

LETTING ITEM NO. 18A
LETTING DATE: JUNE 15, 2012

CONSTRUCTION PLANS

FOR

PE094
TOTAL SHEETS: 34

GENERAL WAYNE A. DOWNING PEORIA INTERNATIONAL AIRPORT

A.I.P. PROJECT NO.: 3-17-0080-XX
ILLINOIS PROJECT NO.: PIA-4180
WIDEN AIR CARRIER APRON

Sheet Number	Sheet Title
01	COVER SHEET
02	SITE PLAN
03	CONSTRUCTION SAFETY PHASING PLAN NOTES
04	CONSTRUCTION ACTIVITY PLAN 1
05	CONSTRUCTION ACTIVITY PLAN 2
06	CONSTRUCTION ACTIVITY PLAN 3
07	CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS
08	EXISTING CONDITIONS AND REMOVALS
09	PROPOSED IMPROVEMENTS
10	TYPICAL SECTIONS
11	STAKING PLAN
12	JOINING PLAN
13	JOINING DETAILS
14	DRAINAGE PLAN
15	DRAINAGE DETAILS 1
16	DRAINAGE DETAILS 2
17	APRON MARKING REMOVAL PLAN
18	PROPOSED APRON MARKING
19	MARKING LAYOUT POINTS
20	MARKING DETAILS
21	ELECTRICAL PLAN
22	ELECTRICAL DETAILS SHEET 1
23	ELECTRICAL DETAILS SHEET 2
24	ELECTRICAL DETAILS SHEET 3
25	TURNING AND EROSION CONTROL PLAN
26	EROSION CONTROL DETAILS
27	GRADING PLAN 1
28	GRADING PLAN 2
29	APRON EXPANSION CROSS SECTIONS 1
30	APRON EXPANSION CROSS SECTIONS 2
31	APRON GRADING CROSS SECTIONS 1
32	APRON GRADING CROSS SECTIONS 2
33	HAUL ROAD CROSS SECTIONS 1
34	HAUL ROAD CROSS SECTIONS 2

MAXIMUM EQUIPMENT HEIGHT = 25'
GROUND FREQUENCY 121.85

GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

TOWNSHIP: 8 NORTH
RANGE: 7 EAST
COUNTY: PEORIA
TOWNSHIP: LIMESTONE

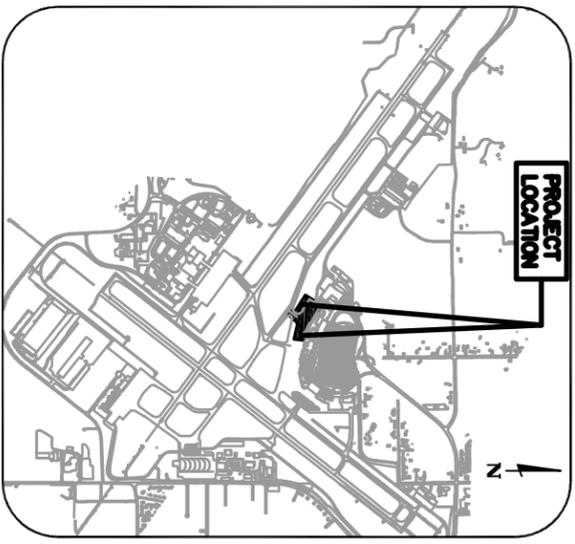
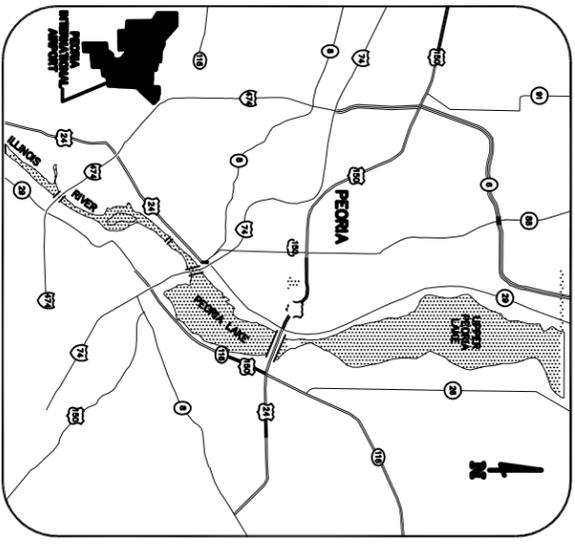


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 www.call811.com or
 Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL, OR EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH UTILITIES PRIOR TO CONSTRUCTION. CONTRACTORS UNDERGOING THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT OF FACILITIES. CONTRACTORS SHALL BE RESPONSIBLE FOR OBTAINING IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED, ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.



MAY 25, 2012

SUMMARY OF QUANTITIES		
ITEM NO.	Description	UNIT QUANTITY
AR108108	1/1" #8 SKV UG CABLE	LF 1,138
AR108706	1/1" #8 COUNTERPOISE	LF 475
AR110215	2" PVC CONDUIT, DIRECT BURY	LF 535
AR110946	ADJUST ELECTRICAL HANDHOLE	EA 2
AR125962	RELOCATE BASE MOUNTED LIGHT	EA 10
AR125964	RELOCATE TAXI GUIDANCE SIGN	EA 2
AR150520	MOBILIZATION	LS 1
AR152455	EMBANKMENT IN PLACE	CY 9,290
AR156500	TEMPORARY EROSION CONTROL	LS 1
AR156540	RIPRAP	SY 139
AR208540	OVERSIZE AGGREGATE	TON 1,822
AR209608	CRUSHED AGG. BASE COURSE - 8"	SY 2,791
AR501520	20" PCC PAVEMENT	SY 2,765
AR501530	PCC TEST BATCH	EA 1
AR501900	REMOVE PCC PAVEMENT	SY 244
AR620510	PAVEMENT MARKING	SF 7,947
AR620585	TEMPORARY MARKING & REMOVAL	SF 254
AR620900	PAVEMENT MARKING REMOVAL	SF 8,439
AR701524	24" RCP, CLASS IV	LF 72
AR701536	36" RCP, CLASS IV	LF 72
AR705506	6" PERFORATED UNDERDRAIN	LF 473
AR751415	INLET - SPECIAL	EA 1
AR751940	ADJUST INLET	EA 2
AR752436	PRECAST REINFORCED CONC. FES 36"	EA 1
AR752960	RELOCATE END SECTION	EA 1
AR901510	SEEDING	AC 3
AR908520	EXCELISIOR BLANKET	SY 12,316

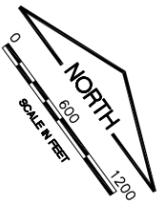


GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

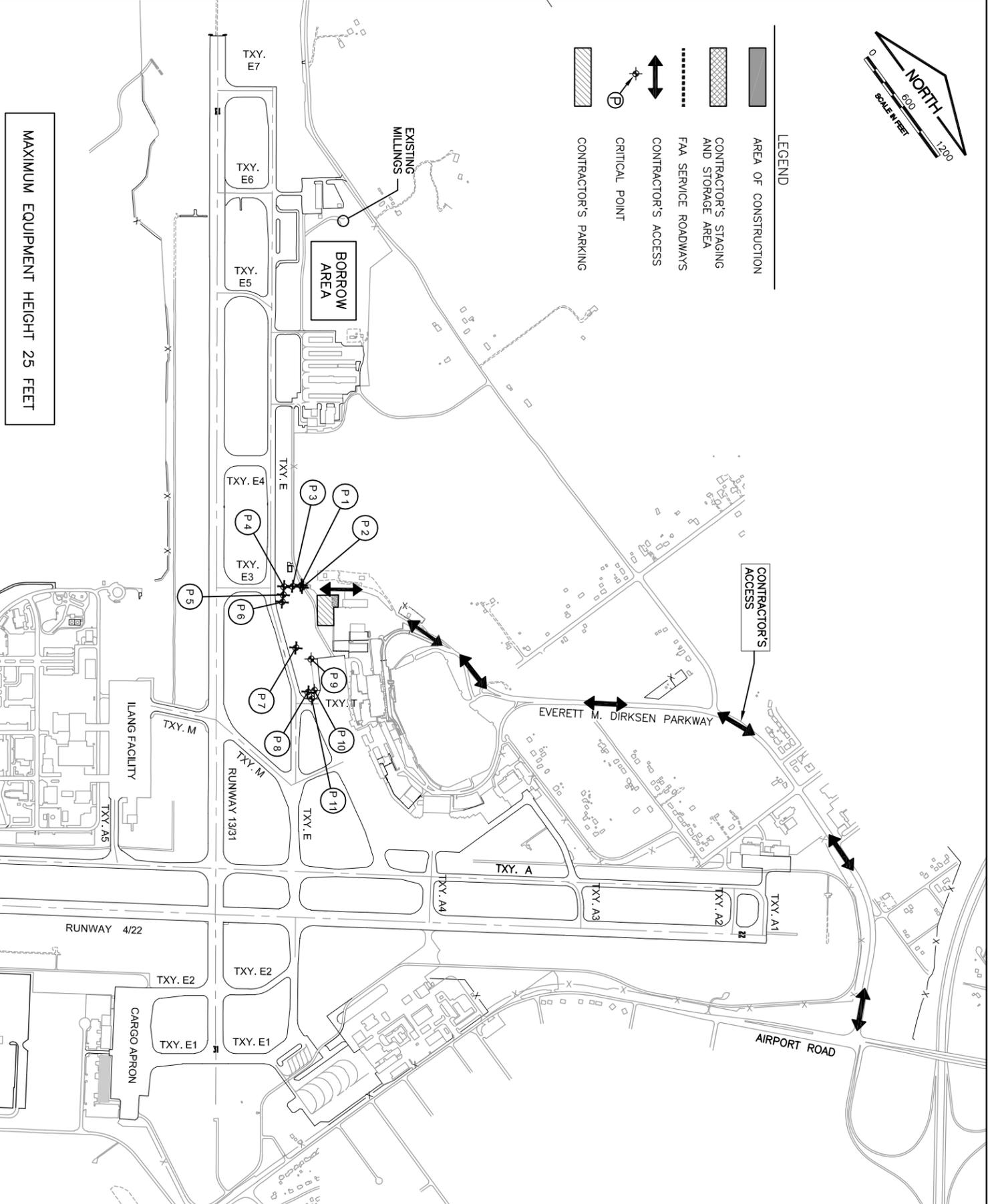
APPROVED: *Charles E. Taylor*
 DATE: 11/09/22/2012

GMT CONSULTING ENGINEERS
 License No. 04-000615

SUBMITTED BY: *W. S. Jr.*
 DATE: 5/22/12
 GMA#T JOB NUMBER 10061-04-00



- LEGEND**
- AREA OF CONSTRUCTION
 - CONTRACTOR'S STAGING AND STORAGE AREA
 - FAA SERVICE ROADWAYS
 - CONTRACTOR'S ACCESS
 - CRITICAL POINT
 - CONTRACTOR'S PARKING



MAXIMUM EQUIPMENT HEIGHT 25 FEET

POINT	NORTHING	EASTING	ELEV.	DESCRIPTION
P1	1457094.047	2427023.053	627.83'	EDGE OF TEMP. RD.
P2	1457093.315	2427047.299	628.54'	EDGE OF TEMP. RD.
P3	1457021.658	2426982.482	625.78'	C/L OF TEMP. RD.
P4	1456948.926	2426935.013	629.34'	EDGE OF TEMP. RD.
P5	1456898.122	2426996.539	632.30'	C/L OF TEMP. RD.
P6	1456849.018	2427047.733	634.22'	EDGE OF TEMP. RD.
P7	1456690.725	2427490.404	639.15'	C/L OF TEMP. RD.
P8	1456530.701	2427911.263	643.00'	END OF TEMP. RD.
P9	1456746.467	2427666.230	645.83'	EDGE OF PAVEMENT
P10	1456590.755	2427933.770	646.32'	EDGE OF PAVEMENT
P11	1456521.173	2427986.628	646.45'	EDGE OF PAVEMENT

CRITICAL POINTS

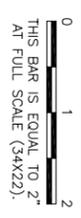
GENERAL NOTES

1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF AN ESCORT OR A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED MAAP/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
3. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT OPERATIONS DIRECTOR.
4. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
5. EXCESS EARTH, BROKEN ASPHALT AND CONCRETE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
6. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 107' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
7. ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
8. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
9. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
10. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
11. SEE CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS SHEET FOR NOTES REGARDING THE CONTRACTOR'S ACCESS.
12. CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE LIGHTED, MARKED, AND FLAGGED PER SECTION 50-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
13. COST OF TEMPORARY EDGE LIGHTING AND CABLING IN ORDER TO MAINTAIN AIRFIELD CIRCUITS WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

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 UPDATE BY: Andrew Bodine
 PLOT DATE: 5/24/2012 1:12 PM
 Plot-Base_Updated
 BASE_Plot-50E0

PE094

REVISIONS	NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**WIDEN AIR CARRIER APRON
 SITE PLAN**

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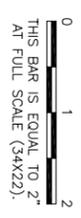
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**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT**

DESIGN BY:	CET/AJB
DRAWN BY:	CMT
CHECKED BY:	CET
APPROVED BY:	CET
DATE:	MAY 25, 2012
JOB No.:	10061-04-00
IL PROJ. NO. PA-4180	
APP PROJ. NO. 3-17-0080-XX	
SHEET	02 OF 34 SHEETS

PE094

REVISIONS	NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

WIDEN AIR CARRIER APRON

CONSTRUCTION SAFETY PHASING PLAN NOTES



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 GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

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IL PROJ. NO. PA-4180	
APP. PROJ. NO. 3-17-0080-XX	
SHEET	03 OF 34 SHEETS

16. PROTECTION

- ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY SAFETY AREA, WHICH EXTENDS 107' FROM THE TAXIWAY CENTERLINE WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

17. OTHER LIMITATIONS ON CONSTRUCTION

- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
 - THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
 - FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
 - IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
 - CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.
- | | |
|--------------------------------------|----------------|
| AIRPORT OPERATOR | (309) 303-1005 |
| GREG HUSER - MAINTENANCE SUPERVISOR | (309) 303-1005 |
| AIRPORT MAINTENANCE | (309) 303-1005 |
| GREG HUSER - MAINTENANCE SUPERVISOR | (309) 303-1005 |
| ENGINEER | (217) 787-8050 |
| CHUCK TAYLOR P.E. - PROJECT ENGINEER | (217) 787-8050 |
| CMT - RESIDENT ENGINEER | (217) 787-8050 |

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL ATTEND A FINAL INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL JULLIE, AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORTS APPROVED CSPP AND THE CONTRACTORS APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- NO RUNWAY OR TAXIWAY CLOSURES ARE REQUIRED FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2F.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

15. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HESHER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5270-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET ON AS DIRECTED BY THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN.
- CERTAIN CONTRACTOR EMPLOYEES SHALL OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS, ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
- ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATIONS AREA (AOA) SHALL ALSO BE RESPONSIBLE FOR THE AIRPORT DRIVER TRAINING PROGRAM. THESE AIRFIELD DRIVERS WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR VEHICLES OR EQUIPMENT ON THE AIRPORT, ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
- DRIVERS OF TRUCKS CONTAINING MATERIAL, DEBRIS, OR AGGREGATE, CONCRETE, ETC., NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVERS LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE, WHILE INSIDE THE AOA. THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR.
- CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS.
- THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT ENVIROTRATE SURFACES DEFINED BY FAR, TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. SEE SECTION 44 ON THE SITE PLAN SHEET.
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOD AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL, ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT RESCUE AND FIRE LIGHTING (ARFF) FACILITY, IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORTS APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.

- ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.

- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

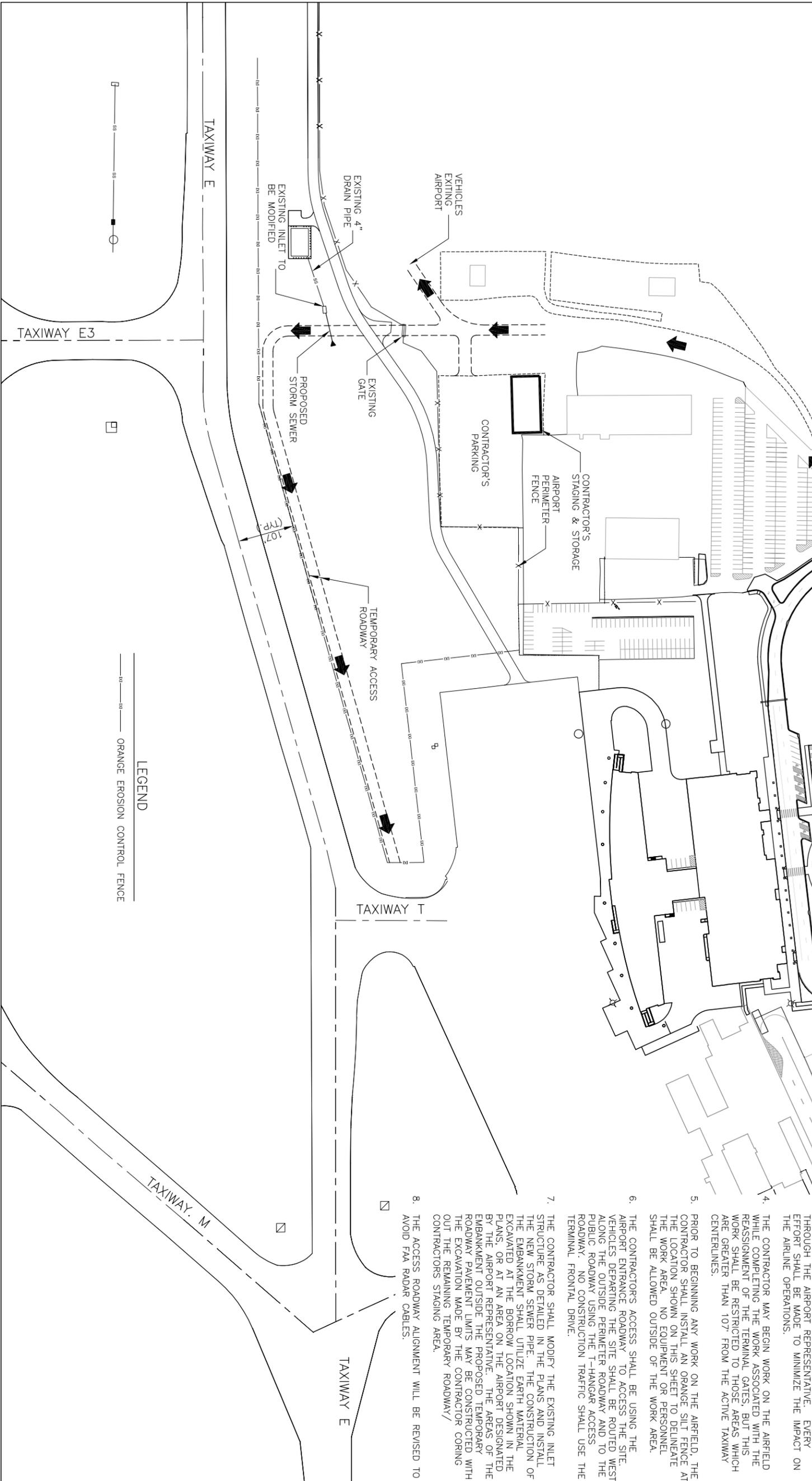
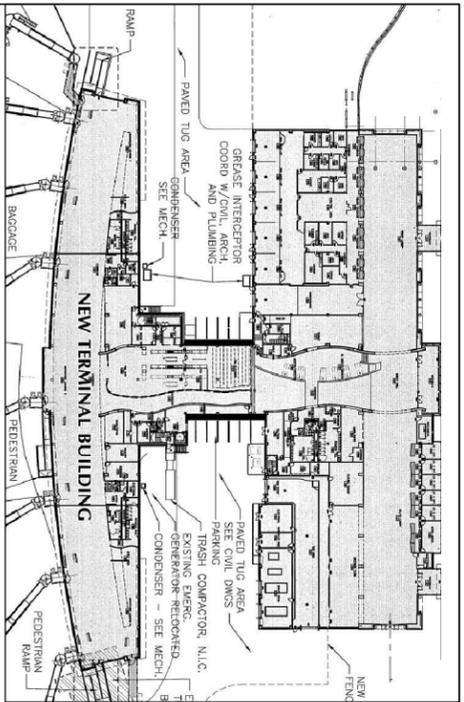
- TOTAL CONTRACT TIME SHALL BE 40 CALENDAR DAYS.
- PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEET.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC. DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN, WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.



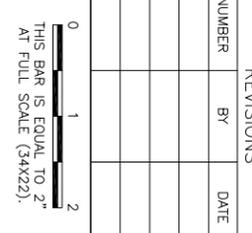
LEGEND
 ———— ORANGE EROSION CONTROL FENCE

PHASE 1 NOTES:

1. PHASE 1 SHALL CONSIST OF PLACEMENT OF TEMPORARY ORANGE EROSION CONTROL FENCE AS SHOWN ON THIS SHEET, CONSTRUCTION OF THE CONTRACTOR'S TEMPORARY ACCESS ROADWAY, RELOCATION OF THE AIRLINE GATE EQUIPMENT BY OTHERS, REMOVAL OF THE EXISTING J-LINES/ AIRCRAFT ENVELOPES AND PAVEMENT MARKING FOR THE NEW GATE POSITIONS.
2. THE CONTRACTOR SHALL COORDINATE THE RELOCATION OF THE GATE EQUIPMENT WITH THE AIRPORT AND THE AIRLINES. THE SCHEDULING OF THE GATE EQUIPMENT RELOCATION SHALL BE COORDINATED WITH THE CONTRACTOR'S PAVEMENT MARKING OPERATIONS. THE CONTRACTOR SHALL PROVIDE A MINIMUM OF TWO WEEKS NOTICE FOR THE RELOCATION OF THE GATE EQUIPMENT AND THE REVISIONS TO THE GATE PARKING POSITIONS. EVERY EFFORT SHALL BE MADE TO MINIMIZE THE IMPACT ON THE AIRLINE OPERATIONS.
3. THE CONTRACTOR SHALL COORDINATE THE REMOVAL OF THE J-LINES AND THE PLACEMENT OF THE NEW J-LINES WITH THE AIRPORT'S OPERATIONAL SCHEDULE THROUGH THE AIRPORT REPRESENTATIVE. EVERY EFFORT SHALL BE MADE TO MINIMIZE THE IMPACT ON THE AIRLINE OPERATIONS.
4. THE CONTRACTOR MAY BEGIN WORK ON THE AIRFIELD WHILE COMPLETING THE WORK ASSOCIATED WITH THE REASSIGNMENT OF THE TERMINAL GATES, BUT THIS WORK SHALL BE RESTRICTED TO THOSE AREAS WHICH ARE GREATER THAN 107' FROM THE ACTIVE TAXIWAY CENTERLINES.
5. PRIOR TO BEGINNING ANY WORK ON THE AIRFIELD, THE CONTRACTOR SHALL INSTALL AN ORANGE SILT FENCE AT THE LOCATION SHOWN ON THIS SHEET TO DELINEATE THE WORK AREA. NO EQUIPMENT OR PERSONNEL SHALL BE ALLOWED OUTSIDE OF THE WORK AREA.
6. THE CONTRACTOR'S ACCESS SHALL BE USING THE AIRPORT ENTRANCE ROADWAY TO ACCESS THE SITE. VEHICLES DEPARTING THE SITE SHALL BE ROUTED WEST ALONG THE OUTSIDE PERIMETER ROADWAY AND TO THE PUBLIC ROADWAY USING THE T-HANGAR ACCESS ROADWAY. NO CONSTRUCTION TRAFFIC SHALL USE THE TERMINAL FRONTAL DRIVE.
7. THE CONTRACTOR SHALL MODIFY THE EXISTING INLET STRUCTURE AS DETAILED IN THE PLANS AND INSTALL THE NEW STORM SEWER PIPE. THE CONSTRUCTION OF THE EMBANKMENT SHALL UTILIZE EARTH MATERIAL EXCAVATED AT THE BORROW LOCATION SHOWN IN THE PLANS, OR AT AN AREA ON THE AIRPORT DESIGNATED BY THE AIRPORT REPRESENTATIVE. THE AREAS OF THE EMBANKMENT OUTSIDE THE PROPOSED TEMPORARY ROADWAY PAVEMENT LIMITS MAY BE CONSTRUCTED WITH THE EXCAVATION MADE BY THE CONTRACTOR CORING OUT THE REMAINING TEMPORARY ROADWAY/ CONTRACTOR'S STAGING AREA.
8. THE ACCESS ROADWAY ALIGNMENT WILL BE REVISED TO AVOID FAA RADAR CABLES.

