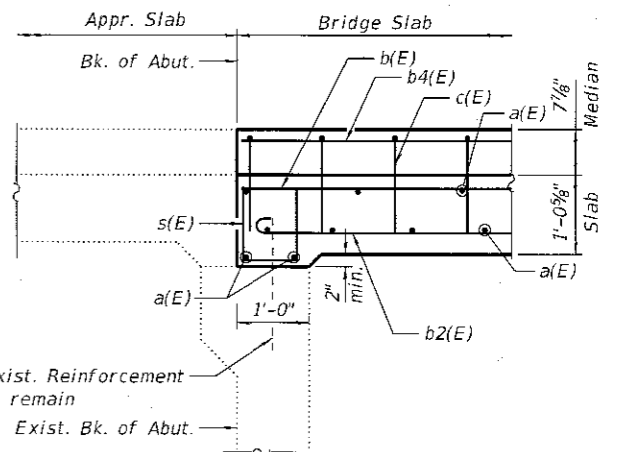
1. Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.
2. Existing Reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.
3. Longitudinal joint removal cost included with Concrete Removal.
4. Hatched areas indicate concrete section to be removed. Perimeters of Concrete Removal areas shall be saw cut $\frac{3}{4}$ " prior to the removal of the concrete.
5. Bar indicated thus 5x2-#8, etc. indicates 5 lines of bars with 2 lengths per line.
6. See Sheet 4 of 6 for the Bill of Materials.



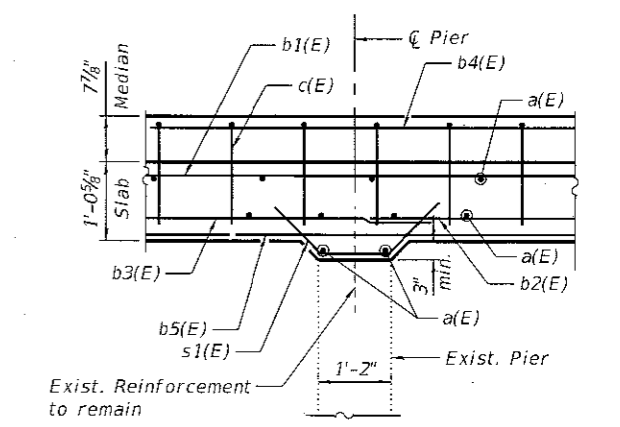
MEDIAN PLAN (Showing Longitudinal Reinforcement)



MEDIAN PLAN (Showing Transverse Reinforcement)



SECTION A-A



SECTION B-B

MIN. BAR LAP

- #5 Bar = 3'-4"
- #7 Bar = 5'-2"

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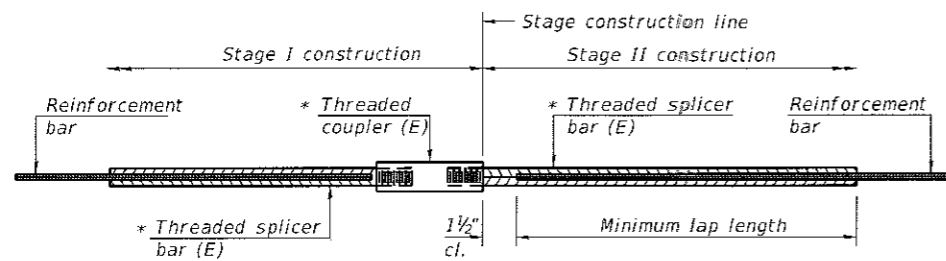


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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS
STRUCTURE NO. 016-0276
SHEET 6 OF 6A SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
363	11-B-13	COOK	32	26
CONTRACT NO. 60M93				
ILLINOIS FED. AID PROJECT				

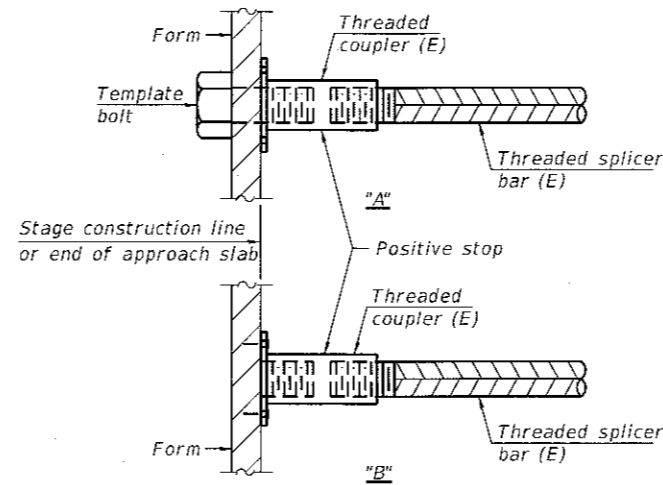


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

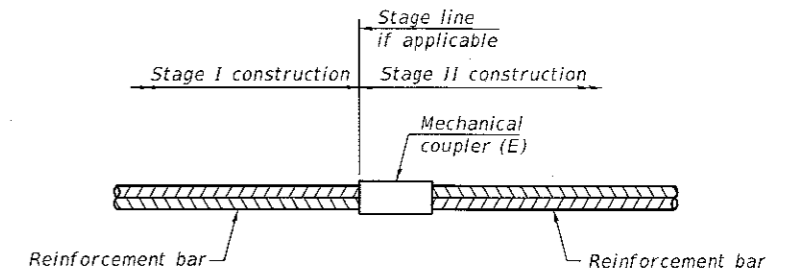
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length



