

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**PROPOSED  
HIGHWAY PLANS**

F.A.P. ROUTE 793 (US 40)  
SECTION 31,37,32RS-3, T-2B-R-2  
PROJECT NHPP-GYQC(909)  
RESURFACING - 3P &  
BRIDGE DECK OVERLAY - WATERPROOFING  
MADISON COUNTY

C-98-137-18

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
793	31,37,32RS-3, T-2B-R-2	MADISON	32	J
		ILLINOIS	CONTRACT NO. 76J98	

**INDEX OF SHEETS:**

1	COVER SHEET
2	HIGHWAY STANDARDS, GENERAL NOTES, & COMMITMENTS
3-8	SUMMARY OF QUANTITIES
9-11	TYPICAL SECTIONS
12-18	SCHEDULE OF QUANTITIES
19	LOCATION MAP
20-31	STRUCTURE PLANS
32	DETAILS

**TRAFFIC DATA:**

US 40 - WEST OF IL 162  
2017 ADT = 7,200 (ACTUAL)  
2018 ADT = 7,300 (ESTIMATED)  
2038 ADT = 9,200 (ESTIMATED)  
SU = 19.4% MU = 5.2%

US 40 - BETWEEN IL 162 AND TRIAD ROAD  
2017 ADT = 8,500 (ACTUAL)  
2018 ADT = 8,600 (ESTIMATED)  
2038 ADT = 10,900 (ESTIMATED)  
SU = 14.7% MU = 5.0%

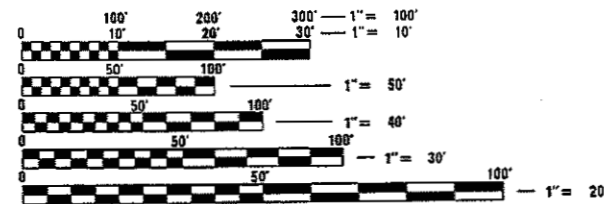
US 40 - EAST OF TRIAD ROAD  
2017 ADT = 8,650 (ACTUAL)  
2018 ADT = 8,750 (ESTIMATED)  
2038 ADT = 9,850 (ESTIMATED)  
SU = 12.4% MU = 4.1%

**FUNCTIONAL CLASSIFICATION:**

OTHER ARTERIAL

**TOWNSHIPS:**

JARVIS  
ST. JACOB

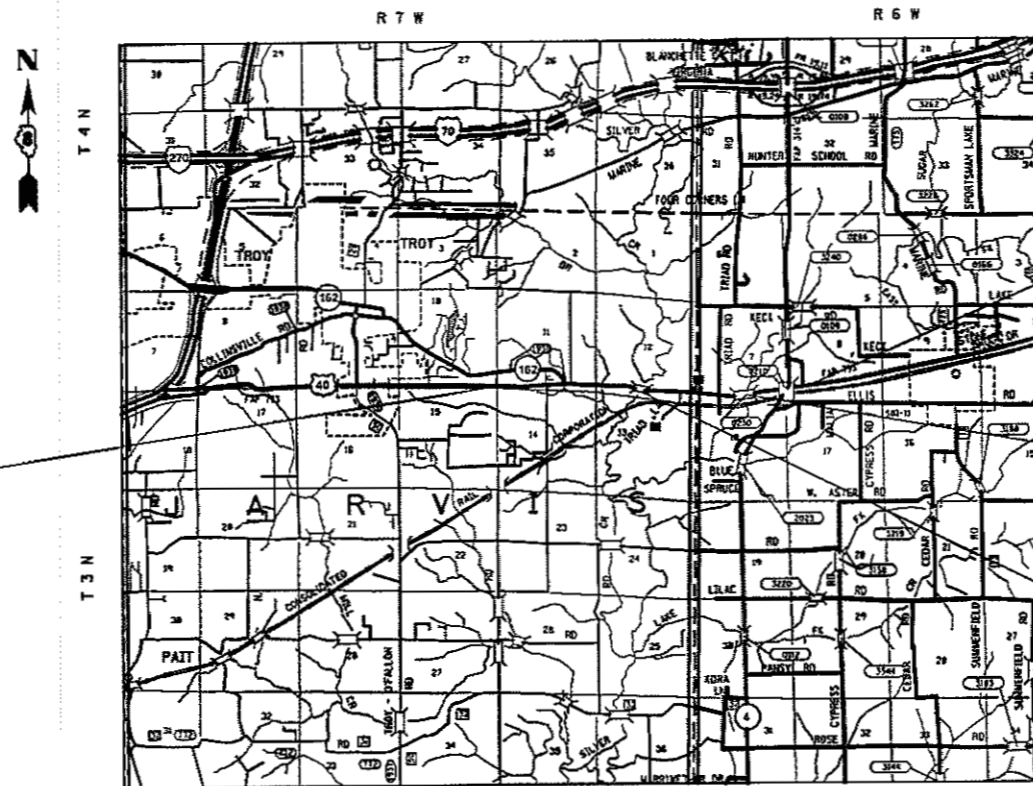


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: TIM PADGETT (618) 346-3325  
PROJECT MANAGER: PHIL FREIMUTH (618) 346-3194

CONTRACT NO. 76J98

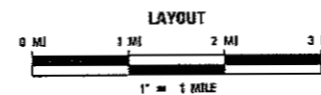


BEGIN PROJECT  
STA. 332 + 89.00  
LAT. 38.7176  
LON. -89.8524

END PROJECT  
STA. 488 + 77.00  
LAT. 38.7162  
LON. -89.7979

BRIDGE  
SN 060-0230  
STA. 458 + 50.00 TO  
STA. 460 + 62.00

BRIDGE OMISSION  
SN 060-0077  
STA. 396 + 67.46 TO  
STA. 401 + 88.54



GROSS LENGTH = 15,588.00 FT. = 2.952 MILE  
NET LENGTH = 15,066.92 FT. = 2.854 MILE



LOCATION OF SECTION INDICATED THIS: -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED *March 23, 2018*  
*Jeffrey Z. Ika*  
REGIONAL ENGINEER

*May 11, 2018*  
*Paul P. Chaf*  
ENGINEER OF DESIGN AND ENVIRONMENT

*May 11, 2018*  
*Paul P. Chaf*  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
406201-01	MAILBOX TURNOUT
420001-09	PAVEMENT JOINTS
442201-03	CLASS C AND D PATCHES
642001-02	SHOULDER RUMBLE STRIPS, 16 IN.
642006	SHOULDER RUMBLE STRIPS, 8 IN.
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701201-04	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS ≥ 45 MPH
701206-04	LANE CLOSURE, 2L, 2W, NIGHT ONLY, FOR SPEEDS ≥ 45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-04	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS ≥ 45 MPH
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701321-17	LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER
701336-06	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS ≥ 45 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701901-07	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

GENERAL NOTES

- ILLINOIS STATE LAW REQUIRES A 48-HOUR NOTICE BE GIVEN TO ALL UTILITIES WITHIN THE PROJECT AREA BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J.U.L.I.E. OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:

	AERIAL	BURIED
* AMEREN ILLINOIS - (GAS & ELECTRIC)	X	X
* AT&T ILLINOIS - (COMMUNICATIONS)	X	X
* AT&T CORPORATION - (COMMUNICATIONS)		X
* BOND MADISON WATER COMPANY - (WATER)		X
* CHARTER COMMUNICATIONS, INC. - (CABLE TV)	X	X
* DEPARTMENT OF CENTRAL MANAGEMENT SERVICES - (COMMUNICATIONS)		X
* CITY OF HIGHLAND - (COMMUNICATIONS)		X
* CITY OF HIGHLAND - (WATER & SANITARY SEWER)		X
* CITY OF HIGHLAND - (ELECTRIC)	X	
* HOME TELEPHONE COMPANY - (COMMUNICATIONS)	X	X
* SOUTHWESTERN ELECTRIC COOPERATIVE, INC. - (ELECTRIC)	X	
* VILLAGE OF ST. JACOB - (WATER & SANITARY SEWER)		X
* CITY OF TROY - (WATER & SANITARY SEWER)		X

MEMBERS OF J.U.L.I.E. CALL TOLL FREE (800) 892-0123 OR 811 AND ARE INDICATED BY \*. NON-J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.
- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- ALL AREAS DISTURBED FOR ANY REASON SHALL BE PERMANENTLY SEEDED AS DIRECTED BY THE ENGINEER. ALL AREAS DISTURBED BY THE CONTRACTOR OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE SEEDED AT THE CONTRACTOR'S EXPENSE.
- THE THICKNESS OF THE BITUMINOUS MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
- ALL TRAFFIC CONTROL DEVICES SHALL BE NEW OR "LIKE NEW" CONDITION AT THE BEGINNING OF THE PROJECT.
- FOUR CHANGEABLE MESSAGE BOARDS SHALL BE REQUIRED FOR THIS PROJECT. THEY SHALL BE PLACED TWO WEEKS PRIOR TO CONSTRUCTION. THE CHANGEABLE MESSAGE BOARDS SHALL BE PLACED AS DIRECTED BY THE ENGINEER.

GENERAL NOTES (CONT.)

- THE ILLINOIS DEPARTMENT OF TRANSPORTATION STRONGLY ENCOURAGES THE PRIME CONTRACTOR AND THEIR APPROVED SUB-CONTRACTORS TO HIRE MINORITY, WOMEN AND DISADVANTAGED INDIVIDUALS FROM ITS FEDERALLY FUNDED HIGHWAY CONSTRUCTION CAREERS TRAINING PROGRAM (HCCTP) TO HELP MEET WORKFORCE AND TRAINEE GOALS. THIS PROGRAM IS TRAINING MINORITIES, WOMEN AND DISADVANTAGED INDIVIDUALS IN HIGHWAY CONSTRUCTION-RELATED SKILLS, E.G., MATH FOR THE TRADES, JOB READINESS, TECHNICAL SKILLS COURSEWORK (CARPENTRY, CONCRETE FLATWORK, BLUEPRINT READING, SITE PLANS, SITE WORK, TOOLS USE, ETC.) AND OSHA 10 HOUR CERTIFICATION, TO PREPARE THEM FOR A CAREER IN THE HIGHWAY CONSTRUCTION TRADES. GRADUATES ARE WELL-TRAINED AND READY TO BECOME PRODUCTIVE ENTRY-LEVEL CONSTRUCTION WORKERS. PLEASE CONTACT THE DISTRICT 8 EEO OFFICE AT 618-346-3360 AND/OR THE HCCTP COORDINATOR AT 618-874-6528 TO LEARN MORE ABOUT THE PROGRAM AND FOR ASSISTANCE IN MEETING WORKFORCE AND TRAINEE GOALS.
- IF THE SURFACE REMOVAL ON THIS PROJECT PRODUCES A MILLED EDGE NEAR CENTERLINE GREATER THAN 1.5 INCHES BETWEEN ADJACENT OPEN LANES OF TRAFFIC, ONE OF THE FOLLOWING SHALL APPLY:
  - \* THE CONTRACTOR SHALL ORGANIZE THE WORK TO AVOID THE MILLED EDGE.
  - \* THE CONTRACTOR SHALL CONSTRUCT A TEMPORARY HOT MIX ASPHALT WEDGE ALONG THE MILLED EDGE.
  - \* THE CONTRACTOR SHALL CONSTRUCT A MILLED SLOPED EDGE (MINIMUM 1:3) ALONG THE MILLED EDGE.

THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST PER SQUARE YARD FOR HOT-MIX ASPHALT SURFACE REMOVAL OF THE DEPTH SPECIFIED.
- AN ESTIMATED QUANTITY OF 6,160 TONS OF CUTTINGS FROM THE HOT-MIX ASPHALT SURFACE REMOVAL OPERATION IS ANTICIPATED.
- THE PROPOSED PAVEMENT MARKING SHALL MATCH THE LOCATIONS OF THE EXISTING PAVEMENT MARKING, AS DIRECTED BY THE ENGINEER.
- MTD CAN OPERATE IN AN UNLOADED CONDITION ON SN 060-0077, 060-0230, & 060-2512.
- THE TEMPORARY RAMPS USED ON ALL SIDE STREETS AND ENTRANCES SHALL BE 2 FEET IN LENGTH SO AS NOT TO INTERFERE WITH THE THRU TRAFFIC LANES.
- SHOULDER WIDENING AND RESURFACING OPERATIONS SHALL EXTEND TO 1' AWAY FROM THE FACE OF THE EXISTING GUARDRAIL LOCATIONS, OR AS APPROVED BY ENGINEER.
- ANY EARTH EXCAVATION WITHIN THE PROJECT LIMITS RESULTING IN SURPLUS MATERIAL SHALL BE PLACED ON R.O.W. WITHIN THE PROJECT LIMITS AND SHALL NOT BE DISPOSED OF OFFSITE.
- A QUANTITY OF 875 FEET OF TEMPORARY PAVEMENT MARKING LINE 6" HAS BEEN INCLUDED IN THE PLANS FOR PAINTING THE BOTTOM 6" OF THE TEMPORARY CONCRETE BARRIER.

COMMITMENTS

NONE

MIXTURE REQUIREMENTS

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT.

MIXTURE USE	BRIDGE DECK	BINDER COURSE	SURFACE	LEVEL BINDER	PATCHING	INCIDENTAL	SHOULDERS ≥ 2.25"	SHOULDERS < 2.25"
AC/PG	SBS PG 76-22	PG 64-22	PG 64-22	PG 64-22	PG 64-22	PG 64-22	PG 64-22	PG 64-22
RAP % (MAX)	SEE SPECIAL PROVISION	SEE SPECIAL PROVISION	SEE SPECIAL PROVISION	SEE SPECIAL PROVISION	SEE SPECIAL PROVISION	SEE SPECIAL PROVISION	SEE SPECIAL PROVISION	SEE SPECIAL PROVISION
DESIGN AIR VOIDS	4.0% @ Ndes=70	4.0% @ Ndes=70	4.0% @ Ndes=70	4.0% @ Ndes=70	4.0% @ Ndes=70	4.0% @ Ndes=70	4.0% @ Ndes=30	4.0% @ Ndes=30
MIX COMPOSITION (GRADATION)	IL 9.5	IL 19.0	IL 9.5	IL 9.5FG	IL 19.0	IL 9.5	IL 19.0L	IL 9.5L
FRICTION AGG	MIXTURE "D"	MIXTURE "B"	MIXTURE "D"	MIXTURE "C"	MIXTURE "B"	MIXTURE "D"		
QUALITY MGMT PROGRAM	QC/QA	QC/QA	QCP	QCP	QC/QA	QC/QA	QC/QA	QC/QA

PLAN QUANTITIES FOR HMA SURFACE COURSE ITEMS ARE CALCULATED USING A UNIT WEIGHT OF 112 LB/SQ YD/IN.

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				ROADWAY	ROADWAY	RURAL
				0005 URBAN	0005 RURAL	0047 S. N. 060-0230
28100105	STONE RIPRAP, CLASS A3	SQ YD	3		3	
28100109	STONE RIPRAP, CLASS A5	SQ YD	1165			1165
28200200	FILTER FABRIC	SQ YD	1165			1165
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	604			604
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	1360			1360
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	31421.2	5913.7	25507.5	
40600637	LEVELING BINDER (MACHINE METHOD), 1L-9.5FG, N70	TON	1689.1	320.4	1368.7	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	302.2	31.1	271.1	
40600990	TEMPORARY RAMP	SQ YD	637.5	276.8	271.7	89
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	3385	639.7	2745.3	
40701841	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8"	SQ YD	604			604
40800029	BITUMINOUS MATERIALS (TACK COAT)	POUND	149	38	111	
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	42	11	31	

FILE NAME :	USER NAME = frcmshpd	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000 "/ in.		DATE -	REVISED -		SCALE: SHEET 1 OF 6 SHEETS STA. TO STA.			CONTRACT NO. 76J98				
Default	PLOT DATE = 3/22/2018				ILLINOIS FED. AID PROJECT							



CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FED 20% STATE		RURAL
				ROADWAY	ROADWAY	BRIDGE
				0005 URBAN	0005 RURAL	0047 S. N. 060-0230
50800515	BAR SPLICERS	EACH	24			24
52000110	PREFORMED JOINT STRIP SEAL	FOOT	85			85
60260100	INLETS TO BE ADJUSTED	EACH	4		4	
63300575	REMOVE AND REERECT RAIL ELEMENT OF EXISTING GUARDRAIL	FOOT	100			100
64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	27625.7	5084	22541.7	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	1	2	3
67100100	MOBILIZATION	L SUM	1	0.33	0.33	0.34
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	1			1
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1		0.5	0.5
70100455	TRAFFIC CONTROL AND PROTECTION, STANDARD 701206	L SUM	1		1	
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	0.33	0.33	0.34
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	0.25	0.75	
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1		

FILE NAME =	USER NAME = frcmdhpd	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 1/8" = 100.0000 ' / in.					REVISED -	SCALE:		SHEET 3 OF 6 SHEETS STA.			TO STA.		
PLOT DATE = 3/22/2018					DATE -			ILLINOIS FED. AID PROJECT			CONTRACT NO. 76J98		

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FED 20% STATE		RURAL BRIDGE
				ROADWAY 0005 URBAN	ROADWAY 0005 RURAL	S. N. 0047 060-0230
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1			1
70106700	TEMPORARY RUMBLE STRIPS	EACH	12			12
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	0.25	0.75	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	6846	937	5786	123
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	788	104	643	41
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	140		140	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	47457	7098	38680	1679
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	875			875
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	540		540	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	437.5			437.5
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	437.5			437.5
70600250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2			2
70600350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2			2
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	140		140	

14

\* SPECIALTY ITEM

FILE NAME =	USER NAME = freimthpd	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 3/22/2019								ILLINOIS FED. AID PROJECT			

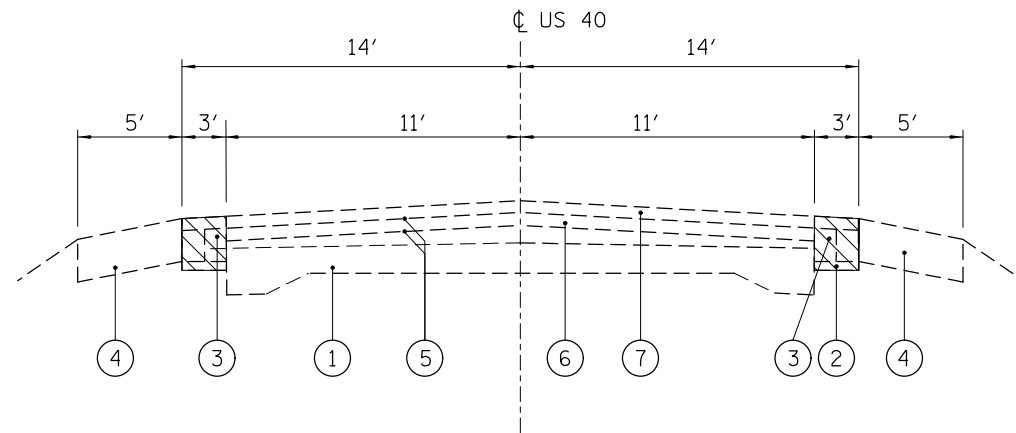


CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FED 20% STATE		RURAL BRIDGE
				ROADWAY 0005 URBAN	ROADWAY 0005 RURAL	S. N. 060-0230
Z0015802	PLUG EXISTING DECK DRAINS	EACH	4			4
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	10			10
Z0016200	DECK SLAB REPAIR (PARTIAL)	SQ YD	20			20
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	18423.5	2945	15478.5	
Z0034105	MATERIAL TRANSFER DEVICE	TON	5074.1	960.1	4114	
Z0038115	PORTLAND CEMENT CONCRETE SURFACE REMOVAL 1/2"	SQ YD	493.3		493.3	
* X2700006	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 4"	FOOT	477			477
Ø Z0076600	TRAINEES	HOUR	500	500		
Ø Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500	500		

\* SPECIALTY ITEM

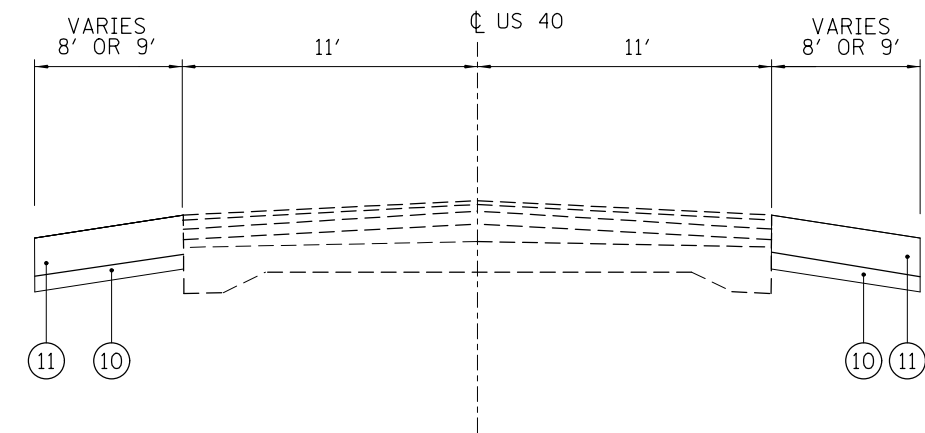
Ø 0042





**EXISTING TYPICAL SECTION**

STA. 456+78 TO STA. 457+80  
 STA. 461+77 TO STA. 462+34



**PROPOSED TYPICAL SECTION**

STA. 456+78 TO STA. 457+80  
 STA. 461+77 TO STA. 462+34

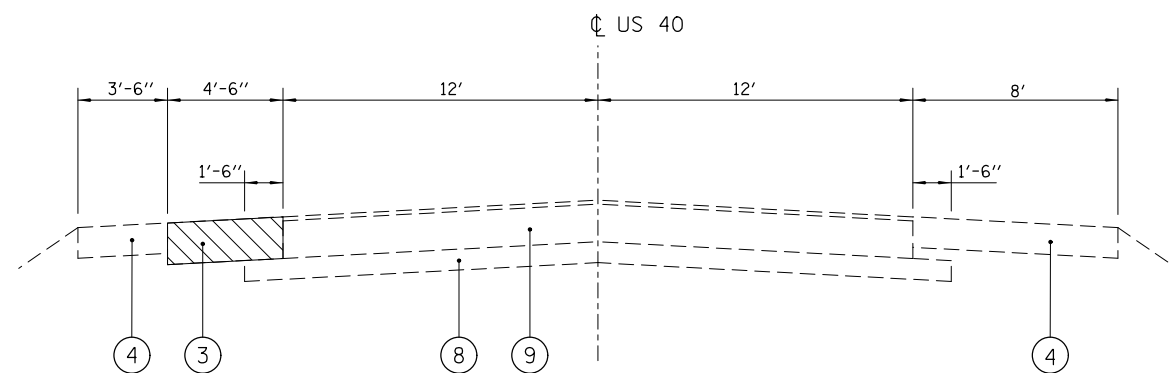
**LEGEND**

- ① EXISTING PCC PAVEMENT 10"-8"-10"
- ② EXISTING BITUMINOUS SHOULDER - 6" (LEVELING BINDER)
- ③ EXISTING BITUMINOUS SHOULDER - 6"
- ④ EXISTING AGGREGATE SHOULDERS - 6"
- ⑤ EXISTING BITUMINOUS AND AGGREGATE PRIME COATS
- ⑥ EXISTING LEVELING BINDER (MACHINE METHOD) SUPERPAVE, N90, 3/4"
- ⑦ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N90, 1 1/2"
- ⑧ EXISTING STABILIZED SUB-BASE, 4"
- ⑨ EXISTING PCC PAVEMENT, 8"
- ⑩ PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B, 4"
- ⑪ PROPOSED HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8"

