

IDOT PROJECT LABOR AGREEMENT DETERMINATION

Item Job

To: Ann L. Schneider, Secretary
From: William R. Frey, Interim Director
Re: FAP Route 742 (IL 2) / Contract #64515

Date: ~~February 16, 2012~~ *June 15, 2012*
~~April 27, 2012~~ *Letting*

In accordance with Executive Order 2003-13 (Blagojevich), it is recommended that a Project Labor Agreement (PLA) be utilized for the above-captioned Project. This recommendation is based on the considerations indicated below:

- 1) The Project is being awarded and administered by IDOT (i.e., not by another government agency).
- 2) The Project is being constructed using state or local funds only (i.e., no federal funds) (See Attachment A).
- 3) The overall size, scope, sequencing, logistics or other aspects of the Project make it particularly challenging to manage, and use of a PLA is expected to help assure that the construction work is performed properly and efficiently under the circumstances (See Attachment A).
- 4) The duration of construction activity on the Project is expected to exceed one construction season (i.e., 110 or more working days), or the nature of the Project results in a heightened need for labor force continuity and stability over a substantial period of time (See Attachment A).
- 5) There is a firm construction completion date (November 18, 2013) established for the Project thereby increasing the adverse consequences of any work stoppage or other labor disruption (See Attachment A).
- 6) The time required to complete the Project is expected to extend beyond the expiration date of one or more existing collective bargaining agreements covering trades likely to be involved in the Project, thereby increasing the likelihood of work stoppage(s) or other labor disruption(s) during construction of the Project.
- 7) In the absence of a PLA, there is an increased likelihood of jurisdictional disputes among unions or of conflict between unionized and non-unionized workers on the Project that could have a potentially material adverse effect on the time, cost, or quality of work performed on the Project (See Attachment A)

- 8) This Project presents specific safety concerns to the traveling public and a PLA will ensure labor force continuity and stability, decreasing the length of the safety concern (See Attachment A).
- 9) Use of a PLA is expected to result in improved access to skilled labor, improved efficiency, or improved safety performance on the Project
- 10) Use of a PLA on the Project is not expected to have a material adverse effect on the competitive bidding process.
- 11) Use of a PLA on the Project is not expected to have a material adverse effect on the ability of the Department to achieve other Departmental goals (e.g., utilization of disadvantaged businesses, utilization of Illinois domiciled businesses, development of competitive vendor alternatives over time, etc.)
- 12) There are other material considerations favoring or disfavoring use of a PLA on this Project as follows:

Based upon the identified considerations, we recommend that you approve use of a PLA on this Project. Upon your approval, the Department shall undertake to negotiate in good faith a PLA with the relevant labor organization(s), and shall include in all necessary bid specifications and other documents information regarding the actual or form of PLA that is to binding upon all contractors and their employees.

Agreed: William R. Frey 4/11/12
 {Division Chief} AAW (Date)

Agreed: [Signature] 3/30/12
 {Bureau of Design & Environment} (Date)

Agreed: [Signature] 3/6/12
 {Regional Engineer} (Date)

Approved: Ann L. Schneider 4/11/12
 Ann L. Schneider, Secretary (Date)

Susan Wolfsberger By Susan Wolfsberger,
 AA II

FHWA concurrence in the PLA for the above-mentioned contract

Gregory G. Nadeau	3/01/2012
FHWA Deputy Administrator	(see attached approval page)

Attachment A

Justification for use of Project Labor Agreement (PLA) on Contract #64515, Winnebago County

The use of a PLA on this project is consistent with all State and Local statutory and regulatory requirements.

Item 2: This project is federally funded

Item 3: Estimated project cost is \$13,500,000

Any disruption in the continuity of this project due to labor issues would result in delayed deadlines that would cause safety concerns for the traveling public. Lapsing trade contracts during the first stage of the Project would be addressed through the use of a PLA. It is the Illinois Department of Transportation's finding that the large skilled workforce needed for this Project can be provided by the union trades involved and is a necessary requirement for a PLA.

Item 4: The Project is being staged over two construction seasons.

