

06-15-2018 LETTING ITEM 010

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**PROPOSED
HIGHWAY PLANS**

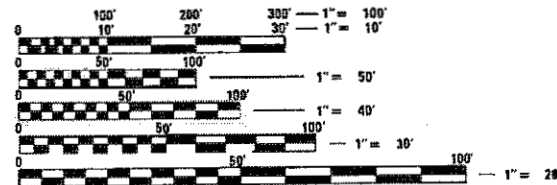
F.A.P. 345: US ROUTE 20 (LAKE STREET)
SHALES PKWY /BLUFF CITY BLVD. TO NORTH AVE.
SECTION: 7Y-RS-8
PROJECT: NHPP-TEFP(087)
RESURFACING (3P), MILLED RUMBLE STRIP
COOK COUNTY

F.A.P. 345	SECTION 7Y-RS-8	COUNTY COOK	TOTAL SHEETS 40	SHEET NO. 1
ILLINOIS CONTRACT NO. 62D12				

THE IMPROVEMENT IS LOCATED IN THE CITY OF ELGIN AND THE VILLAGES OF STREAMWOOD, HANOVER PARK, AND BARTLETT

RESURFACING OMISSIONS:
STA. 92+03 TO STA. 93+69.5
STA. 140+78 TO STA. 211+23
STA. 236+68.5 TO STA. 260+26.5

TRAFFIC DATA:
2017 ADT = 47,800
POSTED SPEED LIMIT = 40-45 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

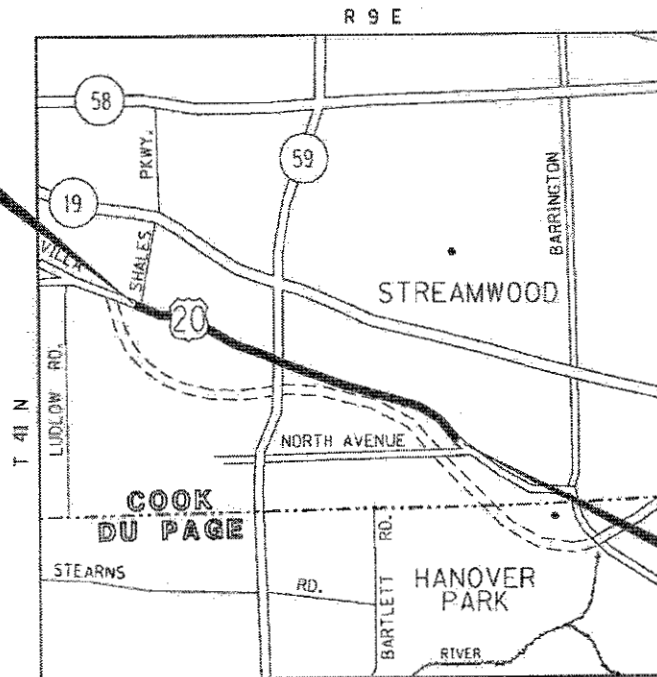
J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER KARI SMITH (847) 705-4437
PROJECT MANAGER FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62D12

C-91-430-16

IMPROVEMENT BEGINS
STA. 80+80.5



IMPROVEMENT ENDS
STA. 297+86.2

GROSS LENGTH = 21,705.7 FT. = 4.11 MILES
NET LENGTH = 12,136.2 FT. = 2.30 MILES



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED *March 22, 2018*
A. P. Smith
REGIONAL ENGINEER

May 11, 2018
Paul P. Smith
ENGINEER OF DESIGN AND ENVIRONMENT

May 11, 2018
Paul P. Smith
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
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40	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)

STATE HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442101-08	CLASS B PATCHES
442201-03	CLASS C AND D PATCHES
602011-02	CATCH BASIN TYPE C
604001-04	FRAMES AND LIDS TYPE 1
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
642001-02	SHOULDER RUMBLE STRIPS, 16 IN.
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH
701421-08	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS ≥ 45 MPH TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≥ 45 MPH
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701602-09	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-07	TRAFFIC CONTROL DEVICES
814001-03	HANDHOLES

GENERAL NOTES (CONTINUED..)

- THE CONTRACTOR SHALL CONTACT DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAME AND LID UNLESS OTHERWISE SPECIFIED IN THE PLANS.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER VIA E-MAIL AT DON.CHIARUGI@ILLINOIS.GOV, A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPLACEMENT OF ANY DETECTOR LOOPS DAMAGED DURING CONSTRUCTION.
- THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
- LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED IN THE PLANS)] WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- CONTACT THE IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING WEED CONTROL AND FORESTRY WORK FOR LAYOUT.
- LONGITUDINAL JOINT AND CRACK PATCHING SHALL BE PERFORMED PRIOR TO OVERLAYING EXISTING PAVEMENT.

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 AND CUAN AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE CITY OF ELGIN AND THE VILLAGES OF STREAMWOOD, HANOVER PARK, AND BARTLETT.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DEVON AVE. (DES PLAINES RIVER RD. TO CANFIELD AVE.) GENERAL NOTES AND INDEX SHEET	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\143016\Meta\Design\143016-sh-t-plen.dgn		CHECKED -	REVISED -			345	7Y-RS-8	COOK	1	1	
PLOT SCALE = 100.0000' / 1in.		DATE -	REVISED -			CONTRACT NO. 62D12					
PLOT DATE = 5/3/2018						SCALE: NONE	SHEET NO.	OF	SHEETS	STA.	TO
ILLINOIS FED. AID PROJECT											

SUMMARY OF QUANTITIES			TOTAL QUANTITIES		CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	74815	74815					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	43779	43779					
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	295	295					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1350	1350					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	635	635					
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	51530	51530					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	1092	1092					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	74815	74815					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	43779	43779					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	295	295					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1350	1350					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	635	635					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	490	490					
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	395	395					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES		CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE					
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	4981	4981					
K0029618	WEED CONTROL, BROADLEAF IN TURF	GALLON	2	2					
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	65.8	65.8					
X0325222	WEED CONTROL, BASAL TREATMENT	GALLON	3	3					
X4060004	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80	TON	10322	10322					
X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SO YD	1798	1798					
X4405030	LONGITUDINAL PARTIAL DEPTH REMOVAL, 3"	FOOT	4300	4300					
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	950	950					
X4420900	LONGITUDINAL PARTIAL DEPTH PATCHING	TON	161	161					
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	8	8					
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	67903	67903					
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	3600	3600					
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	95	95					
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	102.8	102.8					
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	47953	47953					
Z0064800	SELECTIVE CLEARING	UNIT	6	6					
Z0076600	TRAINEES	HOUR	500	500					
Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500	500					

FILE NAME =	USER NAME =	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S 20 (SHALES PARKWAY /BLUFF CITY BLVD. TO NORTH AVE.) SUMMARY OF QUANTITIES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\NLD04E\BID\HT\EG\hinds\pww\DOT\Documents\2007	Or\Task\District\Projects\04306\CA\Drawn\Design\04306	CHECKED -	REVISED -			345	7Y-RS-B	COOK	40	5	
	PLOT SCALE = 100,000' / 1"	DATE -	REVISED -			CONTRACT NO. 62D12		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			
	PLOT DATE = 3/23/2018	DATE -	REVISED -			SCALE: SHEET NO. OF SHEETS STA. TO STA.					

* SPECIALTY ITEM 100% STATE 0042

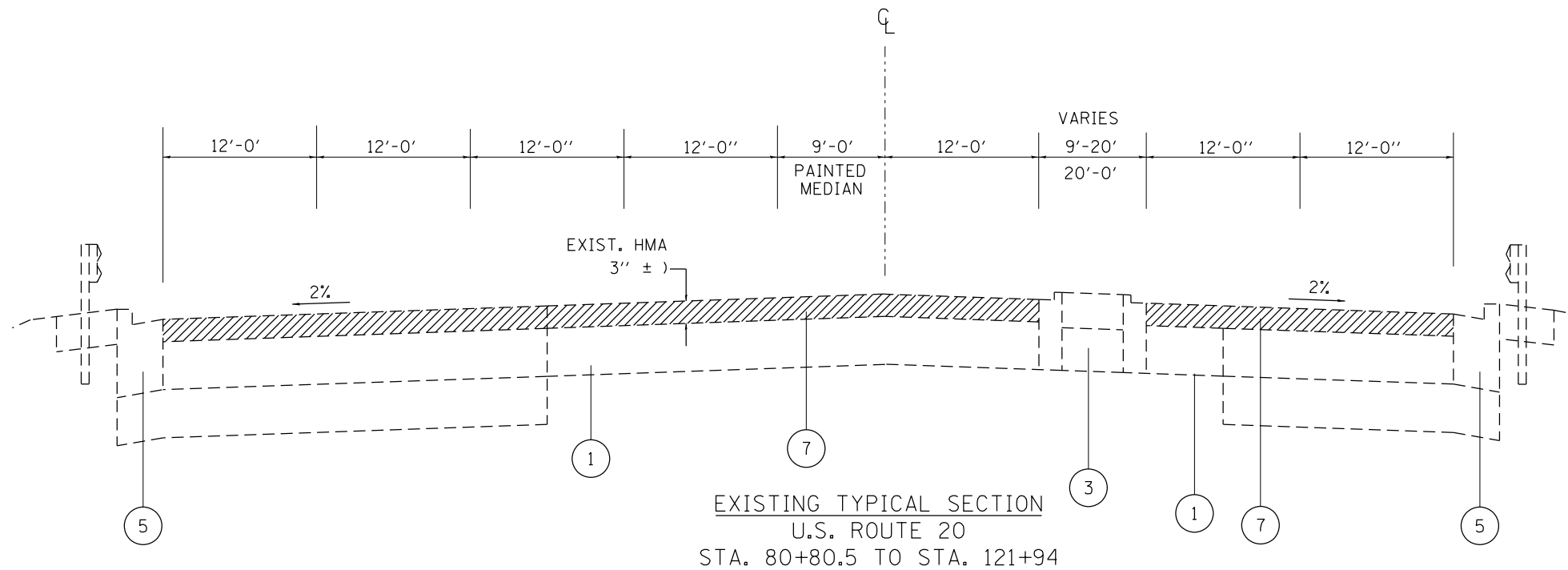
LEGEND

- ① EXISTING PCC PAVEMENT, ± 9"
- ② EXISTING HMA AFTER MILLING, ± 8"
- ③ EXISTING MEDIAN
- ④ EXISTING STABILIZED HMA MEDIAN WITH COMB. CONC. CURB AND GUTTER, TYPE M-2.12
- ⑤ EXISTING COMB. CONCRETE CURB AND GUTTER
- ⑥ EXISTING AGGREGATE SHOULDER
- ⑦ PROPOSED HMA SURFACE REMOVAL, 3" (SEE PLAN FOR LOCATION)
- ⑧ PROPOSED MEDIAN REMOVAL
- ⑨ PROPOSED P.C.C. SURFACE REMOVAL VARIABLE DEPTH (BD-33)
- ⑩ PROPOSED HMA BASE COURSE WIDENING, 9" (SEE PLAN FOR LOCATION)
- ⑪ PROPOSED HMA BINDER COURSE COURSE, MIX "D" N50 (IL 9.5 mm) (± 12")
- ⑫ PROPOSED POLY. HMA SURFACE COURSE, SMA, 9.5, N80, 2"
- ⑬ PROPOSED POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑭ PROPOSED HMA SURFACE COURSE, MIX "D" N50 (2") (STABILIZED MEDIAN HMA SURFACE)
- ⑮ PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B, 6"
- ⑯ PROPOSED COMB. CONC. CURB AND GUTTER, TYPE M-2.12
- ⑰ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑱ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑲ PROPOSED SHOULDER RUMBLE STRIPS, 16 INCH (INSTALL PER HWY STD. 642001)
- ⑳ PROPOSED LONGITUDINAL JOINT SEALANT (TO BE PLACED OVER THE POLY. LEVELING BINDER)

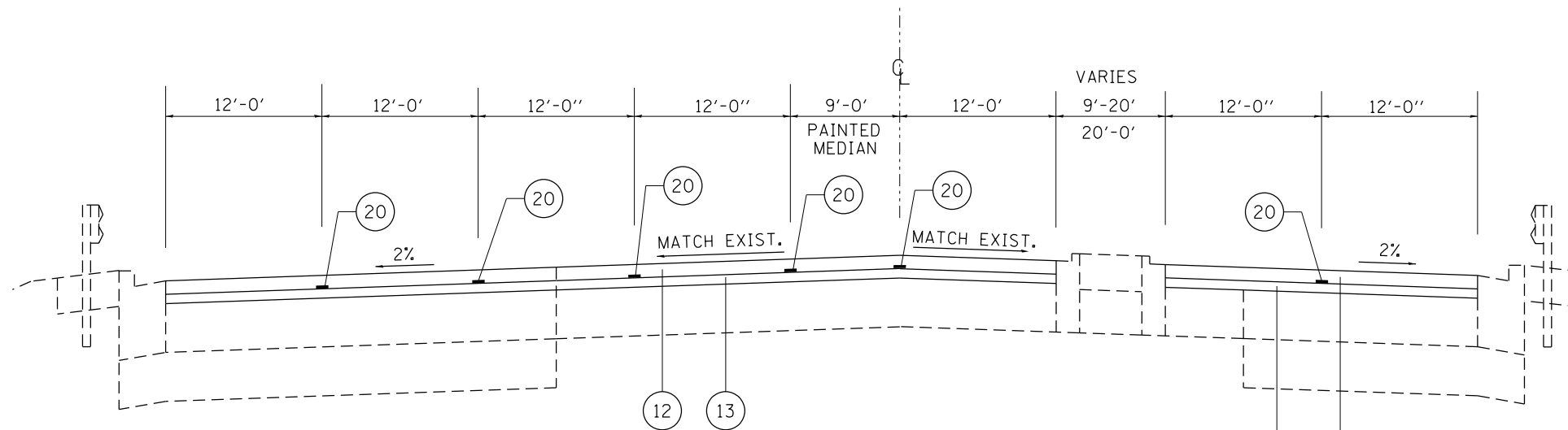
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ N _{DES}	QMP
<i>ROADWAY RESURFACING:</i>		
POLY. HMA SURFACE COURSE, SMA, 9.5, N80 (IL 9.5 mm), 2"	3.5% AT 80 GYR.	PPF
POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"	3.5% AT 50 GYR.	QCP
<i>ROADWAY WIDENING AT PROPOSED MEDIAN</i>		
9" HMA BASE COURSE WIDENING (HMA BINDER, IL-19.0)	4% AT 70 GYR.	QC / QA
<i>STABILIZED MEDIAN SURFACE</i>		
HMA SURFACE COURSE, MIX "D" N50 (IL 9.5 mm) (2")	4% AT 50 GYR.	QC / QA
<i>STABILIZED MEDIAN FILL</i>		
HMA BINDER COURSE COURSE, MIX "D" N50 (IL 9.5 mm) (± 12")	4% AT 50 GYR.	QC / QA
<i>HOT-MIX ASPHALT PATCHING:</i>		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC / QA
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (3")	4% AT 70 GYR.	QC / QA
LONGITUDINAL PARTIAL DEPTH PATCHING (HMA BINDER IL-19.0)	4% AT 70 GYR.	QC / QA

QMP Designation: Quality Control/Quality Assurance (QC/QA);
Quality Control for Performance (QCP); Pay for Performance (PPF)



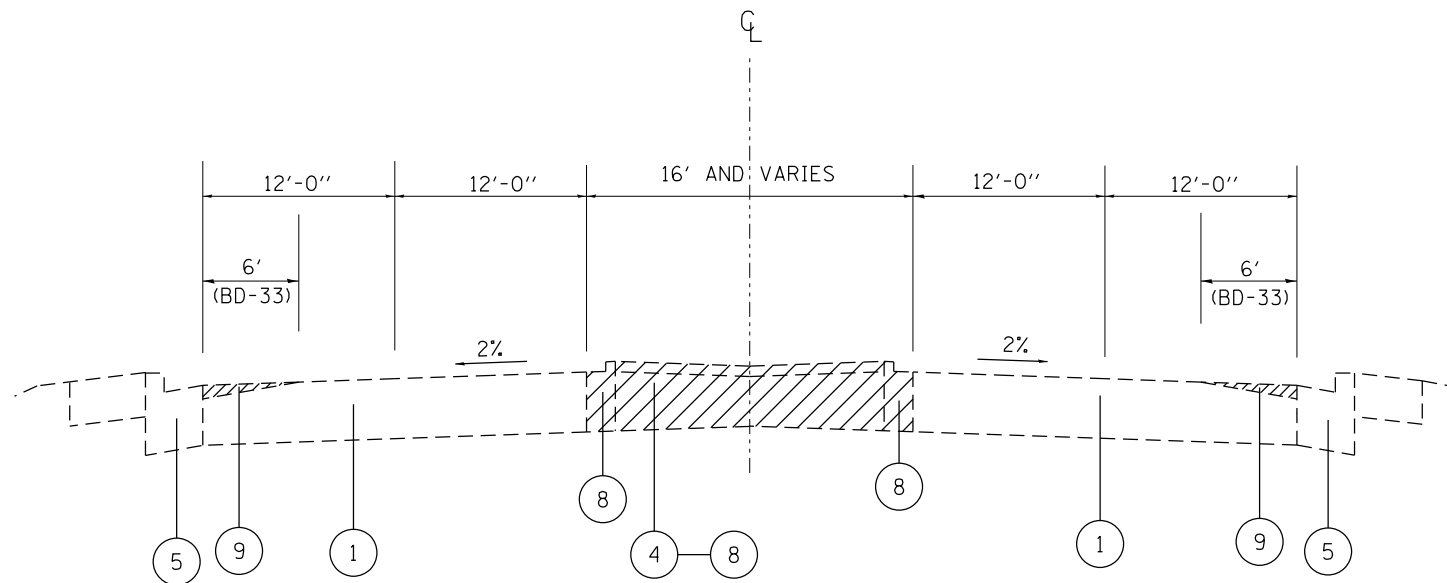
EXISTING TYPICAL SECTION
U.S. ROUTE 20
STA. 80+80.5 TO STA. 121+94



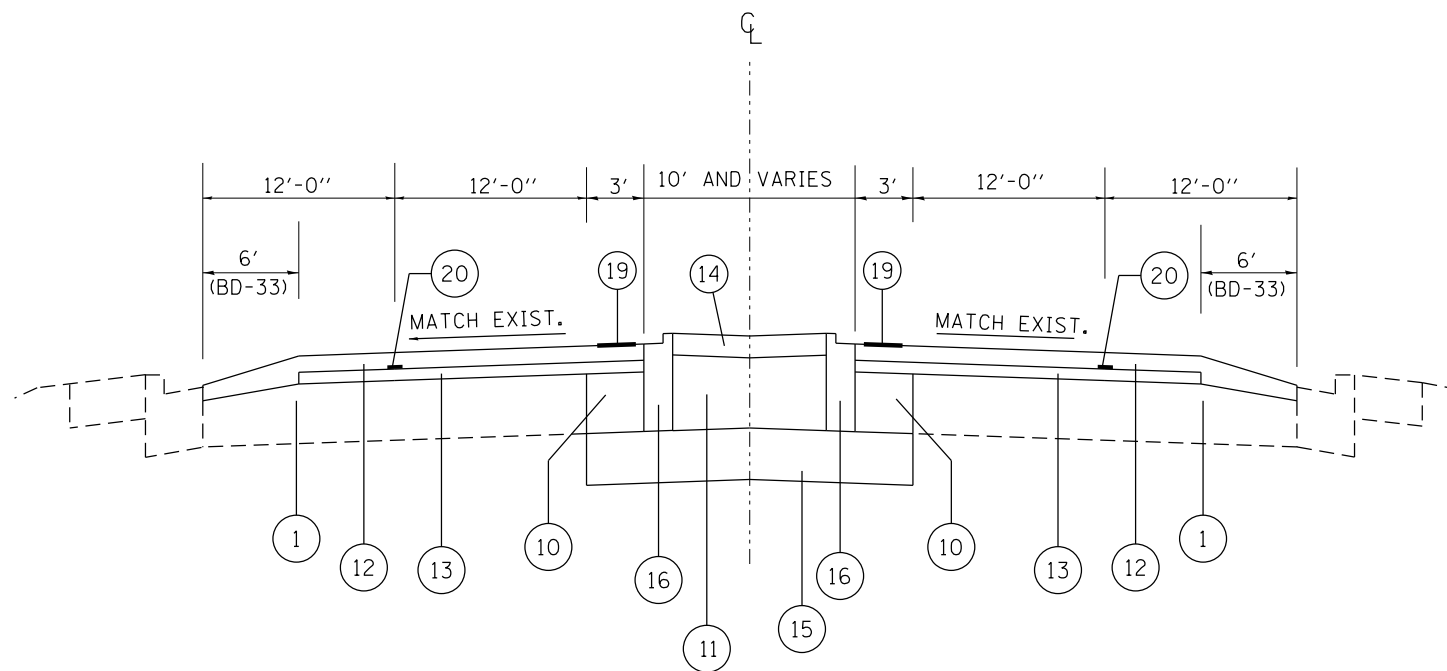
PROPOSED TYPICAL SECTION
U.S. ROUTE 20
STA. 80+80.5 TO STA. 121+94

NOTE:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.
3. THE CONTRACTOR SHALL DO PAVEMENT PATCHING FIRST, PER BD-22 DETAIL, THEN DO ROADWAY MILLING AT STA. 80+80.5 TO STA. 121+94 (EXCEPT AT RESURFACING OMISSIONS).
4. THE CONTRACTOR SHALL MILL THE ROADWAY FIRST, THEN DO PAVEMENT PATCHING PER BD-22 DETAIL AT STA. 211+21 TO STA. 291+62 (EXCEPT AT RESURFACING OMISSIONS).
5. THE PROPOSED LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED LEVELING BINDER WHERE THE SURFACE MIX JOINT WILL BE LOCATED.
6. LONGITUDINAL JOINT AND CRACK PATCHING SHALL BE PERFORMED PRIOR TO OVERLAYING EXISTING PAVEMENT.
7. ASPHALTIC EMULSION SLURRY SEAL SHALL BE APPLIED ON THE BARRIER MEDIAN, FROM INSIDE CURB TO INSIDE CURB INCLUDING UNDER GUARDRAILS, ON US 20 FROM STA. 157+73 TO STA. 271+64 AND AT OTHER VARIOUS LOCATIONS SHOWN IN THE PLANS AND AS DIRECTED BY THE RESIDENT ENGINEER.
8. FOR THE AREAS WHERE A SLURRY SEAL WILL BE PLACED, THE AREA SHALL FIRST BE CRACK SEALED USING FIBER-MODIFIED ASPHALT CRACK SEALING, THEN THE TACK COAT SHALL BE APPLIED AT A RATE THAT WILL PROVIDE A RESIDUAL RATE OF 0.025 LB/SOFT ACCORDING TO ART. 406.05(b).



