

LETTING ITEM NO. 19A
LETTING DATE: JUNE 15, 2012

CONSTRUCTION PLANS
FOR

PE093
TOTAL SHEETS: 62

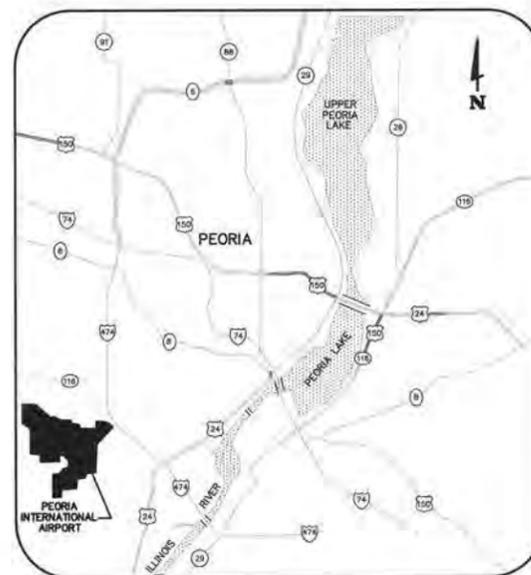
GENERAL WAYNE A. DOWNING PEORIA INTERNATIONAL AIRPORT

A.I.P. PROJECT NO.: 3-17-0080-XX
ILLINOIS PROJECT NO.: PIA-4179

REHABILITATE AIRFIELD LIGHTING

Exp. 11-30-2012
CHRISTOPHER B. GROTT
062-056232
LICENSED
PROFESSIONAL
ENGINEER
OF
ILLINOIS
Chris B. Grott
5/24/2012

APRIL 20, 2012



LOCATION MAP



SITE PLAN

811 Know what's below.
Call before you dig. COMMON GROUND ALLIANCE
www.call811.com or
Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE
RESULTS IN THE RELEASE OF NATURAL GAS.

MAXIMUM EQUIPMENT HEIGHT = 25'
GROUND FREQUENCY 121.85

GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT

TOWNSHIP: 8 NORTH
RANGE: 7 EAST
COUNTY: PEORIA
TOWNSHIP: LIMESTONE

GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT

GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT

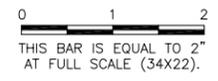
APPROVED *Hersh Olson*
DATE May 22, 2012

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SUBMITTED BY *Chris B. Grott*
DATE 5/24/2012
CMT JOB NUMBER 11061-07-00

PE093

REVISIONS		
NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**
**REHABILITATE AIRFIELD LIGHTING
 SITE PLAN**

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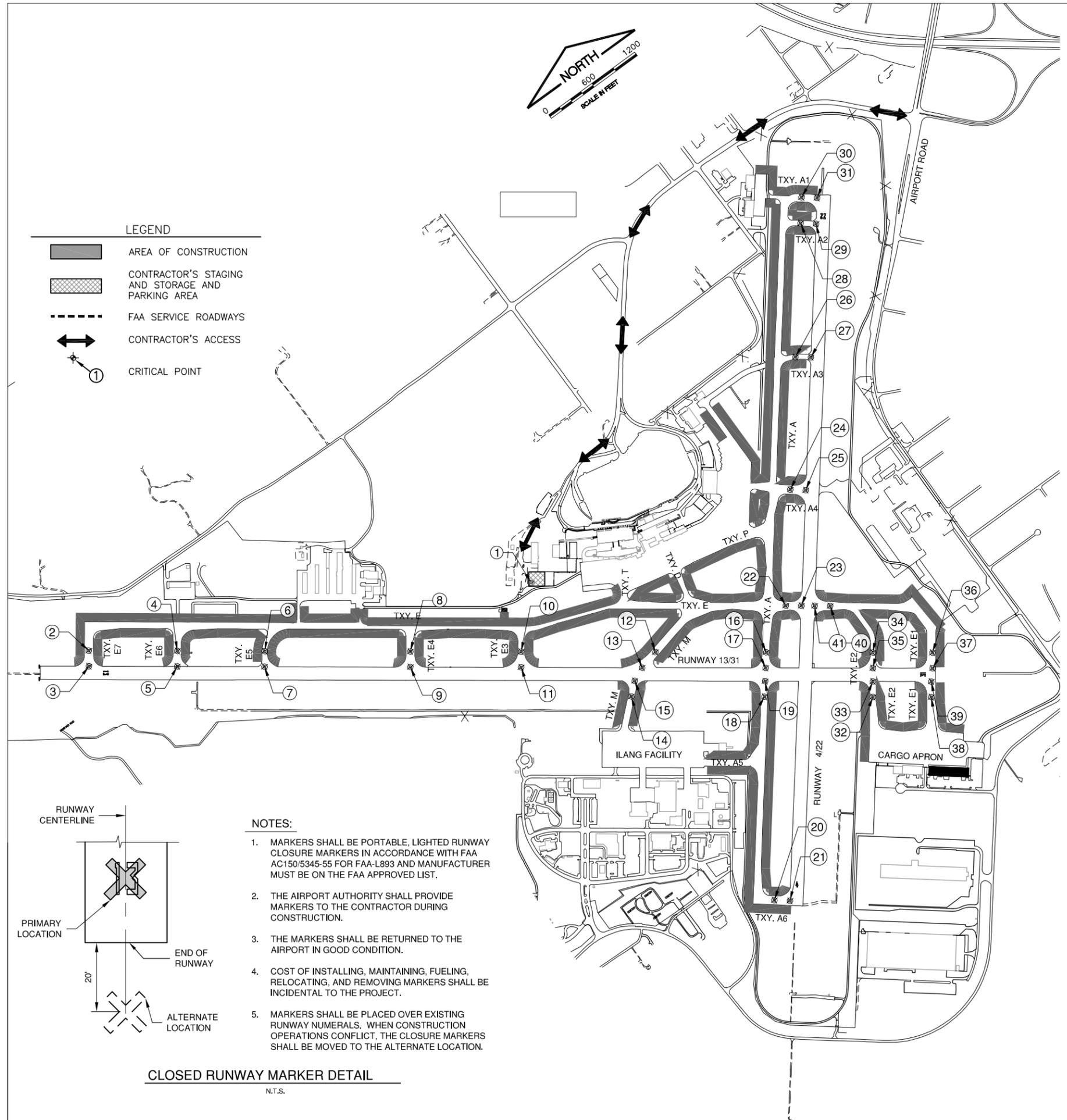


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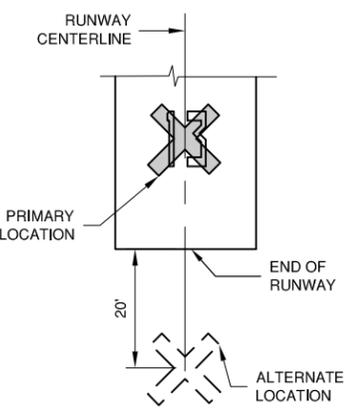
DESIGN BY:	CBG
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APPROVED BY:	CET
DATE:	APRIL 20, 2012
JOB No:	11061-07-00
IL PROJ. NO. PIA-4179 AIP PROJ. NO. 3-17-0080-XX	
SHEET 03 OF 62 SHEETS	

CRITICAL POINT TABLE				
POINT	LATITUDE	LONGITUDE	ELEVATION	DESCRIPTION
1	N 040° 40' 00.9335"	W 089° 41' 44.2399"	640.00	CONTRACTOR'S STAGING
2	N 040° 40' 24.1245"	W 089° 42' 42.2832"	630.00	TXY E7 AT RWY 13/31 HOLD
3	N 040° 40' 22.7200"	W 089° 42' 43.6086"	631.00	TXY E7 at RWY 13/31 EDGE
4	N 040° 40' 18.2487"	W 089° 42' 31.8601"	622.00	TXY E6 AT RWY 13/31 HOLD
5	N 040° 40' 16.8628"	W 089° 42' 33.2031"	624.00	TXY E6 at RWY 13/31 EDGE
6	N 040° 40' 12.4532"	W 089° 42' 21.5803"	622.00	TXY E5 AT RWY 13/31 HOLD
7	N 040° 40' 11.0703"	W 089° 42' 22.9439"	624.00	TXY E5 at RWY 13/31 EDGE
8	N 040° 40' 02.7872"	W 089° 42' 04.4371"	632.00	TXY E4 AT RWY 13/31 HOLD
9	N 040° 40' 01.3975"	W 089° 42' 05.7887"	635.00	TXY E4 at RWY 13/31 EDGE
10	N 040° 39' 55.4383"	W 089° 41' 51.4053"	640.00	TXY E3 AT RWY 13/31 HOLD
11	N 040° 39' 54.0486"	W 089° 41' 52.7569"	643.00	TXY E3 at RWY 13/31 EDGE
12	N 040° 39' 46.5266"	W 089° 41' 35.6042"	652.00	TXY M AT RWY 13/31 HOLD
13	N 040° 39' 46.0116"	W 089° 41' 38.5308"	652.00	TXY M at RWY 13/31 EDGE
14	N 040° 39' 44.1500"	W 089° 41' 42.2917"	648.00	TXY M AT RWY 13/31 HOLD
15	N 040° 39' 45.3186"	W 089° 41' 40.5482"	650.00	TXY M at RWY 13/31 EDGE
16	N 040° 39' 39.1752"	W 089° 41' 22.5713"	651.00	TXY A AT RWY 04/22 HOLD
17	N 040° 39' 37.8230"	W 089° 41' 23.9893"	652.00	TXY A at RWY 13/31 EDGE
18	N 040° 39' 35.3119"	W 089° 41' 26.6226"	649.00	TXY A AT RWY 04/22 HOLD
19	N 040° 39' 36.6640"	W 089° 41' 25.2047"	650.00	TXY A at RWY 13/31 EDGE
20	N 040° 39' 16.4424"	W 089° 41' 43.0877"	639.00	TXY A6 AT RWY 04/22 HOLD
21	N 040° 39' 15.3633"	W 089° 41' 41.3133"	643.00	TXY A AT RWY 04/22 HOLD
22	N 040° 39' 42.0596"	W 089° 41' 16.2251"	654.00	TXY E AT RWY 04/22 HOLD
23	N 040° 39' 41.0243"	W 089° 41' 14.4047"	654.00	TXY E AT RWY 04/22 EDGE
24	N 040° 39' 52.1483"	W 089° 41' 05.6438"	656.00	TXY A4 AT RWY 04/22 HOLD
25	N 040° 39' 51.0699"	W 089° 41' 03.8686"	658.00	TXY A4 at RWY 04/22 EDGE
26	N 040° 40' 03.6713"	W 089° 40' 53.5567"	658.00	TXY A3 AT RWY 04/22 HOLD
27	N 040° 40' 02.6257"	W 089° 40' 51.7471"	670.00	TXY A3 at RWY 04/22 EDGE
28	N 040° 40' 15.3069"	W 089° 40' 41.3501"	656.00	TXY A2 AT RWY 04/22 HOLD
29	N 040° 40' 14.2276"	W 089° 40' 39.5756"	658.00	TXY A2 at RWY 04/22 EDGE
30	N 040° 40' 17.5652"	W 089° 40' 38.9808"	656.00	TXY A1 AT RWY 04/22 HOLD
31	N 040° 40' 16.4858"	W 089° 40' 37.2063"	657.00	TXY A1 at RWY 04/22 EDGE
32	N 040° 39' 28.1187"	W 089° 41' 13.8716"	645.00	TXY E2 AT RWY 13/31 HOLD
33	N 040° 39' 29.5083"	W 089° 41' 12.5200"	646.00	TXY E2 at RWY 13/31 EDGE
34	N 040° 39' 32.0955"	W 089° 41' 10.0216"	647.00	TXY E2 AT RWY 13/31 HOLD
35	N 040° 39' 30.7060"	W 089° 41' 11.3733"	648.00	TXY E2 at RWY 13/31 EDGE
36	N 040° 39' 28.1433"	W 089° 41' 03.0162"	643.00	TXY E1 AT RWY 13/31 HOLD
37	N 040° 39' 26.7432"	W 089° 41' 04.3493"	644.00	TXY E1 at RWY 13/31 EDGE
38	N 040° 39' 24.2471"	W 089° 41' 07.0091"	642.00	TXY E1 AT RWY 13/31 HOLD
39	N 040° 39' 25.6366"	W 089° 41' 05.6575"	643.00	TXY E1 at RWY 13/31 EDGE
40	N 040° 39' 39.0948"	W 089° 41' 11.0311"	652.00	TXY E AT RWY 04/22 HOLD
41	N 040° 39' 40.1349"	W 089° 41' 12.8465"	654.00	TXY A at RWY 04/22 EDGE



LEGEND

-  AREA OF CONSTRUCTION
-  CONTRACTOR'S STAGING AND STORAGE AND PARKING AREA
-  FAA SERVICE ROADWAYS
-  CONTRACTOR'S ACCESS
-  CRITICAL POINT



- NOTES:**
- MARKERS SHALL BE PORTABLE, LIGHTED RUNWAY CLOSURE MARKERS IN ACCORDANCE WITH FAA AC150/5345-55 FOR FAA-L893 AND MANUFACTURER MUST BE ON THE FAA APPROVED LIST.
 - THE AIRPORT AUTHORITY SHALL PROVIDE MARKERS TO THE CONTRACTOR DURING CONSTRUCTION.
 - THE MARKERS SHALL BE RETURNED TO THE AIRPORT IN GOOD CONDITION.
 - COST OF INSTALLING, MAINTAINING, FUELING, RELOCATING, AND REMOVING MARKERS SHALL BE INCIDENTAL TO THE PROJECT.
 - MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS. WHEN CONSTRUCTION OPERATIONS CONFLICT, THE CLOSURE MARKERS SHALL BE MOVED TO THE ALTERNATE LOCATION.

GENERAL

1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE STORM WATER POLLUTION PREVENTION PROGRAM (SWPPP) CERTIFICATION STATEMENT.
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

1. TOTAL CONTRACT TIME SHALL BE 150 CALENDAR DAYS.
2. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEET.

PHASES 1 THROUGH 24 NOTES

1. ALL PHASES SHALL CONSIST OF REMOVAL OF EXISTING EDGE LIGHTING AND SIGNAGE, INSTALLATION OF TEMPORARY CIRCUITS, INSTALLATION OF NEW EDGE LIGHTING AND SIGNAGE, INSTALLATION OF NEW DUCTS (IF SHOWN), COMPLETION OF NEW HOMERUN CIRCUITS (IF SHOWN) AND INSTALLATION OF CABLING FOR NEW EDGE LIGHTING CIRCUITS.
2. ALL WORK SHALL BE COMPLETED IN PREVIOUS PHASE PRIOR TO STARTING WORK IN THE NEXT PHASE UNLESS OTHERWISE PERMITTED BY THE AIRPORT.
3. THE WORK PHASING PLAN PRESENTED IS NOT INTENDED TO RESTRICT THE CONTRACTOR TO THIS SPECIFIC PHASING. THE CONTRACTOR MAY SUBMIT THEIR OWN PHASING SCHEDULE FOR REVIEW AND WRITTEN APPROVAL.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN.
3. CERTAIN CONTRACTOR EMPLOYEES SHALL OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
4. ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATIONS AREA (AOA) SHALL ALSO ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
5. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR.
6. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS.
7. THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN
8. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
9. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
10. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
11. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
12. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
13. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
14. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
15. THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTICE TO AIRMEN (NOTAMS) MAY BE ISSUED BY THE AIRPORT.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
5. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

AIRPORT OPERATOR
 XXXXXXXX - AIRPORT OPERATOR (309) 697-5262

AIRPORT MAINTENANCE
 GREG HUSER - MAINTENANCE SUPERVISOR (309) 303-1005

ENGINEER
 CHRIS GROTH P.E. - PROJECT ENGINEER (217) 787-8050
 CMT - RESIDENT ENGINEER (217) 787-8050

10. INSPECTION REQUIREMENTS

1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL ATTEND A FINAL INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. PENALTIES

1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

1. NO RUNWAY OR TAXIWAY CLOSURES ARE REQUIRED FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2F.
2. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

15. MARKING AND SIGNS FOR ACCESS ROUTES

1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLAN SHEET.

16. HAZARD MARKING AND LIGHTING

1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
5. THE AIRPORT WILL SUPPLY UP TO TWO LIGHTED RUNWAY CLOSURE MARKERS FOR USE DURING THE PROJECT. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTENANCE OF THE RUNWAY CLOSURE MARKERS INCLUDING FUEL, OIL CHANGES AND REPLACEMENT OF THE LIGHTS. IF ADDITIONAL LIGHTED RUNWAY CLOSURE MARKERS ARE NEEDED, THE CONTRACTOR SHALL SUPPLY THE ADDITIONAL LIGHTED RUNWAY CLOSURE MARKERS. COST SHALL BE INCIDENTAL TO THE CONTRACT. UPON COMPLETION OF THE PROJECT, THE CONTRACTOR WILL RETAIN POSSESSION OF ANY ADDITIONAL LIGHTED RUNWAY CLOSURE MARKERS SUPPLIED BY THE CONTRACTOR. THE LIGHTED RUNWAY CLOSURE MARKERS SUPPLIED BY THE AIRPORT SHALL REMAIN PROPERTY OF THE AIRPORT AND SHALL BE RETURNED IN LIKE CONDITION WITH AN ALLOWANCE FOR NORMAL WEAR AND TEAR AS DETERMINED BY THE RESIDENT ENGINEER. ANY DAMAGED TO THE LIGHTED RUNWAY CLOSURE MARKERS SHALL BE REPLACED BY THE CONTRACTOR WITH NEW LIGHTED RUNWAY CLOSURE MARKERS OF SIMILAR TYPE AND DESIGN AT NO ADDITIONAL COST TO THE CONTRACT.

17. PROTECTION

1. ALL WORK REQUIRED INSIDE OF THE RUNWAY 4-22 OR 13-31 SAFETY AREAS, WHICH EXTENDS 250' FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY SAFETY AREA, WHICH EXTENDS 107' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

18. OTHER LIMITATIONS ON CONSTRUCTION

1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

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 FILE: G-4002.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 5/25/2012 7:35 AM
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 1106107-E-AL
 PIA-Base-Existing
 1106107-V-AL
 UTILITY_Updated

PE093

REVISIONS

NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 CONSTRUCTION SAFETY PHASING PLAN NOTES**

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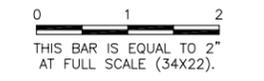
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**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT**

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GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS

REHABILITATE AIRFIELD LIGHTING
CONSTRUCTION ACTIVITY PLAN
GENERAL NOTES AND DETAILS

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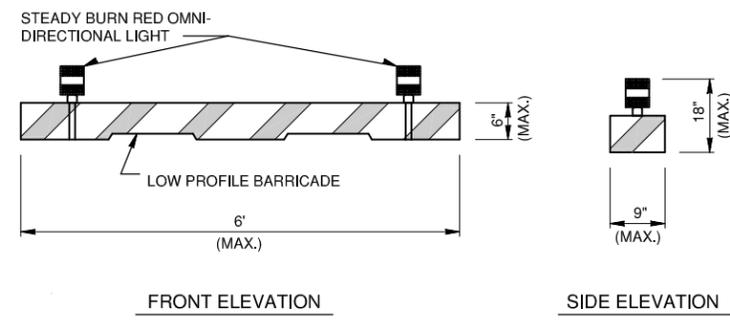
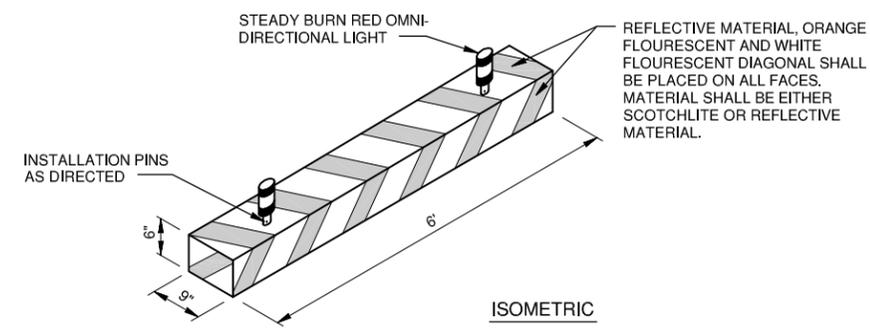
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CONSTRUCTION ACTIVITY PLAN GENERAL NOTES

1. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
2. THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
3. IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT ARE THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
4. WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT AND THE PROJECT.
5. DELAYS, CHANGES IN SCHEDULING OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. VEHICLE ACCESS ON PERIMETER ROADS SHALL BE MAINTAINED THROUGH OUT THE PROJECT.
7. THE CONTRACTOR SHALL BE AWARE THAT DAILY OPERATION OF THE AIRPORT WILL CONTINUE THROUGHOUT THIS PROJECT. AS A RESULT, DAILY COORDINATION WILL BE NECESSARY TO LIMIT DISRUPTION TO AIRPORT/TENANT AND CONTRACTOR OPERATIONS.
8. CONTRACTOR'S STAGING, STORAGE, AND PARKING WILL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS.
9. THE CONTRACTOR SHALL PLACE ALL BARRICADES, CONSTRUCTION SETBACK LINES, AND EROSION CONTROL ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
10. CONSTRUCTION PHASING IS OF CRITICAL IMPORTANCE TO THE AIRPORT FOR THIS PROJECT. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE ALLOWED TO CLOSE BOTH RUNWAYS AT THE SAME TIME.
11. CONSTRUCTION RELATED ITEMS REQUIRING THE CLOSURE OF RUNWAYS AND/OR TAXIWAYS SHALL REQUIRE A MINIMUM OF 72 HOURS NOTICE TO COORDINATION WITH THE AIRPORT AND FAA. NO EXTENSION TO CONTRACT TIME WILL BE GIVEN FOR DELAYS CAUSED BY LACK OF ADEQUATE COORDINATION.
12. CLOSED RUNWAYS MAY BE USED TO TAXI AIRCRAFT DURING CONSTRUCTION ACTIVITIES WITHIN THE RUNWAY SAFETY AREA. CONTRACTOR'S EQUIPMENT AND FORCES SHALL REMAIN OFF THE HARD SURFACE OF THE RUNWAY DURING TAXIING OPERATIONS.
13. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A. THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - B. THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE AIRPORT OPERATIONS BY THE CONTRACTOR.
 - C. THE CONTRACTOR SHALL USE AN EXISTING GATE(S), OR NEW GATES AS CALLED OUT IN THE PLANS, FOR ACCESS TO THE AIRFIELD.
 - D. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - E. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - F. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
 - G. THE CONTRACTOR SHALL CLOSE AND SECURE THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - H. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - I. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
 - J. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE USED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.

BEAM BARRICADE NOTES

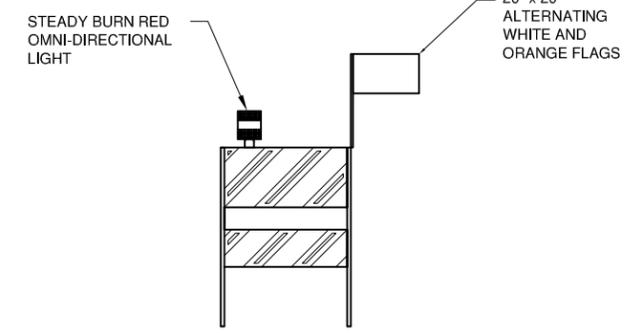
- 1.) BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
- 2.) BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
- 3.) PLACE AT 10' INTERVALS.
- 4.) NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.



BEAM BARRICADE DETAILS
N.T.S.

SECURITY NOTES

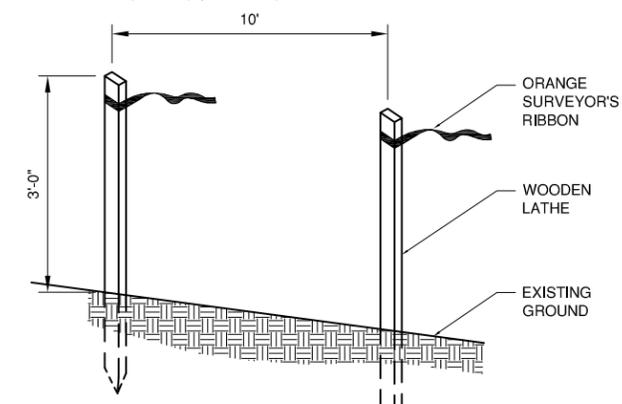
1. MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
2. AT NO TIME ARE UNSUPERVISED GATE OPENINGS PERMITTED AT THE AIRPORT. IF THE PROPOSED WORK REQUIRES GATE OPENING, THE CONTRACTOR SHALL PROVIDE A GUARD TO PREVENT UNAUTHORIZED ACCESS. THE CONTRACTOR SHALL SECURE THE GATE AT THE COMPLETION OF THE WORK DAY.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES DURING THE COURSE OF THE WORK.
4. FINES CAN BE LEVIED AGAINST THE CONTRACTOR BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) FOR NEGLIGENCE IF THE AIRPORT SECURITY IS COMPROMISED AND THE AIRPORT PERIMETER FENCE LINE IS NOT MAINTAINED AS SPECIFIED ABOVE. FINES CAN ALSO BE LEVIED AGAINST THE CONTRACTOR FOR FAILURE TO COOPERATE WITH THE AIRPORT MANAGEMENT AS REQUIRED TO MAINTAIN AIRPORT SECURITY.



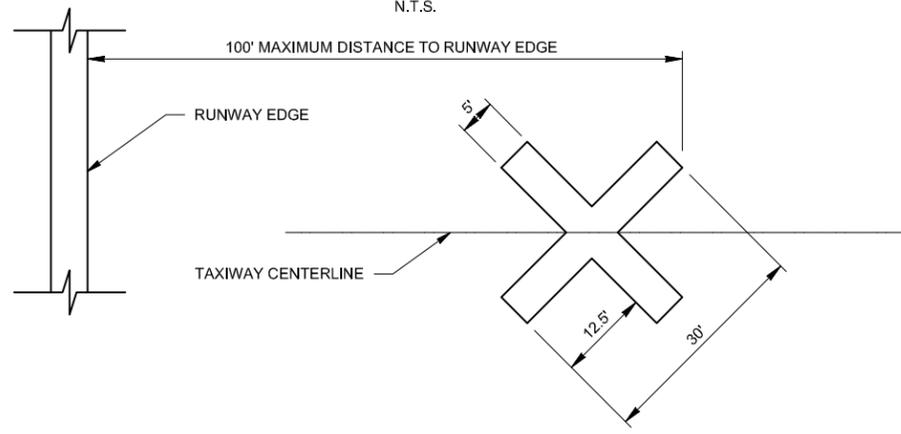
A FRAME BARRICADE DETAIL
N.T.S.

LIGHT BARRICADE NOTES

1. LIGHTS TO BE BATTERY OPERATED. LENS TO BE RED AND BE OMNI-DIRECTIONAL.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 10' INTERVALS.



CONSTRUCTION SETBACK LINE DETAIL
N.T.S.



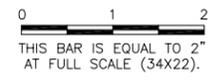
TYPICAL TAXIWAY CLOSURE MARKER DETAIL
N.T.S.

NOTES

- 1.) THE MARKING SHALL BE PAINTED WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING, OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OFA CRITERIA AND ARE APPROVED BY THE AIRPORT
- 2.) PLACE OVER TAXIWAY CENTERLINE.
- 3.) YELLOW X SHALL BE ADEQUATELY SECURED TO WITHSTAND JET BLAST OF 100 MPH WINDS.

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PEORIA, ILLINOIS
REHABILITATE AIRFIELD LIGHTING
CONSTRUCTION ACTIVITY PLAN
INDEX TO PHASES

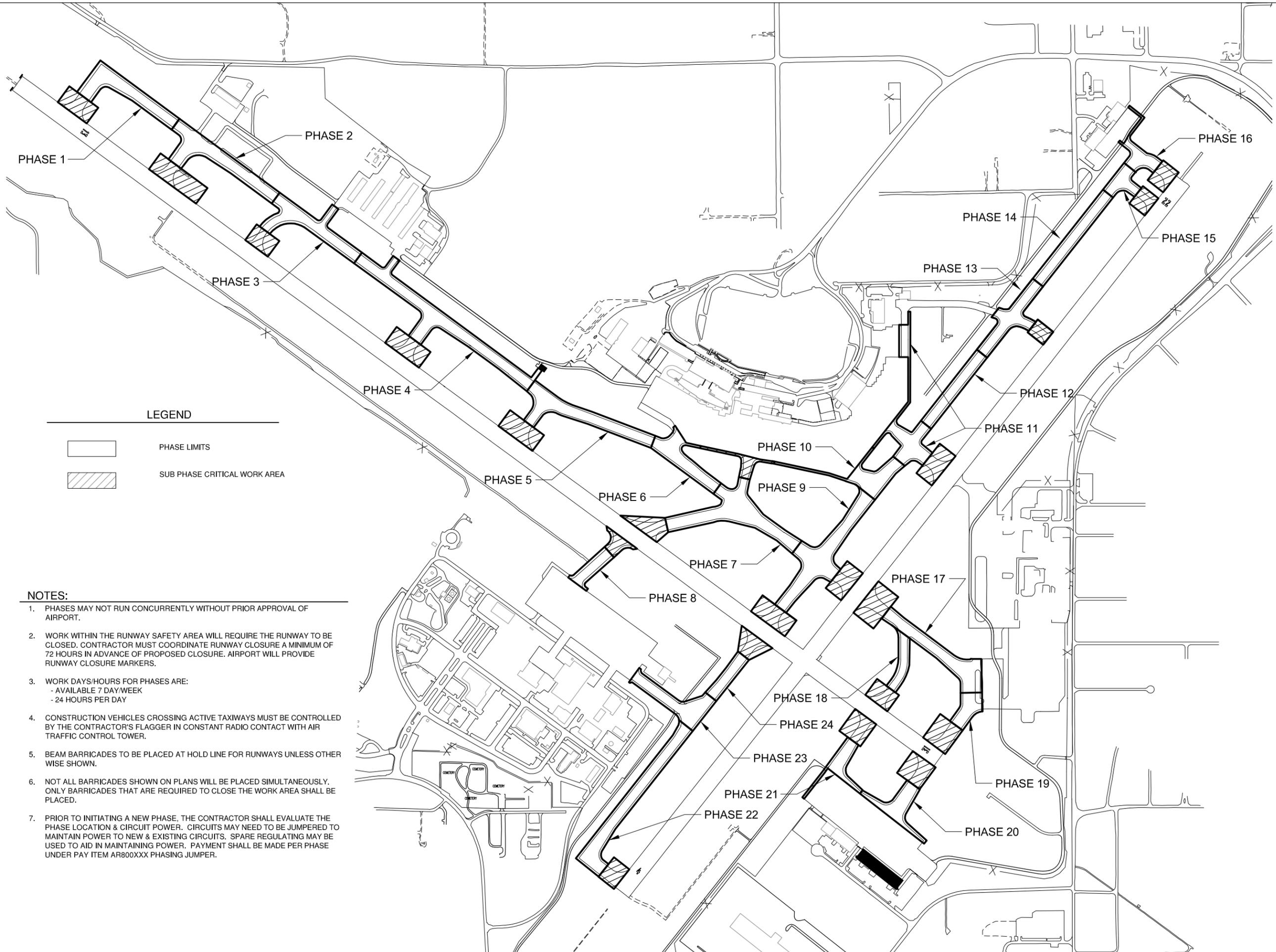
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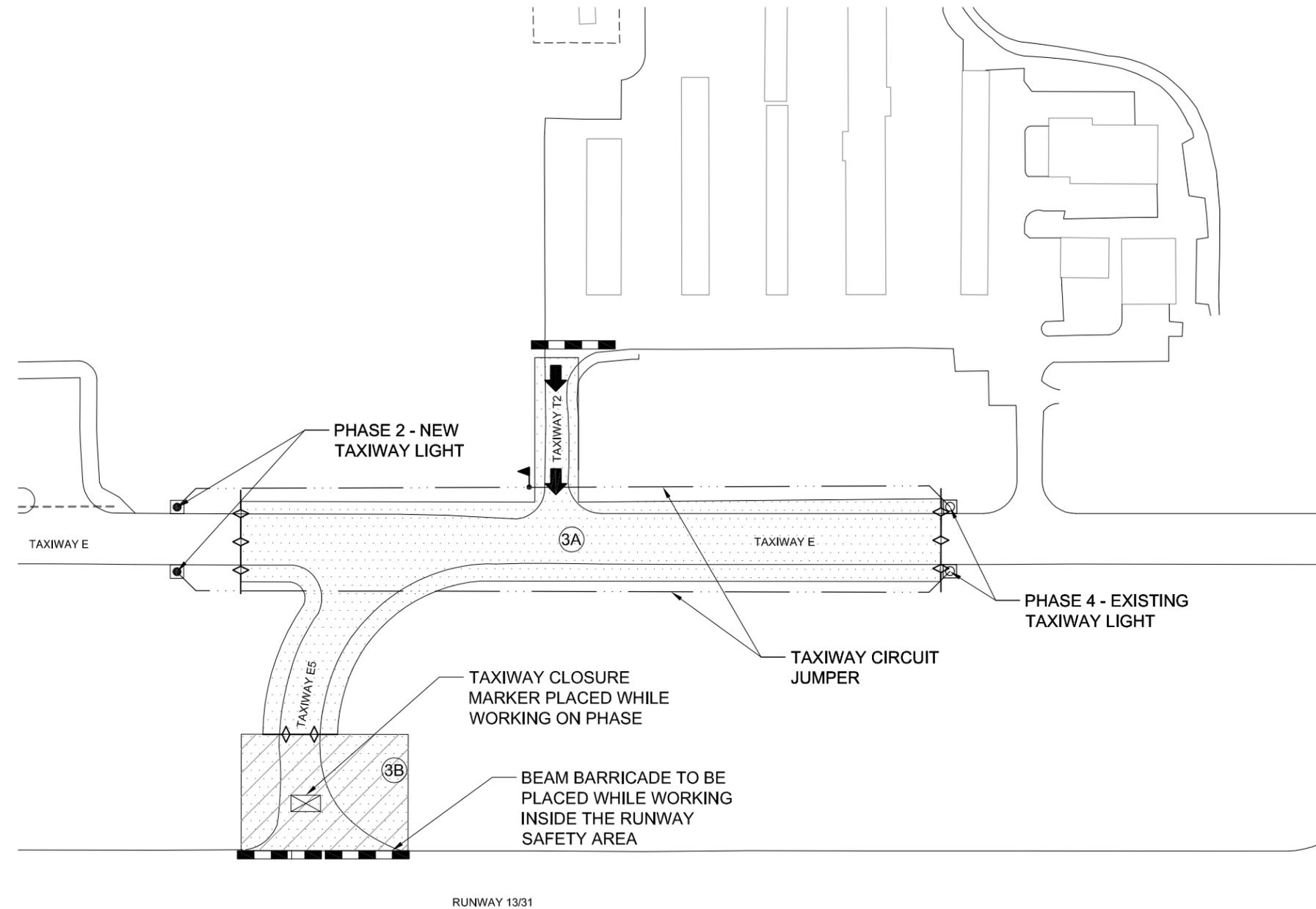
-  PHASE LIMITS
-  SUB PHASE CRITICAL WORK AREA

NOTES:

1. PHASES MAY NOT RUN CONCURRENTLY WITHOUT PRIOR APPROVAL OF AIRPORT.
2. WORK WITHIN THE RUNWAY SAFETY AREA WILL REQUIRE THE RUNWAY TO BE CLOSED. CONTRACTOR MUST COORDINATE RUNWAY CLOSURE A MINIMUM OF 72 HOURS IN ADVANCE OF PROPOSED CLOSURE. AIRPORT WILL PROVIDE RUNWAY CLOSURE MARKERS.
3. WORK DAYS/HOURS FOR PHASES ARE:
 - AVAILABLE 7 DAY/WEEK
 - 24 HOURS PER DAY
4. CONSTRUCTION VEHICLES CROSSING ACTIVE TAXIWAYS MUST BE CONTROLLED BY THE CONTRACTOR'S FLAGGER IN CONSTANT RADIO CONTACT WITH AIR TRAFFIC CONTROL TOWER.
5. BEAM BARRICADES TO BE PLACED AT HOLD LINE FOR RUNWAYS UNLESS OTHER WISE SHOWN.
6. NOT ALL BARRICADES SHOWN ON PLANS WILL BE PLACED SIMULTANEOUSLY. ONLY BARRICADES THAT ARE REQUIRED TO CLOSE THE WORK AREA SHALL BE PLACED.
7. PRIOR TO INITIATING A NEW PHASE, THE CONTRACTOR SHALL EVALUATE THE PHASE LOCATION & CIRCUIT POWER. CIRCUITS MAY NEED TO BE JUMPERED TO MAINTAIN POWER TO NEW & EXISTING CIRCUITS. SPARE REGULATING MAY BE USED TO AID IN MAINTAINING POWER. PAYMENT SHALL BE MADE PER PHASE UNDER PAY ITEM AR800XXX PHASING JUMPER.

TYPICAL PHASING SEQUENCE - PHASE 3 SHOWN

1. COORDINATE WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER 72 HOURS PRIOR TO BEGINNING WORK IN A NEW PHASE.
2. PLACE BARRICADES AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN PER EACH PHASE.
3. LOCATE TAXIWAY CIRCUIT TO BE IMPROVED AND JUMPER AROUND CURRENT PHASE. USE EXISTING SPARE REGULATORS AS OUTLINED IN THE EXISTING VAULT PLAN TO PROVIDE TEMPORARY POWER TO PORTIONS OF THE TAXIWAY CIRCUIT TO PROVIDE CONTINUOUS POWER TO THOSE AREAS OUTSIDE OF THE CURRENT PHASE.
4. LOCATE UTILITIES WITHIN THE PHASE INCLUDING BUT NOT LIMITED TO THE FAA CABLES, AIRPORT HOMERUNS AND EXISTING UNDERGROUND DRAINAGE.
5. VERIFY THAT THE TAXIWAY LIGHTING CIRCUITS ON EITHER SIDE OF THE CURRENT PHASE ARE POWERED.
6. SURVEY AND RECORD THE LOCATION OF THE EXISTING SIGNS. REMOVE EXISTING LIGHTS AND SIGNS.
7. LAYOUT NEW TAXIWAY LIGHT, TAXIWAY GUIDANCE SIGNS, CABLE CONDUIT AND DIRECTIONAL BORE LOCATIONS.
8. INSTALL CABLE CONDUIT, DIRECTIONAL BORES TAXIWAY LIGHTS AND TAXIWAY GUIDANCE SIGNS.
9. COORDINATE WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER A MINIMUM 72 HOURS PRIOR TO BEGINNING WORK WITHIN THE RUNWAY SAFETY AREA.
10. PLACE RUNWAY CLOSURE MARKERS (PROVIDED BY THE AIRPORT) AT THE BEGINNING OF EACH WORKING DAY. BEGIN WORKING WITHIN THE RUNWAY SAFETY AREA. SEE SITE PLAN FOR CLOSURE MARKER DETAIL.
11. WORK AREAS WITHIN THE RUNWAY SAFETY AREA SHALL BE REOPENED TO AIR TRAFFIC AT THE END OF EACH WORK DAY.
12. WORK AREAS WITHIN THE RUNWAY SAFETY AREA SHALL HAVE NO OPEN TRENCHES, NO EQUIPMENT, NO MATERIALS AND MEET THE APPROVAL OF THE RESIDENT ENGINEER PRIOR TO REOPENING THE RUNWAY.
13. COMPLETE ALL WORK WITHIN THE PHASE LIMITS AND TEST ALL INSTALLED EQUIPMENT TO ENSURE THAT LIGHTS AND SIGNS ARE WORKING PROPERLY.
14. COORDINATE COMPLETION OF THE CURRENT PHASE AND INTENTIONS TO BEGIN THE NEXT PHASE AND REPEAT THE PROCESS.



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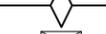
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**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 CONSTRUCTION ACTIVITY PLAN TYPICAL PHASING**

LEGEND

-  WORK AREA
-  WORK AREA - WITHIN RUNWAY SAFETY AREA RUNWAY CLOSURE REQUIRED
-  BEAM BARRICADE
-  A FRAME BARRICADE
-  TAXIWAY CLOSURE MARKER
-  RUNWAY CLOSURE MARKER
-  CONTRACTOR'S ACCESS
-  FLAG PERSON

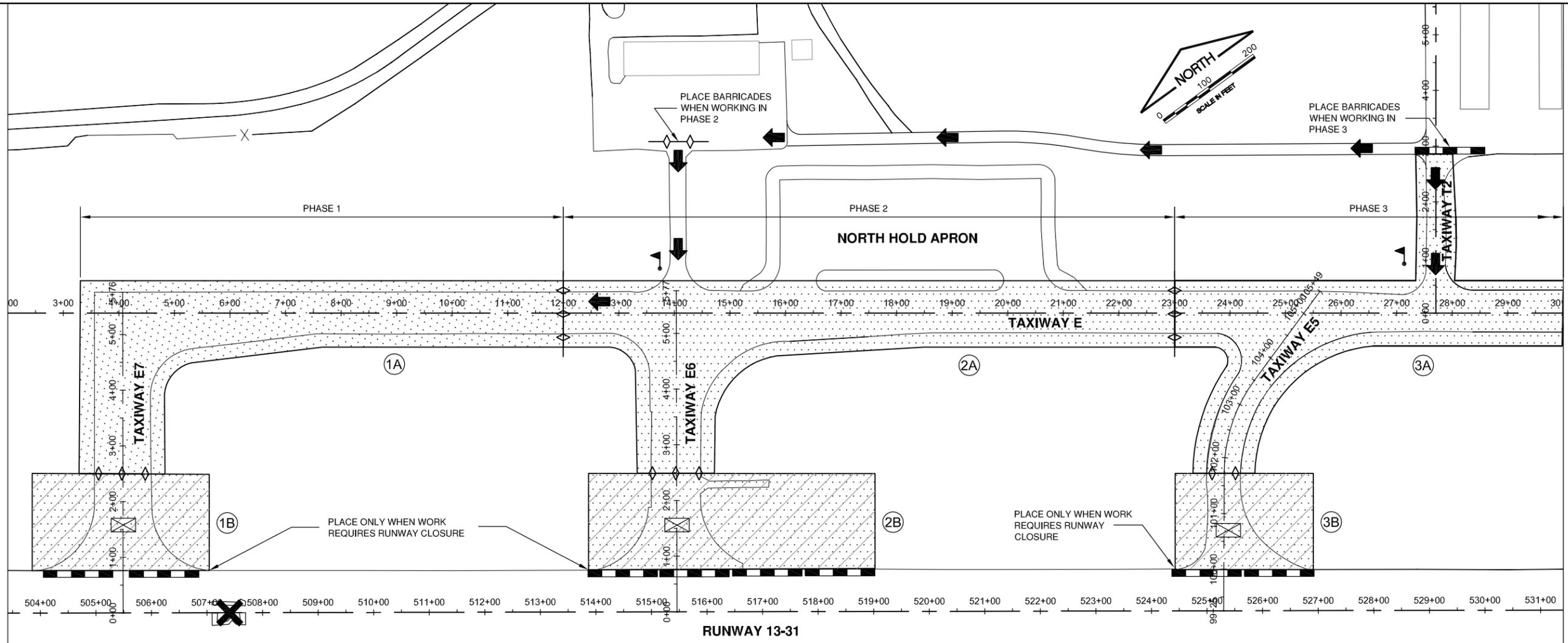
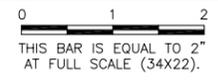
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PHASE 1A & 1B NOTES:

1. PHASE 1A & 1B WILL CLOSE PARALLEL TAXIWAY E FROM WEST OF E6 TO TAXIWAY E7 AND CONNECTING TAXIWAY E7.
2. RUNWAY 13/31 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 1A. RUNWAY 13/31 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 1B.
3. CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 1A & 1B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
4. WORK WITHIN BOTH PHASE 1A & 1B CAN BE CONCURRENT. WORK ON PHASE 1B SHALL BE COMPLETED IN 3 CALENDAR DAYS.

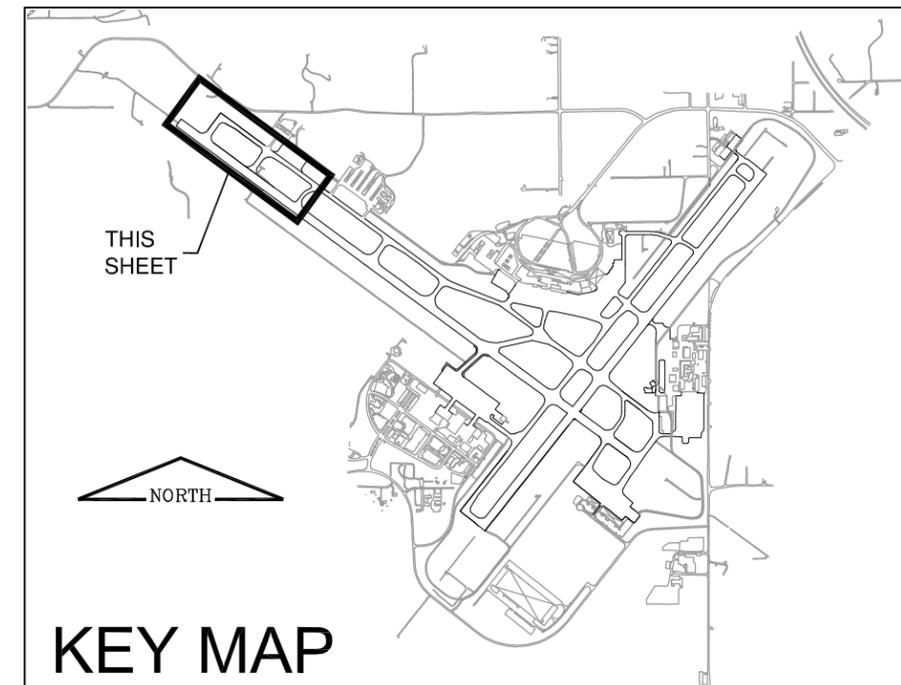
PHASE 2A & 2B NOTES:

1. PHASE 2A & 2B WILL CLOSE PARALLEL TAXIWAY E FROM WEST OF E5 TO EAST OF TAXIWAY E7 AND CONNECTING TAXIWAY E6. THE HOLD APRON WILL BE CLOSED DURING THIS PHASE.
2. RUNWAY 13/31 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 2A. RUNWAY 13/31 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 2B.
3. CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 2A & 2B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
4. WORK WITHIN BOTH PHASE 2A & 2B CAN BE CONCURRENT. WORK ON PHASE 2B SHALL BE COMPLETED IN 3 CALENDAR DAYS.

PHASE 3A & 3B NOTES:

1. PHASE 3A & 3B WILL CLOSE PARALLEL TAXIWAY E FROM WEST OF E4 TO EAST OF TAXIWAY E6 AND CONNECTING TAXIWAY E5. TAXIWAY T2 WILL BE CLOSED DURING THIS PHASE.
2. RUNWAY 13/31 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 3A. RUNWAY 13/31 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 3B.
3. CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 3A & 3B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
4. WORK WITHIN BOTH PHASE 3A & 3B CAN BE CONCURRENT. WORK ON PHASE 3B SHALL BE COMPLETED IN 3 CALENDAR DAYS.

LEGEND	
	WORK AREA
	WORK AREA - RUNWAY CLOSURE REQUIRED
	BEAM BARRICADE
	A FRAME BARRICADE
	TAXIWAY CLOSURE MARKER
	RUNWAY CLOSURE MARKER
	CONTRACTOR'S ACCESS
	FLAG PERSON



KEY MAP

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 CONSTRUCTION ACTIVITY PLAN 1**

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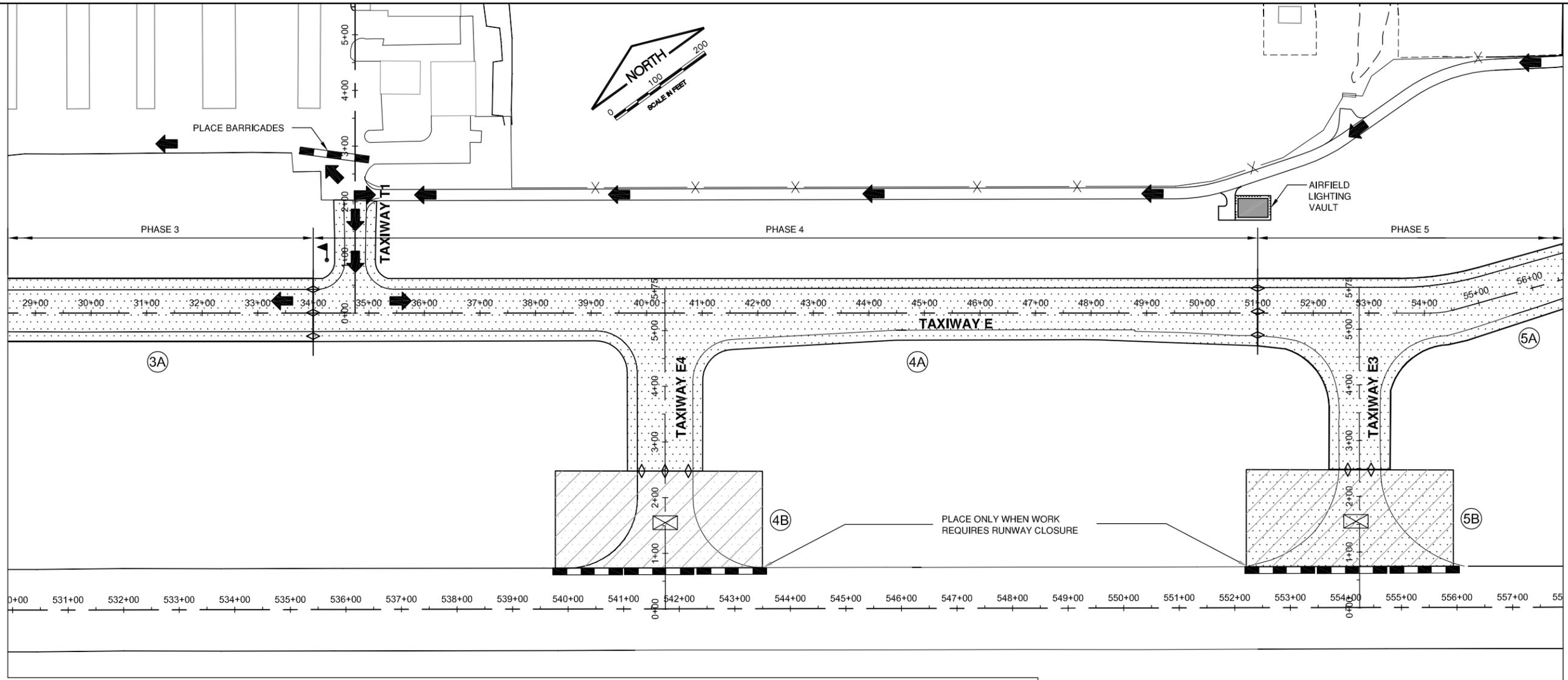
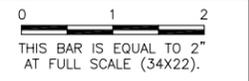
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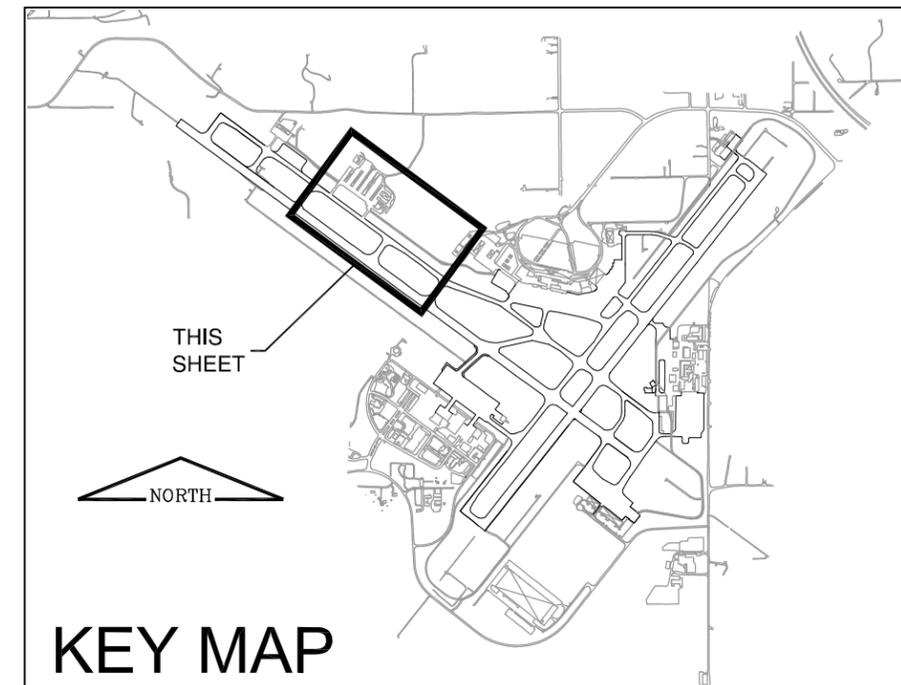
PHASE 4A & 4B NOTES:

1. PHASE 4A & 4B WILL CLOSE PARALLEL TAXIWAY E FROM WEST OF E3 TO JUST WEST OF TAXIWAY T1 AND CONNECTING TAXIWAY E4.
2. RUNWAY 13/31 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 4A. RUNWAY 13/31 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 4B.
3. CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 4A & 4B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
4. WORK WITHIN BOTH PHASE 4A & 4B CAN BE CONCURRENT. WORK ON PHASE 4B SHALL BE COMPLETED IN 3 CALENDAR DAYS.

