06-15-2018 LETTING ITEM 001

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27*	1
	ILLINOIS	CONTRACT NO. 60M74		

* 27 + 1 = 28 TOTAL SHEETS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED
HIGHWAY PLANS

FAU ROUTE 1321: IL 19 (IRVING PARK ROAD)
SECTION 32–B–I
OVER SALT CREEK (0.3 MI. W. OF WOOD DALE ROAD)
PROJECT STP 44DP(185)
BRIDGE DECK OVERLAY, BRIDGE JOINT REPAIRS
DUPAGE COUNTY

C–91–159–11

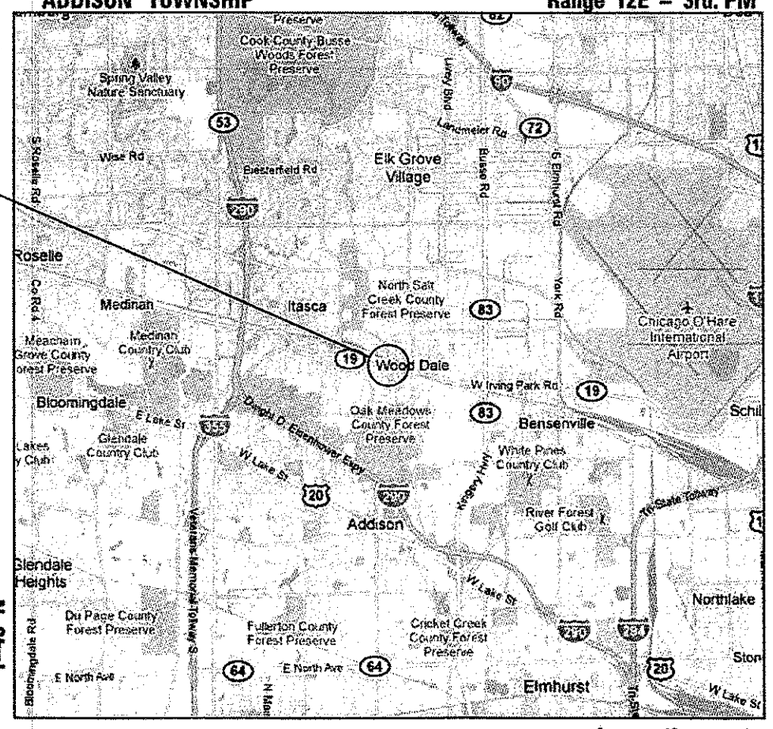
FOR INDEX OF SHEETS, SEE SHEET NO. 2

DESIGN DESIGNATION

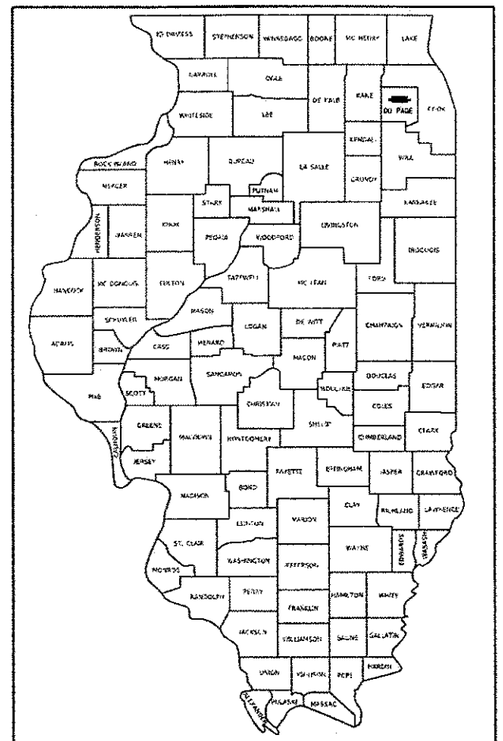
MINOR ARTERIAL (URBAN)
ADT 22,500 (2017)
SPEED LIMIT 35–30 MPH

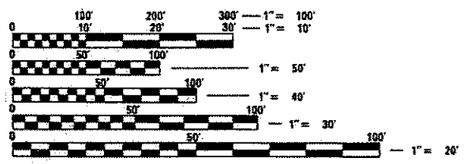
IMPROVEMENT LOCATED IN THE CITY OF WOOD DALE

ADDISON TOWNSHIP — Range 12E – 3rd. PM



GROSS LENGTH = 420.37 FT. = 0.080 MILE
NET LENGTH = 420.37 FT. = 0.080 MILE





FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123
OR 811

PROJECT ENGINEER: RAGHAD ADEIS–DAHHAN (847) 705–4237
PROJECT MANAGER: FAWAD AQUEEL (847)705–4247

CONTRACT NO. 60M74

LICENSED STRUCTURAL ENGINEER — AMBER M. SEIBER 081-007929 — STATE OF ILLINOIS

AMBER M. SEIBER, S.E., P.E.
EXP. 11/30/2018
SHEETS: 9–19

ZACHARY J. TANNER — 062-065582 — REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS

ZACHARY J TANNER, P.E.
EXP. 11/30/2019
SHEETS: 1–8

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED March 22 20 18
REGIONAL ENGINEER
May 11 20 18
ENGINEER OF DESIGN AND ENVIRONMENT
May 20 18
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

COLLINS ENGINEERS INC
123 N. WACKER DR., SUITE 900
CHICAGO, IL 60606
(312) 704-9300
ILLINOIS PROFESSIONAL DESIGN FIRM
LICENSE NO. 184-000993

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	GENERAL NOTES, INDEX OF SHEETS AND HIGHWAY STANDARDS
3-5A	SUMMARY OF QUANTITIES
6-7	MAINTENANCE OF TRAFFIC
8	ROADWAY PLAN
9-19	STRUCTURE PLANS (S1-S9)
20-27	DISTRICT 1 STANDARDS

INDEX OF HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
420001-09	PAVEMENT JOINTS
515001-03	NAME PLATE FOR BRIDGES
643001-02	SAND MODULE IMPACT ATTENUATORS
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' to 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS, DAY ONLY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD OPERATIONS, MULTILANE, WITH MOUNTABLE MEDIAN
701801-06	LANE CLOSURE, MULTILANE 1W OR 2W, CROSSWALK OR SIDEWALK CLOSURE
701901-07	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
782006	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

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SHEET NO.	DESCRIPTION
18	BD24 - CURB AND GUTTER REMOVAL AND REPLACEMENT
19	BD32 - BUTT JOINT AND HMA DETAILS
20	TC10 - TRAFFIC CONTROL & PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
21	TC11 - TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
22	TC13 - DISTRICT ONE TYPICAL PAVEMENT MARKINGS
23	TC14 - TRAFFIC CONTROL & PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
24	TC22 - ARTERIAL ROAD INFORMATION SIGN
25	TC26 - DRIVEWAY ENTRANCE SIGNING

COMMITMENTS

NONE

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL J.U.L.I.E. AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES (48 HOUR NOTICE IS REQUIRED).
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE LOCAL MUNICIPALITY.
3. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE RIGHT-OF-WAY OR PROPERTY WITHOUT PRIOR WRITTEN PERMISSION FROM THE ENGINEER.
4. WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 40 MPH (80 KM/HR) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80KM/HR). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
5. 10 FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED ITEMS OF WORK TO EXISTING ITEMS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEM OF WORK SPECIFIED.
6. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH DISTRICT ONE STANDARD BD32 "BUTT JOINT AND HMA TAPER DETAILS," UNLESS OTHERWISE SPECIFIED.
7. WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
8. BEFORE BEGINNING ANY WORK THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
9. THE CONTRACTOR SHALL MAINTAIN ALL ROADWAYS OPEN TO TRAFFIC AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS.
10. THE CONTRACTOR SHALL CONTACT THE IDOT DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT 847-705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
11. THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTOR'S OWN EXPENSE.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.
13. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE THE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
14. DURING CONSTRUCTION OPERATIONS, IF ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DUST AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
15. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
16. THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD ENGINEER, DON CHIARUGI AT DON.CHIARUGI@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
17. DO NOT SCALE PLANS FOR CONSTRUCTION PURPOSES.
18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.
19. THE CONTRACTOR SHALL TAKE WHATEVER PRECAUTIONS WHICH MAY BE NECESSARY TO PROTECT THE PROPERTY OF THE VARIOUS PUBLIC UTILITIES WHICH MAY BE LOCATED UNDERGROUND OR ABOVE GROUND, AT OR ADJACENT TO THE SITE OF THIS IMPROVEMENT. HE WILL BE REQUIRED TO REPAIR OR REPLACE AT HIS OWN EXPENSE, OR BEAR THE COST, TO REPAIR OR REPLACE, ANY PUBLIC UTILITY PROPERTY WHICH HAS BEEN DAMAGED THROUGH HIS EFFORTS.
20. THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM DISTRICT 1 BRIDGE INSPECTORS.
21. MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS: A NOMINAL QUANTITY HAS BEEN INCLUDED IN THE CONTRACT. THE LOCATIONS AND LIMITS OF ALL JOINT OR CRACK FILLING SHALL BE DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION. IF THE MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS IS NOT REQUIRED, THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ NDES	QMP
APPROACH SLAB OVERLAY AND BUTT JOINTS:		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm), 1½"	4% @ 70% Gyr.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QA/QC); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

1) THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN
2) THE AC TYPE FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS"
3) FOR USE OF RECYCLED MATERIALS, SEE SPECIAL PROVISIONS.
4) QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

COLLINS ENGINEERS
ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-001745

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PLOT DATE = 3/22/2018	DATE - SEPTEMBER, 2014	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, INDEX OF SHEETS AND HIGHWAY STANDARDS

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-1	DuPAGE	27	2
		CONTRACT NO. 60M74		
	ILLINOIS FED. AID PROJECT			

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 80% Federal 20% State ROADWAY 0013 URBAN	80% Federal 20% State BRIDGE 0013 S.N. 022-0147
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	845	192	653
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	1	1	0
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	427	427	0
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	158	36	122
50102400	CONCRETE REMOVAL	CU YD	46.7	0	46.7
50300255	CONCRETE SUPERSTRUCTURE	CU YD	52.9	0	52.9
50300260	BRIDGE DECK GROOVING	SQ YD	1055	0	1055
50300300	PROTECTIVE COAT	SQ YD	448	22	426
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	4270	0	4270
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	10650	0	10650
50800515	BAR SPLICERS	EACH	24	0	24
52000110	PREFORMED JOINT STRIP SEAL	FOOT	215	0	215
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	1	1	0
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6	0

* - SPECIALTY ITEM

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COLLINS ENGINEERS

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PLOT DATE = 3/22/2019	DATE - SEPTEMBER, 2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	3

CONTRACT NO. 60M74

ILLINOIS FED. AID PROJECT

	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 80% Federal 20% State ROADWAY 0013 URBAN	CONSTRUCTION CODE 80% Federal 20% State BRIDGE 0013 S.N. 022-0147
	67100100	MOBILIZATION	L SUM	1	1	0
	70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DAY	40	40	0
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	289	289	0
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	97	97	0
	70300904	PAVEMENT MARKING TAPE, TYPE IV 4"	FOOT	7420	7420	0
	70400100	TEMPORARY CONCRETE BARRIER	FOOT	487.5	487.5	0
	70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	487.5	487.5	0
	70600240	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 2	EACH	2	2	0
	70600340	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 2	EACH	2	2	0
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	16	16	0
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3862	3862	0
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	130	130	0
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	140	140	0
*	78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	707	707	0

* - SPECIALTY ITEM

	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 80% Federal 20% State ROADWAY 0013 URBAN	CONSTRUCTION CODE 80% Federal 20% State BRIDGE 0013 S.N. 022-0147
*	78008239	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	148	148	0
*	78008250	POLYUREA PAVEMENT MARKING TYPE I - LINE 12"	FOOT	50	50	0
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	73	73	0
	78100200	TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER	EACH	55	55	0
*	78100300	REPLACEMENT REFLECTOR	EACH	102	102	0
*	78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	20	20	0
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	73	73	0
	X0322215	CLEANING BRIDGE SCUPPERS AND DOWNSPOUTS	EACH	14	0	14
	X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	1794	1794	0
	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	0
	X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	2474	2474	0
	Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	28	0	28
	Z0001903	STRUCTURAL STEEL REMOVAL	POUND	1470	0	1470
	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	150	120	30
	Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	

* - SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 80% Federal 20% State ROADWAY 0013 URBAN	CONSTRUCTION CODE 80% Federal 20% State BRIDGE 0013 S.N. 022-0147
Z0006012	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/4 INCHES	SQ YD	967	0	967
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	967	0	967
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	52	52	0
Z0033028	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	6	6	0

* - SPECIALTY ITEM

COLLINS ENGINEERS 123 N. Wacker Dr. Suite 200 Chicago, IL 60606 Telephone: (312) 704-9300 Fax: (312) 704-9200 www.collinsengr.com ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-001913

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	5A
		CONTRACT NO. 60M74		
	ILLINOIS FED. AID PROJECT			

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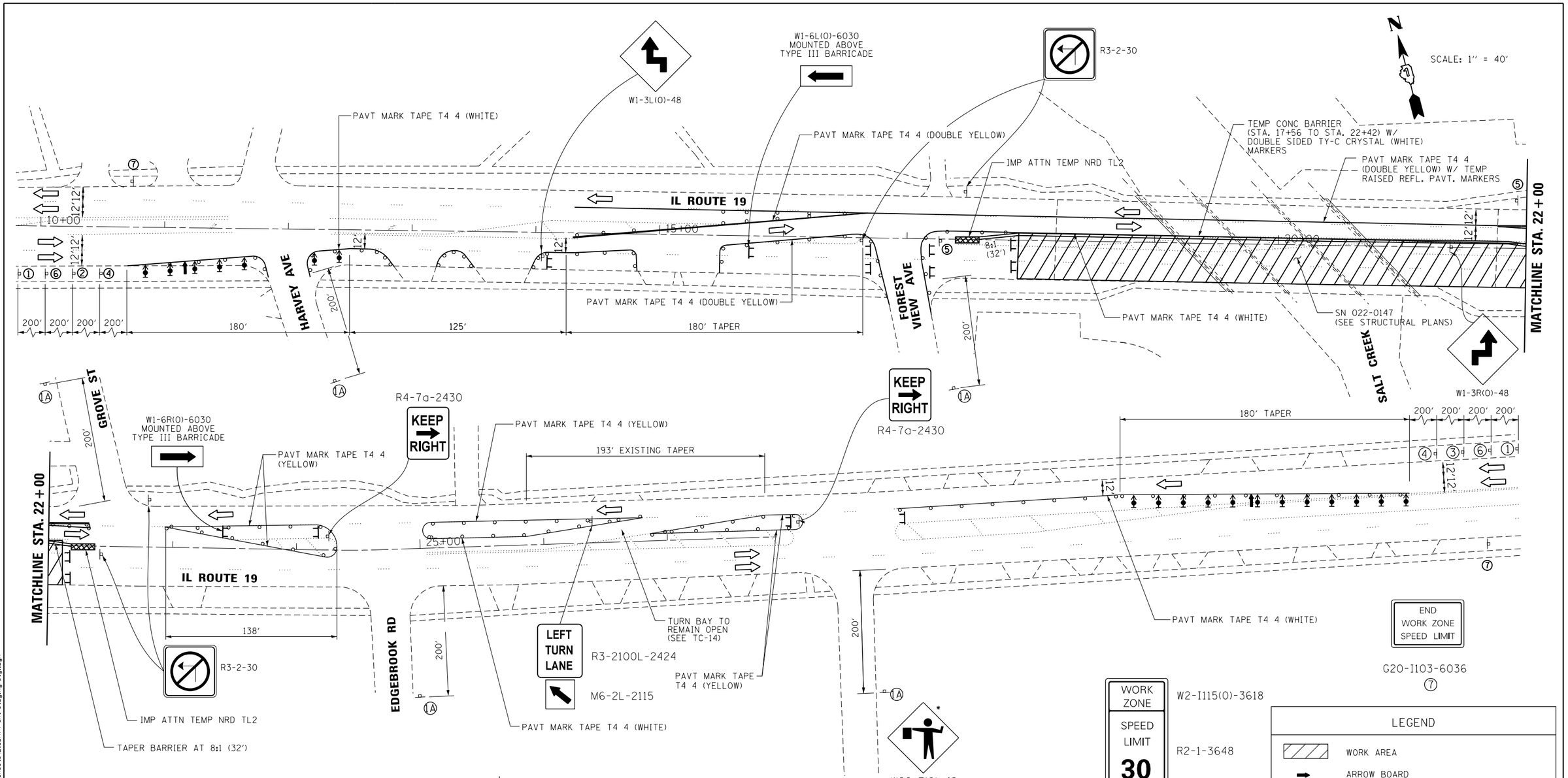

STAGE I (IL 19)

ESTABLISH TRAFFIC CONTROL PER STD. 701611 AND AS DETAILED IN THE TRAFFIC CONTROL PLAN.

