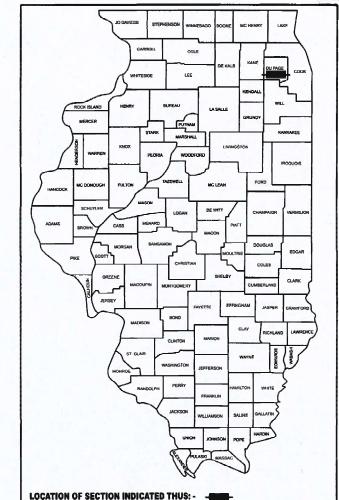
STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

D-91-098-23



STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

March 22, 202

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PROPOSED

FAP ROUTE 344: IL 83 (KINGERY HWY) AT BLUFF ROAD **SECTION: FAP 344 23 TSM** PROJECT: HSIP-1RP5(547) TRAFFIC SIGNAL MODERNIZATION, INTERSECTION IMPROVEMENTS

C-91-130-23

DUPAGE COUNTY

ILLINOIS ROUTE 83 AT BLUFF ROAD

FOR INDEX OF SHEETS, SEE SHEET NO. 2

GROVE TOWNSHIP

IL 83 (KINGERY HIGHWAY)

POSTED SPEED = 45 MPH

POSTED SPEED = 35 MPH

TRAFFIC DATA:

2021 ADT = 24700

2020 ADT = 4850

BLUFF RD

PROJECT IS LOCATED IN THE DOWNERS

DESIGN CLASSIFICATION = OTHER PRINCIPAL ARTERIAL

DESIGN CLASSIFICATION = LOCAL ROAD OR STREET

ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS. THE ABOVE SCALES MAY BE USED.

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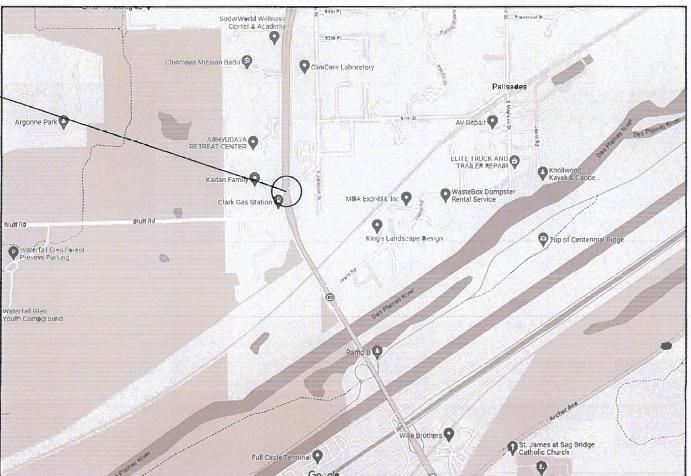
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JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION 1-800-892-0123

PROJECT ENGINEER: LUKASZ POCIECHA (847) 705-4255 **PROJECT MANAGER: FAWAD AQUEEL**

CONTRACT NO. 62U73



DOWNERS GROVE TOWNSHIP

REV-SEP

HIGHWAY PLANS

GROSS & LENGTH = 2000 FT. = 0.379 MILES

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- TRAFFIC SIGNAL MOUNTING DETAILS

MIXTURE TABLE

| MIXTURE TYPE | AIR VOIDS @ NDES | QUALITY MANAGEMENT PROGRAM (QMP) | | | |
|---|---------------------|---|--|--|--|
| PAVEMENT WIDENING | | | | | |
| HMA SURFACE COURSE, IL-9.5, MIX "D", N50, 2" | 4% @ 50 GYR. | QC/QA | | | |
| HMA BASE COURSE WIDENING, 8" (HMA BINDER IL-19.0) | 4% @ 50 GYR. | QC/QA | | | |
| DRIVEWAY | | | | | |
| HMA SURFACE COURSE, MIX "D", N50, IL-9.5, 2" | 4% @ 50 GYR. | QC/QA | | | |
| HMA BASE COURSE, 8", (HMA BINDER, IL-19.0) | 4% @ 50 GYR. | QC/QA | | | |
| QMP DESIGNATION: QUALITY CONTROL / QUALITY ASSURANCE (QC / QA); QUALITY CONTROL FOR | | | | | |
| PERFORMANCE; (QCP); PAY FOR PERFORMANCE (PFP) | | | | | |

- THE UNIT WEIGHT USED TO CALCAULTE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- THE AC TYPE FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE AC TYPE SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED)
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT
- TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE MPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF
- THE CONTRACTOR SHALL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEET OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR SHALL DELIVER THE RECORD TO THE ENGINEER
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER.
 REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN
- THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, AREA TRAFFIC FIELD ENGINEER AT WALTER.CZARNY@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR SHALL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD, FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE
- THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN
- THE CONTRACTOR SHALL TAKE EXTRA CARE IN GRADING AND EXCAVATING NEAR TREES WHICH ARE NOT MARKED FOR REMOVAL SO AS NOT TO CAUSE INJURY TO THE ROOT SYSTEM OR TRUNKS. MAJOR ROOTS OF A TREE THAT ARE TO REMAIN IN PLACE EXTENDING INTO THE EXCAVATION AREAS AT AN ELEVATION THAT WOULD INTERFERE WITH ANY PORTION OF THE PLANNED CONSTRUCTION SHALL BE SEVERED AT A POINT IMMEDIATELY OUTSIDE OF THE EXACTION AREA IN A MANNER THAT WILL CAUSE THE LEAST AMOUNT OF SYSTEMIC TO THE REMAINING TREE STRUCTURE, ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
- THE CONTRACTOR SHALL ERECT A TEMPORARY FENCE AROUND ALL TREES WITHIN THE CONSTRUCTION AREA TO ESTABLISH A "TREE PROTECTION ZONE" AND AROUND EXISTING WETLANDS TO ESTABLISH A "WETLAND PROTECTION ZONE" BEFORE ANY WORK BEGINS OR ANY MATERIAL IS DELIVERED TO THE JOBSITE. NO WORK IS TO BE PERFORMED (OTHER THAN ROOT PRUNING), MATERIALS STORED OR VEHICLE DRIVEN OR PARKED WITHIN THE "TREE PROTECTION ZONE" AND "WETLAND PROTECTION ZONE." REMOVE PROTECTIVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED
- THE CONTRACTOR SHALL ATTACH AN ALUMINUM SIGN WITH THE FOLLOWING TEXT: PROTECTED WETLAND - NOT INTRUSION. THE SIGN(S) SHALL BE ATTACHED TO THE STAKES BY A METHOD APPROVED BY THE ENGINEER. THE SIGN(S) WILL BE PROVIDED BY THE DEPARTMENT AND SHALL BE PICKED UP BY THE CONTRACTOR FROM THE DISTRICT ONE ROADSIDE DEVELOPMENT ARCHITECT IN SCHAUMBURG, ILLINOIS BY CONTACTING 847-705-4171. WHEN WORK HAS BEEN COMPLETED, THE SIGN SHALL BE RETURNED TO THE DISTRICT ONE ROADSIDE DEVELOPMENT UNIT. THE COST OF PICKING UP, ATTACHING THE SIGNS TO THE TEMPORARY FENCE STAKES AND RETURNING THE SIGNS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TEMPORARY FENCE.

404 PERMIT NOTE

THE PROJECT REQUIRES A U.S. ARMY CORPS OF ENGINEERS (USACE) 404 PERMIT THAT WILL BE SECURED BY THE DEPARTMENT, AS A CONDITION OF THIS PERMIT, THE CONTRACTOR WILL NEED TO SUBMIT AN IN-STREAM WORK PLAN TO THE DEPARTMENT FOR APPROVAL. GUIDELINES ON ACCEPTABLE IN-STREAM AND WETLAND WORK TECHNIQUES CAN BE FOUND ON THE USACE WEBSITE. THE USACE DEFINES AND DETERMINES IN-STREAM WORK. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT AND IN-STREAM WORK PLAN WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED WITH THE EXCEPTION OF COFFERDAMS, WHICH WILL BE PAID FOR AS COFFERDAM (TYPE 1) (IN-STREAM / WETLAND WORK) WITH A BASIS OF PAYMENT OF EACH

GENERAL NOTES CONTINUED

- IF THIS CONTRACT REQUIRES THE SERVICES OF AN ELECTRICAL CONTRACTOR, THE CONTRACTOR SHALL BE RESPONSIBLE AT HIS/HER OWN EXPENSE FOR LOCATING EXISTING IDOT ELECTRICAL FACILITIES PRIOR TO PERFORMING ANY WORK. IF THIS CONTRACT DOES NOT REQUIRE THE SERVICES OF AN ELECTRICAL CONTRACTOR, THE CONTRACTOR MAY REQUEST ONE FREE LOCATE FOR EXISTING IDOT ELECTRICAL FACILITIES FROM THE DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR PRIOR TO THE START OF ANY WORK. ADDITIONAL REQUESTS MAY BE AT THE EXPENSE OF THE CONTRACTOR. THE LOCATION OF UNDERGROUND TRAFFIC FACILITIES DOES NOT RELIEVE THE CONTRACTOR OF THEIR RESPONSIBILITY TO REPAIR ANY FACILITIES DAMAGED DURING CONSTRUCTION AT THEIR EXPENSE.
- THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL FOLLIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
- RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.
- THE SEEDING DATES FOR BARE EARTH SEEDING OF MIXTURE CLASS 4B (MODIFIED) AND 5B (MODIFIED) SHALL BE FROM NOVEMBER 15 TO MARCH 15. ALL SEEDING NOT SOWN ACCORDING TO THE SPECIFIED SEASONAL DATE SHALL REQUIRE PRIOR WRITTEN APPROVAL FROM THE ENGINEER. FAILURE TO SECURE SUCH APPROVAL SHALL RESULT IN THE REJECTION OF THE SEEDING AND REPLACEMENT BY THE CONTRACTOR AT
- THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE PRESERVATION OF EXISTING TREES IS OF UTMOST IMPORTANCE TO THE VILLAGE OF WILLOWBROOK AND THE FOREST PRESERVE OF DUPAGE COUNTY.
- EXISTING VEGETATED AREAS (TREES, SHRUBS, VEGETATIVE BUFFERS, TURF AREAS, ETC.) WHERE DISTURBANCE IS NOT OCCURRING (INCLUDING AREAS OUTSIDE THE PROJECT LIMITS) SHALL NOT BE DISTURBED TO ENSURE THAT EXISTING VEGETATION IS PRESERVED TO MINIMIZE SOIL EROSION AND TO ELIMINATE SOIL COMPACTION. NO MATERIALS ARE TO BE STORED OR VEHICLES DRIVEN OR PARKED WITHIN THESE UNDISTURBED AREAS AT ANY TIME.
- THE CONTRACTOR WILL CONTACT THE ROADSIDE DEVELOPMENT UNIT IN THE BUREAU OF MAINTENANCE AT 847.705.4171 AT LEAST 7 DAYS PRIOR TO PLANTING FOR LAYOUT APPROVAL OF THE NATIVE SEEDING, TREES AND SHRUBS
- IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT THE ROADSIDE DEVELOPMENT UNIT AT 847.705.4171 TO SCHEDULE A WALK THROUGH TO DETERMINE TREE PROTECTION, TREE REMOVAL, SELECTIVE CLEARING, AND OTHER FORESTRY WORK A MINIMUM OF 7 DAYS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. ALL TREE PROTECTION, TREE REMOVAL, SELECTIVE CLEARING, PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ROADSIDE DEVELOPMENT UNIT
- THE CONTRACTOR SHALL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171, AT LEAST 7 DAYS PRIOR TO PLANTING FOR LAYOUT OF THE SEEDING AND PLUGS
- THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS. IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) and USE/WASTE REVIEW(BDE 2290) SUBMITTALS, THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND INSECTION II.G.1 AND 2 of the SWPPP. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED INTHE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED
- THE AGGREGATE GRADATION FOR THE AGGREGATE SUBGRADE IMPROVEMENT 12" LOWER LIFT SHALL BE CS 1 OR RR
- "WETLANDS NO INTRUSION" SIGNAGE SHOULD ALSO BE PROVIDED AT THE BOUNDARY OF ALL UN-IMPACTED WETLANDS AND/OR WOUS. THE CONTRACTOR CAN BORROW THE SIGNS FROM THE BUREAU OF MAINTENANCE, INCLUDE TEMPORARY FENCING AND NETLAND SIGNAGE WITHIN THE EROSION AND SEDIMENT CONTROL STRATEGY.

JSER NAME = Jammal, Alramahi DESIGNED -REVISED - 03/27/2019 REVISED -DRAWN 04/23/2019 HECKED REVISED PLOT DATE = 2/1/2024 DATE REVISED -01/15/2020

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

INDEX. HIGHWAY STANDARDS & GENERAL NOTES IL. ROUTE 83 (KINGERY HIGHWAY) AT BLUFF ROAD SCALE: NONE SHEET 1 OF 1 SHEETS STA. +

SECTION COUNTY FAP 344 23 TSM 344 DUPAGE 87 2 CONTRACT NO. 62U73

GENERAL NOTES CONTINUED

- 27. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTINGN IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARMS LENGTHS.
- 28. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL JULIE AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO. CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
- 29. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS, THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
- 30. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.

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| F.A.P. RTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
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| 344 | FAP 344 23 TSM | DUPAGE | 87 | 3 | |
| | | CONTRACT NO. 62U73 | | | |
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| | NO. | IIEM | UNII | QUANTITY | 90% FED 10% STATE | 90% FED 5% STATE 5% DOWNERS GROVE TWP | 90% FED 10% STATE | FIRE PROTECTION DISTRICT | 90% FED 10% STATE |
|-----|-------------|--|--------|----------|----------------------|--|----------------------|--------------------------|----------------------|
| L | | | | | | | | | |
| 1 | 20101000 | TEMPORARY FENCE | FOOT | 186 | 186 | | | | |
| L | | | | | | | | | |
| | 20200100 | EARTH EXCAVATION | CU YD | 54 | 54 | | | | |
| | | | | | | | | | |
| L | 21101615 | TOPSOIL FURNISH AND PLACE, 4" | SQ YD | 437 | 437 | | | | |
| | | | | | | | | | |
| , | 28000400 | PERIMETER EROSION BARRIER | FOOT | 638 | 638 | | | | |
| | | | | | | | | | |
| | 30300112 | AGGREGATE SUBGRADE IMPROVEMENT 12" | SQ YD | 192 | 192 | | | | |
| | | | | | | | | | |
| | 35501316 | HOT-MIX ASPHALT BASE COURSE, 8" | SQ YD | 275 | 275 | | | | |
| | | | | | | | | | |
| | 35600708 | HOT-MIX ASPHALT BASE COURSE WIDENING, 8" | SQ YD | 192 | 192 | | | | |
| | | | | | | | | | |
| | 40600290 | BITUMINOUS MATERIALS (TACK COAT) | POUND | 128 | 128 | | | | |
| | | | | | | | | | |
| | 40604060 | HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 | TON | 56 | 56 | | | | |
| | | | | | | | | | |
| | 44000200 | DRIVEWAY PAVEMENT REMOVAL | SQ YD | 373 | 373 | | | | |
| | | | | | | | | | |
| F | 48102100 | AGGREGATE WEDGE SHOULDER, TYPE B | TON | 6 | 6 | | | | |
| | | | | | | | | | |
| F | 60603800 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 | FOOT | 625 | 625 | | | | |
| | | | | | | | | | |
| _ | 60905305 | BOX CULVERTS TO BE CLEANED | FOOT | 140 | 140 | | | | |
| | | | | | | | | | |
| * | 66900200 | NON-SPECIAL WASTE DISPOSAL | CU YD | 215 | 215 | | | | |
| | | | | | | | | | |
| | 66900530 | SOIL DISPOSAL ANALYSIS | EACH | 2 | 2 | | | | |
| | | | | _ | _ | | | | |
| | 66901001 | REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN | L SUM | 1 | 1 | | | | |
| | | THE SECTION OF THE SE | 200111 | <u>'</u> | | | | | |
| _ | 66901003 | REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT | L SUM | 1 | 1 | | | | |
| - | 0030 1003 | NEOGRIED GODDIANGES FINAL CONSTRUCTION REPORT | L SUM | ' | ' | | | | |
| - | 66001006 | DECLII ATED CUDOTANICES MONITODINIC | CALDA | 6 | | | | | |
| * | 66901006 | REGULATED SUBSTANCES MONITORING | CAL DA | 6 | 6 | | | | |
| - | 07400400 | MODILIZATION | 1 0000 | | | | | | |
| - | 67100100 | MOBILIZATION | L SUM | 1 | 1 | | | | |
| - | 70103815 | TRAFFIC CONTROL SURVEILANCE | CAL DA | 30 | 30 | | | | |
| L | 70107025 | CHANGEABLE MESSAGE SIGN | CAL DA | 30 | 30 | | | | |
| * ; | SPECIAL ITE | M | | | | | | | |

ROADWAY

0004

TOTAL

UNIT

SIGNALS

0021

CODE

ITEM

SCALE:

EVP

0021

100% TRI-STATE

LIGHTING

0021

INTERCONNECT

0021

90% FED

ROADWAY

0004

TOTAL

UNIT

SIGNALS

0021

* SPECIAL ITEM

CODE

ITEM

INTERCONNECT

0021

90% FED

EVP

0021

100% TRI-STATE

LIGHTING

| + 0050141 175 | |
|---------------|---|
| * SPECIAL ITE | M |

| USER NAME - Jammal.Alramani | DESIGNED - | KEVISED - |
|---------------------------------|------------|-----------|
| | DRAWN - | REVISED - |
| PLOT SCALE = 0.16666633 ' / in. | CHECKED - | REVISED - |
| PLOT DATE = 2/1/2024 | DATE - | REVISED - |

INTERCONNECT

EVP

| hi@illinois.gov\d0868327\P120117-sht-SOQ.dgn | |
|--|--|
| ammal.alrama | |
| work\pwidot\ja | |
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| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | ROADWAY 0004 90% FED 10% STATE | SIGNALS 0021 90% FED 5% STATE 5% DOWNERS GROVE TWP | LIGHTING 0021 90% FED 10% STATE | EVP 0021 100% TRI-STATE FIRE PROTECTION DISTRICT | INTERCONNEC 0021 90% FED 10% STATE |
|-------------|---|------|-------------------|---|---|--|--|---|
| | | | | | | | | |
| 87800100 | CONCRETE FOUNDATION, TYPE A | FOOT | 20 | | 20 | | | |
| 87800150 | CONCRETE FOUNDATION, TYPE C | FOOT | 4 | | 4 | | | |
| | , | | | | | | | |
| 87800415 | CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER | FOOT | 48 | | 48 | | | |
| 88030020 | SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED | EACH | 12 | | 12 | | | |
| | | | | | | | | |
| 88030050 | SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 9 | | 9 | | | |
| 88200510 | TRAFFIC SIGNAL BACKPLATE, RETROREFLECTIVE | EACH | 12 | | 12 | | | |
| 66200310 | TRAFFIC SIGNAL BACKFLATE, RETROREFLECTIVE | EACH | 12 | | 12 | | | |
| 88500100 | INDUCTIVE LOOP DETECTOR | EACH | 6 | | 6 | | | |
| | | | | | | | | |
| 88600100 | DETECTOR LOOP, TYPE I | FOOT | 244 | | 244 | | _ | |
| 88700200 | LIGHT DETECTOR | EACH | 3 | | | | 3 | |
| 88700300 | LIGHT DETECTOR, AMPLIFIER | EACH | 1 | | | | 1 | |
| 89000100 | TEMPORARY TRAFFIC SIGNAL INSTALLATION | EACH | 1 | | 1 | | | |
| | | | | | | | | |
| 89502300 | REMOVE ELECTRIC CABLE FROM CONDUIT | FOOT | 5500 | | | | | 5500 |
| | | | | | | | | |
| 89502375 | REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT | EACH | 1 | | 1 | | | |
| 89502380 | REMOVE EXISTING HANDHOLE | EACH | 10 | | 10 | | | |
| | | | | | | | | |
| 89502382 | REMOVE EXISTING DOUBLE HANDHOLE | EACH | 1 | | 1 | | | |
| 89502385 | REMOVE EXISTING CONCRETE FOUNDATION | EACH | 9 | | 9 | | | |
| X0324085 | EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C | FOOT | 516 | | | | 516 | |
| | | | | | | | | |
| X0324599 | ROD AND CLEAN EXISTING CONDUIT | FOOT | 5000 | | | | | 5000 |
| X5021512 | COFFERDAM (TYPE 1) (IN-STREAMWETLAND WORK) | EACH | 1 | 1 | | | | |
| | | | | | | | | |

DATE -

REVISED -

REVISED -

PLOT SCALE = 0.16666633 ' / in.

PLOT DATE = 2/1/2024

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | SUMMARY OF QUANTITIES | | | | | |
|--|-----------------------|-------|------|--------|------|---------|
| IL. ROUTE 83 (KINGERY HIGHWAY) AT BLUFF ROAD | | | | | | |
| | SCALE: | SHEET | OF 5 | SHEETS | STA. | TO STA. |

F.A. RTE. COUNTY TOTAL SHEET NO.

DUPAGE 87 7 SECTION FAP 344 23 TSM CONTRACT NO. 62U73

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| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | ROADWAY 0004 90% FED 10% STATE | SIGNALS 0021 90% FED 5% STATE 5% DOWNERS GROVE TWP | LIGHTING 0021 90% FED 10% STATE | EVP 0021 100% TRI-STATE FIRE PROTECTION DISTRICT | INTERCONNECT 0021 90% FED 10% STATE |
|-------------|---|---------|-------------------|---|---|--|--|--|
| × X1400108 | FULL-ACTUATED CONTROLLER AND TYPE SUPER R CABINET (SPECIAL) | EACH | 1 | | 1 | | | |
| × X1400150 | SERVICE INSTALLATION, GROUND MOUNTED, METERED | EACH | 1 | | 1 | | | |
| * X1400341 | REMOVAL OF LUMINAIRE, SALVAGE | EACH | 15 | | | 15 | | |
| × X8891009 | VIDEO VEHICLE DETECTION SYSTEM, SINGLE APPROACH | EACH | 2 | | 2 | | | |
| | | | | | - | | | |
| X2020110 | GRADING AND SHAPING SHOULDERS | UNIT | 2 | 2 | | | | |
| X6700407 | ENGINEER'S FIELD OFFICE, TYPE A (D1) | CAL MO | 6 | 6 | | | | |
| X7010216 | TRAFFIC CONTROL AND PROTECTION, (SPECIAL) | L SUM | 1 | 1 | | | | |
| X8100105 | CONDUIT SPLICE | EACH | 1 | | | | | 1 |
| * X8211008 | TEMPORARY LUMINAIRE, LED, ROADWAY, OUTPUT DESIGNATION H | EACH | 2 | | | 2 | | |
| X8250091 | COMBINATION LIGHTING CONTROLLER | EACH | 1 | | 1 | | | |
| * X8620200 | UNINTERRUPTABLE POWER SUPPLY (SPECIAL) | EACH | 1 | | 1 | | | |
| A0020200 | UNIVERSEL TOWER SUITER (OF ECIAL) | LAGIT | ' | | ' | | | |
| X8710024 | FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F | FOOT | 5500 | | | | | 5500 |
| X8772115 | TEMPORARY MAST ARM, ALUMINUM, 15FT | EACH | 2 | | | 2 | | |
| * X8809005 | LED SIGNAL FACE, LENS COVER | EACH | 21 | | 21 | | | |
| Z0013798 | CONSTRUCTION LAYOUT | L SUM | 1 | 1 | | | | |
| * Z0030850 | TEMPORARY INFORMATION SIGNING | SQ FT | 154.2 | 154.2 | | | | |
| * Z0033020 | LUMINAIRE SAFETY CABLE ASSEMBLY | EACH | 16 | | | 16 | | |
| 7000000 | MAINTENANCE OF LIGHTING OVERTEN | 211.115 | | | | _ | | |
| Z0033028 | MAINTENANCE OF LIGHTING SYSTEM | CAL MO | 6 | | | 6 | | |
| * Z0033046 | RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2 | EACH | 1 | | | | | 1 |
| * Z0073510 | TEMPORARY TRAFFIC SIGNAL TIMING | EACH | 1 | | | | | 1 |

* SPECIAL ITEM

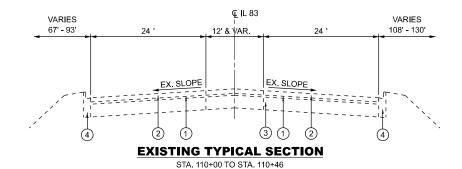
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

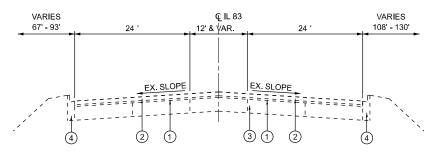
SUMMARY OF QUANTITIES

IL. ROUTE 83 (KINGERY HIGHWAY) AT BLUFF ROAD

SHEET OF 5 SHEETS STA. TO STA.

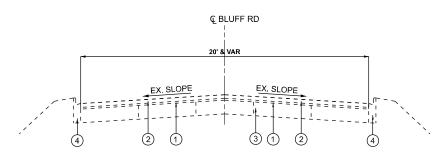
SCALE:





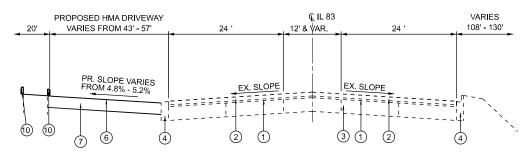
EXISTING TYPICAL SECTION

STA. 111+00 TO STA. 111+48



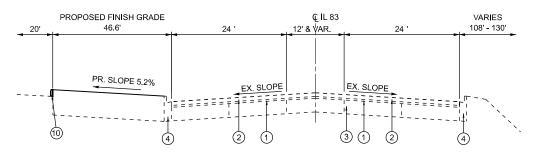
EXISTING TYPICAL SECTION

STA. 19+00 TO STA. 21+00



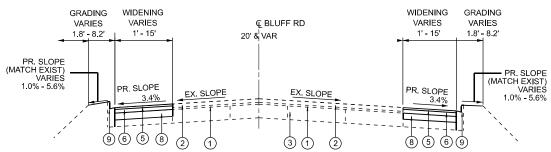
PROPOSED TYPICAL SECTION

STA. 110+00 TO STA. 110+46



PROPOSED TYPICAL SECTION

STA. 111+00 TO STA. 111+48



LEGEND

- 1 EXISTING +/-1" POLYMERIZED LEVELING BINDER
- ② EXISTING +/-2" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE

SCALE:

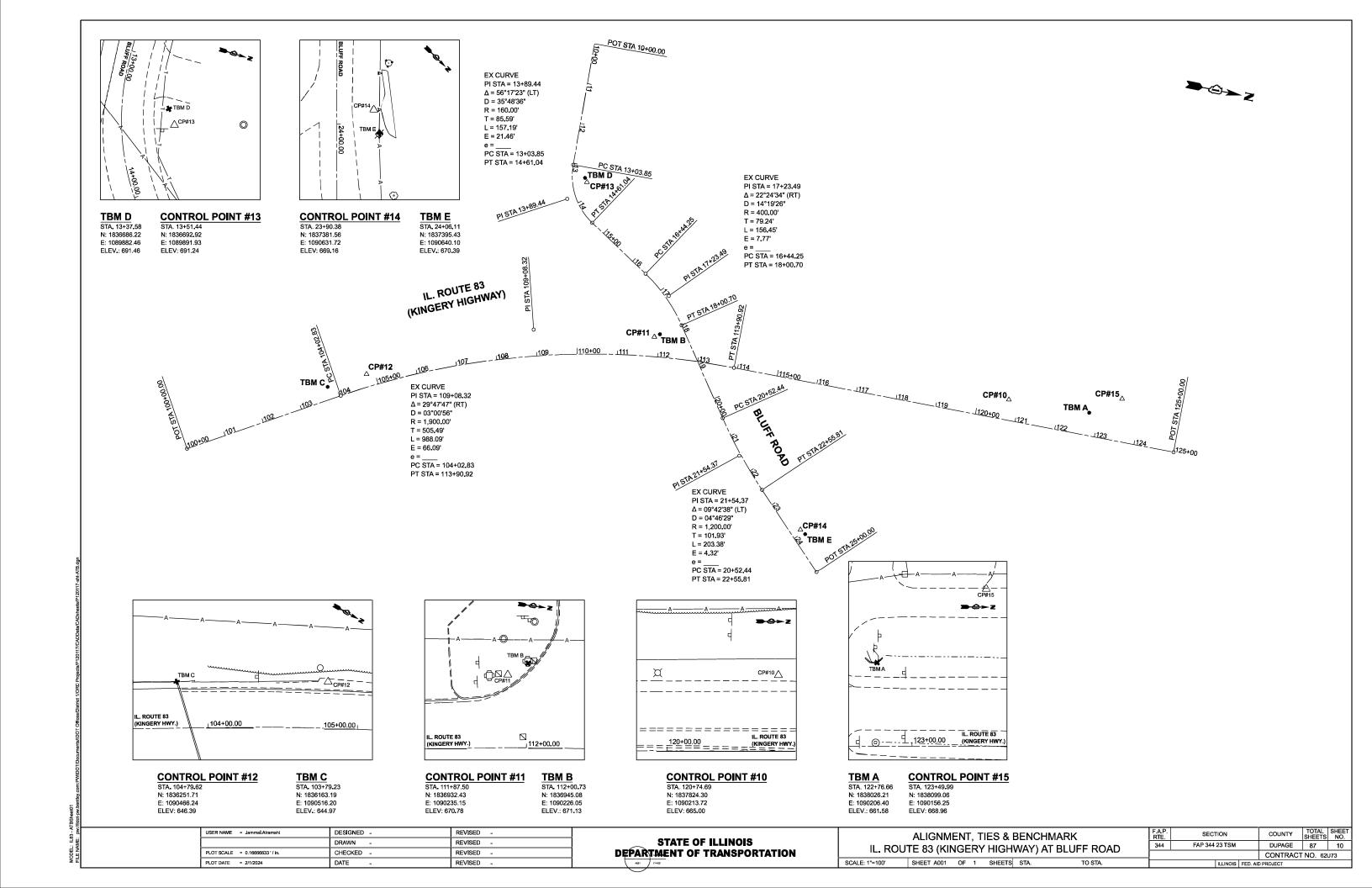
- ③ EXISTING +/-10" P.C.C. BASE
- 4 EXISTING B-6.24 CURB AND GUTTER
- (5) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2"
- 6 HOT-MIX ASPHALT BASE COURSE WIDENING, 8"
- 7 HOT-MIX ASPHALT BASE COURSE, 8"
- 8 AGGREGATE SUBGRADE IMPROVEMENT , 12"
- 9 PROPOSED B-6.24 CURB AND GUTTER
- 10 PROPOSED B-6 CURB

| PROPOSED | TVDICA | SECTION |
|----------|--------|-----------|
| PROPUSED | ITPICA | L SECTION |

STA. 19+00 TO STA. 21+00

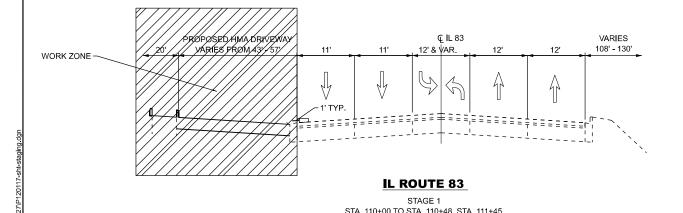
| USER NAME = Jammal.Alramahi | DESIGNED - | REVISED - |
|---------------------------------|------------|-----------|
| | DRAWN - | REVISED - |
| PLOT SCALE = 0.16666633 ' / in. | CHECKED - | REVISED - |
| PLOT DATE = 2/1/2024 | DATE - | REVISED - |

| E | EXISTING AND PROPOSED TYPICAL SECTIONS | | | | F.APP RTE | SEC ⁻ | TION | | COUNTY | TOTAL SHEETS | SHEET NO. | |
|---------------------------------------|--|----|--------|------|----------------|------------------|------|----------|----------|-----------------|--------------|--|
| IL 83 (KINGERY HIGHWAY) AT BLUFF ROAD | | | | 344 | FAP 344 23 TSM | | | DUPAGE | 87 | 9 | | |
| | IL 03 (NINGERT HIGHWAT) AT BEOFF ROAD | | | | | | | | CONTRACT | NO. 62 | J73 | |
| | SHEET | OF | SHEETS | STA. | TO STA. | | | ILLINOIS | FED. AII | PROJECT | | |