PHASE 5A & 5B NOTES:

1. PHASE 5A & 5B WILL CLOSE PARALLEL TAXIWAY E FROM JUST WEST OF TAXIWAY T TO JUST WEST OF TAXIWAY E3 AND CONNECTING TAXIWAY E3.
2. RUNWAY 13/31 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 5A. RUNWAY 13/31 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 5B.
3. CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 5A & 5B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
4. WORK WITHIN BOTH PHASE 5A & 5B CAN BE CONCURRENT. WORK ON PHASE 5B SHALL BE COMPLETED IN 3 CALENDAR DAYS.

LEGEND	
	WORK AREA
	WORK AREA - RUNWAY CLOSURE REQUIRED
	BEAM BARRICADE
	A FRAME BARRICADE
	TAXIWAY CLOSURE MARKER
	RUNWAY CLOSURE MARKER
	CONTRACTOR'S ACCESS
	FLAG PERSON



GENERAL WAYNE A. DOWNING
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PEORIA, ILLINOIS
REHABILITATE AIRFIELD LIGHTING
CONSTRUCTION ACTIVITY PLAN 2

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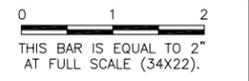
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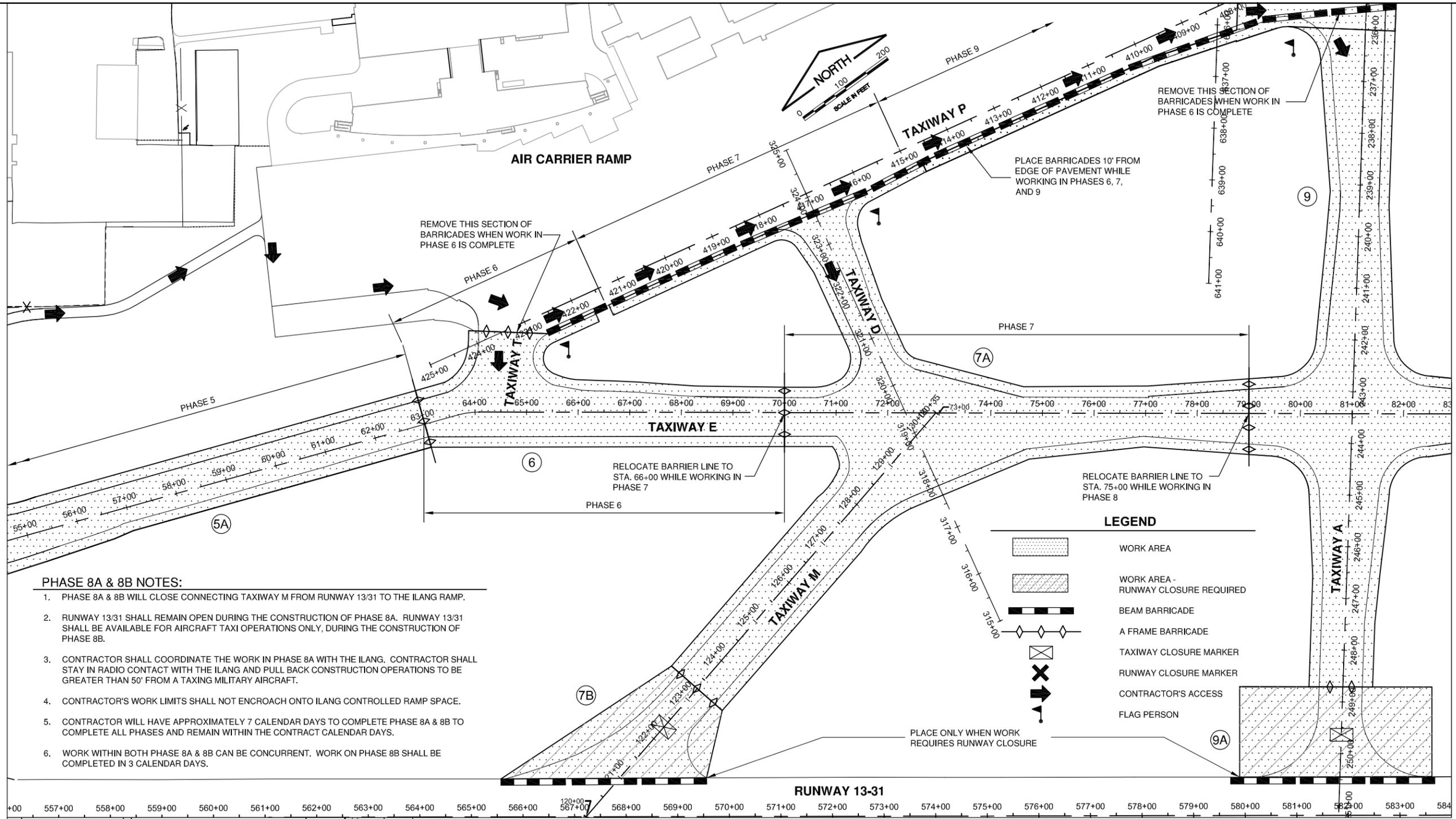
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PHASE 8A & 8B NOTES:

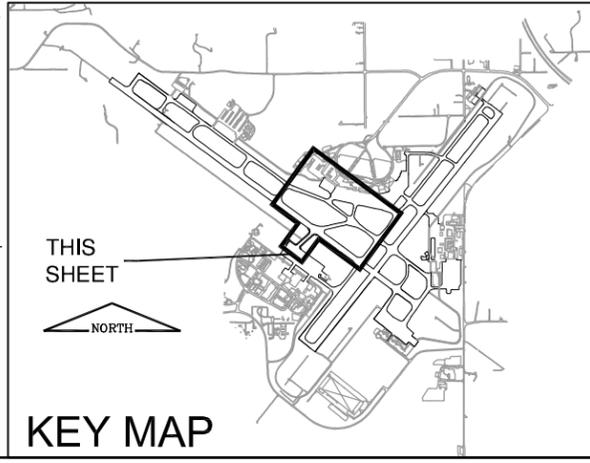
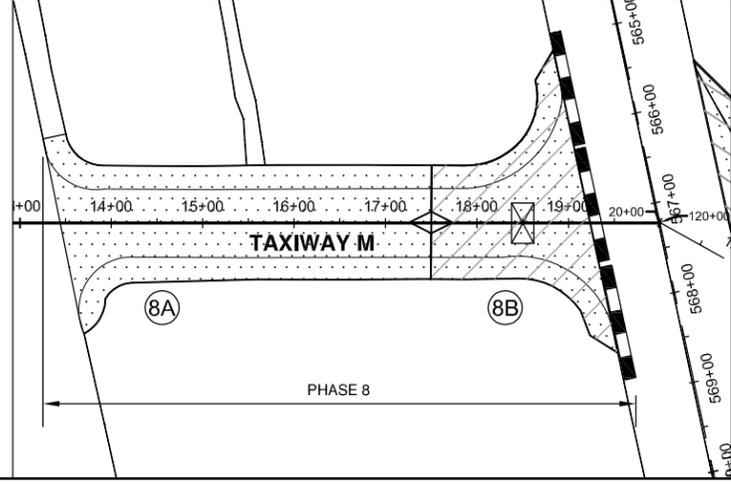
1. PHASE 8A & 8B WILL CLOSE CONNECTING TAXIWAY M FROM RUNWAY 13/31 TO THE ILANG RAMP.
2. RUNWAY 13/31 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 8A. RUNWAY 13/31 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 8B.
3. CONTRACTOR SHALL COORDINATE THE WORK IN PHASE 8A WITH THE ILANG. CONTRACTOR SHALL STAY IN RADIO CONTACT WITH THE ILANG AND PULL BACK CONSTRUCTION OPERATIONS TO BE GREATER THAN 50' FROM A TAXIING MILITARY AIRCRAFT.
4. CONTRACTOR'S WORK LIMITS SHALL NOT ENCROACH ONTO ILANG CONTROLLED RAMP SPACE.
5. CONTRACTOR WILL HAVE APPROXIMATELY 7 CALENDAR DAYS TO COMPLETE PHASE 8A & 8B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
6. WORK WITHIN BOTH PHASE 8A & 8B CAN BE CONCURRENT. WORK ON PHASE 8B SHALL BE COMPLETED IN 3 CALENDAR DAYS.

PHASE 6 NOTES:

1. PHASE 6 WILL CLOSE APRON TAXIWAY T AT THE AIR CARRIER RAMP AND RESTRICT TAXILANES TO THE AIR CARRIER RAMP.
2. RUNWAY 13/31 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 6.
3. CONTRACTOR SHALL PAINT TEMPORARY TAXILANE MARKINGS TO REROUTE AIR TRAFFIC TAXIING TO THE WEST GATES OF THE TERMINAL BUILDING. SEE TEMPORARY MARKING SHEET FOR MORE DETAILS.
4. CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 6 TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
5. WORK WITHIN BOTH PHASE 6 & 7 SHALL NOT BE CONSTRUCTED CONCURRENTLY. TAXIWAY D OR TAXIWAY T SHALL REMAIN OPEN THROUGHOUT ALL CONSTRUCTION PHASES.

PHASE 7A & 7B NOTES:

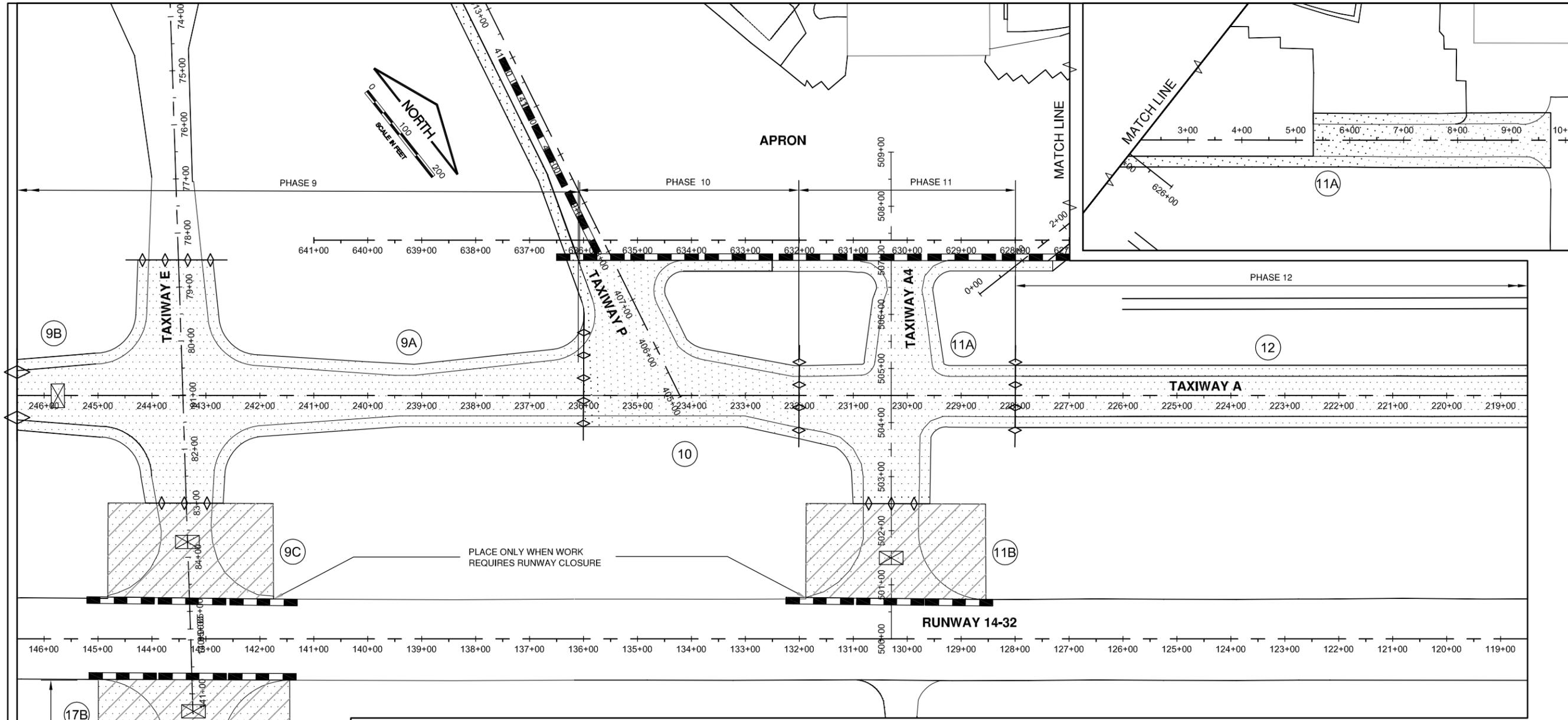
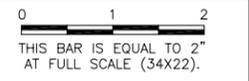
1. PHASE 7A & 7B WILL CLOSE PARALLEL TAXIWAY E FROM EAST OF TAXIWAY A AND CONNECTING TAXIWAY M AND TAXIWAY D.
2. CONTRACTOR SHALL PAINT TEMPORARY TAXILANE MARKINGS TO REROUTE AIR TRAFFIC AROUND THE CONSTRUCTION ALONG THE AIR CARRIER RAMP APRON. SEE TEMPORARY MARKING SHEET FOR MORE DETAILS.
3. RUNWAY 13/31 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 7A. RUNWAY 13/31 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 7B.
4. CONTRACTOR WILL HAVE APPROXIMATELY 13 CALENDAR DAYS TO COMPLETE PHASE 7A & 7B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
5. WORK WITHIN BOTH PHASE 7A & 7B CAN BE CONCURRENT. WORK ON PHASE 7B SHALL BE COMPLETED IN 3 CALENDAR DAYS.
6. WORK WITHIN BOTH PHASE 6 & 7 SHALL NOT BE CONSTRUCTED CONCURRENTLY. TAXIWAY D OR TAXIWAY T SHALL REMAIN OPEN THROUGH ALL CONSTRUCTION PHASES.



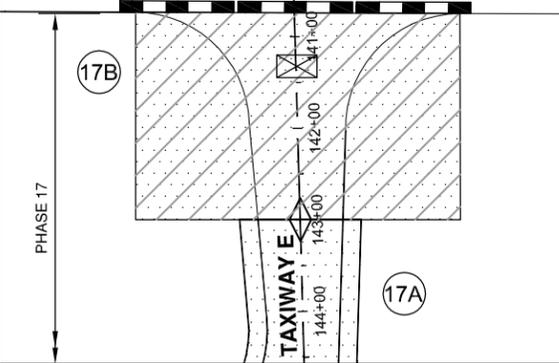
KEY MAP

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PLACE ONLY WHEN WORK REQUIRES RUNWAY CLOSURE



LEGEND

- WORK AREA
- WORK AREA - RUNWAY CLOSURE REQUIRED
- BEAM BARRICADE
- A FRAME BARRICADE
- TAXIWAY CLOSURE MARKER
- RUNWAY CLOSURE MARKER
- CONTRACTOR'S ACCESS
- FLAG PERSON

PHASE 9A, 9B & 9C NOTES:

- PHASE 9A, 9B & 9C WILL CLOSE TAXIWAY E AND A FROM RUNWAY 4/22 TO JUST WEST OF PARALLEL TAXIWAY A AND FROM RUNWAY 13/31 TO TAXIWAY P.
- RUNWAY 13/31 AND 4/22 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 9A.
- CONTRACTOR WILL HAVE APPROXIMATELY 14 CALENDAR DAYS TO COMPLETE PHASE 9A, 9B, & 9C TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
- WORK WITHIN BOTH PHASE 9A & 9B CAN BE CONCURRENT. WORK WITHIN BOTH PHASE 9A & 9C CAN BE CONCURRENT. WORK WITHIN BOTH PHASE 9B & 9C SHALL NOT BE CONCURRENT.
- THE CONTRACTOR CAN NOT CLOSE BOTH RUNWAY 4/22 AND 13/31 AT THE SAME TIME FOR CONSTRUCTION.

PHASE 10 NOTES:

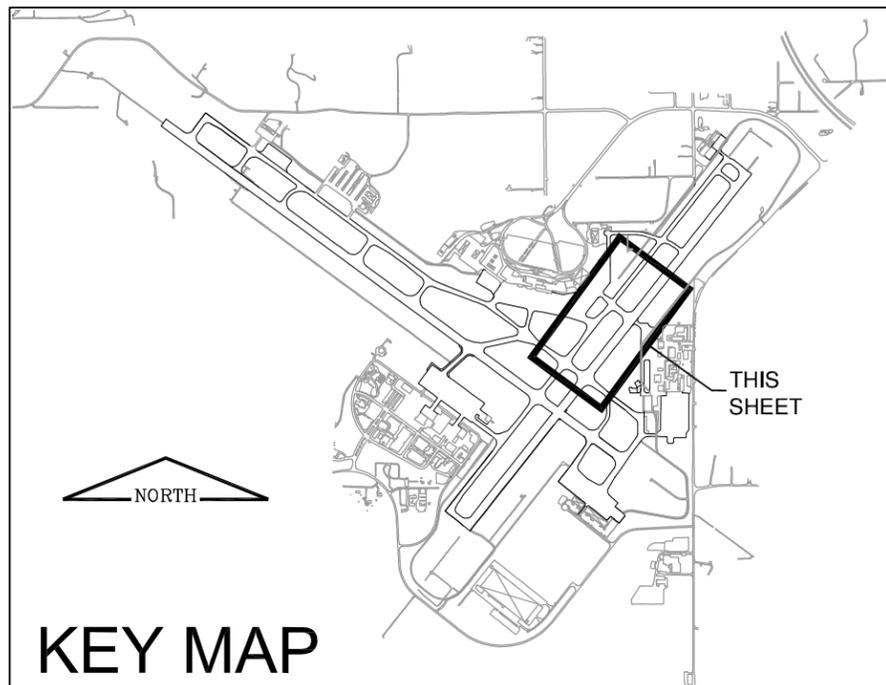
- PHASE 10 WILL CLOSE TAXIWAY P AT TAXIWAY A AND THE AIR CARRIER RAMP.
- PHASE 10 WILL ALSO INCLUDE WORK ALONG THE APRON.
- CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 10 TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.

PHASE 11A & 11B NOTES:

- PHASE 11A & 11B WILL CLOSE TAXIWAY A4 FROM THE APRON TO RUNWAY 4/22 AND PARALLEL TAXIWAY A ON EITHER SIDE OF TAXIWAY A4.
- RUNWAY 4/22 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 11A. RUNWAY 4/22 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 11B.
- CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 11A & 11B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
- WORK WITHIN BOTH PHASE 11A & 11B CAN BE CONCURRENT. WORK ON PHASE 11B SHALL BE COMPLETED IN 3 CALENDAR DAYS.

PHASE 12 NOTES:

- PHASE 12 WILL CLOSE PARALLEL TAXIWAY A FROM JUST NORTH OF TAXIWAY A4 TO SOUTH OF TAXIWAY A3.
- RUNWAY 4/22 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 12.
- CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 12 TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.



KEY MAP

GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
REHABILITATE AIRFIELD LIGHTING
CONSTRUCTION ACTIVITY PLAN 4

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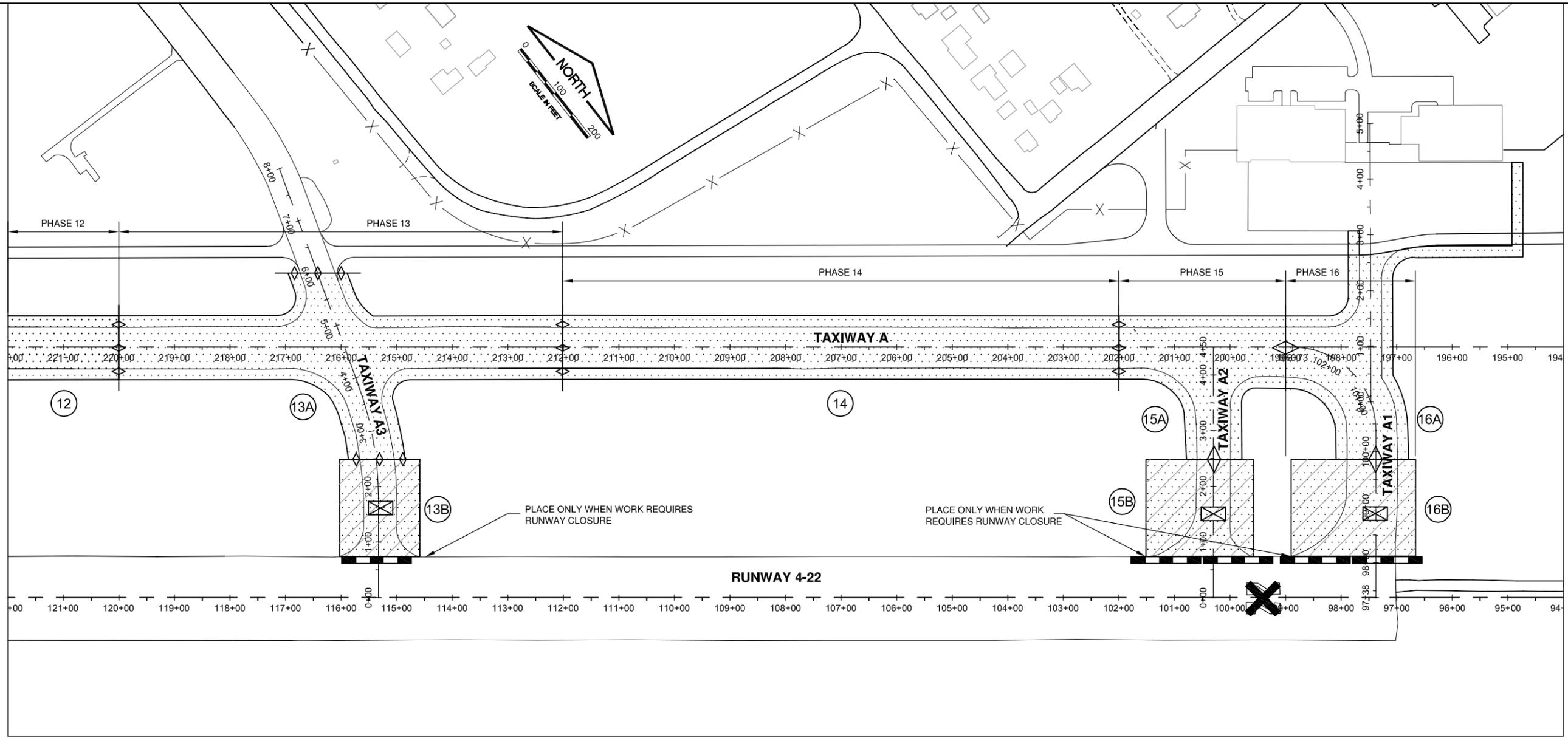
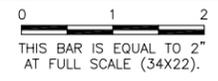
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GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
REHABILITATE AIRFIELD LIGHTING
CONSTRUCTION ACTIVITY PLAN 5

PHASE 13A & 13B NOTES:

- PHASE 13A & 13B WILL CLOSE TAXIWAY A3 FROM PARALLEL TAXIWAY A TO RUNWAY 4/22 AND PARALLEL TAXIWAY A ON EITHER SIDE OF TAXIWAY A3.
- RUNWAY 4/22 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 13A. RUNWAY 4/22 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 13B.
- CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 13A & 13B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
- WORK WITHIN BOTH PHASE 13A & 13B CAN BE CONCURRENT. WORK ON PHASE 13B SHALL BE COMPLETED IN 3 CALENDAR DAYS.

PHASE 14 NOTES:

- PHASE 14 WILL CLOSE PARALLEL TAXIWAY A FROM NORTH OF TAXIWAY A3 TO JUST SOUTH OF TAXIWAY A2.
- CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 14 TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.

PHASE 15A & 15B NOTES:

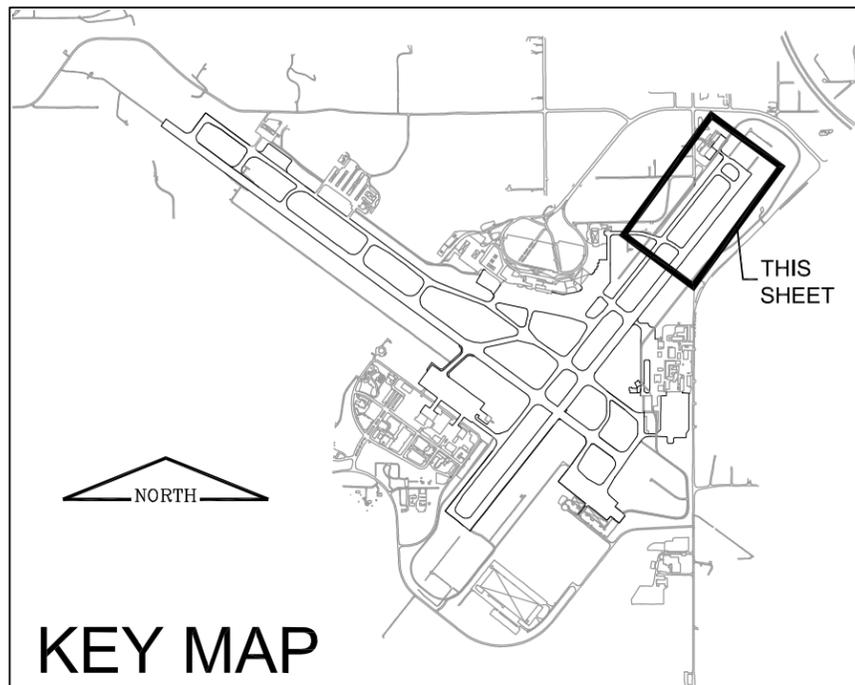
- PHASE 15A & 15B WILL CLOSE CONNECTING TAXIWAY A2 AND PARALLEL TAXIWAY A ON EITHER SIDE OF TAXIWAY A2.
- RUNWAY 4/22 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 15A. RUNWAY 4/22 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 15B.
- CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 15A & 15B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
- WORK WITHIN BOTH PHASE 15A & 15B CAN BE CONCURRENT. WORK ON PHASE 15B SHALL BE COMPLETED IN 3 CALENDAR DAYS.

PHASE 16A & 16B NOTES:

- PHASE 16A & 16B WILL CLOSE CONNECTING TAXIWAY A1 AND PARALLEL TAXIWAY A SOUTH OF TAXIWAY A1.
- PHASE 16A WORK WILL ALSO INCLUDE CONSTRUCTION ALONG THE GA APRON.
- RUNWAY 4/22 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 16A. RUNWAY 4/22 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 16B.
- CONTRACTOR WILL HAVE APPROXIMATELY 14 CALENDAR DAYS TO COMPLETE PHASE 16A & 16B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
- WORK WITHIN BOTH PHASE 16A & 16B CAN BE CONCURRENT. WORK ON PHASE 16B SHALL BE COMPLETED IN 3 CALENDAR DAYS.

LEGEND

	WORK AREA
	WORK AREA - RUNWAY CLOSURE REQUIRED
	BEAM BARRICADE
	A FRAME BARRICADE
	TAXIWAY CLOSURE MARKER
	RUNWAY CLOSURE MARKER
	CONTRACTOR'S ACCESS
	FLAG PERSON



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PHASE 17A & 17B NOTES:

1. PHASE 17A & 17B WILL CLOSE TAXIWAY E FROM RUNWAY 4/22 TO MID POINT OF SOUTH APRON.
2. RUNWAY 4/22 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 17A. RUNWAY 4/22 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 17B.
3. CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 17A & 17B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
4. WORK WITHIN BOTH PHASE 17A & 17B CAN BE CONCURRENT. WORK ON PHASE 17B SHALL BE COMPLETED IN 3 CALENDAR DAYS.
5. WORK IN PHASE 17A SHALL NOT BE CONCURRENT WITH PHASE 19A.

PHASE 18A & 18B NOTES:

1. PHASE 18A & 18B WILL CLOSE CONNECTING TAXIWAY E2 FROM RUNWAY 13/31 TO PARALLEL TAXIWAY E.
2. RUNWAY 13/31 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 18A. RUNWAY 13/31 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 18B.
3. CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 18A & 18B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
4. WORK WITHIN BOTH PHASE 18A & 18B CAN BE CONCURRENT. WORK ON PHASE 18B SHALL BE COMPLETED IN 3 CALENDAR DAYS.
5. PHASE 18A & 18B CAN BE CONSTRUCTED CONCURRENT WITH PHASE 21A & 21B. PHASE 18A & 18B SHALL NOT BE CONSTRUCTED CONCURRENT WITH PHASE 19A & 19B.

PHASE 19A & 19B NOTES:

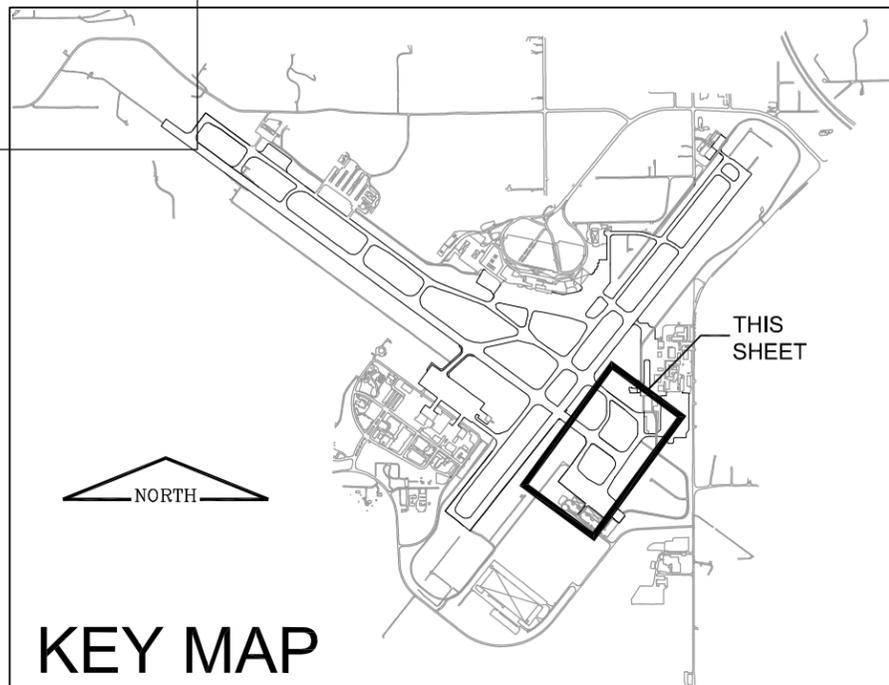
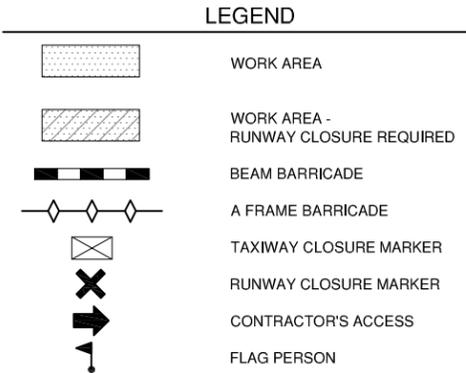
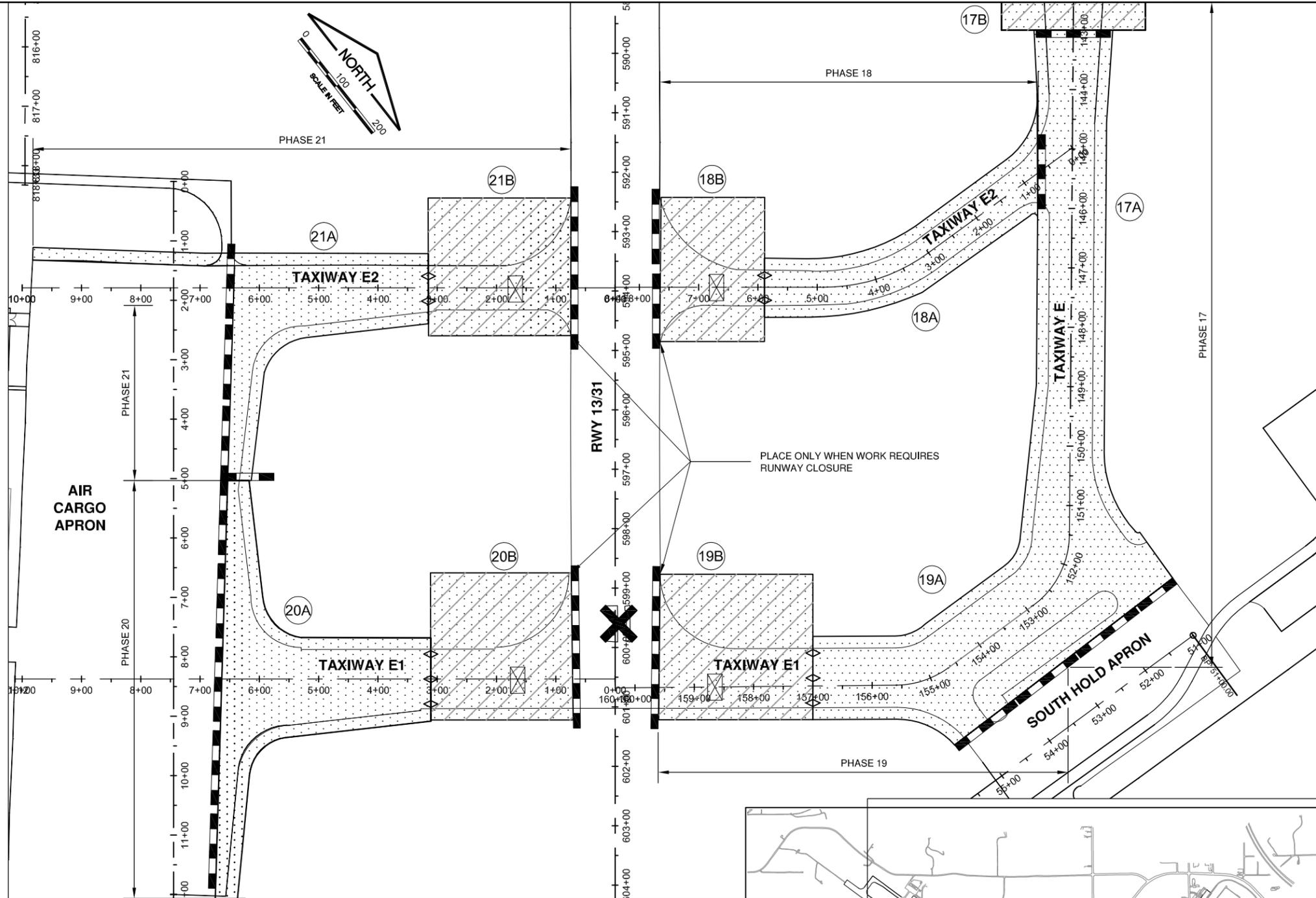
1. PHASE 19A & 19B WILL CLOSE CONNECTING TAXIWAY E1 FROM RUNWAY 13/31 TO PARALLEL TAXIWAY E.
2. RUNWAY 13/31 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 19A. RUNWAY 13/31 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 19B.
3. CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 19A & 19B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
4. WORK WITHIN BOTH PHASE 19A & 19B CAN BE CONCURRENT. WORK ON PHASE 19B SHALL BE COMPLETED IN 3 CALENDAR DAYS.
5. PHASE 19A & 19B CAN BE CONSTRUCTED CONCURRENT WITH PHASE 20A & 20B. PHASE 19A & 19B SHALL NOT BE CONSTRUCTED CONCURRENT WITH PHASE 17A & 17B AND 18A & 18B.

PHASE 20A & 20B NOTES:

1. PHASE 20A & 20B WILL CLOSE CONNECTING TAXIWAY E2 FROM RUNWAY 13/31 TO THE AIR CARGO APRON.
2. PHASE 20A WILL ALSO INCLUDE WORK ALONG THE NORTHEAST AND NORTHWEST PORTIONS OF THE CARGO APRON.
3. RUNWAY 13/31 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 20A. RUNWAY 13/31 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 20B.
3. CONTRACTOR SHALL COORDINATE THE WORK ALONG THE APRON WITH THE AIR CARGO OPERATOR IN PHASE 20A. CONTRACTOR SHALL STAY IN RADIO CONTACT WITH THE AIR CARGO OPERATOR AND BE PREPARED TO PULL BACK CONSTRUCTION OPERATIONS FOR AIRCRAFT TO TAXI.
4. CONTRACTOR WILL HAVE APPROXIMATELY 14 CALENDAR DAYS TO COMPLETE PHASE 20A & 20B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
5. WORK WITHIN BOTH PHASE 20A & 20B CAN BE CONCURRENT. WORK ON PHASE 20B SHALL BE COMPLETED IN 3 CALENDAR DAYS.
6. PHASE 20A & 20B CAN BE CONSTRUCTED CONCURRENT WITH PHASE 19A & 19B. PHASE 20A & 20B SHALL NOT BE CONSTRUCTED CONCURRENT WITH PHASE 21A & 21B.

PHASE 21A & 21B NOTES:

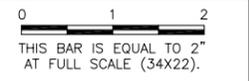
1. PHASE 21A & 21B WILL CLOSE CONNECTING TAXIWAY E1 FROM RUNWAY 13/31 TO THE AIR CARGO APRON.
2. PHASE 21A WILL ALSO INCLUDE WORK ALONG THE NORTHEAST PORTIONS OF THE CARGO APRON.
3. RUNWAY 13/31 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 21A. RUNWAY 13/31 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 21B.
3. CONTRACTOR SHALL COORDINATE THE WORK ALONG THE APRON WITH THE AIR CARGO OPERATOR IN PHASE 21A. CONTRACTOR SHALL STAY IN RADIO CONTACT WITH THE AIR CARGO OPERATOR AND BE PREPARED TO PULL BACK CONSTRUCTION OPERATIONS FOR AIRCRAFT TO TAXI.
4. CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 21A & 21B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
5. WORK WITHIN BOTH PHASE 21A & 21B CAN BE CONCURRENT. WORK ON PHASE 21B SHALL BE COMPLETED IN 3 CALENDAR DAYS.
6. PHASE 21A & 21B CAN BE CONSTRUCTED CONCURRENT WITH PHASE 18A & 18B. PHASE 21A & 21B SHALL NOT BE CONSTRUCTED CONCURRENT WITH PHASE 20A & 20B.



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 PLOT DATE: 5/25/2012 7:37 AM
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PEORIA, ILLINOIS

REHABILITATE AIRFIELD LIGHTING
CONSTRUCTION ACTIVITY PLAN 6

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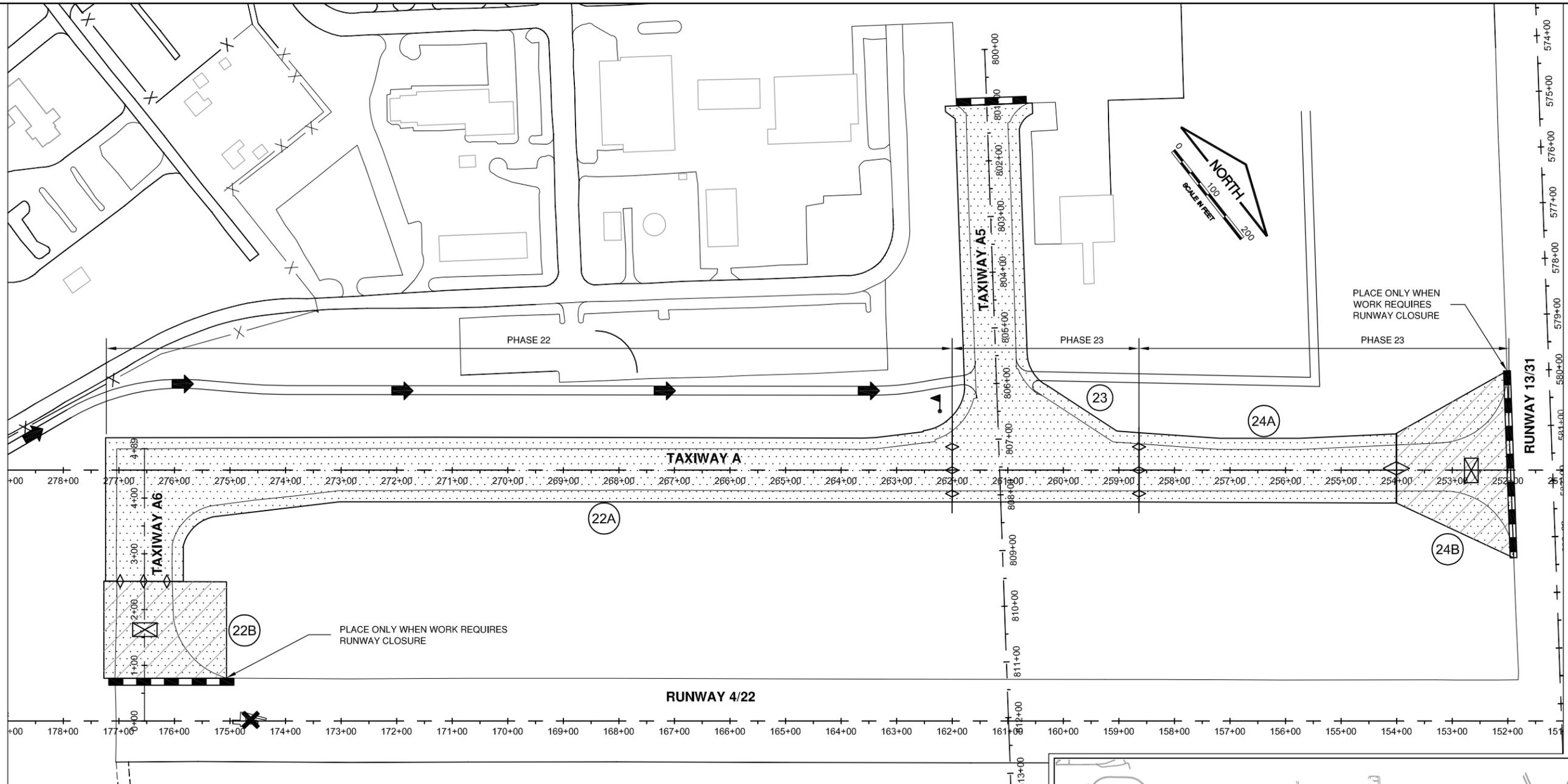
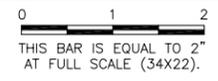
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IL PROJ. NO. PIA-4179	
AIP PROJ. NO. 3-17-0080-XX	
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**GENERAL WAYNE A. DOWNING
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 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 CONSTRUCTION ACTIVITY PLAN 7**

PHASE 22A & 22B NOTES:

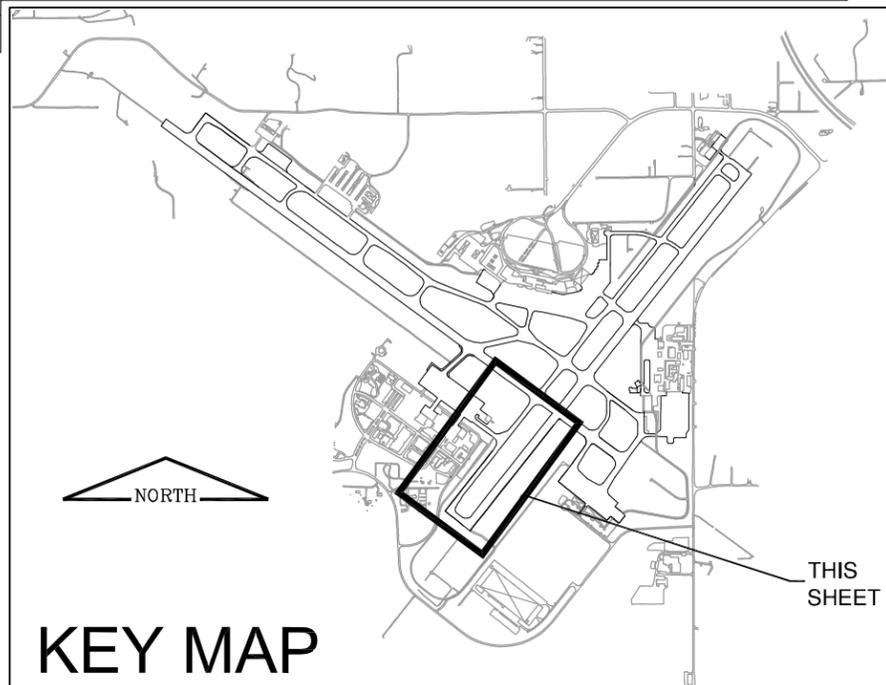
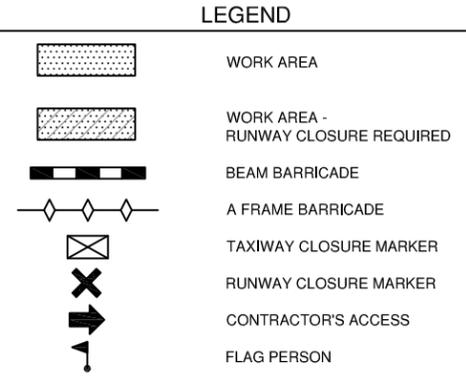
1. PHASE 22A & 22B WILL CLOSE PARALLEL TAXIWAY A SOUTH OF TAXIWAY A5 AND TAXIWAY A6.
2. RUNWAY 4/22 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 22A. RUNWAY 4/22 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 22B.
3. CONTRACTOR WILL HAVE APPROXIMATELY 14 CALENDAR DAYS TO COMPLETE PHASE 22A & 22B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
4. WORK WITHIN BOTH PHASE 22A & 22B CAN BE CONCURRENT. WORK ON PHASE 22B SHALL BE COMPLETED IN 3 CALENDAR DAYS.

PHASE 23 NOTES:

1. PHASE 23 WILL CLOSE TAXIWAY A5 FROM PARALLEL TAXIWAY A TO THE ILANG RAMP AND PARALLEL TAXIWAY A ON BOTH SIDES OF TAXIWAY A5.
2. RUNWAY 4/22 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 23.
3. CONTRACTOR SHALL COORDINATE THE WORK IN PHASE 23 WITH THE ILANG. CONTRACTOR SHALL STAY IN RADIO CONTACT WITH THE ILANG AND PULL BACK CONSTRUCTION OPERATIONS TO BE GREATER THAN 50' FROM A TAXING MILITARY AIRCRAFT.
4. CONTRACTOR WILL HAVE APPROXIMATELY 7 CALENDAR DAYS TO COMPLETE PHASE 23 TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.

PHASE 24A & 24B NOTES:

1. PHASE 24A & 24B WILL CLOSE PARALLEL TAXIWAY A FROM RUNWAY 13/31 TO NORTH OF TAXIWAY A5.
2. RUNWAY 13/31 SHALL REMAIN OPEN DURING THE CONSTRUCTION OF PHASE 24A. RUNWAY 13/31 SHALL BE AVAILABLE FOR AIRCRAFT TAXI OPERATIONS ONLY, DURING THE CONSTRUCTION OF PHASE 24B.
3. CONTRACTOR WILL HAVE APPROXIMATELY 10 CALENDAR DAYS TO COMPLETE PHASE 24A & 24B TO COMPLETE ALL PHASES AND REMAIN WITHIN THE CONTRACT CALENDAR DAYS.
4. WORK WITHIN BOTH PHASE 24A & 24B CAN BE CONCURRENT. WORK ON PHASE 24B SHALL BE COMPLETED IN 3 CALENDAR DAYS.