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 BASE_PROJ_DRAIN
 BASE_PROJ_Existing
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**WIDEN AIR CARRIER APRON
 CONSTRUCTION ACTIVITY PLAN 1**

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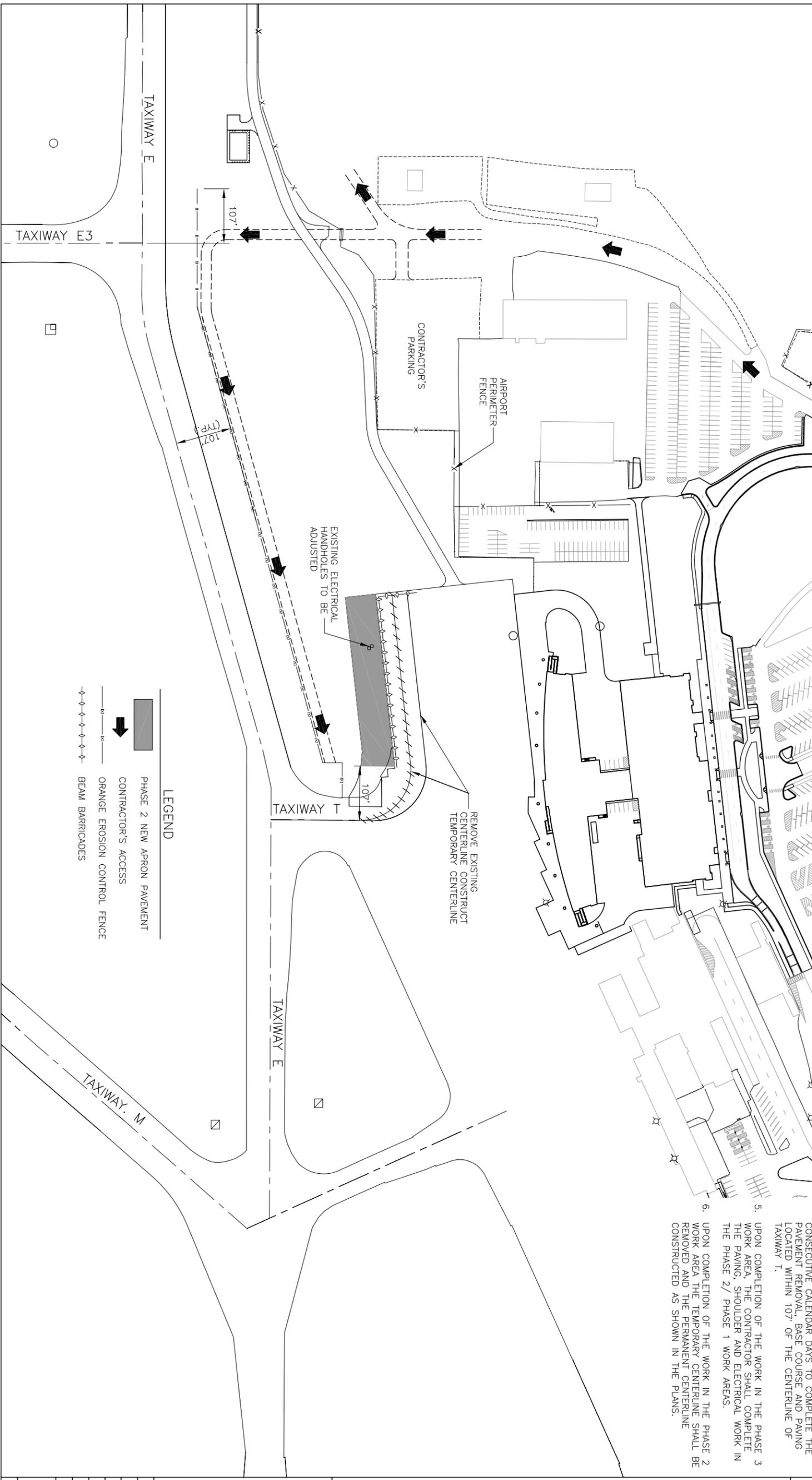
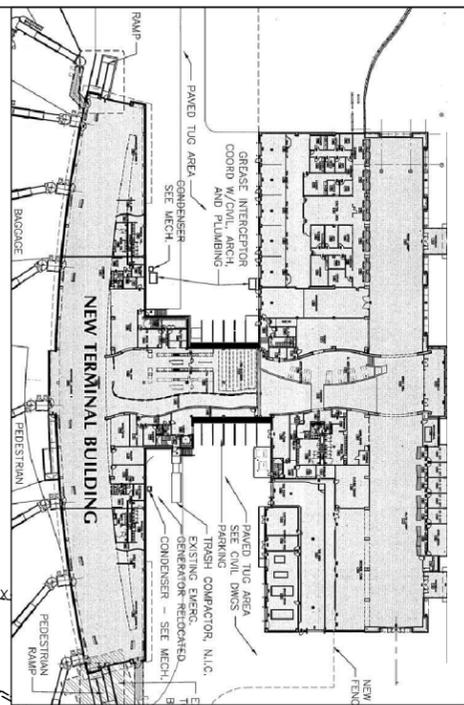
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IL PROJ. NO.:	PIA-4180
APP PROJ. NO.:	3-17-0080-XX

SHEET 04 OF 34 SHEETS

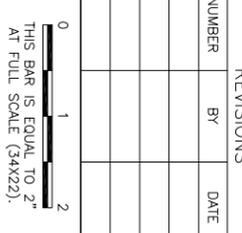


LEGEND

- PHASE 2 NOTES**
- TAXIWAY T WILL REMAIN OPEN DURING PHASE 2.
 - PRIOR TO BEGINNING WORK IN THE PHASE 2 WORK AREA, THE CONTRACTOR SHALL REMOVE THE EXISTING TAXIWAY CENTERLINE AND MARK A NEW 6' CENTERLINE AS SHOWN. THIS NEW CENTERLINE WILL MAINTAIN THE FAA REQUIRED WINGTIP CLEARANCE TO A LINE 5' INSIDE THE EDGE OF THE EXISTING PAVEMENT. THE CONTRACTOR'S EQUIPMENT WILL NOT BE ALLOWED BEYOND THIS LINE WHEN AIRCRAFT ARE ENTERING AND EXITING THE GATES.
 - THE PHASE 2 WORK SHALL CONSIST OF EMBANKMENT, BASE COURSE AND PREPARATION FOR PAVEMENT CONSTRUCTION FOR THE NEW WORK OUTSIDE OF THE LIMIT WHICH IS 107' FROM THE CENTERLINE OF TAXIWAYS T AND E.
 - THE CONTRACTOR SHALL BE ALLOWED TO CLOSE TAXIWAY T AS SHOWN ON CONSTRUCTION ACTIVITY PLAN 3 FOR A PERIOD OF NOT MORE THAN TEN CONSECUTIVE CALENDAR DAYS TO COMPLETE THE PAVEMENT REMOVAL, BASE COURSE AND PAVING LOCATED WITHIN 107' OF THE CENTERLINE OF TAXIWAY T.
 - UPON COMPLETION OF THE WORK IN THE PHASE 3 WORK AREA, THE CONTRACTOR SHALL COMPLETE THE PAVING, SHOULDER AND ELECTRICAL WORK IN THE PHASE 2/ PHASE 1 WORK AREAS.
 - UPON COMPLETION OF THE WORK IN THE PHASE 2 WORK AREA THE TEMPORARY CENTERLINE SHALL BE REMOVED AND THE PERMANENT CENTERLINE CONSTRUCTED AS SHOWN IN THE PLANS.

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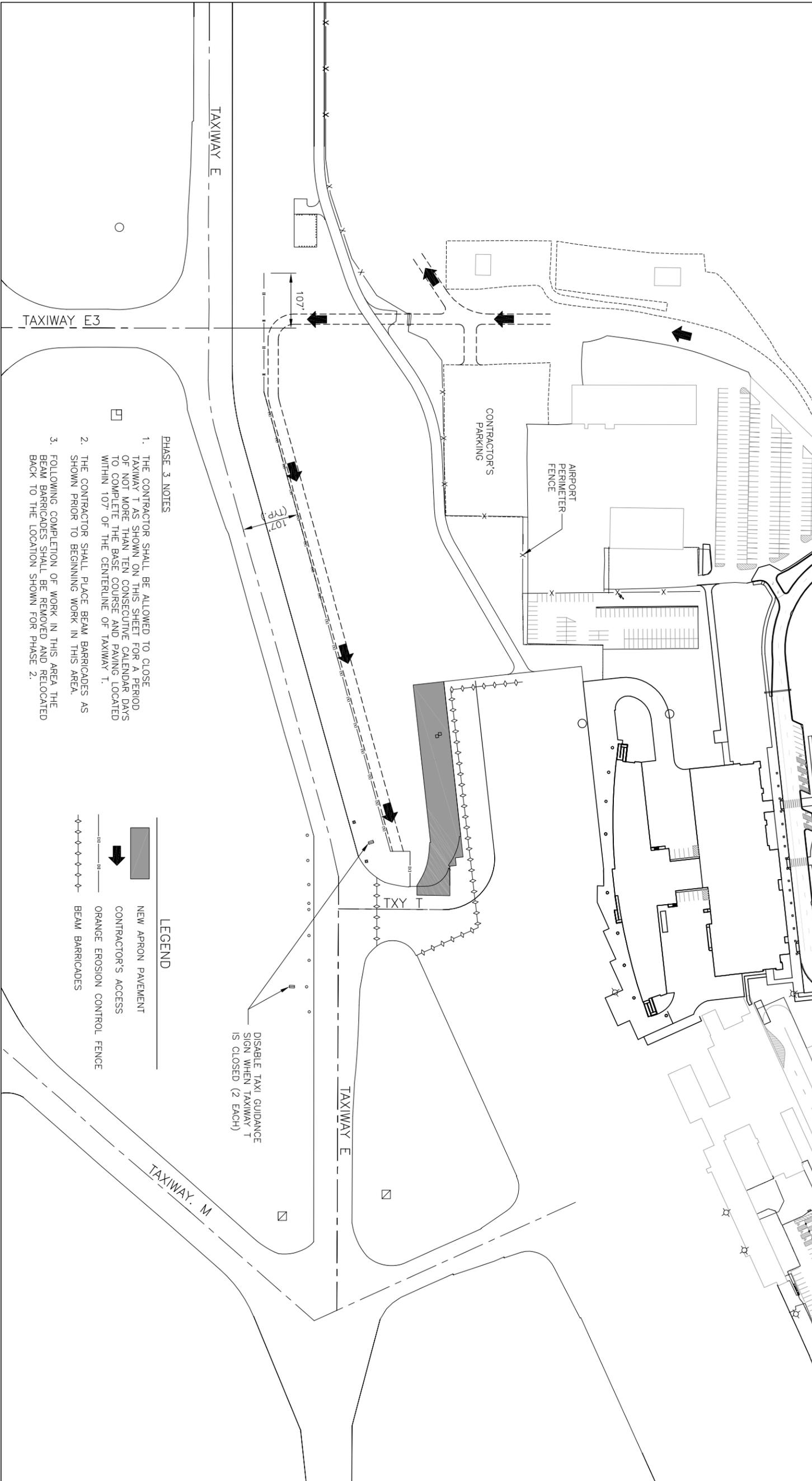
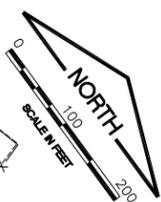
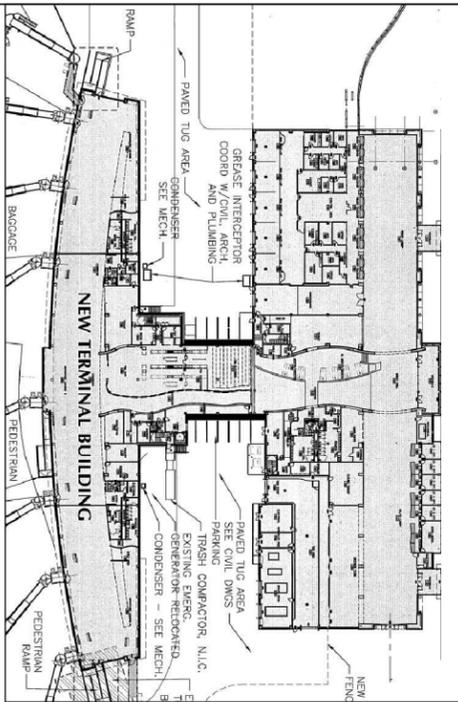
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SHEET	05 OF 34 SHEETS



- PHASE 3 NOTES**
1. THE CONTRACTOR SHALL BE ALLOWED TO CLOSE TAXIWAY T AS SHOWN ON THIS SHEET FOR A PERIOD OF NOT MORE THAN TEN CONSECUTIVE CALENDAR DAYS TO COMPLETE THE BASE COURSE AND PAVING LOCATED WITHIN 107' OF THE CENTERLINE OF TAXIWAY T.
 2. THE CONTRACTOR SHALL PLACE BEAM BARRICADES AS SHOWN PRIOR TO BEGINNING WORK IN THIS AREA.
 3. FOLLOWING COMPLETION OF WORK IN THIS AREA THE BEAM BARRICADES SHALL BE REMOVED AND RELOCATED BACK TO THE LOCATION SHOWN FOR PHASE 2.



DISABLE TAXI GUIDANCE SIGN WHEN TAXIWAY T IS CLOSED (2 EACH)

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WIDEN AIR CARRIER APRON

CONSTRUCTION ACTIVITY PLAN 3

PE094

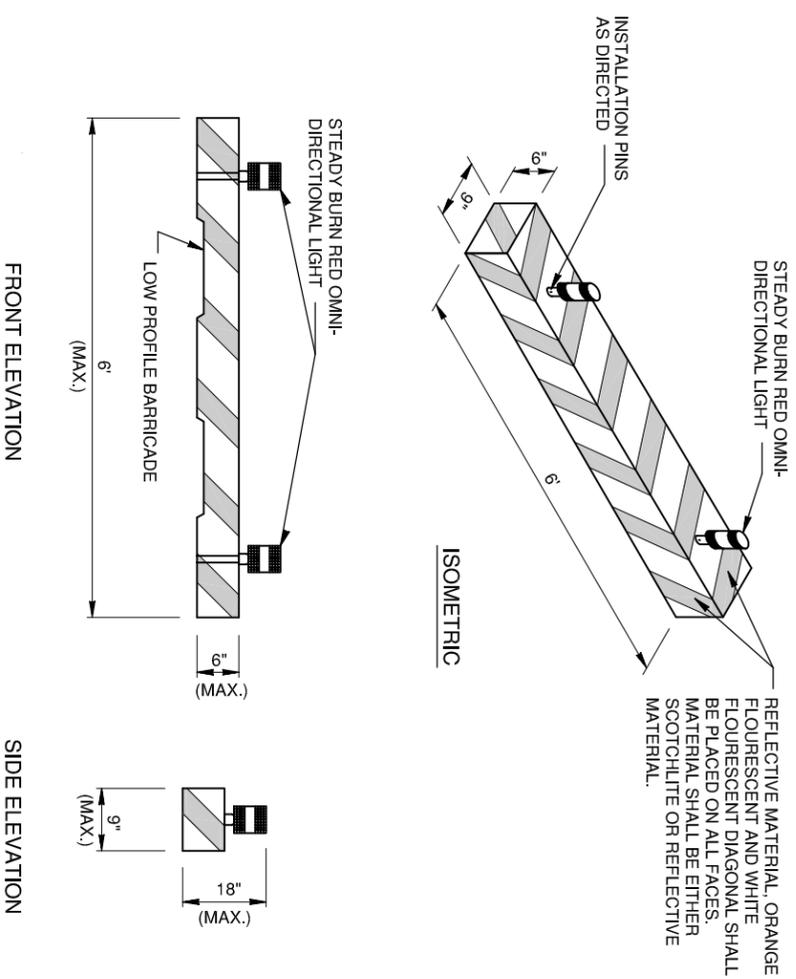
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PROP. GOVERNMENT
TERM. LOWER
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BASE PROJ GEO

BEAM BARRICADE NOTES

- 1) BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
- 2) BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
- 3) PLACE AT 10' INTERVALS.
- 4) NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.



BEAM BARRICADE DETAILS

N.T.S.

SECURITY NOTES

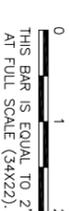
1. MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
2. AT NO TIME ARE UNSUPERVISED GATE OPENINGS PERMITTED AT THE AIRPORT. IF THE PROPOSED WORK REQUIRES GATE OPENING, THE CONTRACTOR SHALL PROVIDE A GUARD TO PREVENT UNAUTHORIZED ACCESS. THE CONTRACTOR SHALL SECURE THE GATE AT THE COMPLETION OF THE WORK DAY.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES DURING THE COURSE OF THE WORK.
4. FINES CAN BE LEVIED AGAINST THE CONTRACTOR BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) FOR NEGLIGENCE IF THE AIRPORT SECURITY IS COMPROMISED AND THE AIRPORT PERIMETER FENCE LINE IS NOT MAINTAINED AS SPECIFIED ABOVE. FINES CAN ALSO BE LEVIED AGAINST THE CONTRACTOR FOR FAILURE TO COOPERATE WITH THE AIRPORT MANAGEMENT AS REQUIRED TO MAINTAIN AIRPORT SECURITY.

CONSTRUCTION ACTIVITY PLAN GENERAL NOTES

1. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
2. THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
3. IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT ARE THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
4. WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT AND THE PROJECT.
5. DELAYS, CHANGES IN SCHEDULING OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. VEHICLE ACCESS ON PERIMETER ROADS SHALL BE MAINTAINED THROUGH OUT THE PROJECT.
7. THE CONTRACTOR SHALL BE AWARE THAT DAILY OPERATION OF THE AIRPORT WILL CONTINUE THROUGHOUT THIS PROJECT. AS A RESULT, DAILY COORDINATION WILL BE NECESSARY TO LIMIT DISRUPTION TO AIRPORT/TENANT AND CONTRACTOR OPERATIONS.
8. CONTRACTOR'S STAGING, STORAGE, AND PARKING WILL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS.
9. THE CONTRACTOR SHALL PLACE ALL BARRICADES, CONSTRUCTION SETBACK LINES, AND EROSION CONTROL ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
10. CONSTRUCTION PHASING IS OF CRITICAL IMPORTANCE TO THE AIRPORT FOR THIS PROJECT. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE ALLOWED TO CLOSE BOTH RUNWAYS AT THE SAME TIME.
11. CONSTRUCTION RELATED ITEMS REQUIRING THE CLOSURE OF RUNWAYS AND/OR TAXIWAYS SHALL REQUIRE A MINIMUM OF 72 HOURS NOTICE TO COORDINATION WITH THE AIRPORT AND FAA. NO EXTENSION TO CONTRACT TIME WILL BE GIVEN FOR DELAYS CAUSED BY LACK OF ADEQUATE COORDINATION.
12. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A. THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - B. THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE AIRPORT OPERATIONS BY THE CONTRACTOR.
 - C. THE CONTRACTOR SHALL USE AN EXISTING GATE(S), OR NEW GATES AS CALLED OUT IN THE PLANS, FOR ACCESS TO THE AIRFIELD.
 - D. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - E. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - F. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
 - G. THE CONTRACTOR SHALL CLOSE AND SECURE THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - H. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - I. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
 - J. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE USED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.

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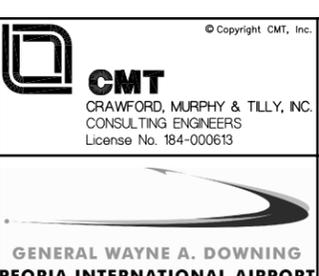


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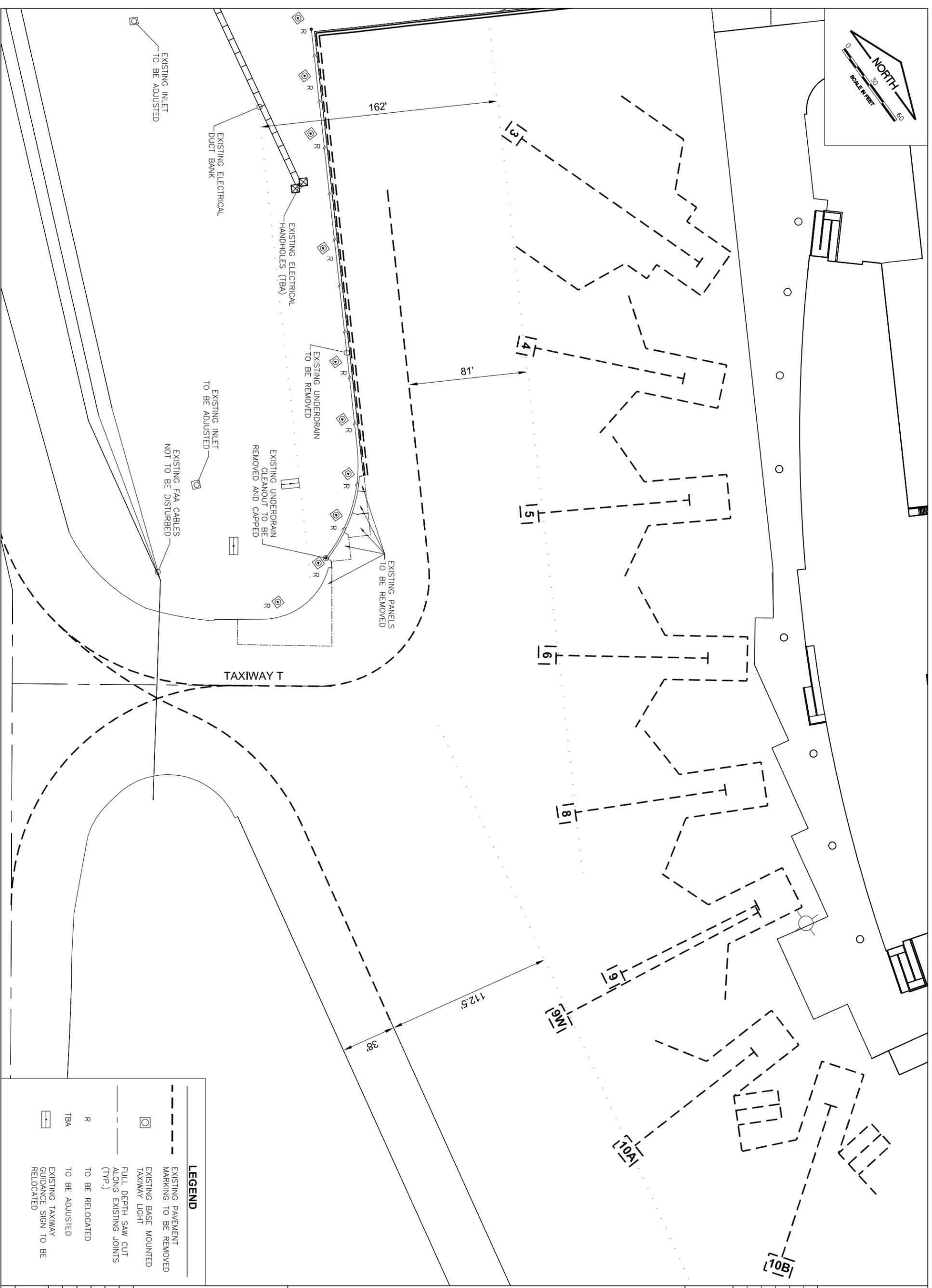
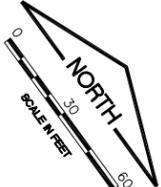
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 AND DETAILS**

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APP PROJ. NO. 3-17-0080-XX	
SHEET	07 OF 34 SHEETS



LEGEND

- EXISTING PAVEMENT MARKING TO BE REMOVED
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- FULL DEPTH SAW CUT ALONG EXISTING JOINTS (TYP)
- TO BE RELOCATED
- TO BE ADJUSTED
- EXISTING TAXIWAY GUIDANCE SIGN TO BE RELOCATED