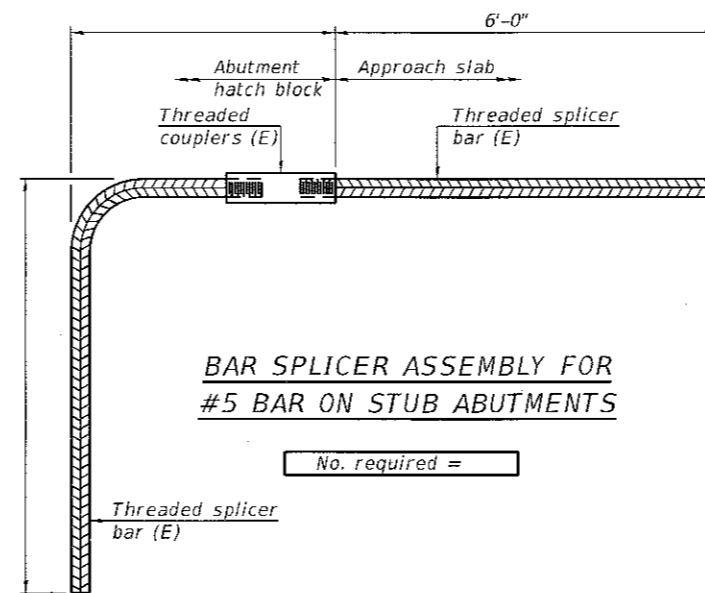
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required
Median	#5	240



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 STRUCTURE NO. 016-0276

SHEET 6A OF 6A SHEETS

F.A.P. RTE. 353	SECTION 11-B-1-3	COUNTY COOK	TOTAL SHEETS 32	SHEET NO. 26A
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60M93	

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

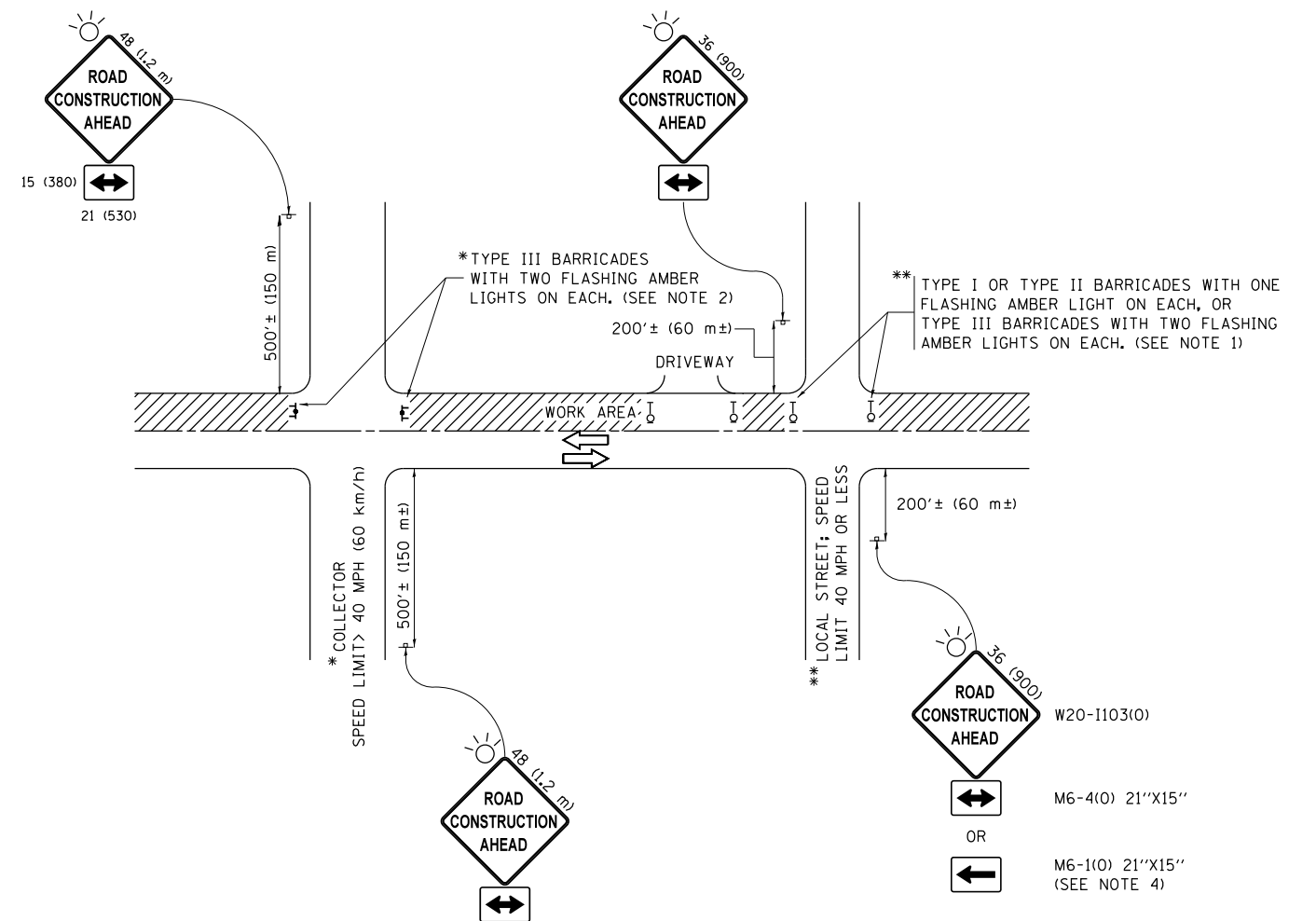
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw\work\p1dot\drivakosgn\0108315\bd24.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			353	11-B-I-3	COOK	32	27
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PLOT DATE = 12/15/2009	DATE - 03-11-94					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

