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GREATER ROCKFORD AIRPORT AUTHORITY ROCKFORD, ILLINOIS

CONSTRUCTION PLANS FOR CHICAGO ROCKFORD INTERNATIONAL AIRPORT

REHABILITATE RUNWAY 1/19 PHASE 3

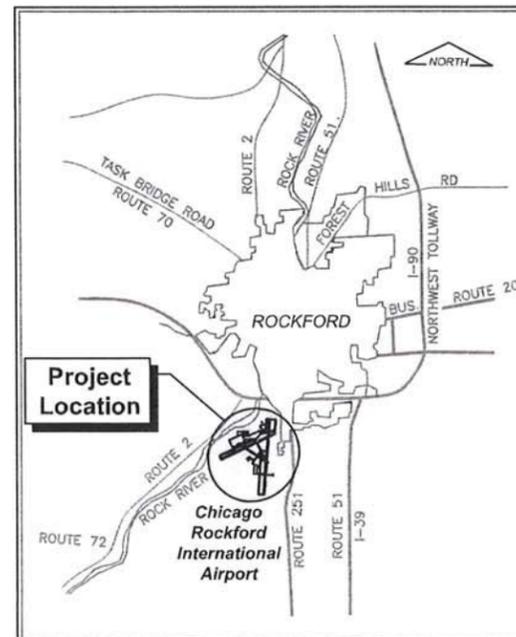
FINAL SUBMITTAL

**ILLINOIS PROJECT: RFD-4360
A.I.P. PROJECT: 3-17-0088-XX**

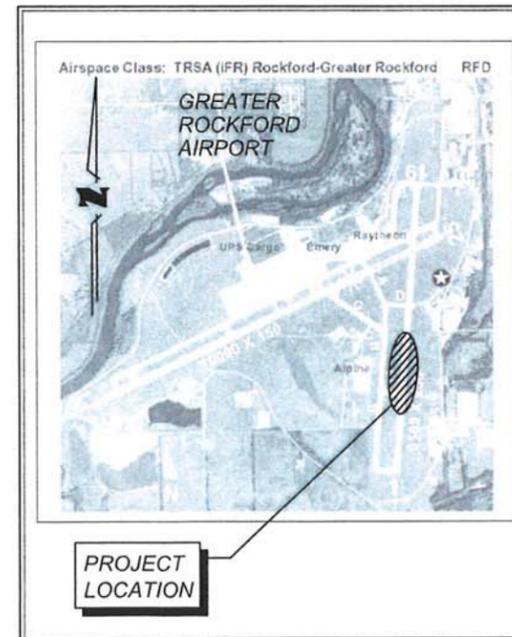
APRIL 19, 2013
(JUNE 14, 2013 LETTING)



Expires 11-30-2013



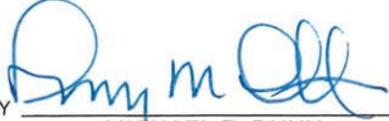
LOCATION MAP



SITE PLAN



Greater Rockford Airport Authority

APPROVED BY 
MICHAEL P. DUNN
DIRECTOR

DATE APRIL 19, 2013

DESIGN INFORMATION

TOWNSHIP: 43 NORTH COUNTY: WINNEBAGO
RANGE: 1 EAST ROCKFORD TOWNSHIP

ALP DATA: APPROACH CAT. / DESIGN GROUP: D5
DESIGN AIRCRAFT: 747-400
(MAXIMUM EQUIPMENT HEIGHT = 25')



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1-800-892-0123**

Illinois Professional Engineering Practice Act of 1989



SUBMITTED BY 
Travis A. Strait, P.E.

DATE 4/19/13
license expires 11-30-2013



MICHAEL P. DUNN
DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4360

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:

SITE PLAN AND CONTROL

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

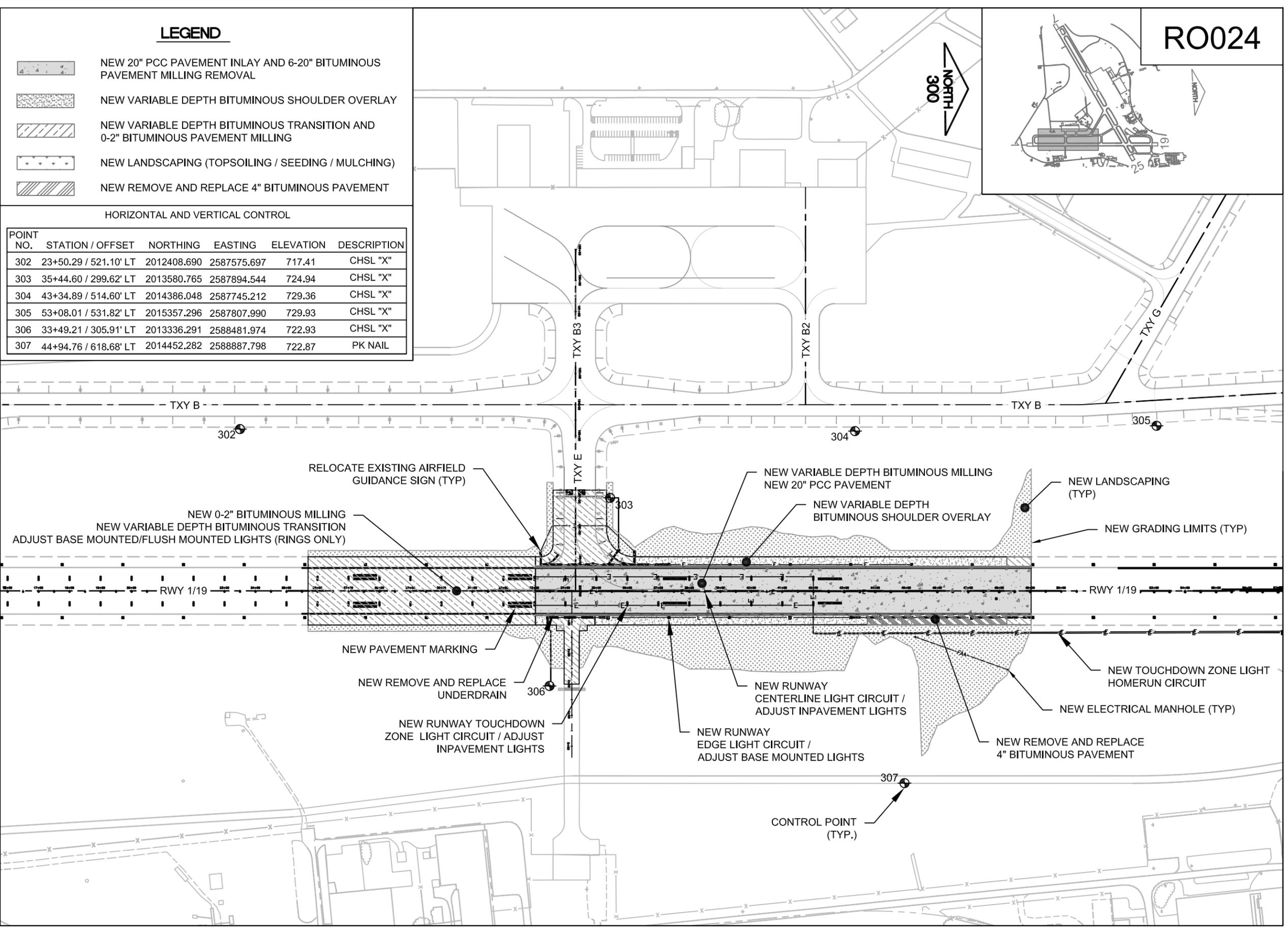
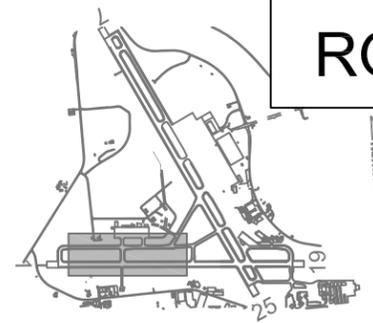
PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. REVISION

SPL1

SHEET 3 OF 65 SHEETS

RO024



LEGEND

- NEW 20" PCC PAVEMENT INLAY AND 6-20" BITUMINOUS PAVEMENT MILLING REMOVAL
- NEW VARIABLE DEPTH BITUMINOUS SHOULDER OVERLAY
- NEW VARIABLE DEPTH BITUMINOUS TRANSITION AND 0-2" BITUMINOUS PAVEMENT MILLING
- NEW LANDSCAPING (TOPSOILING / SEEDING / MULCHING)
- NEW REMOVE AND REPLACE 4" BITUMINOUS PAVEMENT

HORIZONTAL AND VERTICAL CONTROL

POINT NO.	STATION / OFFSET	NORTHING	EASTING	ELEVATION	DESCRIPTION
302	23+50.29 / 521.10' LT	2012408.690	2587575.697	717.41	CHSL "X"
303	35+44.60 / 299.62' LT	2013580.765	2587894.544	724.94	CHSL "X"
304	43+34.89 / 514.60' LT	2014386.048	2587745.212	729.36	CHSL "X"
305	53+08.01 / 531.82' LT	2015357.296	2587807.990	729.93	CHSL "X"
306	33+49.21 / 305.91' LT	2013336.291	2588481.974	722.93	CHSL "X"
307	44+94.76 / 618.68' LT	2014452.282	2588887.798	722.87	PK NAIL



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International Airport
Rockford, Illinois

Greater Rockford
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AIP Proj: 3-17-0088-XX
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APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

SHEET TITLE:
**TYPICAL SECTION -1
RWY 1/19**

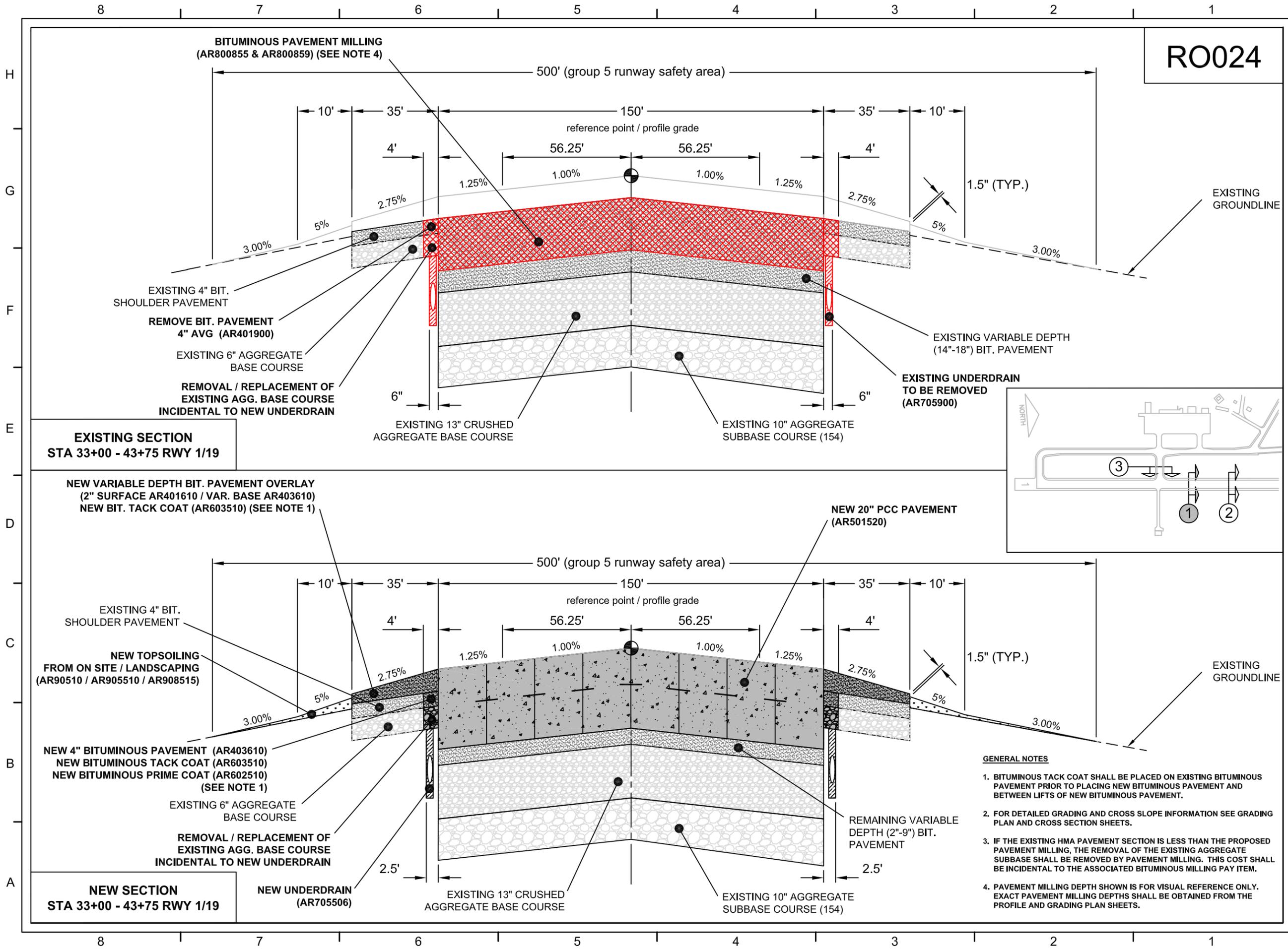
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PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **TYP1** REVISION

SHEET 4 OF 65 SHEETS

RO024



GENERAL NOTES

1. BITUMINOUS TACK COAT SHALL BE PLACED ON EXISTING BITUMINOUS PAVEMENT PRIOR TO PLACING NEW BITUMINOUS PAVEMENT AND BETWEEN LIFTS OF NEW BITUMINOUS PAVEMENT.
2. FOR DETAILED GRADING AND CROSS SLOPE INFORMATION SEE GRADING PLAN AND CROSS SECTION SHEETS.
3. IF THE EXISTING HMA PAVEMENT SECTION IS LESS THAN THE PROPOSED PAVEMENT MILLING, THE REMOVAL OF THE EXISTING AGGREGATE SUBBASE SHALL BE REMOVED BY PAVEMENT MILLING. THIS COST SHALL BE INCIDENTAL TO THE ASSOCIATED BITUMINOUS MILLING PAY ITEM.
4. PAVEMENT MILLING DEPTH SHOWN IS FOR VISUAL REFERENCE ONLY. EXACT PAVEMENT MILLING DEPTHS SHALL BE OBTAINED FROM THE PROFILE AND GRADING PLAN SHEETS.



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DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
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APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:

PHASING PLAN
SUMMARY

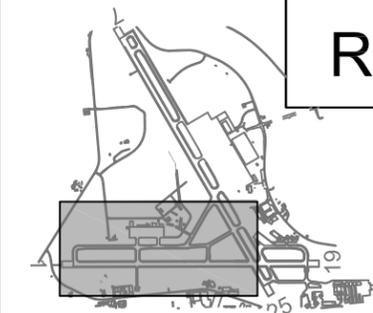
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PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **PPM1** REVISION

SHEET 7 OF 65 SHEETS

RO024



CRITICAL POINTS

POINT NO.	LATITUDE	LONGITUDE	ELEV.*
A	42° 11' 54.67"	89° 05' 11.86"	751.03
B	42° 11' 53.43"	89° 05' 19.68"	755.30
C	42° 11' 34.21"	89° 05' 24.79"	757.52
D	42° 11' 22.97"	89° 05' 32.80"	752.23
E	42° 11' 16.06"	89° 05' 41.18"	746.40

* ELEVATION INCLUDES 25' ANTICIPATED
MAXIMUM EQUIPMENT HEIGHT

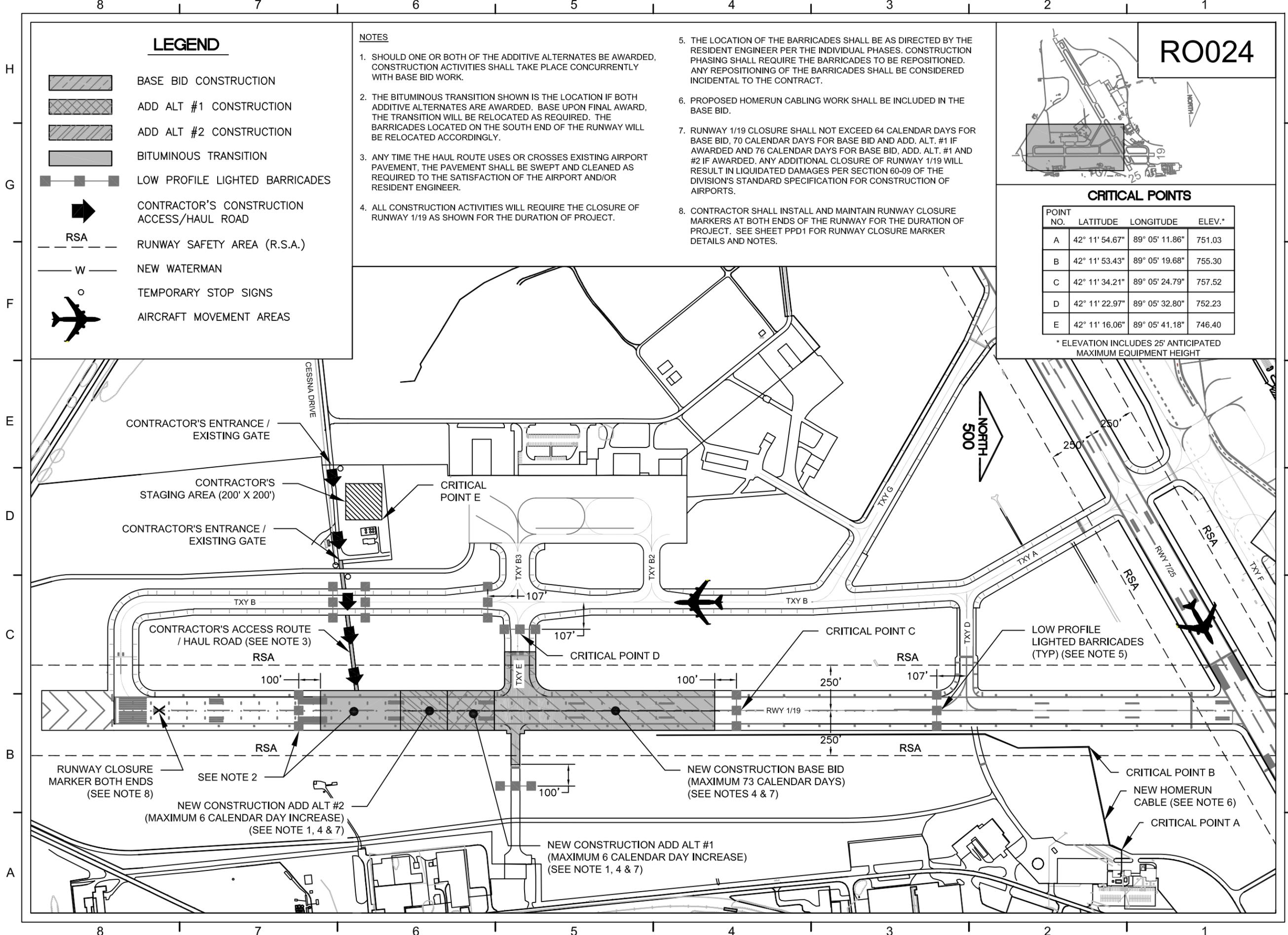
- THE LOCATION OF THE BARRICADES SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER PER THE INDIVIDUAL PHASES. CONSTRUCTION PHASING SHALL REQUIRE THE BARRICADES TO BE REPOSITIONED. ANY REPOSITIONING OF THE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- PROPOSED HOMERUN CABLING WORK SHALL BE INCLUDED IN THE BASE BID.
- RUNWAY 1/19 CLOSURE SHALL NOT EXCEED 64 CALENDAR DAYS FOR BASE BID, 70 CALENDAR DAYS FOR BASE BID AND ADD. ALT. #1 IF AWARDED AND 76 CALENDAR DAYS FOR BASE BID, ADD. ALT. #1 AND #2 IF AWARDED. ANY ADDITIONAL CLOSURE OF RUNWAY 1/19 WILL RESULT IN LIQUIDATED DAMAGES PER SECTION 60-09 OF THE DIVISION'S STANDARD SPECIFICATION FOR CONSTRUCTION OF AIRPORTS.
- CONTRACTOR SHALL INSTALL AND MAINTAIN RUNWAY CLOSURE MARKERS AT BOTH ENDS OF THE RUNWAY FOR THE DURATION OF PROJECT. SEE SHEET PPD1 FOR RUNWAY CLOSURE MARKER DETAILS AND NOTES.

NOTES

- SHOULD ONE OR BOTH OF THE ADDITIVE ALTERNATES BE AWARDED, CONSTRUCTION ACTIVITIES SHALL TAKE PLACE CONCURRENTLY WITH BASE BID WORK.
- THE BITUMINOUS TRANSITION SHOWN IS THE LOCATION IF BOTH ADDITIVE ALTERNATES ARE AWARDED. BASE UPON FINAL AWARD, THE TRANSITION WILL BE RELOCATED AS REQUIRED. THE BARRICADES LOCATED ON THE SOUTH END OF THE RUNWAY WILL BE RELOCATED ACCORDINGLY.
- ANY TIME THE HAUL ROUTE USES OR CROSSES EXISTING AIRPORT PAVEMENT, THE PAVEMENT SHALL BE SWEEPED AND CLEANED AS REQUIRED TO THE SATISFACTION OF THE AIRPORT AND/OR RESIDENT ENGINEER.
- ALL CONSTRUCTION ACTIVITIES WILL REQUIRE THE CLOSURE OF RUNWAY 1/19 AS SHOWN FOR THE DURATION OF PROJECT.

LEGEND

- BASE BID CONSTRUCTION
- ADD ALT #1 CONSTRUCTION
- ADD ALT #2 CONSTRUCTION
- BITUMINOUS TRANSITION
- LOW PROFILE LIGHTED BARRICADES
- CONTRACTOR'S CONSTRUCTION ACCESS/HAUL ROAD
- RSA
- RUNWAY SAFETY AREA (R.S.A.)
- NEW WATERMAN
- TEMPORARY STOP SIGNS
- AIRCRAFT MOVEMENT AREAS



PHASING PLAN PROJECT NOTES:

PHASING PLAN GENERAL NOTES:

RO024

1. APPROVED PROGRESS SCHEDULE:
PRIOR TO THE START OF CONSTRUCTION, AN APPROVED PROGRESS SCHEDULE SHALL BE EXECUTED BY THE CONTRACTOR AND MONITORED BY THE RESIDENT ENGINEER. THIS SCHEDULE SHALL SHOW START/ STOP DATES OF ALL PHASES, INCLUDING ALL AOA WORK REQUIRING DAYTIME ONLY CLOSURES OF TAXIWAYS AND RUNWAYS. THE APPROVED PROGRESS SCHEDULE SHALL BE DISTRIBUTED TO ALL PARTIES AT LEAST 3 WORKING DAYS PRIOR TO START OF CONSTRUCTION. THE CONTRACTOR MAY BE REQUIRED TO SUBMIT A REVISED PROGRESS SCHEDULE TO ACCOMODATE AIRPORT EVENTS (I.E. AIRSHOW). SHOULD A REVISED SCHEDULE BE REQUIRED, THE REVISION SHALL BE COMPLETED AT NO ADDITIONAL COST TO THE CONTRACT.

2. NOTAM (NOTICE TO AIRMEN) COORDINATION:
THE RESIDENT ENGINEER SHALL COORDINATE NOTAM AND FAA FACILITY COORDINATION WITH AIRPORT / FAA PERSONNEL.

3. CONSTRUCTION SITE ACCESS AND STAGING AREA:
THE CONTRACTOR ACCESS ROAD AND STAGING AREAS SHALL BE AS SHOWN ON THE REFERENCED PLAN. THE CONTRACTOR SHALL MAINTAIN AND REPAIR THE CONSTRUCTION ACCESS ROAD AND STAGING AREA TO ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT. ALTERNATE STAGING AREAS AND ACCESS FOR THIS AREA WILL NOT BE ALLOWED. THIS WORK AREA SHALL BE RESTRICTED FROM ACTIVE AOA AREAS WITH THE BARRICADE/FENCING PERIMETERS SHOWN.

THE ENTRANCES SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLE ACCESS AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO THE CITY OF ROCKFORD AND IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.

4. CONSTRUCTION EQUIPMENT FLAGGING / BEACON REQUIREMENTS:
THE CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON (FLASHING YELLOW) LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION.

5. GATE SECURITY:
THE GATE SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE DEPUTY DIRECTOR OF OPERATIONS. SHOULD THE CONTRACTOR'S OPERATIONS REQUIRE THE GATE TO REMAIN OPEN TO PROVIDE ACCESS TO HAULING OPERATIONS, A COMPETENT GATE GUARD EMPLOYED BY THE CONTRACTOR SHALL BE REQUIRED TO CONTROL ACCESS TO THE AIRFIELD. A \$1,000 FINE SHALL BE ASSESSED FOR ANY OCCURRENCE OF AN UNSECURE GATE THAT IS THE CONTRACTOR'S RESPONSIBILITY. AN UNSECURED GATE SHALL BE DEFINED AS ANY GATE THAT IS NOT WITHIN THE SIGHT AND PHYSICAL CONTROL OF THE CONTRACTOR'S GUARD. IN THE EVENT THAT THE GATE MAY NOT BE SECURED, THE CONTRACTOR WILL BE CHARGED FOR AIRPORT PERSONNEL TO REMAIN AT THE GATE UNTIL SECURED.

6. CONSTRUCTION OUTSIDE OF BARRICADED AREAS REQUIRING TAXIWAY / RUNWAY CLOSURES:
WORK OUTSIDE THE BARRICADED LINES WITHIN THE AOA AREAS SHOWN SHALL REQUIRE TEMPORARY DAYTIME ONLY CLOSURES OF THE RUNWAY 7/25 AND TAXIWAY F. THIS WORK SHALL BE EXPEDITED AND PRIORITIZED TO MINIMIZE CLOSURE TIME OF THE ACTIVE PAVEMENTS. CONSTRUCTION ACTIVITIES WILL NOT BE PERMITTED WITHIN 250' OF CENTERLINE ON ALL ACTIVE RUNWAYS AND WITHIN 160' OF CENTERLINE ON ALL ACTIVE TAXIWAYS. IN ADDITION, THIS WORK WILL REQUIRE ALL CREWS TO SUPPLY AND HAVE IN THEIR POSSESSION AT ALL TIMES AT LEAST ONE AIRPORT RADIO TO COMMUNICATE DIRECTLY WITH THE ATCT (AIR TRAFFIC CONTROL TOWER). THE OPERATOR OF THE AIRPORT RADIO SHALL BE FAMILIAR WITH AIRPORT RADIO PROCEDURES AND BE TUNED INTO THE GROUND CONTROL FREQUENCY AT ALL TIMES.

7. UNAUTHORIZED ACCESS TO AIRFIELD:
THE CONTRACTOR SHALL RESTRICT ALL CONSTRUCTION ACTIVITIES TO THE CONSTRUCTION AREA DETAILED IN THE PHASING PLAN. ANY UNAUTHORIZED MOVEMENTS, PEDESTRIAN OR VEHICULAR, BEYOND THE CONSTRUCTION LIMITS SHOWN SHALL BE CONSIDERED AN AIRFIELD INCURSION. AIRFIELD INCURSIONS, AT THE DISCRETION OF THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS, MAY BE FINED \$10,000.00 PER INCIDENT. INCURSION FINES WILL BE ASSESSED IMMEDIATELY AND DEDUCTED FROM MONIES DUE THE CONTRACTOR ON THE NEXT CONSTRUCTION PAYMENT.

1. SUGGESTED SEQUENCE OF CONSTRUCTION:
THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE RESIDENT ENGINEER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS.

2. STAGING AREA RESTORATION:
ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A STAGING AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE PAVEMENTS / AREAS SHALL BE INCIDENTAL TO THE CONTRACT.

3. HAUL ROUTE / HAUL ROUTE RESTORATION:
THE CONTRACTOR SHALL USE THE HAUL ROUTE(S) AS SHOWN IN THESE PLANS. THE COST OF MAINTAINING THE HAUL ROUTE(S) SHALL BE INCIDENTAL TO CONTRACT. ALL HAUL ROUTE(S) INCLUDING EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE HAUL ROUTE(S) SHALL BE INCIDENTAL TO CONTRACT.

4. AIRPORT APPROVAL OF PHASING:
THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT OPERATIONS. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. VEHICULAR TRAFFIC AND CONTRACTOR ACTIVITIES SHALL YIELD TO AIRCRAFT OPERATIONS. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

5. AIRFIELD PAVEMENT / SITE DEBRIS REMOVAL:
THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.

6. PROJECT LIGHTING OUTSIDE OF DAYLIGHT HOURS:
WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.

7. EXISTING UTILITY COORDINATION:
COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATIONS.

8. TRAFFIC CONTROL PAYMENT:
PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A.) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES USED ON THE AIRFIELD MUST BE INTERLOCKING LOW PROFILE BARRICADES. INTERLOCKING LOW PROFILE BARRICADES WITH STEADY BURN RED LIGHTS SHALL BE INTERLOCKED WITH NO GAPS BETWEEN BARRICADES. BARRICADES SHALL BE PLACED AS SHOWN ON THIS PLAN AND AS DIRECTED BY THE RESIDENT ENGINEER. WHEN NOT IN USE, THESE BARRICADES SHALL BE STORED AT THE CONTRACTOR'S STAGING AREA OR OFF SITE. ACCESS TO THE ACTIVE RUNWAY AND TAXIWAY PAVEMENTS (TOWER CONTROLLED AREAS) SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON THE CLOSEST BARRICADES (2 EACH, RIGHT AND LEFT) AT THE ENTRANCE. IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "TOWER CONTROL AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE." ALL NON AIRFIELD LOCATIONS REQUIRING BARRICADES SHALL BE TYPE II BARRICADES AND SHALL CONFORM TO IDOT STANDARD 702001. FOR AIRSIDE BARRICADE PLACEMENT, SEE PHASING PLAN SUMMARY SHEETS.

(NOTES CONTINUED ON PHASING PLAN DETAILS - PPD1)



MICHAEL P. DUNN
DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4360

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:

PHASING PLAN
NOTES

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **PPN1** REVISION



MICHAEL P. DUNN
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PHASING PLAN
DETAILS

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SHEET NO. **PPD1** REVISION

SHEET 9 OF 65 SHEETS

RO024

PHASING PLAN GENERAL NOTES (CONT'D):

(NOTES CONTINUED ON PHASING PLAN NOTES - PPN1)

9. DRIVERS TRAINING AND BADGING:

DRIVER'S TRAINING AND BADGING SHALL BE REQUIRED FOR THE CONTRACTOR'S SUPERVISORY PERSONNEL, OTHER CONSTRUCTION PERSONNEL CAN BE WITHIN THE AIRFIELD LIMITS PROVIDED THAT THEY ARE UNDER ESCORT AND IN THE PRESENCE OF AN AUTHORIZED SUPERVISOR. THE DRIVER'S TRAINING AND BADGING OF THE INITIAL SUPERVISORY PERSONNEL MUST BE COMPLETED PRIOR TO THE START OF CONSTRUCTION.

10. DUST CONTROL REQUIREMENTS:

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ON SITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE DEPUTY DIRECTOR OF OPERATIONS. PAYMENT FOR DUST CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.

11. OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION (AC 150/5370-2F):

ALL WORK SHALL CONFORM TO AC 150/5370-2F OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION. THIS AC IS AVAILABLE AT www.faa.gov/arp/pdf/5370-2f.pdf.

12. STAGING AREA:

THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFERS SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.

13. AIRFIELD LIGHTING COORDINATION:

THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD LIGHTING CIRCUITS AT THE START AND END OF EACH CONSTRUCTION DAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE DONE SO AT NO EXTRA COST TO THE CONTRACT.

14. WEEKLY COORDINATION MEETINGS:

WEEKLY COORDINATION MEETINGS SHALL BE REQUIRED TO DISCUSS PROJECT PROGRESS. AS A MINIMUM, PROJECT SCHEDULE AND GATE VISITOR LOGS SHALL BE DISCUSSED. REPRESENTATION BY THE PRIME CONTRACTOR IS MANDATORY.

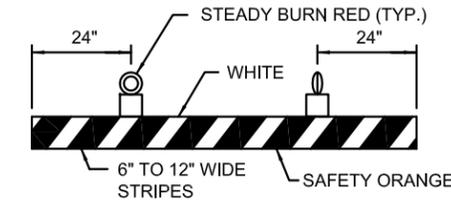
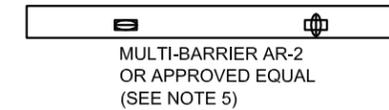
15. TEMPORARY CABLES:

ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT: • REPLACE PERIMETER FENCE, PHASES 1 & 2
• REHABILITATE RUNWAY 1/19 PHASE 2

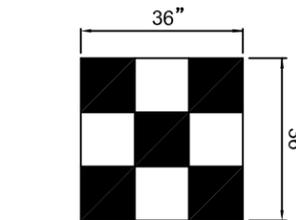
LIGHTED RUNWAY CLOSURE MARKERS

1. THE AIRPORT SHALL FURNISH TWO LIGHTED RUNWAY CLOSURE MARKERS FOR THE DURATION OF THE PROJECT. IF LIGHTED RUNWAY CLOSURE MARKERS ARE NOT AVAILABLE THE CONTRACTOR SHALL REFER TO THE "CLOSED RUNWAY MARKER DETAIL" ON THIS SHEET.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRANSPORTING, INSTALLING, MAINTAINING, REFUELING, REPOSITIONING AND REMOVING THE LIGHTED RUNWAY CLOSURE MARKERS AS SHOWN IN THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
3. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS.
4. ALL COST ASSOCIATED WITH THE LIGHTED RUNWAY CLOSURE MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
5. DURING CONSTRUCTION, IT WILL BE NECESSARY TO CLOSE RUNWAY 1/19 TO AIR TRAFFIC FOR THE DURATION OF THE PROJECT. THE CONTRACTOR SHALL MARK RUNWAY 1/19 CLOSED BY PLACING LIGHTED RUNWAY CLOSURE MARKER AT THE LOCATION DETAILED ON THE PLANS OR AS DIRECTED BY THE ENGINEER AND AIRPORT. THE LIGHTED RUNWAY CLOSURE MARKERS ARE REQUIRED TO BE IN OPERATION ANYTIME THE RUNWAY IS CLOSED.

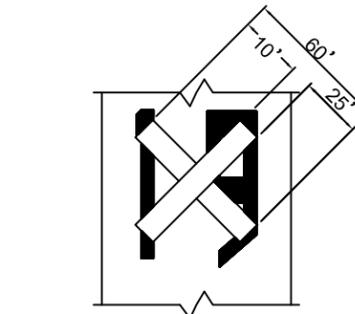


INTERLOCKING LOW PROFILE BARRICADES
ON PAVEMENT - NO SCALE

1. LOW PROFILE BARRICADES SHALL BE PLACED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. THE BARRICADES SHALL BE INTERLOCKED WITH NO GAPS BETWEEN BARRICADES. BARRICADES SHALL BE WEIGHTED WITH A MINIMUM OF 6 SAND BAGS TO PREVENT THEM FROM BEING BLOWN OVER.
2. THE BARRICADE LINE SHALL EXTEND ONE BARRICADE PAST THE EDGE OF PAVEMENT INTO THE TURF.
3. LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
4. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
5. ALTERNATE LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
6. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT'S COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR AS LOW POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND.
7. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

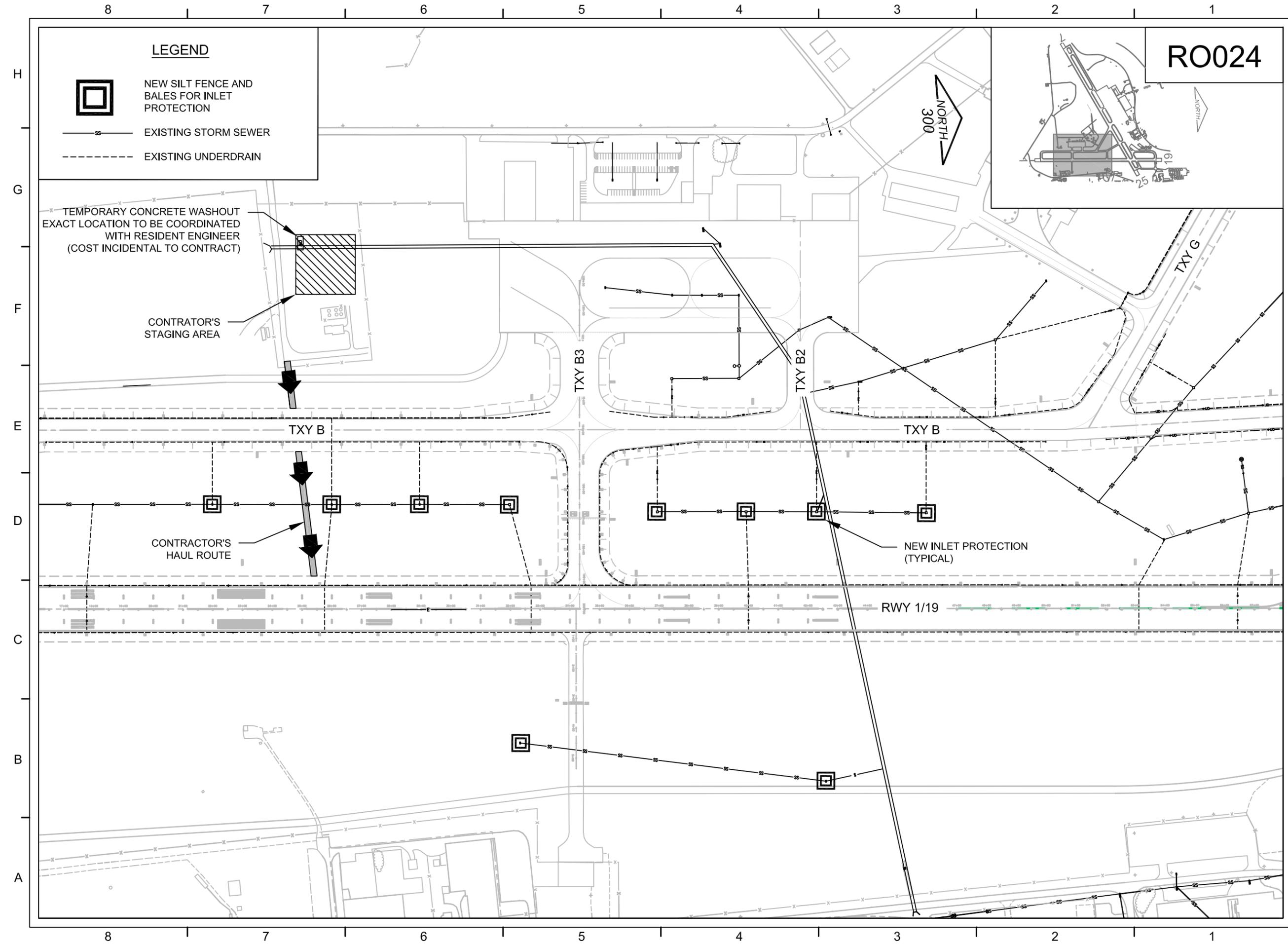


**CONSTRUCTION EQUIPMENT
AND TRUCK SIGNAL FLAG**
NOT TO SCALE



CLOSED RUNWAY MARKER DETAIL
ON PAVEMENT - NO SCALE

1. THE CONTRACTOR SHALL USE THE LIGHTED RUNWAY CLOSURE MARKERS PROVIDED BY THE AIRPORT. IF THE LIGHTED RUNWAY CLOSURE MARKERS ARE NOT AVAILABLE THE CONTRACTOR WILL BE REQUIRED TO INSTALL RUNWAY CLOSURE MARKERS AS DETAILED ABOVE.
2. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
3. MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER AND THE AIRPORT.
4. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
5. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
6. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
7. DURING CONSTRUCTION, IT WILL BE NECESSARY TO CLOSE RUNWAY 1/19 TO AIR TRAFFIC FOR THE DURATION OF THE PROJECT. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET OR AS DIRECTED BY THE ENGINEER AND AIRPORT. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.



LEGEND

-  NEW SILT FENCE AND BALES FOR INLET PROTECTION
-  EXISTING STORM SEWER
-  EXISTING UNDERDRAIN

RO024



MICHAEL P. DUNN
DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4360

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

**REHABILITATE
RUNWAY 1/19
PHASE 3**

SHEET TITLE:
**STORMWATER POLLUTION
PREVENTION PLAN**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **SWP1** REVISION

SHEET 10 OF 65 SHEETS

STORM WATER POLLUTION PREVENTION PLAN

RO024

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATING RUNWAY 1/19 AT THE CHICAGO ROCKFORD INTERNATIONAL AIRPORT. THE PROJECT INCLUDES GRADING, EXCAVATION, FILL, TOPSOIL PLACEMENT, PAVEMENT CONSTRUCTION, ELECTRICAL, LANDSCAPING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

1. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS PERIMETER SILT FENCE AND INLET PROTECTION.
2. EXCAVATION AND EMBANKMENT WILL BE COMPLETED WITHIN THE PROJECT LIMITS.
3. PAVEMENT CONSTRUCTION.
4. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
5. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCHING.
6. REMOVAL OF TEMPORARY EROSION CONTROL / PROTECTION FACILITIES.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 9 ACRES OF WHICH 8 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE KISHWAUKEE RIVER VIA OVERLAND FLOW AND THROUGH A STORM SEWER SYSTEM.

CONTROLS - EROSION CONTROLS AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
- B. CONSTRUCT DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.
- C. BUILD NECESSARY EMBANKMENT AT CULVERT/STORM SEWER LOCATIONS AND THEN EXCAVATE AND PLACE PIPE.
- D. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.

4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

5. THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2 INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD.

6. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

7. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.



MICHAEL P. DUNN
DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4360

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:

STORMWATER POLLUTION
PREVENTION PLAN NOTES

DESIGNED: CMT-RFD	DRAWN: CMT-RFD	CHECKED: CMT-ARR
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PROJECT NO.:	12258-05-00/06
DATE:	APRIL 19, 2013

SHEET NO.	SWN1	REVISION
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MICHAEL P. DUNN
DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4360

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

SHEET TITLE:

**STORMWATER POLLUTION
PREVENTION PLAN DETAILS**

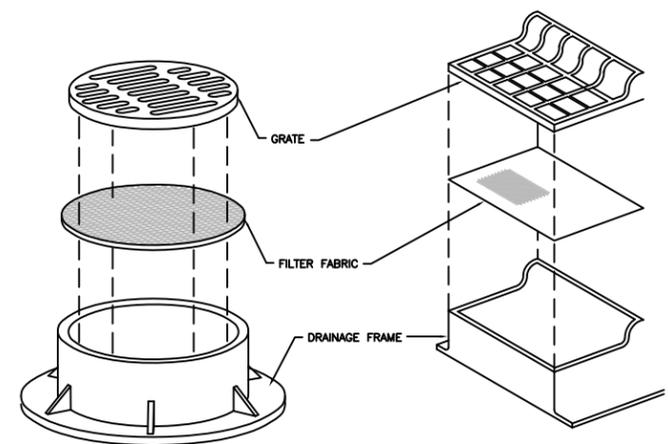
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **SWD1** REVISION

SHEET 12 OF 65 SHEETS

RO024



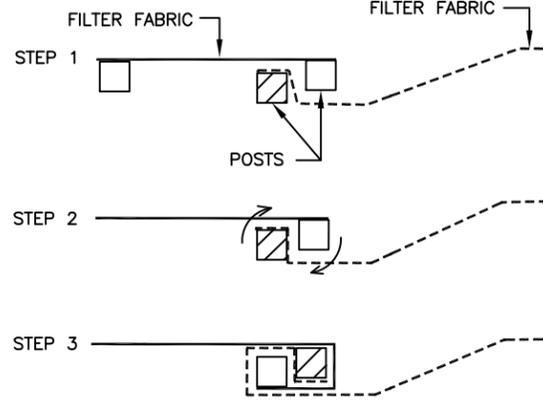
DRAINAGE STRUCTURE FILTER WRAP
N.T.S.