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SHEET 08 OF 34 SHEETS

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WIDEN AIR CARRIER APRON

EXISTING CONDITIONS AND REMOVALS

REVISIONS

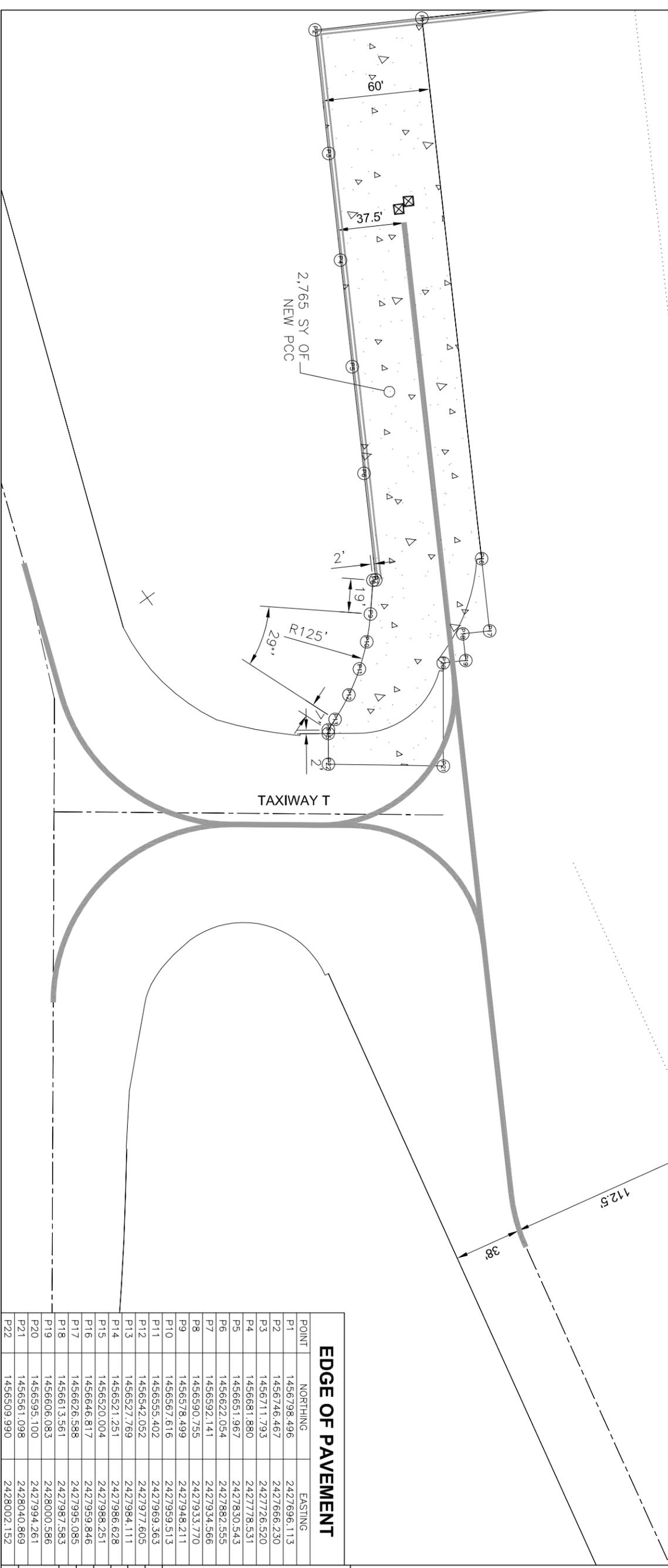
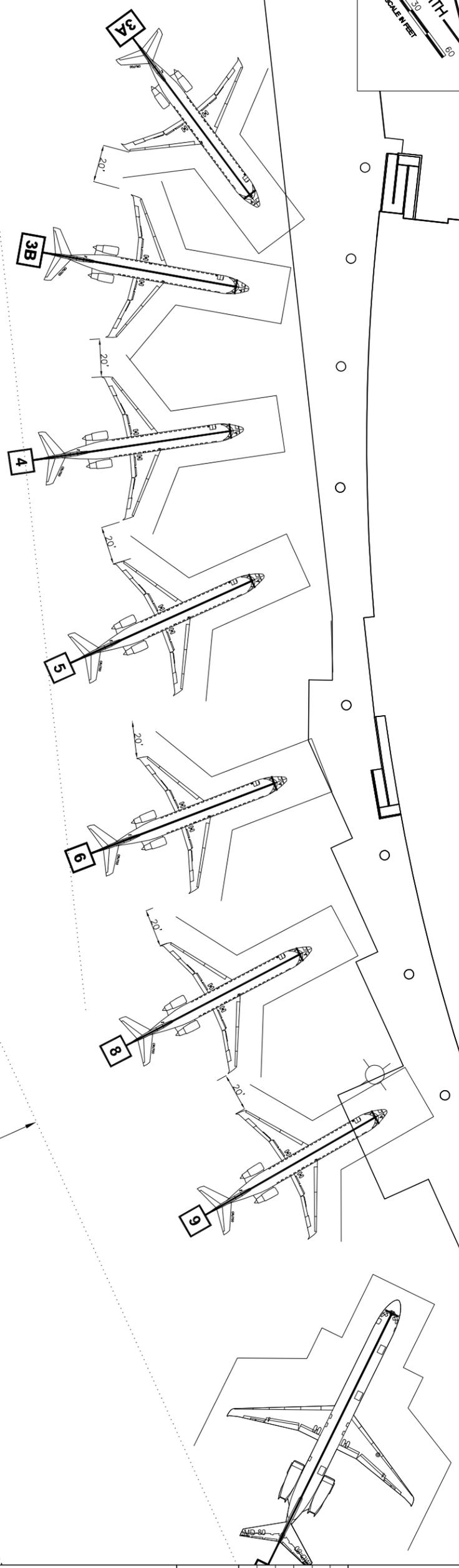
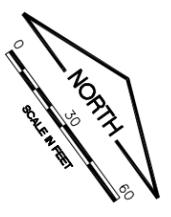
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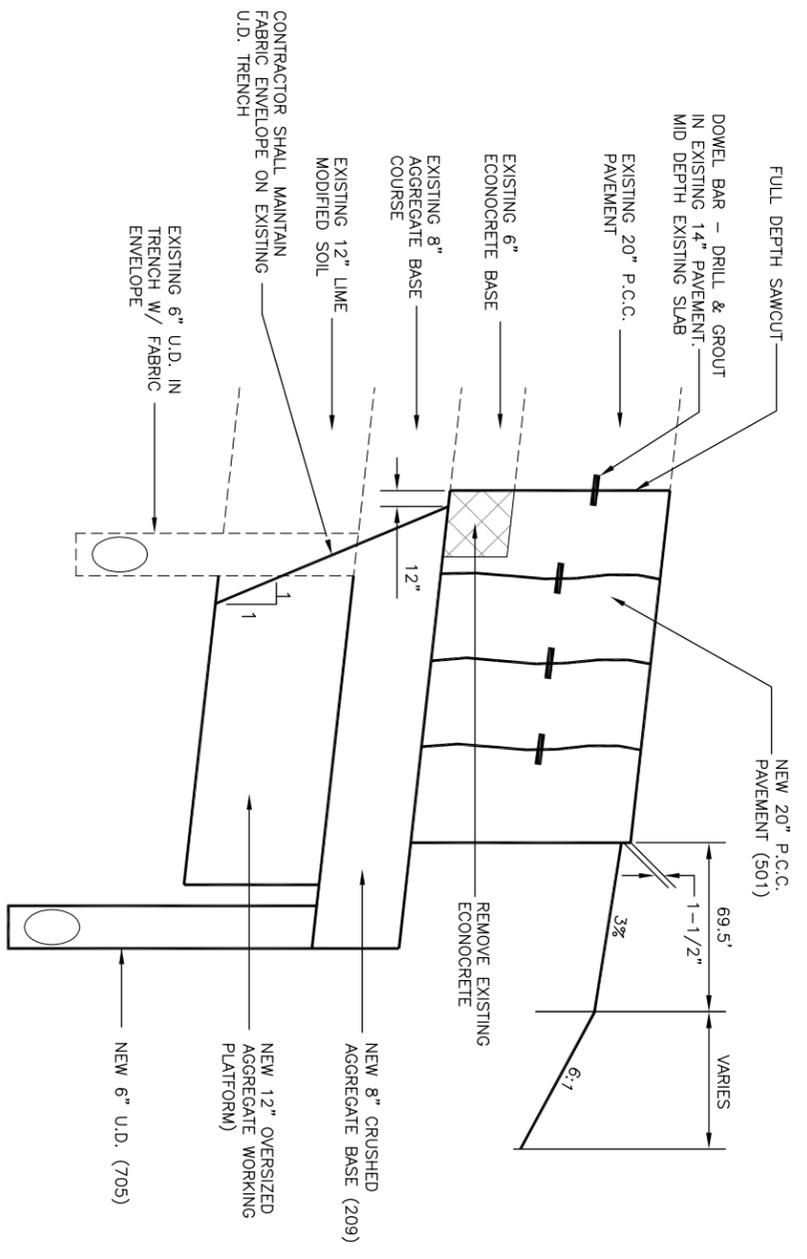
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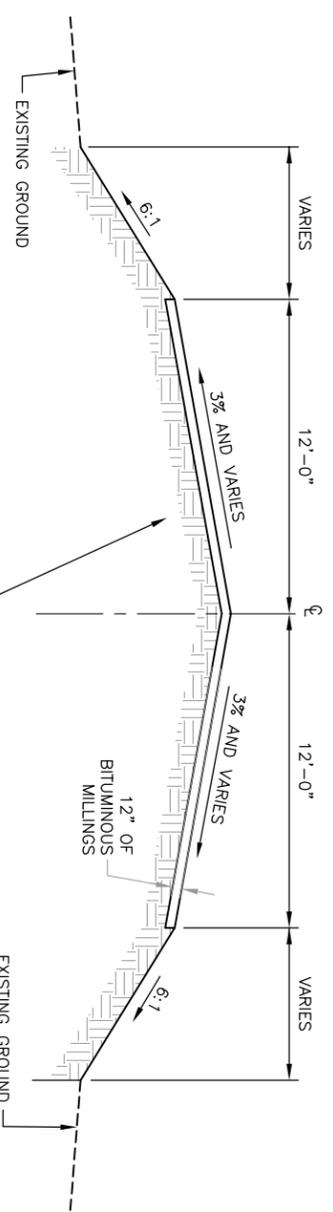
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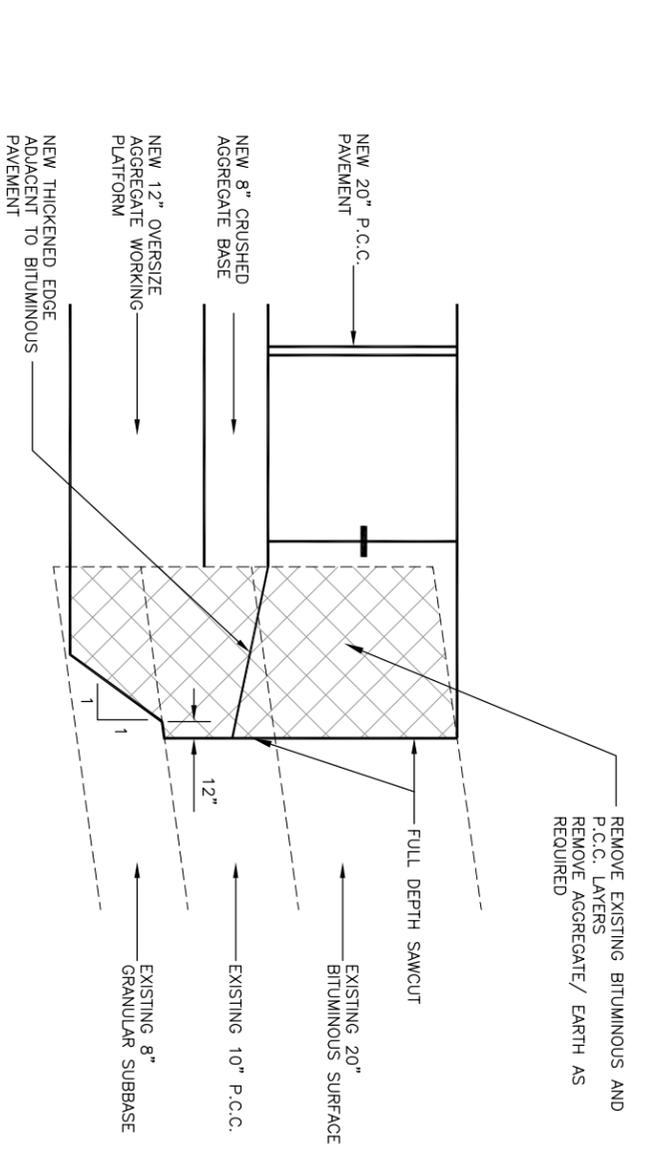
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A PROPOSED TYPICAL SECTION
N.T.S.



C PROPOSED TYPICAL SECTION
N.T.S.



B PROPOSED TYPICAL SECTION
N.T.S.

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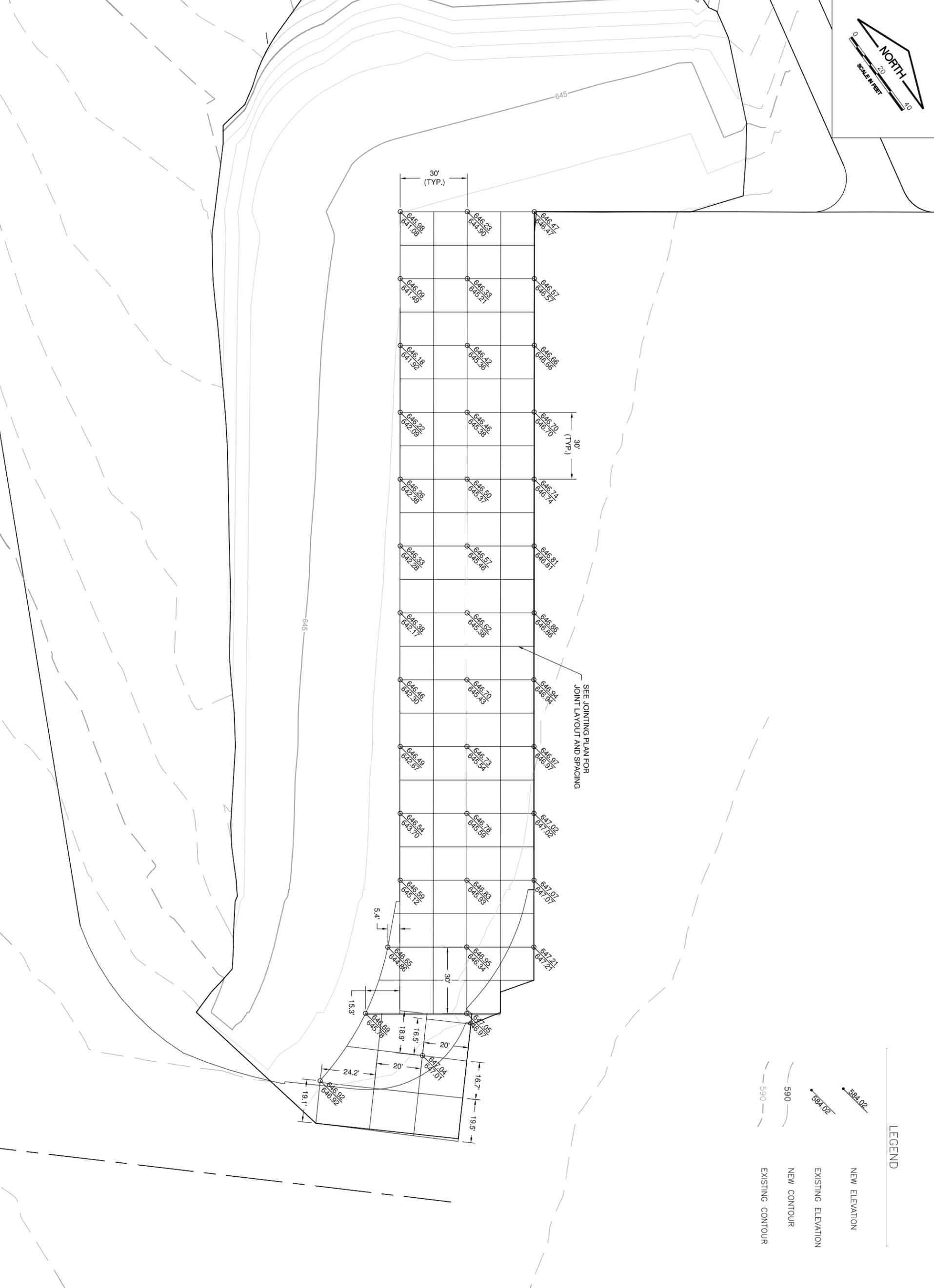
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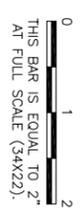
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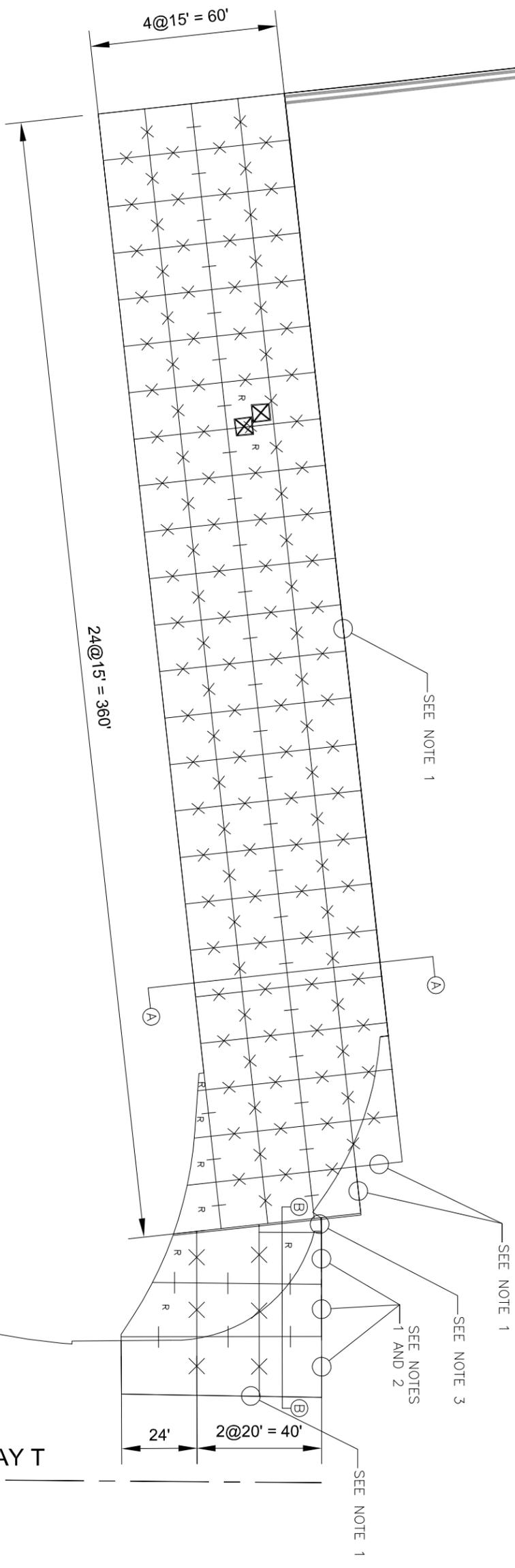
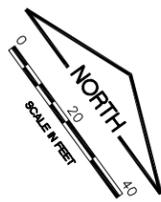
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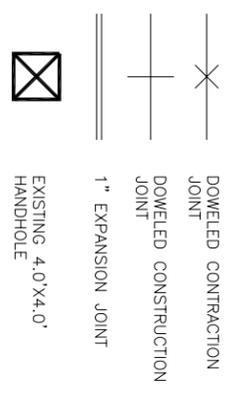
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 SHEET 11 OF 34 SHEETS

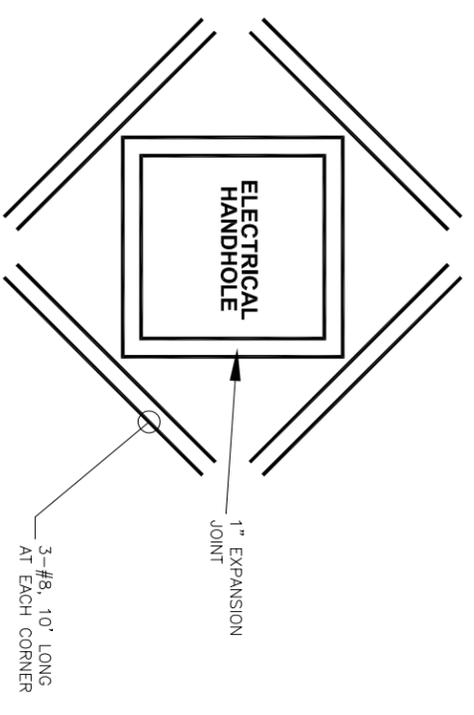


LEGEND



NOTES

1. CONTRACTOR SHALL DRILL AND GROUT DOWEL BARS INTO EXISTING PAVEMENT
2. EXTEND EXISTING JOINT PATTERN
3. NO DOWELS TO BE DRILLED IN THIS SECTION OF JOINT
4. IF THE CONTRACTOR CHOOSES TO PAVE IN 15' LANES, THE CENTER JOINT SHALL BE A DOWELED CONSTRUCTION JOINT

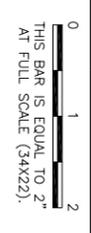


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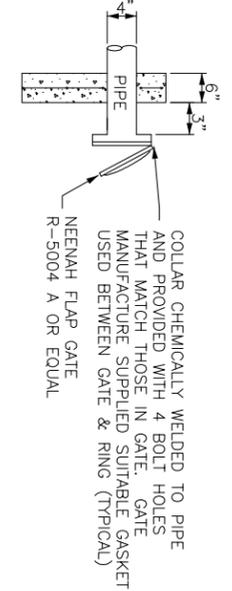
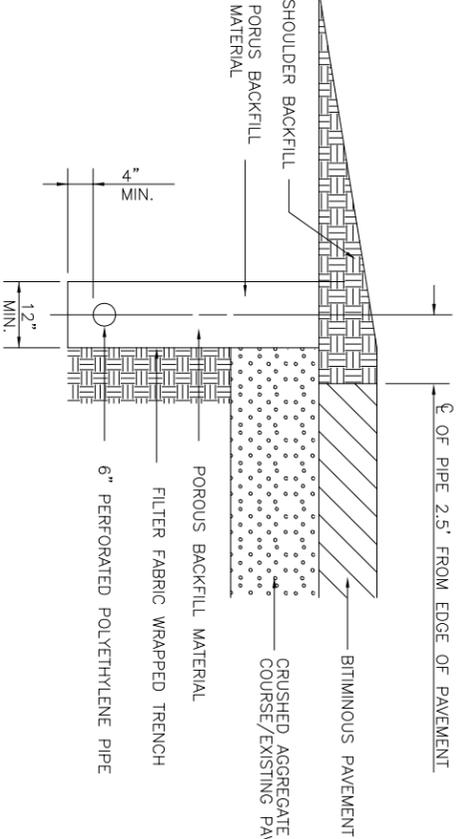
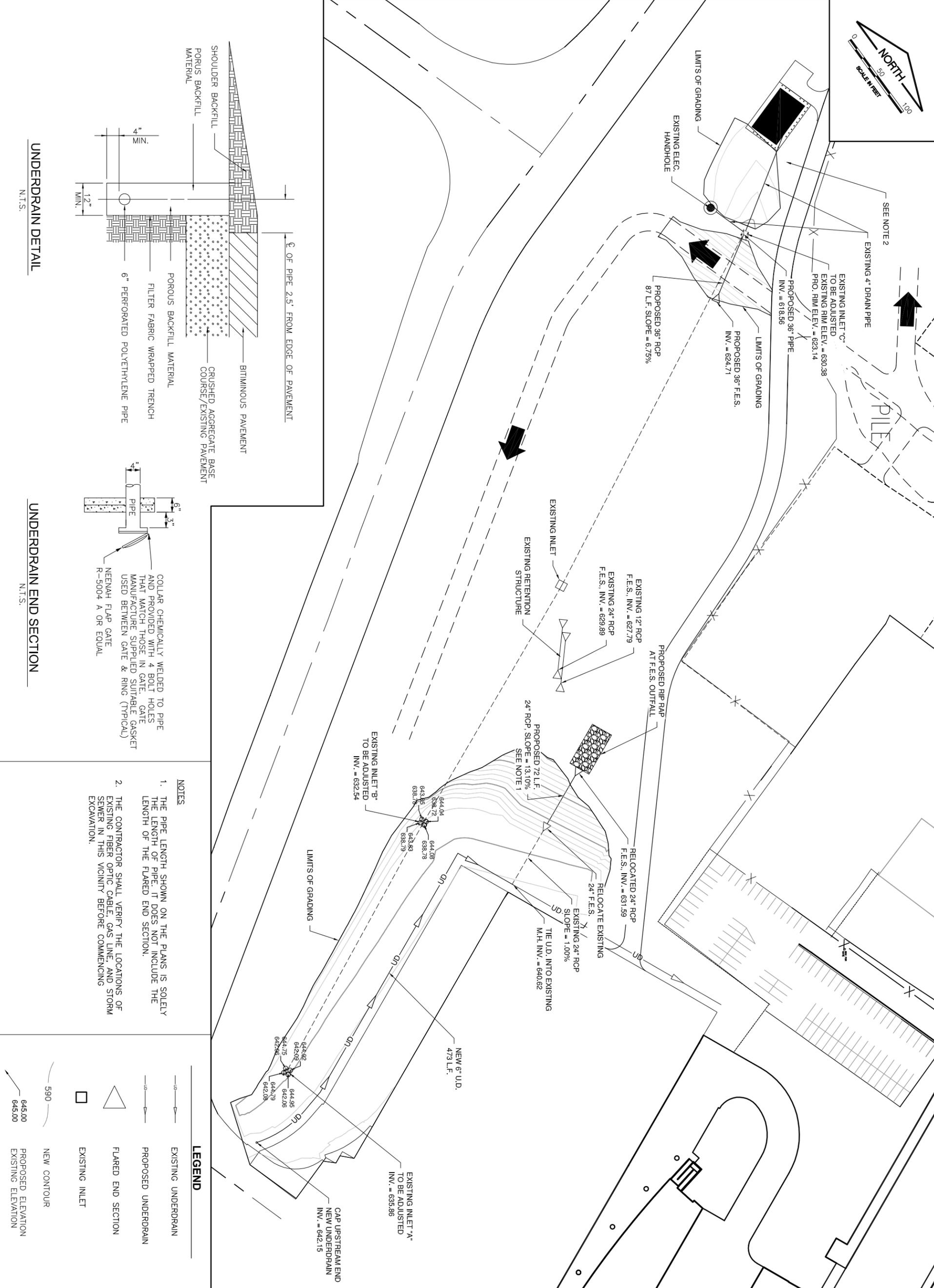
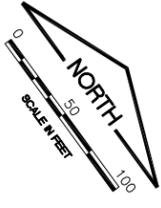
WIDEN AIR CARRIER APRON
JOINTING PLAN

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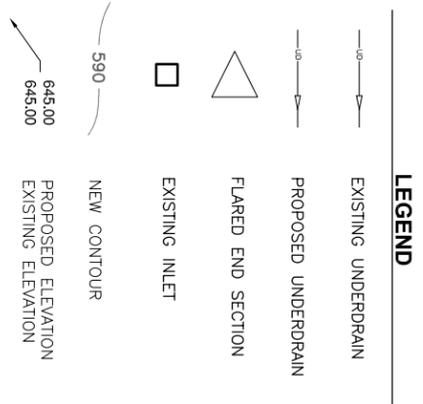
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IL. PROJ. NO. PIA-4180	
AP PROJ. NO. 3-17-0080-XX	
SHEET 12 OF 34 SHEETS	



- NOTES**
1. THE PIPE LENGTH SHOWN ON THE PLANS IS SOLELY THE LENGTH OF PIPE. IT DOES NOT INCLUDE THE LENGTH OF THE FLARED END SECTION.
 2. THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF EXISTING FIBER OPTIC CABLE, GAS LINE, AND STORM EXCAVATION.