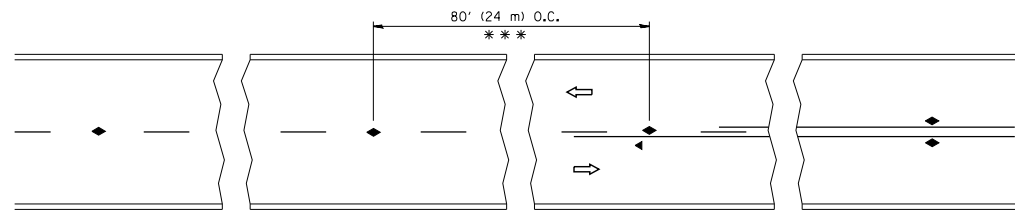
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	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

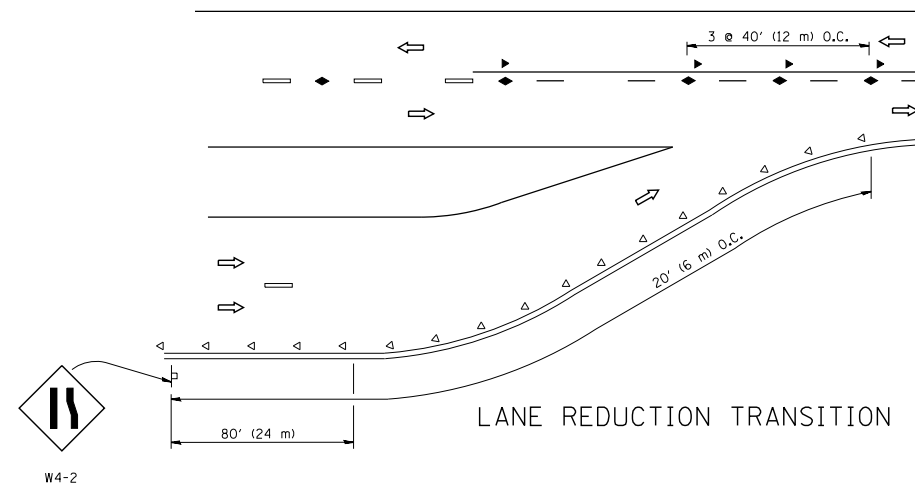
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10		CONTRACT NO. 60M93		
ILLINOIS FED. AID PROJECT				