SCARIFY BRIDGE DECK.

PERFORM DECK SLAB REPAIR AND APPROACH SLAB REPAIR.

RECONSTRUCT LONGITUDINAL JOINT AND DECK JOINTS AT EACH ABUTMENT.

PLACE LATEX-MODIFIED OVERLAY AND POLYMER CONCRETE ON STRUCTURE.

REMOVE AND REPLACE CONCRETE CURB & GUTTER AS DETAILED.

PLACE HMA OVERLAY ON BRIDGE APPROACHES AND BUTT JOINT.

* MUST BE REMOVED WHEN FLAGGER OR WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR

LEGEND

- WORK AREA
- ARROW BOARD
- CONSTRUCTION SIGN
- DRUMS OR TYPE II BARRICADES
- TYPE III BARRICADE W/2 FLASHING BEACONS (NO. OF BARRICADES AS DIRECTED BY THE ENGINEER
- DIRECTION OF TRAFFIC
- IMPACT ATTENUATOR
- DIRECTION INDICATOR BARRICADE (SPACED 20' C-C)

COLLINS ENGINEERS
123 N. Wacker Dr.
Suite 800
Chicago, IL 60606
Tele. (312) 704-9500
Fax (312) 704-9520
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PLOT DATE = 3/22/2018	DATE - SEPTEMBER, 2014	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 19 (IRVING PARK ROAD)
MAINTENANCE OF TRAFFIC STAGE I

SCALE: | SHEET NO. 1 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-1	DuPAGE	27	6
		CONTRACT NO. 60M74		
	ILLINOIS FED. AID PROJECT			

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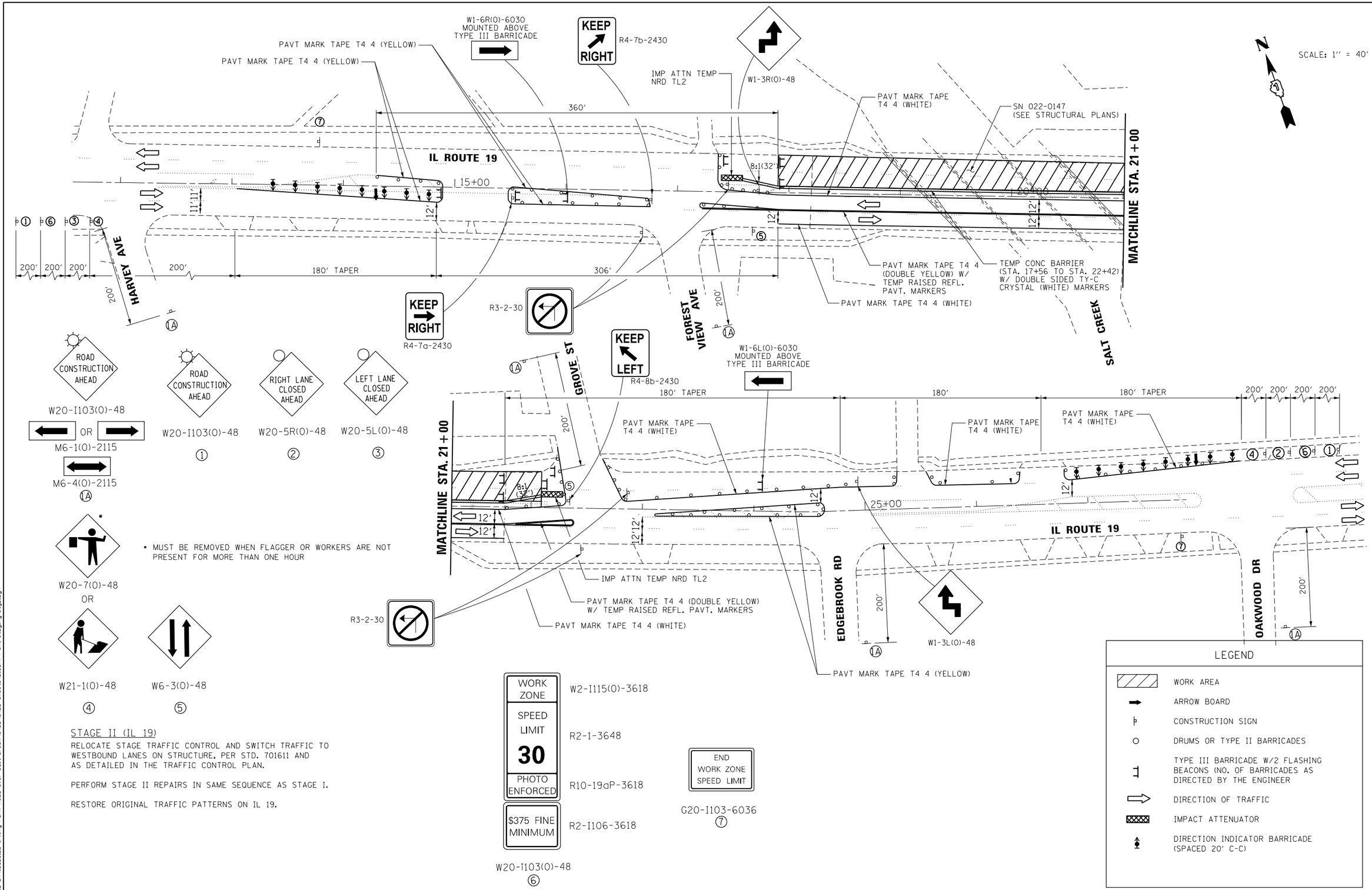

* MUST BE REMOVED WHEN FLAGGER OR WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR

STAGE II (IL 19)

RELOCATE STAGE TRAFFIC CONTROL AND SWITCH TRAFFIC TO WESTBOUND LANES ON STRUCTURE, PER STD. 701611 AND AS DETAILED IN THE TRAFFIC CONTROL PLAN.

PERFORM STAGE II REPAIRS IN SAME SEQUENCE AS STAGE I.

RESTORE ORIGINAL TRAFFIC PATTERNS ON IL 19.

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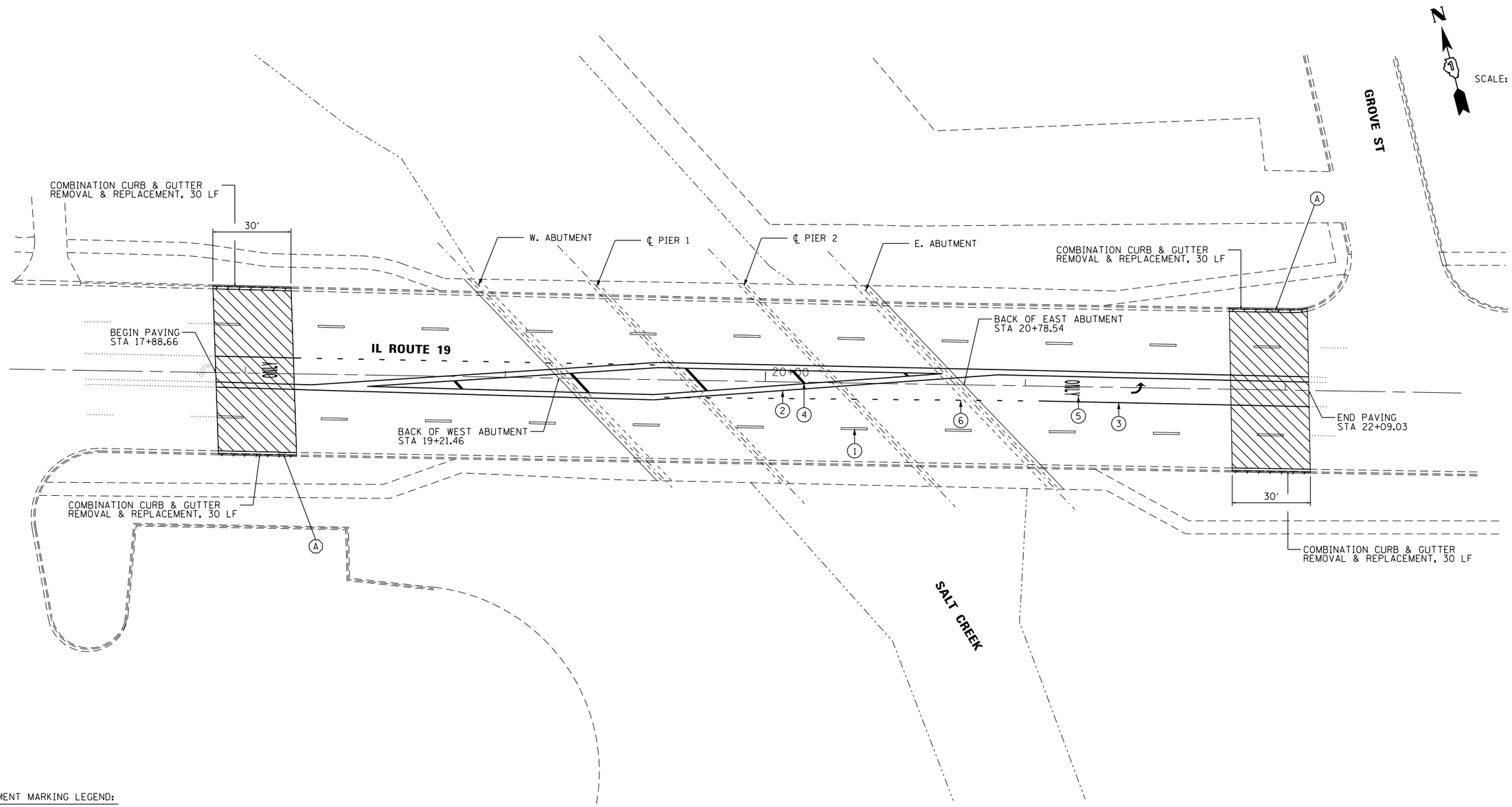
PLOT DATE = 3/22/2018	DATE - SEPTEMBER, 2014	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 19 (IRVING PARK ROAD)
MAINTENANCE OF TRAFFIC STAGE II

SCALE: | SHEET NO. 2 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	7
		CONTRACT NO. 60M74		
		ILLINOIS FED. AID PROJECT		



PAVEMENT MARKING LEGEND:

(1)	4" WHITE LANE LINE (10' DASH, 30' SKIP)
(2)	4" DOUBLE YELLOW LINE (11" C-C)
(3)	6" WHITE LANE LINE
(4)	12" YELLOW DIAGONALS AT 45° ANGLE, 75' C-C SPACING
(5)	LETTERS AND SYMBOLS
(6)	6" WHITE (2' DASH, 6' SKIP)

PAVEMENT MARKING NOTES:

POLYUREA PAVEMENT MARKING TO BE USED ON PCC PAVEMENT SURFACES

THERMOPLASTIC PAVEMENT MARKINGS TO BE USED ON HMA SURFACES

ROADWAY LEGEND

▨ – HMA SURFACE REMOVAL-BUTT JOINT (30') & PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

(A) – ADJUST FRAME AND LID

ROADWAY NOTES:

REFER TO DETAIL BD-24 FOR COMBINATION CURB & GUTTER REMOVAL & REPLACEMENT DETAIL.
REFER TO DETAIL BD-32 FOR BUTT JOINT AND HMA TAPER DETAIL.

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USER NAME = aseiber	DESIGNED - MR	REVISED -	
	DRAWN - MAP	REVISED -	
PLOT SCALE = 40.0000 ' / in.	CHECKED - MGR	REVISED -	
PLOT DATE = 3/22/2018	DATE - SEPTEMBER, 2014	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 19 (IRVING PARK ROAD)
ROADWAY PLAN

SCALE: | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	8
		CONTRACT NO. 60M74		
		ILLINOIS FED. AID PROJECT		

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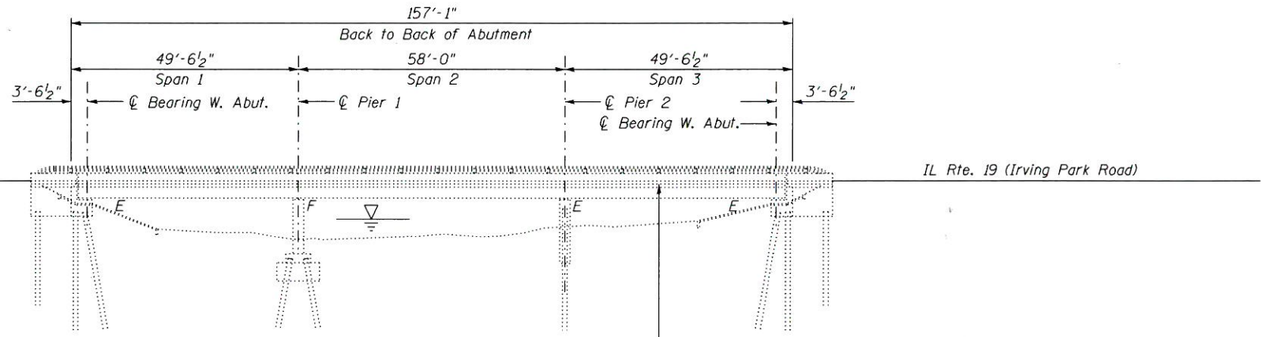
Existing Structure: The Structure is a three span continuous steel Superstructure with a reinforced concrete deck and substructure. The original Structure was built in 1983 as Section 32 BR-79.

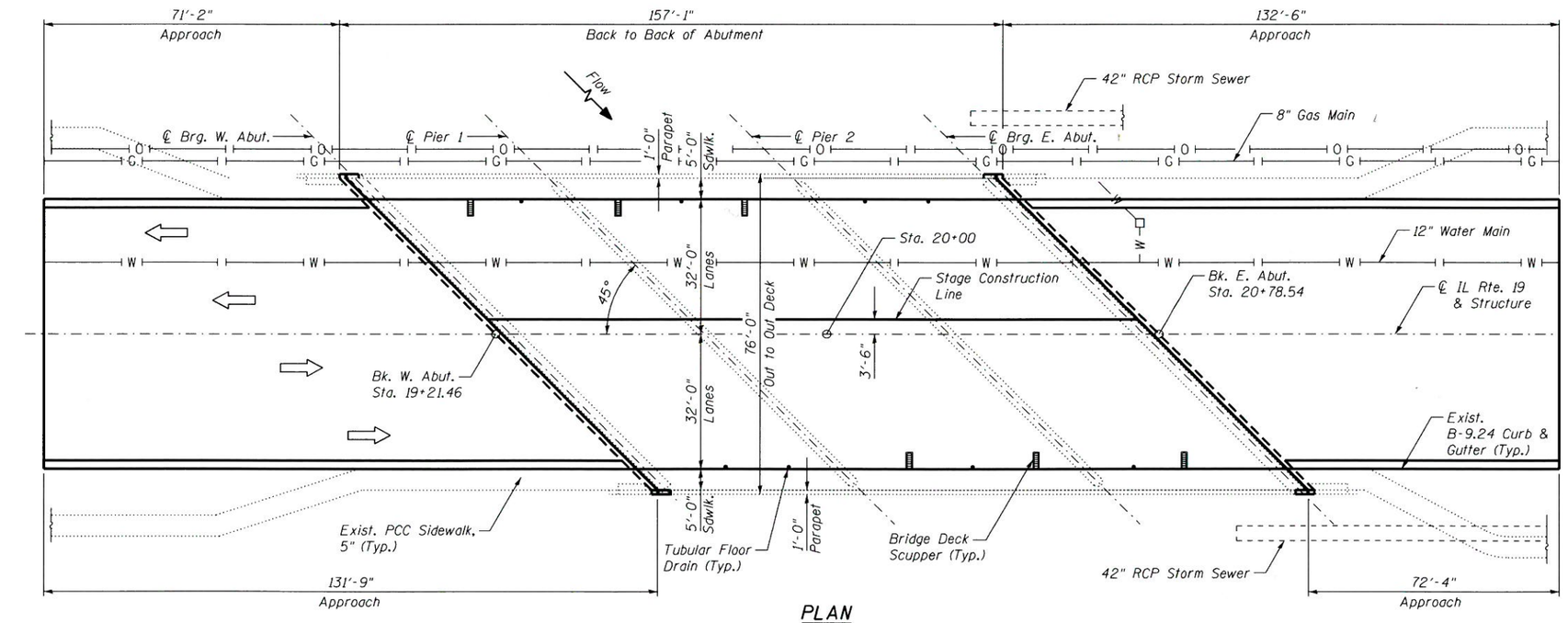
Stage construction will be utilized to maintain traffic during construction.