NOTES

1. FILTER WRAP TO BE PLACED IN ALL INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
2. FABRIC SHALL BE IN CONFORMANCE WITH MATERIALS SPECIFIED FOR SILT FENCE.
3. FABRIC SHALL OVERLAY FRAME BY 2" (MIN.).
4. CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
5. FABRIC SHALL REMAIN IN PLACE UNTIL TURFED AREAS HAVE DEVELOPED A MIN. OF 80% OF COVERAGE.
6. COST OF FILTER WRAP SHALL BE CONSIDERED INCIDENTAL TO BALES.

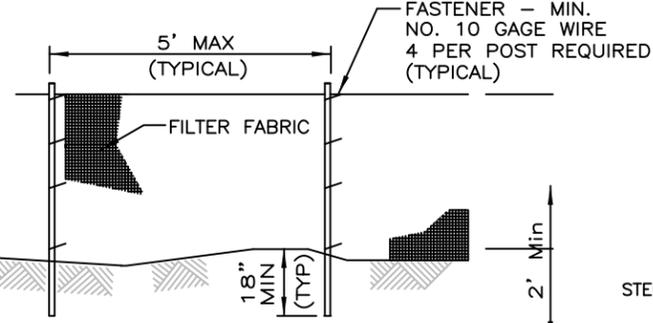
NOTES:

1. TEMPORARY SEDIMENT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. THEY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD AND REMOVED IN CONJUNCTION WITH THE FINAL GRADING AND SITE STABILIZATION.
2. FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED FOR ITEM AR156000 EROSION CONTROL IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF AERONAUTICS SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.
3. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE, SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.
4. SILT FENCE SHALL BE INSTALLED PER STORM WATER POLLUTION PREVENTION PLAN OR AS DIRECTED BY THE ENGINEER.
5. FENCE POSTS SHALL BE EITHER STANDARD STEEL POSTS OR WOOD POSTS WITH A MINIMUM CROSS-SECTIONAL AREA OF 3.0 SQ. IN..
6. PLACE THE END POST OF THE SECOND FENCE INSIDE THE END POST OF THE FIRST FENCE.
7. ROTATE BOTH POSTS AT LEAST 180 DEGREES IN A CLOCKWISE DIRECTION TO CREATE A TIGHT SEAL WITH THE FABRIC MATERIAL.
8. DRIVE BOTH POSTS A MINIMUM OF 18 INCHES INTO THE GROUND AND BURY THE FLAP.

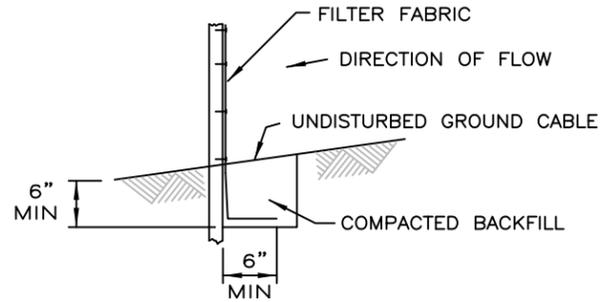


ATTACHING TWO SILT FENCES

NOT TO SCALE
FROM NRCS STANDARD DRAWING NO. IL-620

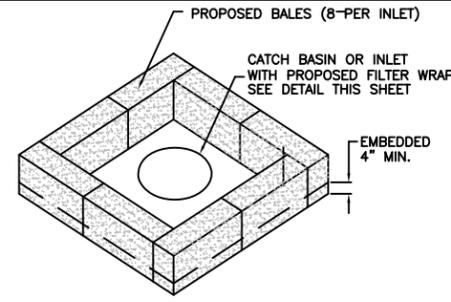


ELEVATION

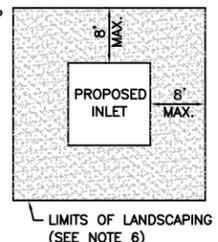


FABRIC ANCHOR DETAIL

NOT TO SCALE
FROM NRCS STANDARD DRAWING NO. IL-620



INLET PLACEMENT



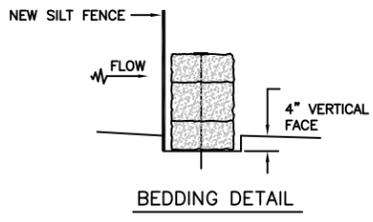
INLET PLACEMENT

INLET PROTECTION - TURF AREAS

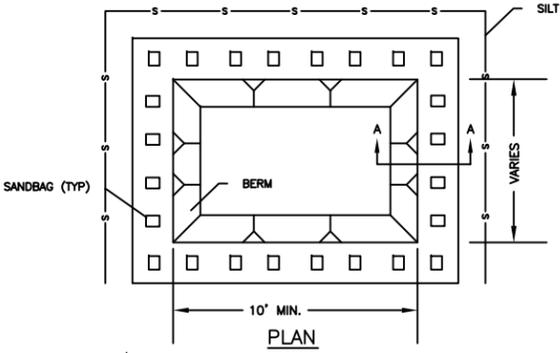
N.T.S.

INLET PROTECTION NOTES:

1. BALES SHALL BE PLACED AT THE TOE OF SLOPE OR ON A CONTOUR AND IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.
2. EACH BALE SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 4 INCHES, AND PLACED SO THE BINDINGS ARE HORIZONTAL.
3. BALES SHALL BE SECURELY ANCHORED IN PLACE BY EITHER TWO STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST STAKE IN EACH BALE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE AT AN ANGLE TO FORCE THE BALES TOGETHER. STAKES SHALL BE DRIVEN FLUSH WITH THE BALE.
4. INSPECTION SHALL BE FREQUENT AND REPAIR / REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
5. BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. COST OF REMOVAL / REPLACEMENT TO BE INCLUDED IN UNIT PRICE FOR BALES.
6. AFTER FINAL APPROVAL OF THE ENGINEER, STRAW BALES MAY BE REMOVED. CONTRACTOR SHALL PLACE SEED AND MULCH OVER THE DISTURBED AREAS, COST INCIDENTAL TO BALES.
7. SILT FENCE SHALL BE MEASURED AND PAID FOR SEPARATELY.



BEDDING DETAIL



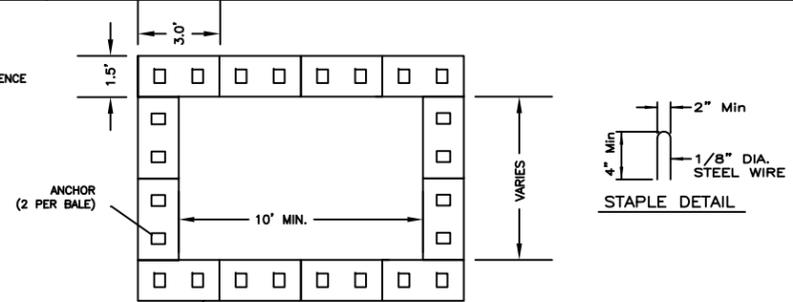
SECTION A-A

BELOW GROUND TEMPORARY WASHOUT

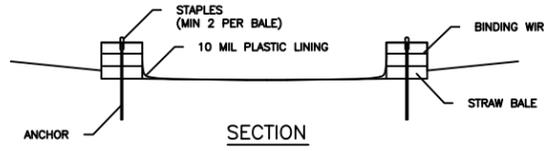
NOTES:

1. CONTRACTOR SHALL DETERMINE LOCATION AND SIZE OF WASHOUT.
2. WASHOUT SIZE AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
3. SANDBAGS SHALL BE INSTALLED TO ANCHOR THE LINING. THE NUMBER OF SANDBAGS SHALL BE DETERMINED BY THE CONTRACTOR. THE CONTRACTOR SHALL ADD SANDBAGS SO AS TO MAINTAIN ANCHORING OF THE LINING.
4. A CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 20 FEET OF THE TEMPORARY CONCRETE WASHOUT FACILITY. AT A MINIMUM, THE SIGN SHALL READ "CONCRETE WASHOUT" IN 6" TALL LETTERS.
5. THE TEMPORARY WASHOUT FACILITY SHALL BE SURROUNDED BY SILT FENCE ON ALL SIDES.
6. INSPECTION SHALL OCCUR ONCE PER WEEK AND DAILY DURING CONCRETE OPERATIONS. REPAIR/REPLACEMENT OF THE FACILITY SHALL BE MADE SUCH THAT CONCRETE WASTE IS CONTAINED.
7. MEDIA SHALL BE REMOVED AND DISPOSED OF AT A LEGAL OFF-SITE LOCATION WHEN THE FACILITY HAS REACHED 50% CAPACITY.
8. UPON COMPLETION OF CONCRETE OPERATIONS, THE CONCRETE WASHOUT AND ALL MATERIALS CONTAINED WITHIN SHALL BE DISPOSED OF AT A LEGAL OFF-SITE LOCATION.
9. COST INCIDENTAL TO ITEM AR501515.

CONCRETE WASHOUT
NOT TO SCALE



PLAN



SECTION

ABOVE GROUND TEMPORARY WASHOUT

NOTES:

1. CONTRACTOR SHALL DETERMINE LOCATION AND SIZE OF WASHOUT.
2. WASHOUT SIZE AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
3. A CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 20 FEET OF THE TEMPORARY CONCRETE WASHOUT FACILITY. AT A MINIMUM, THE SIGN SHALL READ "CONCRETE WASHOUT" IN 6" TALL LETTERS.
4. INSPECTION SHALL OCCUR ONCE PER WEEK AND DAILY DURING CONCRETE OPERATIONS. REPAIR/REPLACEMENT OF THE FACILITY SHALL BE MADE SUCH THAT CONCRETE WASTE IS CONTAINED.
5. MEDIA SHALL BE REMOVED AND DISPOSED OF AT A LEGAL OFF-SITE LOCATION WHEN THE FACILITY HAS REACHED 50% CAPACITY.
6. UPON COMPLETION OF CONCRETE OPERATIONS, THE CONCRETE WASHOUT AND ALL MATERIALS CONTAINED WITHIN SHALL BE DISPOSED OF AT A LEGAL OFF-SITE LOCATION.
7. COST INCIDENTAL TO ITEM AR501515.



MICHAEL P. DUNN
DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4360

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

SHEET TITLE:
**EXISTING CONDITIONS
AND REMOVALS - 1**

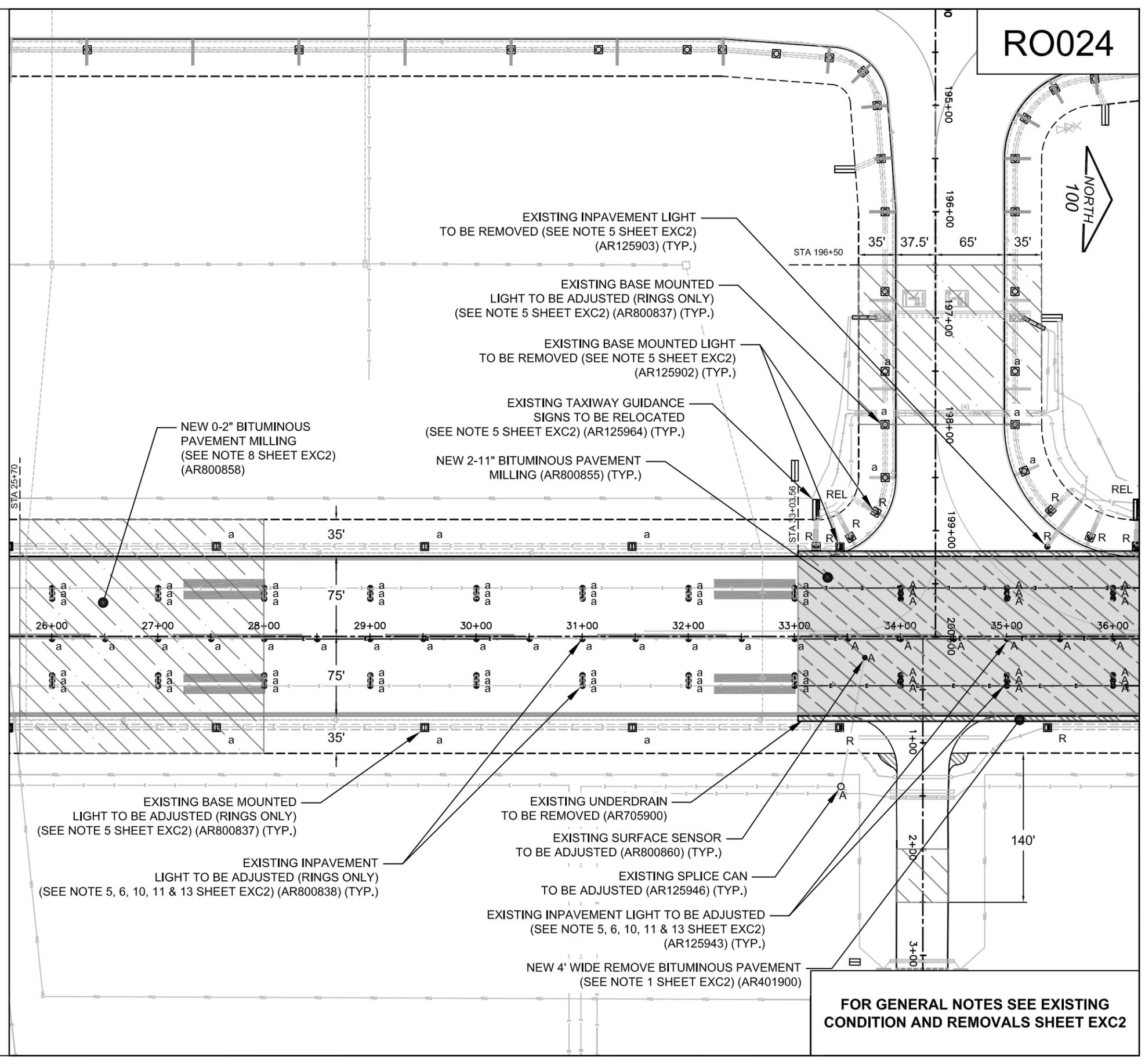
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **EXC1** REVISION

SHEET 13 OF 65 SHEETS

RO024



LEGEND

- NEW 0-2" BITUMINOUS PAVEMENT MILLING
- NEW 2-11" BITUMINOUS PAVEMENT MILLING
- NEW 11-24" BITUMINOUS PAVEMENT MILLING
- REMOVE BITUMINOUS PAVEMENT
- EXISTING SANITARY LINE
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- EXISTING WATERMAIN
- EXISTING RWY GUIDANCE LIGHT CIRCUIT
- EXISTING AIRFIELD CIRCUIT
- EXISTING LIGHTING CIRCUIT IN EXISTING DUCT
- REMOVE EXISTING DUCT
- EXISTING PAVEMENT MARKING
- EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- ADJUST EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- REMOVE BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- EXISTING ELEV. RUNWAY GUARD LIGHT
- REMOVE ELECTRICAL HANDHOLE
- EXISTING ELECTRICAL HANDHOLE
- EXISTING CONDUIT/DUCT
- RELOCATE EXISTING AIRFIELD GUIDANCE SIGN
- EXISTING AIRFIELD GUIDANCE SIGN
- REMOVE BASE CAN (BASE MOUNTED RUNWAY EDGE LIGHT)
- ADJUST BASE MOUNTED RUNWAY EDGE LIGHT
- ADJUST BASE MOUNTED RUNWAY EDGE LIGHT (RINGS ONLY)
- REMOVE BASE CAN (INPAVEMENT LIGHT)
- ADJUST INPAVEMENT LIGHT
- ADJUST INPAVEMENT LIGHT (RINGS ONLY)
- EXISTING SPLICE CAN
- ADJUST SPLICE CAN
- EXISTING SURFACE SENSOR
- ADJUST SURFACE SENSOR

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS AND FIELD SURVEYS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WITH RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION PRESENTED HEREIN AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

**FOR GENERAL NOTES SEE EXISTING
CONDITION AND REMOVALS SHEET EXC2**



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IL Proj: RFD-4360

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

SHEET TITLE:
**EXISTING CONDITIONS
AND REMOVALS - 2**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **EXC2** REVISION

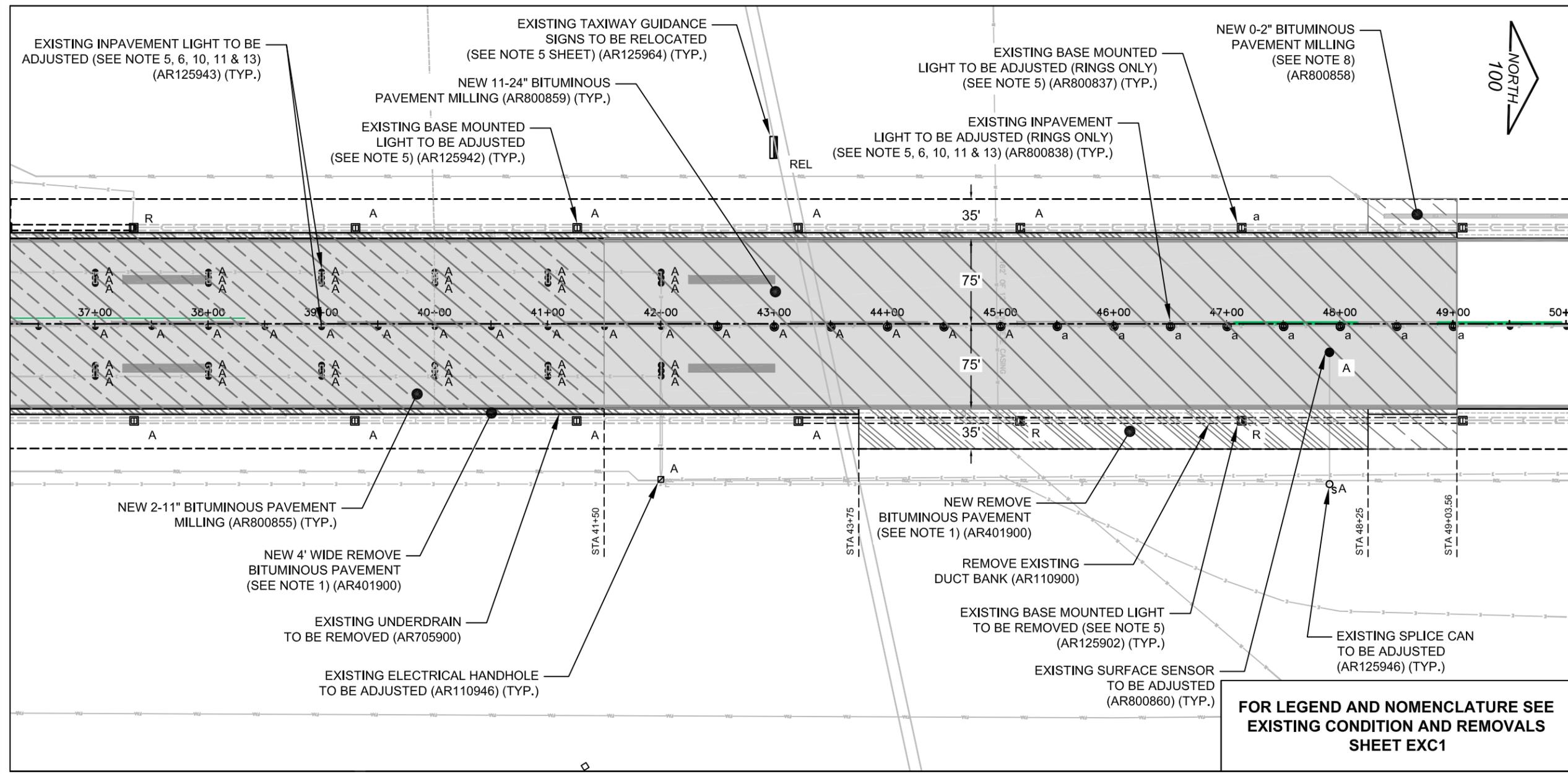
SHEET 14 OF 65 SHEETS

RO024

NOTES:

- EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
- TRANSFORMER TO BE REMOVED SHALL BE TURNED OVER TO THE AIRPORT. CONCRETE LIGHT BASES SHALL BE DISPOSED OF OFF SITE.
- ANY TEMPORARY CABLING REQUIRED FOR THIS PROJECT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- ALL LIGHT BASES REMOVED SHALL BE BACKFILLED IN ACCORDANCE WITH ITEM 208 COST OF BACKFILL SHALL BE INCLUDED IN THE REMOVAL PAY ITEM.
- ALL EXISTING LIGHT FIXTURES AND SIGNS WILL BE REUSED FOR THE FINAL ASSEMBLY OF THE ASSOCIATED PAY ITEM.
- SHOULD ANY ADDITIONAL WORK BE REQUIRED TO REMOVE THE EXISTING PAVEMENT STRUCTURE (BITUMINOUS AND/OR PCC PAVEMENT) FROM AROUND THE EXISTING INPAVEMENT LIGHT BASE CAN, THE COST SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAVEMENT MILLING PAY ITEM.
- ANY PAVEMENT REMOVED BY THE CONTRACTOR BEYOND THE LIMITS SHOWN IN THE PLANS TO FACILITATE PCC PAVEMENT CONSTRUCTION AND SUBSEQUENT PAVEMENT REPLACEMENT SHALL BE CONSIDERED INCIDENTAL TO THE RESPECTIVE PAVEMENT REMOVAL ITEM.
- THE EXACT PAVEMENT MILLING LIMIT AND DEPTH REQUIRED SHALL BE DETERMINED FROM THE PROFILE AND GRADING PLAN SHEETS.
- ALL REGRADING AND RESHAPING OF EXISTING AGGREGATE BASE COURSE SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAVING PAY ITEM.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN EXISTING INPAVEMENT BASE CAN AND CONDUIT. ANY BASE CAN OR CONDUIT DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- ALL WORK REQUIRED TO PREPARE EXISTING INPAVEMENT BASE CAN AND CONDUIT FOR PAVING OPERATIONS SHALL BE INCIDENTAL TO THE ASSOCIATED PAVING PAY ITEM.
- IF THE HMA PAVEMENT SECTION IS LESS THAN THE PAVEMENT MILLING DEPTH SHOWN, THE REMOVAL OF THE EXISTING AGGREGATE SUBBASE TO THE CORRECT PROFILE AND DIMENSIONS SHALL BE REMOVED BY PAVEMENT MILLING. THIS COST SHALL BE INCIDENTAL TO ASSOCIATED BITUMINOUS MILLING PAY ITEM.
- SEE ELECTRICAL DETAIL SHEET ELD4 FOR INPAVEMENT TOUCHDOWN ZONE LIGHT SPACING DETAIL.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS AND FIELD SURVEYS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WITH RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION PRESENTED HEREIN AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



**FOR LEGEND AND NOMENCLATURE SEE
EXISTING CONDITION AND REMOVALS
SHEET EXC1**



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REV	DATE	DESCRIPTION

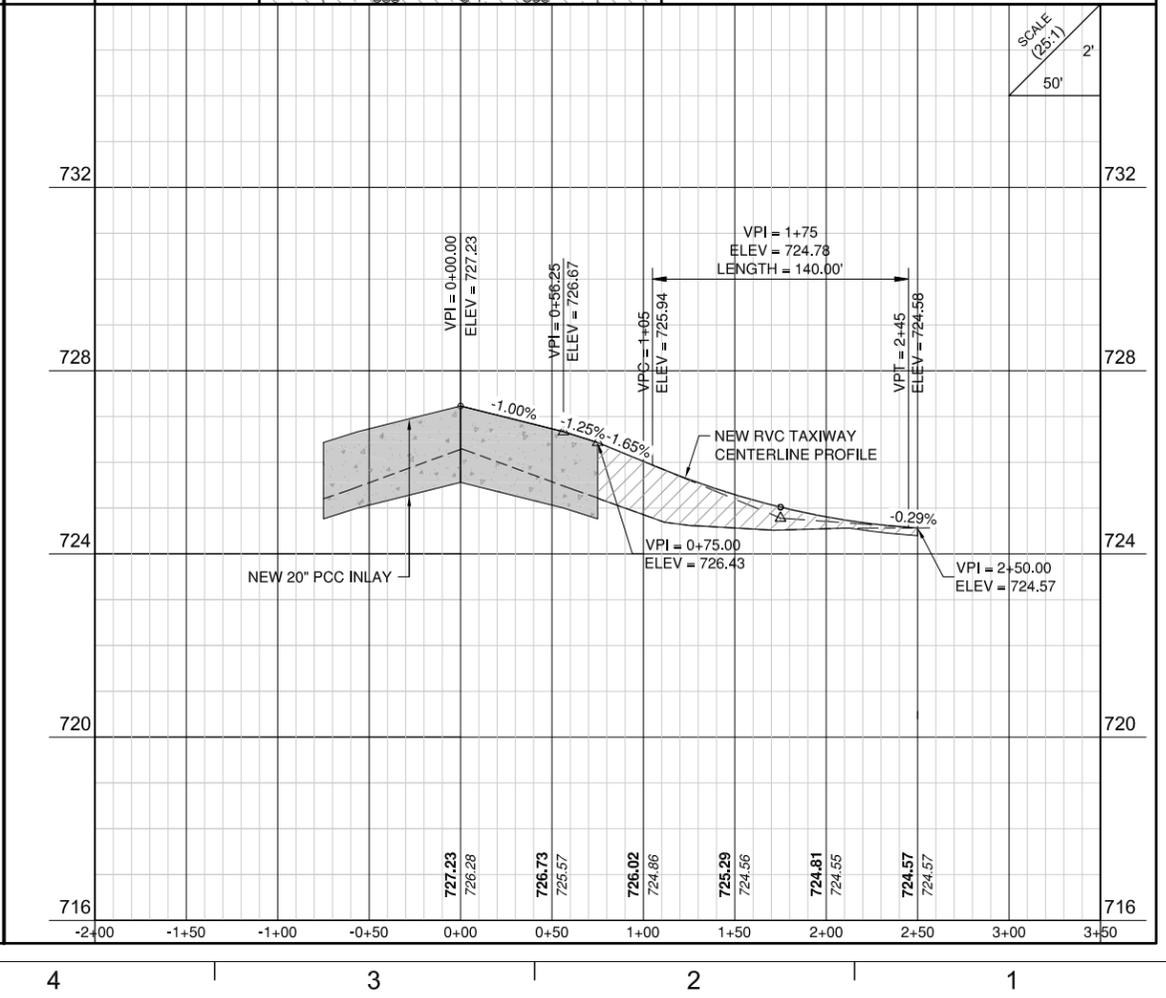
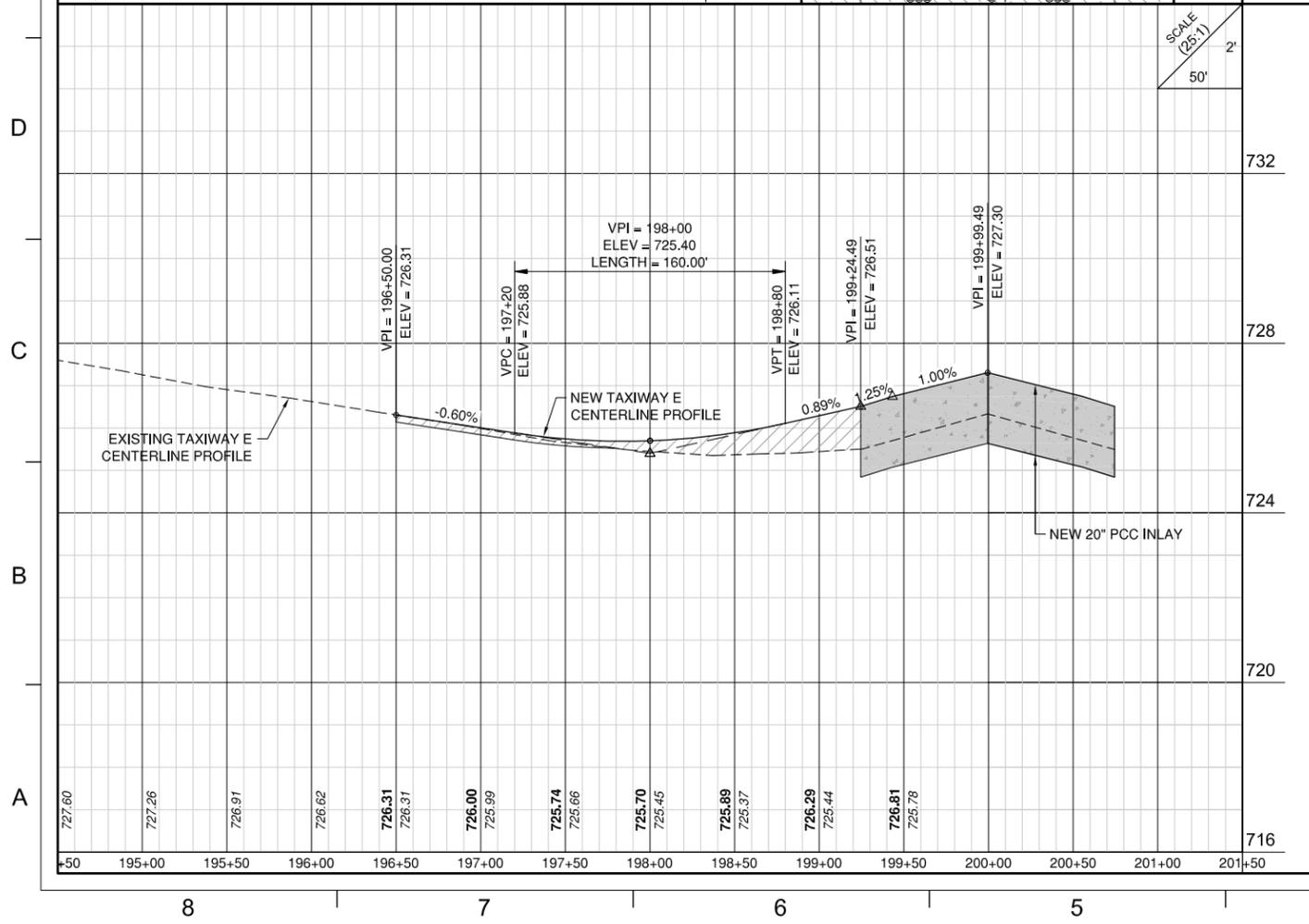
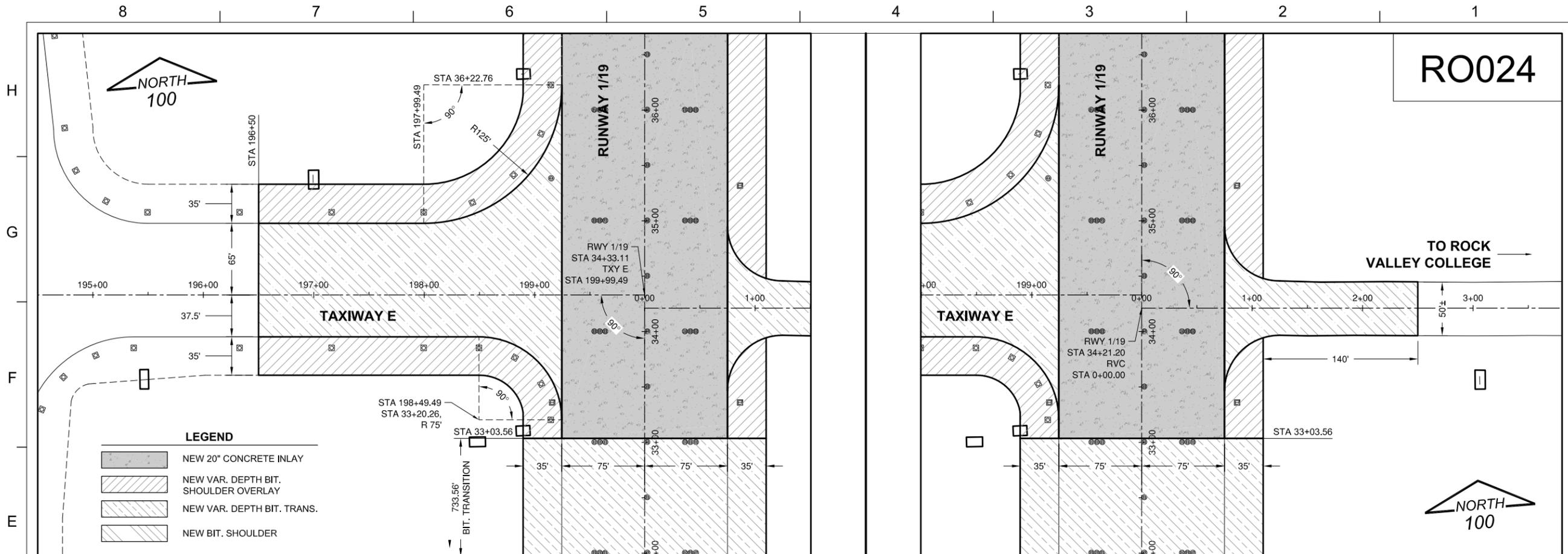
PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

SHEET TITLE:
**TAXIWAY E
PLAN AND PROFILE**

DESIGNED: SMS DRAWN: SMS CHECKED: DLP

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **TPP1** REVISION



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REV DATE DESCRIPTION
PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

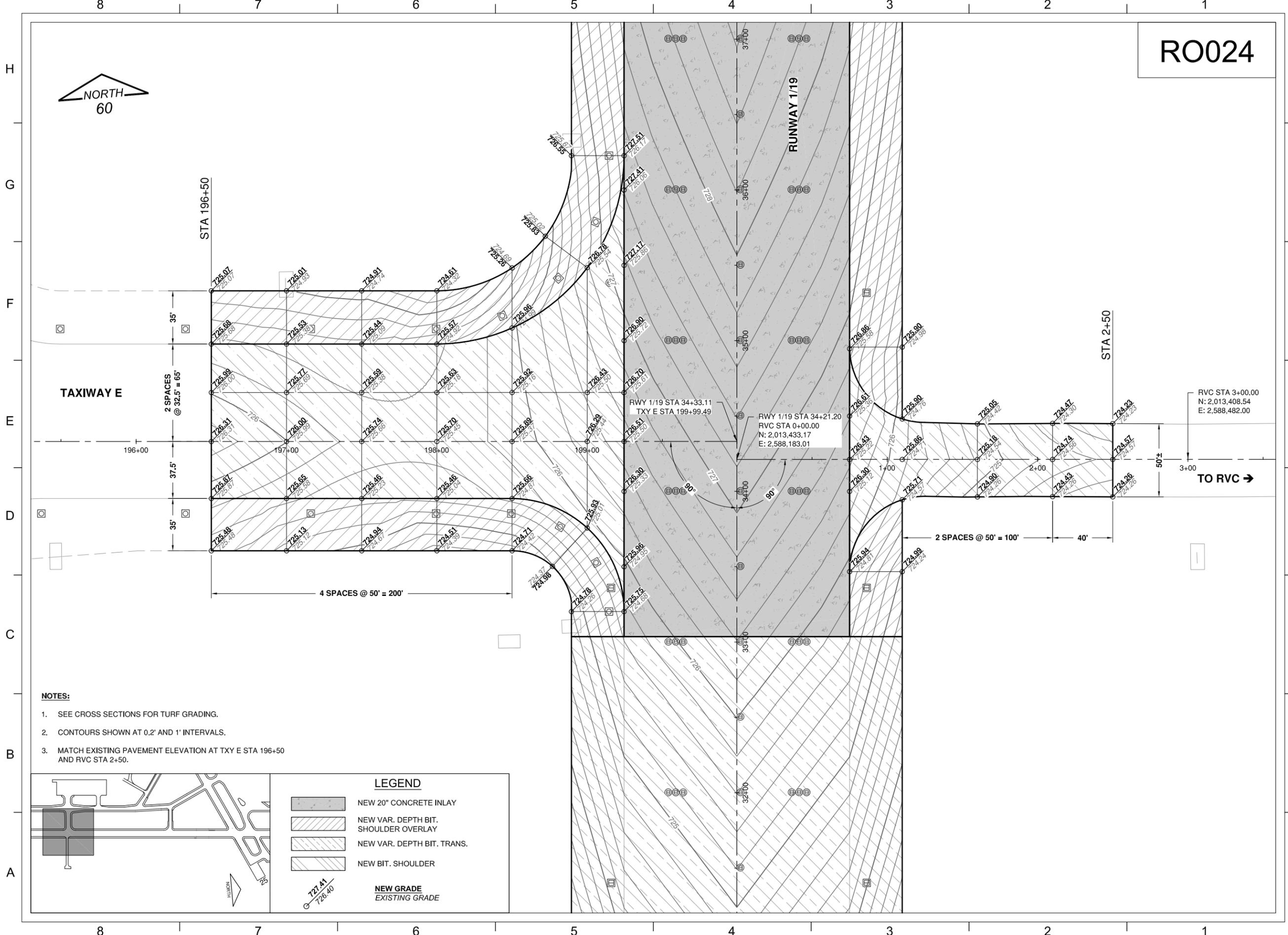
SHEET TITLE:
TAXIWAY E
GRADING PLAN

DESIGNED: SMS DRAWN: SMS CHECKED: DLP

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. TGP1 REVISION

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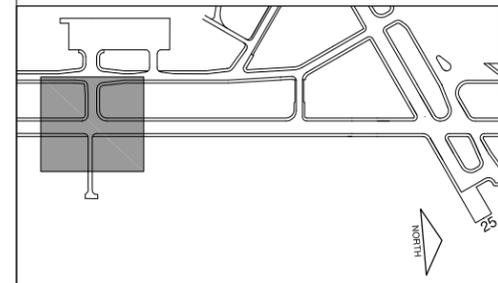


- NOTES:**
- SEE CROSS SECTIONS FOR TURF GRADING.
 - CONTOURS SHOWN AT 0.2' AND 1' INTERVALS.
 - MATCH EXISTING PAVEMENT ELEVATION AT TXY E STA 196+50 AND RVC STA 2+50.

LEGEND

- NEW 20" CONCRETE INLAY
- NEW VAR. DEPTH BIT. SHOULDER OVERLAY
- NEW VAR. DEPTH BIT. TRANS.
- NEW BIT. SHOULDER

NEW GRADE
EXISTING GRADE





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REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:

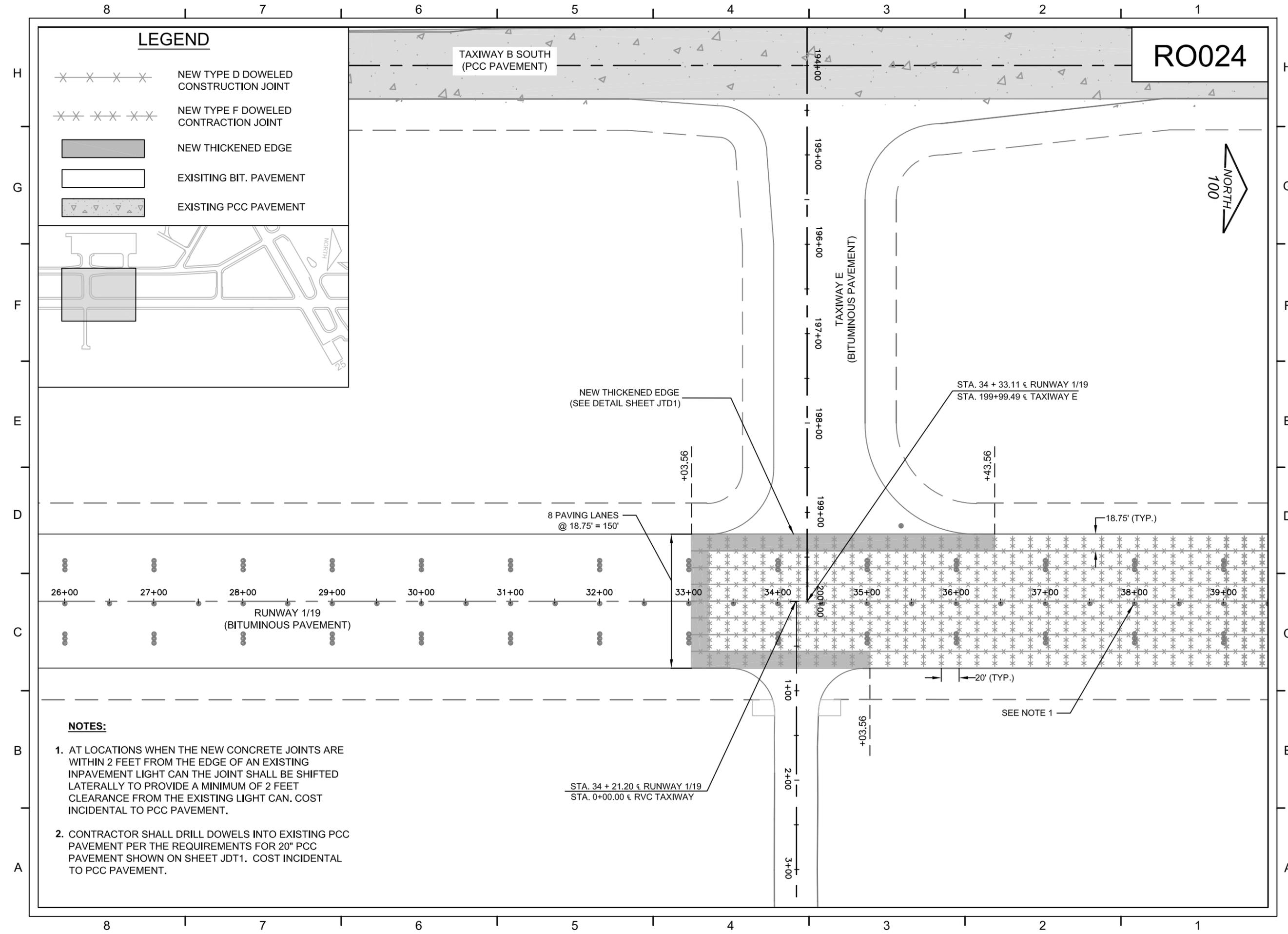
JOINTING PLAN - 1

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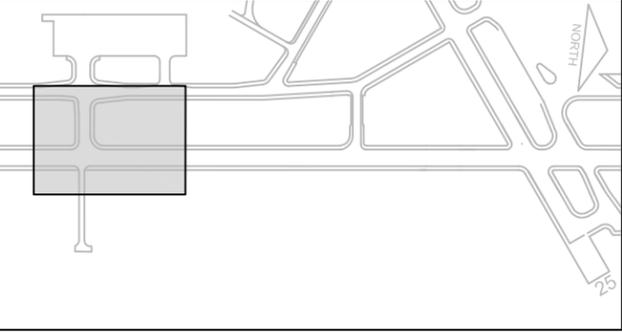
SHEET NO. **JTP1** REVISION

SHEET 21 OF 65 SHEETS



LEGEND

- NEW TYPE D DOWELED CONSTRUCTION JOINT
- NEW TYPE F DOWELED CONTRACTION JOINT
- NEW THICKENED EDGE
- EXISTING BIT. PAVEMENT
- EXISTING PCC PAVEMENT



NOTES:

1. AT LOCATIONS WHEN THE NEW CONCRETE JOINTS ARE WITHIN 2 FEET FROM THE EDGE OF AN EXISTING INPAVEMENT LIGHT CAN THE JOINT SHALL BE SHIFTED LATERALLY TO PROVIDE A MINIMUM OF 2 FEET CLEARANCE FROM THE EXISTING LIGHT CAN. COST INCIDENTAL TO PCC PAVEMENT.
2. CONTRACTOR SHALL DRILL DOWELS INTO EXISTING PCC PAVEMENT PER THE REQUIREMENTS FOR 20" PCC PAVEMENT SHOWN ON SHEET JDT1. COST INCIDENTAL TO PCC PAVEMENT.