MAINTENANCE OF TRAFFIC GENERAL NOTES

- 1. TEMPORARY INFORMATION SIGNS ON TEMPORARY SUPPORTS SHALL BE PROVIDED FOR ALL COMMERCIAL DRIVEWAYS THAT ARE LOCATED WITHIN A WORK AREA. THIS WORK SHALL BE PAID FOR PER DISTRICT 1 DETIAL TC-26. THESE SIGNS SHALL BE RELOCATED AS REQUIRED FOR EACH CONSTRUCTION STAGE AND SHALL BE PLACED AS DIRECTED BY THE ENGINEER. THIS SIGN RELOCATION WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR TEMPORARY INFORMATION SIGNING.
- 2. THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- 3. CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED TWO WEEKS PRIOR TO ALL ROAD CLOSURE, TRAFFIC STAGE CHANGES AND NEW TRAFFIC SIGNAL TURN-ON EVENTS ON EACH APPROACH OF THE EFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGNS SHALL BE REMOVED TWO WEEKS THEREAFTER UNLESS THE SIGNS ARE NEEDED AGAIN FOR A SUBSEQUENT FUTURE EVENT THAT WILL OCCUR WITHIN TWO WEEKS ON THE SAME APPROACH OF THE EFFECTED ROADWAY. THE SIGN LOCATIONS SHALL BE (DETERMINED BY THE ENGINEER) PLACED AS DIRECTED BY THE ENGINEER.
- 4. THE "ROAD CONSTRUCTION AHEAD" SIGN SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN THE WORK ZONE.
- 5. WHEN WORKING ADJACENT TO THE ROAD AND UTILIZING DAILY LANE CLOSURES, DROP-OFFS ADJACENT TO THE TRAVEL LANE SHALL BE KEPT TO A MINIMUM. DROP-OFFS GREATER THAN (OR GREATER THAN TO EQUAL TO) 12" WILL NOT BE ALLOWED AT LOCATIONS WHERE THE DROP-OFF IS LOCATED WITHIN 8 FT OF THE EDGE OF THE TRAVEL LANE. THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE EXCAVATION REQURIED FOR THE CONSTRUCTION DURING THE TIME THAT THE ADJACENT LANE IS CLOSED. AS NOTED ABOVE, PRIOR TO REOPENING THE LANE TO TRAFFIC THE CONTRACTOR SHALL PLACE SUFFICIENT MATERIAL TO REDUCE THE DROP-OFF TO LESS THAN 12". THE CONTRACTOR SHALL BE RESPONSIBLE TO DETERMINE THE AMOUNT OF WORK THAN CAN BE COMPLETED WITHIN THE TIME OF THE DAILY LANE CLOSRE. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED TO COMPLY WITH THIS REQUIREMENT.



SUGGESTED SEQUENCE OF CONSTRUCTION

STAGE 1

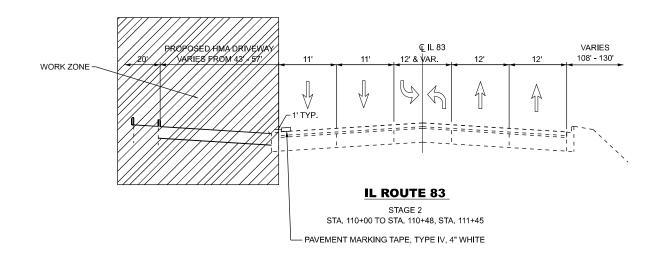
TEMPORARY LANE CLOSURES SHALL BE UTILIZED WHEN REQUIRED DURING WORK HOURS. WORK IN STAGE 1 SHALL BE THE RECONSTRUCTION OF THE EXIT DRIVEWAY FOR THE GAS STATION LOCATED ON THE SOUTHWEST CORNER OF THE INTERSECTION. THE ONLY ENTRANCE TO THE GAS STATION WILL BE THE DRIVEWAY LOCATED ON BLUFF ROAD WHILE THE EXITING ENTRANCE ON ILLINOIS ROUTE 83 SHALL BE USED AS A TEMPORARY EXIT.

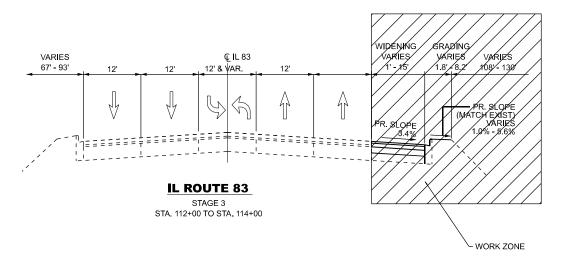
STAGE 2

STAGE 2 SHALL MAINTAIN THE SAME CONFIGURATION AS STAGE 1. THE ONLY DIFFERENCE SHALL BE THE LOCATION OF THE WORK ZONE. IN STAGE 2 THE NEW WORK ZONE WILL BE AT THE GAS STATION'S ENTRANCE DRIVEWAY ON ILLINOIS ROUTE 83 AND EXTENDING INTO THE FIRST LANE OF TRAFFIC UTILIZING DAILY LANE CLOSURES, TRAFFIC ENTERING THE GAS STATION WILL STILL USE THE ENTRANCE ON BLUFF ROAD, BUT WILL NOW EXIT THROUGH THE NEW RECONSTRUCTED DRIVEWAY THAT WAS COMPLETED IN STAGE 1 AND LEADS SOUTHBOUND ON ILLINOIS ROUTE 83.

STAGE 3

REDUCE THE LANES ON THE EAST LEG OF BLUFF ROAD TO 10' TO WIDEN THE NORTHEAST AND SOUTHEAST RADIUSES.





 USER NAME
 = Jammal.Airamahi
 DESIGNED
 REVISED
 03/27/2019

 DRAWN
 REVISED
 04/23/2019

 PLOT SCALE
 = 0.166666831 / ln.
 CHECKED
 REVISED
 08/15/2019

 PLOT DATE
 = 2/1/2024
 DATE
 REVISED
 01/15/2020

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING GENERAL NOTES

IL. ROUTE 83 (KINGERY HIGHWAY) AT BLUFF ROAD

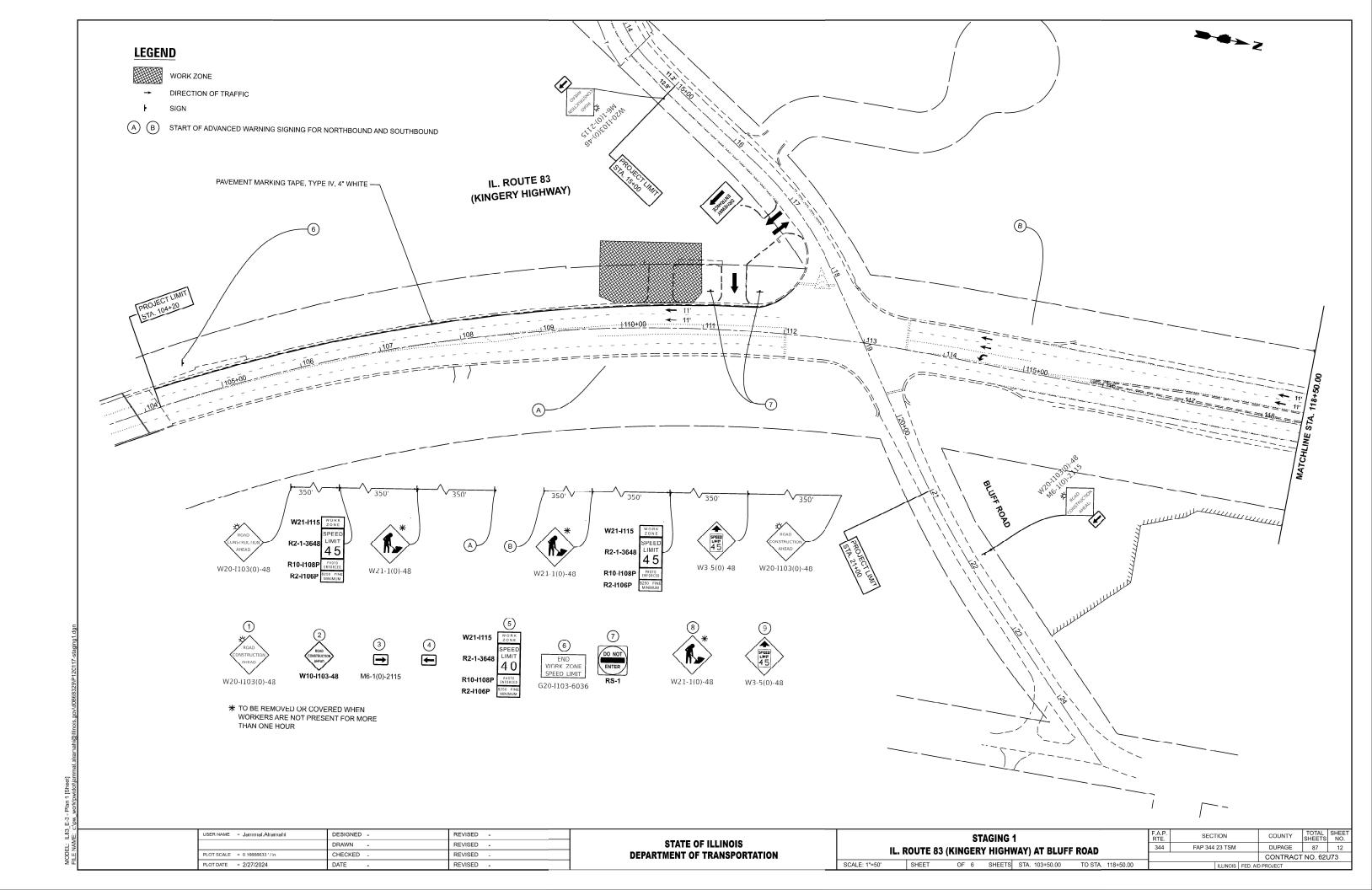
SHEET 1 OF 1 SHEETS STA. TO STA.

STAGING GENERAL NOTES

RTE. SECTION COUNTY SHEETS NO.

344 FAP 344 23 TSM DUPAGE 87 11

CONTRACT NO. 62U73



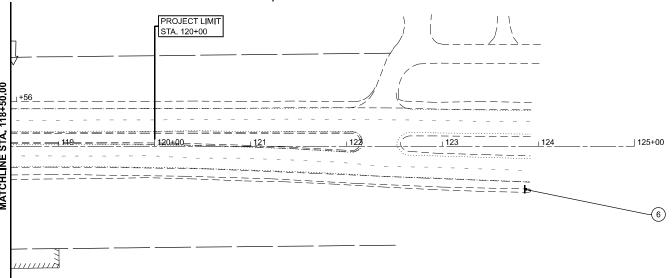
LEGEND



→ DIRECTION OF TRAFFIC

SIGN

IL. ROUTE 83 (KINGERY HIGHWAY)



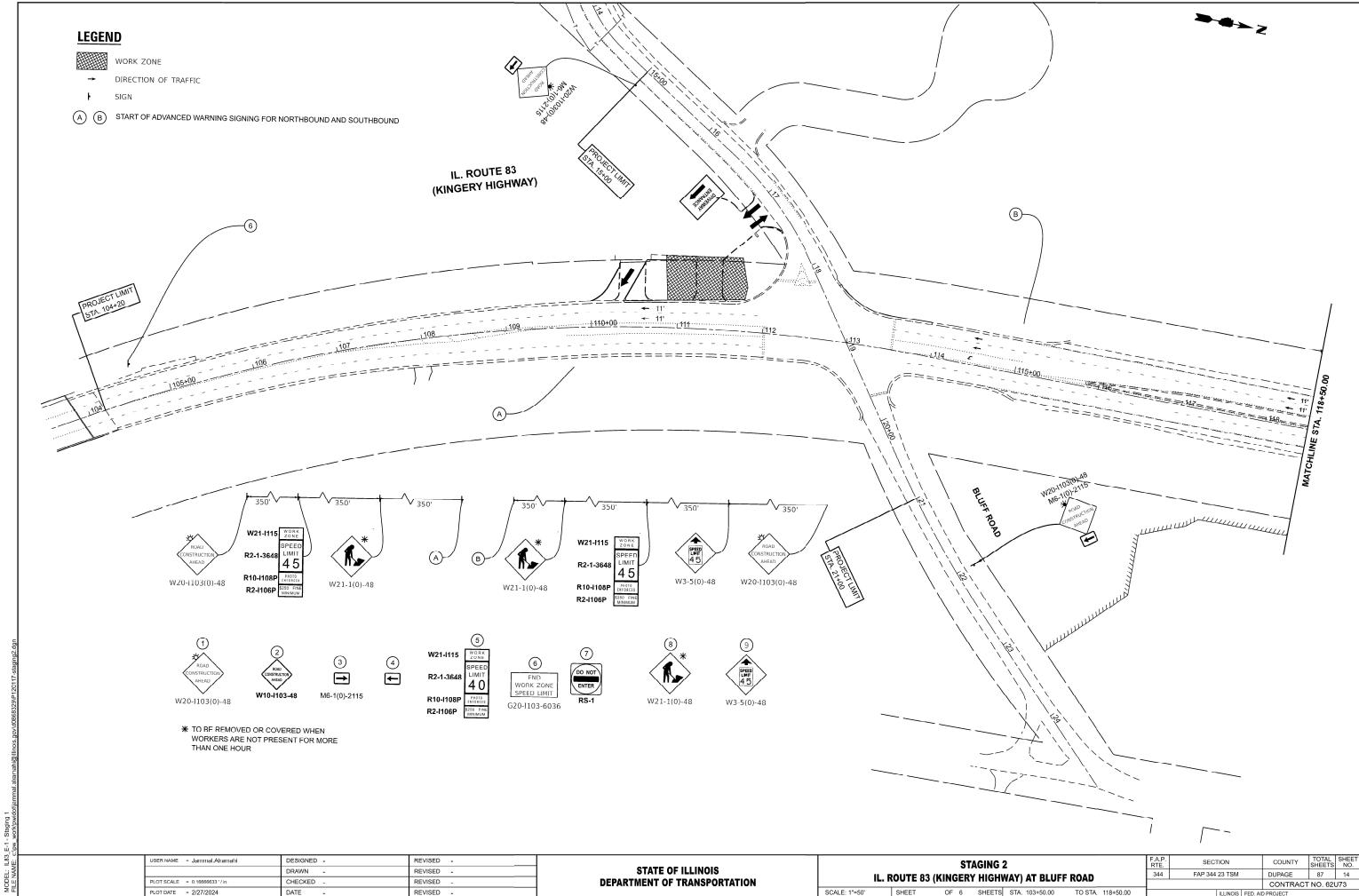
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| USER NAME = Jammal.Alramahi | DESIGNED - | REVISED - |
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| | DRAWN - | REVISED - |
| PLOT SCALE = 0.16666633 ' / in. | CHECKED - | REVISED - |
| PLOT DATE = 1/30/2024 | DATE - | REVISED - |

| STATE OF ILLINOIS |
|------------------------------|
| DEPARTMENT OF TRANSPORTATION |

| | | ST | 'AGING | 1 | | F.A.P. RTE | SEC- |
|---------------|-------------|----------|---------|----------------|-------------------|---------------|---------|
| | I ROUTE 83 | (KINGER | A HIGHA | NAY) AT BLUF | F ROAD | 344 | FAP 344 |
| | EI KOOTE 03 | (MINOLIN | | IAI, AI DEGI | I NOAD | | |
| SCALE: 1"=50' | SHEET | OF 6 | SHEETS | STA. 118+50.00 | TO STA. 133+50.00 | | |

| RTE. | SEC | TION | | COUNTY | SHEETS | NO. |
|------|---------|----------|----------|-----------|--------|-----|
| 344 | FAP 344 | 23 TSM | | DUPAGE | 87 | 13 |
| | | CONTRACT | NO. 621 | J73 | | |
| | | ILLINOIS | FED, AII | D PROJECT | | |



OF 6 SHEETS STA. 103+50.00 TO STA. 118+50.00

LEGEND

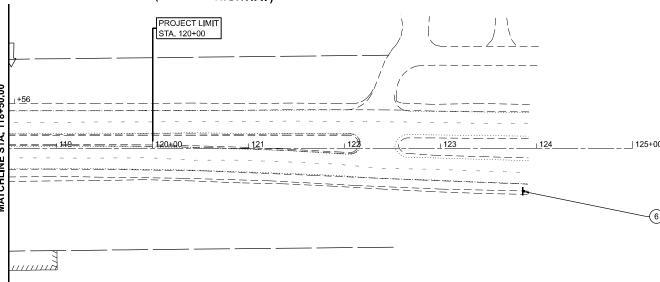


WORK ZONE

→ DIRECTION OF TRAFFIC

SIGN

IL. ROUTE 83 (KINGERY HIGHWAY)



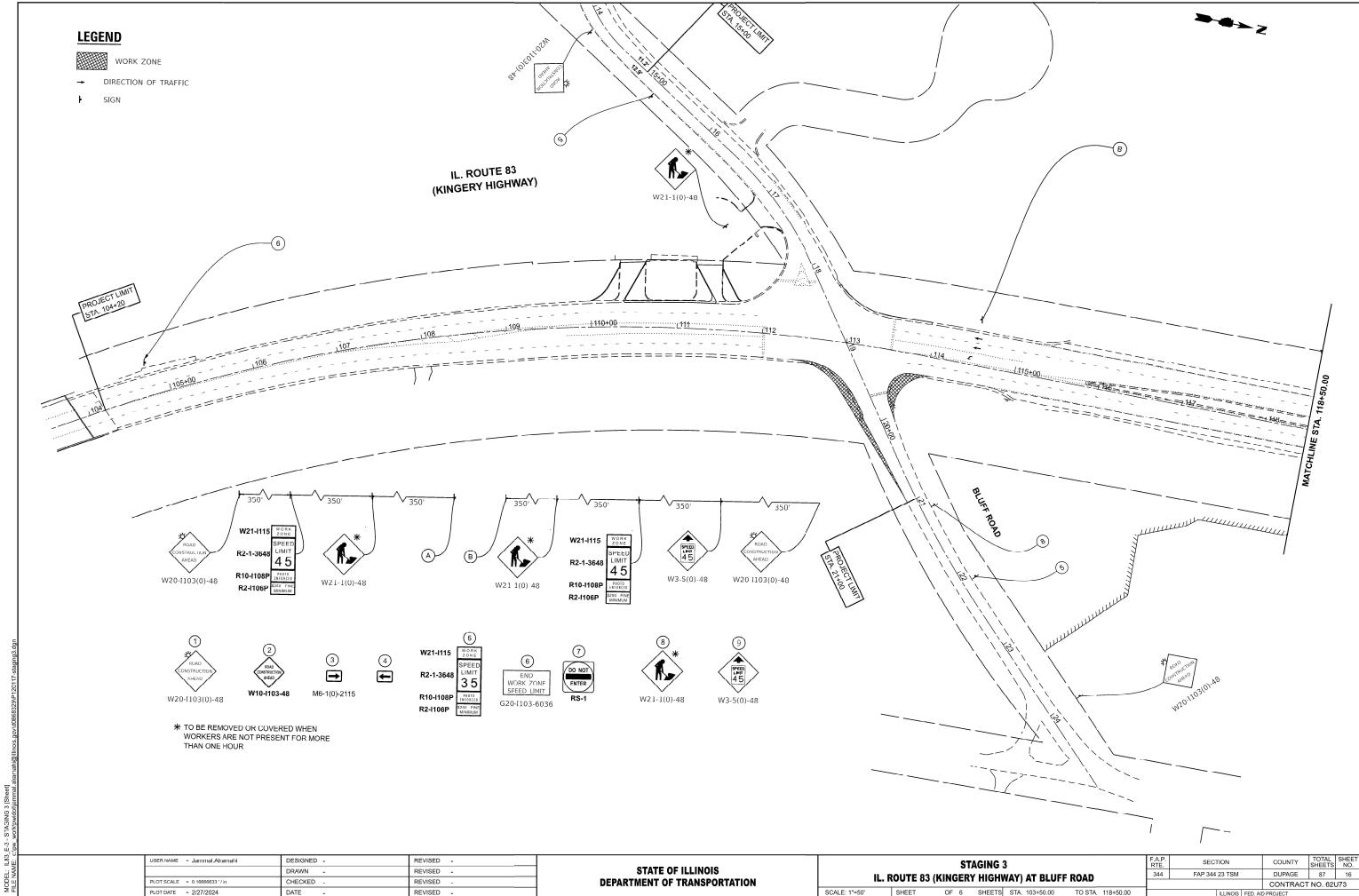
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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

STAGING 2

IL. ROUTE 83 (KINGERY HIGHWAY) AT BLUFF ROAD

SCALE: 1"=50" SHEET OF 6 SHEETS STA. 118+50.00 TO STA. 133+50.00



OF 6 SHEETS STA. 103+50.00 TO STA. 118+50.00

LEGEND



WORK ZONE

► DIRECTION OF TRAFFIC

BARRICADE OR DRUM WITH FLASHING LIGHT

SIGN

IL. ROUTE 83 (KINGERY HIGHWAY) PROJECT LIMIT STA. 120+00 IL ROUTE 83 IL ROUTE 83 FINAL 120+00 IL ROUTE 83 FINAL 120+00 EXIST. R.O.W. EXIST. R.O.W.

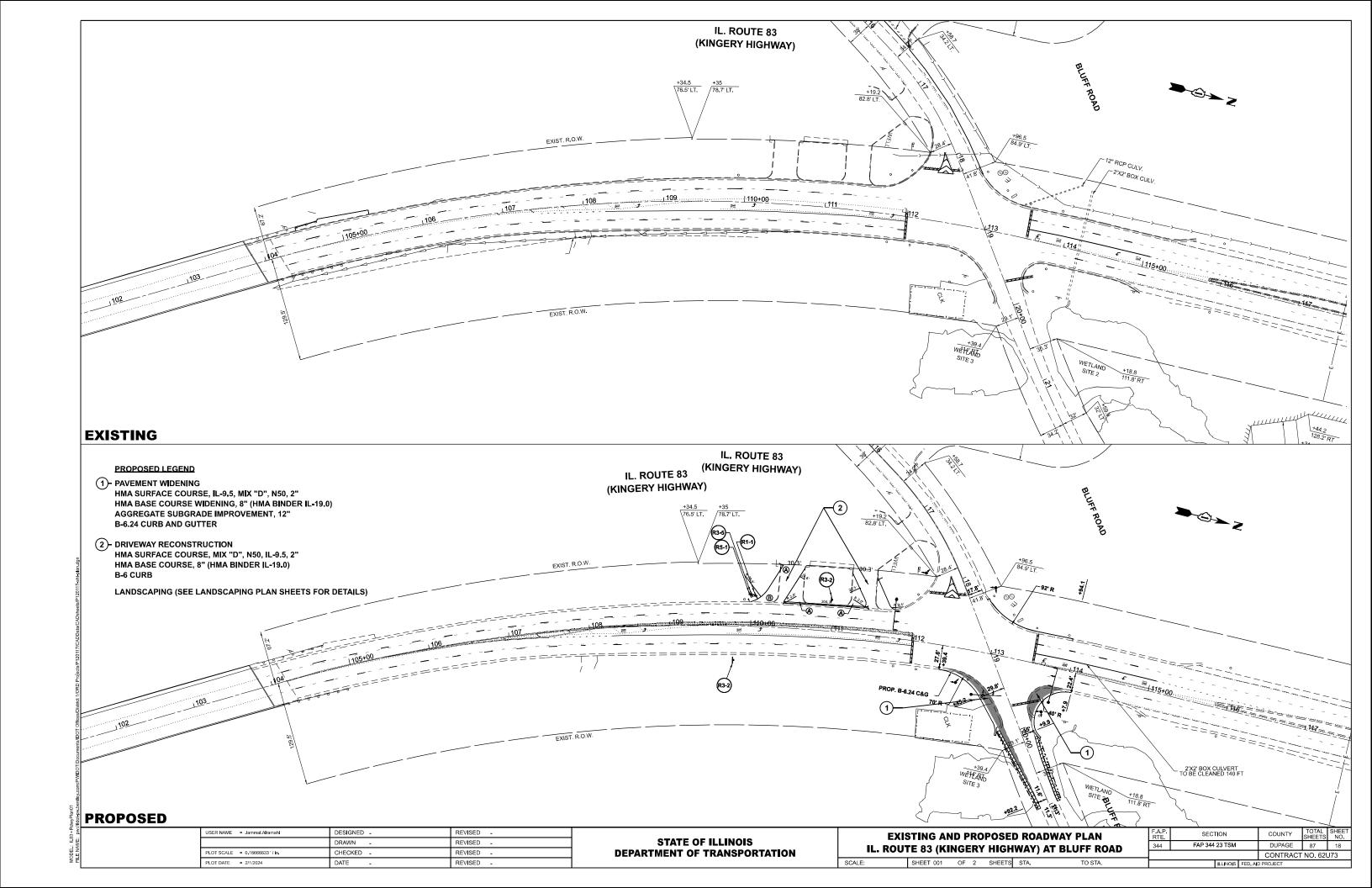
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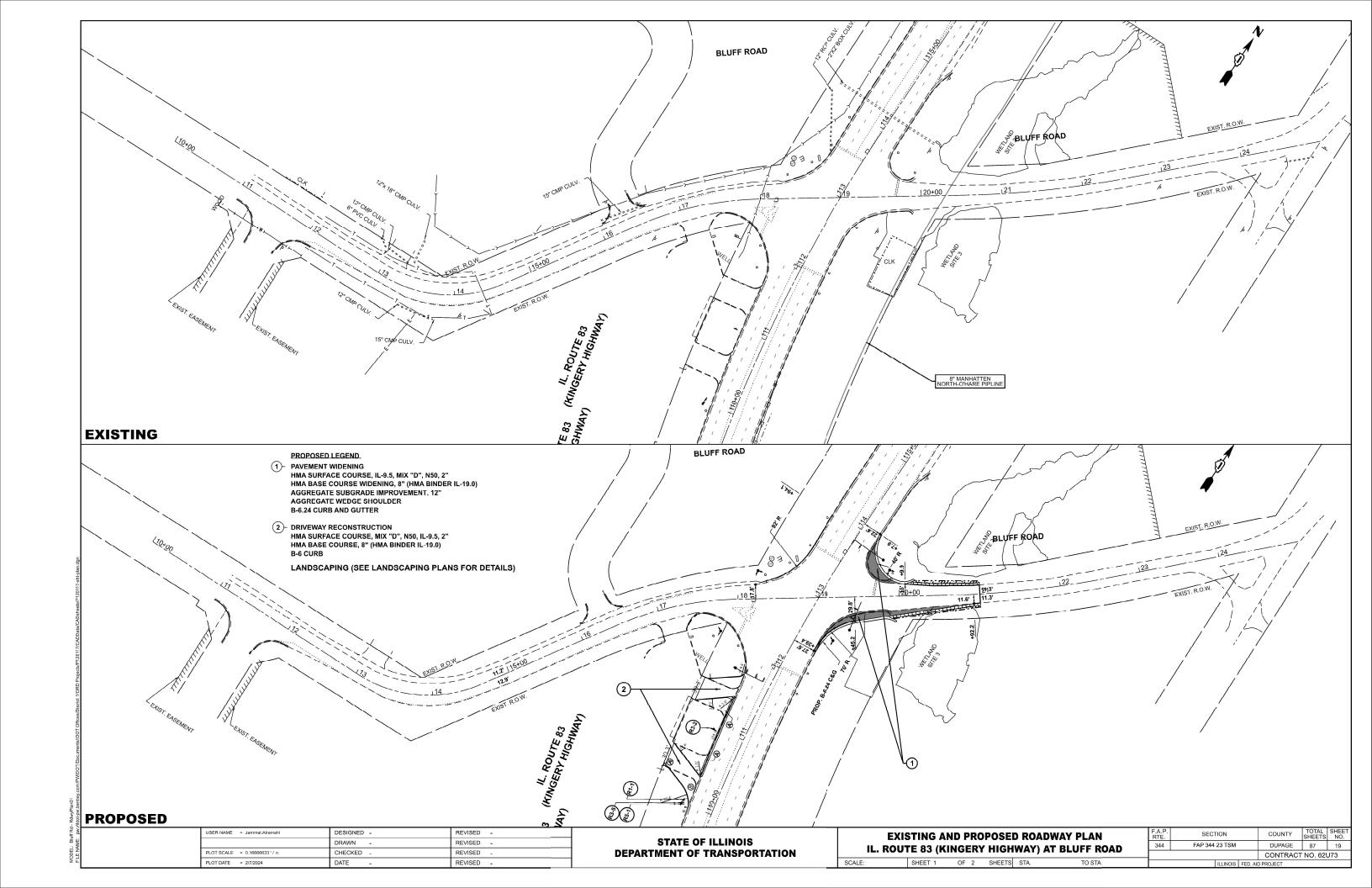
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