EXISTING TYPICAL SECTION
U.S. ROUTE 20
STA. 129+18 TO STA. 138+74.1

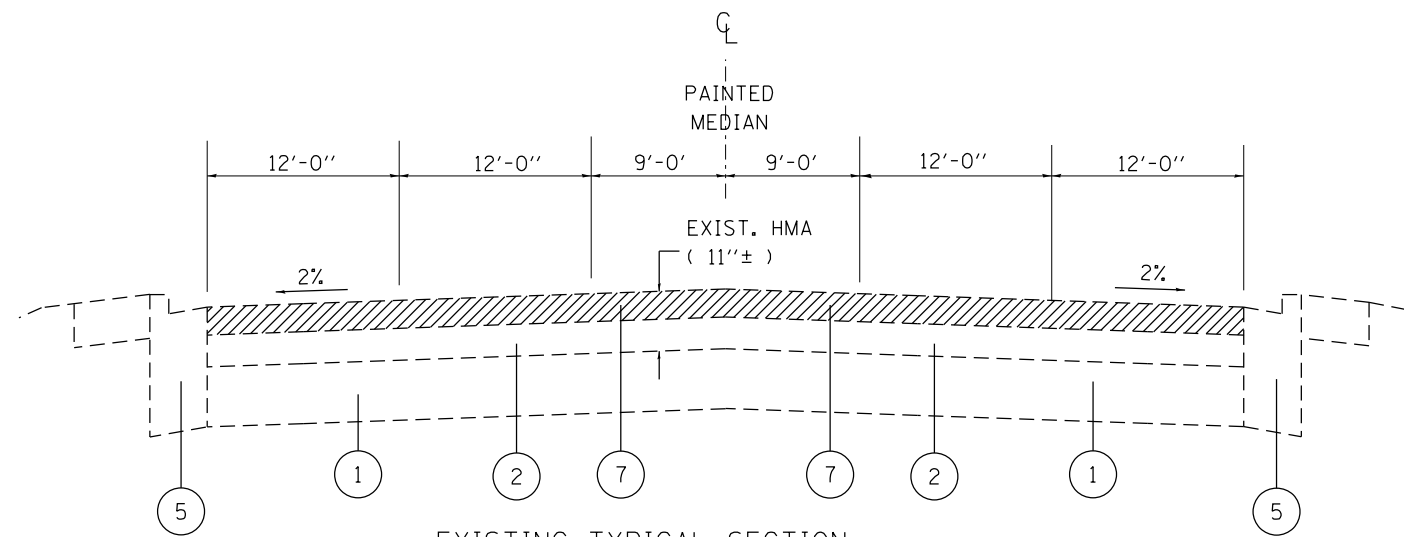


PROPOSED TYPICAL SECTION
U.S. ROUTE 20
STA. 129+18 TO STA. 138+74.1

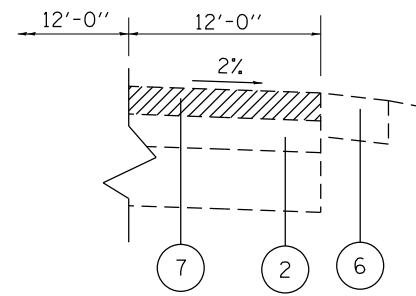
LEGEND

- ① EXISTING PCC PAVEMENT, ± 9"
- ② EXISTING HMA AFTER MILLING, ± 8"
- ③ EXISTING MEDIAN
- ④ EXISTING STABILIZED HMA MEDIAN WITH COMB. CONC. CURB AND GUTTER, TYPE M-2.12
- ⑤ EXISTING COMB. CONCRETE CURB AND GUTTER
- ⑥ EXISTING AGGREGATE SHOULDER
- ⑦ PROPOSED HMA SURFACE REMOVAL, 3" (SEE PLAN FOR LOCATION)
- ⑧ PROPOSED MEDIAN REMOVAL
- ⑨ PROPOSED P.C.C. SURFACE REMOVAL VARIABLE DEPTH (BD-33)
- ⑩ PROPOSED HMA BASE COURSE WIDENING, 9" (SEE PLAN FOR LOCATION)
- ⑪ PROPOSED HMA BINDER COURSE COURSE, MIX "D" N50 (IL 9.5 mm) (± 12")
- ⑫ PROPOSED POLY. HMA SURFACE COURSE, SMA, 9.5, N80, 2"
- ⑬ PROPOSED POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑭ PROPOSED HMA SURFACE COURSE, MIX "D" N50 (2") (STABILIZED MEDIAN HMA SURFACE)
- ⑮ PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B, 6"
- ⑯ PROPOSED COMB. CONC. CURB AND GUTTER, TYPE M-2.12
- ⑰ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑱ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑲ PROPOSED SHOULDER RUMBLE STRIPS, 16 INCH (INSTALL PER HWY STD. 642001)
- ⑳ PROPOSED LONGITUDINAL JOINT SEALANT (TO BE PLACED OVER THE POLY. LEVELING BINDER)

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S 20 (SHALES PARKWAY /BLUFF CITY BLVD. TO NORTH AVE.) EXISTING AND PROPOSED TYPICAL SECTIONS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\143016\Design\143016-sh-t-plen.dgn	DRWN	REVISION	REVISION			345	7Y-RS-8	COOK	40	8
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISION	REVISION			CONTRACT NO. 62D12				
PLOT DATE = 5/1/2018	DATE -	REVISION	REVISION			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
				SCALE:	SHEET NO. OF SHEETS	STA.	TO STA.			



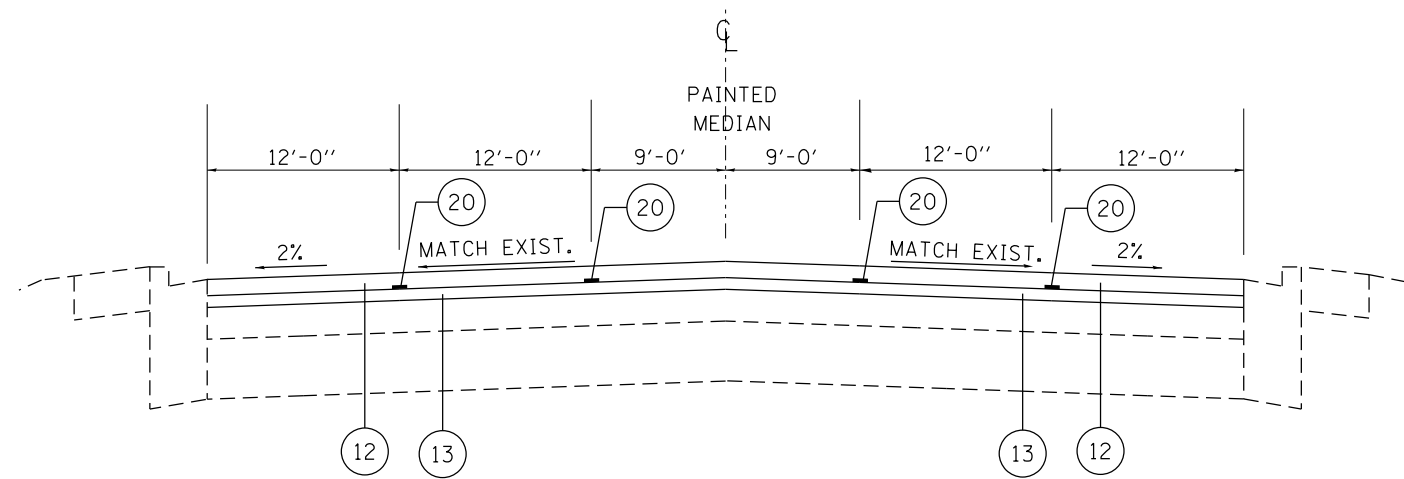
EXISTING TYPICAL SECTION
U.S. ROUTE 20
STA. 211+21 TO STA. 236+68
STA. 260+26 TO STA. 297+86.2



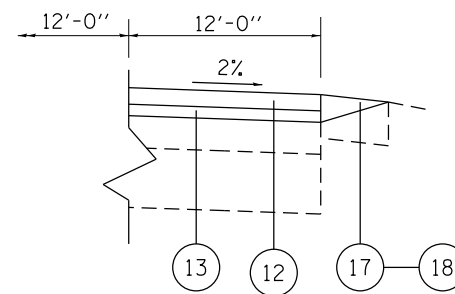
NOTE: SEE PLAN FOR LOCATION OF AGGREGATE SHOULDER

LEGEND

- ① EXISTING PCC PAVEMENT, ± 9"
- ② EXISTING HMA AFTER MILLING, ± 8"
- ③ EXISTING MEDIAN
- ④ EXISTING STABILIZED HMA MEDIAN WITH COMB. CONC. CURB AND GUTTER, TYPE M-2.12
- ⑤ EXISTING COMB. CONCRETE CURB AND GUTTER
- ⑥ EXISTING AGGREGATE SHOULDER
- ⑦ PROPOSED HMA SURFACE REMOVAL, 3" (SEE PLAN FOR LOCATION)
- ⑧ PROPOSED MEDIAN REMOVAL
- ⑨ PROPOSED P.C.C. SURFACE REMOVAL VARIABLE DEPTH (BD-33)
- ⑩ PROPOSED HMA BASE COURSE WIDENING, 9" (SEE PLAN FOR LOCATION)
- ⑪ PROPOSED HMA BINDER COURSE COURSE, MIX "D" N50 (IL 9.5 mm) (± 12")
- ⑫ PROPOSED POLY. HMA SURFACE COURSE, SMA, 9.5, N80, 2"
- ⑬ PROPOSED POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑭ PROPOSED HMA SURFACE COURSE, MIX "D" N50 (2") (STABILIZED MEDIAN HMA SURFACE)
- ⑮ PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B, 6"
- ⑯ PROPOSED COMB. CONC. CURB AND GUTTER, TYPE M-2.12
- ⑰ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑱ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑲ PROPOSED SHOULDER RUMBLE STRIPS, 16 INCH (INSTALL PER HWY STD. 642001)
- ⑳ PROPOSED LONGITUDINAL JOINT SEALANT (TO BE PLACED OVER THE POLY. LEVELING BINDER)



PROPOSED TYPICAL SECTION
U.S. ROUTE 20
STA. 211+21 TO STA. 236+68
STA. 260+26 TO STA. 297+86.2



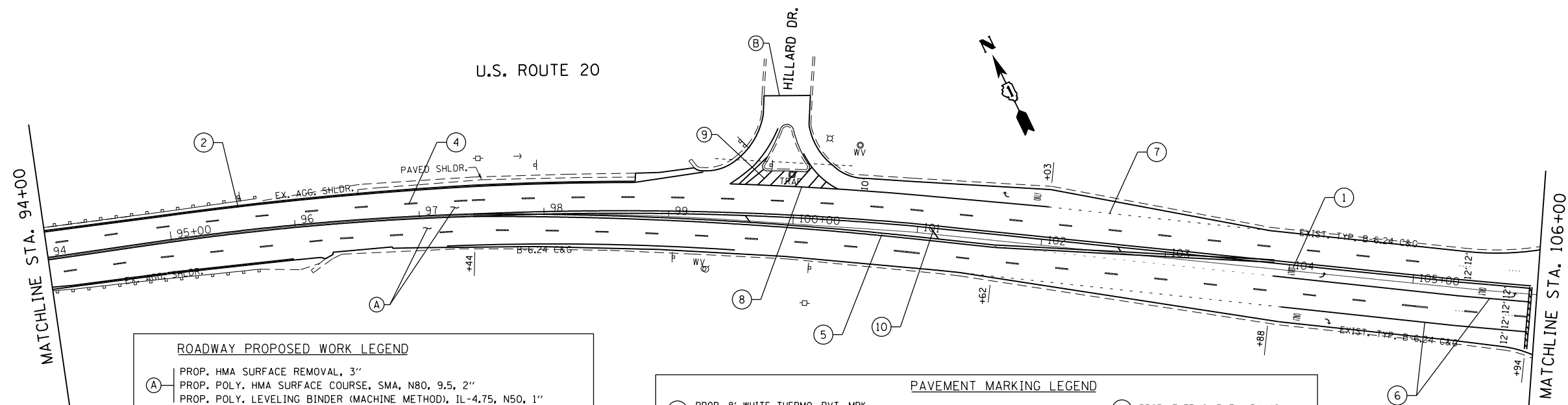
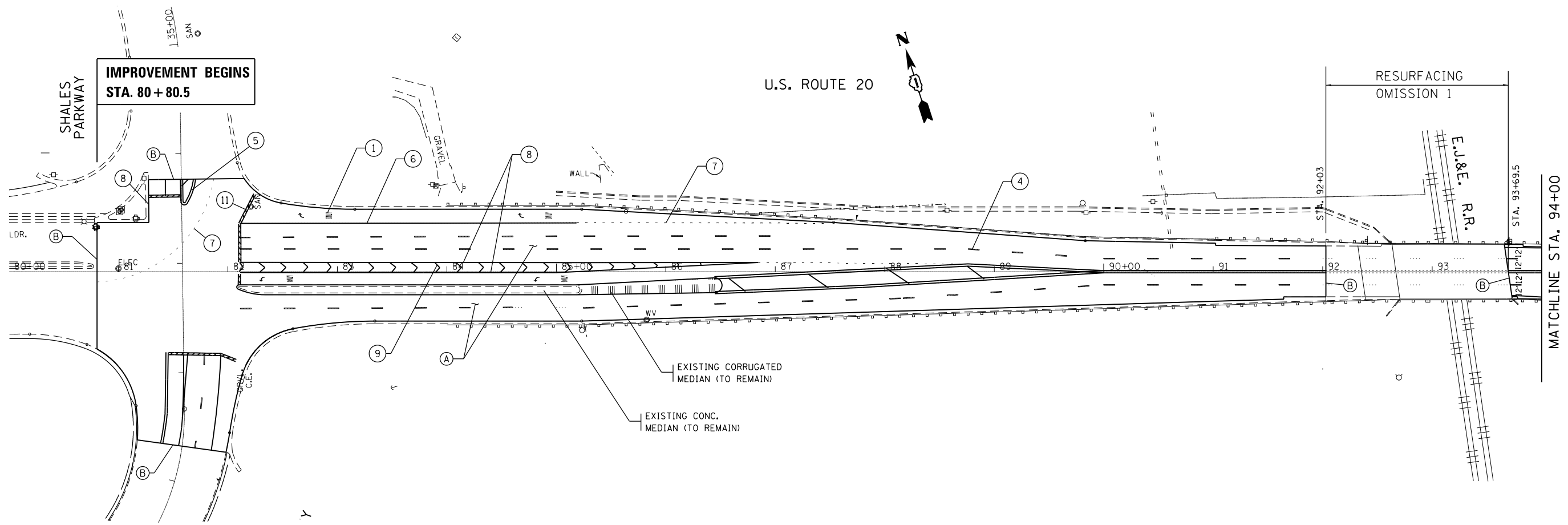
NOTE: SEE PLAN FOR LOCATION OF AGGREGATE SHOULDER

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S 20 (SHALES PARKWAY /BLUFF CITY BLVD. TO NORTH AVE.) EXISTING AND PROPOSED TYPICAL SECTIONS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\143016\Design\143016-sh-t-plan.dgn	DRW	REVISIONS	REVISIONS			345	7Y-RS-8	COOK	40	9
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISIONS	REVISIONS			CONTRACT NO. 62D12				
PLOT DATE = 5/1/2018	DATE -	REVISIONS	REVISIONS			SCALE:	SHEET NO. OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1

US ROUTE 20: FROM SHALES PARKWAY / BLUFF CITY BOULEVARD
TO NORTH AVENUE

DIRECTION, SHAPE, AND LANE	STATION	PATCH WIDTH (FT)	PATCH LENGTH (FT)	PATCH AREA (SQ FT)	PATCH AREA (SQ YD)	TYPE I (SQ YD)	TYPE II (SQ YD)	TYPE III (SQ YD)	TYPE IV (SQ YD)
	140+78			0					
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
EB, LN2 INTERSECTION		12	6	72	8.00		8.00		
EB, LN2		12	6	72	8.00		8.00		
EB, LN1		12	6	72	8.00		8.00		
EB, LN2		12	6	72	8.00		8.00		
WB, LN1 CHURCH EXT		12	6	72	8.00		8.00		
EB, LN2		12	8	96	10.67		10.67		
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
EB, LN1 RV ENTRANCE		12	12	144	16.00			16.00	
EB, LN2		12	12	144	16.00			16.00	
WB, LN1		12	6	72	8.00		8.00		
WB, LN1		12	6	72	8.00		8.00		
WB, LN1 2ND RV ENTR		12	8	96	10.67		10.67		
WB, LN1		12	6	72	8.00		8.00		
WB, LN1		12	6	72	8.00		8.00		
EB, LN2		12	6	72	8.00		8.00		
WB, LN2 MORETTIS ENT		12	8	96	10.67		10.67		
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
EB, LN1		12	6	72	8.00		8.00		
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
EB, LEFT LN MORETTIS ENT		6	6	36	4.00	4.00			
EB, LN1		12	6	72	8.00		8.00		
EB, LN2		12	6	72	8.00		8.00		
WB, LN1		12	6	72	8.00		8.00		
EB, LN1		12	6	72	8.00		8.00		
EB, LN2		12	6	72	8.00		8.00		
WB, LN1		12	10	120	13.33		13.33		
WB, LN2		12	10	120	13.33		13.33		
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
EB, LN1		12	6	72	8.00		8.00		
EB, LN2		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
MONARCH DR				0					