PROPOSED PAVED SHOULDER REMOVAL

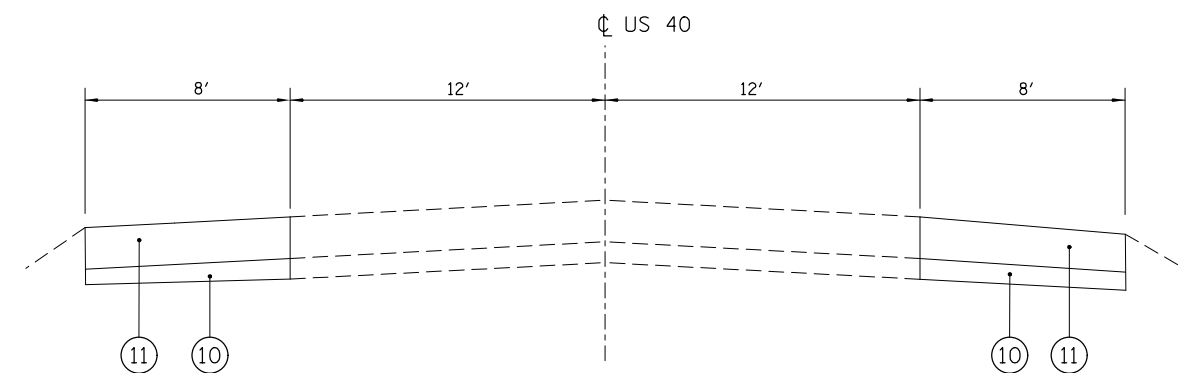
NOTE:

PROPOSED HMA PAVEMENT (FULL-DEPTH), 8"  
 CONSISTS OF: HMA BINDER COURSE 8"



**EXISTING TYPICAL SECTION**

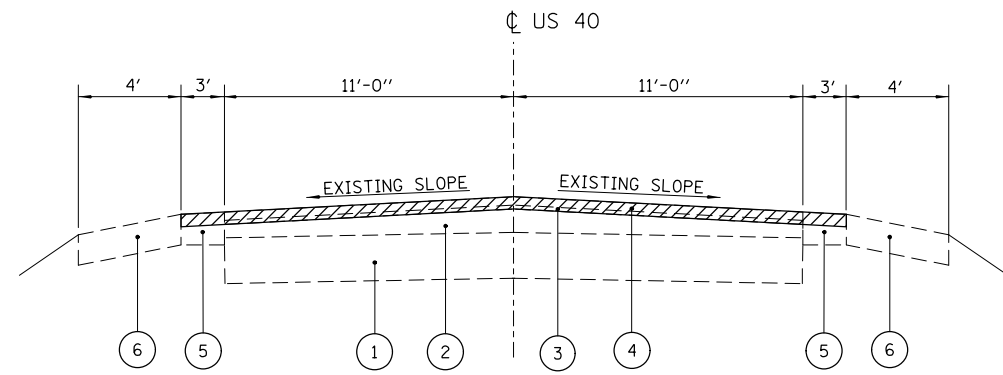
STA. 457+80 TO STA. 458+38  
 STA. 458+38 TO STA. 460+74 (EXISTING BRIDGE DECK & CONCRETE SHOULDERS)  
 STA. 460+74 TO STA. 461+77



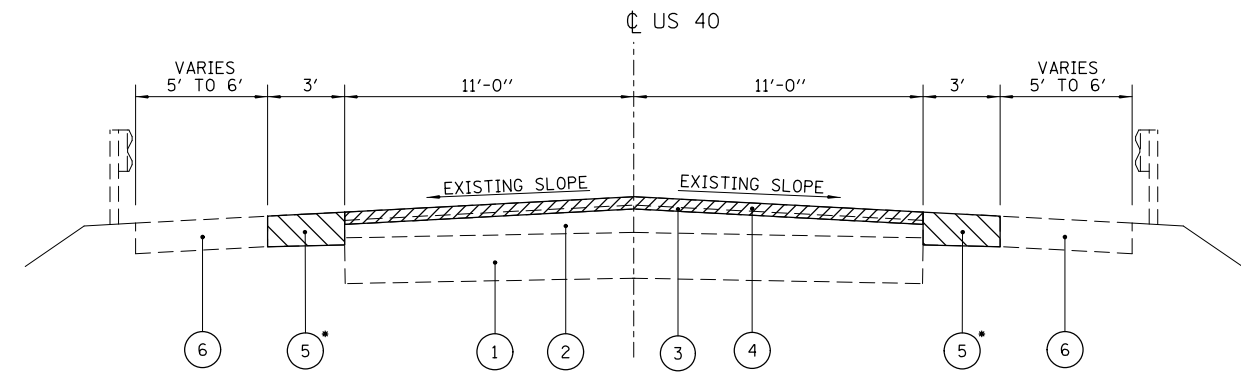
**PROPOSED TYPICAL SECTION**

STA. 457+80 TO STA. 458+38  
 STA. 458+38 TO STA. 460+74 (EXISTING BRIDGE DECK & CONCRETE SHOULDERS)  
 STA. 460+74 TO STA. 461+77

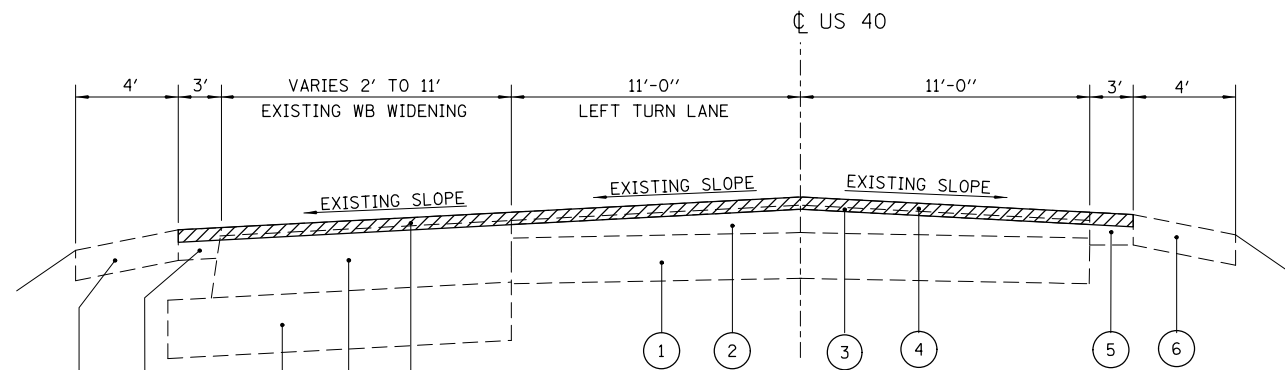
FILE NAME =	USER NAME = freimthpd	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SECTIONS (FOR TRAFFIC STAGING DURING BRIDGE CONSTRUCTION)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 8\Projects\0876\DRAWING\DATA\CADsheets\0876J98-sht-plan.dwg	PLotted	CHECKED -	REVISED -			793	31,37,32RS-3, T-2B-R-2	MADISON	32	9
Default	PLOT SCALE = 100.0000' / 1in.	DATE -	REVISED -			CONTRACT NO. 76J98				
	PLOT DATE = 3/22/2018					ILLINOIS FED. AID PROJECT				



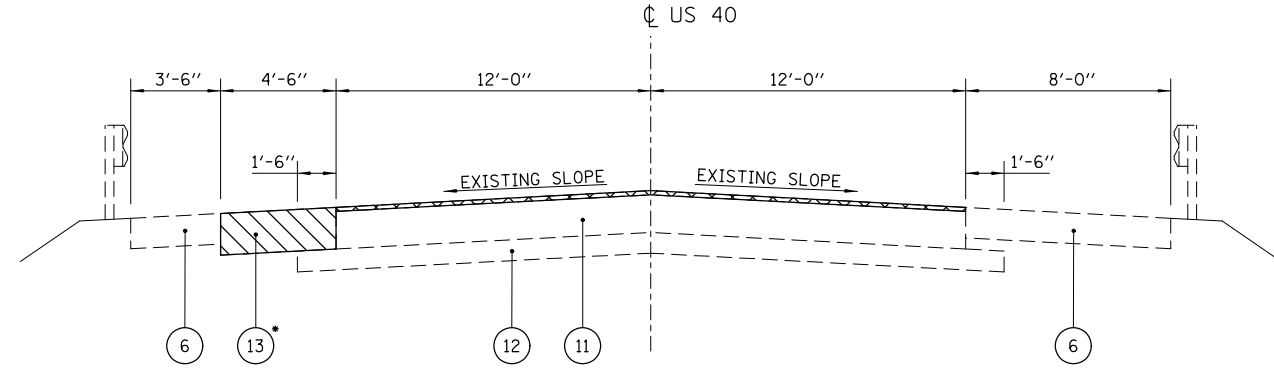
**EXISTING TYPICAL SECTION**  
 STA. 332+89.00 TO STA. 396+67.46  
 STA. 396+67.46 TO STA. 401+88.54 (BRIDGE OMISSION)  
 STA. 401+88.54 TO STA. 416+42.42  
 STA. 435+09.89 TO STA. 456+78.00  
 STA. 462+34.00 TO STA. 475+76.90  
 STA. 488+67.00 TO STA. 488+77.00



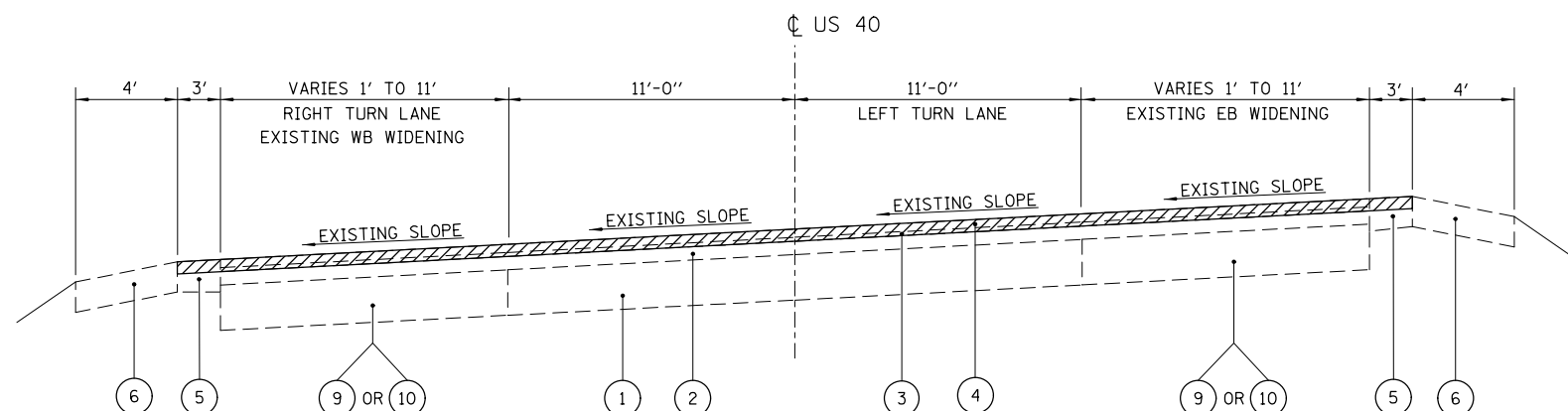
**EXISTING TYPICAL SECTION**  
 STA. 456+78.00 TO STA. 457+80.00  
 STA. 461+77.00 TO STA. 462+34.00  
 \* TO BE REMOVED FOR TRAFFIC STAGING DURING BRIDGE CONSTRUCTION.



**EXISTING TYPICAL SECTION**  
 (WIDENING AT TRIAD MIDDLE SCHOOL)  
 STA. 416+42.42 TO STA. 420+97.90 (WB TAPER FROM 2' TO 11')  
 STA. 420+97.90 TO STA. 430+69.89 (WB FULL WIDTH, 11')  
 STA. 430+69.89 TO STA. 435+09.89 (WB TAPER FROM 11' TO 2')



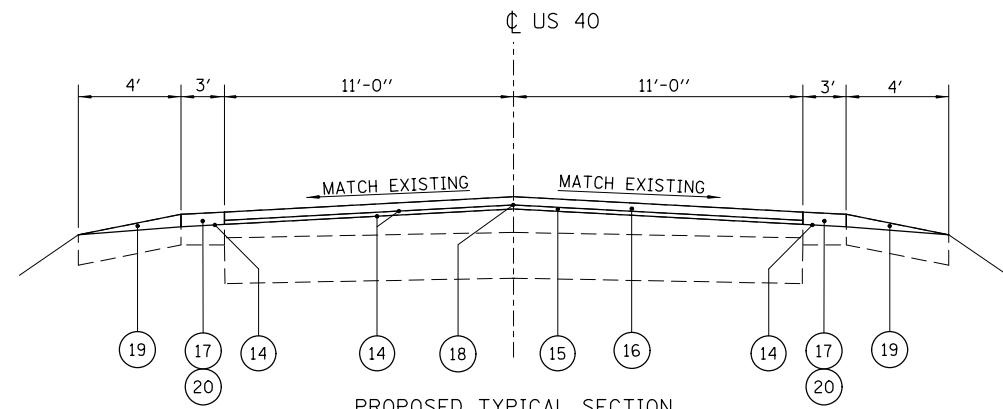
**EXISTING TYPICAL SECTION**  
 STA. 457+80.00 TO STA. 458+50.00  
 STA. 458+50.00 TO STA. 460+62.00 (SEE BRIDGE PLANS)  
 STA. 460+62.00 TO STA. 461+77.00  
 \* TO BE REMOVED FOR TRAFFIC STAGING DURING BRIDGE CONSTRUCTION.



**EXISTING TYPICAL SECTION**  
 (WIDENING AT IL 4 RAMP)  
 STA. 475+76.90 TO STA. 480+16.90 (EB TAPER 1' TO 11')  
 STA. 480+16.90 TO STA. 484+00.00 (EB FULL WIDTH, 11')  
 STA. 484+00.00 TO STA. 484+29.00 (EB BEGIN TAPER 11' TO 1' & WB FULL WIDTH, 11')  
 STA. 484+29.00 TO STA. 486+17.00 (CONT. EB TAPER & BEGIN WB TAPER 11' TO 1')  
 STA. 486+17.00 TO STA. 488+40.00 (END EB TAPER 11' TO 1' & CONT. WB TAPER)  
 STA. 488+40.00 TO STA. 488+67.00 (END WB TAPER 11' TO 1')

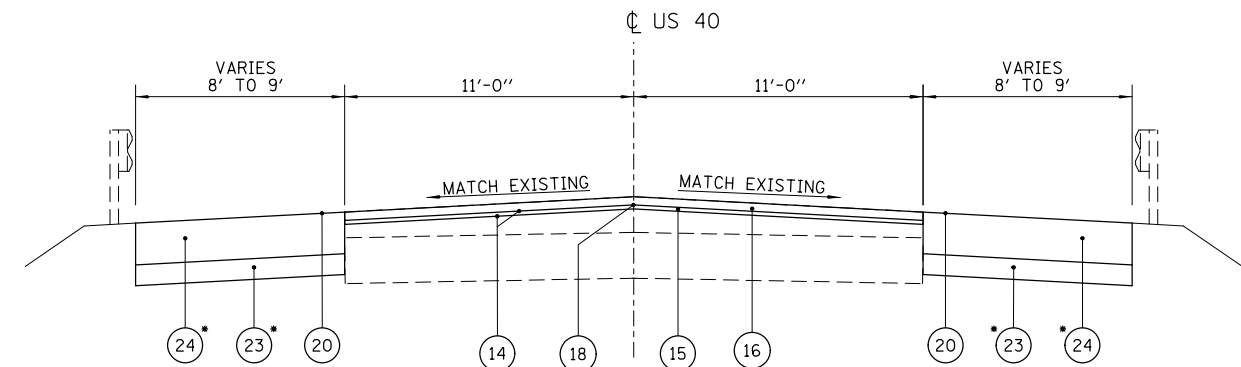
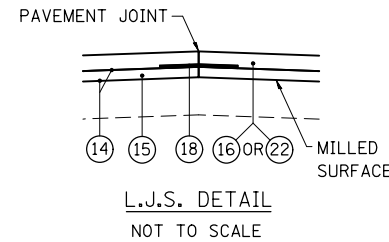
**LEGEND**

- ① EXISTING PCC PAVEMENT 10"-8"-10"
  - ② EXISTING BITUMINOUS RESURFACING
  - ③ EXISTING LEVELING BINDER, 3/4"
  - ④ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, 1/2"
  - ⑤ EXISTING BITUMINOUS SHOULDER - 6"
  - ⑥ EXISTING AGGREGATE SHOULDERS - 6"
  - ⑦ EXISTING LEVELING BINDER, 1 1/2"
  - ⑧ EXISTING AGGREGATE BASE COURSE, 12"
  - ⑨ EXISTING PCC BASE COURSE WIDENING, 9"
  - ⑩ EXISTING BITUMINOUS BASE COURSE, 9"
  - ⑪ EXISTING PCC PAVEMENT, 8"
  - ⑫ EXISTING STABILIZED SUB-BASE, 4"
  - ⑬ EXISTING BITUMINOUS WIDENING, 8"
  - ⑭ PROPOSED BITUMINOUS MATERIALS (TACK COAT)
  - ⑮ PROPOSED LEVELING BINDER (MM), IL-9.5FG, N70, 3/4"
  - ⑯ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
  - ⑰ PROPOSED HMA SHOULDER, 2 1/4"
  - ⑱ PROPOSED LONGITUDINAL JOINT SEALANT
  - ⑲ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
  - ⑳ PROPOSED SHOULDER RUMBLE STRIPS, 8 INCH
  - ㉑ PROPOSED CENTER LINE - RUMBLE STRIP - 16"
  - ㉒ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1 3/4"
  - ㉓ PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
  - ㉔ PROPOSED HMA PAVEMENT (FULL-DEPTH), 8"
  - ㉕ PROPOSED HMA SHOULDER, 2"
- HMA SURFACE REMOVAL, 2 1/4"  
 PCC SURFACE REMOVAL, 1/2"  
 PAVED SHOULDER REMOVAL



PROPOSED TYPICAL SECTION

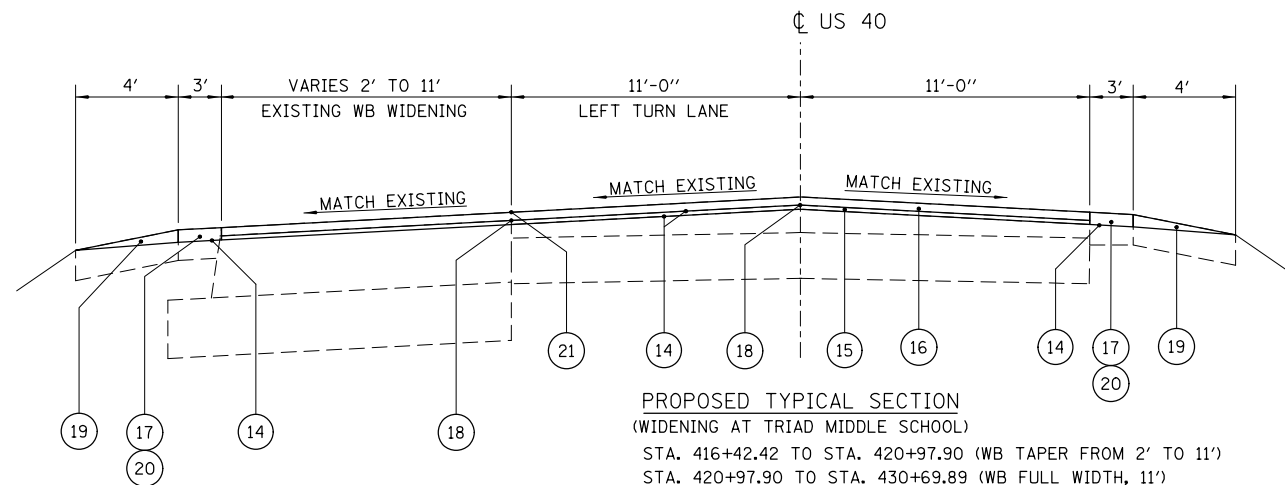
STA. 332+89.00 TO STA. 396+67.46  
 STA. 396+67.46 TO STA. 401+88.54 (BRIDGE OMISSION)  
 STA. 401+88.54 TO STA. 416+42.42  
 STA. 435+09.89 TO STA. 456+78.00  
 STA. 462+34.00 TO STA. 475+76.90  
 STA. 488+67.00 TO STA. 488+77.00



PROPOSED TYPICAL SECTION

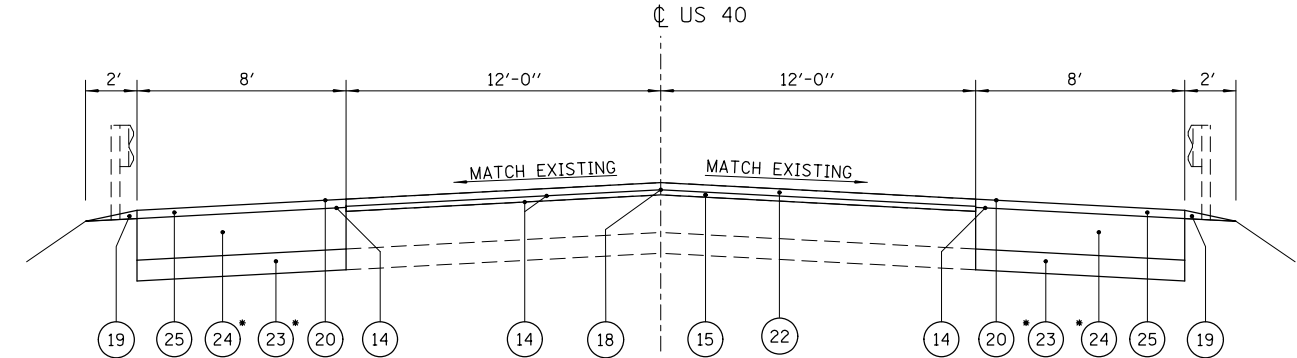
STA. 456+78.00 TO STA. 457+80.00  
 STA. 461+77.00 TO STA. 462+34.00

• TO BE CONSTRUCTED FOR TRAFFIC STAGING DURING BRIDGE CONSTRUCTION



PROPOSED TYPICAL SECTION  
 (WIDENING AT TRIAD MIDDLE SCHOOL)

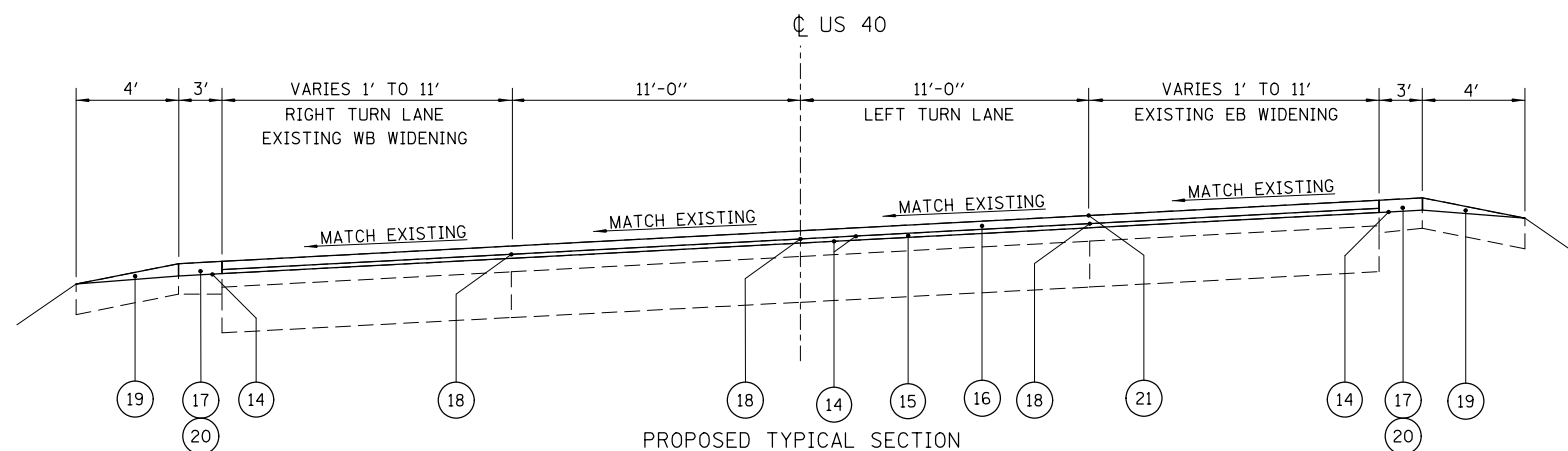
STA. 416+42.42 TO STA. 420+97.90 (WB TAPER FROM 2' TO 11')  
 STA. 420+97.90 TO STA. 430+69.89 (WB FULL WIDTH, 11')  
 STA. 430+69.89 TO STA. 435+09.89 (WB TAPER FROM 11' TO 2')