No Salvage.

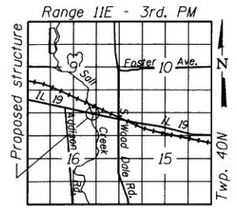


ELEVATION

(4)-Conduits Attached to Face of South Parapet
(1)-Conduit Attached to Face of North Parapet
Note: Conduits to be maintained. Cost of this work shall not be paid for seperately but shall be included in the applicable pay items.



PLAN



LOCATION SKETCH

SCOPE OF WORK

1. Bridge deck scarification.
2. Reconstruct longitudinal joint.
3. Install intermediate diaphragms.
4. Repair beam ends at abutments.
5. Repair approach slab.
6. Remove and replace curb & gutter.
7. Reconstruct deck joints at each abutment with preformed strip seal.
8. Place overlay on bridge deck and approach slabs.
9. Clean deck scuppers and floor drains.

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications for Highway Bridges (17th Edition)

LOADING HS20-44

DESIGN STRESSES

FIELD UNITS (Existing)

f'c = 3,500 psi
fy = 60,000 psi (reinforcement)
Fy = 50,000 psi (structural steel)

LICENSED STRUCTURAL ENGINEER
AMBER M. SEIBER
081-007929
NAPERVILLE, ILLINOIS
EXP. 11/30/2018

GENERAL PLAN AND ELEVATION
IL. RT. 19 OVER SALT CREEK
F.A.U. RT. 1321 - SEC. 32-B-I
DuPAGE COUNTY
STATION 20+00
STRUCTURE NO. 022-0147

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION
STRUCTURE NO. 022-0147
SHEET NO. S1 OF S11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	9
		CONTRACT NO. 60M74		
	ILLINOIS FED. AID PROJECT			

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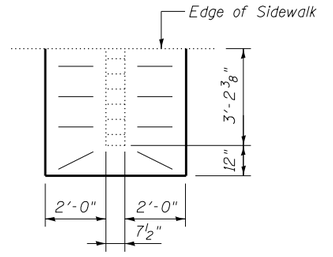
INDEX OF SHEETS

GENERAL NOTES:

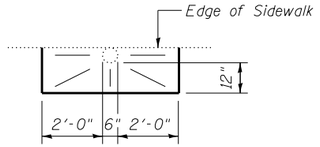
1. Reinforcement bars designated (E) shall be epoxy coated.
2. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
3. Cost for removal and disposal of existing expansion joints is included in the cost of "Concrete Removal".
4. The removal and reattachment of guardrail, hand rail, steel railings, traffic barrier terminal, etc. required for repair work (e.g. transverse joint replacement concrete) shall be included in the contract unit price of the work item being performed.
5. Staged construction shall be utilized to maintain traffic during construction.
6. All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.
7. The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead in this project.
8. Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".
9. All new structural steel shall be shop painted with the inorganic zinc rich primer per AASHTO M300, Type 1. Cost included with Structural Steel repair.
10. Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.
11. Fasteners shall be AASHTO M164 Type 1, mechanically galvanized bolts. Bolts $^{3}/_{4}$" dia., holes $^{13}/_{16}$" dia., unless otherwise noted.
12. Synthetic fibers shall be added to the "Bridge Deck Latex Concrete Overlay", see Special Provisions.

TOTAL BILL OF MATERIAL

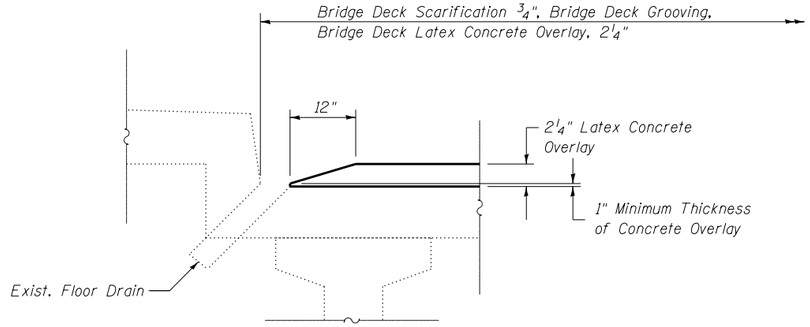
ITEM DESCRIPTION	UNIT	QUANTITY
Bituminous Materials (Tack Coat)	Pound	653
Hot-Mix Asphalt Surface Course, Mix "D", N70	Ton	122
Concrete Removal	Cu. Yd.	46.7
Concrete Superstructure	Cu. Yd.	52.9
Bridge Deck Grooving	Sq. Yd.	1,055
Protective Coat	Sq. Yd.	426
Furnishing and Erecting Structural Steel	Pound	4,270
Reinforcement Bars, Epoxy Coated	Pound	10,650
Bar Splicers	Each	24
Preformed Joint Strip Seal	Foot	215.0
Cleaning Bridge Scuppers and Downspouts	Each	14
Approach Slab Repair (Partial Depth)	Sq. Yd.	28
Structural Steel Removal	Pound	1,470
Combination Concrete Curb and Gutter Removal and Replacement	Foot	30
Bridge Deck Latex Concrete Overlay, $2^{1}/_{4}$"	Sq. Yd.	967
Bridge Deck Scarification, $^{3}/_{4}$"	Sq. Yd.	967



PLAN-DETAIL OF CONCRETE OVERLAY AT SCUPPER



PLAN-DETAIL OF CONCRETE OVERLAY AT DRAIN



CONCRETE OVERLAY AT FLOOR DRAIN/SCUPPER

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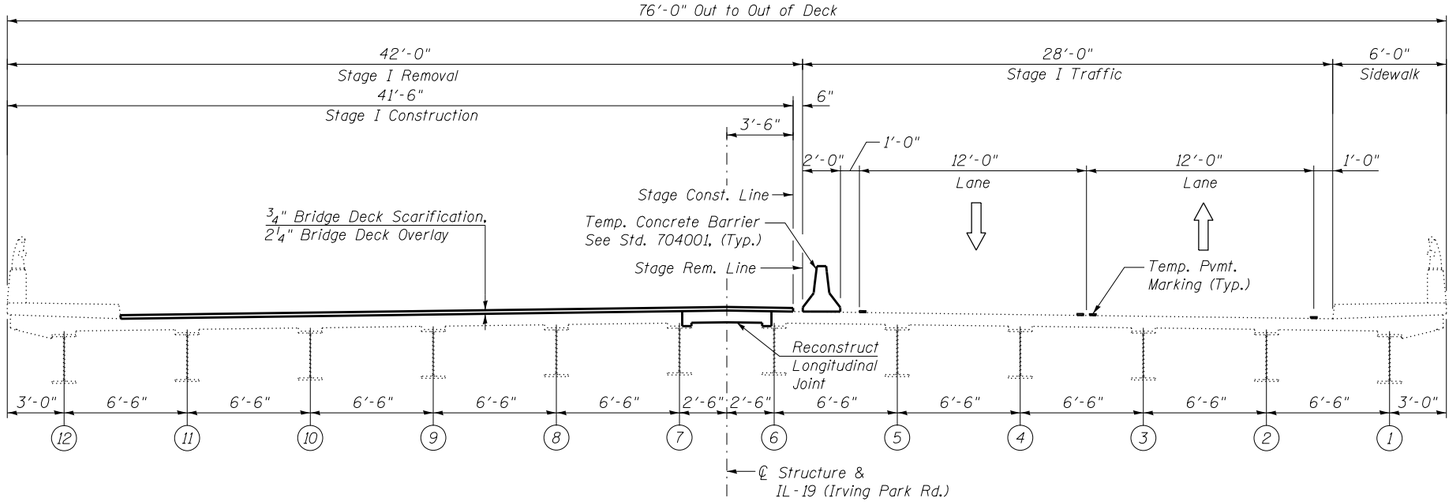
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, BILL OF MATERIALS AND INDEX OF SHEETS
STRUCTURE NO. 022-0147
SHEET NO. S2 OF S11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	10
		CONTRACT NO. 60M74		
	ILLINOIS FED. AID PROJECT			



STAGE I CROSS SECTION

Looking West

STAGE II CROSS SECTION

Looking West

Notes:

The Contractor shall maintain a minimum of one through lane in each direction on IL 19 (Irving Park Road) at all times.

The Contractor shall keep sidewalk open on one side of structure at all times.

The exact number, location, and spacing of all signs and traffic control devices may be adjusted to fit field conditions as directed by the Engineer.

The Contractor will be required to provide and maintain access to all private drives and commercial property during the construction period. Signing shall be provided for all driveway entrances per district detail TC-26.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 022-0147
SHEET NO. S3 OF S11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	11
		CONTRACT NO. 60M74		
	ILLINOIS FED. AID PROJECT			

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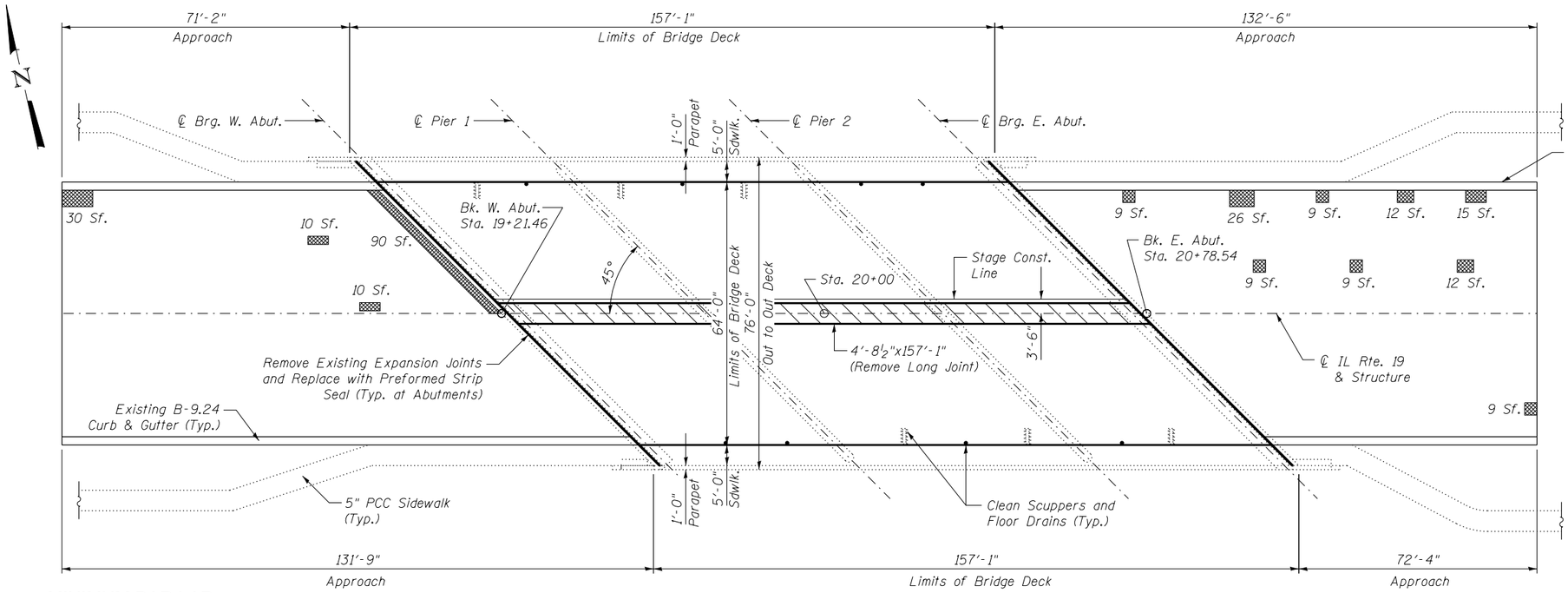


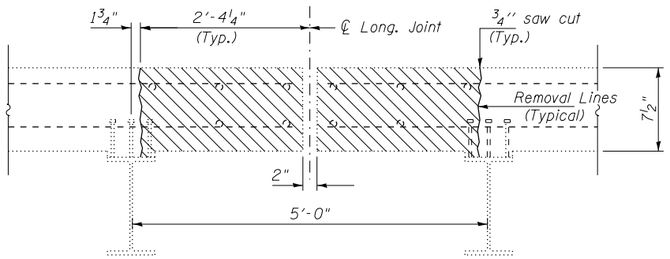
PLAN

NOTES:

1. Approach slab repair areas are estimated based on visual inspection. Actual repair areas and locations shall be determined by the Engineer and shown on as-built plans.
2. Bridge Deck Scarification, ¾" and Bridge Deck Latex Concrete Overlay, 2¼" shall be performed over the limits of the bridge deck, excluding transverse and longitudinal joint reconstruction areas.
3. The following items apply to the extents of the approaches:
   Bituminous Materials (Tack Coat).
   Hot-Mix Asphalt Surface Course, Mix "D", N70. (1½" Thick)
4. Bars indicated thus 4x5-#6 etc. indicates 4 lines of bars with 5 lengths per line.
5. Protective coat shall be applied to the bridge deck sidewalks, inside and top faces of parapets, and deck areas adjacent to the transverse and longitudinal joint reconstruction areas only.

MINIMUM BAR LAP

#6 bar = 3'-7"

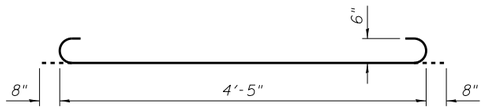


EXISTING LONGITUDINAL JOINT CROSS SECTION

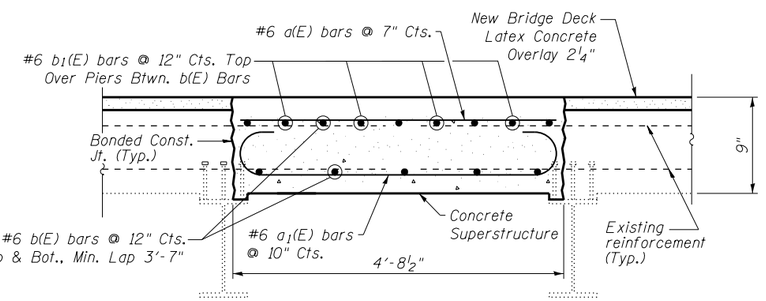
Notes:

Hatched areas indicate concrete sections to be removed. Perimeters of Concrete Removal areas shall be saw cut ¾" prior to the removal of the concrete. Existing transverse reinforcement shall be cleaned straightened and incorporated into the new construction. New reinforcement bars shall be epoxy coated.

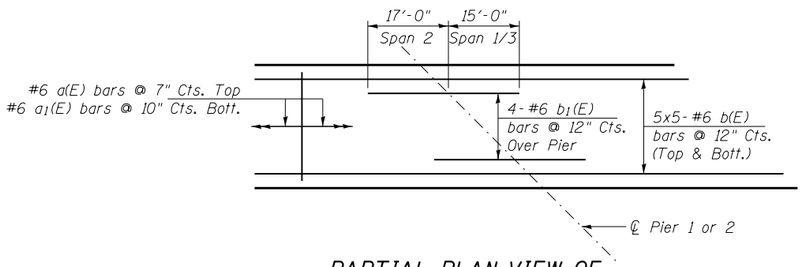
The Contractor shall exercise care during removal of existing joints to ensure that the slab, and beams, diaphragms' integrity will not be detrimentally impacted. The Contractor shall repair any damage(s) to the slab, beams, diaphragms caused by his operation as directed by the Engineer at no additional cost to the Department.