DIRECTION, SHAPE, AND LANE	STATION	PATCH WIDTH (FT)	PATCH LENGTH (FT)	PATCH AREA (SQ FT)	PATCH AREA (SQ YD)	TYPE I (SQ YD)	TYPE II (SQ YD)	TYPE III (SQ YD)	TYPE IV (SQ YD)
MONARCH DR				0					
WB, LN1		12	6	72	8.00		8.00		
EB, LN1		12	8	96	10.67		10.67		
EB, LN2		12	8	96	10.67		10.67		
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
WB, LN2		12	8	96	10.67		10.67		
EB, LN1		12	6	72	8.00		8.00		
EB, RT LN		6	6	36	4.00	4.00			
EB, LN1		12	6	72	8.00		8.00		
EB, RT LN		12	6	72	8.00		8.00		
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
WB, LN1		12	12	144	16.00			16.00	
WB, LN2		12	12	144	16.00			16.00	
WB RAMP		12	12	144	16.00			16.00	
WB SHLDR		8	12	96	10.67		10.67		
WB SHLDR		8	45	360	40.00				40.00
EB, RT LN		12	6	72	8.00		8.00		
EB, RT LN		12	6	72	8.00		8.00		
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
WB SHLDR		12	12	144	16.00			16.00	
EB RT LN		12	12	144	16.00			16.00	
EB SHLDR		12	12	144	16.00			16.00	
US20/IL59 BRIDGE				0					
EB, LN1		12	12	144	16.00			16.00	
EB, LN2		12	12	144	16.00			16.00	
EB ENT		10	12	120	13.33		13.33		
WB SHLDR		6	6	36	4.00	4.00			
WB SHLDR		6	6	36	4.00	4.00			
WB, LN1		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
WB, LN2		12	6	72	8.00		8.00		
EB, LN2		12	10	120	13.33		13.33		
EB, LT LN		12	10	120	13.33		13.33		
EB, LN1		12	10	120	13.33		13.33		
EB, LN2		12	10	120	13.33		13.33		
EB, LN1		12	6	72	8.00		8.00		
OLD LAKE ST/FRNTAGE RD #1				0					



ROADWAY PROPOSED WORK LEGEND

(A)	PROP. HMA SURFACE REMOVAL, 3"
(B)	PROP. POLY. HMA SURFACE COURSE, SMA, N80, 9.5, 2"
(C)	PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
(D)	PROP. HMA SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(E)	PROP. PCC SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(F)	PROP. LONGITUDINAL PARTIAL DEPTH REMOVAL, 3"
(G)	PROP. LONGITUDINAL PARTIAL PATCHING
(H)	PROP. POLY. HMA SURFACE COURSE, SMA, N80, 9.5, 2"
(I)	PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
(J)	(TO BE PLACED PER DISTRICT DETAIL BD-33)

PAVEMENT MARKING LEGEND

(1)	PROP. 8' WHITE THERMO. PVT. MKR. LETTERS AND SYMBOLS (TYP.)	(9)	PROP. THERMO. PVT. MKR.-12" GORE MKR., CHEVRON WHITE (TYP.)
(2)	PROP. THERMO. PVT. MKR.-4" EDGELINE, SOLID WHITE (TYP.)	(10)	PROP. THERMO. PVT. MKR.-12" DIAGONAL, YELLOW (TYP.)
(3)	PROP. THERMO. PVT. MKR.-4" EDGELINE, SOLID YELLOW (TYP.)	(11)	PROP. THERMO. PVT. MKR.-24" STOP LINE, WHITE (TYP.)
(4)	PROP. THERMO. PVT. MKR.-4" 10' DASH - 30' SKIP, WHITE (TYP.)	(5)	PROP. THERMO. PVT. MKR.-4" CENTERLINE, DOUBLE YELLOW (TYP.)
(6)	PROP. THERMO. PVT. MKR.-6" SOLID LINE, WHITE (TYP.)	(8)	PROP. THERMO. PVT. MKR.-8" GORE MKR., SOLID WHITE (TYP.)
(7)	PROP. THERMO. PVT. MKR.-6" DOTTED LINE, WHITE (2' DASH, 6" SKIP) (TYP.)		

NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-11 AND TC-13

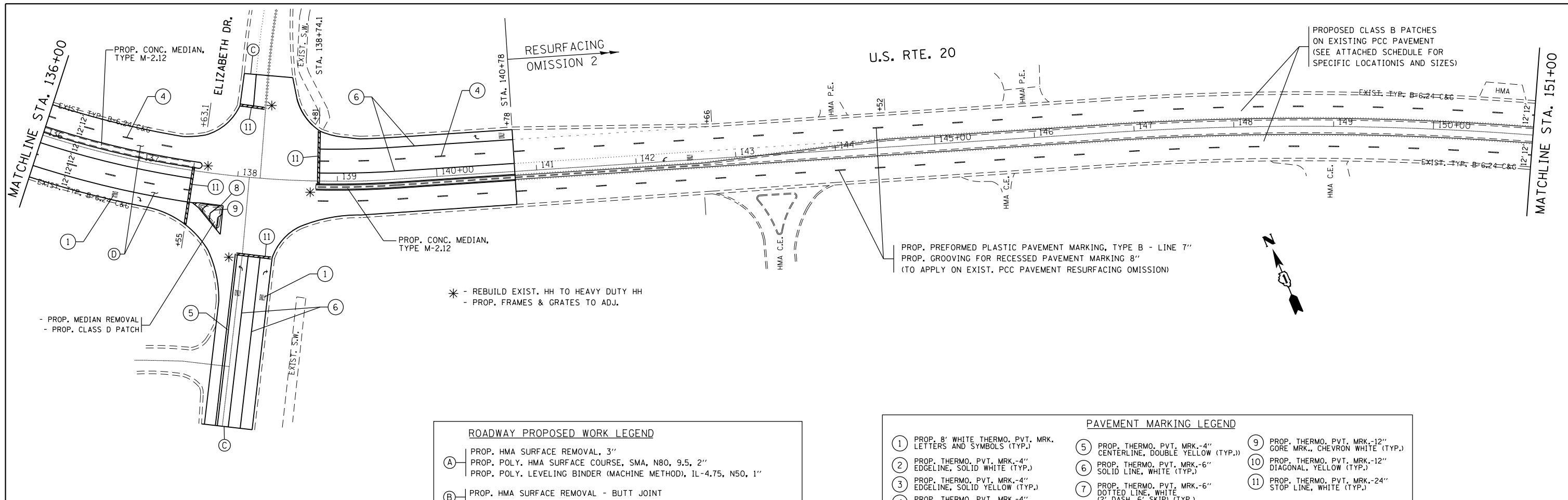
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Default	DATE -	REVISED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN U.S. RTE. 20
(SHALES PARKWAY /BLUFF CITY BLVD. TO NORTH AVE.)**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA. 106+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-RS-B	COOK	40	12
CONTRACT NO. 62D12				
ILLINOIS FED. AID PROJECT				

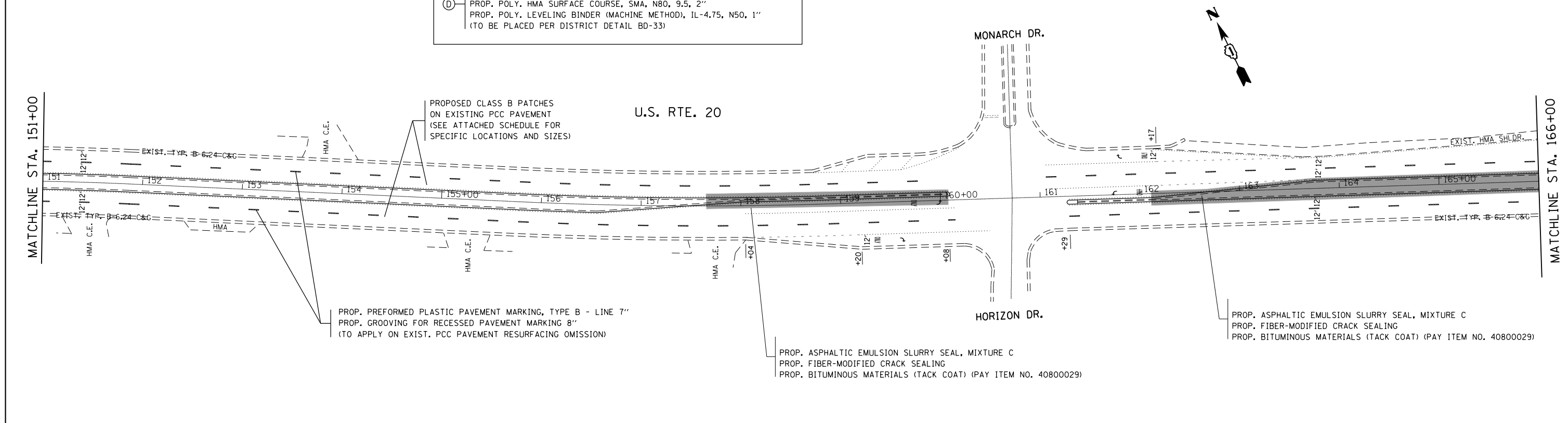


* - REBUILD EXIST. HH TO HEAVY DUTY HH
 - PROP. FRAMES & GRATES TO ADJ.

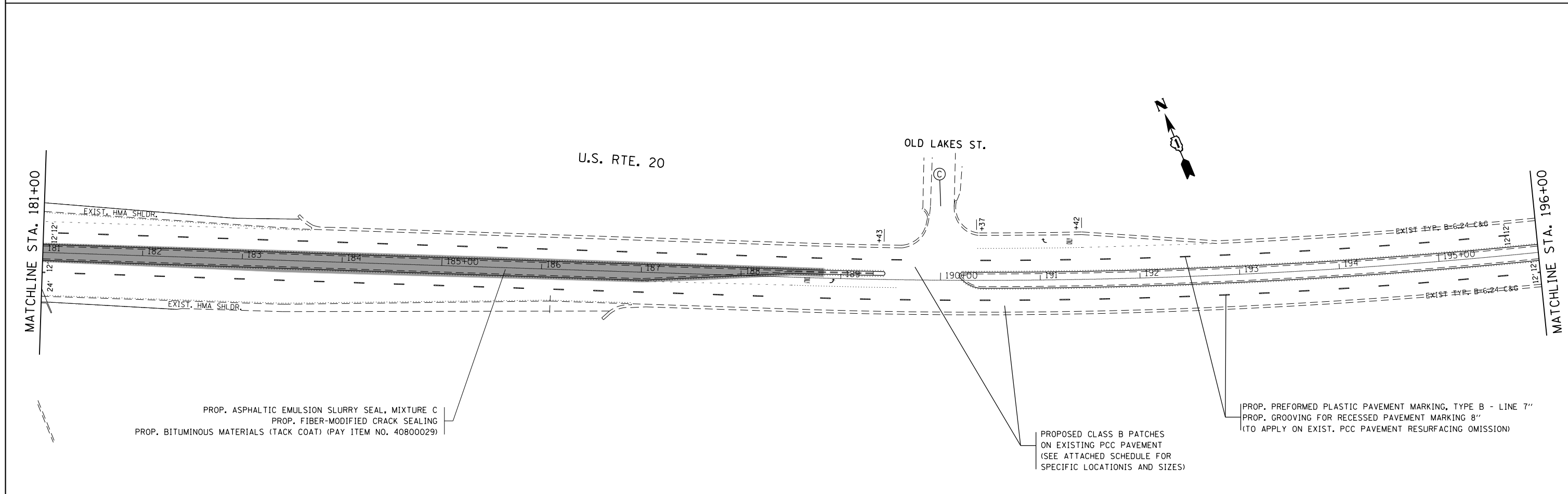
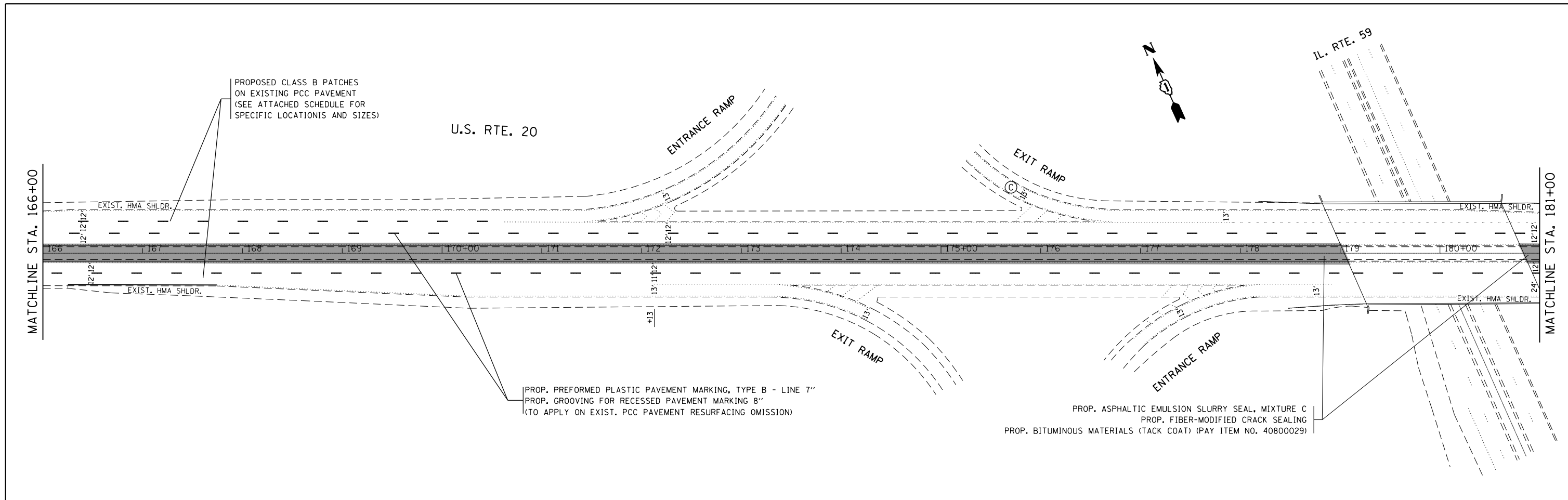
ROADWAY PROPOSED WORK LEGEND	
(A)	PROP. HMA SURFACE REMOVAL, 3" PROP. POLY. HMA SURFACE COURSE, SMA, N80, 9.5, 2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
(B)	PROP. HMA SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(C)	PROP. PCC SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(D)	PROP. LONGITUDINAL PARTIAL DEPTH REMOVAL, 3" PROP. LONGITUDINAL PARTIAL PATCHING PROP. POLY. HMA SURFACE COURSE, SMA, N80, 9.5, 2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1" (TO BE PLACED PER DISTRICT DETAIL BD-33)

PAVEMENT MARKING LEGEND			
(1)	PROP. 8" WHITE THERMO. PVT. MRK. LETTERS AND SYMBOLS (TYP.)	(9)	PROP. THERMO. PVT. MRK. -12" GORE MRK., CHEVRON WHITE (TYP.)
(2)	PROP. THERMO. PVT. MRK. -4" EDGELINE, SOLID WHITE (TYP.)	(10)	PROP. THERMO. PVT. MRK. -12" DIAGONAL, YELLOW (TYP.)
(3)	PROP. THERMO. PVT. MRK. -4" EDGELINE, SOLID YELLOW (TYP.)	(11)	PROP. THERMO. PVT. MRK. -24" STOP LINE, WHITE (TYP.)
(4)	PROP. THERMO. PVT. MRK. -4" 10' DASH - 30' SKIP, WHITE (TYP.)		
(5)	PROP. THERMO. PVT. MRK. -4" CENTERLINE, DOUBLE YELLOW (TYP.)		
(6)	PROP. THERMO. PVT. MRK. -6" SOLID LINE, WHITE (TYP.)		
(7)	PROP. THERMO. PVT. MRK. -6" DOTTED LINE, WHITE (2' DASH, 6' SKIP) (TYP.)		
(8)	PROP. THERMO. PVT. MRK. -8" GORE MRK., SOLID WHITE (TYP.)		

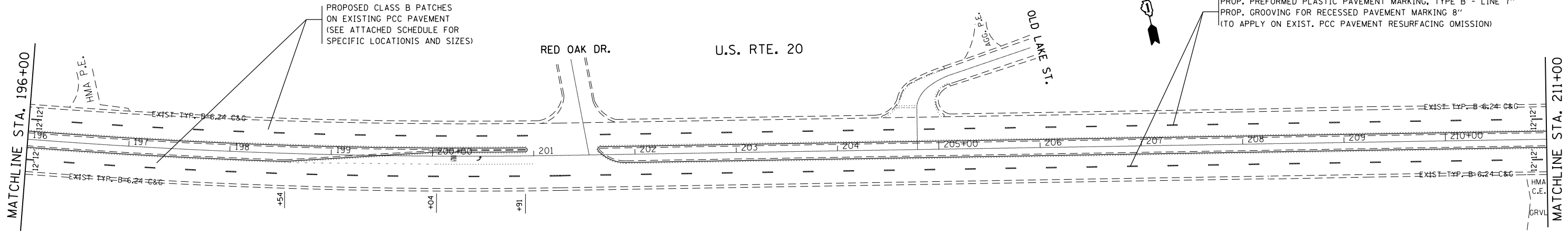
NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-11 AND TC-13



FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN U.S. RTE. 20 (SHALES PARKWAY /BLUFF CITY BLVD. TO NORTH AVE.)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -	REVISED -			CONTRACT NO. 62D12				
Default	PLOT DATE = 5/1/2018	DATE -	REVISED -			ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN U.S. RTE. 20 (SHALES PARKWAY /BLUFF CITY BLVD. TO NORTH AVE.)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 62D12					
	PLOT DATE = 5/1/2018	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



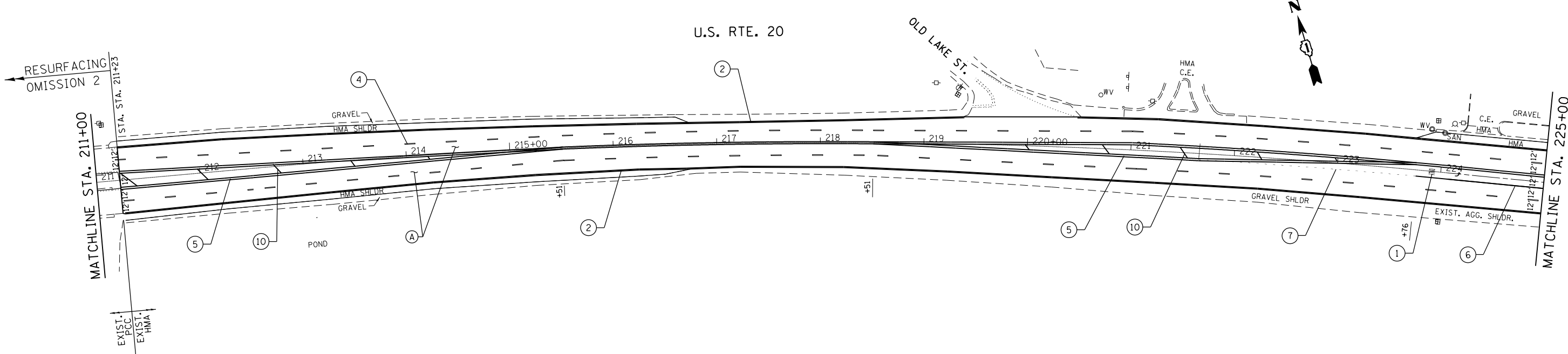
ROADWAY PROPOSED WORK LEGEND

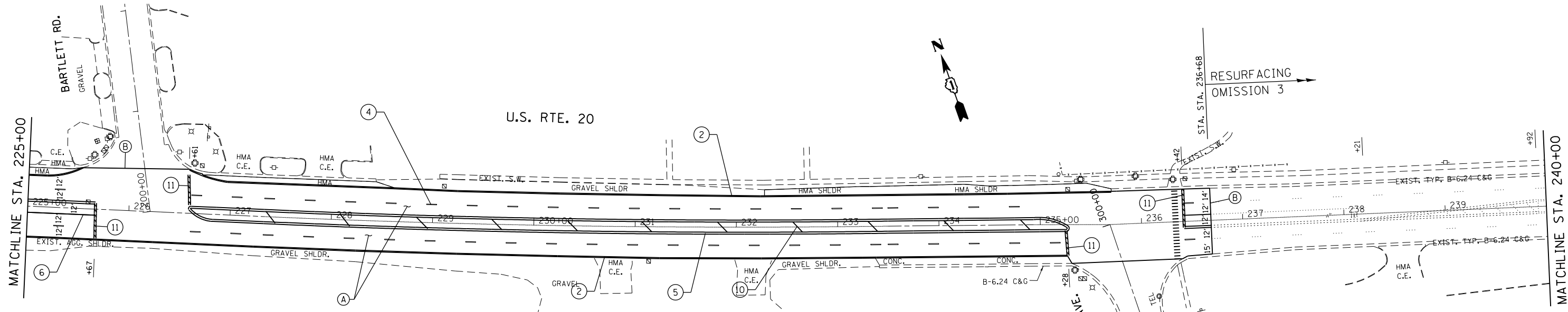
(A)	PROP. HMA SURFACE REMOVAL, 3"
	PROP. POLY. HMA SURFACE COURSE, SMA, N80, 9.5, 2"
	PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
(B)	PROP. HMA SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(C)	PROP. PCC SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(D)	PROP. LONGITUDINAL PARTIAL DEPTH REMOVAL, 3"
	PROP. LONGITUDINAL PARTIAL PATCHING
	PROP. POLY. HMA SURFACE COURSE, SMA, N80, 9.5, 2"
	PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
	(TO BE PLACED PER DISTRICT DETAIL BD-33)

PAVEMENT MARKING LEGEND

(1)	PROP. 8" WHITE THERMO. PVT. MRK. LETTERS AND SYMBOLS (TYP.)	(9)	PROP. THERMO. PVT. MRK.-12" GORE MRK., CHEVRON WHITE (TYP.)
(2)	PROP. THERMO. PVT. MRK.-4" EDGELINE, SOLID WHITE (TYP.)	(10)	PROP. THERMO. PVT. MRK.-12" DIAGONAL, YELLOW (TYP.)
(3)	PROP. THERMO. PVT. MRK.-4" EDGELINE, SOLID YELLOW (TYP.)	(11)	PROP. THERMO. PVT. MRK.-24" STOP LINE, WHITE (TYP.)
(4)	PROP. THERMO. PVT. MRK.-4" 10' DASH - 30' SKIP, WHITE (TYP.)		
(5)	PROP. THERMO. PVT. MRK.-4" CENTERLINE, DOUBLE YELLOW (TYP.)		
(6)	PROP. THERMO. PVT. MRK.-6" SOLID LINE, WHITE (TYP.)		
(7)	PROP. THERMO. PVT. MRK.-6" DOTTED LINE, WHITE (2' DASH, 6' SKIP) (TYP.)		
(8)	PROP. THERMO. PVT. MRK.-8" GORE MRK., SOLID WHITE (TYP.)		

NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-11 AND TC-13





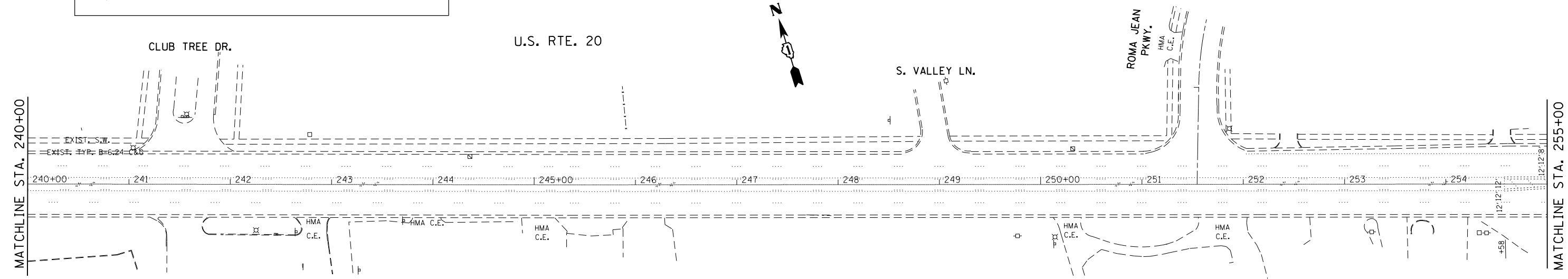
ROADWAY PROPOSED WORK LEGEND

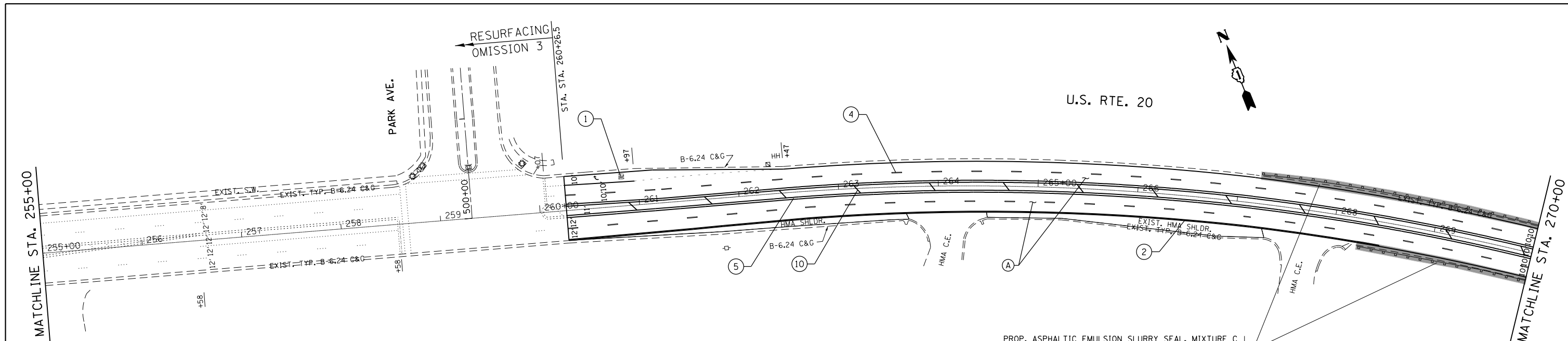
(A)	PROP. HMA SURFACE REMOVAL, 3"
(B)	PROP. POLY. HMA SURFACE COURSE, SMA, N80, 9.5, 2"
(C)	PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
(D)	PROP. HMA SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(E)	PROP. PCC SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(F)	PROP. LONGITUDINAL PARTIAL DEPTH REMOVAL, 3"
(G)	PROP. LONGITUDINAL PARTIAL PATCHING
(H)	PROP. POLY. HMA SURFACE COURSE, SMA, N80, 9.5, 2"
(I)	PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1" (TO BE PLACED PER DISTRICT DETAIL BD-33)

PAVEMENT MARKING LEGEND

(1)	PROP. 8" WHITE THERMO. PVT. MRK. LETTERS AND SYMBOLS (TYP.)	(9)	PROP. THERMO. PVT. MRK.-12" GORE MRK., CHEVRON WHITE (TYP.)
(2)	PROP. THERMO. PVT. MRK.-4" CENTERLINE, DOUBLE YELLOW (TYP.)	(10)	PROP. THERMO. PVT. MRK.-12" DIAGONAL, YELLOW (TYP.)
(3)	PROP. THERMO. PVT. MRK.-4" EDGELINE, SOLID WHITE (TYP.)	(11)	PROP. THERMO. PVT. MRK.-24" STOP LINE, WHITE (TYP.)
(4)	PROP. THERMO. PVT. MRK.-4" EDGELINE, SOLID YELLOW (TYP.)		
(5)	PROP. THERMO. PVT. MRK.-6" SOLID LINE, WHITE (TYP.)		
(6)	PROP. THERMO. PVT. MRK.-6" DOTTED LINE, WHITE (2' DASH, 6' SKIP) (TYP.)		
(7)	PROP. THERMO. PVT. MRK.-8" GORE MRK., SOLID WHITE (TYP.)		
(8)	PROP. THERMO. PVT. MRK.-4" EDGELINE, SOLID YELLOW (TYP.)		

NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-11 AND TC-13



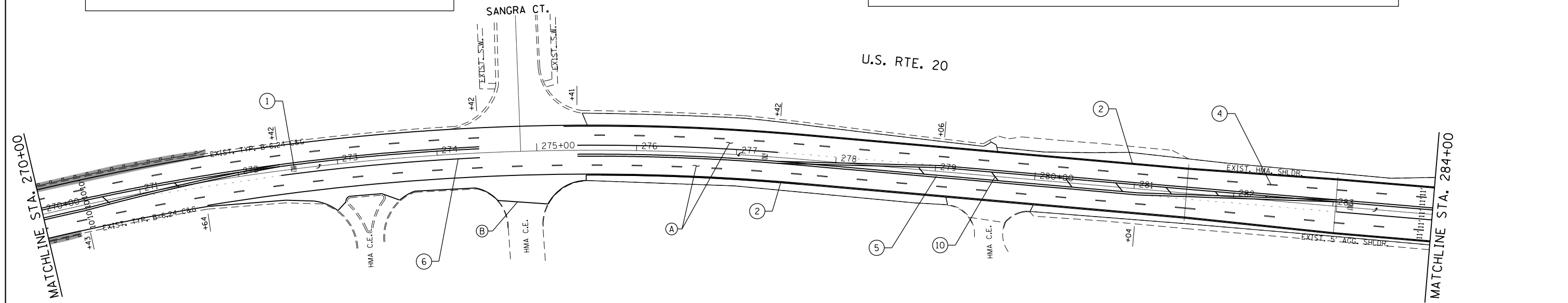


PROP. ASPHALTIC EMULSION SLURRY SEAL, MIXTURE C
 PROP. FIBER-MODIFIED CRACK SEALING
 PROP. BITUMINOUS MATERIALS (TACK COAT) (PAY ITEM NO. 40800029)

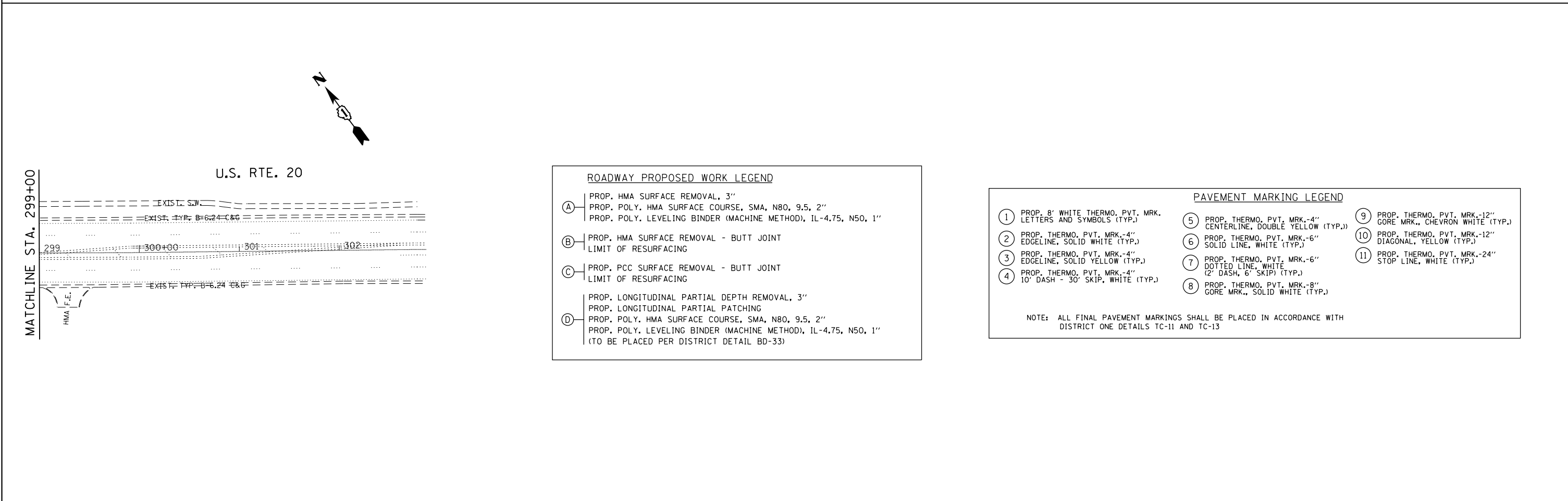
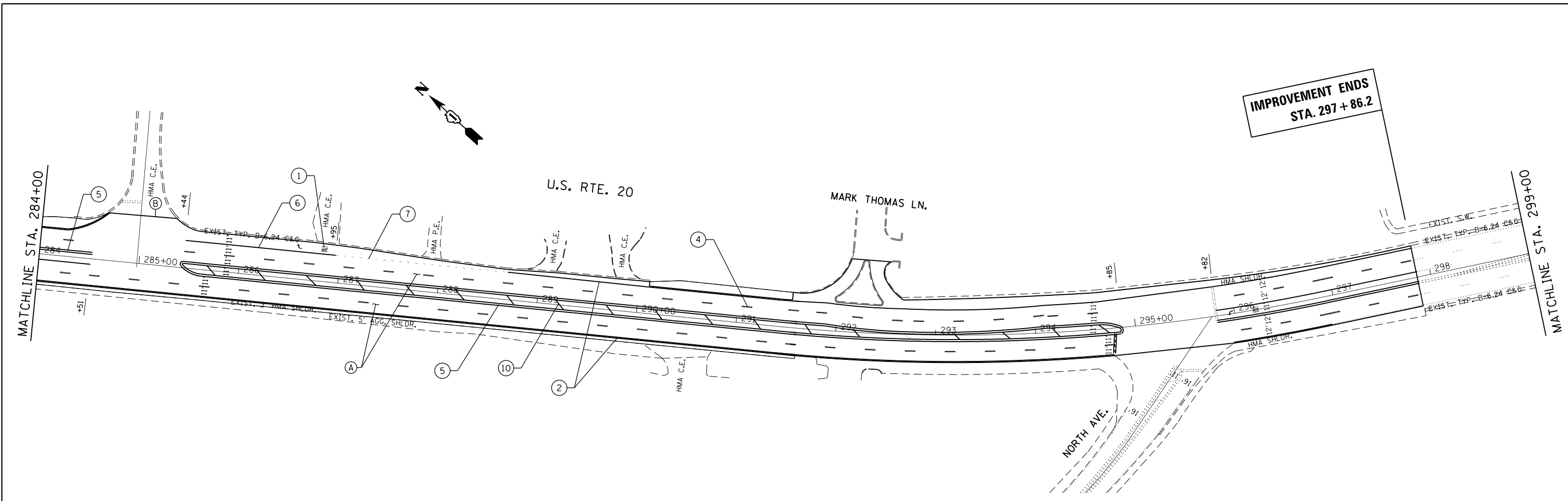
ROADWAY PROPOSED WORK LEGEND	
(A)	PROP. HMA SURFACE REMOVAL, 3" PROP. POLY. HMA SURFACE COURSE, SMA, N80, 9.5, 2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
(B)	PROP. HMA SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(C)	PROP. PCC SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(D)	PROP. LONGITUDINAL PARTIAL DEPTH REMOVAL, 3" PROP. LONGITUDINAL PARTIAL PATCHING PROP. POLY. HMA SURFACE COURSE, SMA, N80, 9.5, 2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1" (TO BE PLACED PER DISTRICT DETAIL BD-33)

PAVEMENT MARKING LEGEND			
(1)	PROP. 8' WHITE THERMO. PVT. MRK. LETTERS AND SYMBOLS (TYP.)	(9)	PROP. THERMO. PVT. MRK.-12" GORE MRK., CHEVRON WHITE (TYP.)
(2)	PROP. THERMO. PVT. MRK.-4" EDGELINE, SOLID WHITE (TYP.)	(10)	PROP. THERMO. PVT. MRK.-12" DIAGONAL, YELLOW (TYP.)
(3)	PROP. THERMO. PVT. MRK.-4" EDGELINE, SOLID YELLOW (TYP.)	(11)	PROP. THERMO. PVT. MRK.-24" STOP LINE, WHITE (TYP.)
(4)	PROP. THERMO. PVT. MRK.-4" 10' DASH - 30' SKIP, WHITE (TYP.)	(5)	PROP. THERMO. PVT. MRK.-4" CENTERLINE, DOUBLE YELLOW (TYP.)
		(6)	PROP. THERMO. PVT. MRK.-6" SOLID LINE, WHITE (TYP.)
		(7)	PROP. THERMO. PVT. MRK.-6" DOTTED LINE, WHITE (2' DASH, 6' SKIP) (TYP.)
		(8)	PROP. THERMO. PVT. MRK.-8" GORE MRK., SOLID WHITE (TYP.)

NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-11 AND TC-13



FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN U.S. RTE. 20 (SHALES PARKWAY /BLUFF CITY BLVD. TO NORTH AVE.)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI43016\Design\DI43016-sh1-plan.dgn		REVISOR -	REVISOR -			345	7Y-RS-B	COOK	40	18
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISOR -			CONTRACT NO. 62D12				
Default		DATE -	REVISOR -			ILLINOIS FED. AID PROJECT				



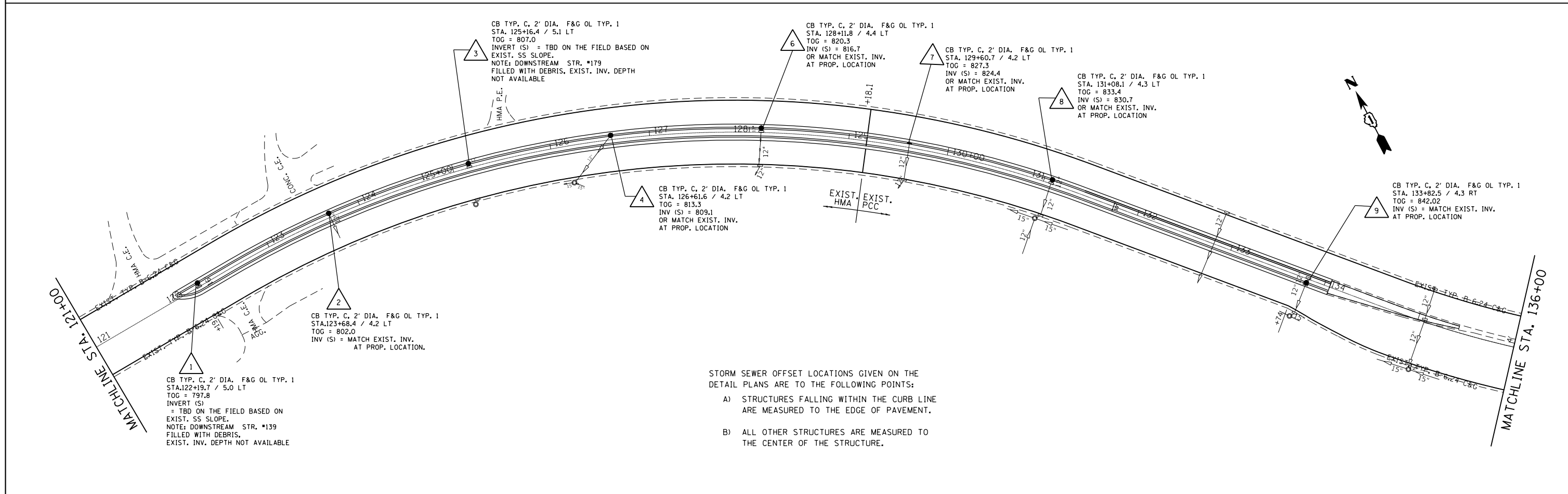
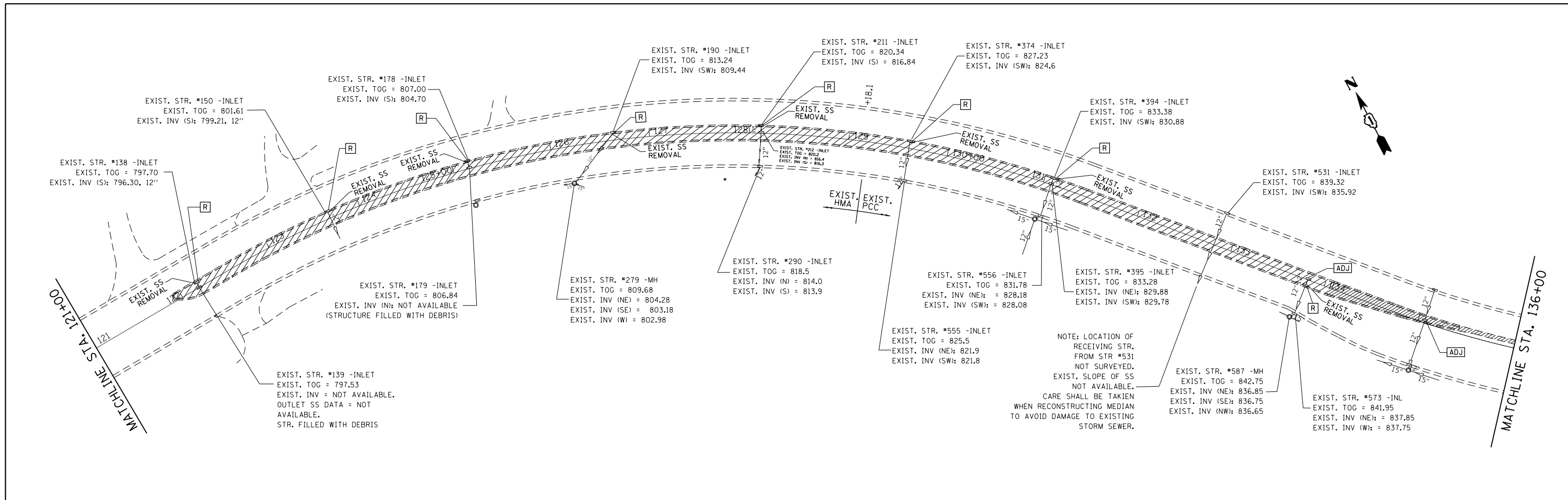
ROADWAY PROPOSED WORK LEGEND