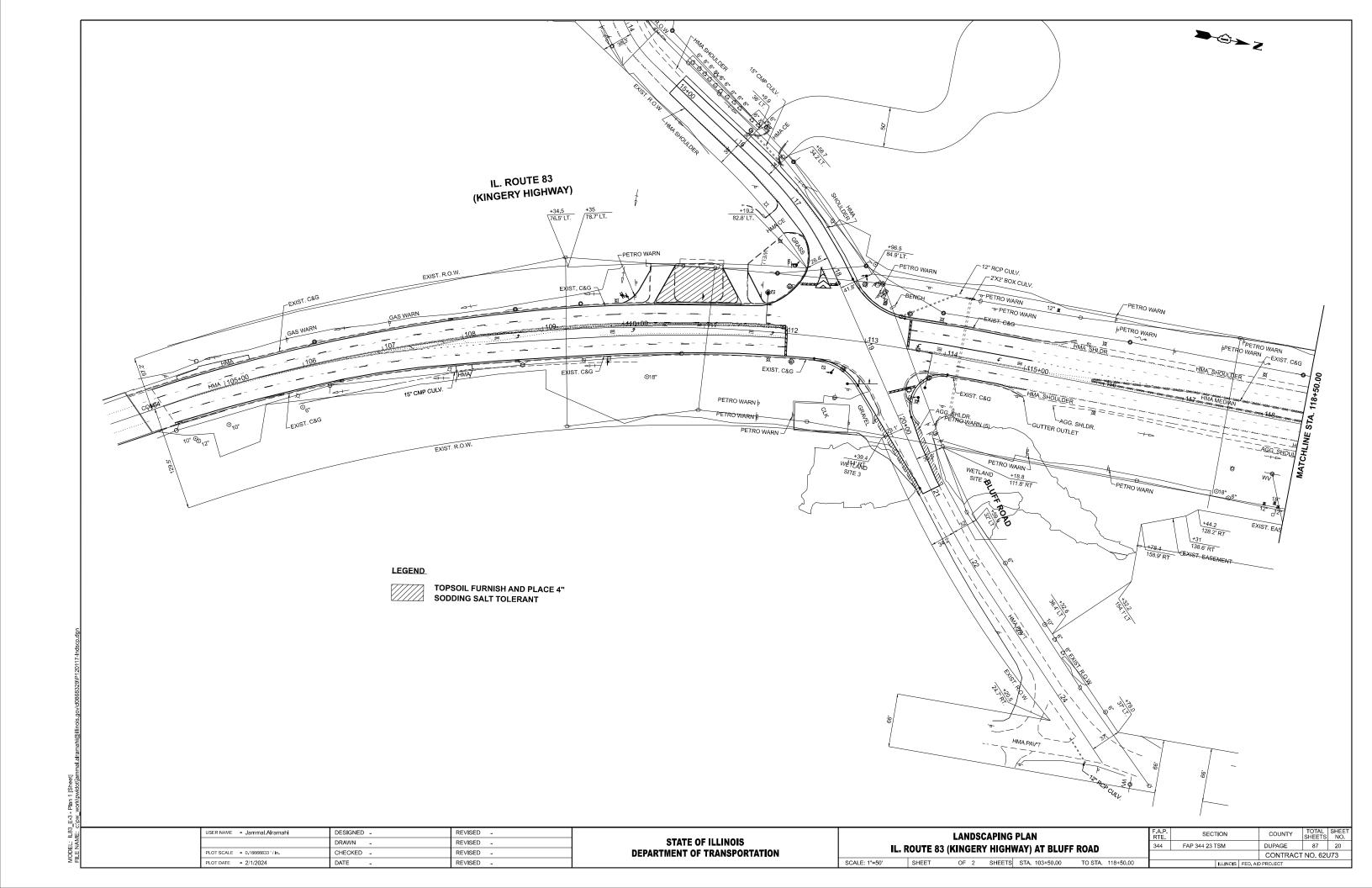
STAGING 3

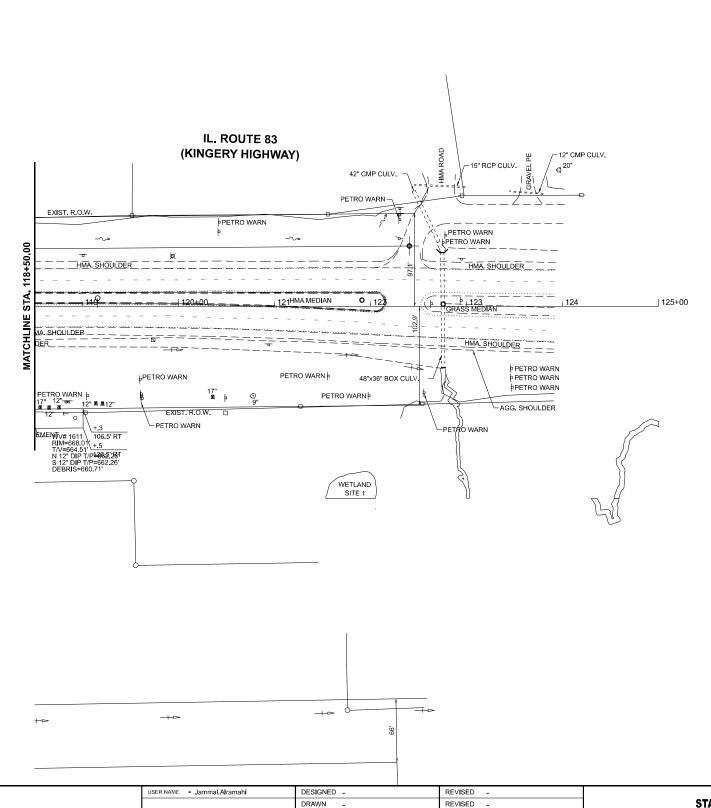
IL. ROUTE 83 (KINGERY HIGHWAY) AT BLUFF ROAD

SCALE: 1"=50" SHEET OF 6 SHEETS STA. 118+50.00 TO STA. 133+50.00









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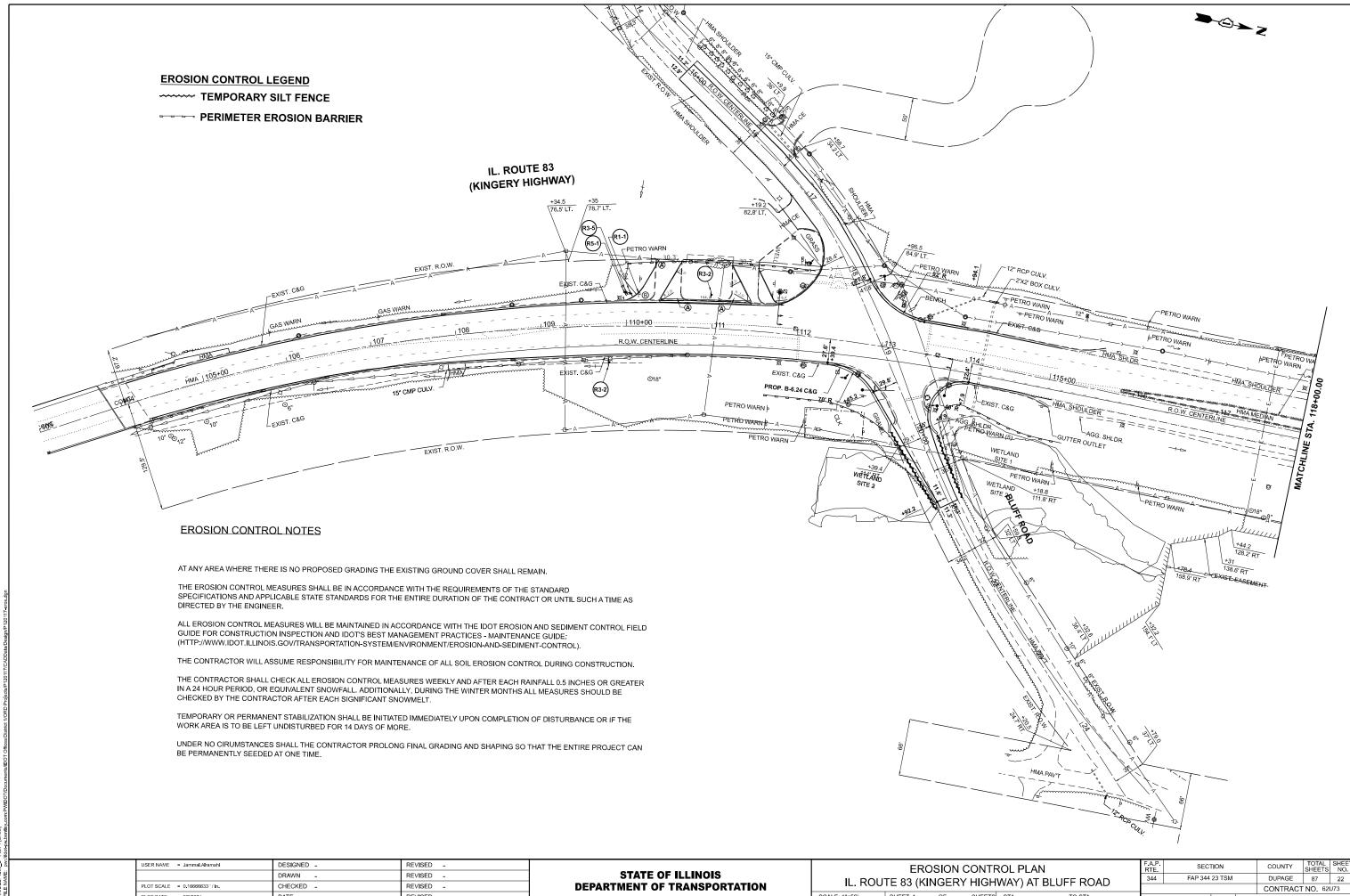
DATE

PLOT DATE = 2/1/2024

REVISED -

REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION



DATE REVISED SCALE: 1"=50" SHEET 1 OF SHEETS STA.



EROSION CONTROL LEGEND TEMPORARY FENCE

EROSION CONTROL NOTES

AT ANY AREA WHERE THERE IS NO PROPOSED GRADING THE EXISTING GROUND COVER SHALL REMAIN.

THE EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE STATE STANDARDS FOR THE ENTIRE DURATION OF THE CONTRACT OR UNTIL SUCH A TIME AS DIRECTED BY THE ENGINEER.

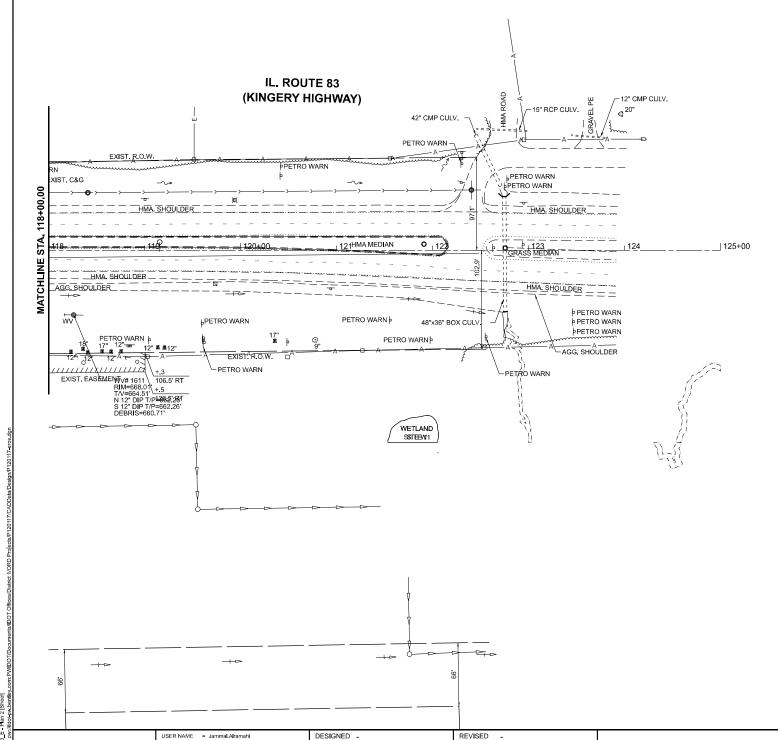
ALL EROSION CONTROL MEASURES WILL BE MAINTAINED IN ACCORDANCE WITH THE IDOT EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION AND IDOT'S BEST MANAGEMENT PRACTICES - MAINTENANCE GUIDE: (HTTP://WWW.IDOT.ILLINOIS.GOV/TRANSPORTATION-SYSTEM/ENVIRONMENT/EROSION-AND-SEDIMENT-CONTROL).

THE CONTRACTOR WILL ASSUME RESPONSIBILITY FOR MAINTENANCE OF ALL SOIL EROSION CONTROL DURING CONSTRUCTION.

THE CONTRACTOR SHALL CHECK ALL EROSION CONTROL MEASURES WEEKLY AND AFTER EACH RAINFALL 0.5 INCHES OR GREATER IN A 24 HOUR PERIOD, OR EQUIVALENT SNOWFALL. ADDITIONALLY, DURING THE WINTER MONTHS ALL MEASURES SHOULD BE CHECKED BY THE CONTRACTOR AFTER EACH SIGNIFICANT SNOWMELT.

TEMPORARY OR PERMANENT STABILIZATION SHALL BE INITIATED IMMEDIATELY UPON COMPLETION OF DISTURBANCE OR IF THE WORK AREA IS TO BE LEFT UNDISTURBED FOR 14 DAYS OF MORE.

UNDER NO CIRUMSTANCES SHALL THE CONTRACTOR PROLONG FINAL GRADING AND SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.



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DATE

CHECKED .

PLOT SCALE = 0.16666633 '/ in.

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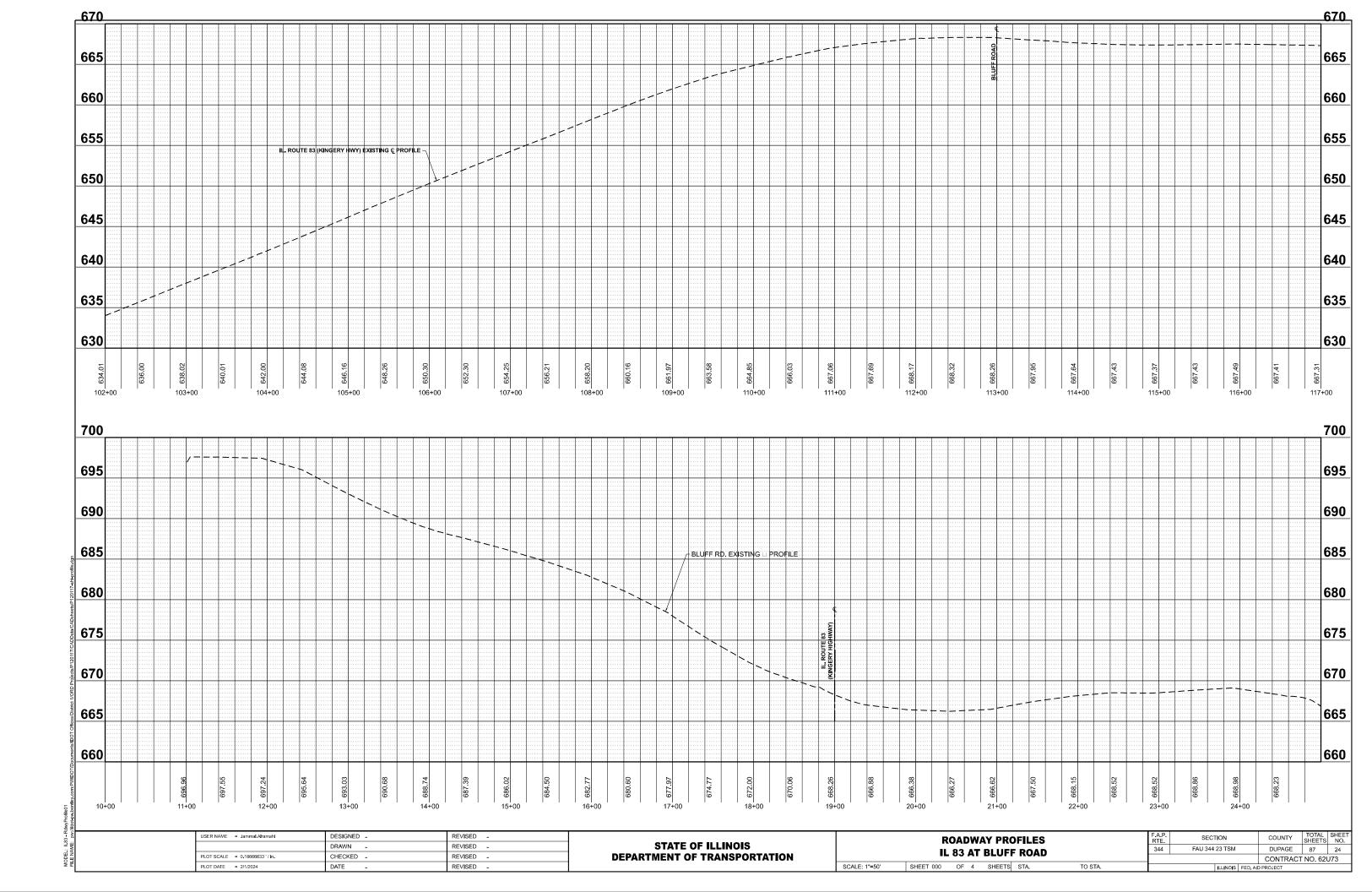
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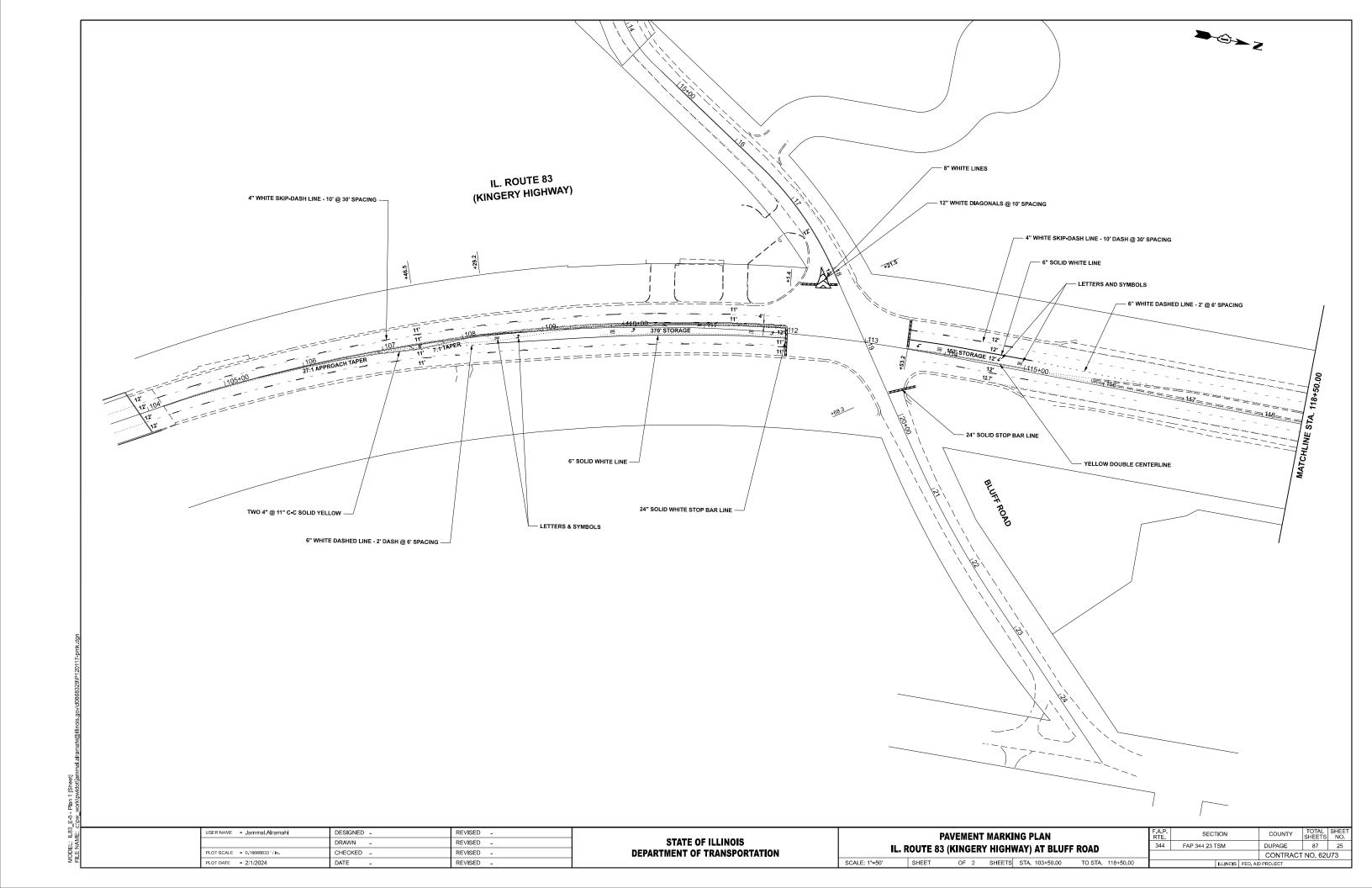
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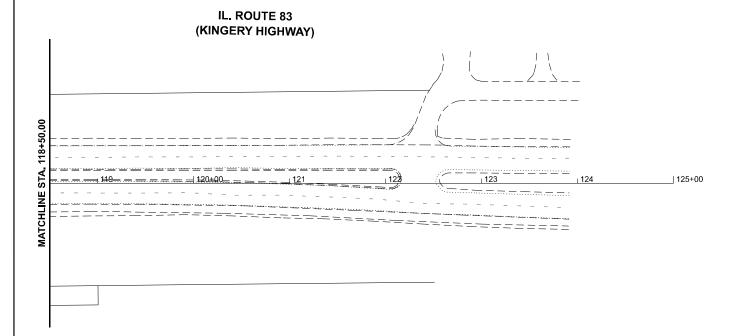
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL PLAN
IL. ROUTE 83 (KINGERY HIGHWAY) AT BLUFF ROAD

SCALE: 1"=50" SHEET 3 OF SHEETS STA. TO STA.







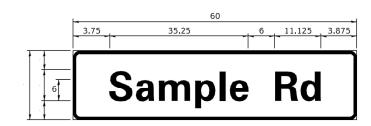
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

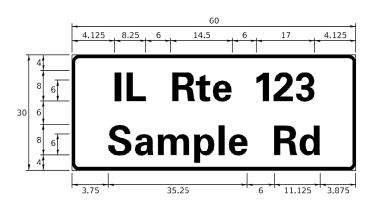
PAVEMENT MARKING PLAN

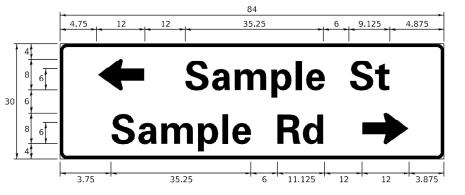
IL. ROUTE 83 (KINGERY HIGHWAY) AT BLUFF ROAD

SCALE: 1"=50" SHEET OF 2 SHEETS STA. 118+50.00 TO STA. 133+50.00

SIGN PANEL - TYPE 1 OR TYPE 2







| DESIGN | AREA | SIGN PANEL | SHEETING | QTY. |
|--------|---------|------------|----------|----------|
| SERIES | (SQ FT) | TYPE | TYPE | REQUIRED |
| D OR C | | 1 OR 2 | ZZ | 1 |

COMMON STREET NAME ABBREVIATIONS AND WIDTHS

| NAME | ABBREVATION | WIDTH (INCH) | | | |
|---------------|-------------|--------------|------------|--|--|
| NAME | ADDREVALION | SERIES "C" | SERIES "D" | | |
| AVENUE | Ave | 15.000 | 18.250 | | |
| BOULEVARD | Blvd | 17. 125 | 20.000 | | |
| CIRCLE | Cir | 11.125 | 13.000 | | |
| COURT | Ct | 8. 250 | 9.625 | | |
| DRIVE | Dr | 8.625 | 10.125 | | |
| HIGHWAY | Hwy | 18. 375 | 22.000 | | |
| ILLINOIS | ΙL | 7. 000 | 8. 250 | | |
| LANE | Ln | 9.125 | 10.750 | | |
| PARKWAY | Pkwy | 23.375 | 27.375 | | |
| PLACE | PΙ | 7. 125 | 7. 750 | | |
| ROAD | Rd | 9. 625 | 11.125 | | |
| ROUTE | Rte | 12.625 | 14.500 | | |
| STREET | St | 8. 000 | 9.125 | | |
| TERRACE | Ter | 12.625 | 14.625 | | |
| TRAIL | Tr | 7. 750 | 9.125 | | |
| UNITED STATES | US | 10.375 | 12.250 | | |

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS. LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- 2. ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ
- 3. THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL, A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.
- 4. A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH, IF SERIES "D" DOES NOT FIT ON A 8"-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THE ABBREVIATION CANNOT FIT ON THE FIRST LINE.
- 5. LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLOGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.
- 6. SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

LOCAL SUPPLIERS: PARTS LISTING:

- J.O. HERBERT COMPANY, INC MIDLOTHIAN, VA

- WESTERN REMAC, INC.

WOODRIDGE, IL

SIGN CHANNEL SIGN SCREWS

PART #HPN053 (MED. CHANNEL) 1/4" x 14 x 1" H.W.H. #3 SELF TAPPING WITH NEOPRENE WASHER

BRACKETS

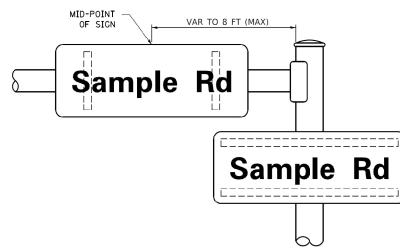
PART #HPN034 (UNIVERSAL)

CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

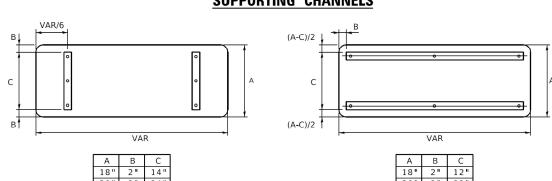
OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BRACKET OF THE ABOVE PRODUCT.