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REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:

ELECTRICAL AND
MARKING PLAN -1

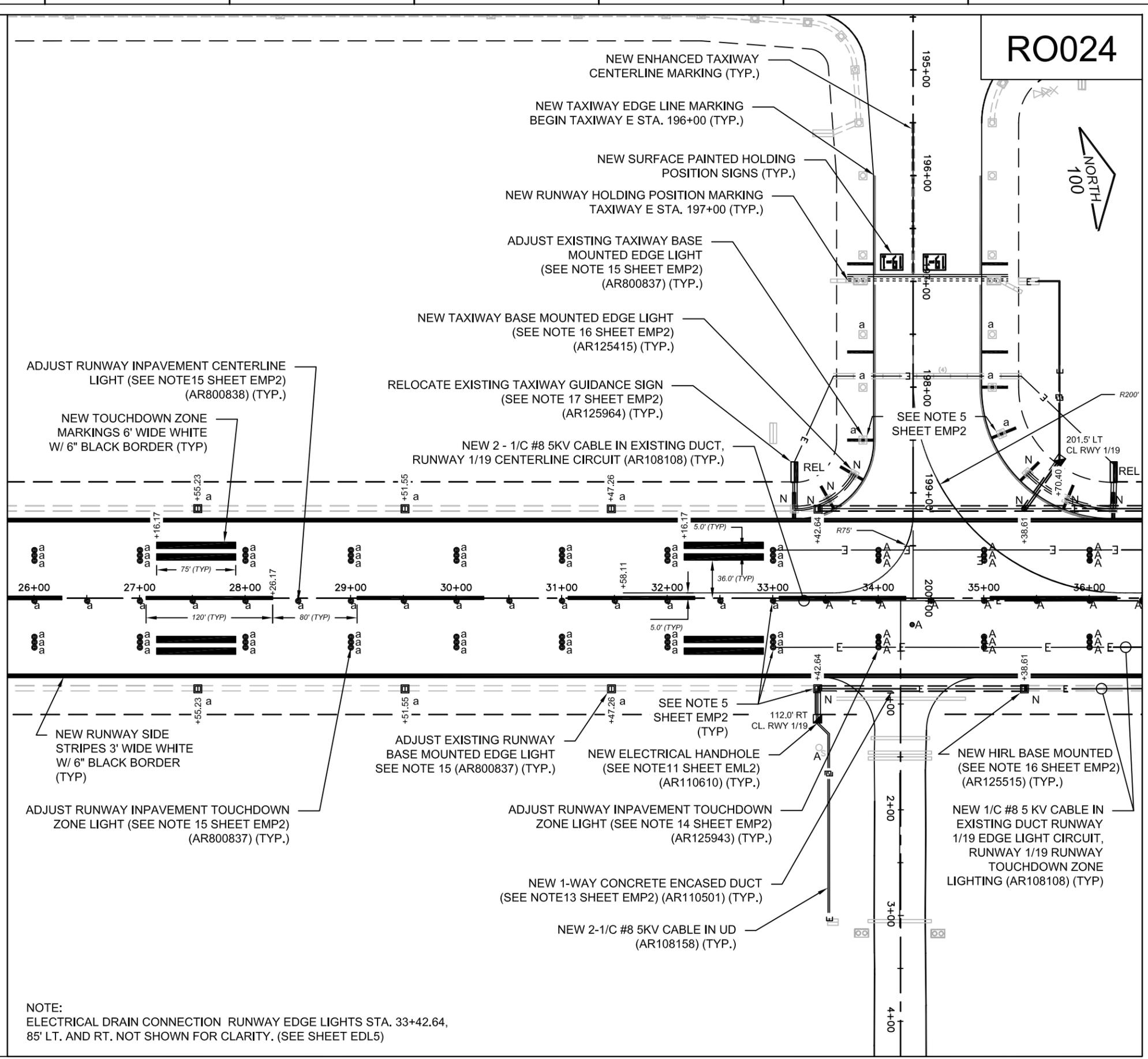
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. REVISION

EMP1

SHEET 24 OF 65 SHEETS

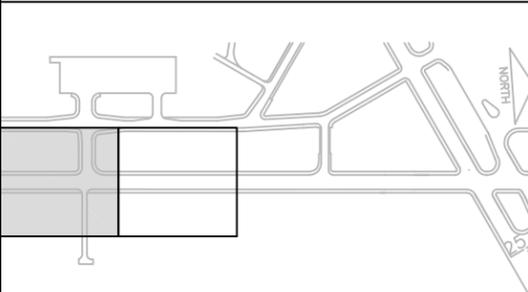


NOTE:
ELECTRICAL DRAIN CONNECTION RUNWAY EDGE LIGHTS STA. 33+42.64,
85' LT. AND RT. NOT SHOWN FOR CLARITY. (SEE SHEET EDL5)

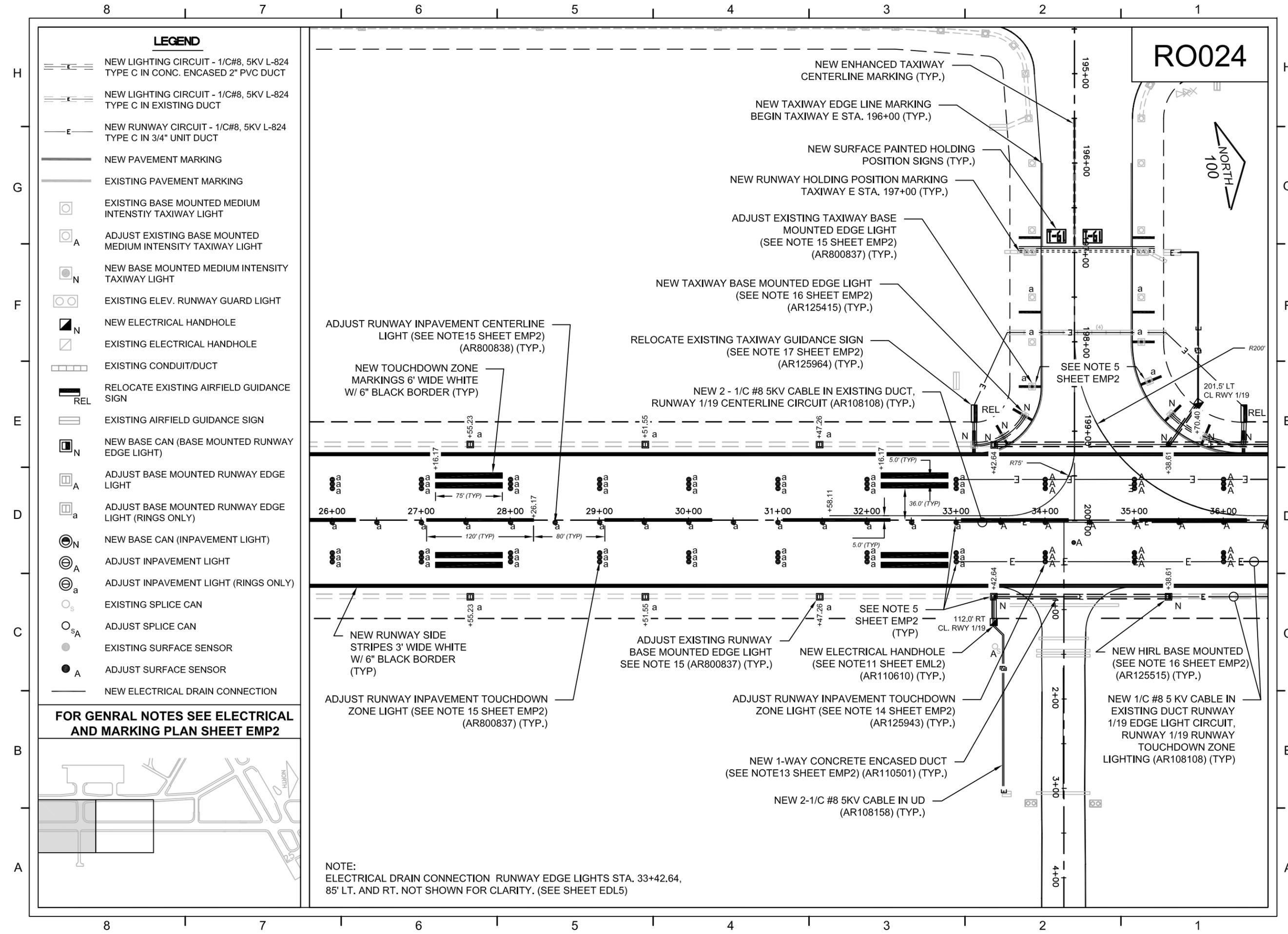
LEGEND

- NEW LIGHTING CIRCUIT - 1/C#8, 5KV L-824 TYPE C IN CONC. ENCASED 2" PVC DUCT
- NEW LIGHTING CIRCUIT - 1/C#8, 5KV L-824 TYPE C IN EXISTING DUCT
- NEW RUNWAY CIRCUIT - 1/C#8, 5KV L-824 TYPE C IN 3/4" UNIT DUCT
- NEW PAVEMENT MARKING
- EXISTING PAVEMENT MARKING
- EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- ADJUST EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- NEW BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- EXISTING ELEV. RUNWAY GUARD LIGHT
- NEW ELECTRICAL HANDHOLE
- EXISTING ELECTRICAL HANDHOLE
- EXISTING CONDUIT/DUCT
- RELOCATE EXISTING AIRFIELD GUIDANCE SIGN
- EXISTING AIRFIELD GUIDANCE SIGN
- NEW BASE CAN (BASE MOUNTED RUNWAY EDGE LIGHT)
- ADJUST BASE MOUNTED RUNWAY EDGE LIGHT
- ADJUST BASE MOUNTED RUNWAY EDGE LIGHT (RINGS ONLY)
- NEW BASE CAN (INPAVEMENT LIGHT)
- ADJUST INPAVEMENT LIGHT
- ADJUST INPAVEMENT LIGHT (RINGS ONLY)
- EXISTING SPLICE CAN
- ADJUST SPLICE CAN
- EXISTING SURFACE SENSOR
- ADJUST SURFACE SENSOR
- NEW ELECTRICAL DRAIN CONNECTION

FOR GENERAL NOTES SEE ELECTRICAL AND MARKING PLAN SHEET EMP2



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APPROVED AS WORKING PLAN
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REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:

HOMERUN PLAN

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **HRP1** REVISION

SHEET 26 OF 65 SHEETS

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NOTES

1. CONTRACTOR SHALL INSTALL L-823 CONNECTORS FOR RUNWAY 1 TOUCHDOWN ZONE LIGHTING CIRCUIT INSIDE VAULT. INSTALL NEW CABLES FROM TDZ REGULATOR TO HOMERUNS. COST SHALL BE INCIDENTAL TO PAY ITEM AR108108.
2. EACH CABLE SHALL HAVE A MINIMUM OF 10' SLACK IN EACH ELECTRICAL MANHOLE AND SHALL BE TAGGED AND SHALL BE PAID PER LINEAR FOOT. ALL OTHER CABLE SLACK IN LIGHTS AND SPLICE CANS WILL BE INCIDENTAL TO THE LIGHT OR SPLICE CAN.
3. CONTRACTOR SHALL REMOVE EXISTING RUNWAY TOUCHDOWN ZONE CIRCUIT. COST SHALL BE INCIDENTAL TO NEW CIRCUIT.

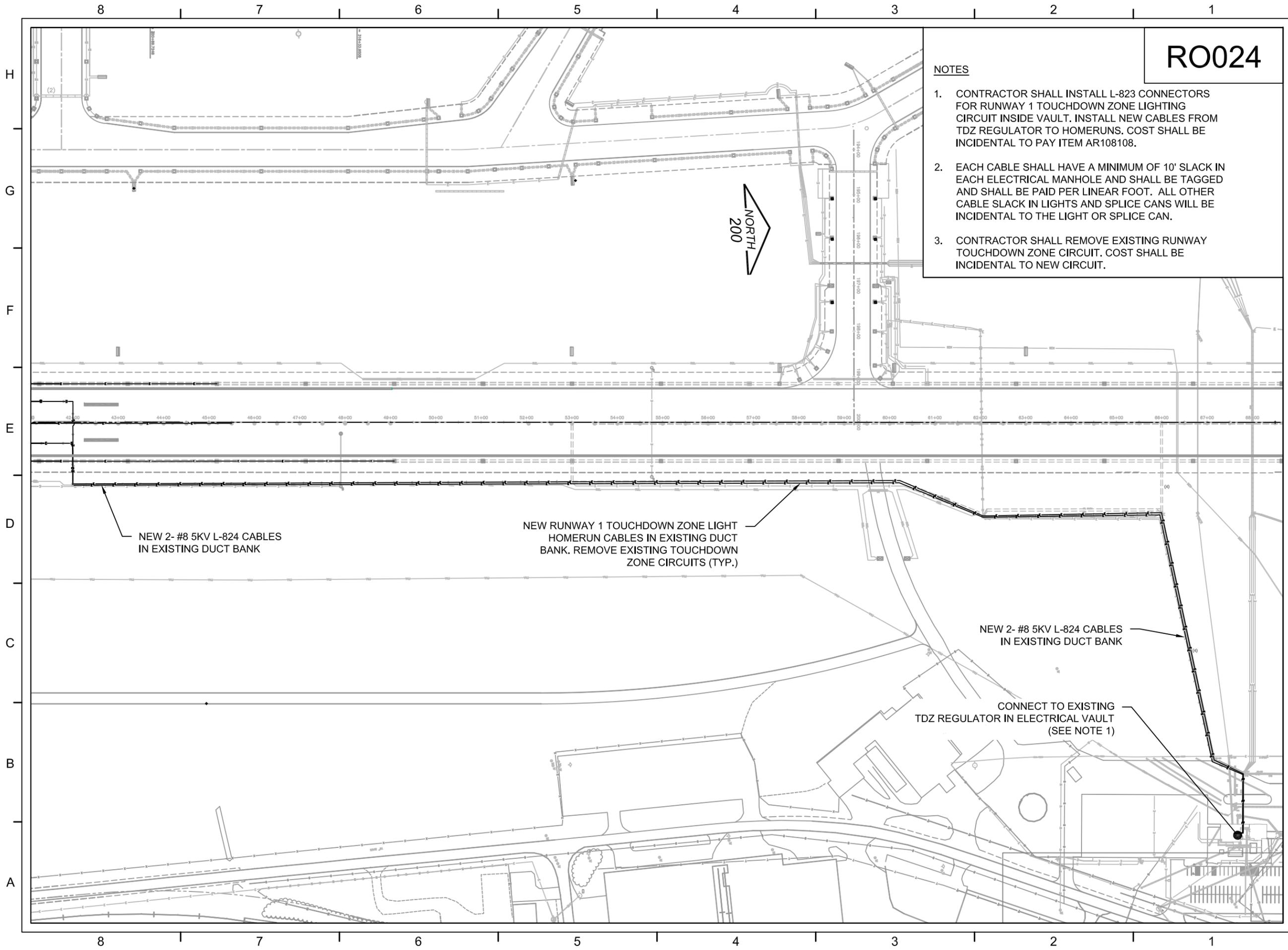


NEW 2- #8 5KV L-824 CABLES
IN EXISTING DUCT BANK

NEW RUNWAY 1 TOUCHDOWN ZONE LIGHT
HOMERUN CABLES IN EXISTING DUCT
BANK. REMOVE EXISTING TOUCHDOWN
ZONE CIRCUITS (TYP.)

NEW 2- #8 5KV L-824 CABLES
IN EXISTING DUCT BANK

CONNECT TO EXISTING
TDZ REGULATOR IN ELECTRICAL VAULT
(SEE NOTE 1)





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BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

SHEET TITLE:

ELECTRICAL DETAILS - 5

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **ELD5** REVISION

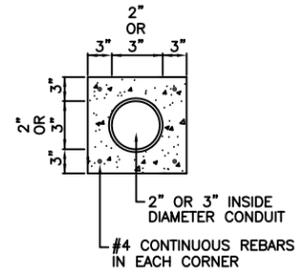
SHEET 31 OF 65 SHEETS

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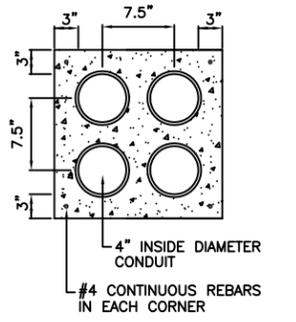
CONCRETE ENCASED

DUCT NOTES:

- DIMENSIONS ARE MINIMUM.
- CONCRETE SHALL CONFORM TO ITEM 610.
- ALL CONDUIT SHALL BE SCHEDULE 40 PVC.
- TOP OF CONCRETE ENCASEMENT IN TURF AREAS SHALL NOT BE LESS THAN 24" BELOW FINISHED GRADE.
- REBAR WILL NOT BE REQUIRED IN ONE-WAY CONCRETE ENCASED DUCT INSTALLED IN EXISTING PAVEMENT.

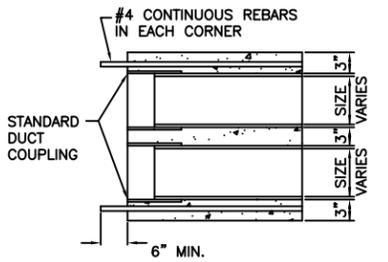


1-WAY

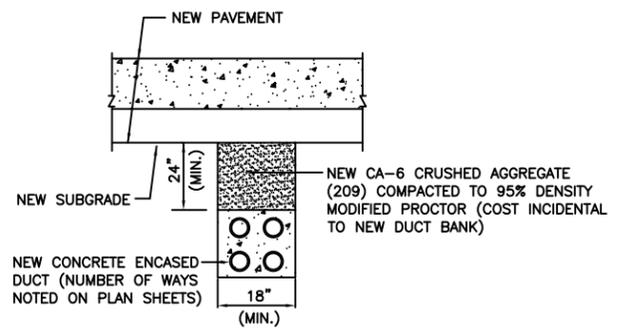


4-WAY

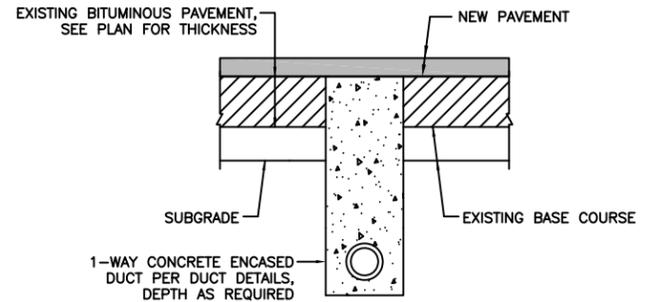
CONCRETE ENCASED DUCT BANKS
NOT TO SCALE



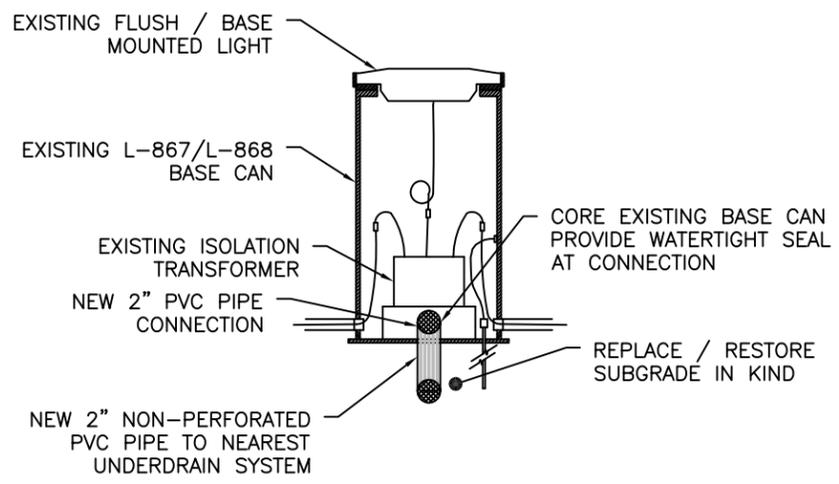
CONCRETE ENCASED DUCT END DETAIL
NO SCALE



CONCRETE ENCASED DUCT BACKFILL
NOT TO SCALE



1-WAY CONCRETE ENCASED DUCT INSTALLED IN EXISTING PAVEMENT
NOT TO SCALE

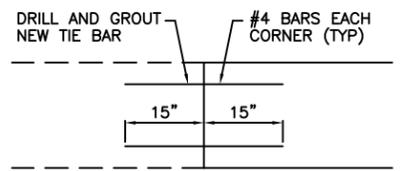


ELECTRICAL DRAIN CONNECTION DETAIL
NOT TO SCALE

NOTES

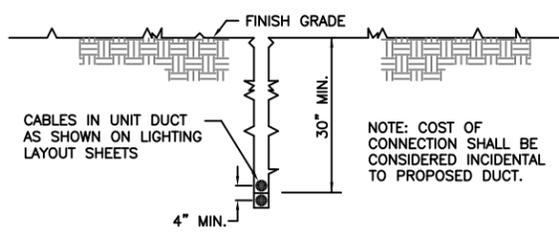
- THE EXISTING PAVEMENT STRUCTURE IN WHICH THE PROPOSED ELECTRICAL DRAIN CONNECTION SHALL BE INSTALLED VARIES. REMOVAL OF THE MATERIAL REGARDLESS OF PAVEMENT SECTION SHALL BE CONSIDERED INCIDENTAL TO ELECTRICAL DRAIN CONNECTION.
- INSTALL PVC DRAIN PIPE PER 1-WAY CONCRETE ENCASED DUCT DETAIL THIS SHEET.
- CONCRETE BACKFILL SHALL BE PLACED FROM THE DRAIN/CAN CONNECTION POINT TO THE ENTRY POINT OF THE TRENCH ENVELOPE. THE VOID SHALL BE FILLED VERTICALLY TO THE PROPOSED SUBGRADE LEVEL OR THE EXISTING PAVEMENT GRADE PRIOR TO IMPROVEMENTS.
- MAX INVERT SHALL BE 0.08' FROM THE BOTTOM OF THE BASE CAN.

**ELECTRICAL DETAILS - 5
(ELD5)**



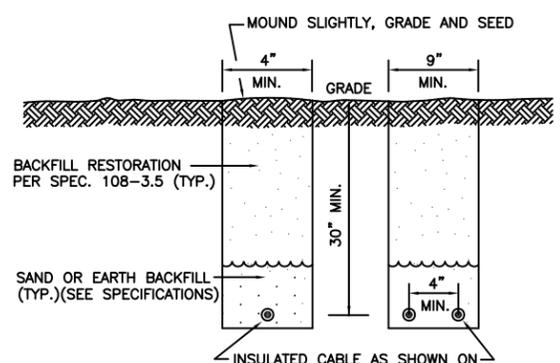
EXTENSION OF EXISTING DUCT
NOT TO SCALE

NOTE: COST OF CONNECTION SHALL BE CONSIDERED INCIDENTAL TO NEW DUCT.



CABLE IN UNIT DUCT - PLOWED
NOT TO SCALE

NOTE: AT CONTRACTOR'S OPTION, CABLE PLOWING MAY BE USED IN LIEU OF TRENCHING.



TRENCH DETAIL
NOT TO SCALE

NOTES

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL.

NOTE: AT CONTRACTOR'S OPTION, CABLE PLOWING MAY BE USED IN LIEU OF TRENCHING.



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APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION
PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:

MARKING DETAILS - 1

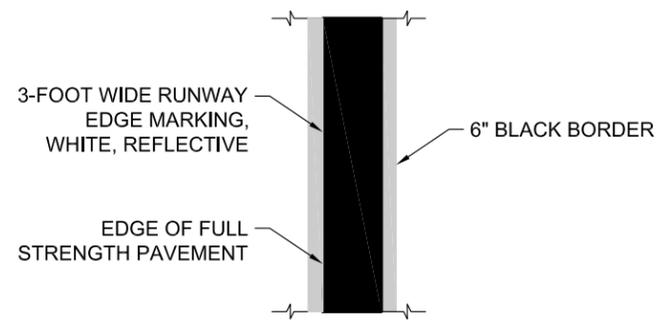
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

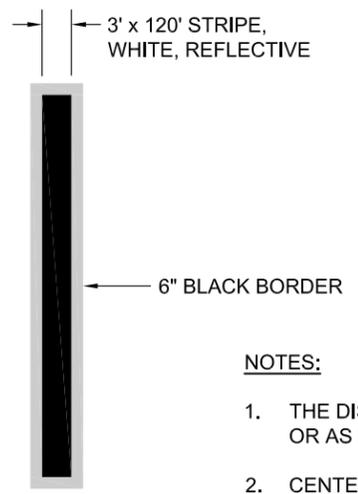
SHEET NO. **MRD1** REVISION

SHEET 33 OF 65 SHEETS

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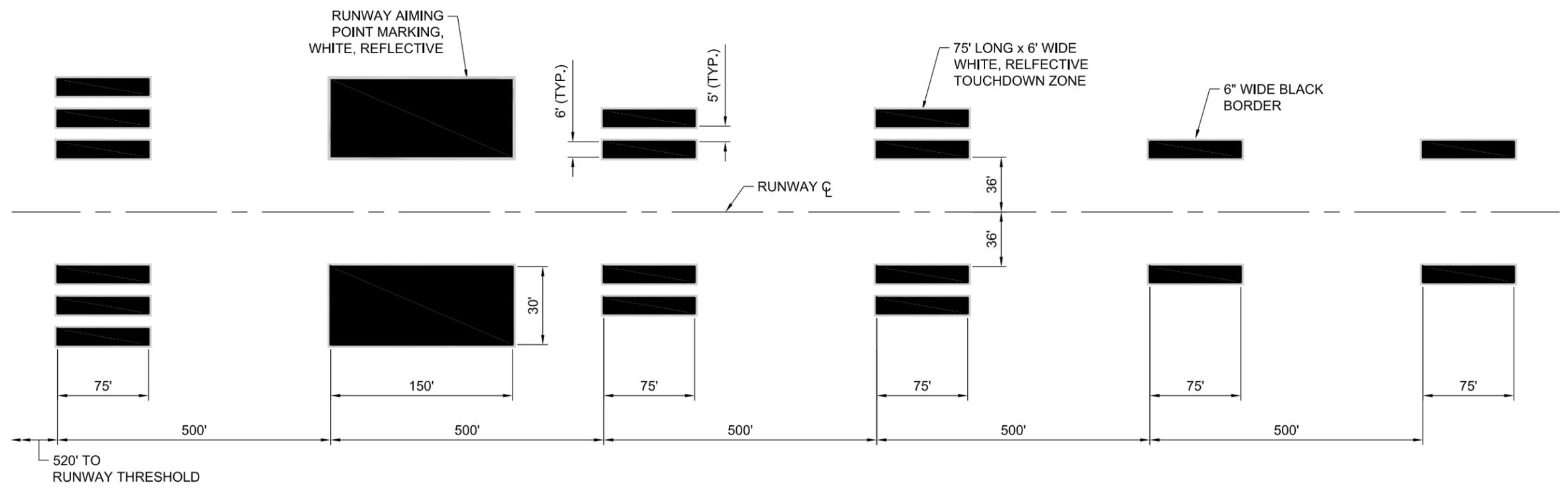
**RUNWAY EDGE MARKINGS,
CONTINUOUS**
NO SCALE



**RUNWAY CENTERLINE,
DASHED**
NO SCALE

NOTES:

1. THE DISTANCE BETWEEN STRIPES SHALL BE 80' OR AS NOTED ON THE PLAN DRAWINGS.
2. CENTERLINE STRIPES ARE CENTERED ABOUT THE RUNWAY CENTERLINE.



RUNWAY TOUCHDOWN ZONE MARKING
NO SCALE

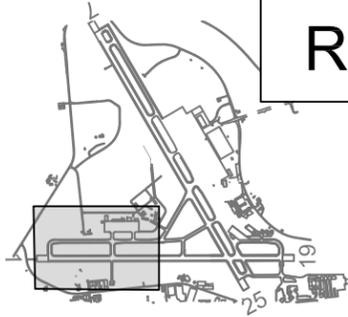
**MARKING DETAILS - 1
(MRD1)**

BITUMINOUS PAVEMENT CORES

BORING / CORE NUMBER	HMA THICKNESS SURFACE & BINDER (INCHES)	AGG. BASE THICKNESS (INCHES)
B-1	12.6	5.0
B-2	11.0	13.0
B-3	9.8	14.0
B-4	10.2	14.0
B-5	14.5	11.0
B-6	17.8	13.0
B-7	18.2	17.0

NOTES:
ADDITIONAL SOIL BORING INFORMATION IS AVAILABLE UPON REQUEST.

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Greater Rockford Airport Authority

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BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:

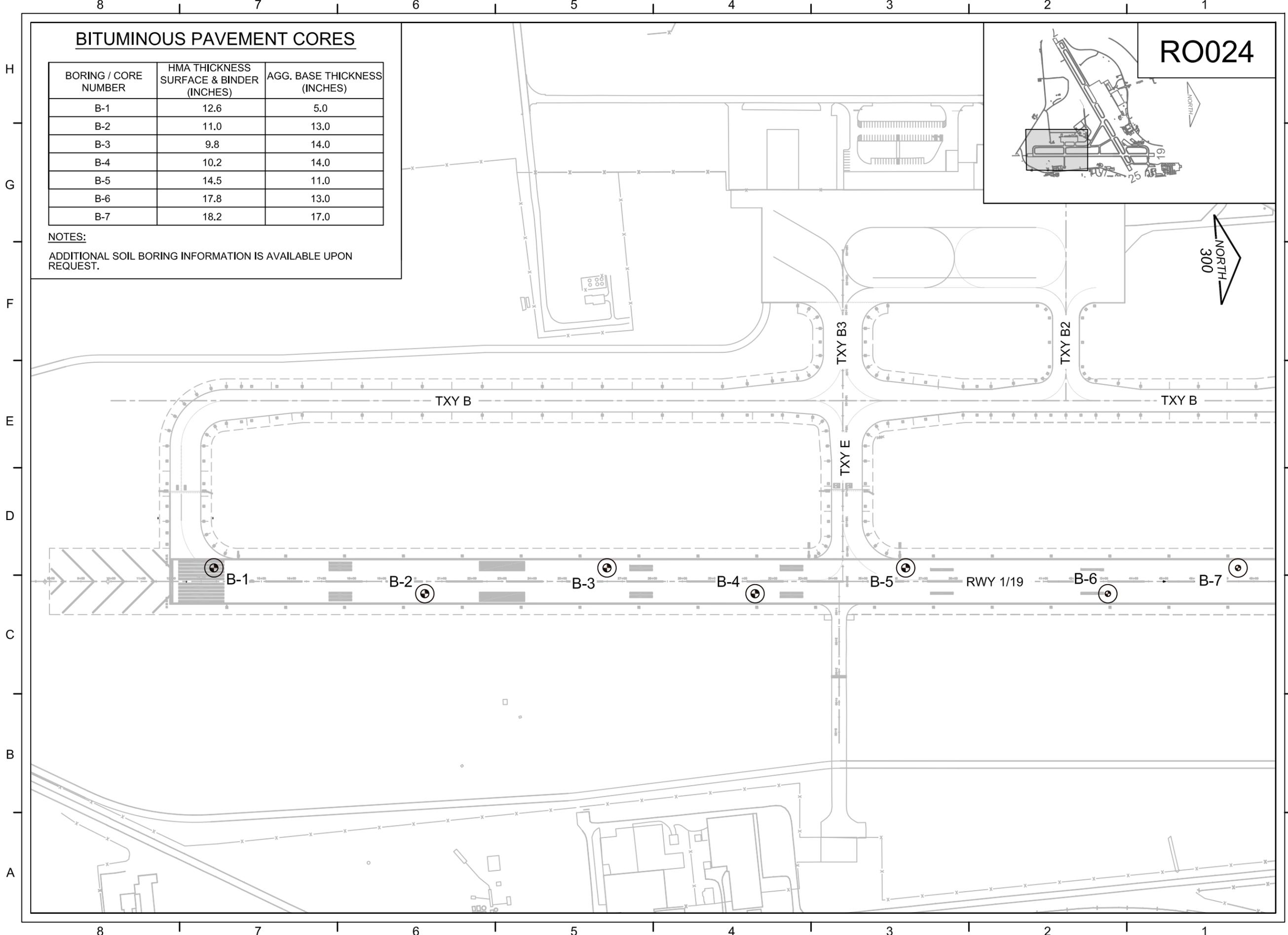
ENGINEERING
INFORMATION

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **ENG1** REVISION

SHEET 37 OF 65 SHEETS





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REV DATE DESCRIPTION
PROJECT NAME:

**REHABILITATE
RUNWAY 1/19
PHASE 3**

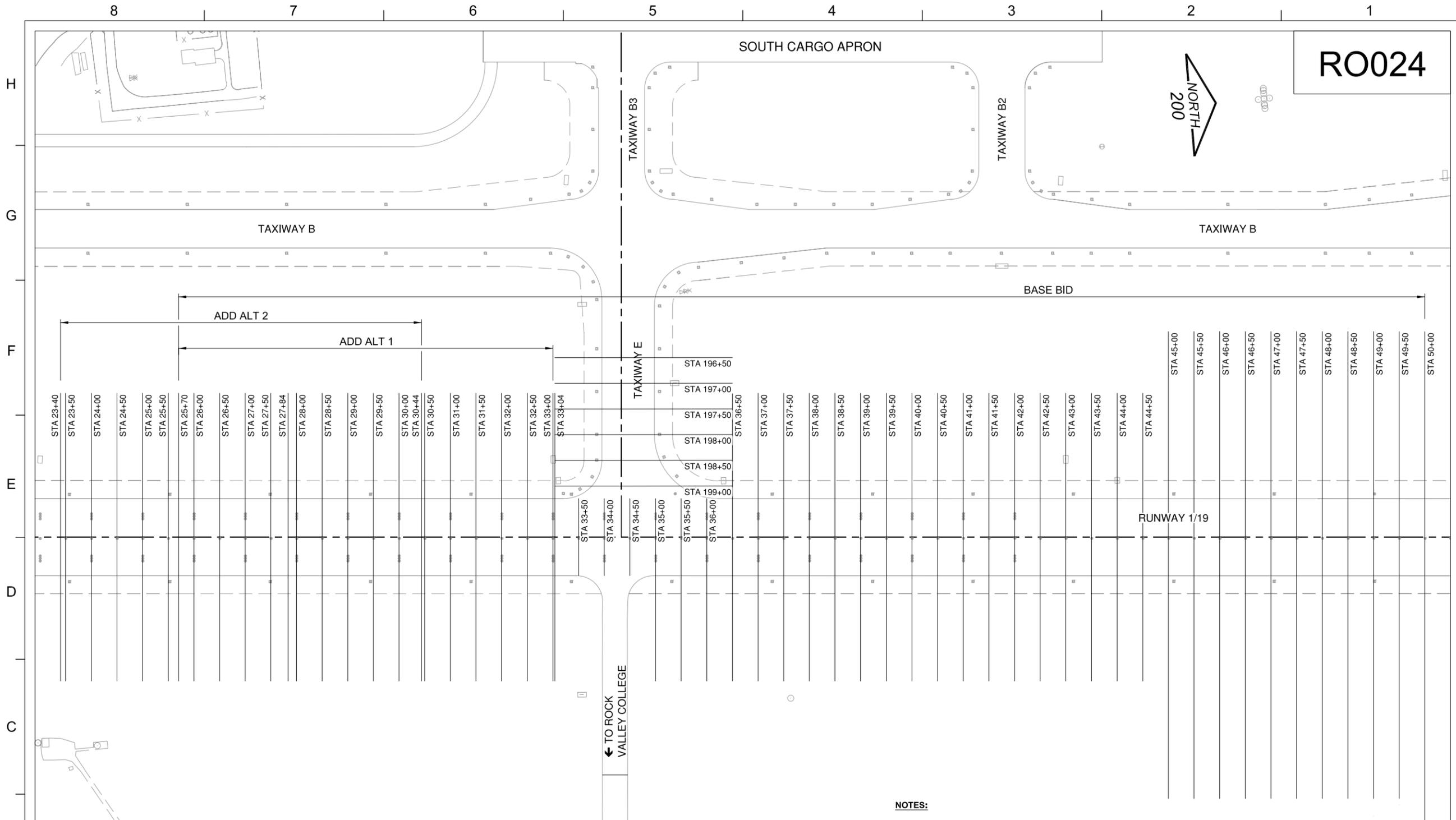
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**INDEX TO CROSS
SECTIONS AND
EARTHWORK SUMMARY**

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PROJECT NO.: 12258-05-00/06
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- NOTES:**
- NO SHRINKAGE FACTOR HAS BEEN APPLIED TO TOPSOIL FILL MATERIALS.
 - A SHRINKAGE FACTOR OF 1.15 HAS BEEN APPLIED TO NON-TOPSOIL FILL MATERIALS.
 - ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON MATERIAL IN ITS INITIAL OR FINAL POSITION AS SHOWN IN THE PLANS AND QUANTIFIED BY THE METHOD OF AVERAGE END AREAS.
 - PAYMENT FOR UNCLASSIFIED EXCAVATION IS THE SUM OF TOPSOIL STRIPPING AND UNCLASSIFIED EXCAVATION AND IS TO BE PAID UNDER ITEM AR152410 IN ITS INITIAL POSITION.
 - TOPSOIL CAN BE USED AS SHOULDER FILL.
 - ANY STOCKPILING AND RE-HANDLING OF EARTH MATERIALS REQUIRED DURING THE PROJECT SHALL BE INCIDENTAL TO THE CONTRACT.
 - EXCESS EARTH MATERIALS SHALL BE HAULED OFF AIRPORT PROPERTY, INCIDENTAL TO THE CONTRACT.
 - TOPSOIL PLACEMENT AND SHOULDER FILL ARE INCIDENTAL TO UNCLASSIFIED EXCAVATION (ITEM AR152410).
 - PAYMENT FOR AR905510, TOPSOILING (FROM ON SITE) IS FOR ANY FILL NECESSARY TO COMPLETE THE PROJECT AND SHALL BE PRE-APPROVED BY THE RESIDENT ENGINEER.