<p>(A) PROP. HMA SURFACE REMOVAL, 3"</p> <p>(B) PROP. HMA SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING</p> <p>(C) PROP. PCC SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING</p> <p>(D) PROP. LONGITUDINAL PARTIAL DEPTH REMOVAL, 3"</p> <p>PROP. LONGITUDINAL PARTIAL PATCHING</p> <p>PROP. POLY. HMA SURFACE COURSE, SMA, N80, 9.5, 2"</p> <p>PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"</p> <p>(TO BE PLACED PER DISTRICT DETAIL BD-33)</p>	<p>PROP. POLY. HMA SURFACE COURSE, SMA, N80, 9.5, 2"</p> <p>PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"</p>
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PAVEMENT MARKING LEGEND

<p>(1) PROP. 8' WHITE THERMO. PVT. MRK. LETTERS AND SYMBOLS (TYP.)</p> <p>(2) PROP. THERMO. PVT. MRK.-4" EDGELINE, SOLID WHITE (TYP.)</p> <p>(3) PROP. THERMO. PVT. MRK.-4" EDGELINE, SOLID YELLOW (TYP.)</p> <p>(4) PROP. THERMO. PVT. MRK.-4" 10' DASH - 30' SKIP, WHITE (TYP.)</p>	<p>(5) PROP. THERMO. PVT. MRK.-4" CENTERLINE, DOUBLE YELLOW (TYP.)</p> <p>(6) PROP. THERMO. PVT. MRK.-6" SOLID LINE, WHITE (TYP.)</p> <p>(7) PROP. THERMO. PVT. MRK.-6" DOTTED LINE, WHITE (2' DASH, 6' SKIP) (TYP.)</p> <p>(8) PROP. THERMO. PVT. MRK.-8" GORE MRK., SOLID WHITE (TYP.)</p>	<p>(9) PROP. THERMO. PVT. MRK.-12" GORE MRK., CHEVRON WHITE (TYP.)</p> <p>(10) PROP. THERMO. PVT. MRK.-12" DIAGONAL, YELLOW (TYP.)</p> <p>(11) PROP. THERMO. PVT. MRK.-24" STOP LINE, WHITE (TYP.)</p>
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NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-11 AND TC-13



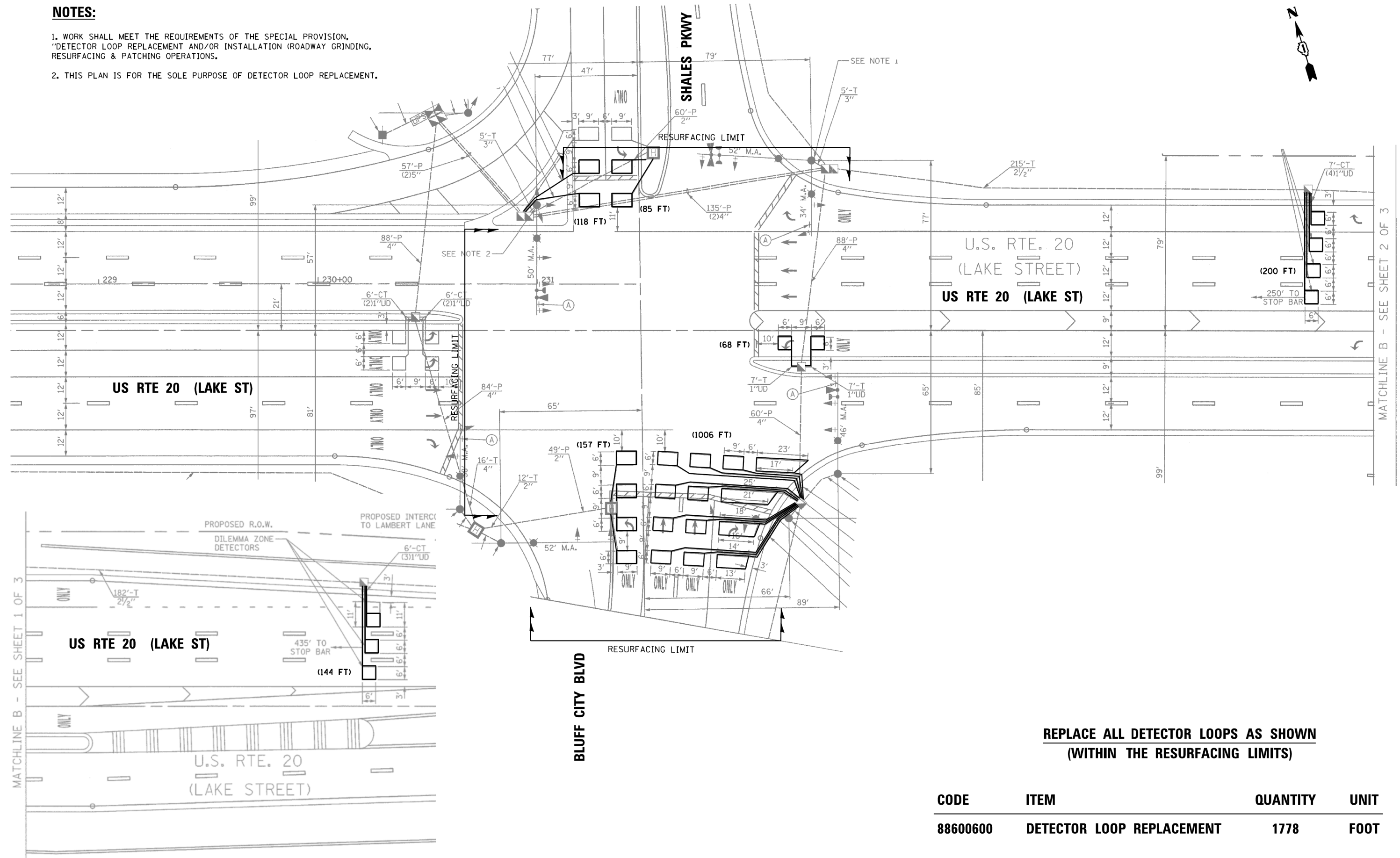
STORM SEWER OFFSET LOCATIONS GIVEN ON THE DETAIL PLANS ARE TO THE FOLLOWING POINTS:

- A) STRUCTURES FALLING WITHIN THE CURB LINE ARE MEASURED TO THE EDGE OF PAVEMENT.
- B) ALL OTHER STRUCTURES ARE MEASURED TO THE CENTER OF THE STRUCTURE.

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. RTE. 20: MEDIAN REMOVAL AND RECONSTRUCTION DETAIL EXISTING AND PROPOSED DRAINAGE PLAN			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBID\INTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI43016\Design\DI43016-shr-plan.dgn		CHECKED -	REVISED -		SCALE: 1"=50'	SHEET	OF	SHEETS	345	7Y-RS-B	COOK	40	20
Default		DATE -	REVISED -		STA. 106+00	TO STA. 136+00		CONTRACT NO. 62D12					
					ILLINOIS FED. AID PROJECT								

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	1778	FOOT

FILE NAME =	USER NAME = snowba	DESIGNED - BAS	REVISED -
TS1320 - US RTE 20 AT SHALES PKWY_09112017.dgn		DRAWN - BAS	REVISED -
Default	PLOT SCALE = 48.0000' / in.	CHECKED - LP	REVISED -
	PLOT DATE = 9/11/2017	DATE - 09/11/2017	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
US RTE 20 (LAKE ST) AT SHALES PKWY /BLUFF CITY BLVD**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

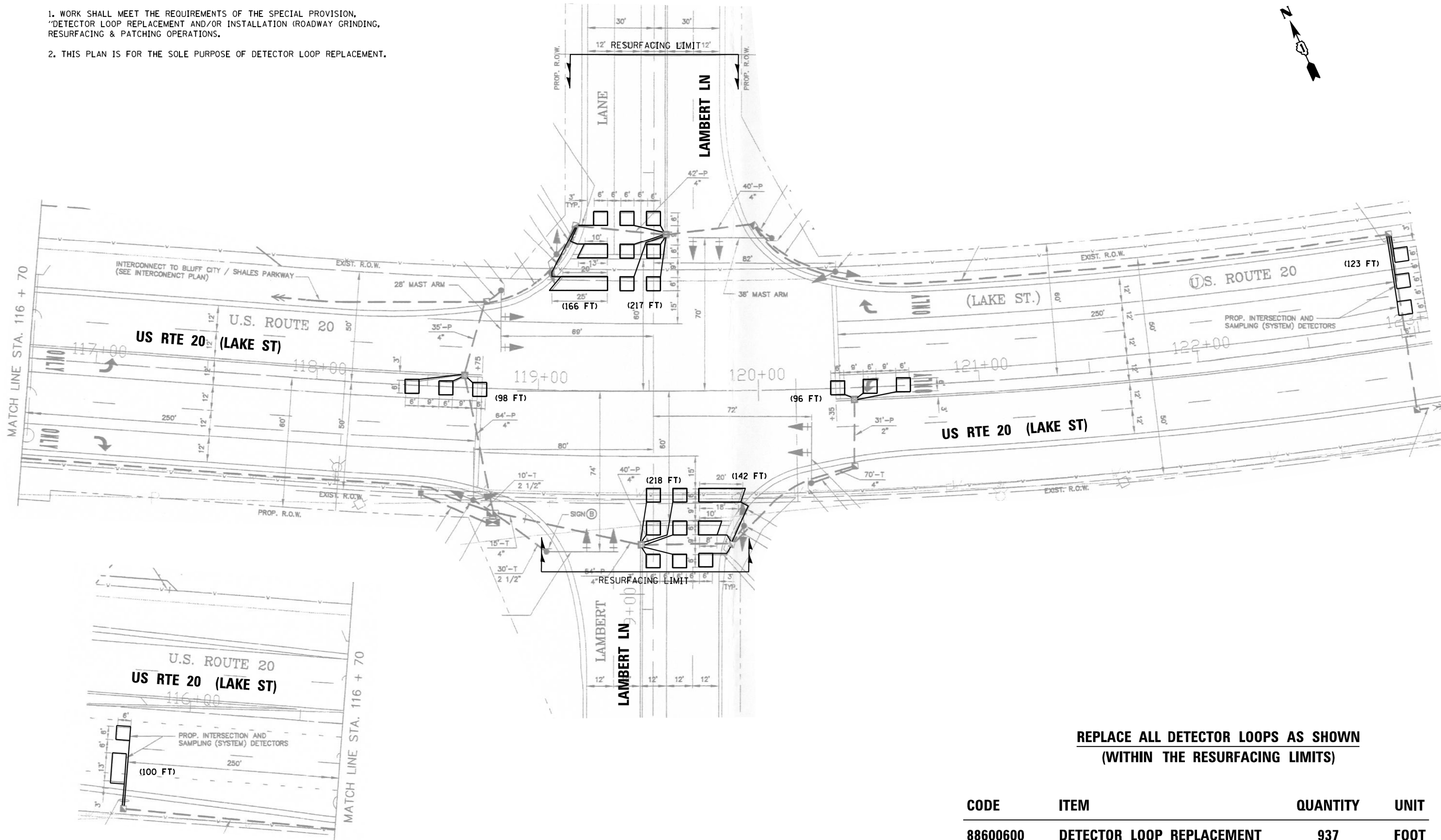
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-RS-8	COOK	40	21
CONTRACT NO. 62D12				
ILLINOIS FED. AID PROJECT				

MATCHLINE B - SEE SHEET 2 OF 3

MATCHLINE B - SEE SHEET 1 OF 3

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	937	FOOT

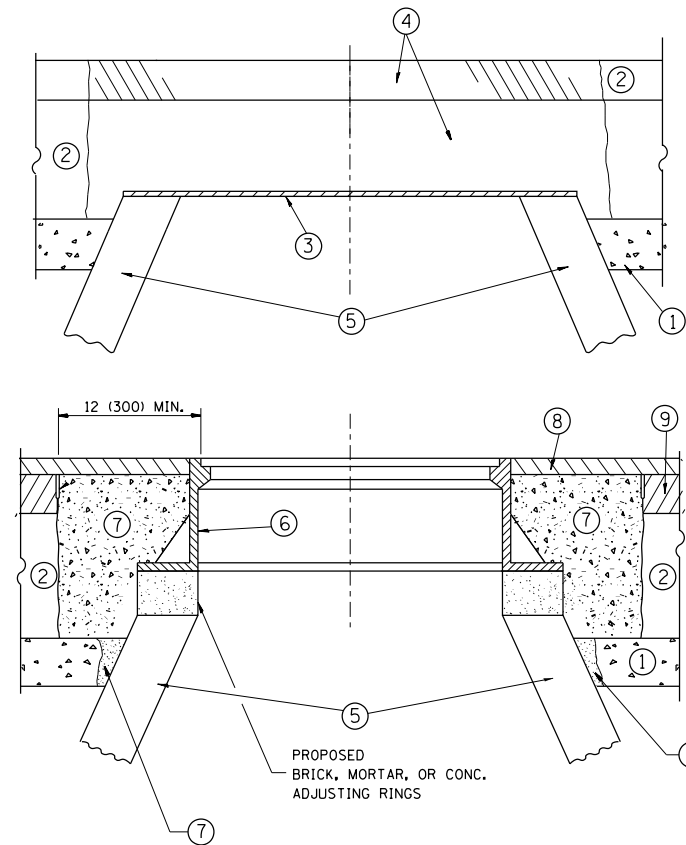
FILE NAME = TS503 - US RTE 20 AT LAMBERT LN.dgn	USER NAME = snowbe	DESIGNED - BAS	REVISED -
		DRAWN - BAS	REVISED -
		CHECKED - LP	REVISED -
		DATE - 09/11/2017	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
US RTE 20 (LAKE ST) AT LAMBERT LN**

F.A.P. RTE. 345	SECTION 7Y-RS-8	COUNTY COOK	TOTAL SHEETS 40	SHEET NO. 22
CONTRACT NO. 62D12				
ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET OF SHEETS STA. TO STA.



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

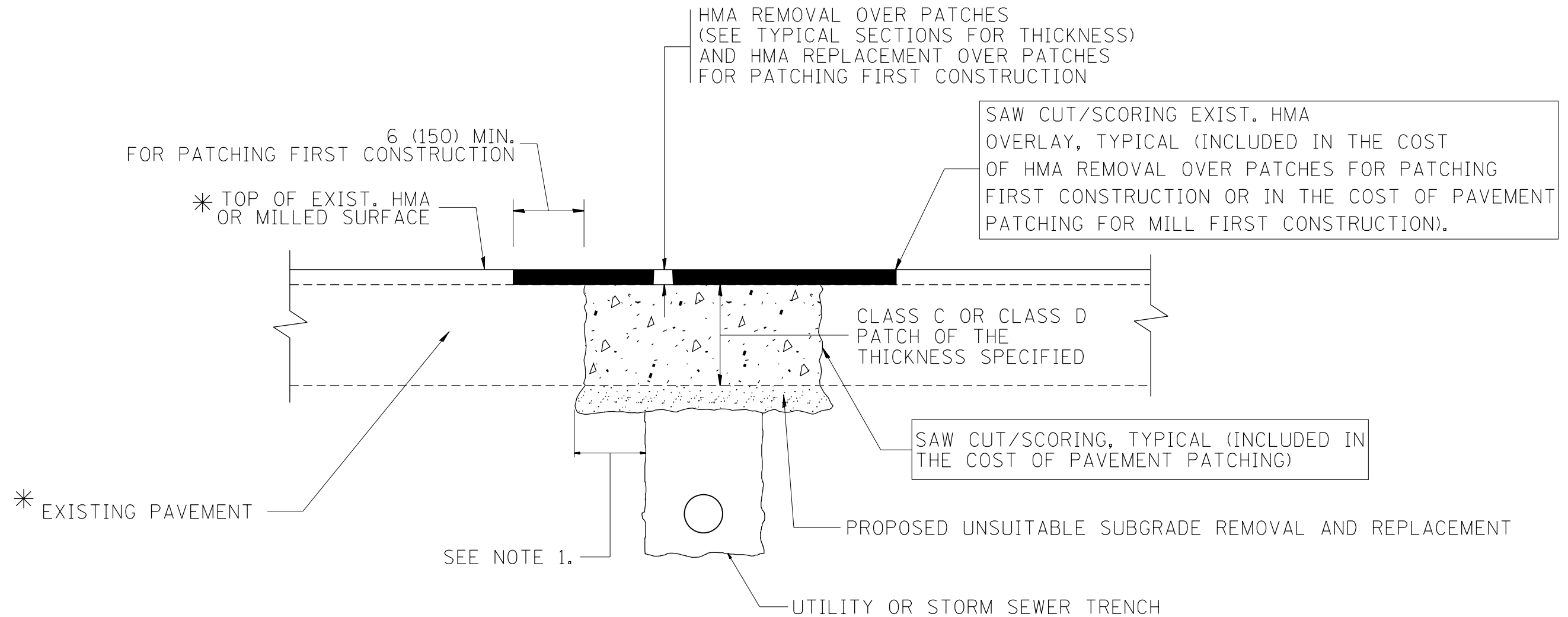
FILE NAME =	USER NAME = paraynoal	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
p:\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\1430\Drawings\Design\Diststd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 3/23/2018	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-RS-B	COOK	40	27
BD600-03 (BD-8)		CONTRACT NO. 62D12		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = paraynoal	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI430\Drawings\Design\Diststd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					345	7Y-RS-B	COOK	40	28
PLOT DATE = 3/23/2018	DATE - 10-25-94	REVISED - R. BORO 09-04-07	REVISED - K. ENG 10-27-08		BD400-04 (BD-22)			CONTRACT NO. 62D12				
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

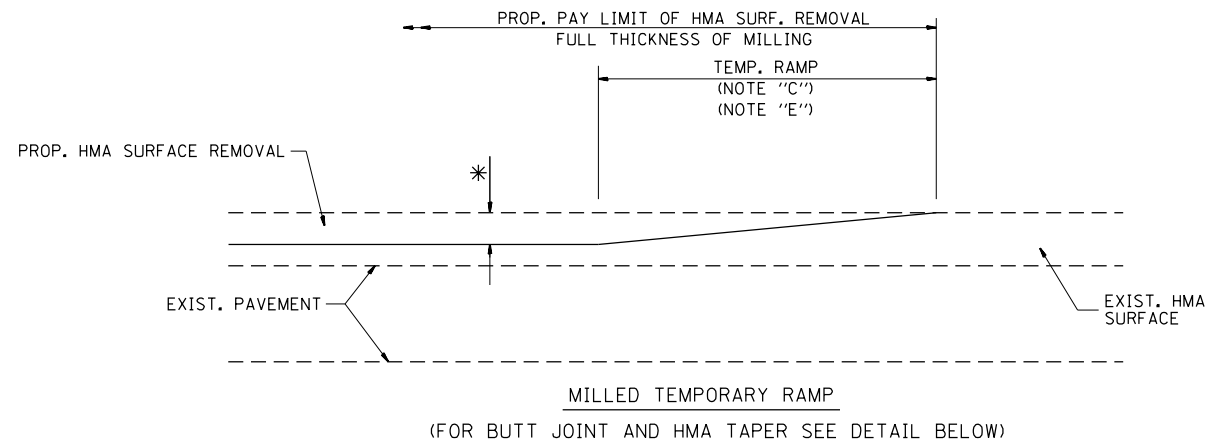
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