BAR $a_1(E)$



RECONSTRUCTED LONGITUDINAL JOINT WITH CONCRETE BRIDGE DECK OVERLAY DETAIL



PARTIAL PLAN VIEW OF LONG JOINT RECONSTRUCTION

BILL OF MATERIAL

SYMBOL	ITEM	UNIT	QUANTITY
	Bituminous Materials (Tack Coat)	Pound	653
	Hot-Mix Asphalt Surface Course Mix "D", N70	Ton	122
	Concrete Removal	Cu. Yd.	15.4
	Concrete Superstructure	Cu. Yd.	19.1
	Bridge Deck Grooving	Sq. Yd.	1,055
	Protective Coat	Sq. Yd.	426
	Reinforcement Bars, Epoxy Coated	Pound	6,390
	Cleaning Bridge Scuppers and Downspouts	Each	14
▩	Approach Slab Repair (Partial Depth)	Sq. Yd.	28
	Combination Concrete Curb and Gutter Removal and Replacement	Foot	30
	Bridge Deck Latex Concrete Overlay, 2¼"	Sq. Yd.	967
	Bridge Deck Scarification, ¾"	Sq. Yd.	967

BAR	NO.	SIZE	LENGTH	SHAPE
a(E)	270	#6	4'-5"	———
$a_1(E)$	189	#6	5'-9"	⌒
b(E)	50	#6	34'-3"	———
$b_1(E)$	8	#6	32'-0"	———

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BRIDGE DECK AND APPROACH SLAB REPAIRS
STRUCTURE NO. 022-0147
SHEET NO. S4 OF S11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	12
		CONTRACT NO. 60M74		
	ILLINOIS FED. AID PROJECT			

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Notes:

End diaphragms removed for beam end repairs shall be replaced.

The existing end diaphragms are welded to the beams. Contractor shall exercise extreme care during removal to not damage the beams during removal. Removal shall be by air-arc method. The Contractor shall grind smooth all weld material.

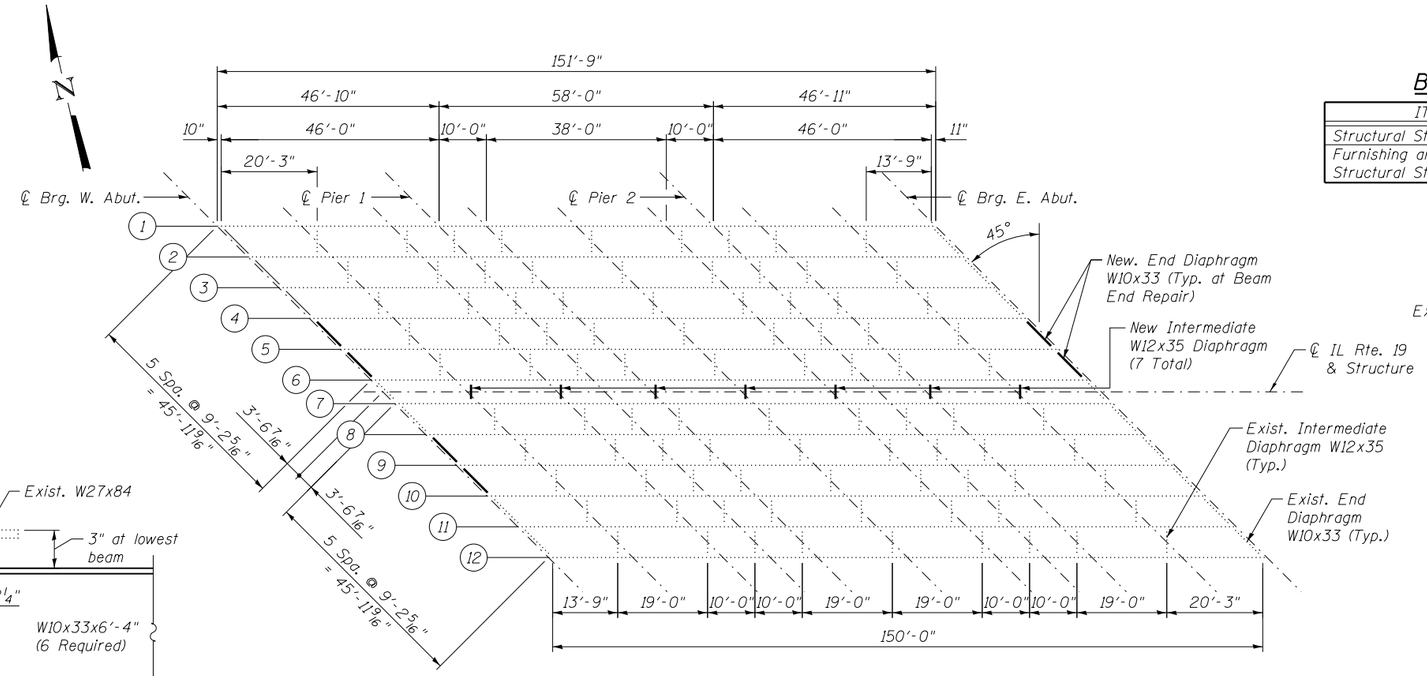
The diaphragms should be removed and replaced when the joint is being reconstructed and the existing deck adjacnt to the joint is removed. If the diaphragms are removed and replaced after the joint has been reconstructed, gaps between the deck and new diaphragms shall be injected with epoxy to ensure good fit.

Prior to ordering any material, the Contractor shall verify in the field all existing bolt hole dimensions in existing beam web and revise the location of holes in bottom flange as required.

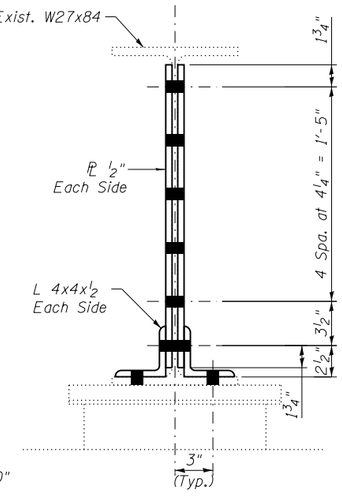
Conduits located throughout underside of deck. Contractor shall exercise extreme care with existing conduits to protect and support the conduits. Broken clips shall be replaced. Cost included in Furnishing and Erecting Structural Steel. The Contractor will be required to repair any damage done to the conduit to the satisfaction of the Engineer. No splicing will be allowed to any cable damage resulting from this work, instead the Contractor will be required to repair the entire span of any damaged cable at no additional cost to the Department.

BILL OF MATERIAL

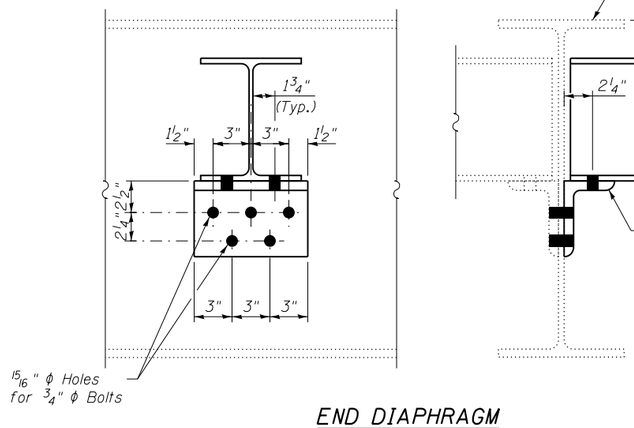
ITEM	UNIT	QUANTITY
Structural Steel Removal	Pound	1,470
Furnishing and Erecting Structural Steel	Pound	4,270



FRAMING PLAN



SECTION B-B



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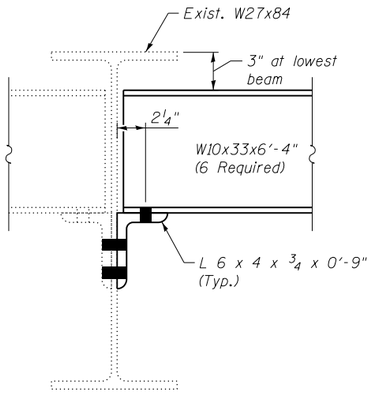

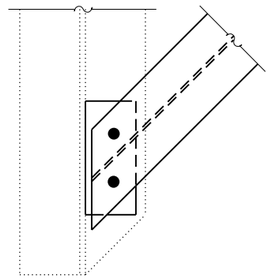

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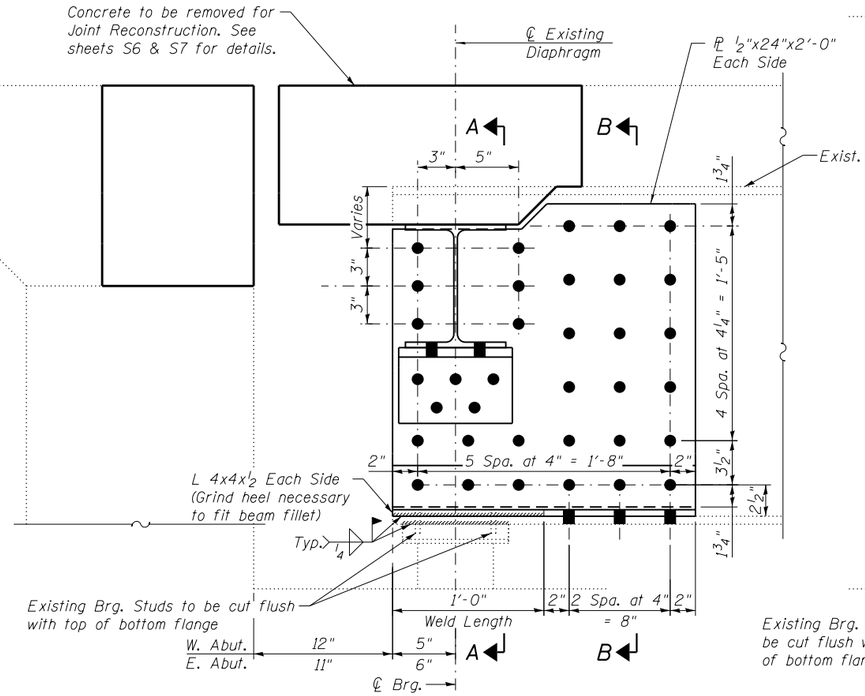
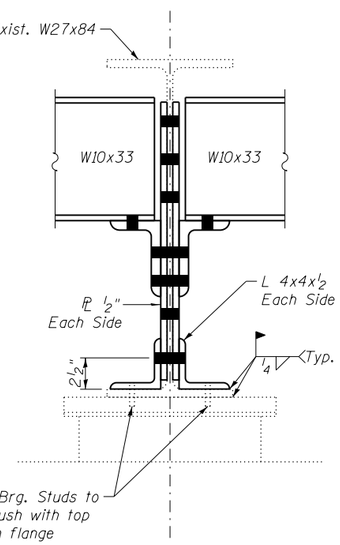
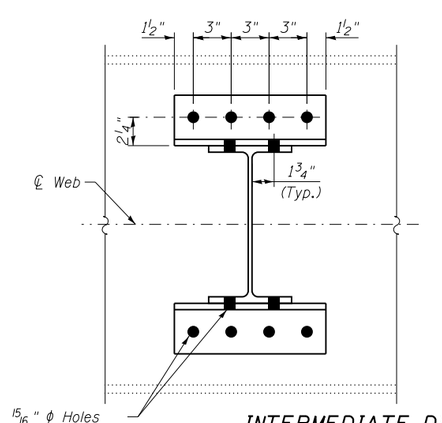


BEAM END REPAIR DETAIL

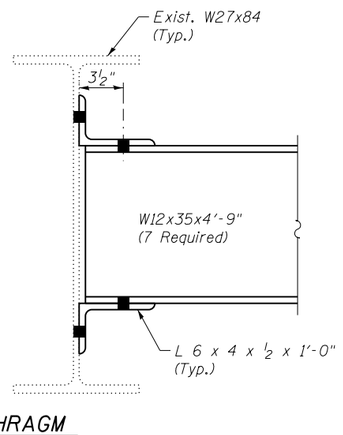
(W. Abut. shown, E. Abut. similar)
Beams 5 & 9 at W. Abut. & Beam 5 at the E. Abut.

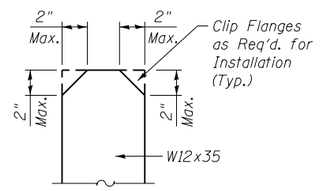


SECTION A-A



INTERMEDIATE DIAPHRAGM CONNECTION





DIAPHRAGM PLAN

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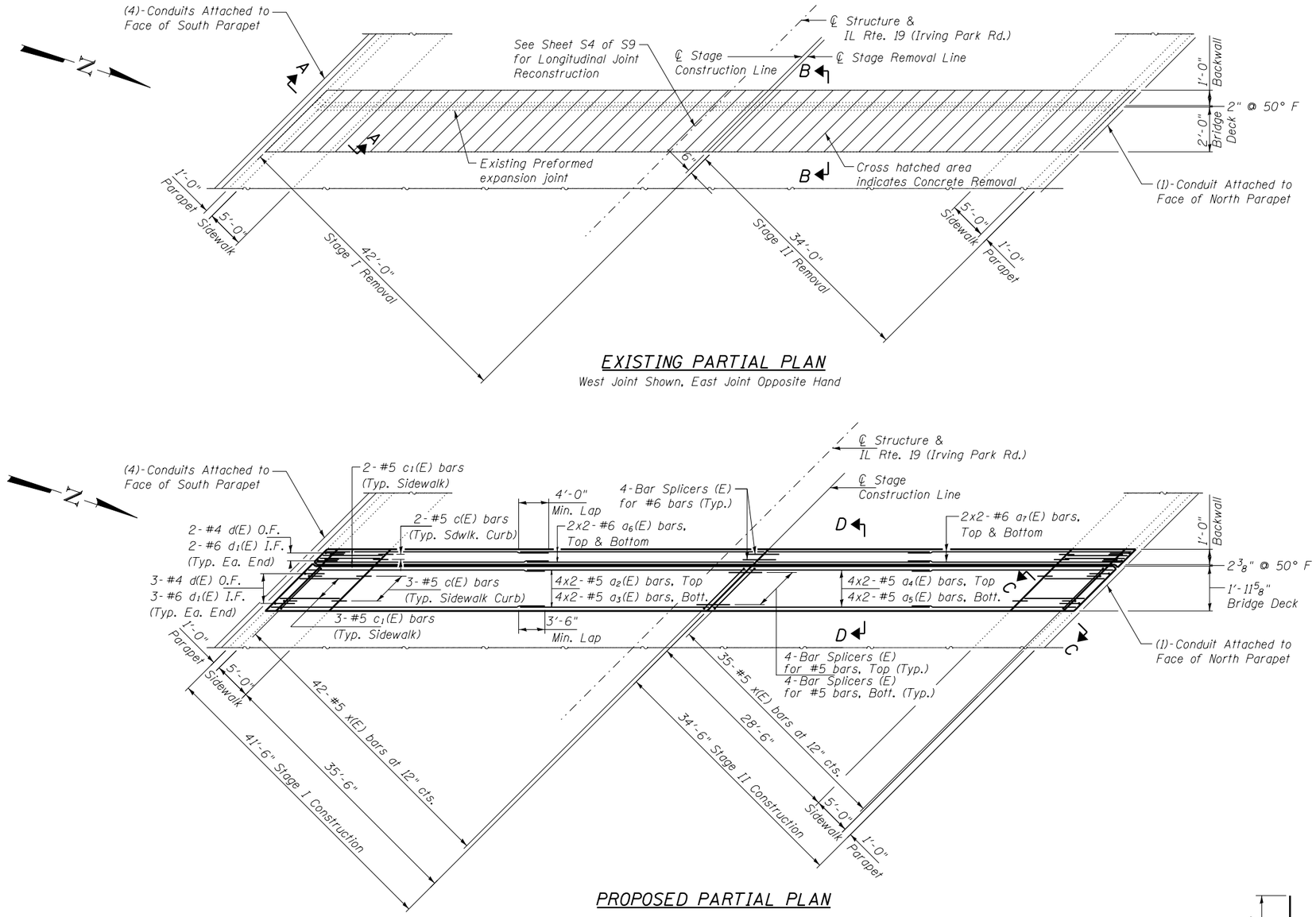
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FRAMING PLAN
STRUCTURE NO. 022-0147
SHEET NO. S5 OF S11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	13
		CONTRACT NO. 60M74		
	ILLINOIS FED. AID PROJECT			



MINIMUM BAR LAPS

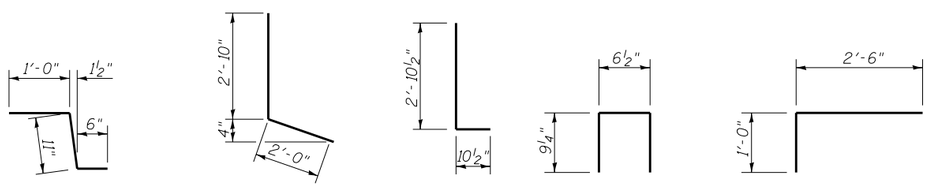
#5 Bars = 3'-6"
#6 Bars = 4'-0"

EAST AND WEST JOINT BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a_2(E)	16	#5	31'-0"	
a_3(E)	16	#5	30'-8"	
a_4(E)	16	#5	26'-0"	
a_5(E)	16	#5	25'-9"	
a_6(E)	16	#6	31'-3"	
a_7(E)	16	#6	26'-3"	
c(E)	20	#5	2'-5"	
c_1(E)	20	#5	8'-2"	
d(E)	20	#4	4'-10"	
d_1(E)	20	#6	3'-9"	
d_2(E)	16	#4	2'-1"	
x(E)	154	#5	3'-6"	
Concrete Removal			Cu. Yd.	31.3
Concrete Superstructures			Cu. Yd.	33.8
Reinforcement Bars, Epoxy Coated			Pound	4,260

Notes:

1. Work this sheet with Sheets S7-S9.
2. I.F. denotes Inside Face.
   O.F. denotes Outside Face.
3. Bars indicated thus 6x2-#5 a2(E) indicates 6 lines of bars with 2 lengths.