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REHABILITATE AIRFIELD LIGHTING

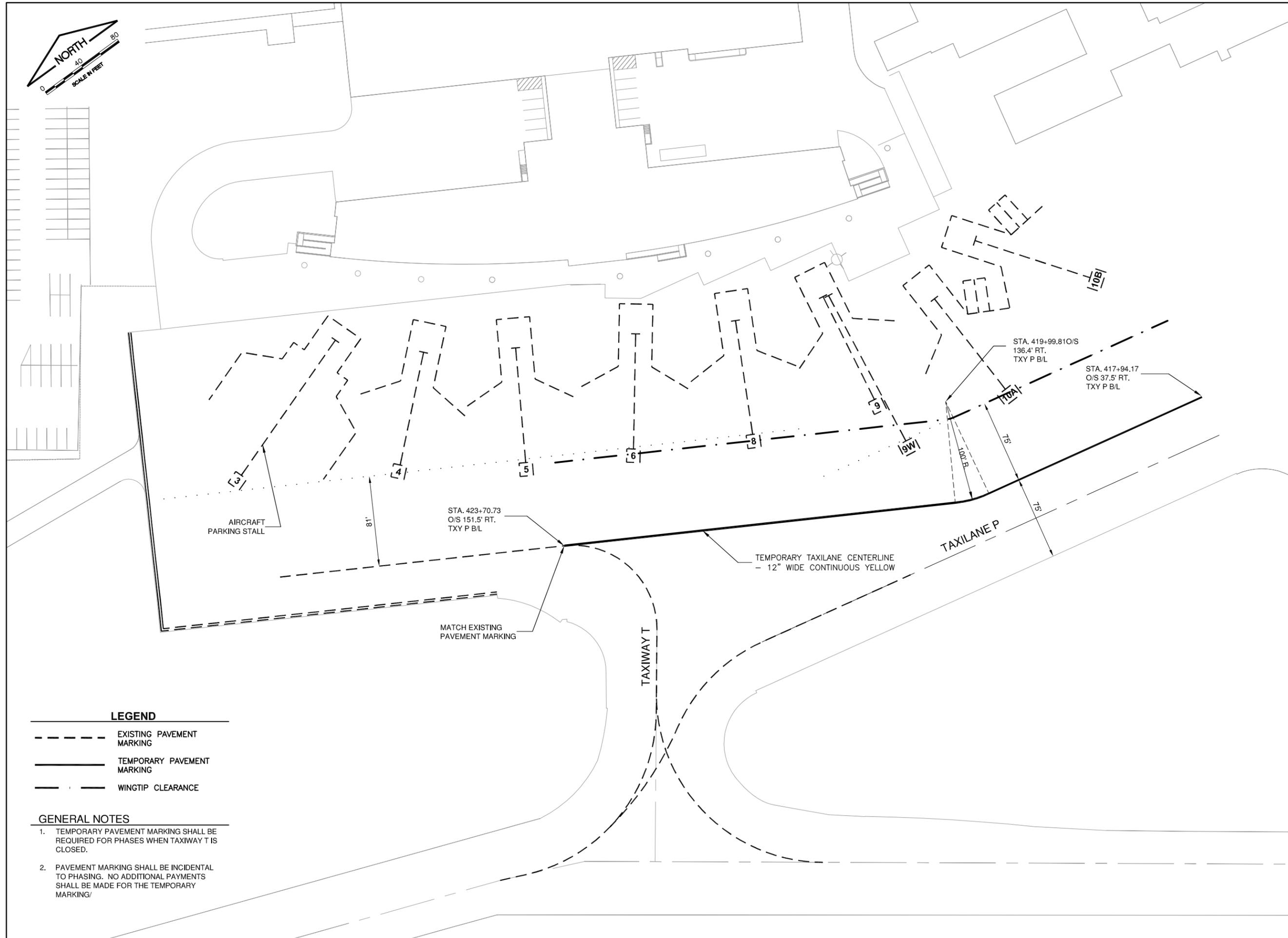
TEMPORARY APRON MARKING PLAN

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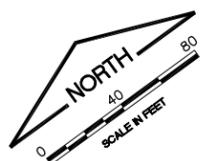
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LEGEND

	EXISTING PAVEMENT MARKING
	TEMPORARY PAVEMENT MARKING
	WINGTIP CLEARANCE

- GENERAL NOTES**
- TEMPORARY PAVEMENT MARKING SHALL BE REQUIRED FOR PHASES WHEN TAXIWAY T IS CLOSED.
 - PAVEMENT MARKING SHALL BE INCIDENTAL TO PHASING. NO ADDITIONAL PAYMENTS SHALL BE MADE FOR THE TEMPORARY MARKING.



CONTRACTOR'S RESPONSIBILITY FOR UTILITY SERVICE

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDER- GROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

UNDERGROUND UTILITIES INCLUDE ALL UNDERGROUND CABLES, CONDUITS, SEWERS, DRAINS, STRUCTURES OWNED BY FAA AND THE UNIVERSITY OF ILLINOIS.

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 PIA-Base-Existing

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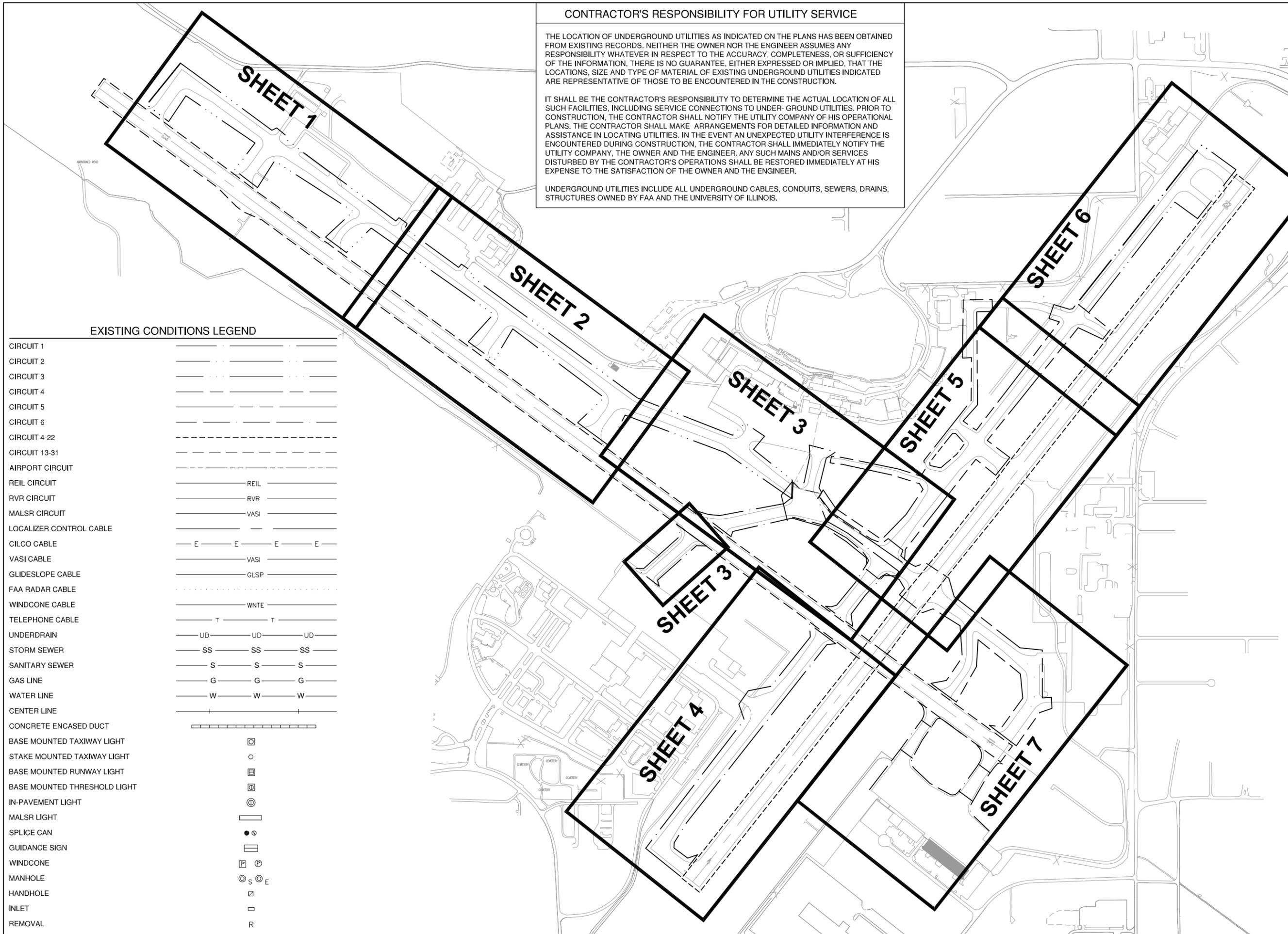
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 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 EXISTING CONDITIONS AND REMOVALS
 LEGENDS, NOTES & INDEX**

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SHEET	16 OF 62 SHEETS

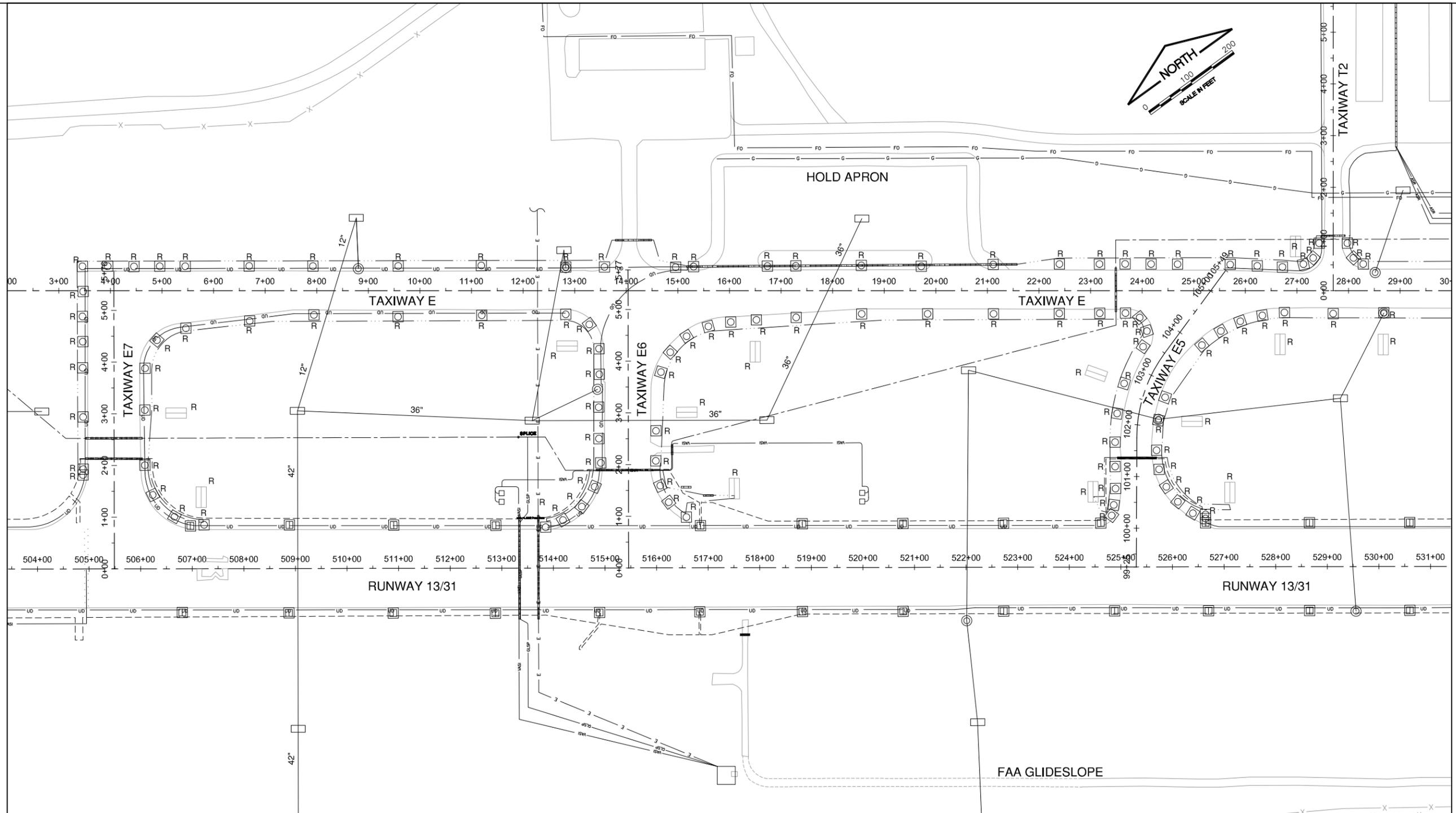
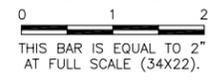


EXISTING CONDITIONS LEGEND

CIRCUIT 1	_____
CIRCUIT 2	_____
CIRCUIT 3	_____
CIRCUIT 4	_____
CIRCUIT 5	_____
CIRCUIT 6	_____
CIRCUIT 4-22	_____
CIRCUIT 13-31	_____
AIRPORT CIRCUIT	_____
REIL CIRCUIT	_____ REIL
RVR CIRCUIT	_____ RVR
MALSR CIRCUIT	_____ VASI
LOCALIZER CONTROL CABLE	_____
CILCO CABLE	— E — E — E — E —
VASI CABLE	_____ VASI
GLIDESLOPE CABLE	_____ GLSP
FAA RADAR CABLE	_____
WINDCONE CABLE	_____ WNTC
TELEPHONE CABLE	_____ T — T
UNDERDRAIN	— UD — UD — UD
STORM SEWER	— SS — SS — SS
SANITARY SEWER	— S — S — S
GAS LINE	— G — G — G
WATER LINE	— W — W — W
CENTER LINE	— + — + — +
CONCRETE ENCASED DUCT	_____
BASE MOUNTED TAXIWAY LIGHT	⊠
STAKE MOUNTED TAXIWAY LIGHT	○
BASE MOUNTED RUNWAY LIGHT	⊠
BASE MOUNTED THRESHOLD LIGHT	⊠
IN-PAVEMENT LIGHT	⊙
MALSR LIGHT	▬
SPLICE CAN	● ⊙
GUIDANCE SIGN	▬
WINDCONE	⊠ ⊙
MANHOLE	⊙ S ⊙ E
HANDHOLE	⊠
INLET	□
REMOVAL	R

PE093

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NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 EXISTING CONDITIONS AND REMOVALS SHEET 1**

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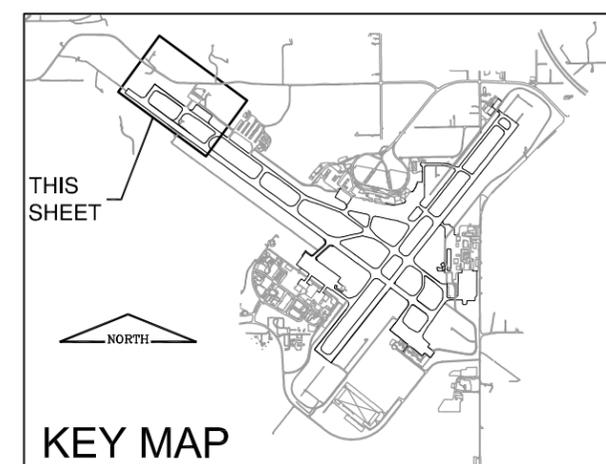
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 PEORIA INTERNATIONAL AIRPORT**

DESIGN BY:	CBG
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	APRIL 20, 2012
JOB No:	11061-07-00
IL. PROJ. NO. PIA-4179 AIP PROJ. NO. 3-17-0080-XX	
SHEET	17 OF 62 SHEETS

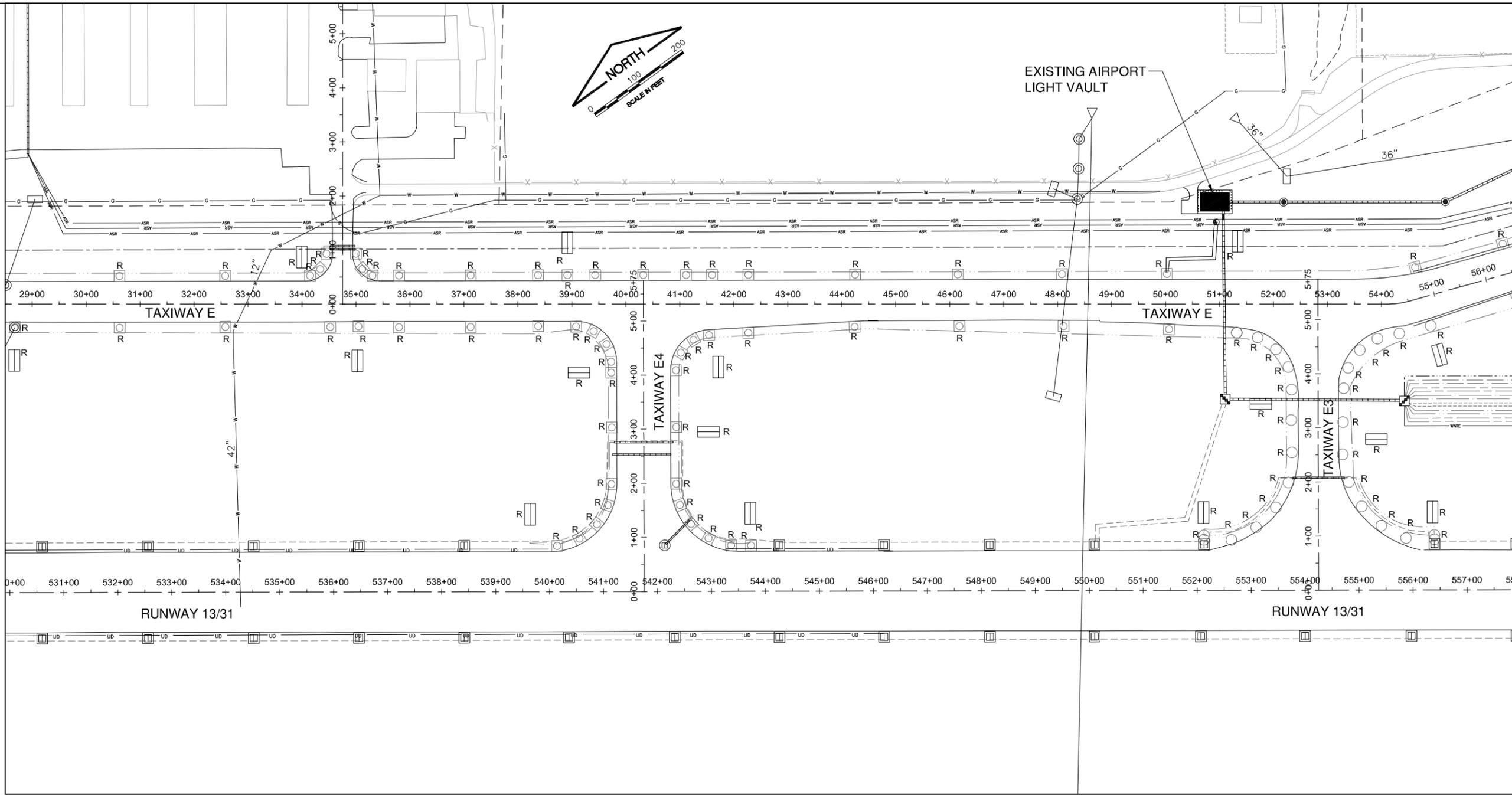
LEGEND

	REMOVE BASE MOUNTED LIGHT - AR125902
	REMOVE INPAVEMENT LIGHT - AR125903
	REMOVE TAXI GUIDANCE SIGN - AR125904
	EXISTING CIRCUIT 1
	EXISTING CIRCUIT 2
	EXISTING CIRCUIT 3
	EXISTING CIRCUIT 4
	EXISTING CIRCUIT 5
	EXISTING CIRCUIT 6
	EXISTING RUNWAY 4/22 CIRCUIT
	EXISTING RUNWAY 13/31 CIRCUIT

- NOTES**
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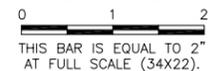
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 UPDATE BY: Chris Groth
 PLOT DATE: 5/25/2012 7:39 AM
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 1106107-E-AL
 PIA-Base-Existing
 1106107-V-AL
 KEYMAP
 UTILITY_Updated

PE093

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PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
REHABILITATE AIRFIELD LIGHTING
EXISTING CONDITIONS AND REMOVALS SHEET 2

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SHEET	18 OF 62 SHEETS

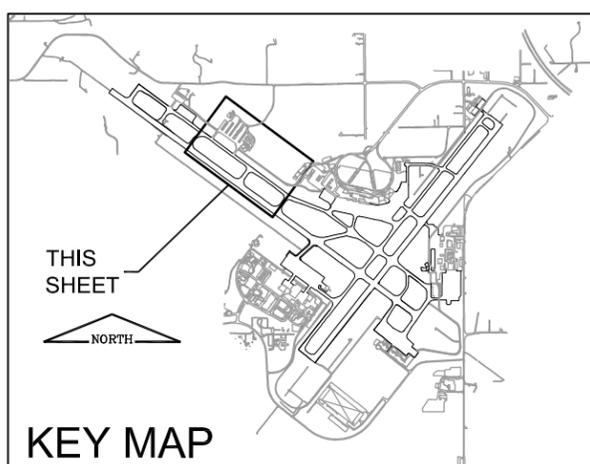
LEGEND

- R REMOVE BASE MOUNTED LIGHT - AR125902
- R REMOVE INPAVEMENT LIGHT - AR125903
- R REMOVE TAXI GUIDANCE SIGN - AR125904

- EXISTING CIRCUIT 1
- EXISTING CIRCUIT 2
- EXISTING CIRCUIT 3
- EXISTING CIRCUIT 4
- EXISTING CIRCUIT 5
- EXISTING CIRCUIT 6
- EXISTING RUNWAY 4/22 CIRCUIT
- EXISTING RUNWAY 13/31 CIRCUIT

NOTES

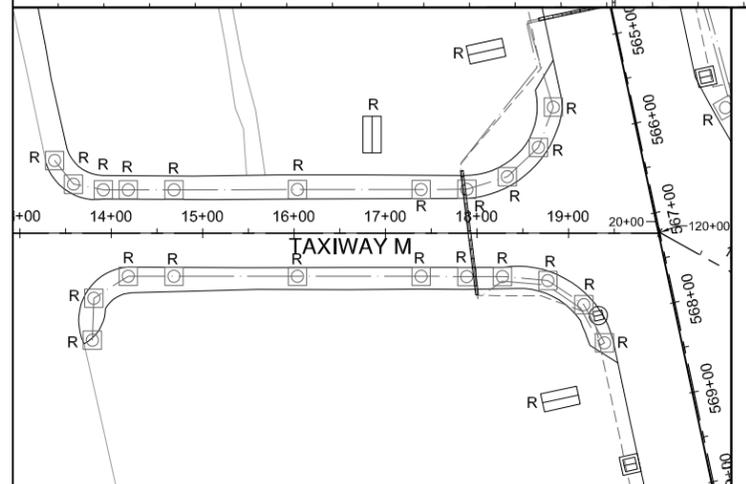
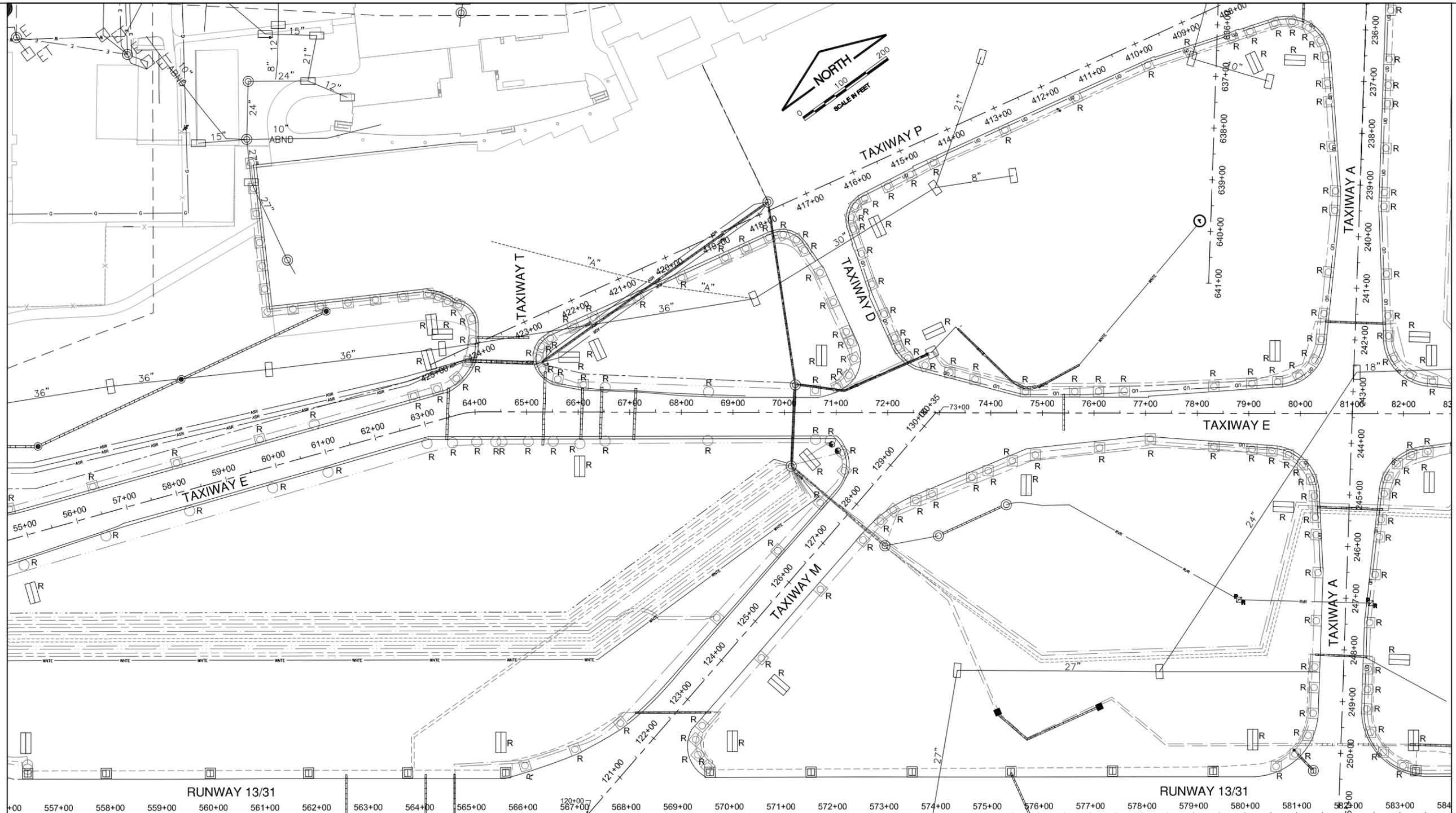
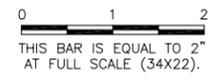
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KEY MAP

PE093

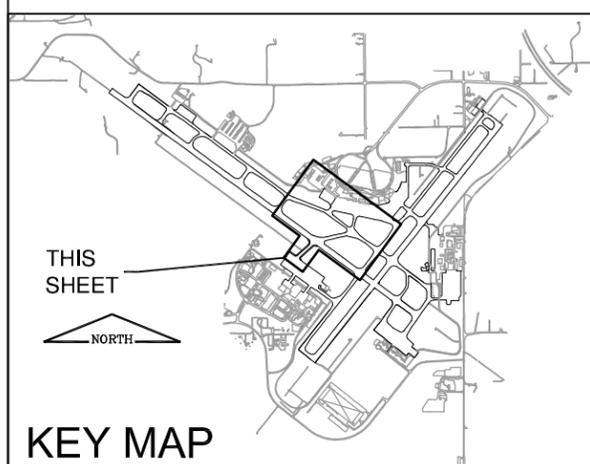
REVISIONS		
NUMBER	BY	DATE



LEGEND

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	REMOVE INPAVEMENT LIGHT - AR125903
	REMOVE TAXI GUIDANCE SIGN - AR125904
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REHABILITATE AIRFIELD LIGHTING
EXISTING CONDITIONS AND REMOVALS SHEET 3

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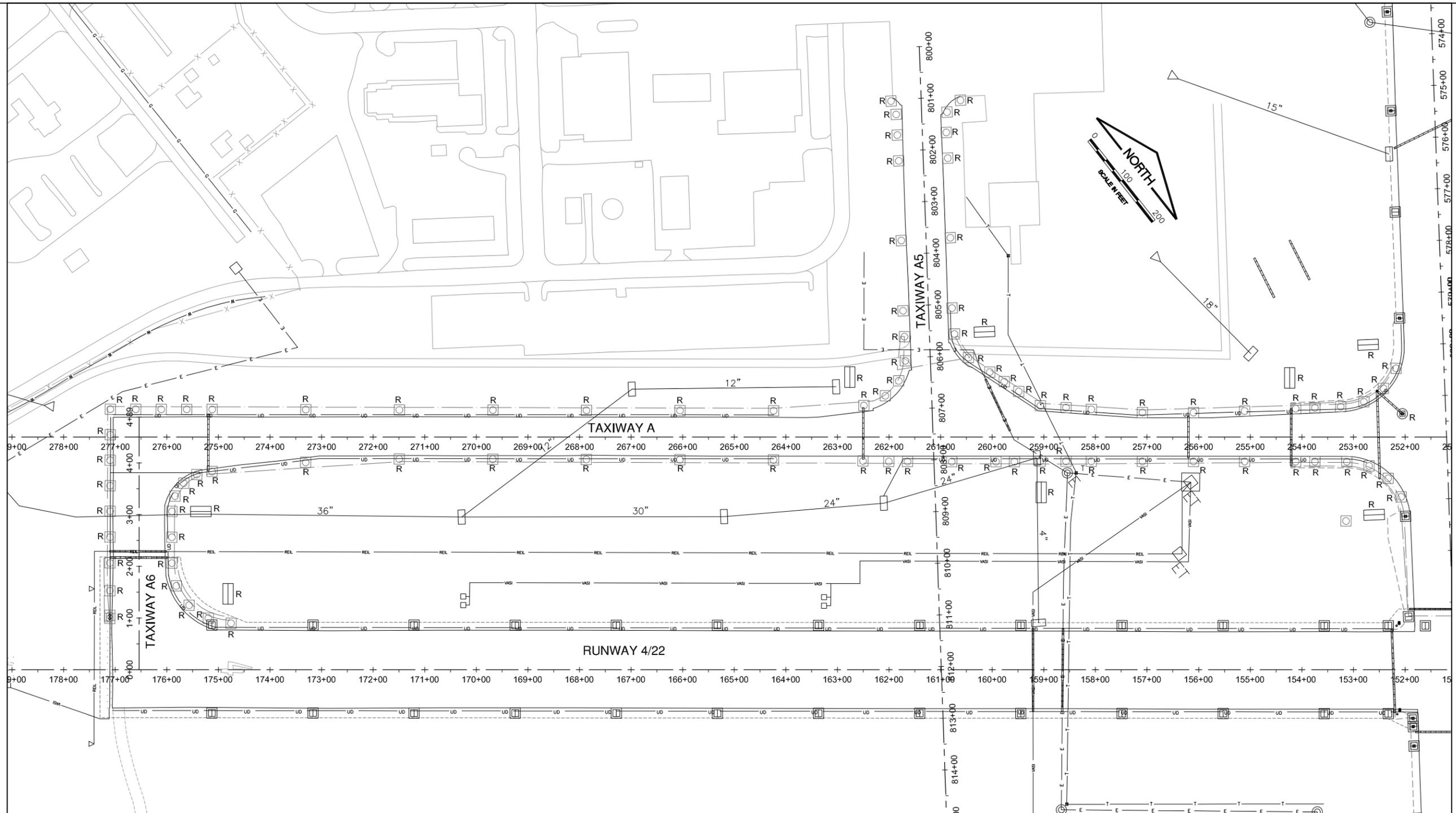
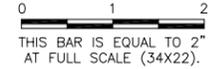
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JOB No:	11061-07-00
IL PROJ. NO. PIA-4179	
AIP PROJ. NO. 3-17-0080-XX	
SHEET 19 OF 62 SHEETS	

PE093

REVISIONS		
NUMBER	BY	DATE



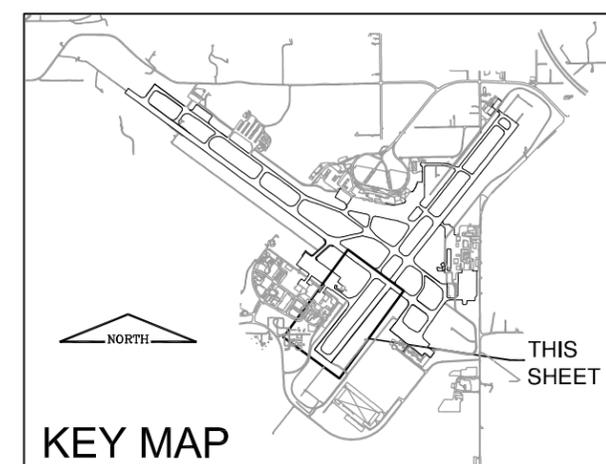
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PEORIA, ILLINOIS
REHABILITATE AIRFIELD LIGHTING
EXISTING CONDITIONS AND REMOVALS SHEET 4

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AIP PROJ. NO.	3-17-0080-XX
SHEET	20 OF 62 SHEETS



KEY MAP

LEGEND

- REMOVE BASE MOUNTED LIGHT - AR125902
- REMOVE INPAVEMENT LIGHT - AR125903
- REMOVE TAXI GUIDANCE SIGN - AR125904

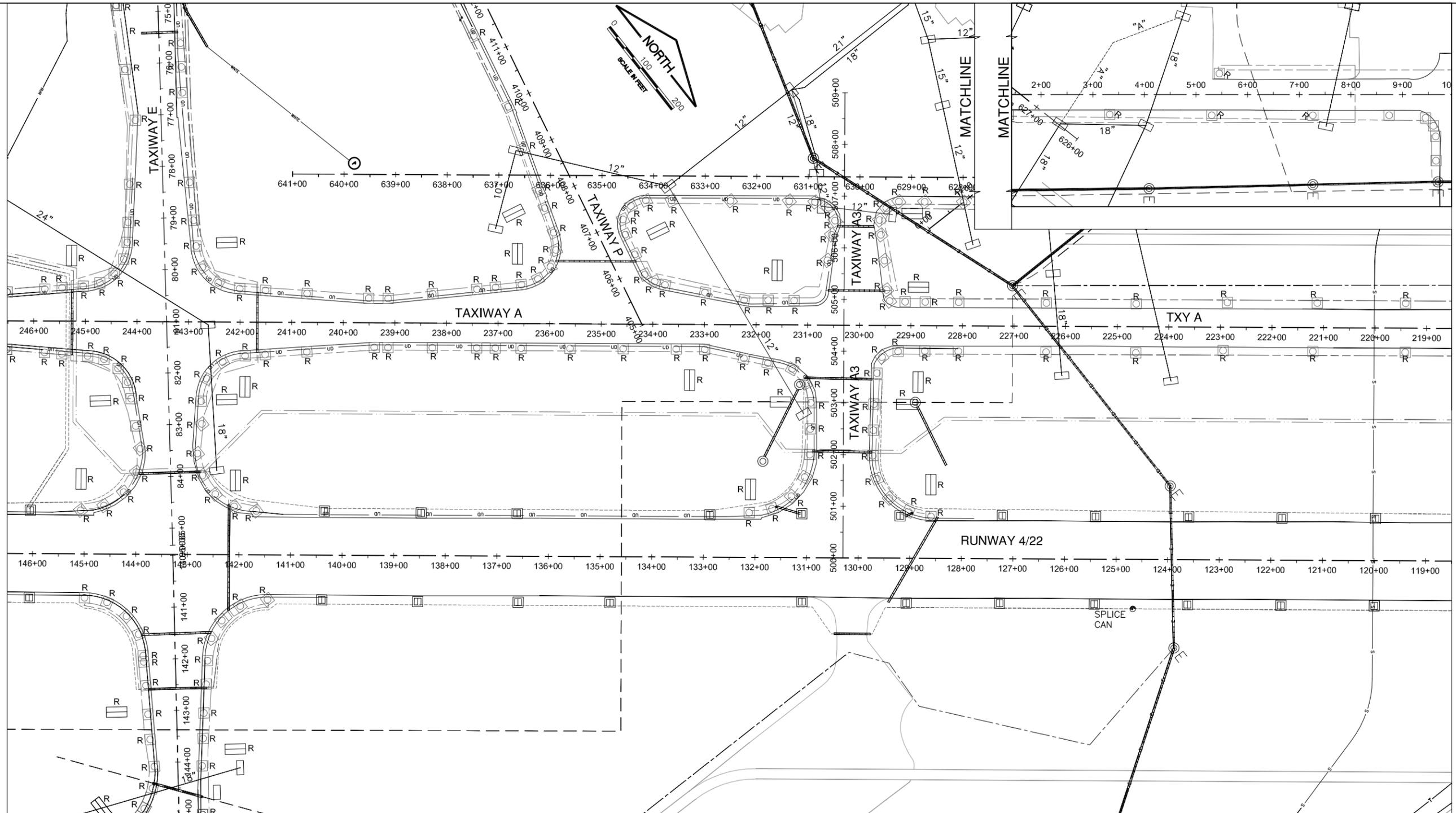
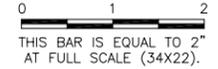
- EXISTING CIRCUIT 1
- EXISTING CIRCUIT 2
- EXISTING CIRCUIT 3
- EXISTING CIRCUIT 4
- EXISTING CIRCUIT 5
- EXISTING CIRCUIT 6
- EXISTING RUNWAY 4/22 CIRCUIT
- EXISTING RUNWAY 13/31 CIRCUIT

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PE093

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NUMBER	BY	DATE



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 PEORIA, ILLINOIS**

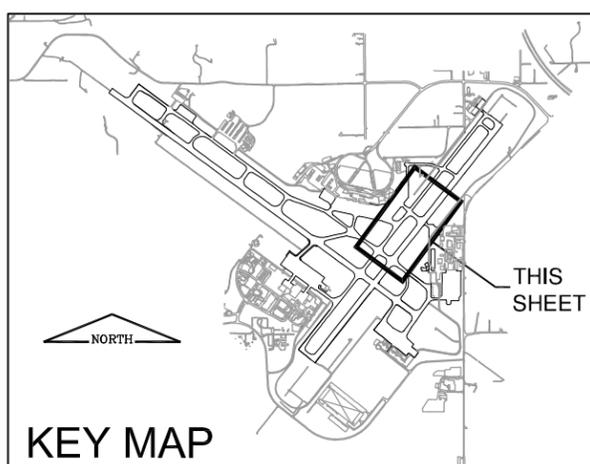
**REHABILITATE AIRFIELD LIGHTING
 EXISTING CONDITIONS AND REMOVALS SHEET 5**

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SHEET	21 OF 62 SHEETS



LEGEND

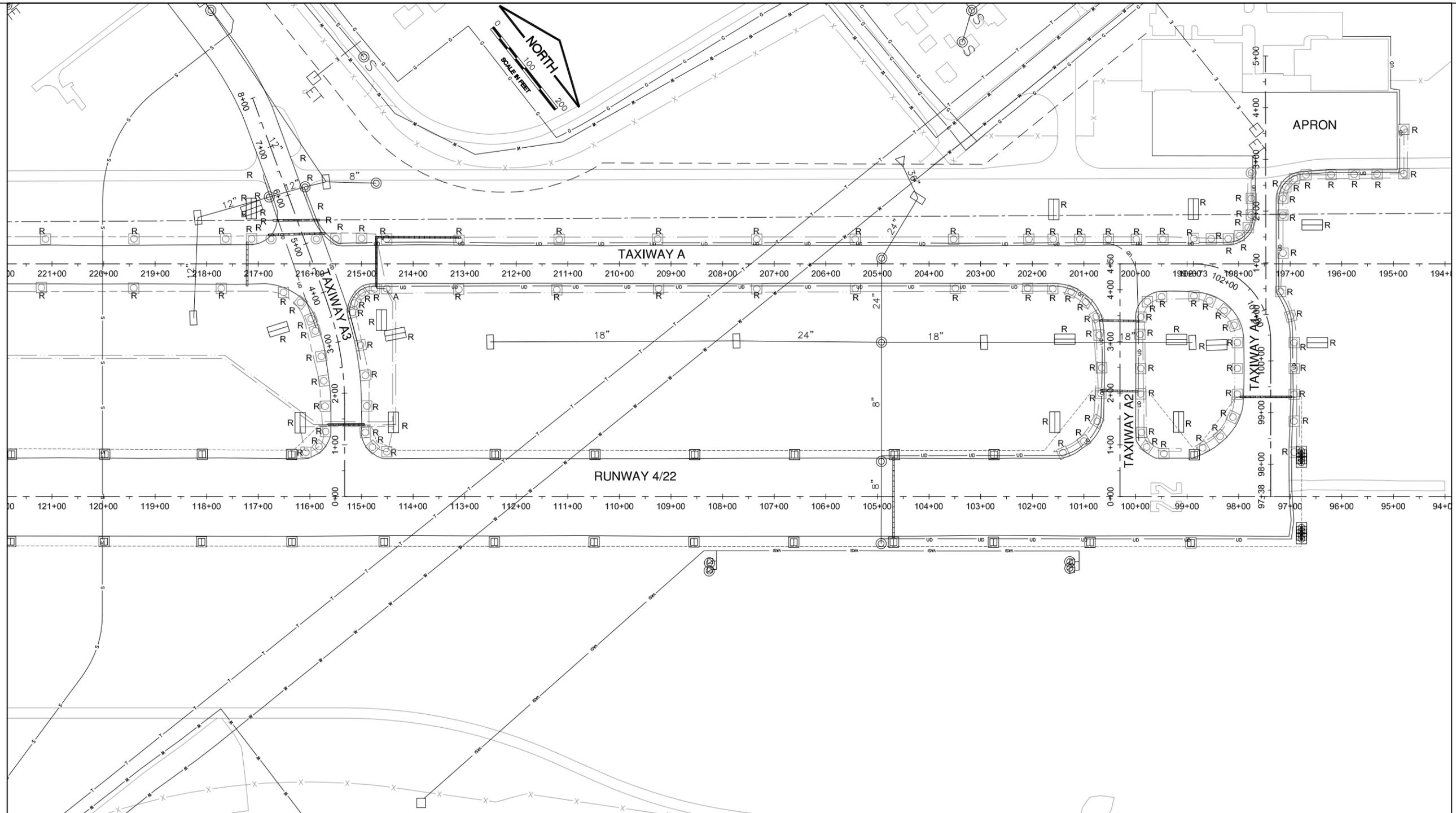
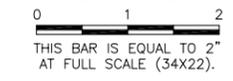
- REMOVE BASE MOUNTED LIGHT - AR125902
- REMOVE INPAVEMENT LIGHT - AR125903
- REMOVE TAXI GUIDANCE SIGN - AR125904
- EXISTING CIRCUIT 1
- EXISTING CIRCUIT 2
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REVISIONS		
NUMBER	BY	DATE



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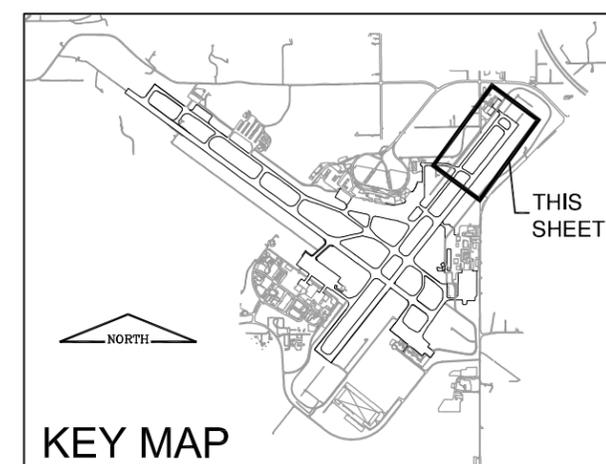
**REHABILITATE AIRFIELD LIGHTING
 EXISTING CONDITIONS AND REMOVALS SHEET 6**

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IL PROJ. NO.	PIA-4179
AIP PROJ. NO.	3-17-0080-XX
SHEET	22 OF 62 SHEETS



LEGEND

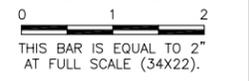
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- REMOVE INPAVEMENT LIGHT - AR125903
- REMOVE TAXI GUIDANCE SIGN - AR125904
- EXISTING CIRCUIT 1
- EXISTING CIRCUIT 2
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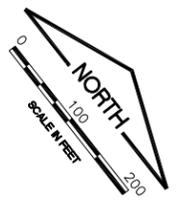
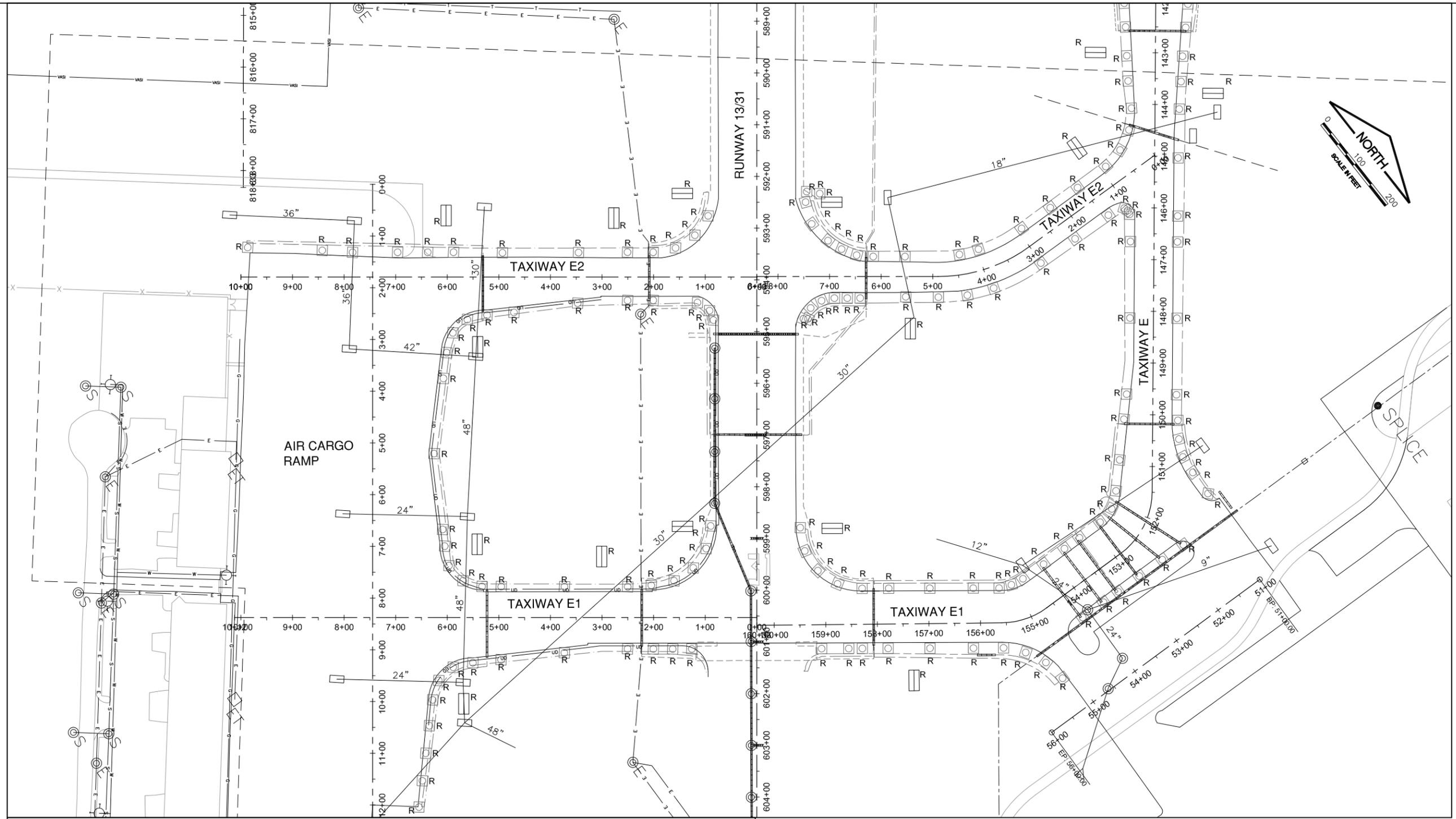
**REHABILITATE AIRFIELD LIGHTING
 EXISTING CONDITIONS AND REMOVALS SHEET 7**

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SHEET	23 OF 62 SHEETS



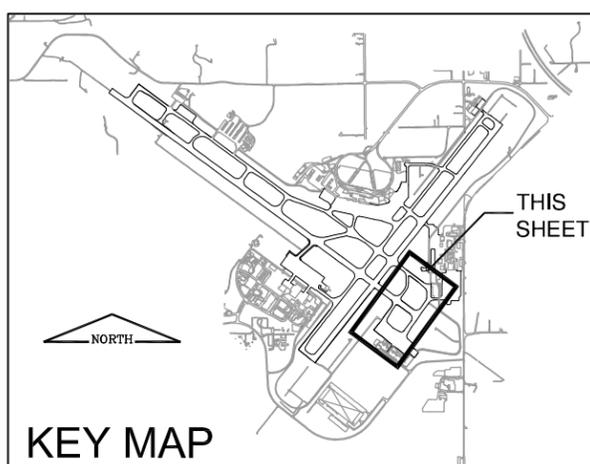
LEGEND

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- REMOVE INPAVEMENT LIGHT - AR125903
- REMOVE TAXI GUIDANCE SIGN - AR125904

- EXISTING CIRCUIT 1
- EXISTING CIRCUIT 2
- EXISTING CIRCUIT 3
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NOTES

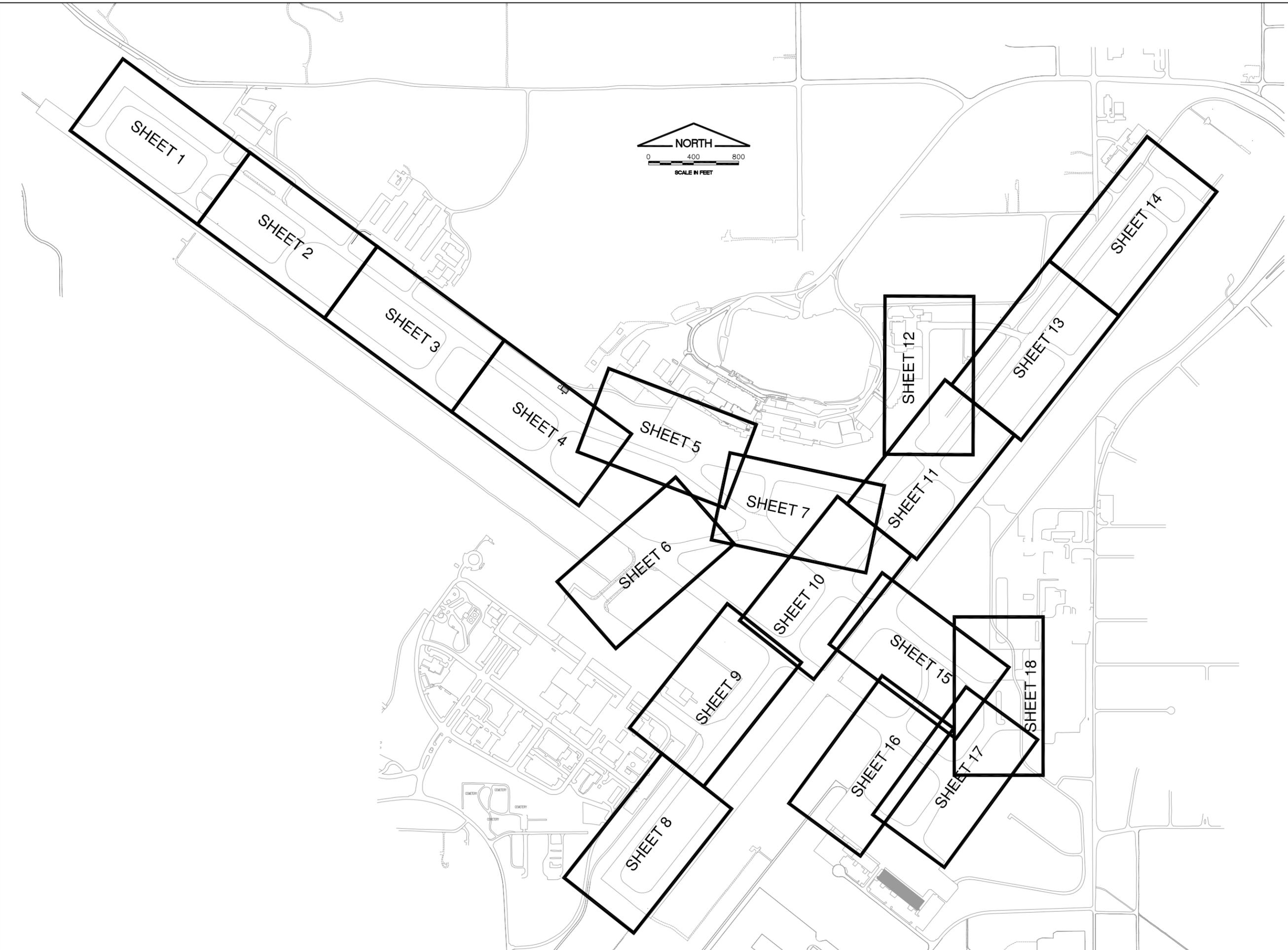
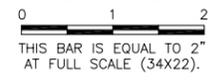
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KEY MAP

PE093

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NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 INDEX TO LIGHT LOCATION SHEETS**

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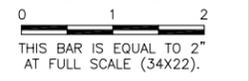
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 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

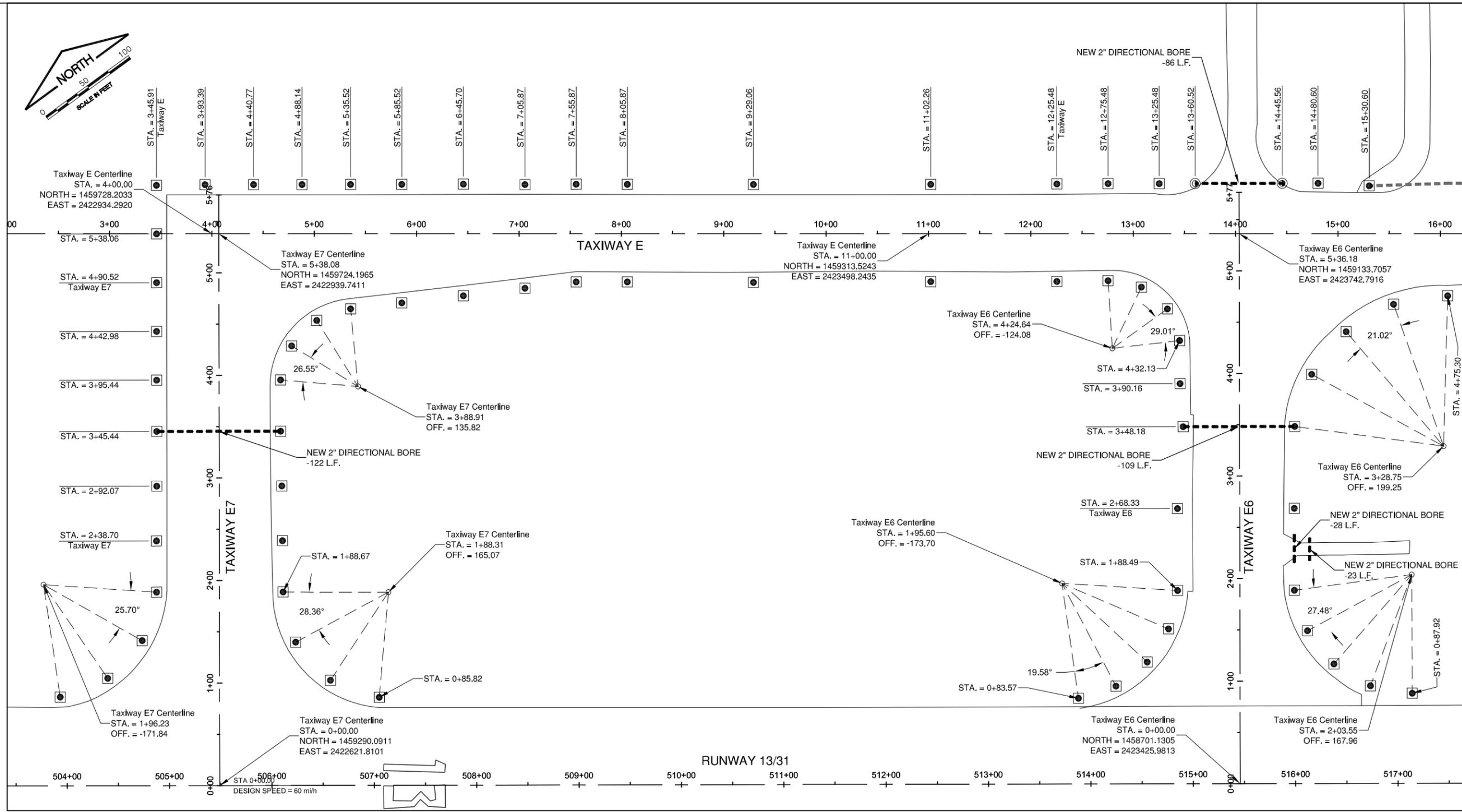
**REHABILITATE AIRFIELD LIGHTING
 LIGHT LOCATIONS SHEET 1**

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AIP PROJ. NO.	3-17-0080-XX
SHEET	25 OF 62 SHEETS

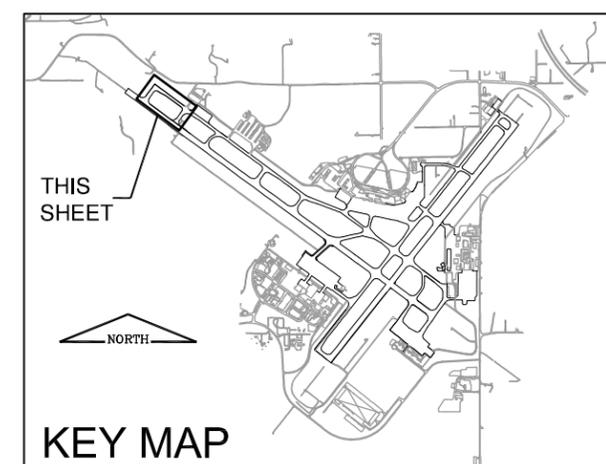


LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
	NEW 2" DIRECTIONAL BORE - AR110012
	EXISTING AIRFIELD DUCT TO BE USED

- NOTES**
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 - THE LOCATION OF THE P.T. AND P.C. HAVE BEEN TAKEN FROM RECORD DRAWINGS. CONTRACTOR SHALL VERIFY LAYOUT OF LIGHTS WITH R.E. PRIOR TO INSTALLING.
 - LIGHTS SHALL BE INSTALLED IN A STRAIGHT LINE FROM P.T. OR P.C. OR TAPER POINT.