PROPOSED TYPICAL SECTION

STA. 457+80.00 TO STA. 458+50.00  
 STA. 458+50.00 TO STA. 460+62.00 (SEE BRIDGE PLANS)  
 STA. 460+62.00 TO STA. 461+77.00

• TO BE CONSTRUCTED FOR TRAFFIC STAGING DURING BRIDGE CONSTRUCTION



PROPOSED TYPICAL SECTION  
 (WIDENING AT IL 4 RAMP)

STA. 475+76.90 TO STA. 480+16.90 (EB TAPER 1' TO 11')  
 STA. 480+16.90 TO STA. 484+00.00 (EB FULL WIDTH, 11')  
 STA. 484+00.00 TO STA. 484+29.00 (EB BEGIN TAPER 11' TO 1' & WB FULL WIDTH, 11')  
 STA. 484+29.00 TO STA. 486+17.00 (CONT. EB TAPER & BEGIN WB TAPER 11' TO 1')  
 STA. 486+17.00 TO STA. 488+40.00 (END EB TAPER 11' TO 1' & CONT. WB TAPER)  
 STA. 488+40.00 TO STA. 488+67.00 (END WB TAPER 11' TO 1')

LEGEND

- |   |   |
|---|---|
| ① EXISTING PCC PAVEMENT 10"-8"-10"                    | ⑭ PROPOSED BITUMINOUS MATERIALS (TACK COAT)               |
| ② EXISTING BITUMINOUS RESURFACING                     | ⑮ PROPOSED LEVELING BINDER (MM), IL-9.5FG, N70, 3/4"      |
| ③ EXISTING LEVELING BINDER, 3/4"                      | ⑯ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1 1/2"       |
| ④ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, 1 1/2" | ⑰ PROPOSED HMA SHOULDER, 2 1/4"                           |
| ⑤ EXISTING BITUMINOUS SHOULDER - 6"                   | ⑱ PROPOSED LONGITUDINAL JOINT SEALANT (SEE L.J.S. DETAIL) |
| ⑥ EXISTING AGGREGATE SHOULDERS - 6"                   | ⑲ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B               |
| ⑦ EXISTING LEVELING BINDER, 1 1/2"                    | ⑳ PROPOSED SHOULDER RUMBLE STRIPS, 8 INCH                 |
| ⑧ EXISTING AGGREGATE BASE COURSE, 12"                 | ㉑ PROPOSED CENTER LINE - RUMBLE STRIP - 16"               |
| ⑨ EXISTING PCC BASE COURSE WIDENING, 9"               | ㉒ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1 3/4"       |
| ⑩ EXISTING BITUMINOUS BASE COURSE, 9"                 | ㉓ PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"           |
| ⑪ EXISTING PCC PAVEMENT, 8"                           | ㉔ PROPOSED HMA PAVEMENT (FULL-DEPTH), 8"                  |
| ⑫ EXISTING STABILIZED SUB-BASE, 4"                    | ㉕ PROPOSED HMA SHOULDER, 2"                               |
| ⑬ EXISTING BITUMINOUS WIDENING, 8"                    |   |
- HMA SURFACE REMOVAL, 2 1/4"  
 PCC SURFACE REMOVAL, 1/2"  
 PAVED SHOULDER REMOVAL

FILE NAME =	USER NAME = freimthpd	DESIGNED -	REVISED -
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 8\Projects\0876\DRAWING\DATA\60\Sheets\0876J98-sht\plan.dwg		CHECKED -	REVISED -
Default	PLOT DATE = 3/22/2018	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS  
 (FOR 3P RESURFACING)

SCALE: SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
793	31,37,32RS-3, T-2B-R-2	MADISON	32	11
CONTRACT NO. 76J98				
ILLINOIS FED. AID PROJECT				

STAGING SCHEDULE									
LOCATION	TEMPORARY CONCRETE BARRIER	RELOCATE TEMPORARY CONCRETE BARRIER	TEMPORARY BRIDGE TRAFFIC SIGNALS	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	TEMPORARY PAVEMENT MARKING - LINE 6"	TEMPORARY PAVEMENT MARKING REMOVAL	TEMPORARY RUMBLE STRIPS	REMOVE AND REERECT RAIL ELEMENT OF EXISTING GUARDRAIL
US 40 OVER E FORK SILVER CREEK	FOOT	FOOT	EACH	EACH	EACH	FOOT	SQ FT	EACH	FOOT
STAGE I	437.5		1	2		437.5	423.3	6	50
STAGE II		437.5			2	437.5	423.3	6	50
<b>TOTAL</b>	<b>437.5</b>	<b>437.5</b>	<b>1</b>	<b>2</b>	<b>2</b>	<b>875</b>	<b>847</b>	<b>12</b>	<b>100</b>

TEMPORARY PAVEMENT MARKING SCHEDULE						
LOCATION			SHORT TERM PAVEMENT MARKING	SHORT TERM PAVEMENT MARKING REMOVAL	TEMPORARY PAVEMENT MARKING LINE - 4"	PAVEMENT MARKING REMOVAL WATER BLASTING
STATION	TO	STATION	FOOT	SQ FT	FOOT	SQ FT
455+83	TO	458+50	42.70	14.23	600.75	200.25
458+50	TO	460+62	37.20	12.40	477.00	159.00
460+62	TO	463+29	42.70	14.23	600.75	200.25
<b>TOTAL</b>			<b>123</b>	<b>41</b>	<b>1,679</b>	<b>560</b>

PAVEMENT MARKING SCHEDULE							
LOCATION			PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 4"		GROOVING FOR RECESSED PAVEMENT MARKING, 5"	RAISED REFLECTIVE PAVEMENT MARKING REMOVAL	
			SOLID WHITE EDGE LINE	YELLOW SKIP DASH LINE			
STATION	TO	STATION	FOOT		FOOT	FOOT	EACH
458+50	TO	460+62	212	212	53.00	477	3
<b>SUB-TOTAL</b>			<b>212</b>	<b>212</b>	<b>53</b>	<b>477</b>	<b>3</b>
<b>TOTAL</b>			<b>477</b>		<b>477</b>	<b>477</b>	<b>3</b>

SHOULDER WIDENING SCHEDULE							
LOCATION			PAVED SHOULDER REMOVAL	SUBBASE GRANULAR MATERIAL, TYPE B 4"	BITUMINOUS MATERIALS (PRIME COAT)	BITUMINOUS MATERIALS (TACK COAT)	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8"
STATION	TO	STATION	SQ YD	SQ YD	POUND	POUND	SQ YD
456+78	TO	457+80	68.00	204.00	459.00	45.90	204.00
457+80	TO	458+38	29.00	103.11	232.00	23.20	103.11
458+38	TO	460+74	EXISTING BRIDGE DECK & CONCRETE SHOULDERS				
460+74	TO	461+77	51.50	183.11	412.00	41.20	183.11
461+77	TO	462+34	38.00	114.00	256.50	25.65	114.00
<b>SUB TOTAL</b>			<b>186.50</b>	<b>604.22</b>	<b>1359.50</b>	<b>135.95</b>	<b>604.22</b>
<b>TOTAL</b>			<b>187</b>	<b>604</b>	<b>1360</b>	<b>136</b>	<b>604</b>

TEMPORARY RAMPS					
LOCATION			WIDTH	LENGTH	AREA
STATION	TO	STATION	FOOT	FOOT	SQ YD
SN 060-0230 WEST APPROACH					
458+40.00	TO	458+50.00	40	10	44.44
SN 060-0230 EAST APPROACH					
460+62.00	TO	460+72.00	40	10	44.44
<b>TOTAL</b>			<b>89</b>		

NOTE: THE EARTH EXCAVATION REQUIRED FOR THE CONSTRUCTION OF THE PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B, 4" AND HMA PAVEMENT FULL DEPTH, 8" SHALL BE INCLUDED IN THE COST PER SQ YD FOR SUBBASE GRANULAR MATERIAL, TYPE B, 4".

MAINLINE PAVEMENT SCHEDULE															
LOCATION			PROPOSED RDWAY WIDTH			LENGTH (FT)	PRP. AREA RDWY LANES (SQ YD)	HMA SURFACE REMOVAL - BUTT JOINT	PCC SURFACE REMOVAL, 1/2"	HMA SURFACE REMOVAL, 2 1/4"	LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70	BITUMINOUS MATERIALS (TACK COAT)	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	LONGITUDINAL JOINT SEALANT	CENTER LINE - RUMBLE STRIP - 16 INCH
			STA. (FT)	TO (FT)	STA. (FT)			AVG (FT)	SQ YD	SQ YD	SQ YD	TON	POUND	TON	FOOT
US 40 - URBAN															
332+89.00	TO	332+99.00	22.00	22.00	22.00	10.00	24.44	31.1			1.0	16.5	2.1	10.0	
332+99.00	TO	362+34.00	22.00	22.00	22.00	2935.00	7174.44			7174.4	301.3	4842.8	602.7	2935.0	
<b>SUBTOTAL</b>								<b>31.1</b>	<b>0.0</b>	<b>7174.4</b>	<b>302.4</b>	<b>4859.3</b>	<b>604.7</b>	<b>2945.0</b>	
US 40 - RURAL															
362+34.00	TO	396+57.46	22.00	22.00	22.00	3423.46	8368.46			8368.5	351.5	5648.7	703.0	3423.5	
396+57.46	TO	396+67.46	22.00	22.00	22.00	10.00	24.44	31.1			1.0	16.5	2.1	10.0	
396+67.46	TO	401+88.54				521.08									
BRIDGE OMISSION															
401+88.54	TO	401+98.54	22.00	22.00	22.00	10.00	24.44	31.1			1.0	16.5	2.1	10.0	
401+98.54	TO	416+42.42	22.00	22.00	22.00	1443.88	3529.48			3529.5	148.2	2382.4	296.5	1443.9	
416+42.42	TO	420+97.90	24.00	33.00	28.50	455.48	1442.35			1442.4	60.6	973.6	121.2	911.0	
420+97.90	TO	430+69.89	33.00	33.00	33.00	971.99	3563.96			3564.0	149.7	2405.7	299.4	1944.0	
430+69.89	TO	435+09.89	33.00	24.00	28.50	440.00	1393.33			1393.3	58.5	940.5	117.0	880.0	
435+09.89	TO	457+80.00	22.00	22.00	22.00	2270.11	5549.16			5549.2	233.1	3745.7	466.1	2270.1	
457+80.00	TO	458+50.00	24.00	24.00	24.00	70.00	186.67		186.7		7.8	126.0	18.3	70.0	
458+50.00	TO	460+62.00	24.00	24.00	24.00	212.00	565.33								
SEE BRIDGE PLANS															
460+62.00	TO	461+77.00	24.00	24.00	24.00	115.00	306.67		306.7		12.9	207.0	30.1	115.0	
461+77.00	TO	475+76.90	22.00	22.00	22.00	1399.90	3421.98			3422.0	143.7	2309.8	287.4	1399.9	
475+76.90	TO	480+16.90	23.00	33.00	28.00	440.00	1368.89			1368.9	57.5	924.0	115.0	880.0	
480+16.90	TO	481+81.90	33.00	33.00	33.00	165.00	605.00			605.0	25.4	408.4	50.8	330.0	
481+81.90	TO	484+00.00	35.00	35.00	35.00	218.10	848.17			848.2	35.6	572.5	71.2	436.2	
484+00.00	TO	484+29.00	35.00	34.34	34.67	29.00	111.72			111.7	4.7	75.4	9.4	58.0	
484+29.00	TO	486+17.00	45.34	39.07	42.20	188.00	881.61			881.6	37.0	595.1	74.1	564.0	
486+17.00	TO	488+40.00	39.07	25.08	32.07	223.00	794.72			794.7	33.4	536.4	66.8	669.0	
488+40.00	TO	488+67.00	24.08	23.00	23.54	27.00	70.62			70.6	3.0	47.7	5.9	54.0	
488+67.00	TO	488+77.00	22.00	22.00	22.00	10.00	24.44	31.1			1.0	16.5	2.1	10.0	
<b>SUBTOTAL</b>								<b>93.3</b>	<b>493.3</b>	<b>31949.5</b>	<b>1365.7</b>	<b>21948.4</b>	<b>2738.3</b>	<b>15478.5</b>	
<b>TOTAL</b>								<b>124.4</b>	<b>493.3</b>	<b>39123.9</b>	<b>1668.1</b>	<b>26807.7</b>	<b>3343.0</b>	<b>18423.5</b>	

1. HMA SURFACE REMOVAL - BUTT JOINT INCLUDES SHOULDER WIDTHS.

ADDITIONAL QUANTITY FOR LEVELING BINDER, BITUMINOUS MATERIALS (TACK COAT), AND HMA SURFACE COURSE ARE INCLUDED IN THE SIDEROAD & ENTRANCE SCHEDULE.

SHOULDER SCHEDULE															
LOCATION			PROPOSED SHLDR WIDTH			LENGTH (FT)	PRP. AREA SHLDR LANES (SQ YD)	SIDEROAD/ ENTRANCE (FT)	HMA SURFACE REMOVAL - BUTT JOINT SQ YD	HMA SURFACE REMOVAL, 2 1/4" SQ YD	BITUMINOUS MATERIALS (TACK COAT) POUND	HMA SHOULDER TON	AGGREGATE WEDGE SHOULDER, TYPE B TON	SHOULDER RUMBLE STRIPS, 8 INCH FOOT	INLETS TO BE ADJUSTED EACH
			STA1 (FT)	STA2 (FT)	AVG (FT)										
STA.	TO	STA.													
US 40 - URBAN															
332+89.00	TO	332+99.00	6.00	6.00	6.00	10.00	6.67		6.7	3.0	0.8	0.6	20.0		
332+99.00	TO	358+20.00	6.00	6.00	6.00	2521.00	1680.67	392.00	1680.7	697.5	195.3	147.0	4650.0		
358+20.00	TO	358+50.00	6.00	12.00	9.00	30.00	30.00	30.00	30.0	6.8	1.9	1.7	30.0		
358+50.00	TO	360+70.00	12.00	12.00	12.00	220.00	293.33	220.00	293.3	33.0	9.2	12.8	220.0		
360+70.00	TO	361+70.00	12.00	6.00	9.00	100.00	100.00	100.00	100.0	22.5	6.3	5.8	100.0		
361+70.00	TO	362+34.00	6.00	6.00	6.00	64.00	42.67	64.00	42.7	9.6	2.7	3.7	64.0		
SUBTOTAL									0.0	2153.3	772.4	216.3	171.8	5084.0	0
US 40 - RURAL															
362+34.00	TO	396+71.46	6.00	6.00	6.00	3437.46	2291.64	175.00	2291.6	1005.0	281.4	200.5	6699.9		
362+34.00	TO	401+84.54	BRIDGE OMISSION												
401+84.54	TO	429+57.00	6.00	6.00	6.00	2772.46	1848.31	285.00	1848.3	789.0	220.9	161.7	5259.9		
429+57.00	TO	430+29.00	3.00	3.00	3.00	72.00	24.00	30.00	24.0	6.3	1.8	4.2	42.0		
430+29.00	TO	456+78.00	6.00	6.00	6.00	2649.00	1766.00	320.00	1766.0	746.7	209.1	154.5	4978.0		
456+78.00	TO	457+30.00	16.00	16.00	16.00	52.00	92.44						104.0		
457+30.00	TO	457+80.00	16.00	16.00	16.00	50.00	88.89		88.9	40.0	10.0	1.3	100.0		
457+80.00	TO	458+50.00	16.00	16.00	16.00	70.00	124.44			56.0	13.9	1.8	140.0	2	
458+50.00	TO	460+62.00	16.00	16.00	16.00	212.00	376.89		SEE BRIDGE PLANS						
460+62.00	TO	461+77.00	16.00	16.00	16.00	115.00	204.44			92.0	22.9	3.0	230.0	2	
461+77.00	TO	462+27.00	16.00	16.00	16.00	50.00	88.89		88.9	40.0	10.0	1.3	100.0		
462+27.00	TO	462+34.00	16.00	16.00	16.00	7.00	12.44						14.0		
462+34.00	TO	481+81.90	6.00	6.00	6.00	1947.90	1298.60	165.00	1298.6	559.6	156.7	113.6	3730.8		
481+81.90	TO	484+29.00	3.00	3.00	3.00	247.10	82.37		82.4	37.1	10.4	14.4	247.1		
484+29.00	TO	488+67.00	6.00	6.00	6.00	438.00	292.00		292.0	131.4	36.8	25.5	876.0		
488+67.00	TO	488+77.00	6.00	6.00	6.00	10.00	6.67		6.7	3.0	0.8	0.6	20.0		
SUBTOTAL									177.8	7609.6	3506.1	974.6	682.5	22541.7	4
TOTAL									177.8	9762.9	4278.5	1190.9	854.3	27625.7	4

PAVEMENT MARKING SCHEDULE

STATION			LENGTH FOOT	THERMOPLASTIC PAVEMENT MARKING						PAINT PAVEMENT MARKING - LINE 4" FOOT	TEMPORARY PAVEMENT MARKING REMOVAL SQ FT	SHORT TERM PAVEMENT MARKING (3 APPLICATIONS) FOOT	SHORT TERM PAVEMENT MARKING REMOVAL (FINAL SURFACE ONLY) SQ FT		
				4" YELLOW SKIP DASH FOOT	4" YELLOW SOLID FOOT	4" WHITE SKIP DASH FOOT	4" WHITE SOLID FOOT	12" DIAGONAL FOOT	LETTERS & SYMBOLS SQ FT						
STA.	TO	STA.	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	FOOT	SQ FT	FOOT	SQ FT		
US 40 - URBAN															
332+89.02	TO	337+80.00	491.0		982.0		982.0				654.6	267.8	29.8		
337+80.00	TO	347+98.00	1018.0	254.5			2036.0				763.5	277.6	30.8		
347+98.00	TO	348+74.00	76.0	19.0							6.3	20.7	2.3		
348+74.00	TO	358+53.00	979.0	244.8			1958.0				734.3	267.0	29.7		
358+53.00	TO	359+88.00	135.0	33.8			135.0				56.3	36.8	4.1		
359+88.00	TO	361+33.00	145.0	36.3			290.0				108.8	39.5	4.4		
361+33.00	TO	362+34.00	101.0	25.3			101.0				42.1	27.5	3.1		
SUBTOTAL				7098						0	0	0	2366	937	104
US 40 - RURAL															
362+34.00	TO	396+67.46	3433.5	858.4			6866.9				2575.1	936.4	104.0		
396+67.46	TO	401+88.54	521.1	BRIDGE OMISSION											
401+88.54	TO	412+42.42	1053.9	263.5			2107.8				790.4	287.4	31.9		
412+42.42	TO	416+42.42	400.0	100.0	400.0		800.0				433.3	218.2	24.2		
416+42.42	TO	420+72.90	430.5		861.0		861.0	111.1		861.0	972.1	469.6	52.2		
420+72.90	TO	420+97.90	25.0		50.0		25.0	14.0		50.0	55.7	27.3	3.0		
420+97.90	TO	421+22.90	25.0		50.0	6.3	25.0	11.0		50.0	54.8	27.3	3.0		
421+22.90	TO	422+62.90	140.0		280.0	35.0	280.0	16.8		280.0	308.5	152.7	17.0		
422+62.90	TO	423+77.90	115.0				230.0		31.2	230.0	184.5	94.1	10.5		
423+77.90	TO	424+27.90	50.0				50.0				16.7				
424+27.90	TO	425+01.04	73.1		146.3		146.3	31.1		146.3	177.4	79.8	8.9		
425+01.04	TO	426+66.04	165.0		330.0	41.3	330.0	38.3		330.0	382.1	180.0	20.0		
426+66.04	TO	429+64.04	298.0				894.0		46.8	596.0	543.5	162.5	18.1		
429+64.04	TO	430+14.04	50.0				50.0				16.7				
430+14.04	TO	435+09.89	495.8		991.7		991.7	130.8		991.7	1122.5	540.9	60.1		
435+09.89	TO	439+09.89	400.0	100.0	400.0		800.0				433.3	218.2	24.2		
439+09.89	TO	441+98.00	288.1	72.0			576.2				216.1	78.6	8.7		
441+98.00	TO	442+41.00	43.0	10.8							3.6	11.7	1.3		
442+41.00	TO	458+50.00	1609.0	402.3			3218.0				1206.8	438.8	48.8		
458+50.00	TO	460+62.00	212.0	SEE BRIDGE PLANS											
460+62.00	TO	471+77.00	1115.0	278.8			2230.0				836.3	304.1	33.8		
471+77.00	TO	475+77.00	400.0		800.0		800.0				533.3	218.2	24.2		
475+77.00	TO	480+42.00	465.0		930.0		930.0	78.5		930.0	1008.5	507.3	56.4		
480+42.00	TO	481+42.00	100.0		200.0	25.0	200.0	14.7		200.0	223.0	109.1	12.1		
481+42.00	TO	481+97.00	55.0				165.0		15.6	110.0	107.3	45.0	5.0		
481+97.00	TO	482+82.00	85.0				170.0		15.6	170.0	128.9	69.5	7.7		
482+82.00	TO	483+50.00	68.0				68.0				22.7				
483+50.00	TO	484+30.00	80.0		160.0		160.0	34.0		160.0	194.0	109.1	12.1		
484+30.00	TO	485+50.00	120.0		240.0		360.0	44.1	31.2	240.0	355.3	163.6	18.2		
485+50.00	TO	488+40.00	290.0		580.0	72.5	580.0	15.5		580.0	619.7	316.4	35.2		
488+40.00	TO	488+67.00	27.0		54.0	6.8	54.0				38.3	14.7	1.6		
488+67.00	TO	488+77.00	10.0		20.0		20.0				13.3	5.5	0.6		
SUBTOTAL				32755						540	140	5925	13573	5786	643
TOTAL				39853						540	140	5925	15939	6723	747





RAISED REFLECTIVE PAVEMENT MARKERS									
LOCATION			LENGTH	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL			RAISED REFLECTIVE PAVEMENT MARKER		
				ONE-WAY CRYSTAL	ONE-WAY AMBER	TWO-WAY AMBER	ONE-WAY CRYSTAL	ONE-WAY AMBER	TWO-WAY AMBER
STATION	TO	STATION	FOOT	EACH	EACH	EACH	EACH	EACH	EACH
US 40 - URBAN									
332+89.00	TO	337+80.00	491.00			13			13
337+80.00	TO	362+34.00	2454.00			31			31
<b>SUBTOTAL</b>				<b>0</b>	<b>0</b>	<b>44</b>	<b>0</b>	<b>0</b>	<b>44</b>
US 40 - RURAL									
362+34.00	TO	396+67.46	3433.46			43			43
396+67.46	TO	401+88.54	521.08	BRIDGE OMISSION			BRIDGE OMISSION		
401+88.54	TO	416+42.42	1453.88			19			19
416+42.42	TO	422+62.90	620.48		32			32	
422+62.90	TO	423+77.90	115.00	4		4	4		4
423+77.90	TO	424+27.90	50.00	GAP AT SCHOOL ENTRANCE			GAP AT SCHOOL ENTRANCE		
424+27.90	TO	426+66.04	238.14		12			12	
426+66.04	TO	429+64.04	298.00	8		8	8		8
429+64.04	TO	430+14.04	50.00	GAP AT SCHOOL ENTRANCE			GAP AT SCHOOL ENTRANCE		
430+14.04	TO	435+09.89	495.85		26			26	
435+09.89	TO	458+50.00	2340.11			30			30
458+50.00	TO	460+62.00	212.00	SEE BRIDGE PLANS			SEE BRIDGE PLANS		
460+62.00	TO	471+77.00	1115.00			14			14
471+77.00	TO	475+77.00	400.00			10			10
475+77.00	TO	481+42.00	565.00		30			30	
481+42.00	TO	482+82.00	140.00	4		7	4		7
482+82.00	TO	483+50.00	68.00	GAP AT IL 4 RAMP			GAP AT IL 4 RAMP		
483+50.00	TO	484+30.00	80.00		4			4	
484+30.00	TO	485+50.00	120.00	3	6		3	6	
485+50.00	TO	488+40.00	290.00		16			16	
488+40.00	TO	488+77.00	37.00			1			1
<b>SUBTOTAL</b>				<b>19</b>	<b>126</b>	<b>136</b>	<b>19</b>	<b>126</b>	<b>136</b>
<b>TOTAL</b>				<b>325</b>			<b>325</b>		