MOUNTING LOCATION

ARM OR POLE MOUNTED



SUPPORTING CHANNELS



STANDARD ALPHABETS SPACING CHART

(8") UPPER CASE AND (6") LOWER CASE

| | FHWA SE | RIES "C" | | | FHWA SE | RIES "D" | |
|---------------|---------------------------|------------------|----------------------------|-----------|---------------------------|------------------|----------------------------|
| CHARACTER | LEFT SPACING (INCH) | WIDTH (INCH) | RIGHT SPACING (INCH) | CHARACTER | LEFT SPACING (INCH) | WIDTH (INCH) | RIGHT SPACING (INCH) |
| Α | 0.240 | 5.122 | 0.240 | Α | 0.240 | 6.804 | 0.240 |
| В | 0.880 | 4.482 | 0.480 | В | 0.960 | 5.446 | 0.400 |
| С | 0.720 | 4. 482 | 0.720 | С | 0.800 | 5.446 | 0.800 |
| D | 0.880 | 4.482 | 0.720 | D E | 0.960 | 5.446 | 0.800 |
| E F | 0.880 0.880 | 4.082 4.082 | 0.480 | F | 0.960 0.960 | 4.962 4.962 | 0.400 |
| G | 0.720 | 4.482 | 0.720 | G | 0.800 | 5.446 | 0.800 |
| Н | 0.880 | 4.482 | 0.880 | Н | 0.960 | 5.446 | 0,960 |
| I | 0.880 | 1.120 | 0.880 | I | 0.960 | 1.280 | 0.960 |
| J | 0.240 | 4.082 | 0.880 | J | 0.240 | 5.122 | 0.960 |
| K | 0.880 | 4.482 | 0.480 | K | 0.960 | 5.604 | 0.400 |
| L | 0.880 | 4.082 | 0.240 | L | 0.960 | 4.962 | 0.240 |
| M | 0.880 | 5. 284 | 0.880 | М | 0.960 | 6. 244 | 0.960 |
| N | 0.880 | 4. 482 | 0.880 | N | 0.960 | 5.446 | 0.960 |
| 0 P | 0.720 | 4. 722 | 0.720 | O P | 0.800 | 5. 684 | 0.800 |
| Q | 0.880 0.720 | 4.482 4.722 | 0.720 0.720 | Q | 0.960 0.800 | 5.446 5.684 | 0.240 |
| R | 0. 120 | 4. 482 | 0. 480 | R | 0.960 | 5. 446 | 0.400 |
| S | 0.480 | 4. 482 | 0.480 | S | 0. 400 | 5. 446 | 0.400 |
| T | 0.240 | 4.082 | 0.240 | T | 0.240 | 4. 962 | 0.240 |
| U | 0.880 | 4.482 | 0.880 | U | 0.960 | 5.446 | 0.960 |
| ٧ | 0.240 | 4.962 | 0.240 | ٧ | 0.240 | 6.084 | 0.240 |
| W | 0.240 | 6.084 | 0.240 | W | 0.240 | 7.124 | 0.240 |
| Χ | 0.240 | 4.722 | 0.240 | Х | 0.400 | 5.446 | 0.400 |
| Y | 0.240 | 5.122 | 0.240 | Y | 0.240 | 6.884 | 0.240 |
| Z | 0.480 | 4.482 | 0.480 | Z | 0.400 | 5.446 | 0.400 |
| a | 0.320 0.720 | 3.842 4.082 | 0.640 | a b | 0.400 0.800 | 4.562 4.802 | 0.720 0.480 |
| c p | 0.120 | 4.002 | 0.480 | С | 0.800 | 4. 722 | 0.480 |
| d | 0.480 | 4.082 | 0.720 | d | 0.480 | 4. 802 | 0.800 |
| e | 0.480 | 4.082 | 0.320 | e | 0.480 | 4. 722 | 0.320 |
| f | 0.320 | 2.480 | 0.160 | f | 0.320 | 2.882 | 0.160 |
| g | 0.480 | 4.082 | 0.720 | g | 0.480 | 4.802 | 0.800 |
| h | 0.720 | 4.082 | 0.640 | h | 0.800 | 4.722 | 0.720 |
| i | 0.720 | 1.120 | 0.720 | į | 0.800 | 1.280 | 0.800 |
| j | 0.000 | 2.320 | 0.720 | J. | 0.000 | 2.642 | 0.800 |
| k I | 0.720 | 4. 322 | 0.160 | k I | 0.800 | 5.122 | 0.160 |
| m | 0.720 0.720 | 1. 120 6. 724 | 0.720 0.640 | m | 0.800 | 1. 280 7. 926 | 0.800 0.720 |
| n | 0.720 | 4. 082 | 0.640 | n | 0.800 | 4. 722 | 0.720 |
| 0 | 0.120 | 4. 082 | 0.480 | 0 | 0.480 | 4.882 | 0. 480 |
| P | 0.720 | 4.082 | 0.480 | р | 0.800 | 4.802 | 0.480 |
| q | 0.480 | 4.082 | 0.720 | q | 0.480 | 4.802 | 0.800 |
| r | 0.720 | 2.642 | 0.160 | r | 0.800 | 3.042 | 0.160 |
| S | 0.320 | 3. 362 | 0.240 | 5 | 0.320 | 3. 762 | 0.240 |
| + | 0.080 | 2.882 | 0.080 | t | 0.080 | 3. 202 | 0.080 |
| U | 0.640 | 4.082 | 0.720 | u | 0.720 | 4.722 | 0.800 |
| V | 0.160 | 4.722 | 0.160 | V | 0.160 | 5. 684 9. 046 | 0.160 |
| × | 0.160 0.000 | 7. 524 5. 202 | 0.160 | w x | 0.160 | 6. 244 | 0.160 |
| У | 0.160 | 4. 962 | 0.160 | y | 0.160 | 6. 004 | 0.160 |
| Z | 0.240 | 3. 362 | 0.240 | Z | 0.240 | 4.002 | 0.240 |
| 1 | 0.720 | 1.680 | 0.880 | 1 | 0.800 | 2.000 | 0.960 |
| 2 | 0.480 | 4.482 | 0.480 | 2 | 0.800 | 5.446 | 0.800 |
| 3 | 0.480 | 4.482 | 0.480 | 3 | 1.440 | 5.446 | 0.800 |
| 4 | 0.240 | 4.962 | 0.720 | 4 | 0.160 | 6.004 | 0.960 |
| 5 | 0.480 | 4.482 | 0.480 | 5 | 0.800 | 5.446 | 0.800 |
| 6 | 0.720 | 4.482 | 0.720 | 6 | 0.800 | 5.446 | 0.800 |
| 7 | 0.240 | 4.482 | 0.720 | 7 | 0.560 | 5.446 | 0.560 |
| <u>8</u> 9 | 0.480 0.480 | 4. 482 4. 482 | 0.480 0.480 | 8 | 0.800 0.800 | 5. 446 5. 446 | 0.800 |
| 0 | 0.720 | 4. 722 | 0.720 | 0 | 0.800 | 5. 684 | 0.800 |
| - | 0. 120 | 2. 802 | 0. 720 | - | 0.240 | 2. 802 | 0.240 |
| | | | | | | | |

| USER NAME = footemj | DESIGNED | - | LP/IP | REVISED | - | LP 07/01/2015 |
|------------------------------|----------|---|------------|---------|---|---------------|
| | DRAWN | - | LP | REVISED | - | |
| PLOT SCALE = 50.0000 ' / in. | CHECKED | - | IP | REVISED | - | |
| PLOT DATE = 3/4/2019 | DATE | _ | 10/01/2014 | REVISED | | |

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

| | | | | DIS | STRICT | 1 | | F.A.P. RTE. | SECT |
|-------------|-------|------|------|-----|--------|----------|---------|----------------|-------|
| | STANE | ΔRN | TRΔI | FIC | SIGNA | L DESIGN | DETAILS | 344 | N/A |
| | SIANE | AIID | ША | 110 | JIGINA | L DESIGN | | | TS-02 |
| SCALE: NONE | SHEE | T 1 | OF | 1 | SHEETS | STA. | TO STA. | | |

| | | TEERTOIS | 1 201 71 | 11107201 | | |
|----------------|-------|----------|----------|------------|--------|-------------|
| | | ILLINOIS | FED. A | ID PROJECT | | |
| | TS-02 | | | CONTRACT | NO. 6 | 52U73 |
| 344 | N/ | 'A | | DUPAGE | 87 | 27 |
| F.A.P. RTE. | SEC | TION | | COUNTY | SHEETS | SHEE NO. |

TRAFFIC SIGNAL LEGEND (NOT TO SCALE)

| | <u>EXISTING</u> | PROPOSED | <u>ITEM</u> | <u>existing</u> | PROPOSED | ITEM | <u>EXISTING</u> | PROPOSED |
|---|------------------------------------|--|---|-------------------|-------------------|--|---|--|
| CONTROLLER CABINET | | | HANDHOLE -SQUARE -ROUND | | | SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD | R | R R Y |
| COMMUNICATION CABINET | ECC | CC | HEAVY DUTY HANDHOLE | | | | | G G → Y |
| MASTER CONTROLLER | ЕМС | MC | -SQUARE -ROUND | | ⊞ ⊕ | | ♣ ∮ P | 4 Y 4 G 4 G P |
| MASTER MASTER CONTROLLER | EMMC | ммс | DOUBLE HANDHOLE | | | SIGNAL HEAD WITH BACKPLATE | | R R R |
| UNINTERRUPTABLE POWER SUPPLY | ₽ | * | JUNCTION BOX | | 0 | -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE | | Y G G G |
| SERVICE INSTALLATION -(P) POLE MOUNTED | - <u>-</u> -P | - - P | RAILROAD CANTILEVER MAST ARM | X OX X | X OX X | | | 4 Y 4 G 4 Y 4 G |
| SERVICE INSTALLATION | | | RAILROAD FLASHING SIGNAL | ∑O ∑ | XeX | | P RB | P RB |
| -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED | $\boxtimes^{G}\boxtimes^{GM}$ | ⊠ ^G ⊠ ^{GM} | RAILROAD CROSSING GATE | ₹0 ₹ | X• X - | PEDESTRIAN SIGNAL HEAD | | |
| TELEPHONE CONNECTION | ET | Т | RAILROAD CROSSBUCK | 查 | * | AT RAILROAD INTERSECTIONS | A | ₽ |
| STEEL MAST ARM ASSEMBLY AND POLE | O | •—— | RAILROAD CONTROLLER CABINET | | ▶∢ | PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER | (F) C | ₽ C ★ D |
| ALUMINUM MAST ARM ASSEMBLY AND POLE | | | UNDERGROUND CONDUIT (UC), GALVANIZED STEEL | | | | | |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE | o-¤— | •* | TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE | | | ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN" | | |
| SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY | 0 | ● BM | SYSTEM ITEM | s | SP | NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. | | |
| | 0 | | INTERSECTION ITEM | I | IP | ALL DETECTOR LOOP CABLE TO BE SHIELDED | \sim | |
| WOOD POLE | ⊗ . | • | REMOVE ITEM | | R | GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN) | 1#6 | 1#6 |
| GUY WIRE SIGNAL HEAD | <i>></i> - | <i>≻</i> | RELOCATE ITEM | | RL | ELECTRIC CABLE IN CONDUIT, TRACER | | |
| SIGNAL HEAD WITH BACKPLATE | #> | +▶ | ABANDON ITEM | | Α | NO. 14 1/C | , | |
| | > P P | - ▶ P + ▶ P | CONTROLLER CABINET AND FOUNDATION TO BE REMOVED | | RCF | COAXIAL CABLE | <u> </u> | <u> </u> |
| SIGNAL HEAD OPTICALLY PROGRAMMED FLASHER INSTALLATION | ots ots FS | ••• FS | MAST ARM POLE AND FOUNDATION TO BE REMOVED | | RMF | VENDOR CABLE | | |
| -(FS) SOLAR POWERED | □t> ^F □t> ^{FS} | ₽ ► FS | SIGNAL POST AND FOUNDATION TO BE REMOVED | | RPF | COPPER INTERCONNECT CABLE. NO. 18, 3 PAIR TWISTED, SHIELDED | 6#18 | |
| PEDESTRIAN SIGNAL HEAD | -0 | -1 | DETECTOR LOOP, TYPE I | | | FIBER OPTIC CABLE -NO. 62.5/125, MM12F | | —(12F)— |
| PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON | | ⊚ ⊚ APS | PREFORMED DETECTOR LOOP | PP | РР | -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F | | —(24F)— |
| RADAR DETECTION SENSOR | R | R | SAMPLING (SYSTEM) DETECTOR | 5 (5) | 5 5 | | — <u>36F</u> — | —(36F)— |
| VIDEO DETECTION CAMERA | (V) | V | INTERSECTION AND SAMPLING (SYSTEM) DETECTOR | IS (IS) | IS (IS) | | | |
| RADAR/VIDEO DETECTION ZONE | # | | QUEUE AND SAMPLING (SYSTEM) DETECTOR | QS QS | QS QS | GROUND ROD -(C) CONTROLLER -(M) MAST ARM | ± ^C ± ^M ± ^P ± ^S | $\dot{\bar{T}}^{C} \dot{\bar{T}}^{M} \dot{\bar{T}}^{P} \dot{\bar{T}}^{S}$ |
| PAN, TILT, ZOOM (PTZ) CAMERA | PTZ] | PTZ. | WIRELESS DETECTOR SENSOR | (W) | © | -(P) POST -(S) SERVICE | | |
| EMERGENCY VEHICLE LIGHT DETECTOR | \bowtie | ~ | WIRELESS ACCESS POINT | | — | | | |
| CONFIMATION BEACON | ○ —① | ⊷ (| | | _ | | | |
| | ~+ | •· · | | | | | | |
| WIRELESS INTERCONNECT | | | İ | | | | | |

| , i II O II i | USER NAME = footemj | DESIGNED - IP | REVISED - |
|--------------------------|------------------------------|------------------|-----------|
| \!N\5H | | DRAWN - IP | REVISED - |
| SINGH + ASSOCIATES, INC. | PLOT SCALE = 50.0000 ' / in. | CHECKED - LP | REVISED - |
| CONSULTING ENGINEERS | PLOT DATE = 3/4/2019 | DATE - 9/29/2016 | REVISED - |

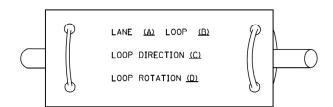
| STAT | E OI | F ILLINOIS |
|------------|------|----------------|
| DEPARTMENT | 0F | TRANSPORTATION |

SCALE: NONE

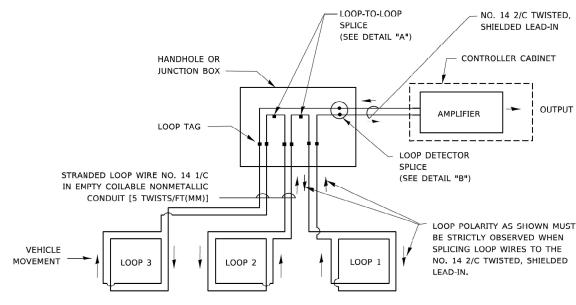
| | DISTRICT 1 | | | | | | | | SECTION | | | | |
|-----|--|----|-------|-----|--------|----------|---------|-----|----------|--------|------------|-----|-------|
| ΔТ2 | STANDARD TRAFFIC SIGNAL DESIGN DETAILS | | | | | | 344 | N/A | | DUPAGE | 87 | 28 | |
| 017 | NIVA | שו | IIIAI | 110 | JIUIVA | . DESIGN | DETAILS | | TS-05 | | CONTRACT | NO. | 62U73 |
| S | SHEET | 1 | OF | 7 | SHEETS | STA. | TO STA. | | ILLINOIS | FED. A | ID PROJECT | | |

- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- 3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- 4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- 5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- 7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

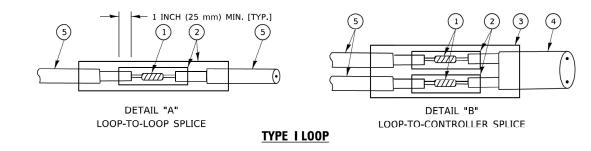


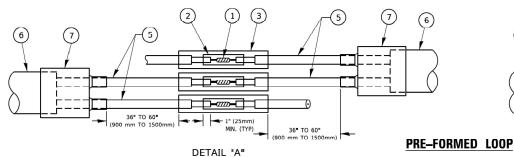
- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



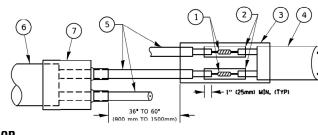
DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
 SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.





LOOP-TO-LOOP SPLICE



DETAIL "B"

LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- 1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- (2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- (3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
- (4) NO. 14 2/C TWISTED, SHIELDED CABLE.

- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
 PRE-FORMED LOOP
- (6) XL POLYOLEFIN 2 CONDUCTOR
- (7) BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

SINGH - ASSOCIATES, INC.

 USER NAME
 = footenij
 DESIGNED
 REVISED

 DRAWN
 REVISED

 PLOT SCALE
 - 50.0000 ° / in.
 CHECKED
 REVISED

 PLOT DATE
 = 3/4/2019
 DATE
 REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SHEET 2 OF 7 SHEETS STA. TO STA

 F.A.P. RTE.
 SECTION
 COUNTY SHEETS
 TOTAL SHEETS
 SHEET NO.

 344
 N/A
 DUPAGE
 87
 29

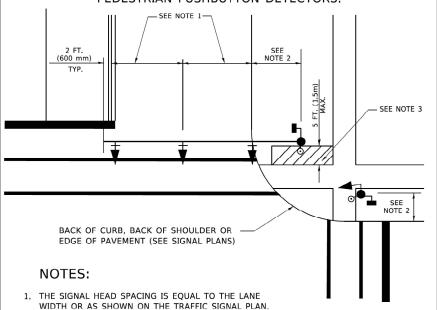
 TS-05
 CONTRACT
 NO.
 62U73

 ILLINOIS
 FED. AID PROJECT

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

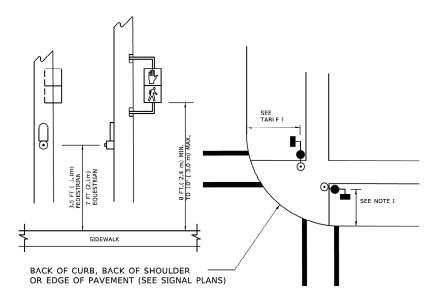
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND

PEDESTRIAN PUSHBUTTON DETECTORS.



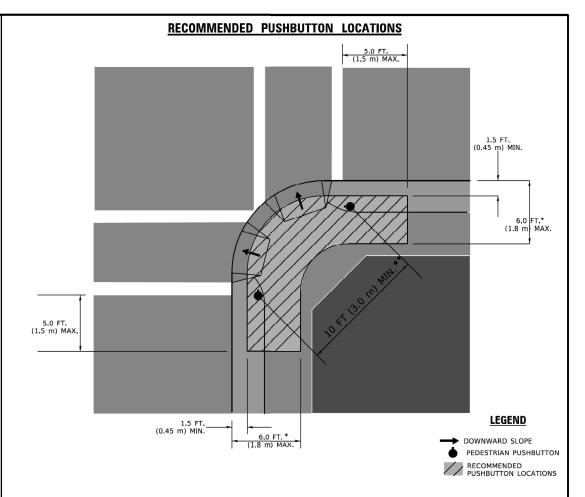
- 2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
- 3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
- 4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
- 5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

PEDESTRIAN SIGNAL POST PEDESTRIAN PUSH BUTTON POST



NOTES:

- 1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
- 2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST
- 3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
- 4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR **BUILDINGS AND FACILITIES."**



- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

TRAFFIC SIGNAL EQUIPMENT OFFSET

| TRAFFIC SIGNAL EQUIPMENT | COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION) | SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION) |
|---------------------------------------|---|---|
| TRAFFIC SIGNAL MAST ARM POLE | 6 FT (1.8m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| TRAFFIC SIGNAL POST | 4 FT (1.2m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| PEDESTRIAN SIGNAL POST | 4 FT (1.2m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| PEDESTRIAN PUSHBUTTON POST | 4 FT (1.2m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| TEMPORARY WOOD POLE | 6 FT (1.8m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| CONTROLLER CABINET | 6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2 | SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3. |
| SERVICE INSTALLATION, GROUND MOUNT | 6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2 | SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3. |

NOTES:

- 1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
- 2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
- 3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
- 4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

SCALE: NONE

NOTES:

- 1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
- 2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY
- 3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
- 4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
- 5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

| USER NAME = footemj | DESIGNED - | REVISED - |
|------------------------------|------------|-----------|
| | DRAWN - | REVISED - |
| PLOT SCALE = 50.0000 ' / in. | CHECKED - | REVISED - |
| PLOT DATE = 3/4/2019 | DATE - | REVISED - |

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

| | | | | DIS | STRICT | 1 | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | |
|--|-------|----|------|-----|--------|--------|----------|----------------|-----------------|------------|-----------------|----|
| c. | TANDA | RN | TRAF | FIC | SIGNA | DESIGN | DETAILS | 344 | N/A | DUPAGE | 87 | 30 |
| STANDARD TRAFFIC SIGNAL DESIGN DETAILS | | | | | | TS-05 | CONTRACT | NO. | 52U73 | | | |
| | SHEET | 3 | OF | 7 | SHEETS | STA. | TO STA. | | TILLINOIS FED A | ID PROJECT | | |

NOTES:

- HANDHOLE COVER

DETAIL "A"

DETAIL "B"

HANDHOLE COVER HANDLE

-HANDHOLE FRAME

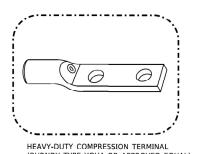
-4-3/8" (9mm) DIA., CORED HOLES

UL LISTED GROUND

COMPRESSION TERMINAL

GROUNDING SYSTEM

- 1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC,). GROUND ROD SHALL BE $3/4\mbox{"}$ DIA, x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS. CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC. ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- 2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- 3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- 4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



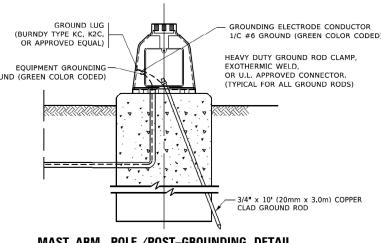
(BURNDY TYPE YGHA OR APPROVED EQUAL)



¾" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EQUAL)

NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
- 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



MAST ARM POLE /POST-GROUNDING DETAIL

(NOT TO SCALE)

MOUNTING PLATE

MANUFACTURER

FABRICATION

TOP & BOTTOM AS PER

CABINET, SHEET ALUMINUM

CONTINUOUS PIANO HINGE

POLE MOUNTED SERVICE CABINET OUTSIDE

POWER INDICATOR LIGHT

-INTERNALLY MOUNTED FOR

GROUND MOUNTED SERVICE

DIMENSIONS L 6" x W 12" x H 14"

0

 \circ

-60A. MAI

DISCONNECT — 15A. MAIN

DISCONNECT

PRESSURE

STANDOFF

FUSE, KLKR 1/4 A

LOCK, HASP

|PADLOCK, FURNISHED BY

CONTRACTOR, KEYED TO

-COMPRESSION LATCH, TYP. (2 MIN. REQ'D)

-1 1/4" (30mm) DIA. COUPLING

-STRAIN RELIEF COUPLING

PANELBOARD

NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508,

TRAFFIC SIGNAL

CONTROLLER CABINET

CIRCUI

BREAKER

NEUTRAL GROUND

ANTI-CORROSION COMPOUND

STAINLESS STEEL WASHERS

SHALL BE APPLIED ON ALL
BOLT/ CONNECTION ASSEMBLIES.

LIL LISTED GROUND

COMPRESSION TERMINAL
WITH STAINLESS STEEL NUT

SHALL BE APPLIED TO THE ASSEMBLY.

INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL,

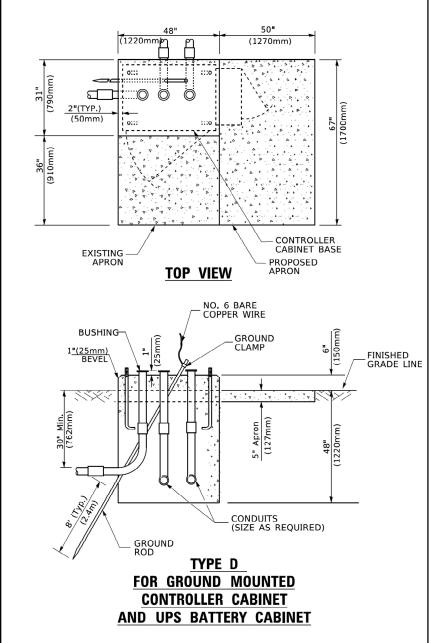
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

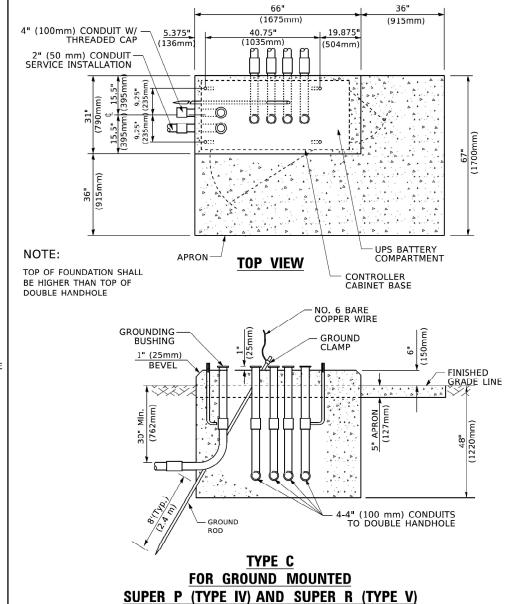
REVISED DESIGNED DRAWN REVISED LOT SCALE = 50.0000 ' / in. CHECKED REVISED DATE REVISED

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

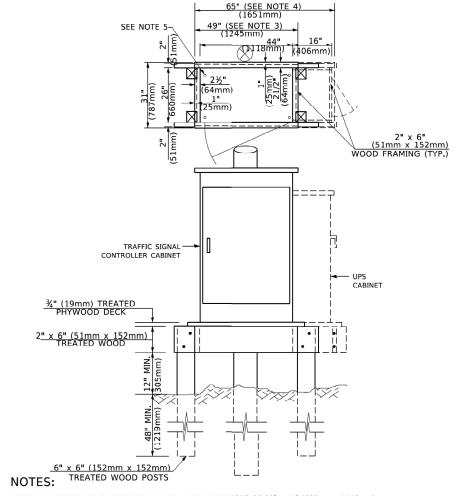
DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS SHEET 4 OF 7 SHEETS STA.

COUNTY 344 N/A DUPAGE 87 31 TS-05 CONTRACT NO. 62U73





CONTROLLER CABINETS



- 1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED
- 2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" \times 25" (406mm \times 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
- $\mathbf{3}_{\bullet}$ PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
- 4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
- 5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
- 6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

TEMPORARY SIGNAL CONTROLLER **WOOD SUPPORT PLATFORM**

| CABLE SLACK LENGTH | FEET | METER |
|---|------|-------|
| HANDHOLE | 6.5 | 2.0 |
| DOUBLE HANDHOLE | 13.0 | 4.0 |
| SIGNAL POST | 2.0 | 0.6 |
| MAST ARM | 2.0 | 0.6 |
| CONTROLLER CABINET | 1.5 | 0.5 |
| FIBER OPTIC AT CABINET | 13.0 | 4.0 |
| ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION) | 1.5 | 0.5 |
| GROUND CABLE (SIGNAL POST, MAST ARM, CABINET) | 1.5 | 0.5 |
| GROUND CABLE (BETWEEN FRAME AND COVER) | 5.0 | 1.6 |

| VERTICAL CABLE LENGTH | FEET | METER |
|---|--------|-------|
| MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) | | |
| (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM) | 20.0+L | 6.0+L |
| BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE) | 13.0 | 4.0 |
| PEDESTRIAN PUSH BUTTON | 6.0 | 2.0 |
| SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP | 13.5 | 4.1 |
| SERVICE INSTALLATION POLE MOUNT TO GROUND | 13.5 | 4.1 |
| SERVICE INSTALLATION GROUND MOUNT | 6.0 | 2.0 |
| FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT) | 3.0 | 1.0 |
| | | |

VERTICAL CABLE LENGTH

CABLE SLACK

| FOUNDATION | DEPTH |
|---|--------------|
| TYPE A - Signal Post | 4'-0" (1.2m) |
| TYPE C - CONTROLLER W/ UPS | 4'-0" (1.2m) |
| TYPE D - CONTROLLER | 4'-0" (1.2m) |
| SERVICE INSTALLATION. GROUND MOUNT, TYPE A - SQUARE | 4'-0" (1.2m) |

DEPTH OF FOUNDATION

SCALE: NONE

| Mast Arm Length | ① Foundation Depth | Foundation Diameter | Spiral Diameter | Quantity of Rebars | Size of Rebors |
|--|-----------------------------|------------------------|--------------------|-----------------------|-------------------|
| Less than 30′ (9.1 m) | 10'-0" (3.0 m) | 30" (750mm) | 24" (600mm) | 8 | 6(19) |
| Greater than or equal to | 13'-6" (4 ₄ 1 m) | 30" (750mm) | 24" (600mm) | 8 | 6(19) |
| 30' (9.1 m) and less than 40' (12.2 m) | 11'-0" (3 ₄ m) | 36" (900mm) | 30" (750mm) | 12 | 7(22) |
| Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m) | 13'-0" (4.0 m) | 36" (900mm) | 30" (750mm) | 12 | 7(22) |
| Greater than or equal to 50' (15,2 m) and up to 55' (16,8 m) | 15'-0" (4.6 m) | 36" (900mm) | 30" (750mm) | 12 | 7(22) |
| Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m) | 21'-0'' (6.4 m) | 42" (1060mm) | 36" (900mm) | 16 | 8(25) |
| Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m) | 25'-0" (7,6 m) | 42" (1060mm) | 36" (900mm) | 16 | 8(25) |

NOTES:

- 1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Ou) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
- 2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
- 3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
- 4. For most arm assemblies with dual arms refer to state standard 878001...

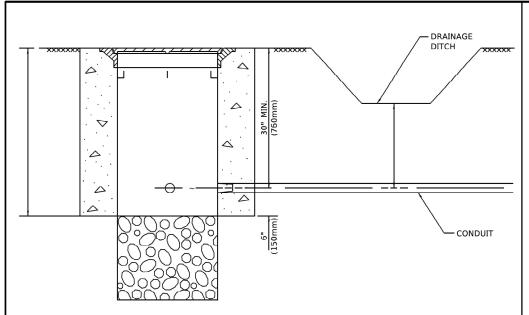
DEPTH OF MAST ARM FOUNDATIONS, TYPE E



| OSEK NAME = TOOLETTI | DESIGNED - | KLVISLD - |
|------------------------------|------------|-----------|
| | DRAWN - | REVISED - |
| PLOT SCALE = 50.0000 ' / in. | CHECKED - | REVISED - |
| PLOT DATE = 3/4/2019 | DATE - | REVISED - |
| | | |

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

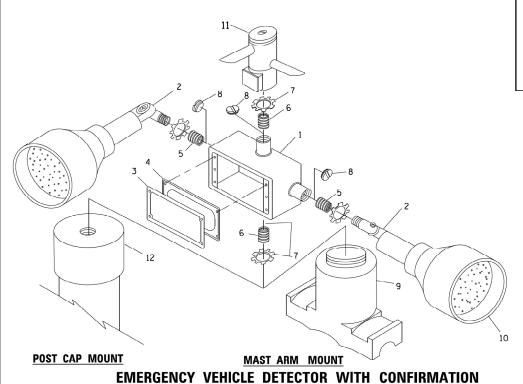
| DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS | | | | | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
|--|-------|---|----|---|--------|------|----------------|---------|------------------|-----------------|--------------|-------|
| | | | | | | | 344 | N/A | DUPAGE | 87 | 32 | |
| | | | | | | | DETAILS | | TS-05 CONTRACT N | | NO. 6 | 52U73 |
| | SHEET | 5 | OF | 7 | SHEETS | STA. | TO STA. | | ILLINOIS FED. A | ID PROJECT | | |



- 1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
- 2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
- 3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

HANDHOLE WITH MINIMUM CONDUIT DEPTH

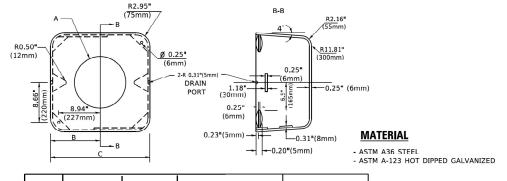
(NOT TO SCALE)



(915mm) (1675mm) 40.75" 19.875" (136mm) (1035mm) TT:::::: \bigcirc CONTROLLER CABINET BASE PROPOSED-**TOP VIEW** APRON -NO. 3 DOWEL 18" (450mm NO. 6 BARE COPPER WIRE LONG (8 REQ.) BUSHING-GROUND/ CLAMP EXISTING-ANCHOR BOLTS **FINISHED** 1"(25mm) BEVEL GRADE LINE (225mm) -EXISTING CONDUITS EXISTING GROUND ROD MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION (NOT TO SCALE)

| ITEM | NO. IDENTIFICATION |
|------|---|
| 1 | OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M) |
| 2 | LAMP HOLDER AND COVER |
| 3 | OUTLET BOX COVER |
| 4 | RUBBER COVER GASKET |
| 5 | REDUCING BUSHING |
| 6 | ¾"(19 mm) CLOSE NIPPLE |
| 7 | ¾"(19 mm) LOCKNUT |
| 8 | ¾"(19 mm) HOLE PLUG |
| 9 | SADDLE BRACKET - GALV. |
| 10 | 6 WATT PAR 38 LED FLOOD LAMP |
| 11 | DETECTOR UNIT |
| 12 | POST CAP [18 FT. (5.4 m) POST MIN.] |

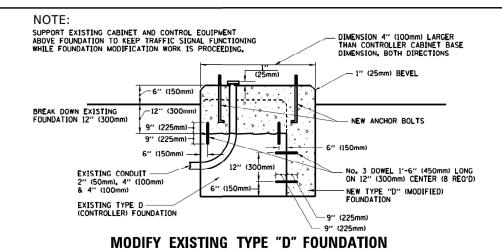
- 1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR
- 2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- 3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 "(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

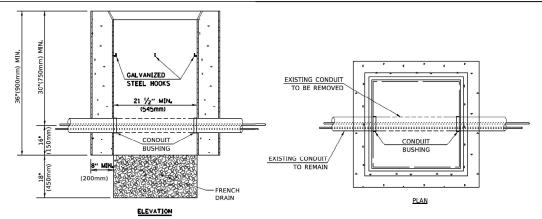


| А | В С | | HEIGHT | WEIGHT |
|--------|---------------|--------------|--------------------------|-----------------|
| VARIES | 9.5"(241mm) | 19"(483mm) | 7" (178mm) - 12" (300mm) | 53 lbs (24kg) |
| VARIES | 10.75"(273mm) | 21.5"(546mm) | 7" (178mm) - 12" (300mm) | 68 lbs (31 kg) |
| VARIES | 13,0"(330mm) | 26"(660mm) | 7" (178mm) - 12" (300mm) | 81 lbs (37 kg) |
| VARIES | 18.5"(470mm) | 37"(940mm) | 7" (178mm) - 12" (300mm) | 126 lbs (57 kg) |

SHROUD

- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD.
 THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
- 2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
- 3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.