EARTHWORK SUMMARY TABLE

LOCATION	CUT		FILL			
	TOPSOIL STRIPPING (INITIAL POSITION) AR152410	UNCLASSIFIED EXCAVATION (INITIAL POSITION) AR152410	TOTAL FILL INCLUDING TOPSOIL (FINAL POSITION)	TOPSOIL FILL (SEE NOTE 1)	NON-TOPSOIL FILL	NON-TOPSOIL FILL AVAILABLE (SEE NOTE 2)
BASE BID STA 25+70 TO STA 49+03.56	4,126 CY	4,083 CY	7,655 CY	4,126 CY	3,529 CY	3,552 CY
BASE BID TXY E	151 CY	11 CY	200 CY	151 CY	48 CY	9 CY
ADD ALT 1 STA 25+70 TO STA 33+03.56	376 CY	13 CY	451 CY	376 CY	75 CY	11 CY
ADD ALT 2 STA 23+40 TO STA 30+43.56	357 CY	3 CY	400 CY	357 CY	43 CY	3 CY



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REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:

RUNWAY 1/19
CROSS SECTIONS 1

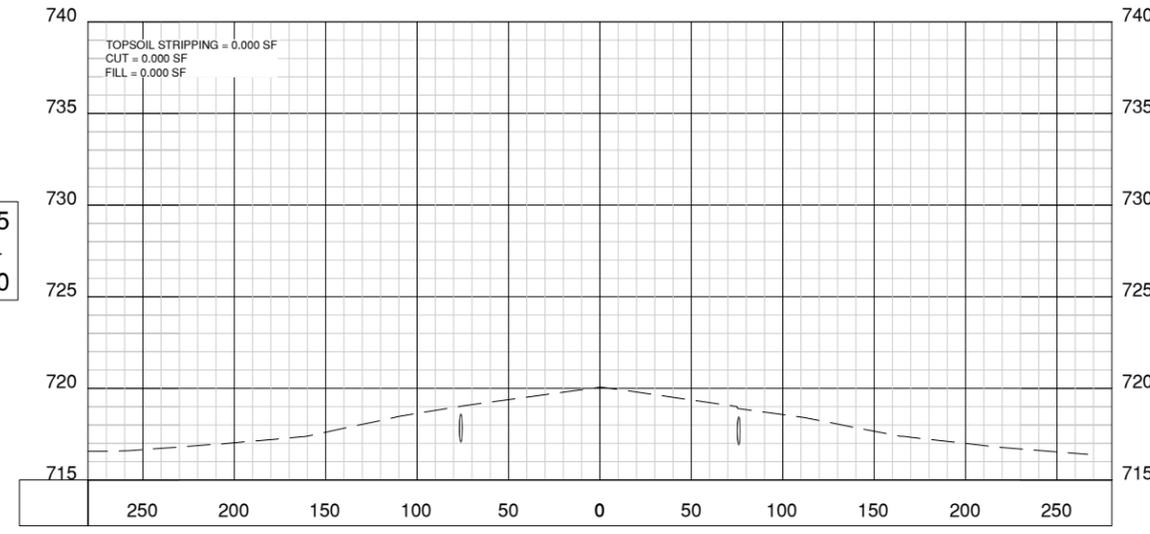
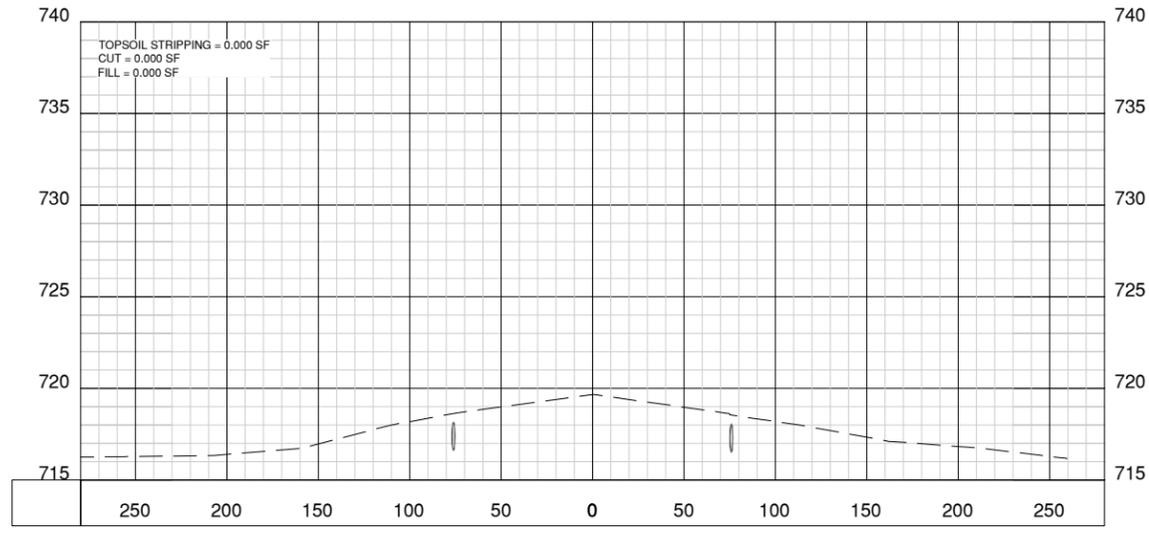
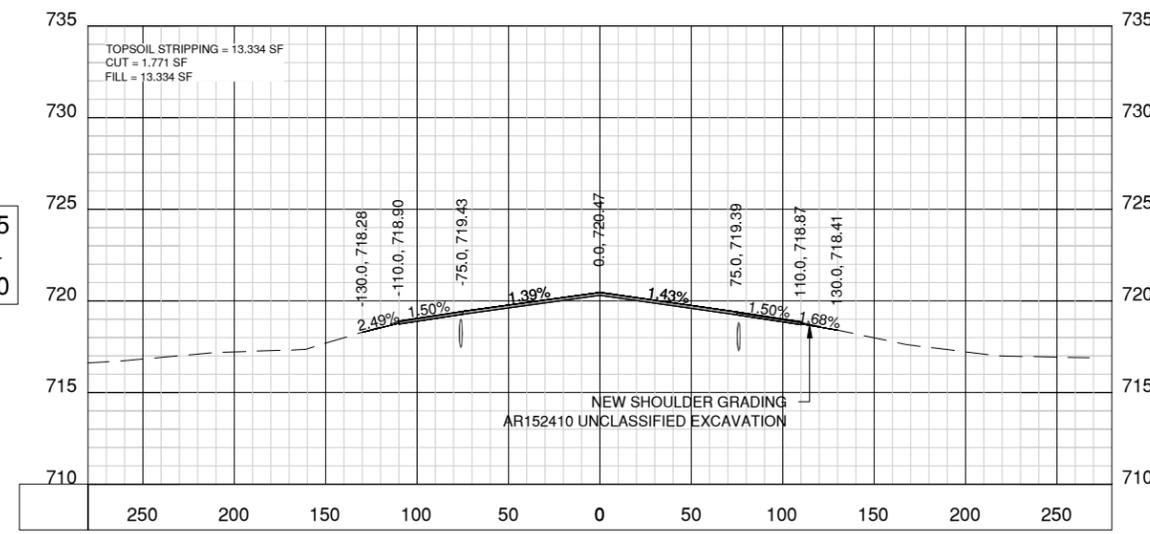
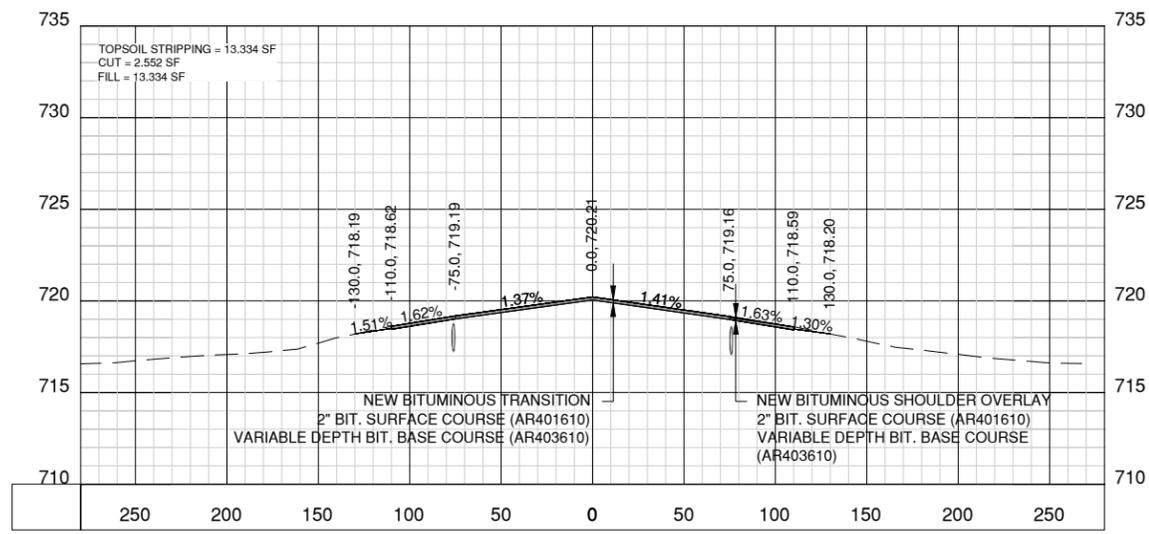
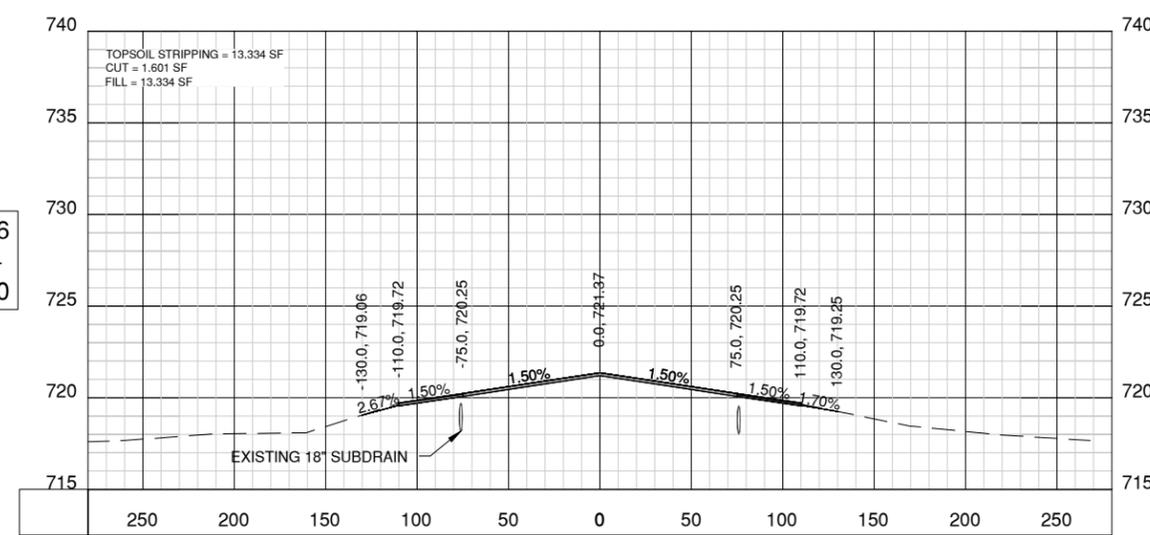
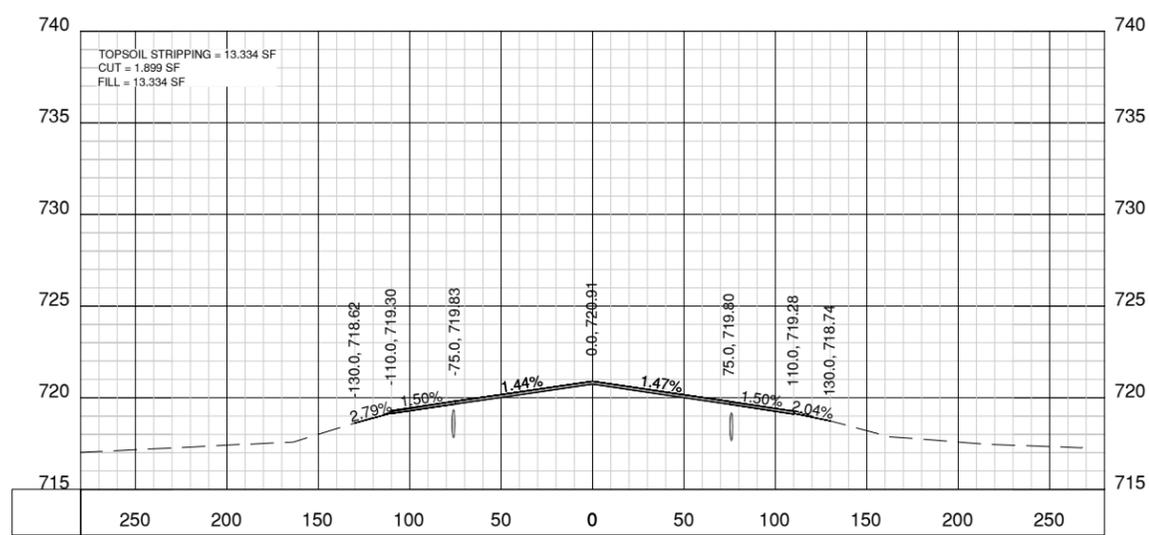
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**REHABILITATE
RUNWAY 1/19
PHASE 3**

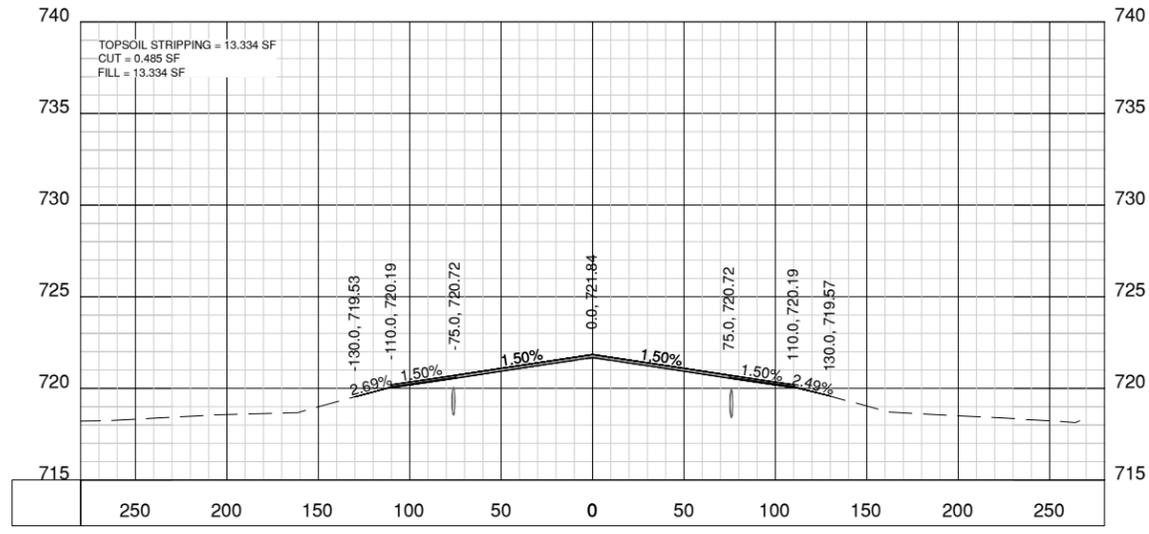
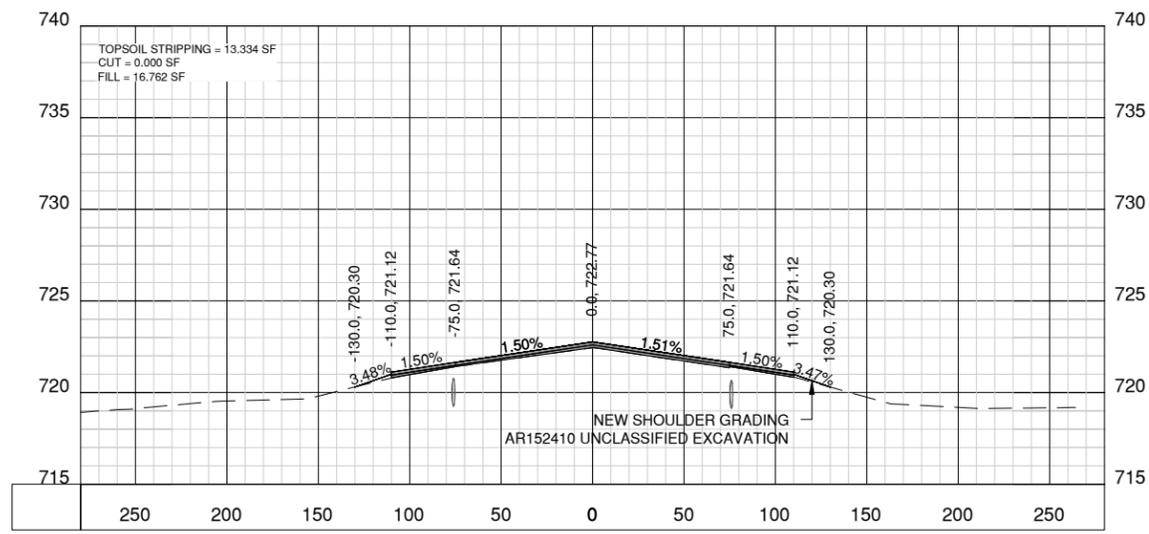
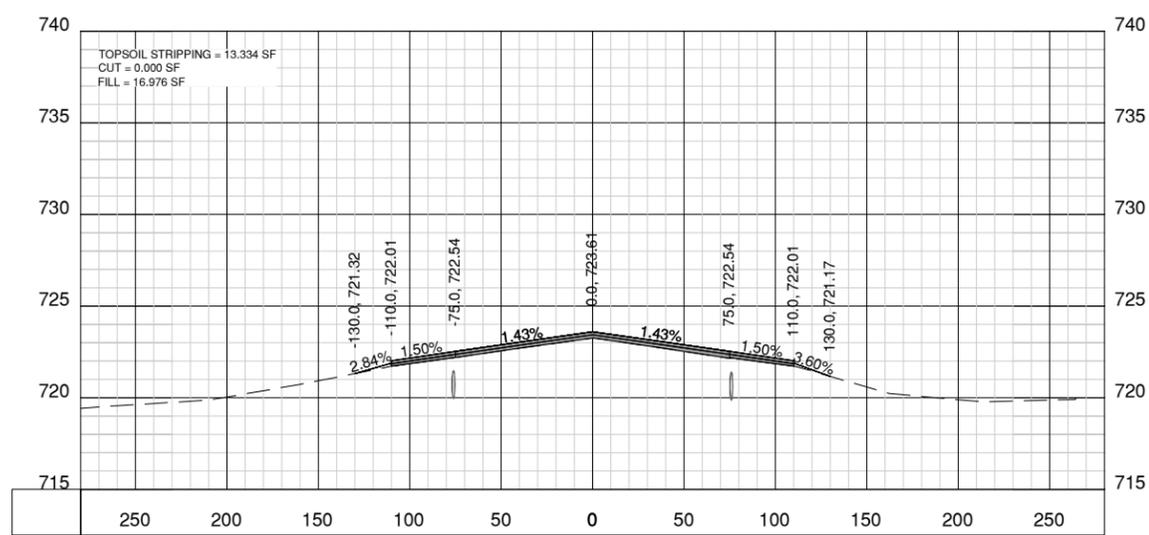
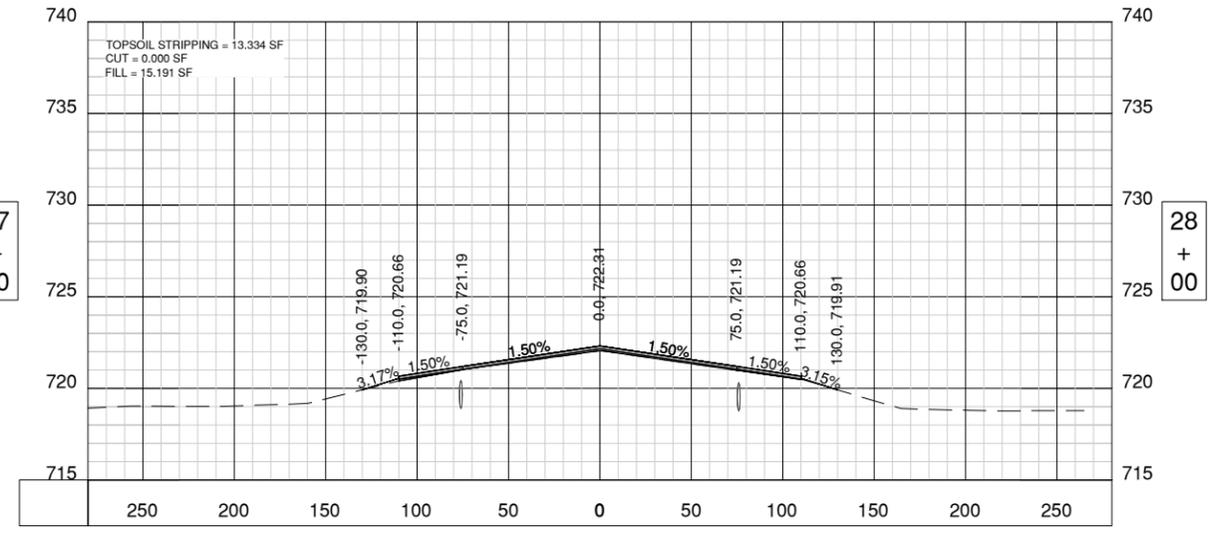
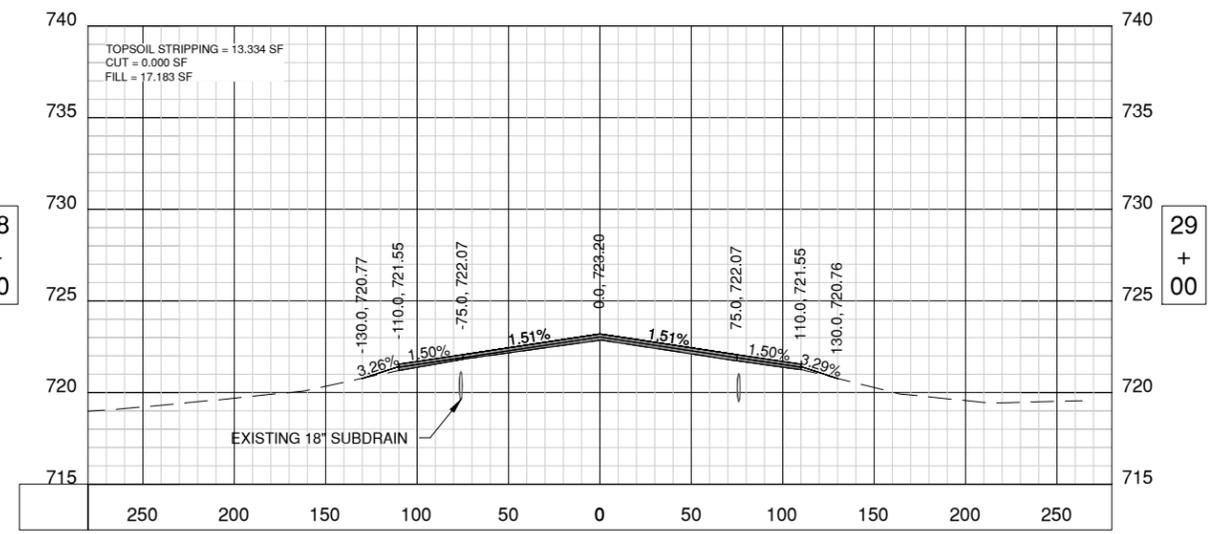
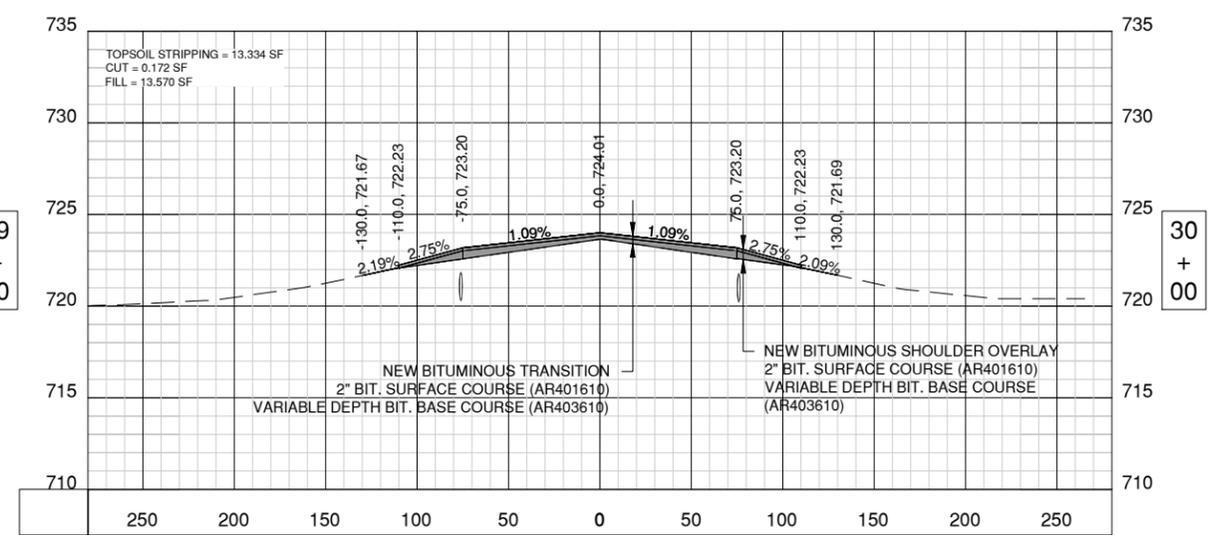
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CROSS SECTIONS 2**

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SHEET NO. **XSC2** REVISION
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REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:

RUNWAY 1/19
CROSS SECTIONS 3

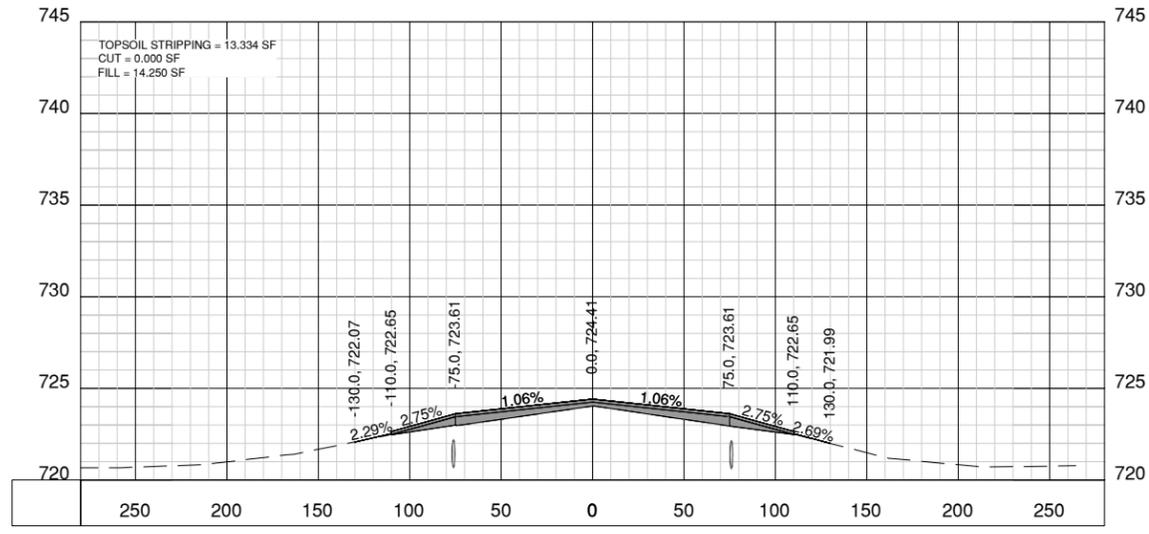
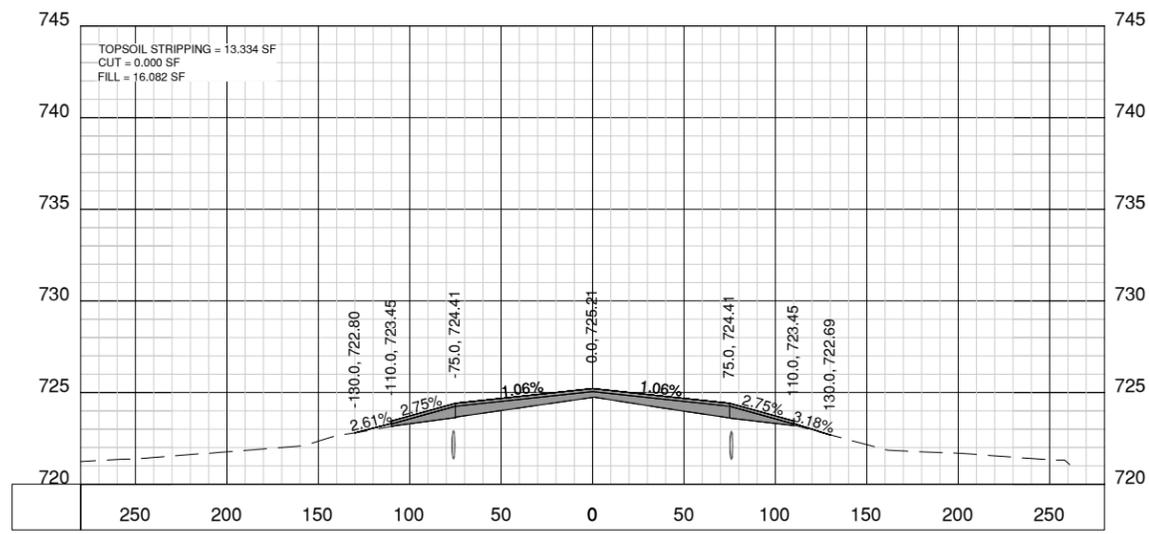
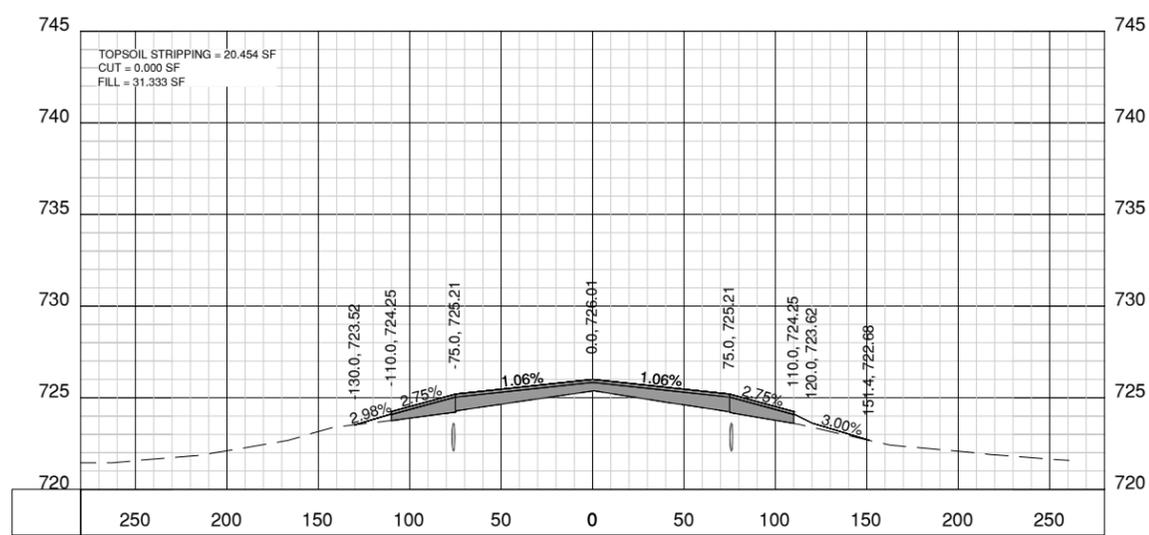
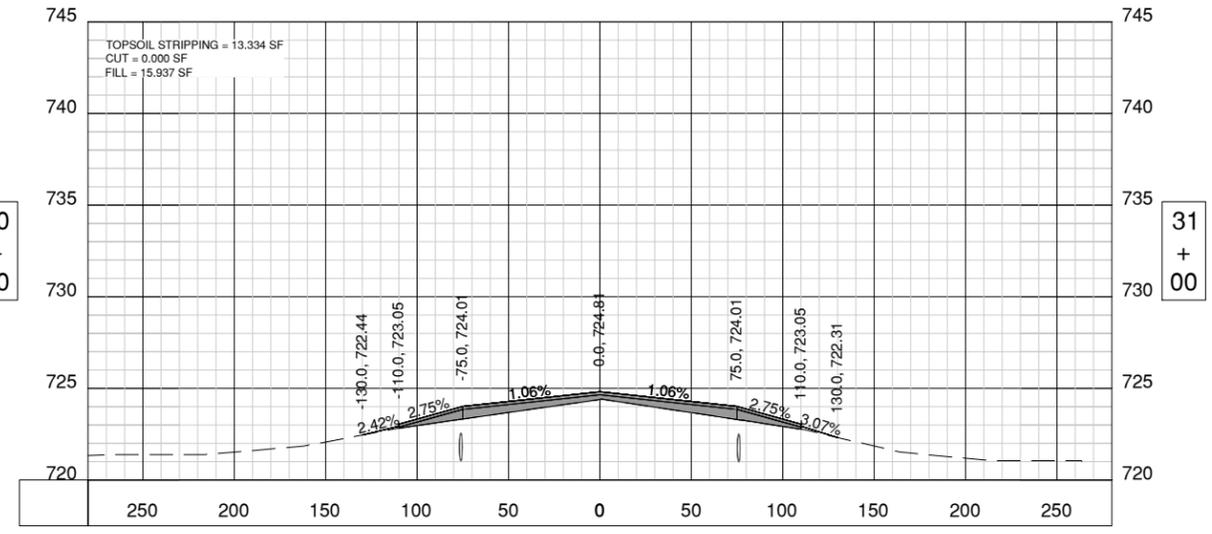
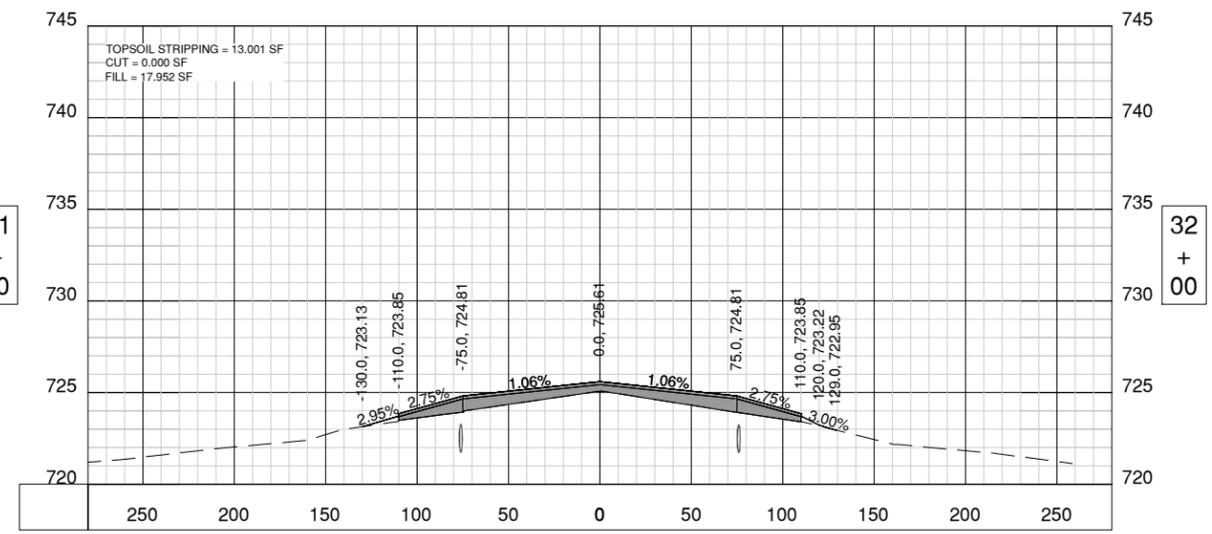
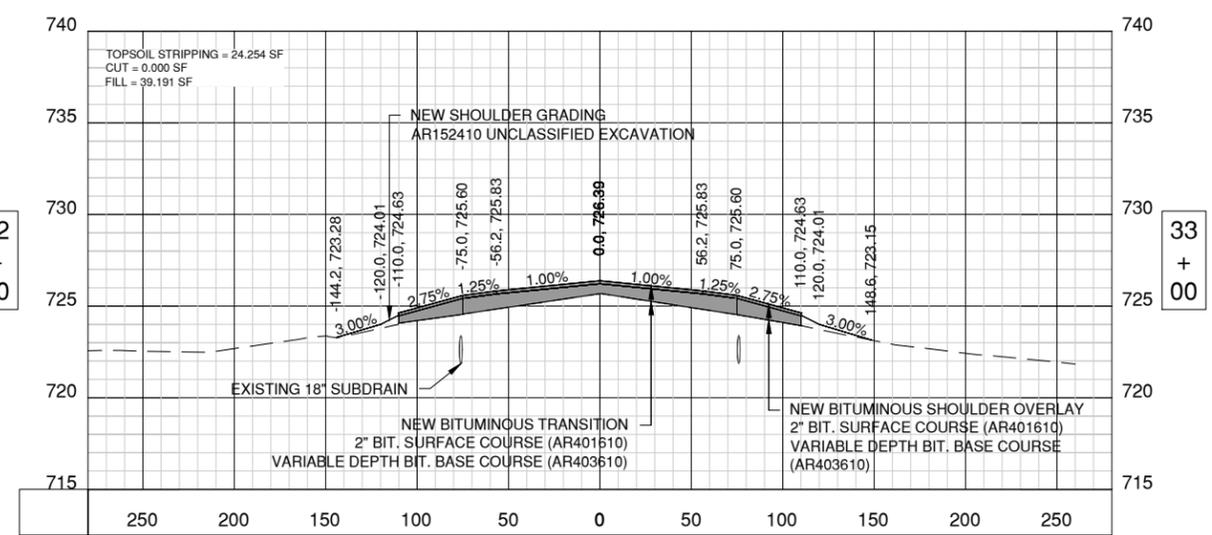
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REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:

RUNWAY 1/19
CROSS SECTIONS 4

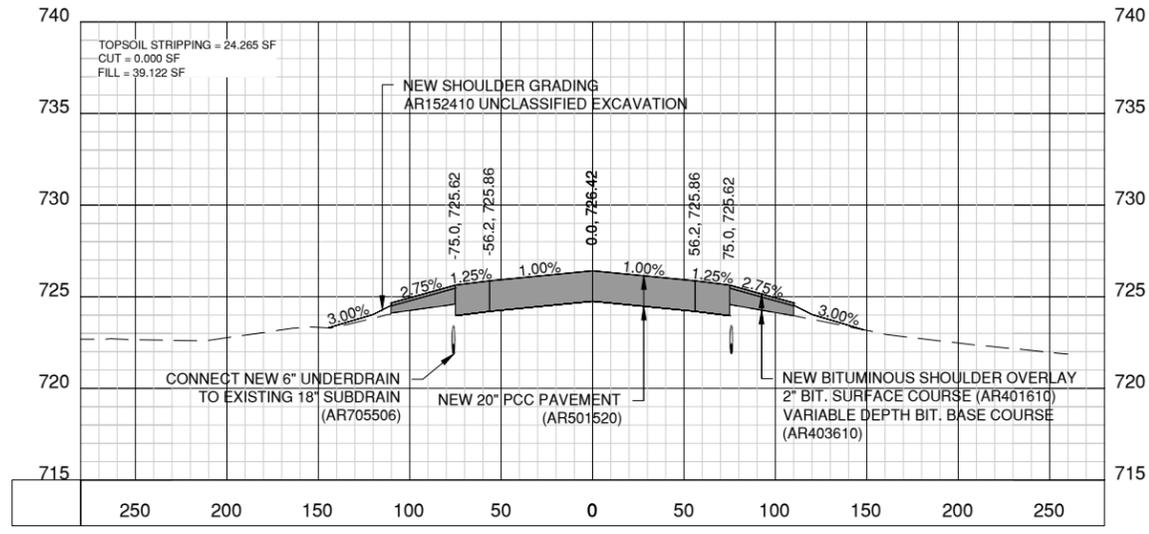
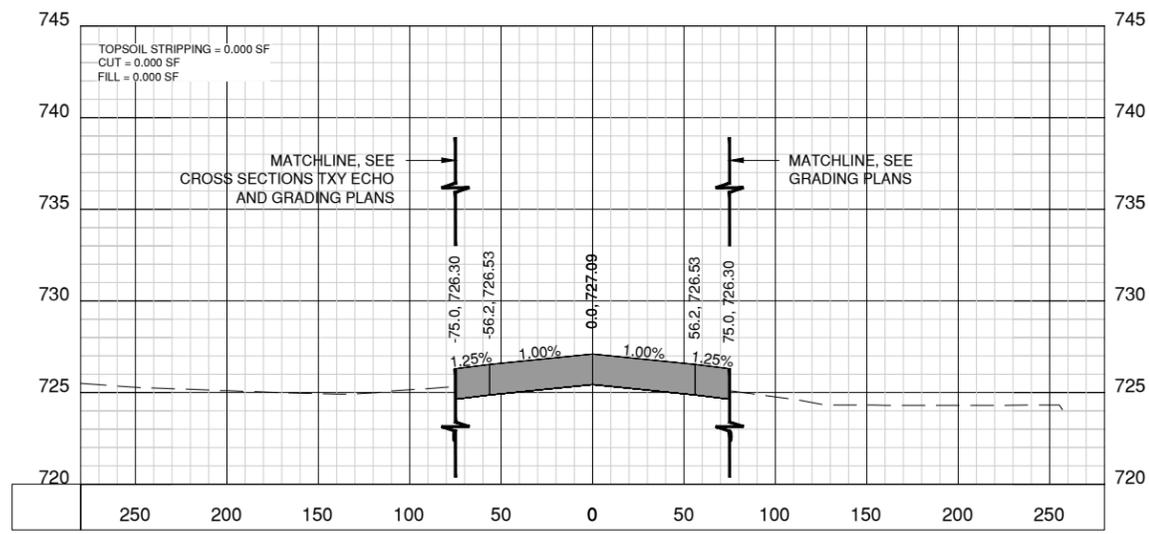
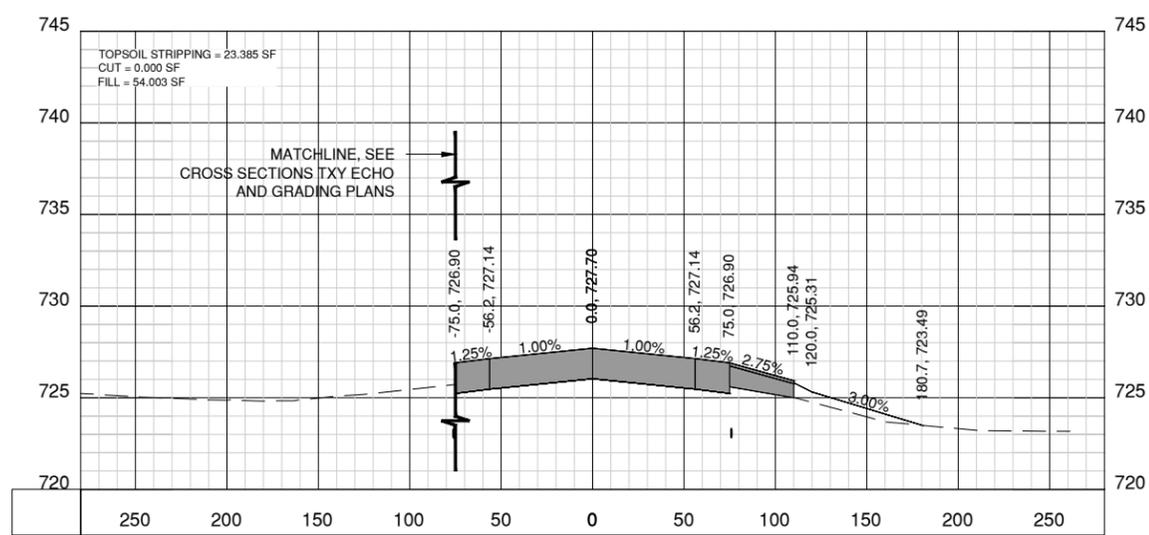
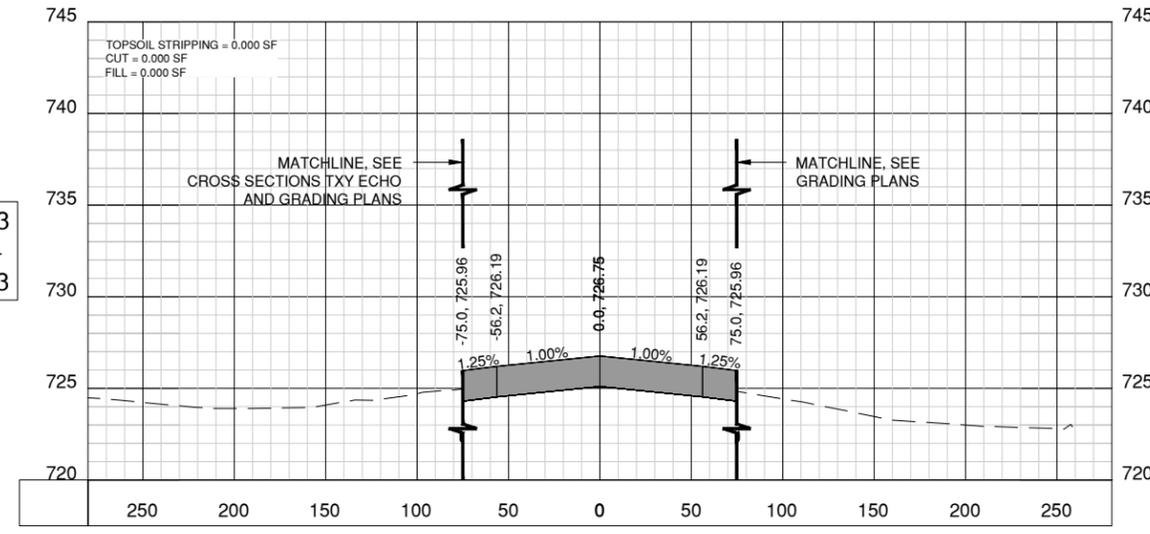
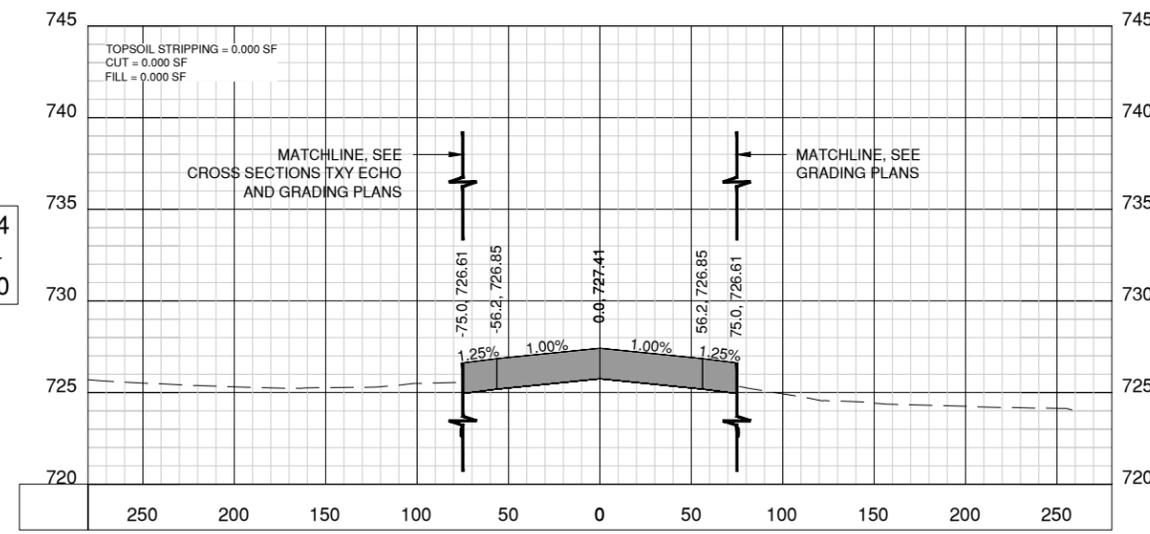
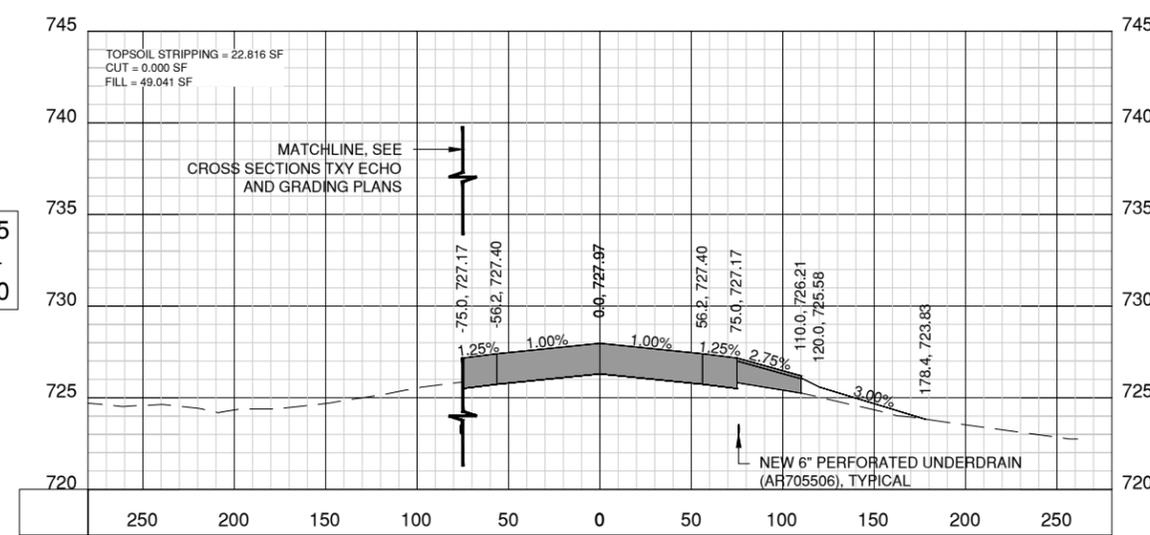
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PROJECT NO.: 12258-05-00/06
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SHEET NO. **XSC4** REVISION