**GENERAL WAYNE A. DOWNING
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**WIDEN AIR CARRIER APRON
DRAINAGE PLAN**

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IL PROJ. NO. PA-4180	
APP PROJ. NO. 3-17-0080-XX	
SHEET 14 OF 34 SHEETS	

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UPDATE BY: Andrew Bodine
PLOT DATE: 5/24/2012 1:29 PM
PA-Base-Existing

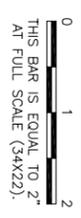
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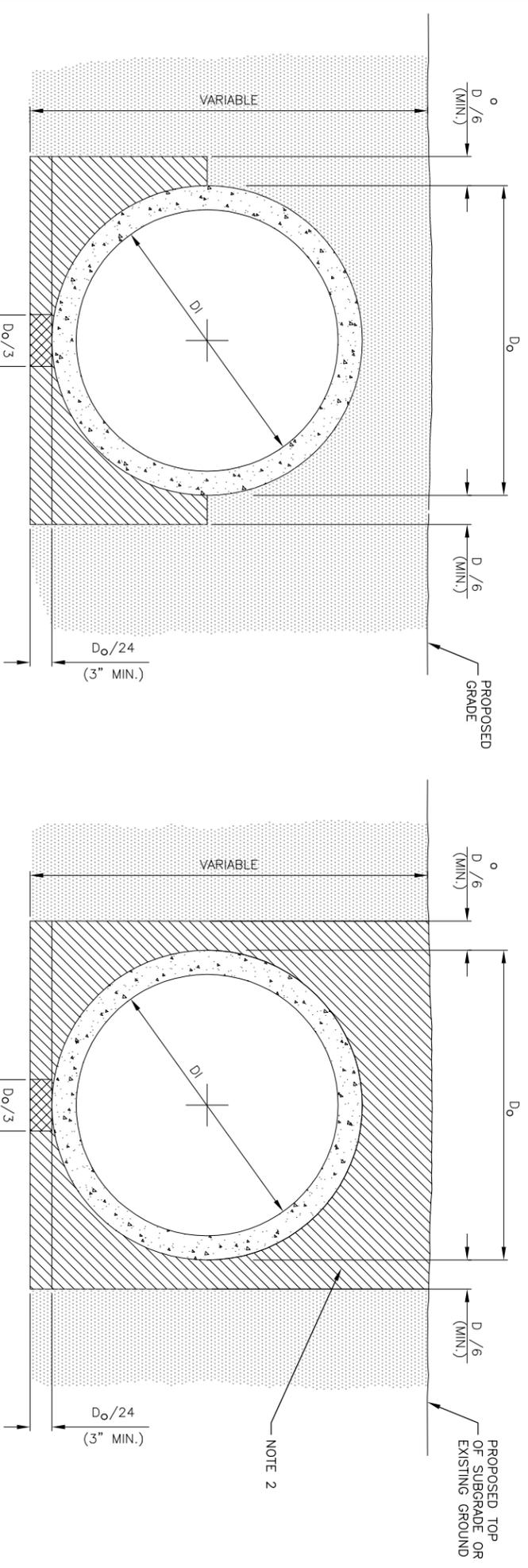
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NUMBER	BY	DATE



TRENCH INSTALLATION LEGEND	
	DRAINAGE CONDUIT MATERIAL—CONCRETE
	MIDDLE BEDDING—LOOSELY PLACED UNCOMPACTED BEDDING
	HAUNCH AND OUTER BEDDING—TO ENGINEER'S SATISFACTION OR 95% STANDARD PROCTOR
	LOWER SIDE AND OVERFILL COMPACTION—SAME AS EMBANKMENT REQUIREMENTS
D_o	PIPE OUTSIDE DIAMETER
D_i	PIPE INSIDE DIAMETER

- TRENCH INSTALLATION NOTES**
- BEDDING SHOWN IS IN ACCORDANCE WITH "STANDARD EMBANKMENT INSTALLATIONS," STANDARD INSTALLATION & BEDDING FACTORS FOR THE INDIRECT DESIGN METHOD (DESIGN DATA 40), AMERICAN CONCRETE PIPE ASSOCIATION.
 - BACKFILL TO EXTEND 3' BEYOND EDGES OF PROPOSED PAVEMENT.



STANDARD TRENCH INSTALLATION PROPOSED PAVED AREA
N.T.S.

STANDARD TRENCH INSTALLATION NON-PAVED AREA
N.T.S.

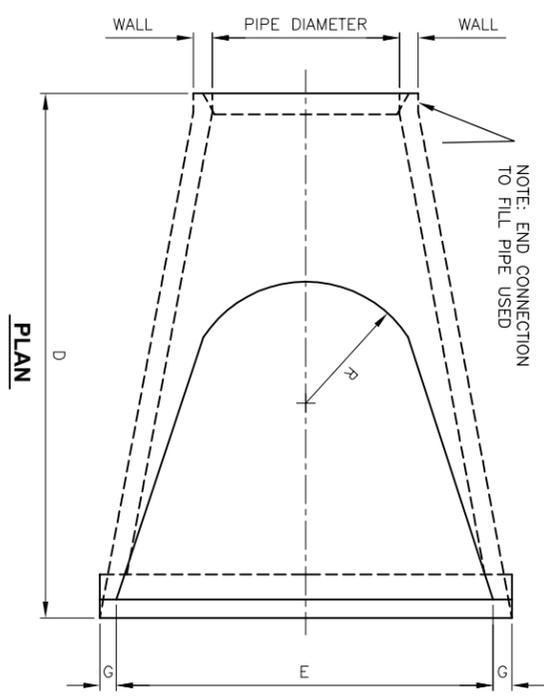
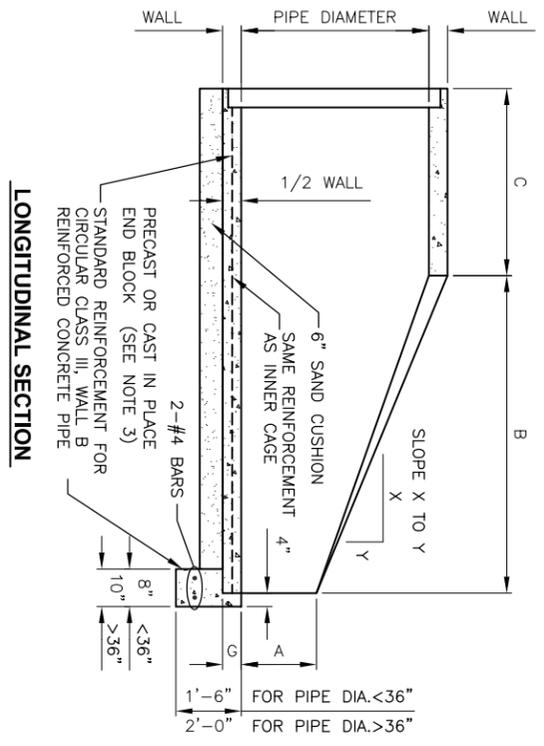
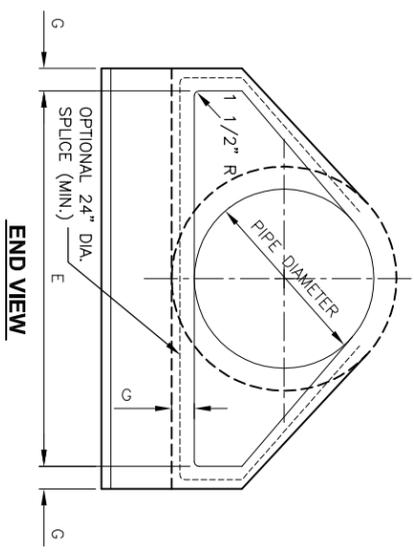
DIMENSIONS - TABLE 1

PIPE DIA.	APPROX WT. (lbs.)	WALL	A	B	C	D	E	G	R	SLOPE
12"	530	2"	4"	2'-0"	4'-0 7/8"	6'-0 7/8"	2'-0"	2"	9"	3:1
15"	740	2 1/4"	6"	2'-3"	3'-10"	6'-1"	2'-6"	2 1/4"	11"	3:1
18"	990	2 1/2"	9"	2'-3"	3'-10"	6'-1"	3'-0"	2 1/2"	12"	3:1
21"	1280	2 3/4"	9"	2'-11"	3'-2"	6'-1"	3'-6"	2 3/4"	13"	3:1
24"	1520	3"	9 1/2"	2'-11"	3'-7 1/2"	6'-1 1/2"	4'-0"	3"	14"	3:1
27"	1930	3 1/4"	10 1/2"	4'-0"	2'-1 1/2"	6'-1 1/2"	4'-6"	3 1/4"	14 1/2"	3:1
30"	2190	3 1/2"	1'-0"	4'-6"	1'-7 3/4"	6'-1 3/4"	5'-0"	3 1/2"	15"	3:1
33"	3200	3 3/4"	1'-1 1/2"	4'-10 1/2"	3'-3 1/4"	8'-1 3/4"	5'-6"	3 3/4"	17 1/2"	3:1
36"	4100	4"	1'-3"	5'-3"	2'-10 3/4"	8'-1 3/4"	6'-0"	4"	20"	3:1
42"	5390	4 1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	4 1/2"	22"	3:1
48"	6550	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	5"	22"	3:1
54"	8240	5 1/2"	2'-3"	5'-5"	2'-11"	8'-4"	7'-6"	5 1/2"	24"	2.4:1
60"	8730	6"	2'-11"	5'-0"	3'-3"	8'-3"	8'-0"	5"	*	2:1
66"	10710	6 1/2"	2'-6"	6'-0"	2'-3"	8'-3"	8'-6"	5 1/2"	*	2:1
72"	12520	7"	3'-0"	6'-6"	1'-9"	8'-3"	9'-0"	6"	*	1.86:1
78"	14770	7 1/2"	3'-0"	7'-6"	1'-9"	9'-3"	9'-6"	6 1/2"	*	1.82:1
84"	18160	8"	3'-0"	7'-6 1/2"	1'-9"	9'-3 1/2"	10'-0"	6 1/2"	*	1.5:1

* RADIUS AS FURNISHED BY MANUFACTURER.

PRECAST FLARE SECTION NOTES

- PRECAST CONCRETE FLARED END SECTIONS SHALL CONFORM TO THE APPLICABLE REQUIREMENTS OF AASHTO M-170 CLASS III, WALL B REINFORCED CONCRETE PIPE.
- PRECAST CONCRETE FLARED END SECTION FOR PIPE DIAMETER REQUIRED SHALL BE AS INDICATED ON DETAIL PLAN FOR EACH INDIVIDUAL INSTALLATION.
- THE END BLOCK SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE FLARED END SECTION. THE END BLOCK SHALL BE BACKFILLED IN ACCORDANCE WITH ITEM 701.



**GENERAL WAYNE A. DOWNING
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**WIDEN AIR CARRIER APRON
 DRAINAGE DETAILS 1**

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IL PROJ. NO. PA-4180	
APP PROJ. NO. 3-17-0080-XX	

PRECAST REINFORCED CONCRETE FLARED END SECTION
 (I.D.O.I. STD. NO. 542301)
 N.T.S.

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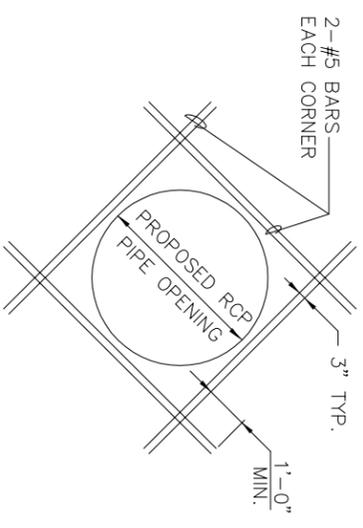
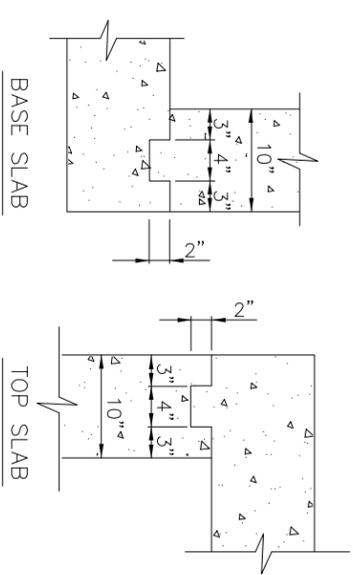
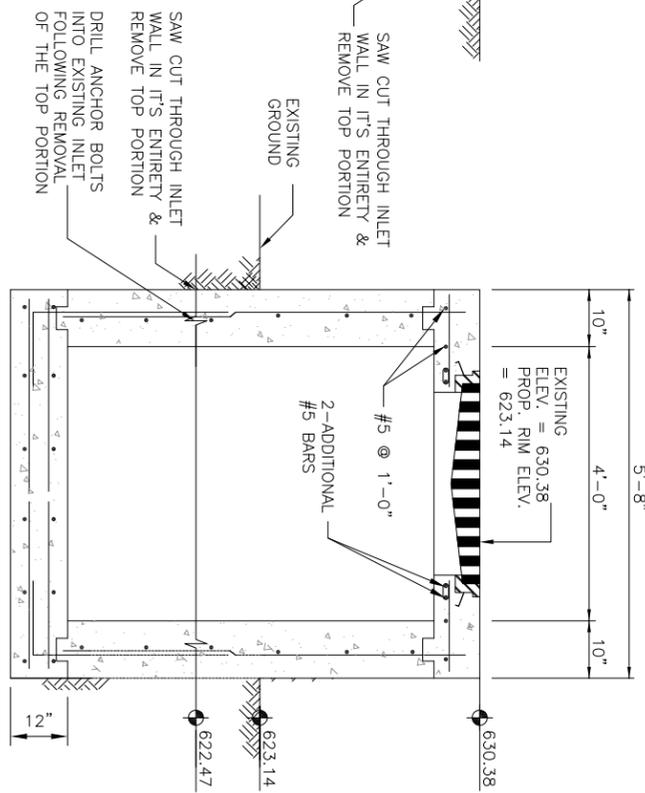
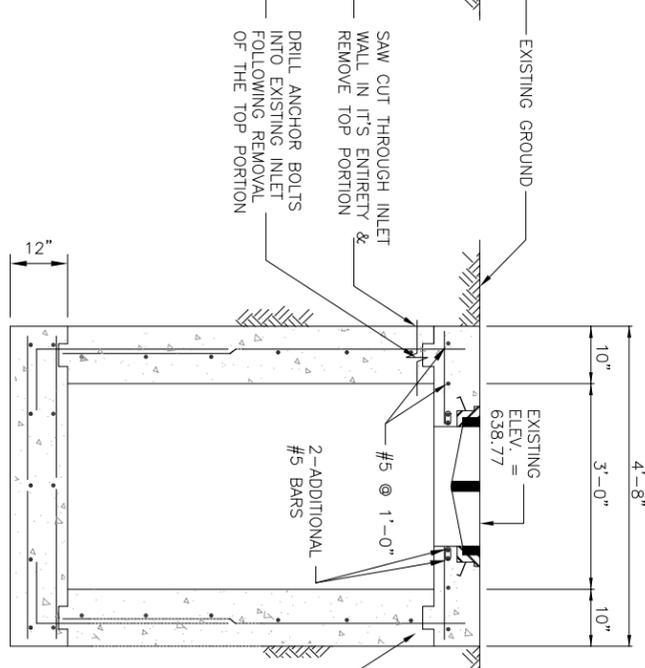
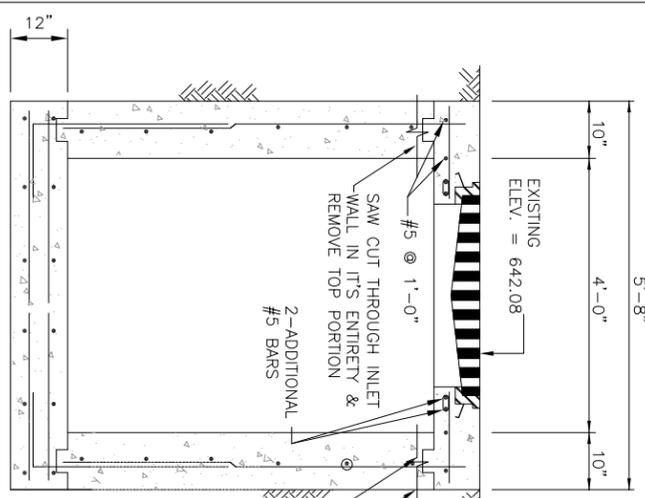
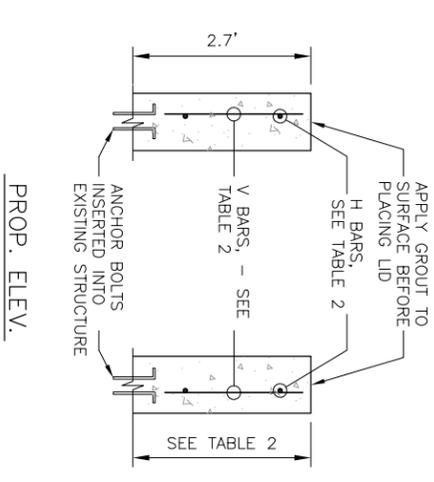
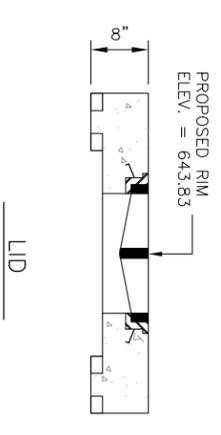
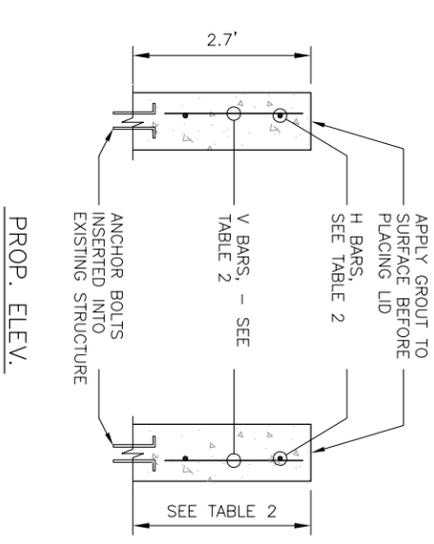
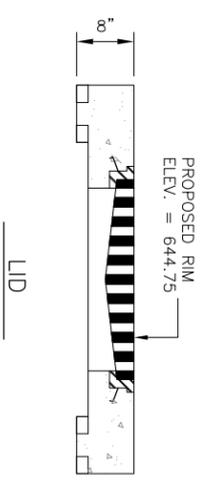
TABLE 1

STRUCTURE	I-1	I-2	I-3
RIM ELEVATION	729.40	732.40	727.75
RCP "A" DIA. & INV.	(18") 726.70	(12") 729.77	(18") 724.80
RCP "B" DIA. & INV.			(6") 726.73
INLET INVERT	726.40	729.40	724.50
INLET HEIGHT (H)	3'-0"	3'-0"	3'-3"

TABLE 2

INLET HEIGHT (H)	H BARS	V AND V ₁ BARS
H ≤ 5'	#5 @ 1'-0"	#5 @ 9"
5' < H ≤ 10'	#5 @ 1'-0"	#5 @ 9"
10' < H ≤ 15' (MAX)	#6 @ 1'-0"	#6 @ 1'-0"

- GENERAL NOTES**
- SEE SPECIFICATIONS FOR MATERIAL REQUIREMENTS, CONSTRUCTION METHODS, BASIS OF PAYMENT, AND OTHER REQUIREMENTS.
 - PIPE PENETRATION OPENINGS FOR PRECAST STRUCTURES SHALL BE CIRCULAR. DIAMETER OF OPENING SHALL BE OUTSIDE PIPE DIAMETER PLUS 6 INCHES. GROUT OPENING SOLID WITH NON-SHRINK GROUT.



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**WIDEN AIR CARRIER APRON
 DRAINAGE DETAILS 2**

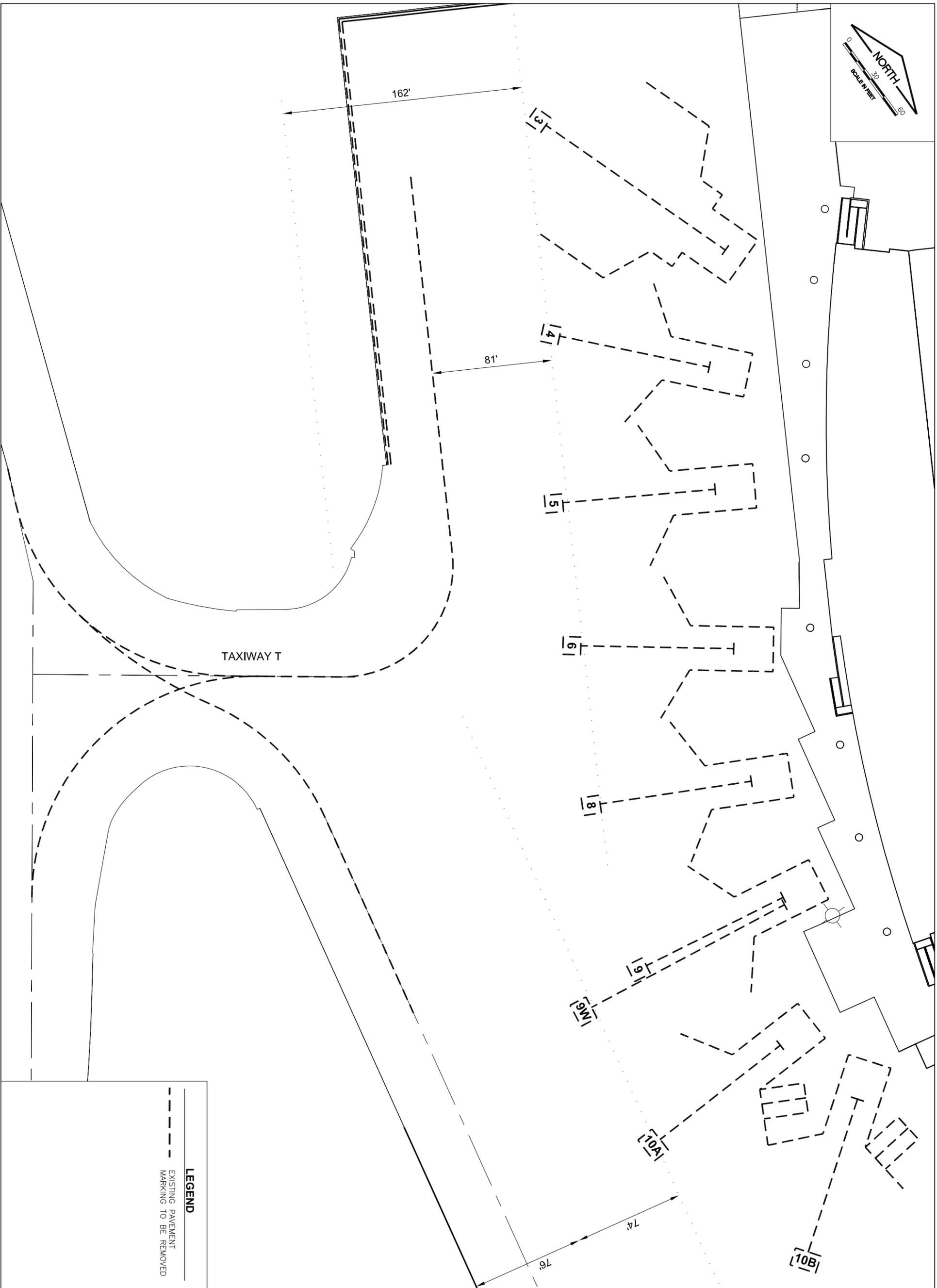
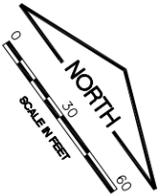
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**ADDITIONAL REINFORCEMENT
 AROUND PIPE PENETRATIONS**

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APPROVED BY:	CET
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LEGEND
 - - - - - EXISTING PAVEMENT MARKING TO BE REMOVED