- 1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
- 2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

BEACON MOUNTING DETAIL DESIGNED -REVISED DRAWN -REVISED CHECKED REVISED PLOT SCALE = 50.0000 ' / in.

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

DISTRICT 1 N/A STANDARD TRAFFIC SIGNAL DESIGN DETAILS

87 33 DUPAGE CONTRACT NO. 62U73 SHEET 6 OF 7 SHEETS STA.



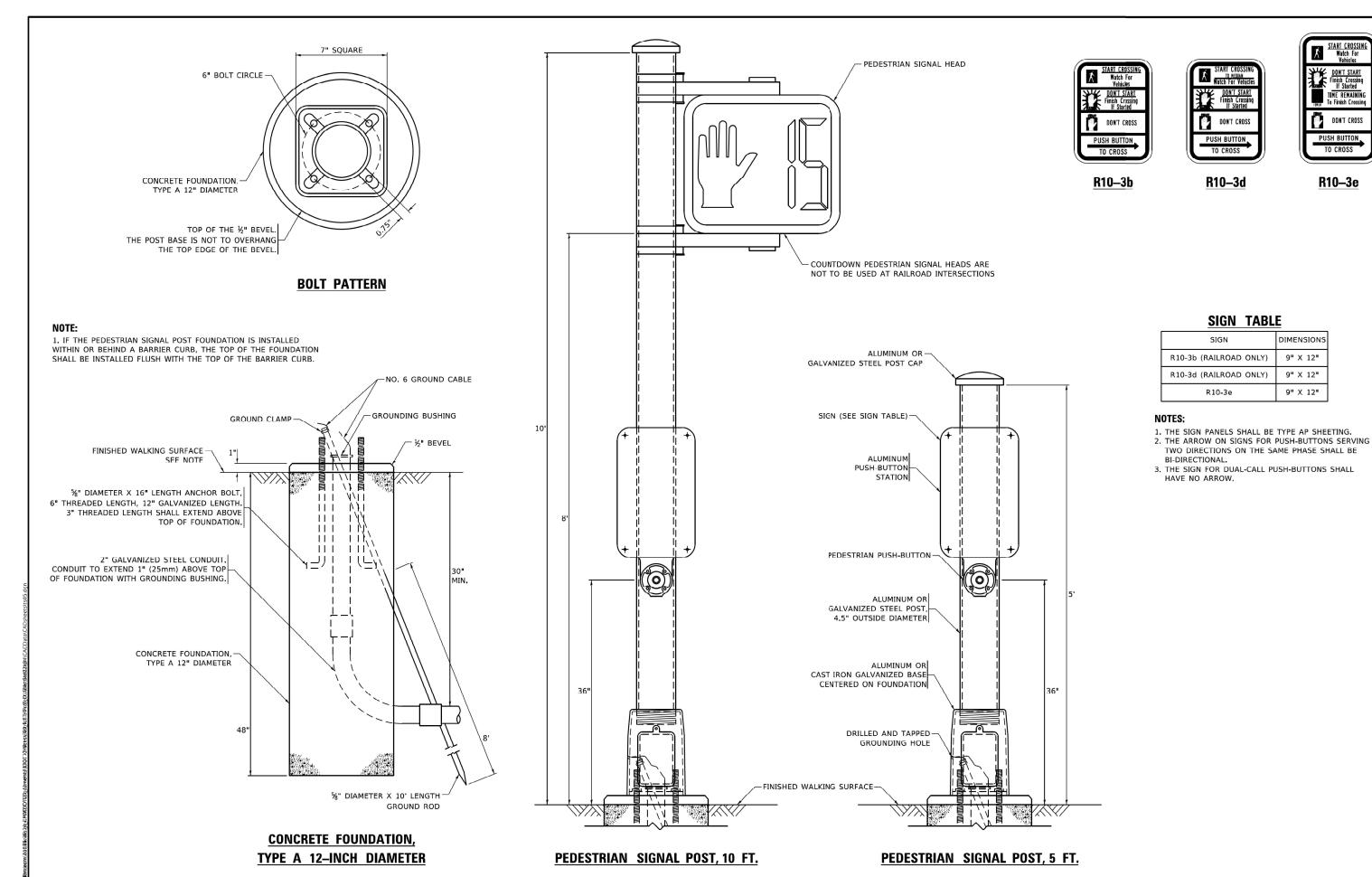
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PLOT SCALE = 100,0000 ' / in.

DESIGNED - IP

DRAWN - IP

CHECKED



STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

DISTRICT 1

STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SHEET 7 OF 7 SHEETS STA.

344

N/A

DUPAGE 87 34

CONTRACT NO. 62U73

REVISED - 10-15-2020

REVISED -

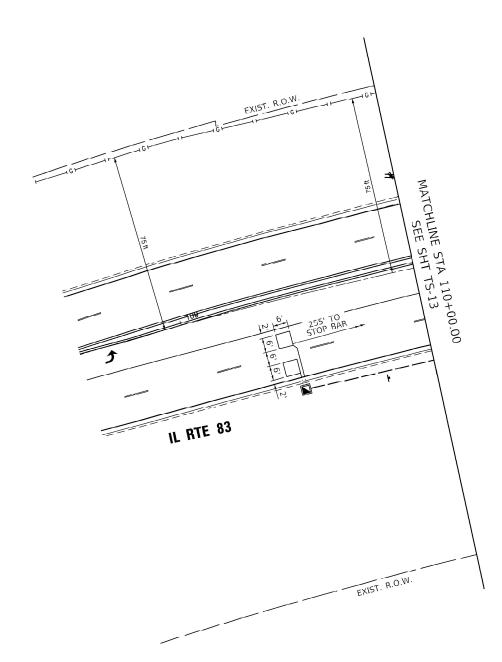
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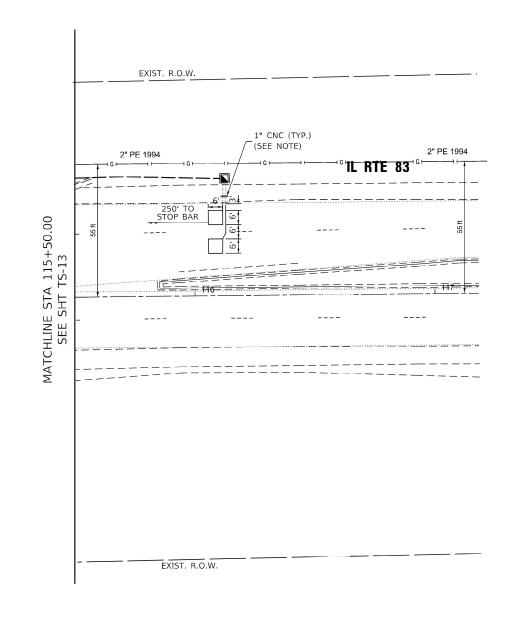
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DESIGNED - EEC REVISED DRAWN -EEC REVISED GJG REVISED PLOT DATE = 2/22/2024 REVISED 2/22/2024

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION** TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN - ALL STAGES AND REMOVAL EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN IL RTE 83 AND BLUFF RD (SHEET 1 OF 2) SHEET 1 OF 2 SHEETS STA.

SECTION COUNTY N/A DUPAGE 87 35 CONTRACT NO. 62U73





TS 580 **ECON 183**

SHT NO. 14

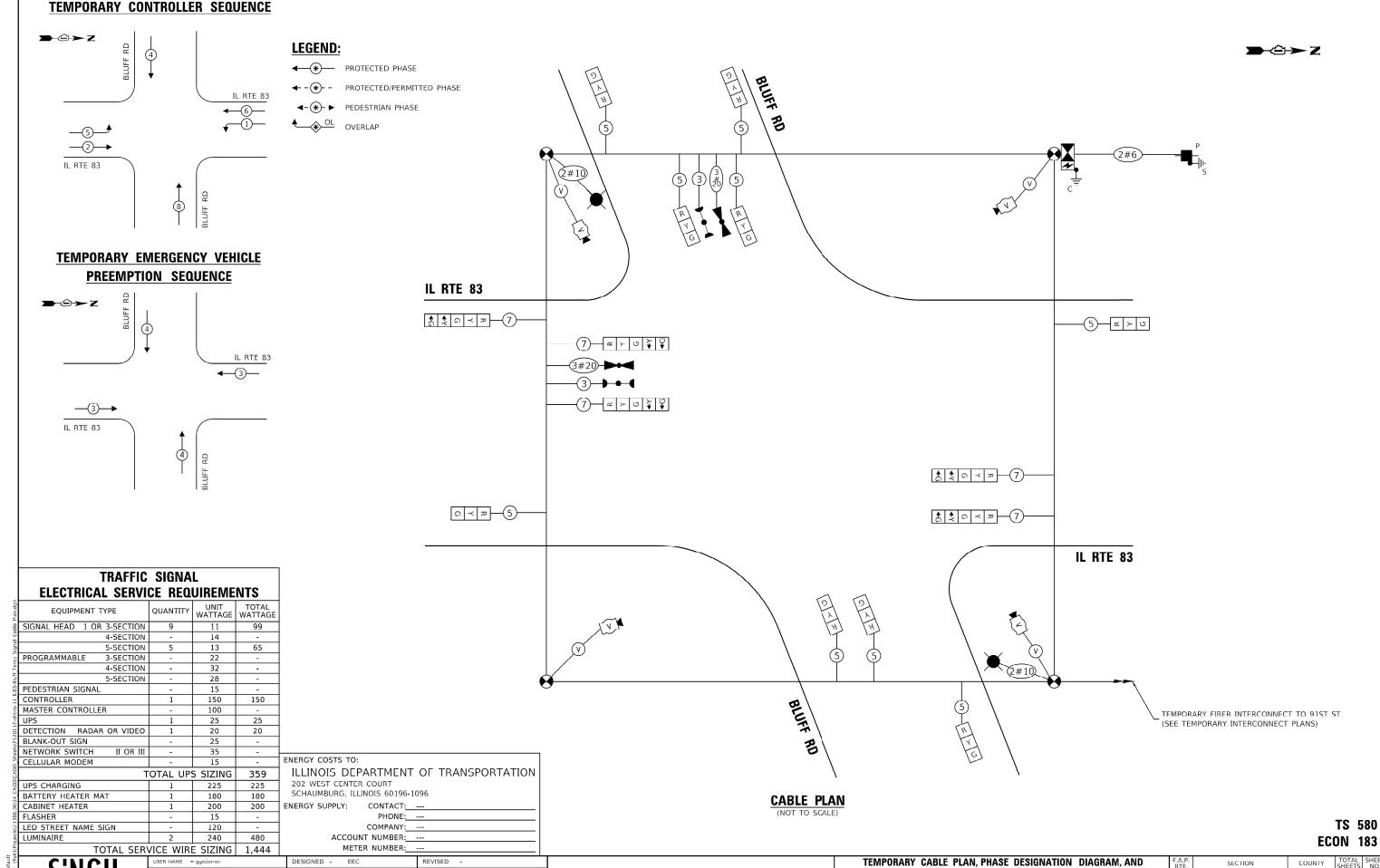
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| USER NAME = ggedemer | DESIGNED | - | EEC | REVISED | - |
|-----------------------------|----------|---|-----------|---------|---|
| | DRAWN | - | EEC | REVISED | |
| PLOT SCALE = 40,000 ' / in. | CHECKED | - | GJG | REVISED | - |
| PLOT DATE = 2/22/2024 | DATE | - | 2/22/2024 | REVISED | - |

| STATE OF ILLINOIS | |
|------------------------------|--|
| DEPARTMENT OF TRANSPORTATION | |

| TRAFFIC SIGNAL MODERNIZATION PLAN (SHEET 2 OF 2) | | | | | | | | | |
|--|-------|---|----|---|--------|------|---------|--|--|
| IL RTE 83 AND BLUFF RD | | | | | | | | | |
| IL RIE 03 AND BEOTT RD | | | | | | | | | |
| SCALE: 1"=20' | SHEET | 2 | OF | 2 | SHEETS | STA. | TO STA. | | |

| SECTION | | | | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------|---------|-----|---|------------|-----------------|--------------|
| N/ | Α | | | DUPAGE | 87 | 36 |
| | | | | CONTRACT | NO. 6 | 32U73 |
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STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

EMERGENCY VEHICLE PREEMPTION SEQUENCE – PRE-STAGE, STAGE 1 & 2 344

IL RTE 83 AT BLUFF RD

SHEET 1 OF 1 SHEETS STA.

SHT

DRAWN - EEC

CHECKED - GJG

DATE - 11/16/2023

PLOT DATE = 11/16/2023

REVISED

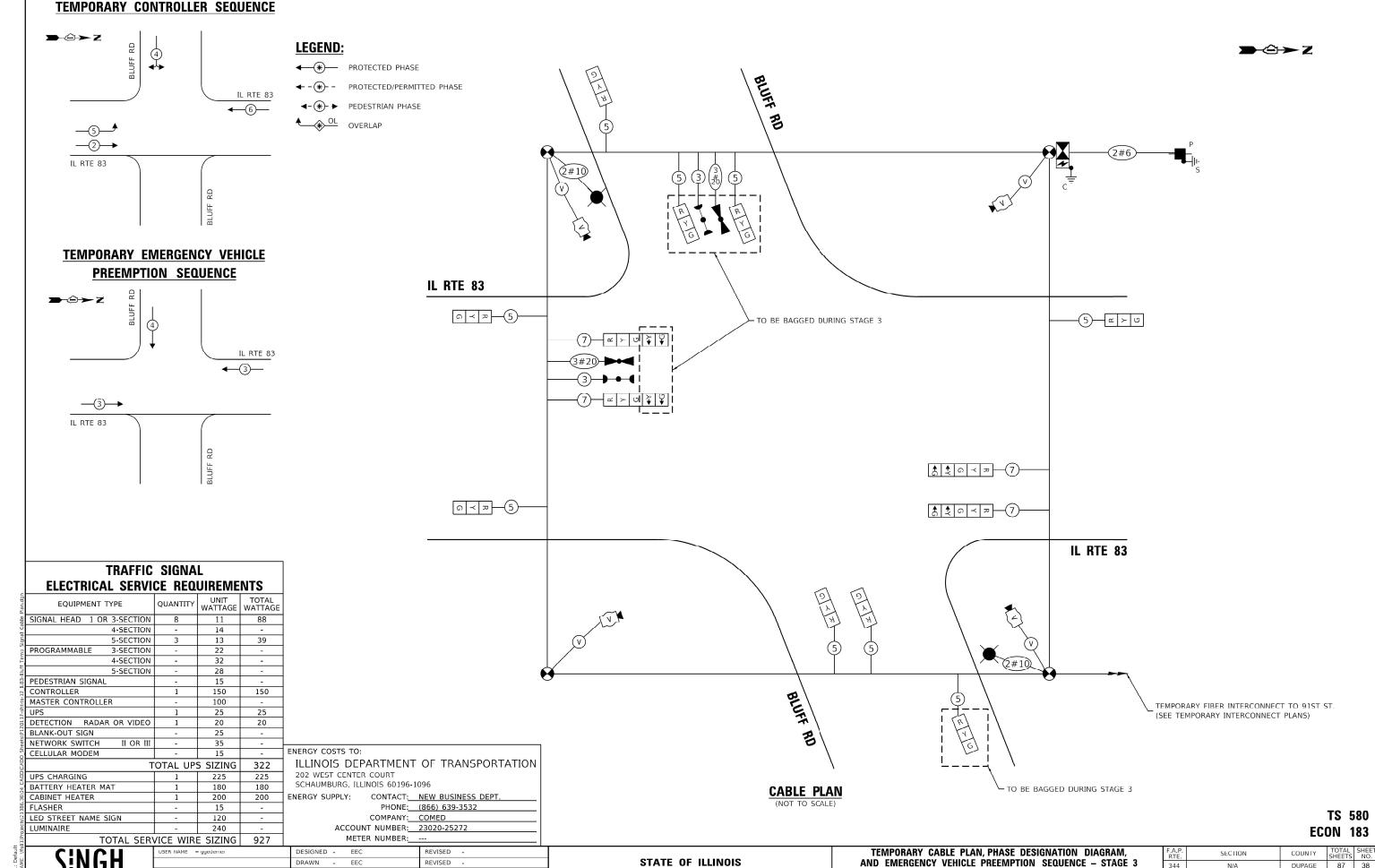
REVISED

REVISED

DUPAGE 87 37

CONTRACT NO. 62U73

N/A



DEPARTMENT OF TRANSPORTATION

N/A

344

IL RTE 83 AT BLUFF RD

SHEET 1 OF 1 SHEETS STA.

DUPAGE 87 38

CONTRACT NO. 62U73

GJG

DATE - 2/22/2024

PLOT DATE = 2/22/2024

REVISED

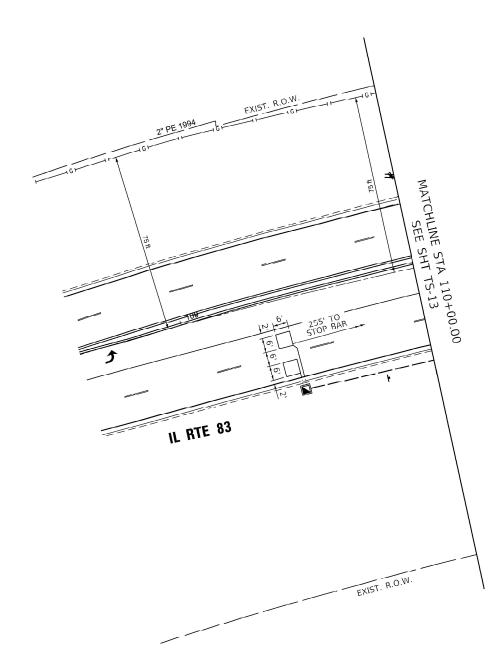
REVISED

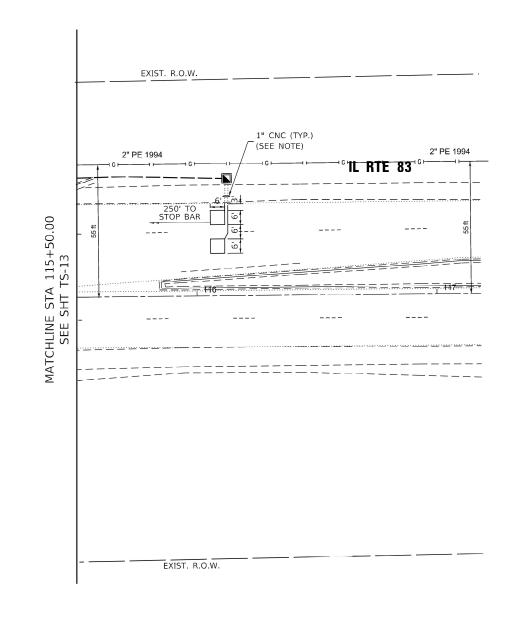
SCALE: 1"=20' SHEET 1 OF 2 SHEETS STA.

PLOT DATE = 2/22/2024

REVISED

2/22/2024





SCALE IN FEET

344

TS 580 **ECON 183**

SHT NO. 14

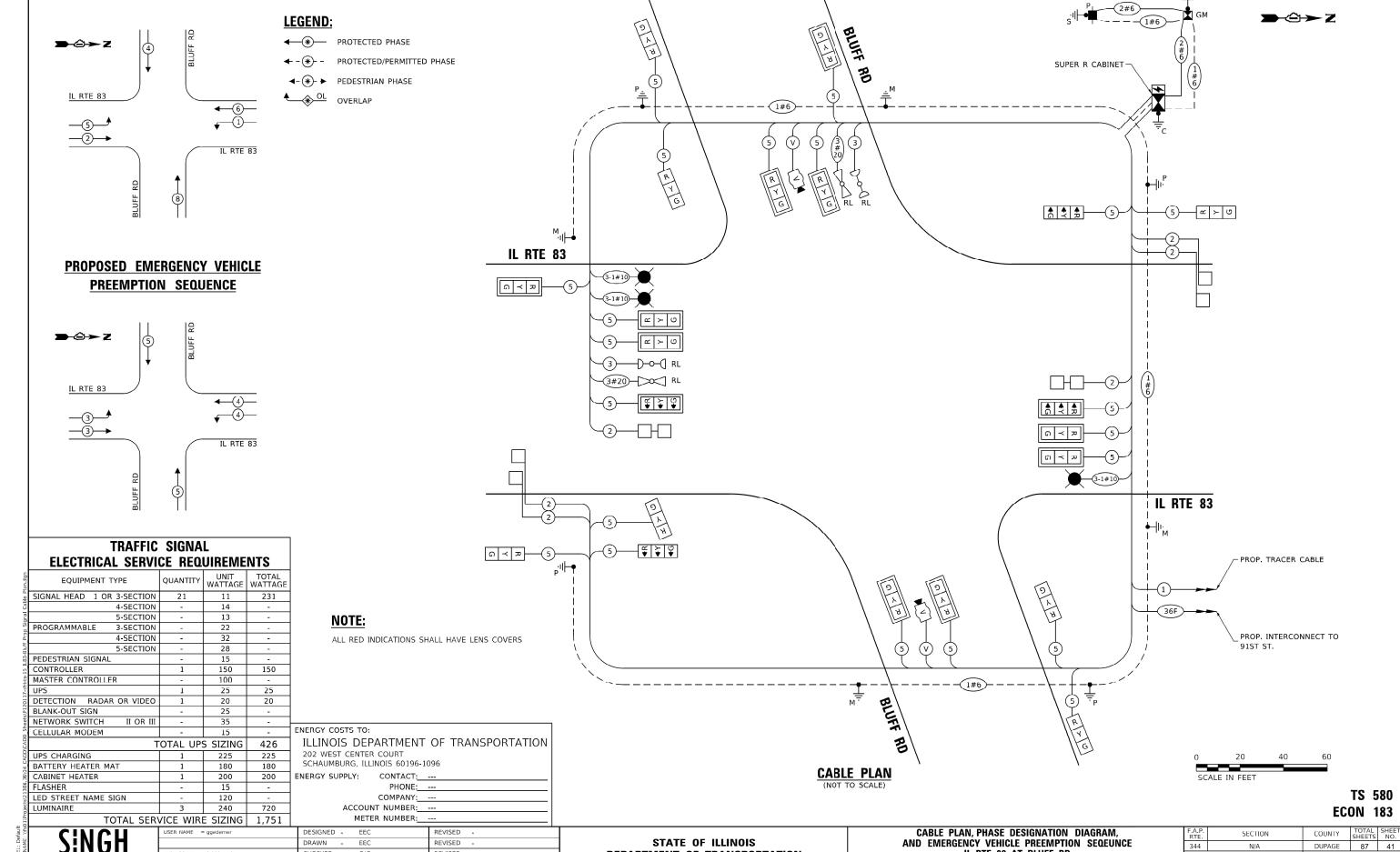
S

| USER NAME = ggederner | DESIGNED | - | EEC | REVISED | - |
|-----------------------------|----------|---|-----------|---------|---|
| | DRAWN | - | EEC | REVISED | - |
| PLOT SCALE = 40,000 ' / in. | CHECKED | - | GJG | REVISED | - |
| PLOT DATE = 2/22/2024 | DATE | - | 2/22/2024 | REVISED | - |

| STATI | E OI | ILLINOIS |
|------------|------|----------------|
| DEPARTMENT | 0F | TRANSPORTATION |

| TRAFFIC | C SIGN | ΑL | MODE | RN | IZATION | PLAN | (SHEET 2 OF 2) |
|--|--------|----|------|----|---------|------|----------------|
| TRAFFIC SIGNAL MODERNIZATION PLAN (SHEET 2 OF 2) IL RTE 83 AND BLUFF RD | | | | | | | |
| SCALE: 1"-20" | SHEET | 2 | OF | 2 | SHEETS | STA | TO STA |

| SECTION | COUNTY | TOTAL SHEETS | SHEE NO. |
|---------|----------|-----------------|-------------|
| N/A | DUPAGE | 87 | 40 |
| | CONTRACT | NO. 6 | 52U73 |



DEPARTMENT OF TRANSPORTATION

IL RTE 83 AT BLUFF RD

SHEET 1 OF 1 SHEETS STA.