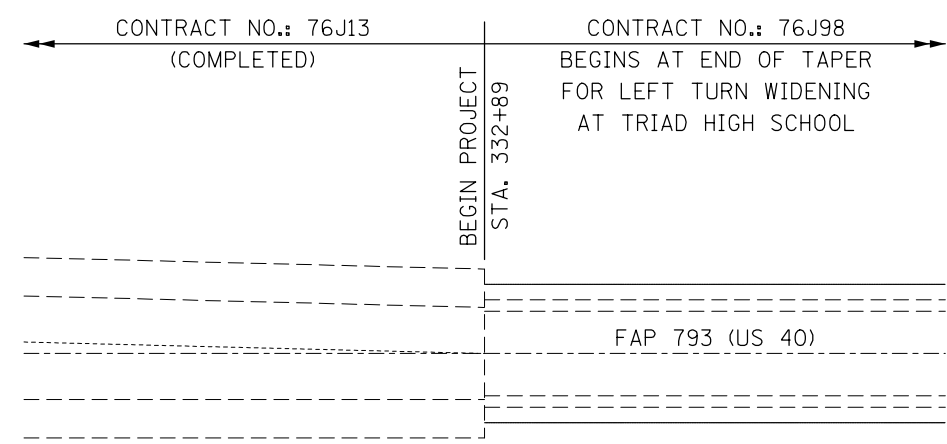
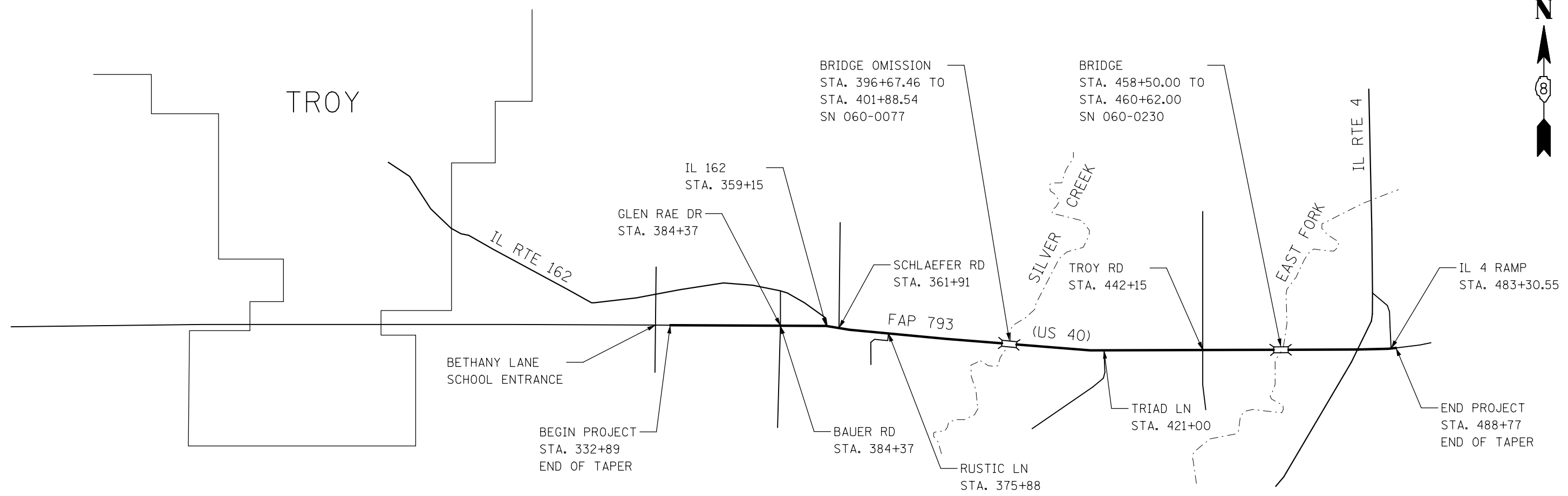
TEMPORARY RAMP SCHEDULE								
LOCATION		TEMP RAMP 2.25"			TEMP RAMP 1.5"			TEMPORARY RAMP
		LENGTH	WIDTH	AREA	LENGTH	WIDTH	AREA	
STATION	DESCRIPTION	(FT)	(FT)	(SY)	(FT)	(FT)	(SY)	(SY)
US 40 - URBAN								
332+89.00	BEGIN PROJECT	7.50	22.00	18.3	5.00	22.00	12.2	30.6
348+37.00	BAUER RD	2.00	70.00	15.6	2.00	70.00	15.6	31.1
348+37.00	GLEN RAE DR	2.00	70.00	15.6	2.00	70.00	15.6	31.1
359+15.00	IL 162	2.00	350.00	77.8	2.00	350.00	77.8	155.6
361+91.00	SCHLAEFER RD	2.00	64.00	14.2	2.00	64.00	14.2	28.4
<b>SUBTOTAL</b>				<b>141.4</b>			<b>135.3</b>	<b>276.8</b>
US 40 - RURAL								
375+88.00	RUSTIC LN	2.00	50.00	11.1	2.00	50.00	11.1	22.2
396+67.46	BRIDGE	7.50	22.00	18.3	5.00	22.00	12.2	30.6
401+88.54	BRIDGE	7.50	22.00	18.3	5.00	22.00	12.2	30.6
421+00.00	TRIAD LN	2.00	55.00	12.2	2.00	55.00	12.2	24.4
424+05.00	SCHOOL ENTRANCE	2.00	95.00	21.1	2.00	95.00	21.1	42.2
429+93.00	SCHOOL ENTRANCE	2.00	75.00	16.7	2.00	75.00	16.7	33.3
442+15.00	TROY RD	2.00	65.00	14.4	2.00	65.00	14.4	28.9
442+15.00	TROY RD	2.00	65.00	14.4	2.00	65.00	14.4	28.9
488+77.00	END PROJECT	7.50	22.00	18.3	5.00	22.00	12.2	30.6
<b>SUBTOTAL</b>				<b>145.0</b>			<b>126.7</b>	<b>271.7</b>
<b>TOTAL</b>				<b>286.4</b>			<b>262.0</b>	<b>548.4</b>

1. PROVIDE 2' TEMPORARY RAMP AT SIDEROADS.

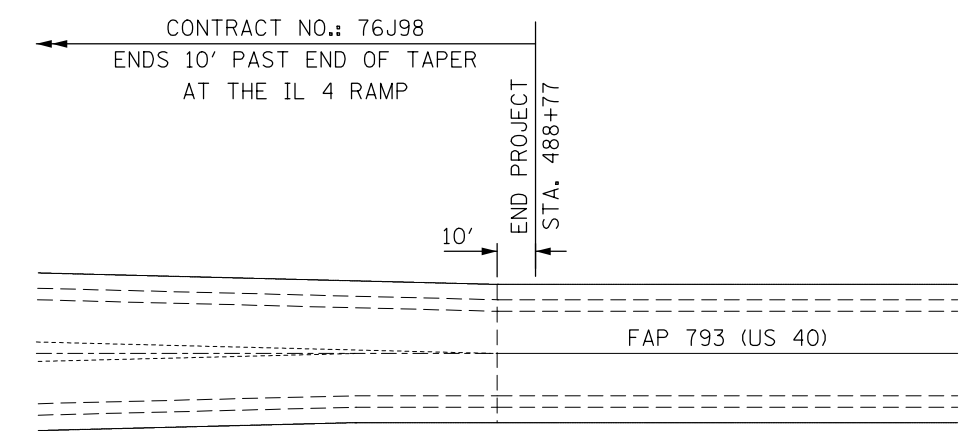
PAVEMENT PATCHING								
STATION	LOCATION	LENGTH	WIDTH	AREA	13"			
					TYPE I	TYPE II	TYPE III	TYPE IV
					FEET	FEET	SQ YD	SQ YD
US 40 - URBAN								
333+42	EB	4	11	4.9	4.9			
333+42	WB	4	11	4.9	4.9			
338+17	EB	8	11	9.8		9.8		
338+17	WB	8	11	9.8		9.8		
343+45	EB	12	11	14.7		14.7		
346+01	EB	8	11	9.8		9.8		
348+73	EB	6	11	7.3		7.3		
348+73	WB	6	11	7.3		7.3		
354+01	WB	4	11	4.9	4.9			
SUBTOTAL					14.7	58.7	0.0	0.0
ANTICIPATED FAILURES (15%)					2.2	8.8	0.0	0.0
SUBTOTAL (URBAN)					16.9	67.5	0.0	0.0

GUTTER REPAIR				
LOCATION		GUTTER REMOVAL	CONCRETE GUTTER SPECIAL	STONE RIPRAP, CLASS A3
STATION	DESCRIPTION	( FT )	( FT )	( SQ YD )
467+42.00	EAST OF S. N. 060-0230	12.0	14.0	3.0
(SEE DETAIL)				
TOTAL		12.0	14.0	3.0

PAVEMENT PATCHING								
STATION	LOCATION	LENGTH	WIDTH	AREA	13"			
					TYPE I	TYPE II	TYPE III	TYPE IV
					FEET	FEET	SQ YD	SQ YD
US 40 - RURAL								
364+57	WB	12	11	14.7		14.7		
367+21	EB	12	11	14.7		14.7		
367+21	WB	12	11	14.7		14.7		
369+85	EB	4	11	4.9	4.9			
372+49	EB	4	11	4.9	4.9			
372+49	WB	4	11	4.9	4.9			
375+13	EB	10	11	12.2		12.2		
376+13	WB	10	11	12.2		12.2		
380+41	WB	12	11	14.7		14.7		
401+53	EB	6	14	9.3		9.3		
406+81	WB	6	14	9.3		9.3		
409+45	EB	8	11	9.8		9.8		
410+45	WB	8	11	9.8		9.8		
412+09	WB	6	13	8.7		8.7		
414+73	WB	6	15	10.0		10.0		
417+37	EB	4	11	4.9	4.9			
418+37	WB	4	11	4.9	4.9			
427+93	WB	4	14	6.2		6.2		
433+21	EB	8	11	9.8		9.8		
434+21	WB	8	11	9.8		9.8		
438+49	WB	25	11	30.6				30.6
433+77	WB	12	11	14.7		14.7		
446+41	WB	8	11	9.8		9.8		
459+61	EB	6	11	7.3		7.3		
459+61	WB	6	11	7.3		7.3		
464+89	WB	6	14	9.3		9.3		
467+00	EB	8	11	9.8		9.8		
467+00	WB	15	11	18.3			18.3	
468+86	WB	25	11	30.6				30.6
470+17	EB	8	11	9.8		9.8		
470+17	WB	8	11	9.8		9.8		
475+45	WB	6	11	7.3		7.3		
480+73	WB	6	11	7.3		7.3		
SUBTOTAL					24.4	258.2	18.3	61.2
ANTICIPATED FAILURES (15%)					3.7	38.7	2.8	9.2
SUBTOTAL (RURAL)					28.1	297.0	21.1	70.4
TOTAL (URBAN + RURAL)					45.0	364.5	21.1	70.4



BEGIN PROJECT STA. 332+89  
(PLAN VIEW)  
NOT TO SCALE



END PROJECT STA. 488+77  
(PLAN VIEW)  
NOT TO SCALE

FILE NAME =	USER NAME = freimthpd	DESIGNED -	REVISED -
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 8\Projects\0876\DRAWING\DATA\CADsheets\0876J98-sht-plan.dwg		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 3/22/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>LOCATION MAP</b>			
SCALE:	SHEET 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
793	31,37,32RS-3, T-2B-R-2	MADISON	32	19
<b>CONTRACT NO. 76J98</b>				
ILLINOIS FED. AID PROJECT				

**GENERAL NOTES**

Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

Joint opening shall be adjusted according to Article 520.04 of the Standard Specs. when the deck is poured at an ambient temperature other than 50° F.

Existing reinforcement bars extending into removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Bridge deck concrete sealer shall be placed on top/inside faces of parapets (full length)/ wingwalls and on top of new concrete at joints.

Cleaning and Painting of the existing structural steel and bearings shall be included with "Cleaning and Painting Structural Steel, Location 1." All beams, bearing, deck drains, and other structural steel within 5 feet (measured along the beam) of the deck joints shall be cleaned per "Near White Blast Cleaning-SSPC-SP10." Existing diaphragms at the abutments will be replaced and are excluded from the Cleaning and Painting requirements.

The designated areas cleaned per "near white blast cleaning SSPC-SP10" shall be painted according to the requirements of paint system 1-0Z/E/U. The color of the final finish coat for all steel surfaces shall be Federal Standard Color Brown 595C 20045.

All structural steel shall conform to AASHTO Classification M 270 Grade 50, unless otherwise noted.

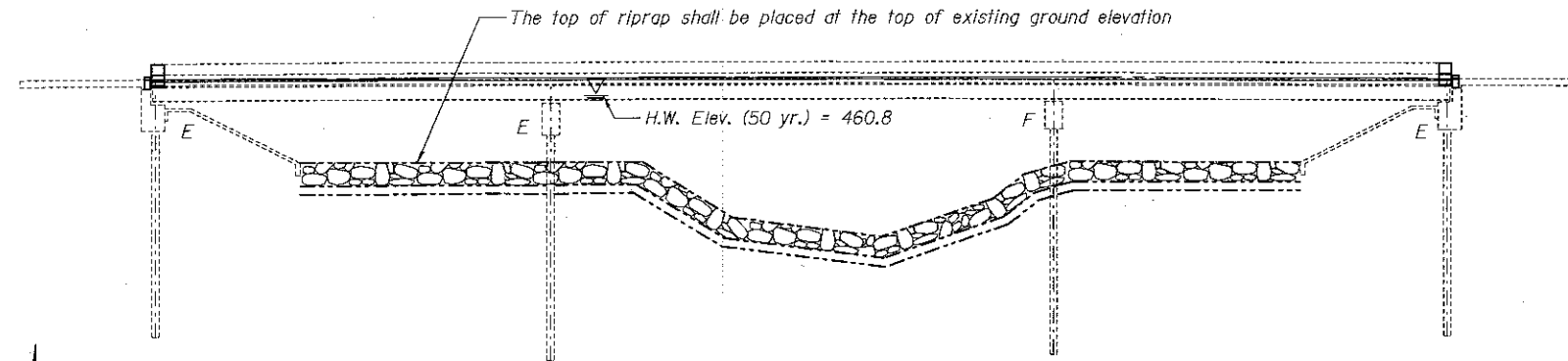
Fasteners shall be high strength bolts. Bolts 7/8" φ, open holes 1 1/8" φ, unless otherwise noted.

The new deck surface adjacent to the joints shall have its final finish tined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete Superstructures.

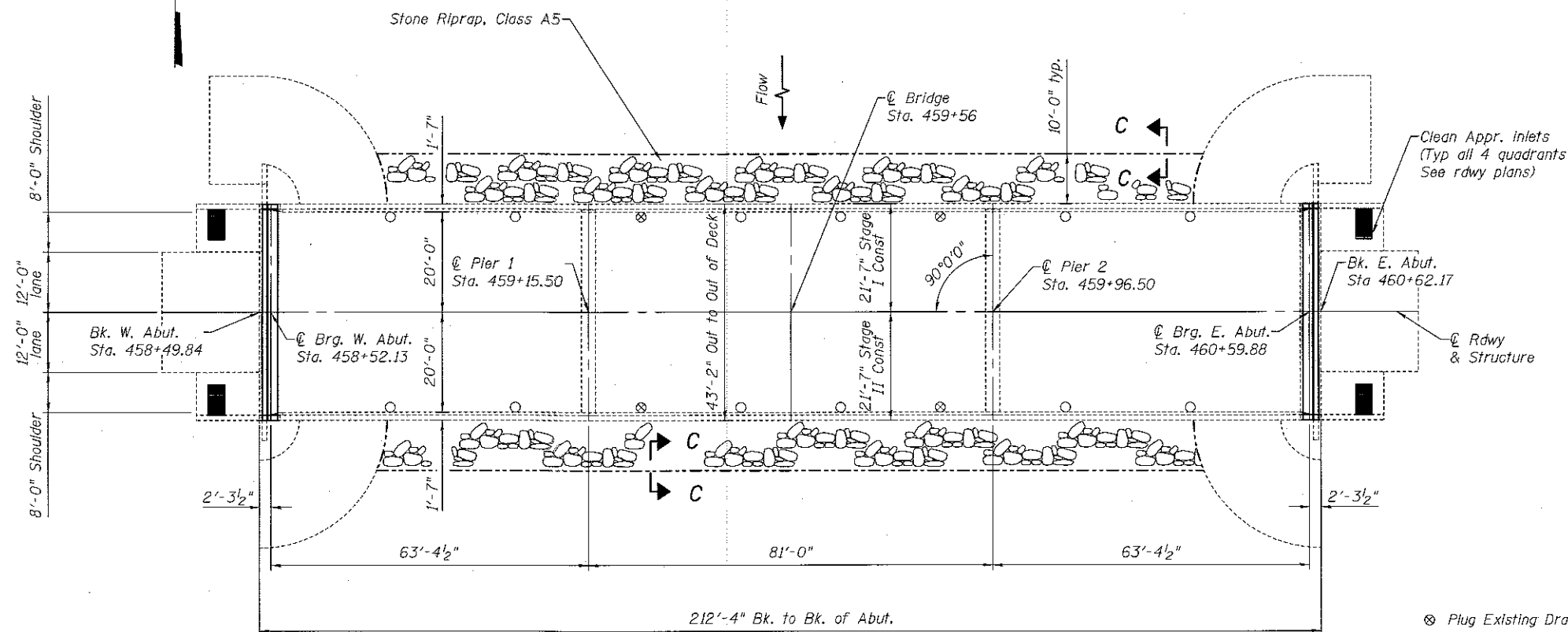
**TOTAL BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Concrete Removal	Cu. Yd.	10.1
Concrete Superstructure	Cu. Yd.	12.1
Bar Splicers	Each	24
Preformed Joint Strip Seal	Foot	85
Reinforcement Bars, Epoxy Coated	Pound	1190
Bridge Deck Concrete Sealer	Sq. Ft.	1640
*Deck Slab Repair (Partial)	Sq. Yd.	20
*Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	10
Stone Riprap, Class A5	Sq. Yd.	1165
Filter Fabric	Sq. Yd.	1165
Structural Steel Removal	Pound	3238
Cleaning and Painting Structural Steel, Location No. 1	L. Sum	1
Containment & Disposal of Lead Paint Cleaning Residues No. 1	L. Sum	1
Furnishing and Erecting Structural Steel	Pound	4190
HMA Surface Course, IL-9.5FG, N70	Ton	77
Plug Existing Deck Drains	Each	4
Full Lane Sealant Waterproofing System	Sq. Yd.	929

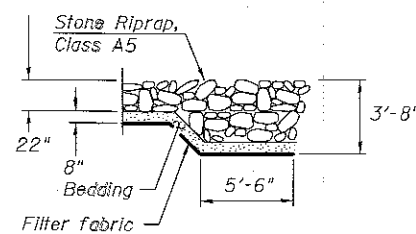
\*The quantity of deck slab repair is estimated. The engineer in the field shall determine the actual quantity and locations.



**ELEVATION**



**PLAN**



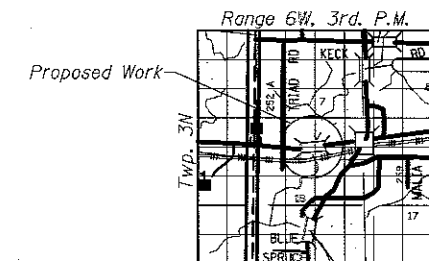
**SECTION C-C**

**SCOPE OF WORK**

- Replace deck ends & hatchblocks
- Install Preformed Joint Strip Seals
- HMA overlay with FLS on deck
- Replace diaphragms & clip angles at abutments with new galvanized diaphragms attached to new galvanized L 7/8" x 6" x 4" bearing stiffeners bolted to the webs
- Clean & paint beam ends & existing elastomeric bearings at abutments
- Deck Slab Repair
- Place A5 riprap from slope wall to slope wall and from 10' upstream to 10' downstream of bridge as shown on the plans.
- Plug Existing Drains

**INDEX OF SHEETS**

1. General Plan & Elevation
2. Deck Cross Section
3. Joint Removal & Replacement
4. Joint & Drain Details & Waterproofing Staging
5. Structural Steel
6. Structural Steel Details
7. Elevation E. & W. Abutment, Hatchblock & Wingwall
8. Strip Seal Details
9. Temporary Concrete Barrier
10. Bar Splicers
11. For Information Only (F.I.O.) - Existing Framing Plan
12. (F.I.O.) - Existing Bearing



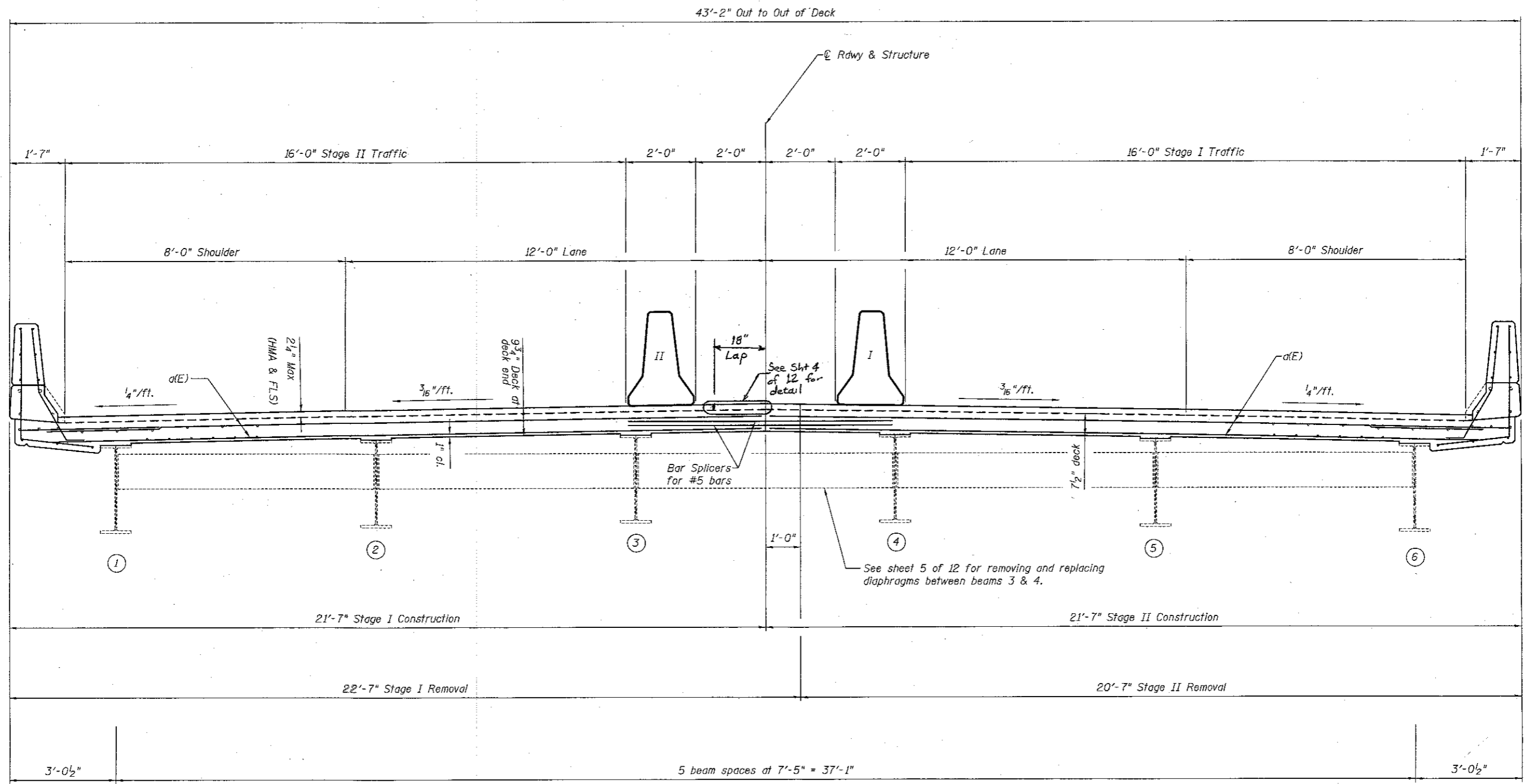
**LOCATION SKETCH**



Expires 11/30/18

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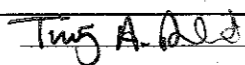
DESIGNED - AYW	 ENGINEER OF BRIDGES AND STRUCTURES	DATE - 5/11/18	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL PLAN & ELEVATION (US 40 over E. Fork Silver Creek) SN 060-0230	F.A.P. RTE. 793	SECTION 31, 37, 32RS-3, T-2B-R-2	COUNTY MADISON	TOTAL SHEETS 32	SHEET NO. 20
CHECKED - JSB		REVISOR			CONTRACT NO. 76J98	ILLINOIS FED. AID PROJECT			
DRAWN - AYW		REVISOR			SHEET NO. 1 OF 12 SHEETS				
CHECKED - JSB		REVISOR							