Bar c(E) | Bar d(E) | Bar d1(E) | Bar d2(E) | Bar x(E)

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT REPAIRS
STRUCTURE NO. 022-0147
SHEET NO. S6 OF S11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-1	DuPAGE	27	14
		CONTRACT NO. 60M74		
		ILLINOIS FED. AID PROJECT		

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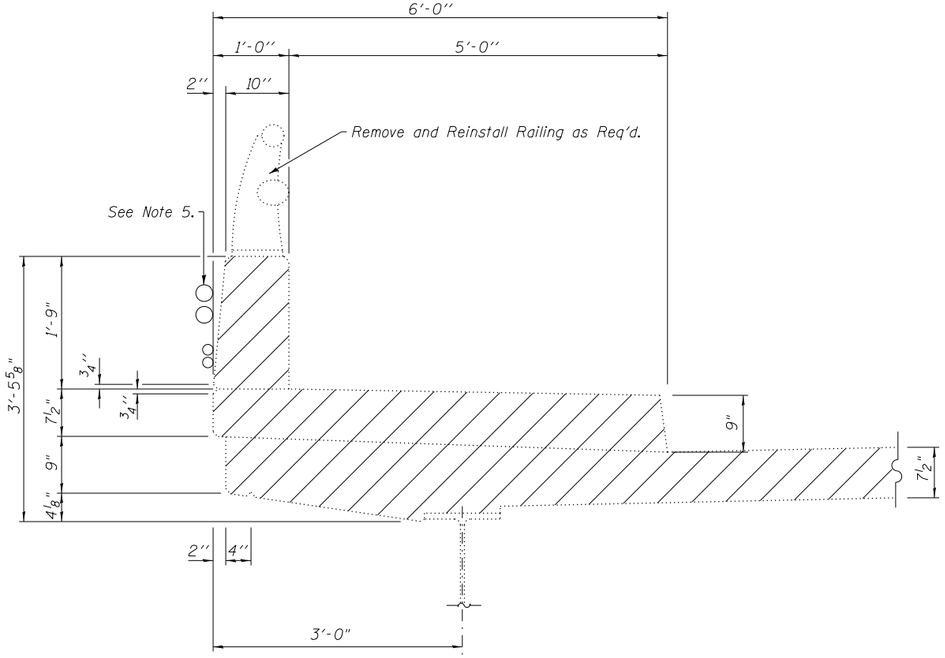
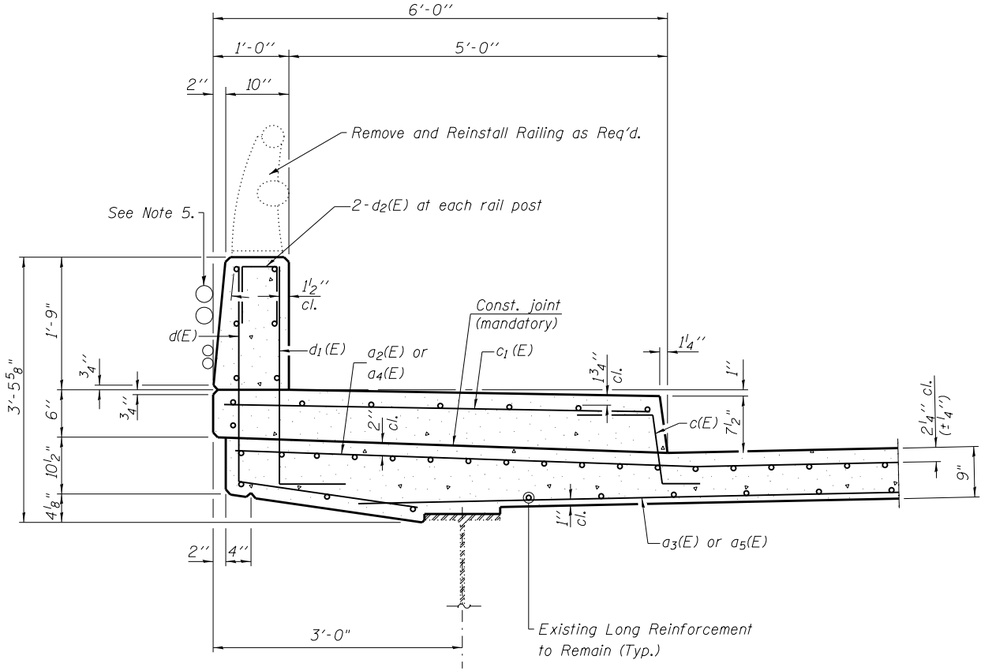
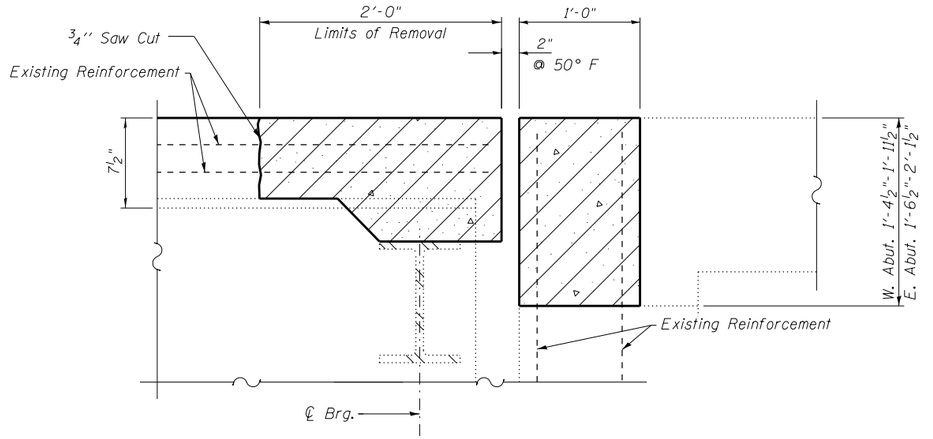


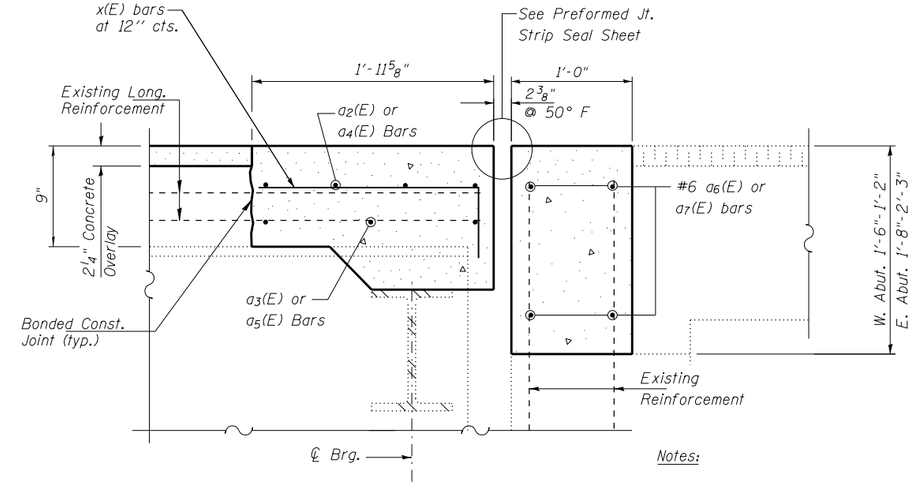
SECTION A-A



SECTION C-C



SECTION B-B



SECTION D-D

Notes:

1. *Existing reinforcement bars extending into the concrete removal area shall be cleaned straightened and incorporated into the new construction. Any reinforcement bars damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.*

2. *Existing reinforcement bars in the concrete removal area parallel to the expansion joints shall be removed.*

3. *Removal and disposal of the existing expansion joints will not be paid for separately, but shall be included with the cost of Concrete Removal.*

4. *The Contractor is responsible for coordinating and providing temporary support of the conduits attached to the parapet with the utility owners. All costs associated with the work shall be included in the pay item "Concrete Removal".*

5. *The contractor shall exercise extreme care with the existing conduits attached to the parapets. During repairs any damage done to the conduits shall be performed at the Contractor's cost to the satisfaction of the Engineer and utility owner.*

6. *Work this sheet with Sheets S6, S8 & S9.*

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STATE OF ILLINOIS
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EXPANSION JOINT DETAILS
STRUCTURE NO. 022-0147
SHEET NO. S7 OF S11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		CONTRACT NO. 60M74		
	ILLINOIS FED. AID PROJECT			

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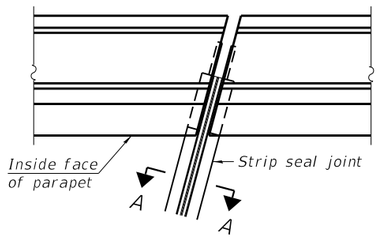
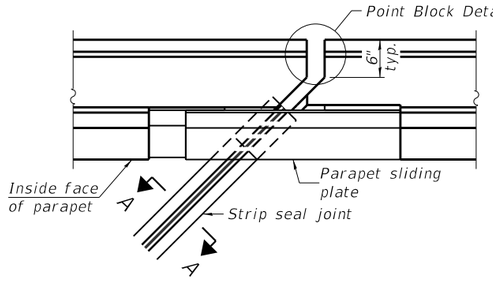
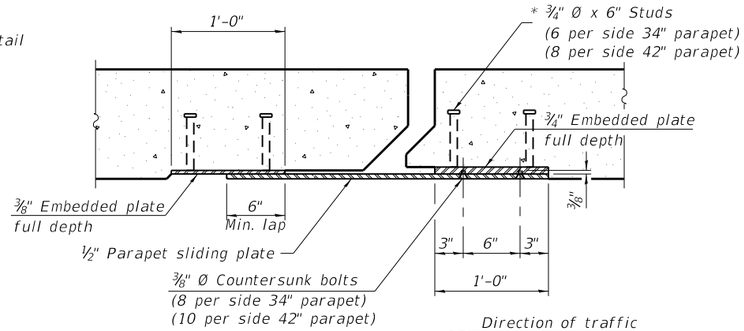


FOR SKEWS ≤ 30°

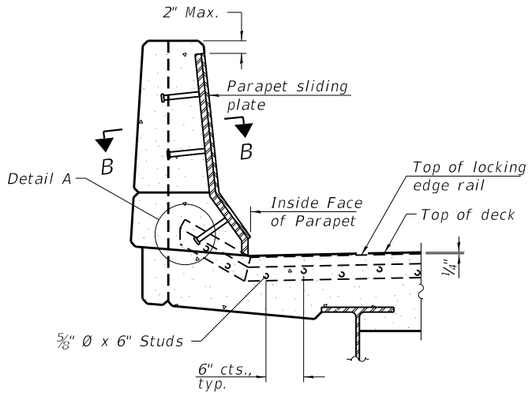


FOR SKEWS > 30°

PLAN AT PARAPET

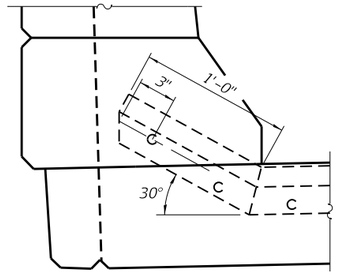


SECTION B-B

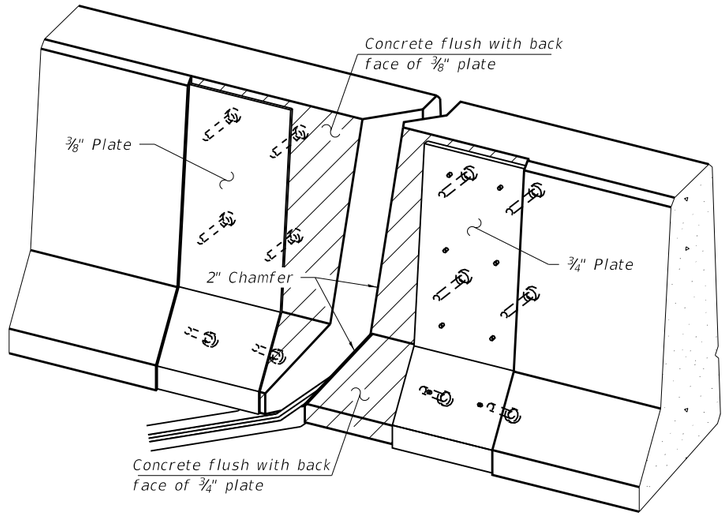


ELEVATION AT PARAPET

(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)



DETAIL A



TRIMETRIC VIEW

(Showing embedded plates only)

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of ¼". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4½" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

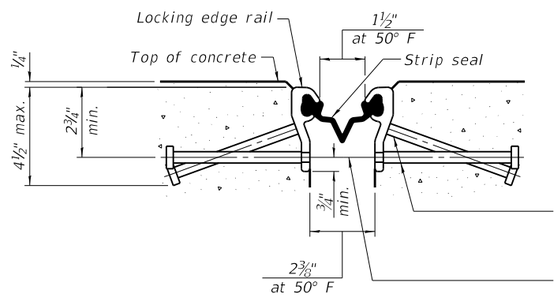
The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

The top surface of sidewalk sliding plates shall have a raised pattern according to ASTM A786.

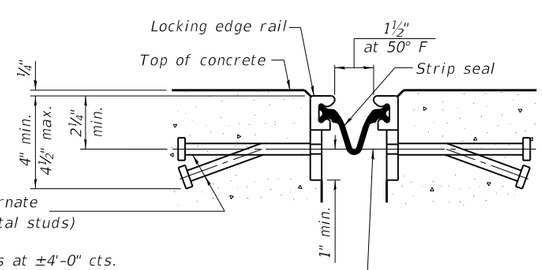
Cost of parapet sliding plates, sidewalk sliding plates, embedded plates, anchorage studs, and expansion anchors included with Preformed Joint Strip Seal.

34" F-shape barrier shown, 42" F-shape similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.