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WIDEN AIR CARRIER APRON

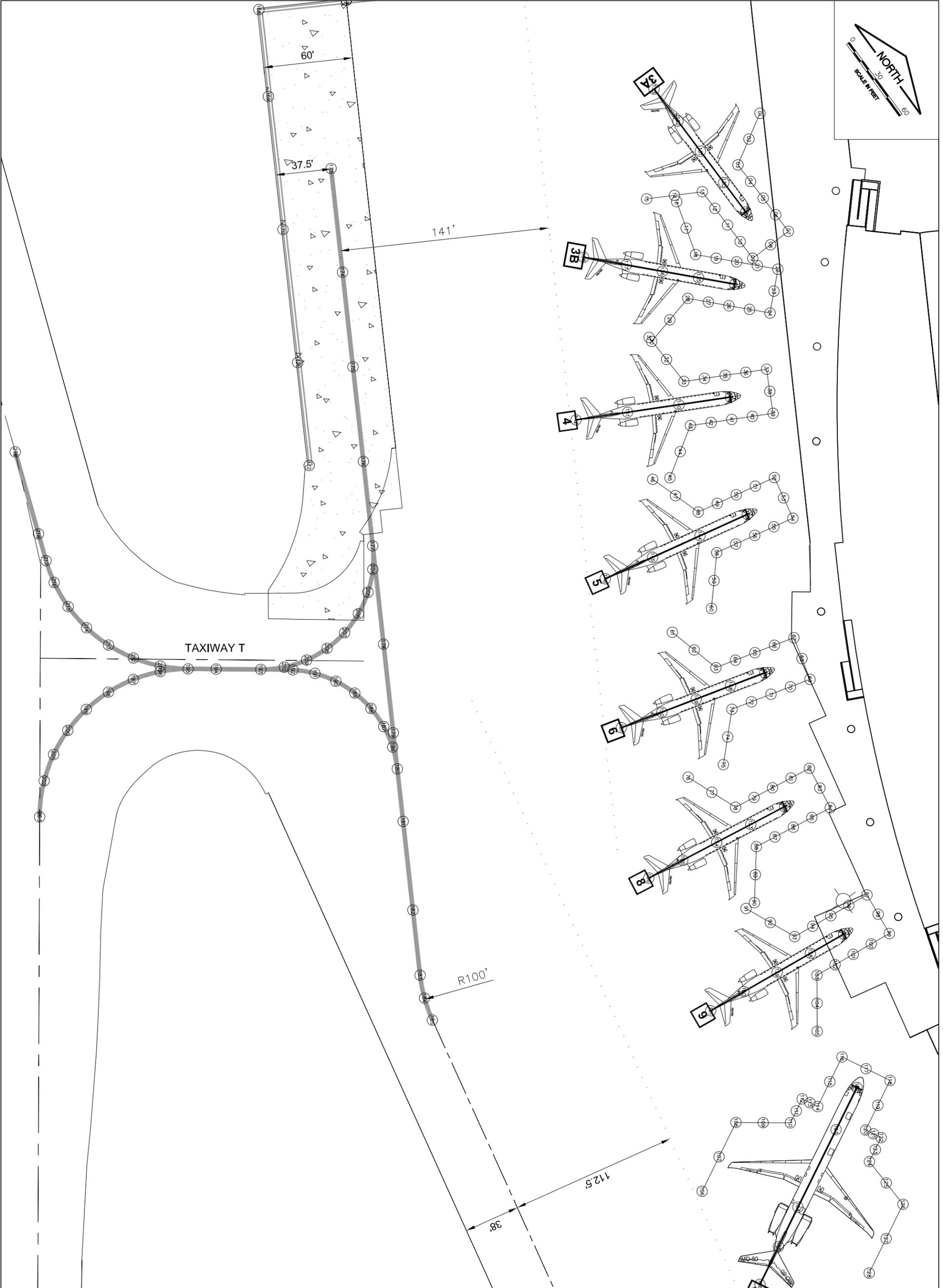
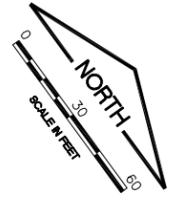
APRON MARKING REMOVAL PLAN

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**WIDEN AIR CARRIER APRON
 PROPOSED APRON MARKING**

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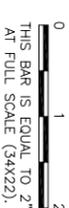
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WIDEN AIR CARRIER APRON

MARKING LAYOUT POINTS

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	AP PROJ. NO. 3-17-0080-XX
SHEET	19 OF 34 SHEETS

LAYOUT POINT TABLE

POINT	NORTHING	EASTING
76	1456674.509	2428254.682
77	1456681.406	2428272.252
78	1456688.303	2428289.822
79	1456702.185	2428291.867
80	1456716.066	2428293.911
81	1456729.948	2428295.956
82	1456743.829	2428298.001
83	1456741.643	2428312.841
84	1456739.457	2428327.681
85	1456725.576	2428325.636
86	1456711.694	2428323.591
87	1456697.813	2428321.546
88	1456683.931	2428319.502
89	1456672.261	2428334.337
90	1456660.591	2428349.171
91	1456653.765	2428348.958
92	1456661.333	2428366.249
93	1456668.901	2428383.541
94	1456682.951	2428385.050
95	1456696.800	2428396.559
96	1456710.750	2428398.068
97	1456724.700	2428389.577
98	1456723.087	2428404.490
99	1456721.473	2428419.403
100	1456707.524	2428417.894

LAYOUT POINT TABLE

POINT	NORTHING	EASTING
51	1456626.631	2428123.263
52	1456640.366	2428126.130
53	1456637.301	2428140.814
54	1456634.236	2428155.497
55	1456620.501	2428152.630
56	1456606.766	2428149.763
57	1456793.031	2428146.897
58	1456779.295	2428144.030
59	1456766.763	2428158.144
60	1456754.231	2428172.268
61	1456723.603	2428169.674
62	1456728.376	2428187.936
63	1456733.149	2428206.198
64	1456746.692	2428209.868
65	1456760.235	2428213.538
66	1456773.777	2428217.209
67	1456787.320	2428220.879
68	1456783.396	2428235.357
69	1456779.472	2428249.835
70	1456765.930	2428246.164
71	1456752.387	2428242.484
72	1456738.844	2428238.823
73	1456725.302	2428235.153
74	1456711.960	2428248.505
75	1456698.619	2428261.867

LAYOUT POINT TABLE

POINT	NORTHING	EASTING
26	1456884.033	2428014.748
27	1456874.342	2428004.602
28	1456864.650	2427994.455
29	1456846.326	2427998.980
30	1456828.002	2428003.506
31	1456828.193	2428000.296
32	1456828.863	2428019.159
33	1456829.533	2428038.022
34	1456841.948	2428044.561
35	1456854.362	2428051.100
36	1456866.776	2428057.639
37	1456879.191	2428064.178
38	1456872.200	2428077.450
39	1456885.210	2428090.721
40	1456892.795	2428084.182
41	1456840.381	2428077.643
42	1456827.967	2428071.104
43	1456815.552	2428064.565
44	1456799.617	2428074.882
45	1456783.683	2428094.798
46	1456773.746	2428078.765
47	1456779.585	2428096.714
48	1456785.425	2428114.663
49	1456799.160	2428117.529
50	1456812.895	2428120.396

LAYOUT POINT TABLE

POINT	NORTHING	EASTING
1	1456977.361	2427922.502
2	1456961.138	2427932.151
3	1456944.915	2427941.799
4	1456945.143	2427955.829
5	1456945.371	2427969.858
6	1456945.599	2427983.887
7	1456945.827	2427997.917
8	1456930.829	2427998.160
9	1456915.831	2427998.404
10	1456915.603	2427984.375
11	1456915.375	2427970.345
12	1456915.147	2427956.316
13	1456914.919	2427942.286
14	1456898.392	2427933.170
15	1456881.865	2427924.053
16	1456897.070	2427937.540
17	1456891.708	2427955.637
18	1456896.345	2427973.734
19	1456896.036	2427983.881
20	1456905.728	2427994.028
21	1456915.419	2428004.175
22	1456925.110	2428014.322
23	1456914.262	2428024.682
24	1456903.415	2428035.042
25	1456893.724	2428024.895

LAYOUT POINT TABLE

POINT	NORTHING	EASTING
126	1456621.265	2428571.934
127	1456598.441	2428583.900
128	1456575.616	2428595.866
129	1456930.387	2427970.529
130	1456930.192	2427945.854
131	1456929.983	2427919.454
132	1456929.783	2427894.251
133	1456929.542	2427867.387
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136	1456861.673	2427969.669
137	1456844.620	2427951.830
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139	1456848.534	2428064.981
140	1456817.620	2428048.694
141	1456786.706	2428032.408
142	1456755.792	2428016.122
143	1456811.115	2428135.349
144	1456776.911	2428128.210
145	1456742.695	2428121.126
146	1456708.484	2428114.015
147	1456757.576	2428228.385
148	1456733.756	2428221.916
149	1456709.936	2428215.466
150	1456686.128	2428208.968

LAYOUT POINT TABLE

POINT	NORTHING	EASTING
151	1456658.033	2428201.341
152	1456715.179	2428308.943
153	1456689.667	2428305.185
154	1456664.156	2428301.427
155	1456638.645	2428297.669
156	1456613.134	2428293.911
157	1456696.492	2428401.613
158	1456670.955	2428398.840
159	1456645.218	2428396.066
160	1456619.581	2428393.293
161	1456593.945	2428390.519
162	1456642.694	2428490.131
163	1456614.677	2428504.818
164	1456563.221	2428531.792
165	1456536.956	2428545.570
166	1456511.933	2428558.678
167	1456797.976	2427697.018
168	1456746.939	2427667.412
169	1456717.569	2427718.401
170	1456672.627	2427796.435
171	1456627.694	2427874.586
172	1456593.024	2427935.076
173	1456722.905	2427782.416
174	1456687.931	2427843.262
175	1456655.968	2427988.816

LAYOUT POINT TABLE

POINT	NORTHING	EASTING
176	1456624.005	2427954.369
177	1456595.516	2428003.915
178	1456562.304	2428001.650
179	1456532.390	2428113.661
180	1456520.205	2428134.847
181	1456502.475	2428165.672
182	1456472.559	2428217.682
183	1456450.544	2428255.956
184	1456443.809	2428270.137
185	1456439.376	2428285.198
186	1456528.348	2428121.157
187	1456529.639	2428106.522
188	1456529.954	2428091.526
189	1456527.279	2428076.766
190	1456521.719	2428062.835
191	1456513.499	2428050.288
192	1456502.946	2428039.628
193	1456485.721	2428026.688
194	1456461.655	2428008.776
195	1456446.555	2427997.536
196	1456430.360	2427987.943
197	1456412.655	2427981.548
198	1456394.066	2427978.585
199	1456375.251	2427979.159
200	1456356.877	2427983.247

LAYOUT POINT TABLE

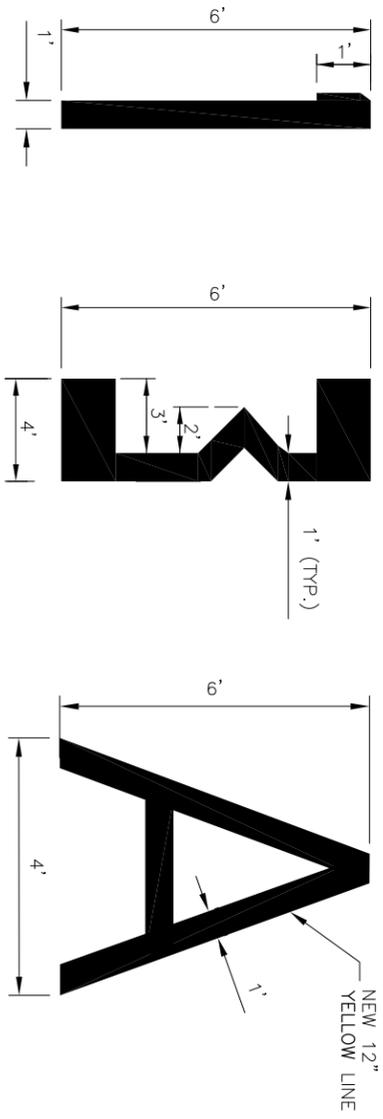
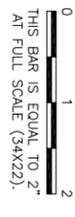
POINT	NORTHING	EASTING
201	1456339.595	2427990.707
202	1456324.016	2428001.274
203	1456307.212	2428019.924
204	1456586.237	2428016.705
205	1456574.485	2428027.266
206	1456560.781	2428035.131
207	1456545.733	2428039.951
208	1456530.009	2428041.511
209	1456514.308	2428039.742
210	1456499.326	2428034.723
211	1456432.580	2427984.926
212	1456421.215	2427969.920
213	1456412.864	2427953.050
214	1456407.823	2427934.914
215	1456406.269	2427916.155
216	1456408.259	2427997.436
217	1456412.546	2427982.430
218	1456419.113	2427864.789
219	1456439.100	2427811.100

LAYOUT POINT TABLE

POINT	NORTHING	EASTING
101	1456693.574	2428416.385
102	1456679.624	2428414.876
103	1456665.674	2428413.367
104	1456654.583	2428428.640
105	1456643.493	2428443.912
106	1456517.576	2428485.157
107	1456540.401	2428473.191
108	1456563.225	2428461.225
109	1456577.904	2428472.130
110	1456592.583	2428483.034
111	1456600.539	2428478.863
112	1456608.495	2428474.692
113	1456611.189	2428479.831
114	1456613.883	2428484.970
115	1456630.341	2428476.342
116	1456646.798	2428467.714
117	1456655.131	2428483.611
118	1456663.465	2428499.507
119	1456647.008	2428508.135
120	1456630.551	2428516.763
121	1456633.245	2428521.992
122	1456635.939	2428527.041
123	1456627.983	2428531.212
124	1456620.028	2428535.382
125	1456620.646	2428533.658

PE094

REVISIONS		
NUMBER	BY	DATE



J-LINE NUMERALS (TYP.)

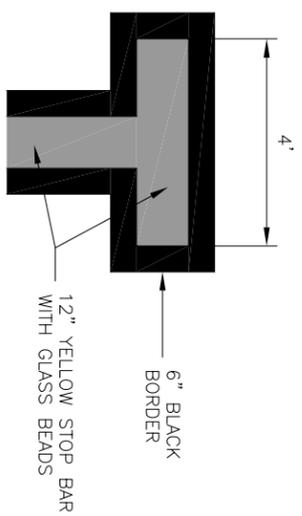
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TAXIWAY CENTERLINE CONTINUOUS (YELLOW)

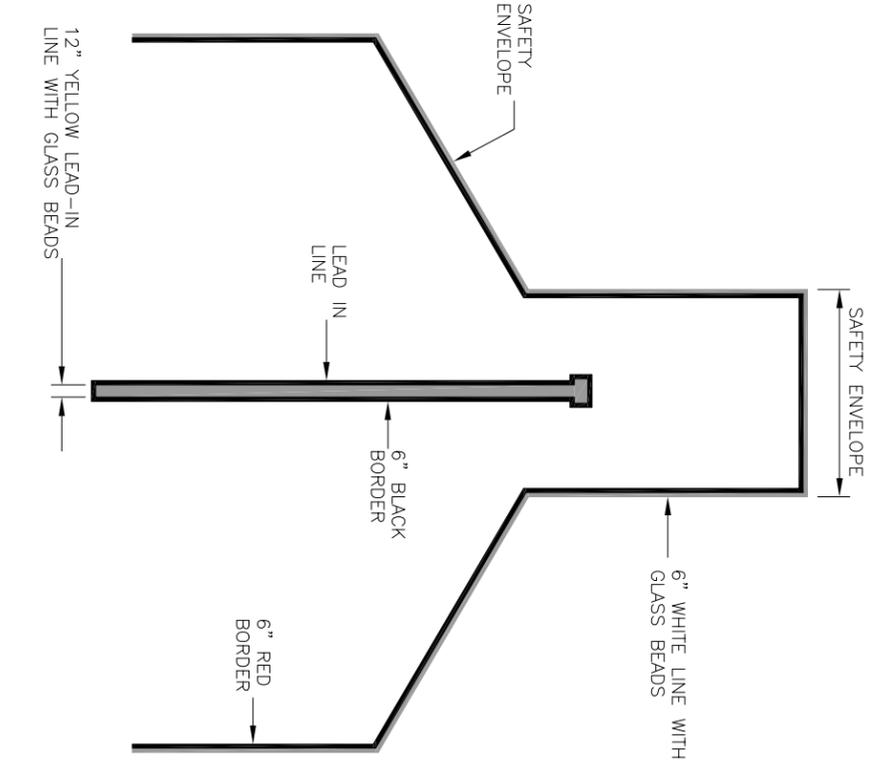
N.T.S.

NOTE:
 1. ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE MEDIA & 6" BLACK BORDER.
 2. BLACK BORDER SHALL NOT REQUIRE REFLECTIVE MEDIA.



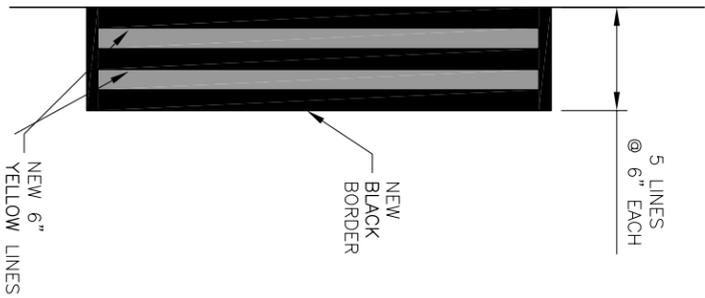
TYPICAL STOP BAR FOR CONCRETE SURFACE

N.T.S.



TYPICAL LEAD-IN AND SAFETY ENVELOPE FOR CONCRETE SURFACE

N.T.S.



TAXIWAY EDGE MARKING CONTINUOUS (YELLOW)

N.T.S.

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**WIDEN AIR CARRIER APRON
 MARKING DETAILS**

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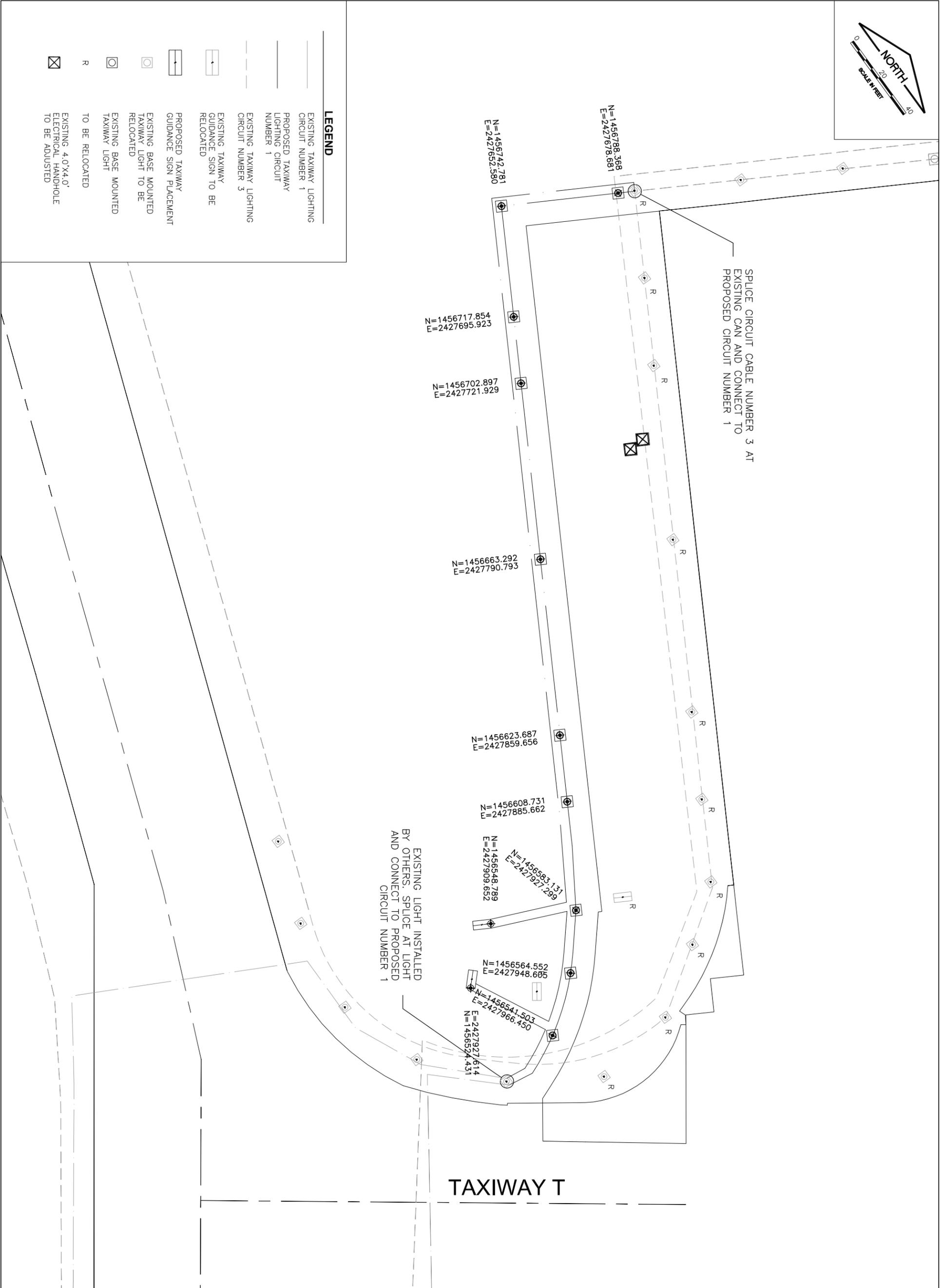
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 PEORIA INTERNATIONAL AIRPORT**

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JOB No:	10061-04-00
IL PROJ. NO. PIA-4180	
APP PROJ. NO. 3-17-0080-XX	



- LEGEND**
- EXISTING TAXIWAY LIGHTING CIRCUIT NUMBER 1
 - PROPOSED TAXIWAY LIGHTING CIRCUIT NUMBER 1
 - EXISTING TAXIWAY LIGHTING CIRCUIT NUMBER 3
 - EXISTING TAXIWAY GUIDANCE SIGN TO BE RELOCATED
 - PROPOSED TAXIWAY GUIDANCE SIGN PLACEMENT
 - EXISTING BASE MOUNTED TAXIWAY LIGHT TO BE RELOCATED
 - EXISTING BASE MOUNTED TAXIWAY LIGHT
 - R TO BE RELOCATED
 - ⊠ EXISTING 4.0'x4.0' ELECTRICAL HANDHOLE TO BE ADJUSTED



K:\Peoria\100610\A\Down Sheets
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 UP/DRAWN BY: Andrew Bodine
 PLOT DATE: 5/24/2012 1:37 PM
 PIA-Base-Existing

PE094

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NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
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PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS

WIDEN AIR CARRIER APRON
ELECTRICAL PLAN

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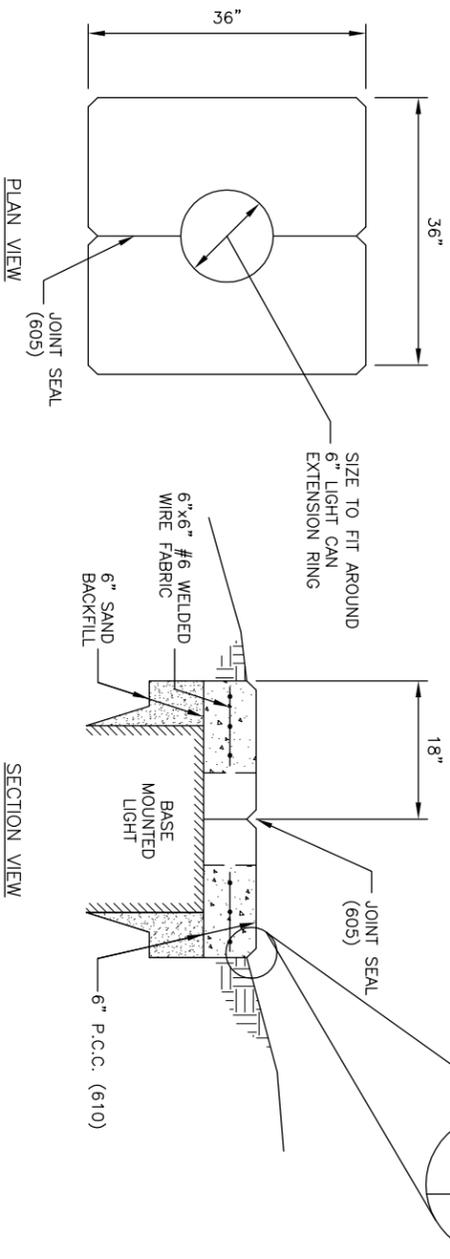
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AIP PROJ. NO. 3-17-0080-XX	
SHEET	21 OF 34 SHEETS

1/2" CHAMFER ON ALL P.C.C. EDGES

PE094

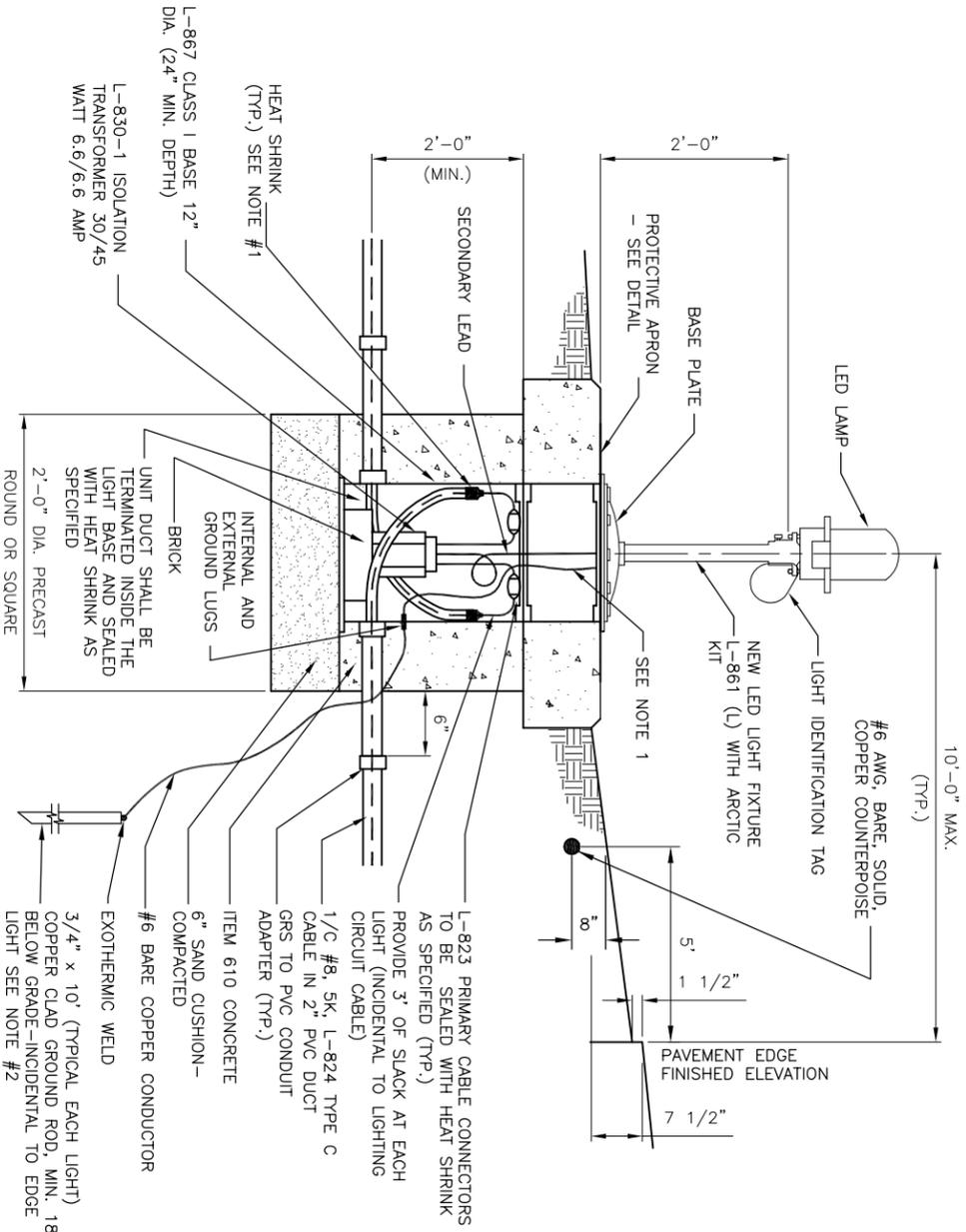
NUMBER	BY	DATE

0 1 2
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PREFORMED PROTECTIVE APRON DETAIL

N.T.S.