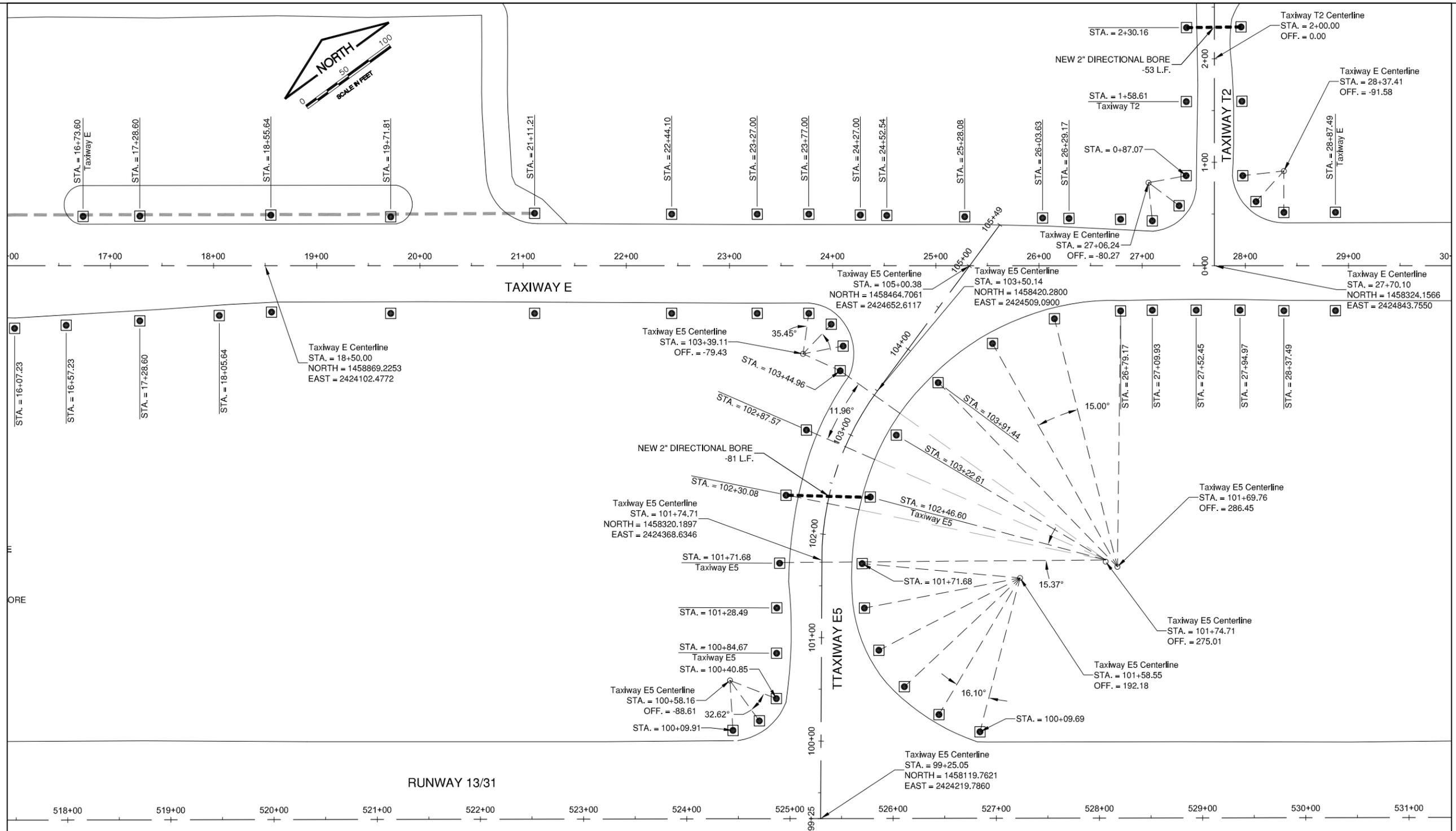
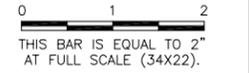
ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L5	Runway 13-31	STA. 495+00.00	N:1459909.9516 E:2421775.4480	STA. 615+00.00	N: 1452819.5772 E: 2431456.6975
L4	Taxiway E7	STA. 0+00.00	N:1459290.0911 E:2422621.8101	STA. 5+76.00	N: 1459754.7937 E: 2422962.1499
L29	Taxiway E6	STA. 0+00.00	N:1458701.1305 E:2423425.9813	STA. 5+76.54	N: 1459166.2669 E: 2423766.6389
L1	Taxiway E	STA. 0+00.00	N:1459965.1627 E:2422612.0340	STA. 54+11.17	N: 1456759.5943 E: 2426971.5145



KEY MAP

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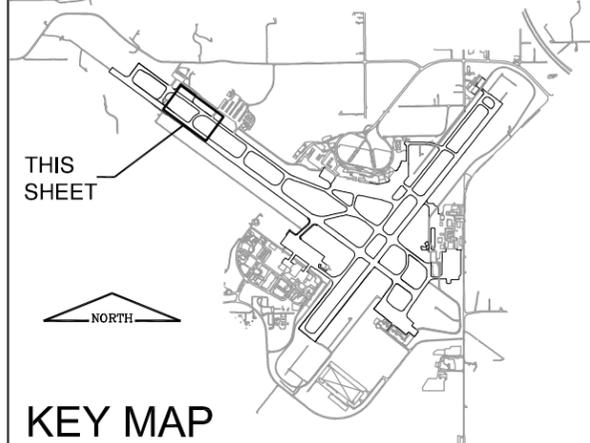
LEGEND

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ALIGNMENT DATA

ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L34	Runway 13-31	STA. 495+00.00	N:1459909.9516 E:2421775.4480	STA. 615+00.00	N: 1452819.5772 E: 2431456.6975
L19	Taxiway E5	STA. 103+50.14	N:1458420.2800 E:2424509.0900	STA. 105+49.45	N: 1458479.2154 E: 2424699.4849
L20	Taxiway E5	STA. 99+25.05	N:1458119.7621 E:2424219.7860	STA. 101+74.71	N: 1458320.1897 E: 2424368.6346
C6	Taxiway E5	CENTER STA. 102+65.52	PI N:1458393.4259 PI E:2424422.3360	R = 275.008	CENTER N: 1458157.5700 E: 2424590.4100
L33	Taxiway E	STA. 0+00.00	N:1459965.1627 E:2422612.0340	STA. 54+11.17	N: 1456759.5943 E: 2426971.5145
L28	Taxiway T2	STA. 0+00.00	N:1458324.1566 E:2424843.7550	STA. 6+00.00	N: 1458807.5436 E: 2425199.1942



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 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 LIGHT LOCATIONS SHEET 2**

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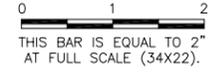
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JOB No:	11061-07-00
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AIP PROJ. NO.	3-17-0080-XX
SHEET	26 OF 62 SHEETS

PE093

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NUMBER	BY	DATE



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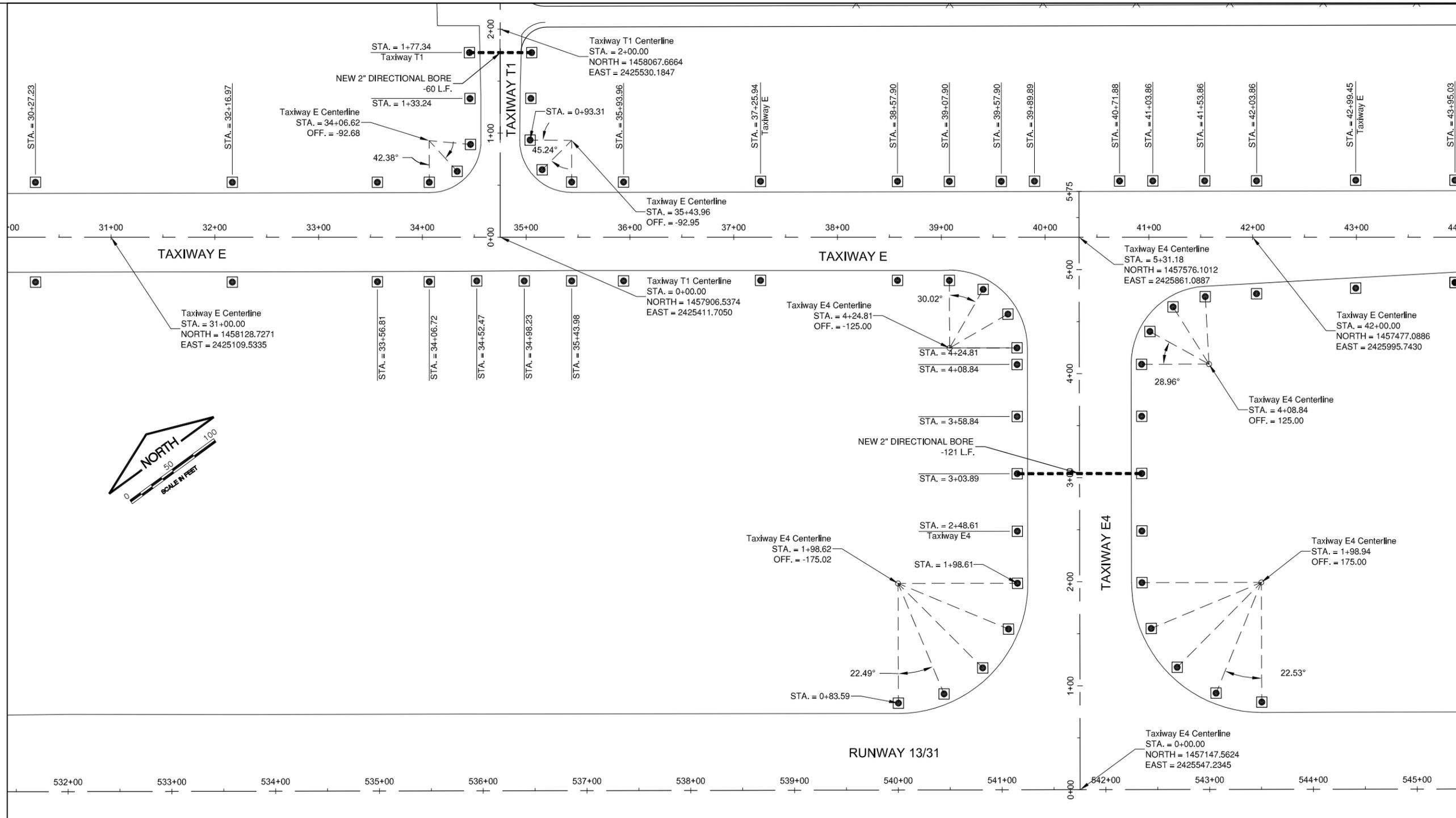
**REHABILITATE AIRFIELD LIGHTING
 LIGHT LOCATIONS SHEET 3**

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SHEET	27 OF 62 SHEETS



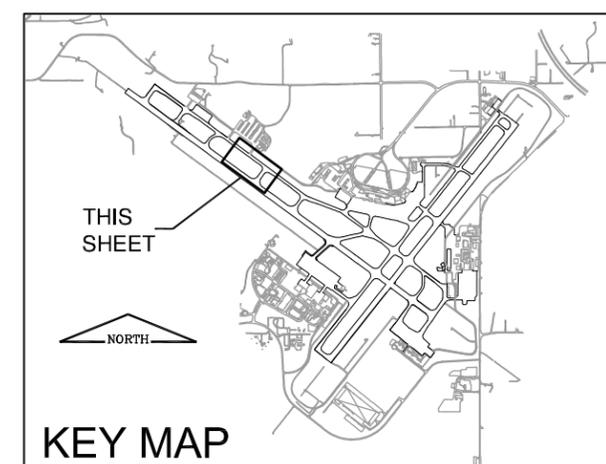
LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
	NEW 2" DIRECTIONAL BORE - AR110012
	EXISTING AIRFIELD DUCT TO BE USED

- NOTES**
- LIGHT SHALL BE LOCATED NO MORE THAN 10' FROM THE EXISTING PAVEMENT EDGE.
 - THE LOCATION OF THE P.T. AND P.C. HAVE BEEN TAKEN FROM RECORD DRAWINGS. CONTRACTOR SHALL VERIFY LAYOUT OF LIGHTS WITH R.E. PRIOR TO INSTALLING.
 - LIGHTS SHALL BE INSTALLED IN A STRAIGHT LINE FROM P.T. OR P.C. OR TAPER POINT.

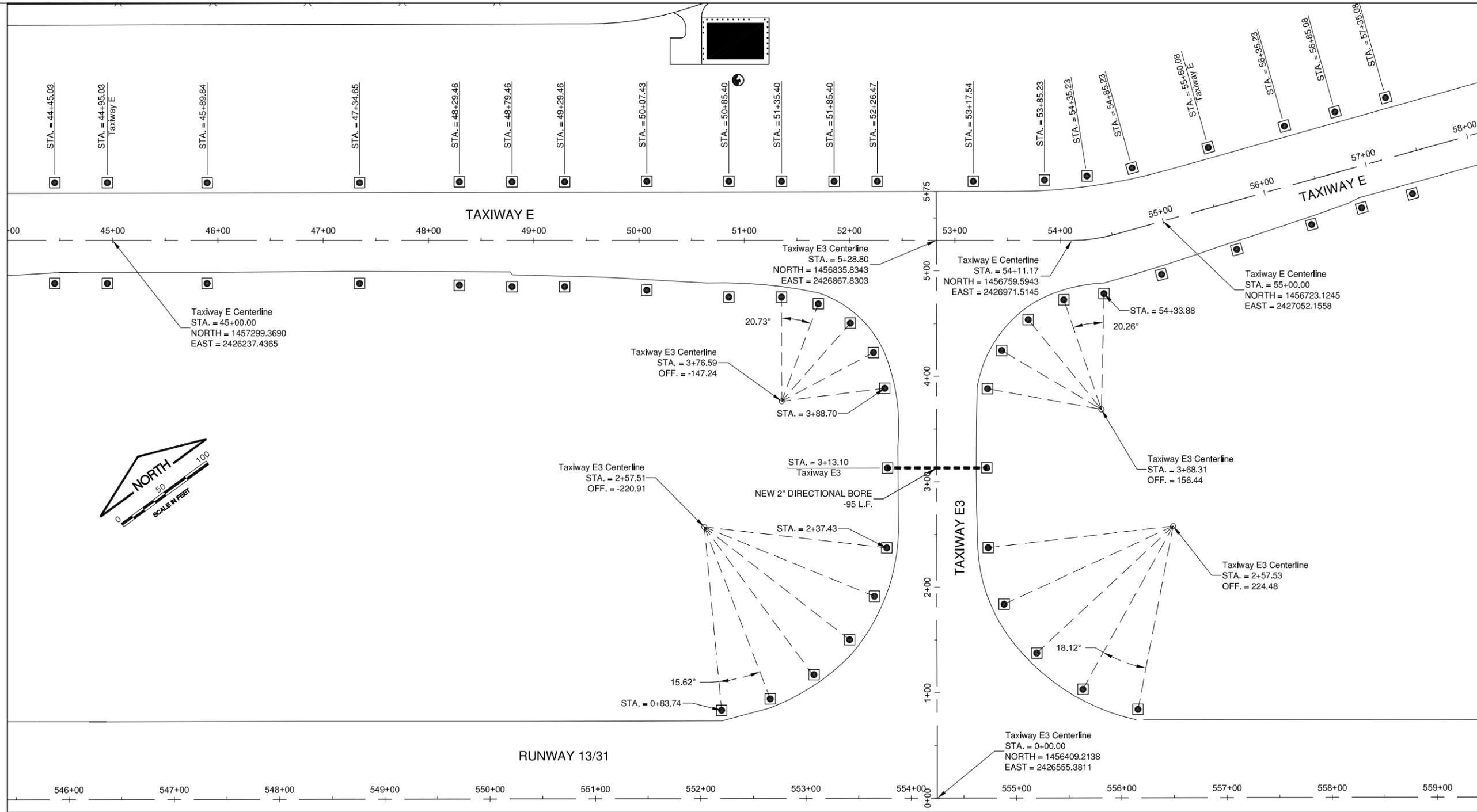
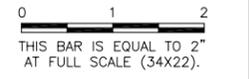
ALIGNMENT DATA

ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L37	Taxiway T1	STA. 0+00.00	N:1457906.5374 E:2425411.7050	STA. 6+00.00	N: 1458389.9244 E: 2425767.1442
L35	Runway 13-31	STA. 495+00.00	N:1459909.9516 E:2421775.4480	STA. 615+00.00	N: 1452819.5772 E: 2431456.6975
L27	Taxiway E4	STA. 0+00.00	N:1457147.5624 E:2425547.2345	STA. 5+75.07	N: 1457611.5150 E: 2425887.0251
L36	Taxiway E	STA. 0+00.00	N:1459965.1627 E:2422612.0340	STA. 54+11.17	N: 1456759.5943 E: 2426971.5145



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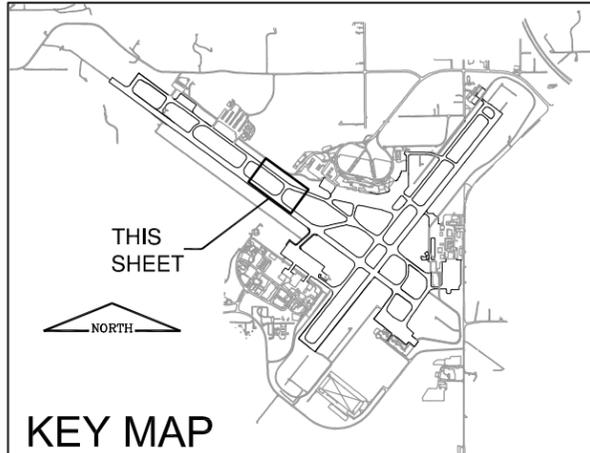
**REHABILITATE AIRFIELD LIGHTING
 LIGHT LOCATIONS SHEET 4**

LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
	NEW 2" DIRECTIONAL BORE - AR110012
	EXISTING AIRFIELD DUCT TO BE USED

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ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L38	Taxiway E	STA. 0+00.00	N:1459965.1627 E:2422612.0340	STA. 54+11.17	N: 1456759.5943 E: 2426971.5145
C9	Taxiway E	CENTER STA. 54+31.65	PI N:1456747.4625 PI E:2426988.0134	R = 150.000	CENTER N: 1456880.4411 E: 2427060.3743
L2	Taxiway E	STA. 54+51.87	N:1456740.1974 E:2427007.1605	STA. 63+67.15	N: 1456415.4940 E: 2427862.9086
L39	Runway 13-31	STA. 495+00.00	N:1459909.9516 E:2421775.4480	STA. 615+00.00	N: 1452819.5772 E: 2431456.6975
L26	Taxiway E3	STA. 0+00.00	N:1456409.2138 E:2426555.3811	STA. 5+75.12	N: 1456873.2021 E: 2426895.1978



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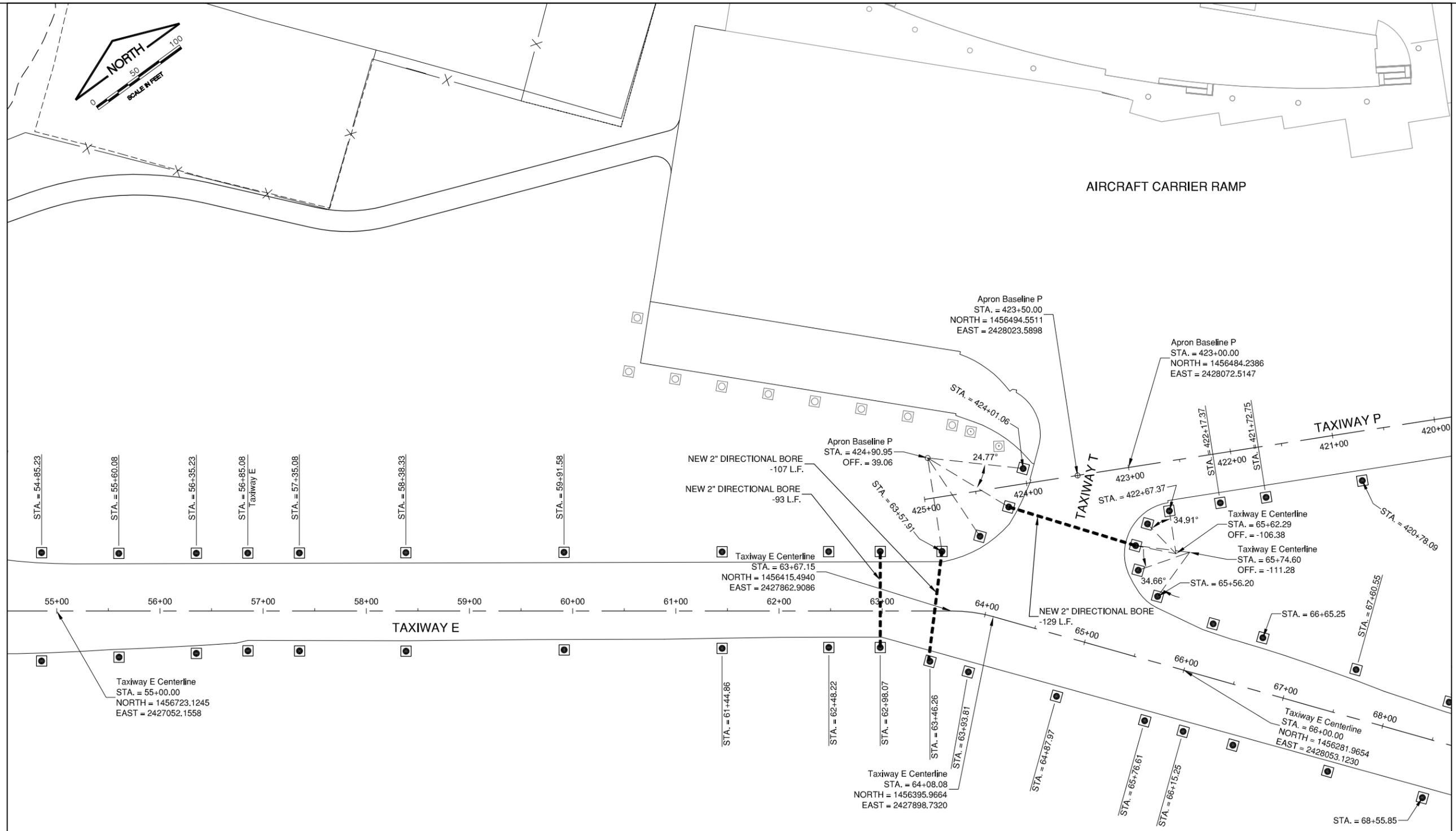
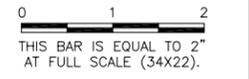
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SHEET	28 OF 62 SHEETS

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**REHABILITATE AIRFIELD LIGHTING
 LIGHT LOCATIONS SHEET 5**

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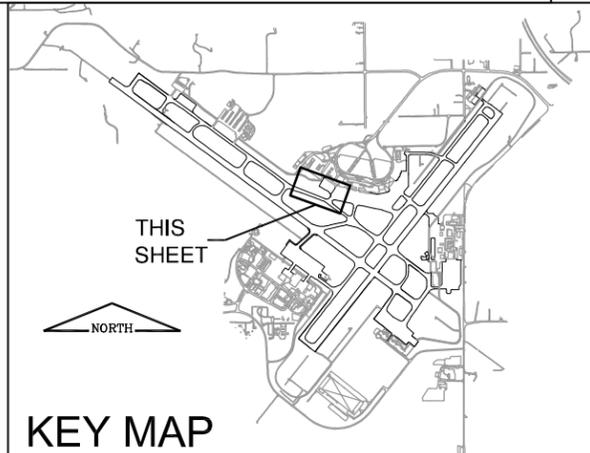
LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
	NEW 2" DIRECTIONAL BORE - AR110012
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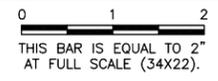
ALIGNMENT DATA

ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L22	Baseline P	STA. 405+00.00	N:1456112.9889 E:2429833.8136	STA. 425+00.00	N: 1456525.4886 E: 2427876.8149
L40	Taxiway E	STA. 54+51.87	N:1456740.1974 E:2427007.1605	STA. 63+67.15	N: 1456415.4940 E: 2427862.9086
L3	Taxiway E	STA. 64+08.08	N:1456395.9664 E:2427898.7320	STA. 85+32.89	N: 1455133.8144 E: 2429608.0582
C2	Taxiway E	CENTER STA. 63+87.75	PI N:1456408.1890 PI E:2427882.1606	R = 150.000	CENTER N: 1456275.2503 E: 2427809.6948



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 SHEET 29 OF 62 SHEETS

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 PEORIA, ILLINOIS**

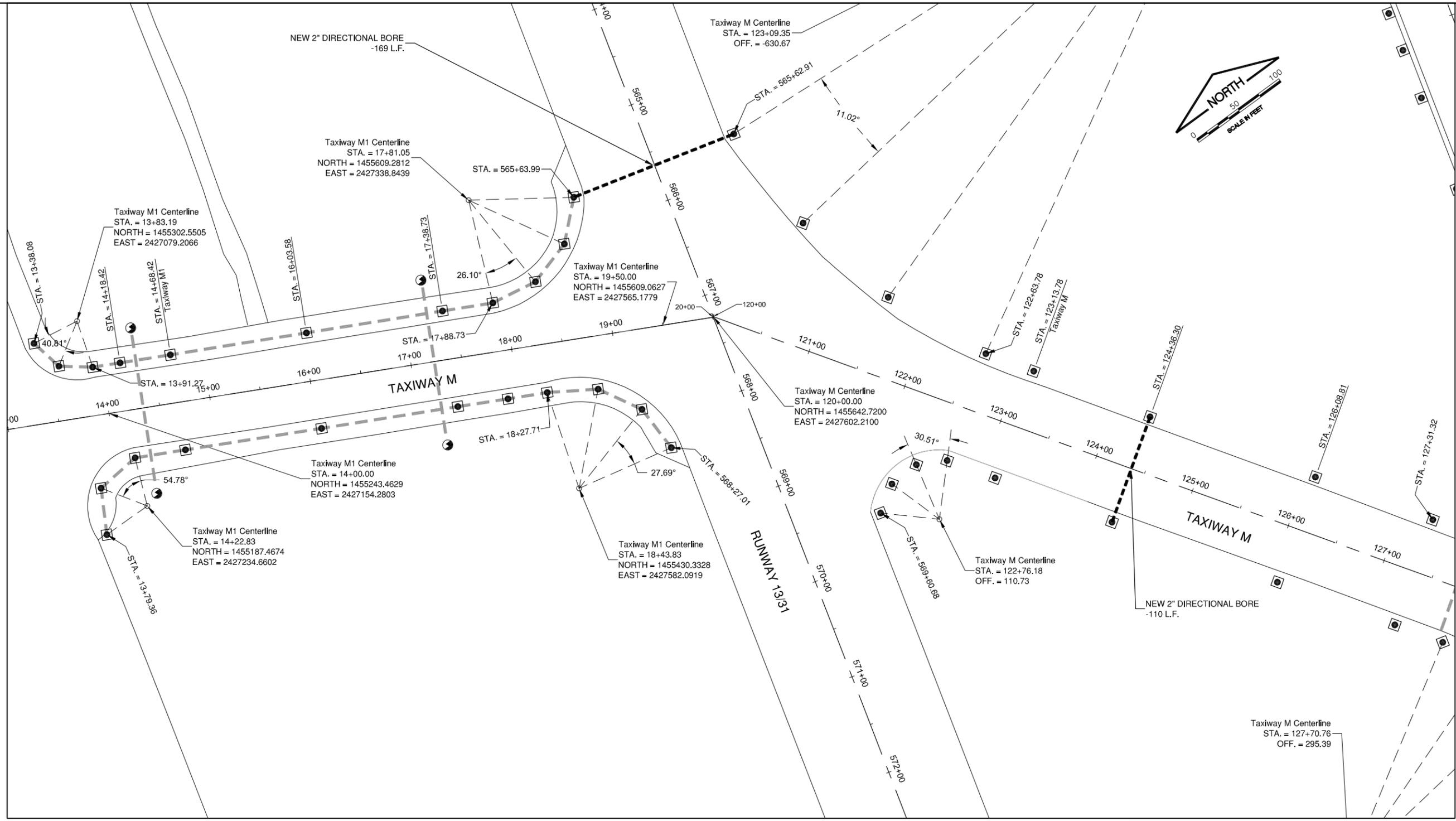
**REHABILITATE AIRFIELD LIGHTING
 LIGHT LOCATIONS SHEET 6**

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AIP PROJ. NO. 3-17-0080-XX	
SHEET 30 OF 62 SHEETS	

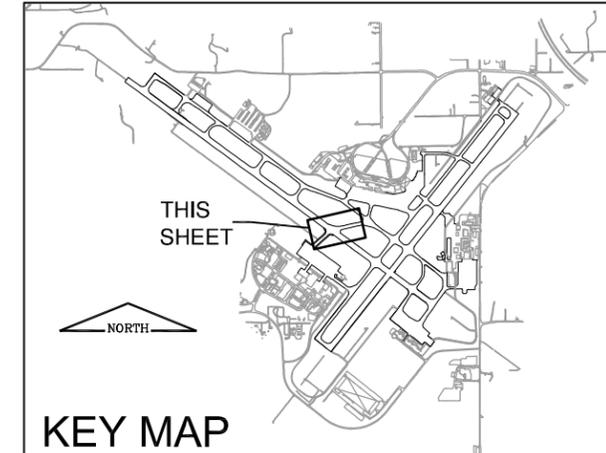


LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
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ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L42	Taxiway M1	STA. 10+00.00	N:1454977.5722 E:2426855.4457	STA. 20+00.00	N: 1455642.2990 E: 2427602.5322
L43	Runway 13-31	STA. 495+00.00	N:1459909.9516 E:2421775.4480	STA. 615+00.00	N: 1452819.5772 E: 2431456.6975
L44	Taxiway M	STA. 120+00.00	N:1455642.7200 E:2427602.2100	STA. 130+34.60	N: 1455870.2000 E: 2428611.4900



KEY MAP

PE093

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

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 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

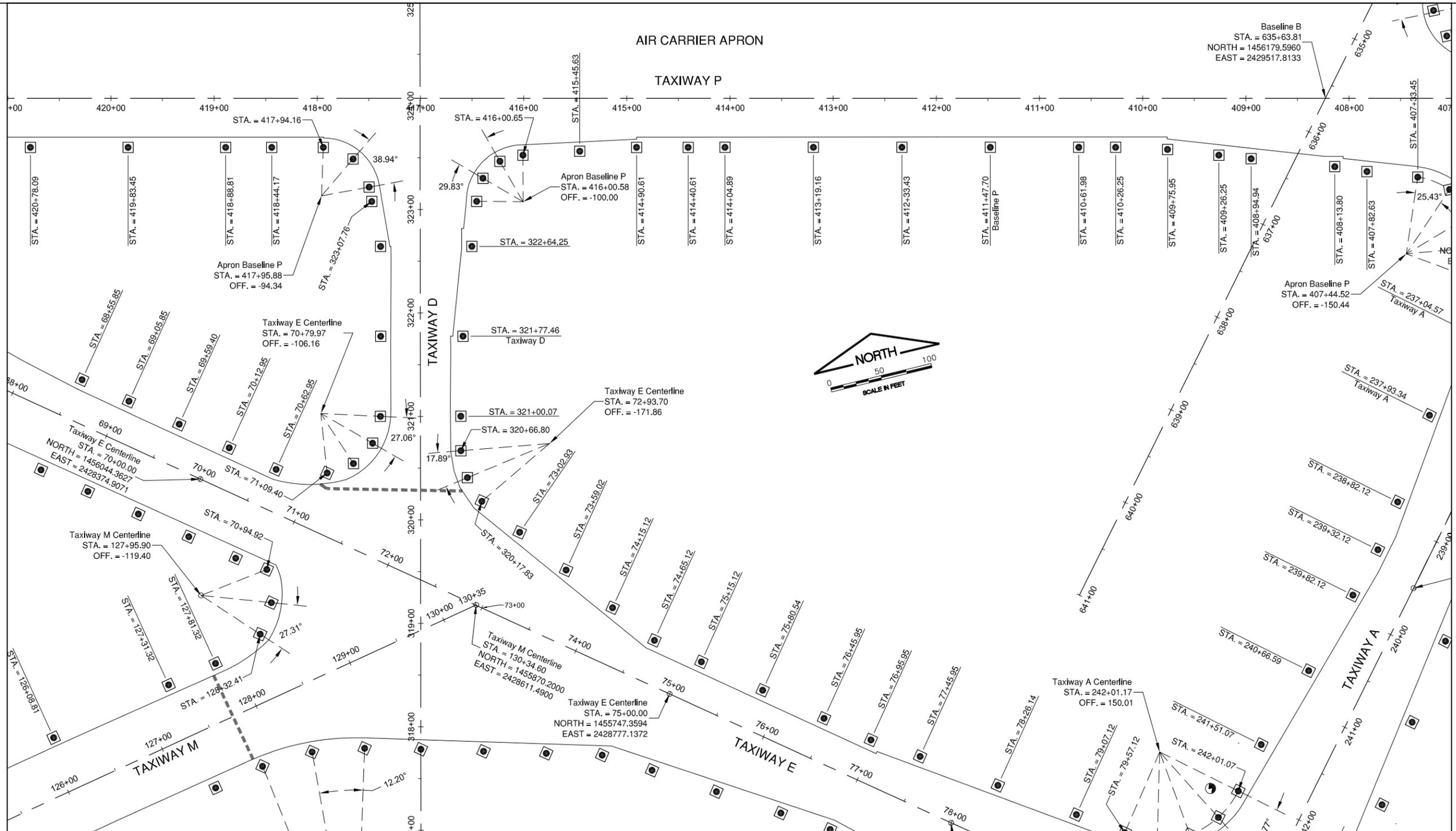
**REHABILITATE AIRFIELD LIGHTING
 LIGHT LOCATIONS SHEET 7**

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 SHEET 31 OF 62 SHEETS



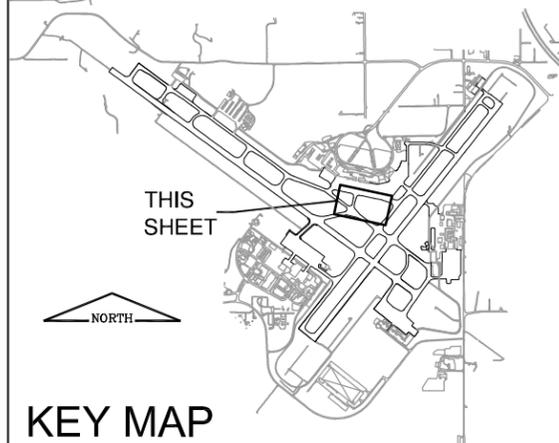
LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
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ALIGNMENT DATA

ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L22	Baseline P	STA. 405+00.00	N:1456112.9889 E:2429833.8136	STA. 425+00.00	N: 1456525.4886 E: 2427876.8149
L47	Taxiway M	STA. 120+00.00	N:1455642.7200 E:2427602.2100	STA. 130+34.60	N: 1455870.2000 E: 2428611.4900
L46	Taxiway E	STA. 64+08.08	N:1456395.9664 E:2427898.7320	STA. 85+32.89	N: 1455133.8144 E: 2429608.0582
L45	Taxiway A	STA. 190+00.00	N:1459583.2177 E:2432571.6840	STA. 285+00.00	N: 1452127.8839 E: 2426683.7306
L23	Taxiway D	STA. 315+00.00	N:1455472.3559 E:2428473.1486	STA. 325+00.00	N: 1456451.1092 E: 2428678.1898



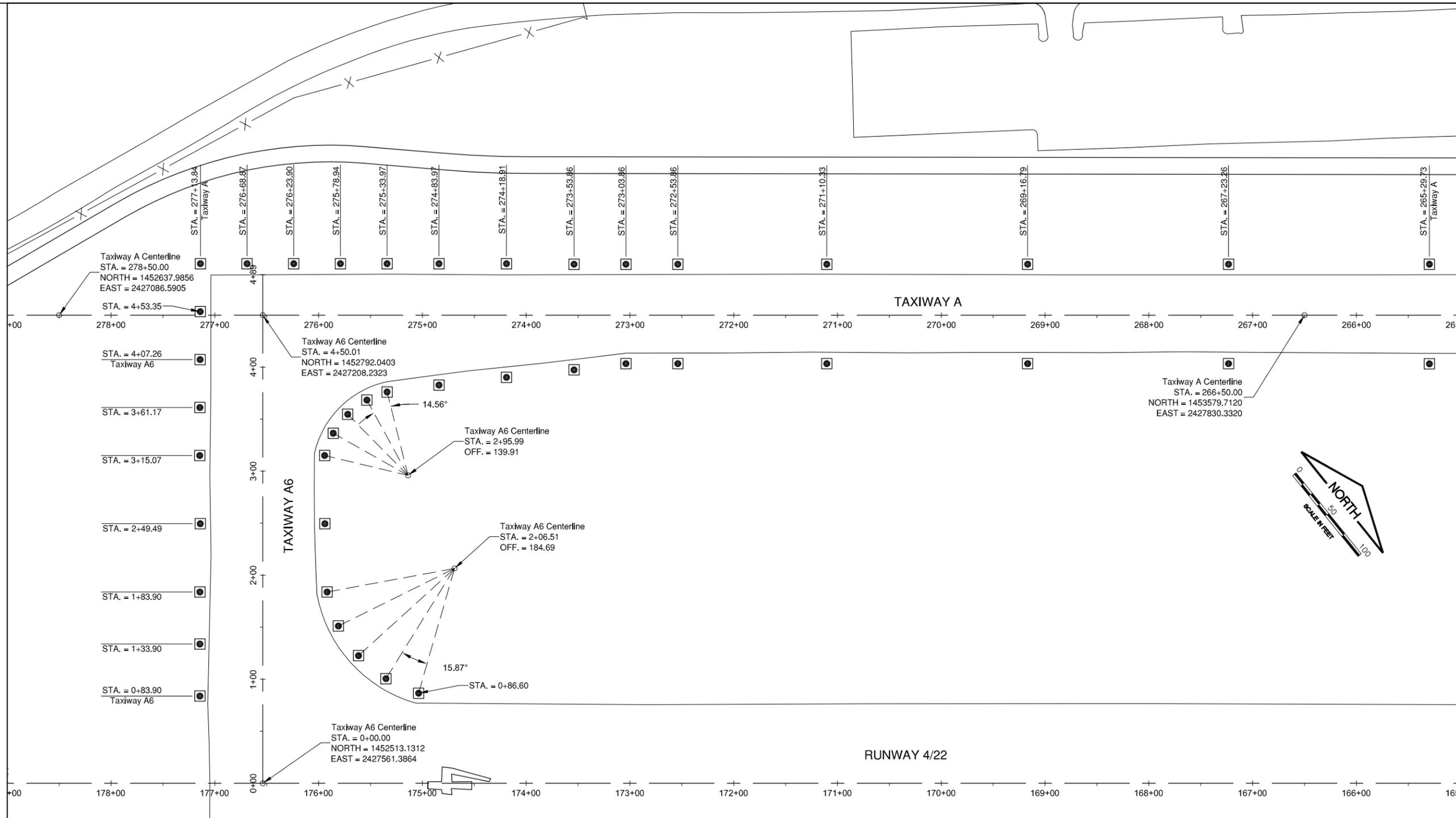
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 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 LIGHT LOCATIONS SHEET 8**

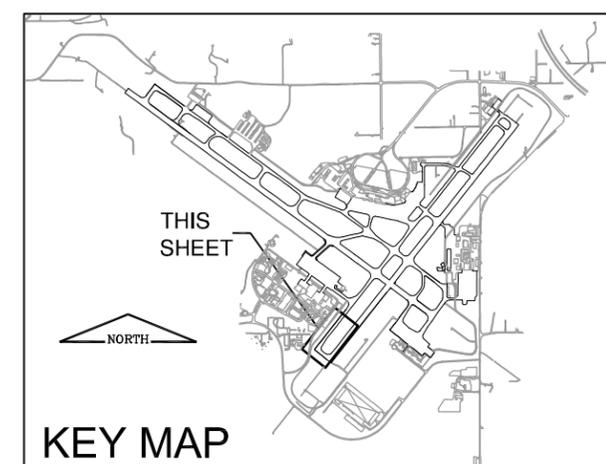


LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
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ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L13	Runway 04-22	STA. 90+00.00	N:1459304.3136 E:2432924.8318	STA. 185+00.00	N: 1451848.9877 E: 2427036.8683
L48	Taxiway A	STA. 190+00.00	N:1459583.2177 E:2432571.6840	STA. 285+00.00	N: 1452127.8839 E: 2426683.7306
L14	Taxiway A6	STA. 0+00.00	N:1452513.1312 E:2427561.3864	STA. 4+88.83	N: 1452816.1007 E: 2427177.7671



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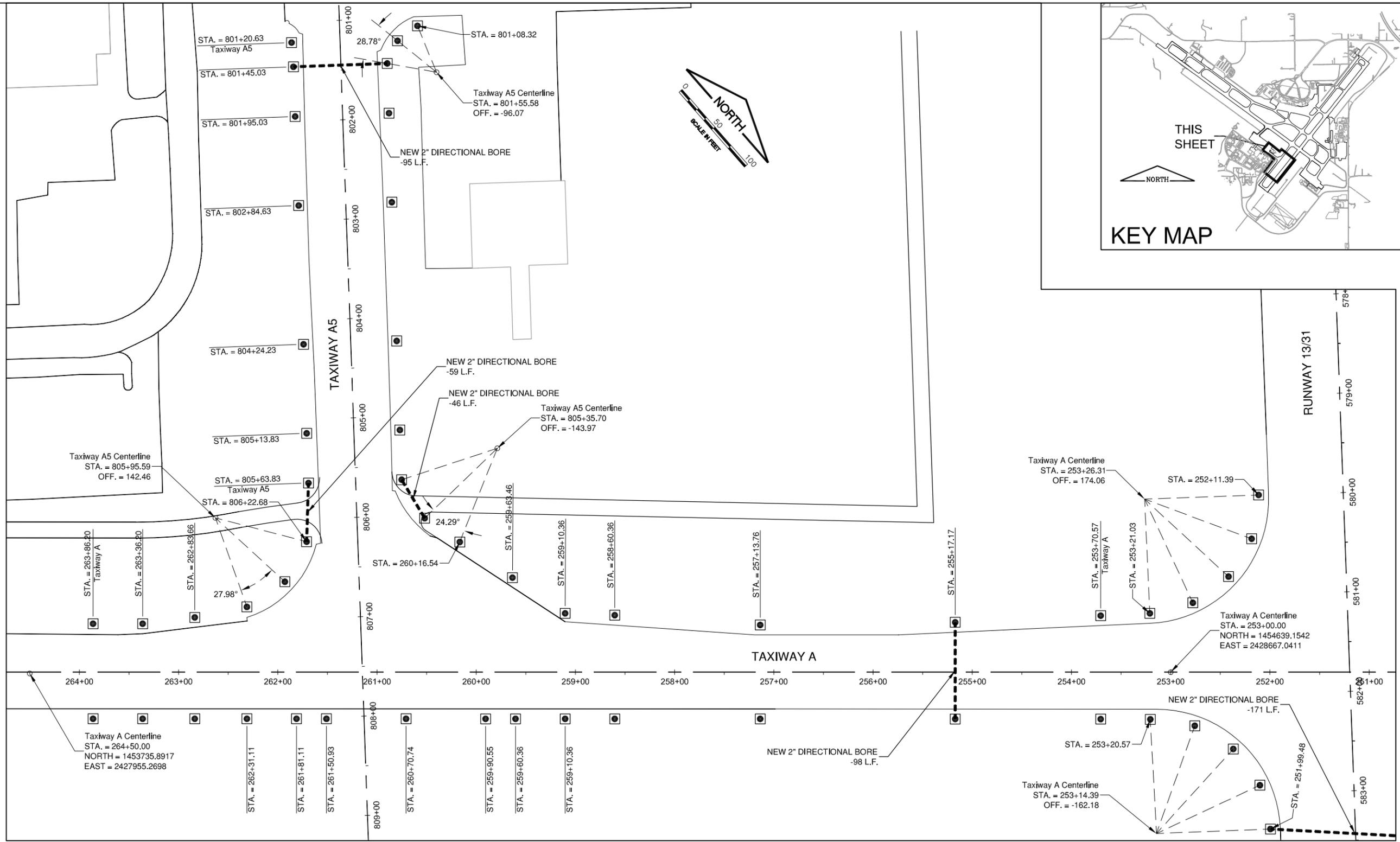
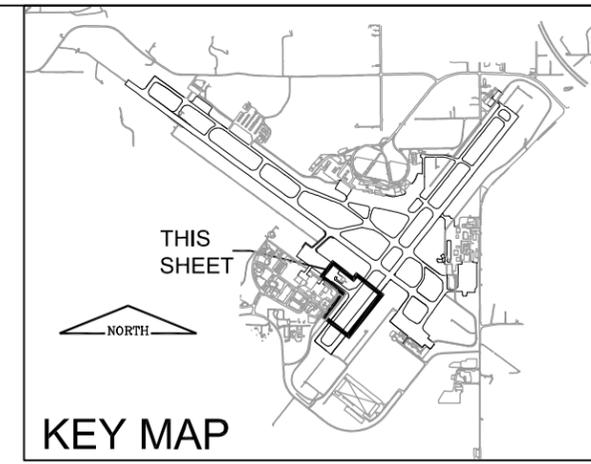
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PE093

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
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ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L51	Runway 13-31	STA. 495+00.00	N:1459909.9516 E:2421775.4480	STA. 615+00.00	N: 1452819.5772 E: 2431456.6975
L49	Taxiway A5	STA. 800+00.00	N:1454446.9160 E:2427552.6300	STA. 818+32.57	N: 1453362.8920 E: 2429030.1950
L50	Taxiway A	STA. 190+00.00	N:1459583.2177 E:2432571.6840	STA. 285+00.00	N: 1452127.8839 E: 2426683.7306

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 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 LIGHT LOCATIONS SHEET 9**