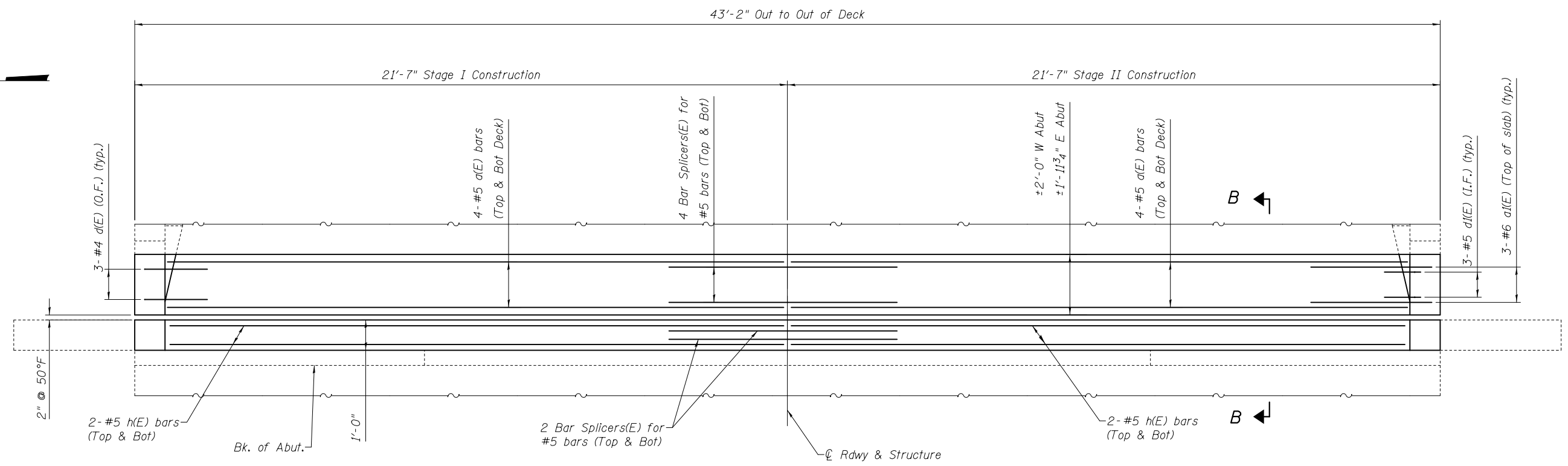
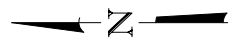


**CROSS SECTION**

Looking East @ Joint Location

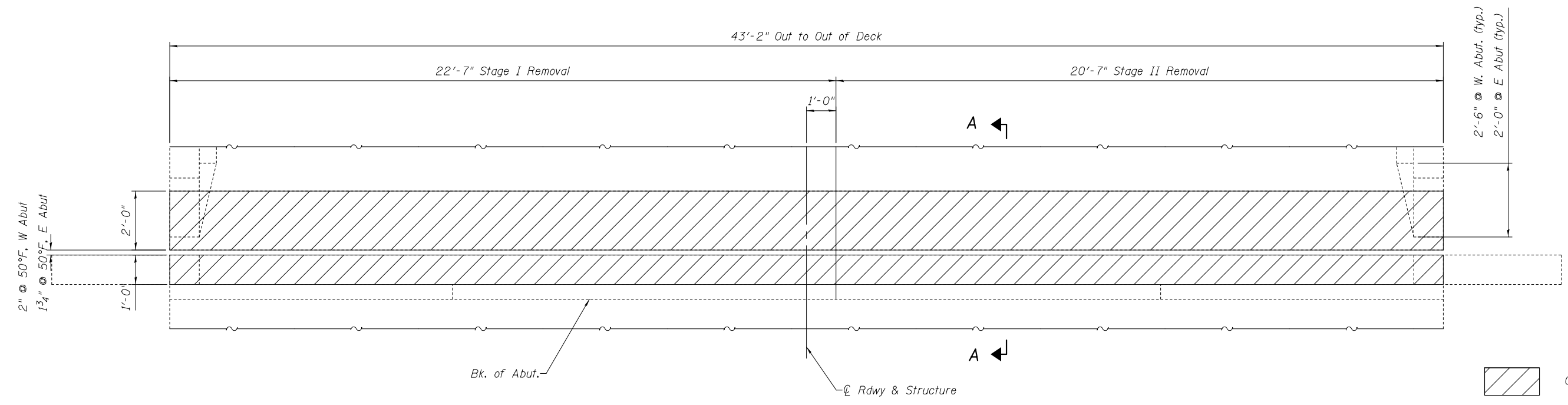
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DESIGNED - AYW	 PASSED ENGINEER OF BRIDGES AND STRUCTURES	DATE 5/11/18	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DECK CROSS SECTION SN 060-0230 SHEET NO. 2 OF 12 SHEETS	F.A.P. RTE. 793	SECTION 31, 37, 32RS-3, T-28-R-2	COUNTY MADISON	TOTAL SHEETS 32	SHEET NO. 21
CHECKED -		REVISOR			CONTRACT NO. 76J98	ILLINOIS FED. AID PROJECT			
DRAWN - AYW		REVISION							
CHECKED -									



**CONCRETE REPLACEMENT**

W. Abut. shown, E. Abut. opposite



**CONCRETE REMOVAL**

W. Abut. shown, E. Abut. opposite

Note:

See sheet 4 of 12 for Bill of Material & Sections A-A & B-B



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

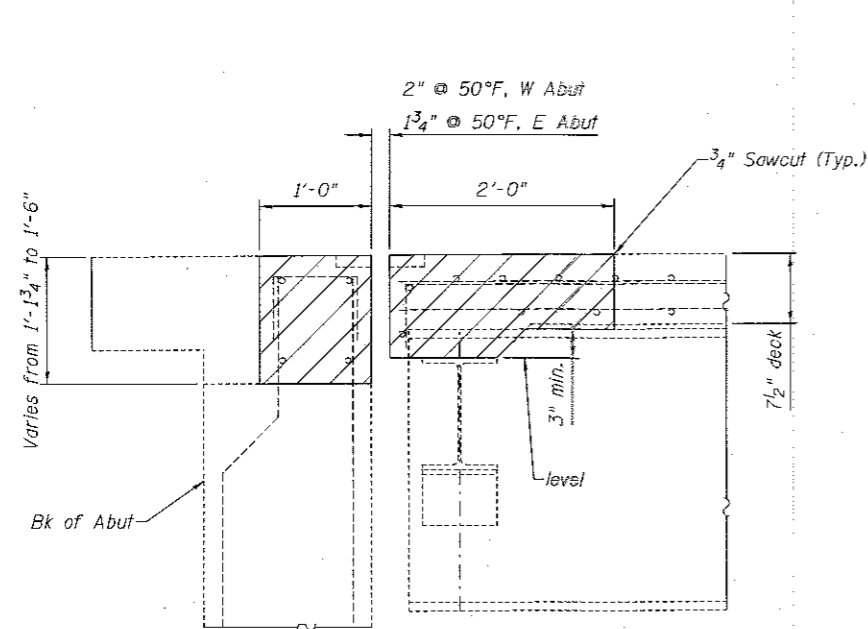
**JOINT REMOVAL & REPLACEMENT  
SN 060-0230**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
793	31, 37, 32RS-3, T-2B-R-2	MADISON	32	22
CONTRACT NO. 76J98				

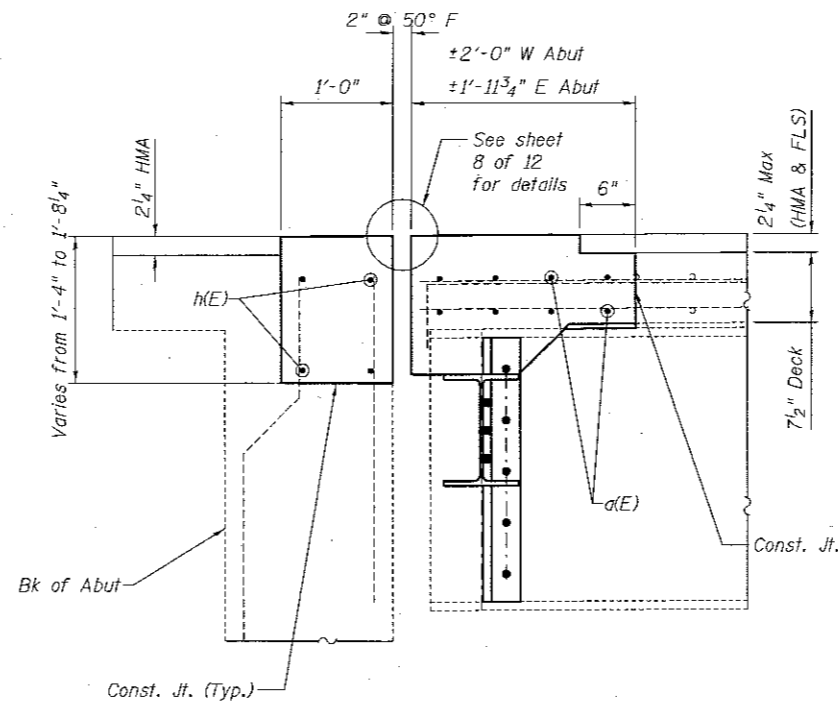
SHEET NO. 3 OF 12 SHEETS

ILLINOIS FED. AID PROJECT

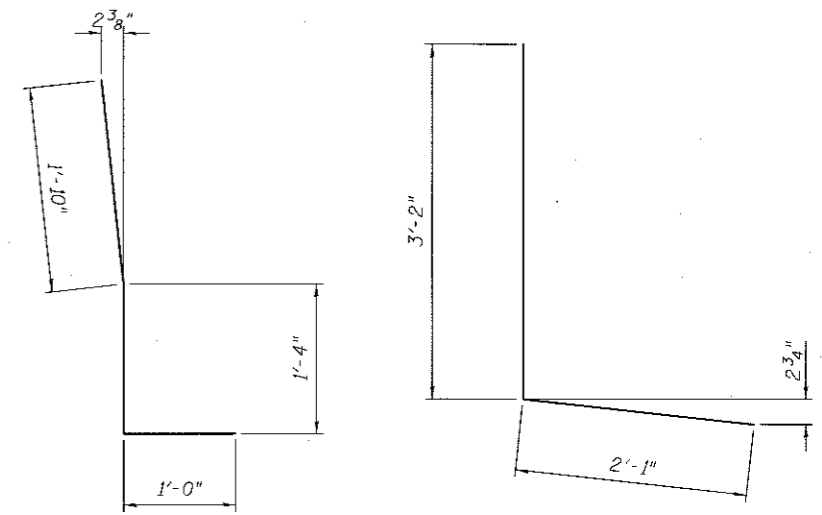
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ENGINEER OF BRIDGES AND STRUCTURES



SECTION A-A

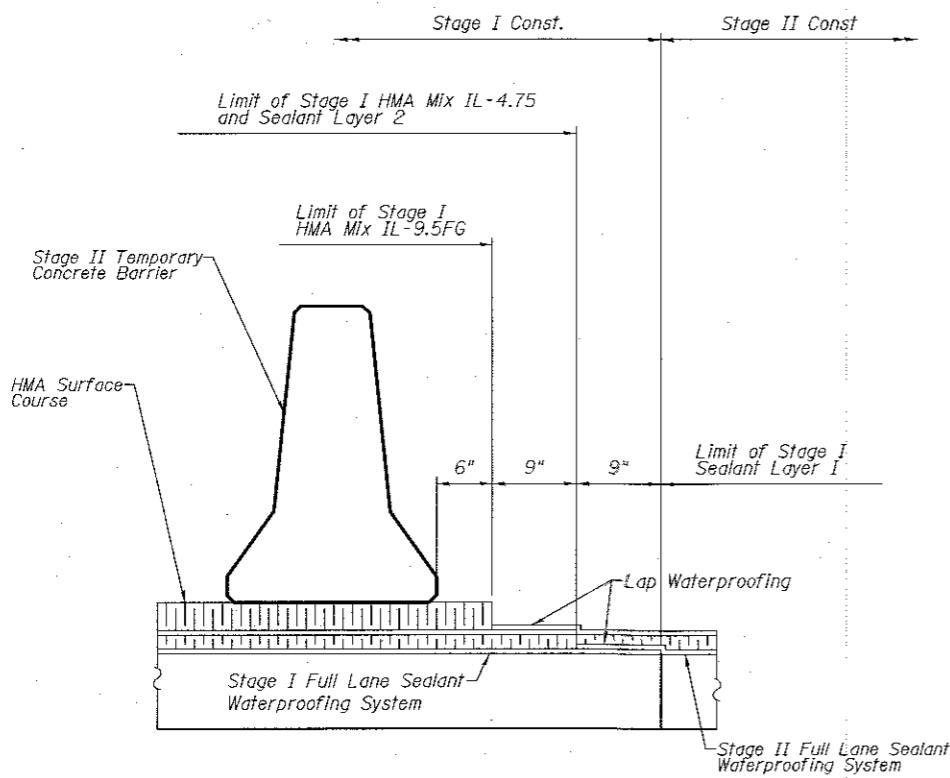


SECTION B-B

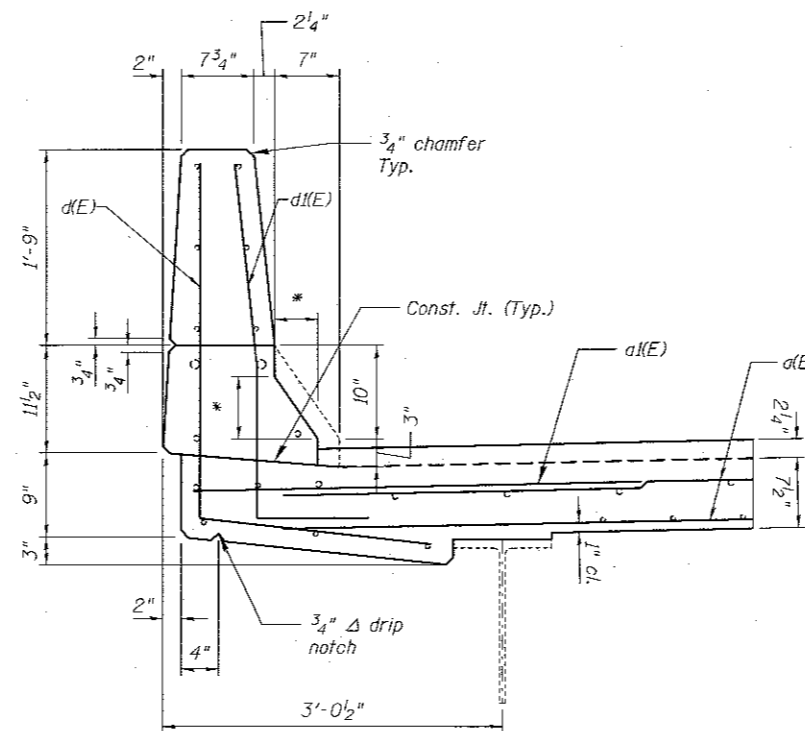


BAR d1(E)

BAR d(E)

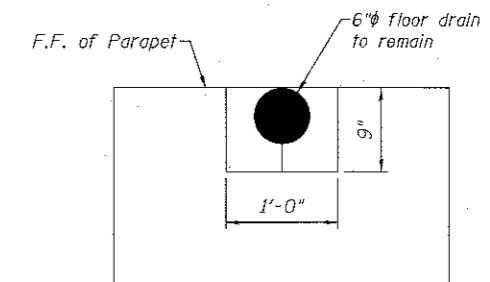


WATERPROOFING STAGING



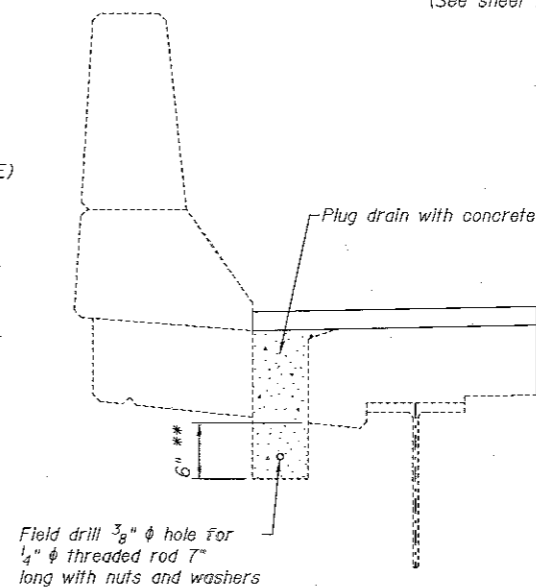
PARAPET SECTION

\* Varies, match existing



DRAIN DETAIL

Slope to drain with 1" minimum HMA at drains edge  
(See sheet 1 of 12 for locations to remain)  
(12 Req'd)



PLUG DRAIN DETAIL

(See sheet 1 of 12 for locations)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape	
d(E)	32	#5	20'-5"	—	
d1(E)	12	#6	4'-0"	—	
d(E)	12	#4	5'-3"	L	
d1(E)	12	#5	4'-2"	L	
d(E)	16	#5	20'-3"	—	
Reinforcement Bars, Epoxy Coated				Pound	1190
HMA Surface Course, IL-9.5FG, N70				Ton	77
Bridge Deck Concrete Sealer				Sq. Ft.	1640
Concrete Superstructure				Cu. Yds.	12.1
Concrete Removal				Cu. Yds.	10.1
Deck Slab Repair (Partial)				Sq. Yd.	20
Deck Slab Repair (Full Depth, Type II)				Sq. Yd.	10
Plug Existing Deck Drains				Each	4

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DATE - 5/11/18  
REVISED

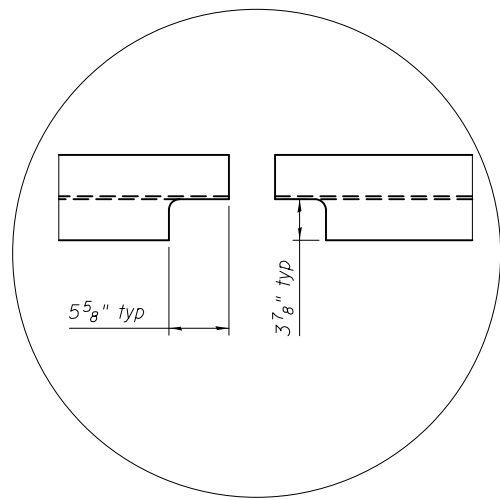
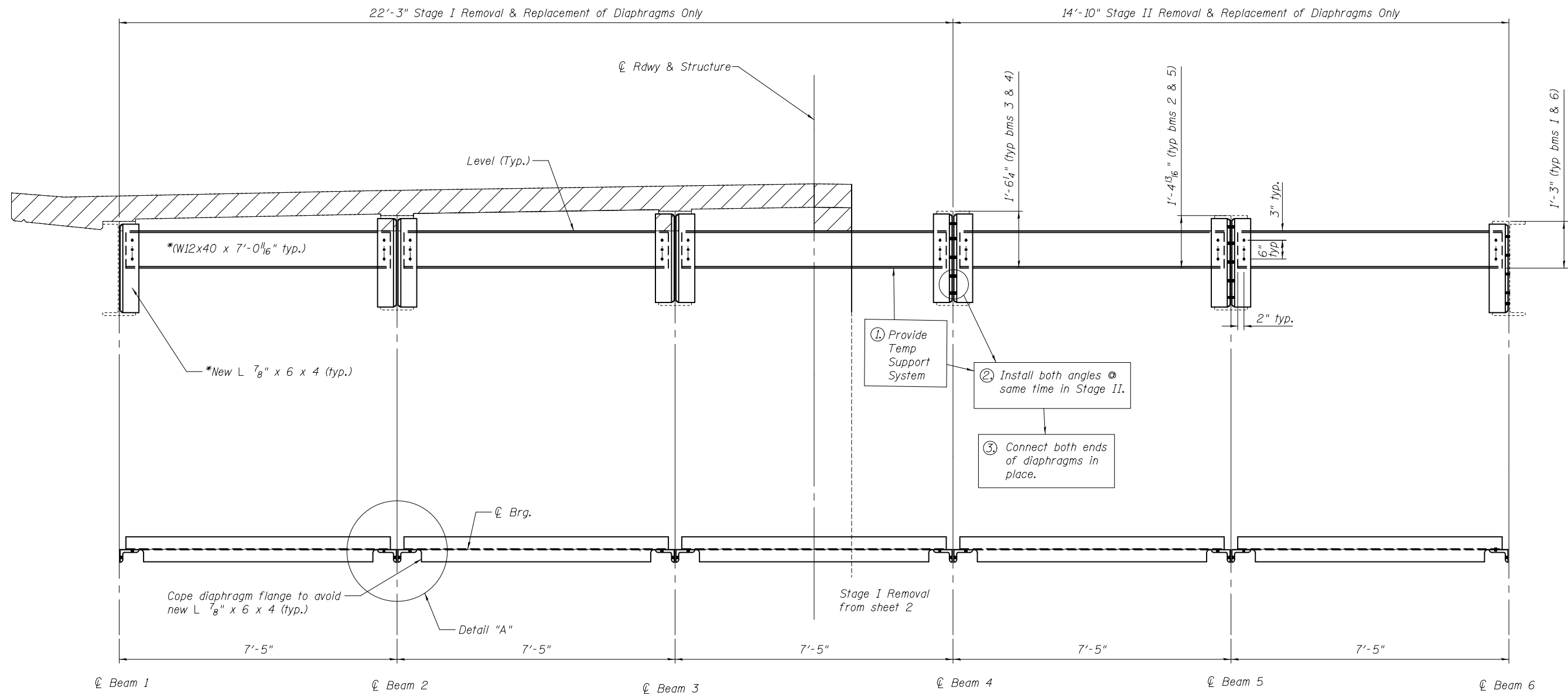
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

JOINT & DRAIN DETAILS & WATERPROOFING STAGING  
SN 060-0230

SHEET NO. 4 OF 12 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
793	31, 37, 32RS-3, T-2B-R-2	MADISON	32	23

CONTRACT NO. 76J98  
ILLINOIS FED. AID PROJECT



DETAIL "A"  
(Only showing diaphragms for clarity)

**DIAPHRAGMS (W12X40)**

- E. Abutment shown, W. Abutment similar
- Replace all end diaphragms @ each end
- \* Galvanize new Ls & diaphragms

Note: Diaphragm connection holes shall be  $1\frac{5}{16}$ "  $\phi$  for  $\frac{3}{4}$ "  $\phi$  bolts. Two hardened washers shall be required at diaphragm connections.