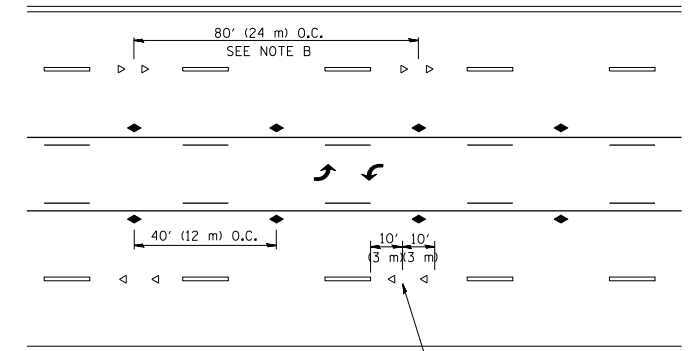


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

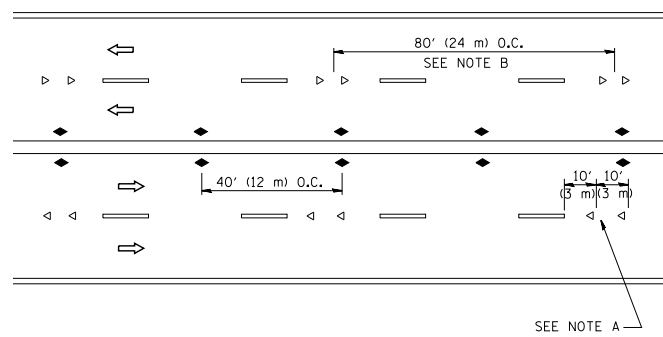
TWO-LANE/TWO-WAY



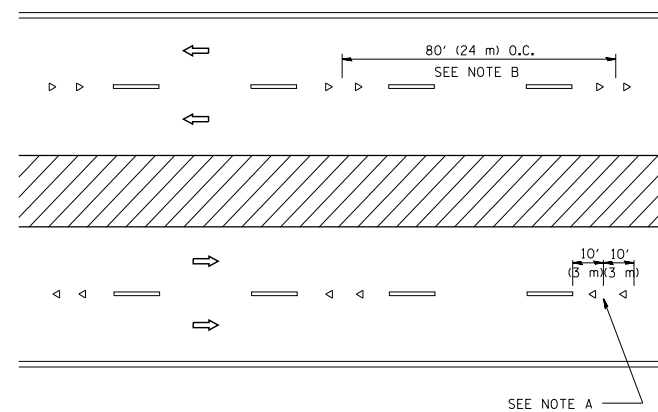
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

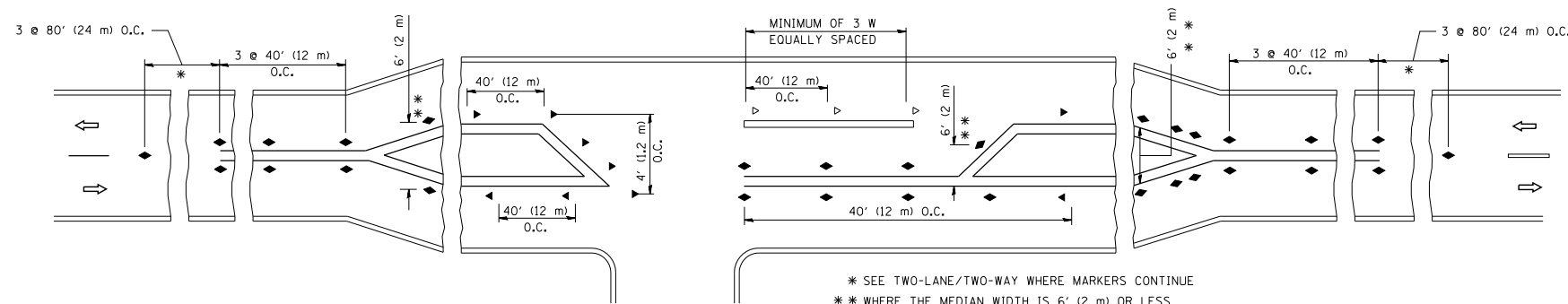
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

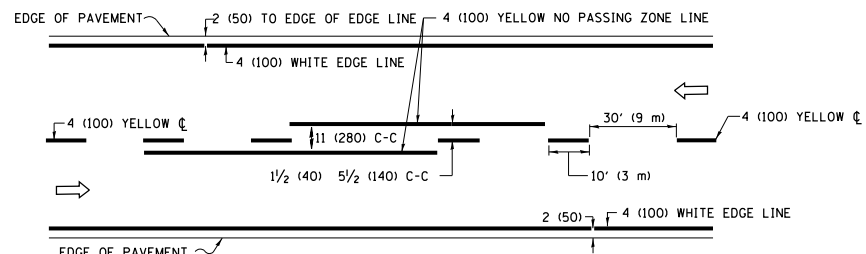
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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 3/2/2011	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

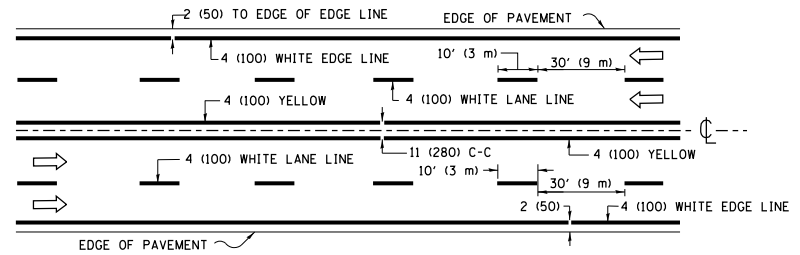
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

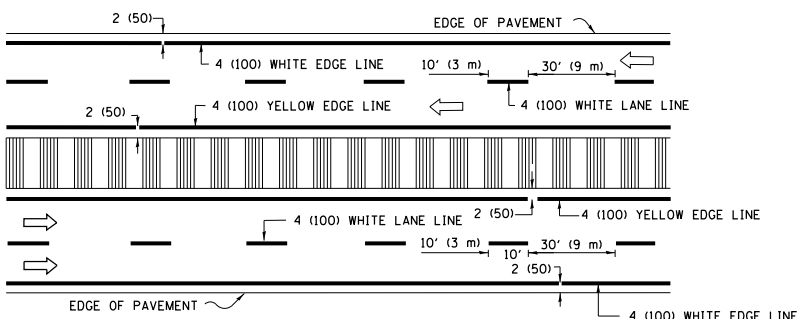
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-I-3	COOK	32	29
TC-11		CONTRACT NO. 60M93		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