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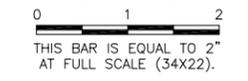
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IL. PROJ. NO. PIA-4179 AIP PROJ. NO. 3-17-0080-XX	
SHEET	33 OF 62 SHEETS

PE093

REVISIONS		
NUMBER	BY	DATE



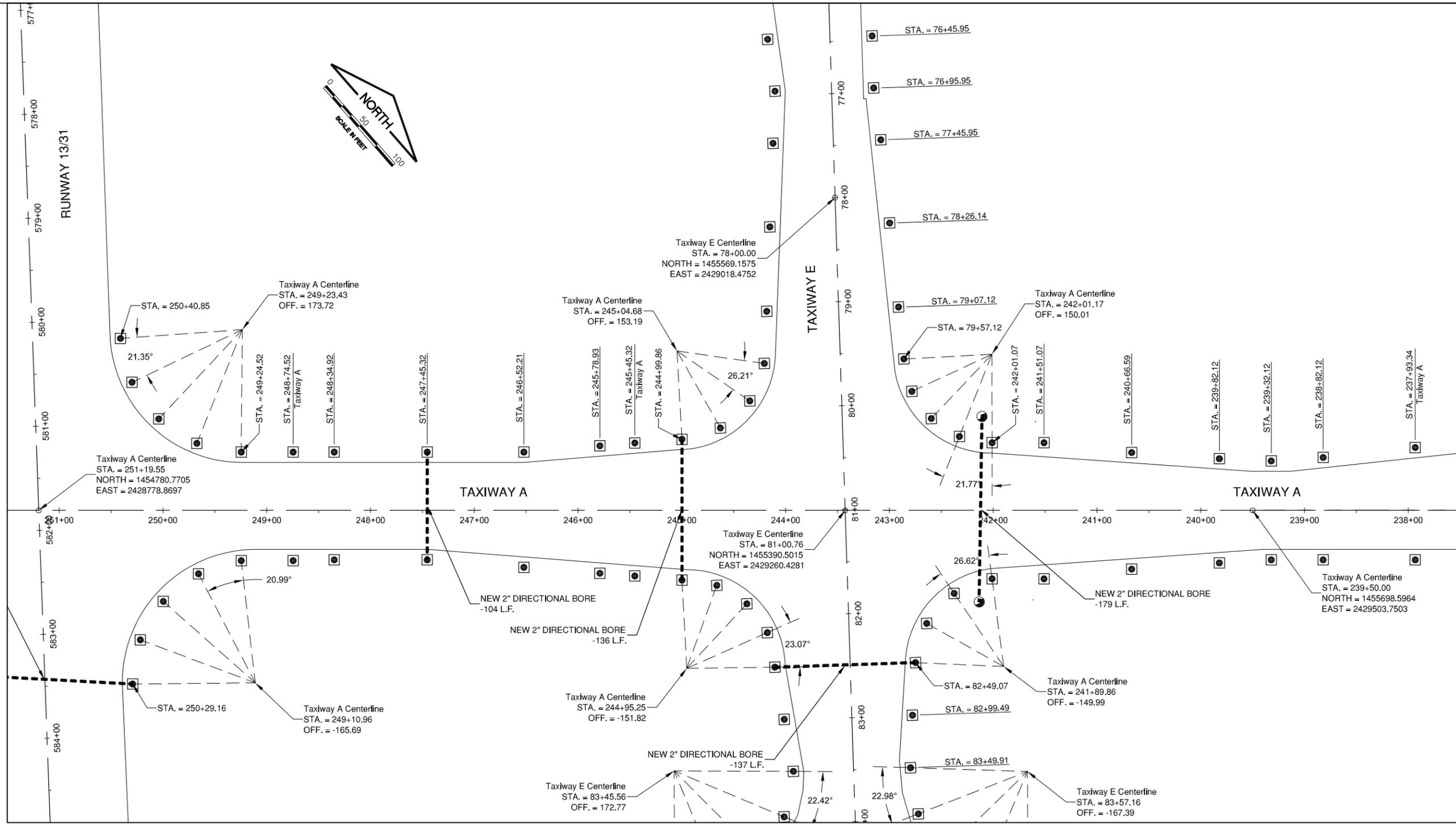
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PEORIA, ILLINOIS
REHABILITATE AIRFIELD LIGHTING
LIGHT LOCATIONS SHEET 10

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SHEET	34 OF 62 SHEETS



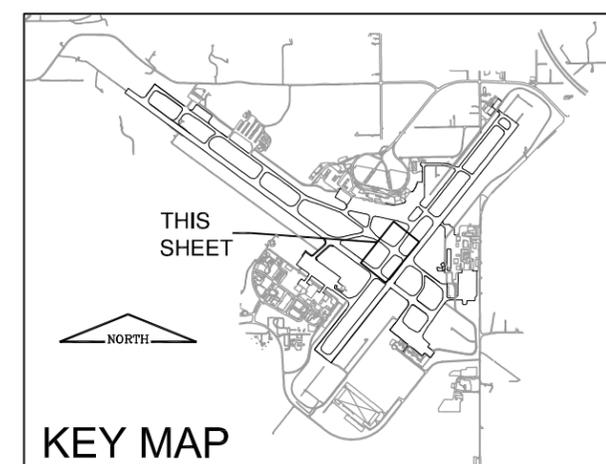
LEGEND

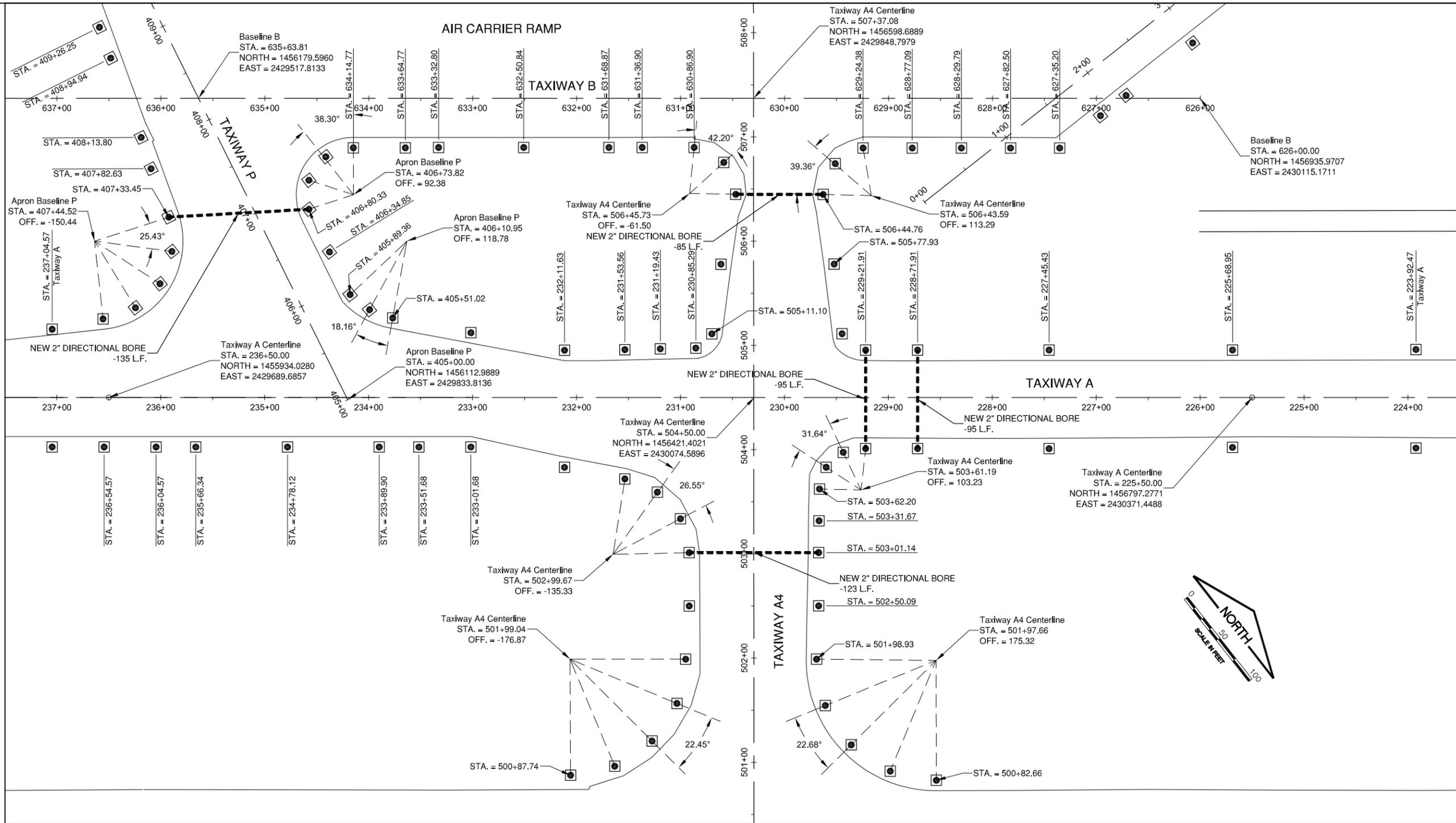
	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
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ALIGNMENT DATA

ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/PI STATION	NORTHING/EASTING	END STATION/CURVE RADIUS	NORTHING/EASTING
L45	Taxiway A	STA. 190+00.00	N:1459583.2177 E:2432571.6840	STA. 285+00.00	N: 1452127.8839 E: 2426683.7306
L52	Taxiway E	STA. 64+08.08	N:1456395.9664 E:2427898.7320	STA. 85+32.89	N: 1455133.8144 E: 2429608.0582

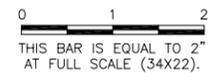




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 PLOT DATE: 5/25/2012 7:48 AM
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 KEYMAP
 LEGEND Prop LIGHT

PE093

REVISIONS		
NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 LIGHT LOCATIONS SHEET 11**

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 PEORIA INTERNATIONAL AIRPORT**

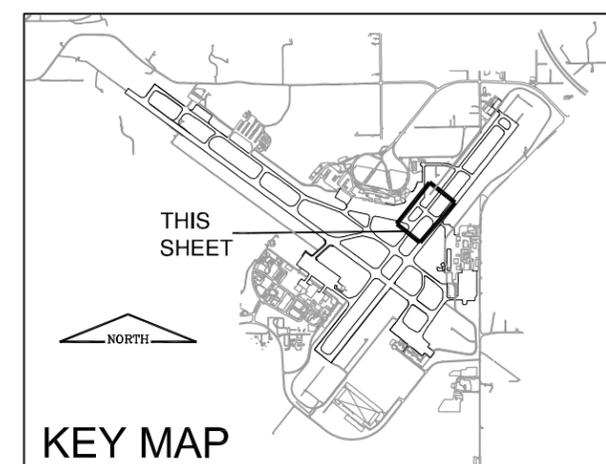
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 DRAWN BY: CMT
 CHECKED BY: CBG
 APPROVED BY: CET
 DATE: APRIL 20, 2012
 JOB No: 11061-07-00
 IL PROJ. NO. PIA-4179
 AIP PROJ. NO. 3-17-0080-XX
 SHEET 35 OF 62 SHEETS

LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
	NEW 2" DIRECTIONAL BORE - AR110012
	EXISTING AIRFIELD DUCT TO BE USED

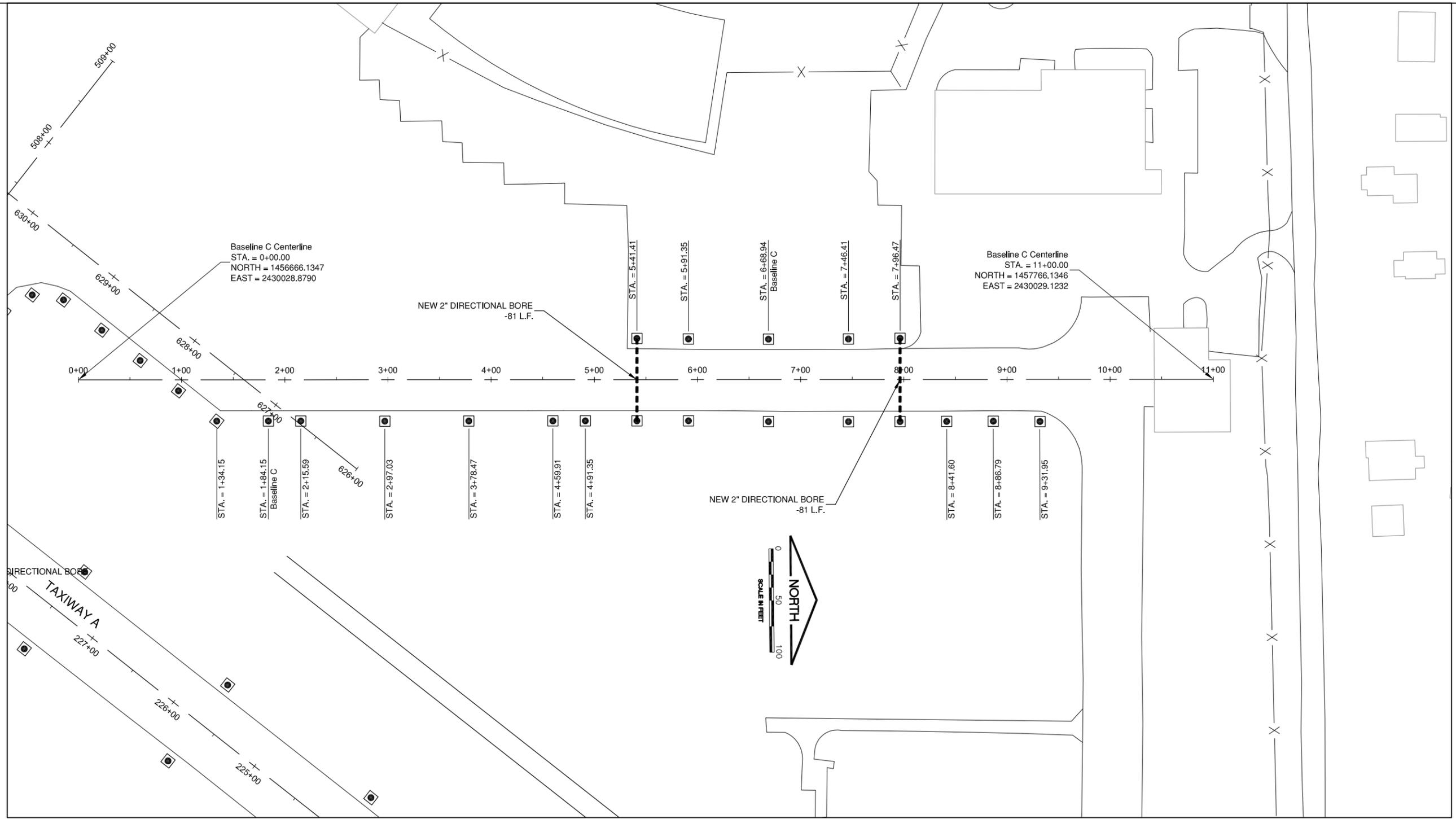
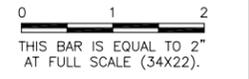
- NOTES**
- LIGHT SHALL BE LOCATED NO MORE THAN 10' FROM THE EXISTING PAVEMENT EDGE.
 - THE LOCATION OF THE P.T. AND P.C. HAVE BEEN TAKEN FROM RECORD DRAWINGS. CONTRACTOR SHALL VERIFY LAYOUT OF LIGHTS WITH R.E. PRIOR TO INSTALLING.
 - LIGHTS SHALL BE INSTALLED IN A STRAIGHT LINE FROM P.T. OR P.C. OR TAPER POINT.

ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L53	Baseline P	STA. 405+00.00	N:1456112.9889 E:2429833.8136	STA. 425+00.00	N: 1456525.4886 E: 2427876.8149
L12	Baseline B	STA. 626+00.00	N:1456935.9707 E:2430115.1711	STA. 641+00.00	N: 1455758.8130 E: 2429185.4939
L11	Taxiway A4	STA. 500+00.00	N:1456142.2060 E:2430427.5060	STA. 509+00.00	N: 1456699.5909 E: 2429720.8789
L54	Taxiway A	STA. 190+00.00	N:1459583.2177 E:2432571.6840	STA. 285+00.00	N: 1452127.8839 E: 2426683.7306



PE093

REVISIONS		
NUMBER	BY	DATE

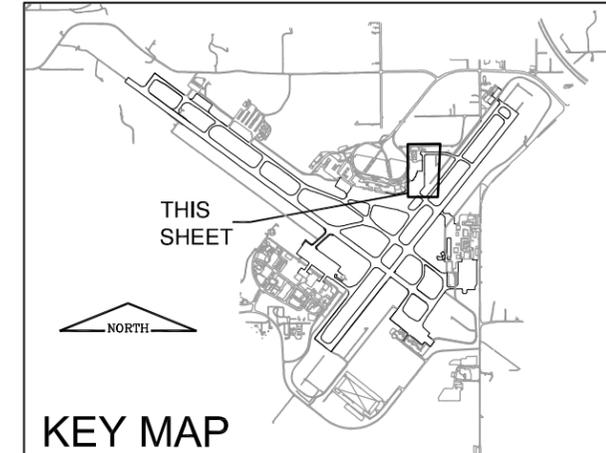


LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
	EXISTING AIRFIELD DUCT TO BE USED

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ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L55	Baseline C	STA. 0+00.00	N:1456666.1347 E:2430028.8790	STA. 11+00.00	N: 1457766.1346 E: 2430029.1232



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 PEORIA INTERNATIONAL AIRPORT

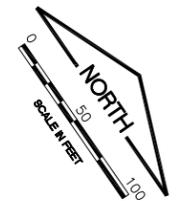
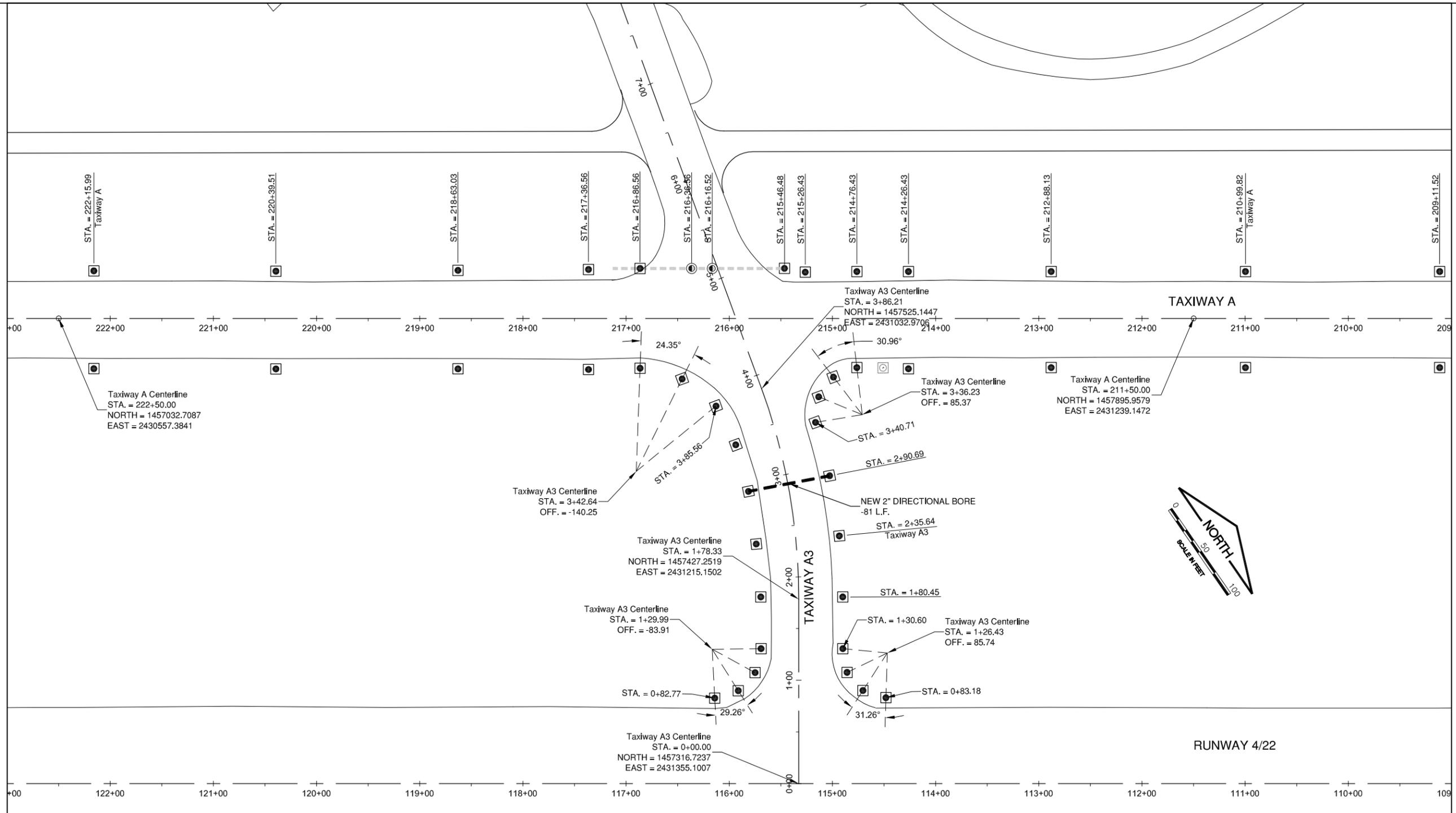
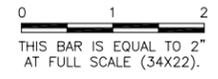
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DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	APRIL 20, 2012
JOB No:	11061-07-00
IL PROJ. NO.	PIA-4179
AIP PROJ. NO.	3-17-0080-XX
SHEET	36 OF 62 SHEETS

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 LIGHT LOCATIONS SHEET 12**

PE093

REVISIONS		
NUMBER	BY	DATE

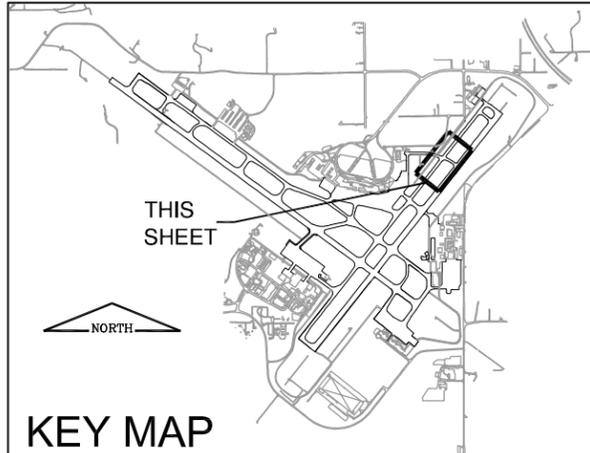


LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
	NEW 2" DIRECTIONAL BORE - AR110012
	EXISTING AIRFIELD DUCT TO BE USED

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ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L56	Runway 04-22	STA. 90+00.00	N:1459304.3136 E:2432924.8318	STA. 185+00.00	N: 1451848.9877 E: 2427036.8683
L57	Taxiway A	STA. 190+00.00	N:1459583.2177 E:2432571.6840	STA. 285+00.00	N: 1452127.8839 E: 2426683.7306
L9	Taxiway A3	STA. 0+00.00	N:1457316.7237 E:2431355.1007	STA. 1+78.33	N: 1457427.2519 E: 2431215.1502
C4	Taxiway A3	CENTER STA. 2+83.35	PI N:1457492.3410 PI E:2431132.7345	R = 592.600	CENTER N: 1456962.1965 E: 2430847.8652
L10	Taxiway A3	STA. 3+86.21	N:1457525.1447 E:2431032.9706	STA. 8+00.00	N: 1457654.3962 E: 2430639.8873



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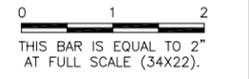
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CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	APRIL 20, 2012
JOB No:	11061-07-00
IL PROJ. NO.	PIA-4179
AIP PROJ. NO.	3-17-0080-XX
SHEET	37 OF 62 SHEETS

PE093

REVISIONS		
NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

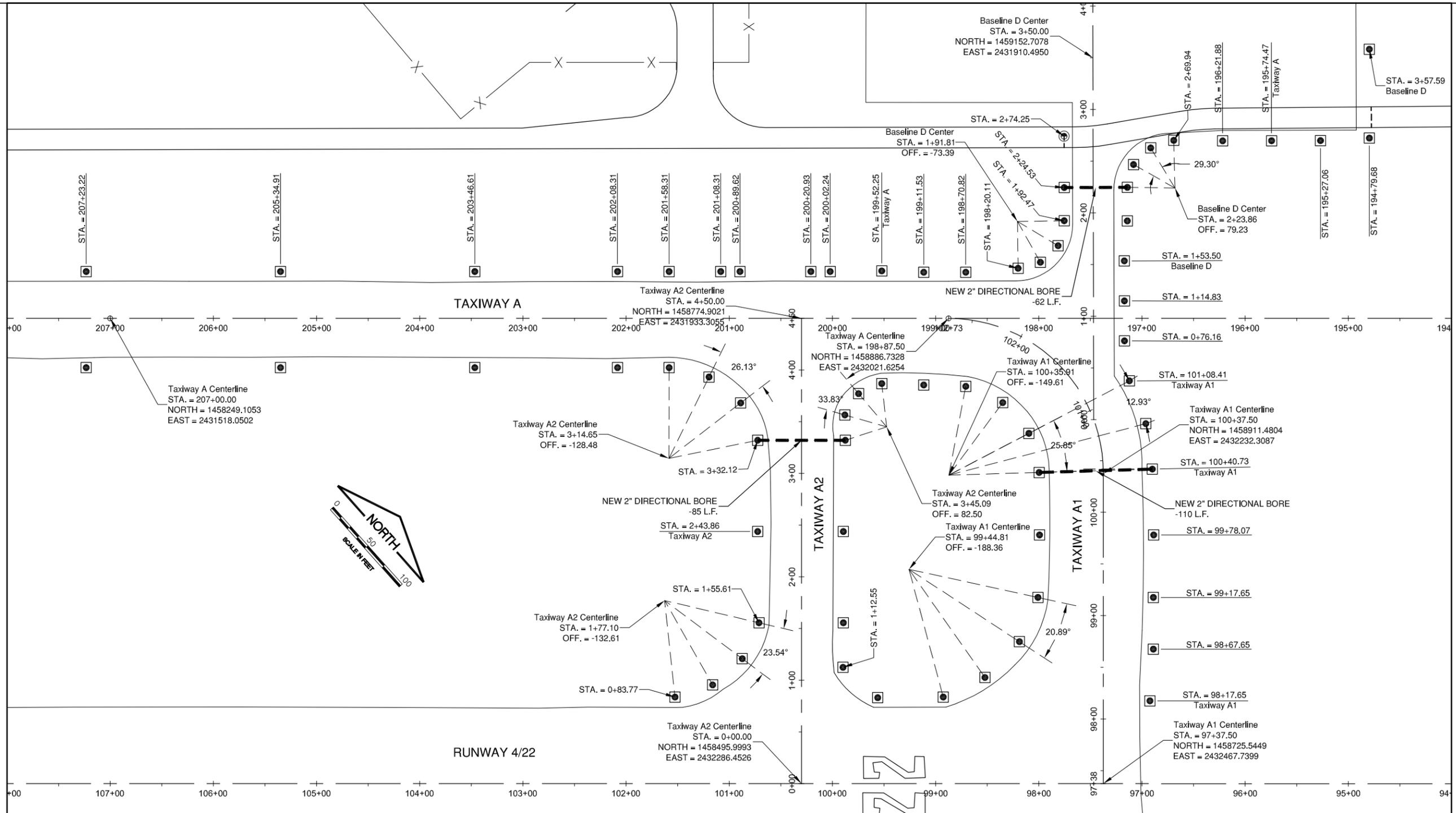
**REHABILITATE AIRFIELD LIGHTING
 LIGHT LOCATIONS SHEET 14**

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AIP PROJ. NO.	3-17-0080-XX
SHEET	38 OF 62 SHEETS

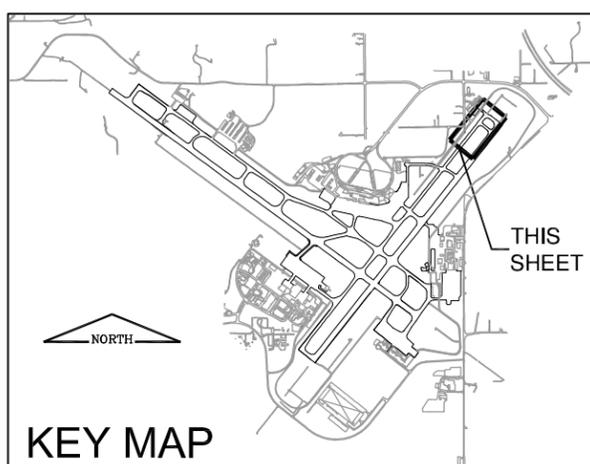


LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
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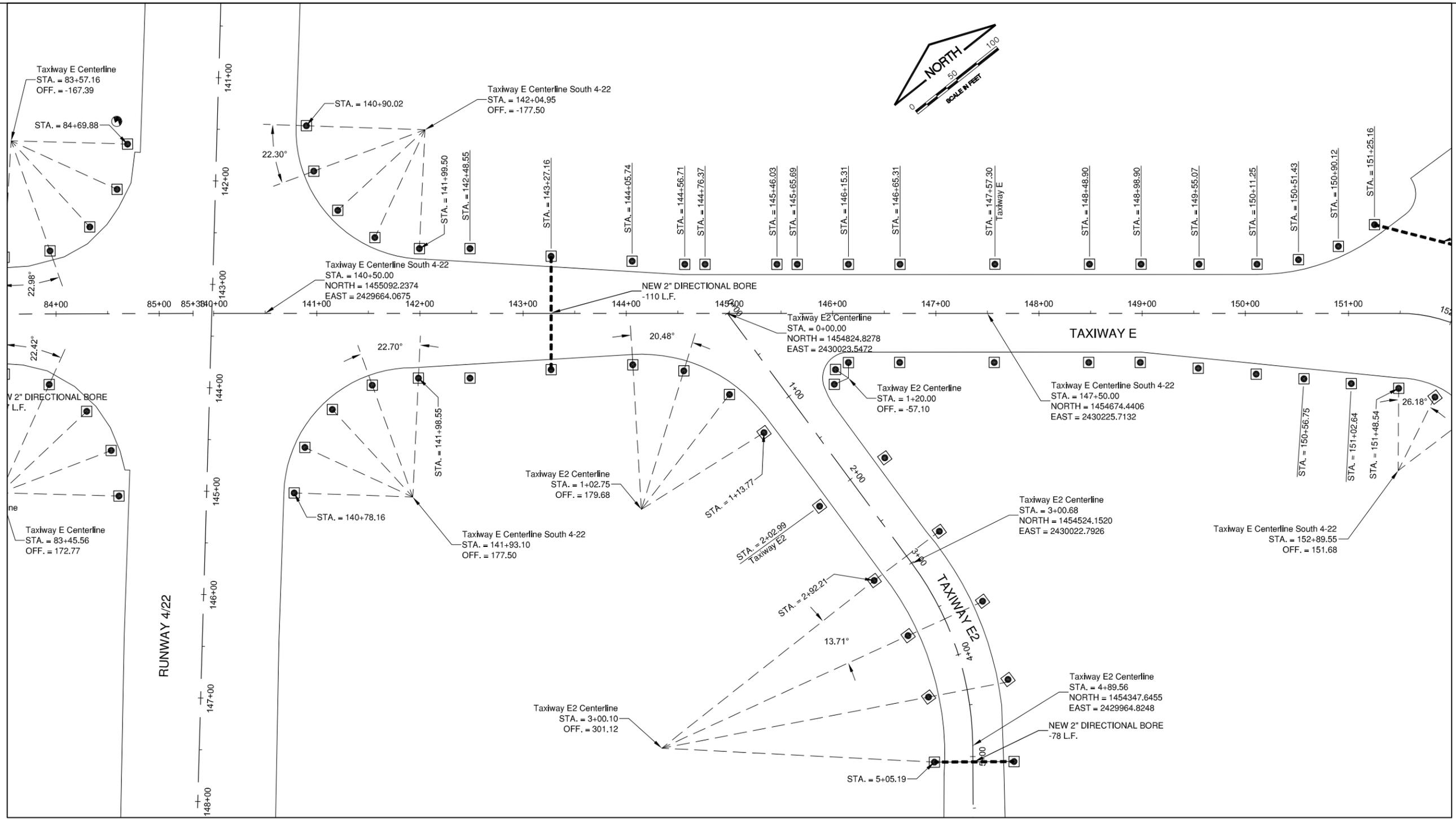
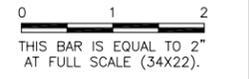
ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L59	Runway 04-22	STA. 90+00.00	N:1459304.3136 E:2432924.8318	STA. 185+00.00	N: 1451848.9877 E: 2427036.8683
L58	Taxiway A	STA. 190+00.00	N:1459583.2177 E:2432571.6840	STA. 285+00.00	N: 1452127.8839 E: 2426683.7306
L60	Baseline D	STA. 0+00.00	N:1458936.4294 E:2432185.6743	STA. 5+00.00	N: 1459245.3986 E: 2431792.5610
C3	Taxiway A1	CENTER STA. 101+87.50	PI N:1459004.4481 PI E:2432114.5933	R = 150.000	CENTER N: 1458793.7647 E: 2432139.3409
L7	Taxiway A1	STA. 97+37.50	N:1458725.5449 E:2432467.7399	STA. 100+37.50	N: 1458911.4804 E: 2432232.3087
L8	Taxiway A2	STA. 0+00.00	N:1458495.9993 E:2432286.4526	STA. 4+50.00	N: 1458774.9021 E: 2431933.3055



KEY MAP

PE093

REVISIONS		
NUMBER	BY	DATE

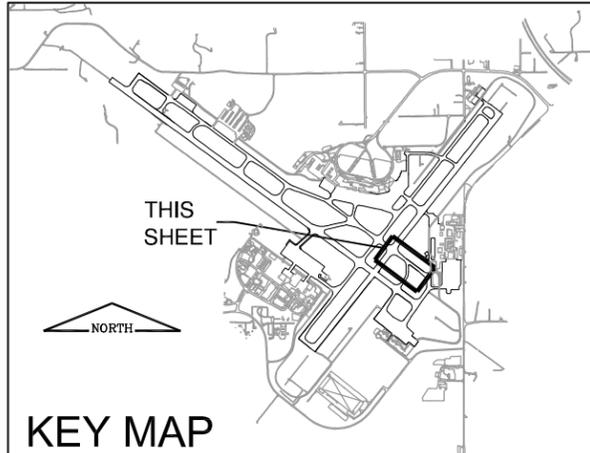


LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
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ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L32	Taxiway E	STA. 140+00.00	N:1455122.0800 E:2429623.9500	STA. 151+48.51	N: 1454436.5902 E: 2430545.4563
L61	Runway 04-22	STA. 90+00.00	N:1459304.3136 E:2432924.8318	STA. 185+00.00	N: 1451848.9877 E: 2427036.8683
L17	Taxiway E2	STA. 4+89.56	N:1454347.6455 E:2429964.8248	STA. 8+40.87	N: 1454064.2219 E: 2429757.2504
C5	Taxiway E2	CENTER STA. 3+98.37	PI N:1454426.4604 PI E:2430022.5475	R = 300.000	CENTER N: 1454524.9048 E: 2429722.7936
L18	Taxiway E2	STA. 0+00.00	N:1454824.8278 E:2430023.5472	STA. 3+00.68	N: 1454524.1520 E: 2430022.7926



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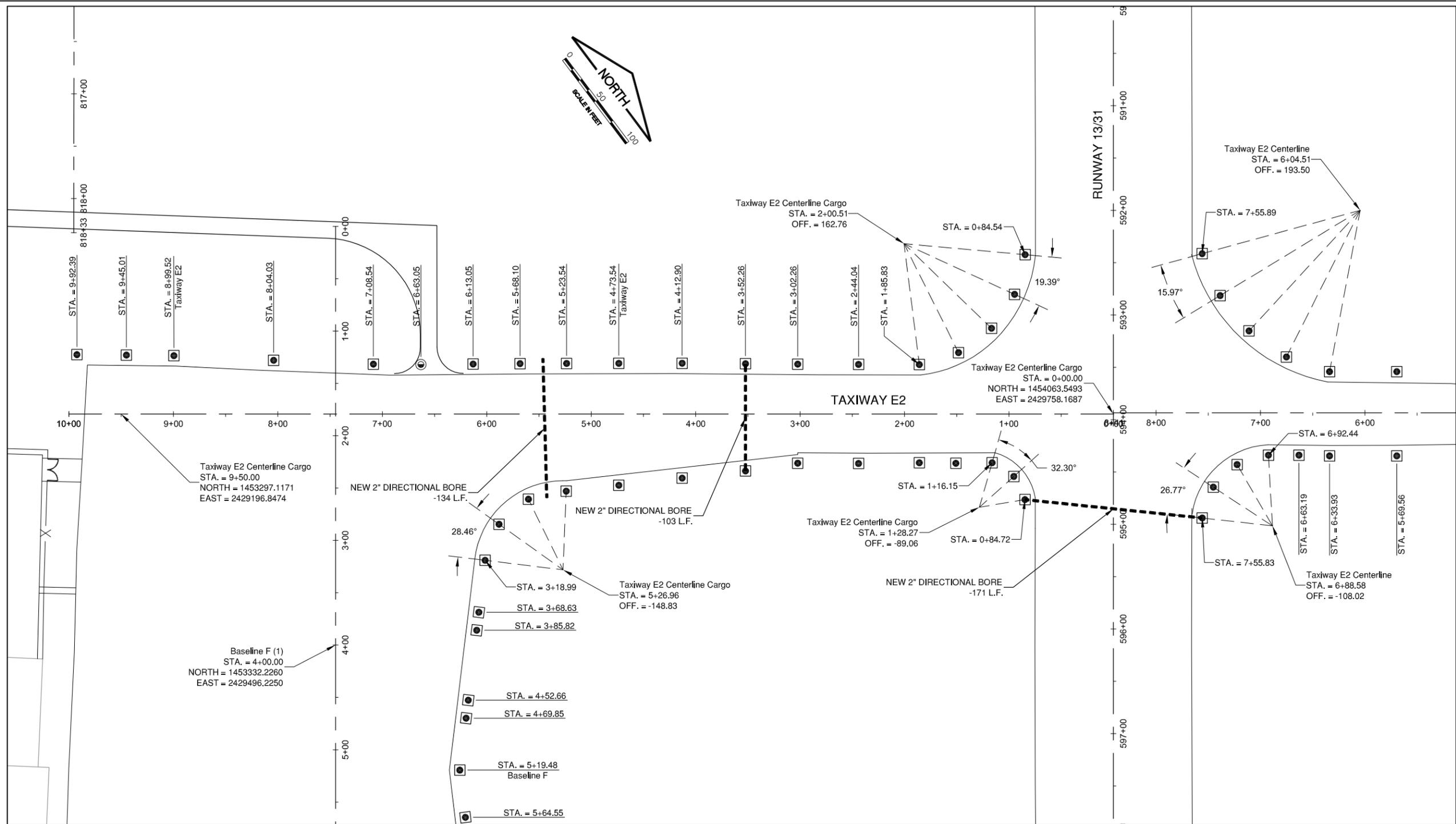
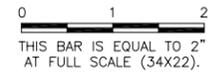
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GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

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DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	APRIL 20, 2012
JOB No:	11061-07-00
IL PROJ. NO.	PIA-4179
AIP PROJ. NO.	3-17-0080-XX
SHEET	39 OF 62 SHEETS

PE093

REVISIONS		
NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 LIGHT LOCATIONS SHEET 16**

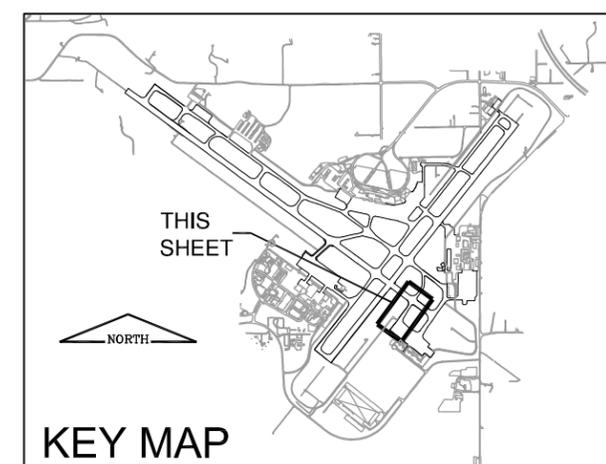
LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
	NEW 2" DIRECTIONAL BORE - AR110012
	EXISTING AIRFIELD DUCT TO BE USED

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ALIGNMENT DATA

ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L63	Runway 13-31	STA. 495+00.00	N:1459909.9516 E:2421775.4480	STA. 615+00.00	N: 1452819.5772 E: 2431456.6975
L62	Baseline F	STA. 0+00.00	N:1453568.5719 E:2429173.5167	STA. 12+50.03	N: 1452829.9735 E: 2430182.0044
L6	Taxiway E2	STA. 0+00.00	N:1454063.5493 E:2429758.1687	STA. 10+00.00	N: 1453256.7781 E: 2429167.3039



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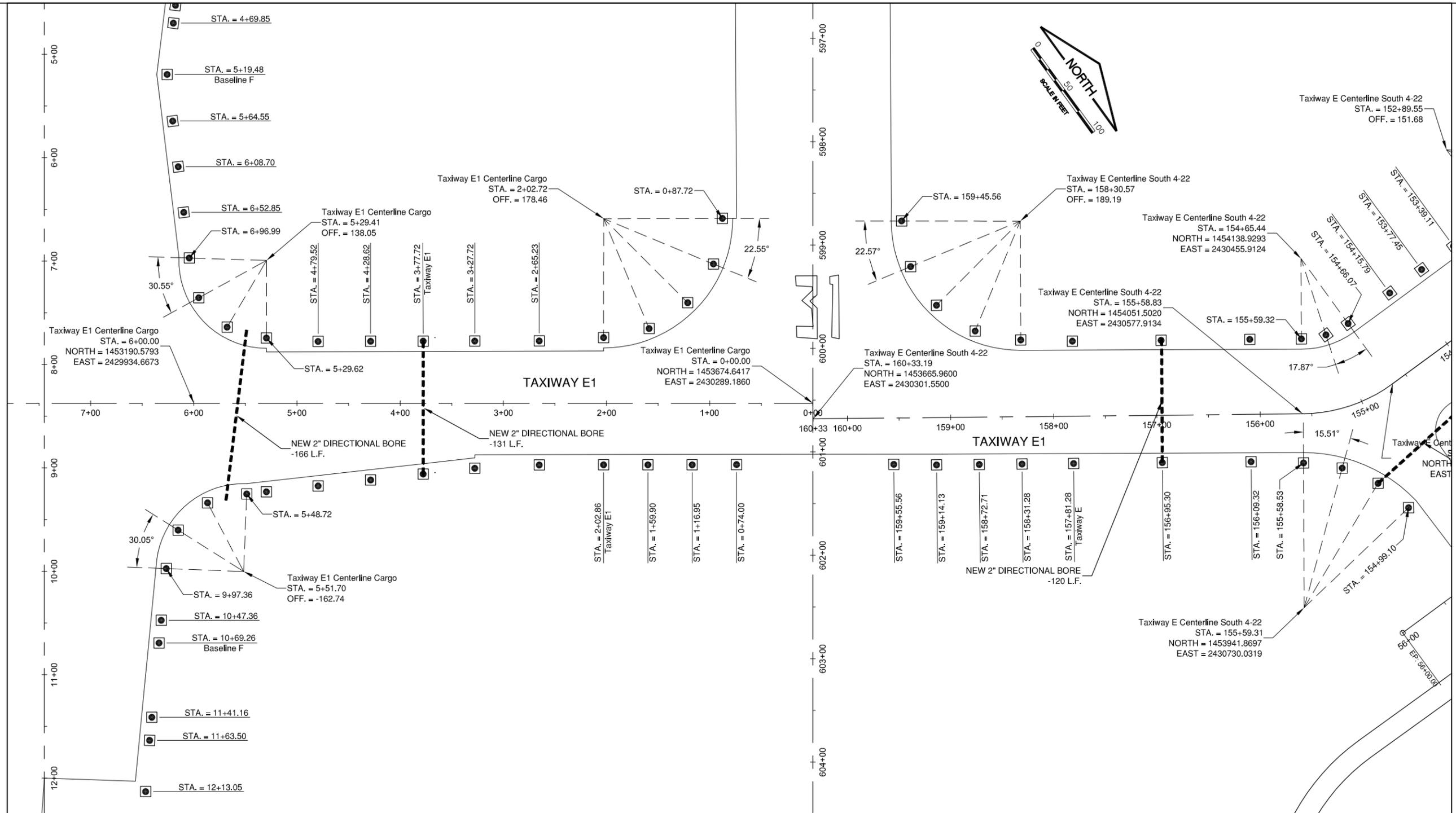
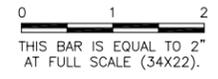
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**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT**

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 CHECKED BY: CBG
 APPROVED BY: CET
 DATE: APRIL 20, 2012
 JOB No: 11061-07-00
 IL PROJ. NO. PIA-4179
 AIP PROJ. NO. 3-17-0080-XX
 SHEET 40 OF 62 SHEETS

PE093

REVISIONS		
NUMBER	BY	DATE



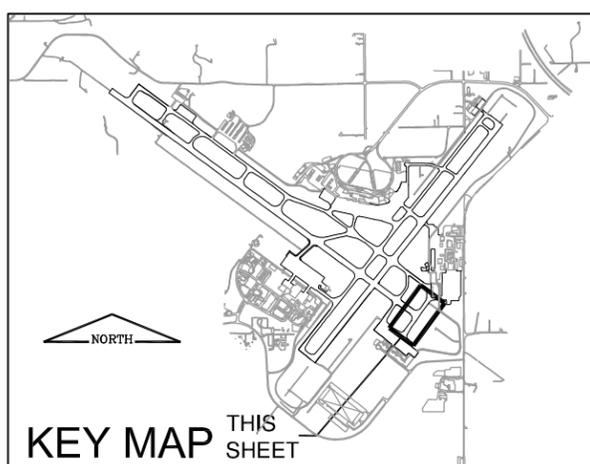
GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
REHABILITATE AIRFIELD LIGHTING
LIGHT LOCATIONS SHEET 17

LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
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ALIGNMENT DATA					
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L64	Baseline F	STA. 0+00.00	N:1453568.5719 E:2429173.5167	STA. 12+50.03	N: 1452829.9735 E: 2430182.0044
L16	Taxiway E1	STA. 0+00.00	N:1453674.6417 E:2430289.1860	STA. 10+12.35	N: 1452857.9097 E: 2429691.0260
L30	Taxiway E	STA. 155+58.83	N:1454051.5020 E:2430577.9134	STA. 160+33.19	N: 1453665.9600 E: 2430301.5500
C7	Taxiway E	CENTER STA. 155+13.72	PI N:1454090.7107 PI E:2430606.0189	R = 150.000	CENTER N: 1454138.8921 E: 2430455.9996
L31	Taxiway E	STA. 152+88.13	N:1454316.2977 E:2430605.9284	STA. 154+65.48	N: 1454138.9522 E: 2430605.9996
L65	Runway 13-31	STA. 495+00.00	N:1459909.9516 E:2421775.4480	STA. 615+00.00	N: 1452819.5772 E: 2431456.6975



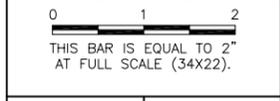
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GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

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 DRAWN BY: CMT
 CHECKED BY: CBG
 APPROVED BY: CET
 DATE: APRIL 20, 2012
 JOB No: 11061-07-00
 IL PROJ. NO. PIA-4179
 AIP PROJ. NO. 3-17-0080-XX
 SHEET 41 OF 62 SHEETS

REVISIONS		
NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

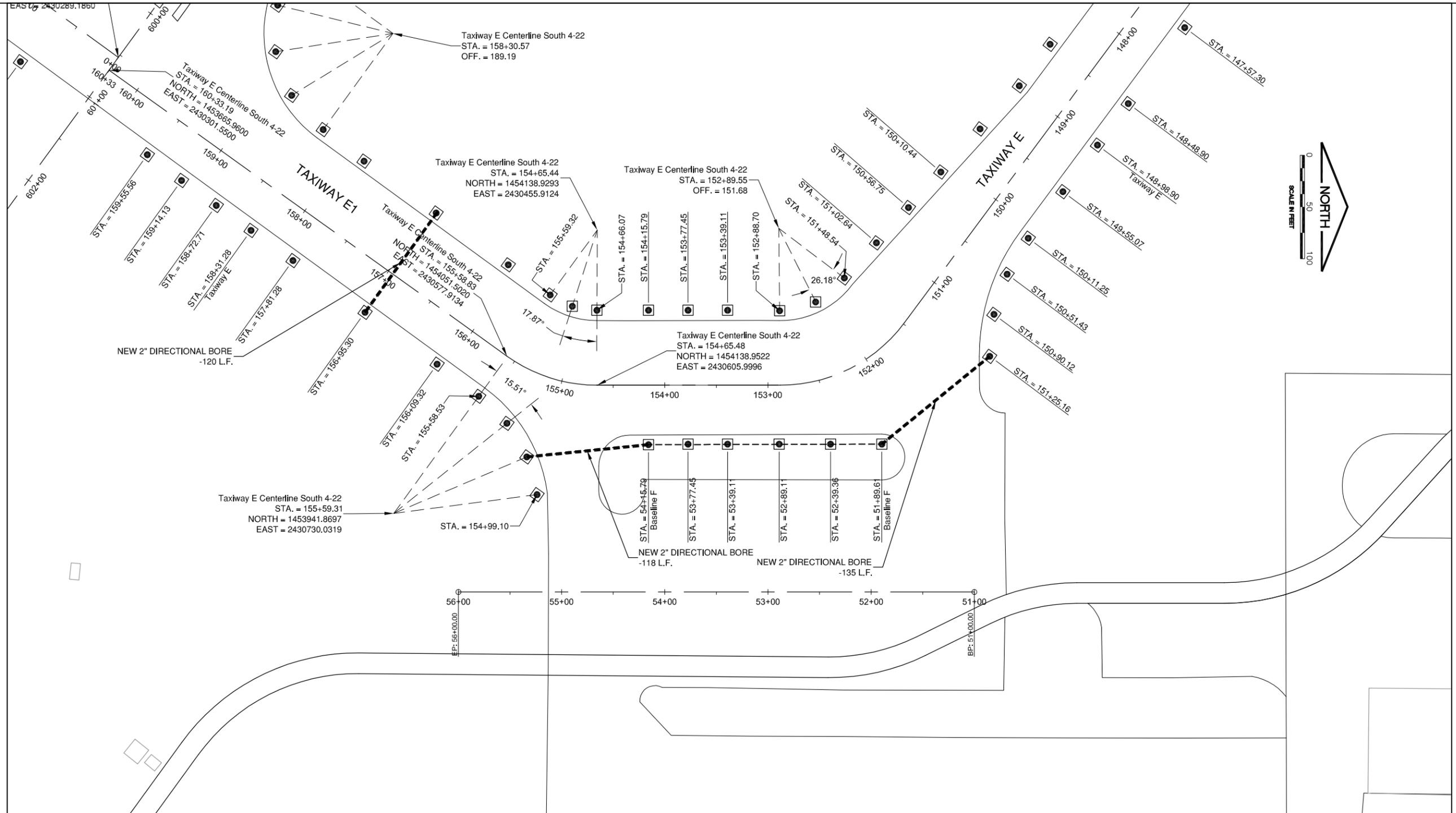
**REHABILITATE AIRFIELD LIGHTING
 LIGHT LOCATIONS SHEET 18**

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**GENERAL WAYNE A. DOWNING
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DATE:	APRIL 20, 2012
JOB No:	11061-07-00
IL PROJ. NO.	PIA-4179
AIP PROJ. NO.	3-17-0080-XX
SHEET	42 OF 62 SHEETS

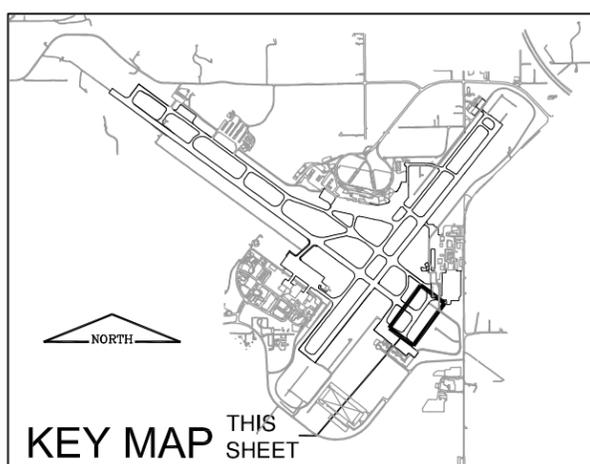


LEGEND

	NEW MITL-BASE MOUNTED - AR125415
	NEW TAXIWAY LIGHT IN-PAVEMENT - AR125420
	NEW TAXIWAY GUIDANCE SIGN - AR12544XX
	NEW SPLICE CAN - AR125565
	NEW 2" DIRECTIONAL BORE - AR110012
	EXISTING AIRFIELD DUCT TO BE USED

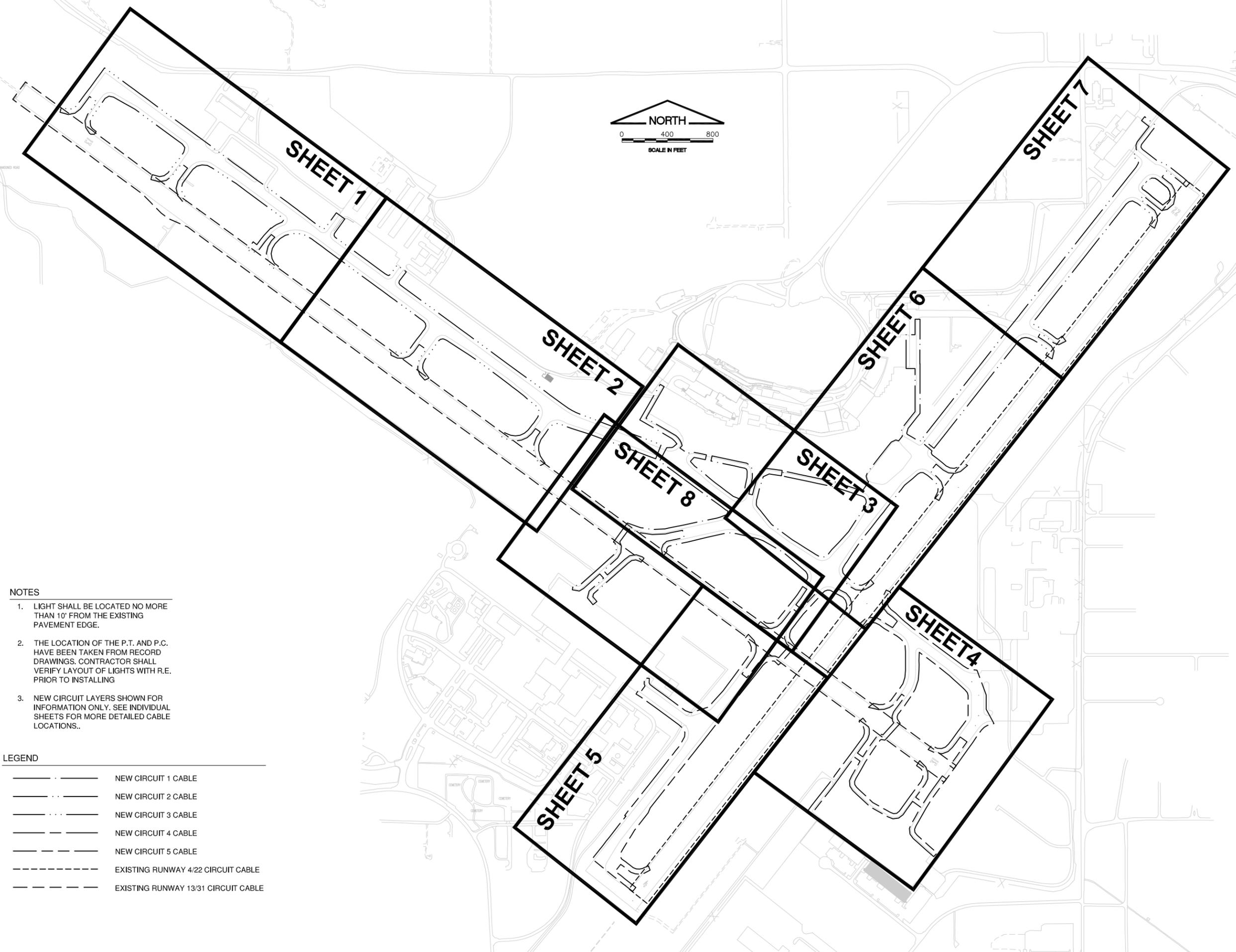
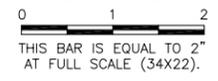
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 - LIGHTS SHALL BE INSTALLED IN A STRAIGHT LINE FROM P.T. OR P.C. OR TAPER POINT.

ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L30	Taxiway E	STA. 155+58.83	N:1454051.5020 E:2430577.9134	STA. 160+33.19	N: 1453665.9600 E: 2430301.5500
C7	Taxiway E	CENTER STA. 155+13.72	PI N:1454090.7107 PI E:2430606.0189	R = 150.000	CENTER N: 1454138.8921 E: 2430455.9996
L31	Taxiway E	STA. 152+88.13	N:1454316.2977 E:2430605.9284	STA. 154+65.48	N: 1454138.9522 E: 2430605.9996
C8	Taxiway E	CENTER STA. 152+23.84	PI N:1454391.6287 PI E:2430605.8982	R = 150.000	CENTER N: 1454316.2375 E: 2430455.9285
L67	Taxiway E	STA. 140+00.00	N:1455122.0800 E:2429623.9500	STA. 151+48.51	N: 1454436.5902 E: 2430545.4563
L66	Baseline F	STA. 51+00.00	N:1454504.5091 E:2430805.8530	STA. 56+00.00	N: 1454004.5091 E: 2430806.0535



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NOTES

1. LIGHT SHALL BE LOCATED NO MORE THAN 10' FROM THE EXISTING PAVEMENT EDGE.
2. THE LOCATION OF THE P.T. AND P.C. HAVE BEEN TAKEN FROM RECORD DRAWINGS. CONTRACTOR SHALL VERIFY LAYOUT OF LIGHTS WITH R.E. PRIOR TO INSTALLING
3. NEW CIRCUIT LAYERS SHOWN FOR INFORMATION ONLY. SEE INDIVIDUAL SHEETS FOR MORE DETAILED CABLE LOCATIONS..

LEGEND

	NEW CIRCUIT 1 CABLE
	NEW CIRCUIT 2 CABLE
	NEW CIRCUIT 3 CABLE
	NEW CIRCUIT 4 CABLE
	NEW CIRCUIT 5 CABLE
	EXISTING RUNWAY 4/22 CIRCUIT CABLE
	EXISTING RUNWAY 13/31 CIRCUIT CABLE

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 INDEX TO CABLE AND SIGN LOCATION SHEETS**

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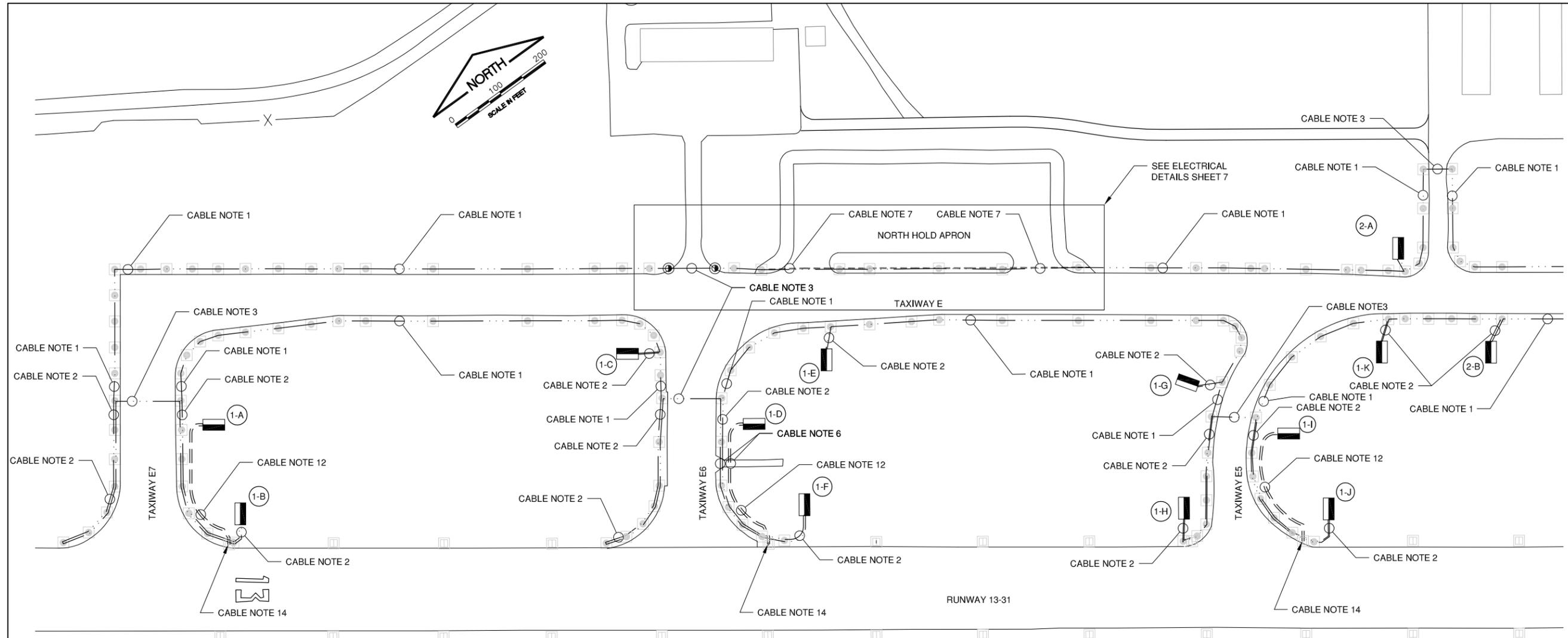
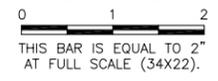
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 PEORIA INTERNATIONAL AIRPORT**

DESIGN BY:	CBG
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	APRIL 20, 2012
JOB No:	11061-07-00
IL PROJ. NO. PIA-4179 AIP PROJ. NO. 3-17-0080-XX	
SHEET 43 OF 62 SHEETS	

PE093

REVISIONS

NUMBER	BY	DATE



GENERAL NOTES

- ALL CABLE SHALL BE INSTALLED IN NEW 2" PVC DIRECT BURY, 2" DIRECTIONAL BORE OR EXISTING DUCTS.

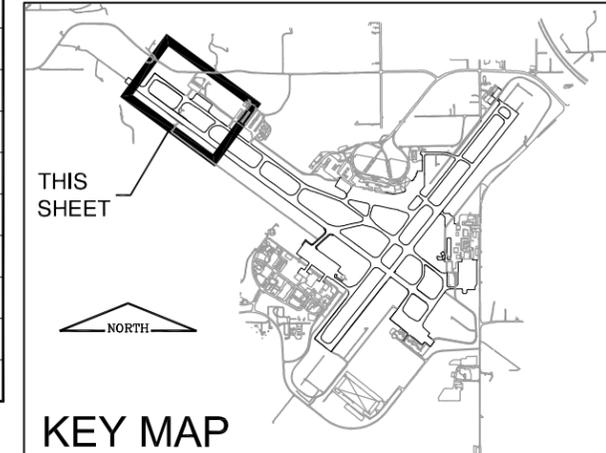
CABLE NOTES

- NEW TAXIWAY CABLE 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL
- NEW TAXIWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL LOOP/SIGN POWER
- NEW TAXIWAY CABLE 1/C #8 5KV UG CABLE IN 2" DIRECTIONAL BORE - TYPICAL PAVEMENT CROSSING
- NEW TAXIWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" DIRECTIONAL BORE - TYPICAL HOMERUN/LOOP PAVEMENT CROSSING
- NEW TAXIWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL HOMERUN
- NEW TAXIWAY CABLE 1/C #8 5KV UG CABLE TRENCH THROUGH BITUMINOUS PAVEMENT
- NEW TAXIWAY CABLE 1/C #8 5KV UG CABLE IN EXISTING DUCT
- COLLECT TAXIWAY CIRCUIT ENDS AND BEGIN HOME RUN
- CONNECT NEW TAXIWAY CABLE HOMERUNS TO EXISTING TAXIWAY CABLE HOMERUNS AT EXISTING MANHOLE
- REMOVE EXISTING LIGHT AND REPLACE WITH SOLID COVER, CONNECT NEW TAXIWAY CABLE HOMERUN TO EXISTING TAXIWAY CABLE HOMERUN
- EXISTING TAXIWAY CABLE HOMERUN
- NEW RUNWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL HOLD SIGN POWER
- NEW RUNWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL HOLD SIGN POWER PAVEMENT CROSSING
- CONNECT CABLE FROM HOLD SIGN TO EXISTING RUNWAY LIGHT
- INSTALL SPLICE CAN AT THE END OF THE DIRECTIONAL BORE

LEGEND

- NEW TAXIWAY GUIDANCE SIGN - AR1254XX
- NEW SPLICE CAN - AR125565
- EXISTING RUNWAY LIGHT
- EXISTING MANHOLE
- NEW CIRCUIT 1 CABLE - AR108108
- NEW CIRCUIT 2 CABLE - AR108108
- NEW CIRCUIT 3 CABLE - AR108108
- NEW CIRCUIT 4 CABLE - AR108108
- NEW CIRCUIT 5 CABLE - AR108108

SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
1-A	NE SW	E7 31-13 E7	31-13		E7 E7	7	RWY 13-31	REPLACE EXIST. SIGN
1-B	SE NW	E7 →		E7 →		3	CKT 3	REPLACE EXIST. SIGN
1-C	NE SW	E6 E →		E →	E6	4	CKT 3	REPLACE EXIST. SIGN
1-D	NE SW	E6 31-13 E6	31-13		E6 E6	7	RWY 13-31	REPLACE EXIST. SIGN
1-E	SE NW	← E6 E		← E6	E	4	CKT 3	REPLACE EXIST. SIGN
1-F	NE SW	E6 →		E6 →		3	CKT 3	REPLACE EXIST. SIGN
1-G	NE SW	E5 E →		E →	E5	4	CKT 3	REPLACE EXIST. SIGN
1-H	NW SE	← E5		← E5		3	CKT 3	REPLACE EXIST. SIGN
1-I	E W	E5 31-13 E5	31-13		E5 E5	7	RWY 13-31	REPLACE EXIST. SIGN
1-J	NW SE	E5 ↗		E5 ↗		3	CKT 3	REPLACE EXIST. SIGN
1-K	SE NW	↖ E5 E		↖ E5	E	4	CKT 3	REPLACE EXIST. SIGN



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REHABILITATE AIRFIELD LIGHTING
PROPOSED CABLES AND SIGN LOCATION SHEET 1

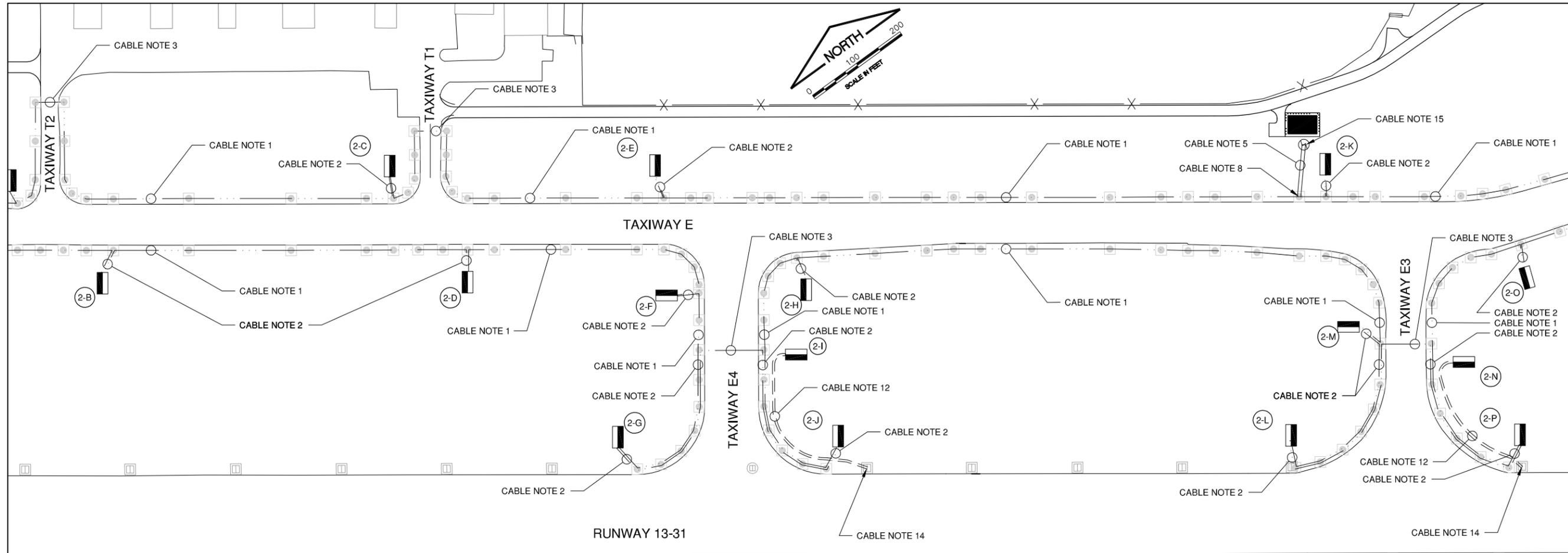
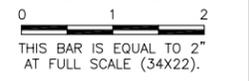
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DATE:	APRIL 20, 2012
JOB No:	11061-07-00
IL PROJ. NO.	PIA-4179
AIP PROJ. NO.	3-17-0080-XX
SHEET	44 OF 62 SHEETS

PE093

REVISIONS		
NUMBER	BY	DATE



GENERAL NOTES

- ALL CABLE SHALL BE INSTALLED IN NEW 2" PVC DIRECT BURY, 2" DIRECTIONAL BORE OR EXISTING DUCTS.

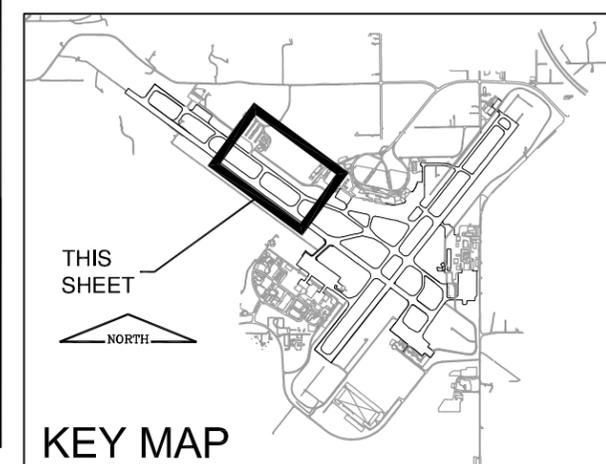
CABLE NOTES

- NEW TAXIWAY CABLE 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL
- NEW TAXIWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL LOOP/SIGN POWER
- NEW TAXIWAY CABLE 1/C #8 5KV UG CABLE IN 2" DIRECTIONAL BORE - TYPICAL PAVEMENT CROSSING
- NEW TAXIWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" DIRECTIONAL BORE - TYPICAL HOMERUN/LOOP PAVEMENT CROSSING
- NEW TAXIWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL HOMERUN
- NEW TAXIWAY CABLE 1/C #8 5KV UG CABLE TRENCH THROUGH BITUMINOUS PAVEMENT
- NEW TAXIWAY CABLE 1/C #8 5KV UG CABLE IN EXISTING DUCT
- COLLECT TAXIWAY CIRCUIT ENDS AND BEGIN HOME RUN
- CONNECT NEW TAXIWAY CABLE HOMERUNS TO EXISTING TAXIWAY CABLE HOMERUNS AT EXISTING MANHOLE
- REMOVE EXISTING LIGHT AND REPLACE WITH SOLID COVER, CONNECT NEW TAXIWAY CABLE HOMERUN TO EXISTING TAXIWAY CABLE HOMERUN
- EXISTING TAXIWAY CABLE HOMERUN
- NEW RUNWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL HOLD SIGN POWER PAVEMENT CROSSING
- NEW RUNWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL HOLD SIGN POWER PAVEMENT CROSSING
- CONNECT CABLE FROM HOLD SIGN TO EXISTING RUNWAY LIGHT
- CONNECT HOMERUN CABLE AT EXISTING SPLICE CAN

LEGEND

- NEW TAXIWAY GUIDANCE SIGN - AR1254XX
- NEW SPLICE CAN - AR125565
- EXISTING RUNWAY LIGHT
- EXISTING MANHOLE
- NEW CIRCUIT 1 CABLE - AR108108
- NEW CIRCUIT 2 CABLE - AR108108
- NEW CIRCUIT 3 CABLE - AR108108
- NEW CIRCUIT 4 CABLE - AR108108
- NEW CIRCUIT 5 CABLE - AR108108

SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
2-A	NW SE	← T2 E		← T2	E	4	CKT 3	REPLACE EXIST. SIGN
2-B	NW SE	E T2 →		T2 →	E	4	CKT 3	REPLACE EXIST. SIGN
2-C	NW SE	← T1 E		← T1	E	4	CKT 3	REPLACE EXIST. SIGN
2-D	NW SE	E T1 →		T1 →	E	4	CKT 3	REPLACE EXIST. SIGN
2-E	NW SE	E E4 →		E4 →	E	4	CKT 3	REPLACE EXIST. SIGN
2-F	NE SW	E4 ← E →		← E →	E4	5	CKT 3	REPLACE EXIST. SIGN
2-G	NW SE	← E4		← E4		3	CKT 3	REPLACE EXIST. SIGN
2-H	NW SE	← E4 E		← E4	E	4	CKT 3	REPLACE EXIST. SIGN
2-I	NE SW	E4 31-13 E4	31-13		E4 E4	7	RWY 13-31	REPLACE EXIST. SIGN
2-J	NW SE	E4 →		E4 →		3	CKT 3	REPLACE EXIST. SIGN
2-K	NW SE	E E3 →		E3 →	E	4	CKT 3	REPLACE EXIST. SIGN
2-L	NW SE	← E3		← E3		3	CKT 3	REPLACE EXIST. SIGN
2-M	NE SW	E3 ← E →		← E →	E3	5	CKT 3	REPLACE EXIST. SIGN
2-N	NE SW	E3 31-13 E3	31-13		E3 E3	7	RWY 13-31	REPLACE EXIST. SIGN
2-O	NW SE	← E3 E		← E3	E	4	CKT 3	REPLACE EXIST. SIGN
2-P	NW SE	E3 →		E3 →		3	CKT 3	REPLACE EXIST. SIGN



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PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS

REHABILITATE AIRFIELD LIGHTING
PROPOSED CABLES AND SIGN LOCATION SHEET 2

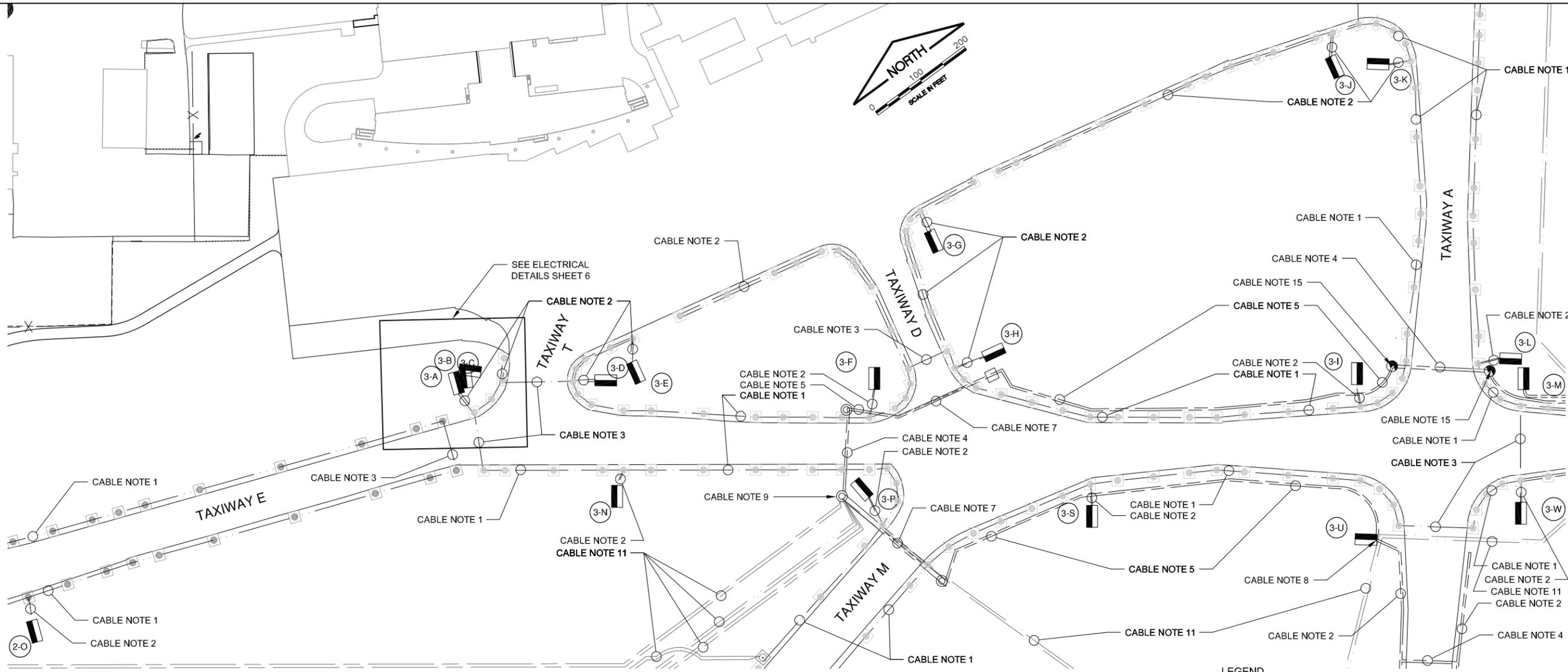
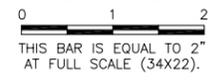
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DATE:	APRIL 20, 2012
JOB No:	11061-07-00
IL PROJ. NO. PIA-4179	
AIP PROJ. NO. 3-17-0080-XX	
SHEET 45 OF 62 SHEETS	

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REVISIONS		
NUMBER	BY	DATE



GENERAL NOTES

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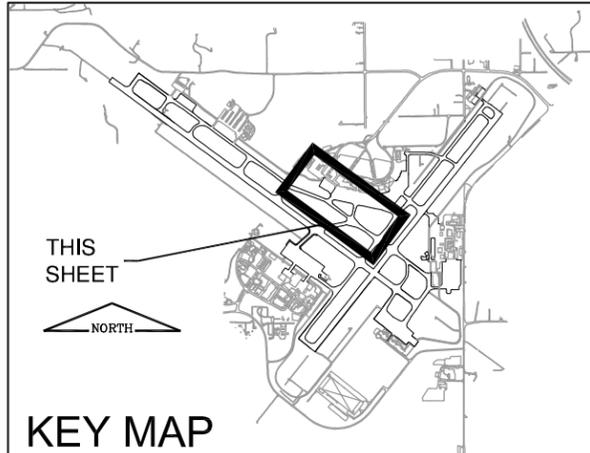
CABLE NOTES

- NEW TAXIWAY CABLE 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL
- NEW TAXIWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL LOOP/SIGN POWER
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- REMOVE EXISTING LIGHT AND REPLACE WITH SOLID COVER, CONNECT NEW TAXIWAY CABLE HOMERUN TO EXISTING TAXIWAY CABLE HOMERUN
- EXISTING TAXIWAY CABLE HOMERUN
- NEW RUNWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL HOLD SIGN POWER
- NEW RUNWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL HOLD SIGN POWER PAVEMENT CROSSING
- CONNECT CABLE FROM HOLD SIGN TO EXISTING RUNWAY LIGHT
- INSTALL SPLICE CAN AT THE END OF THE DIRECTIONAL BORE

SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
3-A	NW SE	↑ T E		↑ T	E	3	CKT 1	REPLACE EXIST. SIGN
3-B	NW SE	T →		T →		2	CKT 1	REPLACE EXIST. SIGN
3-C	NE SW	↑ TERM		↑ TERM		5	CKT 1	REPLACE EXIST. SIGN
3-D	NE SW	T ← E →		← E →	T	4	CKT 1	REPLACE EXIST. SIGN
3-E	E W	↖ T		↖ T		2	CKT 1	REPLACE EXIST. SIGN
3-F	NW SE	↙ D E M ↘		↙ D M ↘	E	5	CKT 1	REPLACE EXIST. SIGN
3-G	E W	← D P		← D	P	3	CKT 1	REPLACE EXIST. SIGN
3-H	N S	↖ E D M ↗		↖ E M ↗	D	7	CKT 1	REPLACE EXIST. SIGN
3-I	NW SE	E ← A →		← A →	E	4	CKT 1	REPLACE EXIST. SIGN
3-J	E W	↖ A P A ↘		↖ A A ↘	P	5	CKT 1	REPLACE EXIST. SIGN
3-K	NE SW	↙ P A		↙ P	A	3	CKT 1	REPLACE EXIST. SIGN
3-L	NE SW	A ← E →		← E →	A	4	CKT 1	REPLACE EXIST. SIGN
3-M	NW SE	E 22-4	22-4		E	5	RWY 13-31	REPLACE EXIST. SIGN
3-N	NW SE	E T →		T →	E	3	CKT 1	REPLACE EXIST. SIGN

LEGEND

- NEW TAXIWAY GUIDANCE SIGN - AR1254XX
- NEW SPLICE CAN - AR125565
- EXISTING RUNWAY LIGHT
- EXISTING MANHOLE
- NEW CIRCUIT 1 CABLE - AR108108
- NEW CIRCUIT 2 CABLE - AR108108
- NEW CIRCUIT 3 CABLE - AR108108
- NEW CIRCUIT 4 CABLE - AR108108
- NEW CIRCUIT 5 CABLE - AR108108



KEY MAP

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**REHABILITATE AIRFIELD LIGHTING
 PROPOSED CABLES AND SIGN LOCATION SHEET 3**

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APPROVED BY:	CET
DATE:	APRIL 20, 2012
JOB No:	11061-07-00
IL PROJ. NO. PIA-4179	
AIP PROJ. NO. 3-17-0080-XX	
SHEET 46 OF 62 SHEETS	

CABLE NOTES

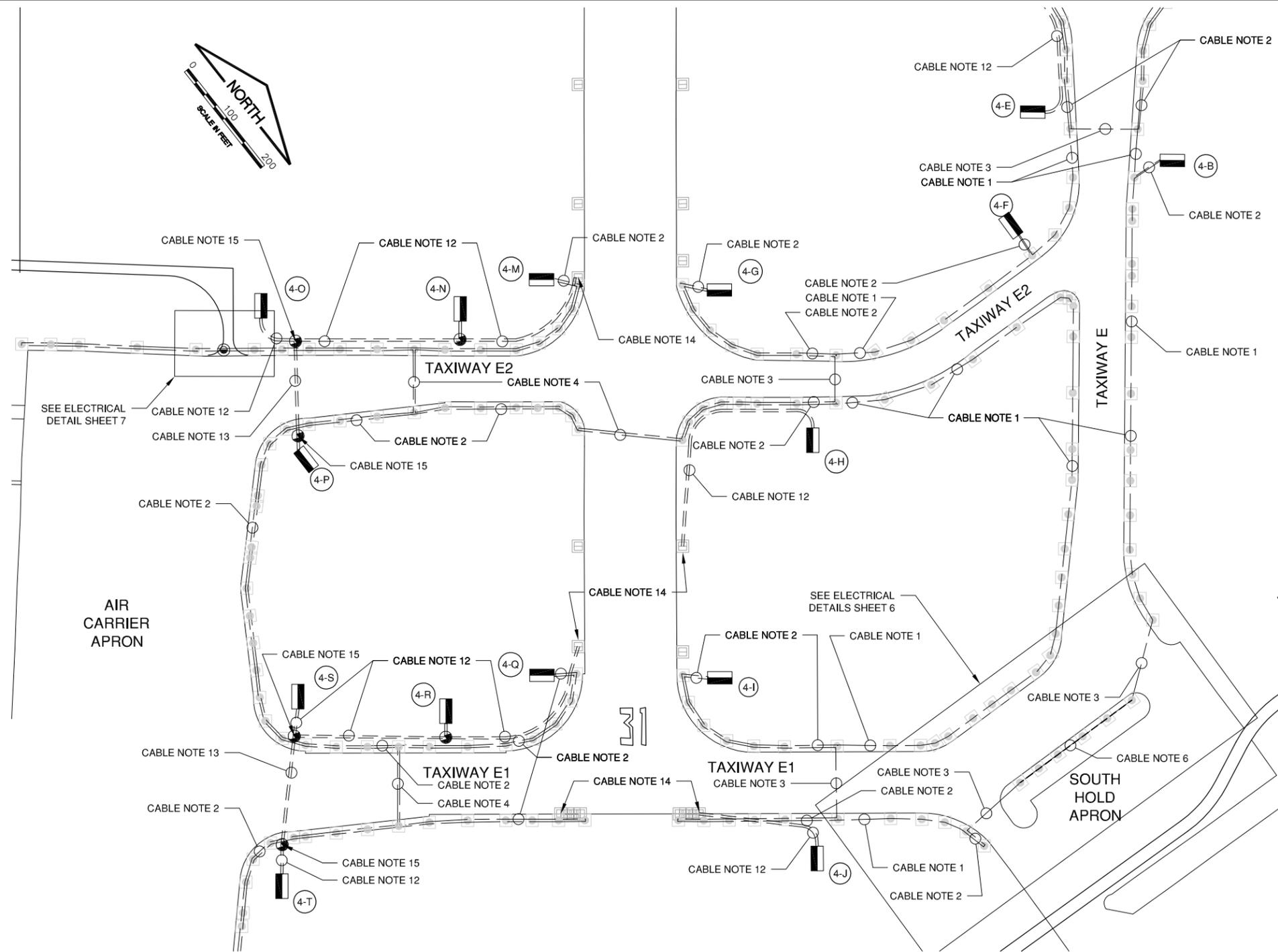
- NEW TAXIWAY CABLE 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL
- NEW TAXIWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL LOOP/SIGN POWER
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- NEW TAXIWAY CABLE 1/C #8 5KV UG CABLE IN EXISTING DUCT
- COLLECT TAXIWAY CIRCUIT ENDS AND BEGIN HOME RUN
- CONNECT NEW TAXIWAY CABLE HOMERUNS TO EXISTING TAXIWAY CABLE HOMERUNS AT EXISTING MANHOLE
- REMOVE EXISTING LIGHT AND REPLACE WITH SOLID COVER, CONNECT NEW TAXIWAY CABLE HOMERUN TO EXISTING TAXIWAY CABLE HOMERUN
- EXISTING TAXIWAY CABLE HOMERUN
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- NEW RUNWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL HOLD SIGN POWER PAVEMENT CROSSING
- CONNECT CABLE FROM HOLD SIGN TO EXISTING RUNWAY LIGHT
- INSTALL SPLICE CAN AT THE END OF THE DIRECTIONAL BORE

GENERAL NOTES

- ALL CABLE SHALL BE INSTALLED IN NEW 2" PVC DIRECT BURY, 2" DIRECTIONAL BORE OR EXISTING DUCTS.

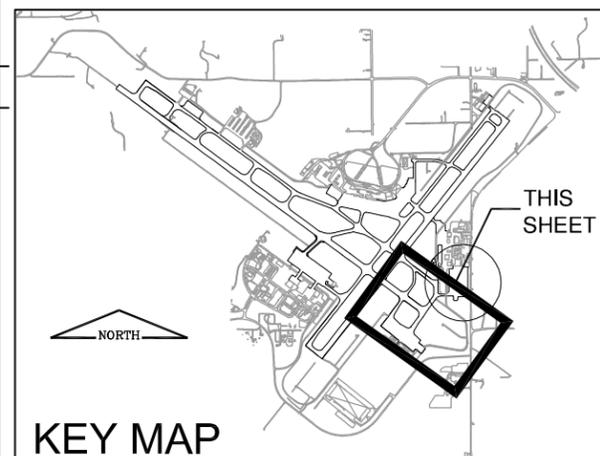
LEGEND

	NEW TAXIWAY GUIDANCE SIGN - AR1254XX
	NEW SPLICE CAN - AR125565
	EXISTING RUNWAY LIGHT
	EXISTING MANHOLE
	NEW CIRCUIT 1 CABLE - AR108108
	NEW CIRCUIT 2 CABLE - AR108108
	NEW CIRCUIT 3 CABLE - AR108108
	NEW CIRCUIT 4 CABLE - AR108108
	NEW CIRCUIT 5 CABLE - AR108108



SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
4-A	NE SW	E →		E →		2	CKT 1	REPLACE EXIST. SIGN
4-B	NW SE	E E2 ↗		E2 ↗	E	4	CKT 4	REPLACE EXIST. SIGN
4-C	NE SW	← E		← E		2	CKT 1	REPLACE EXIST. SIGN
4-D	NE SW	31-13	31-13			6	RWY 13-31	REPLACE EXIST. SIGN
4-E	NW SE	E E 4-22	4-22	E	E	5	RWY 4-22	REPLACE EXIST. SIGN
4-F	N S	↖ E E2		↖ E	E2	4	CKT 4	REPLACE EXIST. SIGN
4-G	NW SE	← E2		← E2		3	CKT 4	REPLACE EXIST. SIGN
4-H	NE SW	E2 31-13	31-13	E2	E2	7	RWY 13-31	REPLACE EXIST. SIGN
4-I	NW SE	← E1 31		← E1 31		3	CKT 4	REPLACE EXIST. SIGN
4-J	NE SW	E1 31	31	E1	E1	4	RWY 13-31	REPLACE EXIST. SIGN

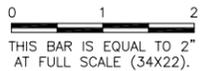
SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
4-K	NE SW	13-31	13-31			6	RWY 13-31	REPLACE EXIST. SIGN
4-L	NW SE	4-22	4-22			4	RWY 4-22	REPLACE EXIST. SIGN
4-M	NW SE	E2 →		E2 →		3	CKT 4	REPLACE EXIST. SIGN
4-N	NE SW	E2 E2 13-31	13-31		E2 E2	7	RWY 13-31	REPLACE EXIST. SIGN
4-O	NE SW	ILS	ILS			3	RWY 13-31	REPLACE EXIST. SIGN
4-P	NW SE	ILS	ILS			3	RWY 13-31	REPLACE EXIST. SIGN
4-Q	NW SE	E1 → 31		E1 → 31		3	CKT 4	REPLACE EXIST. SIGN
4-R	NE SW	E1 E1 31	31		E1 E1	4	RWY 13-31	REPLACE EXIST. SIGN
4-S	NE SW	ILS	ILS			3	RWY 13-31	REPLACE EXIST. SIGN
4-T	NE SW	ILS	ILS			3	RWY 13-31	REPLACE EXIST. SIGN



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 PLOT DATE: 5/25/2012 7:54 AM
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 PIA-Base-Existing
 UTILITY_Updated
 1106107-V-AL
 KEYMAP
 1106107-E-AL
 LEGEND PROP CABLE

PE093

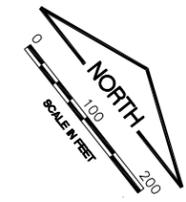
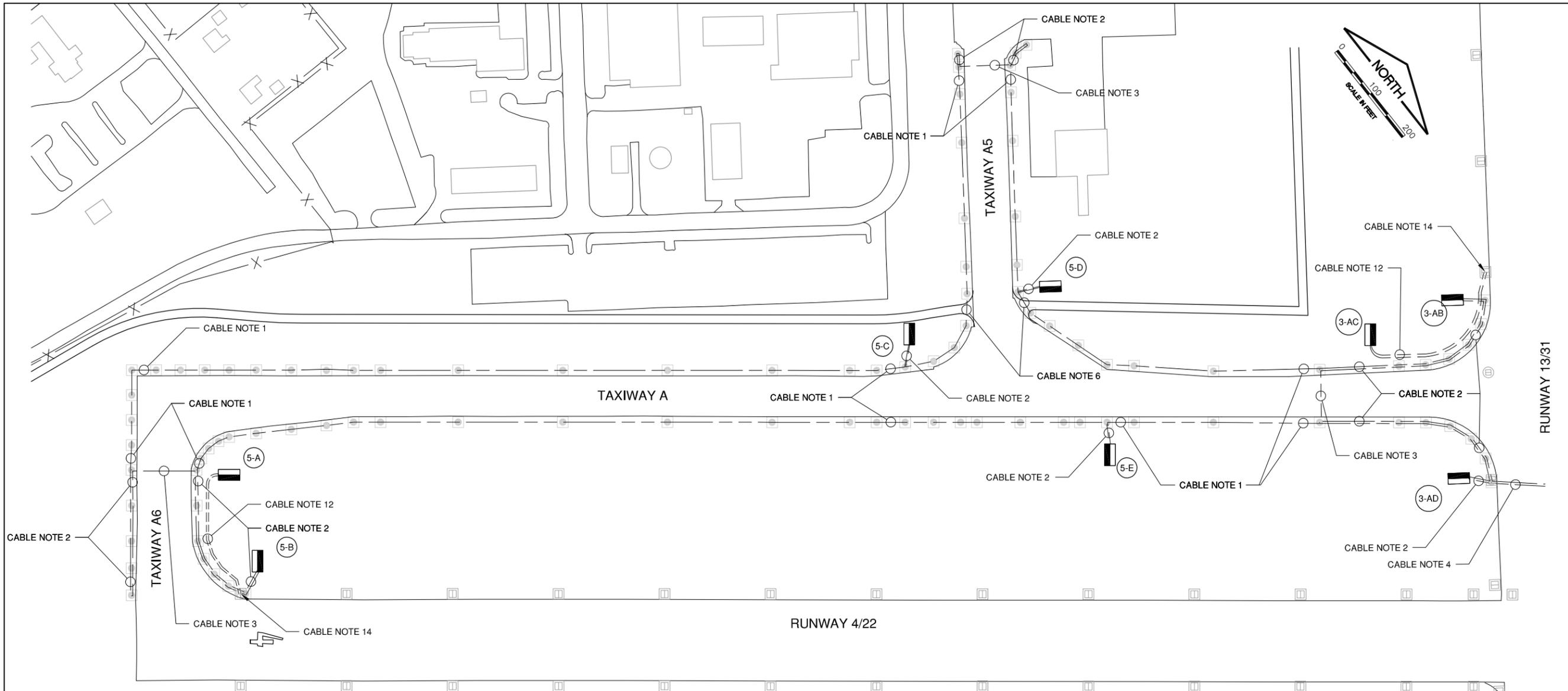
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GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
REHABILITATE AIRFIELD LIGHTING
PROPOSED CABLES AND SIGN LOCATION SHEET 4

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PEORIA INTERNATIONAL AIRPORT

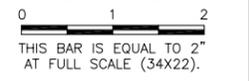
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DATE:	APRIL 20, 2012
JOB No:	11061-07-00
IL PROJ. NO. PIA-4179 AIP PROJ. NO. 3-17-0080-XX	
SHEET	47 OF 62 SHEETS



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 UPDATE BY: Chris Groth
 PLOT DATE: 5/25/2012 7:54 AM
 1106107-C-SPAL
 1106107-E-AL
 PIA-Base-Existing
 UTILITY_Updated
 1106107-V-AL
 KEYMAP
 LEGEND PROP CABLE

PE093

REVISIONS		
NUMBER	BY	DATE



GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
REHABILITATE AIRFIELD LIGHTING
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GENERAL NOTES

- ALL CABLE SHALL BE INSTALLED IN NEW 2" PVC DIRECT BURY, 2" DIRECTIONAL BORE OR EXISTING DUCTS.