SHOWING ROLLED RAIL JOINT



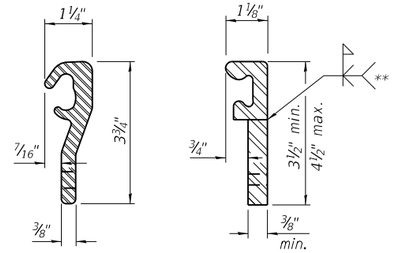
SHOWING WELDED RAIL JOINT

* ⅝" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

⅜" ϕ threaded rods in 7/16" ϕ holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

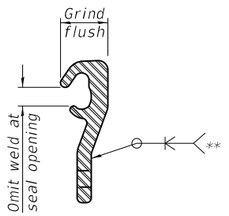


ROLLED (EXTRUDED) RAIL

WELDED RAIL

LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	215.0

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL – SIDEWALK
STRUCTURE NO. 022-0147
SHEET NO. S8 OF S11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		CONTRACT NO. 60M74		
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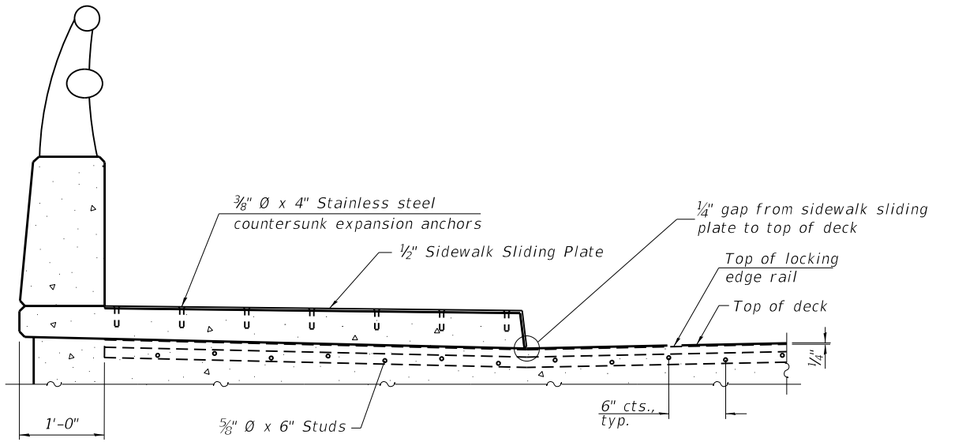
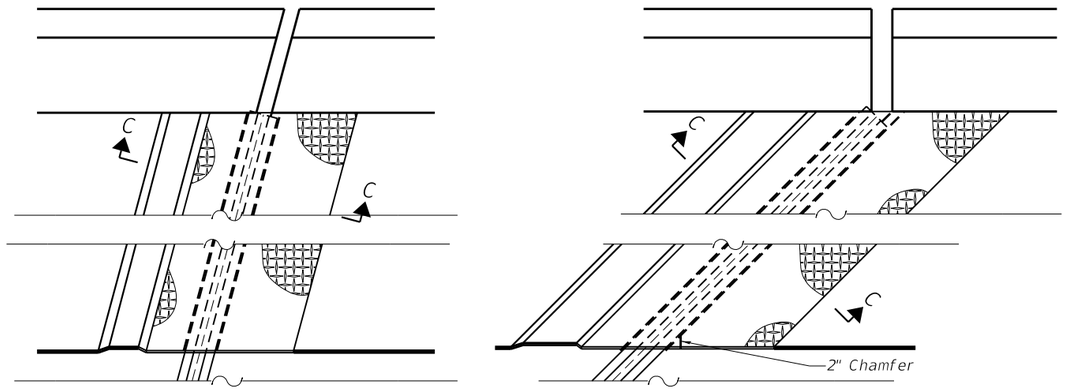


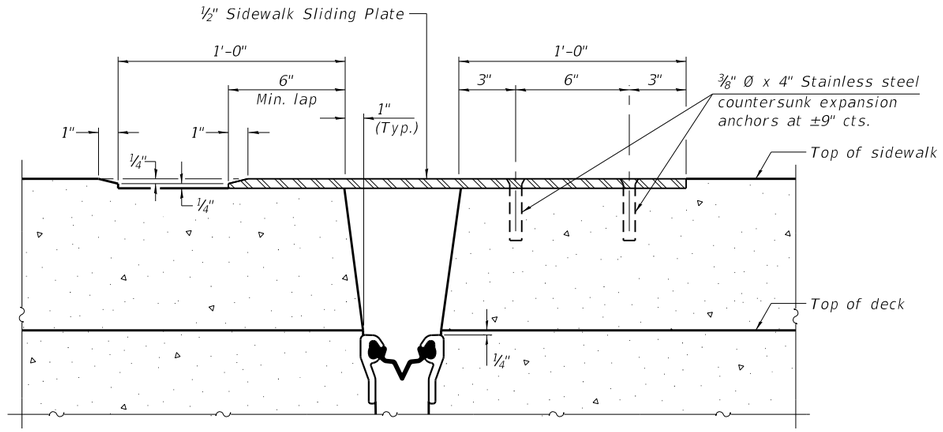
ELEVATION AT RAISED SIDEWALK



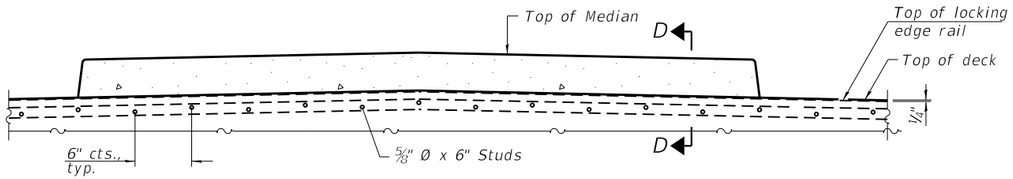
(FOR SKEWS ≤ 30°)

(FOR SKEWS > 30°)

PLAN AT RAISED SIDEWALK

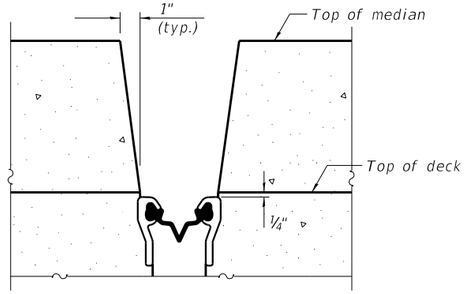


SECTION C-C



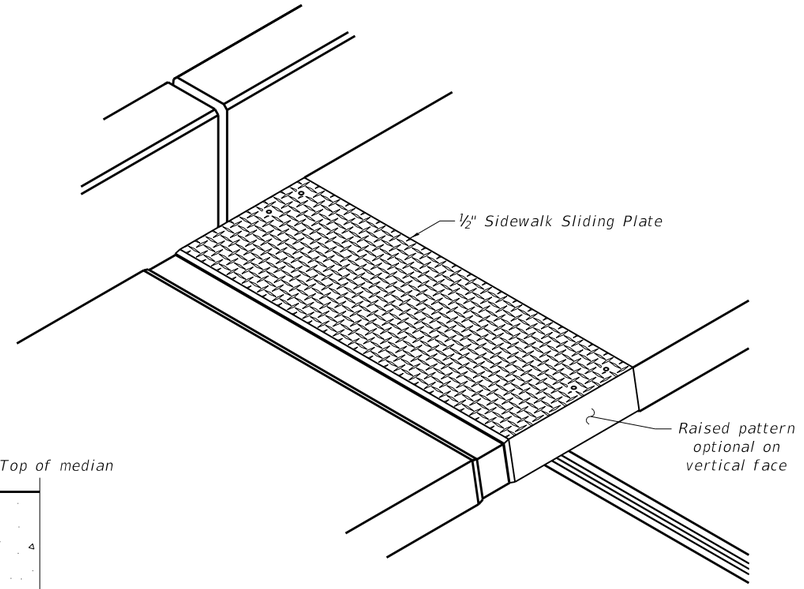
ELEVATION AT MEDIAN

For skews > 30°, chamfer acute corners 2" similar to sidewalk.



SECTION D-D

(at Rt. ∠'s)



TRIMETRIC VIEW

EJ-SS-S 8-11-17

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PREFORMED JOINT STRIP SEAL – SIDEWALK
STRUCTURE NO. 022-0147
SHEET NO. S9 OF S11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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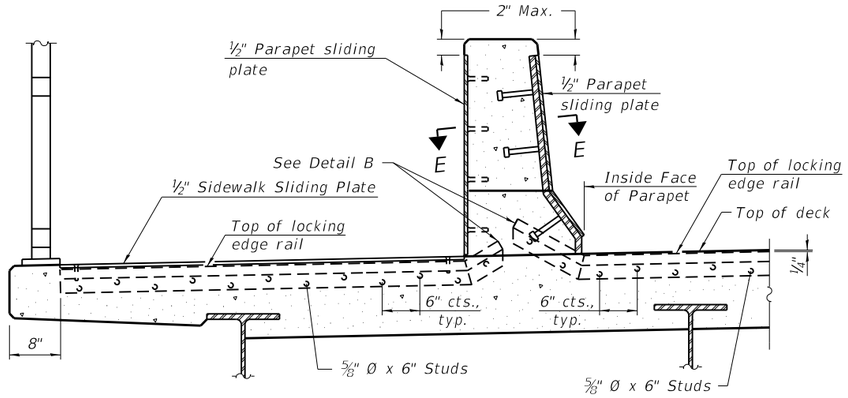
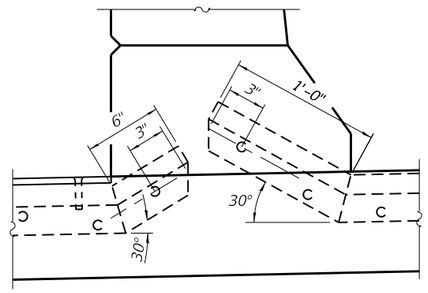
	ILLINOIS	FED. AID PROJECT		

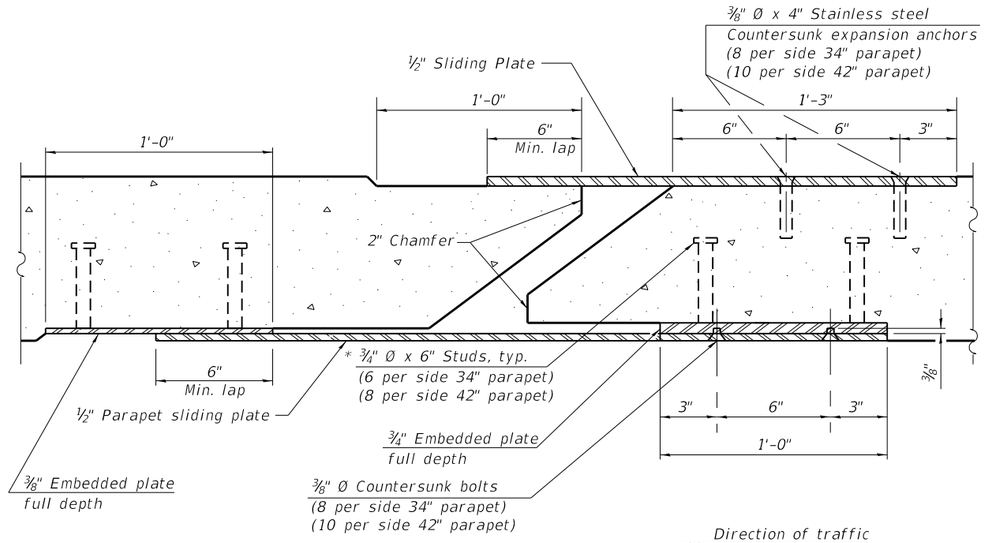


ELEVATION AT DECK LEVEL SIDEWALK

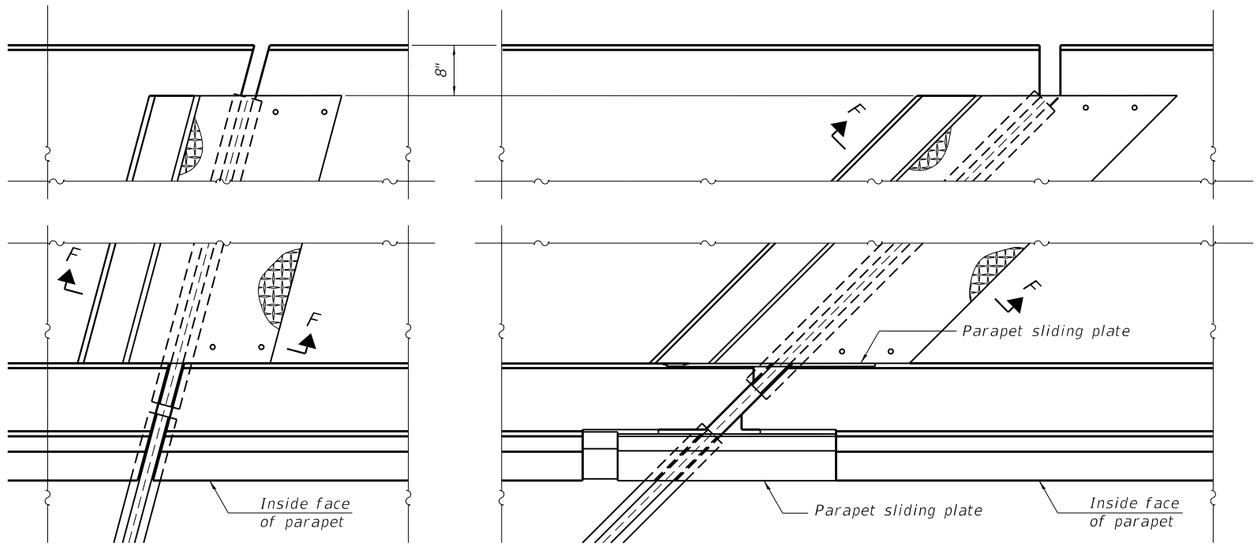
(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)



DETAIL B



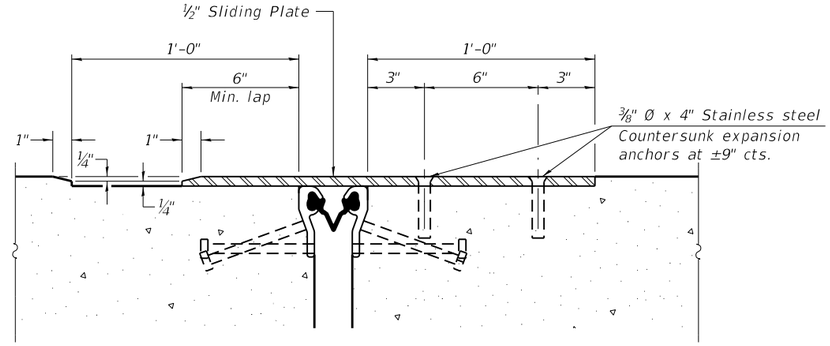
SECTION E-E



(FOR SKEWS ≤ 30°)

(FOR SKEWS > 30°)

PLAN AT DECK LEVEL SIDEWALK



SECTION F-F

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(Sheet 3 of 3)

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PREFORMED JOINT STRIP SEAL – SIDEWALK
STRUCTURE NO. 022-0147

SHEET NO. S10 OF S11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	ILLINOIS FED. AID PROJECT			

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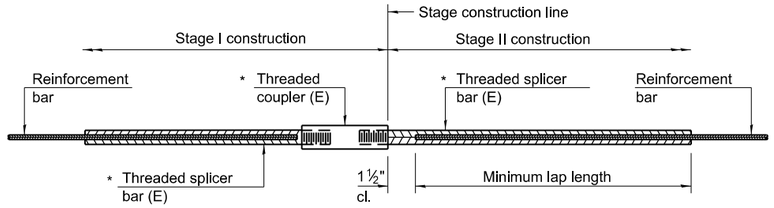
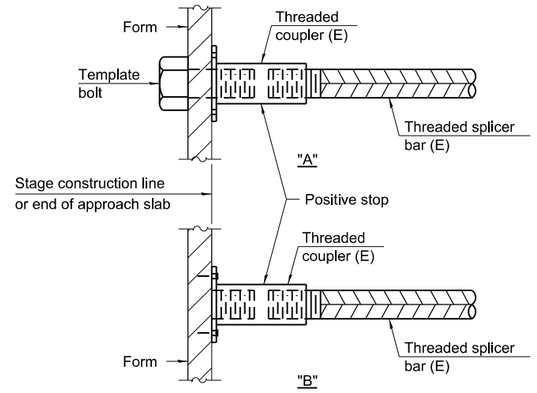


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1 ½" + thread length

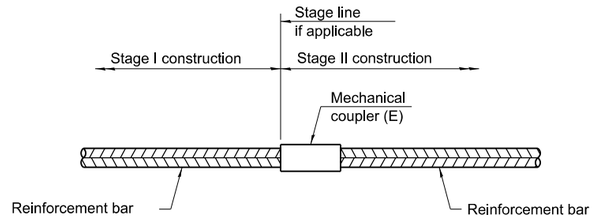
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Deck	#5	16	3'-6"
Backwall	#6	8	4'-0"