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REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:

RUNWAY 1/19
CROSS SECTIONS 5

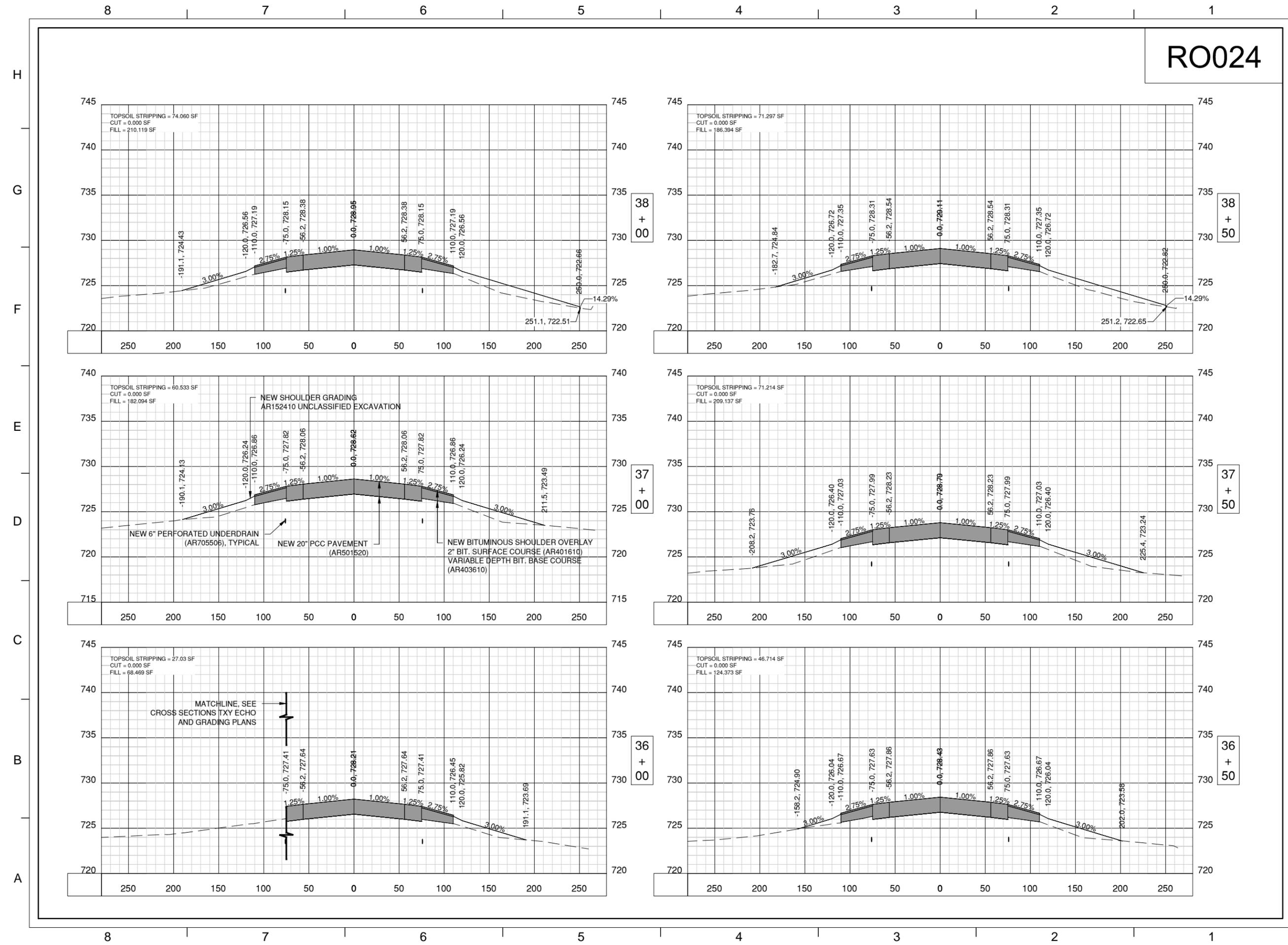
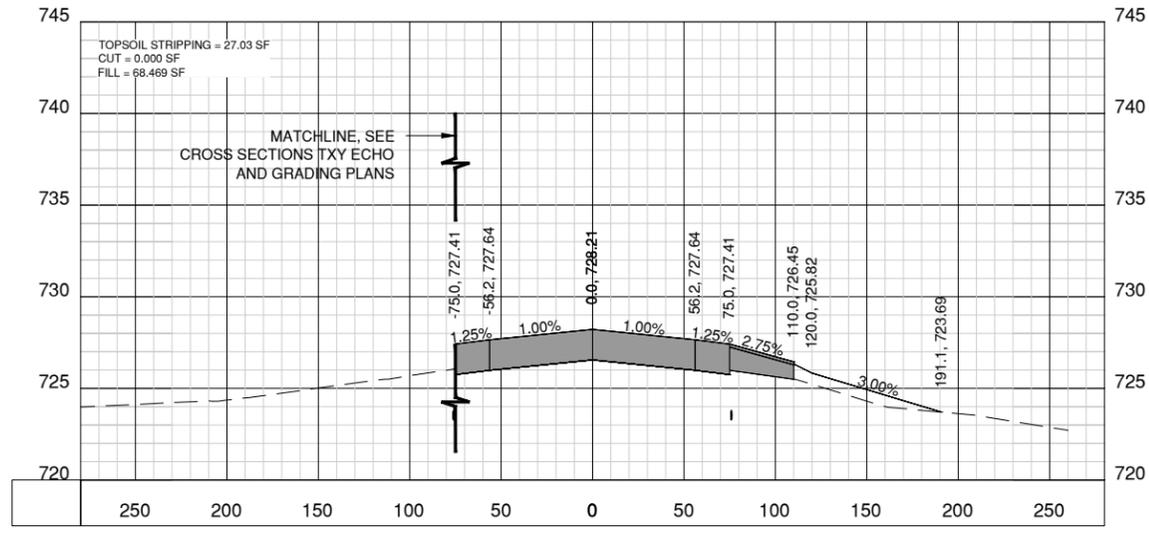
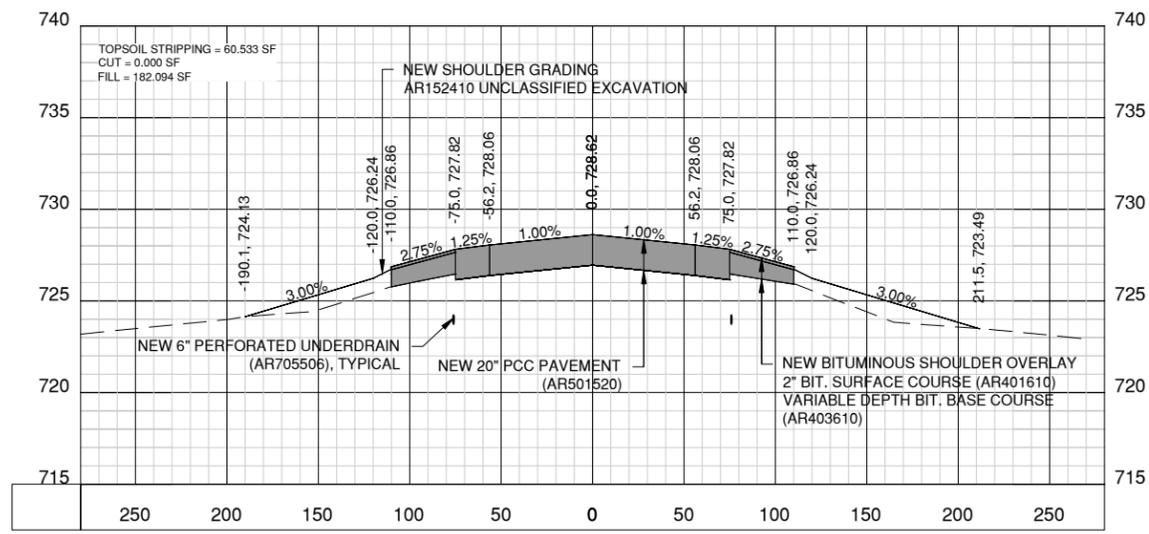
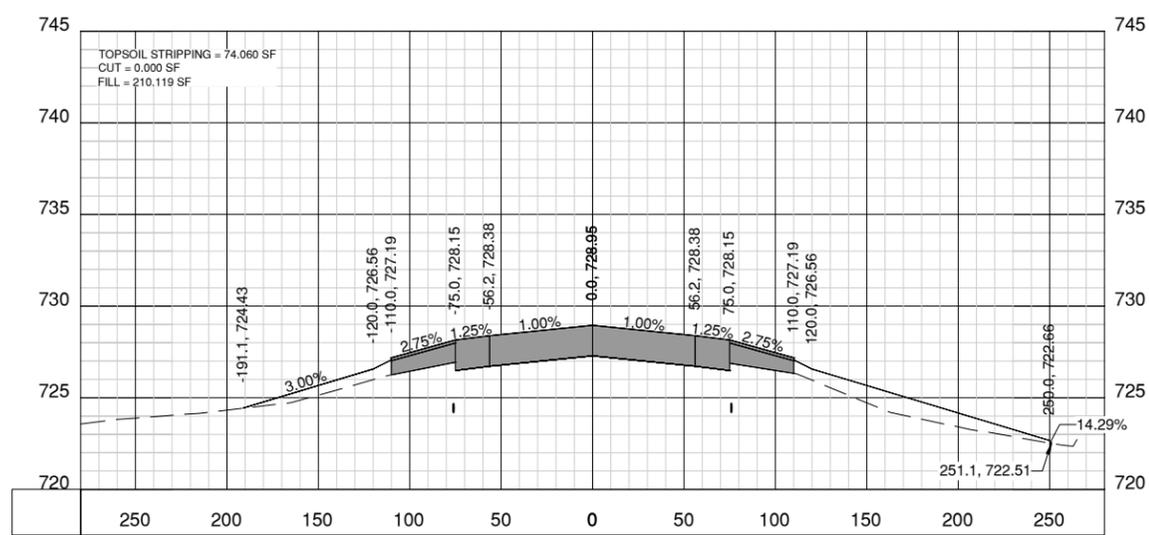
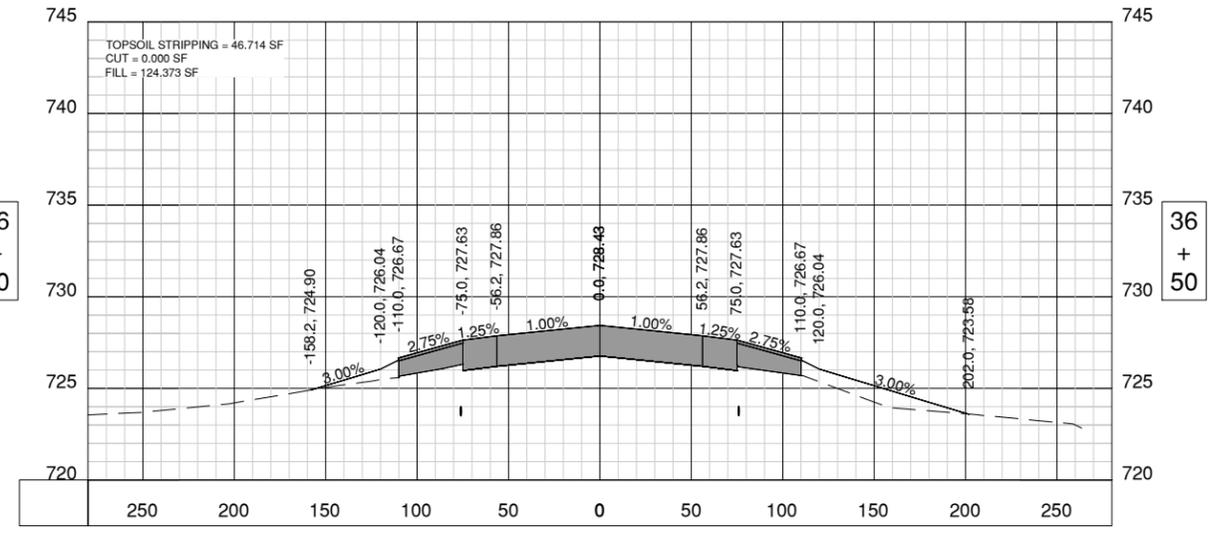
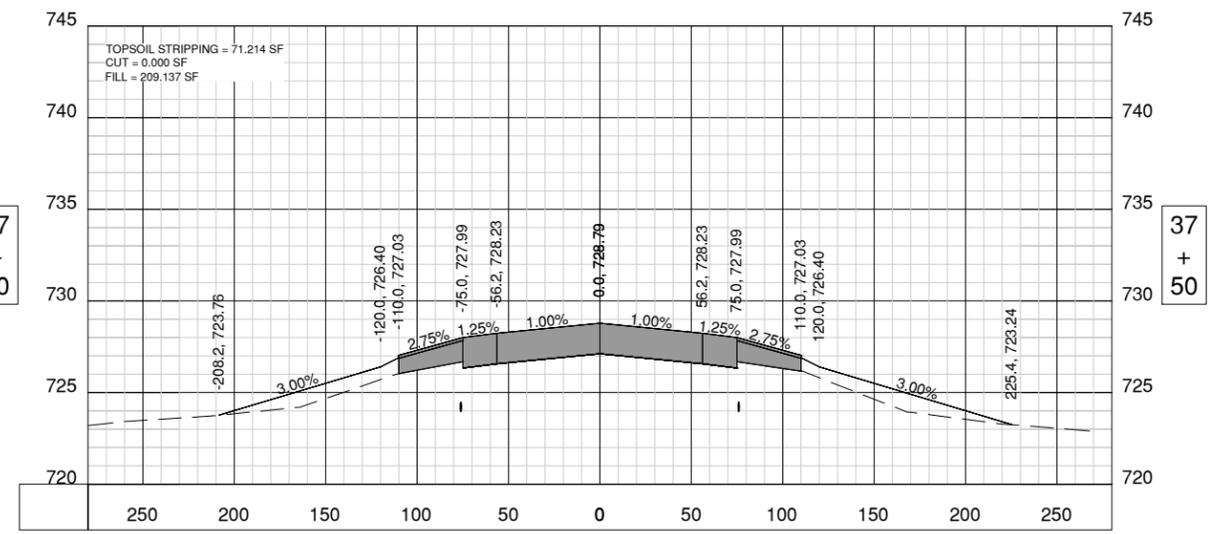
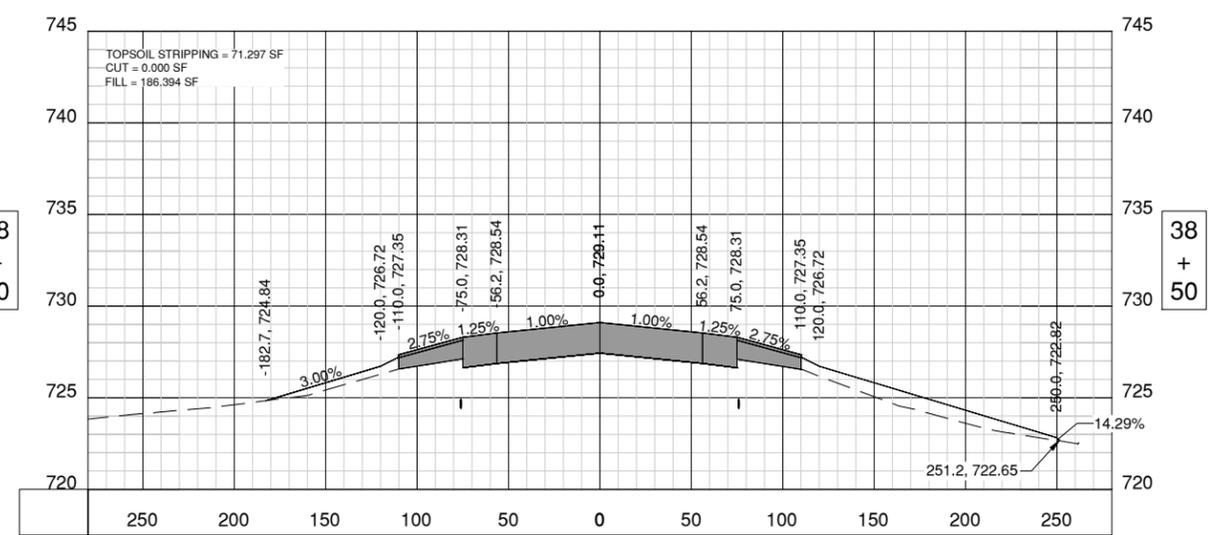
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BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

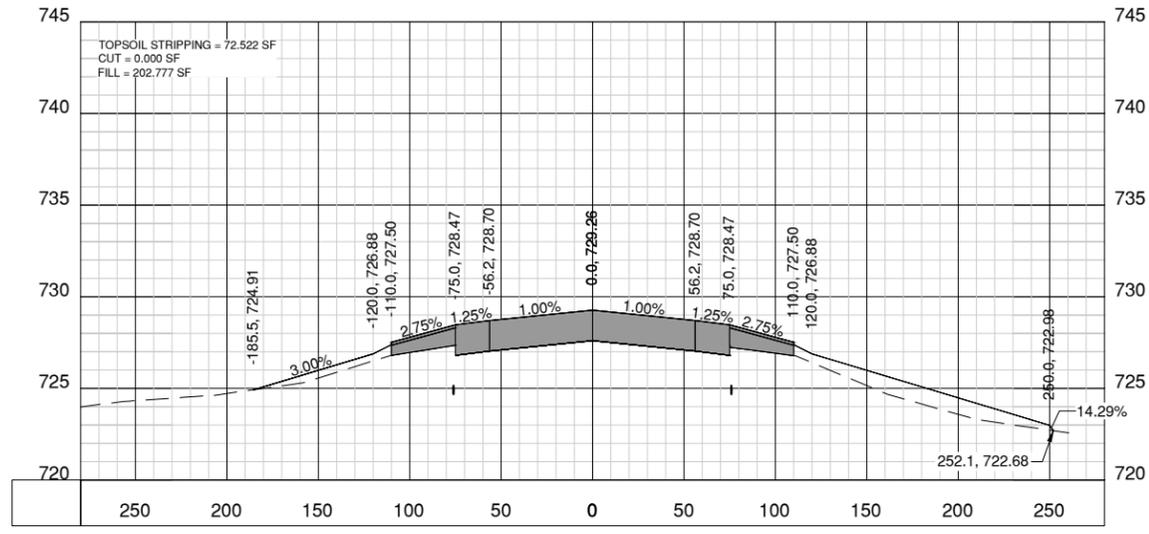
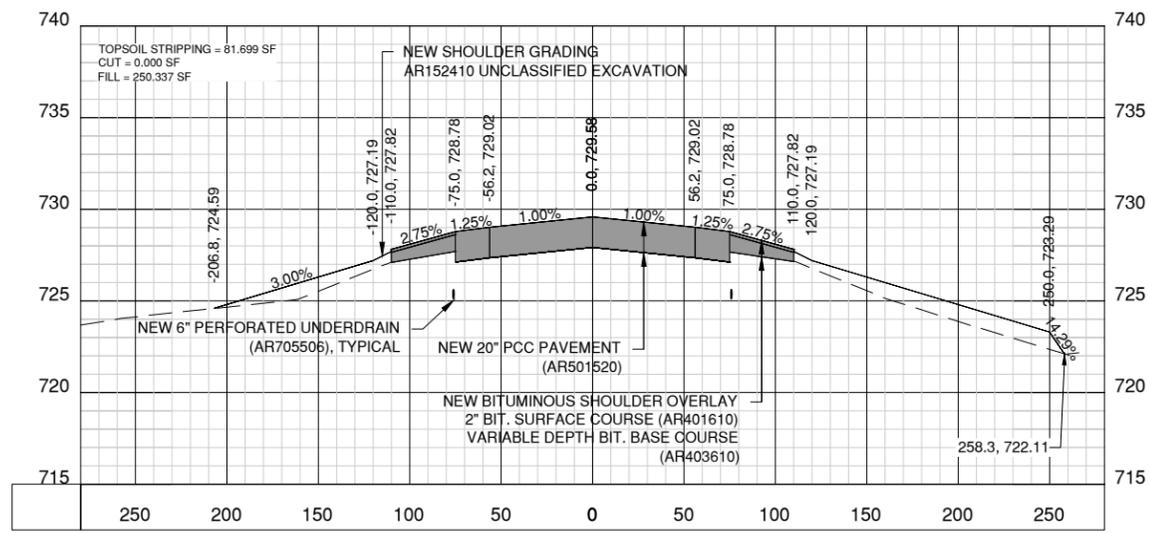
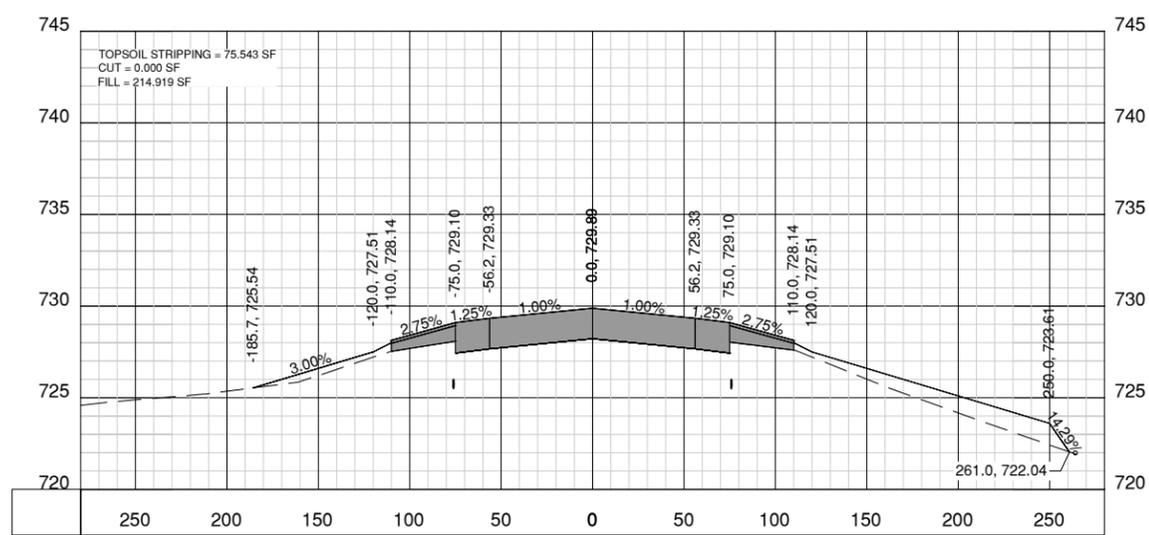
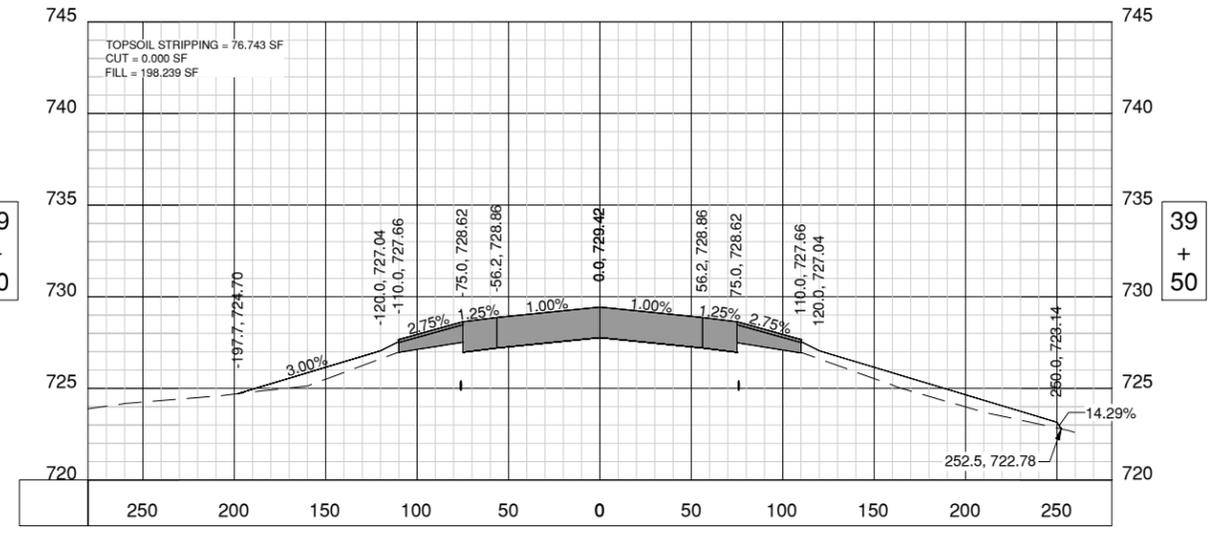
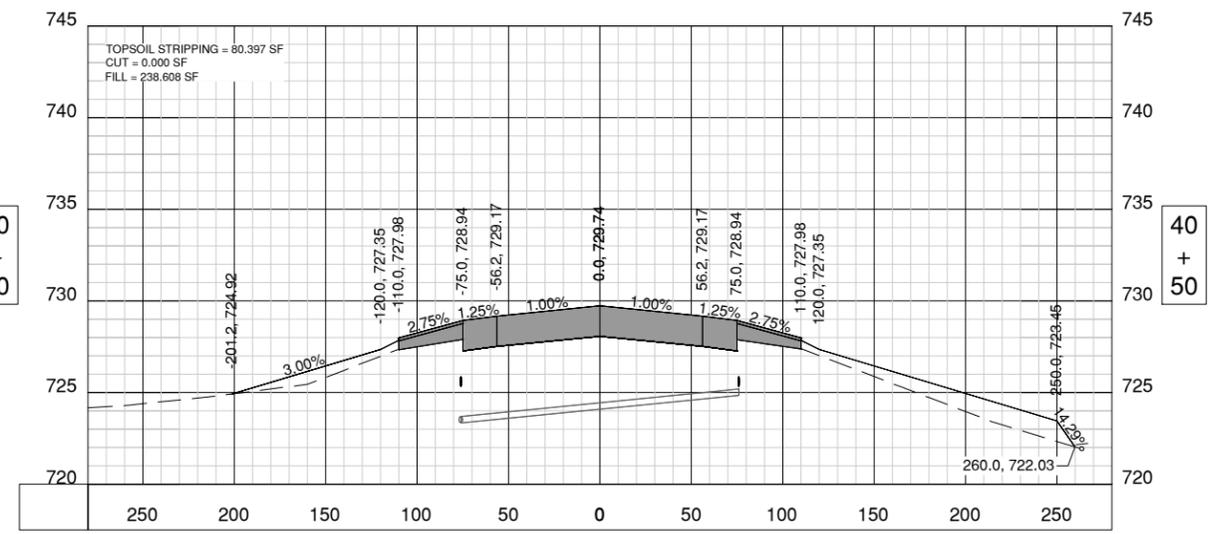
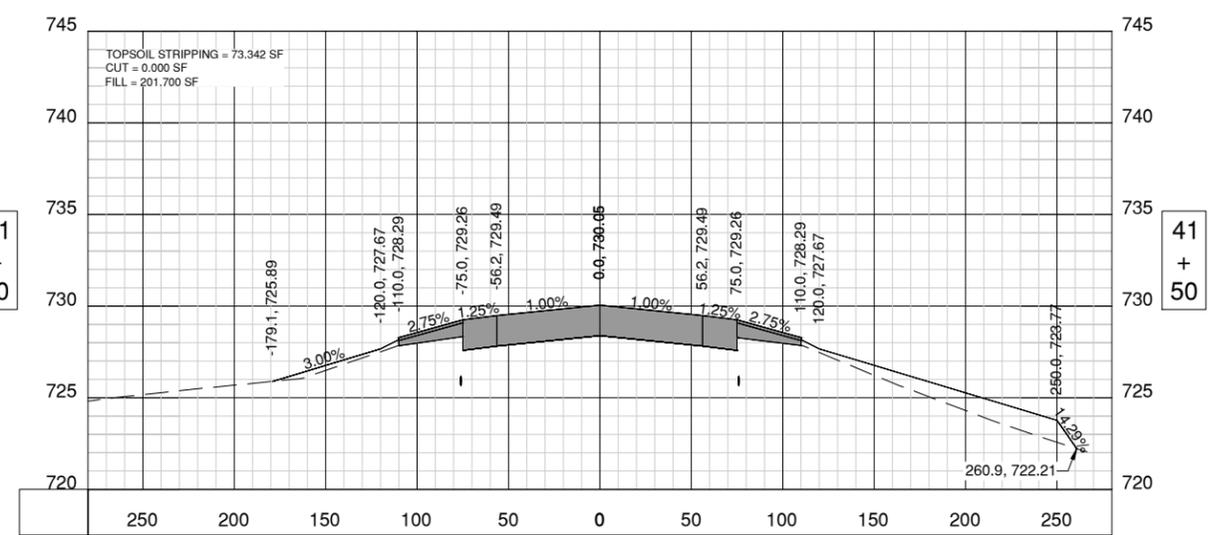
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CROSS SECTIONS 6**

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SHEET NO. **XSC6** REVISION
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IL Proj: RFD-4360

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REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:

RUNWAY 1/19
CROSS SECTIONS 7

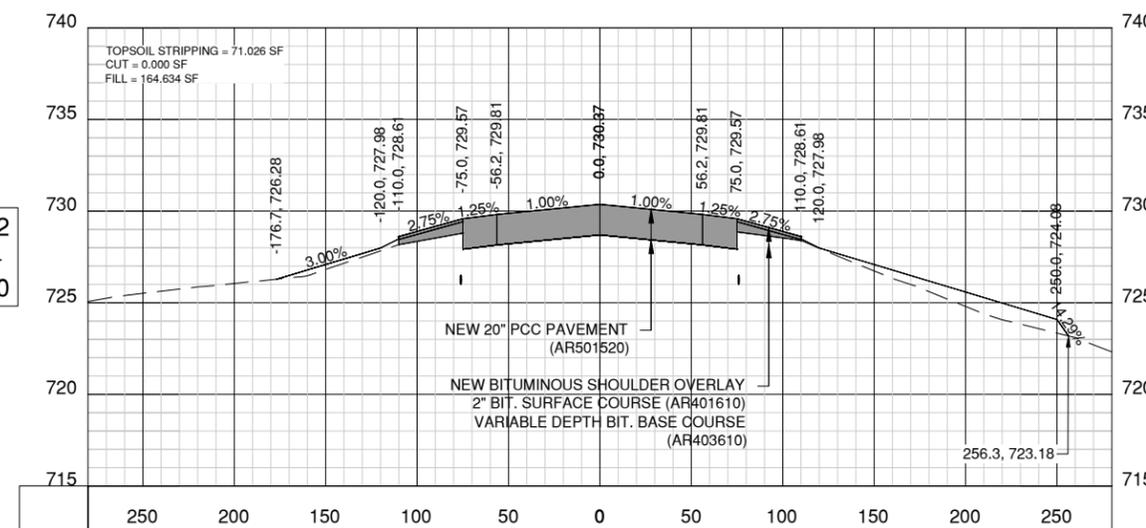
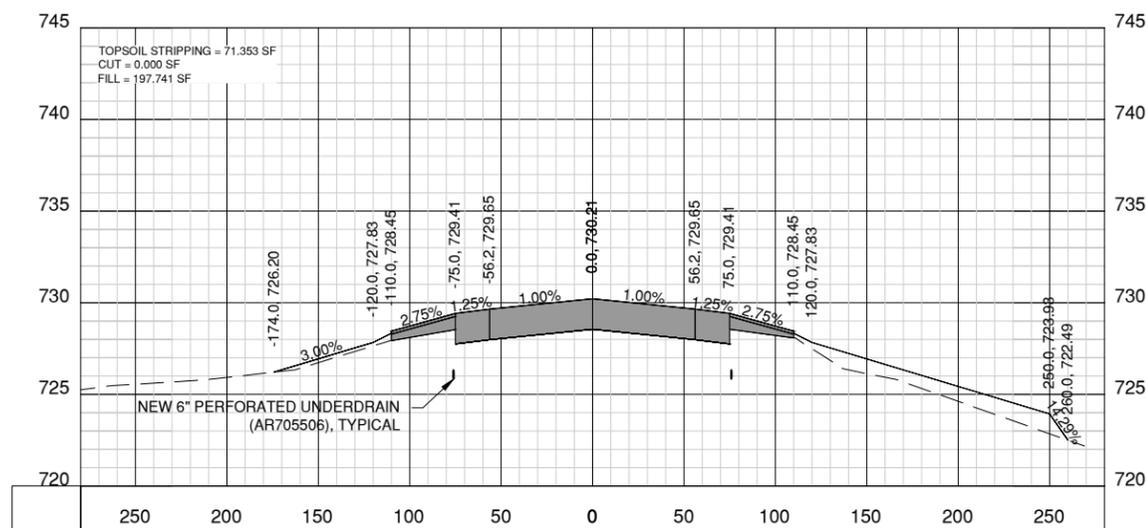
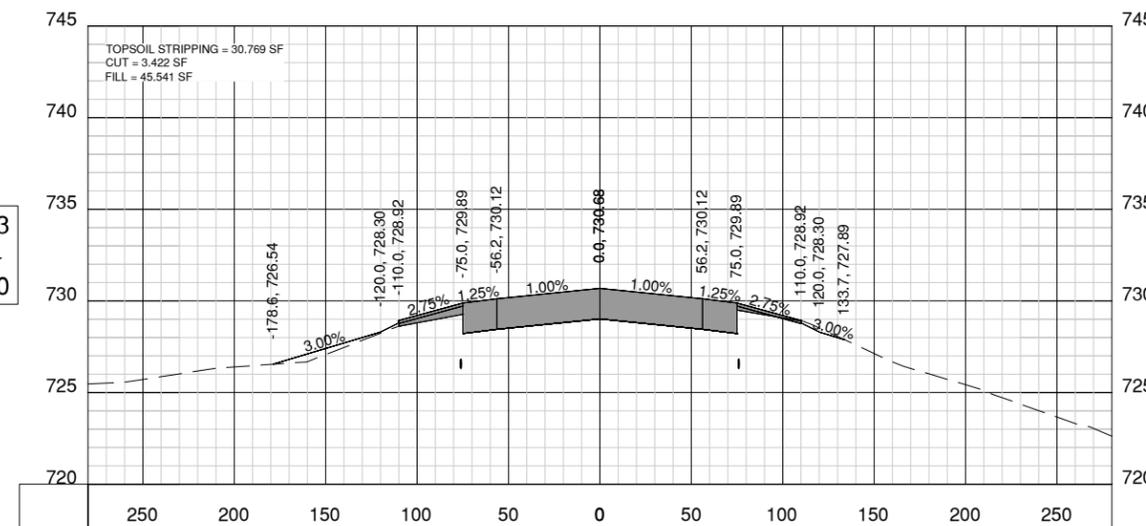
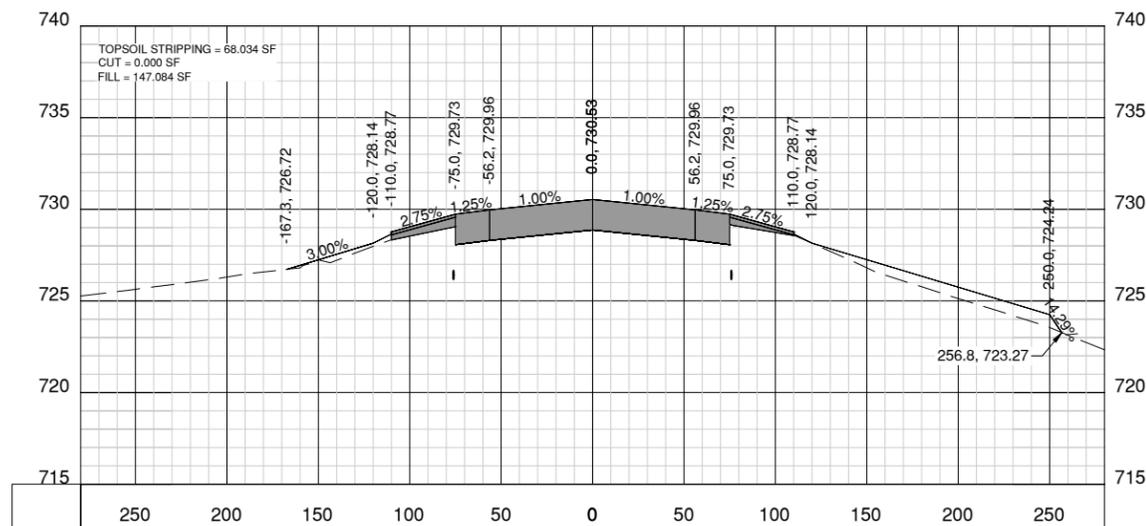
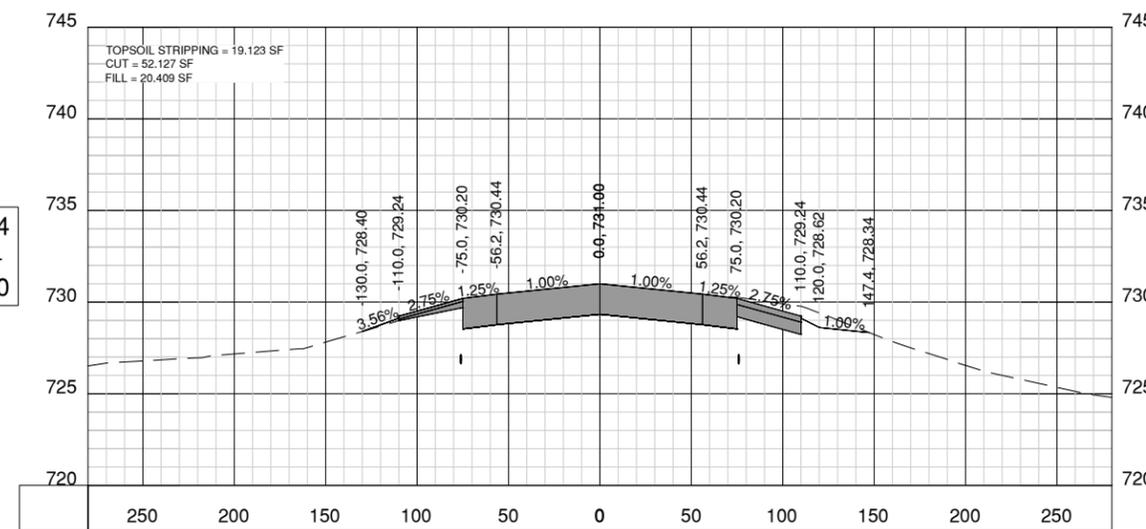
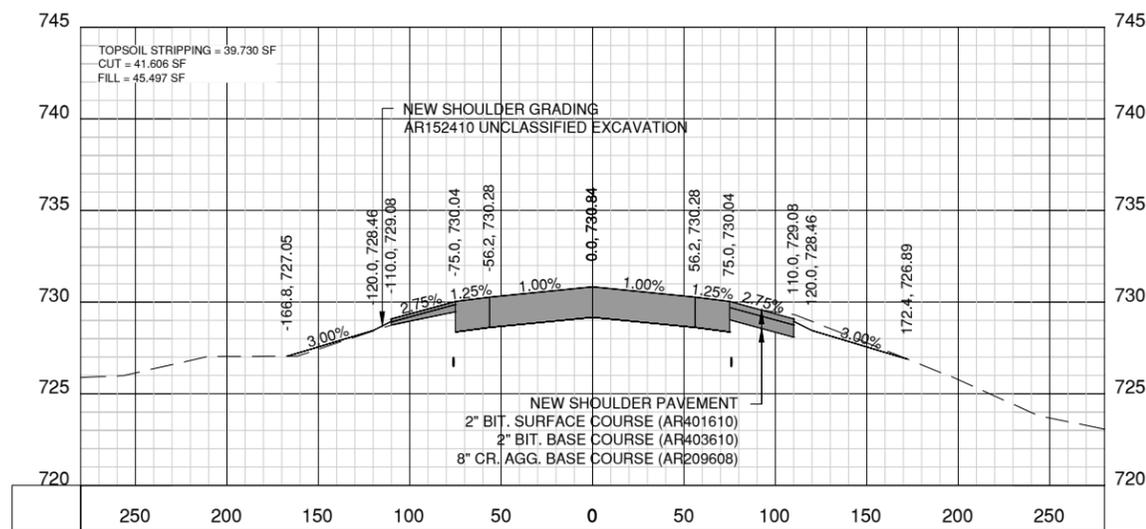
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PROJECT NO.: 12258-05-00/06
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SHEET NO. **XSC7** REVISION

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IL Proj: RFD-4360

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REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

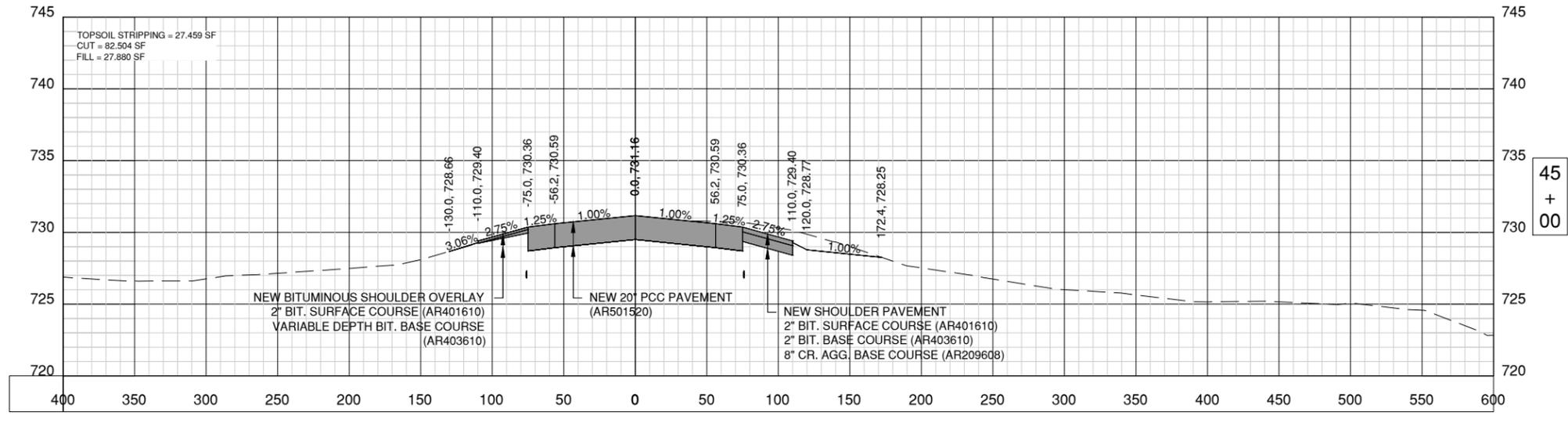
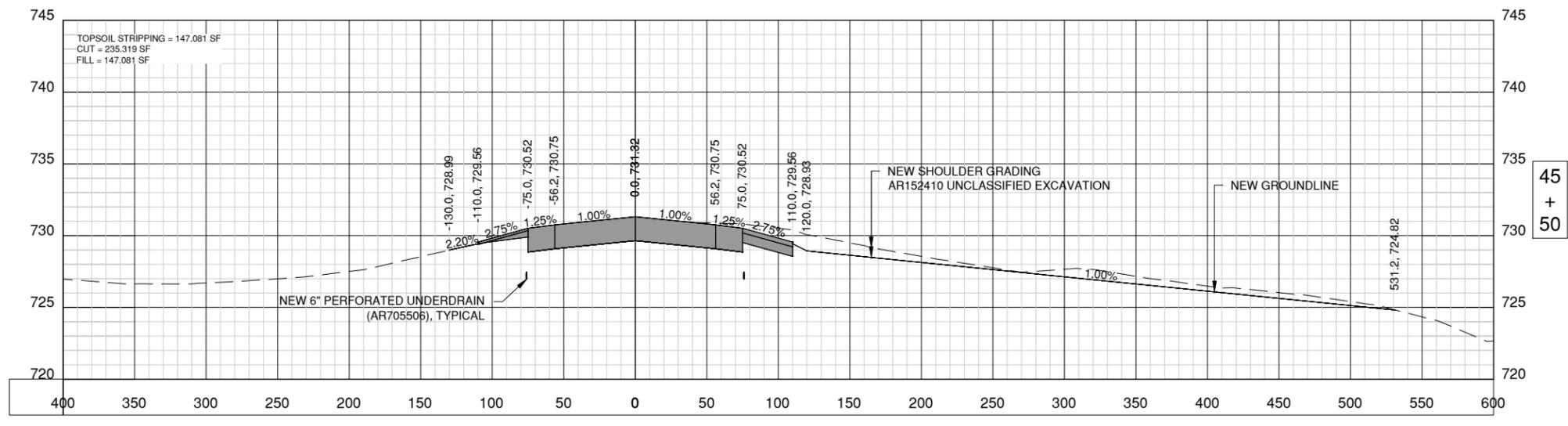
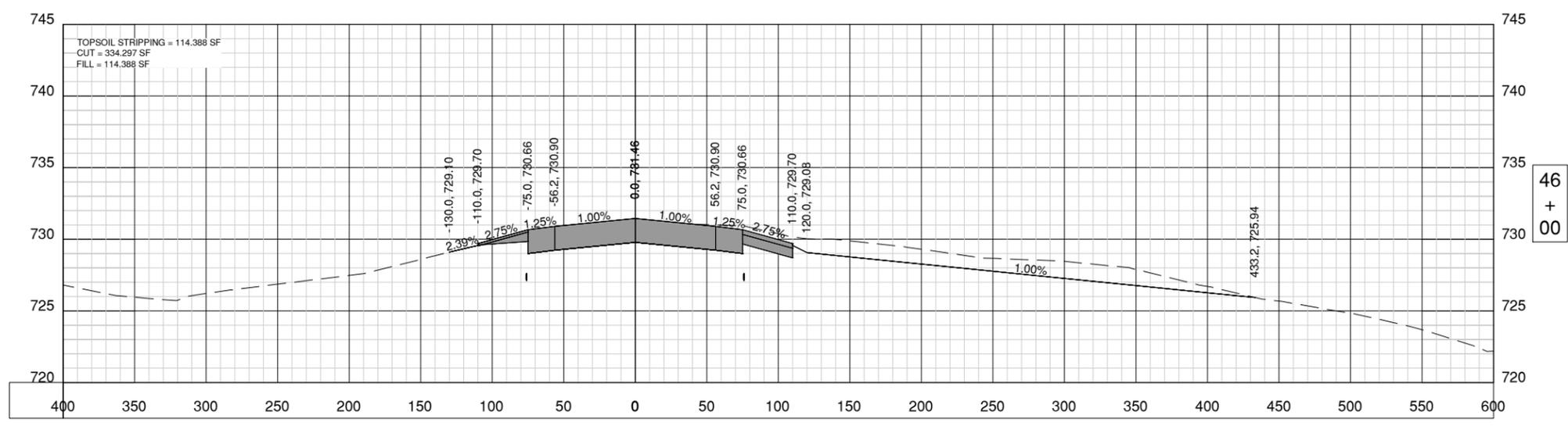
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CROSS SECTIONS 8**