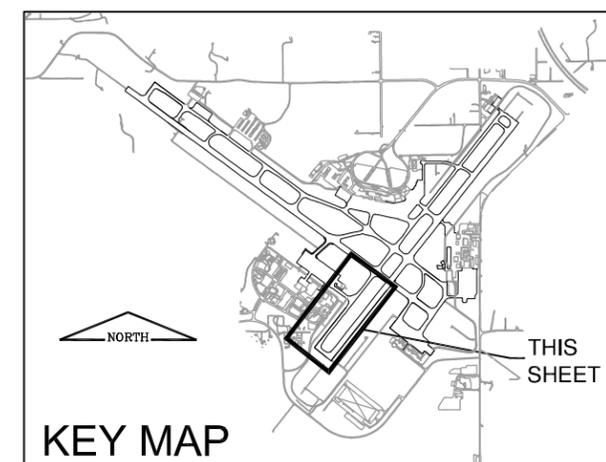
CABLE NOTES

- NEW TAXIWAY CABLE 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL
- NEW TAXIWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL LOOP/SIGN POWER
- NEW TAXIWAY CABLE 1/C #8 5KV UG CABLE IN 2" DIRECTIONAL BORE - TYPICAL PAVEMENT CROSSING
- NEW TAXIWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" DIRECTIONAL BORE - TYPICAL HOMERUN/LOOP PAVEMENT CROSSING
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- NEW TAXIWAY CABLE 1/C #8 5KV UG CABLE IN EXISTING DUCT
- COLLECT TAXIWAY CIRCUIT ENDS AND BEGIN HOME RUN
- CONNECT NEW TAXIWAY CABLE HOMERUNS TO EXISTING TAXIWAY CABLE HOMERUNS AT EXISTING MANHOLE
- REMOVE EXISTING LIGHT AND REPLACE WITH SOLID COVER, CONNECT NEW TAXIWAY CABLE HOMERUN TO EXISTING TAXIWAY CABLE HOMERUN
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- NEW RUNWAY CABLE TWO 1/C #8 5KV UG CABLE IN 2" PVC DIRECT BURY - TYPICAL HOLD SIGN POWER
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- CONNECT CABLE FROM HOLD SIGN TO EXISTING RUNWAY LIGHT
- INSTALL SPLICE CAN AT THE END OF THE DIRECTIONAL BORE

LEGEND

- NEW TAXIWAY GUIDANCE SIGN - AR1254XX
- NEW SPLICE CAN - AR125565
- EXISTING RUNWAY LIGHT
- EXISTING MANHOLE
- NEW CIRCUIT 1 CABLE - AR108108
- NEW CIRCUIT 2 CABLE - AR108108
- NEW CIRCUIT 3 CABLE - AR108108
- NEW CIRCUIT 4 CABLE - AR108108
- NEW CIRCUIT 5 CABLE - AR108108

SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
5-A	NW SE	A6 4 A6	4		A6 A6	3	RWY 4-22	REPLACE EXIST. SIGN
5-B	NE SW	A6 →		A6 →		3	CKT 5	REPLACE EXIST. SIGN
5-C	NE SW	← A5 A		← A5	A	4	CKT 5	REPLACE EXIST. SIGN
5-D	NW SE	A5 ← A → ↑ MIL		← A → ↑ MIL	A5	5	CKT 5	REPLACE EXIST. SIGN
5-E	NE SW	A A5 →		A5 →	A	4	CKT 5	REPLACE EXIST. SIGN

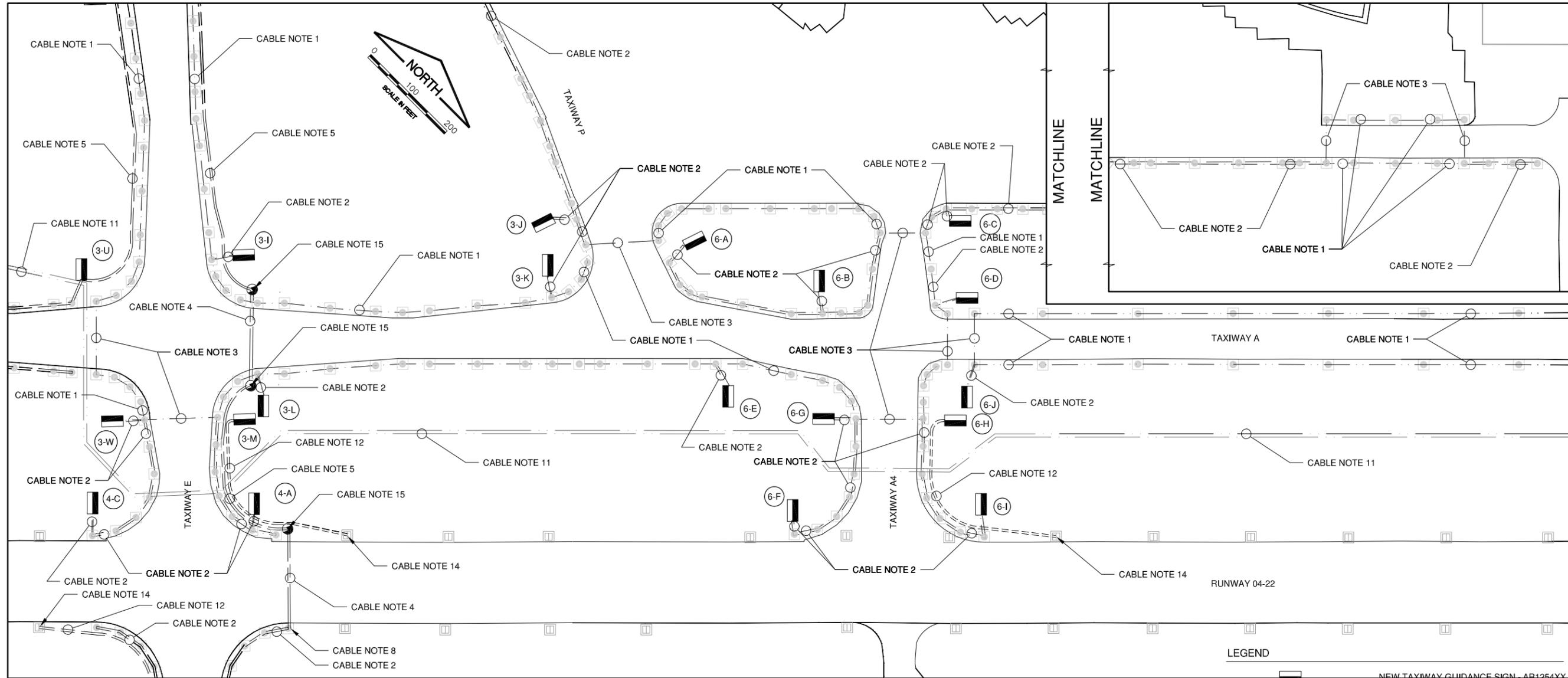
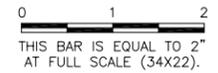


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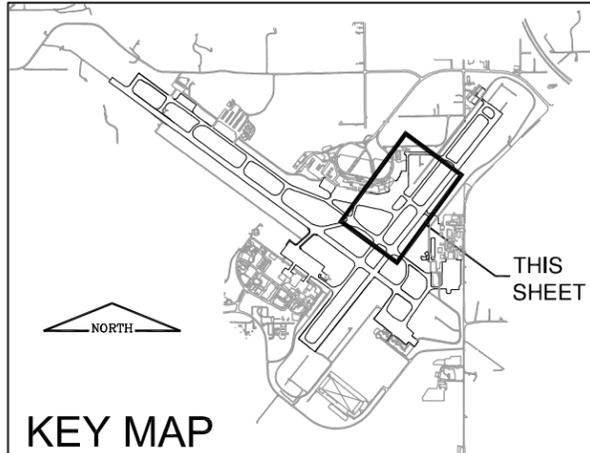
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LEGEND

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SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
6-A	E W	P A P A ↗		↗ A A ↗	P P	5	CKT 1	REPLACE EXIST. SIGN
6-B	NE SW	A A ← A4 →		← A4 →	A A	5	CKT 1	REPLACE EXIST. SIGN
6-C	NW SE	A4 ↑		A4 ↑		3	CKT 1	REPLACE EXIST. SIGN
6-D	NW SE	A4 ← A →		← A →	A4 A4	5	CKT 1	REPLACE EXIST. SIGN
6-E	NE SW	A P ↗ ↑ 22		P ↗ ↑ 22	A	3	CKT 1	REPLACE EXIST. SIGN
6-F	NE SW	← A4		← A4		3	CKT 1	REPLACE EXIST. SIGN
6-G	NW SE	A4 A4 ← A →		← A →	A4 A4	5	CKT 1	REPLACE EXIST. SIGN
6-H	NW SE	A4 22-4 A4	22-4		A4 A4	6	RWY 4-22	REPLACE EXIST. SIGN
6-I	NE SW	A4 →		A4 →		3	CKT 1	REPLACE EXIST. SIGN
6-J	NE SW	A ← A4 → ↑ 22		← A4 → ↑ 22	A	5	CKT 2	REPLACE EXIST. SIGN



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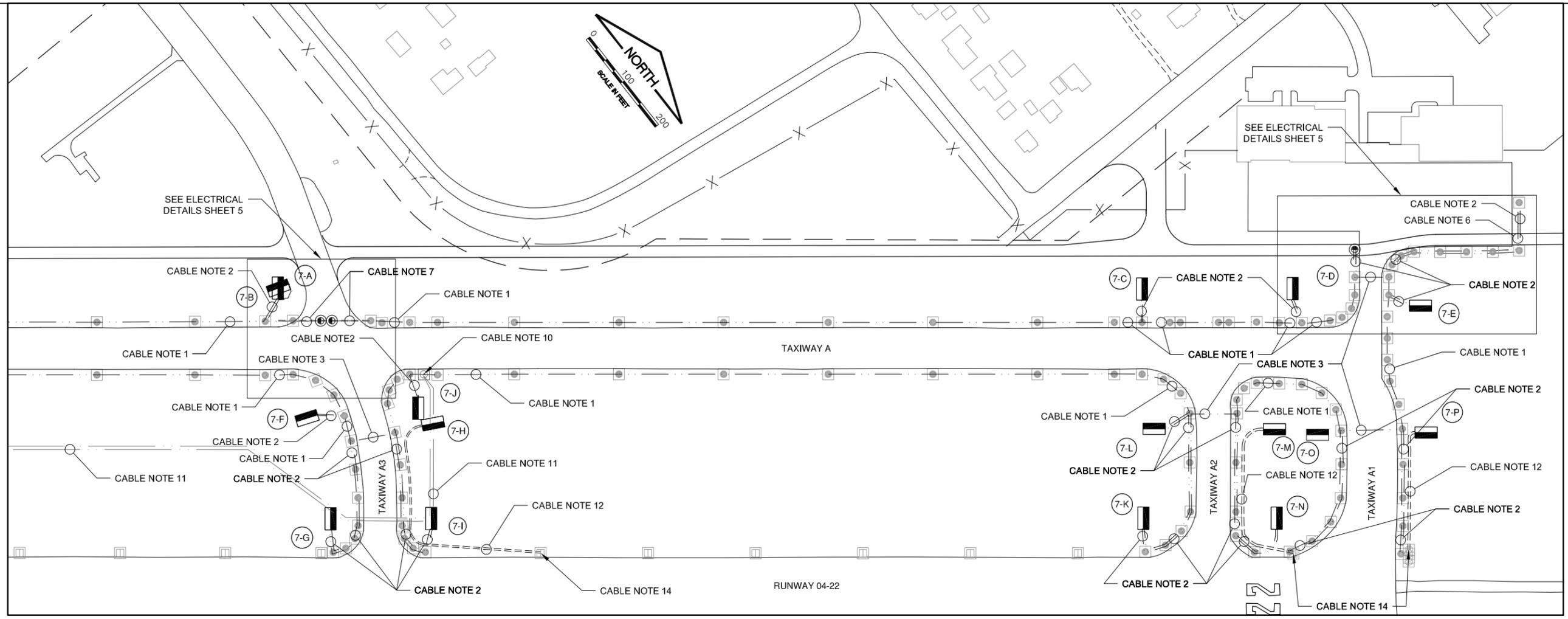
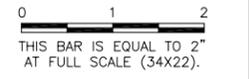
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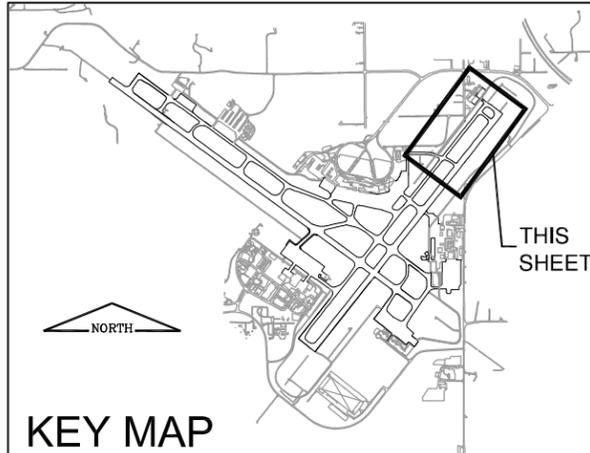
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- INSTALL SPLICE CAN AT THE END OF THE DIRECTIONAL BORE

LEGEND

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- NEW SPLICE CAN - AR125565
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SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
7-A	NE SW	A A A3→		A3→	A A	4	CKT 2	REPLACE EXIST. SIGN
7-B	E W	⊖	⊖			1	CKT 2	REPLACE EXIST. SIGN
7-C	NE SW	A A A2→		A2→	A A	4	CKT 2	REPLACE EXIST. SIGN
7-D	NE SW	A ← A →		← A →	A	4	CKT 2	REPLACE EXIST. SIGN
7-E	NW SE	A1 A →		A →	A1	4	CKT 2	REPLACE EXIST. SIGN
7-F	E W	← A A3 A3		← A	A3 A3	4	CKT 2	REPLACE EXIST. SIGN
7-G	NE SW	← A3		← A3		3	CKT 2	REPLACE EXIST. SIGN
7-H	E W	A3 A3 22-4	22-4		A3 A3	6	RWY 4-22	REPLACE EXIST. SIGN
7-I	NE SW	A3 →		A3 →		3	CKT 2	REPLACE EXIST. SIGN
7-J	NE SW	← A3 A		← A3	A	4	CKT 2	REPLACE EXIST. SIGN
7-K	NE SW	← A2		← A2		3	CKT 2	REPLACE EXIST. SIGN
7-L	NW SE	A2 ← A →		← A →	A2	5	CKT 2	REPLACE EXIST. SIGN
7-M	NW SE	A2 22-4 A2	22-4		A2 A2	6	RWY 4-22	REPLACE EXIST. SIGN
7-N	NE SW	← A1		← A1		3	CKT 2	REPLACE EXIST. SIGN
7-O	NW SE	← A A1		← A	A1	4	CKT 2	REPLACE EXIST. SIGN
7-P	NW SE	A1 22 A1	22		A1 A1	4	RWY 4-22	REPLACE EXIST. SIGN



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 REHABILITATE AIRFIELD LIGHTING
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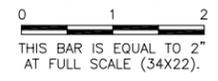
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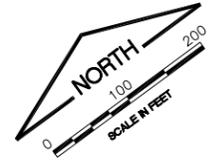
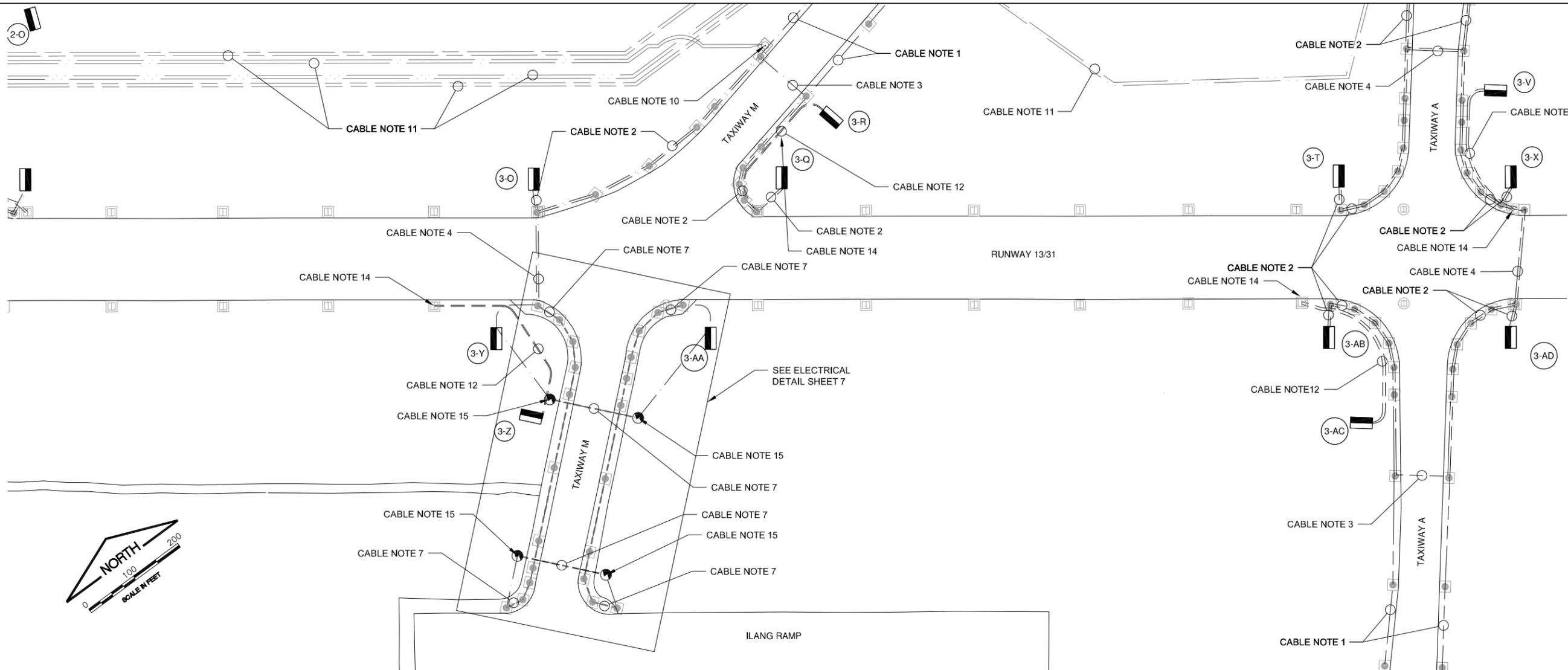
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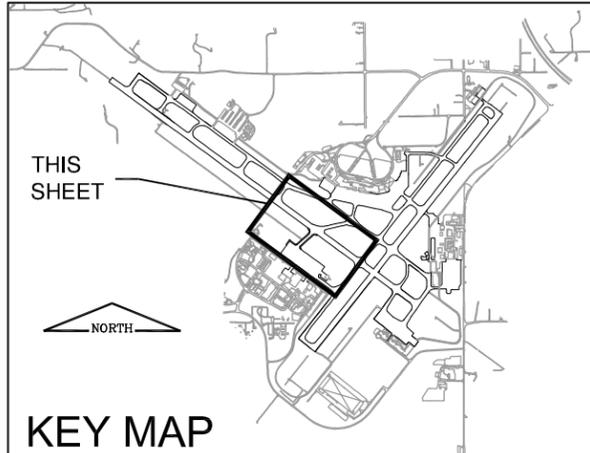
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- INSTALL SPLICE CAN AT THE END OF THE DUCT

SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
3-O	NW SE	↖ M		↖ M		2	CKT 1	REPLACE EXIST. SIGN
3-P	E W	↑ MIL ↖ E ↘ D M E ↗		↑ MIL ↖ E ↘ D E ↗	M	7	CKT 1	REPLACE EXIST. SIGN
3-Q	NW SE	↖ M		↖ M		2	CKT 1	REPLACE EXIST. SIGN
3-R	E W	M 31-13	31-13		M M	6	RWY 13-31	REPLACE EXIST. SIGN
3-S	NW SE	↑ 31 ↖ M E ↘ D ↗		↑ 31 ↖ M ↘ D ↗	E	5	CKT 1	REPLACE EXIST. SIGN
3-T	NW SE	← A		← A		2	CKT 5	REPLACE EXIST. SIGN
3-U	NE SW	A ← E →		← E →	A	4	CKT 5	REPLACE EXIST. SIGN
3-V	NE SW	A 31-13		31-13	A	6	RWY 13-31	REPLACE EXIST. SIGN
3-W	NW SE	E ← A →		← A →	E	4	CKT 1	REPLACE EXIST. SIGN
3-X	NW SE	22-4 A →	22-4	A →		4	CKT 5	REPLACE EXIST. SIGN
3-Y	NW SE	M ↘		M ↘		2	CKT 1	REPLACE EXIST. SIGN
3-Z	NE SW	M M 13-31	13-31		M M	6	RWY 13-31	REPLACE EXIST. SIGN
3-AA	NW SE	↖ M		↖ M		2	CKT 1	REPLACE EXIST. SIGN
3-AB	NW SE	A →		A →		2	CKT 5	REPLACE EXIST. SIGN
3-AC	NE SW	A 13-31	13-31		A	6	RWY 13-31	REPLACE EXIST. SIGN
3-AD	NW SE	22-4 A	22-4	← A		4	CKT 5	REPLACE EXIST. SIGN
3-AE	NE SW	IANG		IANG		4	CKT 5	REPLACE EXIST. SIGN

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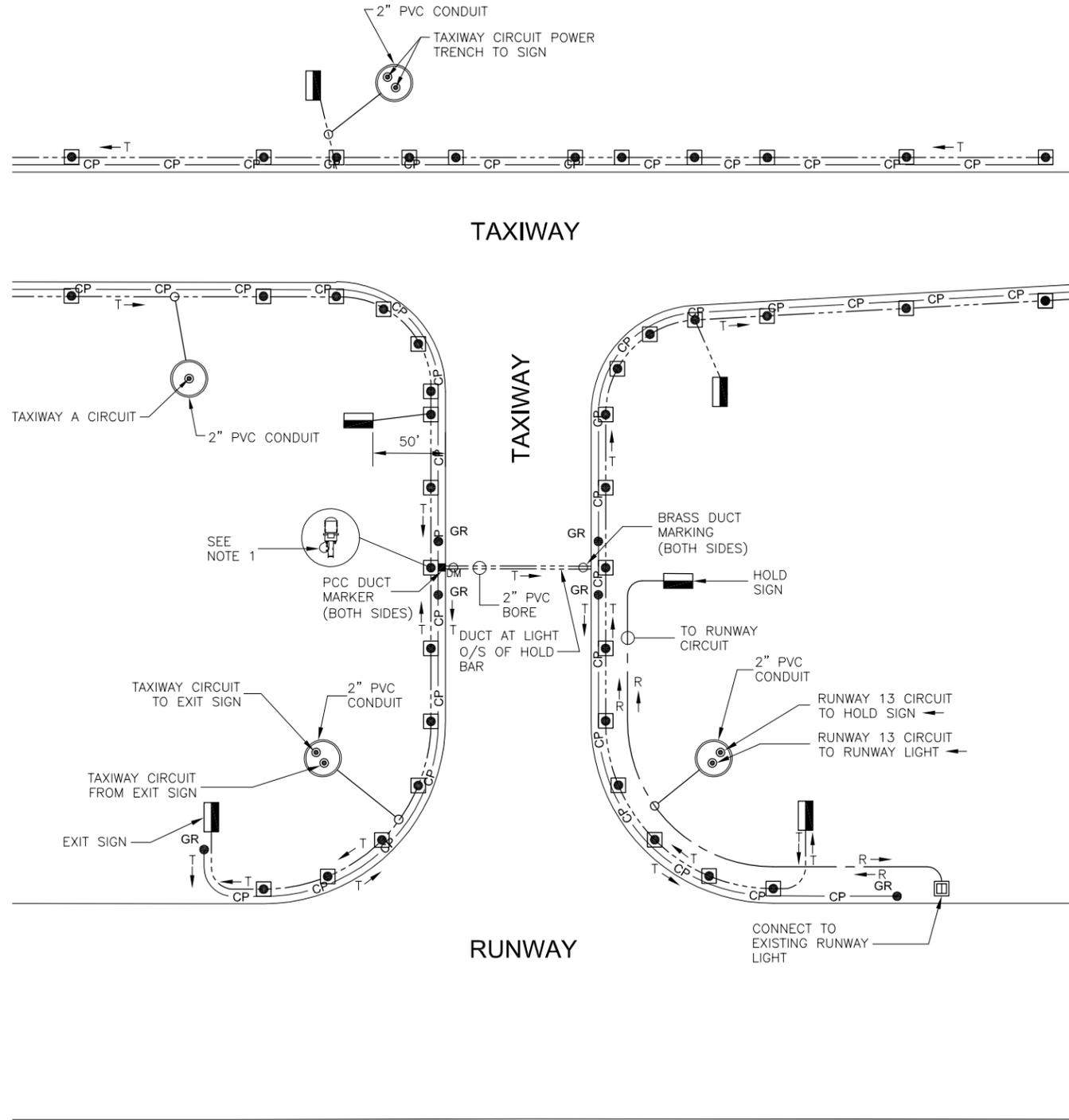
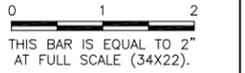


KEY MAP

PE093

REVISIONS

NUMBER	BY	DATE



LEGEND

- NEW TAXIWAY LIGHT
- EXISTING RUNWAY LIGHT
- NEW TAXIWAY GUIDANCE SIGN
- PCC DUCT MARKER
- BRASS DUCT MARKER
- TAXIWAY CIRCUIT
- RUNWAY CIRCUIT
- NEW 2" PVC CONDUIT/
1/C OR 2/C - TAXIWAY CIRCUIT
- NEW 2" PVC CONDUIT BORED UNDER
TAXIWAY
- NEW 2" PVC CONDUIT 2/C RUNWAY CIRCUIT
- NEW #6 BARE COPPER COUNTER POISE
- GROUND ROD

NOTE 1

1. LIGHTS CONNECTED TO THE 2" BORE UNDER PAVE SHALL BE MARKED "DUCT" ON THE LIGHT TAG.

TYPICAL CONDUIT/CABLE LAYOUT AT
 RUNWAY/TAXIWAY INTERSECTION

N.T.S.

GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS

REHABILITATE AIRFIELD LIGHTING
 TYPICAL CONDUIT LAYOUT

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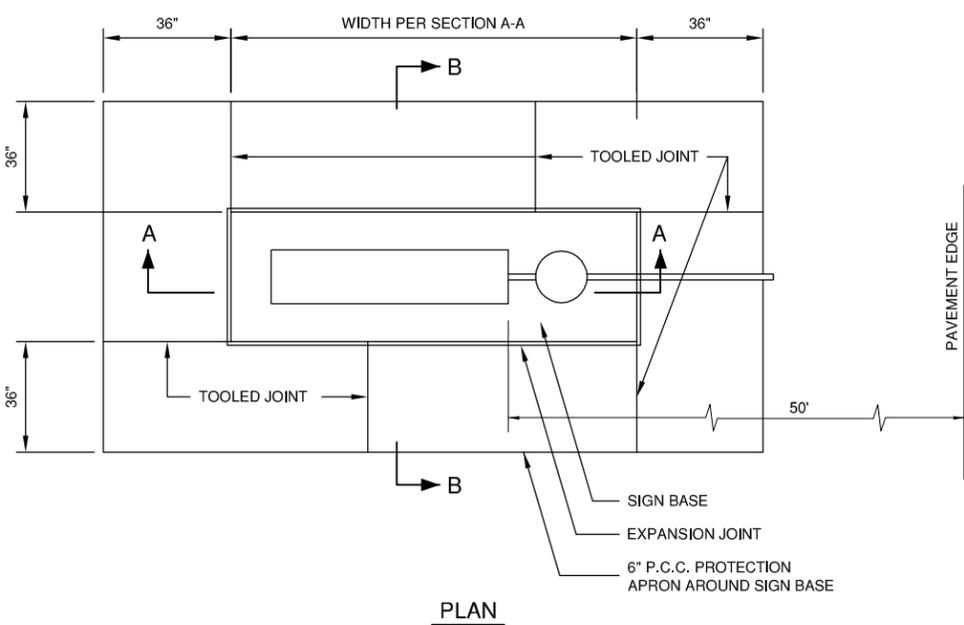
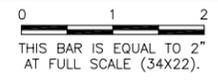
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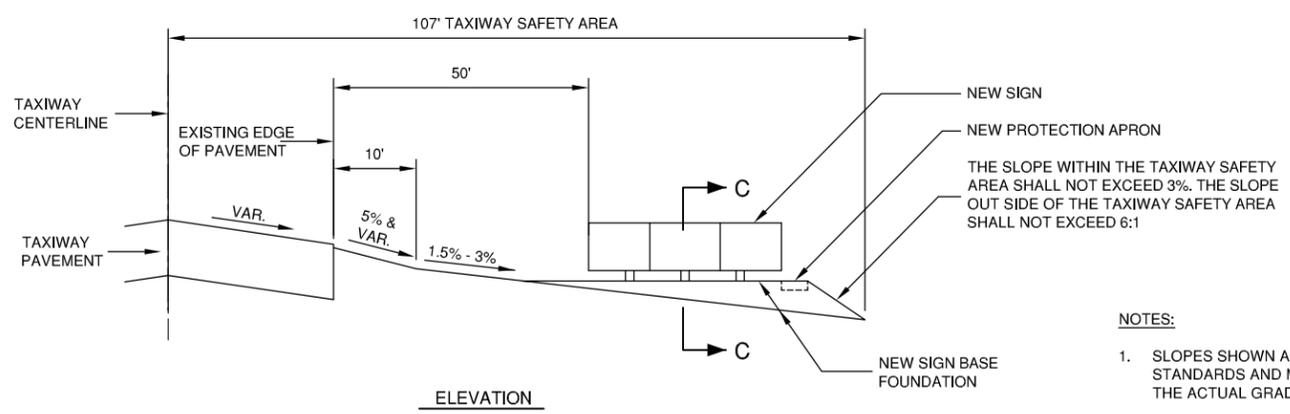
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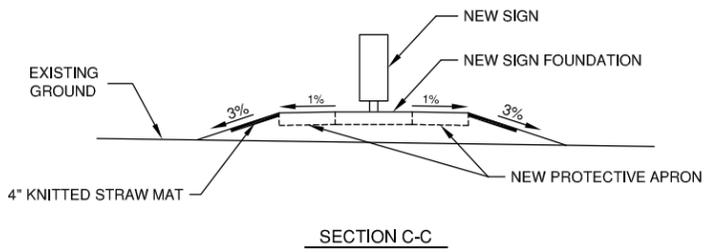
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PLAN



ELEVATION



SECTION C-C

L-858 AIRFIELD SIGN DETAILS
N.T.S.

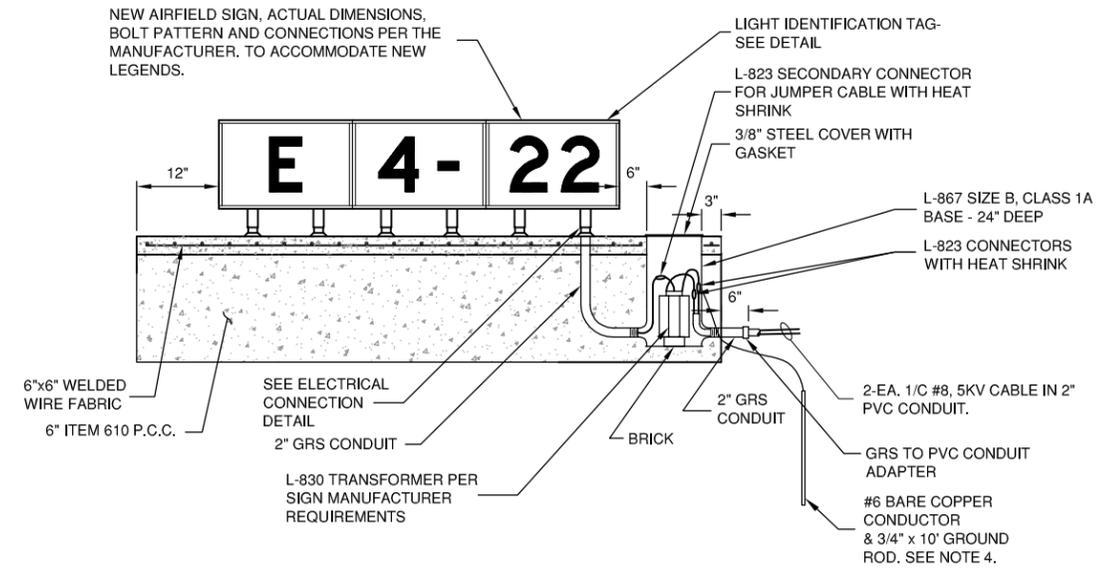
- NOTES:**
1. SLOPES SHOWN ARE FROM FAA STANDARDS AND MAY NOT REFLECT THE ACTUAL GRADES IN THE FIELD
 2. ESTIMATED 2 C.Y. OF EMBANKMENT MAY BE REQUIRED TO CONSTRUCT SIGN BASE FOUNDATION. COSTS TO CONSTRUCT SHALL BE INCIDENTAL TO SIGN PAY ITEM.
 3. ACTUAL LOCATION OF THE SIGN WITHIN THE TAXIWAY SAFETY AREA WILL VARY DUE TO PAVEMENT WIDTHS AND VARIANCES IN SIGN FOUNDATION LENGTHS.
 4. 4" OF KNITTED STRAW MAT SHALL BE PLACED AROUND THE PROTECTION APRON. COST FOR MAT SHALL BE INCIDENTAL TO SIGN PAY ITEM.

AIRFIELD SIGN NOTES

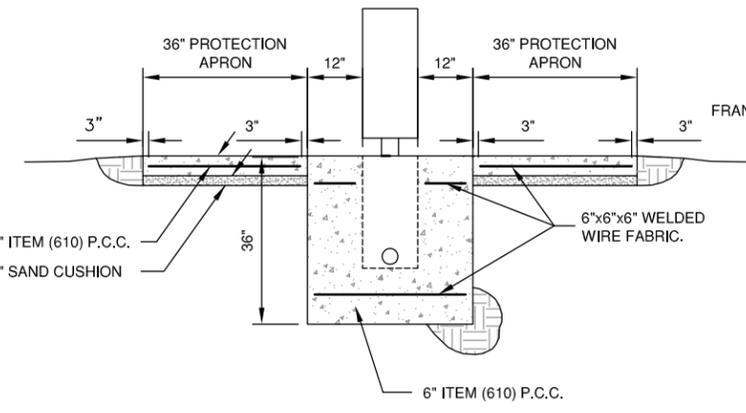
1. TRANSFORMER WATTAGE SHALL BE AS REQUIRED BY SIGN MANUFACTURER.
2. SIGN LEGEND SHALL BE AS SHOWN IN THE PLANS.
3. SIGNS SHALL BE SIZE 3, STYLE 2 OR 3, CLASS 2.
4. SEE "NEW BASE MOUNTED EDGE LIGHT" DETAIL FOR GROUNDING DETAILS.
5. LIGHT I.D. TAG FOR SIGN SHALL INCLUDE SIGN DESIGNATOR SHOWN IN THE PLAN TABLES.

NOTES

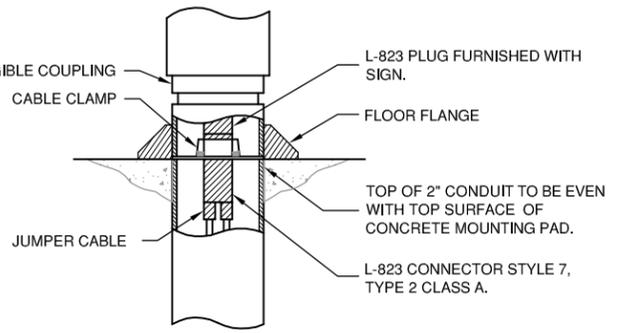
1. SIGN SCHEDULE IS SUBJECT TO FAA APPROVAL OF THE SIGNAGE PLAN.
2. CHANGES TO NEW LEGENDS MAY OCCUR SUBJECT TO 1 ABOVE.
3. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS WITH NEW LEGENDS FOR APPROVAL PRIOR TO STARTING MANUFACTURE.



SECTION A-A



SECTION B-B



ELECTRICAL CONNECTION DETAIL

L-858 AIRFIELD SIGN DETAILS
N.T.S.

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REHABILITATE AIRFIELD LIGHTING
AIRFIELD SIGNAGE DETAIL

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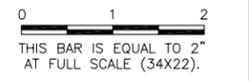
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SHEET	53 OF 62 SHEETS

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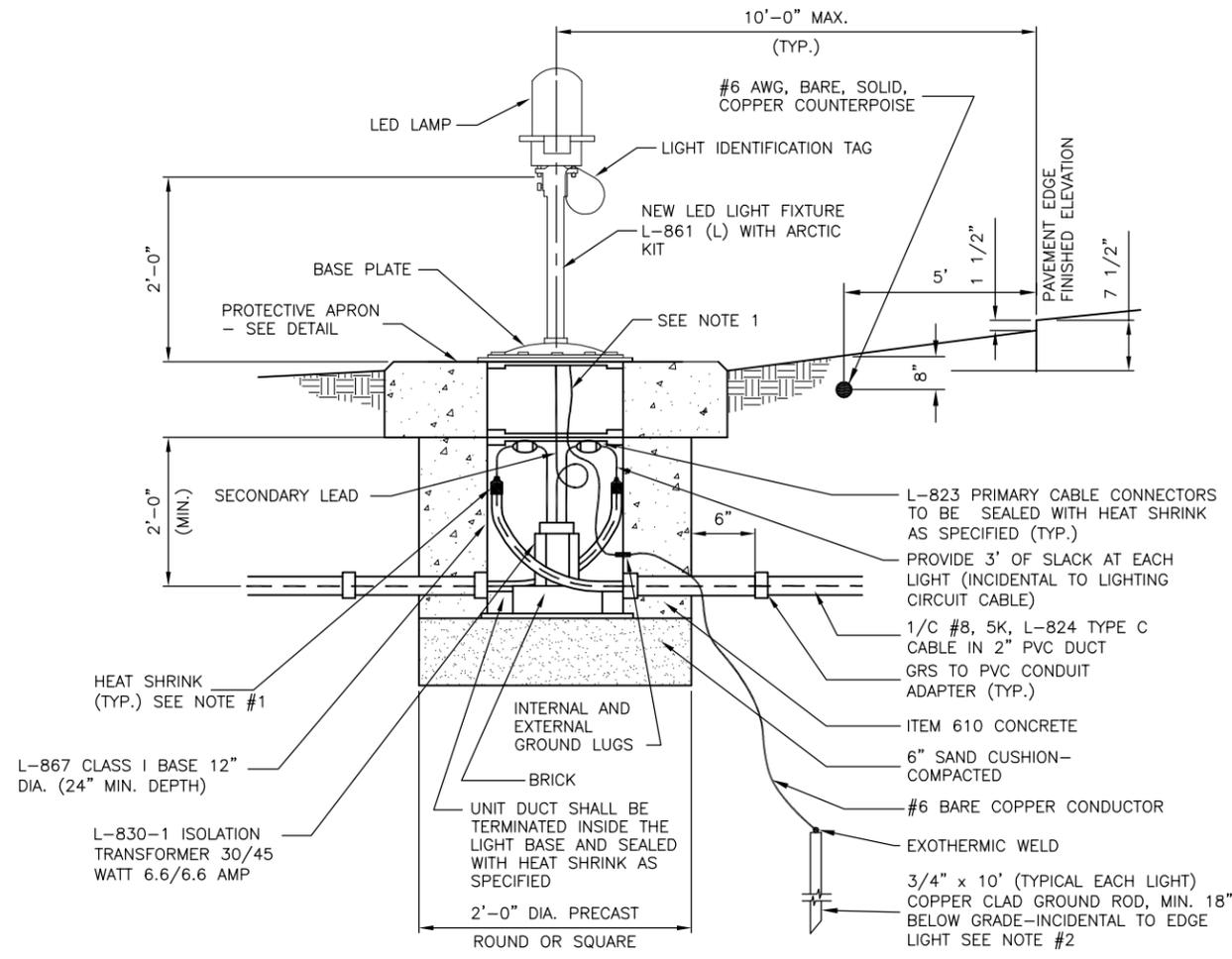
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**REHABILITATE AIRFIELD LIGHTING
 ELECTRICAL DETAILS SHEET 1**

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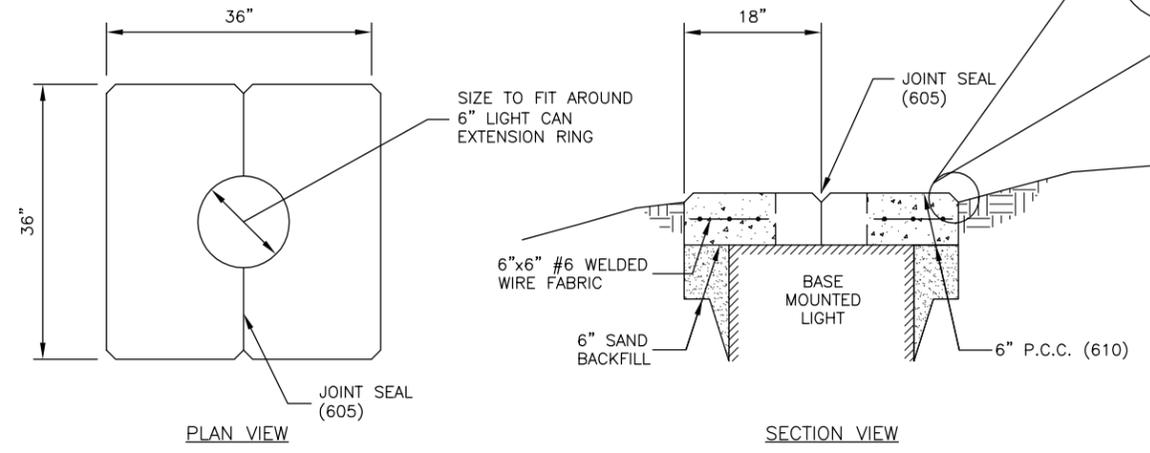
BASE MOUNTED EDGE LIGHTS

N.T.S.

LIGHT NOTES

1. THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.

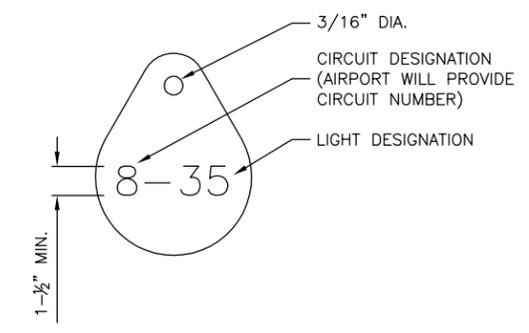
1/2" CHAMFER ON ALL P.C.C. EDGES



PREFORMED PROTECTIVE APRON DETAIL

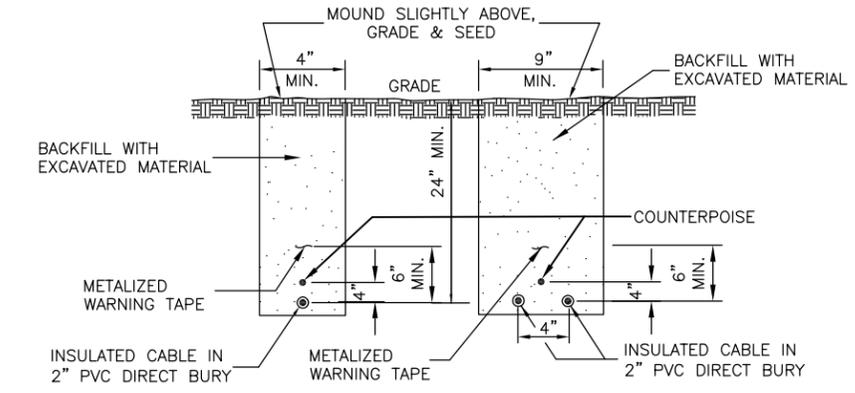
N.T.S.

- NOTES**
1. INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH SET SCREW.
 2. LEGENDS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. CONTRACTOR TO COORDINATE LEGEND WITH AIRPORT.
 3. THE CONTRACTOR SHALL NUMBER THE EXISTING/PROPOSED LIGHTS AND SIGNS IN EACH CIRCUIT STARTING AT THE HOMERUN CONTINUING AROUND THE ENTIRE CIRCUIT BACK TO THE HOMERUN.
 4. AIRFIELD SIGNS SHALL BE TAGGED & NUMBERED.



LIGHT IDENTIFICATION DETAIL

N.T.S.

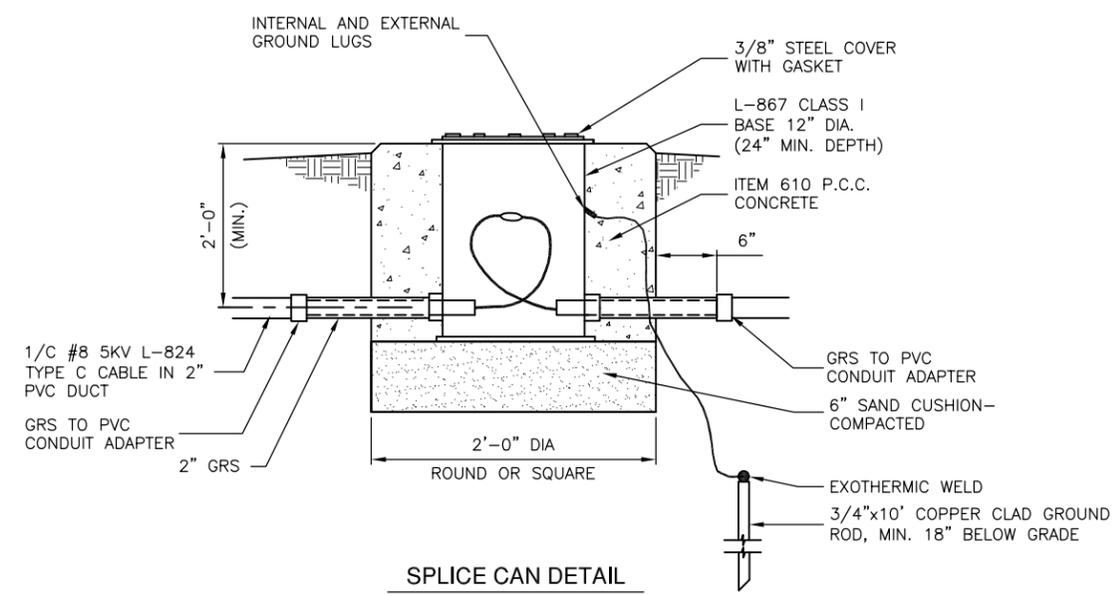


TRENCH DETAIL

N.T.S.

NOTES:

1. TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
2. DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
3. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO ITEM 108.
5. COUNTERPOISE SHALL BE INSTALLED PER THE SPECIFICATIONS.

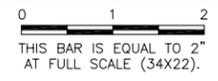


SPLICE CAN DETAIL

N.T.S.

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**REHABILITATE AIRFIELD LIGHTING
 ELECTRICAL DETAILS SHEET 2**

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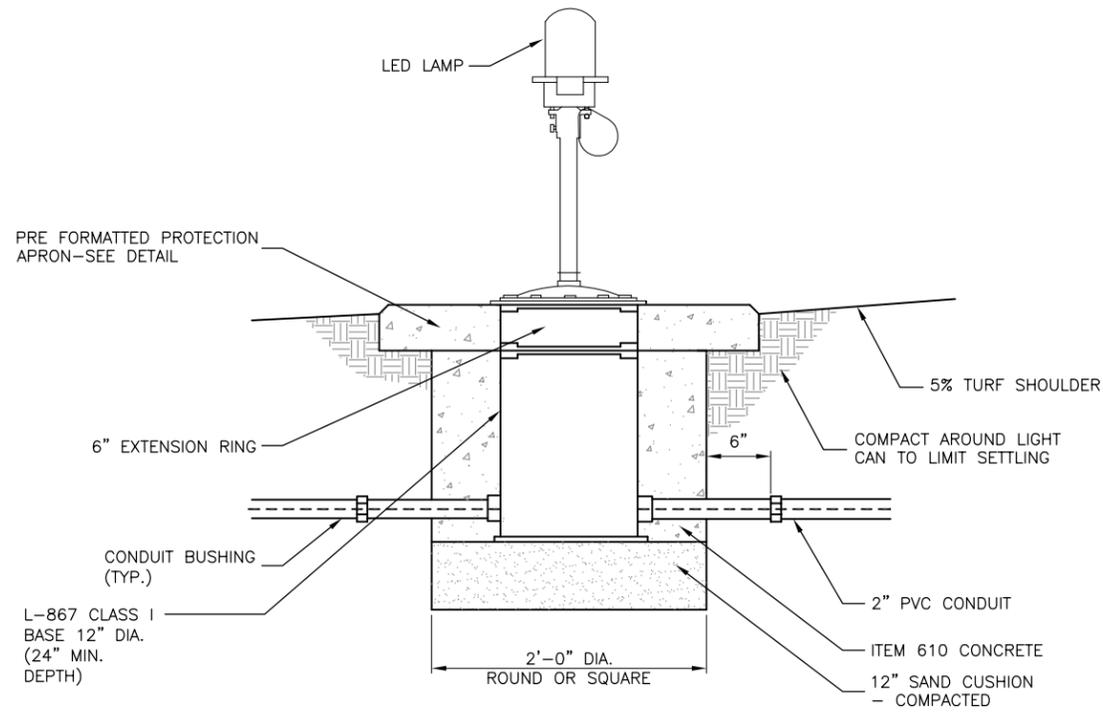


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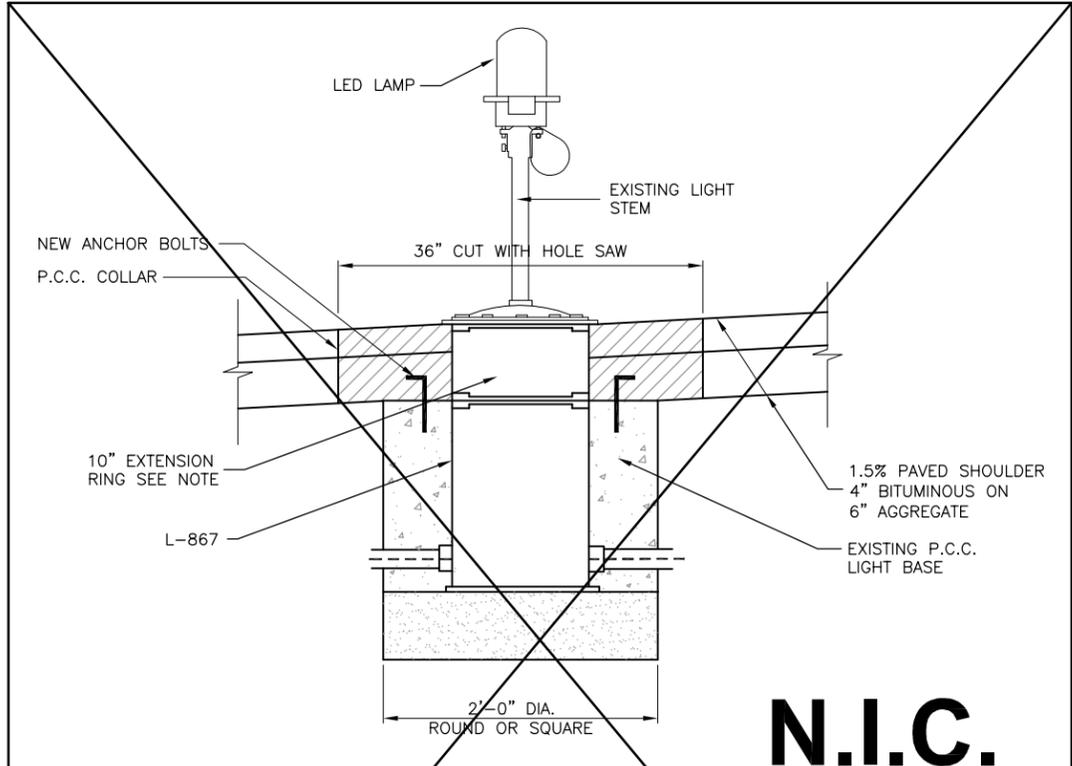
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BASE MOUNTED LIGHT FOR FUTURE PAVED SHOULDER
 N.T.S.

- NOTES:
- SEE LIGHT DETAIL FOR MORE INFORMATION ON ELECTRICAL DETAILS.



BASE MOUNTED LIGHT FOR PAVED SHOULDER
 N.T.S.

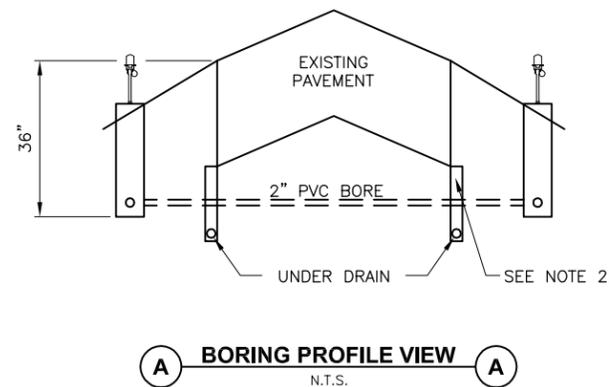
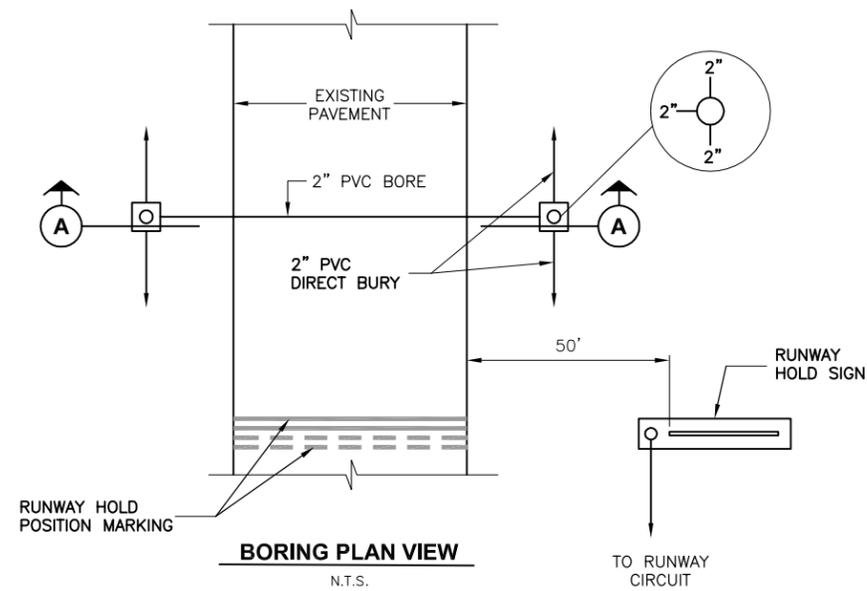
- NOTES:
- PRIOR TO PAVING REMOVE LIGHT STEM AND RESTORE FOR FUTURE USE 6" EXTENSION RING AND REPLACE WITH MUD PLATE AND RECORD THE LOCATION OF THE LIGHT BASE.
 - AFTER PAVING, LOCATE BUSES AND CUT 4" CORE THROUGH BITUMINOUS PAVEMENT AND LOCATE CENTER OF LIGHT CAN.
 - CUT 36" WITH HOLE SAW, CENTERED AT CENTER OF LIGHT BASE.
 - INSTALL MINIMUM 4 ANCHOR BOLTS IN EXISTING LIGHT BASE
 - VERIFY ELEVATION FROM LIGHT BASE TO TOP AT PAVED SHOULDER AND INSTALL EXTENSION RING(S) TO MATCH PAVEMENT ELEVATION.
 - POUR P.C.C. COLLAR AROUND LIGHT BASE AND MATCH ELEVATION.
 - REINSTALL LIGHT STEMS.

N.I.C.

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



NOTES:

1. CONTRACTOR SHALL FIELD LOCATE THE UNDER DRAIN USING NON-DESTRUCTIVE MEANS PRIOR TO BORING.
2. CONTRACTOR SHALL BACK FILL WITH SAND OR CHIPS UP TO THE BOTTOM OF THE BITUMINOUS OR P.C.C. PAVEMENT.
3. CONTRACTOR SHALL DIRECT CONNECT 2" PVC DUCT TO FIRST LIGHT CAN OUT SIDE OF RUNWAY HOLD LINE.
4. COST TO INVESTIGATE AND BACKFILL HOLES SHALL BE INCIDENTAL TO BORING.

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**REHABILITATE AIRFIELD LIGHTING
 ELECTRICAL DETAILS SHEET 3**

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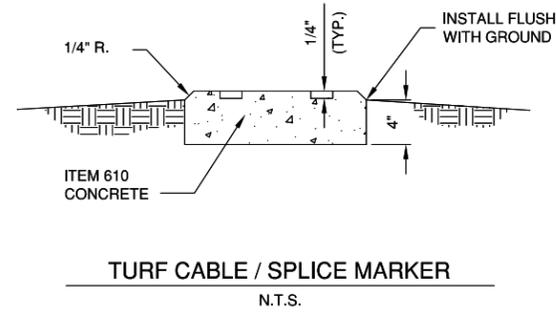
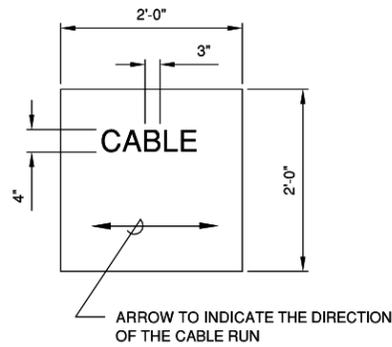
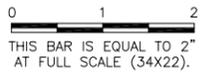


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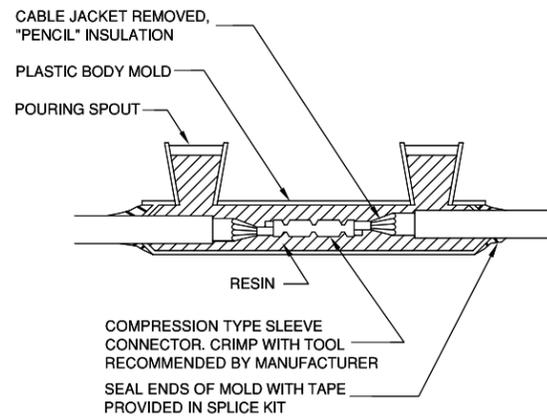
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TURF CABLE / SPLICE MARKER
N.T.S.