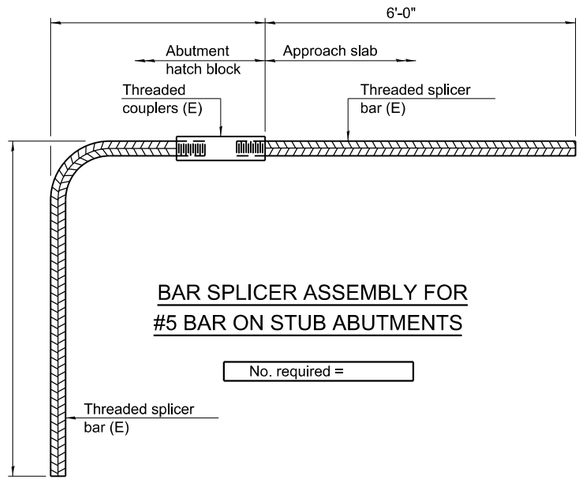
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

2-17-2017

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BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 022-0147
SHEET NO. S11 OF S11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		CONTRACT NO. 60M74		
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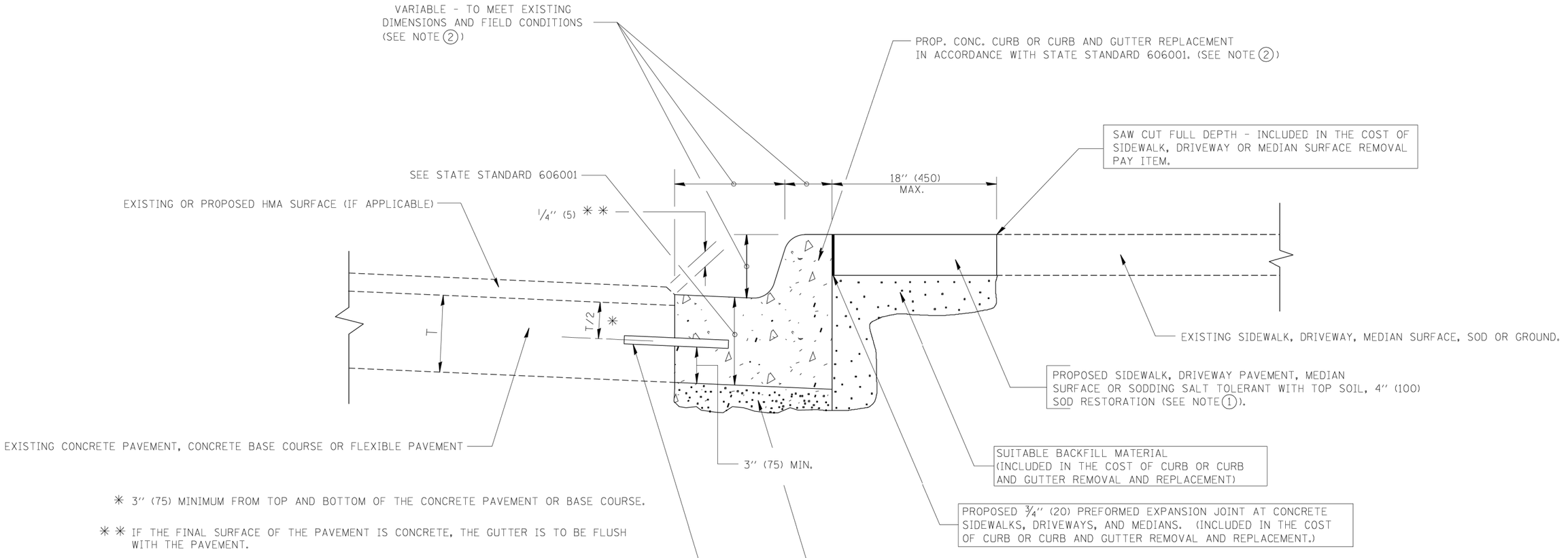

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

(2) FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

(3) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(4) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(5) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(7) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(8) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\pw_work\pwidot\drivakosgn\d0108315\bd24.dgn	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01
	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09

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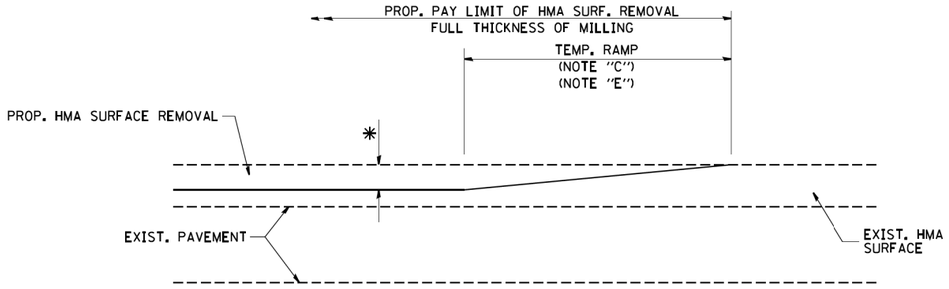
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

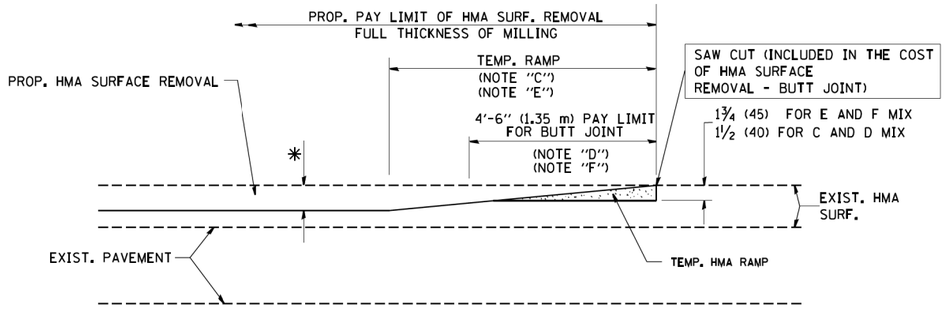
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	20
	BD600-06 (BD-24)	CONTRACT NO. 60M74		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

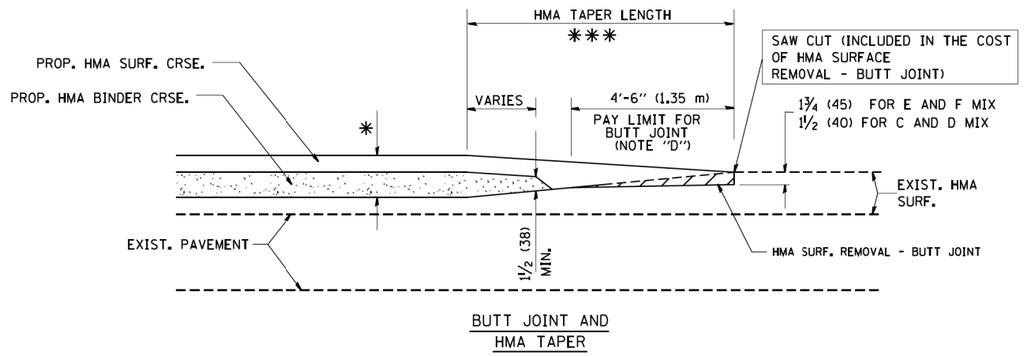


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

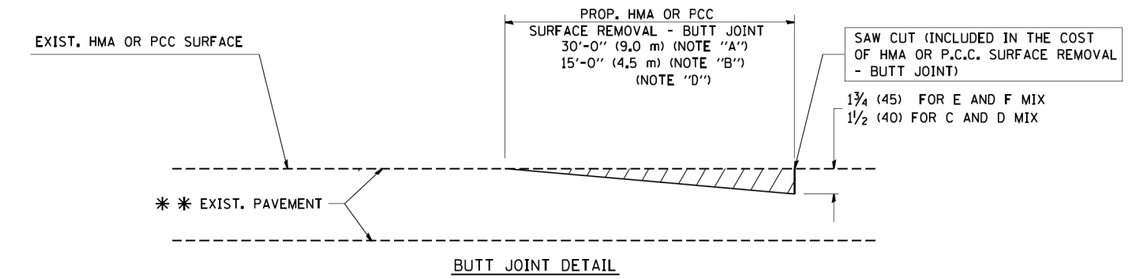
OPTION 2

TYPICAL TEMPORARY RAMP

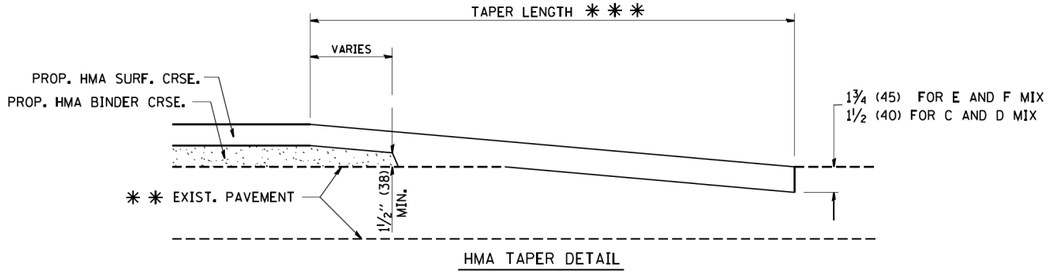


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

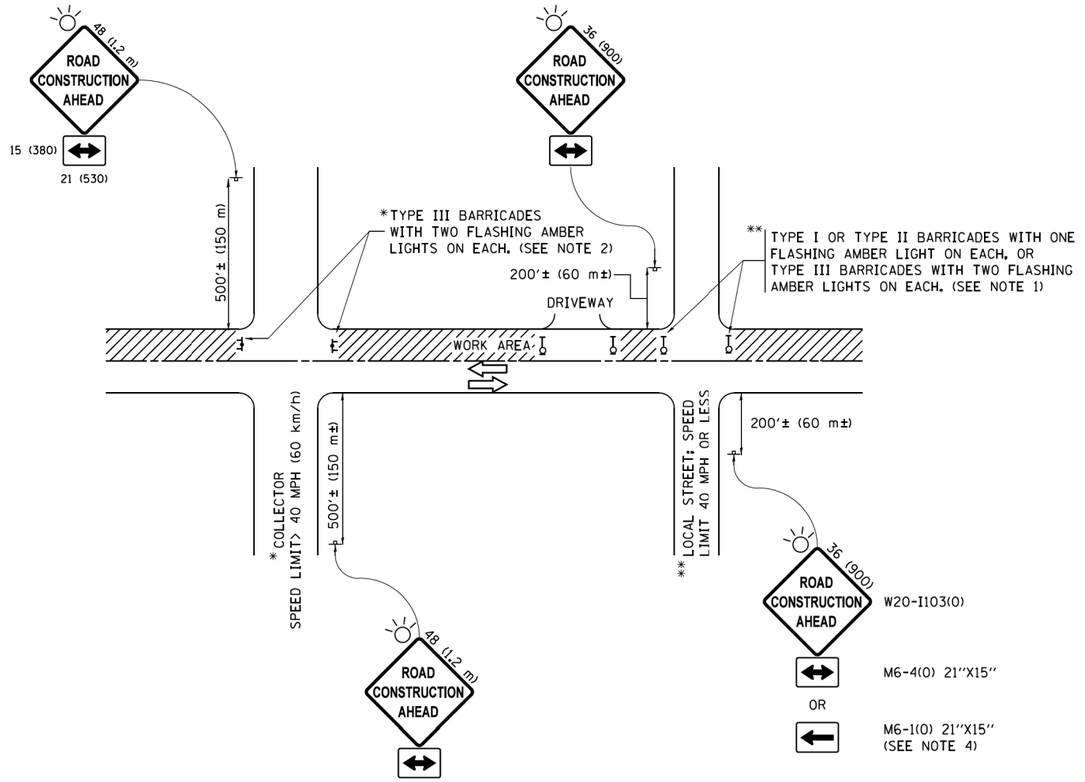
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - A. ABBAS 03-21-97			1321	32-B-I	DuPAGE	27	21
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01				BD400-05 BD32		CONTRACT NO. 60M74	
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE ''ROAD CONSTRUCTION AHEAD'' SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE ''ROAD CONSTRUCTION AHEAD'' SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\Dist\Default	USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
		DRAWN	REVISED -T. RAMMACHER 01-06-00
	PLOT SCALE = 50.000 ' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

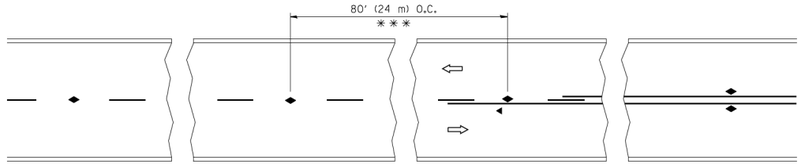
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

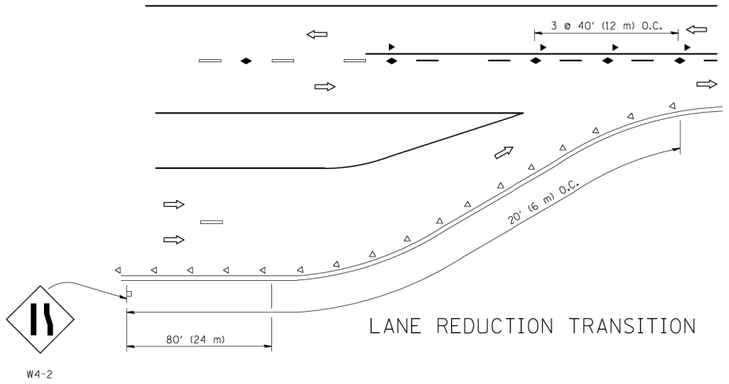
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	22
	TC-10	CONTRACT NO. 60M74		
	ILLINOIS FED. AID PROJECT			

tc10.dgn 9/15/2016 12:46:49 PM User=footemj

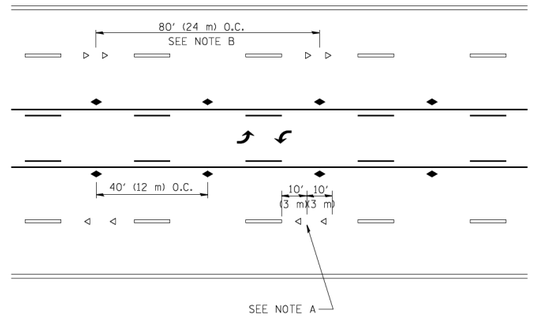


* * * REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

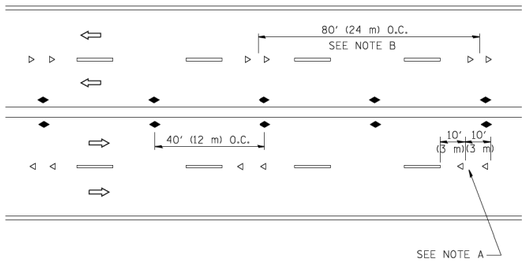
TWO-LANE/TWO-WAY



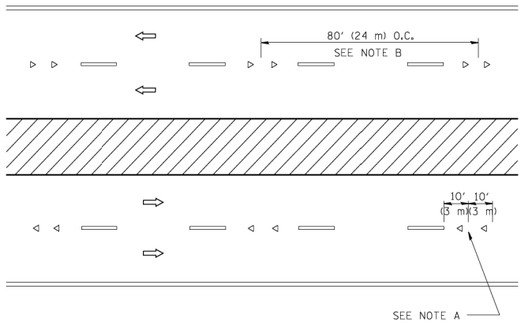
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

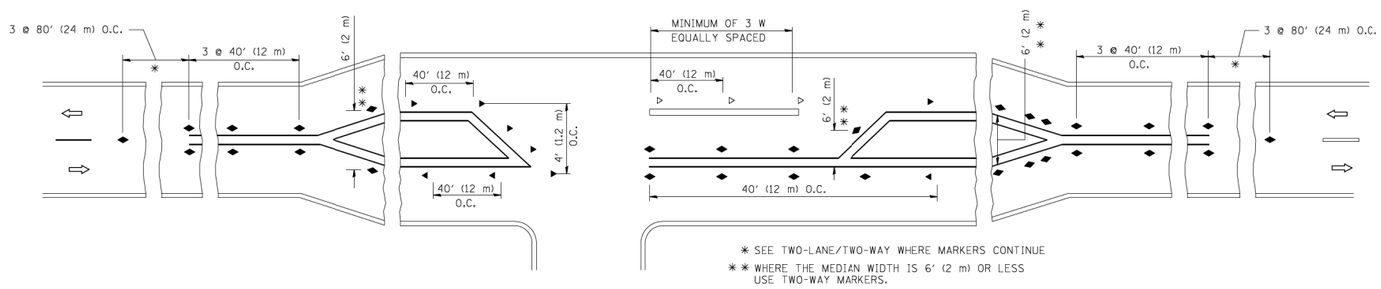
LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