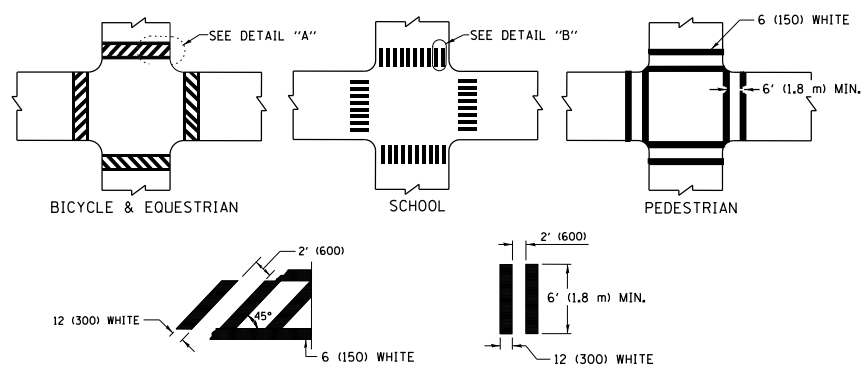


MULTI-LANE UNDIVIDED



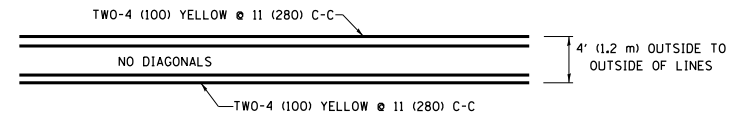
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

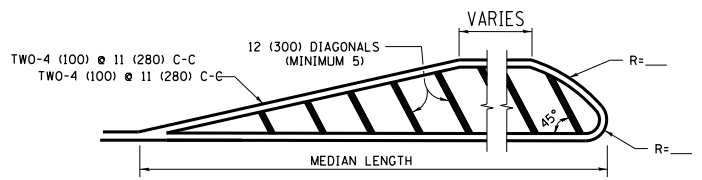


DETAIL "A" TYPICAL CROSSWALK MARKING

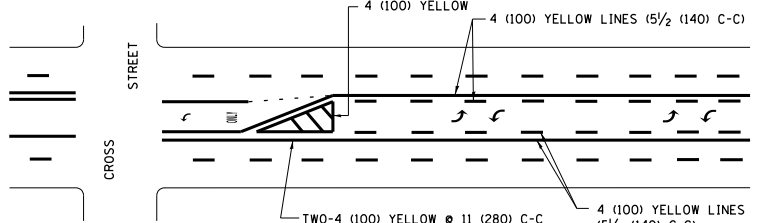
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



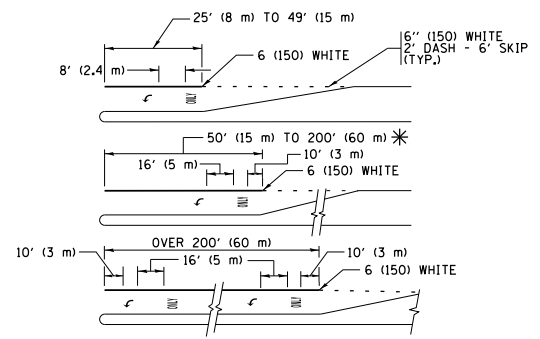
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

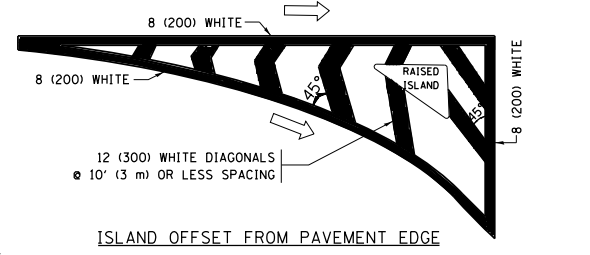


MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

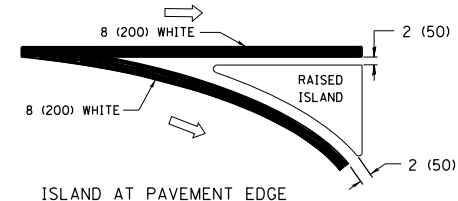


FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

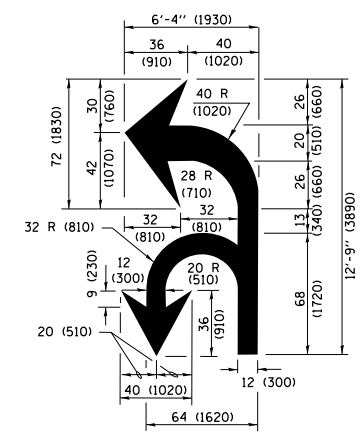
TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING



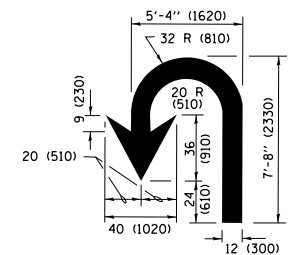
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\to13.dgn	USER NAME = l1eyso	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
Default	PLOT SCALE = 50.000' / in.	DRAWN -	REVISED - C. JUCIUS 07-01-13
	PLOT DATE = 6/23/2017	CHECKED -	REVISED - C. JUCIUS 12-21-15
		DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