**BILL OF MATERIAL**

Item	Unit	Total
Structural Steel Removal	Pound	3238
F & E Structural Steel	Pound	4190

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STRUCTURAL STEEL  
SN 060-0230**

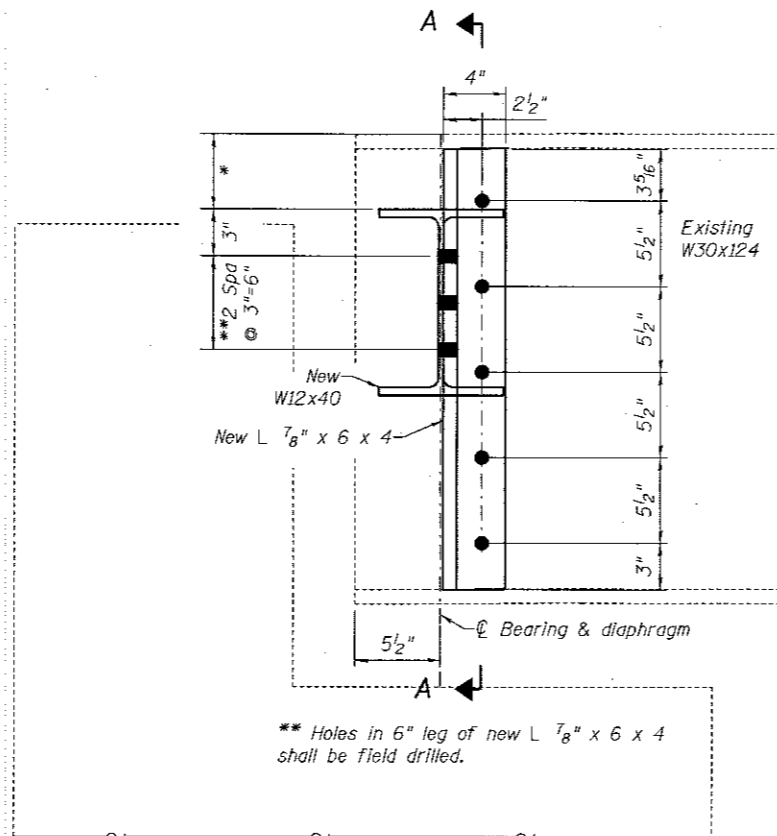
SHEET NO. 5 OF 12 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 76J98				
ILLINOIS FED. AID PROJECT				

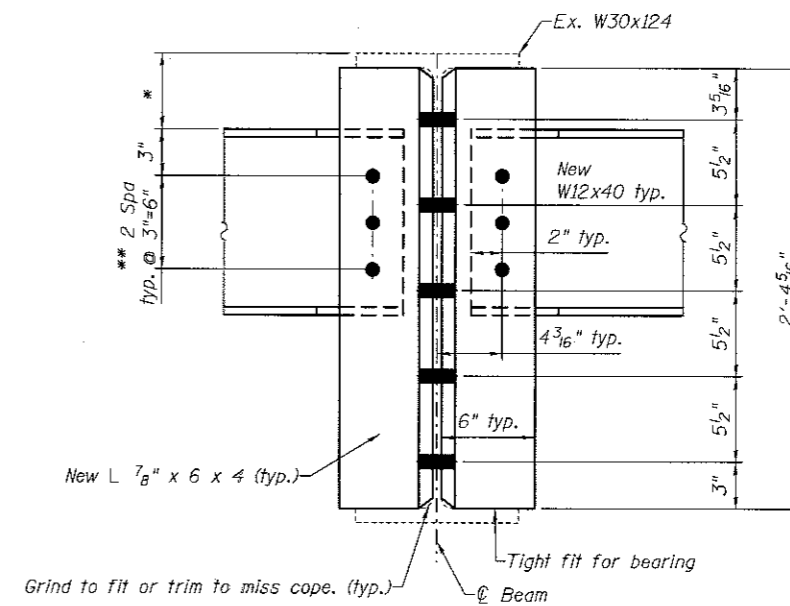
PASSED  
ENGINEER OF BRIDGES AND STRUCTURES



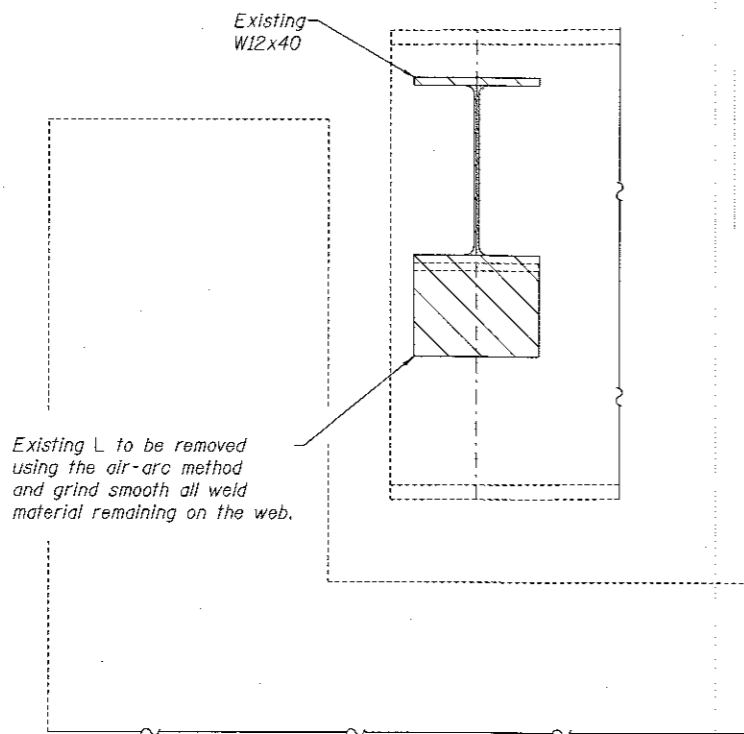
- \*  $3\frac{1}{16}$ " Beams 1 & 6
- 4  $7\frac{7}{8}$ " Beams 2 & 5
- 6  $5\frac{5}{16}$ " Beams 3 & 4



**PROPOSED DIAPHRAGMS**



**VIEW A-A**



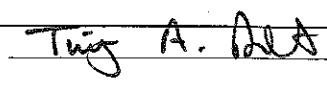
**EXISTING DIAPHRAGMS**

Note: New W12x40 diaphragms, L  $7\frac{7}{8}$ " x 6 x 4 and connection bolts are included in "Furnishing and Erecting Structural Steel".

Hatched area indicates Structural Steel Removal.

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DRAWN -	AYV
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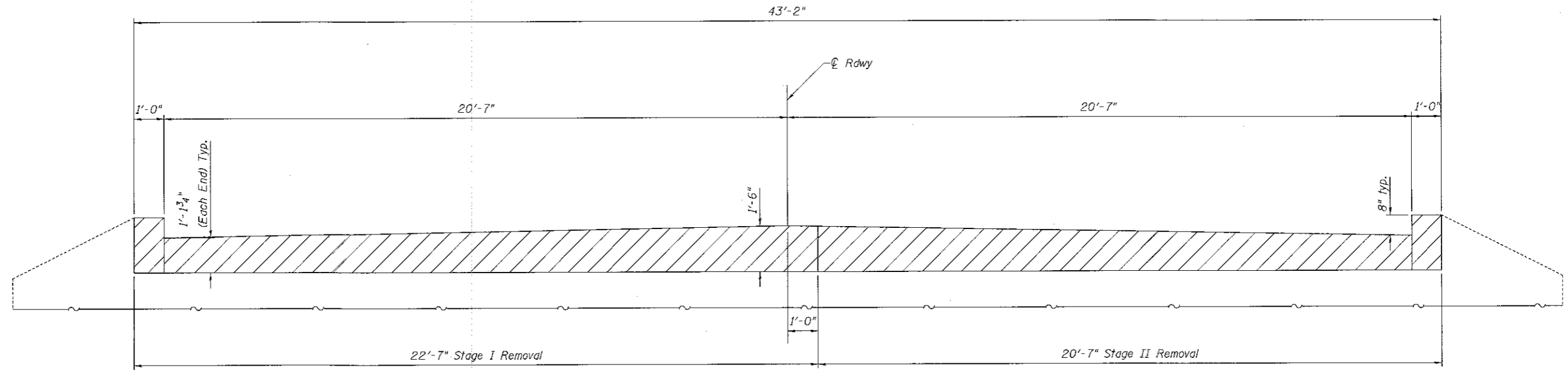
  
 PASSED  
 ENGINEER OF BRIDGES AND STRUCTURES

DATE -	5/11/18
REVISED	
REVISED	

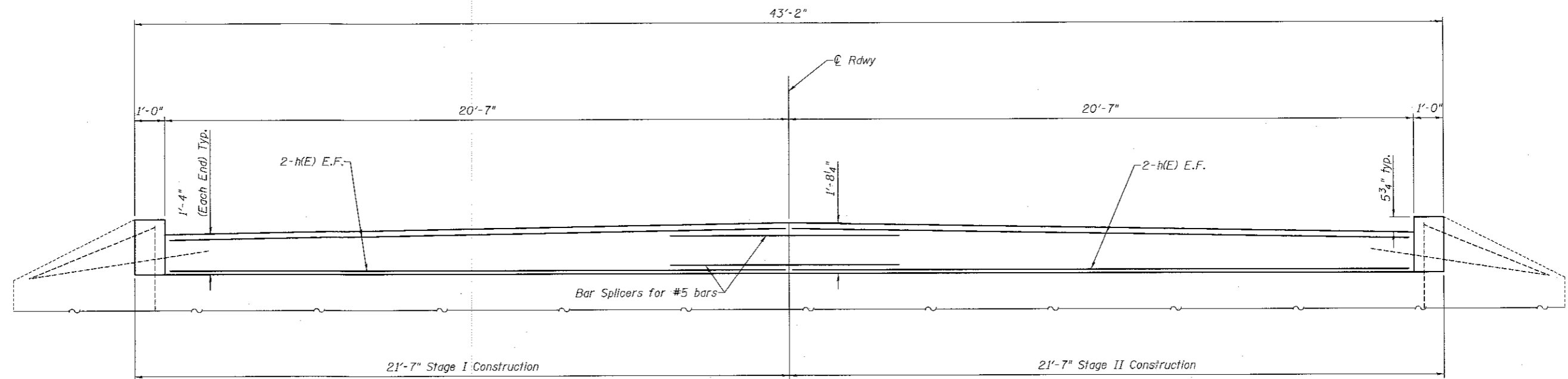
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**STRUCTURAL STEEL DETAILS**  
**SN 060-0230**  
 SHEET NO. 6 OF 12 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. T6J98			ILLINOIS FED. AID PROJECT	



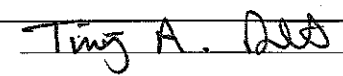
**CONCRETE REMOVAL**  
(Looking East)

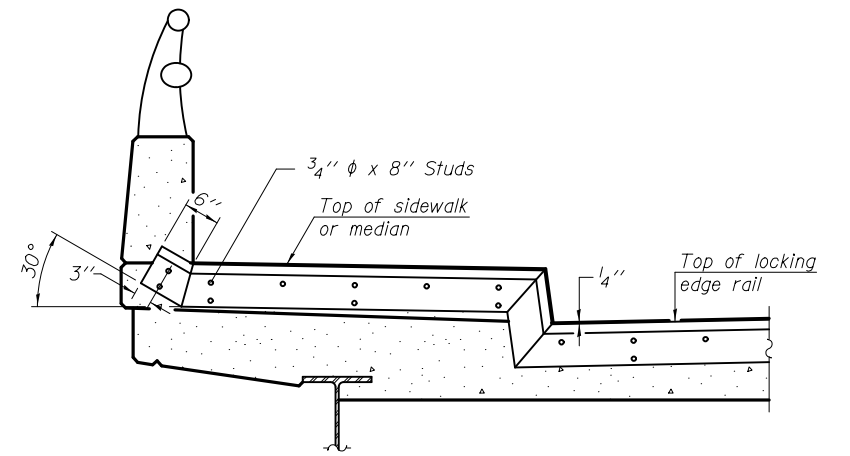
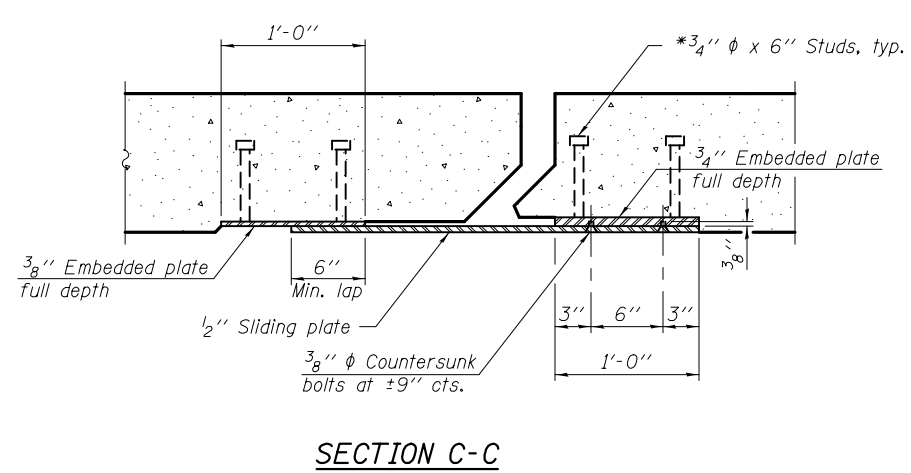
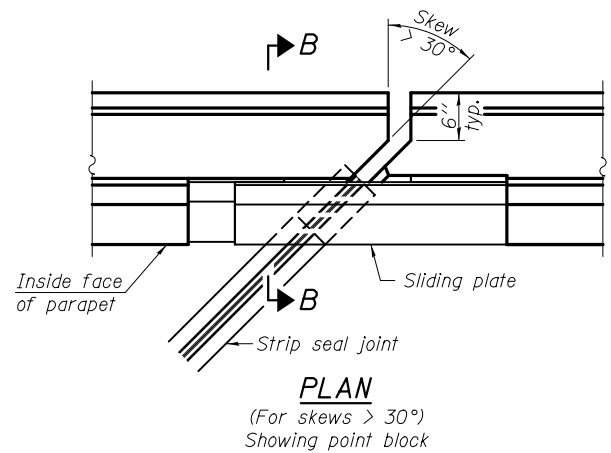
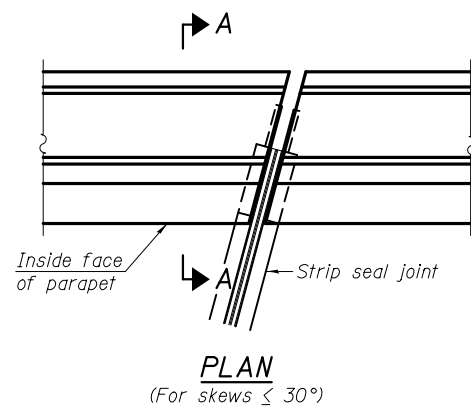


**CONCRETE REPLACEMENT**  
(Looking East)

Note:  
E.F. = Each Face

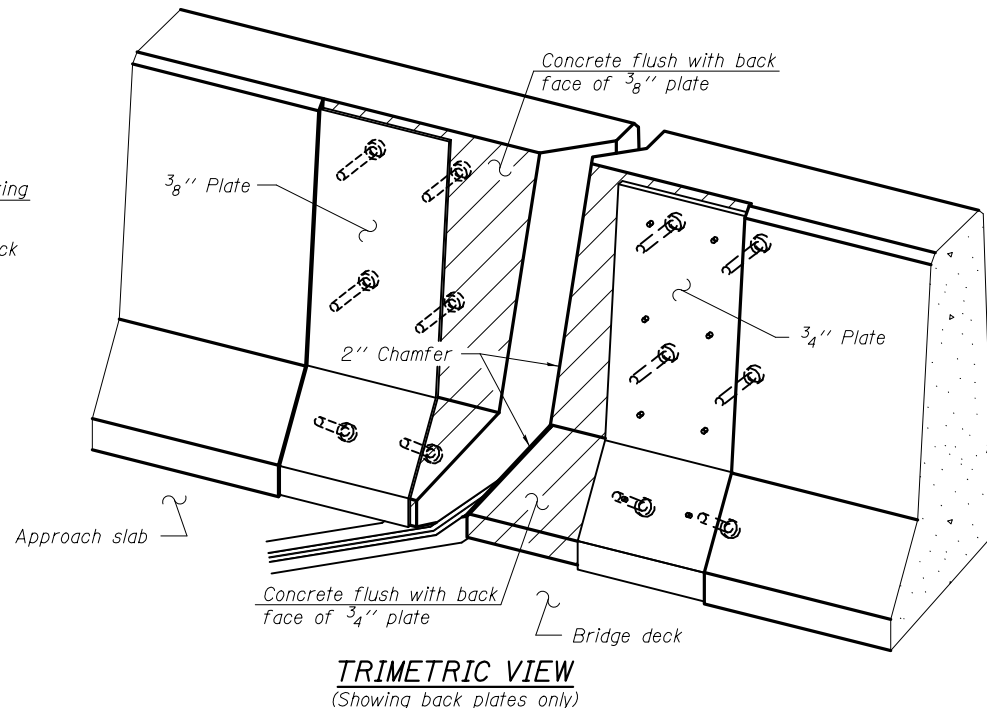
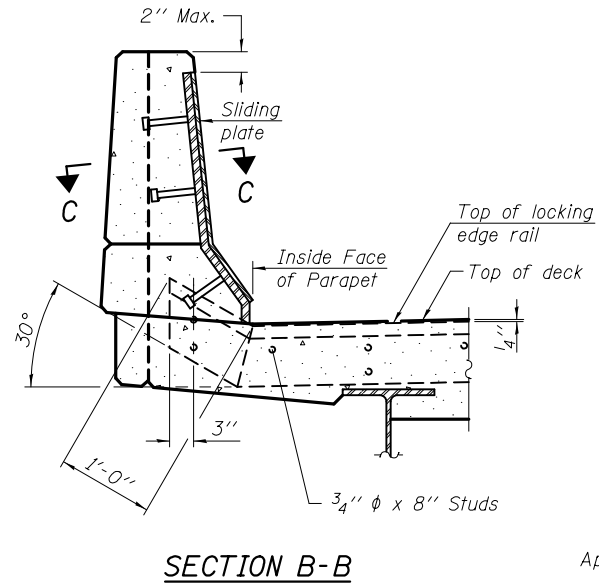
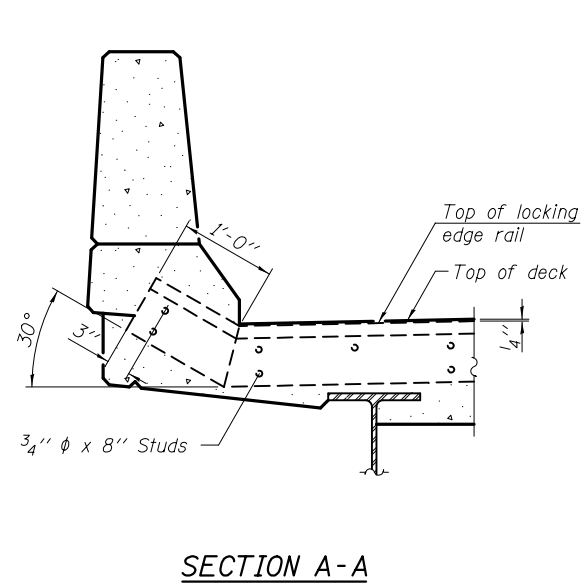
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DESIGNED - AYW	 PASSED ENGINEER OF BRIDGES AND STRUCTURES	DATE	5/11/18	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ELEVATION E. & W. ABUTMENT HATCHBLOCK & WINGWALLS SN 060-0230 SHEET NO. 7 OF 12 SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CHECKED -		REVISOR				793	31, 37, 32RS-3, T-28-R-2	MADISON	32	26
DRAWN - AYW		REVISOR				CONTRACT NO. T6J98				
CHECKED -		REVISOR				ILLINOIS FED. AID PROJECT				

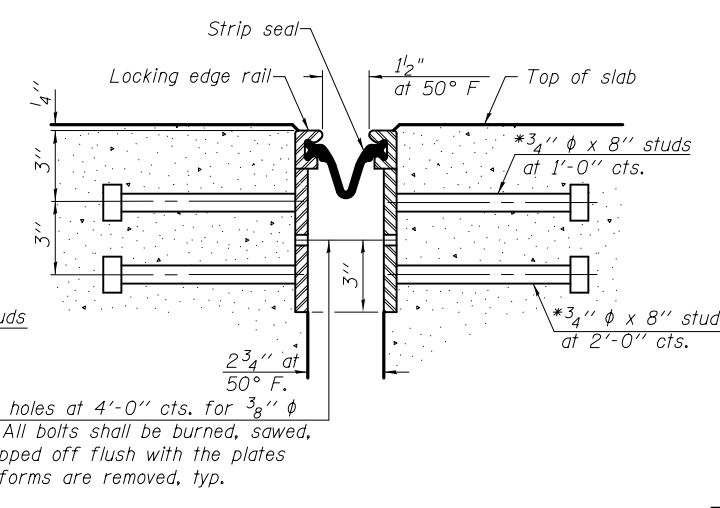
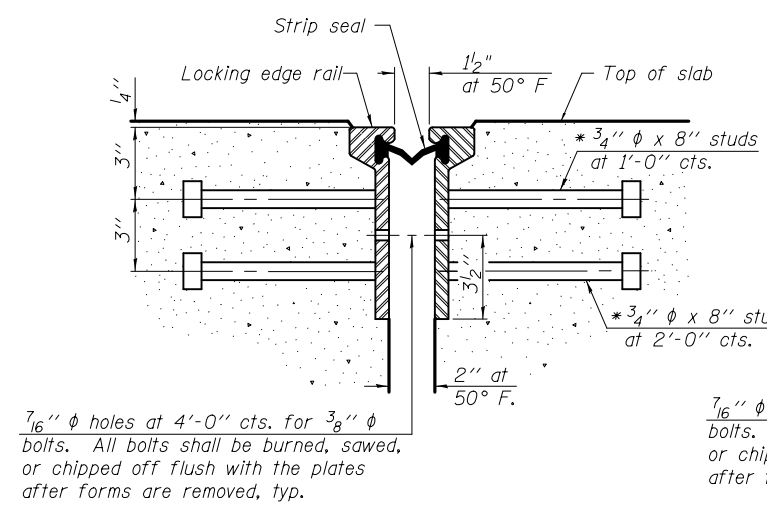


**TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN**

Shorter plates with a single row of studs at 12 inch cts. may be necessary on medians which are shallower than 9 inch. See manufacturer's recommendation.

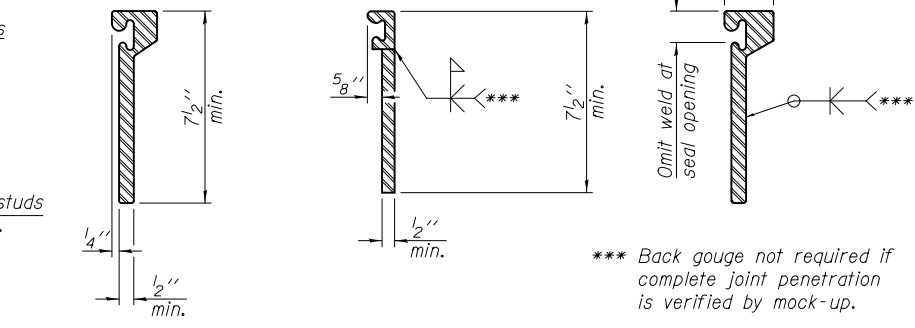


**Notes:**  
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4 inch. The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.  
 The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.  
 The manufacturer's recommended installation methods shall be followed.  
 The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.  
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications. Maximum space between rail segments shall be 3/16 inch, sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.  
 Parapet plates and anchorage studs for skews > 30 degrees included in the cost of Preformed Joint Strip Seal.



**SECTION THRU ROLLED RAIL JOINT**

**SECTION THRU WELDED RAIL JOINT**



**ROULDED EXTRUDED RAIL WELDED RAIL LOCKING EDGE RAIL SPLICE**

The inside of the locking edge rail groove shall be free of weld residue.  
 Rolled rail shown, welded rail similar.