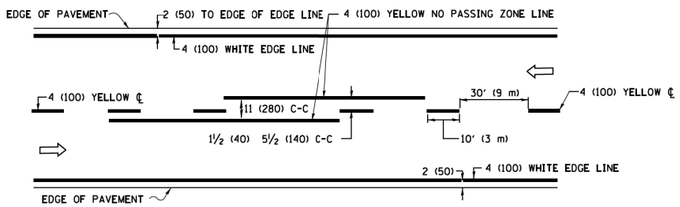
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

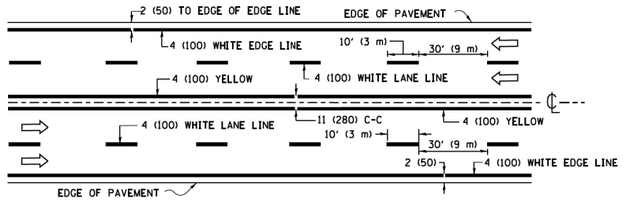
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

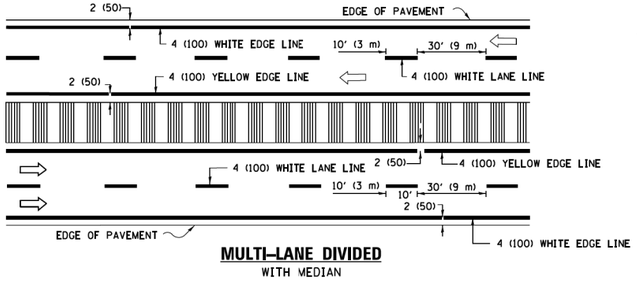
FILE NAME = c:\pw_work\pwidot\drivakosgn\d0108315\tc11.dgn	USER NAME = drivakosgn	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.000 ' / IN.	DRAWN -	REVISED - T. RAMMACHER 03-12-99			1321	32-B-1	DuPAGE	27	23
	PLOT DATE = 9/9/2009	CHECKED -	REVISED - T. RAMMACHER 01-06-00				TC-11	CONTRACT NO. 60M74		
		DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



2–LANE ROADWAY

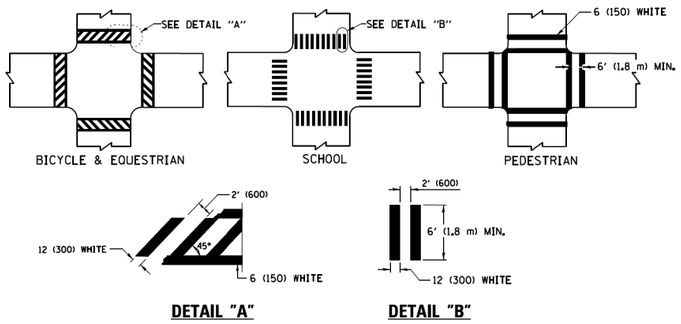


MULTI–LANE UNDIVIDED



MULTI–LANE DIVIDED
WITH MEDIAN

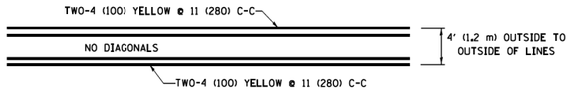
TYPICAL LANE AND EDGE LINE MARKING



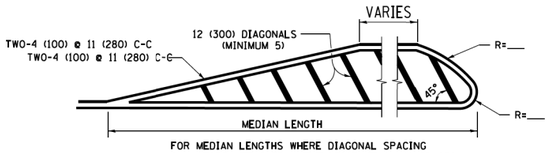
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

✳ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



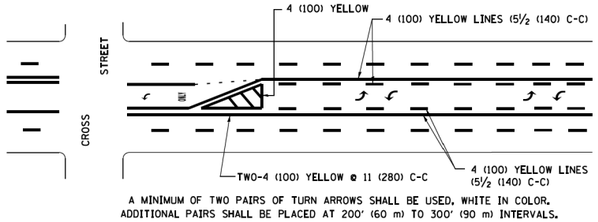
4' (1.2 m) WIDE MEDIANS ONLY



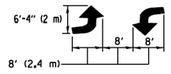
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

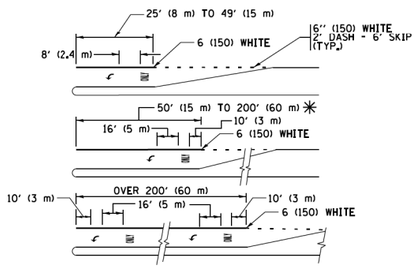


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

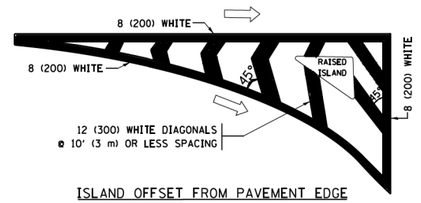


FULL SIZE LETTERS 8' (2,4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1,5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1,9 m^2)

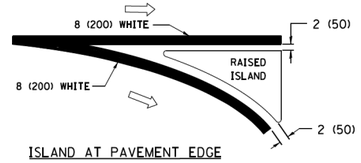
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

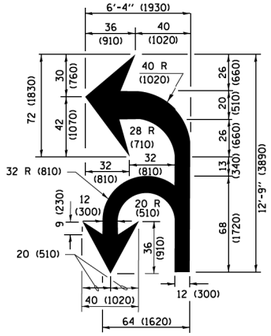


ISLAND OFFSET FROM PAVEMENT EDGE

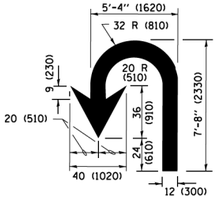


ISLAND AT PAVEMENT EDGE

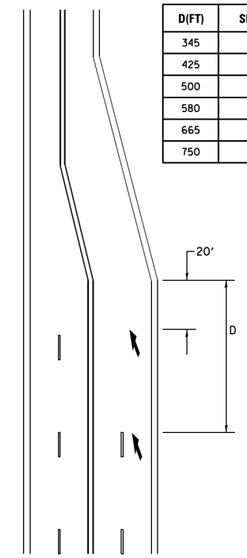
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U–TURN



U–TURN



LANE REDUCTION TRANSITION

✳ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1,8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2,4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2,4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1,8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1,2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1,2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4,5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3,6 SQ. FT. (0,33 m^2) EACH "X"=54,0 SQ. FT. (5,0 m^2)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16,3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30,4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\tcl3.dgn	USER NAME = leysa	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
		DRAWN -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 50.000 ' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 6/23/2017	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	24
	TC-13	CONTRACT NO. 60M74		
	ILLINOIS FED. AID PROJECT			

tcl1.dgn 6/23/2017 9:41:34 AM User=leysa

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

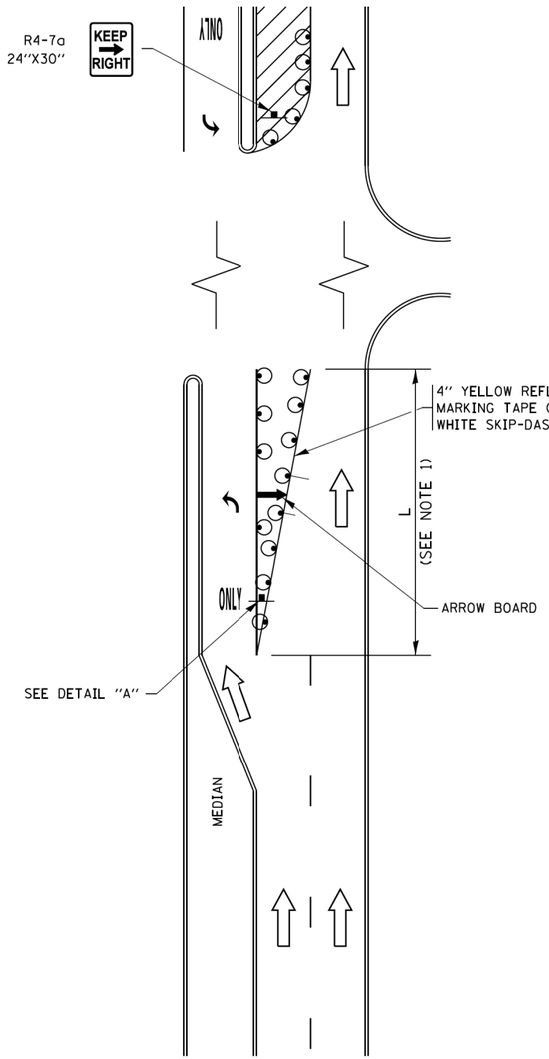


FIGURE 1

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.

   B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.

2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.

3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.

4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.

5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I10OR 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.

6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.

7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.

8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

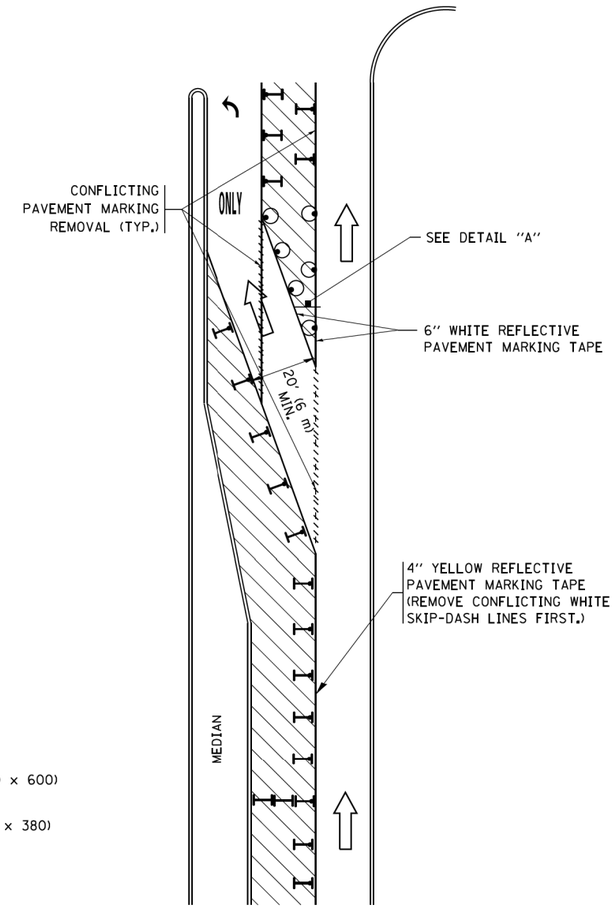
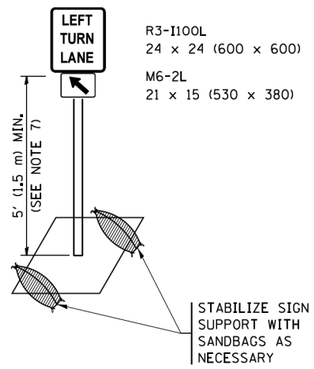


FIGURE 2



DETAIL A

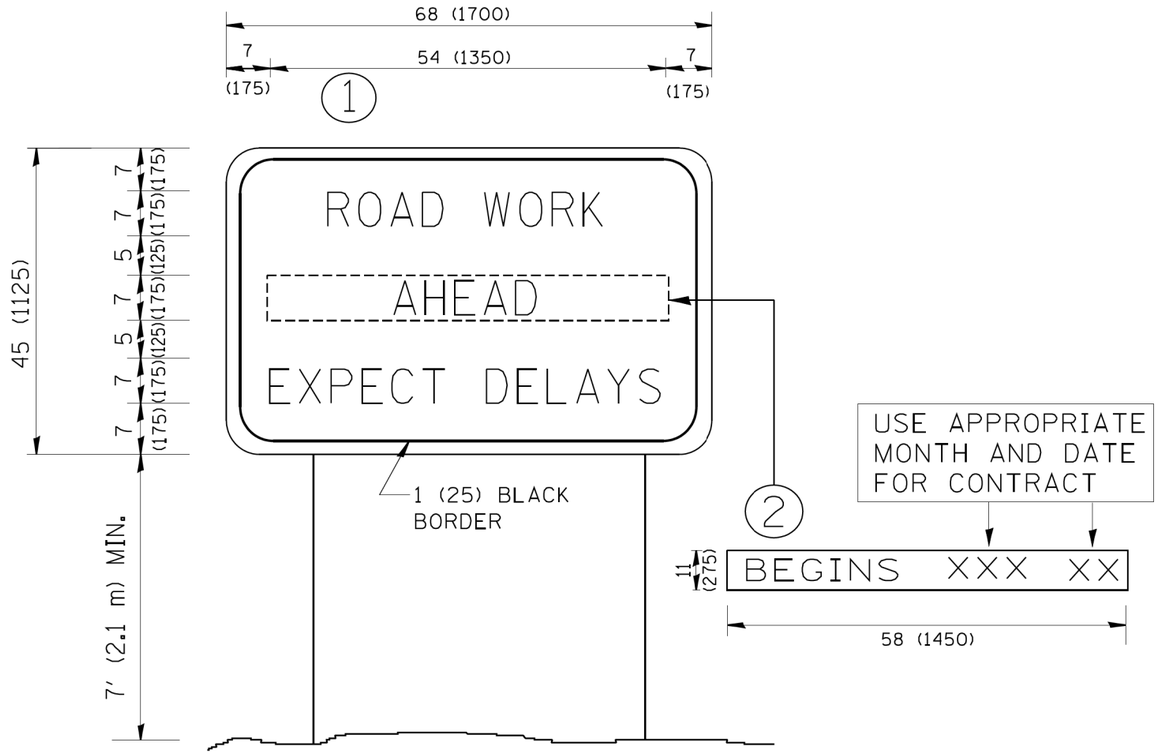
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\Dist	USER NAME = footemj	REVISED -T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
	PLOT SCALE = 50.0000 ' / in.	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13		
Default	PLOT DATE = 9/15/2016	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		
		REVISED -T. RAMMACHER 01-06-00	REVISED -		

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	25
	TC-14	CONTRACT NO. 60M74		
	ILLINOIS FED. AID PROJECT			

Last date: 9/15/2016 12:48:34 PM ShawnBoswell



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

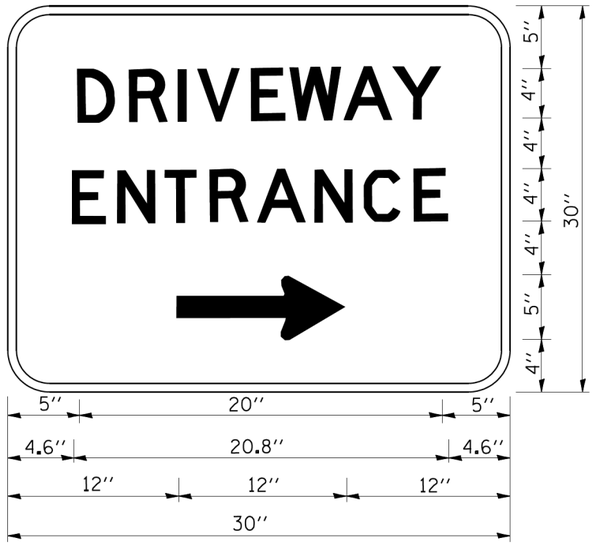
FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	26
	TC-22	CONTRACT NO. 60M74		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



3.0'' RADIUS, 0.5'' BORDER, WHITE ON GREEN; REFLECTORIZED
''DRIVEWAY'' D; ''ENTRANCE'' D; STANDARD ARROW CUSTOM 12.0'' x 5.0''

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM ''TEMPORARY INFORMATION SIGNING''.

FILE NAME = W:\diststd\22x34\tc26.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
		DRAWN -	REVISED -
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-B-I	DuPAGE	27	27
	TC-26	CONTRACT NO. 60M74		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		