BASE MOUNTED EDGE LIGHTS

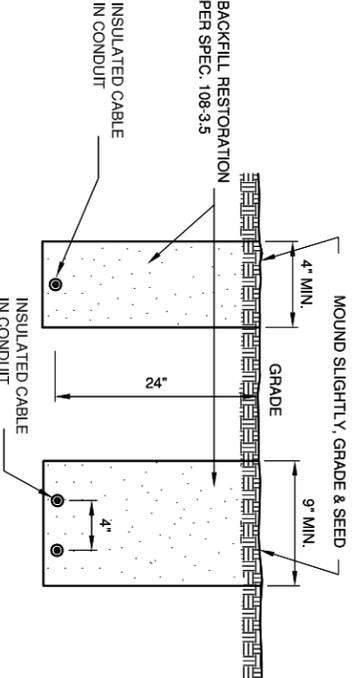
N.T.S.

LIGHT NOTES

1. THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.

NOTES

1. DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
2. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
3. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO ITEM 108.

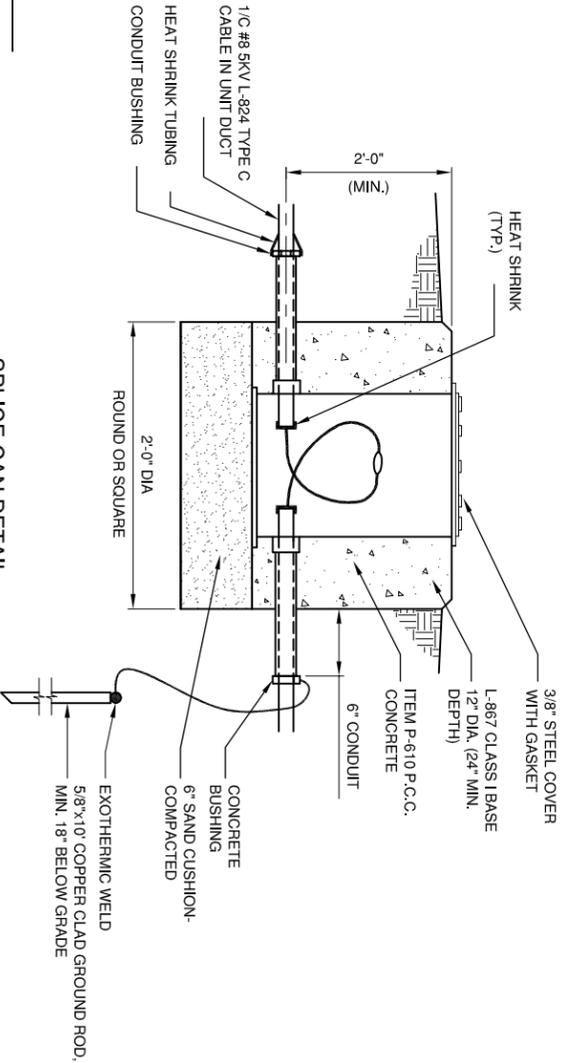


TRENCH DETAIL

N.T.S.

SPLICE CAN DETAIL

N.T.S.



UNIT DUCT SHALL BE TERMINATED AT THE L-823 CONNECTORS AND SEALED WITH HEAT SHRINK AS SPECIFIED.

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 PEORIA, ILLINOIS**

WIDEN AIR CARRIER APRON

ELECTRICAL DETAILS SHEET 1

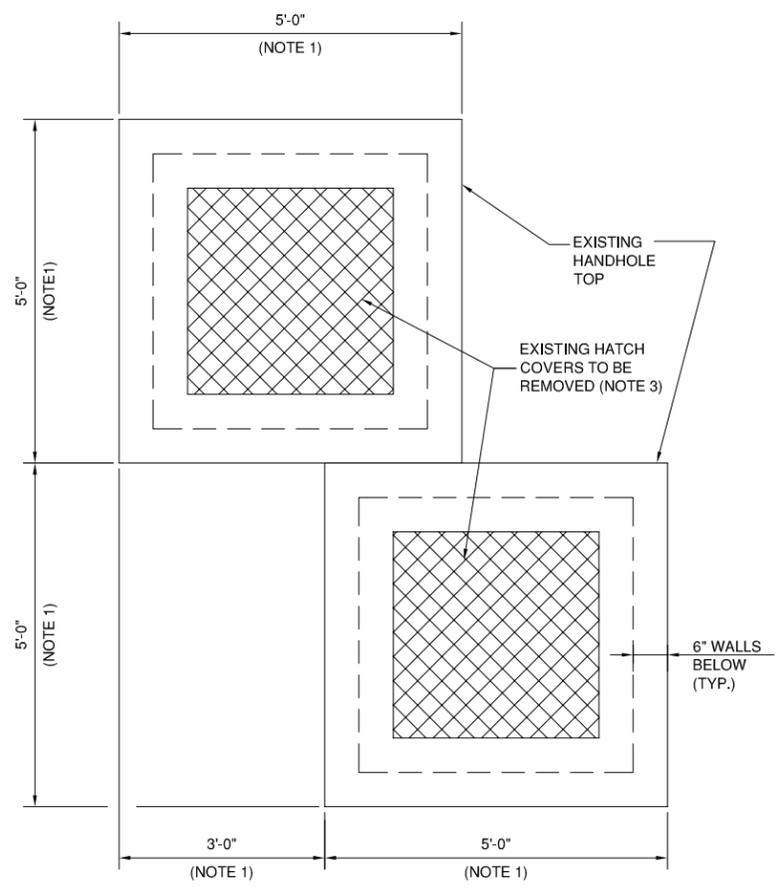
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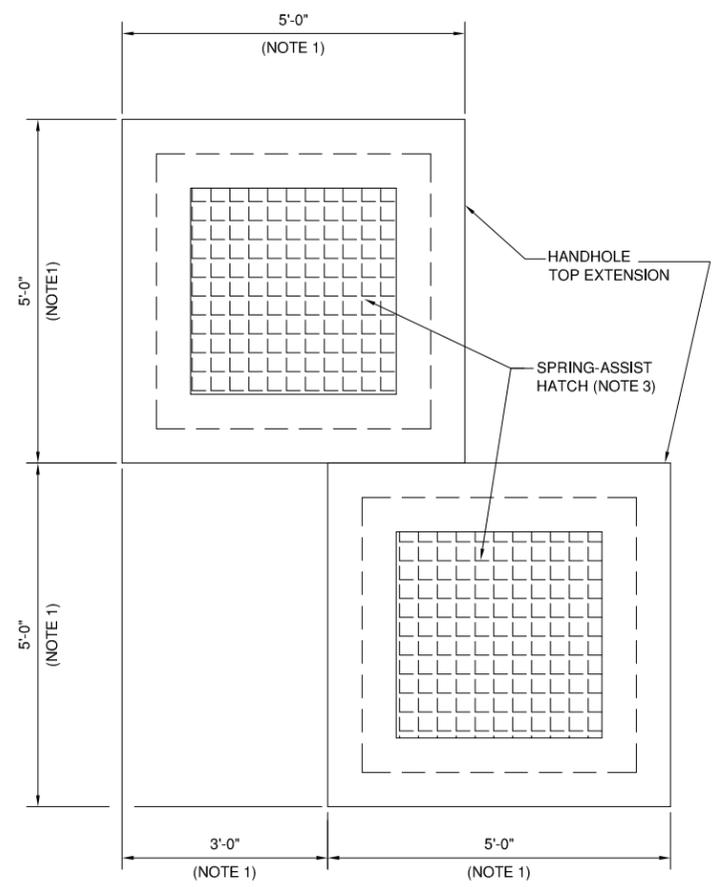
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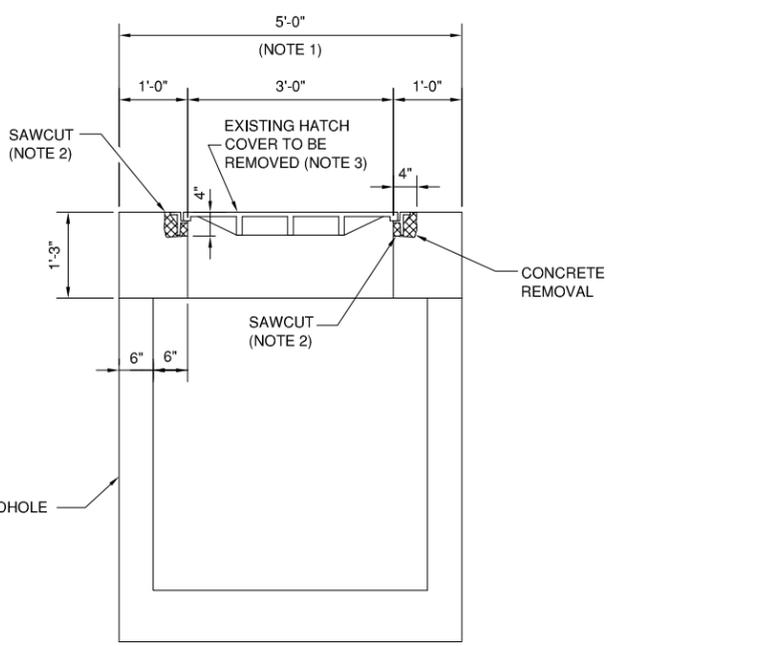
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JOB No.:	10061-04-00
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APP. PROJ. NO. 3-17-0080-XX	
SHEET	22 OF 34 SHEETS



PLAN

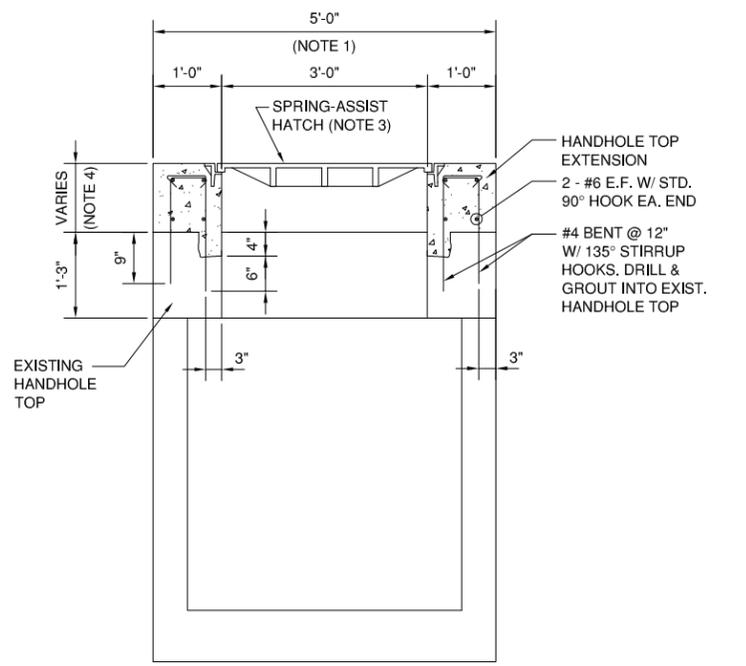


PLAN
N.T.S.



SECTION

REMOVAL
3/4" = 1'-0"



SECTION

MODIFICATIONS
3/4" = 1'-0"

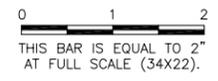
NOTES

- DIMENSIONS FROM EXISTING HANDHOLE SHOP DRAWINGS ARE SHOWN AND VARY SLIGHTLY FROM SURVEY DATA. CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS PRIOR TO SHOP DRAWING PREPARATION AND FABRICATION.
- CONCRETE SHALL BE SAWCUT FOR CONCRETE REMOVAL. SAWCUT SHALL BE 1 1/2" DEEP OR TO FACE OF REINFORCING STEEL, WHICHEVER IS LESS. EXISTING REINFORCING STEEL SHALL NOT BE DAMAGED AND SHALL REMAIN FOR INCORPORATION INTO NEW WORK.
- EXISTING FRAMES AND COVERS ARE SPRING-ASSIST AIRCRAFT RATED. EXISTING FRAMES AND COVERS SHALL BE REMOVED IN THEIR ENTIRETY. IF EXISTING FRAMES AND COVERS ARE NOT DAMAGED, THEY MAY BE RE-USED IN THE NEW WORK. IF NEW FRAMES AND COVERS ARE USED, THEY SHALL BE NEENAH R-3498-R2S, OR EQUAL.
- HANDHOLE TOP EXTENSION THICKNESS VARIES WITH SLOPE OF TAXIWAY. SEE SHEET 11 FOR STAKING PLAN. CONTRACTOR SHALL SURVEY TOP OF EXISTING HANDHOLES TO ESTABLISH THICKNESSES OF HANDHOLE EXTENSIONS.

PE094

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 PEORIA, ILLINOIS

WIDEN AIR CARRIER APRON
 ELECTRICAL DETAILS SHEET 3

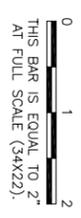
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AIP PROJ. NO.	3-17-0080-XX
SHEET	24 OF 34 SHEETS

PE094

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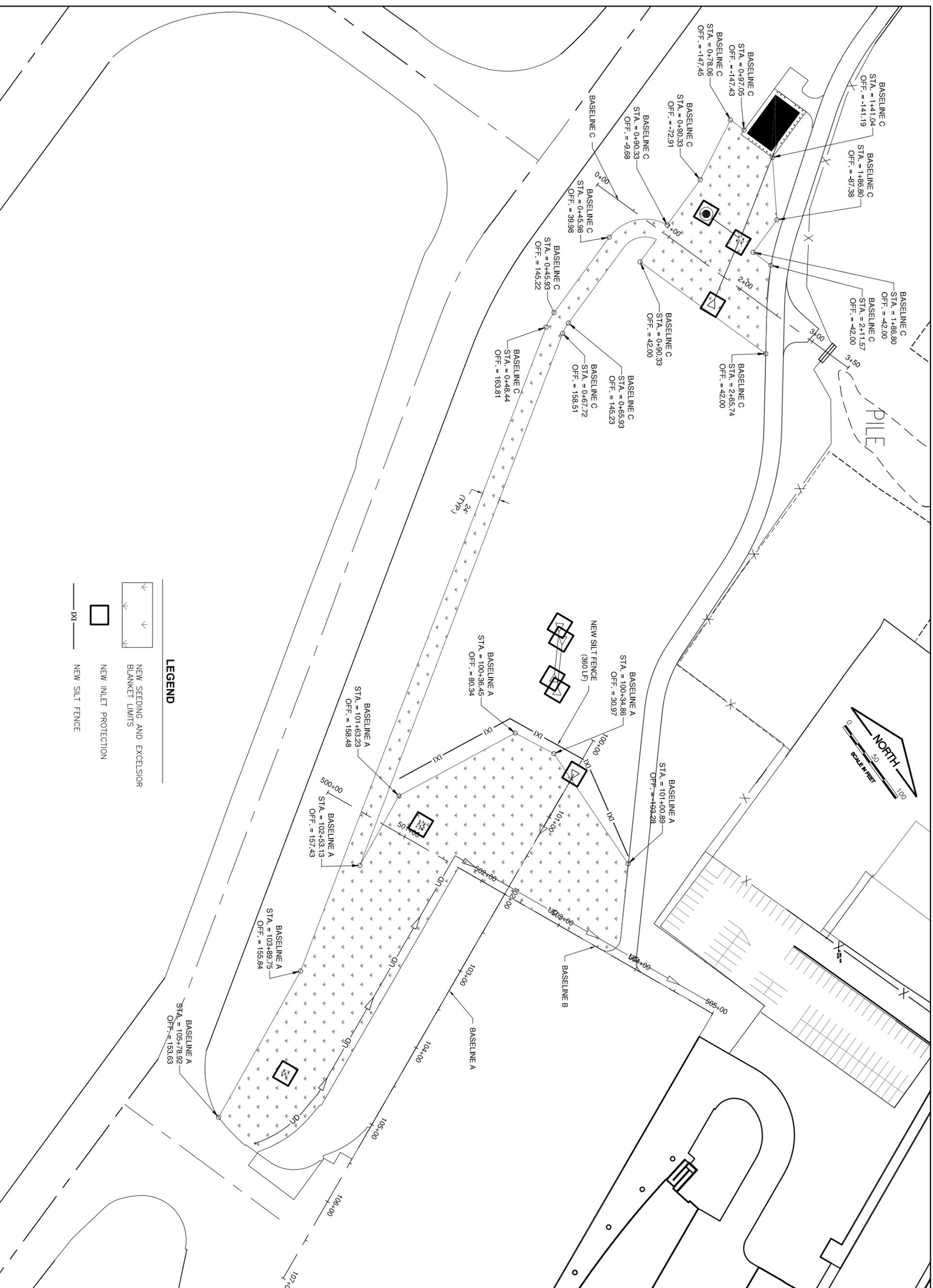


**GENERAL WAYNE A. DOWNING
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 PEORIA, ILLINOIS**

**WIDEN AIR CARRIER APRON
 TURFING AND EROSION CONTROL PLAN**

LEGEND

- NEW SEEDING AND EXCELSIOR BLANKET LIMITS
- NEW INLET PROTECTION
- NEW SILT FENCE



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APP PROJ. NO. 3-17-0080-XX	

SHEET 25 OF 34 SHEETS

PE094

REVISIONS	BY	DATE

NUMBER	BY	DATE

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 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

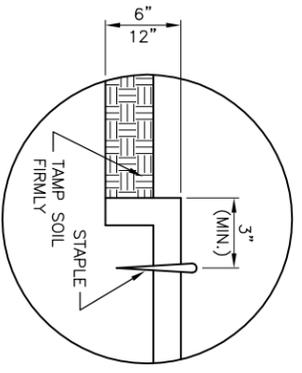
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 PEORIA, ILLINOIS**

WIDEN AIR CARRIER APRON

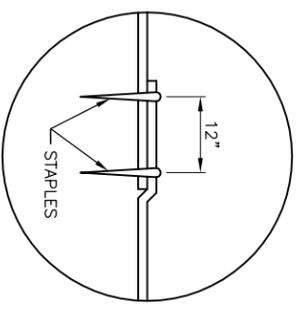
EROSION CONTROL DETAILS

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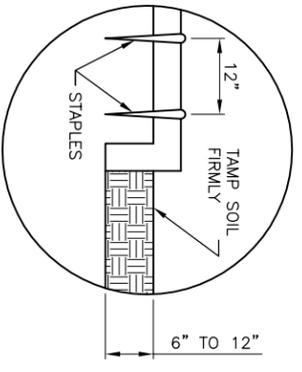
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APP PROJ. NO. 3-17-0080-XX	
SHEET 26 OF 34 SHEETS	



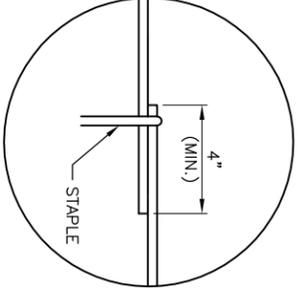
DETAIL 1 - TERMINAL FOLD



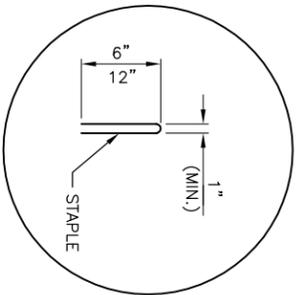
DETAIL 2 - JUNCTION SLOT



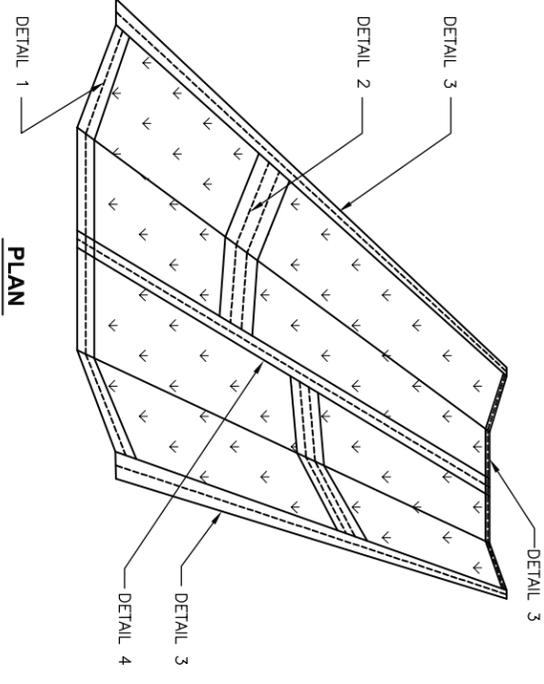
DETAIL 3 - ANCHOR SLOT



DETAIL 4 - LAP JOINT



DETAIL 5 - STAPLE DETAIL

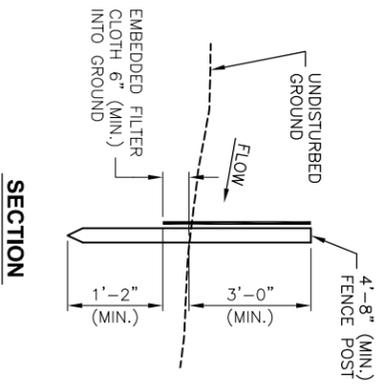


EXCELSIOR BLANKET DETAILS

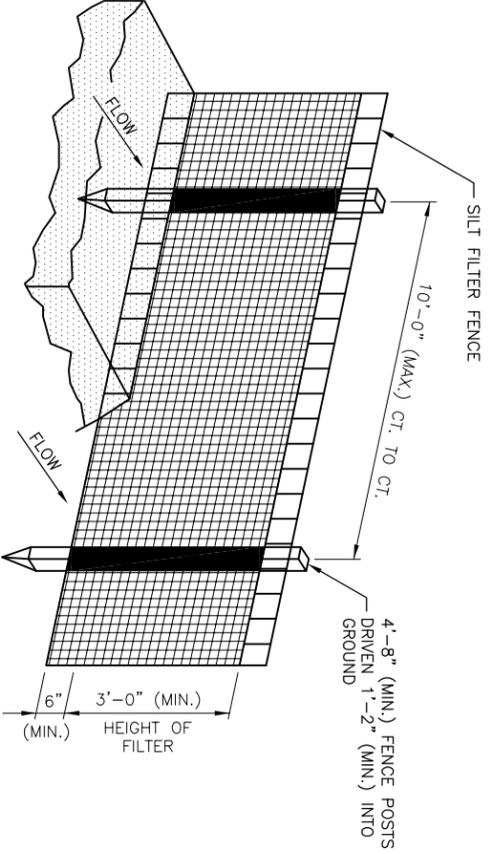
N.T.S.