DESIGNED: SMS DRAWN: SMS CHECKED: DLP

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DATE: APRIL 19, 2013

SHEET NO. **XSC8** REVISION

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REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

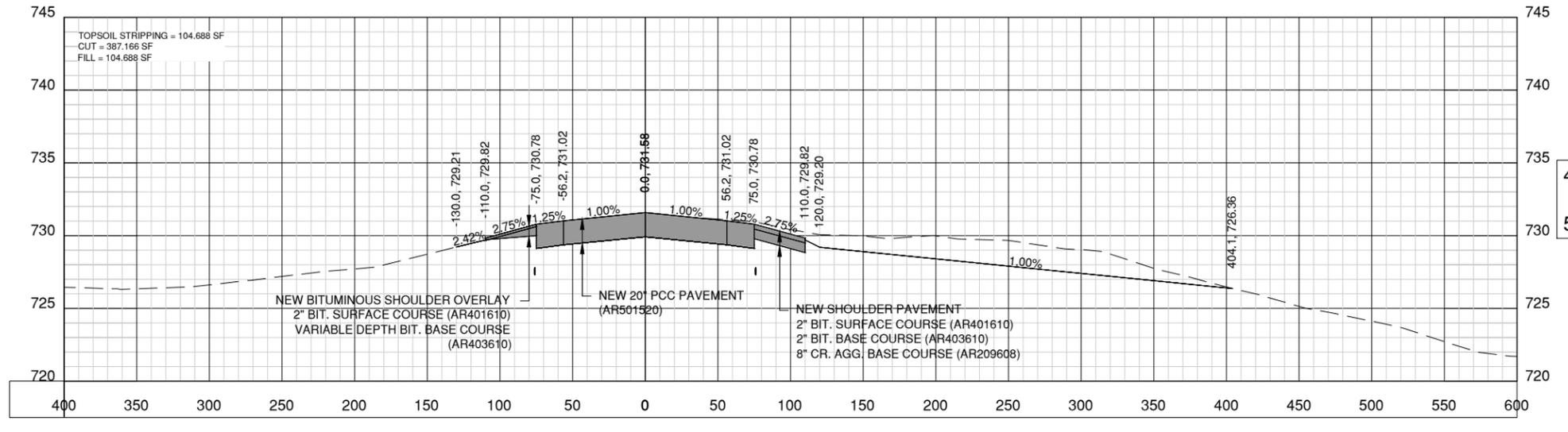
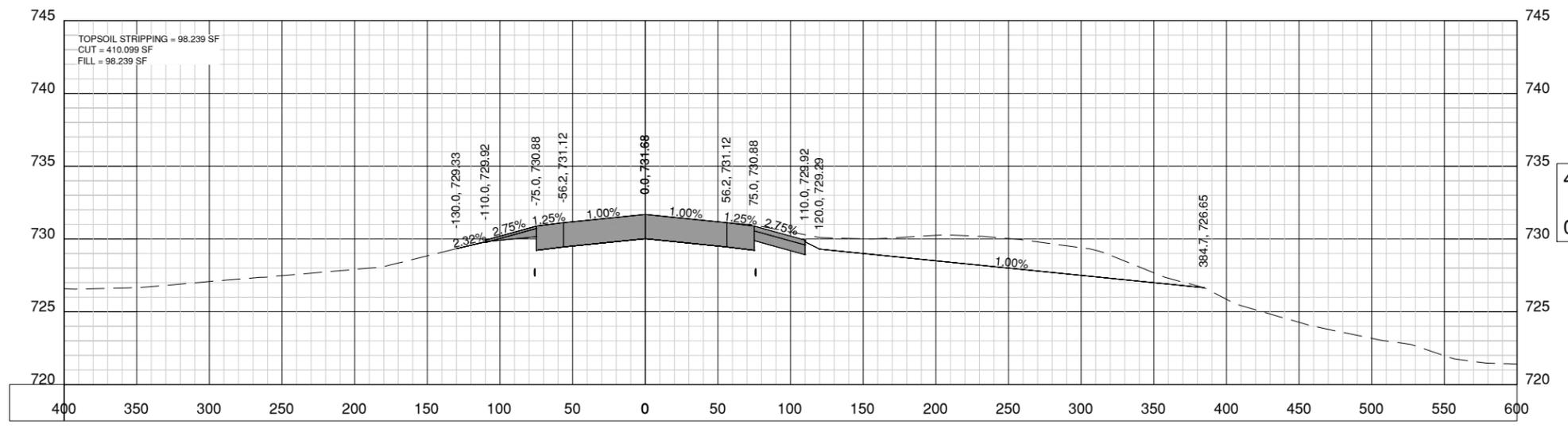
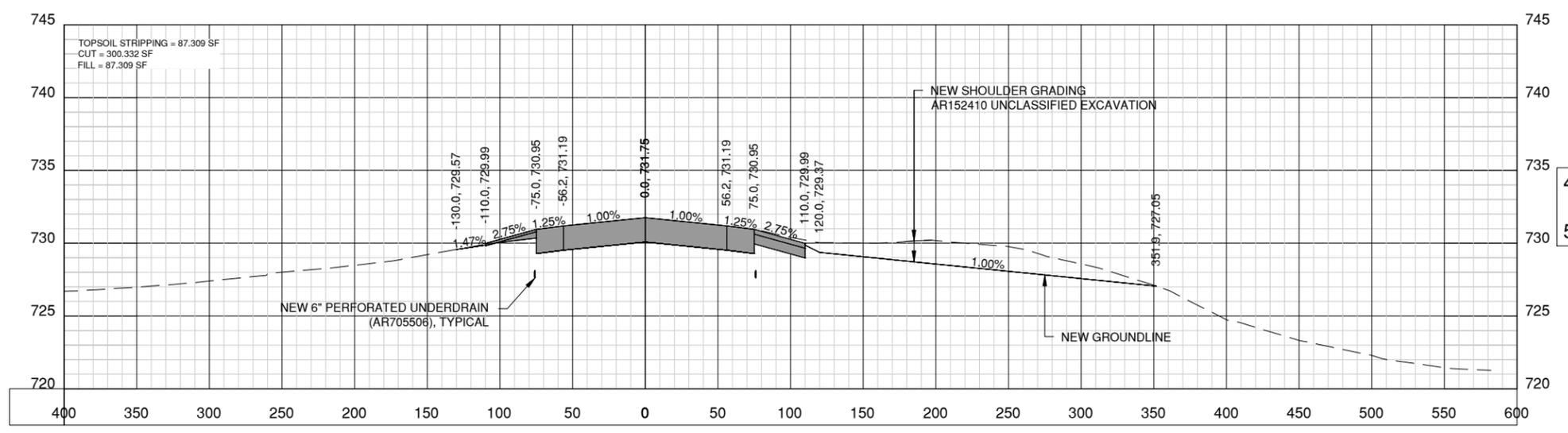
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CROSS SECTIONS 9

DESIGNED: SMS DRAWN: SMS CHECKED: DLP

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **XSC9** REVISION

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REV	DATE	DESCRIPTION

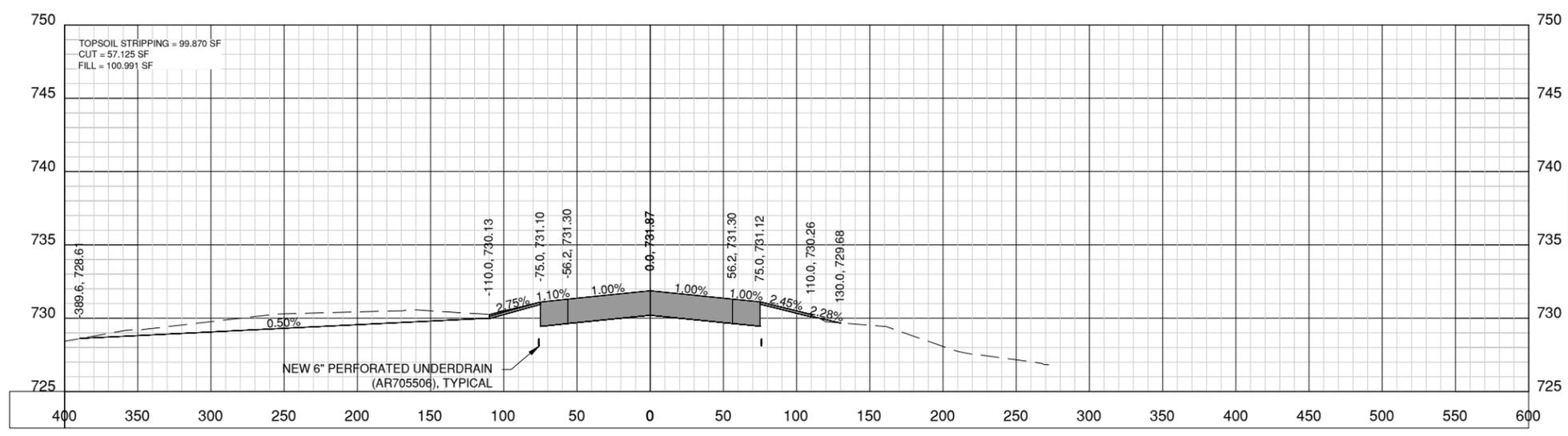
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RUNWAY 1/19
PHASE 3**

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**RUNWAY 1/19
CROSS SECTIONS 10**

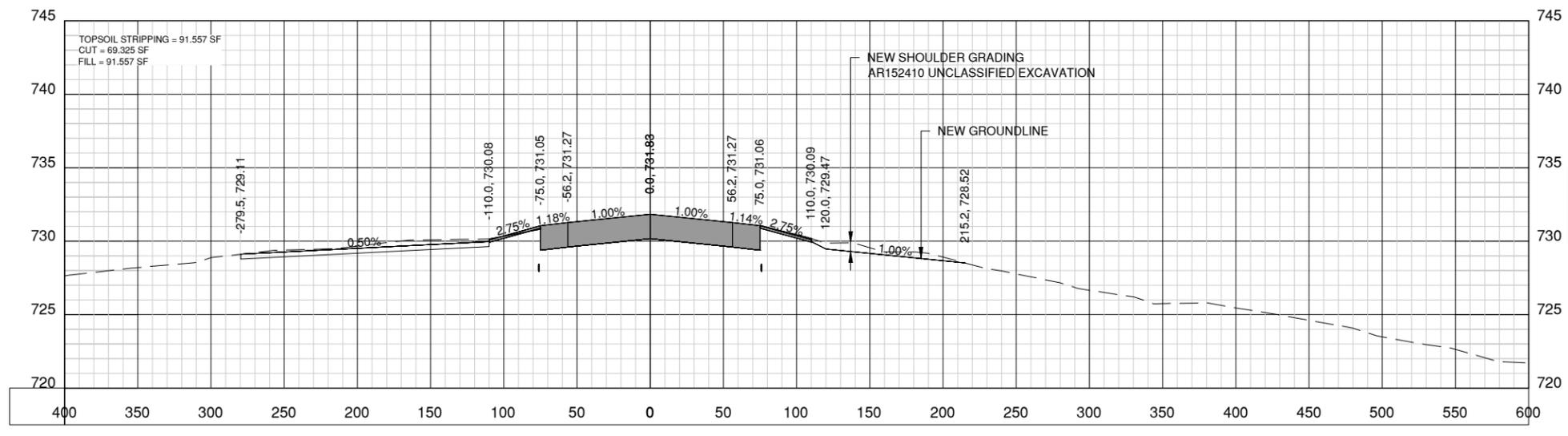
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PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

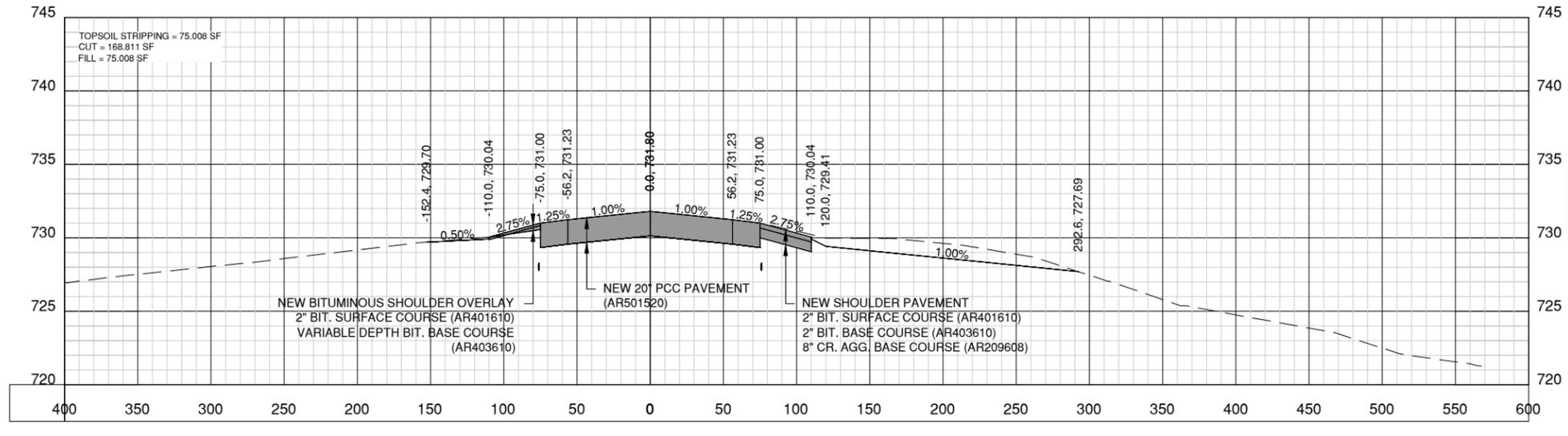
SHEET NO. **XSC10** REVISION
SHEET 48 OF 65 SHEETS



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PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

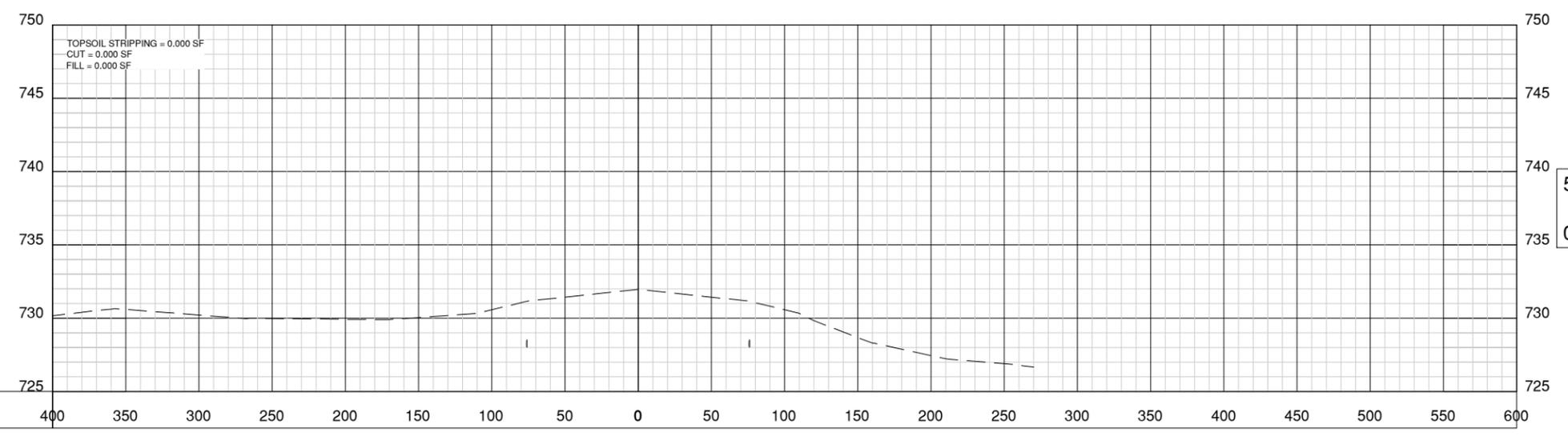
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RUNWAY 1/19
CROSS SECTIONS 11

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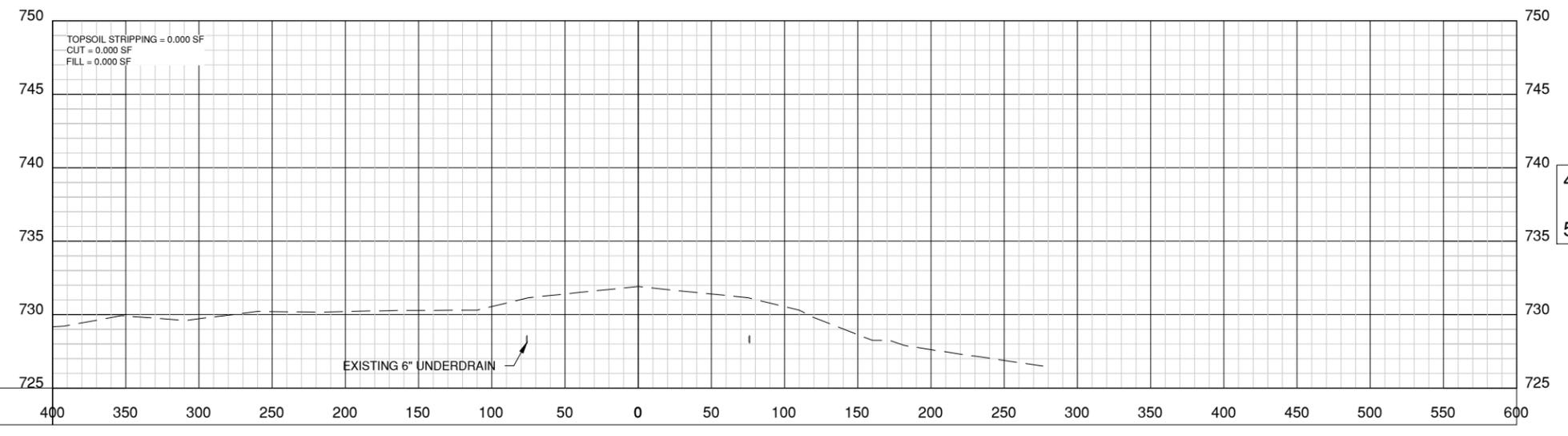
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DATE: APRIL 19, 2013

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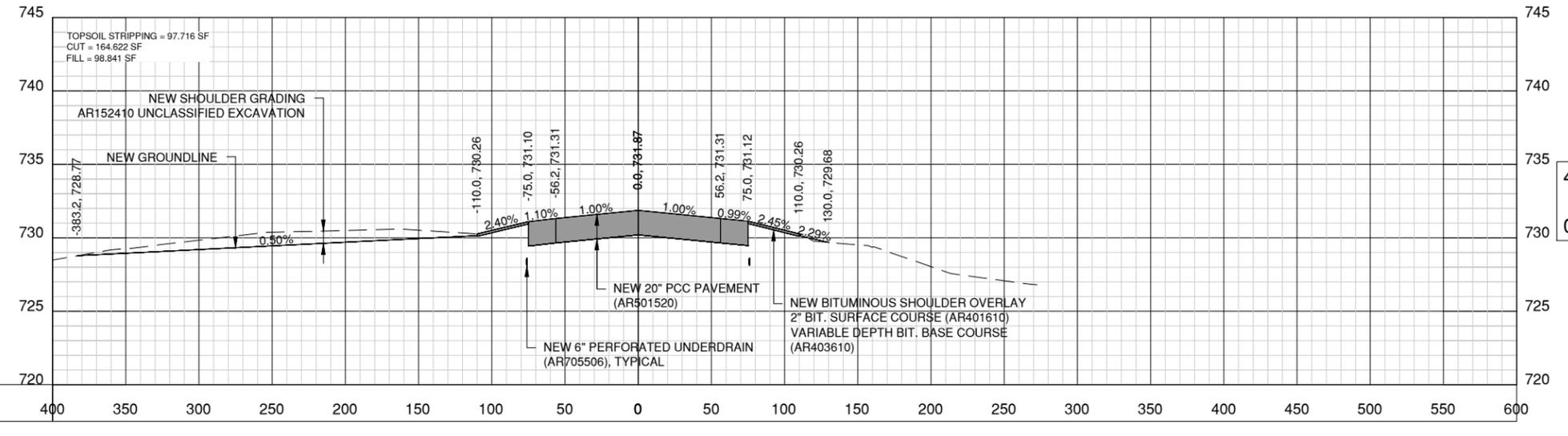
SHEET 49 OF 65 SHEETS



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BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

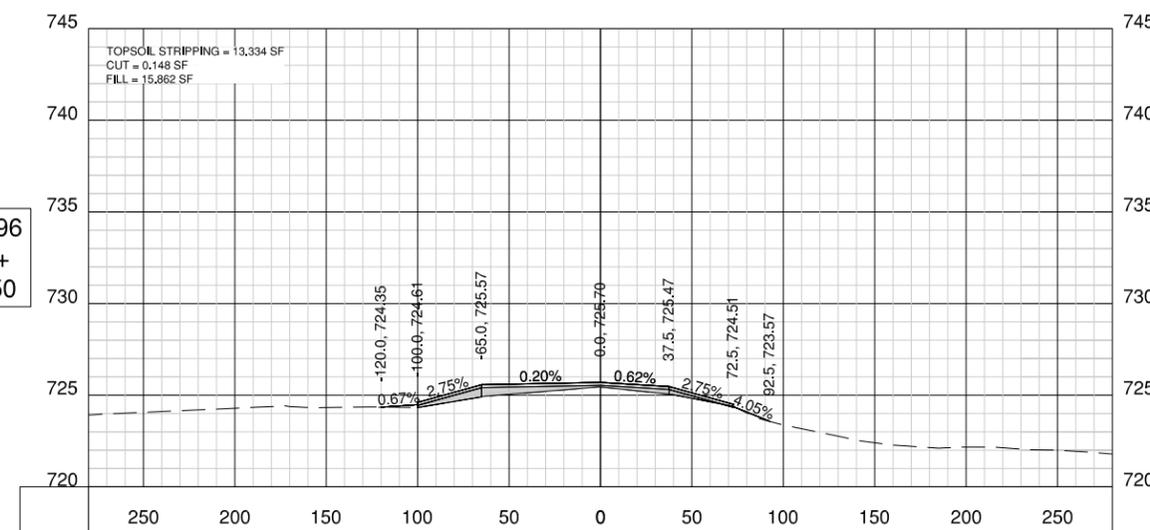
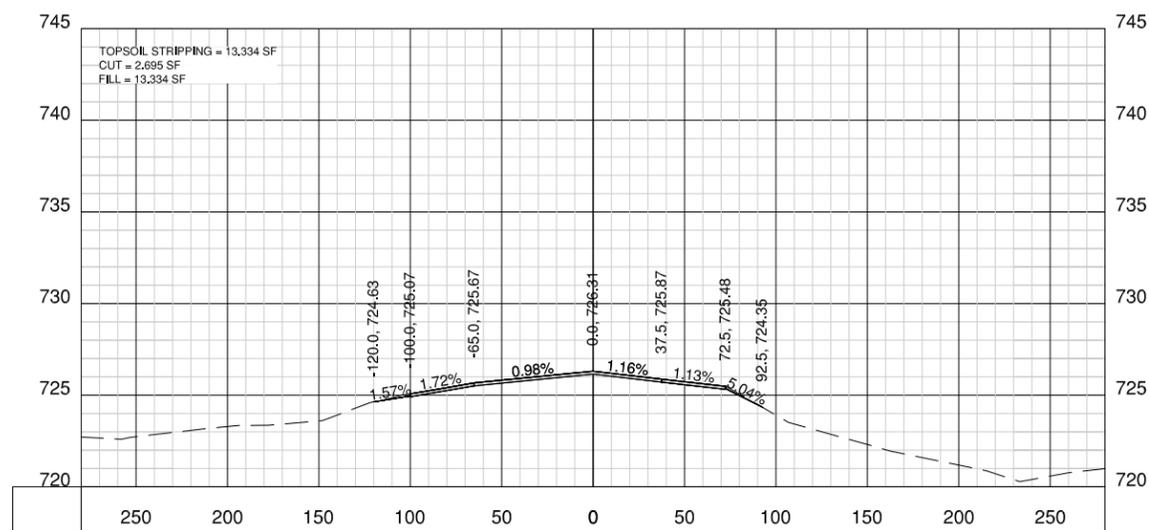
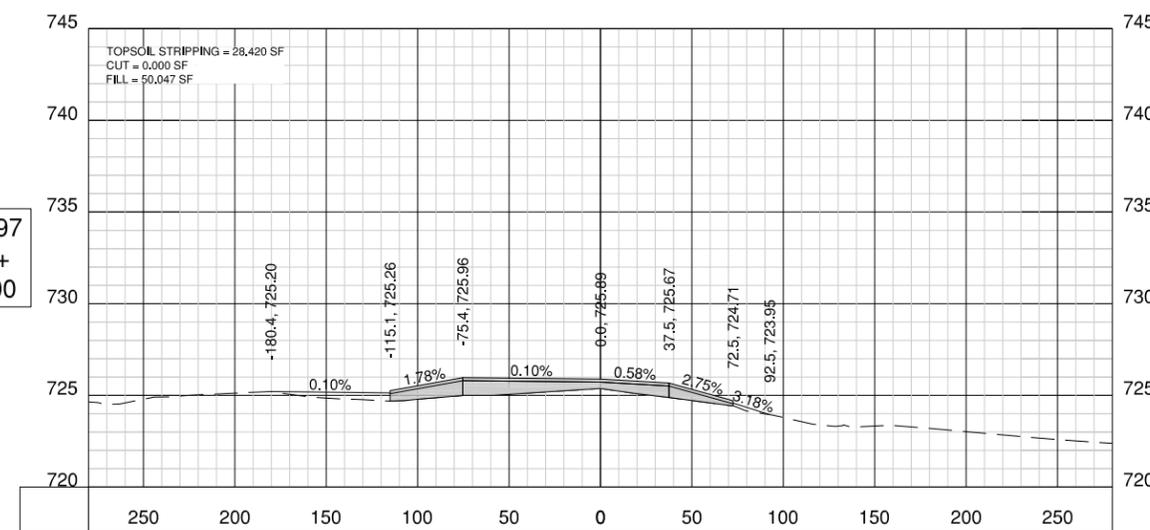
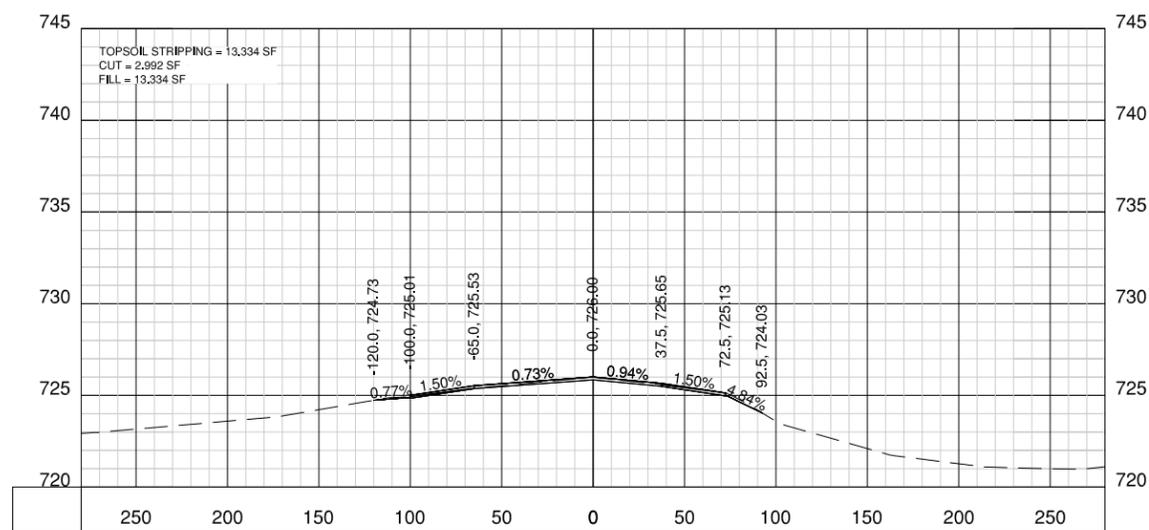
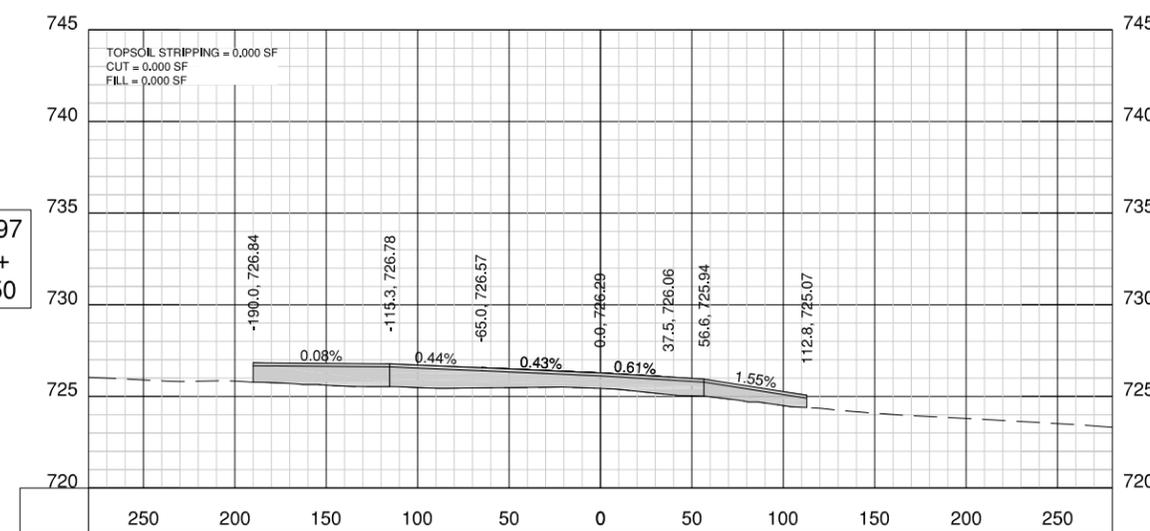
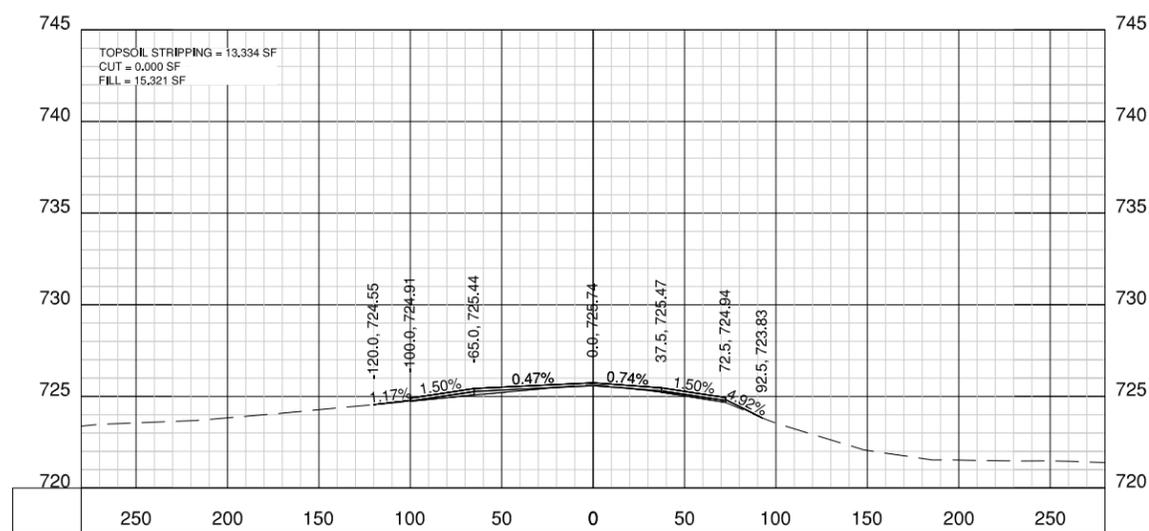
SHEET TITLE:
**TXY E
CROSS SECTIONS**

DESIGNED: SMS	DRAWN: SMS	CHECKED: DLP
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PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **XSC12** REVISION
SHEET 50 OF 65 SHEETS

RO024





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AIP Proj: 3-17-0088-XX
IL Proj: RFD-4360

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:
ADD. ALT. SITE PLAN
AND CONTROL

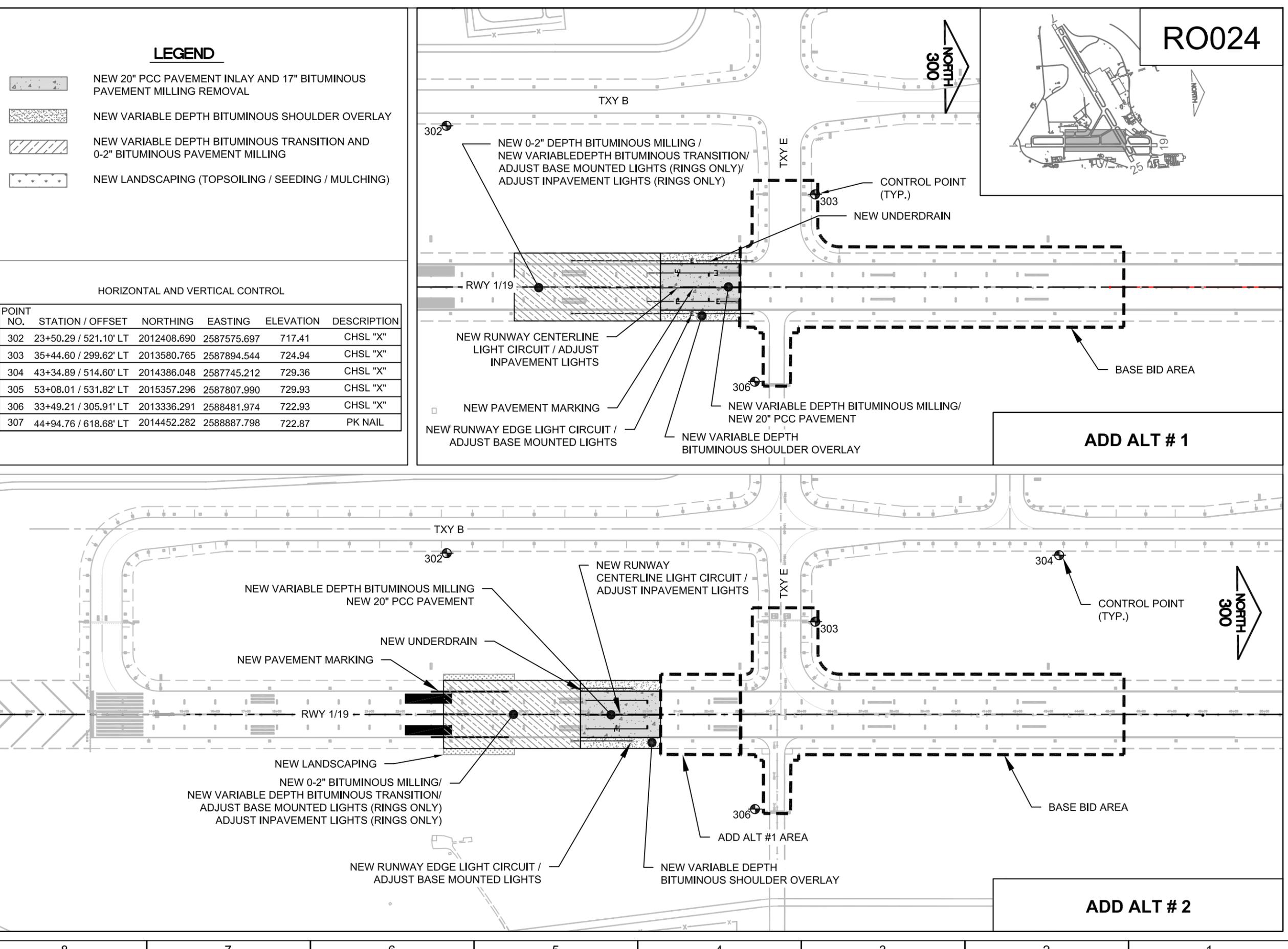
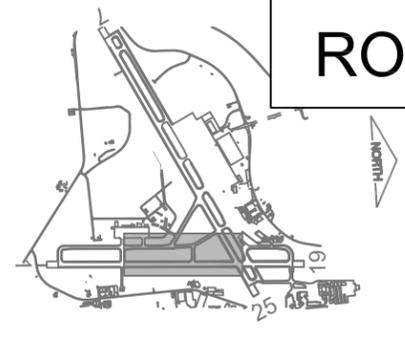
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **ASP1** REVISION

SHEET 51 OF 65 SHEETS

RO024



LEGEND

- NEW 20" PCC PAVEMENT INLAY AND 17" BITUMINOUS PAVEMENT MILLING REMOVAL
- NEW VARIABLE DEPTH BITUMINOUS SHOULDER OVERLAY
- NEW VARIABLE DEPTH BITUMINOUS TRANSITION AND 0-2" BITUMINOUS PAVEMENT MILLING
- NEW LANDSCAPING (TOPSOILING / SEEDING / MULCHING)

HORIZONTAL AND VERTICAL CONTROL

POINT NO.	STATION / OFFSET	NORTHING	EASTING	ELEVATION	DESCRIPTION
302	23+50.29 / 521.10' LT	2012408.690	2587575.697	717.41	CHSL "X"
303	35+44.60 / 299.62' LT	2013580.765	2587894.544	724.94	CHSL "X"
304	43+34.89 / 514.60' LT	2014386.048	2587745.212	729.36	CHSL "X"
305	53+08.01 / 531.82' LT	2015357.296	2587807.990	729.93	CHSL "X"
306	33+49.21 / 305.91' LT	2013336.291	2588481.974	722.93	CHSL "X"
307	44+94.76 / 618.68' LT	2014452.282	2588887.798	722.87	PK NAIL



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BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:
ADD. ALT #1 & #2
TYPICAL SECTION -1
RWY 1/19

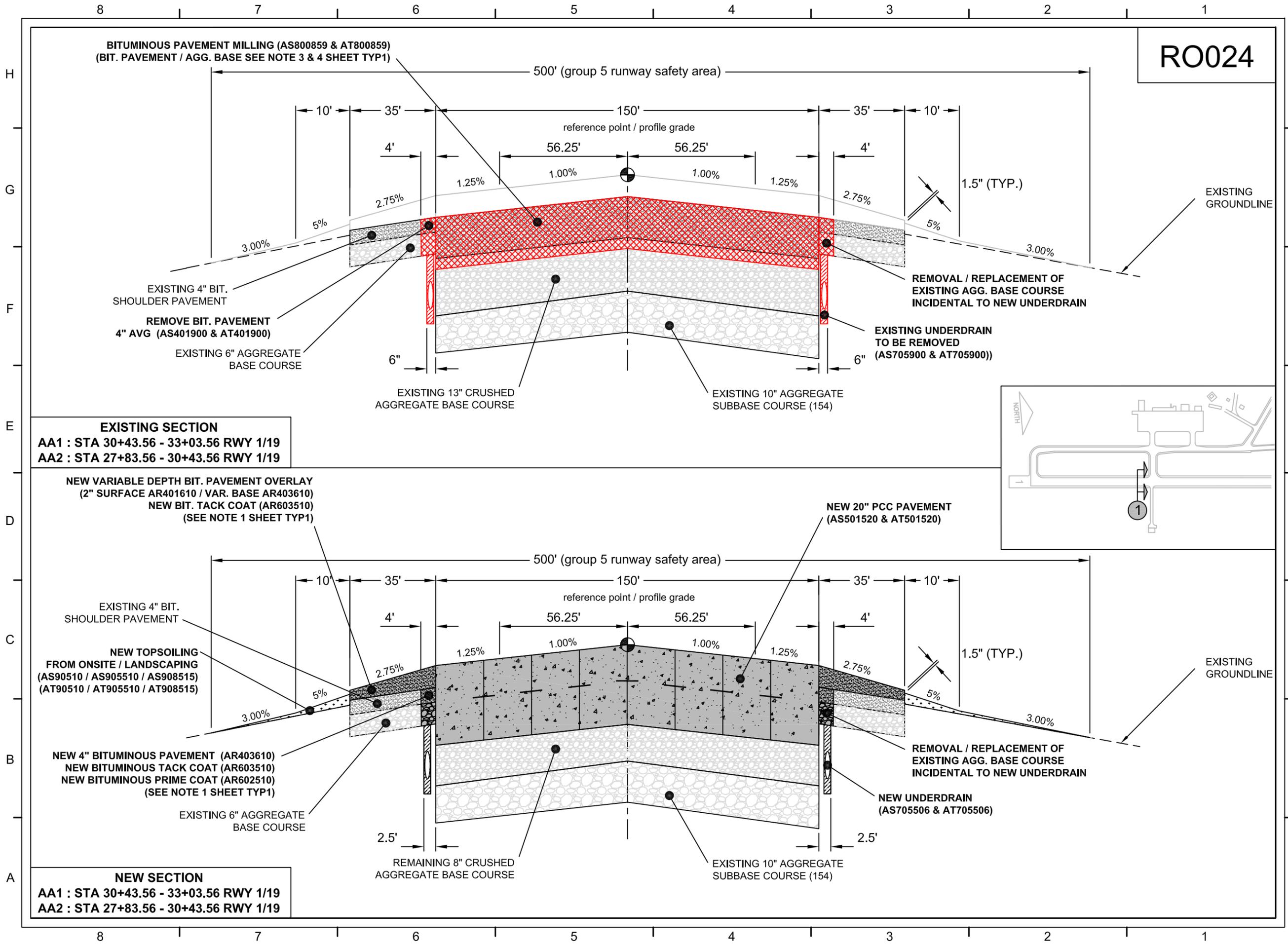
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **ATY1** REVISION