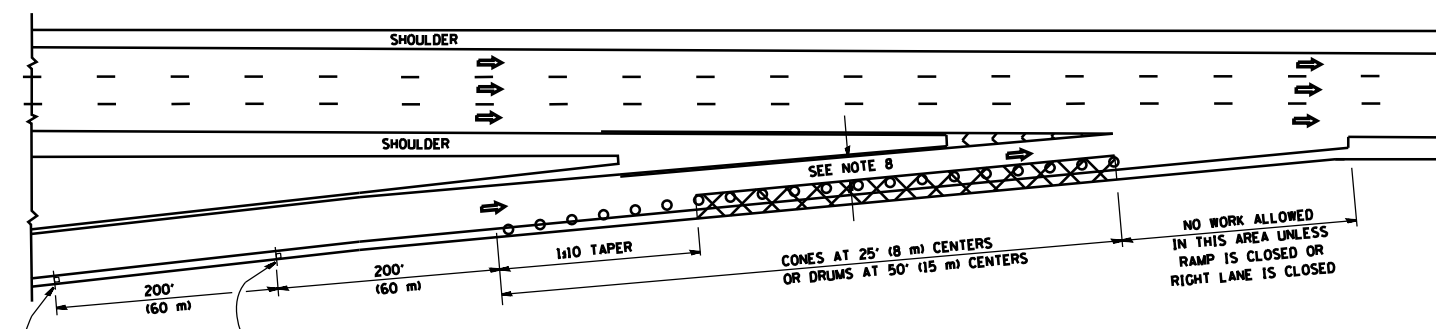
DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

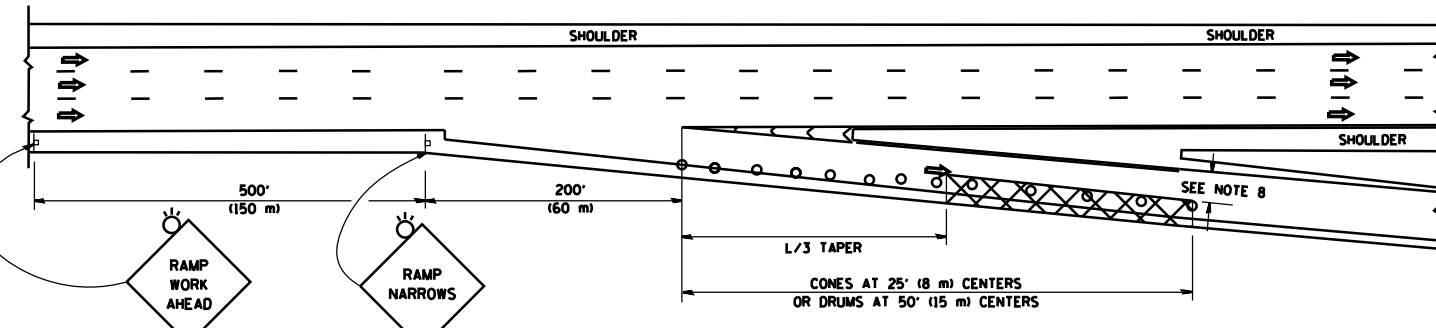
F.A.P. RTE. 353	SECTION 11-B-I-3	COUNTY COOK	TOTAL SHEETS 32	SHEET NO. 30
TC-13		CONTRACT NO. 60M93		
ILLINOIS FED. AID PROJECT				