The IL 2 is a four lane highway with two lanes in each direction throughout the limits of this project. The scope of the project includes replacing existing pavement, resurfacing, along with widening. The project will also include new curb & gutter, sidewalk, multi-use path and storm sewer. The project completion date is November 30, 2013 with an interim completion date for winter shut down of November 30, 2012.

While the project will generally be staged to maintain one lane of traffic in each direction, there will be a 4 week closure for the replacement of a box culvert and intersection reconstruction. A 2 week closure for the ramps of US Bypass 20 with IL 2 will also be required to realign the ramp to a stop condition instead of a free flow.

Item 5: Completion Date is November 30, 2013

Item 7: In the case of PLA disputes, the completion date of the Project will be in jeopardy.

Item 8: Any disruption to the contractor's schedule due to labor issues may result in the delay of the project completion date and may result in the diversion to one lane traffic being in place past the interim and final deadline. This additional diversion, especially if left over the winter months, may contribute to additional exposure to crashes through the work zone due to the inherent risk of driver attention to lane changes, etc. that will be in place during stage construction.

PLA Request

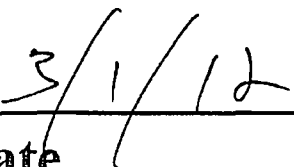
Approval of Project Labor Agreement

Disapproval of Project Labor Agreement

Reason for disapproval:



Signature



Date

Execution Page

Item 206

Illinois Department of Transportation

William R. Frey
William R. Frey, P.E., Interim Director of Highways

Matthew R. Hughes
Matthew R. Hughes, Director Finance & Administration

Ellen Schanzle-Haskins
Ellen Schanzle-Haskins, Chief Counsel

Ann L. Schneider
Ann L. Schneider, Secretary

4/11/12
(Date)

Susan Wolfsberger

By Susan Wolfsberger,
AA II

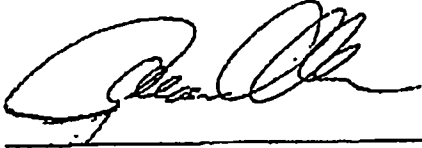
Illinois AFL-CIO Statewide Project Labor Agreement Committee, representing the local unions listed below:

Michael T. Parigian

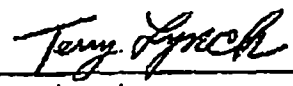
March 28, 2012

(Date)

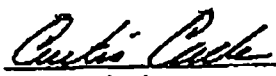
List Union Locals:



Jim Allen
Bricklayers



Terry Lynch
Heat & Frost Insulators & Allied
Workers



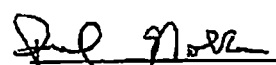
Curtis Cade
United Association




Richard Mathis
Roofers

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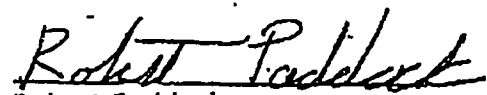
Ed Christensen, Elevator
Constructors



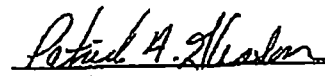
Paul Noble
IBEW



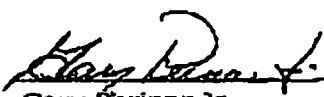
Terry Fitzmaurice
Painters



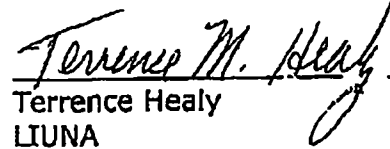
Robert Paddock
IUOE



Pat Gleason
Teamsters



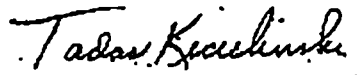
Gary Perinar Jr.
Carpenters



Terrence Healy
LIUNA



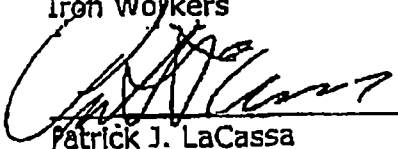
Robert Schneider
Sheet Metal Workers



Tadas Kiceliński
Iron Workers



John Skermont
Boilermakers



Patrick J. LaCassa
OPCMIA

*only if Elevator Constructors master agreement
language is attached to PLA