SHEET 52 OF 65 SHEETS

RO024



EXISTING SECTION
AA1 : STA 30+43.56 - 33+03.56 RWY 1/19
AA2 : STA 27+83.56 - 30+43.56 RWY 1/19

NEW VARIABLE DEPTH BIT. PAVEMENT OVERLAY
(2" SURFACE AR401610 / VAR. BASE AR403610)
NEW BIT. TACK COAT (AR603510)
(SEE NOTE 1 SHEET TYP1)

NEW TOPSOILING FROM ONSITE / LANDSCAPING
(AS90510 / AS905510 / AS908515)
(AT90510 / AT905510 / AT908515)

NEW 4" BITUMINOUS PAVEMENT (AR403610)
NEW BITUMINOUS TACK COAT (AR603510)
NEW BITUMINOUS PRIME COAT (AR602510)
(SEE NOTE 1 SHEET TYP1)

NEW SECTION
AA1 : STA 30+43.56 - 33+03.56 RWY 1/19
AA2 : STA 27+83.56 - 30+43.56 RWY 1/19



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APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

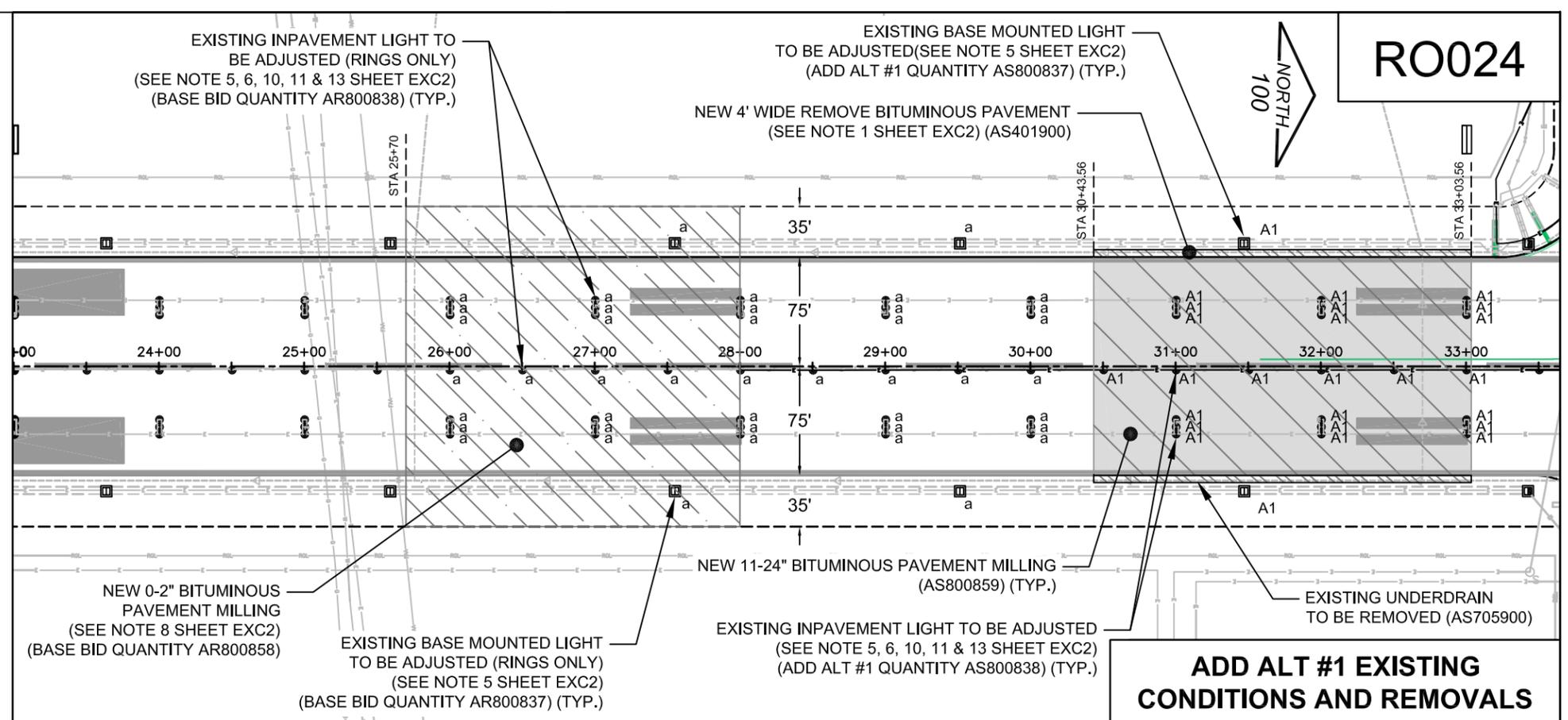
SHEET TITLE:
**ADD ALT #1 & #2
EXISTING CONDITIONS
AND REMOVALS**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

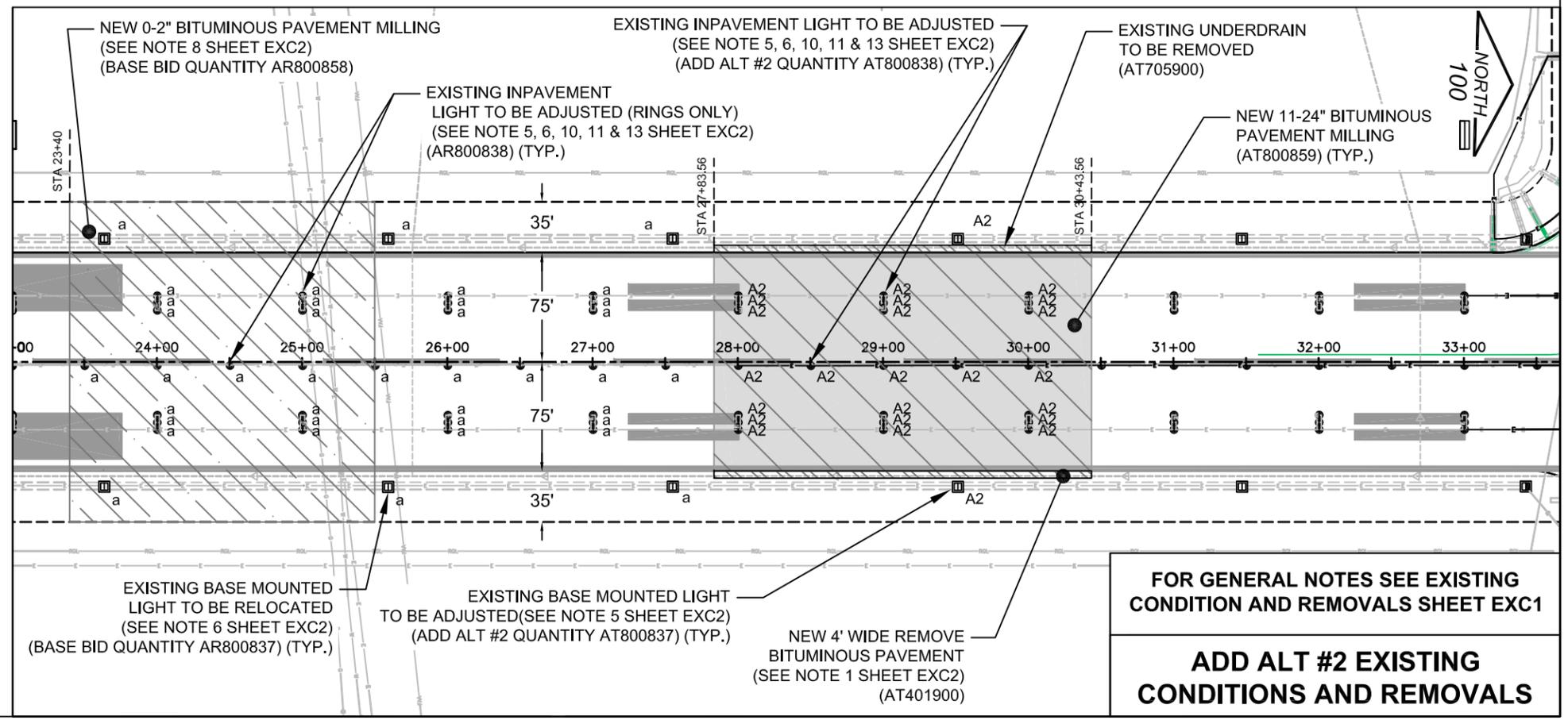
PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **AEX1** REVISION

SHEET 53 OF 65 SHEETS



**ADD ALT #1 EXISTING
CONDITIONS AND REMOVALS**

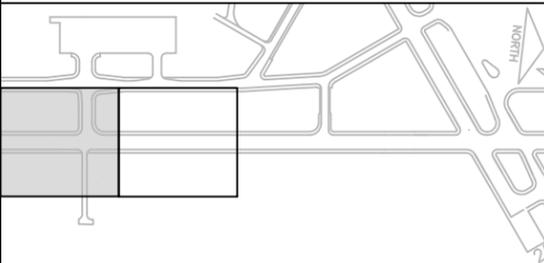


**FOR GENERAL NOTES SEE EXISTING
CONDITION AND REMOVALS SHEET EXC1**

**ADD ALT #2 EXISTING
CONDITIONS AND REMOVALS**

LEGEND

- NEW 0-2" BITUMINOUS PAVEMENT MILLING
- NEW 2-11" BITUMINOUS PAVEMENT MILLING
- NEW 11-24" BITUMINOUS PAVEMENT MILLING
- REMOVE BITUMINOUS PAVEMENT
- EXISTING SANITARY LINE
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- EXISTING WATERMAIN
- EXISTING RWY GUIDANCE LIGHT CIRCUIT
- EXISTING AIRFIELD CIRCUIT
- EXISTING LIGHTING CIRCUIT IN EXISTING DUCT
- REMOVE EXISTING DUCT
- EXISTING PAVEMENT MARKING
- EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- ADJUST EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- REMOVE BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- EXISTING ELEV. RUNWAY GUARD LIGHT
- REMOVE ELECTRICAL HANDHOLE
- EXISTING ELECTRICAL HANDHOLE
- EXISTING CONDUIT/DUCT
- RELOCATE EXISTING AIRFIELD GUIDANCE SIGN
- EXISTING AIRFIELD GUIDANCE SIGN
- REMOVE BASE CAN (BASE MOUNTED RUNWAY EDGE LIGHT)
- ADJUST BASE MOUNTED RUNWAY EDGE LIGHT
- ADJUST BASE MOUNTED RUNWAY EDGE LIGHT (RINGS ONLY)
- REMOVE BASE CAN (INPAVEMENT LIGHT)
- ADJUST INPAVEMENT LIGHT
- ADJUST INPAVEMENT LIGHT (RINGS ONLY)
- EXISTING SPLICE CAN
- ADJUST SPLICE CAN
- EXISTING SURFACE SENSOR
- ADJUST SURFACE SENSOR



THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS AND FIELD SURVEYS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WITH RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION PRESENTED HEREIN AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



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REV DATE DESCRIPTION
PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

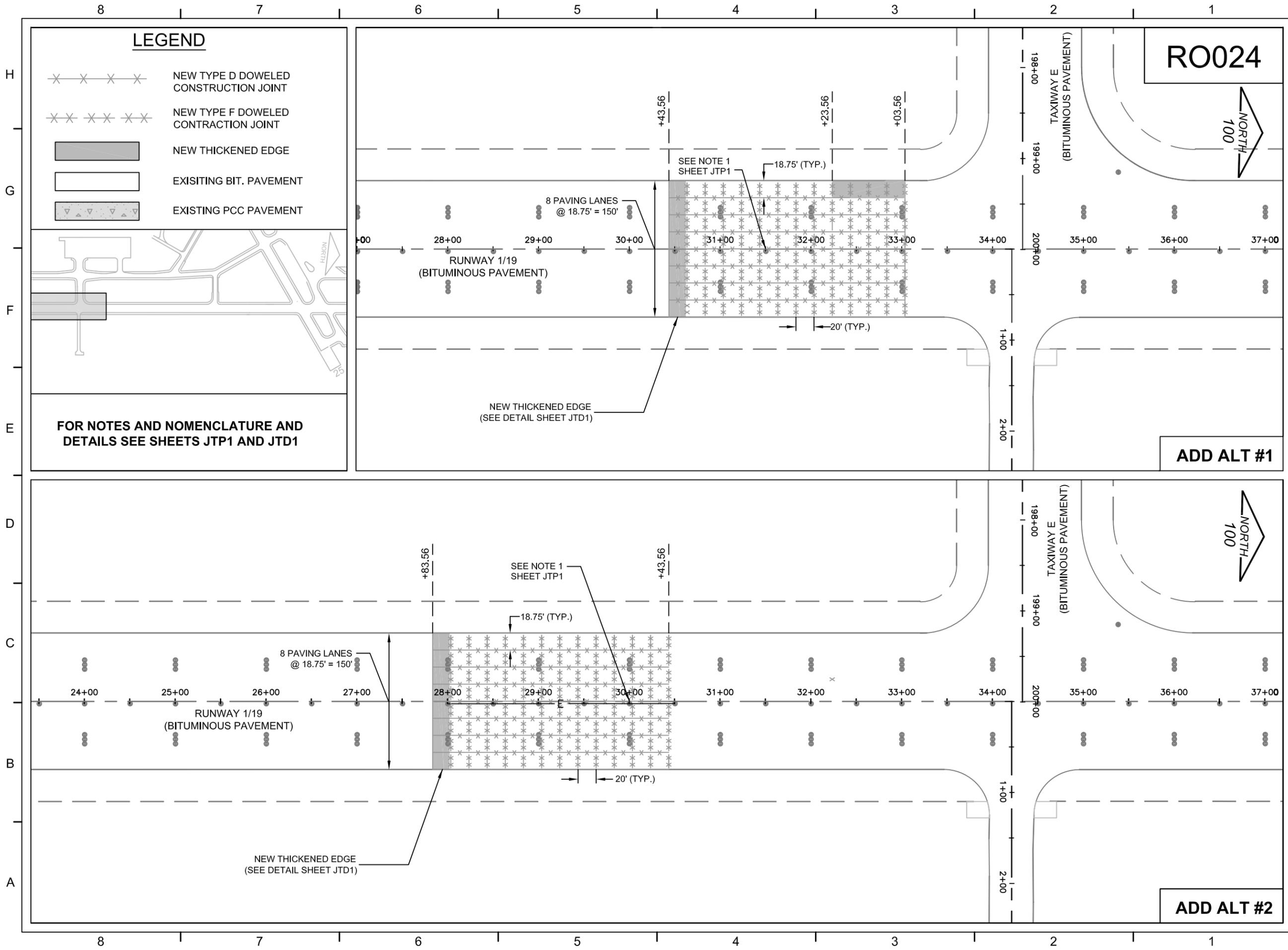
SHEET TITLE:
ADD ALT #1 & #2
JOINTING PLAN

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **AJT1** REVISION

SHEET 57 OF 65 SHEETS



LEGEND

- NEW TYPE D DOWELED CONSTRUCTION JOINT
- NEW TYPE F DOWELED CONSTRUCTION JOINT
- NEW THICKENED EDGE
- EXISTING BIT. PAVEMENT
- EXISTING PCC PAVEMENT



FOR NOTES AND NOMENCLATURE AND
DETAILS SEE SHEETS JTP1 AND JTD1

NEW THICKENED EDGE
(SEE DETAIL SHEET JTD1)

ADD ALT #1

SEE NOTE 1
SHEET JTP1

NEW THICKENED EDGE
(SEE DETAIL SHEET JTD1)

ADD ALT #2



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BY:

REV DATE DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

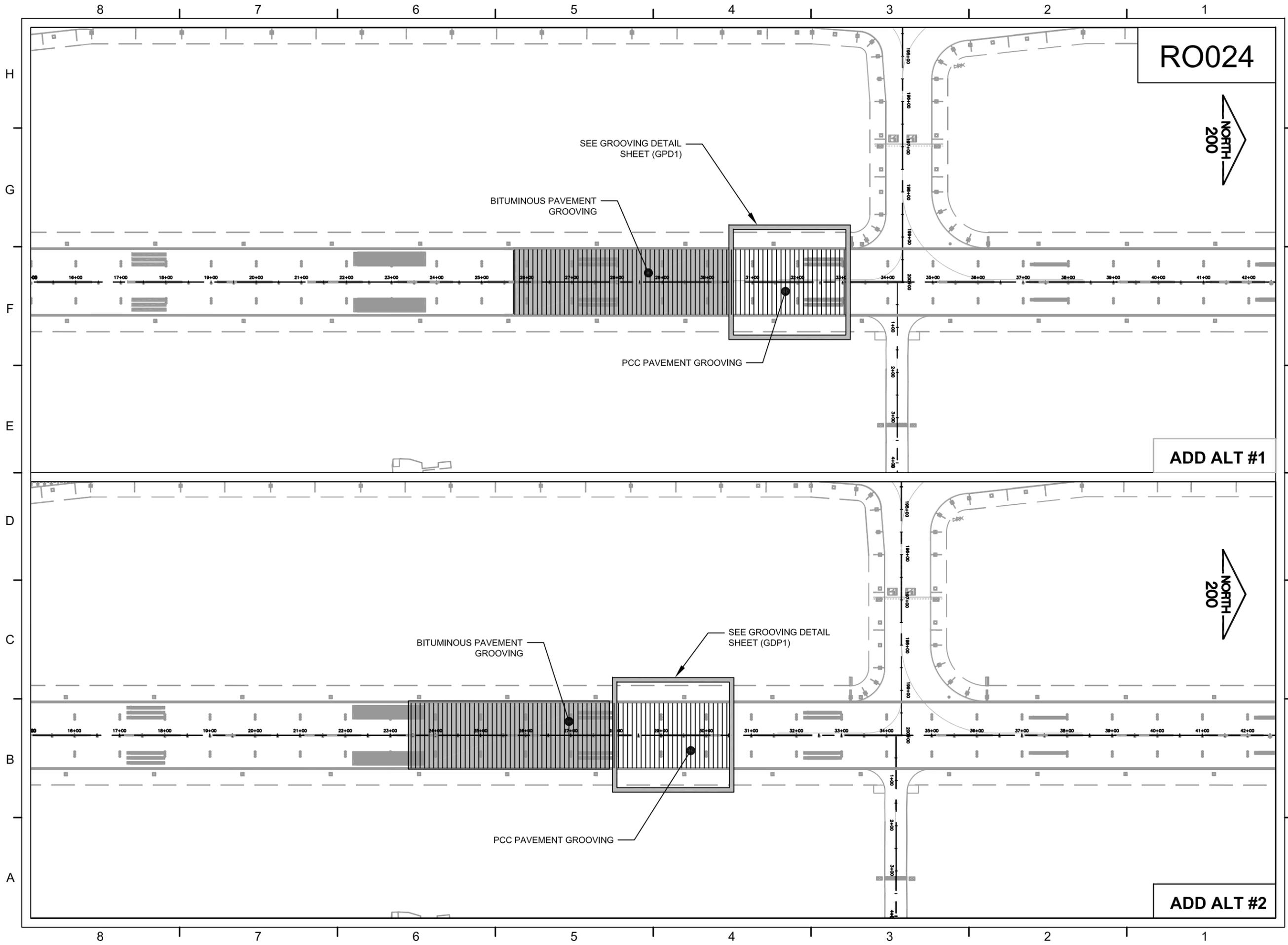
SHEET TITLE:
**ADD ALT #1 & #2
GROOVING PLAN**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **AGP1** REVISION

SHEET 59 OF 65 SHEETS



RO024

ADD ALT #1

ADD ALT #2

SEE GROOVING DETAIL SHEET (GPD1)

BITUMINOUS PAVEMENT GROOVING

PCC PAVEMENT GROOVING

BITUMINOUS PAVEMENT GROOVING

SEE GROOVING DETAIL SHEET (GPD1)

PCC PAVEMENT GROOVING





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APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

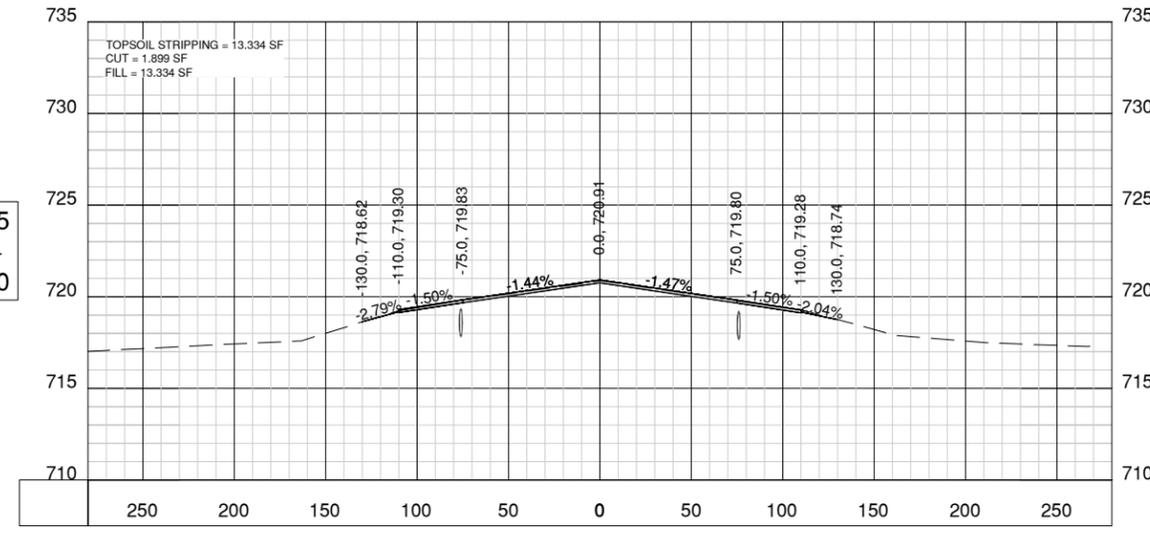
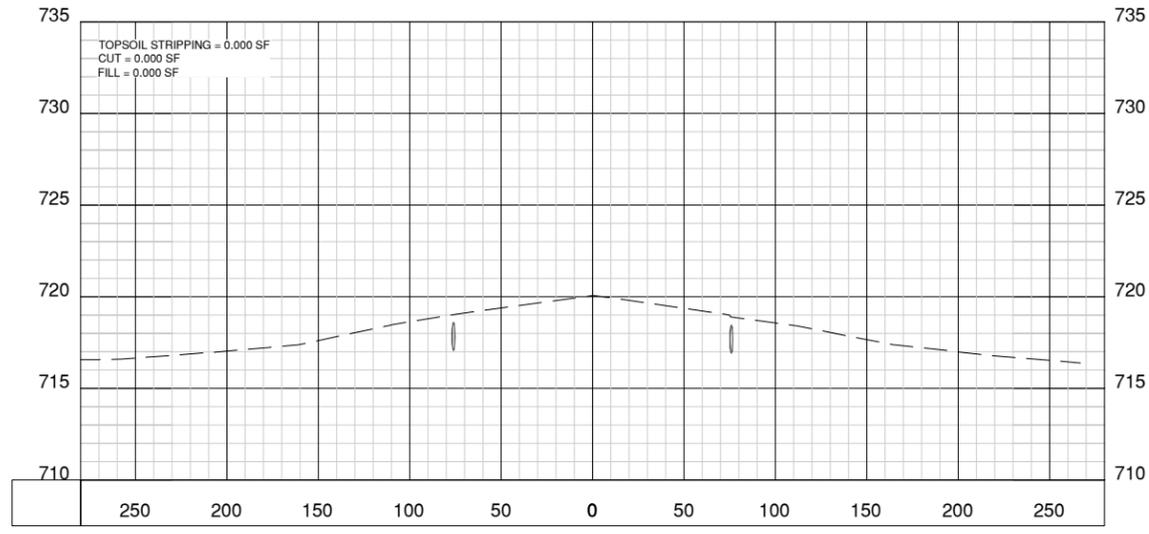
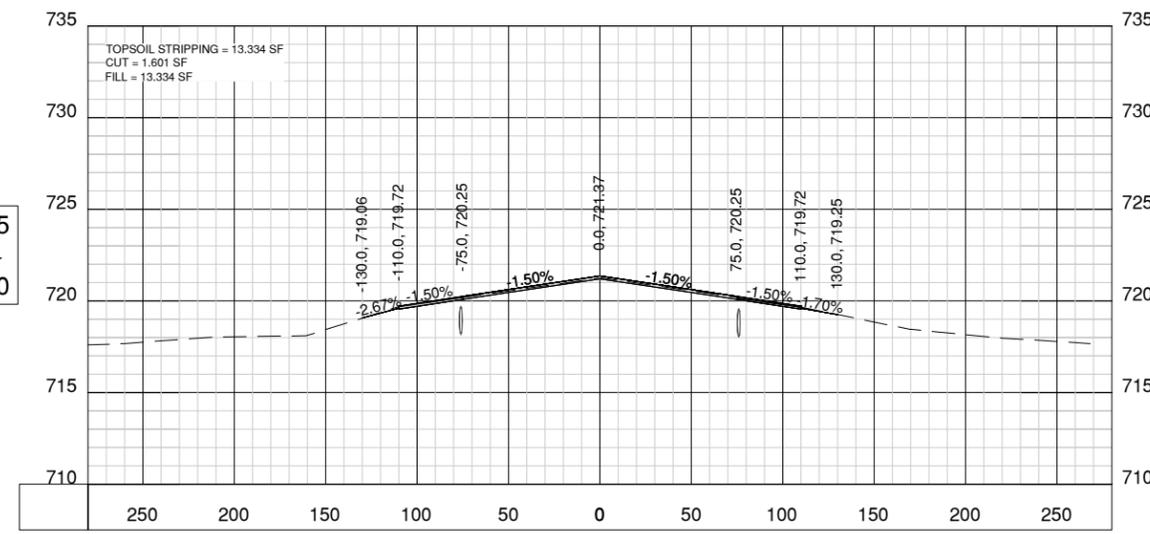
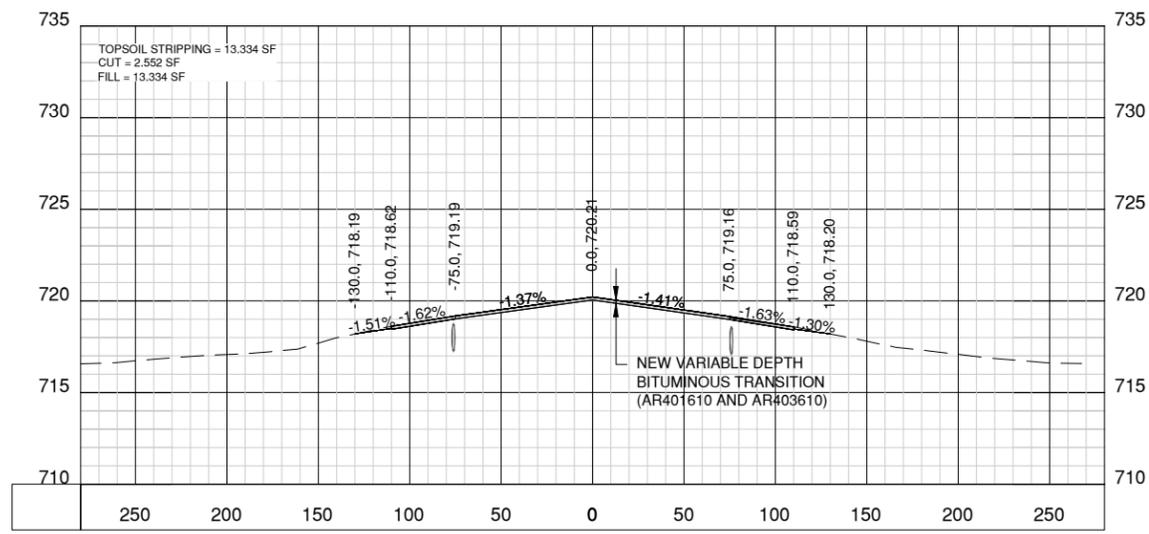
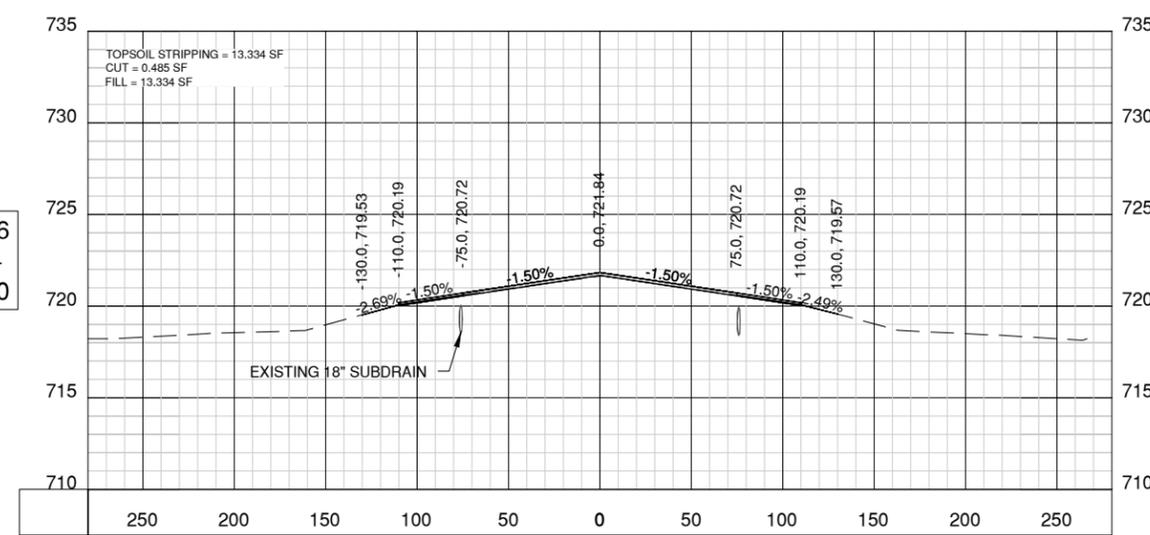
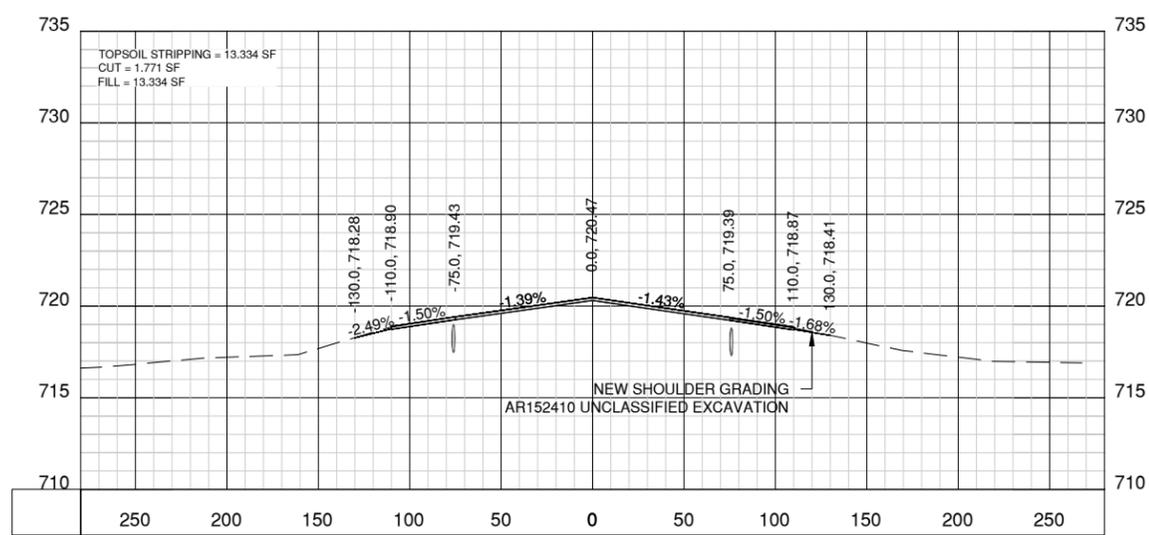
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**RUNWAY 1/19
ADD ALT 1
CROSS SECTIONS 1**

DESIGNED: SMS	DRAWN: SMS	CHECKED: DLP
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PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **A1X1** REVISION
SHEET 60 OF 65 SHEETS

RO024



8 7 6 5 4 3 2 1

8 7 6 5 4 3 2 1

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REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

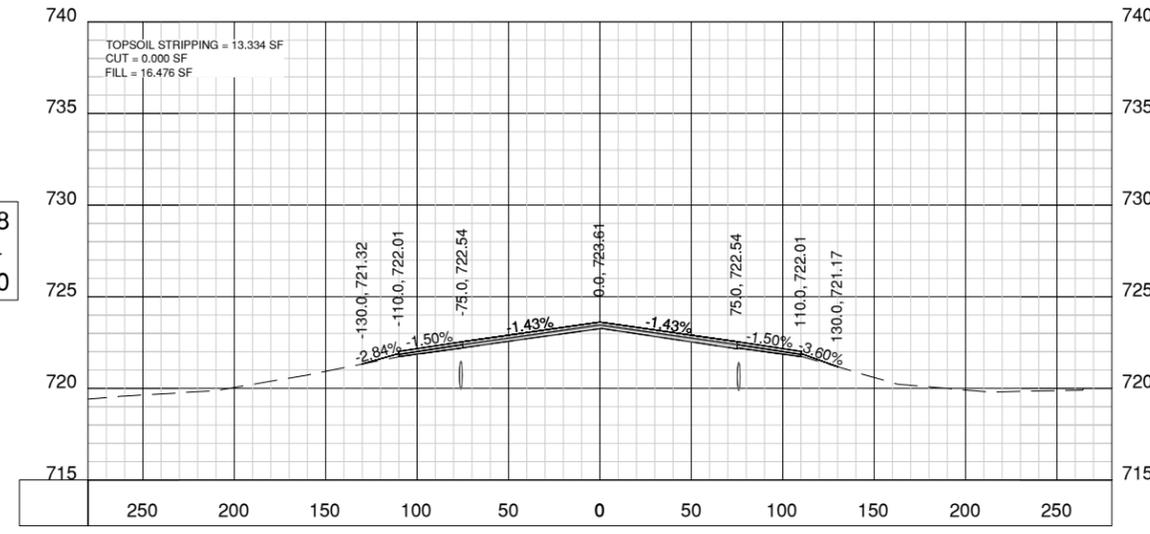
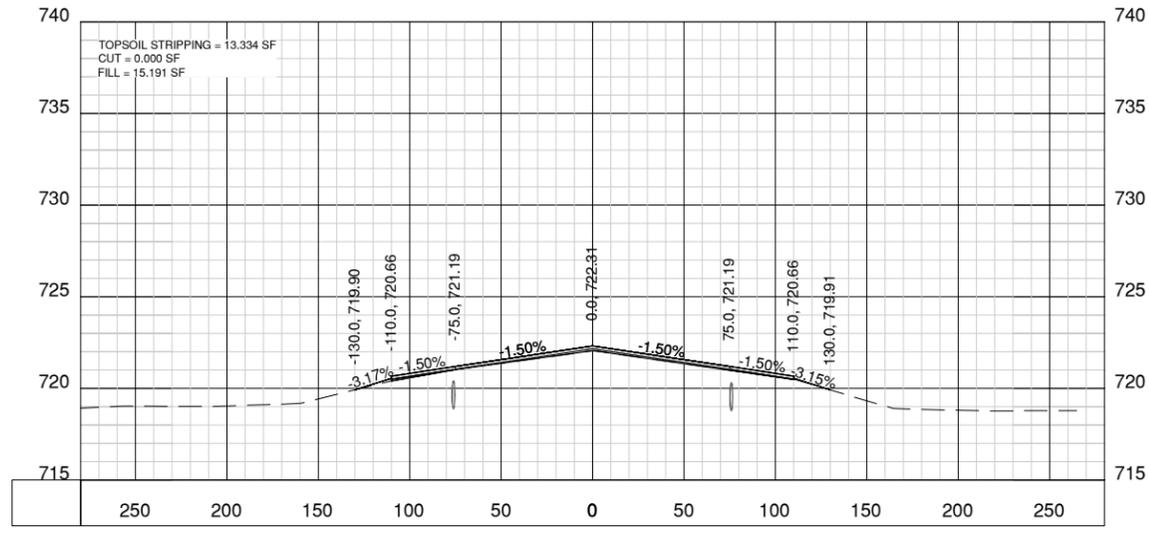
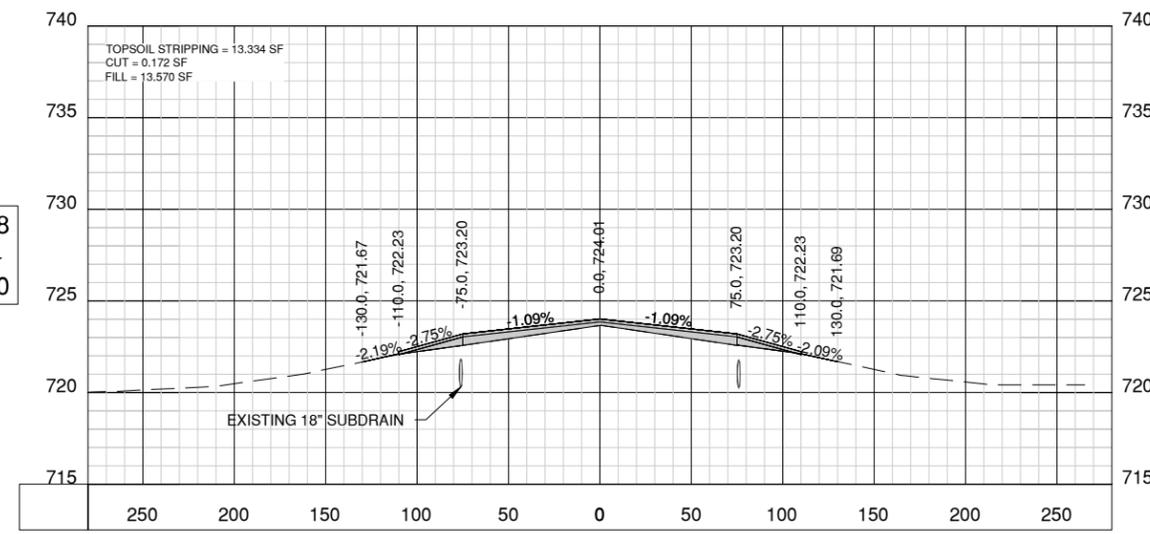
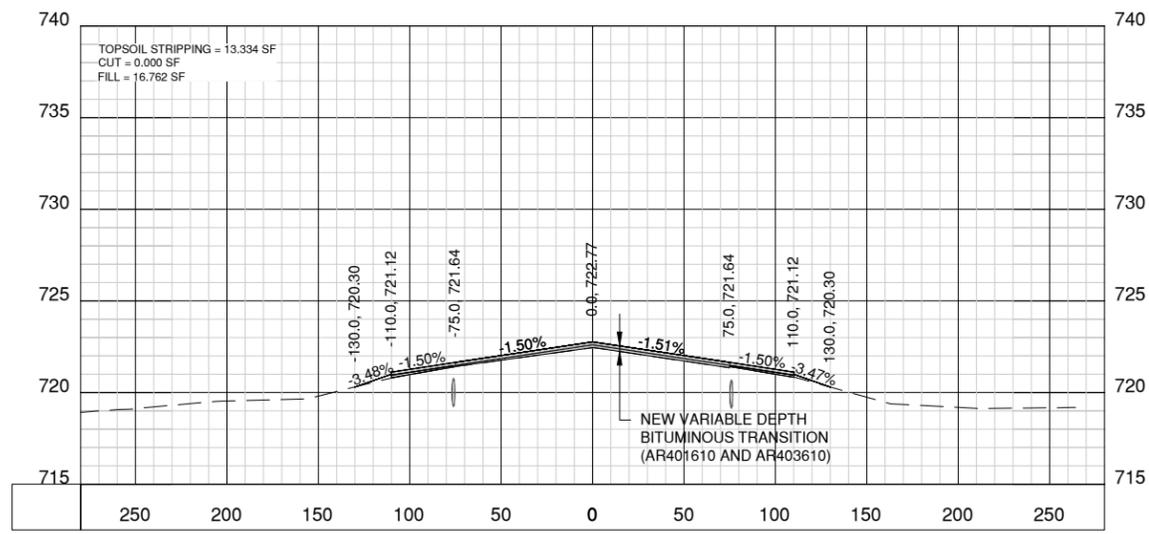
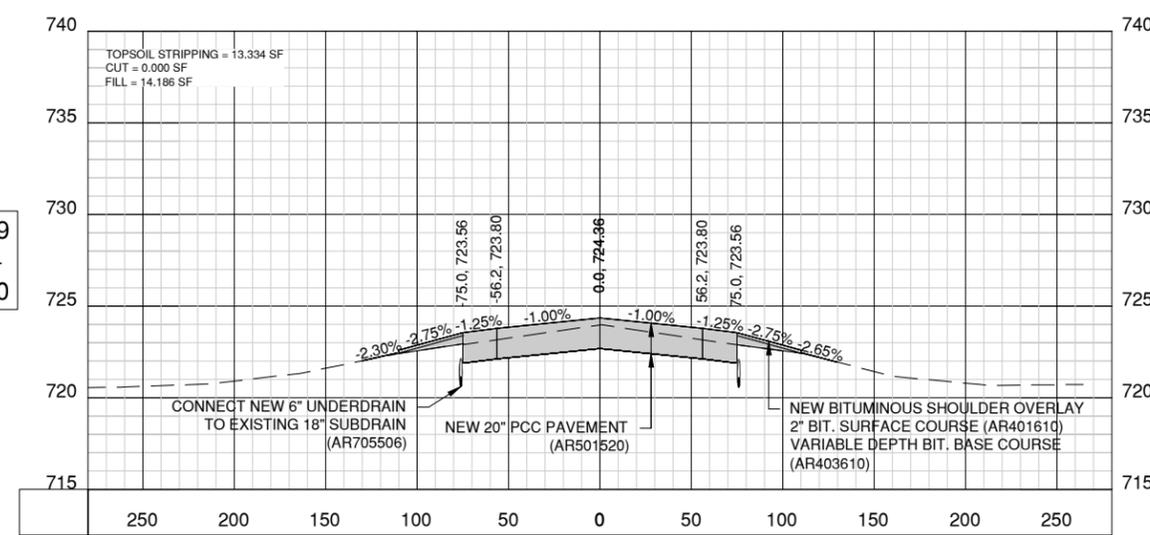
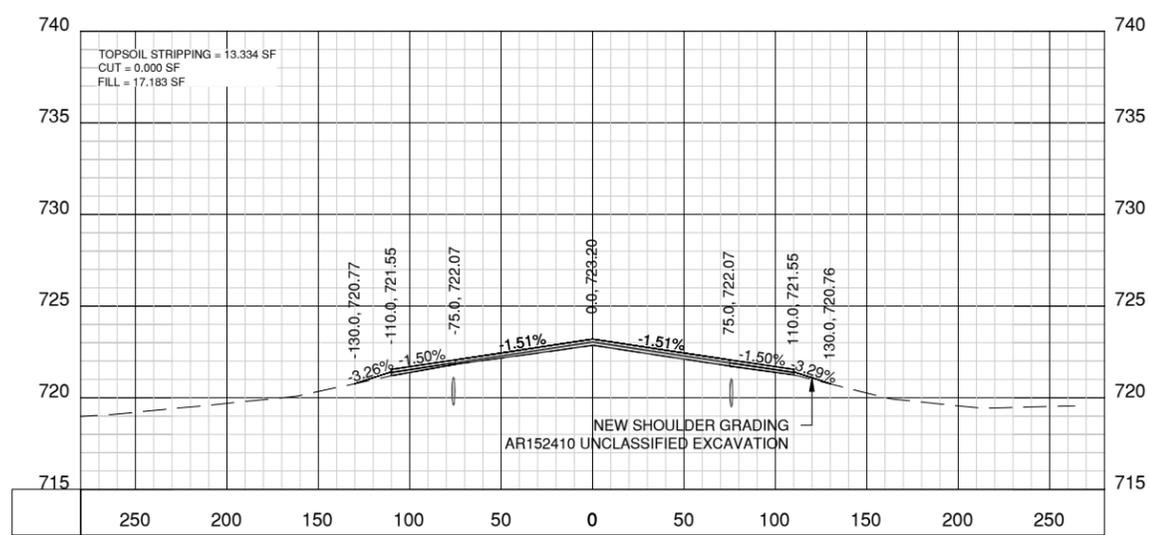
SHEET TITLE:
**RUNWAY 1/19
ADD ALT 1
CROSS SECTIONS 2**