PARTIAL RAMP CLOSURE DETAILS

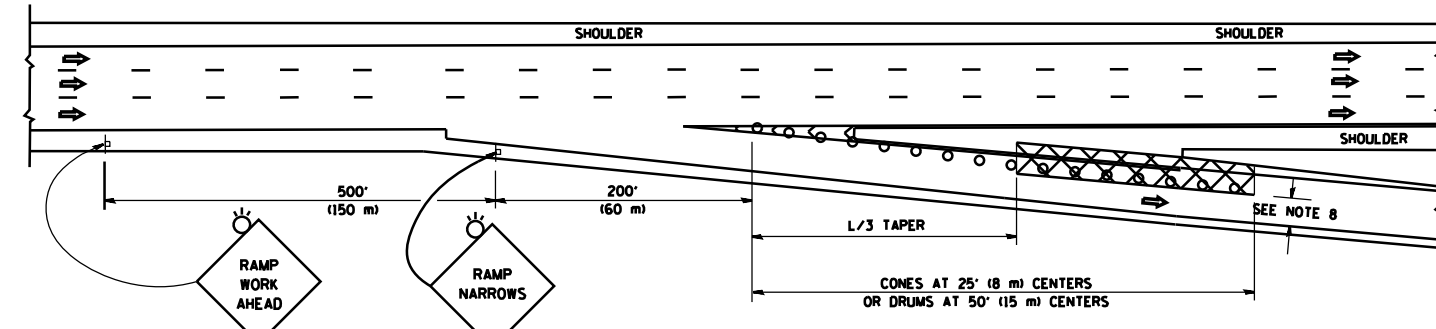
SHOULDER CLOSURE DETAILS



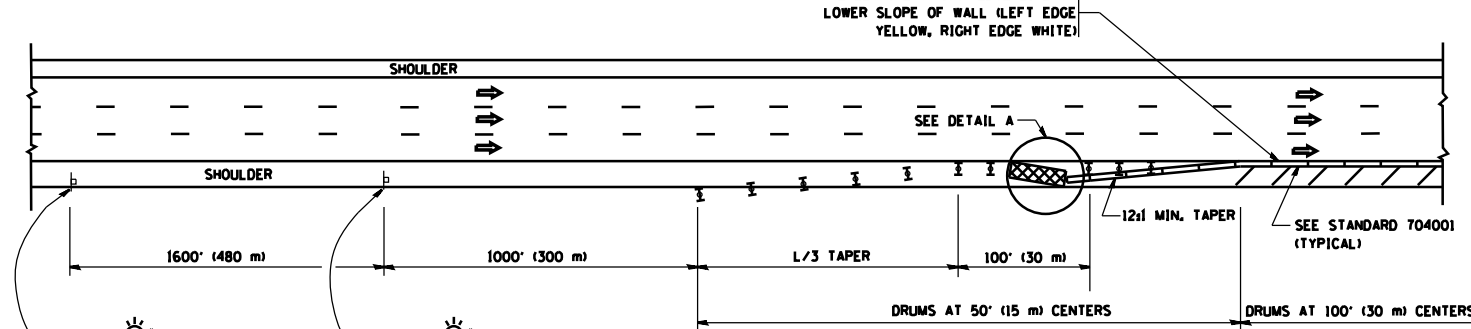
TYPICAL ENTRANCE RAMP



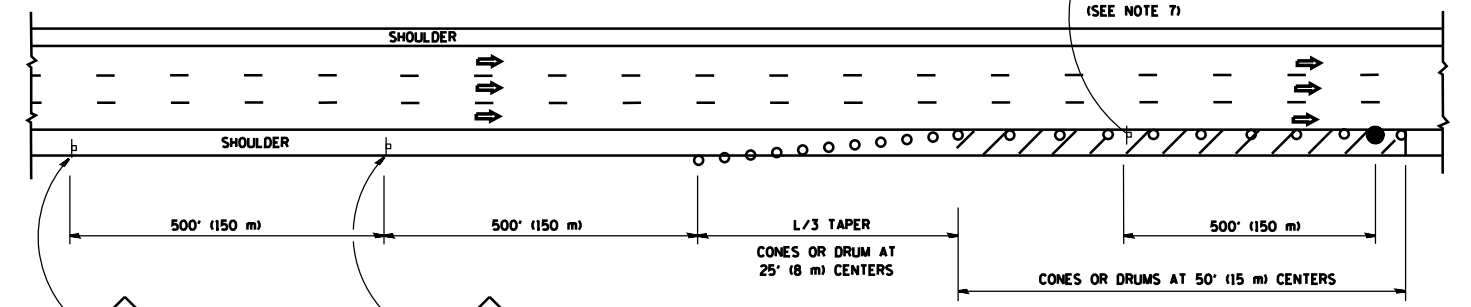
TYPICAL EXIT RAMP



TYPICAL EXIT RAMP



PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH
	$L = 0.65(WNS)$ $L = (WNS)$

W = WIDTH OF OFFSET IN FEET (METERS)
S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