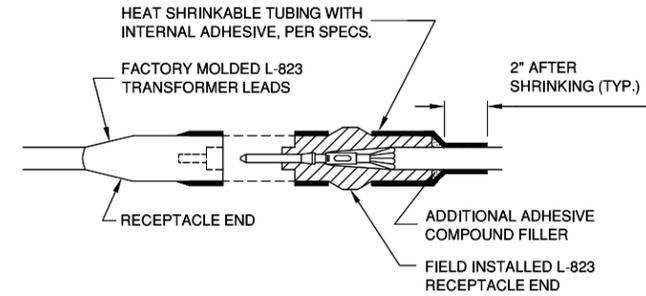
NOTES

- CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE HOMERUN.
- ITEM 610 CONCRETE SHALL BE USED.
- ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
- THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED ITEMS.
- 0.049 CU. YD. CONCRETE PER MARKER.
- A MARKER CONFORMING TO THIS DETAIL MARKED "SPLICE" SHALL BE INSTALLED AT ALL SPLICE LOCATIONS NOT IN LIGHT CANS OR MANHOLES.



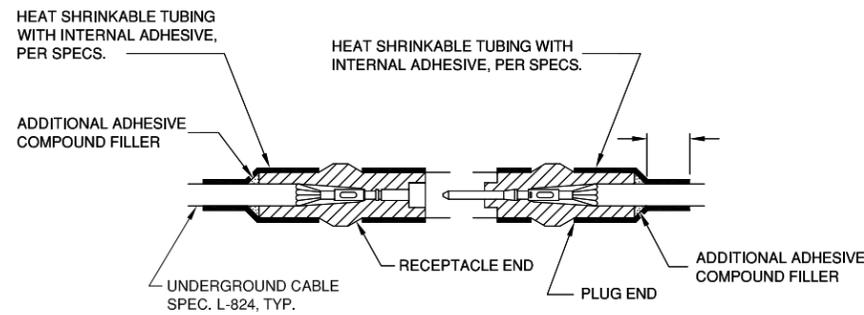
FOR IN-LINE CONNECTIONS OF EXISTING CABLES CUT DURING CONSTRUCTIONS.

TYPE A



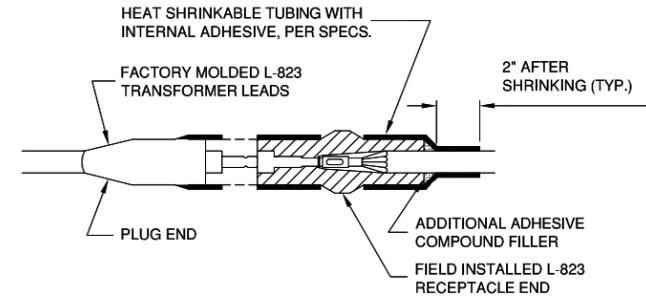
FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.

TYPE C



NOT TO BE USED IN THIS PROJECT UNLESS OTHERWISE DIRECTED BY ENGINEER

TYPE B



FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.

TYPE D

CABLE SPLICES

N.T.S.

NOTES

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.

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 ELECTRICAL DETAILS SHEET 4

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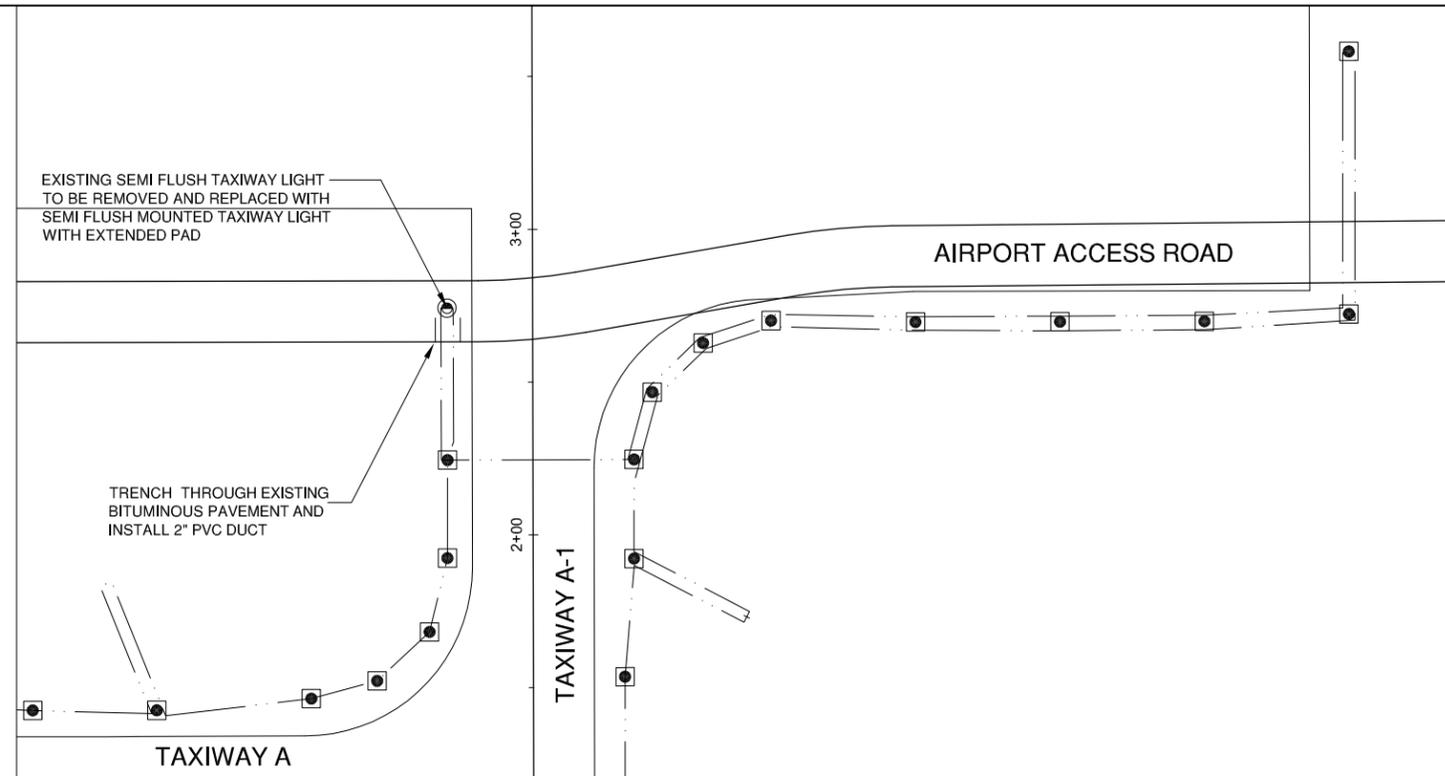
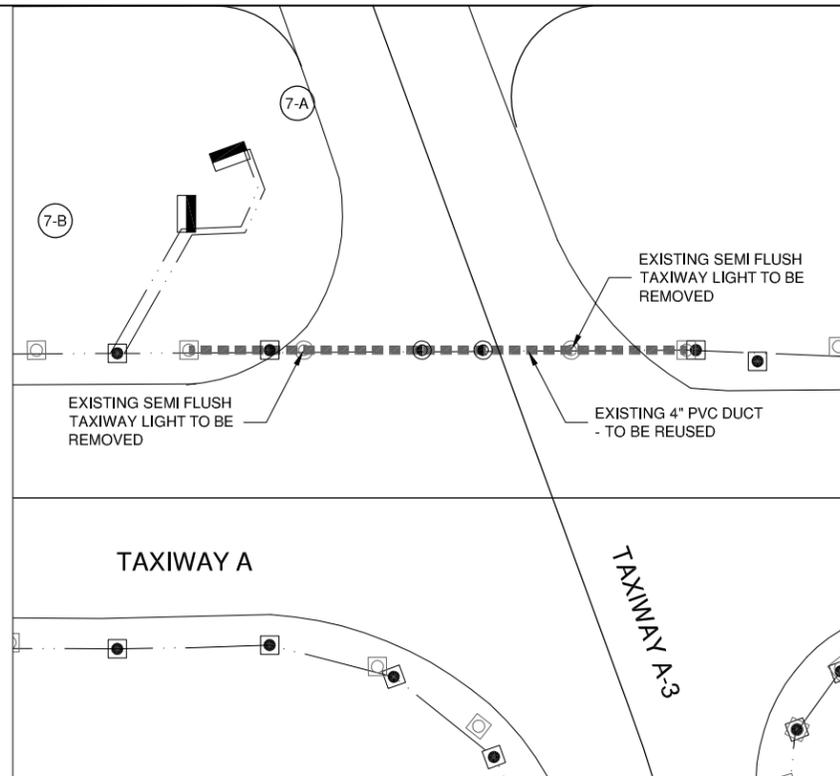
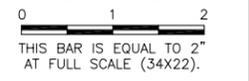
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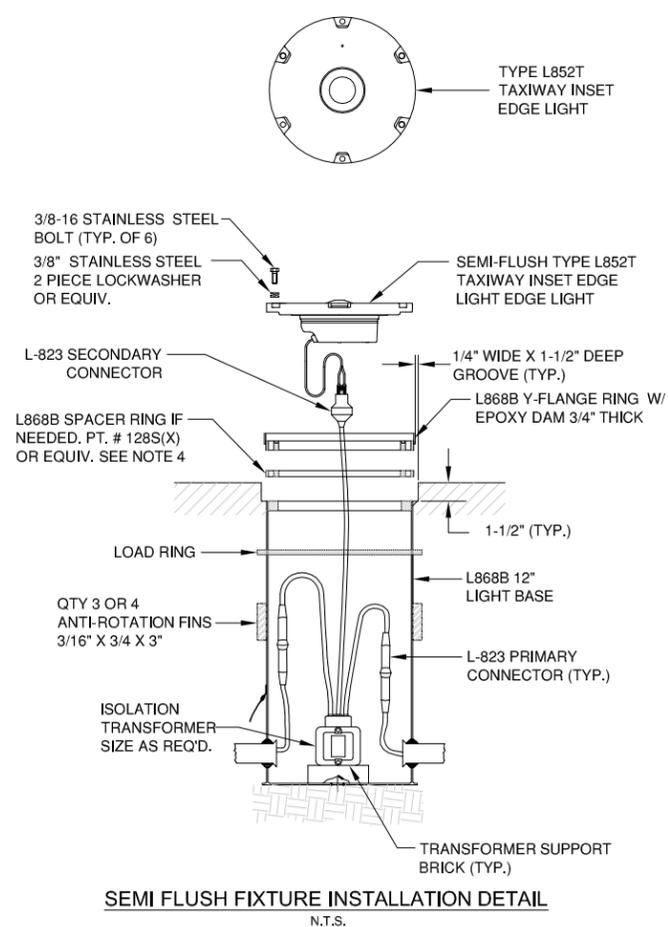
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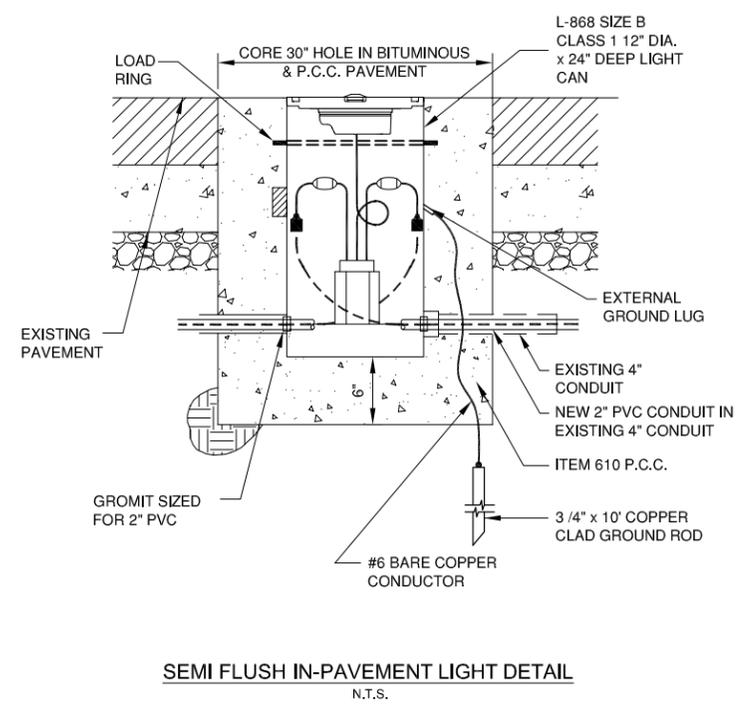
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**REHABILITATE AIRFIELD LIGHTING
 ELECTRICAL DETAILS SHEET 5**



- LIGHT DETAIL NOTES:**
1. LIGHT BASES SHALL BE INSTALLED WITH CARE TO ASSURE VERTICAL & AZIMUTH ALIGNMENT OF FIXTURE. SEE PAVEMENT OR CAN DETAIL FOR CONCRETE REQUIREMENTS.
 2. PROVIDE 2'-3" CABLE SLACK WITHIN LIGHT BASE TO ALLOW TRANSFORMER SERVICING.
 3. BOLTS AND WASHERS USED DURING INSTALLATION OF BASE, CABLE AND TRANSFORMERS SHALL BE REPLACED WITH NEW FOR FINAL BASE IS 0.5".
 4. AS REQUIRED TO MAINTAIN +0/-1/16" BELOW GRADE FAA INSTALLATION TOLERANCE, A MAXIMUM OF THREE SPACER RINGS MAY BE STACKED TOGETHER.



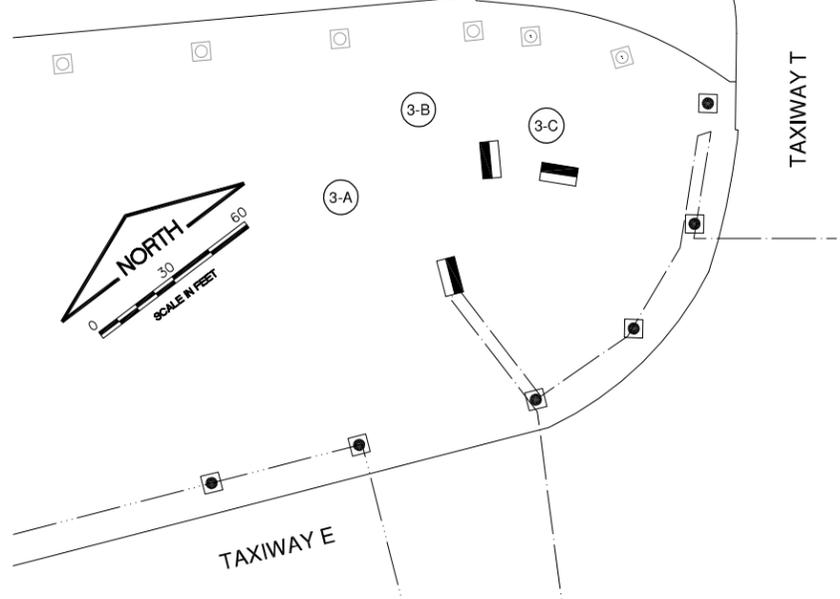
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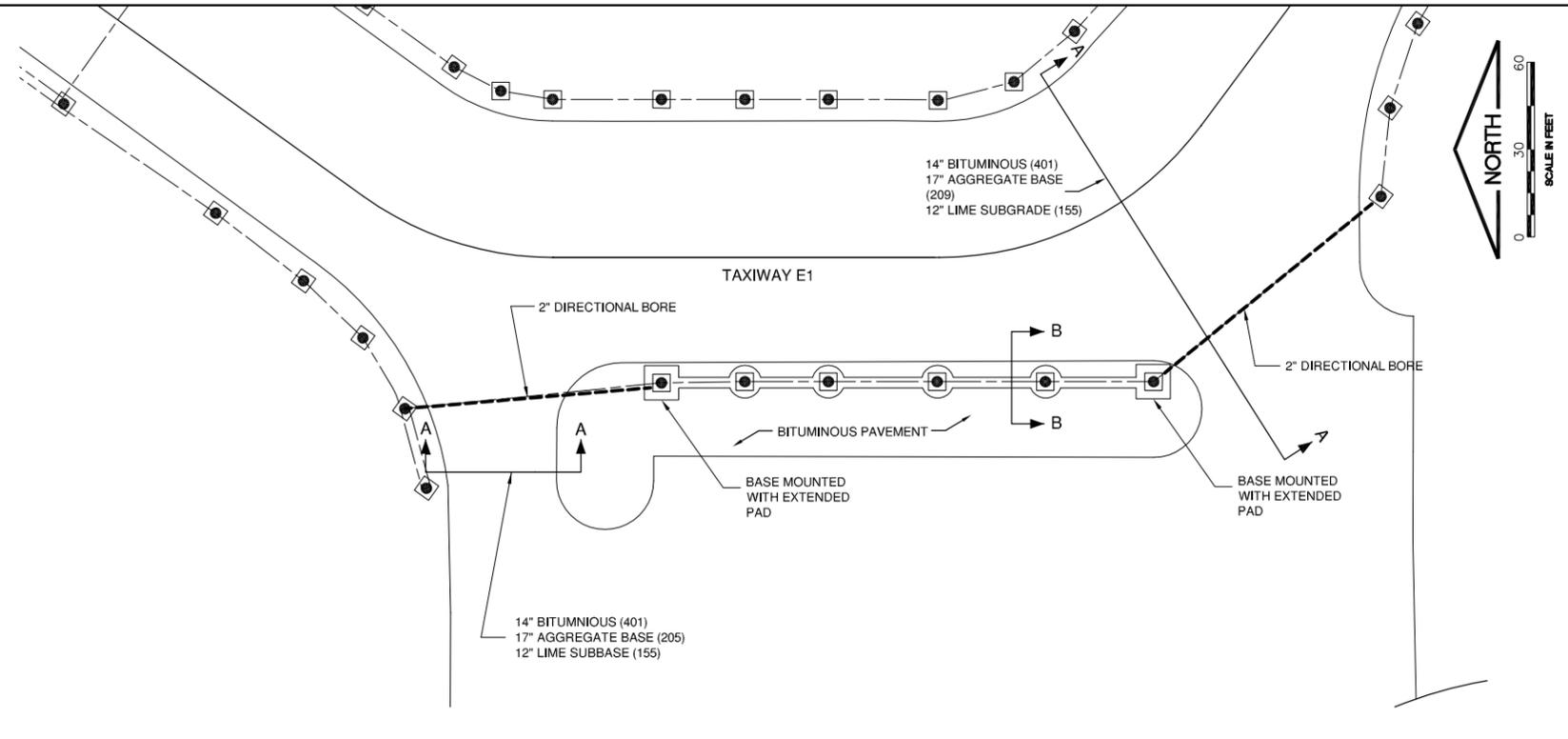
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AIRCARRIER RAMP EXPANSION



NOTES

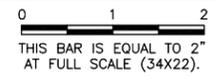
1. CONSTRUCTION OF AIR CARRIER RAMP EXPANSION MAY BE CONSTRUCTED CONCURRENTLY UNDER SEPARATE CONTRACT.
2. SIGNS 3-B AND 3-C ARE TO BE RELOCATED AS PART OF OTHER CONTRACT. LOCATION SHOWN IS THE RELOCATED POSITION. VERIFY LOCATION PRIOR TO REMOVAL AND CONSTRUCTION.



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 UPDATE BY: Chris Groth
 PLOT DATE: 5/25/2012 7:57 AM
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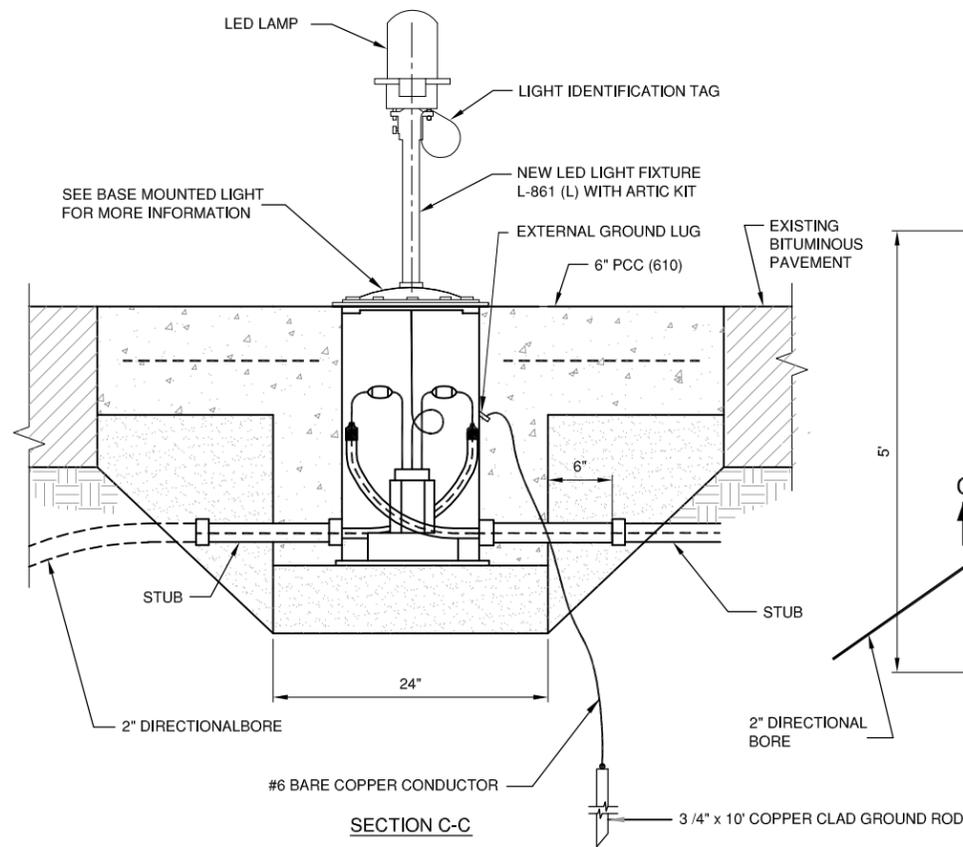
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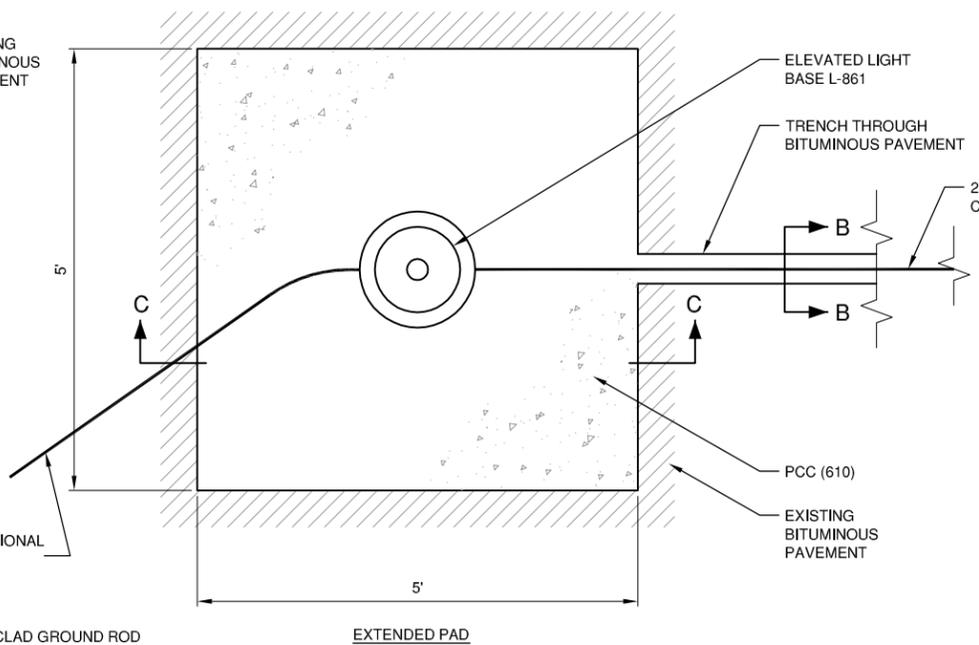


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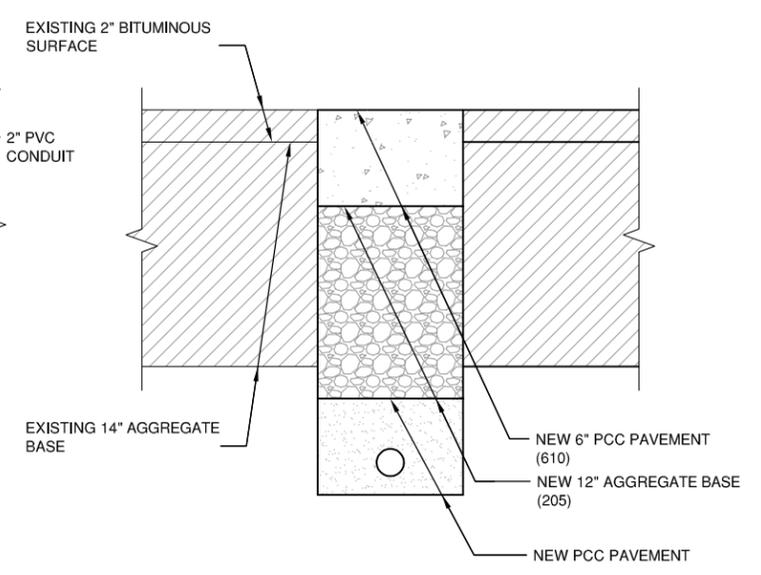
**REHABILITATE AIRFIELD LIGHTING
 ELECTRICAL DETAILS SHEET 6**



ELEVATED LIGHT BASE DETAIL
 N.T.S.



ELEVATED BASE MOUNTED LIGHT IN BITUMINOUS PAVEMENT PLAN VIEW DETAIL
 N.T.S.



CABLE TRENCH DETAIL IN PAVEMENT
 N.T.S.

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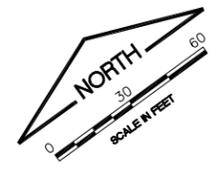
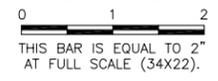
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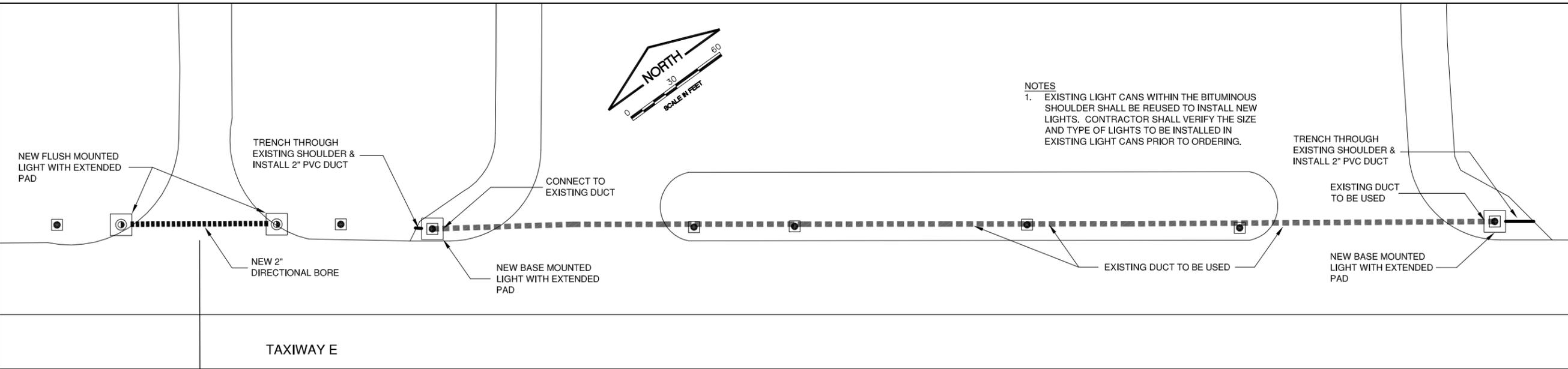
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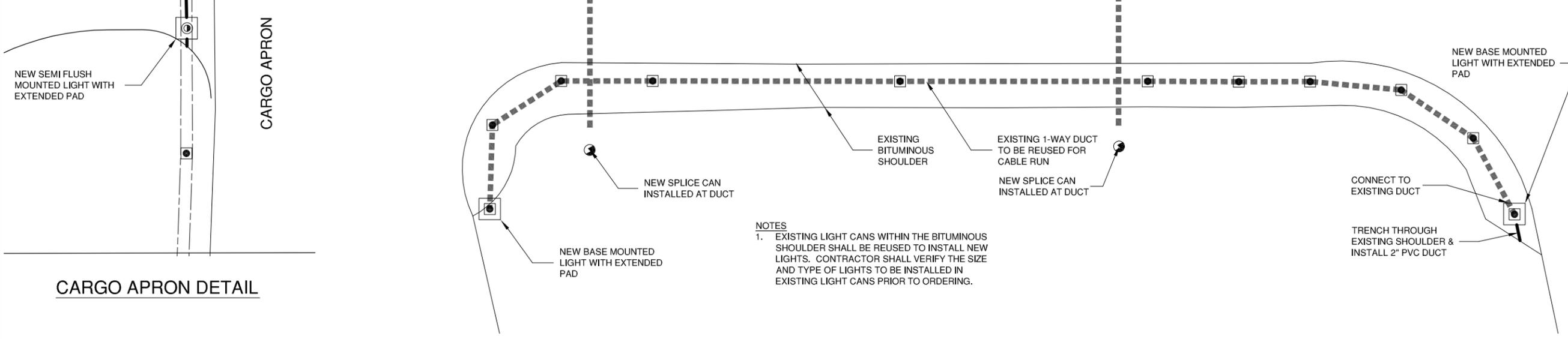
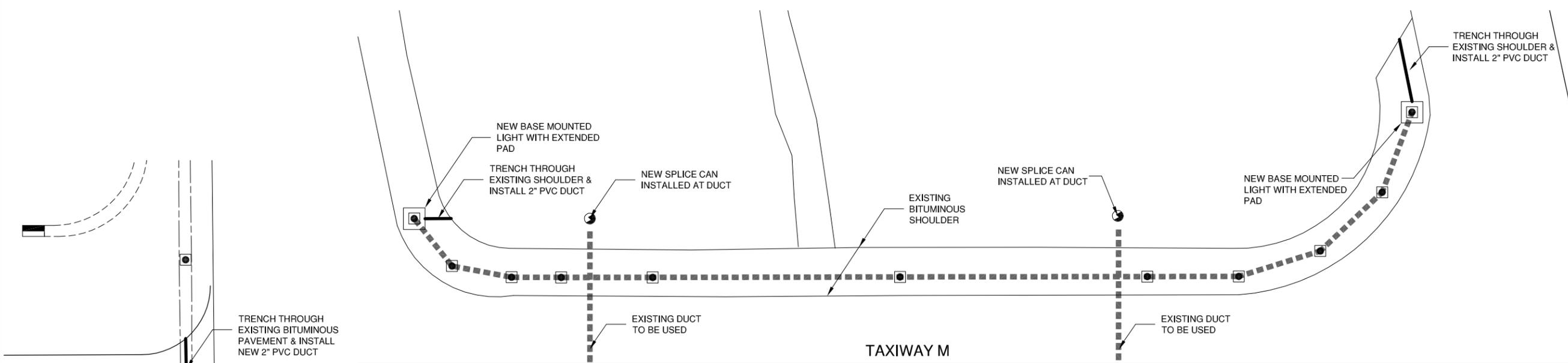
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NOTES
 1. EXISTING LIGHT CANS WITHIN THE BITUMINOUS SHOULDER SHALL BE REUSED TO INSTALL NEW LIGHTS. CONTRACTOR SHALL VERIFY THE SIZE AND TYPE OF LIGHTS TO BE INSTALLED IN EXISTING LIGHT CANS PRIOR TO ORDERING.



NORTH APRON DETAIL



NOTES
 1. EXISTING LIGHT CANS WITHIN THE BITUMINOUS SHOULDER SHALL BE REUSED TO INSTALL NEW LIGHTS. CONTRACTOR SHALL VERIFY THE SIZE AND TYPE OF LIGHTS TO BE INSTALLED IN EXISTING LIGHT CANS PRIOR TO ORDERING.

TAXIWAY M DETAIL

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 ELECTRICAL DETAILS SHEET 7**

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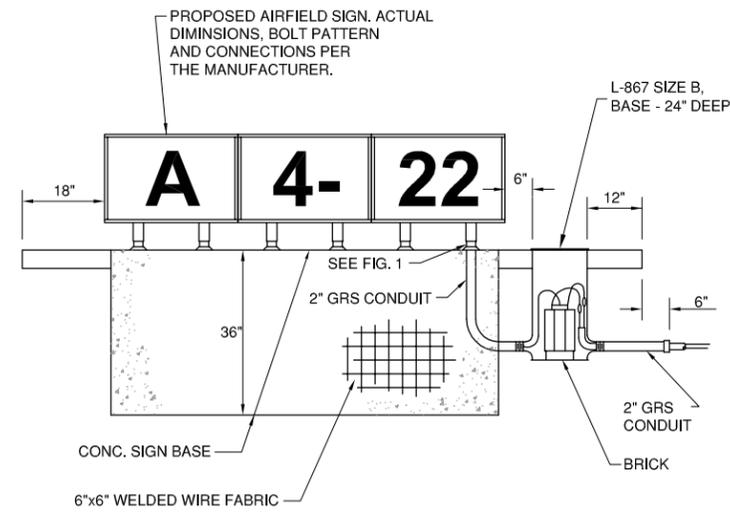
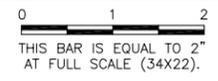
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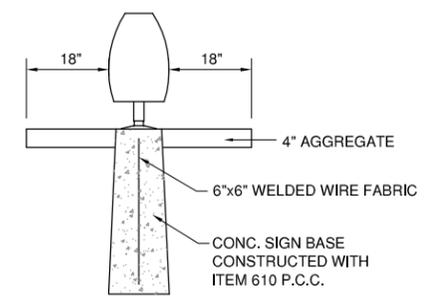
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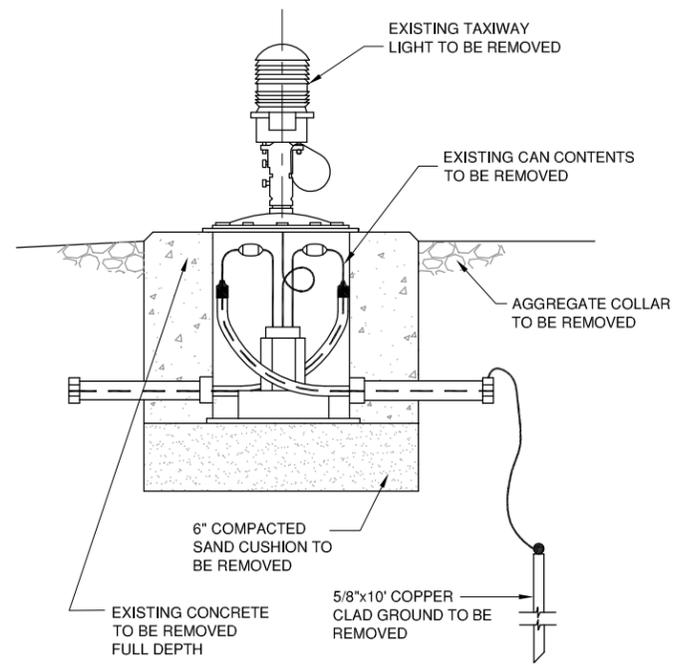


SECTION A-A

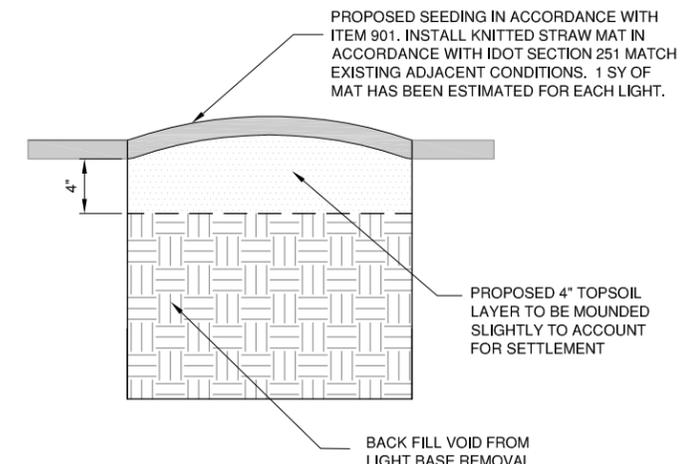


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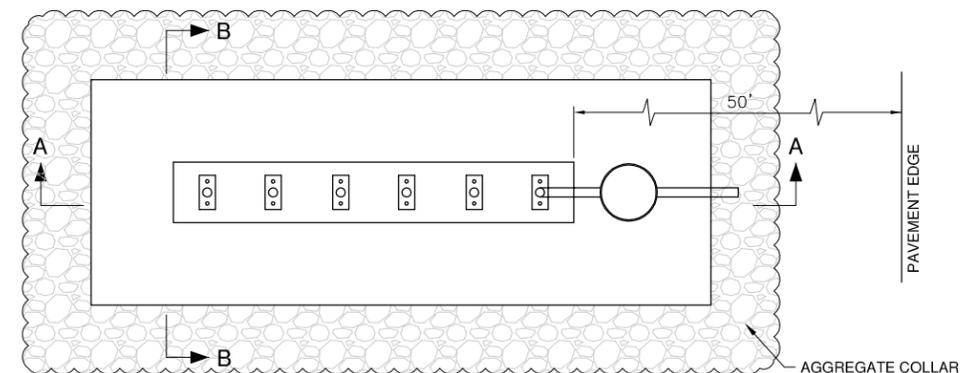
EXISTING AIRFIELD SIGN DETAIL
NOT TO SCALE



EXISTING BASE MOUNTED LIGHT
TO BE REMOVED
N.T.S.

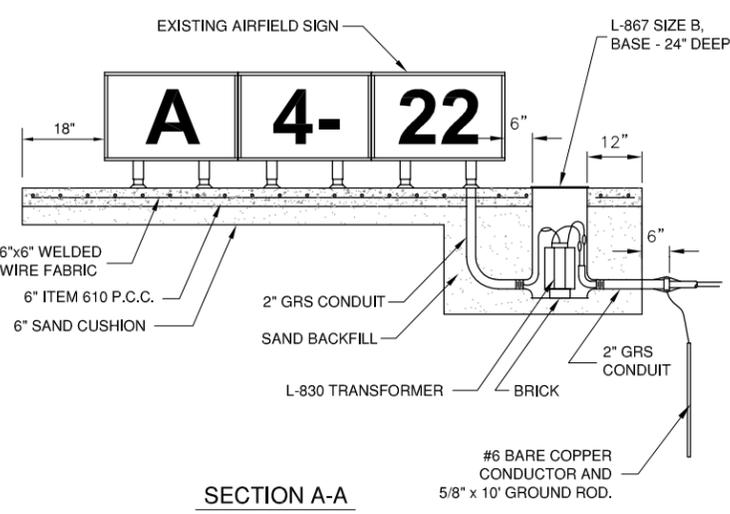


COMPLETED BASE MOUNTED
LIGHT REMOVAL
N.T.S.

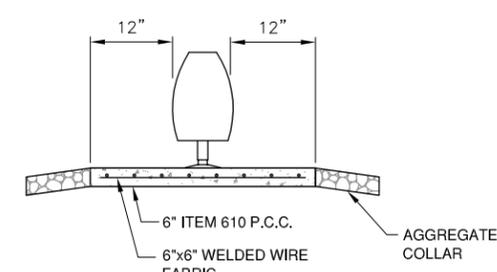


PLAN

- NOTES**
- SIGN & LIGHT DETAILS SHOWN HAVE BEEN TAKEN FROM RECORD DRAWINGS. ACTUAL SIGN DIMENSIONS & FEATURES TO BE REMOVED MAY VARY.
 - NO DISTINCTION IN SIGN TYPE WILL BE MADE FOR PAYMENT RELATED TO SIGN REMOVAL.
 - CONTRACTOR SHALL SURVEY AND RECORD THE LOCATION OF THE EXISTING SIGN PRIOR TO REMOVAL.

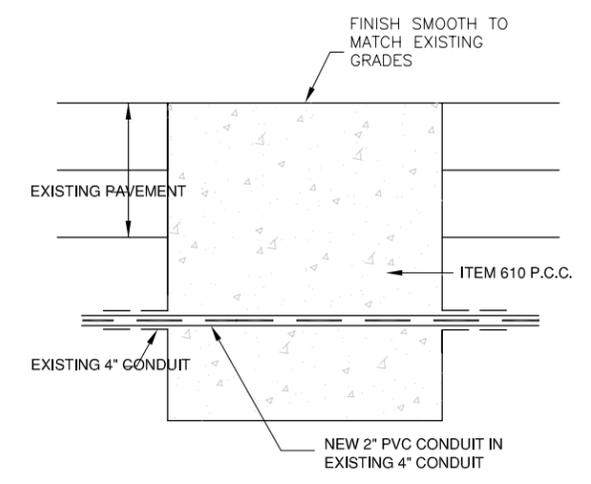
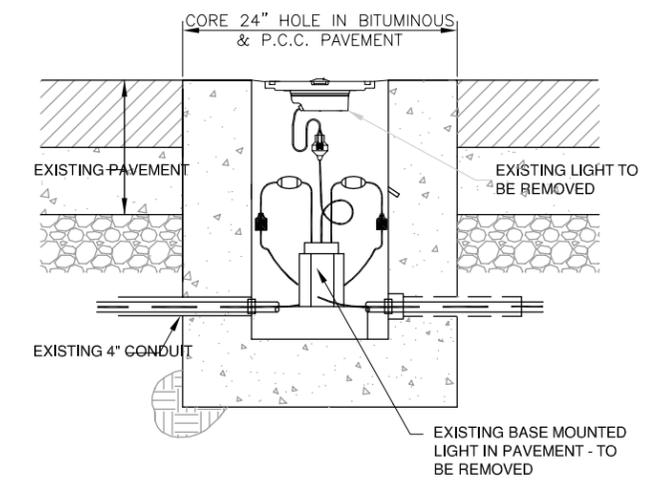


SECTION A-A



SECTION B-B

EXISTING AIRFIELD SIGN DETAIL
N.T.S.



SEMI FLUSH MOUNTED TAXIWAY EDGE LIGHT
REMOVAL DETAIL
N.T.S.

GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS

REHABILITATE AIRFIELD LIGHTING
 SIGN BASE AND LIGHT REMOVAL DETAIL

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GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

DESIGN BY:	AJB
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	APRIL 20, 2012
JOB No:	11061-07-00
IL PROJ. NO.	PIA-4179
AIP PROJ. NO.	3-17-0080-XX
SHEET	61 OF 62 SHEETS

PE093

REVISIONS		
NUMBER	BY	DATE



GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
REHABILITATE AIRFIELD LIGHTING
EXISTING VAULT PLAN

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GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT

DESIGN BY:	CBG
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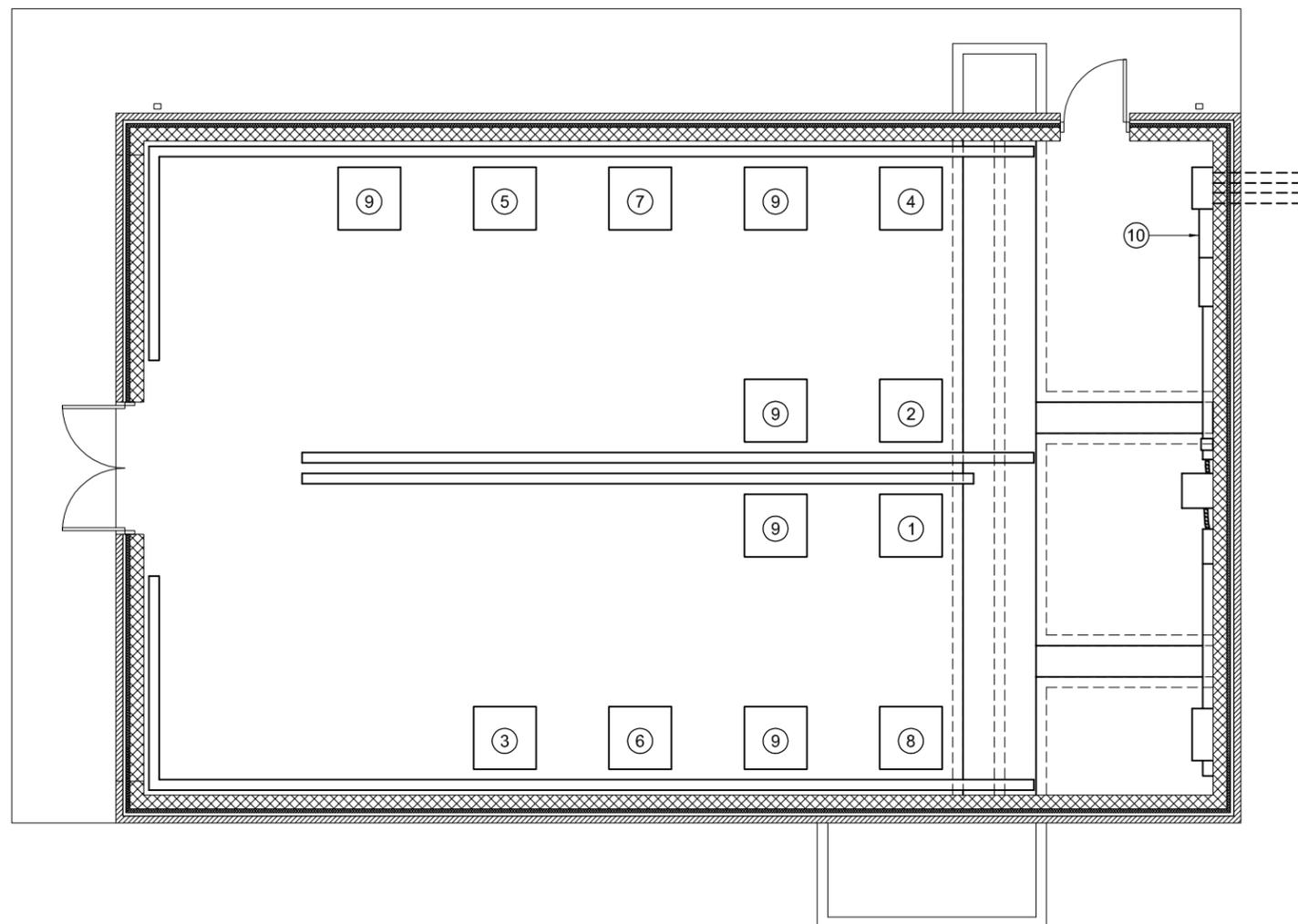
VAULT KEYED NOTES

- 1 RUNWAY 13/31 REGULATOR, 30KW, 480V INPUT, 5-STEP 6.6A OUTPUT.
 - 2 RUNWAY 4/22 REGULATOR, 20KW, 480V INPUT, 5-STEP 6.6A OUTPUT.
 - 3 TAXIWAY CKT #1 REGULATOR, 20KW, 480V INPUT, 3-STEP 6.6A OUTPUT.
 - 4 TAXIWAY CKT #2 REGULATOR, 10KW, 480V INPUT, 3-STEP 6.6A OUTPUT.
 - 5 TAXIWAY CKT #3 REGULATOR, 20KW, 480V INPUT, 3-STEP 6.6A OUTPUT.
 - 6 TAXIWAY CKT #4 REGULATOR, 10KW, 480V INPUT, 3-STEP 6.6A OUTPUT.
 - 7 TAXIWAY CKT #5 REGULATOR, 10KW, 480V INPUT, 3-STEP 6.6A OUTPUT.
 - 8 TAXIWAY CKT #6 REGULATOR, 10KW, 480V INPUT, 3-STEP 6.6A OUTPUT.
- THIS REGULATOR SHALL BECOME A "SPARE" REGULATOR. REMOVE EXISTING LABEL AND TURN OFF FEEDER CIRCUIT BREAKER IN DISTRIBUTION PANEL #1.
- 9 "SPARE" REGULATOR.
 - 10 VAULT DISTRIBUTION PANEL #1, CONTAINING THE FEEDER CIRCUIT BREAKERS FOR THE RUNWAY AND TAXIWAY REGULATORS.

CONTRACTOR SHALL OBSERVE PROPER "LOCKOUT/TAGOUT" PROCEDURES WHEN WORKING ON RESPECTIVE TAXIWAY CIRCUITS TO PREVENT ACCIDENTALLY ENERGIZING AN "OUT OF SERVICE" TAXIWAY CIRCUIT.

REMOVAL OF ABANDONED TAXIWAY SERIES CIRCUIT CABLES

1. AFTER EXISTING TAXIWAY CKT #6 HOMERUN WIRING IS ABANDONED, THE CONTRACTOR SHALL REMOVE ALL ACCESSIBLE SERIES CIRCUIT WIRING AND DISPOSE OF OFFSITE. FOR EXAMPLE, REMOVE WIRING FROM VAULT AND EXISTING CONCRETE ENCASED DUCT BANK, MANHOLES AND HANDHOLES.
2. AFTER EXISTING TAXIWAY CKT #5 HOMERUN WIRING IS ABANDONED, THE CONTRACTOR SHALL REMOVE ALL ACCESSIBLE SERIES CIRCUIT WIRING FROM DUCT BANKS CROSSING TAXIWAY D AND TAXIWAY E BEFORE INSTALLING NEW TAXIWAY CKT #4 HOMERUN CABLES.



VAULT EQUIPMENT PLAN
 SCALE: 1/4" = 1'-0"

RUNWAY AND TAXIWAY REGULATOR CIRCUITING						
CKT. I.D.	TYPE	REGULATOR DATA				AIRFIELD CIRCUITS POWERED
		KW	INPUT	OUTPUT	STEP	
RWY 13/31	L-828	30 KW	480V	6.6A	5-STEP	RUNWAY 13/31
RWY 4/22	L-828	20 KW	480V	6.6A	5-STEP	RUNWAY 4/22
CKT. #1	L-828	20 KW	480V	6.6A	3-STEP	TXY E FROM CENTERFIELD TO RWY 4/22, TXY T, TXY D, TXY M FROM TXY E TO GUARD RAMP, TXY A FROM TXY E TO TXY A4, TXY P, TXY A4, EAST CENTERFIELD
CKT. #2	L-828	10 KW	480V	6.6A	3-STEP	TXY A FROM TXY A4 TO END RWY 22, TXY A1, TXY A2, TXY A3
CKT. #3	L-828	20 KW	480V	6.6A	3-STEP	TXY E FROM CENTERFIELD TO END RWY 13, TXY E3, TXY E4, TXY E5, TXY E6, TXY E7, TXY T1, TXY T2
CKT. #4	L-828	10 KW	480V	6.6A	3-STEP	TXY E FROM RWY 4/22 TO SOUTH HOLD APRON, TXY E1, TXY E2, AIR CARRIER APRON
CKT. #5	L-828	10 KW	480V	6.6A	3-STEP	TXY A FROM TXY E TO END RWY 4, TXY A5