**LOCKING EDGE RAILS**

**BILL OF MATERIAL**

Item	Unit	Total
Preformed Joint Strip Seal	Foot	85

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

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EJ-SSJ

1-27-12

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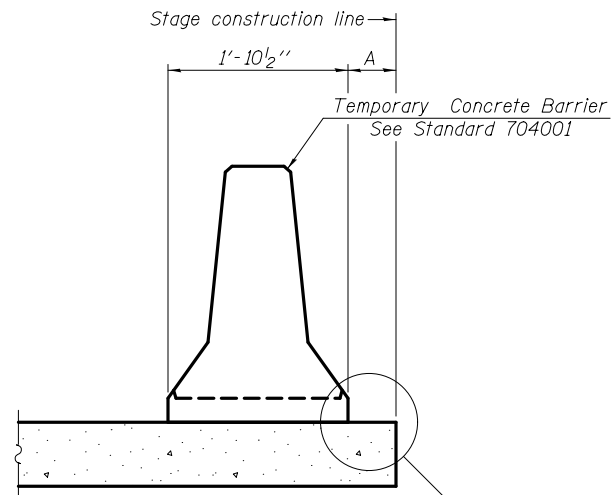
**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**PREFORMED JOINT STRIP SEAL STRUCTURAL NO. 060-0230**

SHEET NO. 8 OF 12 SHEETS

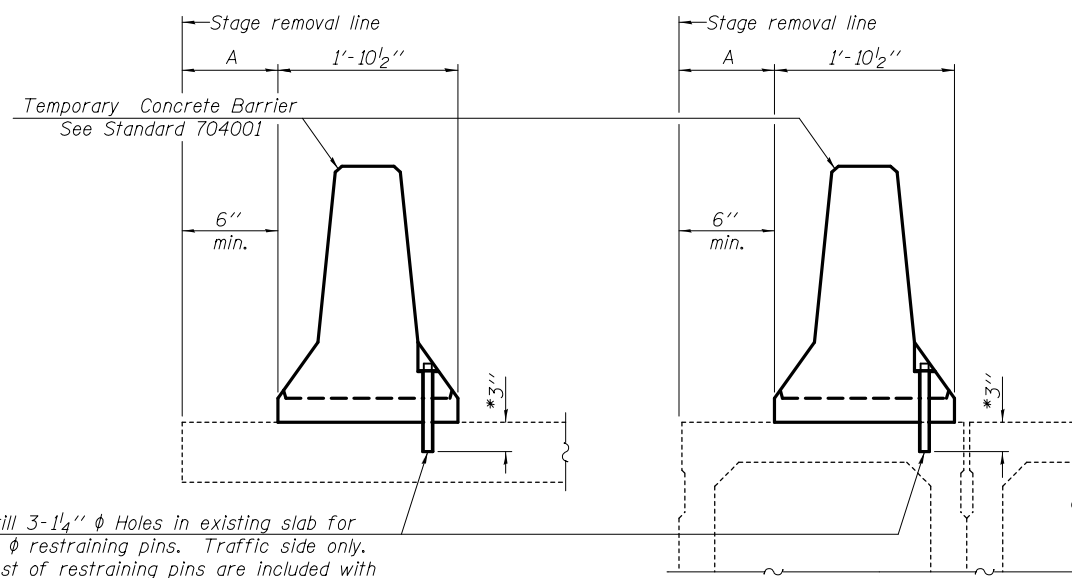
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
793	31, 37, 32RS-3, T-2B-R-2	MADISON	32	27
CONTRACT NO. 76J98				

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When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

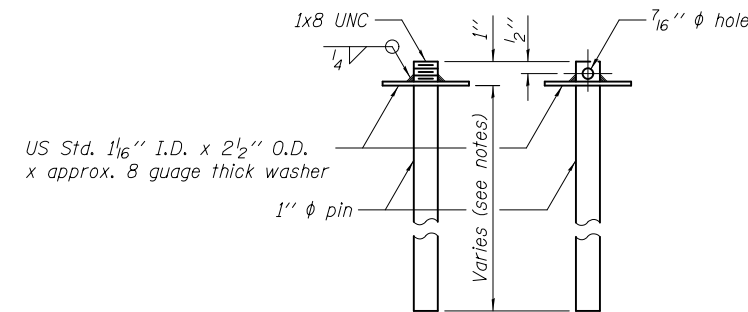
**NEW SLAB OR NEW DECK BEAM**



Drill 3-1/4"  $\phi$  Holes in existing slab for 1"  $\phi$  restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

**EXISTING SLAB**

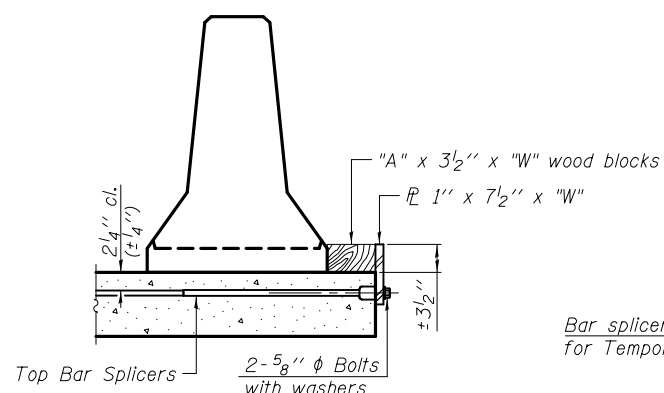
**EXISTING DECK BEAM**



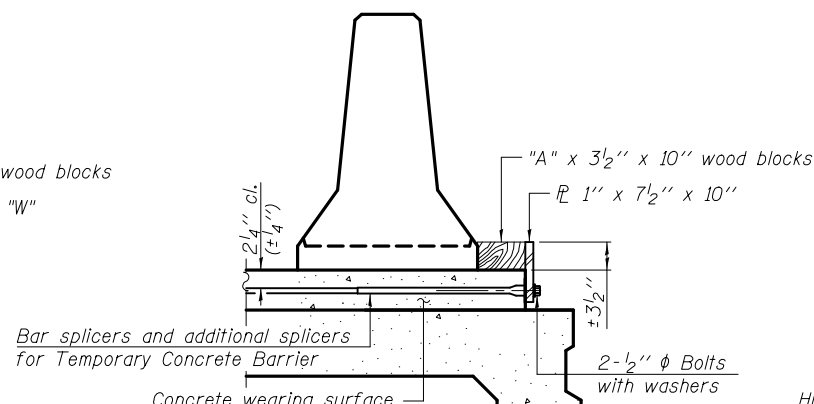
**RESTRAINING PIN**

\* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

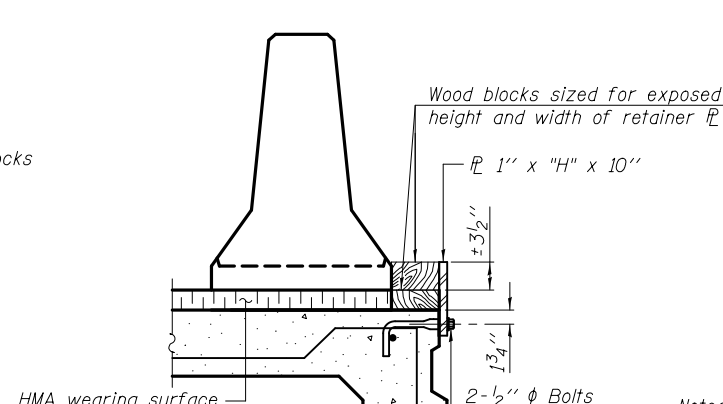
**SECTIONS THRU SLAB OR DECK BEAM**



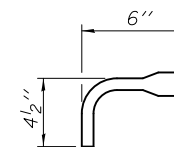
**DETAIL I**



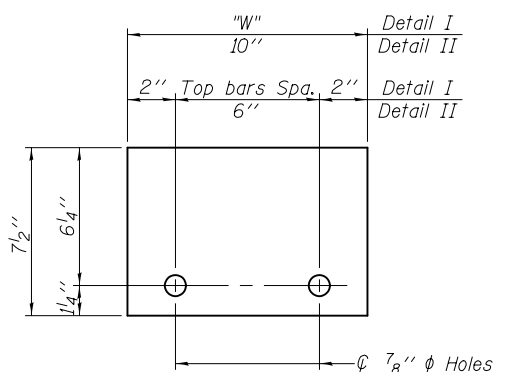
**DETAIL II**



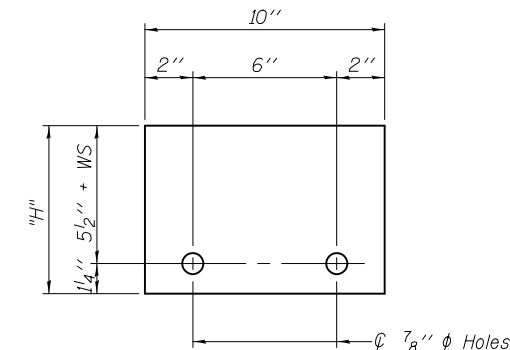
**DETAIL III**



**BAR SPLICER FOR #4 BAR - DETAIL III**



**STEEL RETAINER 1" x 7 1/2" x "W"**  
(Detail I and II)



**STEEL RETAINER 1" x "H" x 10"**  
(Detail III)

Notes:  
 Cost of retainer assembly is included with Temporary Concrete Barrier.  
 A retainer assembly shall be located at the approximate  $\phi$  of each temporary concrete barrier.  
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.  
 When the 'A' dimension is less than 1 1/2", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.  
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.  
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

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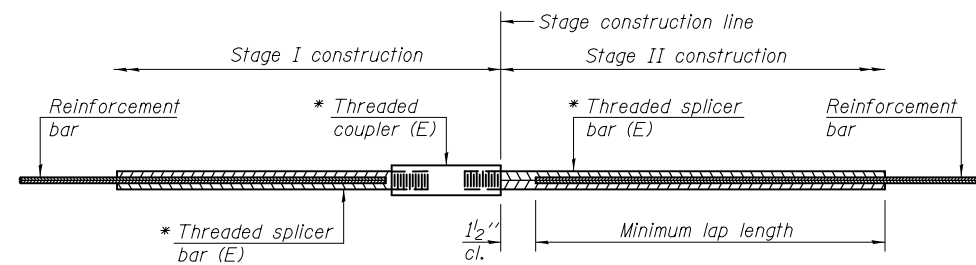
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION  
STRUCTURE NO. 060-0230

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
793	31, 37, 32RS-3, T-2B-R-2	MADISON	32	28
CONTRACT NO. 76J98				

SHEET NO. 9 OF 12 SHEETS

ILLINOIS FED. AID PROJECT

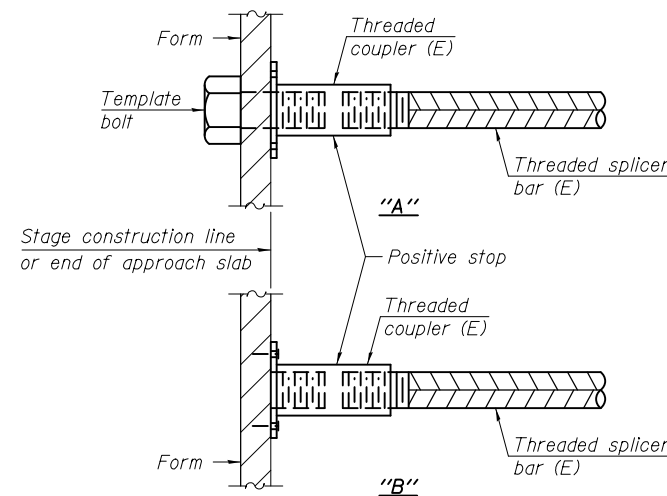


**STANDARD BAR SPLICER ASSEMBLY**

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
W. Abut. Hatch	#5	4	3'-6"
W. Abut. Deck	#5	8	3'-6"
E. Abut. Deck	#5	8	3'-6"
E. Abut. Hatch	#5	4	3'-6"

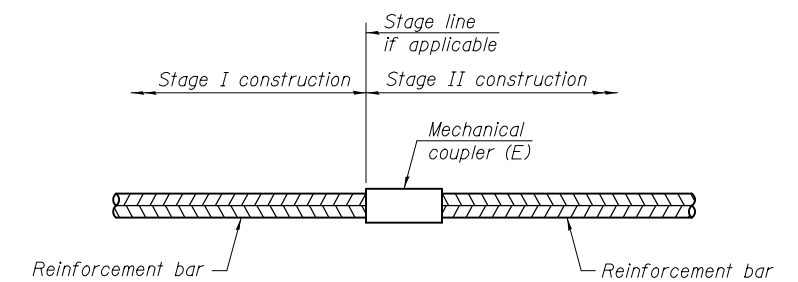


**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.

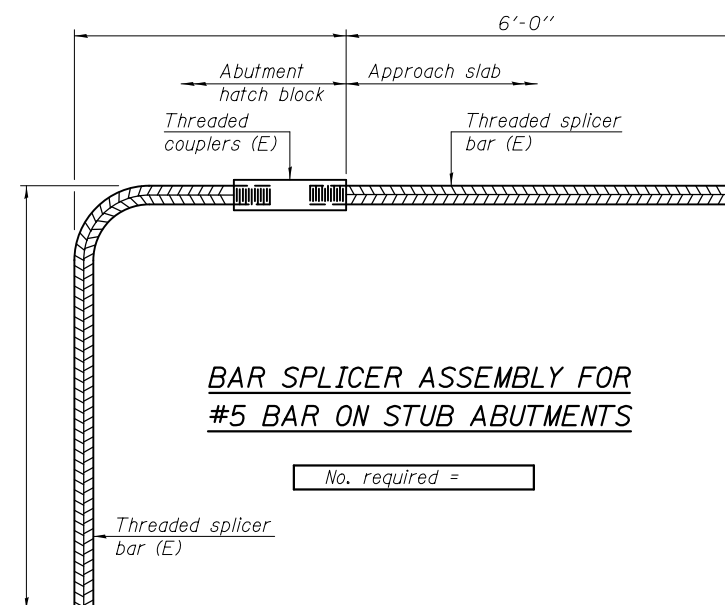
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required =

**NOTES**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars. Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

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BSD-1

6-8-15

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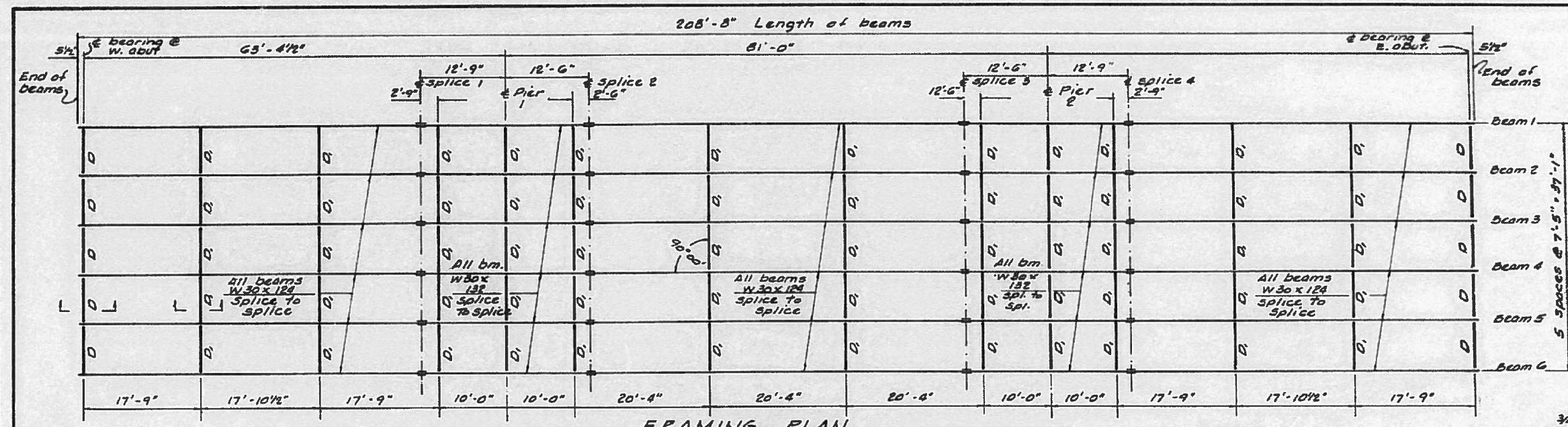
**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
STRUCTURE NO. 060-0230**

SHEET NO. 10 OF 12 SHEETS

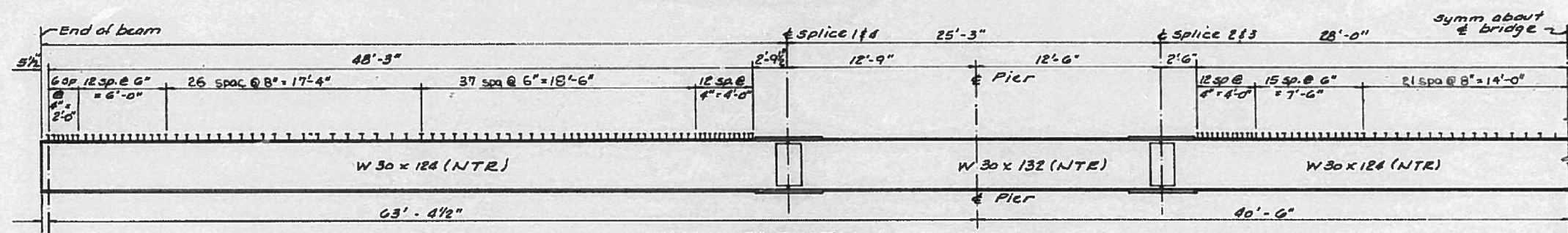
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
793	31, 37, 32RS-3, T-2B-R-2	MADISON	32	29
CONTRACT NO. 76J98				

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Sheet	Section	Count	Total
FA-793 T-28R-2		17	17
Station 459+56			
Bridge Sht. 8 of 13			

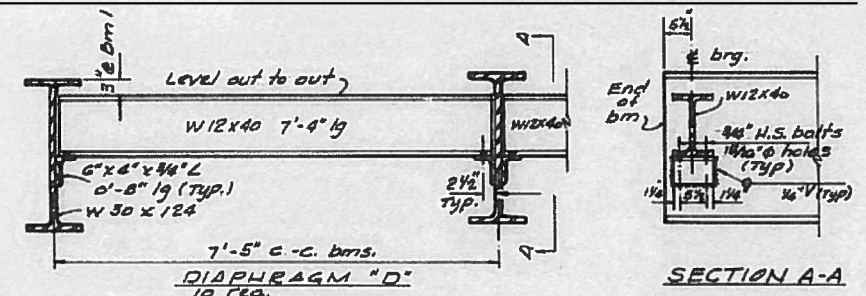


FRAMING PLAN

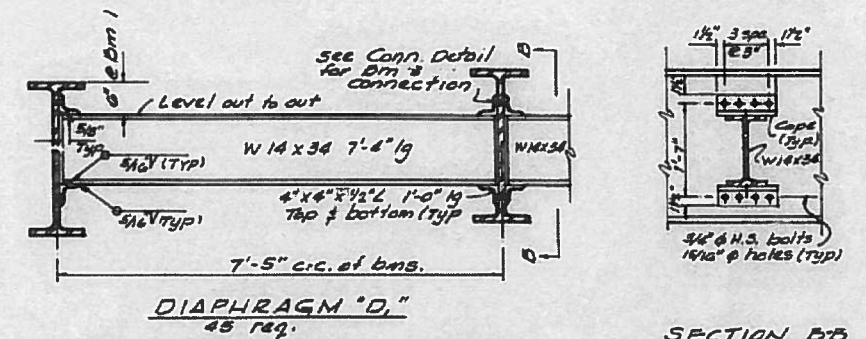


HALF-ELEVATION

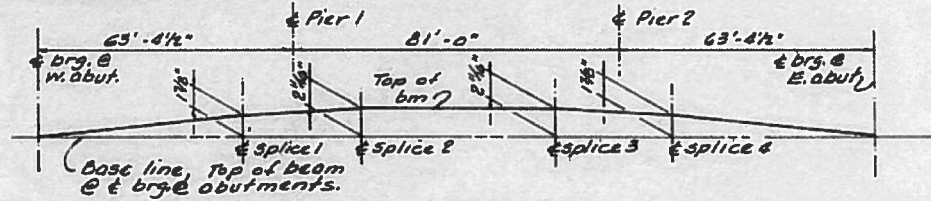
FOR INFORMATION ONLY



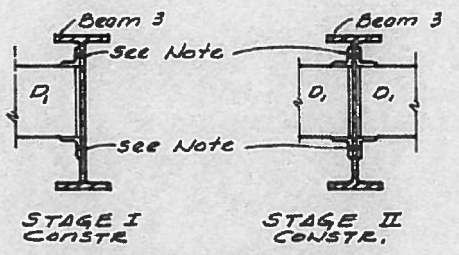
SECTION A-A



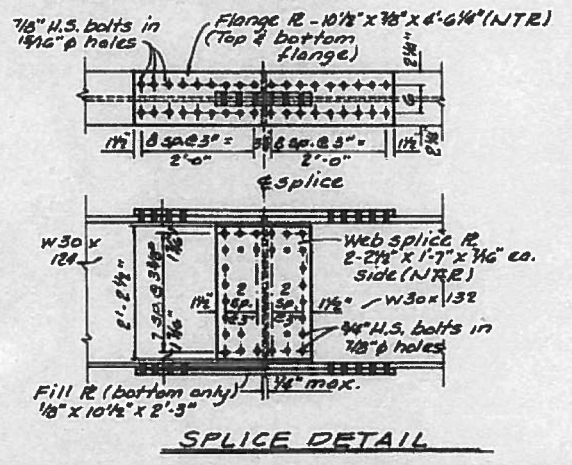
SECTION B-B



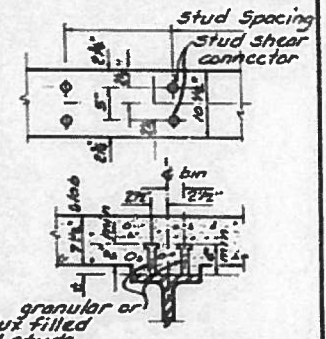
	at brg. W.abut.	at splice 1-5pan 1	at pier 1	at splices 2&3 - span 2	at pier 2	at splice 4-5pan 3	at brg. E.abut.
Beams 1&2	462.38	462.54	462.57	462.61	462.67	462.84	462.98
Beams 3&4	462.33	462.64	462.72	462.76	462.72	462.69	462.93
Beams 5&6	462.68	462.80	462.83	462.87	462.83	462.80	462.65



CONNECTION DETAIL for Beam 3 only



SPlice DETAIL



3/4" granular or solid flux filled headed studs, automatically end welded. 3408 required.  
**SHEAR CONN. DETAIL**  
 NOTE: See br. Sht. 7 for determining fillet height - "c"

\*  $M_u$  = Full Plastic Moment Capacity for Compact Braced section -  $F_y Z$   
 \*\* Non-compact section  
 Applied Load Factors =  $1.3(M_e + M_{se}) + 0.5(M_{e2})$

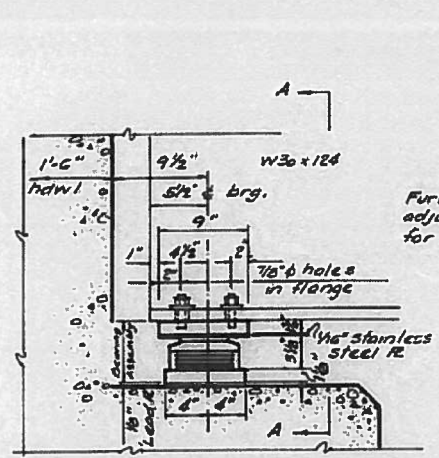
DESIGN DATA TABLE			
BEAM PROPERTIES			
	2.4 FT. END SPAN	8 PIERS	0.5 FT. CENTER SPAN
I OF BEAM ONLY (in <sup>4</sup> )	5,360	5,770	5,360
I OF COMPOSITE BEAM (in <sup>4</sup> )	19,454	-	19,454
SEC. MODULUS BM ONLY (in <sup>3</sup> )	355	380	355
SEC. MOD. COMPOS. BM (in <sup>3</sup> )	527.0	-	527.0
PLASTIC MODULUS (in <sup>3</sup> ) Z	-	437	-
INTERIOR BEAM MOMENT TABLE			
INITIAL DEAD LOAD (k/ft)	0.858	0.858	0.858
SUPERIMPOSED P (k/ft)	0.305	0.305	0.305
INITIAL P MOMENT (k-ft)	223.8	462.0	241.6
SUPERIMP. P MOMENT (k-ft)	89.7	139.0	111.1
E + I MOMENT (k-ft)	642.4	393.1	709.2
F <sub>a</sub> FOR MAX. DES. MOM. (ksi)	44.4	-	48.9
F <sub>a</sub> FOR OVERLOAD (ksi)	31.6	34.5	35.0
E + I SHEAR RANGE (k)	43.8	-	47.2
ULT. MOMENT (k-ft)	-	1820	-
MOMENT APPLIED (k-ft)	-	1633	-
INTERIOR BEAM REACTION TABLE			
R FOR INITIAL P (k)	19.7 @ ABUTS.	69.1 @ PIERS	
R FOR SUPERIMPOSED P (k)	7.4 @ ABUTS.	24.2 @ PIERS	
R FOR E (k)	40.8 @ ABUTS.	53.4 @ PIERS	
R FOR IMPACT (k)	10.9 @ ABUTS.	13.5 @ PIERS	

NOTES:  
 STRUCTURAL SHAPES AND PLATES DESIGNATED (NTR) SHALL MEET THE CHARTER V-NOTCH IMPACT REQUIREMENTS FOR THE TEMPERATURE ZONE 2.  
 DURING STAGE I, ATTACH "D<sub>1</sub>" DIAPHRAGMS TO BEAM 3 WITH TEMPORARY BOLTS. DURING STAGE II, REMOVE THE TEMPORARY BOLTS AND REPLACE WITH PERMANENT BOLTS REQUIRED DURING ERECTION OF THE "D<sub>1</sub>" DIAPHRAGMS OF STAGE II CONSTRUCTION.

