



NOTES FOR TEMPORARY TRAFFIC SIGNALS

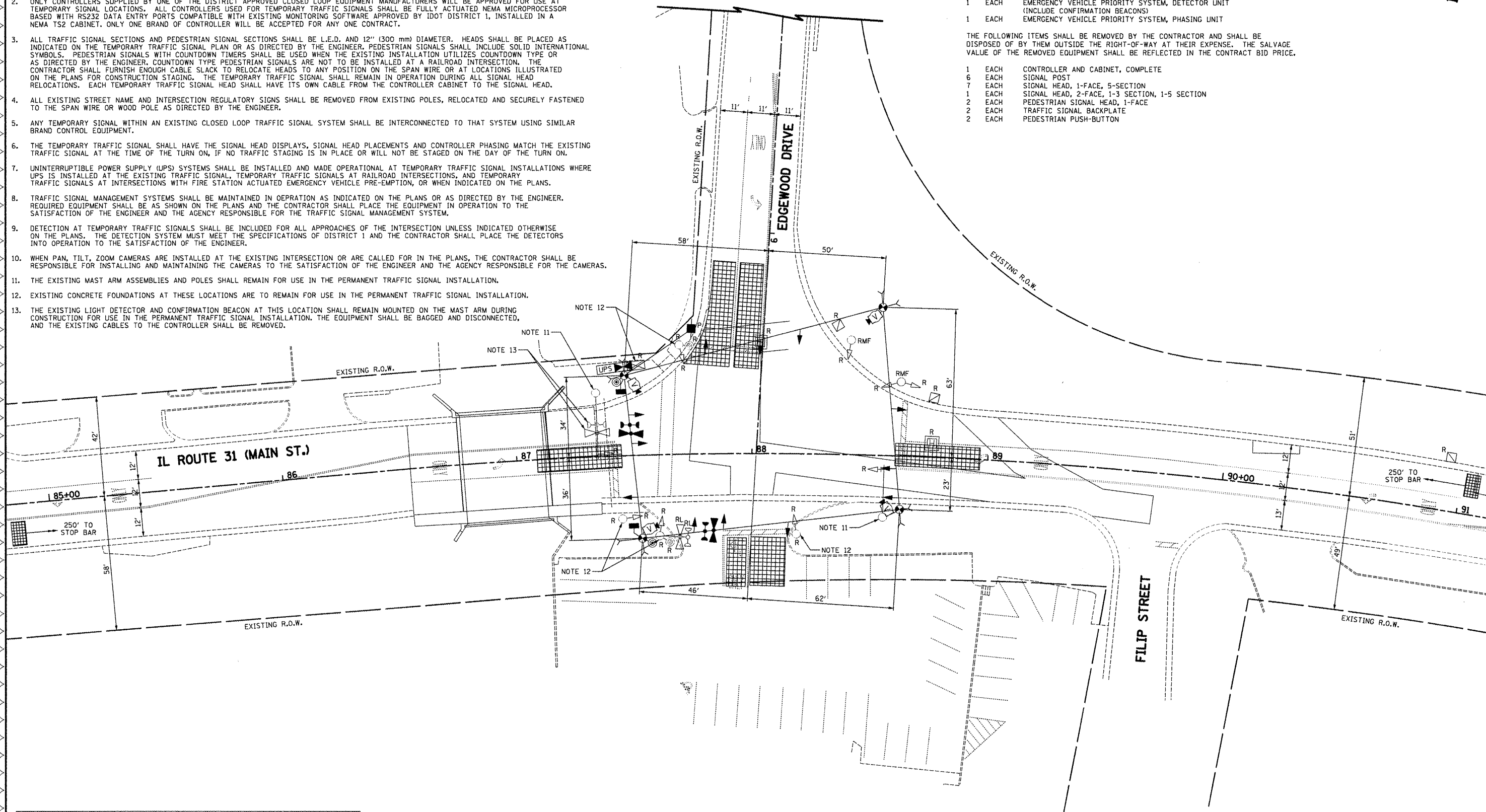
1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE L.E.D. AND 12" (300 mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL. TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
10. WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.
11. THE EXISTING MAST ARM ASSEMBLIES AND POLES SHALL REMAIN FOR USE IN THE PERMANENT TRAFFIC SIGNAL INSTALLATION.
12. EXISTING CONCRETE FOUNDATIONS AT THESE LOCATIONS ARE TO REMAIN FOR USE IN THE PERMANENT TRAFFIC SIGNAL INSTALLATION.
13. THE EXISTING LIGHT DETECTOR AND CONFIRMATION BEACON AT THIS LOCATION SHALL REMAIN MOUNTED ON THE MAST ARM DURING CONSTRUCTION FOR USE IN THE PERMANENT TRAFFIC SIGNAL INSTALLATION. THE EQUIPMENT SHALL BE BAGGED AND DISCONNECTED, AND THE EXISTING CABLES TO THE CONTROLLER SHALL BE REMOVED.

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE RELOCATED TO THE PROPOSED SIGNAL. THE CONTRACTOR SHALL SAFELY STORE ALL EQUIPMENT TO BE RELOCATED TO THE PROPOSED SIGNAL AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

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|---|------|---|
| 1 | EACH | EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT (INCLUDE CONFIRMATION BEACONS) |
| 1 | EACH | EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT |

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

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|---|------|---|
| 1 | EACH | CONTROLLER AND CABINET, COMPLETE |
| 6 | EACH | SIGNAL POST |
| 7 | EACH | SIGNAL HEAD, 1-FACE, 5-SECTION |
| 1 | EACH | SIGNAL HEAD, 2-FACE, 1-3 SECTION, 1-5 SECTION |
| 2 | EACH | PEDESTRIAN SIGNAL HEAD, 1-FACE |
| 2 | EACH | TRAFFIC SIGNAL BACKPLATE |
| 2 | EACH | PEDESTRIAN PUSH-BUTTON |



RESTORATION OF WORK AREA.
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDER, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SALT TOLERANT SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FILE NAME =
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USER NAME = oth

DESIGNED - BRD

REVISED - 5/21/2012

DRAWN - OJT

REVISED -

PLOT SCALE = 40.0000' / 1" in.

CHECKED - JJE

REVISED -

PLOT DATE = 5/31/2012

DATE - 5/3/2012

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
IL ROUTE 31 (MAIN ST.) AT EDGEWOOD DRIVE

SCALE: 1"=20'

SHEET NO. 1 OF 1 SHEETS

STA.

TO STA.

ADDENDUM A REV. 5/21/2012 CTB

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0003	18A-2	MCHENRY	825	263
CONTRACT NO. 60F72				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				