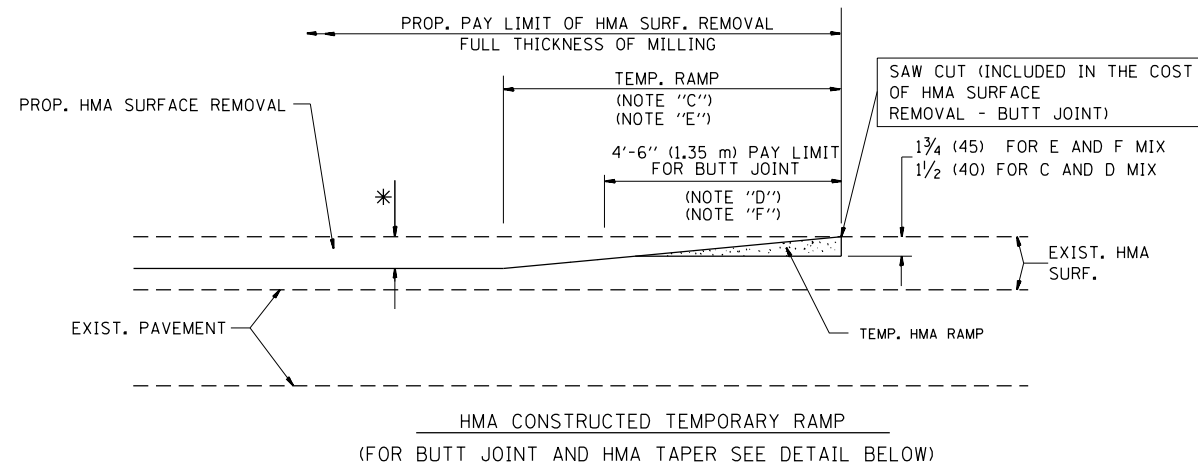
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = paraynoal	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI430\Drawings\Design\Diststd.dgn		REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01					345	7Y-RS-8	COOK	40	29
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 12-15-09						BD600-06 (BD-24)				CONTRACT NO. 62D12
PLOT DATE = 3/23/2018	DATE - 03-11-94							SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

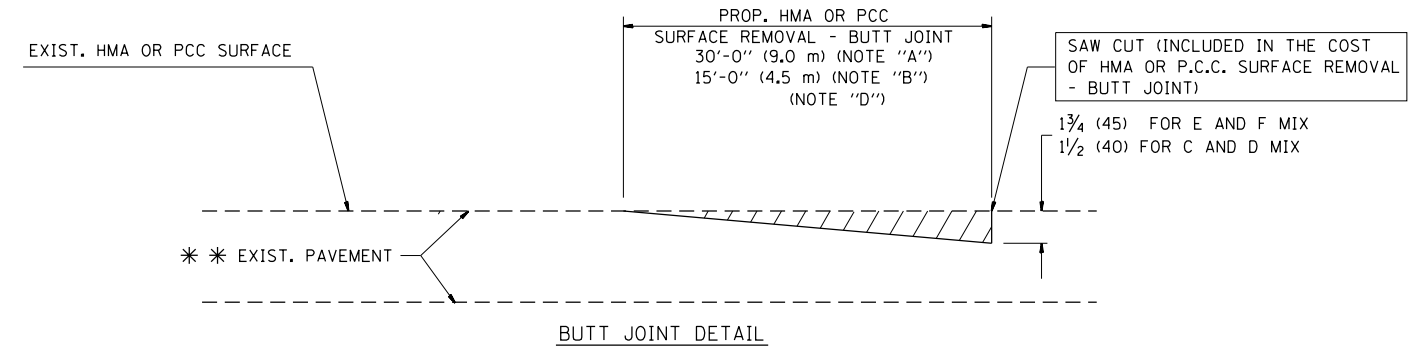


OPTION 1

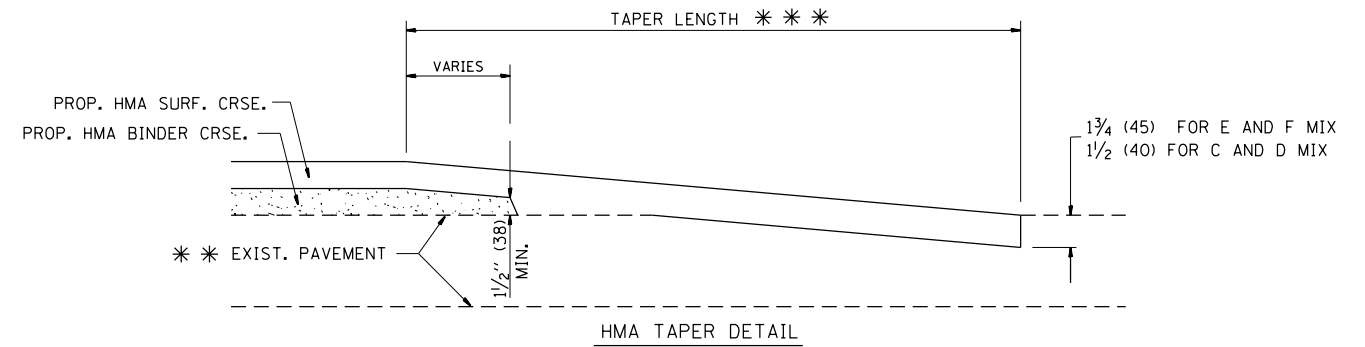


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

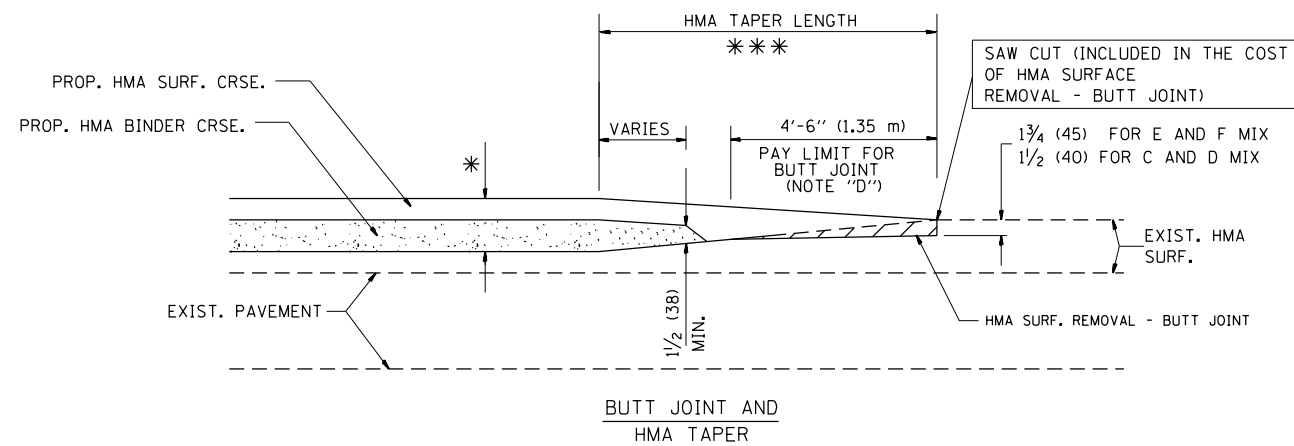
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



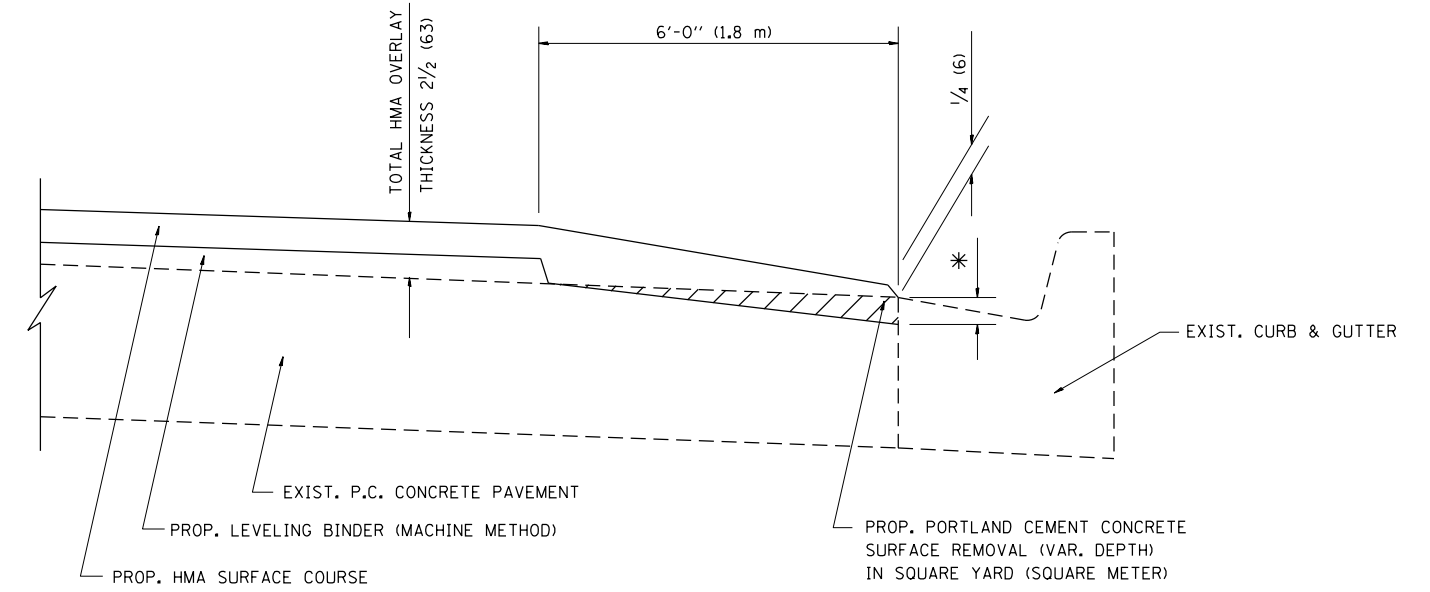
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME =	USER NAME = paraynoal	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01430\Drawings\Design\Diststd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 3/23/2018	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-RS-8	COOK	40	30
BD400-05 BD32		CONTRACT NO. 62D12		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



HMA TAPER AT
EDGE OF P.C.C PAVEMENT

HMA SURFACE	THICKNESS	LEVELING BINDER THICKNESS	* MILLING AT GUTTER FLAG
MIX			
C OR D	1 1/2 (38)	1 (25)	1/4 (33)
E	1 3/4 (44)	3/4 (19)	1/2 (38)

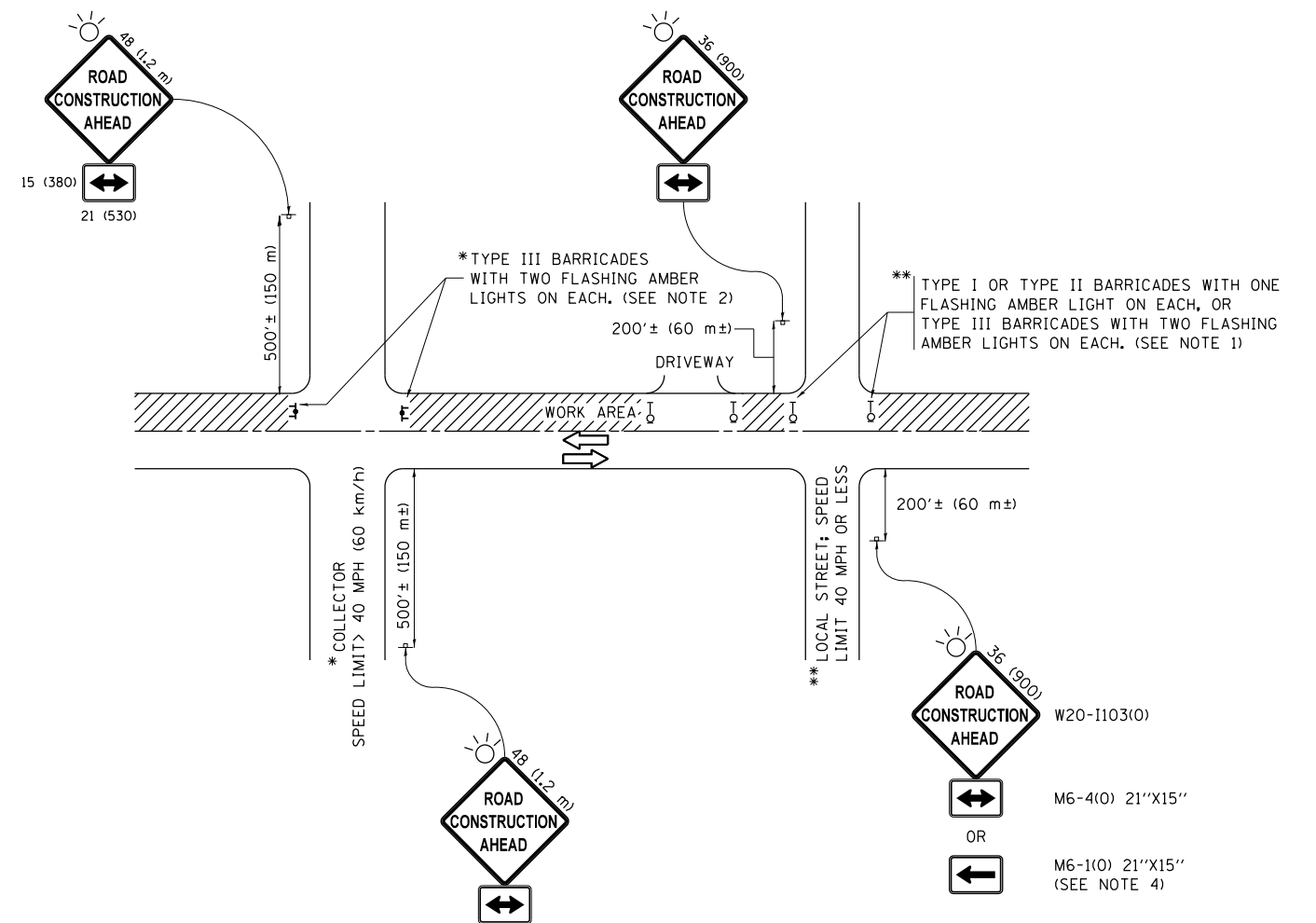
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = paraynoal	DESIGNED - R. SHAH	REVISED - A. ABBAS 05-05-9
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Default	PLOT SCALE = 100.0000' / in.	CHECKED - A. ABBAS	REVISED - R. BORO 01-01-07
	PLOT DATE = 3/23/2018	DATE - 09-10-94	REVISED - JP CHANG 07-08-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HMA TAPER AT EDGE OF P.C.C PAVEMENT			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-RS-8	COOK	40	31
BD400-06 (BD33)		CONTRACT NO.	62D12	
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

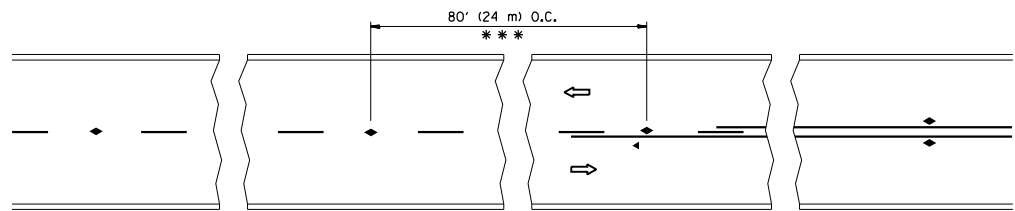
FILE NAME =	USER NAME = paraynoal	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
p:\11\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\01430\Drawings\Design\Diststd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 3/23/2018	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

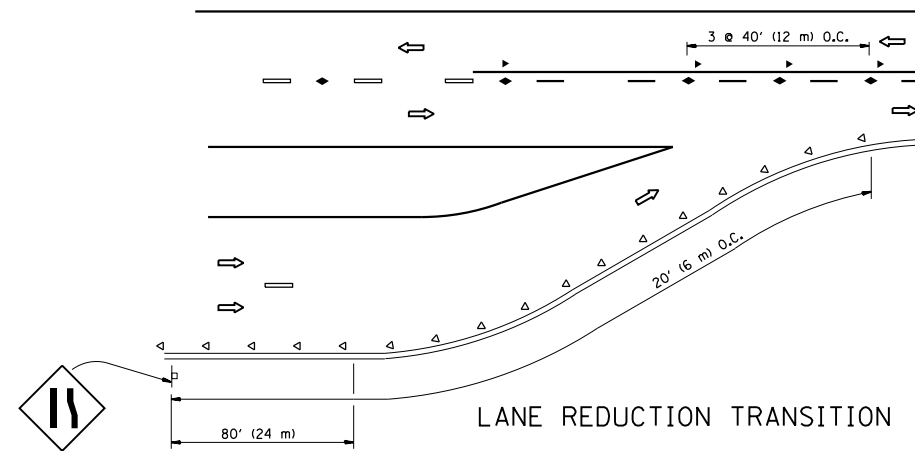
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 62D12	
ILLINOIS FED. AID PROJECT				

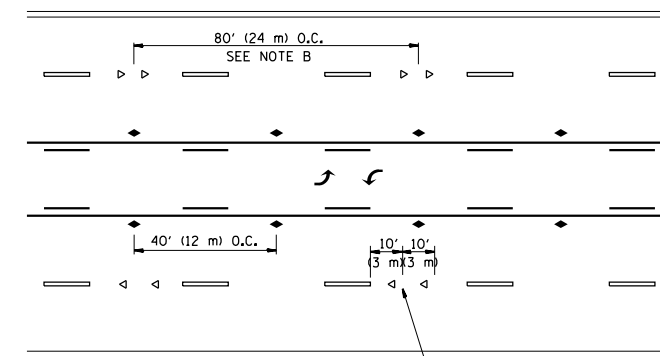


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

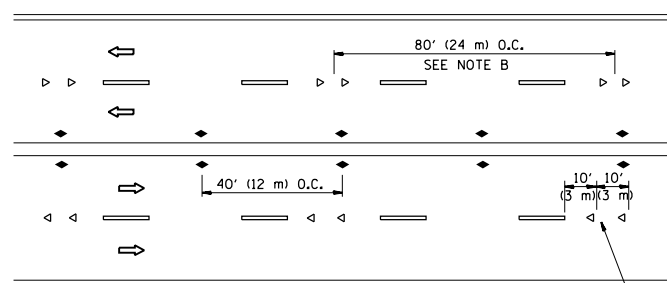
TWO-LANE/TWO-WAY



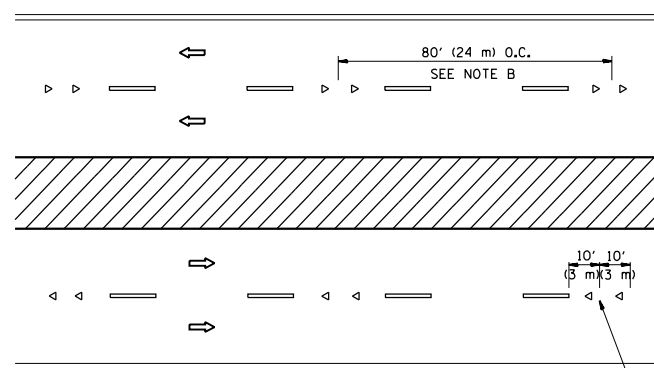
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

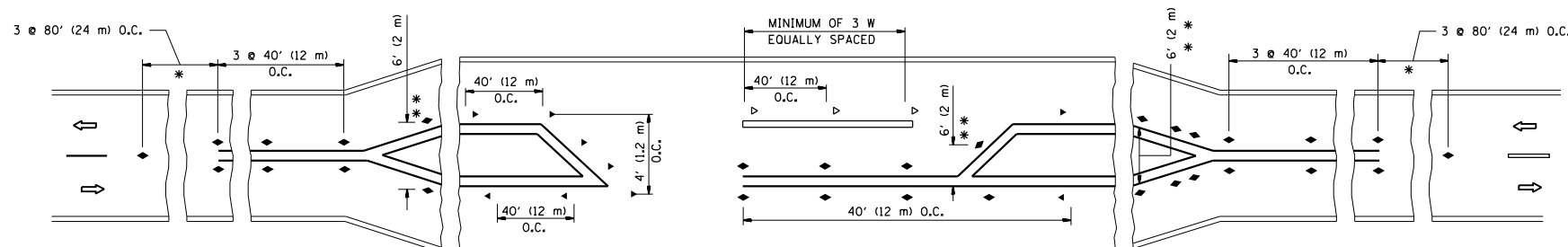
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

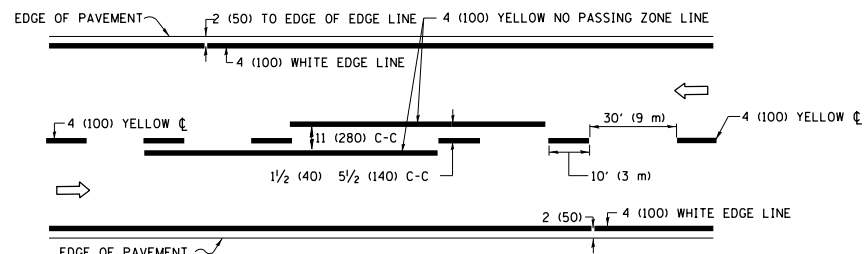
All dimensions are in inches (millimeters) unless otherwise shown.