CONTRACT NO. 62U73

PROPOSED CONTROLLER SEQUENCE

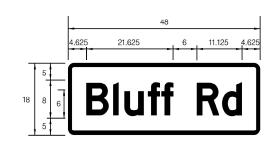
CHECKED -

PLOT DATE = 11/16/2023

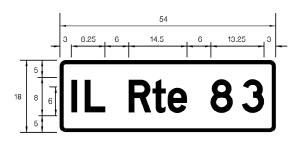
GJG

11/16/2023

REVISED



| DESIGN | AREA | SIGN PANEL | SHEETING | QTY. |
|--------|---------|------------|----------|----------|
| SERIES | (SQ FT) | TYPF | TYPF | REQUIRED |
| D | 6 | 1 | ZZ | 2 |



| DESIGN | AREA | SIGN PANEL | SHEETING | QTY. |
|--------|---------|------------|----------|----------|
| SERIES | (SQ FT) | TYPE | TYPE | REQUIRED |
| D | 6.75 | 1 | ZZ | 2 |

SCHEDULE OF QUANTITIES

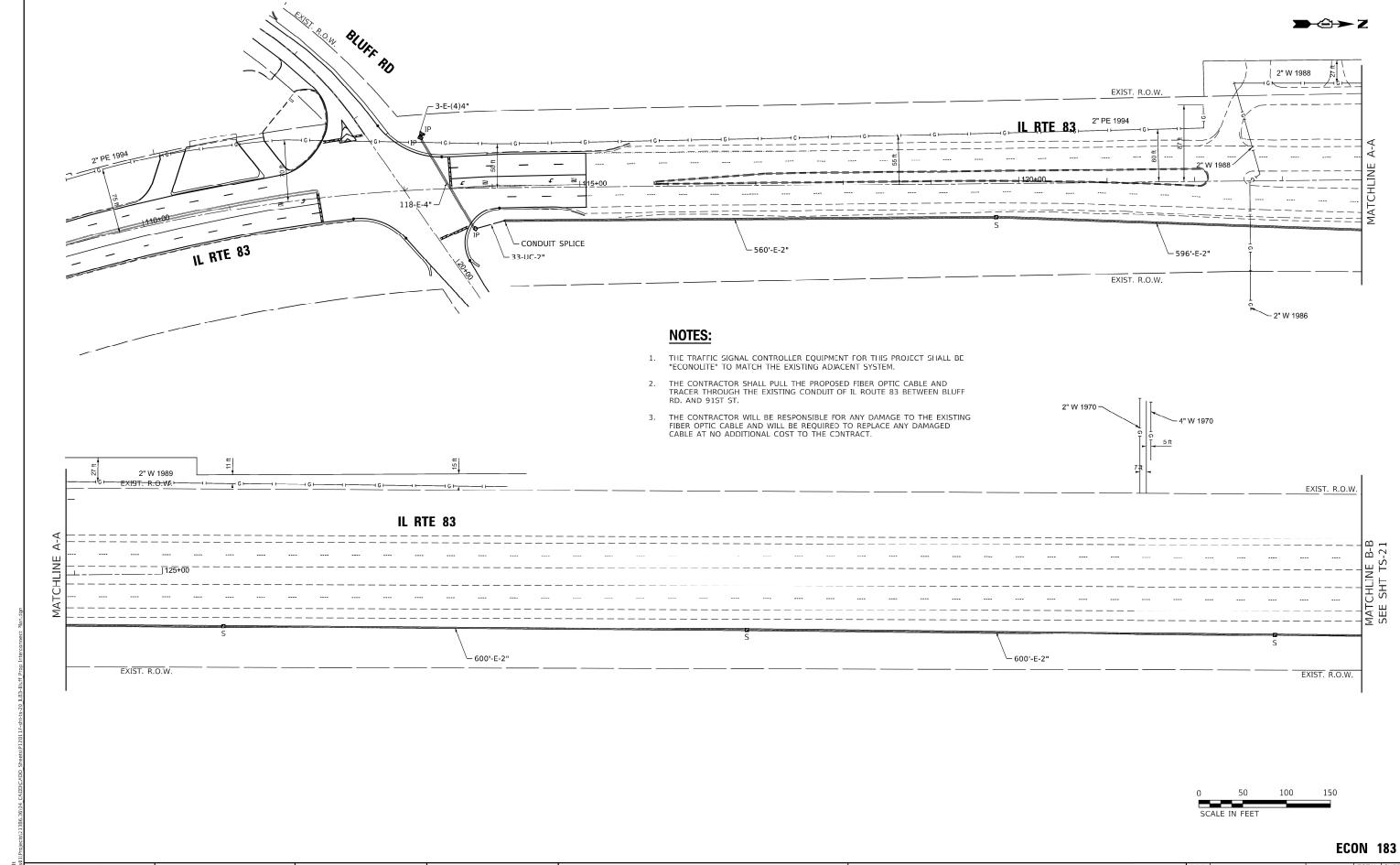
| ITEM DESCRIPTION | UNITS | TOTA QTY |
|--|-------|-------------|
| SIGN PANEL - TYPE 1 | SQ FT | 56 |
| UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA. | FOOT | 742 |
| UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA. | FOOT | 124 |
| UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA. | FOOT | 403 |
| HANDHOLE | EACH | 5 |
| HEAVY-DUTY HANDHOLE | EACH | 2 |
| DOUBLE HANDHOLE | EACH | 1 |
| ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 10 | FOOT | 600 |
| LUMINAIRE, LED, ROADWAY, OUTPUT DESIGNATION G | EACH | 3 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | FOOT | 516 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | FOOT | 4,24 |
| ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR | FOOT | 2,09 |
| ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C | FOOT | 62 |
| ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C | FOOT | 1,30 |
| TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT. | EACH | 1 |
| STEEL MAST ARM ASSEMBLY AND POLE. 38 FT. | EACH | 1 |
| STEEL MAST ARM ASSEMBLY AND POLE, 48 FT. | EACH | 1 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 38 FT. | EACH | 1 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 42 FT. | EACH | 1 |
| CONCRETE FOUNDATION, TYPE A | FOOT | 20 |
| CONCRETE FOUNDATION, TYPE C | FOOT | 4 |
| CONCRETE FOUNCATION, TYPE E 36-INCH DIAMETER | FOOT | 48 |
| SIGNAL HEAD, LED 1-FACE, 3-SECTION, MAST-ARM MOUNTED | EACH | 12 |
| SIGNAL HEAD, LED 1-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 9 |
| TRAFFIC SIGNAL BACKPLATE, RETROREFLECTIVE | EACH | 12 |
| INDUCTIVE LOOP DETECTOR | EACH | 6 |
| DETECTOR LOOP, TYPE I | F001 | 244 |
| LIGHT DETECTOR | EACH | 3 |
| LIGHT DETECTOR AMPLIFIER | EACH | 1 |
| TEMPORARY TRAFFIC SIGNAL INSTALLATION | EACH | 1 1 |
| REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT | EACH | 1 |
| REMOVE EXISTING HANDHOLE | EACH | 10 |
| REMOVE EXISTING DOUBLE HANDHOLE | EACH | 1 |
| REMOVE EXISTING CONCRETE FOUNDATION | EACH | 9 |
| EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C | FOOT | 516 |
| FULL-ACTUATED CONTROLLER AND TYPE SUPER R CABINET (SPECIAL) | EACH | 1 |
| SERVICE INSTALLATION, GROUND MOUNTED, METERED | EACH | 1 |
| VIDEO VEHICLE DETECTION SYSTEM, SINGLE APPROACH | EACH | 2 |
| COMBINATION LIGHTING CONTROLLER | EACH | 1 |
| UNINTERRUPTABLE POWER SUPPLY (SPECIAL) | EACH | 1 |
| | EACH | 21 |
| LED SIGNAL FACE. LENS COVER | | . /1 |

* 100% COST TO TRI-STATE FIRE PROTECTION DISTRICT

TS 580 **ECON 183**

| USER NAME = ggedemer | DESIGNED | - | EEC | REVISED - |
|----------------------------|----------|---|-----------|-----------|
| | DRAWN | - | EEC | REVISED - |
| PLOT SCALE = 2,000 ' / in. | CHECKED | - | GJG | REVISED - |
| PLOT DATE = 2/22/2024 | DATE | - | 2/22/2024 | REVISED - |
| | | | | |

SHT NO. 16



TS SHT NO. 20

SING

 USER NAME
 = ggedernier
 DESIGNED
 EEC
 REVISED

 DRAWN
 EEC
 REVISED

 PLOT SCALE
 = 100.000 ° / in.
 CHECKED
 GJG
 REVISED

 PLOT DATE
 = 2/22/2024
 DATE
 2/22/2024
 REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED INTERCONNECT PLAN (SHEET 1 OF 2)

IL RTE 83 - BLUFF RD TO CENTRAL AVE

SCALE: 1"=50' SHEET 1 OF 2 SHEETS STA. TO STA

 F.A.P. RTE.
 SECTION
 COUNTY SHEETS NO.
 TOTAL SHEETS NO.

 344
 N/A
 DUPAGE
 87
 43

 CONTRACT NO. 62U73

 ILLINOIS FED. AID PROJECT

SCALE: 1"=50' SHEET 2 OF 2 SHEETS STA.



SPLICE 24 FIBER (12SM, 12MM) CABLE TO EXISTING FIBER CABLE TO MAINTAIN EXISTING INTERCONNECT UNTIL PROPOSED INTERCONNECT IS FULLY OPERATIONAL.

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

TEMPORARY INTERCONNECT SCHEMATIC IL RTE 83 - BLUFF RD TO CENTRAL AVE OF SHEETS STA.

COUNTY TOTAL SHEET NO.

DUPAGE 87 45 F.A.P. RTE. SECTION N/A CONTRACT NO. 62U73

→②→ Z

CENTRAL AVE 24F)-___(24F)__ IL RTE 83

ECON 183

ECON 183

SINGH

SINGH

DESIGNED - EEC REVISED - STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

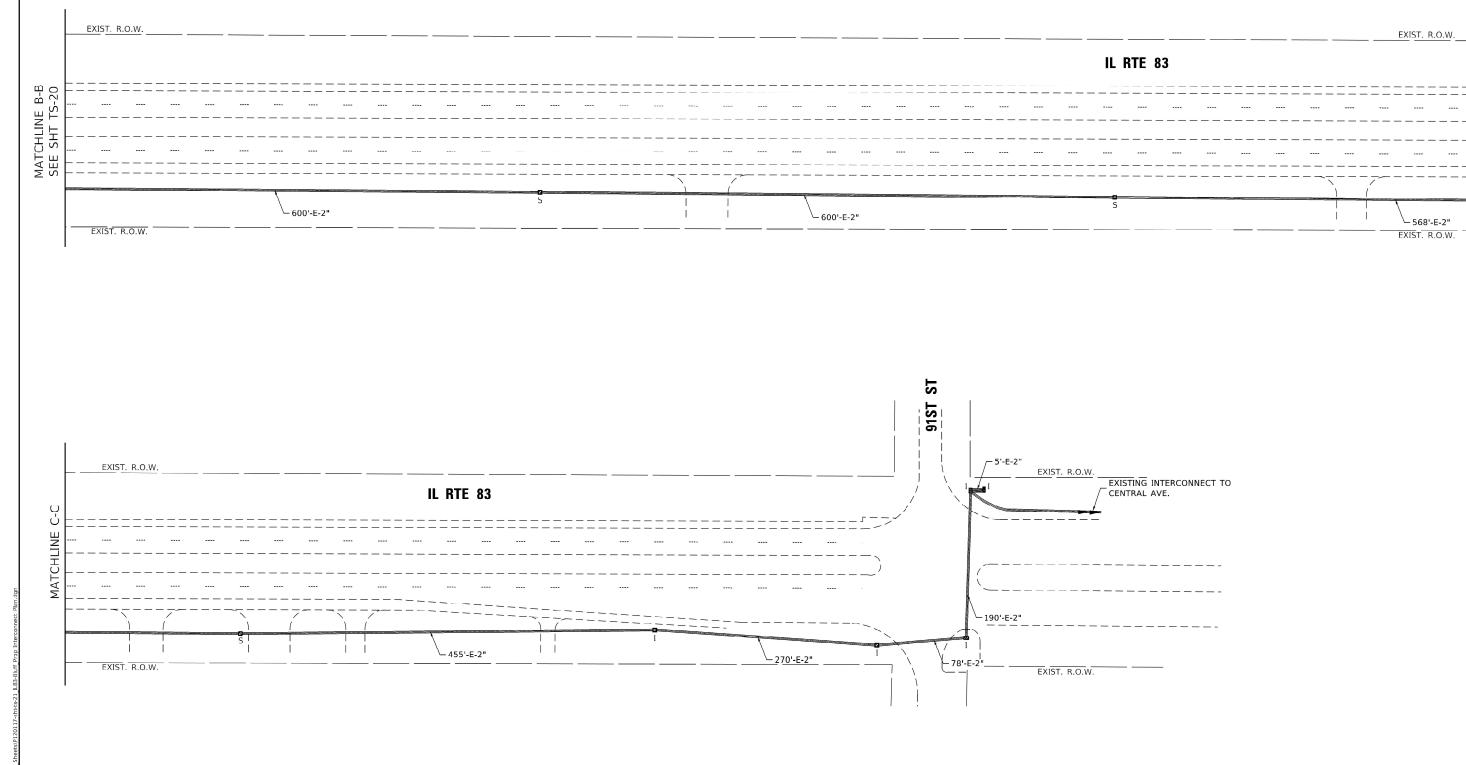
SCALE: 1"=50" SHEET 2 OF 2 SHEETS STA. TO STA. TO STA. ILLINOIS FED. AID PROJECT

SCALE IN FEET

BROWN - DESIGNED - EEC REVISED - SHEETS STA. TO STA. TO STA. ILLINOIS FED. AID PROJECT

SCALE: 1"=50" SHEET 2 OF 2 SHEETS STA. TO STA. ILLINOIS FED. AID PROJECT

SCALE: 1"=50" SHEET 2 OF 2 SHEETS STA. TO STA. ILLINOIS FED. AID PROJECT



NO. 20 SHT S

| USER NAME = ggederner | DESIGNED | - | EEC | REVISED | |
|------------------------------|----------|---|------------|---------|---|
| | DRAWN | - | EEC | REVISED | |
| PLOT SCALE = 100.000 ' / in. | CHECKED | - | GJG | REVISED | - |
| PLOT DATE = 11/16/2023 | DATE | - | 11/16/2023 | REVISED | - |

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

| PRO | POSED | IN | TERC | INC | NECT P | PLAN (SHEET | 1 OF 2) |
|---------------|--------|----|------|------|--------|-------------|---------|
| | IL RTE | 83 | – BL | UFI. | F RD 1 | TO CENTRAL | AVE |
| SCALE: 1"=50' | SHEET | 1 | OF | 2 | SHEETS | STA. | TO STA. |

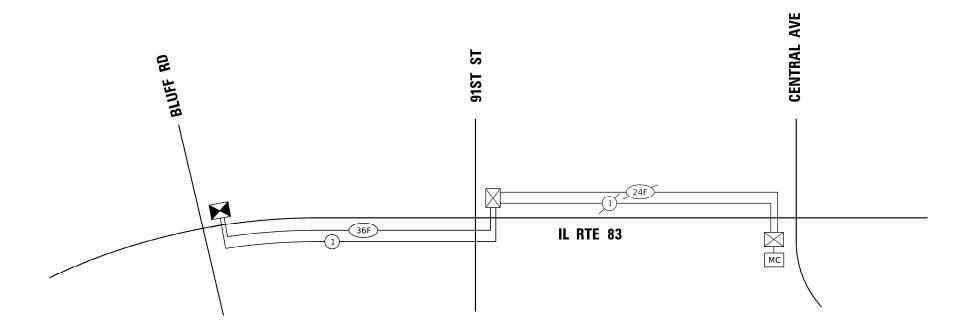
| RTE. | SECT | TION | | COUNTY | SHEETS | NC |
|------|------|----------|--------|------------|--------|------|
| 344 | N/ | A | | DUPAGE | 87 | 47 |
| | | | | CONTRACT | NO. 6 | 52U7 |
| | | ILLINOIS | FED. A | ID PROJECT | | |

PROPOSED INTERCONNECT SCHEMATIC IL RTE 83 - BLUFF RD TO CENTRAL AVE SHEET OF SHEETS STA.

SECTION N/A

344

ECON 183



SCHEDULE OF QUANTITIES

| ITEM DESCRIPTION | UNITS | TOTAL QTY |
|---|-------|--------------|
| UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA. | FOOT | 33 |
| MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION | EACH | 1 |
| TRANSCEIVER - FIBER OPTIC | EACH | 1 |
| ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C | FOOT | 5,500 |
| REMOVE ELECTRIC CABLE FROM CONDUIT | FOOT | 5,500 |
| * ROD AND CLEAN EXISTING CONDUIT | FOOT | 5,000 |
| CONDUIT SPLICE | EACH | 1 |
| FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F | FOOT | 5,500 |
| RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2 | EACH | 1 |

* NOMINAL QUANTITY TO BE USED AS NEEDED AND AS APPROVED BY THE ENGINEER

LIGHTING AND ELECTRICAL LEGEND

| LIGHTIN | IG AND LECTINOAL LEGEND |
|---------------------|--|
| | |
| ₩ | EXISTING LIGHT POLE AND FOUNDATION TO REMAIN, EXISTING LUMINAIRE SAFETY CABLE TO BE REMOVED, EXISTING LUMINAIRE TO BE REMOVED, SALVAGE. |
| o—∰ | INSTALL LUMINAIRE, LED, HORIZONTAL MOUNT, OUTPUT DESTINATION G (240 VAC) AND LUMINAIRE SAFETY CABLE ON EXISTING LIGHT POLE |
| ○ R ≜ | EXISTING IDOT COMBINATION LIGHT TO BE REMOVED |
| <u>0-₩</u> | PROPOSED IDOT LIGHTING UNIT MOUNTED ON PROPOSED TRAFFIC SIGNAL POLE, 45 FT LUMINAIRE M.H. U.N.O., 15 FT MAST ARM, LUMINAIRE TO BE LED HORIZONTAL MOUNT., DESIGNATION G, 120V WITH 6 AMP FUSE & NEUTRAL SLUG |
| T _{Q-4} | PROPOSED IDOT LIGHTING UNIT MOUNTED ON PROPOSED TRAFFIC SIGNAL POLE, 45 FT LUMINAIRE M.H. U.N.O., 2-15 FT MAST ARM, LUMINAIRE TO BE LED HORIZONTAL MOUNT., DESIGNATION G, 120V WITH 6 AMP FUSE & NEUTRAL SLUG |
| | EXISTING UNIT DUCT OR CONDUIT TO REMAIN |
| - R | EXISTING UNDERGROUND UNIT DUCT OR CONDUIT TO BE ABANDONED IN PLACE |
| | PROPOSED UNIT DUCT, SIZE AND TYPE AS NOTED |
| | PROPOSED UNIT DUCT IN GALVANIZED STEEL CONDUIT, SIZE AND TYPE AS NOTED |
| | PROPOSED UNDERGROUND CONDUIT |
| △ _R | EXISTING UTILITY SERVICE TO BE REMOVE |
| <u> </u> | PROPOSED UTILITY SERVICE CONNECTION |
| | EXISTING LIGHTING CONTROLLER, 240/480V, 1-PHASE, 3-WIRE |

PROPOSED SINGLE DOOR LIGHTING CONTROLLER, 240/480V,

1-PHASE, 3-WIRE

| | ABBREVIATIONS |
|----------------|--|
| ABBREVIATION | DESCRIPTION |
| AC | ALTERNATING CURRENT |
| A/C | AERIAL CABLE |
| ATS | ATTACHED TO STRUCTURE |
| B.O.C. CB | BACK OF CURB CIRCUIT BREAKER |
| CKT | CIRCUIT |
| СМ | CENTIMETER |
| COMED | COMMONWEALTH EDISON COMPANY |
| CP | CONTROL PANEL |
| CT DA | CURRENT TRANSFORMER DAVIT ARM |
| DC | DIRECT CURRENT |
| DIA | DIAMETER |
| DP | DISTRIBUTION PANEL |
| E | EXISTING UNIT TO REMAIN |
| EX. | EXISTING ELECTRIC CABLE ASSEMBLY |
| ECA EIS | ELECTRIC CABLE ASSEMBLY EMBEDDED IN STRUCTURE |
| E.O.P. | EDGE OF PAVEMENT |
| F.O.C. | FACE OF CURB |
| FT | FEET OR FOOT |
| FU | FUSE |
| GND | GROUND HIGH INTENSITY DISCHARGE |
| JB | JUNCTION BOX |
| KVA | KILOVOLT-AMPERE |
| KW | KILOWATTS |
| LED | LIGHT EMITTING DIODE |
| LP M | LIGHT POLE METER |
| MA | MAST ARM |
| MC | MULTI-CONDUCTOR |
| ММ | MILLIMETER |
| M.H. | MOUNTING HEIGHT |
| MW NESC | MESSENGER WIRE NATIONAL ELECRIC SAFETY CODE |
| NO. # | NUMBER |
| N.T.S. | NOT TO SCALE |
| Р | PROPOSED |
| PB | PUSH BUTTON |
| PNL PVC | PANEL POLYVINYL CHLORIDE |
| PVCC RGC | PVC COATED RIGID GALVANIZED CONDUIT |
| PT | POTENTIAL TRANSFORMER |
| R | EXISTING UNIT TO BE REMOVED |
| | (OWNER SALVAGED U.N.O.) |
| RR | EXISTING UNIT TO BE REMOVED AND REINSTALLED |
| RECP | RECEPTACLE |
| RGC | RIGID GALVANIZED CONDUIT |
| SEL SW | SELECTOR SWITCH |
| SPARE | SPARE |
| SPACE SS | SPACE STAINLESS STEEL |
| STA | STATION |
| T/F | TOP OF FOUNDATION |
| UD | UNIT DUCT |
| U.N.O. | UNLESS NOTED OTHERWISE |
| UGC, GS VAC | UNDERGROUND CONDUCT, GALVANIZED STEEL VOLTS, ALTERNATING CURRENT |
| W | WATTS |
| WP | WOOD POLE |
| XFMR | TRANSFORMER |
| HPS | HIGH PRESSURE SODIUM |

LOW PRESSURE SODIUM
LIQUID TIGHT FLEXIBLE METALLIC

LTFM

CALL-OUT SAMPLE DEFINITION AND EXAMPLE CONDUIT QUANTITY, SIZE, TYPE, LENGTH 3" DIA. UGC, GS 40' CKT: CONDUCTORS RACEWAY A&B: 3#2 & 1#4 GND 1 ½" DIA. UD CONTROLLER DESIGNATION DESCRIPTION LOCATION

GENERAL NOTES

- THE ELECTRICAL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE LATEST CODES, STANDARDS AND THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 2022, AND SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.
- THE CONTRACTOR SHALL CONTACT MEADE ELECTRIC CO. DISTRICT ONE ELECTRICAL
 MAINTENANCE CONTRACTOR TO LOCATE IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND
 CABLES 773-287-7672.

IDOT-D1 STANDARDS:

STANDARD NO. TITLE

BE-215 LIGHTING CONTROLLER SINGLE DOOR

BE-240 COMBINATION LIGHTING, TRAFFIC SIGNAL SCHEMATIC

BE-701 LUMINAIRE SAFETY CABLE ASSEMBLY
BE-702 MISC. ELECTRICAL DETAILS SHEET A

BE-800 TEMPORARY LIGHT POLE DETAILS

INDEX OF DRAWINGS:

DRAWING NO. LIGHTING LEGEND, GENERAL NOTES, BILL OF MATERIALS AND INDEX OF DRAWINGS LT-01 LT-02 IL ROUTE 83 AT BLUFF ROAD TEMPORARY LIGHTING AND REMOVAL PLAN LT-03 IL ROUTE 83 AT BLUFF ROAD LIGHTING PROPOSED PLAN LT-04 WIRING DIAGRAM - IDOT CONTROLLER "DI" IL ROUTE 83 AT BLUFF ROAD COMBINATION LIGHTING PLAN IDOT D1 STANDARD DRAWING BE-215 LT-06 IDOT D1 STANDARD DRAWING BE-240 LT-08 IDOT D1 STANDARD DRAWING BE-701 IDOT D1 STANDARD DRAWING BE-702 IDOT D1 STANDARD DRAWING BE-800

| ITEM | UNIT | TOTAL QTY |
|---|--------|-----------|
| ELECTRIC SERVICE INSTALLATION | EACH | 1 |
| ELECTRIC UTILITY SERVICE CONNECTION | L SUM | 1 |
| UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA. | FOOT | 50 |
| UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA. | FOOT | 190 |
| UNIT DUCT. 500V. 3-1C NO.4. 1/C NO.4 GROUND. (XLP-TYPE USE). 1 1/4" DIA. POLYETHYLENE | FOOT | 810 |
| ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 2 | FOOT | 150 |
| AERIAL CABLE, 3-1/C NO. 2 WITH MESSENGER WIRE | FOOT | 300 |
| LUMINAIRE, LED, ROADWAY, OUTPUT DESIGNATION G | EACH | 13 |
| LIGHTING CONTROLLER, BASE MOUNTED, 480VOLT, 100AMP | EACH | 1 |
| REMOVAL OF LIGHTING CONTROLLER | EACH | 1 |
| REMOVAL OF ELECTRIC SERVICE INSTALLATION | EACH | 1 |
| REMOVAL OF LIGHTING CONTROLLER FOUNDATION | EACH | 1 |
| REMOVAL OF LUMINAIRE, SALVAGE | EACH | 15 |
| TEMPORARY LUMINAIRE, LED, ROADWAY, OUTPUT DESIGNATION H | EACH | 2 |
| TEMPORARY MAST ARM, ALUMINUM, 15FT | EACH | 2 |
| LUMINAIRE SAFETY CABLE ASSEMBLY | FACH | 16 |
| MAINTENANCE OF LIGHTING SYSTEM | CAL MO | 6 |

BILL OF MATERIALS:

LT-01

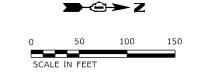
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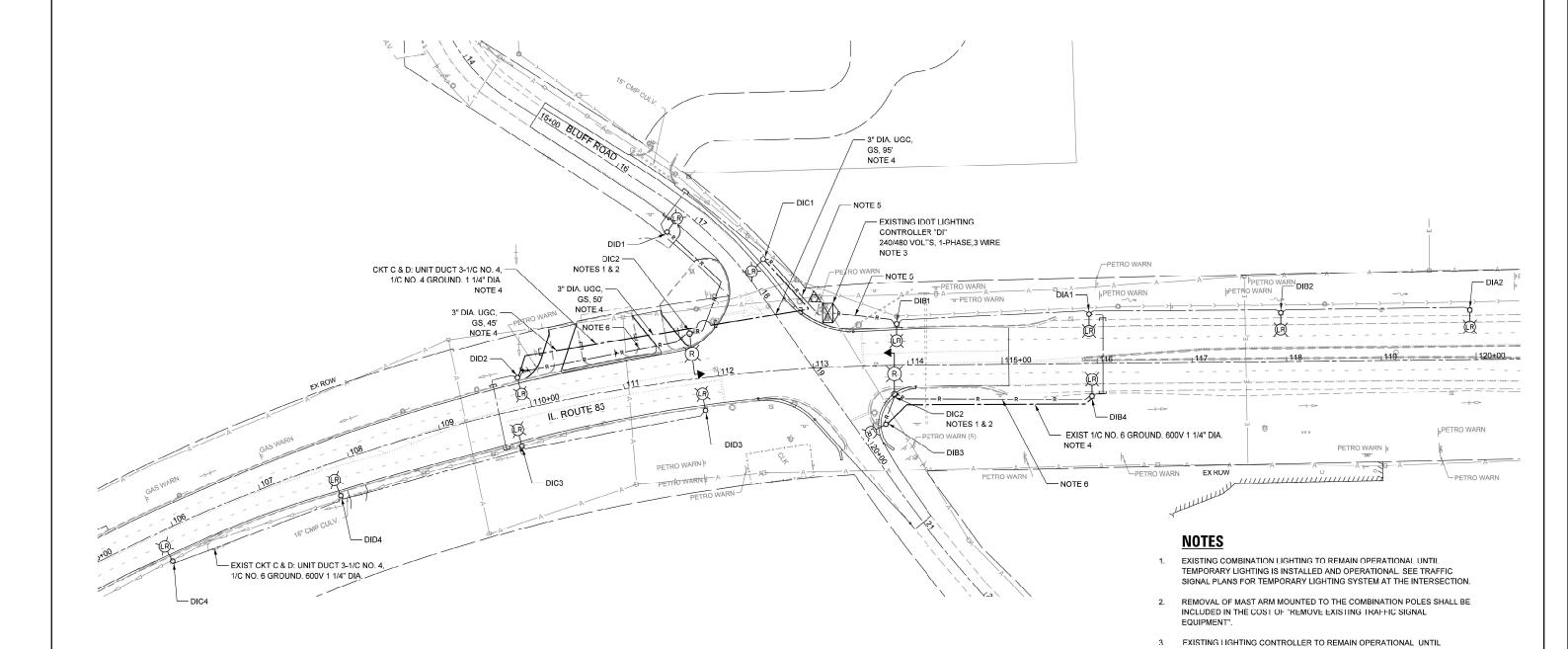
| USER NAME = mgarvida | DESIGNED | - | MAG | REVISED | - |
|------------------------------|----------|---|------------|---------|---|
| | DRAWN | - | MAG/ML | REVISED | - |
| PLOT SCALE = 60.0000 ' / in. | CHECKED | - | SG/MG | REVISED | - |
| PLOT DATE = 11/21/2023 | DATE | - | 11/21/2023 | REVISED | - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:

| | | | | | | | | | | | • |
|---|-------|----|--------|------|---------|-----|--------|-------------|-------------|-----------------|-------------|
| LIGHTING LEGEND, GENERAL NOTES, BILL OF MATERIALS | | | | | | | SECTIO | N | COUNTY | TOTAL SHEETS | SHEE NO. |
| AND INDEX OF DRAWINGS | | | | | | 344 | N/A | | DUPAGE | 87 | 49 |
| AND INDEX OF DRAWINGS | | | | | | | | | CONTRACT | NO. 6 | 52U73 |
| | SHEET | OF | SHEETS | STA. | TO STA. | | ILI | LINOIS FED. | AID PROJECT | | |





UNTIL THE PROPOSED LIGHTING SYSTEM IS OPERATIONAL.

 EXISTING CIRCUIT SHALL REMAIN IN PLACE UNTIL TEMPORARY LIGHTING SYTEM IS INSTALLED AND OPERATIONAL.

PROPOSED LIGHTING CONTROLLER HAS BEEN INSTALLED AND

OPERATIONAL THE SAME DAY BEFORE DUSK.

PROPOSED UNIT DUCT SHALL BE INSTALLED PRIOR TO THE REMOVAL OF EXISTING COMBINATION POLES. THE TEMPORARY LIGHTING SYSTEM SHALL BE DONE IN A WAY THAT THE TEMPORARY LIGHTING SYSTEM BE MADE

EXISTING CIRCUIT SHALL REMAIN IN PLACE DURING CONSTRUCTION AND

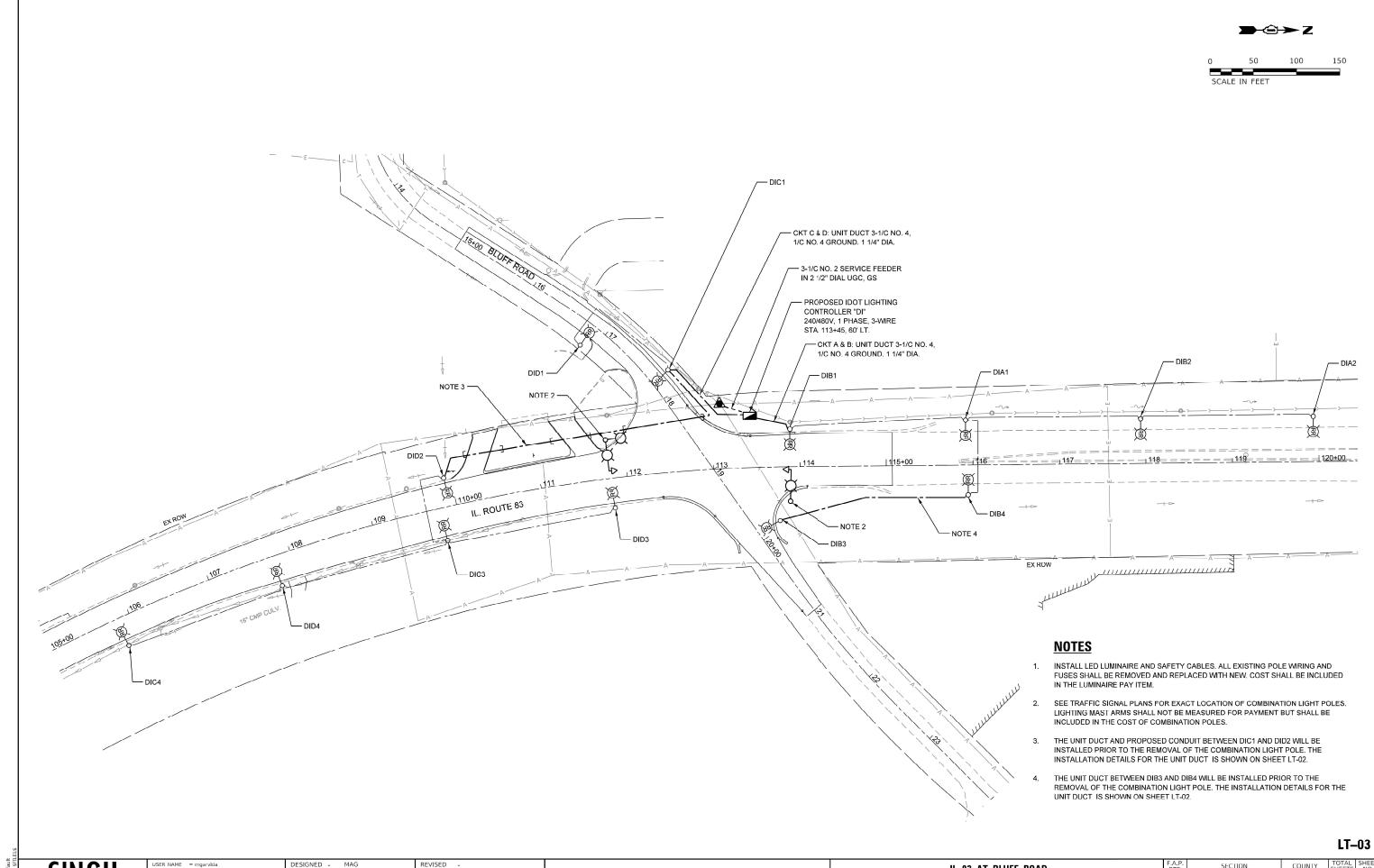
LT-02



| USER NAME = mgarvida | DESIGNED | - | MAG | REVISED | - |
|-------------------------------|----------|---|------------|---------|---|
| | DRAWN | - | MAG/ML | REVISED | - |
| PLOT SCALE = 100,0000 ' / in. | CHECKED | - | SG/MG | REVISED | - |
| PLOT DATE = 11/21/2023 | DATE | _ | 11/21/2023 | REVISED | _ |

SCALE: 1"=50"

OPERATIONAL.