EXCELSIOR BLANKET NOTES

1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINALS ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.



SECTION



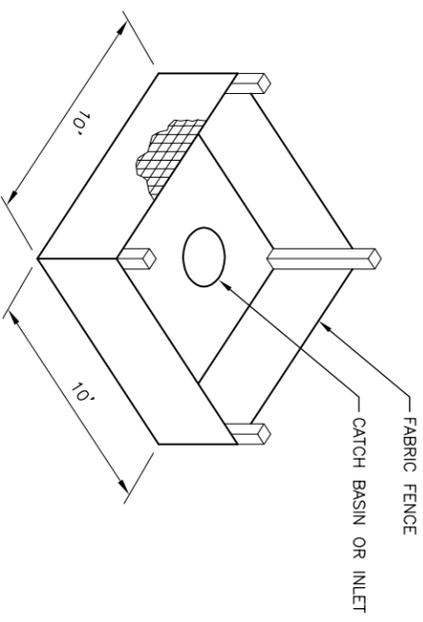
PERSPECTIVE VIEW

EROSION CONTROL FABRIC FENCE DETAILS

N.T.S.

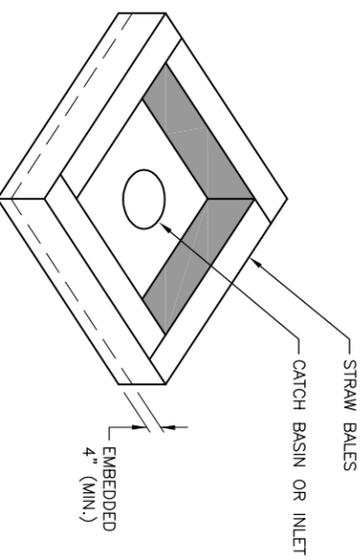
EROSION CONTROL FABRIC FENCE NOTES

1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.



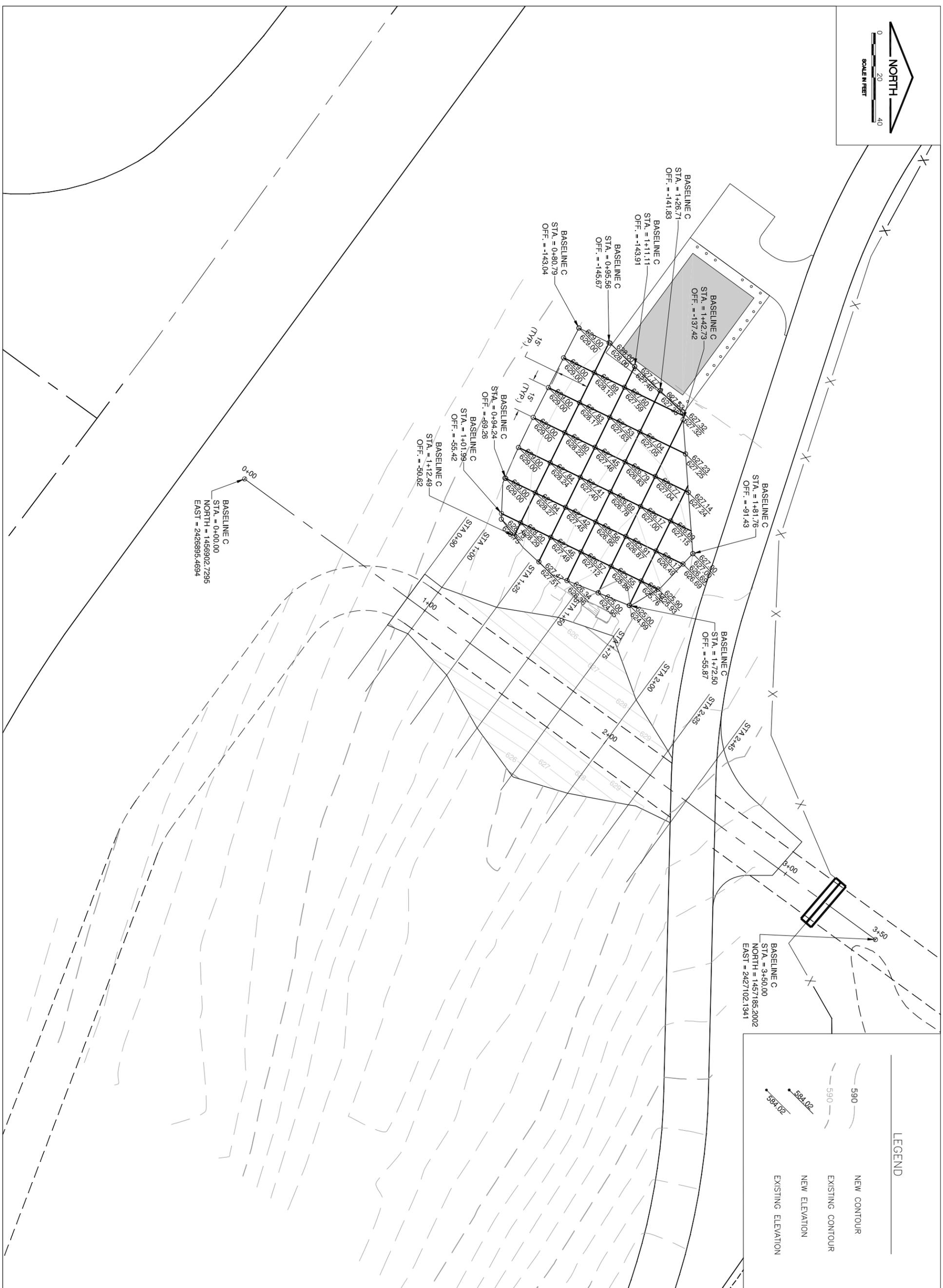
INLET PROTECTION WITH FABRIC

N.T.S.



INLET PROTECTION WITH STRAW BALES

N.T.S.



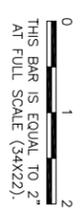
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LEGEND

- NEW CONTOUR
- EXISTING CONTOUR
- NEW ELEVATION
- EXISTING ELEVATION

PE094

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**WIDEN AIR CARRIER APRON
 GRADING PLAN 2**

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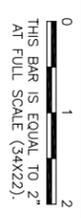
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APPROVED BY:	CET
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SHEET 28 OF 34 SHEETS

PE094

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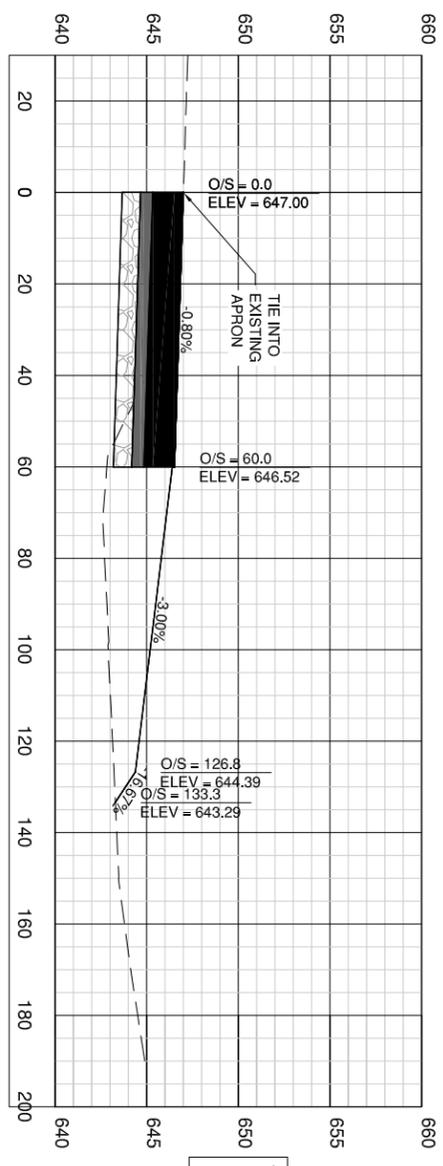
**WIDEN AIR CARRIER APRON
 APRON EXPANSION CROSS SECTIONS 1**

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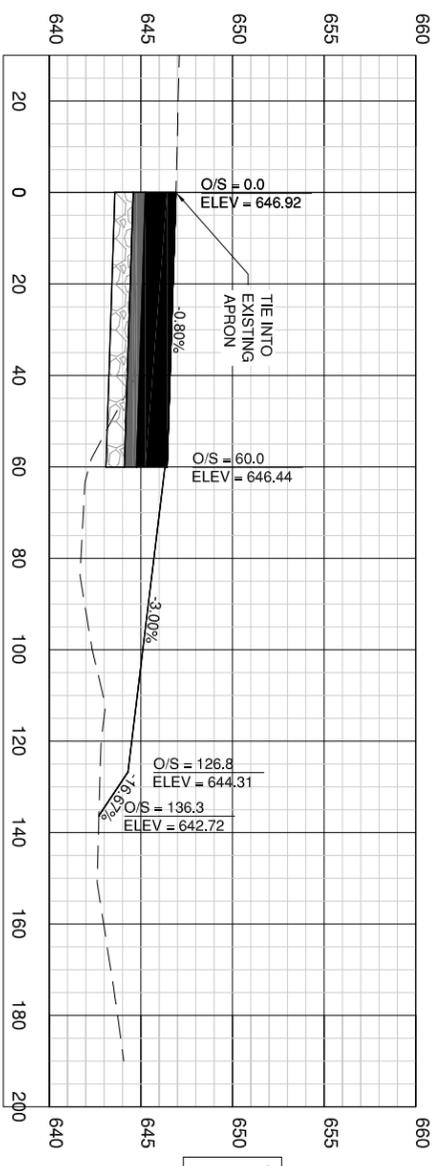


**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT**

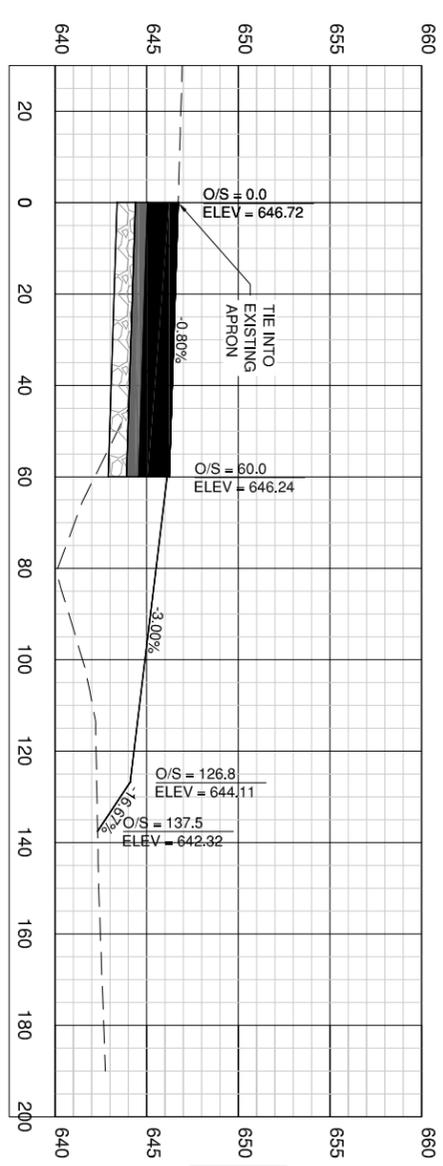
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JOB NO.:	10061-04-00
IL PROJ. NO. PIA-4180	
APP PROJ. NO. 3-17-0080-XX	



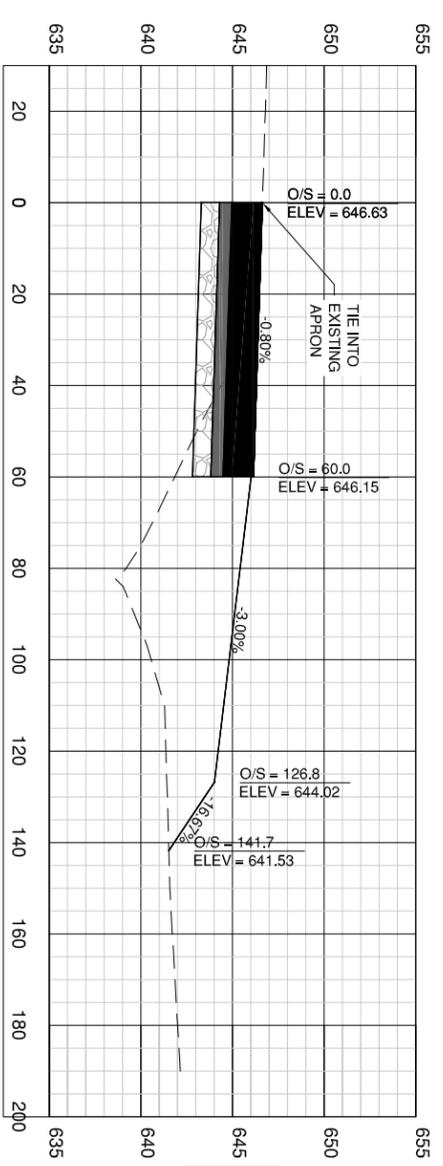
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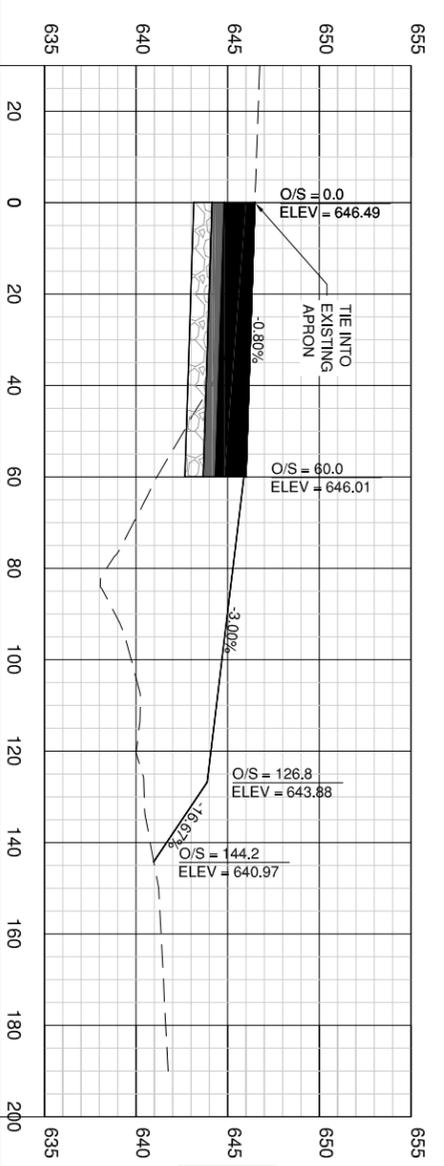
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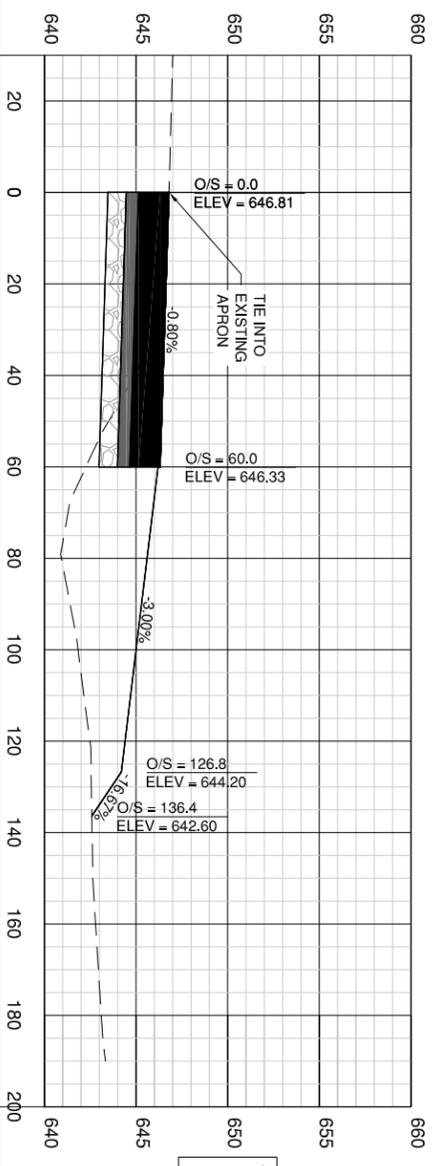
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102
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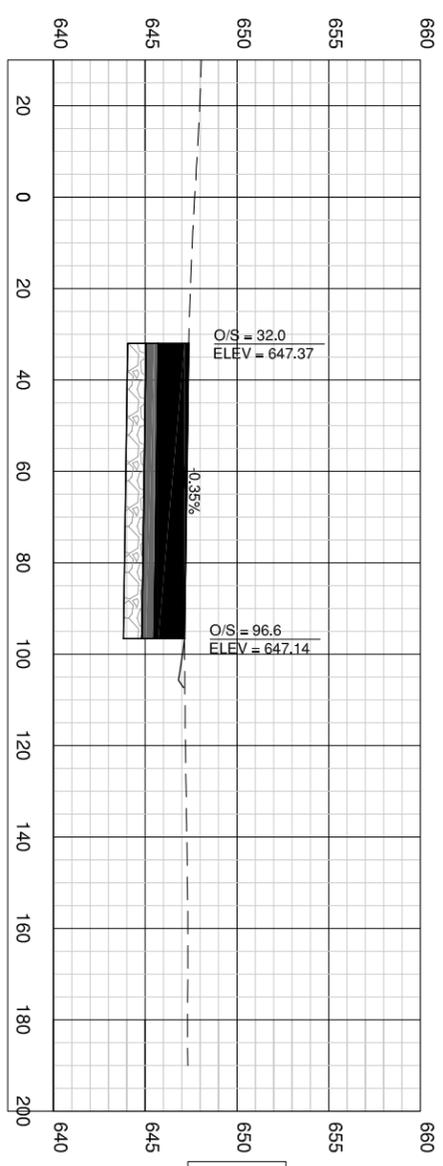


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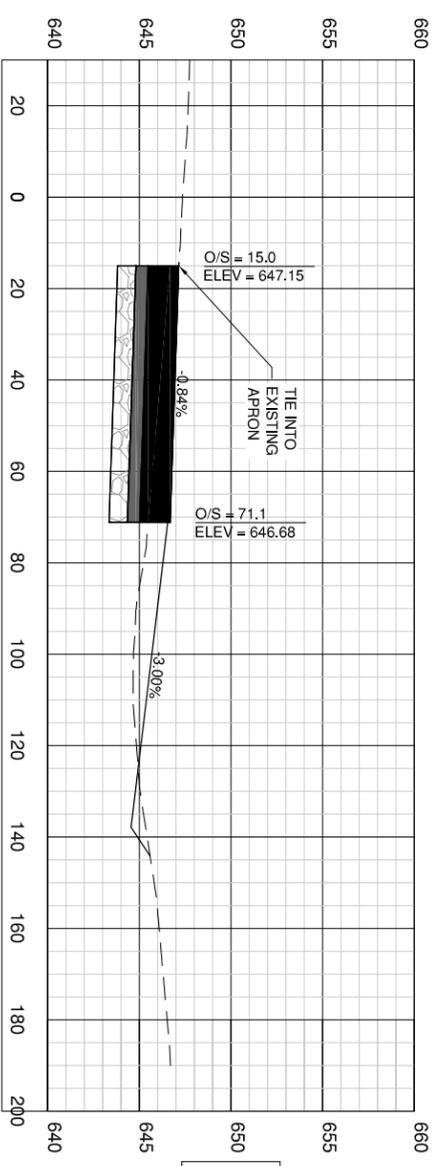
PE094

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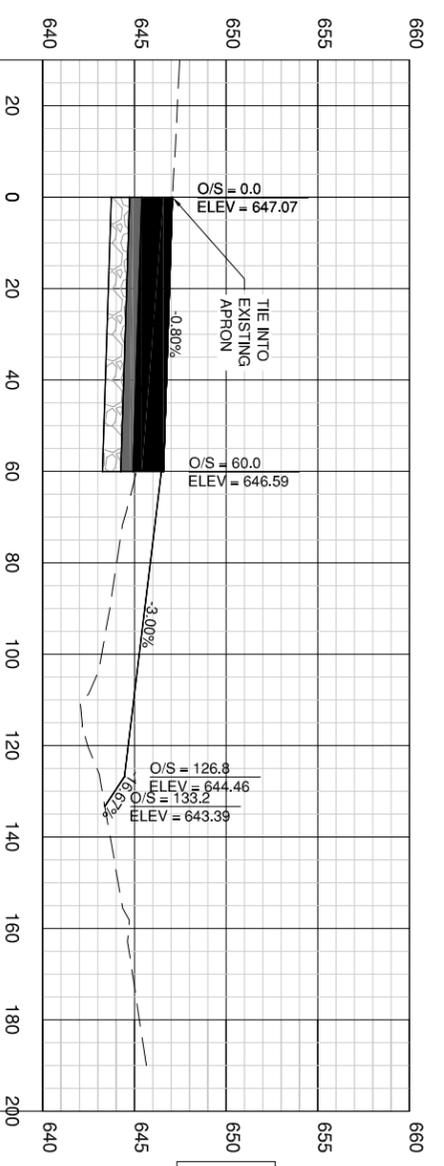
NUMBER	BY	DATE



106
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105
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105
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 PEORIA, ILLINOIS**