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		DATE -	REVISED - T. RAMMACHER 01-06-00
			REVISED - C. JUCIUS 09-09-09

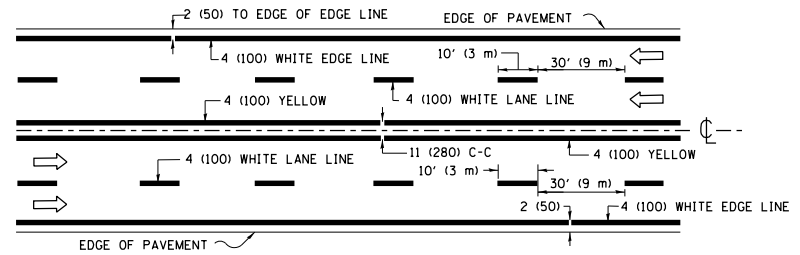
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
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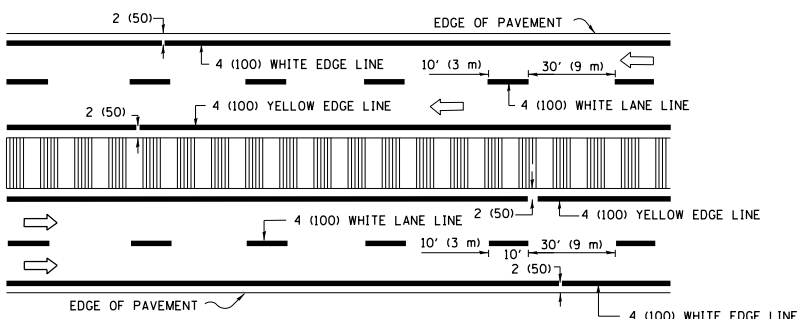
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-RS-8	COOK	40	33
TC-11			CONTRACT NO. 62D12	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

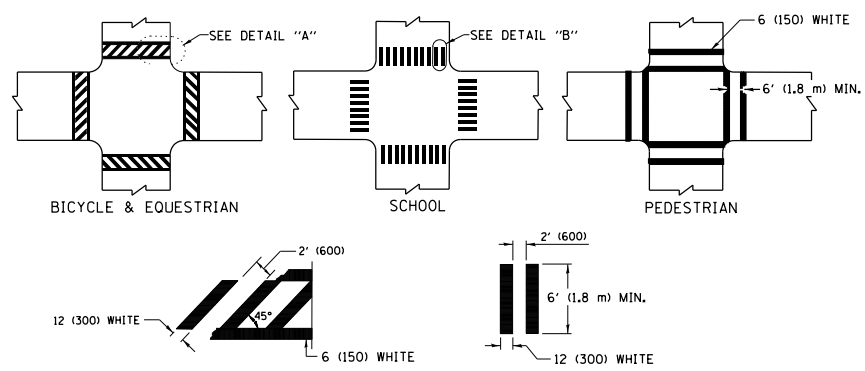


MULTI-LANE UNDIVIDED



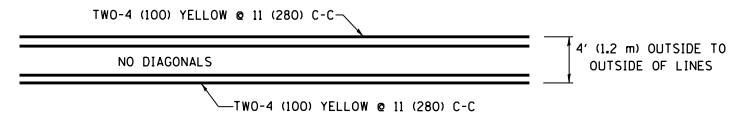
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

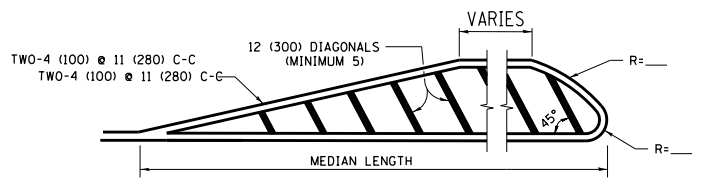


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

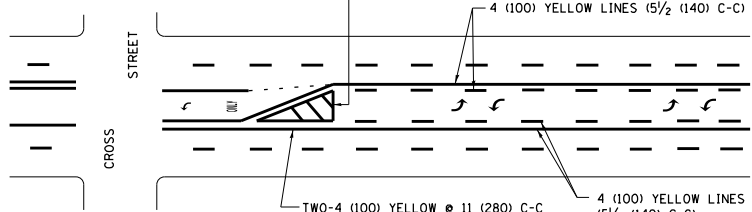


4' (1.2 m) WIDE MEDIANS ONLY



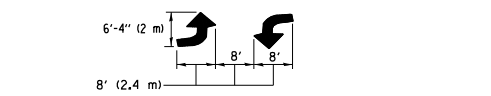
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

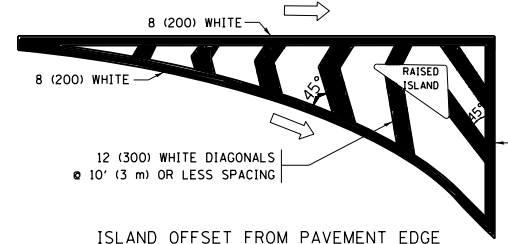
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



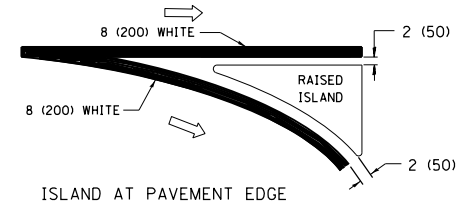
TYPICAL LEFT (OR RIGHT) TURN LANE

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

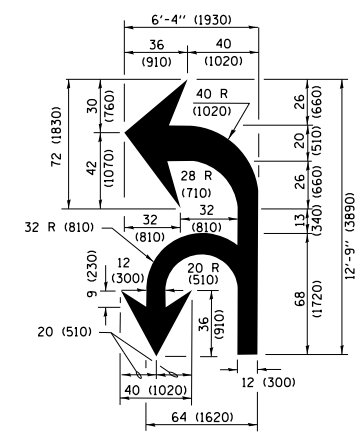
TYPICAL TURN LANE MARKING



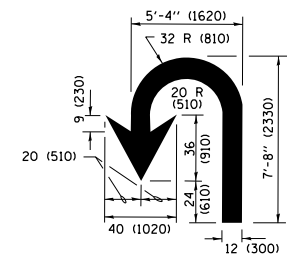
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = paraynoal	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
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Default	PLOT SCALE = 100.0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 3/23/2018		REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-RS-B	COOK	40	34
TC-13		CONTRACT NO. 62D12		
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

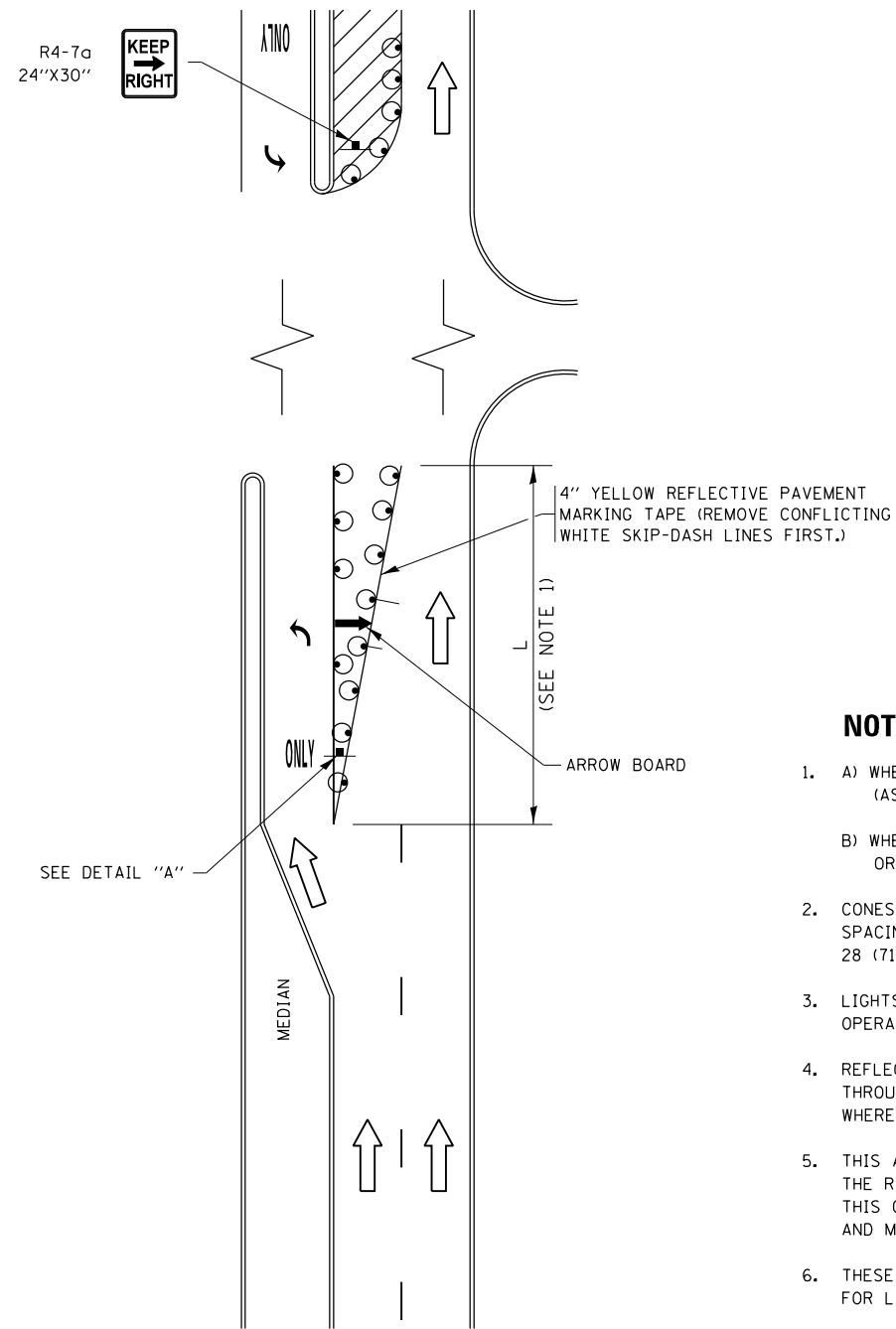


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

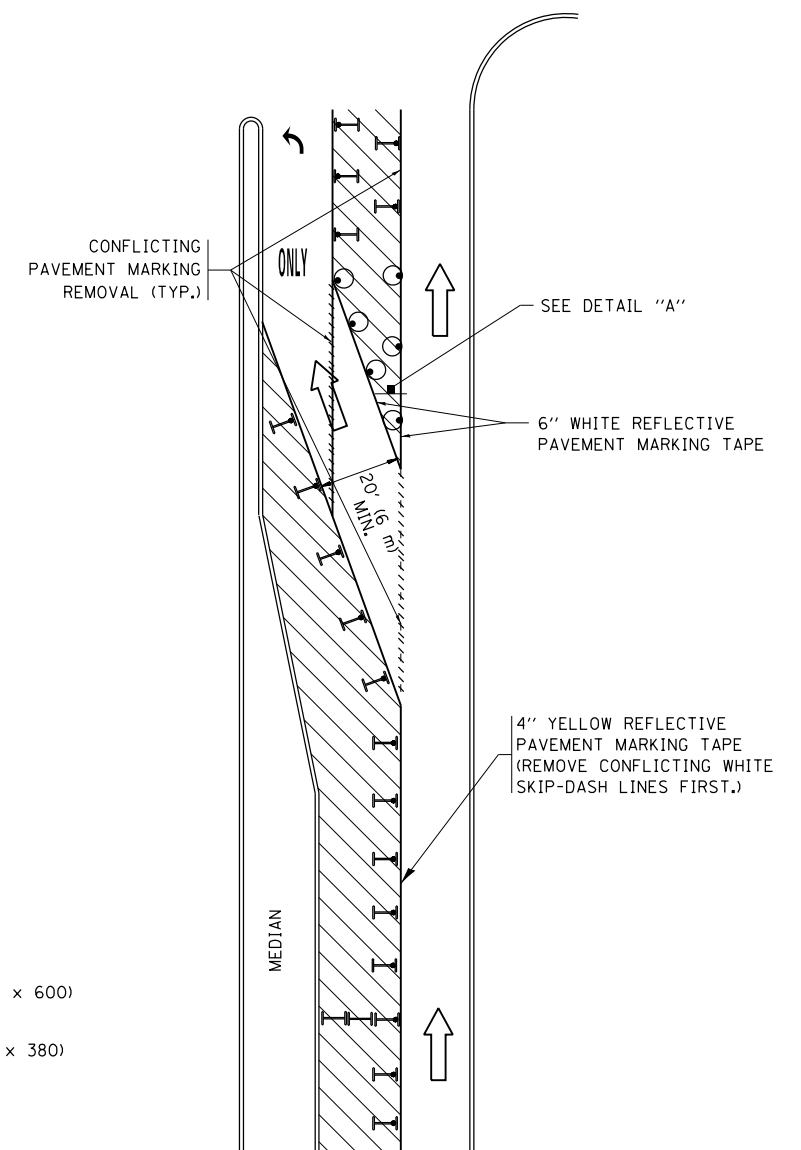
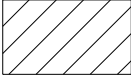
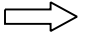







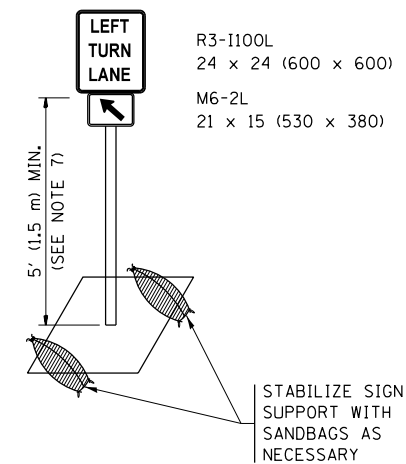
FIGURE 2

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  ARROW BOARD
-  TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  SIGN ASSEMBLY
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PRE REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

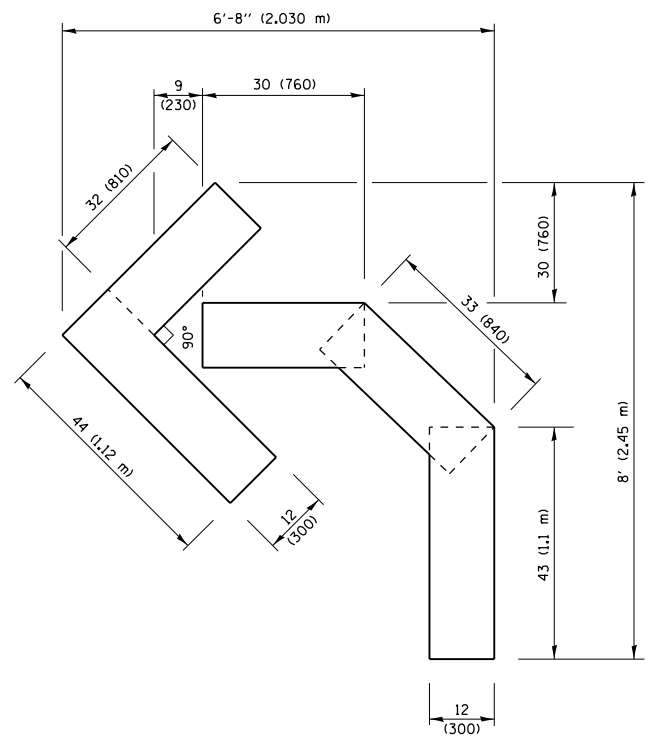


DETAIL A

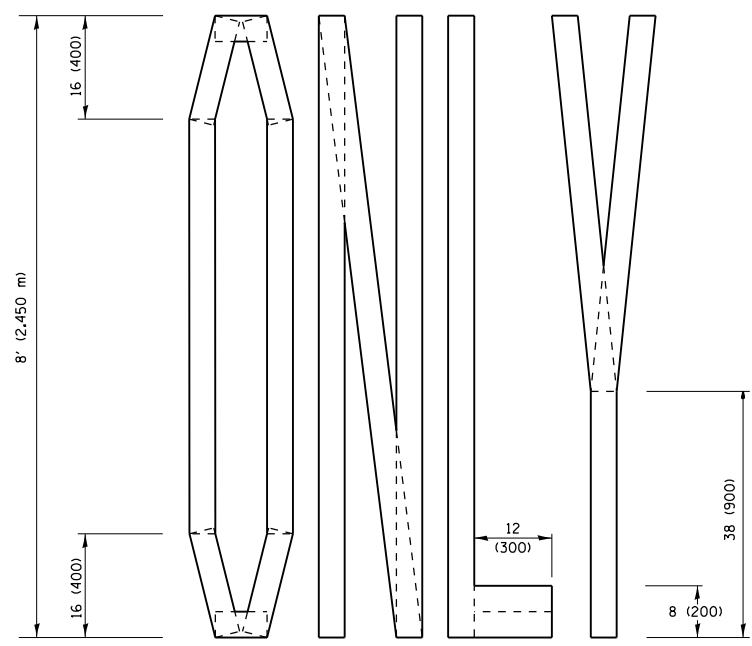
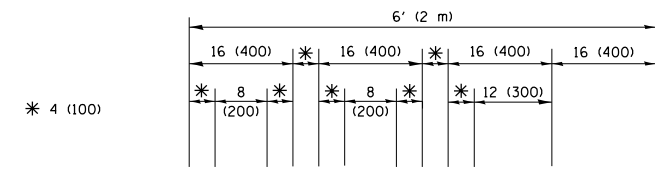
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = paraynoal	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\01430\REVISED Design\HOUSEH 11-07-95	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	REVISED - A. SCHUETZE 07-01-13			345	7Y-RS-8	COOK	40	35
Default	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			TC-14		CONTRACT NO. 62D12		
	PLOT DATE = 3/23/2018	REVISED - T. RAMMACHER 01-06-00	REVISED -			SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.