DRAWN -MAG/ML REVISED SG/MG REVISED REVISED

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

IL-83 AT BLUFF ROAD PROPOSED LIGHTING PLAN OF SHEETS STA.

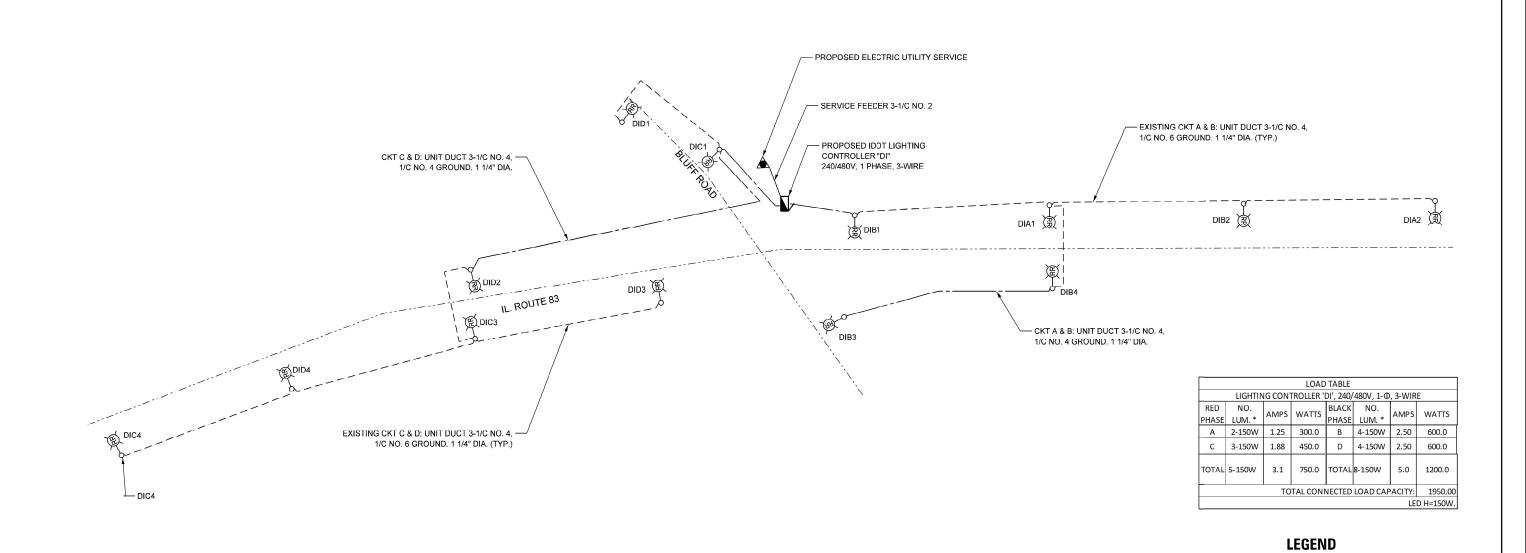
TO STA.

SCALE: 1"=50' SHEET

SECTION COUNTY SHEETS NO.

DUPAGE 87 51 N/A CONTRACT NO. 62U73





LT-04



| USER NAME = mgarvida | DESIGNED | - | MAG | REVISED | - | |
|-------------------------------|----------|---|------------|---------|---|--|
| | DRAWN | - | MAG/ML | REVISED | - | |
| PLOT SCALE = 100,0000 ' / in. | CHECKED | - | SG/MG | REVISED | - | |
| PLOT DATE = 11/21/2023 | DATE | - | 11/21/2023 | REVISED | - | |
| | | | | | | |

| STATE OF ILLINOIS | | | | | | |
|-------------------|-------------------|--|--|--|--|--|
| DEPARTMENT O | OF TRANSPORTATION | | | | | |

| | WIRING | DIAGRAM | – ID | OT CONT | ROLLER |
|--------|--------|---------|--------|---------|---------|
| SCALE: | SHEET | OF | SHEETS | STA. | TO STA. |

| A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEE NO. | | | | | | |
|-------------------|--------------------------|--------|-----------------|-------------|--|--|--|--|--|--|
| 344 | N/A | DUPAGE | 87 | 52 | | | | | | |
| CONTRACT NO. 62U7 | | | | | | | | | | |
| | ILLINOIS FED AID PROJECT | | | | | | | | | |

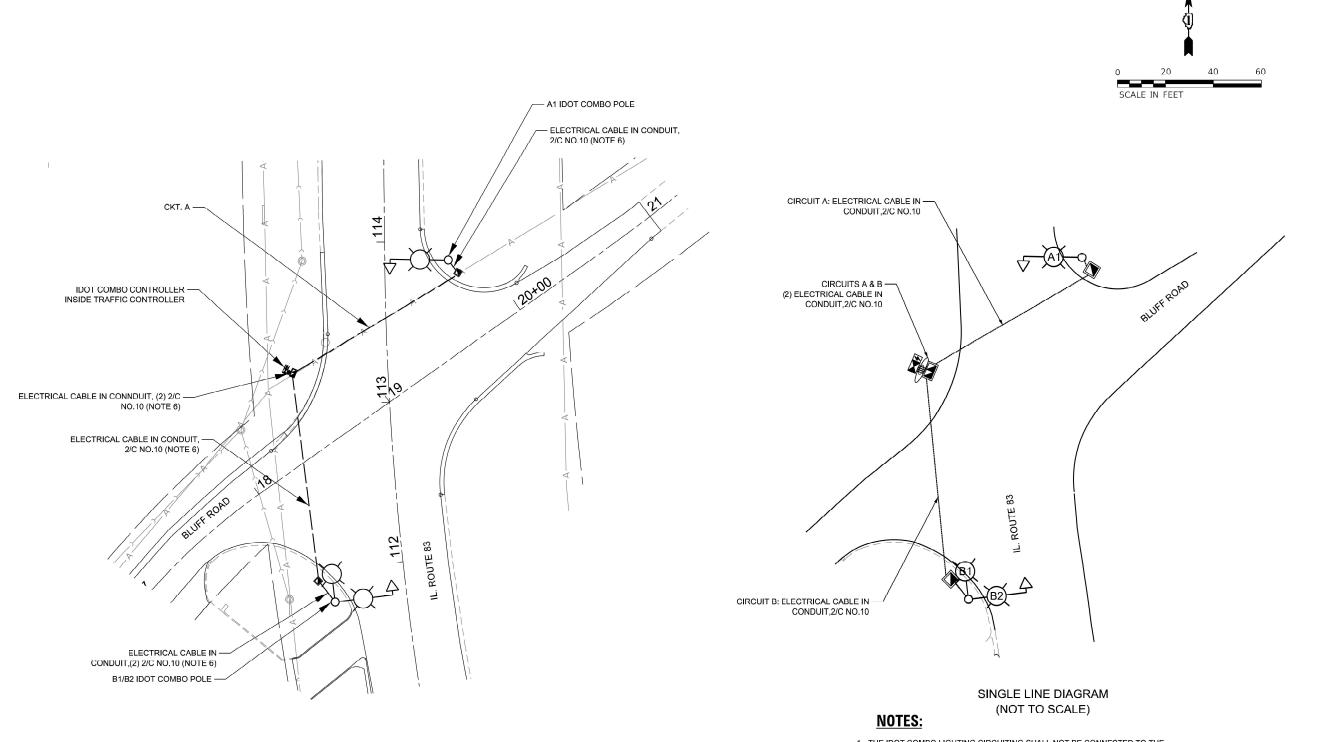
PROPOSED IDOT LED LUMINAIRE

– EXISTING CIRCUIT– PROPOSED CIRCUIT

EXISTING SERVICE FEEDER

PROPOSED ELECTRIC UTILITY SERVICE

PROPOSED IDOT LIGHTING CONTROLLER "DI", 240/480V, 1 PHASE, 3 WIRE



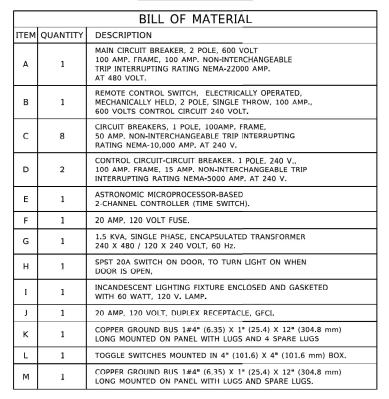
- 1. THE IDOT COMBO LIGHTING CIRCUITING SHALL NOT BE CONNECTED TO THE ROADWAY LIGHTING CIRCUITS.
- 2. LUMINAIRES ARE POWERED FROM IDOT TRAFFIC CONTROLER. SEE BE-240.
- 3. THE COMBO LIGHTING CABLE AND SIGNAL CABLES WILL BE IN SHARED CONDUIT.
- 4. SEE TRAFFIC PLANS FOR LOCATION OF COMBO POLES, HANDHOLES AND CONDUIT.
- 5. CIRCUIT DECALS SHALL NOT BE INSTALLED ON COMBO POLES. CIRCUITING DECALS SHOWN ARE FOR GUIDANCE ONLY.
- 6. ELECTRICAL CABLE SHALL BE ROUTED THROUGH THE TRAFFIC SIGNAL CONDUITS. SEE TRAFFIC SIGNAL PLANS FOR CONDUIT SIZE AND CONDUIT ROUTE.

LT-05



| USER NAME = mgarvida | DESIGNED | - | MAG | REVISED - | |
|------------------------------|----------|---|------------|-----------|--|
| | DRAWN | - | MAG/ML | REVISED - | |
| PLOT SCALE = 80,0000 ' / in. | CHECKED | - | SG/MG | REVISED - | |
| PLOT DATE = 11/21/2023 | DATE | - | 11/21/2023 | REVISED - | |

PANEL EQUIPMENT



NOTE

- 1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- FOUNDATION SIZE SHALL BE COORDINATED WITH CABINET SIZE AND MFR.
- 3. IN FRONT OF CONTROL CABINET DOOR, REMOVE VEGETATION AND 2" (50.8 mm) TOP SOIL, LEVEL THE AREA AND ON TOP, PLACE LENGTH WISE PARALLEL TO CONTROL CABINET, A CONCRETE PAD 36" (914.4 mm) x 60" (18.288 m) x 4" (101 mm) MIN. SIZE. THE COST OF LABOR AND MATERIALS ARE INCLUDED IN THE COST OF THE CONTROLLER.
- DOOR SHALL BE CONSTRUCTED FROM SAME TYPE OF MATERIAL AND THICKNESS AS CABINET.
- 5. DOOR SHALL BE EQUIPPED WITH THREE POINT LATCHING MECHANISM WITH NYLON ROLLERS AT TOP THE BOTTOM.
- DOOR HINGE SHALL BE A HEAVY GAUGE CONTINUOUS HINGE WITH A 1#4" (6.35 mm) DIA. STAINLESS STEEL HINGE PIN.
- 7. ALL EXTERNAL HARDWARE SHALL BE STAINLESS STEEL.
- CONTROL WIRING TO BE #12 AWG, 600V, TYPE "SIS" GRAY SWITCH BOARD WIRE, STRANDED COPPER.
- METER BOX SHALL BE MOUNTED ON THE SIDE OF CONTROL CABINET, NEAR TO THE SERVICE POLE.

- 10. CABINETS SHALL BE PRIMED AND PAINTED AS SPECIFIED.
- 11. THE HEADS OF CONNECTORS SCREWS SHALL
 BE PAINTED WHITE FOR NEUTRAL BAR CONNECTION
 AND GREEN FOR GROUND BAR CONNECTORS.
- 12. ALL WIRING WITHIN THE CABINET SHALL BE COLOR CODED AS INDICATED.

 R = RED

 B = BLACK

 Y = YELLOW

 G = GREEN
- 13. PROVIDE SEALING GROMMETS FOR ALL OPEN WIRING EXTENDED FROM DEVICES IN BOXES OR CABINETS WITHIN THE CONTROL CABINET.
- 14. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.
- 15. THE CONTROLLER SHALL BE CONSTRUCTED TO U.L. STD. 508 AND BEAR THE U.L. LABEL "ENCLOSED INDUSTRIAL CONTROL PANEL".
- 16. 12" (304.8) X 16" (406.4 mm) STAINLESS STEEL EXTERIOR NAMEPLATE SHALL BE ENGRAVED TO "STATE OF ILLINOIS LIGHTING CONTROLS" UNLESS OTHERWISE SPECIFIED.

COUNTY TOTAL SHEE SER NAME = footen DESIGNED . REVISED - 08-20-04 SECTION LIGHTING CONTROLLER STATE OF ILLINOIS DRAWN REVISED . SINGLE DOOR **DEPARTMENT OF TRANSPORTATION** OT SCALE = 50.0000 1 / in CHECKED -EVISED -CONTRACT NO. BE-215 SCALE: NONE SHEET 1

21" MIN. (533.4)

- AUXILIARY CIRCUIT BREAKER

½" (6.35) MINIMUM

MOUNTING PANEL.

- CABINET ENCLOSURE

3-1/C SERVICE ENTRANCE CABLE FROM ELECTRIC

UTILITY METER BOX

240/480 VOLT, 1},3 WIRES, 60 CYCLES.

SCREENED AIR VENT

3 POINT LOCKING

PAD LOCK PROVISION

CONCRETE

WORK PAD

(1.52 m)

GROUNDING CONDUCTOR #2 AWG.

(483) MIN.

ANCHOR RODS, 4 MIN.

COORD. WITH CAEINET

MFR. REQUIREMENTS

SCALE:

NON-ASBESTOS INORGANIC

NONCONDUCTING MATERIAL

— MAIN BREAKER <u>(POWER)</u>

#2/0

(E)

①

(

AUXILIAR

(K)

BONDING JUMPER -

#6 AWG. 600V GROUND ROD %" (15.875)

DIA. x 10' (3.048 m) LONG

PANEL WIRING DIAGRAM

CONTROL RELAY

TWO POSITION

20A, 240V, TYPE SPDT

(762) MIN.

11

36" (914) MIN.

NEMA 3R

50" |

ENCLOUSERE

CABINET ALUMINUM

ALLOY SHEET

0.125" (3.175)

TYPE 5052-H32

1" (25) DIA.

2 PVC CONDUIT

GROUND ROD -

- NAME PLATE (SEE NOTE 16)

12%" (63.5)

GROUND LINE 7

PVC RACEWAYS

6-MIN. COORD.

REQUIREMENTS

WITH CKT

GALV. STEEL

-METER AND BASE

TOGGLE SWITCH

TOGGLE SWITCH MOMENTARY CONTACT TYPE SPDT 20 A, 240V AC AND TOGGLE SWITCH

ha015 days 4/19/2019 2-13-22 PM | Jave-footseri

LT-06

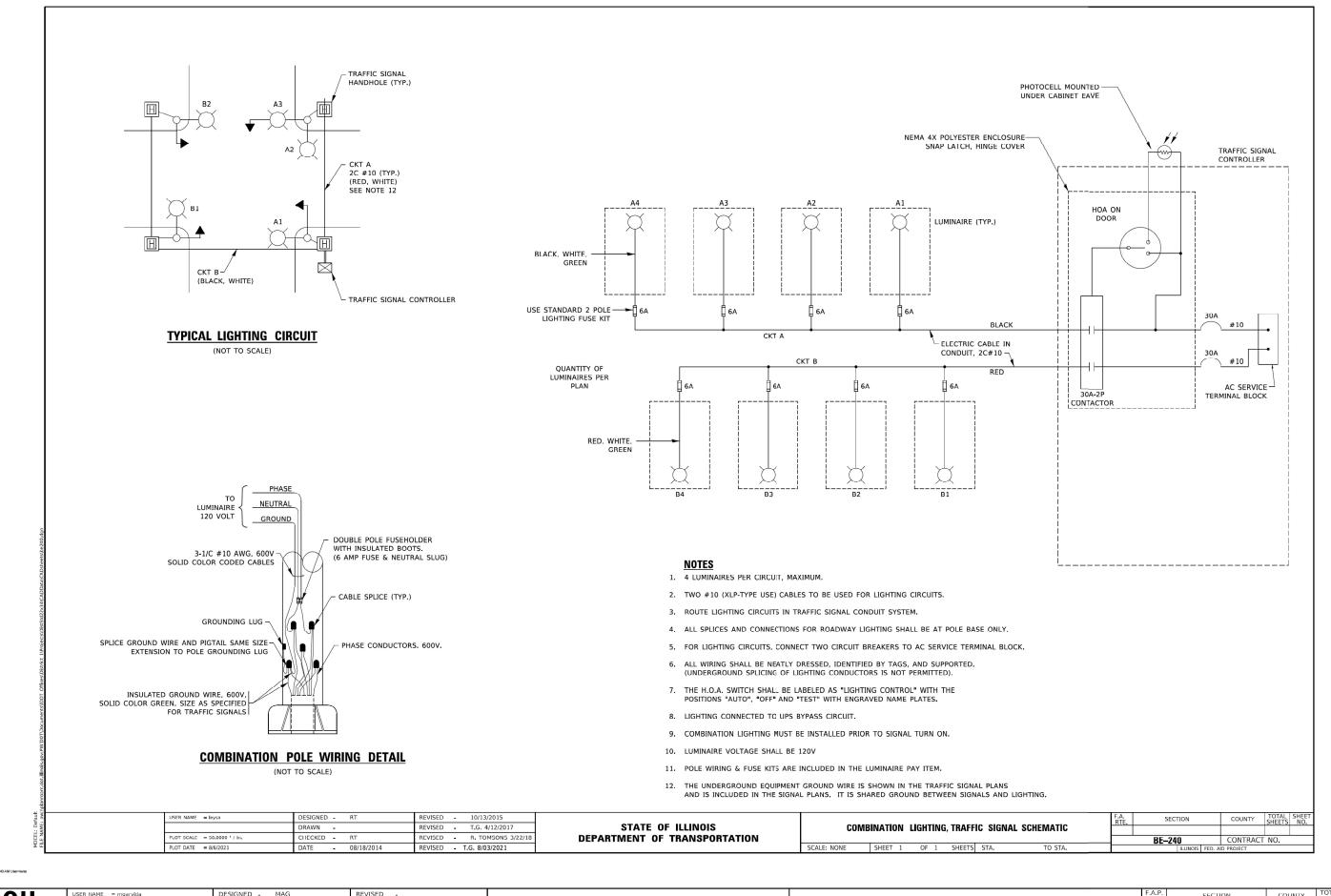


| USER NAME = mgarvida | DESIGNED | - | MAG | REVISED | - |
|-----------------------------|----------|---|------------|---------|---|
| | DRAWN | - | MAG/ML | REVISED | |
| PLOT SCALE = 2,0000 ' / in. | CHECKED | - | SG/MG | REVISED | - |
| PLOT DATE = 11/21/2023 | DATE | - | 11/21/2023 | REVISED | - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | IDOT STA | NDARD | DRAWINGS | |
|-------|----------|--------|----------|--------|
| SHEET | OF | SHEETS | STA. | TO STA |

| E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|----|---------|----------|-----------------|--------------|
| 14 | N/A | DUPAGE | 87 | 54 |
| | | CONTRACT | NO. 6 | 52U73 |
| | | | | |



SINGH

 USER NAME
 = mgarvida
 DESIGNED
 MAG
 REVISED

 PLOT SCALE
 = 2,0000 ' / in
 CHECKED
 SG/MG
 REVISED

 PLOT DATE
 = 11/21/2023
 DATE
 11/21/2023
 REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:

SHEET

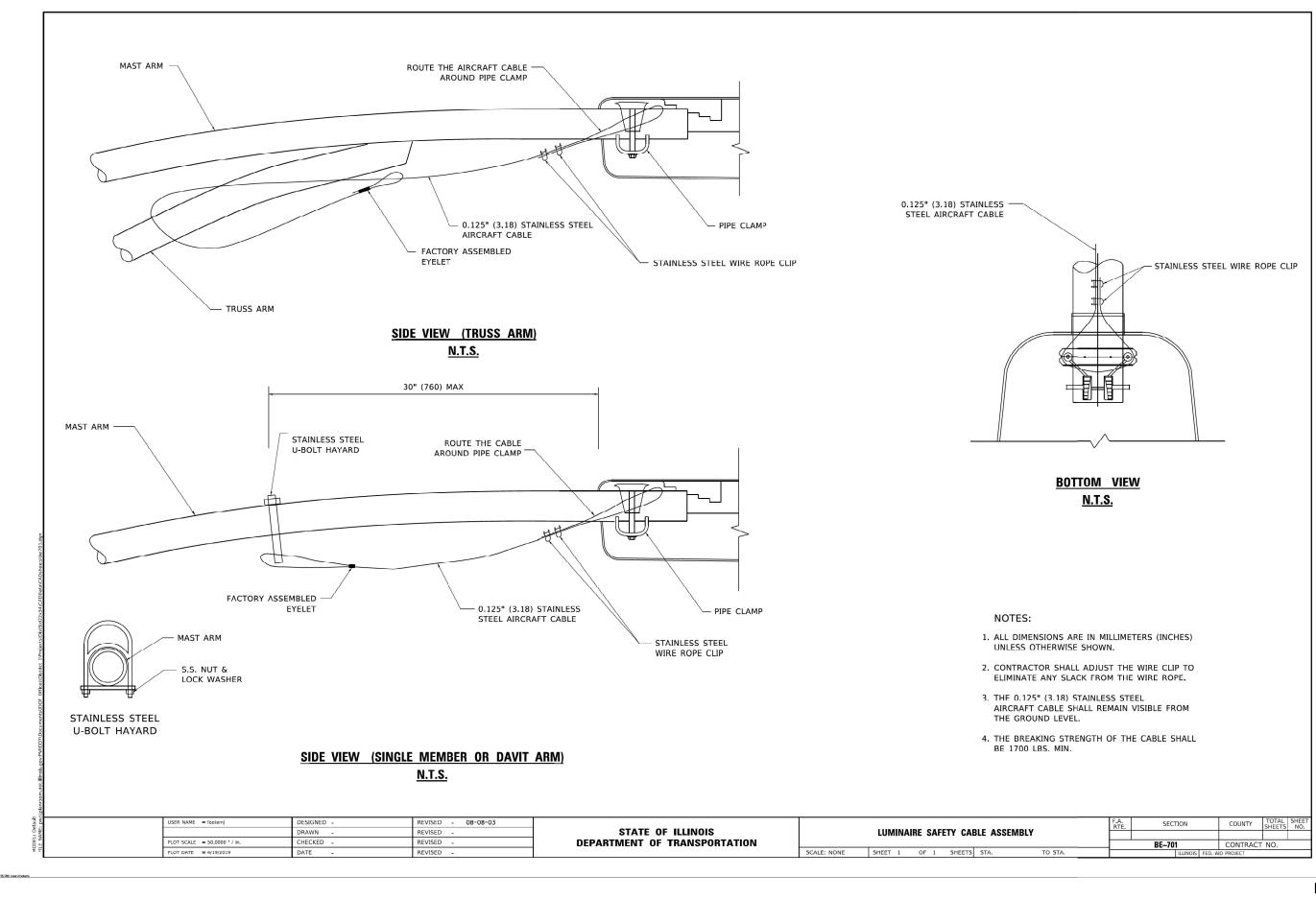
| F.A.P. | SECTION | RTE. | SECTION | RT

LT-07

COUNTY TOTAL SHEET NO.

DUPAGE 87 55

CONTRACT NO. 62U73



LT-08

SINGH

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

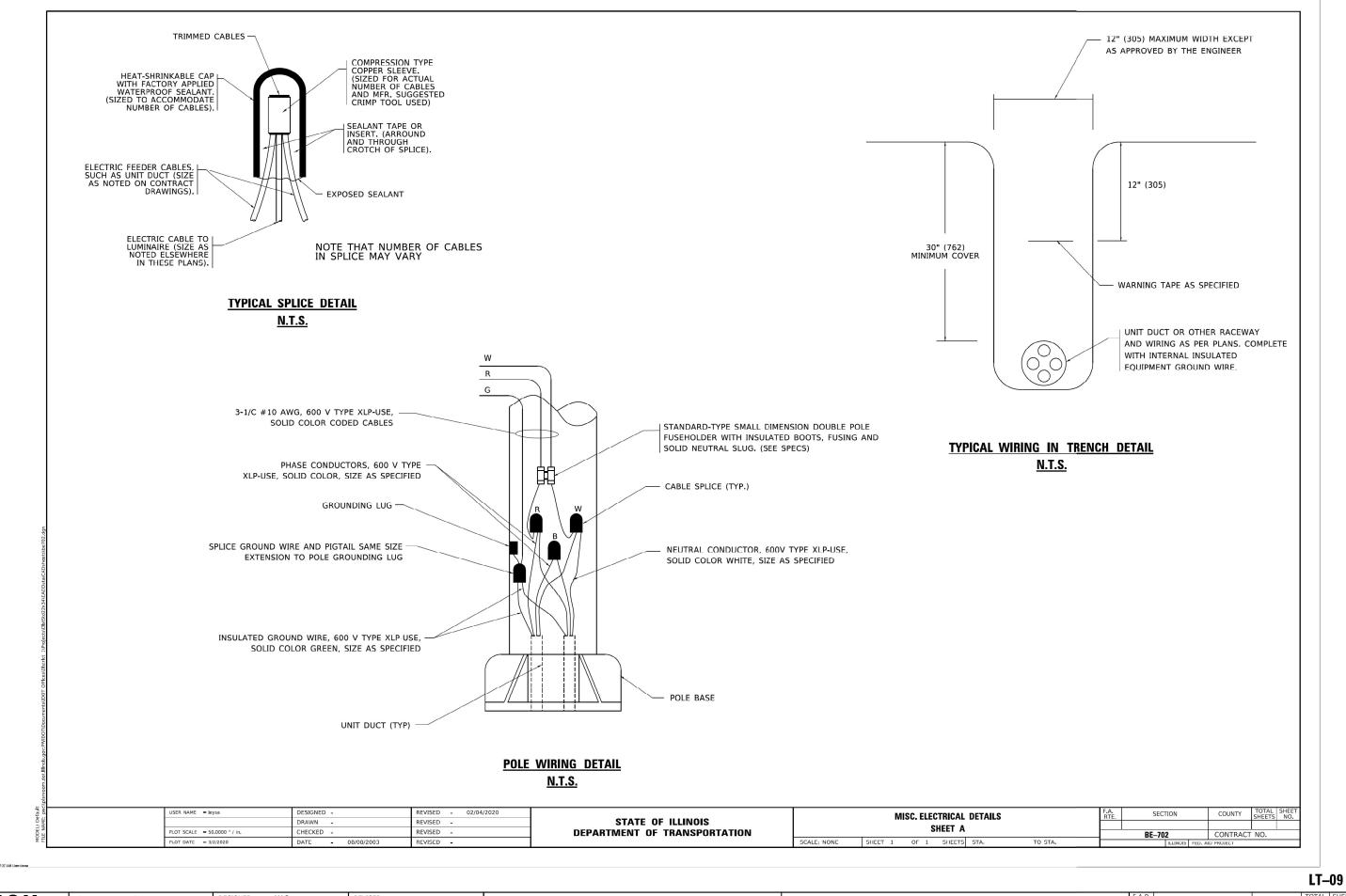
| | IDO | OT STAI | NDARD D | RAWING | s |
|--------|-------|---------|---------|--------|---------|
| SCALE: | SHEET | OF | SHEETS | STA. | TO STA. |

 F.A.P. RTE.
 SECTION
 COUNTY SHEETS
 TOTAL SHEETS
 SHEETS NO.