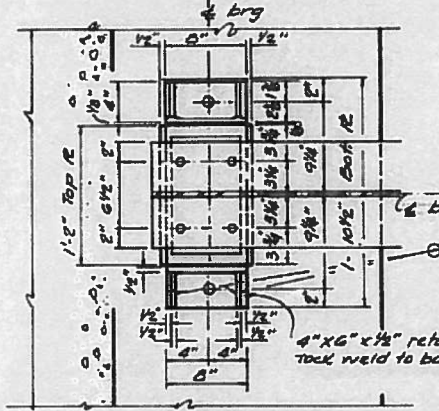
**FRAMING PLAN**

PROJECT: 81-F-793 ( )  
 RTE: FA 793  
 SECTION: T-28R-2 over East Fork of Silver Creek  
 COUNTY: Madison  
 STATION: 459+56

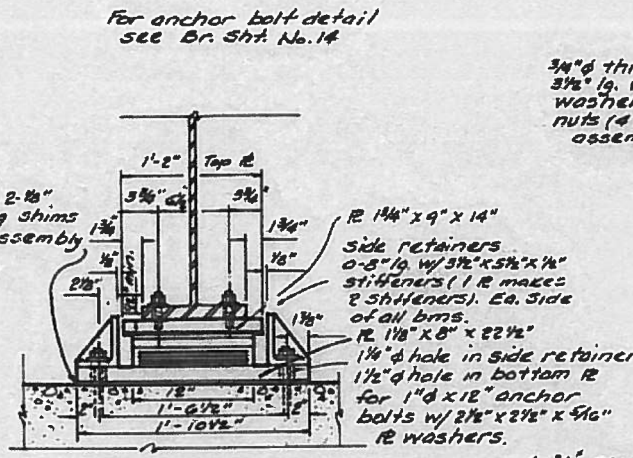
3/21/2018 1:07:52 PM



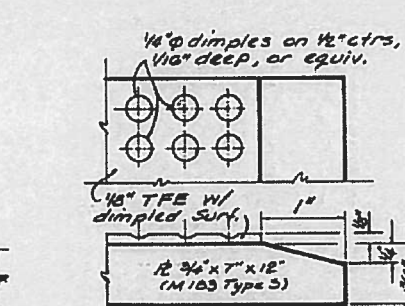
SECTION THRU W. ABUT. CAP



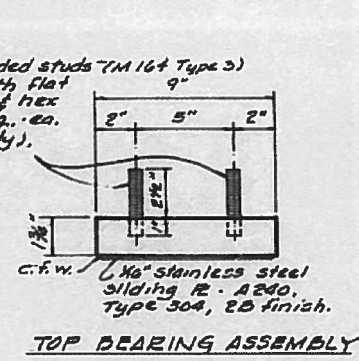
PLAN



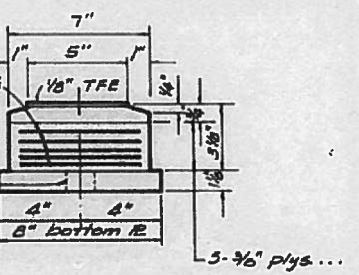
SECTION A-A



TFE SHEET DETAIL



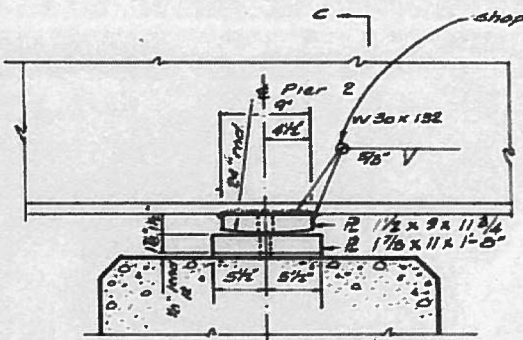
TOP BEARING ASSEMBLY



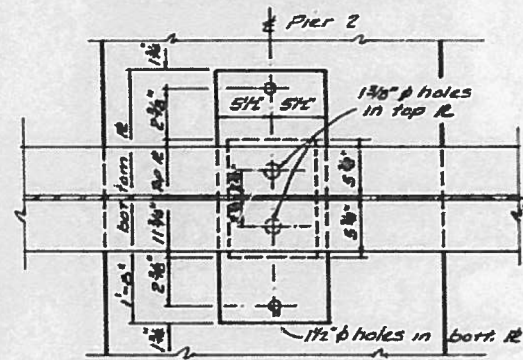
8" bottom R

NOTE: The 1/8" TFE sheet shall be bonded directly to the top steel R with a two compound, medium viscosity epoxy resin, conforming to the requirements of the Federal spec. MMM-A-134 Type I. The bonding agent shall be applied on the full area of the contact surfaces.

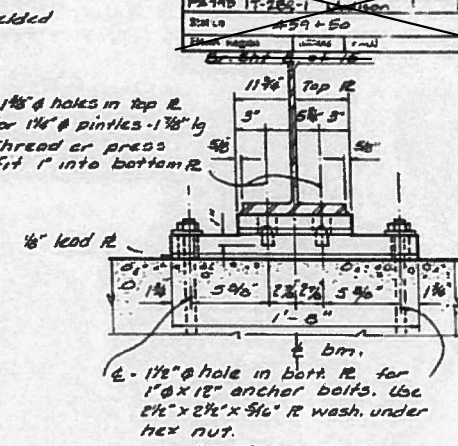
FOR INFORMATION ONLY  
TYPE II BEARING DETAILS @ WEST ABUTMENT



SECTION THRU PIER CAP

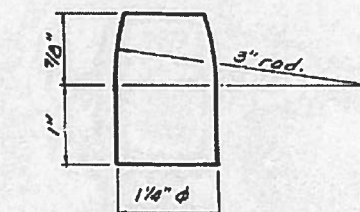


PLAN



SECTION C-C

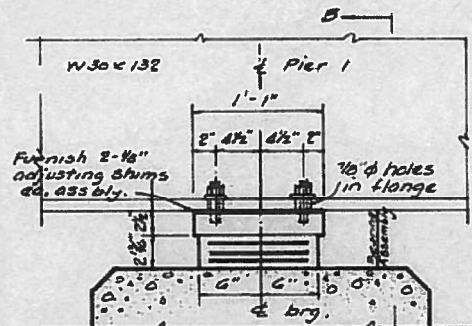
For anchor bolt detail, see Br. Sht. No. 14



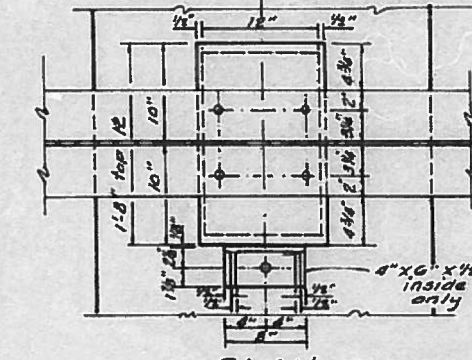
PINTLE DETAIL

BEARING DETAILS @ PIER 2

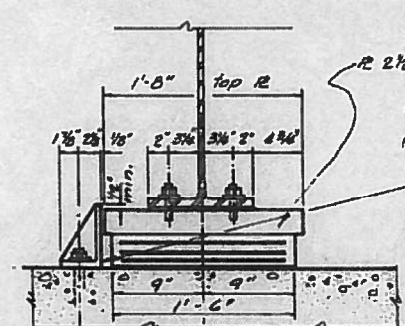
NOTE: STEEL PLATES AND RETAINERS, EXCEPT FOR PLATES EMBEDDED IN ELASTOMER AND STAINLESS STEEL PLATES, FOR ALL BEARINGS SHALL BE A572M 50, AND 9 THE ABUT. BRGS., ALL EXPOSED STEEL SHALL BE CLEANED AND GIVEN ONE COAT OF ZINC SILICATE PRIMER AND A DARK MAROON VINYL FINISH COAT. BOTH COATS MAY BE APPLIED IN THE SHOP, WITH FIELD SPOT PAINTING ONLY IN THE FIELD.



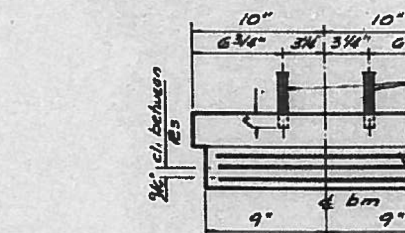
SECTION THRU PIER CAP



PLAN

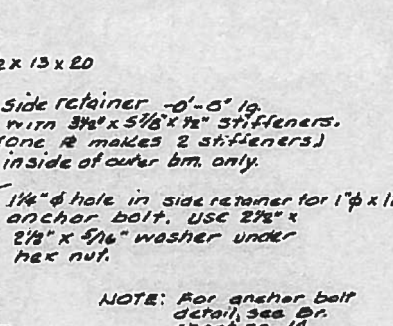


SECTION B-B

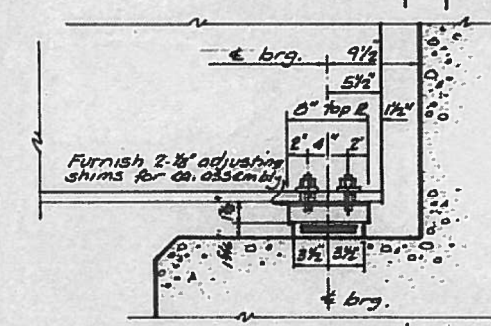


BEARING ASSEMBLY

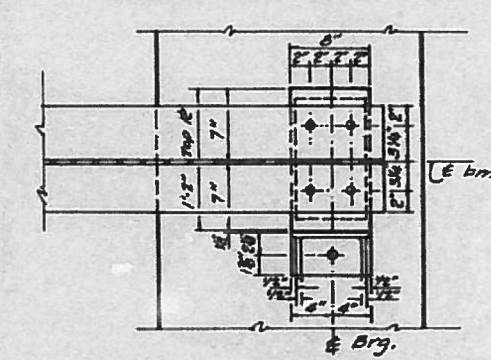
TYPE I BEARING DETAILS @ PIER 1



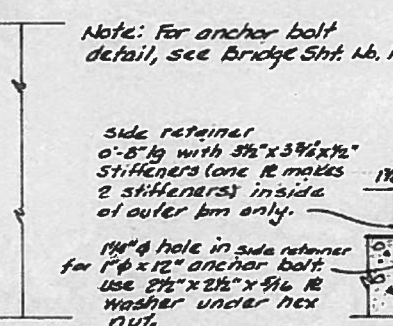
SECTION D-D



SECTION THRU E. ABUT. CAP



TYPE I BEARING DETAILS @ E. ABUTMENT



BEARING ASSEMBLY

BRIDGE BEARINGS

PROJECT: BH-F-793 ( )  
RTE: FA 793  
SECTION: T-2BE-1 over  
East Fork of Silver Creek  
COUNTY: MADISON  
STATION: 459+56

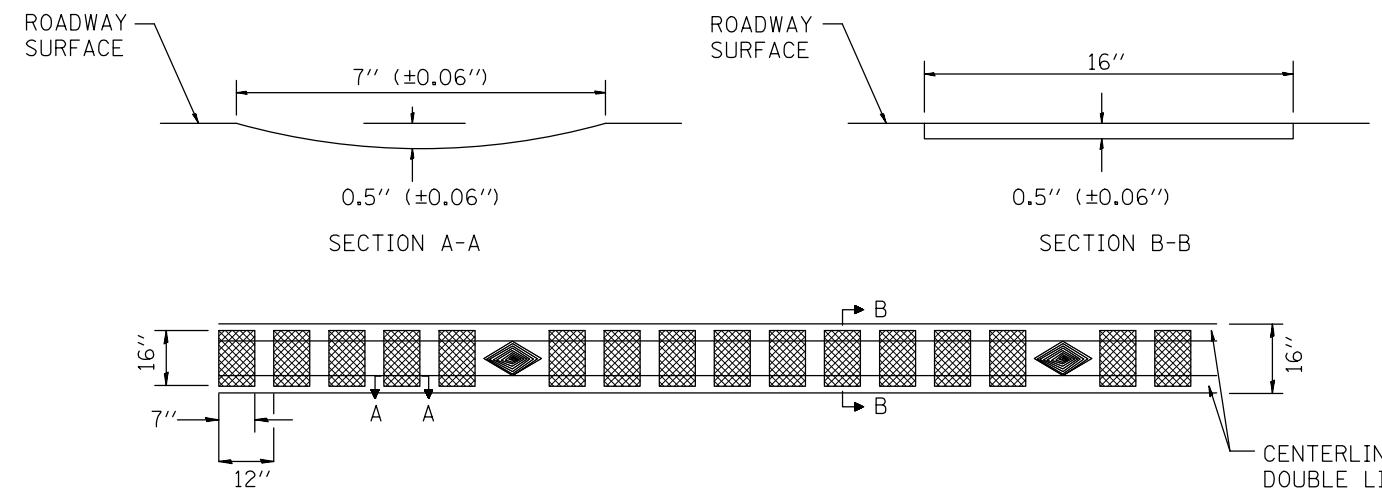
3/21/2018 1:08:05 PM

DESIGNED - AYV	DATE -
CHECKED -	REVISED
DRAWN - AYV	REVISED
CHECKED -	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

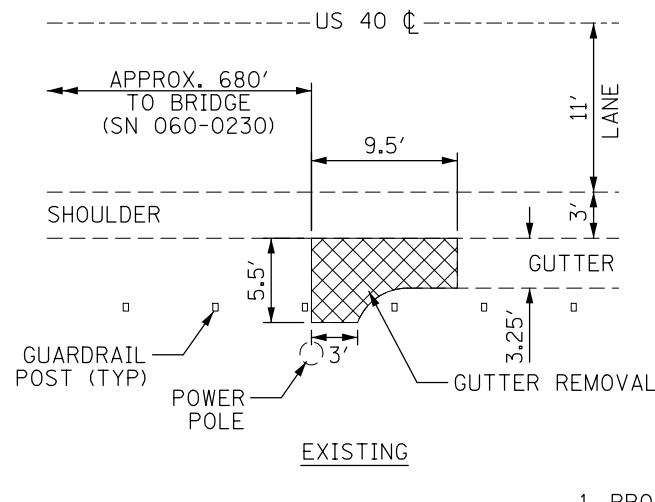
FOR INFORMATION ONLY - EXISTING BEARINGS  
SN 060-0230  
SHEET NO. 12 OF 12 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
793	31, 37, 32RS-3, T-2B-R-2	MADISON	32	31
			CONTRACT NO. 76J98	
ILLINOIS FED. AID PROJECT				

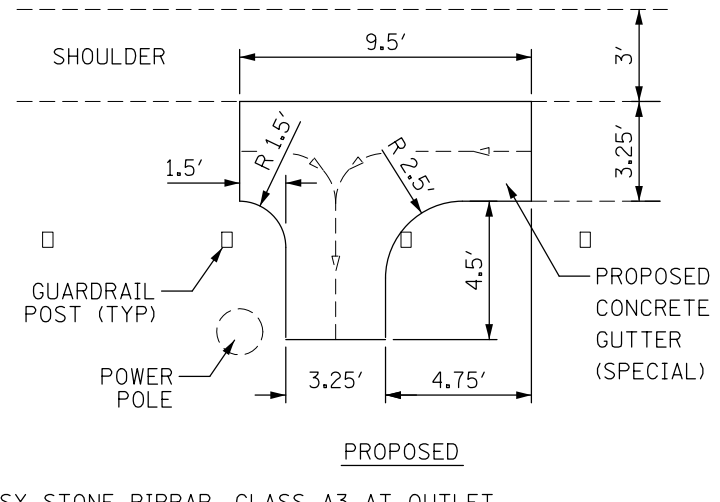


**CENTERLINE RUMBLE STRIP DETAIL**  
NOT TO SCALE

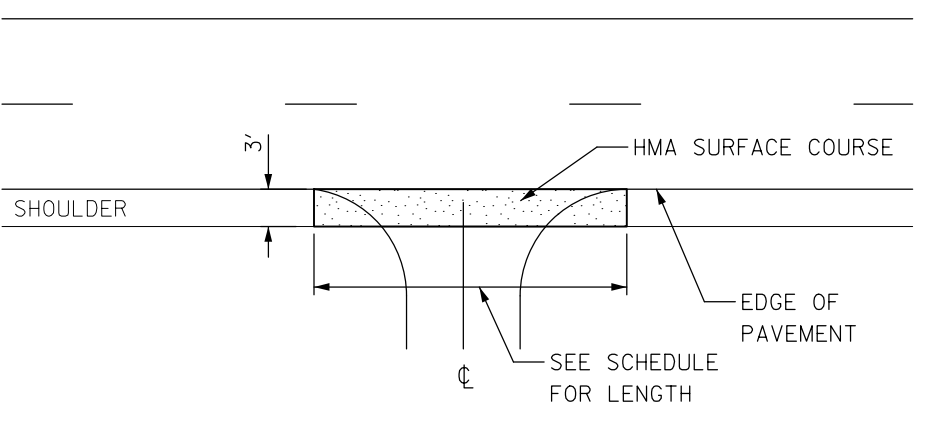
1. THE PROPOSED CENTERLINE PAVEMENT MARKINGS SHALL BE PLACED AFTER THE CONSTRUCTION OF THE RUMBLE STRIP. RUMBLE STRIP IS TO BE SPACED BETWEEN RRPM'S.
2. PAVEMENT MARKINGS USED ON CENTERLINE RUMBLE STRIP SHALL BE PAINT PAVEMENT MARKING - LINE 4".



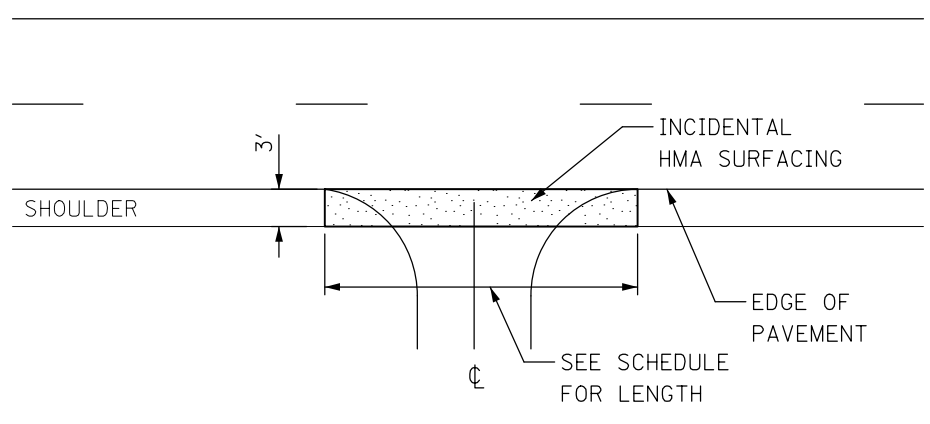
**GUTTER REPAIR DETAIL**  
(PLAN VIEW)  
NOT TO SCALE



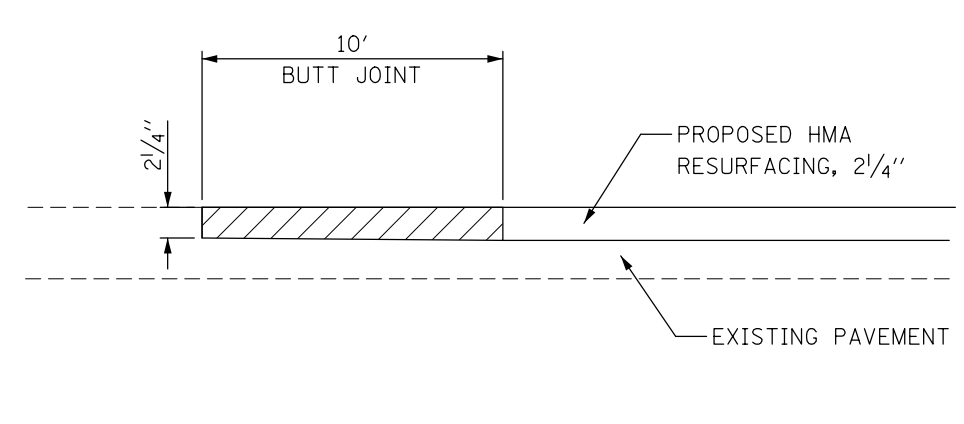
1. PROVIDE 3 SY STONE RIPRAP, CLASS A3 AT OUTLET
2. MINIMUM CONCRETE GUTTER THICKNESS OF 6 INCHES
3. MATCH EXISTING FLOWLINES AND PROVIDE POSITIVE DRAINAGE
4. IF THE GUARDRAIL IS TO BE REMOVED AND REERECTED FOR THE PROPOSED CONCRETE GUTTER (SPECIAL), THIS WORK SHALL BE INCLUDED IN THE COST OF GUTTER REMOVAL AND/OR CONCRETE GUTTER (SPECIAL).



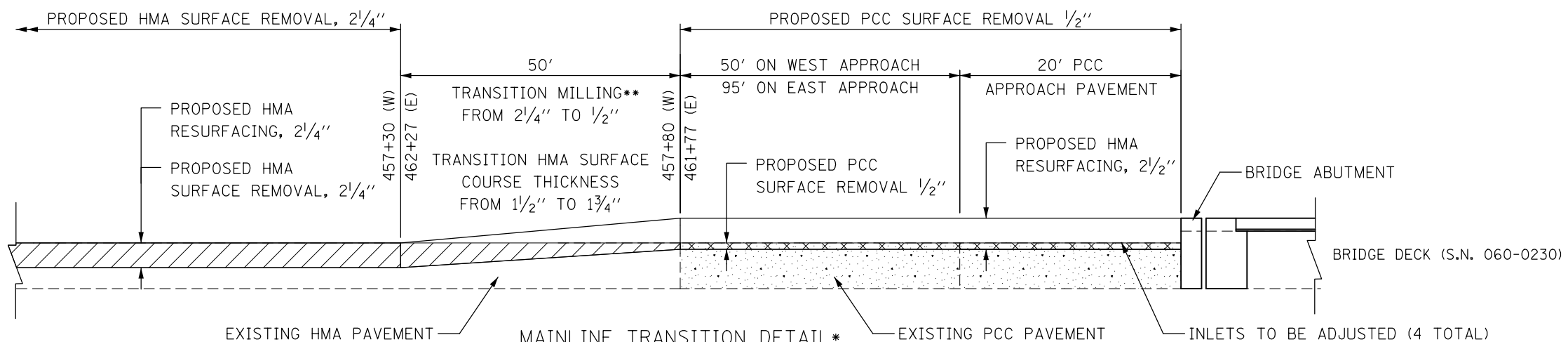
**SIDEROAD DETAIL**  
(PLAN VIEW)  
NOT TO SCALE



**ENTRANCE/DRIVEWAY DETAIL**  
(PLAN VIEW)  
NOT TO SCALE



**BUTT JOINT - HMA**  
(ELEVATION VIEW)  
NOT TO SCALE



(W) - WEST APPROACH  
(E) - EAST APPROACH

**MAINLINE TRANSITION DETAIL\***  
(ELEVATION VIEW)  
NOT TO SCALE

\* MIRROR DETAIL FOR EAST APPROACH  
\*\* TRANSITION MILLING PAID FOR AS HMA SURFACE REMOVAL 2 1/4"