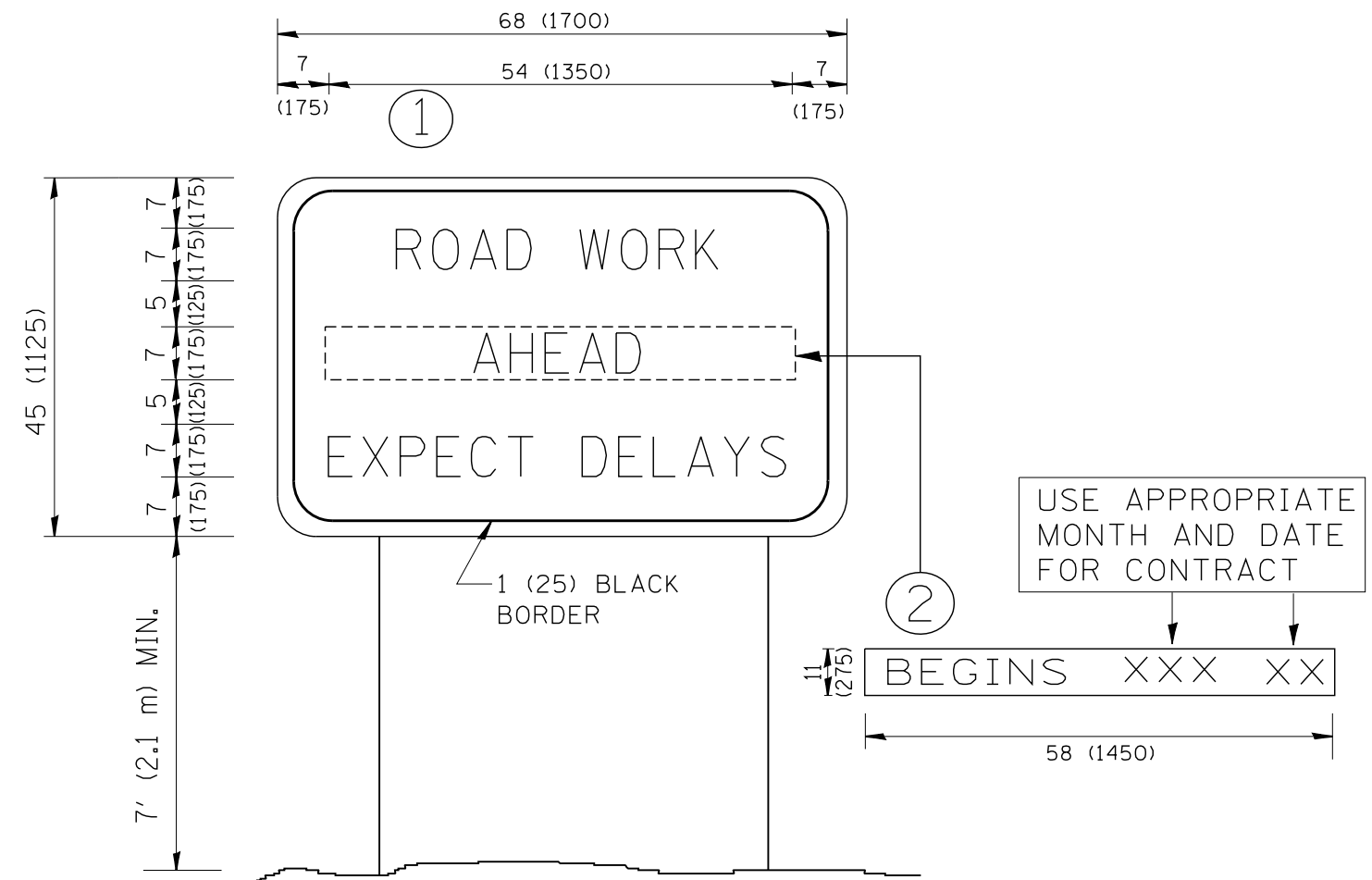
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350/MASH COMPLIANT.

DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = lveysa	DESIGNED -	REVISED - J.A.F. 12-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwwork\pwwork\lveysa\d0188315\cl17.dgn		DRAWN - D.W.S.	REVISED - S.P.B. 01-07		353	11-B-1-3	COOK	32	30A			
PLOT SCALE = 100.0000' / 1" =		CHECKED -	REVISED - S.P.B. 12-09		TC-17		CONTRACT NO. 60M93					
PLOT DATE = 4/17/2014		DATE - 11-96	REVISED - M.D. 06-13		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

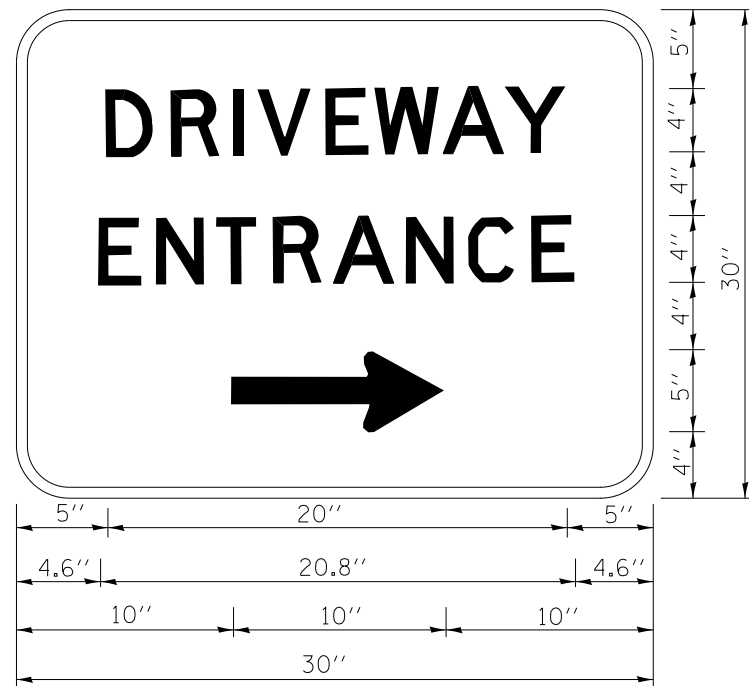
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		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 11-B-I-3	COUNTY COOK	TOTAL SHEETS 32	SHEET NO. 31
TC-22		CONTRACT NO. 60M93		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = gegl1enobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
ct:\pw\work\p1dot\gagl1enobt\d0108315\to26.dgn		DRAWN -	REVISED -
	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/13/2012	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	11-B-I-3	COOK	32	32
TC-26			CONTRACT NO. 60M93	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				