 344
 N/A
 DUPAGE
 87
 56

 CONTRACT NO. 62U73

 ILLINOIS FED. AID PROJECT



SINGH SINGH + ASSOCIATES, IN CONSULTING ENGINEER
 USER NAME
 = mgarvida
 DESIGNED DRAWN
 MAG
 REVISED

 PLOT SCALE
 = 2,0000 ° / in
 CHECKED
 SG/MG
 REVISED

 PLOT DATE
 = 11/21/2023
 DATE
 11/21/2023
 REVISED

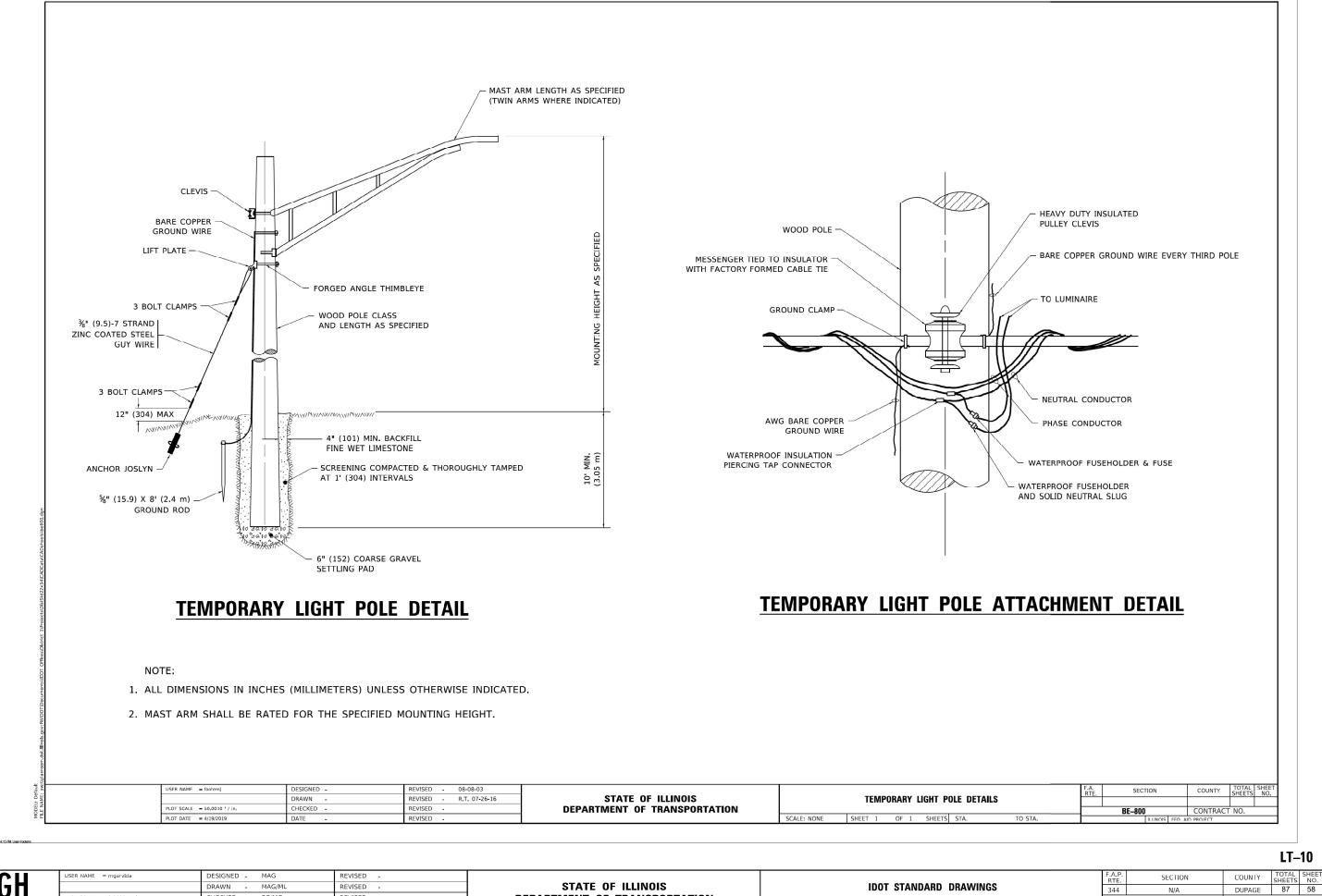
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:

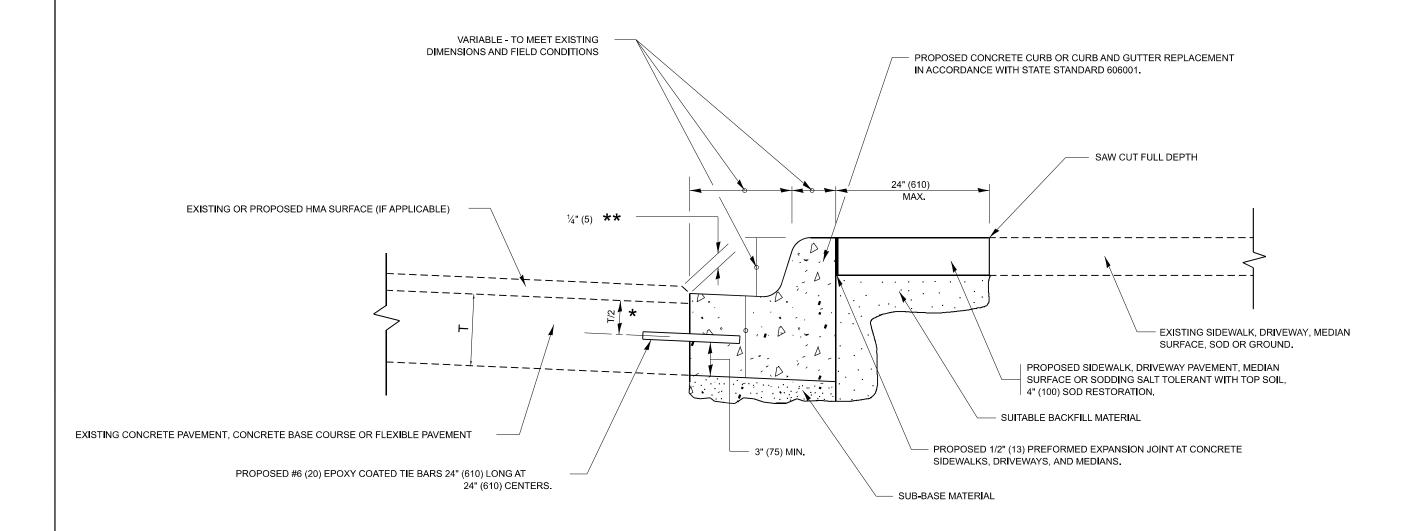
COUNTY TOTAL SHEET NO.

DUPAGE 87 57

CONTRACT NO. 62U73



CONTRACT NO. 62U73

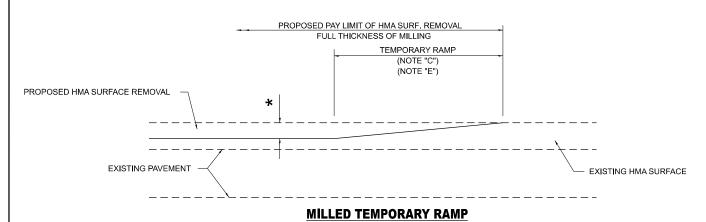


- ★ 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

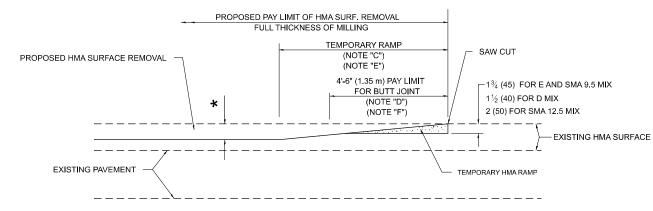
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| USER NAME = Jammal.Alramahi | DESIGNED - A. HOUSEH | REVISED - A. ABBAS 03-21-97 | STATE OF ILLINOIS | | CUR | B OR CL | URB AN | D GUTTER | | F.A.P. RTF | SECTION | | COUNTY | TOTAL | SHEE NO. |
|---------------------------------|----------------------|-----------------------------|------------------------------|-------------|---------|---------|--------|----------|---------|---------------|-----------------|-------------|----------|---------|-------------|
| | DRAWN - | REVISED - M. GOMEZ 01-22-01 | STATE OF ILLINOIS | | | | | LACEMENT | | 344 | FAP 344 23 TSM | | DUPAGE | 87 | 59 |
| PLOT SCALE = 0.16666633 ' / in. | CHECKED - | REVISED - R. BORO 12-15-09 | DEPARTMENT OF TRANSPORTATION | | KEIVI | UVAL AI | ND REF | LACEMENT | | BD | 0600-06 (BD-24) | С | CONTRACT | NO. 62U | J73 |
| PLOT DATE = 2/1/2024 | DATE - 03-11-94 | REVISED - K. SMITH 07-11-19 | | SCALE: NONE | SHEET 1 | OF 1 | SHEETS | STA+ | TO STA+ | | ILLINOIS | FED. AID PR | OJECT | | |



(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

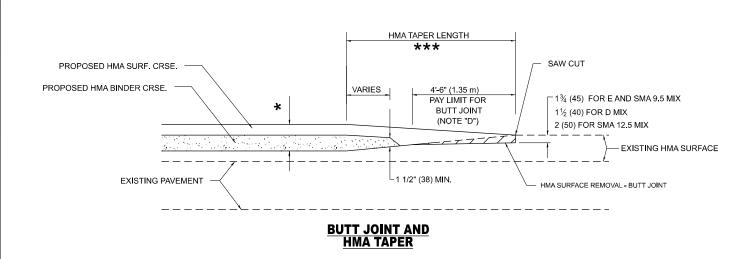


HMA CONSTRUCTED TEMPORARY RAMP

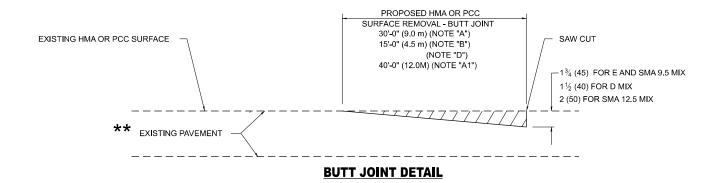
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

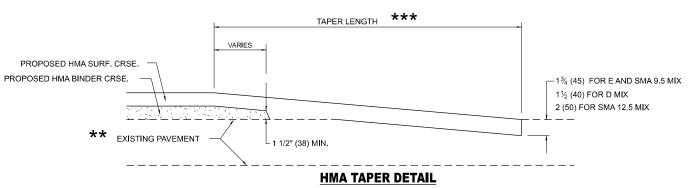
OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING





TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

 $\star\star$ PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

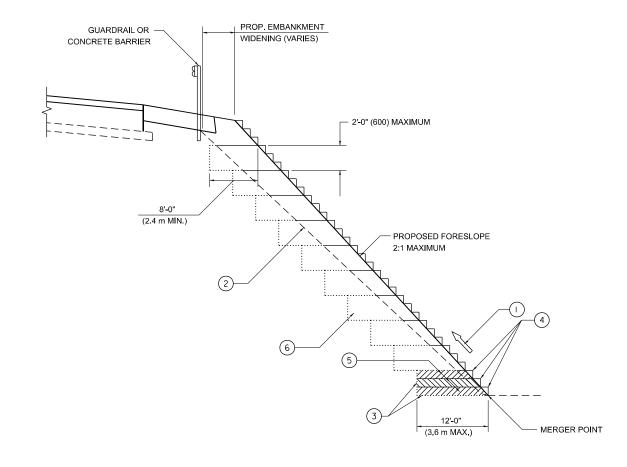
**
20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT"
- THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

JSER NAME = Jammal.Alramahi DESIGNED - M. DE YONG REVISED - A. ABBAS 03-21-97 **BUTT JOINT AND STATE OF ILLINOIS** REVISED -DRAWN M. GOMEZ 04-06-01 FAP 344 23 TSM DUPAGE 87 60 **HMA TAPER DETAILS** CHECKED -**DEPARTMENT OF TRANSPORTATION** BD400-05 BD-32 CONTRACT NO. 62U73 SHEET 1 OF 1 SHEETS STA. SCALE: NONE REVISED - K. SMITH 11-18-22 TO STA. DATE



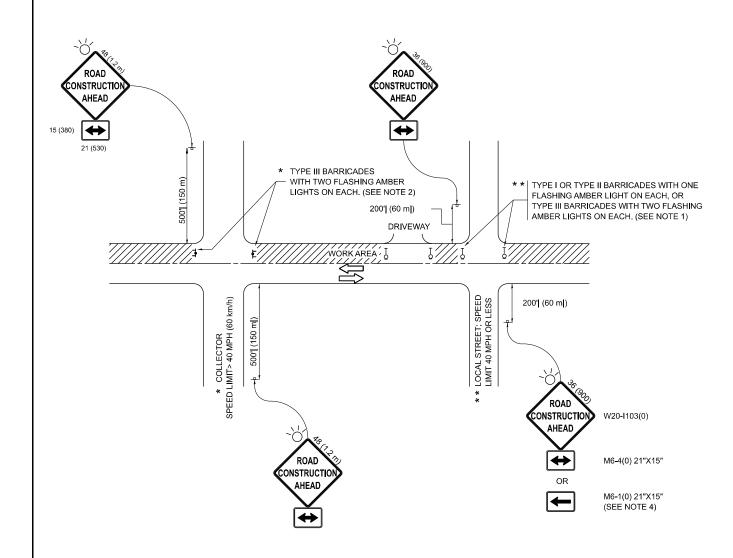
TYPICAL BENCHING DETAIL FOR EMBANKMENT

NOTES:

- CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- 2 EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- 3 BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- 4 TRIM TO FINAL SLOPE.
- 5 EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- 7 SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| USER NAME = Jammal.Alramahi | DESIGNED - | REVISED - | | | | RENC | HING D | FTAII | | F.A.P. | SECTION | COUNTY | TOTAL | SH |
|---------------------------------|------------------|-----------|------------------------------|--|--|------|--------|-------|-------|--------------------|---------|---------|-------|----|
| | DRAWN - CADD | REVISED - | | | | | | | | 344 FAP 344 23 TSM | | DUPAGE | 87 | |
| PLOT SCALE = 0.16666633 ' / in. | CHECKED - S.E.B. | REVISED - | DEPARTMENT OF TRANSPORTATION | FOR EMBANKMENT WIDENING | | | | | BD-51 | CONTRACT | NO. 62 | J73 | | |
| PLOT DATE = 2/1/2024 | DATE - 06-16-04 | REVISED - | | STATE OF ILLINOIS BENCHING DETAIL FOR EMBANKMENT WIDENING BENCHING DETAIL 344 FAP 344 23 TSM DUPAGE 87 | | | | | | | | | | |



NOTES:

- 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h)
 AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500" (150 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

- 5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- 6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- 7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

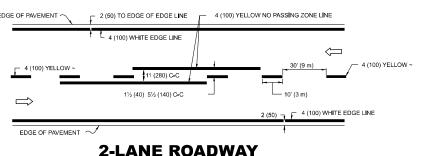
All dimensions are in inches (millimeters) unless otherwise shown.

| USER NAME = Jammal.Alramahi | DESIGNED - L.H.A. | REVISED - A. HOUSEH 10-15-96 |
|---------------------------------|-------------------|---------------------------------|
| | DRAWN - | REVISED - T. RAMMACHER 01-06-00 |
| PLOT SCALE = 0.16666633 ' / in. | CHECKED - | REVISED - A. SCHUETZE 07-01-13 |
| PLOT DATE = 2/1/2024 | DATE - 06-89 | REVISED _ A. SCHUETZE 09-15-16 |

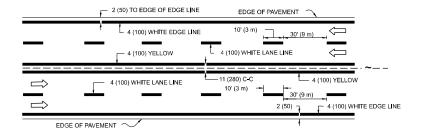
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| TRAFFIC (E ROADS, | | | | | |
|-----------------------|----|--------|------|---|---------|
| SHEET | OF | SHEETS | STA. | + | TO STA. |

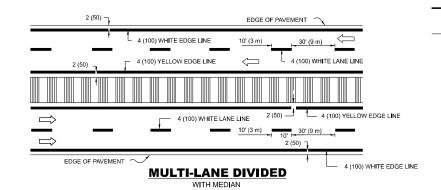
| | F.A.P. RTE | SEC | TION | | COUNTY | TOTAL SHEETS | SHE |
|---|---------------|---------|----------|--------|--------|-----------------|-----|
| ı | 344 | FAP 344 | 23 TSM | DUPAGE | 87 | 62 | |
| 4 | | TC-10 | CONTRACT | NO. 62 | J73 | | |
| ı | | | | | | | |



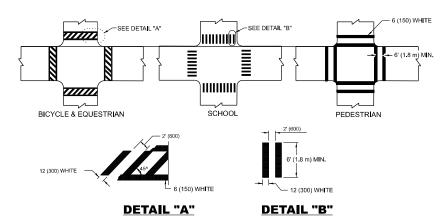
2-LANE ROADWAY



MULTI-LANE UNDIVIDED



TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

TWO-4 (100) YELLOW @ 11 (280) C-C 4' (1.2 m) OUTS DE TO NO DIAGONALS

@ 10' (3 m) OR LESS SPACING

8 (200) WHITE -

ISLAND OFFSET FROM PAVEMENT EDGE

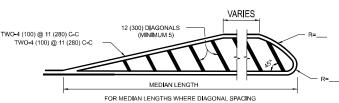
8 (200) WHITE -

ISLAND AT PAVEMENT EDGE

RAISED

TWO-4 (100) YELLOW @ 11 (280) C-C

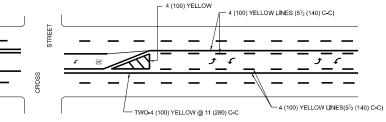
4' (1.2 m) WIDE MEDIANS ONLY



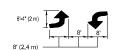
CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

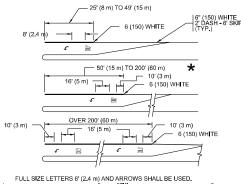
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

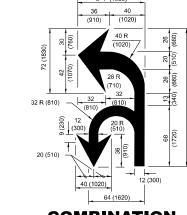


 $\begin{tabular}{ll} FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. \\ & AREA = 15.6 SQ. FT. (1.5 m ^2) \\ \hline \end{tabular} AREA = 20.8 SQ. FT. (1.9 m ^2) \\ \end{tabular}$

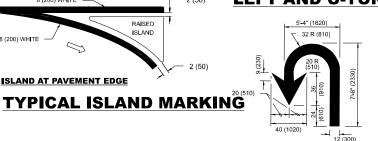
TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



COMBINATION **LEFT AND U-TURN**



LANE REDUCTION **TRANSITION**

U-TURN

★ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR

D(FT)

SPEED LIMIT

50

| | | | | GREATER OR WHEN SPECIFIED IN PLANS. |
|---|---|------------------------------------|---|---|
| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 @ 4 (100) | SOLID SOLID | YELLOW YELLOW | 5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) G-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MEDIANS IN YELLOW |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4" (1.2 m) NADVANCE OF AND PARALLE TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESINED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "X"=3.8 SQ. FT. (0.33 m 2) EACH "X"=54.0 SQ. FT. (5.0 m 2) |
| SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS \geq 8') | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h)) |
| U TURN ARROW | SEE DETAIL | SOLID | WHITE | 16.3 SF |
| 2 ARROW COMBINATION LEFT AND U TURN | SEE DETAIL | SOLID | WHITE | 30.4 SF |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

SCALE: NONE

All dimensions are in inches (millimeters) unless otherwise shown.

| USER NAME = Jammal.Alramahi | DESIGNED - EVERS | REVISED | - | C. JUCIUS 09-09-09 |
|---------------------------------|------------------|---------|---|--------------------|
| | DRAWN - | REVISED | - | C. JUCIUS 07-01-13 |
| PLOT SCALE = 0.16666633 ' / in. | CHECKED - | REVISED | - | C. JUCIUS 12-21-15 |
| PLOT DATE = 2/1/2024 | DATE - 03-19-90 | REVISED | - | C. JUCIUS 04-12-16 |

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

| | DIST | TRICT O | NE | | F.A.P. RTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|----------------------------------|---------|---------|----------|--|---------------|-------------------|-------------|-----------------|--------------|
| TVDI | CAL DAV | EMENT | MARKINGS | | 344 | FAP 344 23 TSM | DUPAGE 87 6 | | 63 |
| | OAL IAV | LIVILIA | MARKINGS | | | TC-13 | CONTRACT | NO. 62 | U73 |
| SHEET 1 OF 1 SHEETS STA+ TO STA+ | | | | | | ILLINOIS FED. AII | D PROJECT | | |

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

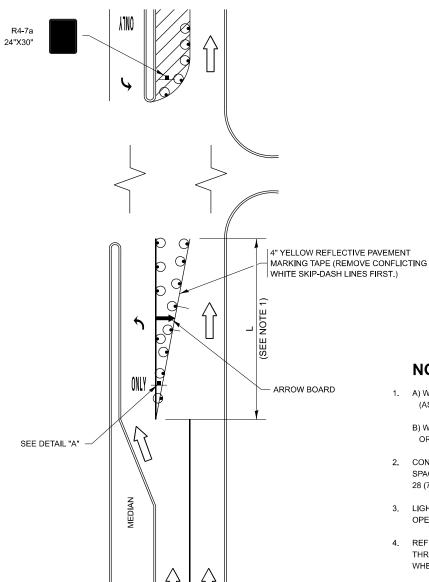


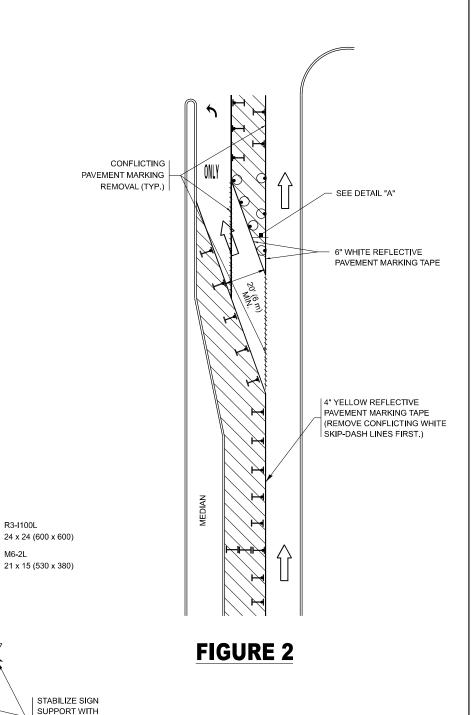
FIGURE 1

LEGEND WORK AREA LANE OPEN TO TRAFFIC ARROW BOARD TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT DRUM WITH STEADY BURN LIGHT SIGN ASSEMBLY TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

- 1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
 - B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- 2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- 3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- 4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- 5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- 7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
- 8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE



DETAIL A

R3-I100L

M6-2L

All dimensions are in inches (millimeters) unless otherwise shown.

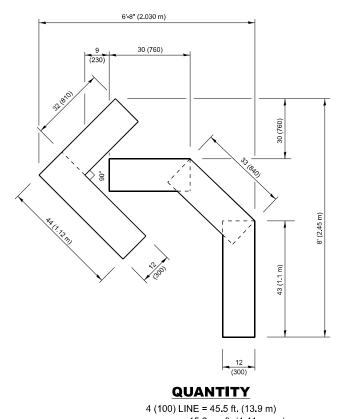
REVISED - R. BORO 09-14-09 DRAWN - A. HOUSEH 11-07-95 REVISED - A. SCHUFTZF 07-01-13 CHECKED - A. HOUSEH 10-12-96 REVISED - A. SCHUETZE 09-15-16 PLOT DATE = 2/1/2024 DATE - T RAMMACHER 01-06-00 REVISED

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHEET 1 OF 1 SHEETS STA. __+_. TO STA.

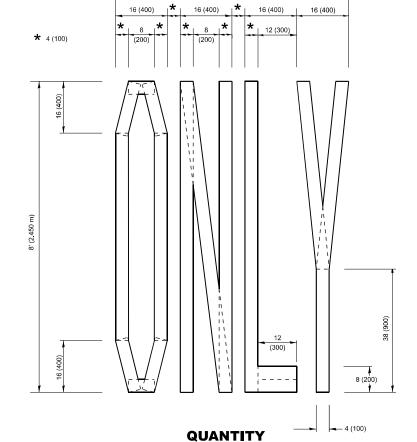
SANDBAGS AS

SECTION FAP 344 23 TSM DUPAGE 87 64 TC-14 CONTRACT NO. 62U73

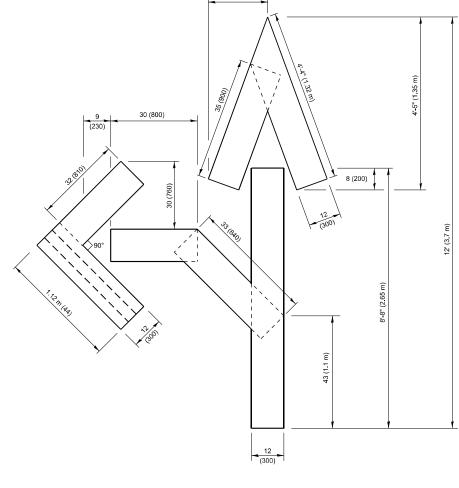
JSER NAME = Jammal.Alramahi DESIGNED - T. RAMMACHER 09-08-94



15.2 sq. ft. (1.41 sq. m)



4 (100) LINE = 64.1 ft. (19.5 m)

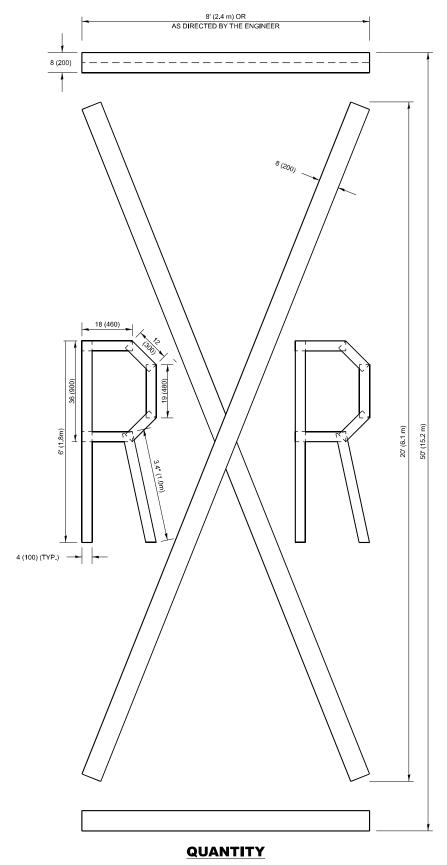


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m) 27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



4 (100) LINE = 225.9 ft. (68.9 m) 75.3 sq. ft. (6.99 sq. m)

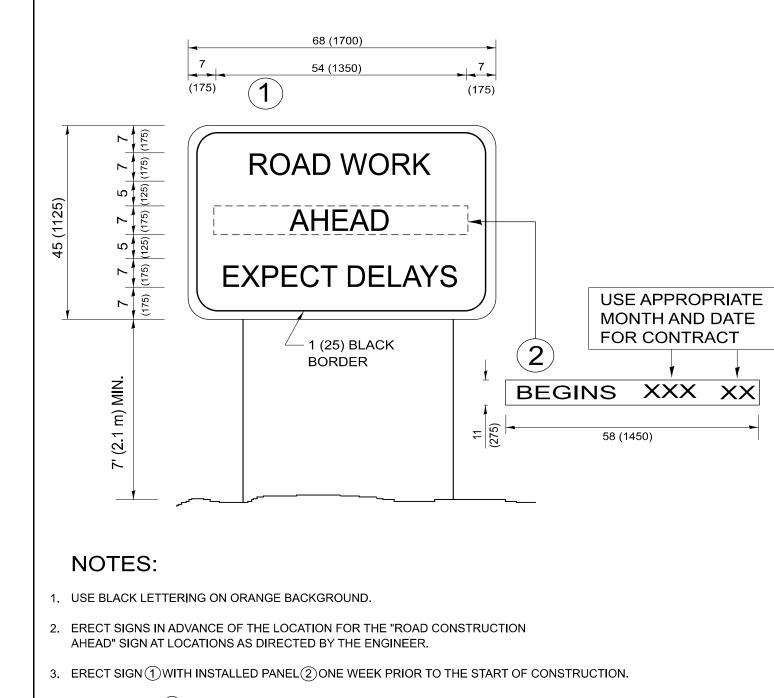
> All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = Jammal.Alramahi DESIGNED -REVISED - T. RAMMACHER 03-02-98 DRAWN REVISED - E. GOMEZ 08-28-00 CHECKED -REVISED - E. GOMEZ 08-28-00 PLOT DATE = 2/1/2024 DATE - 09-18-94 REVISED - A. SCHUETZE 09-15-16

21.4 sq. ft. (1.99 sq. m)

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

| SHORT TE | RM PAVEI | MENT M | F.A.P. RTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | | | |
|-------------|----------|--------|---------------|---------|----------------|-----------------|------------------|-----------|--------|-----|
| | | | | 344 | FAP 344 23 TSM | DUPAGE | 87 | 65 | | |
| | | | | | | | TC-16 | CONTRACT | NO. 62 | U73 |
| SCALE: NONE | SHEET 1 | OF 1 | SHEETS | STA+ | TO STA+ | | ILLINOIS FED. AI | D PROJECT | | |



- 4. REMOVE PANEL 2) SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)

SCALE: NONE

7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

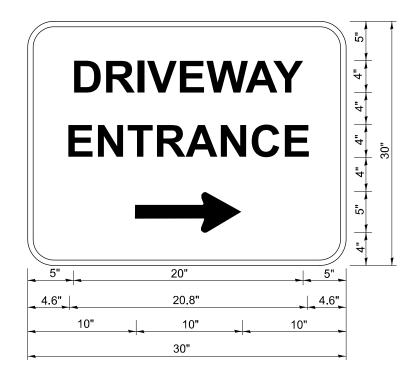
| USER NAME = Jammal.Alramahi | DESIGNED - | REVISED | R. MIRS 09-15-97 |
|---------------------------------|------------|---------|---------------------------------------|
| | DRAWN - | REVISED | - R. MIRS 12-11-97 |
| PLOT SCALE = 0.16666633 ' / in. | CHECKED - | REVISED | - T. RAMMACHER 02-02-99 |
| PLOT DATE = 2/1/2024 | DATE - | REVISED | C.JUCIUS 01-31-07 |

| ARTERIAL ROAD | | | | | | | F.A.P. RTE | SECT | | COUNTY | ; | | |
|------------------|---------|------|--------|-----|---|---------|----------------|------|--|----------|----------|---------|---|
| INFORMATION SIGN | | | | | | 344 | FAP 344 23 TSM | | | DUPAGE | Τ | | |
| | | | | | | TC-22 | | | | CONTRACT | | | |
| | SHEET 1 | OF 1 | SHEETS | STA | + | TO STA. | + | | | ILLINOIS | FED, AIL | PROJECT | Т |

/P12011 //CADData/CADSheets/P12011 / -shi

,

EL: TC-26 [Sheet]



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

- 1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
- 2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
- 3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

| USER NAME = Jammal.Alramahi | DESIGNED - | REVISED | - | C. JUCIUS 02-15-07 |
|---------------------------------|------------|---------|---|--------------------|
| | DRAWN - | REVISED | - | |
| PLOT SCALE = 0.16666633 ' / in. | CHECKED - | REVISED | - | |
| PLOT DATE = 1/30/2024 | DATE - | REVISED | - | |

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

SCALE: NONE

DRIVEWAY ENTRANCE SIGNING FAP 344 23 TSM DUPAGE 87 67 TC-26 CONTRACT NO. 62U73 SHEET 1 OF 1 SHEETS STA.