DESIGNED: SMS DRAWN: SMS CHECKED: DLP

PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **A1X2** REVISION
SHEET 61 OF 65 SHEETS

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8 7 6 5 4 3 2 1

8 7 6 5 4 3 2 1



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IL Proj: RFD-4360

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

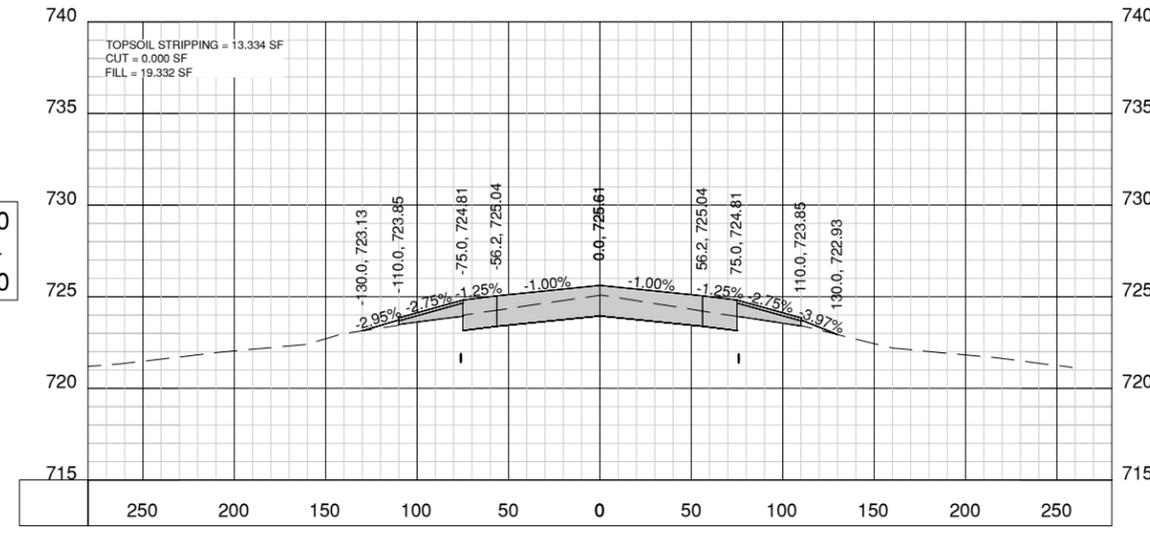
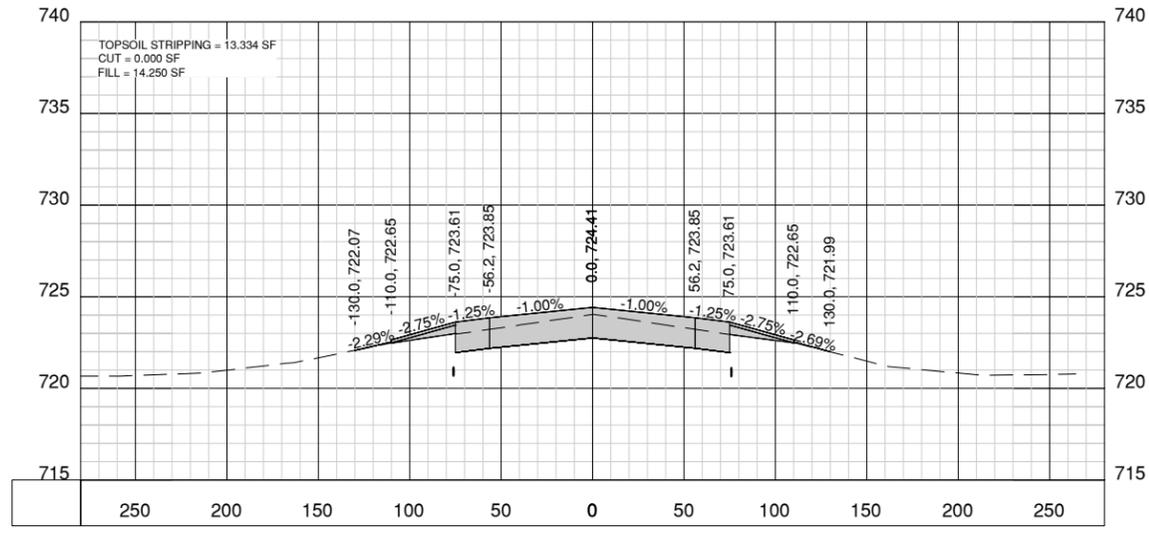
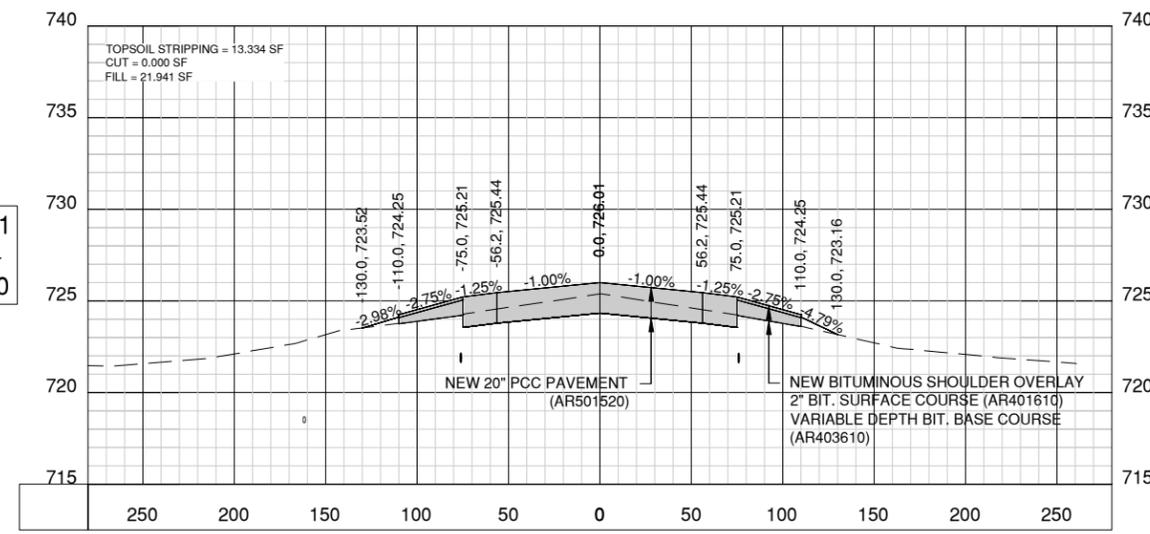
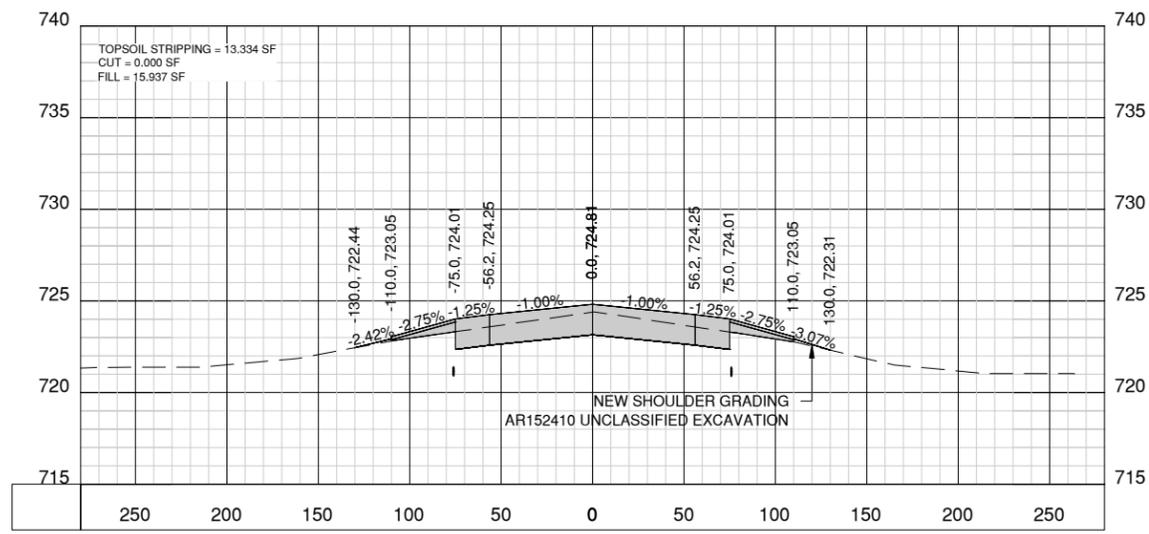
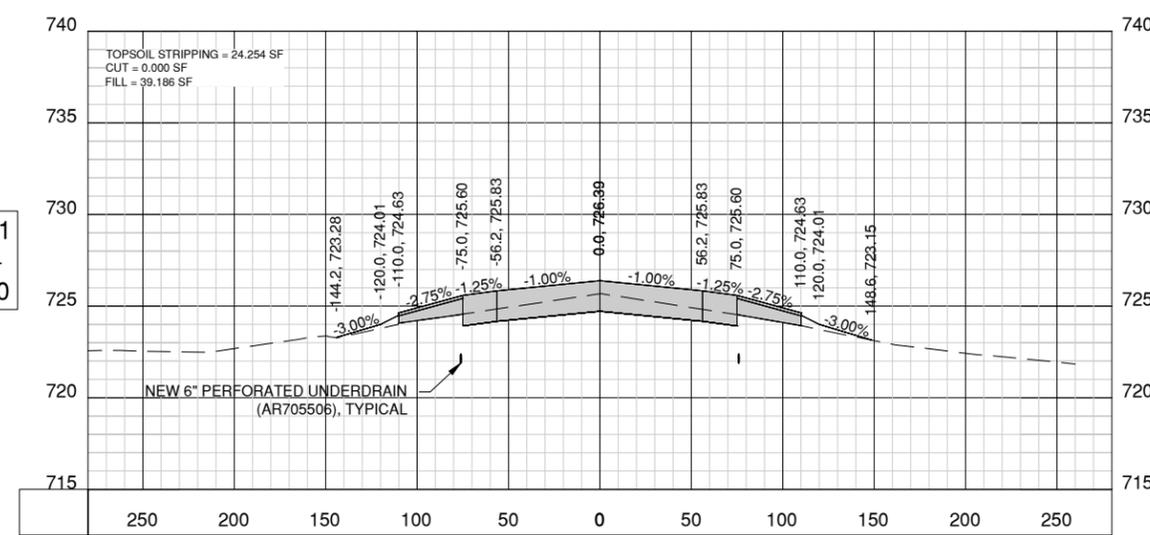
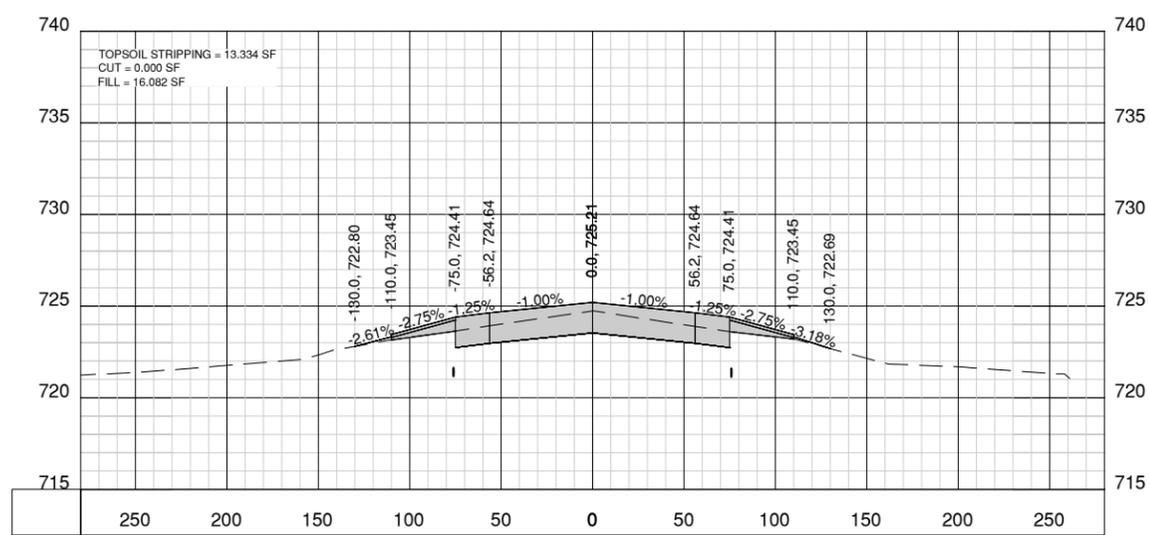
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**RUNWAY 1/19
ADD ALT 1
CROSS SECTIONS 3**

DESIGNED: SMS	DRAWN: SMS	CHECKED: DLP
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PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **A1X3** REVISION
SHEET 62 OF 65 SHEETS

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IL Proj: RFD-4360

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:
RUNWAY 1/19
ADD ALT 2
CROSS SECTIONS 1

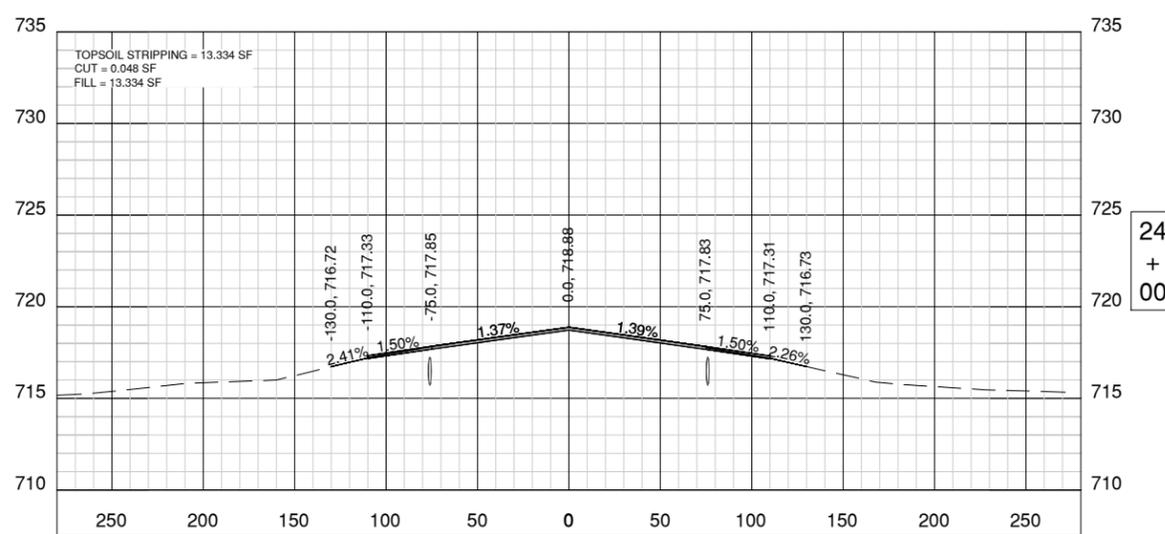
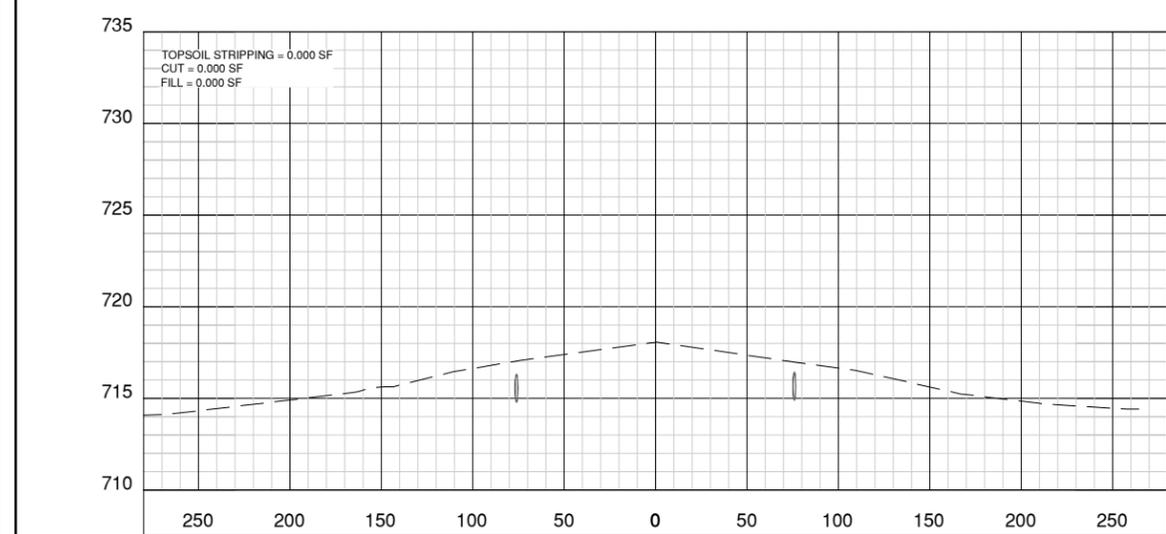
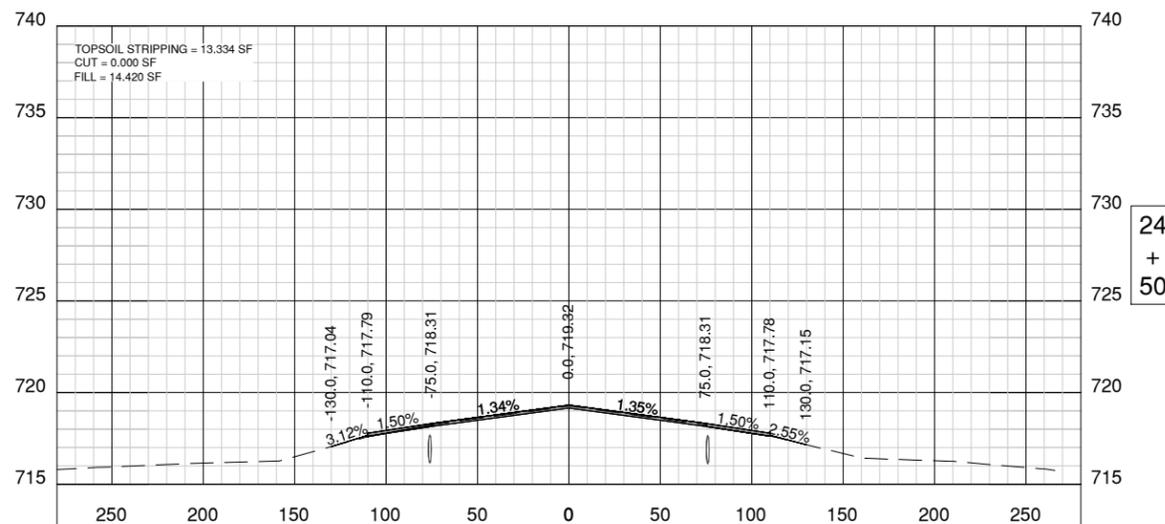
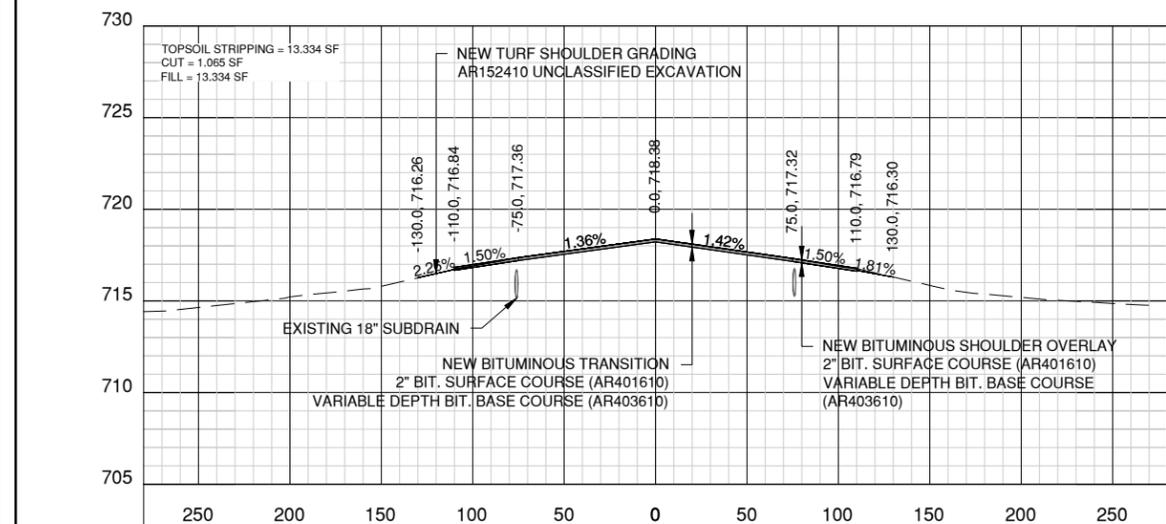
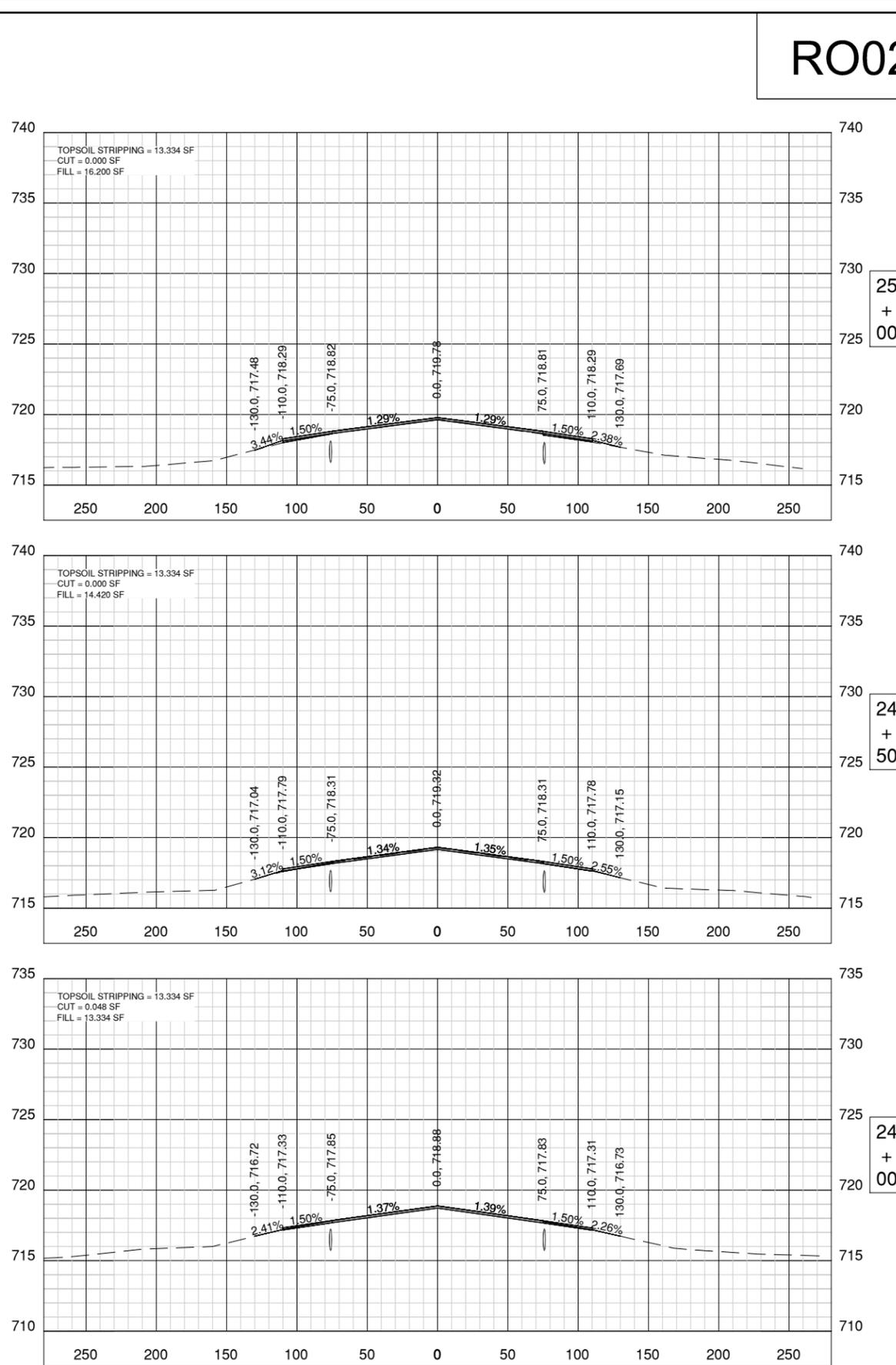
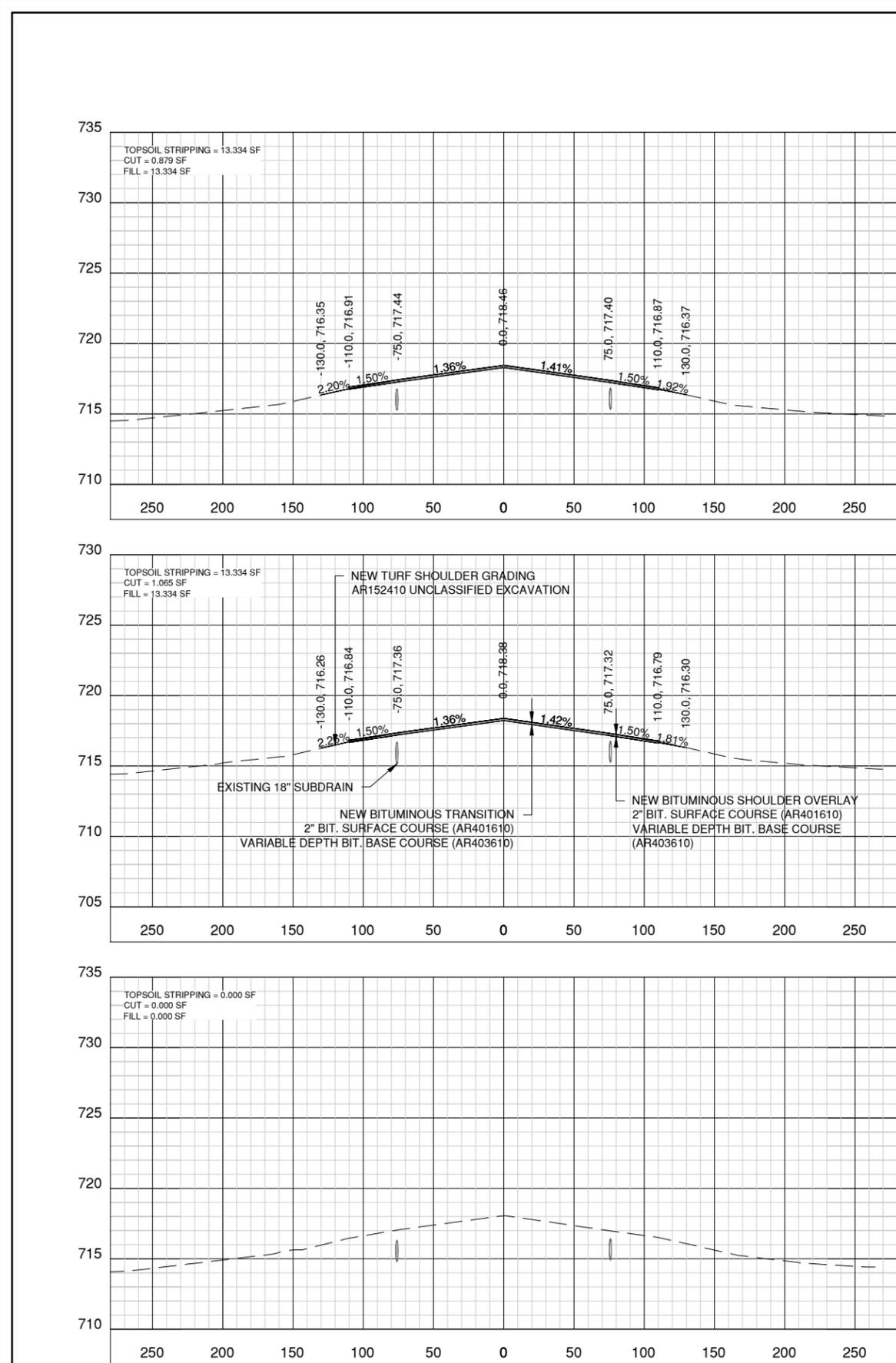
DESIGNED: SMS	DRAWN: SMS	CHECKED: DLP
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PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **A2X1** REVISION

SHEET 63 OF 65 SHEETS

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AIP Proj: 3-17-0088-XX
IL Proj: RFD-4360

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 3**

SHEET TITLE:
**RUNWAY 1/19
ADD ALT 2
CROSS SECTIONS 2**

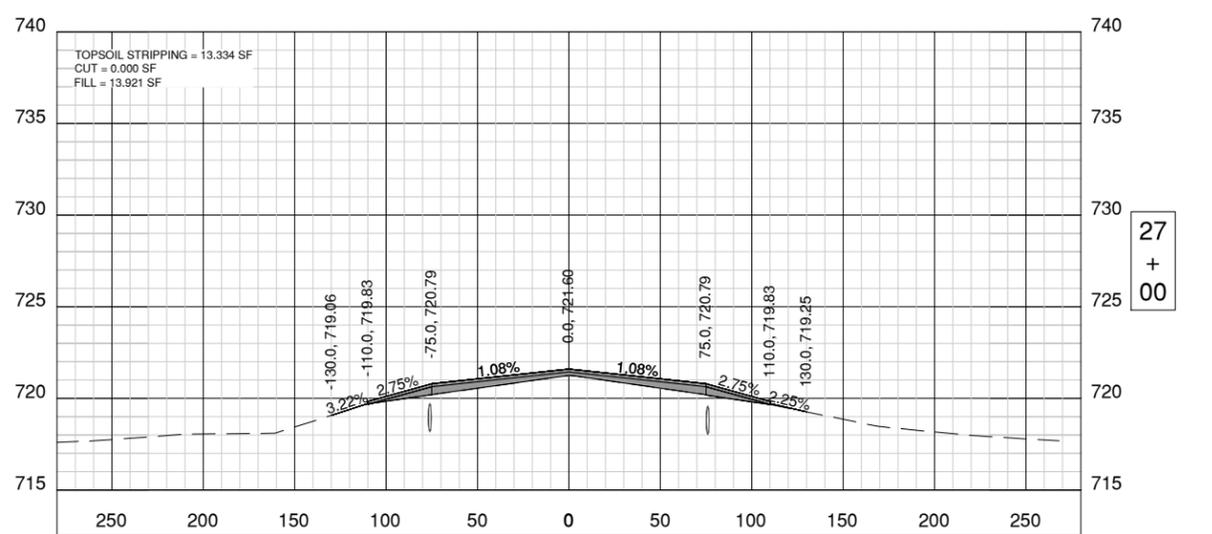
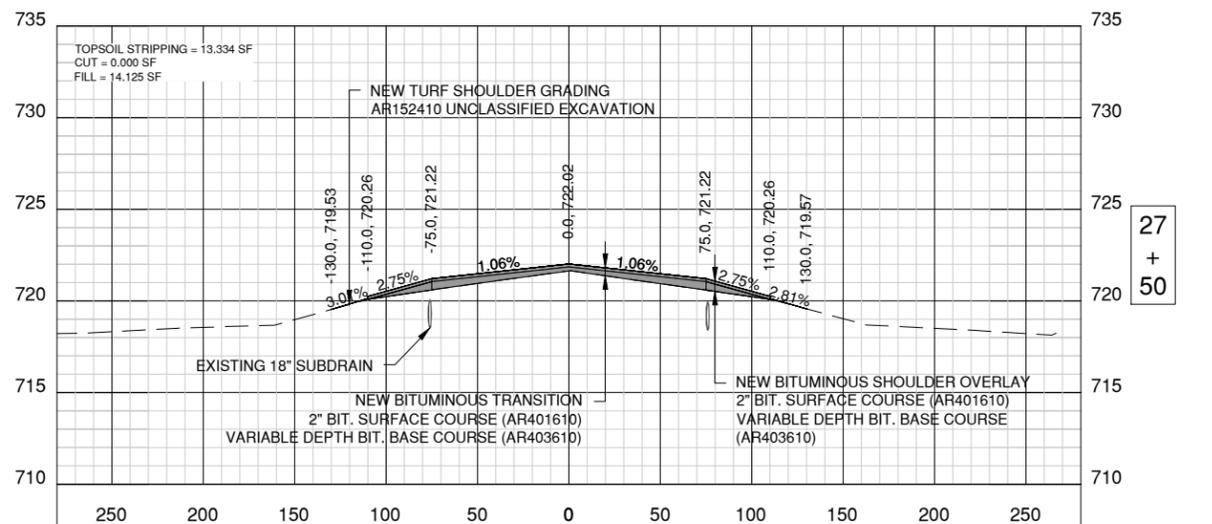
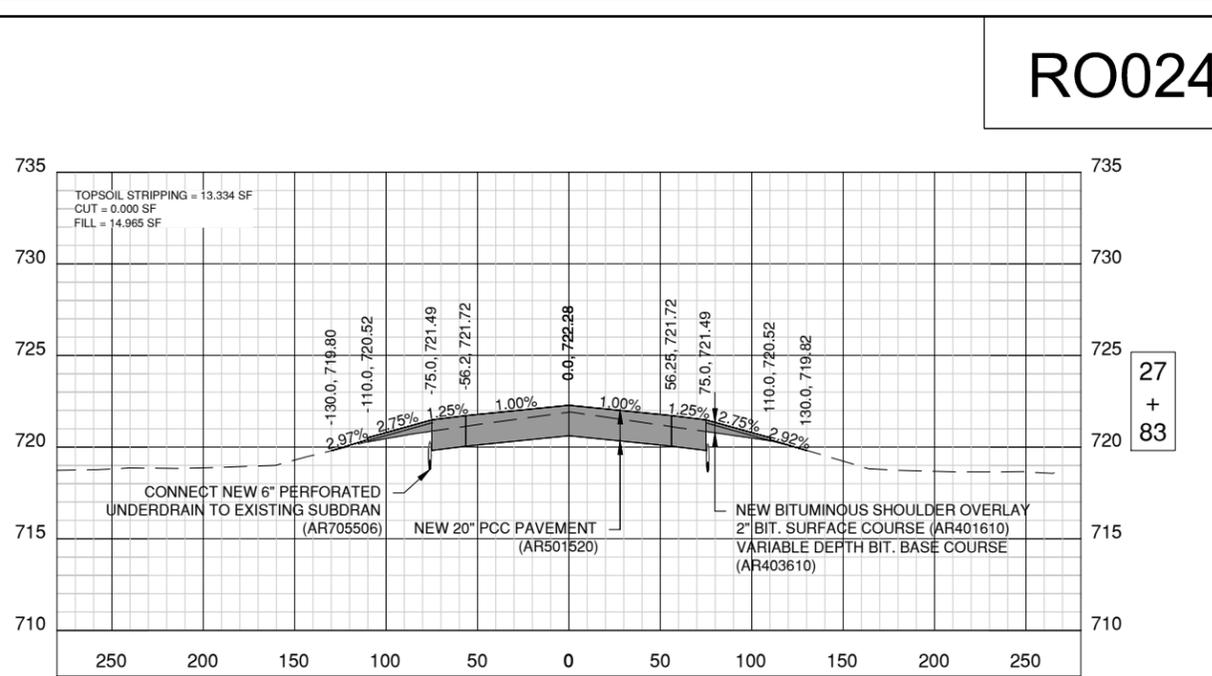
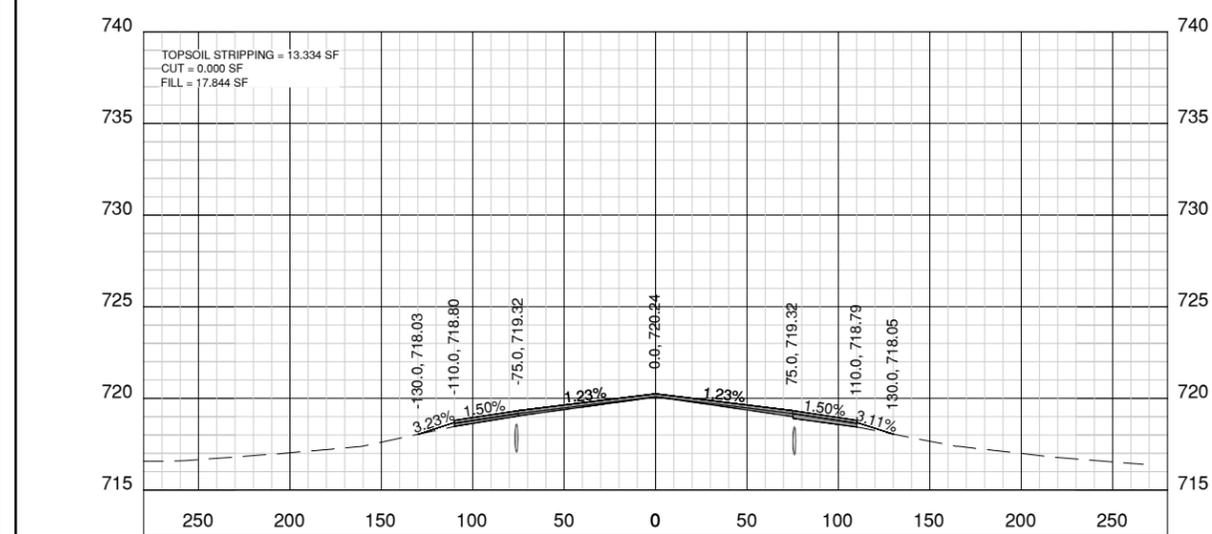
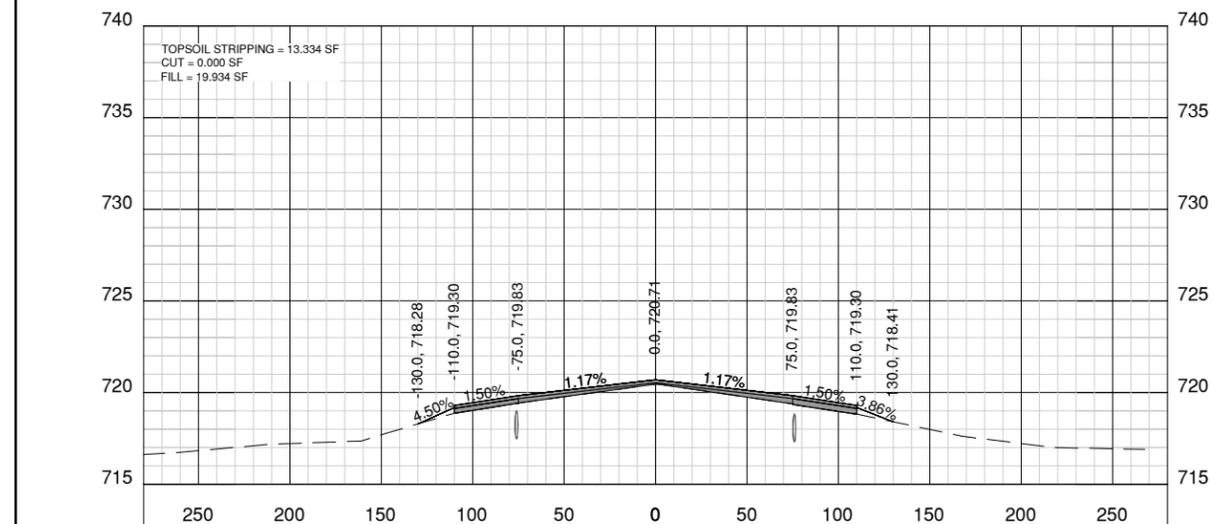
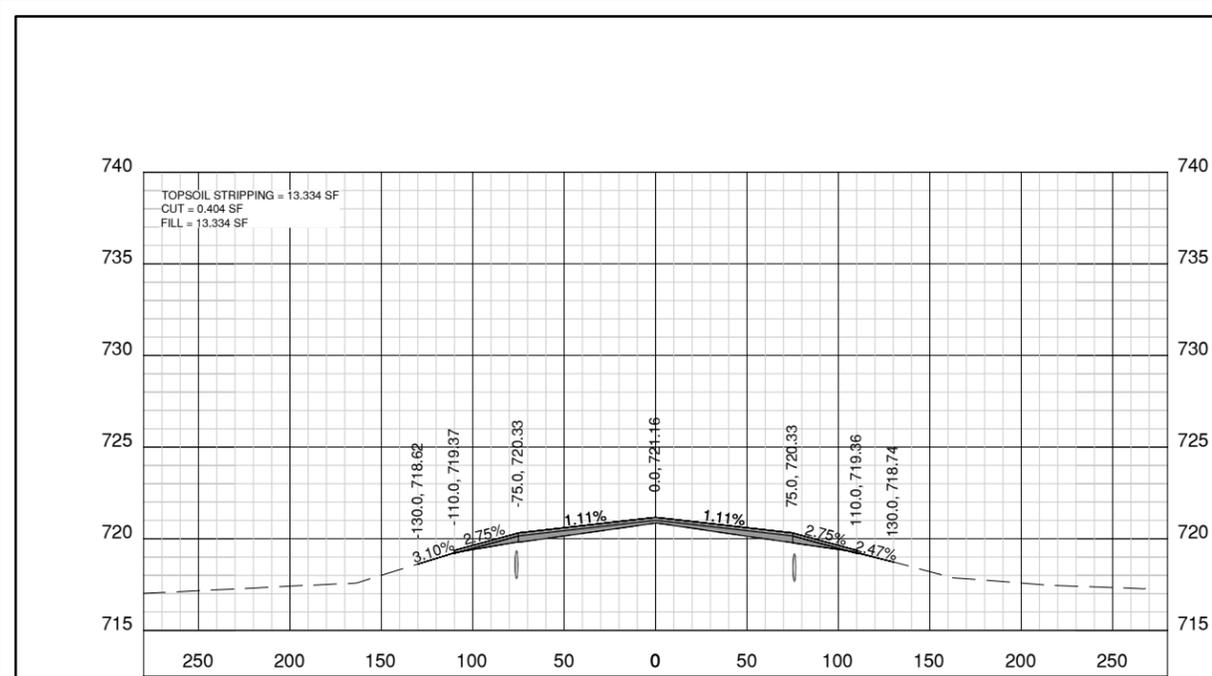
DESIGNED: SMS	DRAWN: SMS	CHECKED: DLP
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PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO. **A2X2** REVISION

SHEET 64 OF 65 SHEETS

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IL Proj: RFD-4360

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 3

SHEET TITLE:
RUNWAY 1/19
ADD ALT 2
CROSS SECTIONS 3

DESIGNED: SMS	DRAWN: SMS	CHECKED: DLP
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PROJECT NO.: 12258-05-00/06
DATE: APRIL 19, 2013

SHEET NO.	A2X3	REVISION
SHEET 65 OF 65 SHEETS		

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