**WIDEN AIR CARRIER APRON
 APRON EXPANSION CROSS SECTIONS 2**

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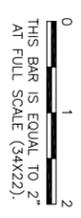
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JOB No:	10061-04-00
IL PROJ. NO. PIA-4180	
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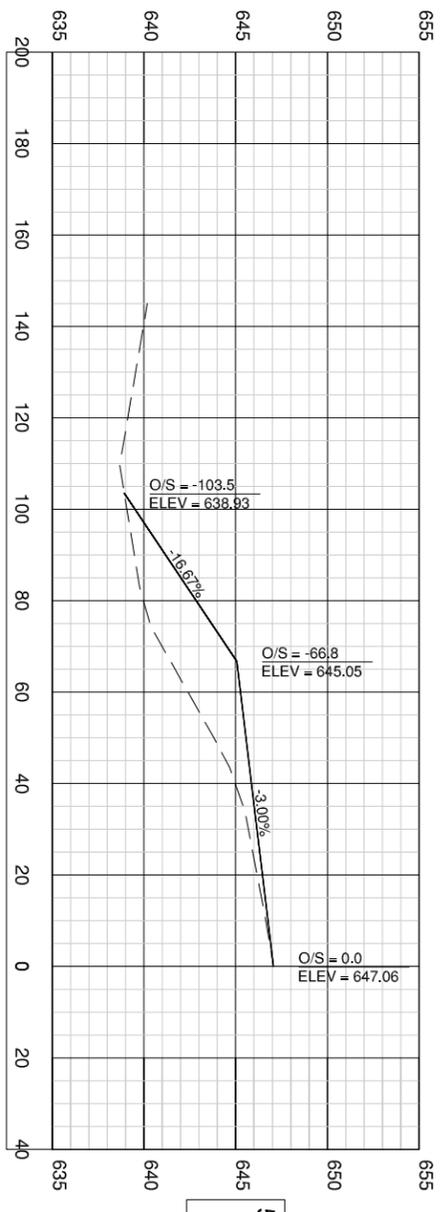
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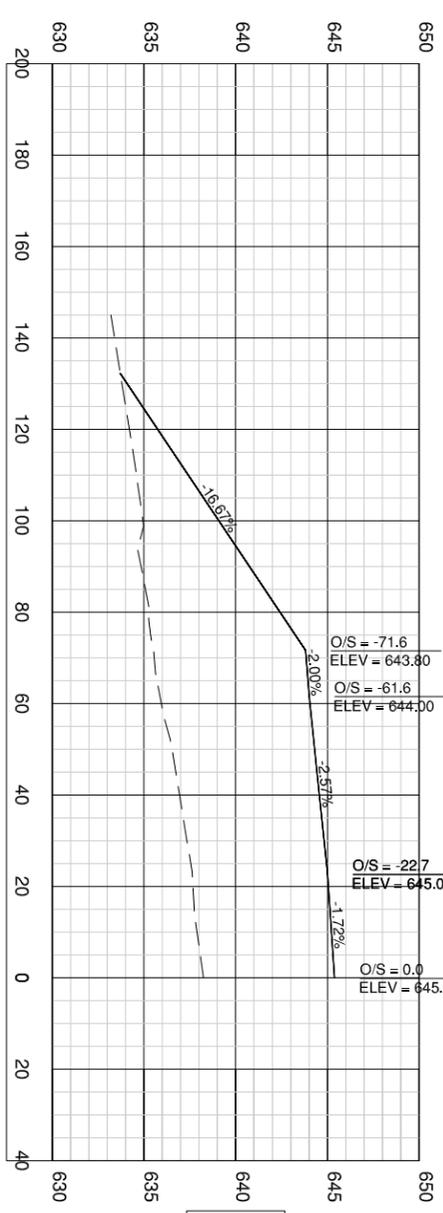
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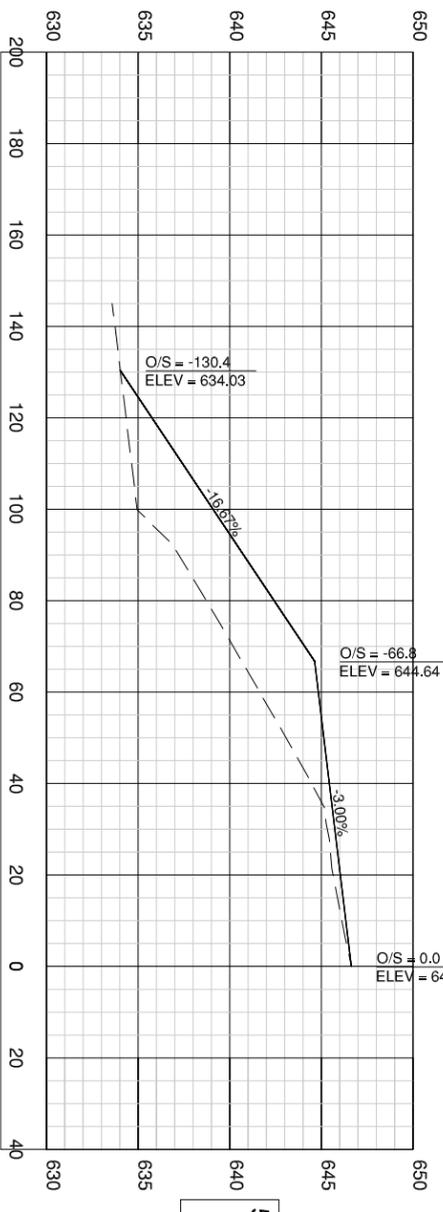
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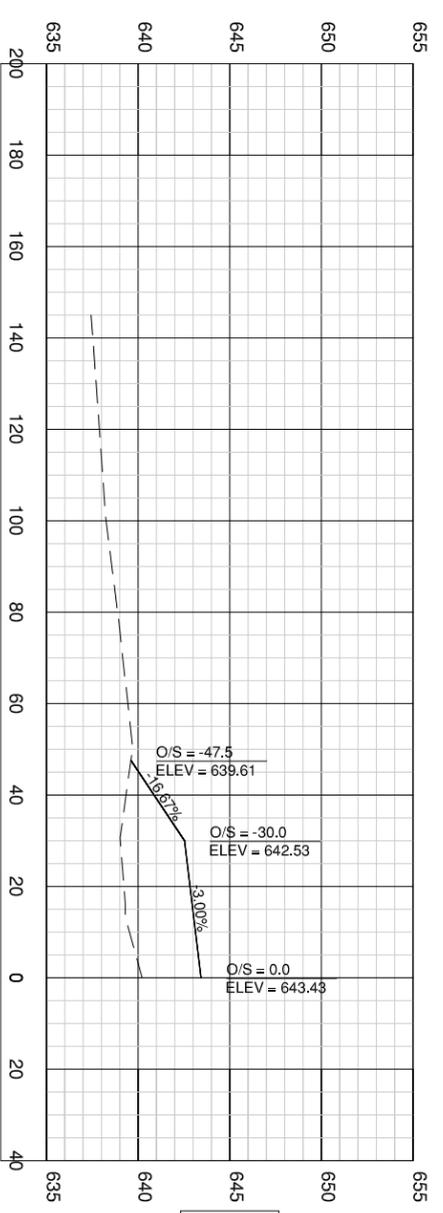
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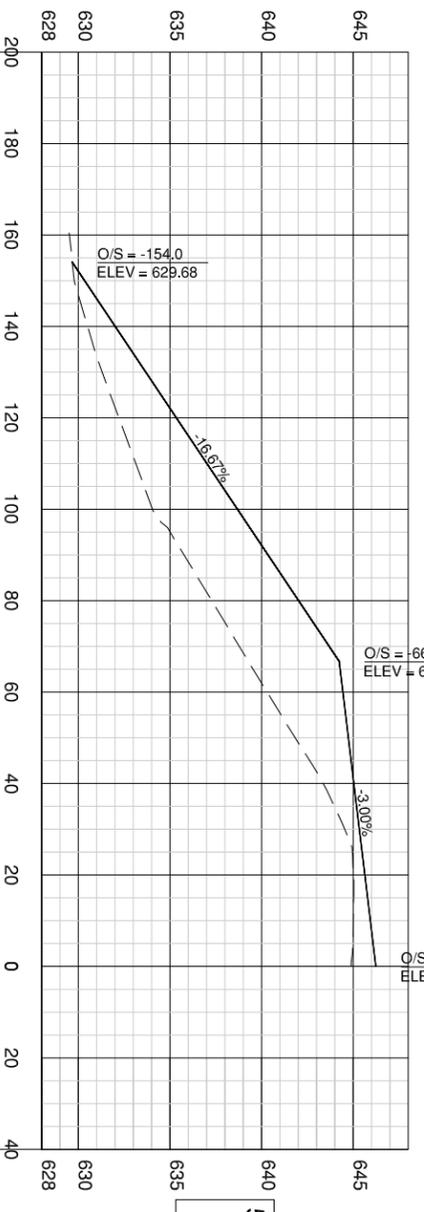
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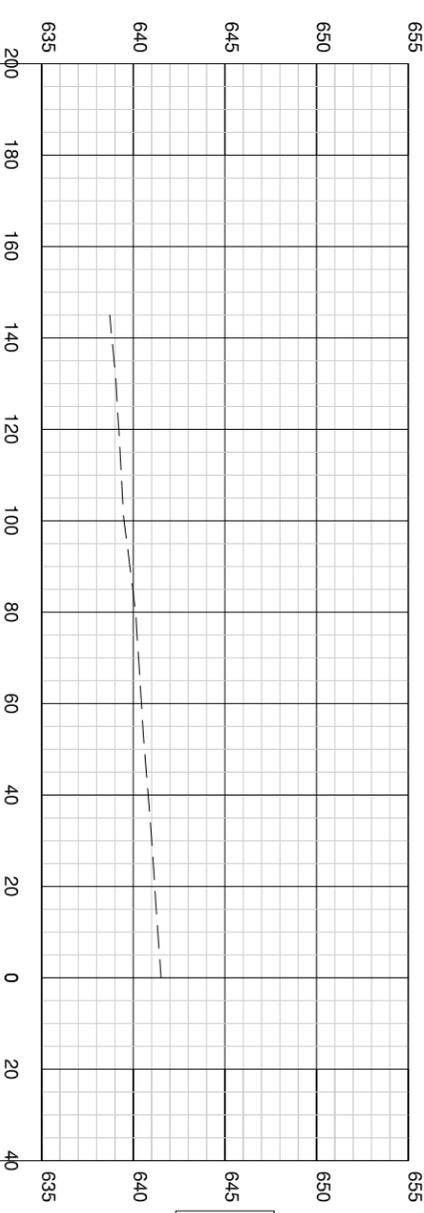
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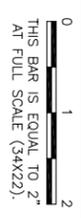


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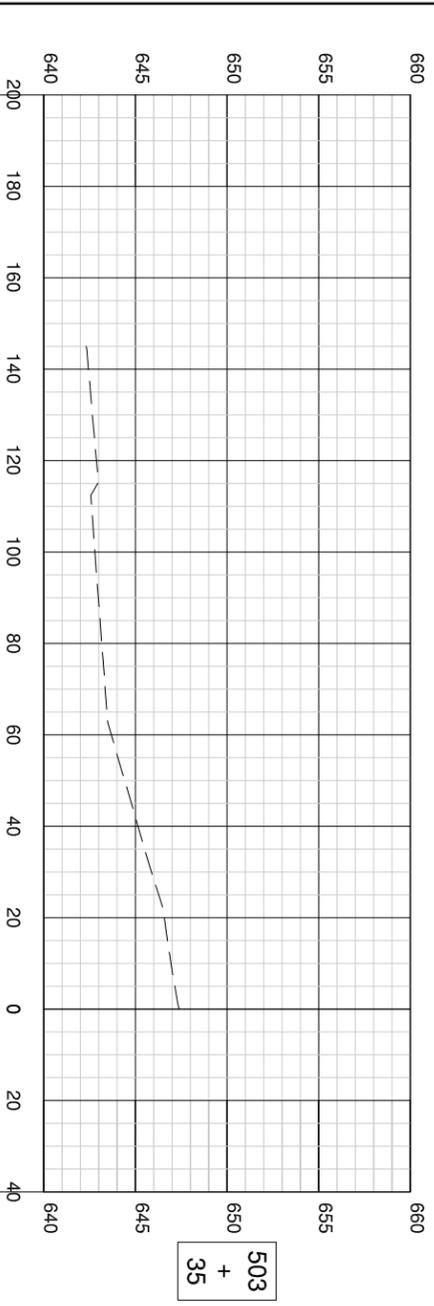
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**WIDEN AIR CARRIER APRON
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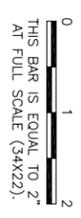
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SHEET 32 OF 34 SHEETS

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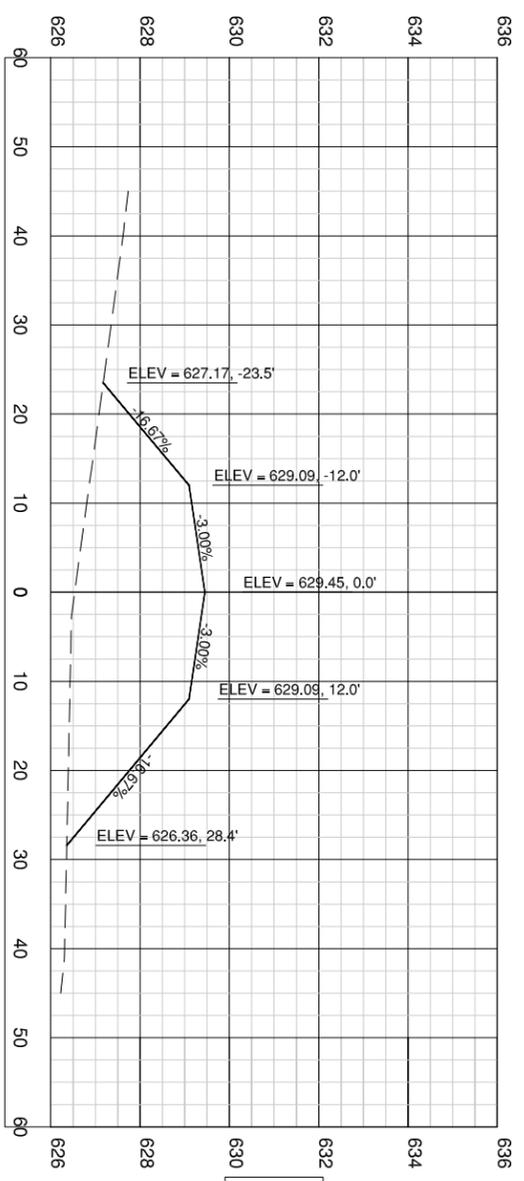


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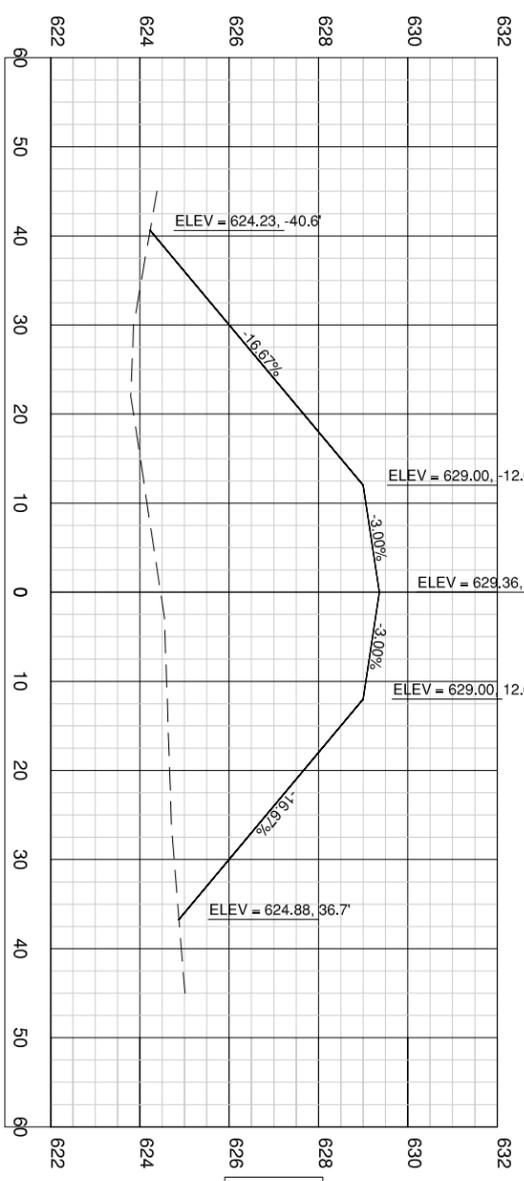


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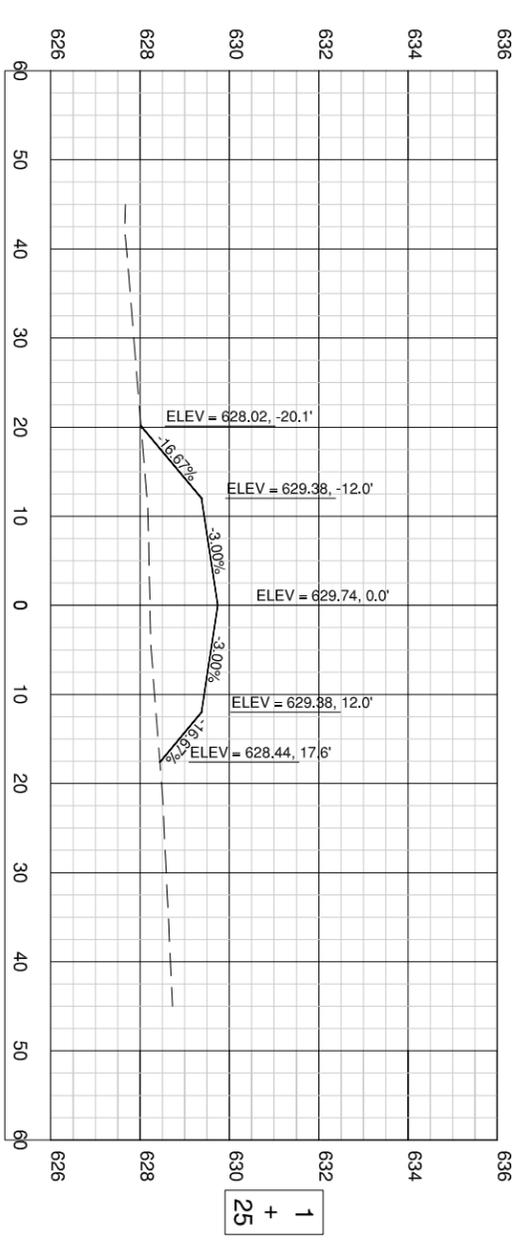
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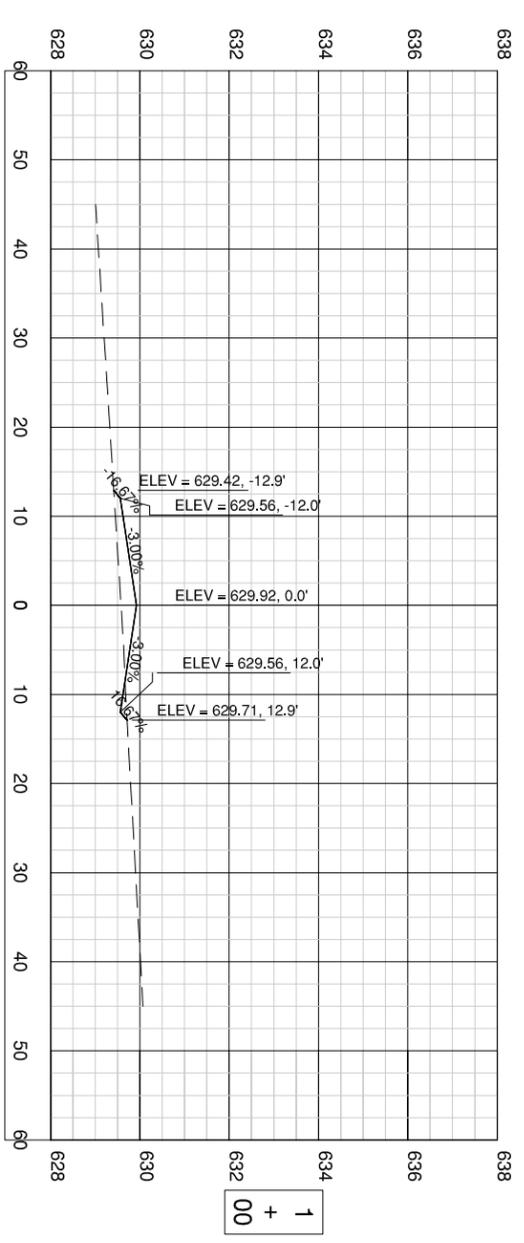
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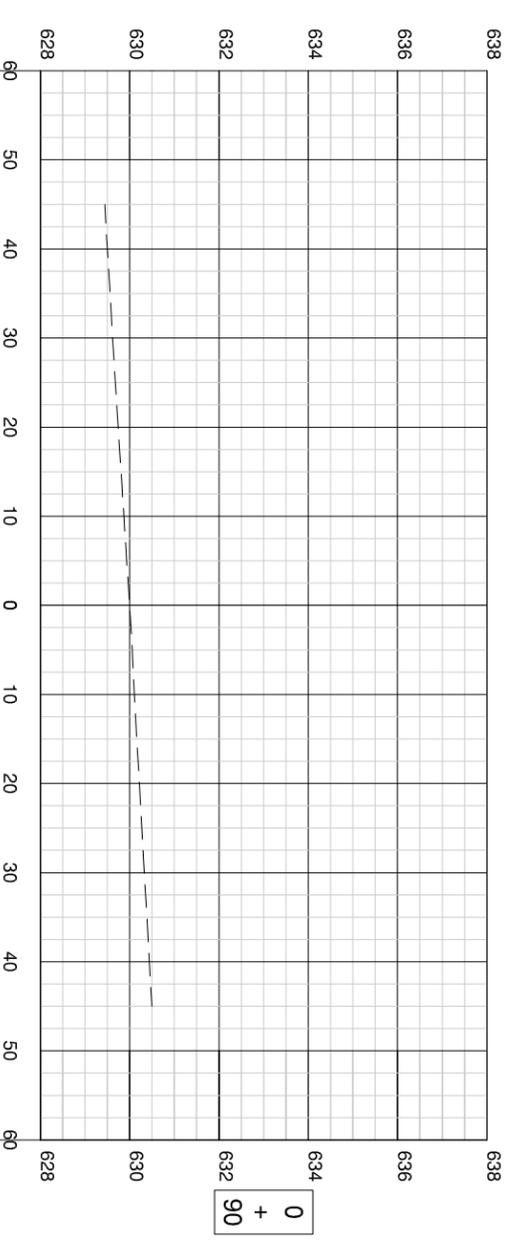
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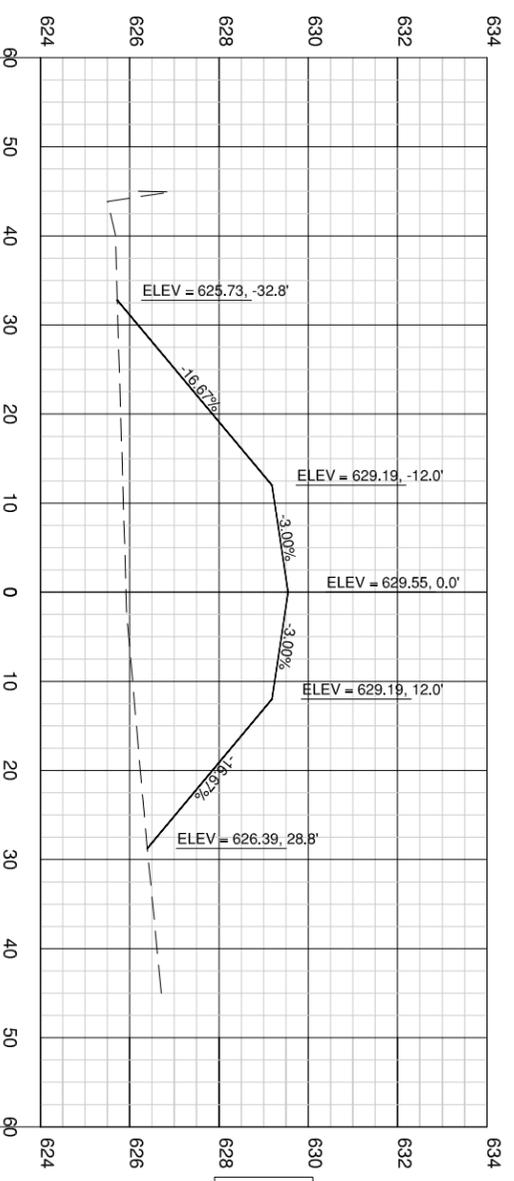
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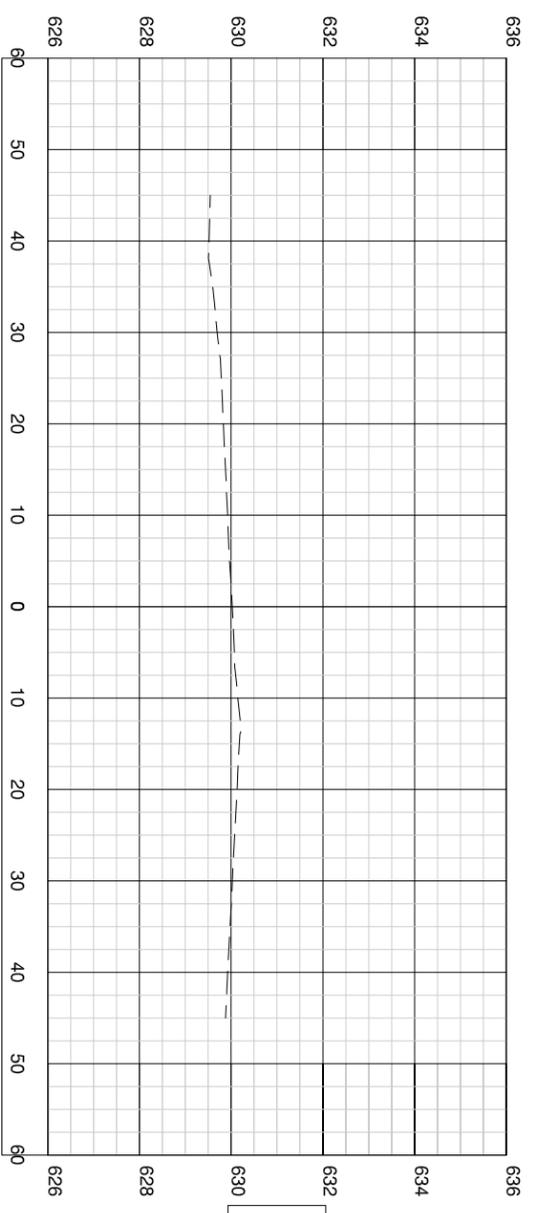
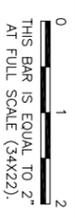


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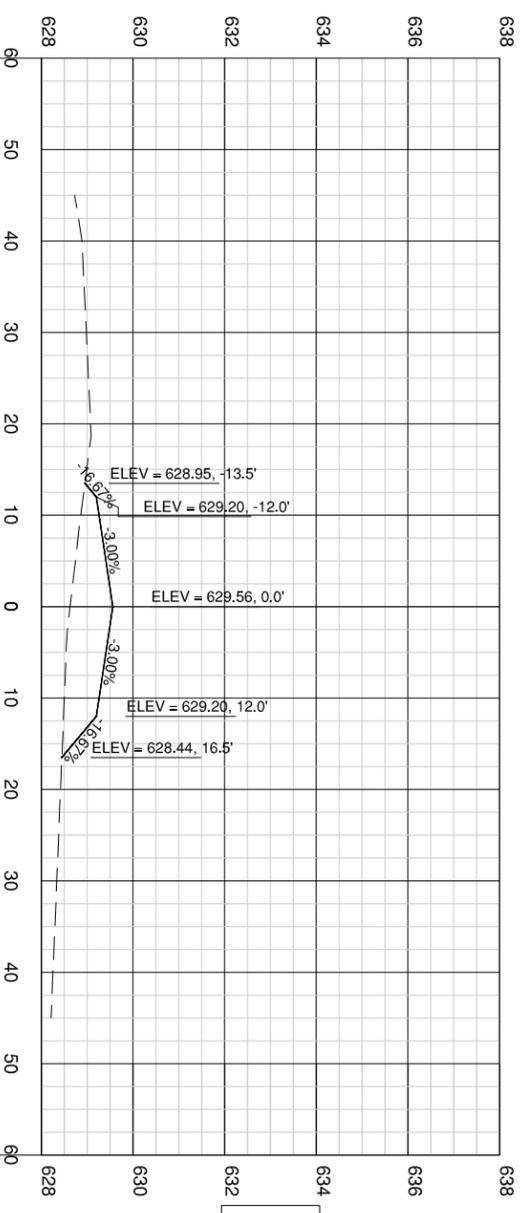
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2 + 44



2 + 25

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 HAUL ROAD CROSS SECTIONS 2**

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