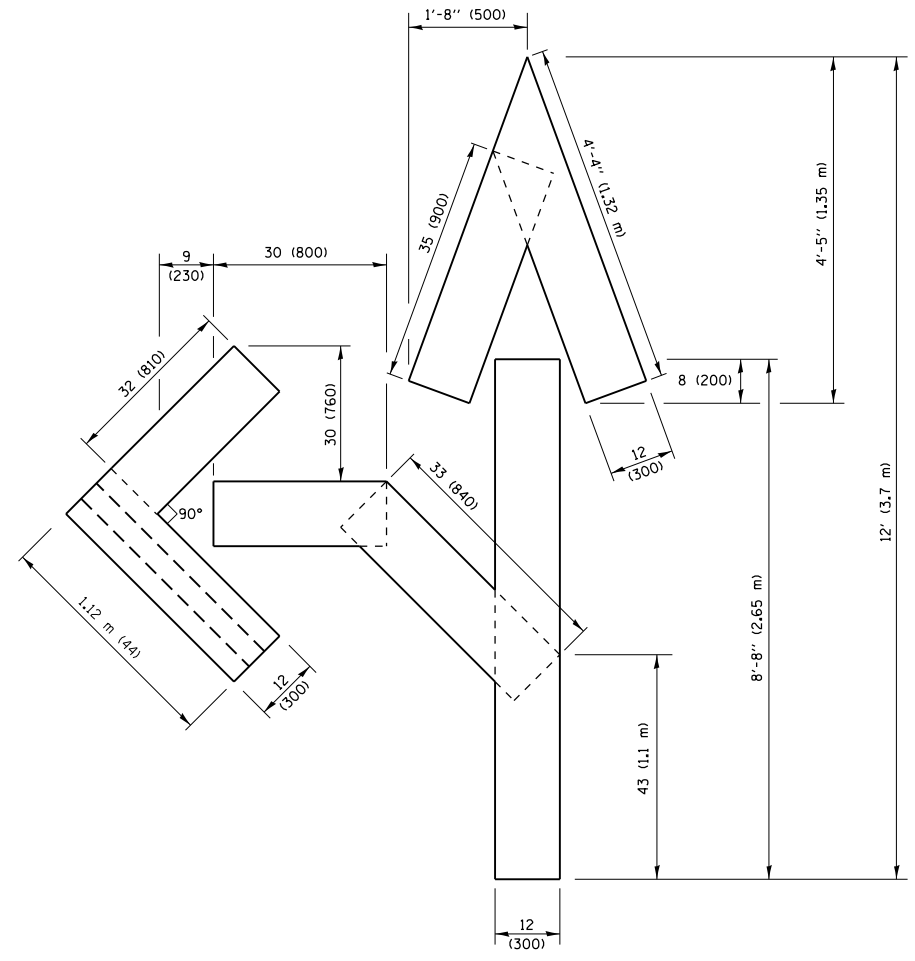
ILLINOIS FED. AID PROJECT



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

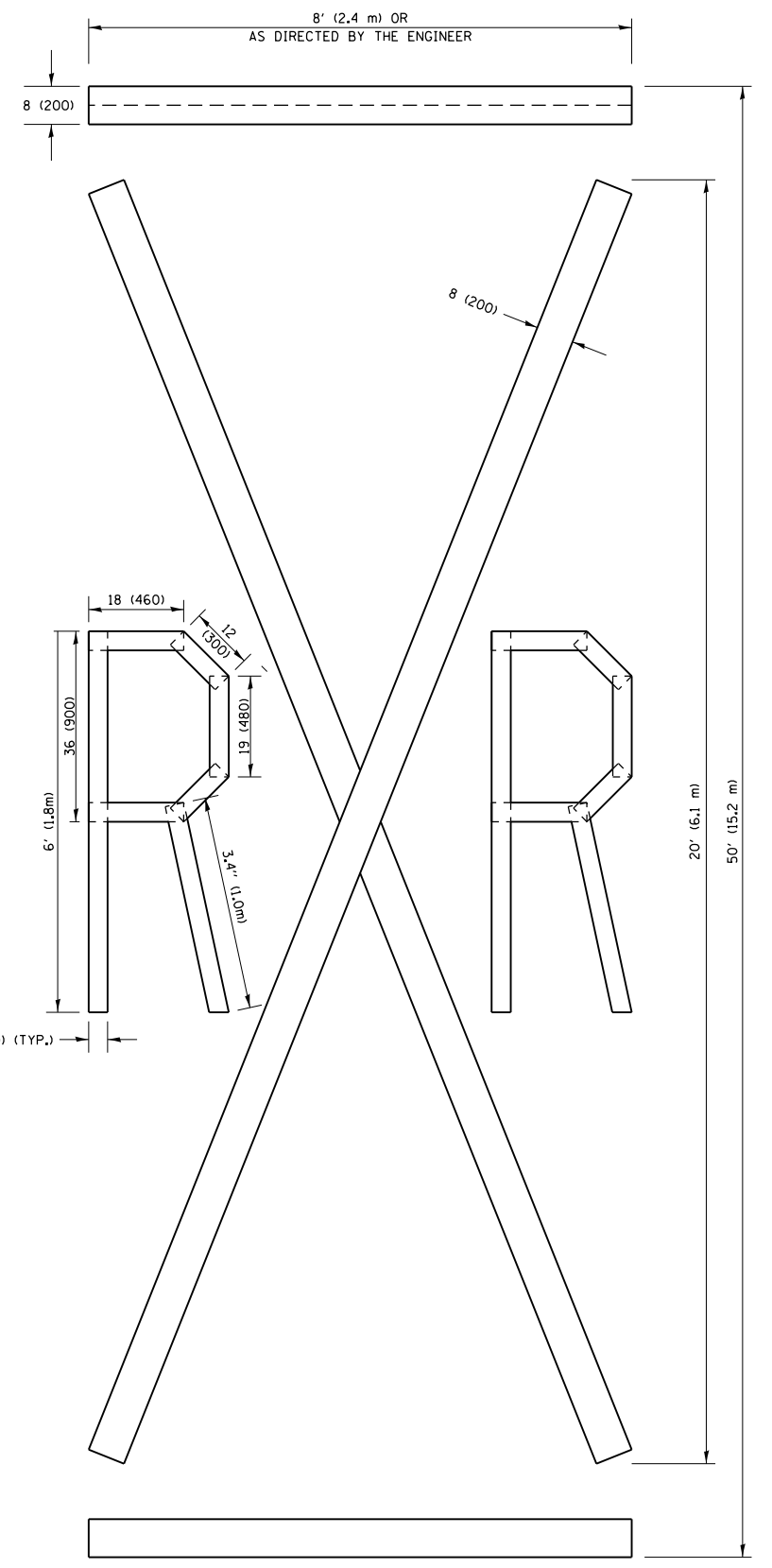


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

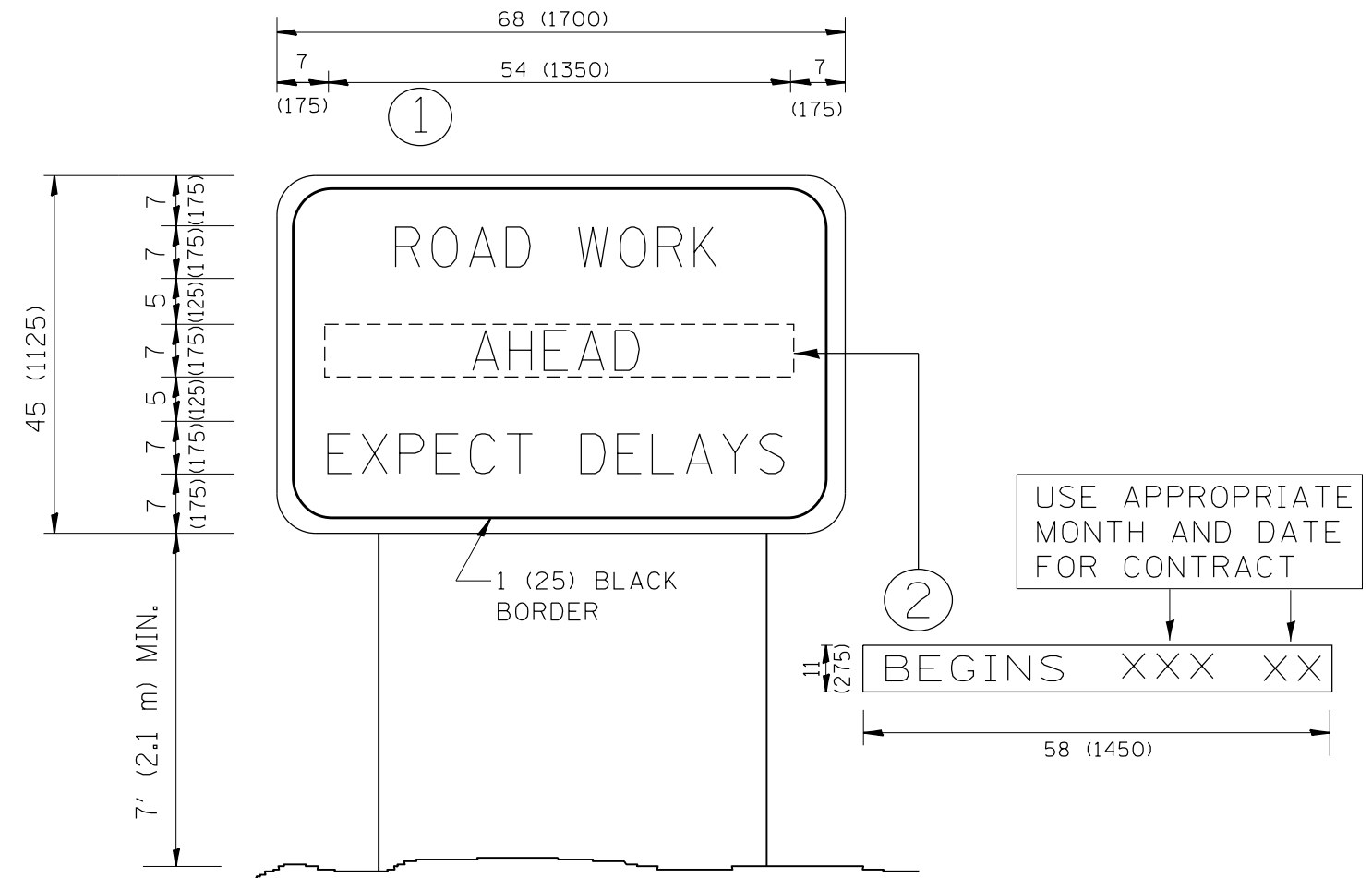
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01430\Drawings\Design\Diststd.dgn		CHECKED -	REVISED -E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00
			REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-RS-B	COOK	40	36
TC-16		CONTRACT NO. 62D12		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

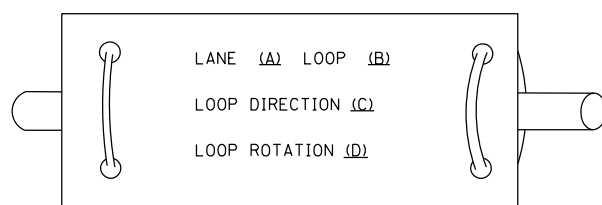
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI430\Drawings\Design\Diststd.dgn		REVISED - R. MIRS 12-11-97				345	7Y-RS-B	COOK	40	37
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99				TC-22		CONTRACT NO. 62D12		
PLOT DATE = 3/23/2018	DATE -	REVISED - C. JUCIUS 01-31-07				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

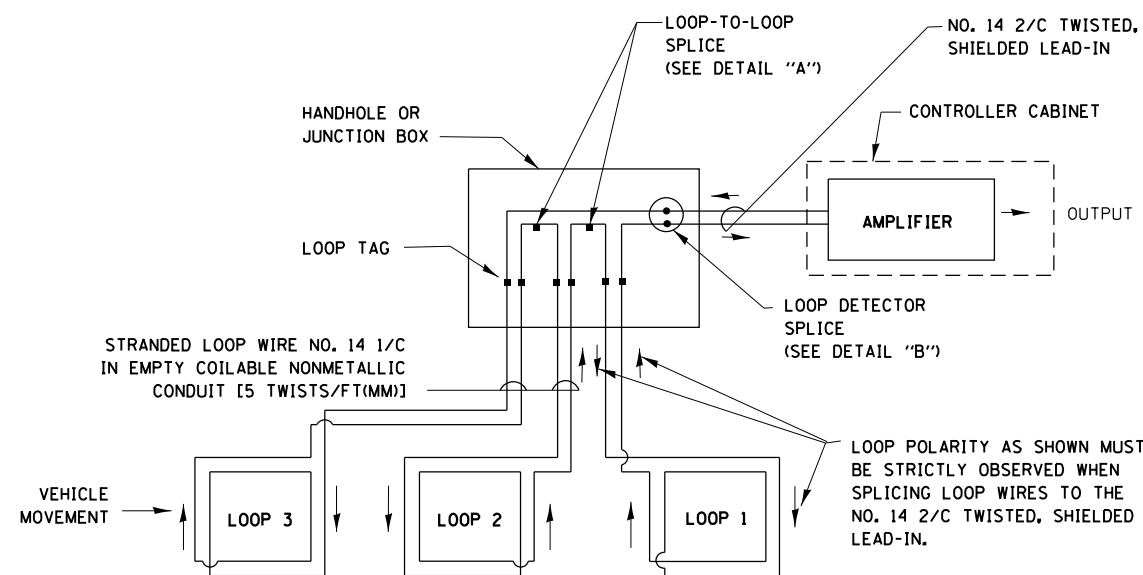
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

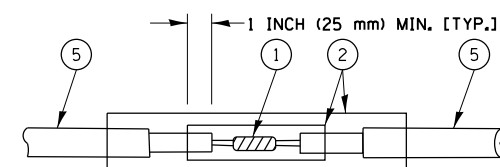


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

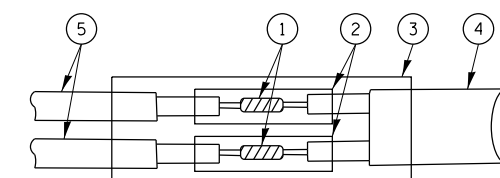


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

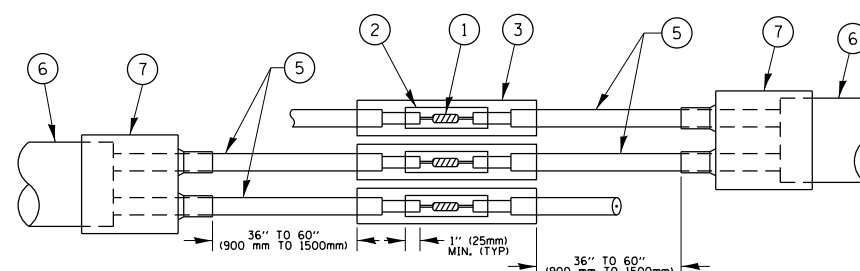


DETAIL "A"
LOOP-TO-LOOP SPLICE

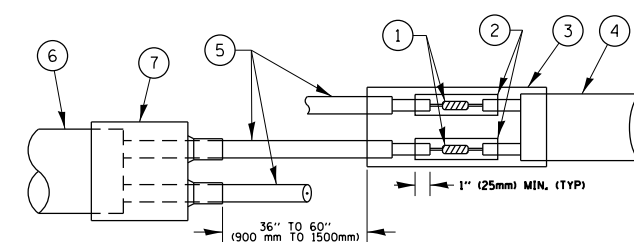


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH, THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = parayno1	DESIGNED -	REVISED -
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI430\Drawings\Design\Diststd.dgn		CHECKED -	REVISED -
Default	PLOT DATE = 3/23/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

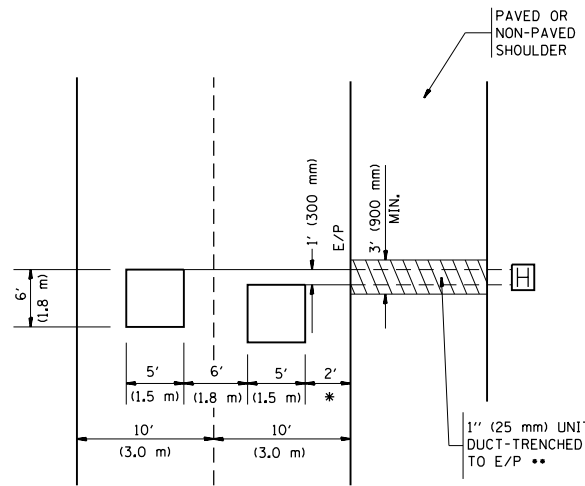
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-RS-8	COOK	40	39
TS-05		CONTRACT NO.	62D12	
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

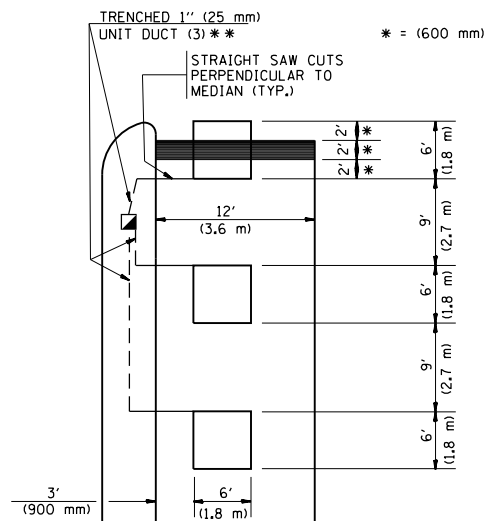


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

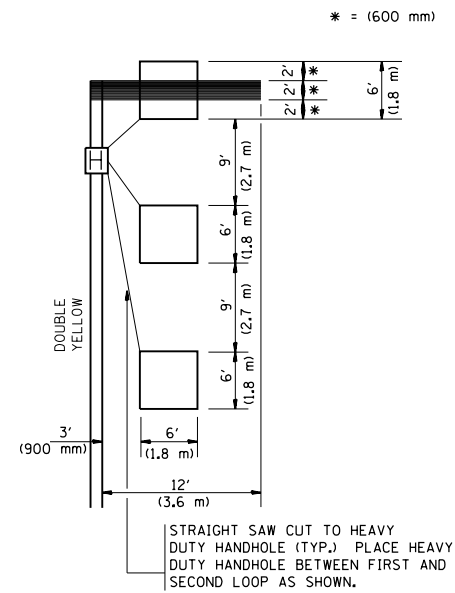
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

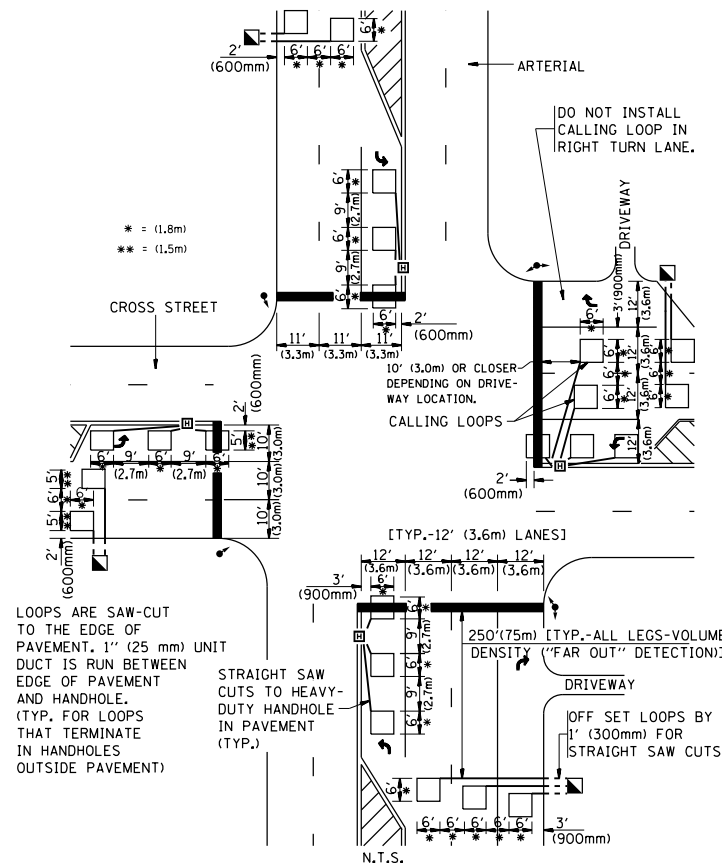
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



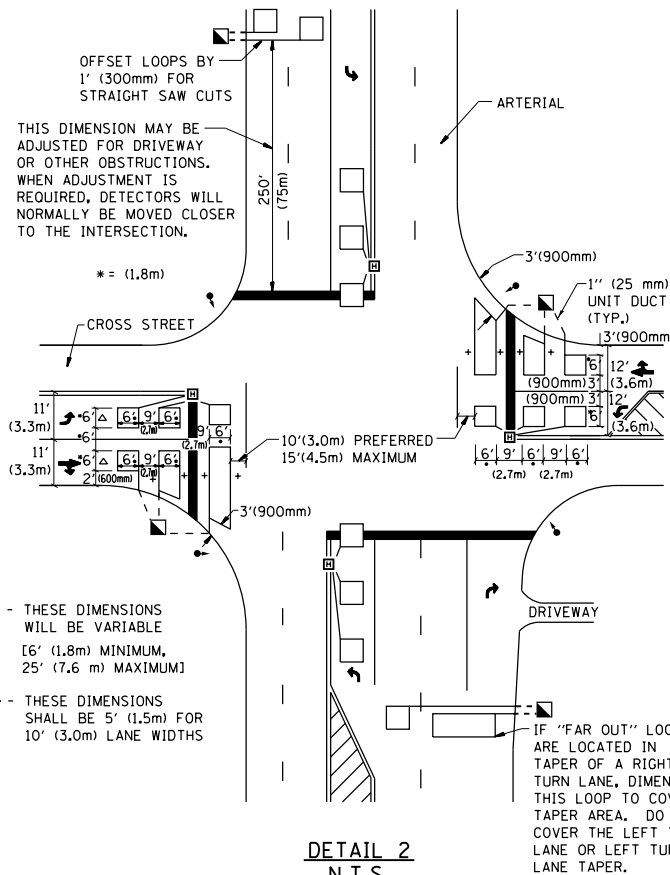
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBID\INTEG\11inois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\01430\01430\Design\Diststd.dgn		CHECKED - R.K.F.	REVISED -					345	7Y-RS-8	COOK	40	40
PLOT SCALE = 100.0000' / 1"		DATE -	REVISED -					TS-07		CONTRACT NO. 62D12		
PLOT DATE = 3/23/2018			REVISED -					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT