

# CONSTRUCTION PLANS FOR QUAD CITY INTERNATIONAL AIRPORT

## METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND MOLINE, ILLINOIS

IL. PROJ. NO. MLI-4275

### SOUTH GA RAMP EXPANSION - PHASE 1

APRIL 19, 2013

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
AR125100	ELEVATED RETROREFLECTIVE MARKER	EA	2
AR125106	RELOC. ELEV. RETROREFLECTIVE MARKER	EA	6
AR150510	ENGINEER'S FIELD OFFICE	LS	1
AR150520	MOBILIZATION	LS	1
AR152410	UNCLASSIFIED EXCAVATION	CY	1750
AR156510	SILT FENCE	LF	737
AR156520	INLET PROTECTION	EA	1
AR209610	CRUSHED AGG. BASE COURSE - 10"	SY	3724
AR501510	10" PCC PAVEMENT	SY	3702
AR501530	PCC TEST BATCH	EA	1
AR510510	TIE DOWN	EA	15
AR510515	GROUND ROD	EA	4
AR620520	PAVEMENT MARKING - WATERBORNE	SF	100
AR620595	TEMPORARY MARKING & REMOVAL	SF	188
AR705524	4" PERFORATED UNDERDRAIN W/SOCK	LF	125
AR705640	UNDERDRAIN CLEANOUT	EA	1
AR705645	UNDERDRAIN CONNECTION	EA	1
AR705900	REMOVE UNDERDRAIN	LF	252
AR751927	REPLACE FRAME & GRATE	EA	1
AR901510	SEEDING	AC	0.75
AR904510	SODDING	SY	260
AR908510	MULCHING	AC	0.75
AR908520	EXCELSIOR BLANKET	SY	500

Sheet List Table	
Sheet Number	Sheet Title
01	COVER SHEET
02	SITE PLAN
03	CONSTRUCTION ACTIVITY PLAN
04	CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS
05	CONSTRUCTION SAFETY PHASING PLAN NOTES
06	EXISTING CONDITIONS & REMOVALS
07	EXISTING PAVEMENT STRUCTURES
08	PROPOSED IMPROVEMENTS
09	TYPICAL SECTIONS
10	STAKING PLAN
11	JOINTING PLAN
12	JOINTING DETAILS
13	PAVING AND MISCELLANEOUS DETAILS
14	GRADING & DRAINAGE PLAN
15	INLET DETAILS
16	UNDERDRAIN DETAILS
17	MARKING PLAN
18	MARKING & TIE DOWN DETAILS
19	TURFING PLAN & INDEX TO CROSS SECTIONS
20	EROSION CONTROL DETAILS
21	GA RAMP CROSS SECTIONS 1
22	GA RAMP CROSS SECTIONS 2

**811** Know what's below. Call before you dig.

J.U.L.I.E.  
 JOINT UTILITY LOCATING  
 INFORMATION FOR EXCAVATORS  
[www.illinoiscall.com](http://www.illinoiscall.com)

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

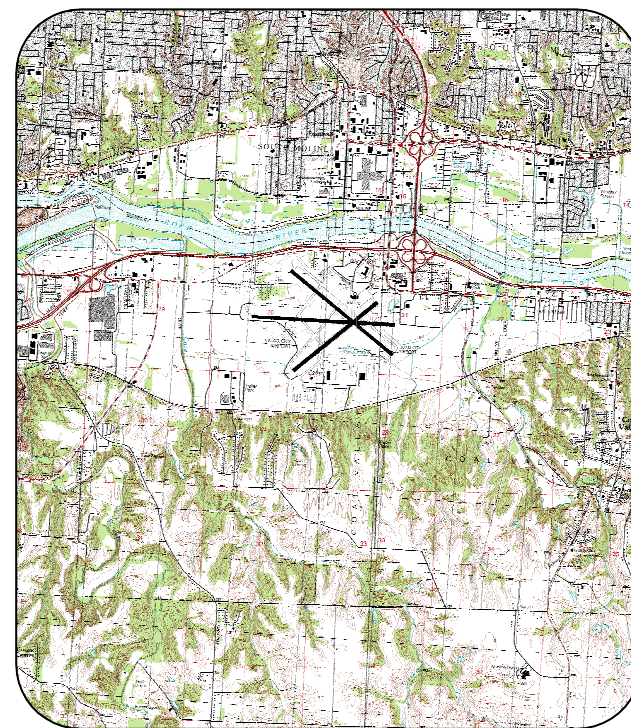
**DESIGN INFORMATION**

**GEOMETRIC CRITERIA**  
 TAXIWAY DESIGN GROUP III  
 AIRCRAFT APPROACH CATEGORY C  
 WHEELBASE OF 37' 4"  
 MAIN GEAR WIDTH OF 21' 0"

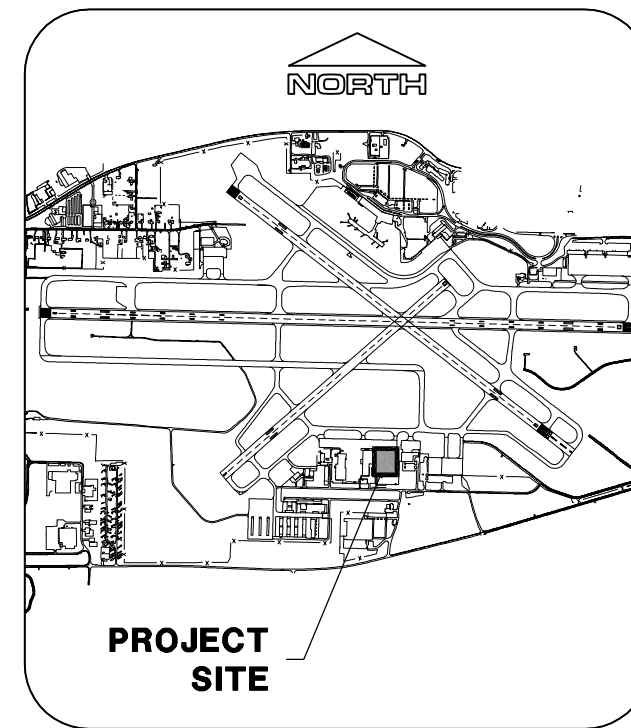
**PAVEMENT DESIGN CRITERIA**  
 DESIGN WEIGHT = 69,830 LBS.

CALL J.U.L.I.E.  
 BEFORE EXCAVATING  
 1-800-892-0123

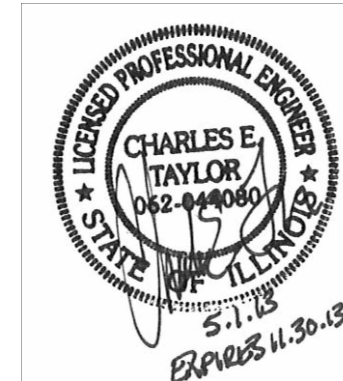
TOWNSHIP: 17 NORTH  
 RANGE: 1 WEST  
 SECTION: 20  
 COUNTY: ROCK ISLAND  
 CIVIL TOWNSHIP: UNINCORPORATED  
 ROCK ISLAND COUNTY



LOCATION MAP



SITE PLAN



MAXIMUM EQUIPMENT HEIGHT - 25'  
 UNICOM FREQUENCY - 122.95

**METROPOLITAN AIRPORT AUTHORITY  
 OF ROCK ISLAND  
 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**

APPROVED *Bruce Carter*  
 EXECUTIVE DIRECTOR OF AVIATION

DATE *May 1, 2013*

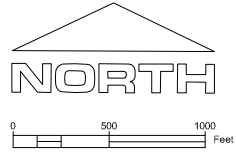
**GMT**  
**CRAWFORD MURPHY & TILLY, INC.**  
 CONSULTING ENGINEERS

■ SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO

SUBMITTED BY *Charles E. Taylor*  
 DATE *May 1, 2013*

CMT JOB NUMBER: 13014-02-00

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 FILE: 01 COVER SHEET.dwg  
 UPDATE BY: Andrew Bodine  
 PLOT DATE: 5/20/2013 9:51 AM



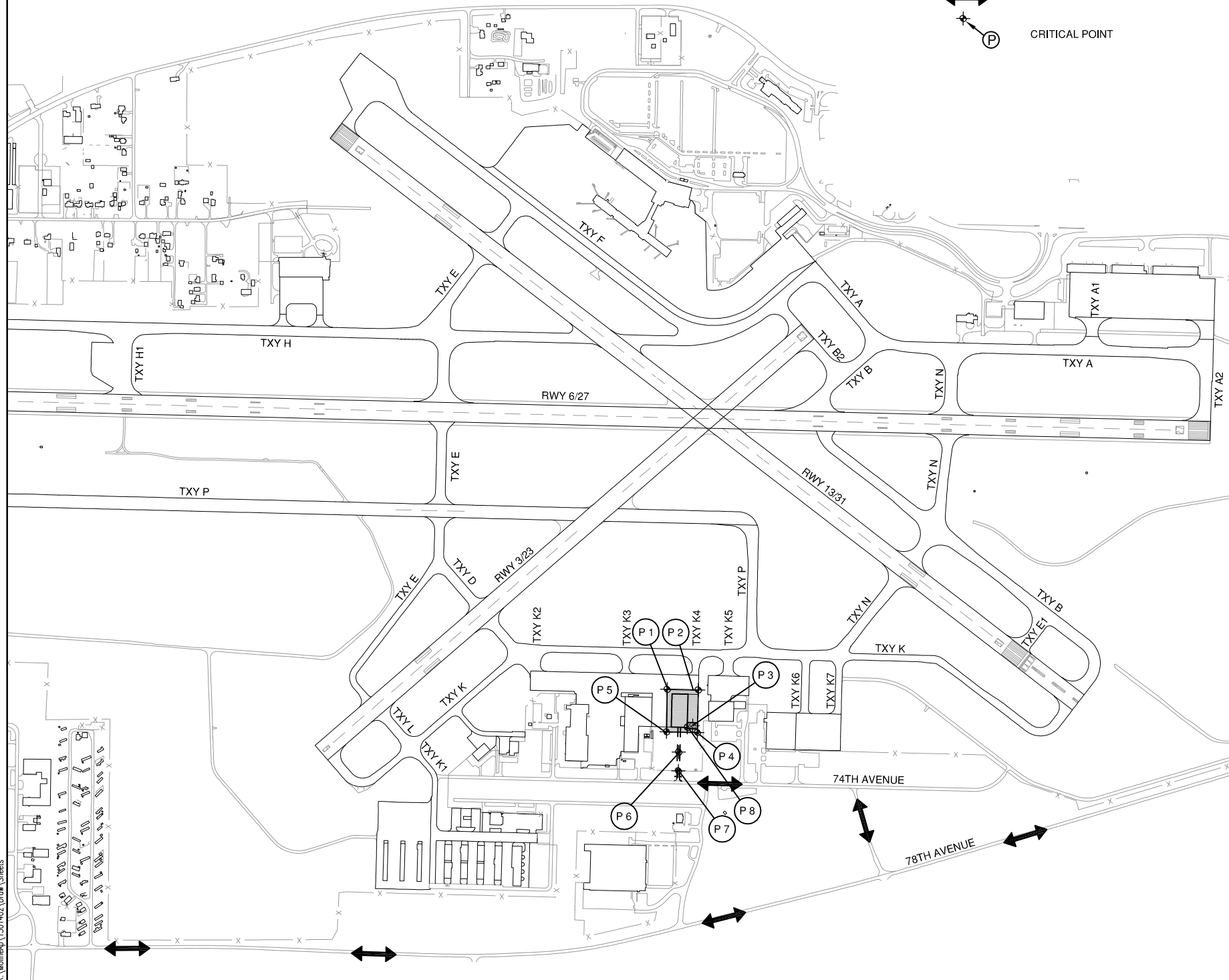
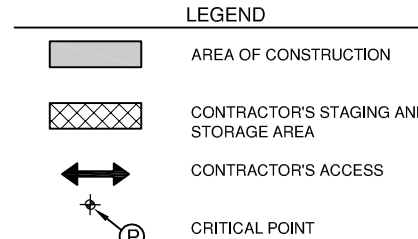
CRITICAL POINTS						
POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	MAX. EQUIP. HEIGHT	OVERALL ELEVATION	DESCRIPTION
P1	41°26'34.97"	90°30'21.44"	579.5'	25'	604.5'	LIMITS OF WORK
P2	41°26'34.93"	90°30'18.25"	579.3'	25'	604.3'	UNITS OF WORK
P3	41°26'32.17"	90°30'18.82"	578.2'	25'	603.2'	STAGING AND STORAGE
P4	41°26'31.65"	90°30'18.33"	577.9'	25'	602.9'	LIMITS OF WORK
P5	41°26'31.69"	90°30'21.49"	577.8'	25'	602.8'	LIMITS OF WORK
P6	41°26'30.14"	90°30'20.24"	577.1'	25'	602.1'	TEMPORARY ACCESS ROADWAY C/L
P7	41°26'28.69"	90°30'20.27"	576.8'	25'	601.8'	TEMPORARY ACCESS ROADWAY C/L
P8	41°26'32.04"	90°30'19.37"	579.7'	25'	604.7'	NEW PAVEMENT CORNER

BASELINE INFORMATION			
BASELINE	STATION	NORTHING	EASTING
RAMP	100+00	1738873.5750	2203644.6290
RAMP	110+07.17	1739880.4390	2203669.3600

UNICOM FREQUENCY - 121.95  
 MAXIMUM HEIGHT OF EQUIPMENT - 25'

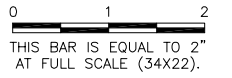
**GENERAL NOTES**

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT DIRECTOR.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
- BROKEN ASPHALT AND OR CONCRETE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 93' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
- ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND TO ORIGINAL CONDITION BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
- THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH ARE OR WILL BE OPENED TO AIR TRAFFIC.
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- SEE CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS SHEET FOR NOTES REGARDING THE CONTRACTOR'S ACCESS.
- CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE LIGHTED, MARKED AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
  - THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
  - THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE AIRPORT OPERATIONS BY THE CONTRACTOR.
  - THE CONTRACTOR SHALL USE A GATE TEMPORARY AS CALLED OUT IN THE PLANS, FOR ACCESS TO THE AIRFIELD.
  - CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
  - DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED BY THE AIRPORT.
  - THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
  - ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
  - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY SAFETY AREAS AND TAXIWAY OBJECT FREE AREAS.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE USED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.



**QU017**

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**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**

**SOUTH GA RAMP EXPANSION - PHASE 1  
 SITE PLAN**

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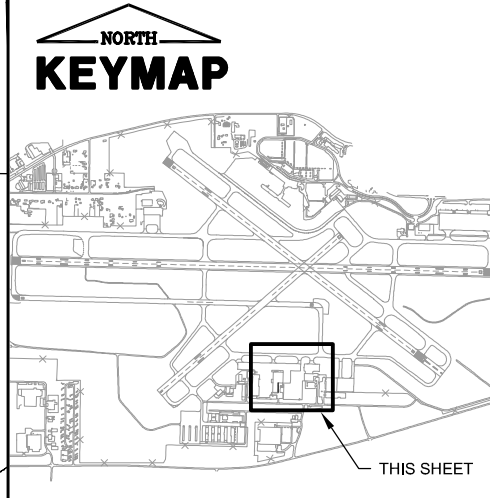
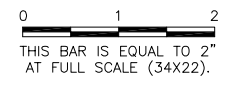
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**LEGEND**

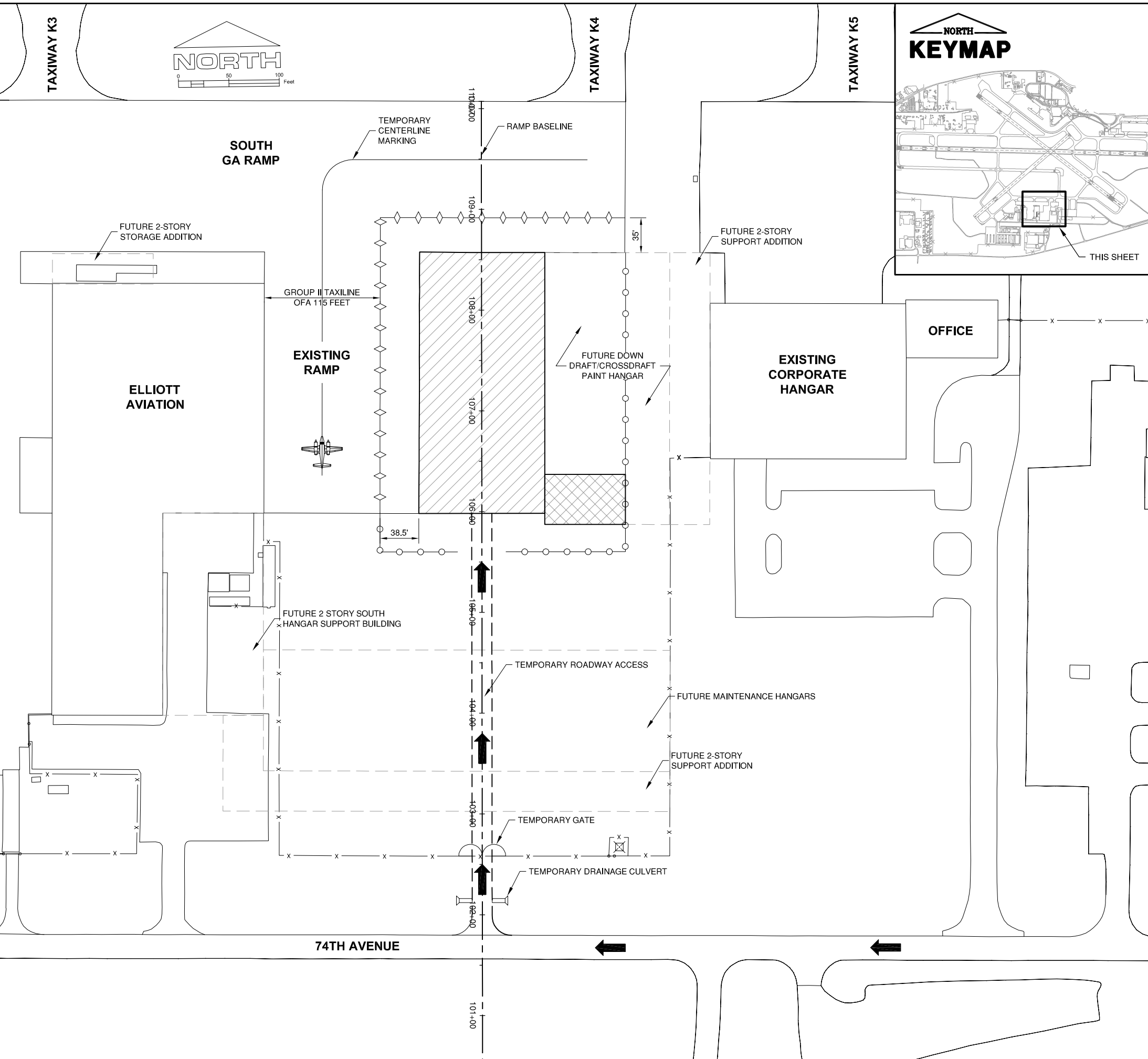
- WORK AREA
- CONTRACTOR'S ACCESS ROUTE
- BEAM BARRICADES
- CONTRACTOR'S STAGING AND STORAGE AREA
- FENCE
- CONSTRUCTION SETBACK LINE
- AIR TRAFFIC ACCESS

**PHASING NOTES:**

1. ALL TAXIWAYS, TAXILANES, AND RUNWAYS SHALL REMAIN OPEN FOR THE DURATION OF THE WORK.
2. PRIOR TO BEGINNING THIS PHASE, THE CONTRACTOR SHALL PLACE ALL BARRICADES AND SETBACK LINES AS SHOWN ON THE PLAN.
3. CONSTRUCTION OPERATIONS SHALL REMAIN CLEAR OF THE BEAM BARRICADES AT ALL TIMES.
4. CONTRACTOR'S WORK IN THIS AREA SHALL BE COORDINATED WITH THE TENANTS THROUGH THE AIRPORT.
5. PAYMENT FOR THE TEMPORARY GATE, TEMPORARY ACCESS ROAD, AND TEMPORARY DRAINAGE CULVERT SHALL BE INCIDENTAL TO THE MOBILIZATION PAY ITEM.
6. CONTRACTOR SHALL MARK A TEMPORARY CENTERLINE FOR THE AIRCRAFT THAT WILL TAXI TO AND FROM ELLIOTT AVIATION. FINAL LAYOUT WILL BE DETERMINED BY THE RESIDENT ENGINEER.

**PHASING - WORK DETAIL NOTES:**

- CONSTRUCTION WORK SHALL INCLUDE THE FOLLOWING:
- CONSTRUCT A TEMPORARY ACCESS ROADWAY AND INSTALL A TEMPORARY GATE FOR SITE ACCESS.
  - GRADING AND COMPACTING OF SUBGRADE FOR THE SOUTH GENERAL AVIATION RAMP EXPANSION.
  - PLACING, GRADING, AND COMPACTING CRUSHED AGGREGATE BASE COURSE FOR THE SOUTH GENERAL AVIATION RAMP EXPANSION.
  - PLACING PCC FOR THE SOUTH GENERAL AVIATION RAMP EXPANSION PAVEMENT STRUCTURE.
  - INSTALL INLETS AND STORM SEWER UNDERDRAIN AROUND PORTIONS OF THE SOUTH GENERAL AVIATION RAMP EXPANSION.
  - NEW AIRFIELD PAVEMENT MARKING.



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND**  
**QUAD CITY INTERNATIONAL AIRPORT**  
**MOLINE, ILLINOIS**  
**SOUTH GA RAMP EXPANSION - PHASE 1**  
**CONSTRUCTION ACTIVITY PLAN**

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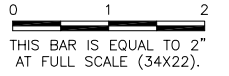
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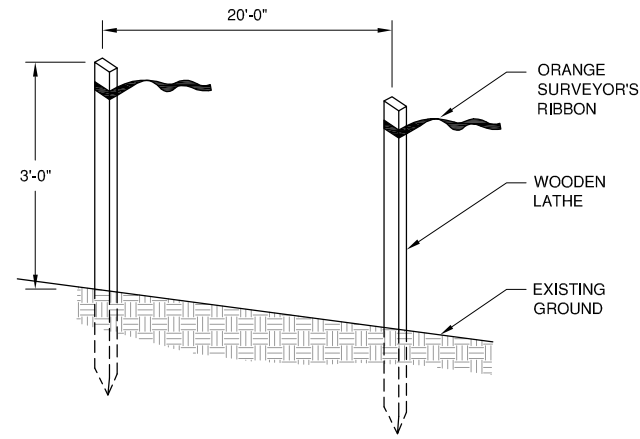
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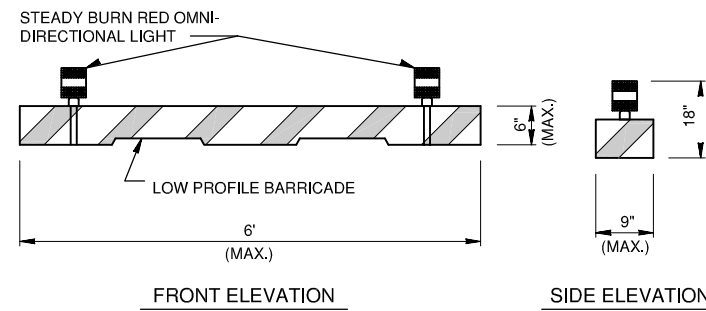
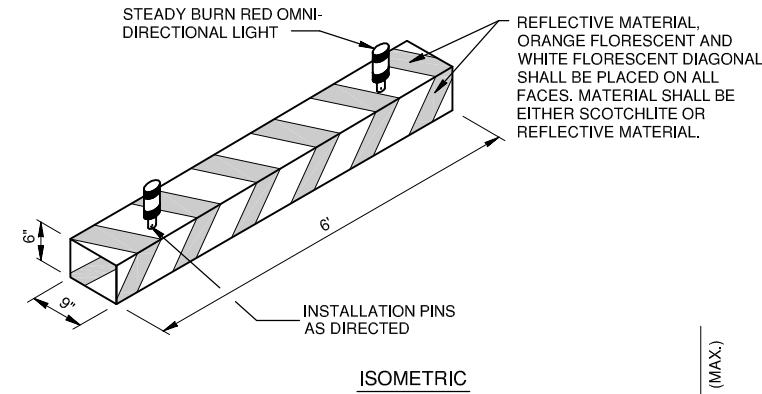


**CONSTRUCTION ACTIVITY PLAN GENERAL NOTES**

1. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
2. THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
3. IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT ARE THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
4. WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT AND THE PROJECT.
5. DELAYS, CHANGES IN SCHEDULING OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. THE CONTRACTOR SHALL BE AWARE THAT DAILY OPERATION OF THE AIRPORT WILL CONTINUE THROUGHOUT THIS PROJECT. AS A RESULT, DAILY COORDINATION WILL BE NECESSARY TO LIMIT DISRUPTION TO AIRPORT/TENTANT AND CONTRACTOR OPERATIONS.
7. CONTRACTOR'S STAGING AND STORARE WILL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS.
8. THE CONTRACTOR SHALL PLACE ALL BARRICADES, CONSTRUCTION SETBACK LINES, AND EROSION CONTROL ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.



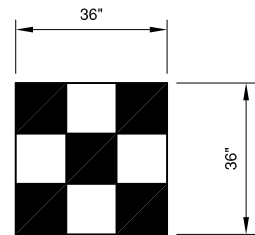
**CONSTRUCTION SETBACK LINE DETAIL**  
N.T.S.



**BEAM BARRICADE DETAILS**  
N.T.S.

**BEAM BARRICADE NOTES**

- 1.) BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
- 2.) BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
- 3.) PLACE AT 10' INTERVALS.
- 4.) NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.



**CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG**  
N.T.S.

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND**  
**QUAD CITY INTERNATIONAL AIRPORT**  
**MOLINE, ILLINOIS**  
**SOUTH GA RAMP EXPANSION - PHASE 1**  
**CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS**

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**GENERAL**

1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING SAFETY REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL SUBMIT THE SPCD FOR APPROVAL.
6. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
7. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

**1. COORDINATION**

1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

**2. PHASING**

1. TOTAL CONTRACT TIME SHALL BE 60 CALENDAR DAYS.
2. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

**PHASING - WORK DETAIL NOTES**

1. THE WORK SHALL CONSIST OF THE CONSTRUCTION OF THE EMBANKMENT, STORM DRAINAGE, SUBGRADE, AGGREGATE BASE, P.C.C. PAVEMENT, AND MARKING ON THE SOUTH GENERAL AVIATION RAMP EXPANSION.

**3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY**

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

**4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)**

1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

**5. CONTRACTOR ACCESS**

1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE TEMPORARY GATE SHOWN.
3. CERTAIN CONTRACTOR EMPLOYEES SHALL OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
4. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE.
5. THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN.
6. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
7. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
8. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
9. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
10. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
11. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
12. THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

**6. WILDLIFE MANAGEMENT**

1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

**7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT**

1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

**8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT**

1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

**9. NOTIFICATION OF CONSTRUCTION ACTIVITIES**

1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
5. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

**AIRPORT OPERATOR**  
 BRUCE CARTER - DIRECTOR OF AVIATION (309) 757-1732  
 BRYAN JOHNSON - ASSISTANT DIRECTOR OF AVIATION (309) 757-1754

**ENGINEER**  
 CHUCK TAYLOR P.E. - PROJECT MANAGER (217) 787-8050  
 CMT - RESIDENT ENGINEER (217) 787-8050

**10. INSPECTION REQUIREMENTS**

1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

**11. UNDERGROUND UTILITIES**

1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

**12. PENALTIES**

1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

**13. SPECIAL CONDITIONS**

1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

**14. RUNWAY AND TAXIWAY VISUAL AIDS**

1. NO RUNWAY OR TAXIWAY CLOSURES ARE REQUIRED FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2F.
2. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

**15. HAZARD MARKING AND LIGHTING**

1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

**16. PROTECTION**

1. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 93' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

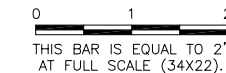
**17. OTHER LIMITATIONS ON CONSTRUCTION**

1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

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**REVISIONS**

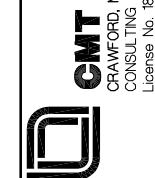
NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**

**SOUTH GA RAMP EXPANSION - PHASE 1  
 CONSTRUCTION SAFETY PHASING PLAN  
 NOTES**

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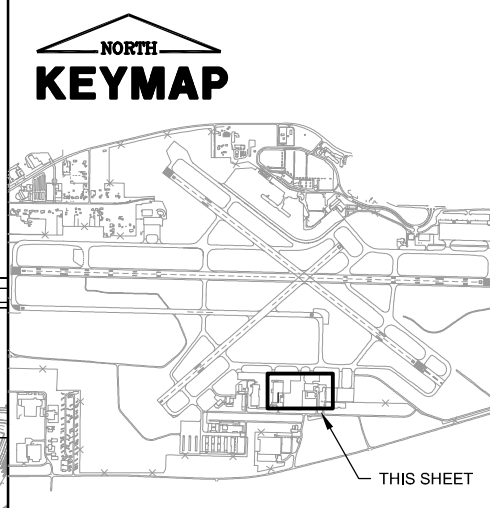


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**CONTRACTORS CONTROL POINTS**

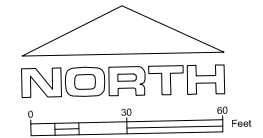
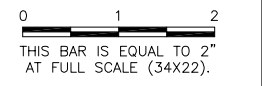
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2	GROUND ROD	1739758.3460	2203458.6030	579.66



FILE: 06 EXISTING CONDITIONS & REMOVALS  
 UPDATE BY: Andrew Bodine  
 PLOT DATE: 5/20/2013 9:53 AM  
 MLI\_Bose\_Ext\_Util  
 BASE PROP GEO  
 EXIST-UTIL  
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**ELLIOTT AVIATION**

**EXISTING SOUTH GA RAMP**

**PROPOSED SOUTH GA RAMP EXPANSION**

**EXISTING CORPORATE HANGAR**

**LEGEND**

	EXISTING PAVEMENT
	TO BE RELOCATED
	EXISTING PAVEMENT MARKING
	EXISTING MOORING EYE/GROUND ROD
	EXISTING INLET
	PROPOSED PAVEMENT
	EXISTING SANITARY SEWER
	EXISTING FENCE
	EXISTING JOINT
	EXISTING WATER LINE
	EXISTING STORM SEWER
	EXISTING UNDERGROUND TELEPHONE
	EXISTING UNDERGROUND ELECTRIC
	EXISTING GAS LINE
	EXISTING UNDERDRAIN
	EXISTING RETROREFLECTIVE MARKER
	EXISTING SANITARY SEWER MANHOLE
	EXISTING STORM SEWER MANHOLE
	TO BE REMOVED

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, THE DIVISION, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

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 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**

**SOUTH GA RAMP EXPANSION - PHASE 1  
 EXISTING CONDITIONS &  
 REMOVALS**

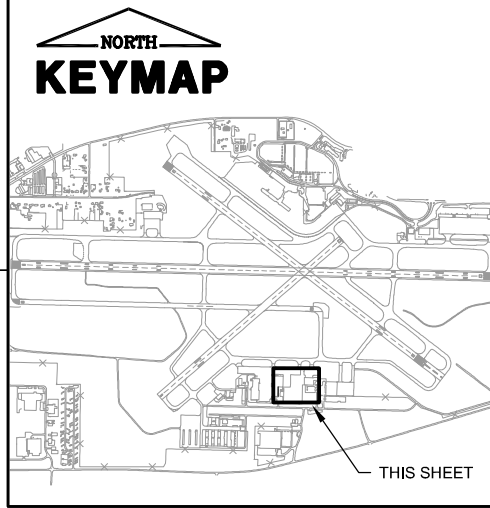
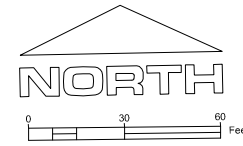
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SHEET	06 OF 22 SHEETS

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TAXIWAY K3

TAXIWAY K4



FILE: 07 EXISTING PAVEMENT STRUCTURE  
UPDATE BY: Andrew Bodine  
PLOT DATE: 5/20/2013 9:53 AM

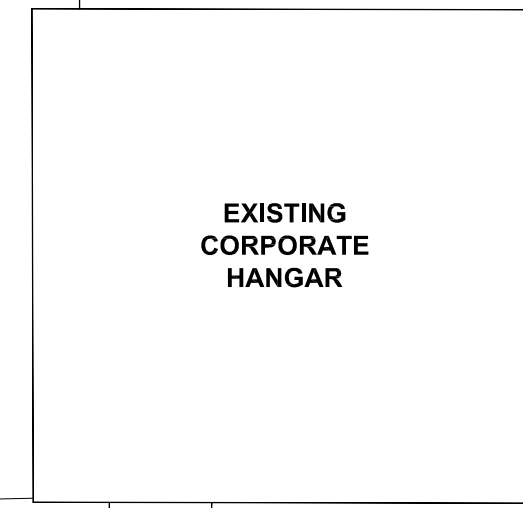
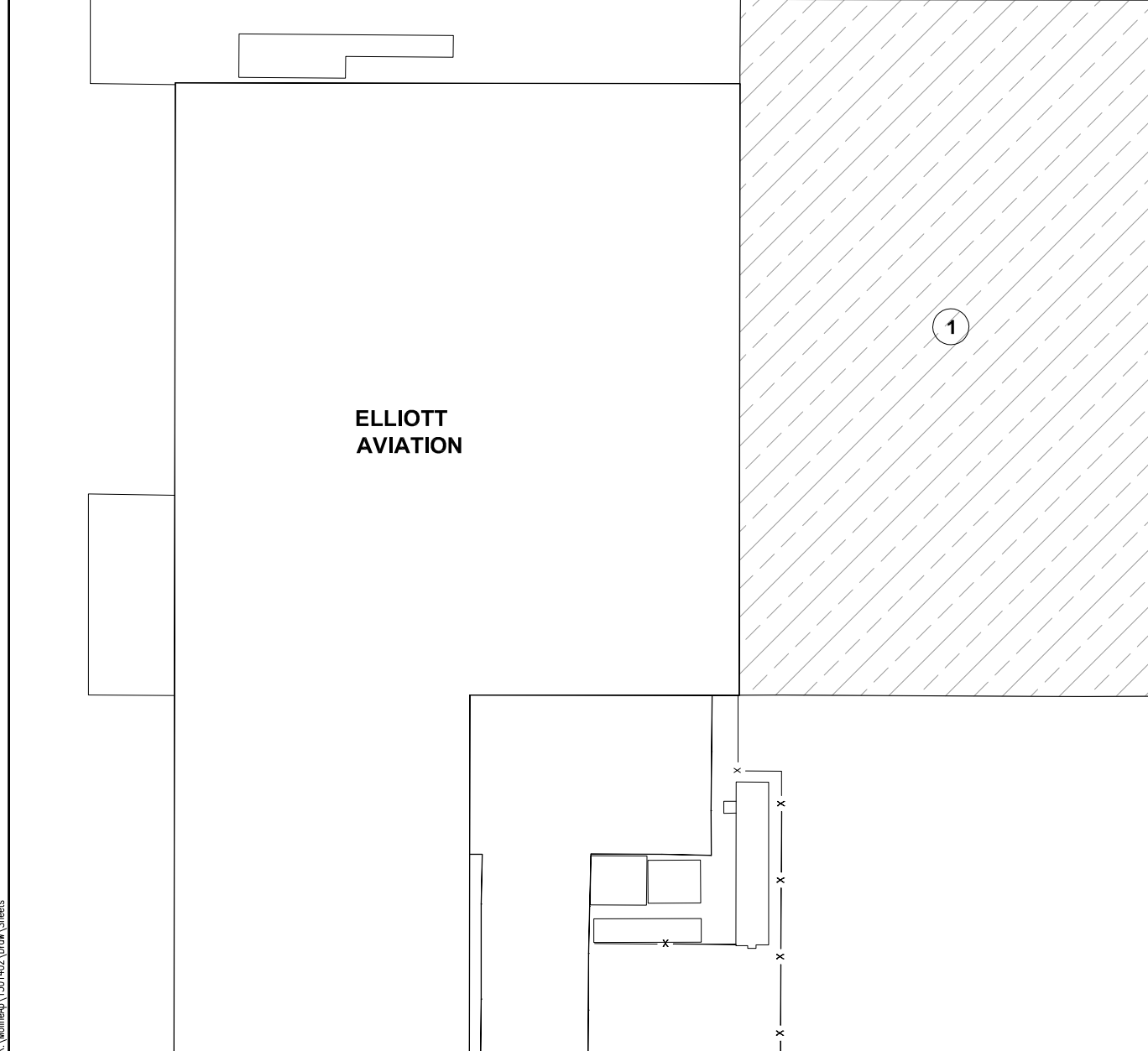
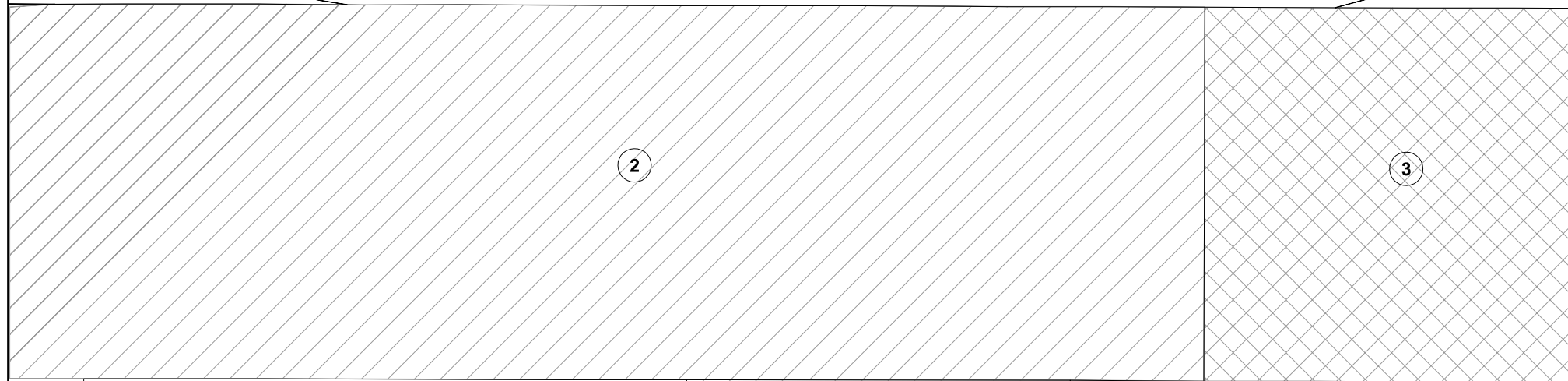
quad-plan  
Keymap

QU017




REVISIONS

NUMBER	BY	DATE

0 1 2  
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



LEGEND

-  ① 10" PCC PAVEMENT (501)  
10" CRUSHED AGGREGATE BASE COURSE (209)
-  ② 9" PCC PAVEMENT (501)  
14" PCC PAVEMENT (501)  
7" BITUMINOUS BASE COURSE (201)  
6" TREATED LIME SUBGRADE (403)
-  ③ 9" PCC PAVEMENT (501)  
4" BITUMINOUS SURFACE COURSE (401)  
9.5" BITUMINOUS BASE COURSE (403)

NOTE:  
THE INFORMATION ON THIS SHEET HAS BEEN  
OBTAINED FROM AVAILABLE RECORD DRAWINGS.

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SOUTH GA RAMP EXPANSION - PHASE 1  
EXISTING PAVEMENT STRUCTURES

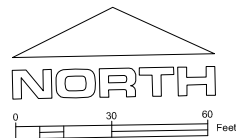
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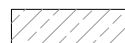
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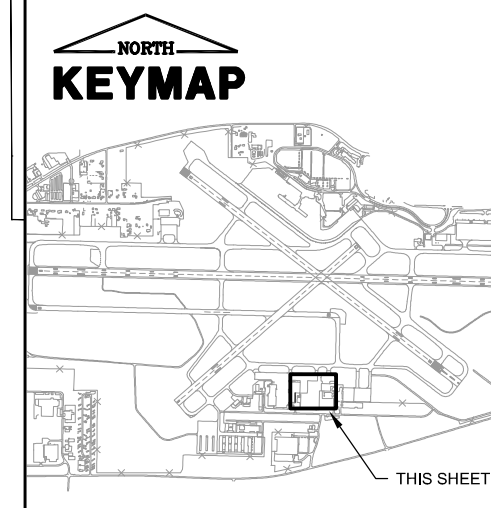
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NOTE:  
SEE SITE PLAN FOR BASELINE INFORMATION.

**LEGEND**



10" P.C.C. PAVEMENT (501)  
10" CRUSHED AGGREGATE BASE COURSE (209)  
NEW COMPACTED SUBGRADE (152)



FILE: 08 PROPOSED IMPROVEMENTS.dwg  
UPDATE BY: Andrew Bodine  
PLOT DATE: 5/20/2013 9:53 AM

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EXISTING CORPORATE HANGAR

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SOUTH GA RAMP EXPANSION - PHASE 1  
PROPOSED IMPROVEMENTS

EXISTING AIRPORT PERIMETER FENCE

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SHEET 08 OF 22 SHEETS	

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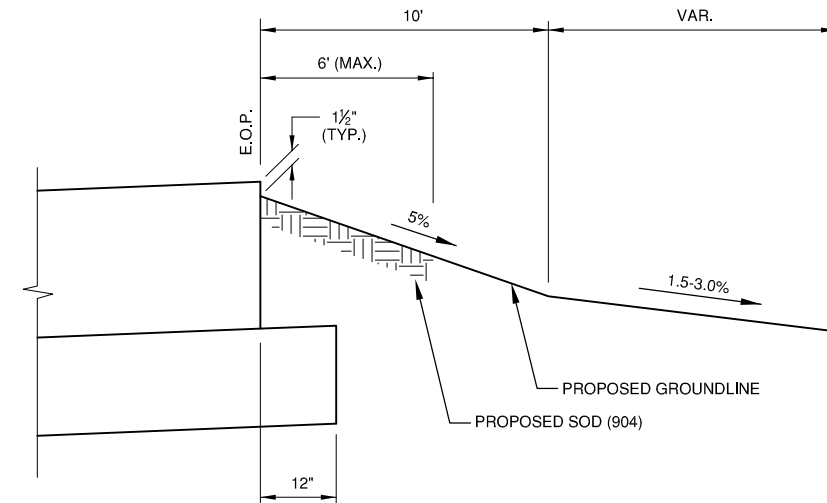
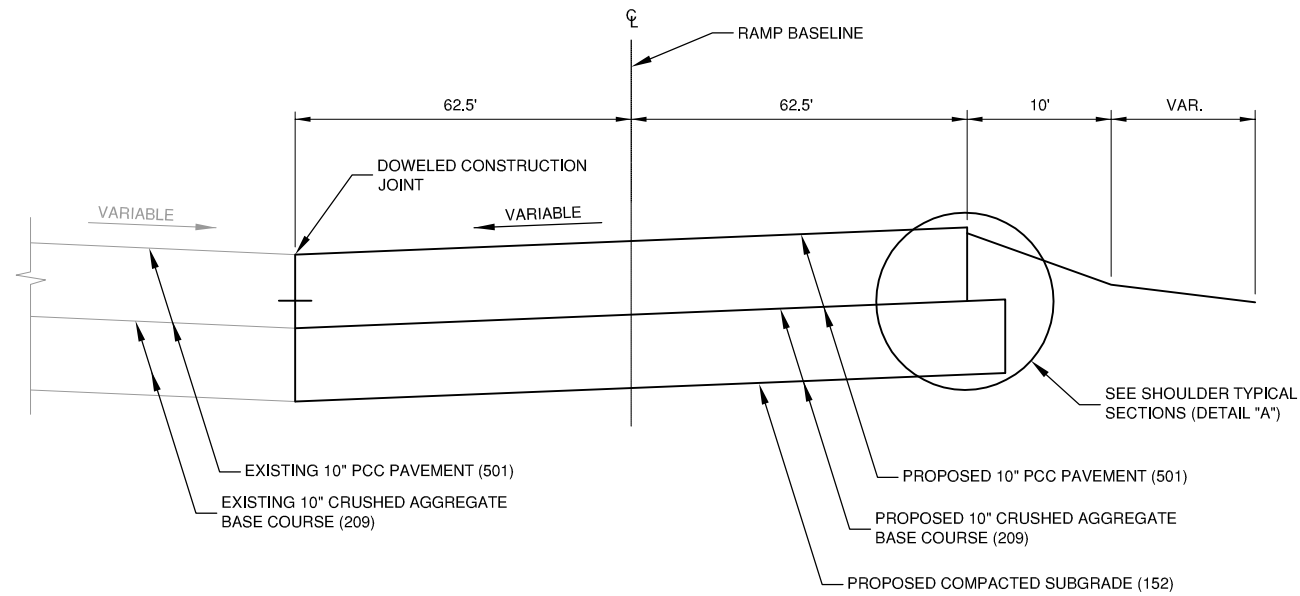
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**SOUTH GA RAMP EXPANSION - PHASE 1  
 TYPICAL SECTIONS**

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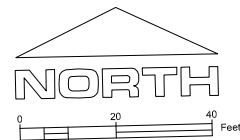


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**NOTES**

- SEE "PROPOSED IMPROVEMENTS" SHEETS FOR SECTION LOCATIONS.

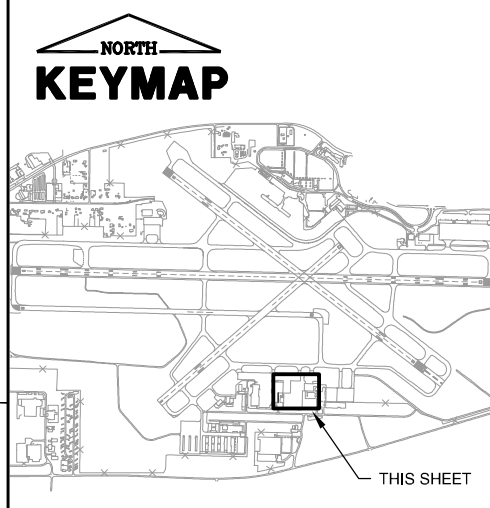
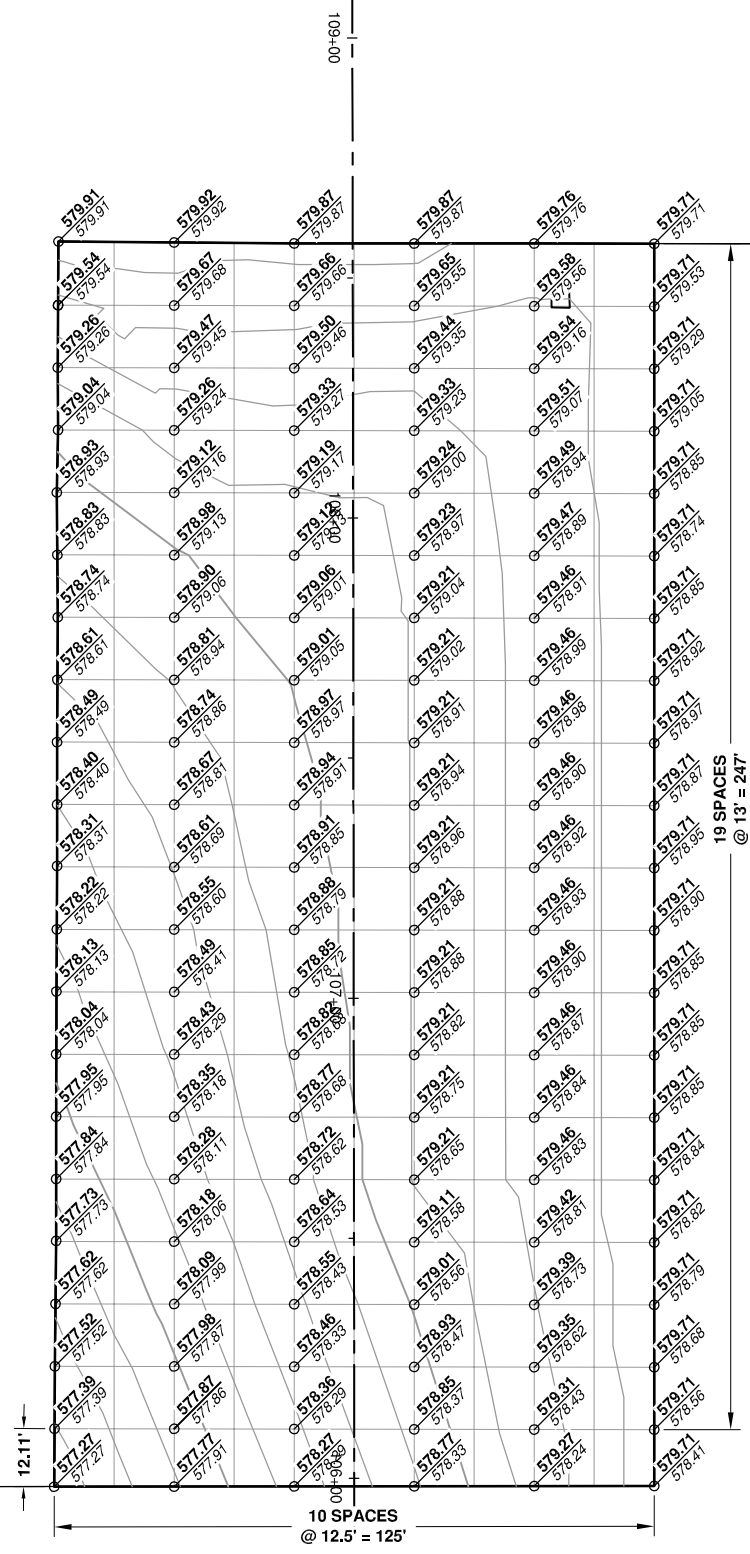


**LEGEND**

- NEW ELEVATION
- EXISTING ELEVATION
- NEW CONTOUR
- EXISTING CONTOUR

**NOTES**

1. PROPOSED CONTOURS CAN BE FOUND ON THE DRAINAGE PLAN.
2. SEE PROPOSED IMPROVEMENTS FOR GEOMETRY DATA.
3. SEE JOINTING PLAN FOR JOINT TYPES.



FILE: 10 STAKING PLAN.dwg  
 UPDATE BY: Andrew Bodine  
 PLOT DATE: 5/20/2013 9:54 AM

quad-plan  
 BASE PROP GEO  
 Keymap

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**SOUTH GA RAMP EXPANSION - PHASE 1  
 STAKING PLAN**

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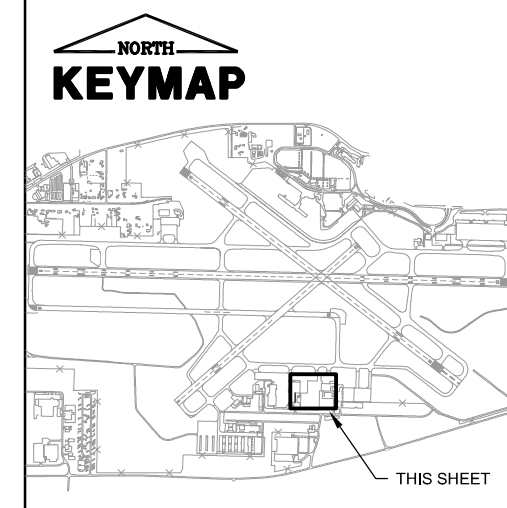
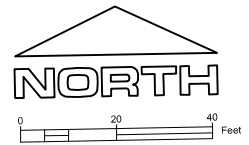
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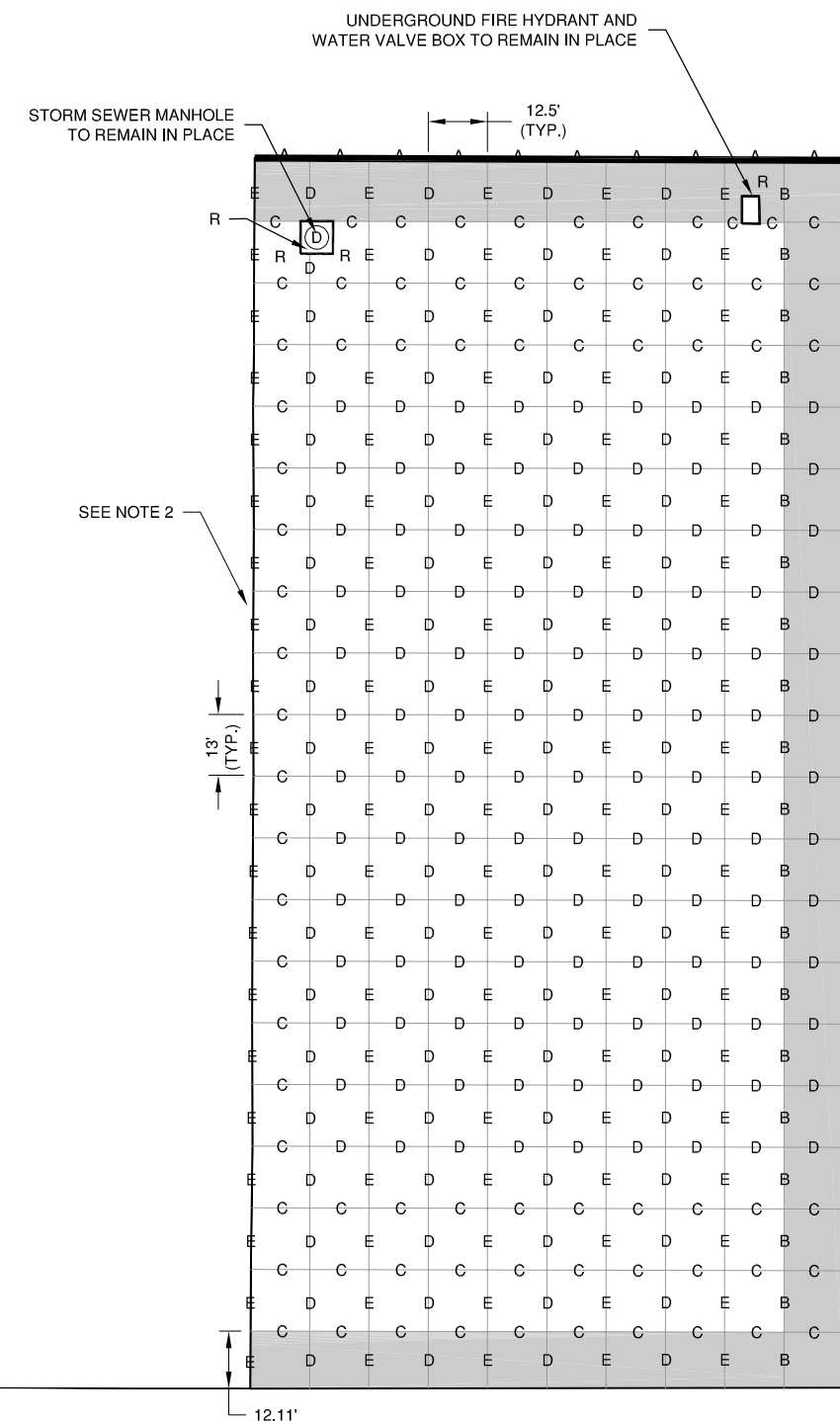
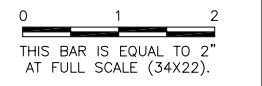




FILE: 11 JOINTING PLAN.dwg  
 UPDATE BY: Andrew Bodine  
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 BASE PROP GEO  
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**LEGEND**

	TYPE A ISOLATION JOINT (3/4")
	TYPE B HINGED (TIED) CONTRACTION JOINT
	TYPE C DOWELED CONTRACTION JOINT
	TYPE D DUMMY CONTRACTION JOINT
	TYPE E DOWELED CONSTRUCTION JOINT
	REINFORCED PANEL
	THICKENED EDGE
	PROPOSED INLET

- NOTE:**
- SEE STAKING PLAN FOR GRADES AROUND IN-PAVEMENT DRAINAGE STRUCTURES.
  - CONTRACTOR SHALL MATCH EXISTING JOINT SPACING ALONG THIS EDGE.

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SOUTH GA RAMP EXPANSION - PHASE 1  
 JOINTING PLAN

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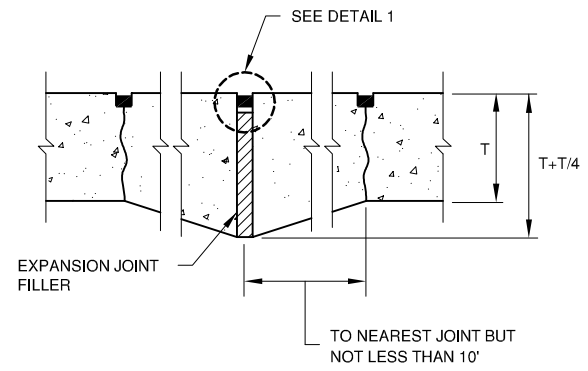


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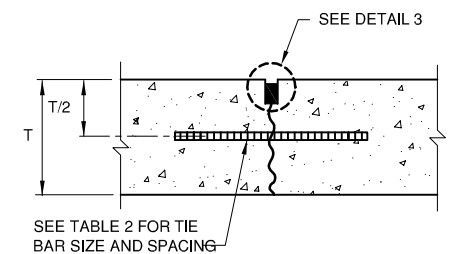
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SHEET 11 OF 22 SHEETS	

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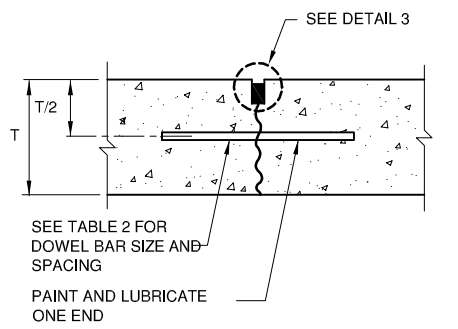
**TYPE A THICKENED ISOLATION**

SYMBOL — A — T



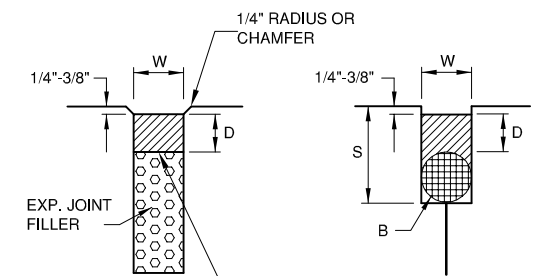
**TYPE B HINGED (TIED) CONTRACTION**

SYMBOL — B —



**TYPE C DOWELED CONTRACTION**

SYMBOL — C —

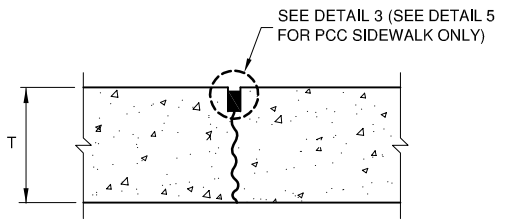
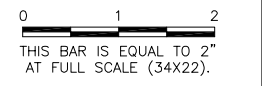


**DETAIL 1**

**DETAIL 2**

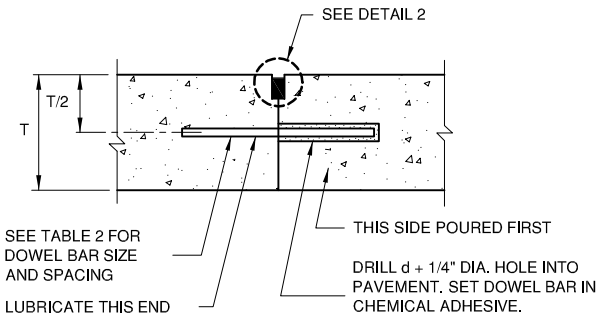
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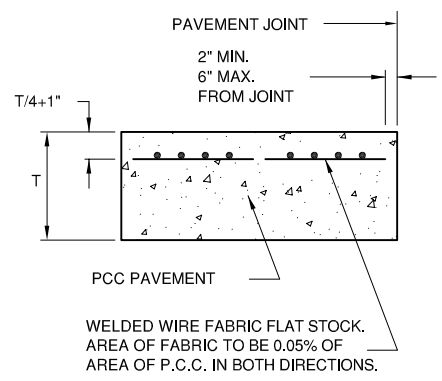
**TYPE D DUMMY CONTRACTION**

SYMBOL — D —



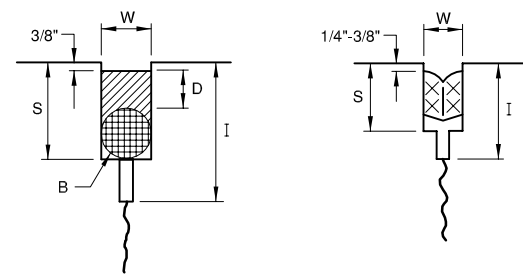
**TYPE E DOWELED CONSTRUCTION**

SYMBOL — E —



**ODD SHAPED PANEL REINFORCEMENT**

SYMBOL R



**DETAIL 3**

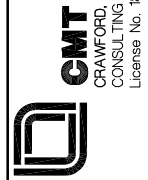
**DETAIL 4**

**JOINT SEALING DETAILS**

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 MOLINE, ILLINOIS**

**SOUTH GA RAMP EXPANSION - PHASE 1  
 JOINTING DETAILS**

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PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES $I = (T/3) \pm 1/4"$
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"

	DETAIL 1	DETAIL 2	DETAIL 3	DETAIL 4	DETAIL 5
	HOT/ COLD POUR	HOT/ COLD POUR	SILICONE	PRE FORMED	HOT/ COLD POUR
W=WIDTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/2	3/8" COMPRESSED	3/16"
D=DEPTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/4	N/A	I
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8	N/A	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-3/8	1-3/8	1-1/2	N/A

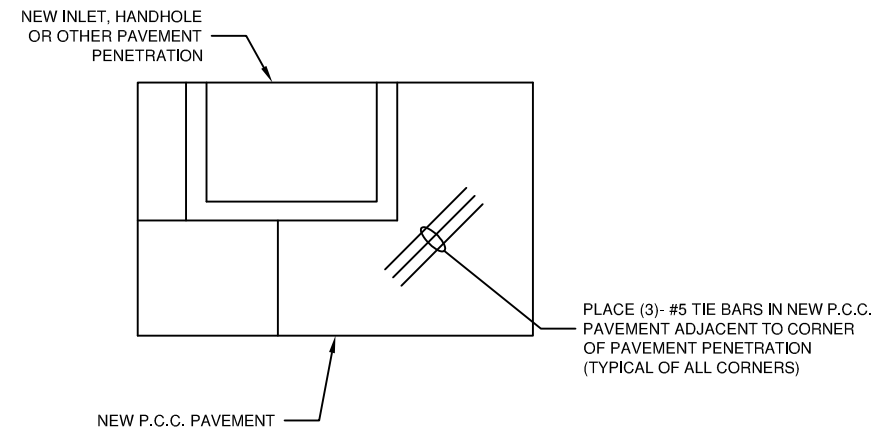
**JOINT NOTES**

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.

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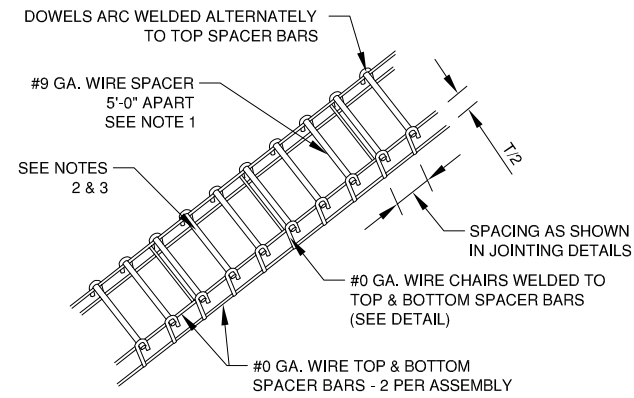
0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).



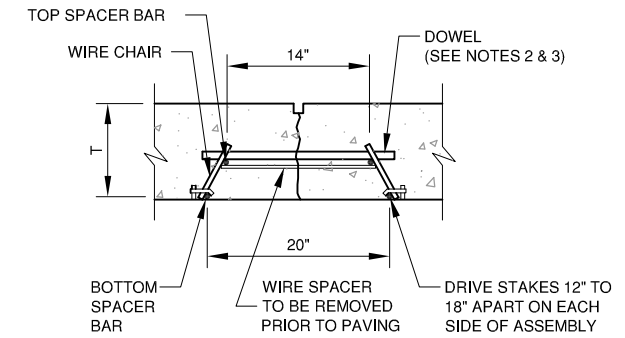
**PAVEMENT PENETRATION DETAIL**

N.T.S.

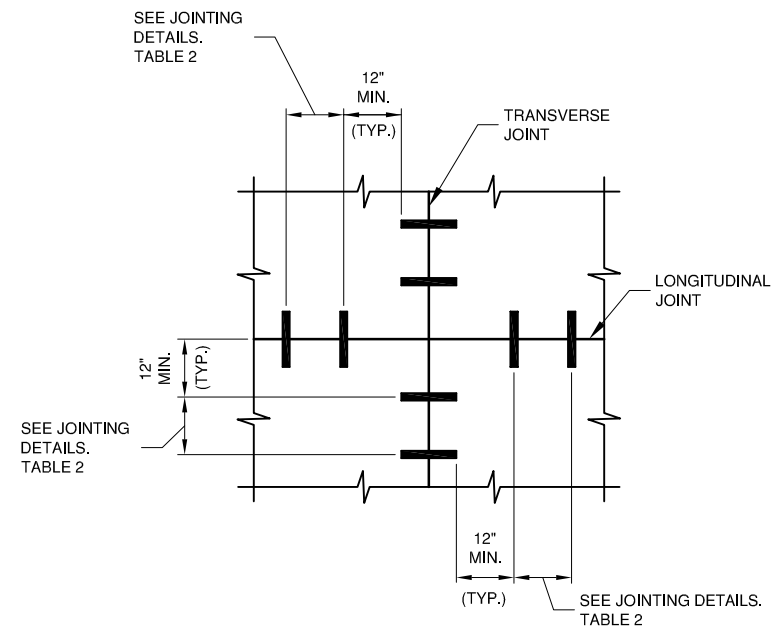
PLACE (3)- #5 TIE BARS IN NEW P.C.C. PAVEMENT ADJACENT TO CORNER OF PAVEMENT PENETRATION (TYPICAL OF ALL CORNERS)



**DOWEL BASKET ASSEMBLY DETAIL**

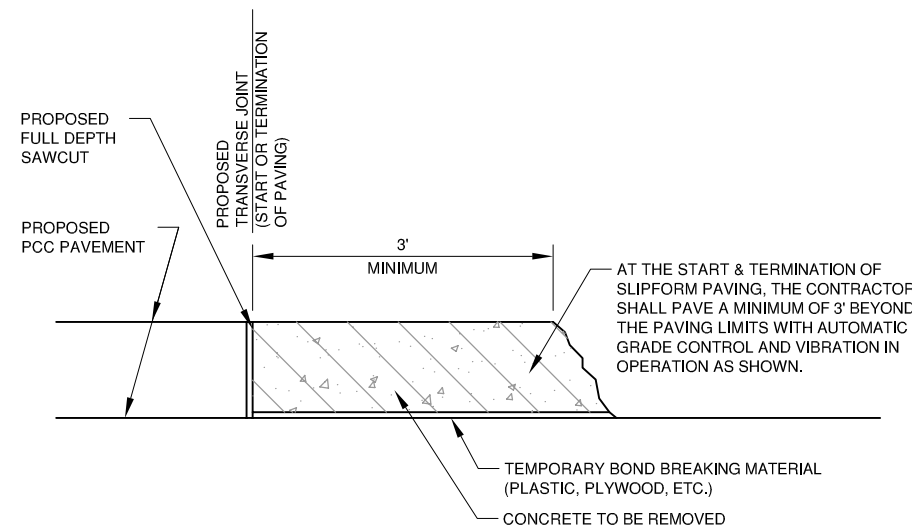


**DOWEL BAR INSTALLATION DETAIL**



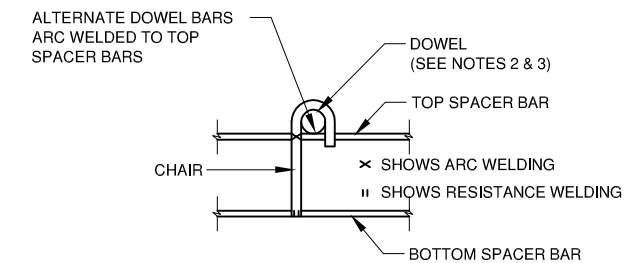
**DOWEL PLACEMENT DETAIL**

N.T.S.



**REQUIREMENTS AT START & TERMINATION OF SLIPFORM PAVING**

N.T.S.



**TYPICAL DOWEL BASKET ELEVATION DETAIL SHOWING CHAIR**

**DOWEL BASKET DETAILS**

N.T.S.

**DOWEL BASKET NOTES**

- #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
- DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 2.
- DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.

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SOUTH GA RAMP EXPANSION - PHASE 1  
 PAVING AND MISCELLANEOUS DETAILS

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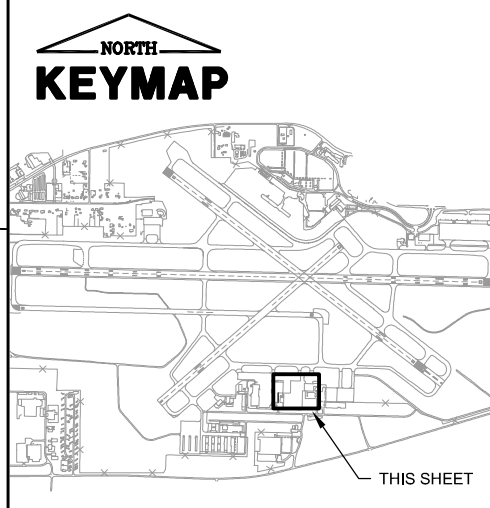
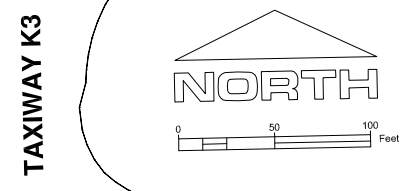


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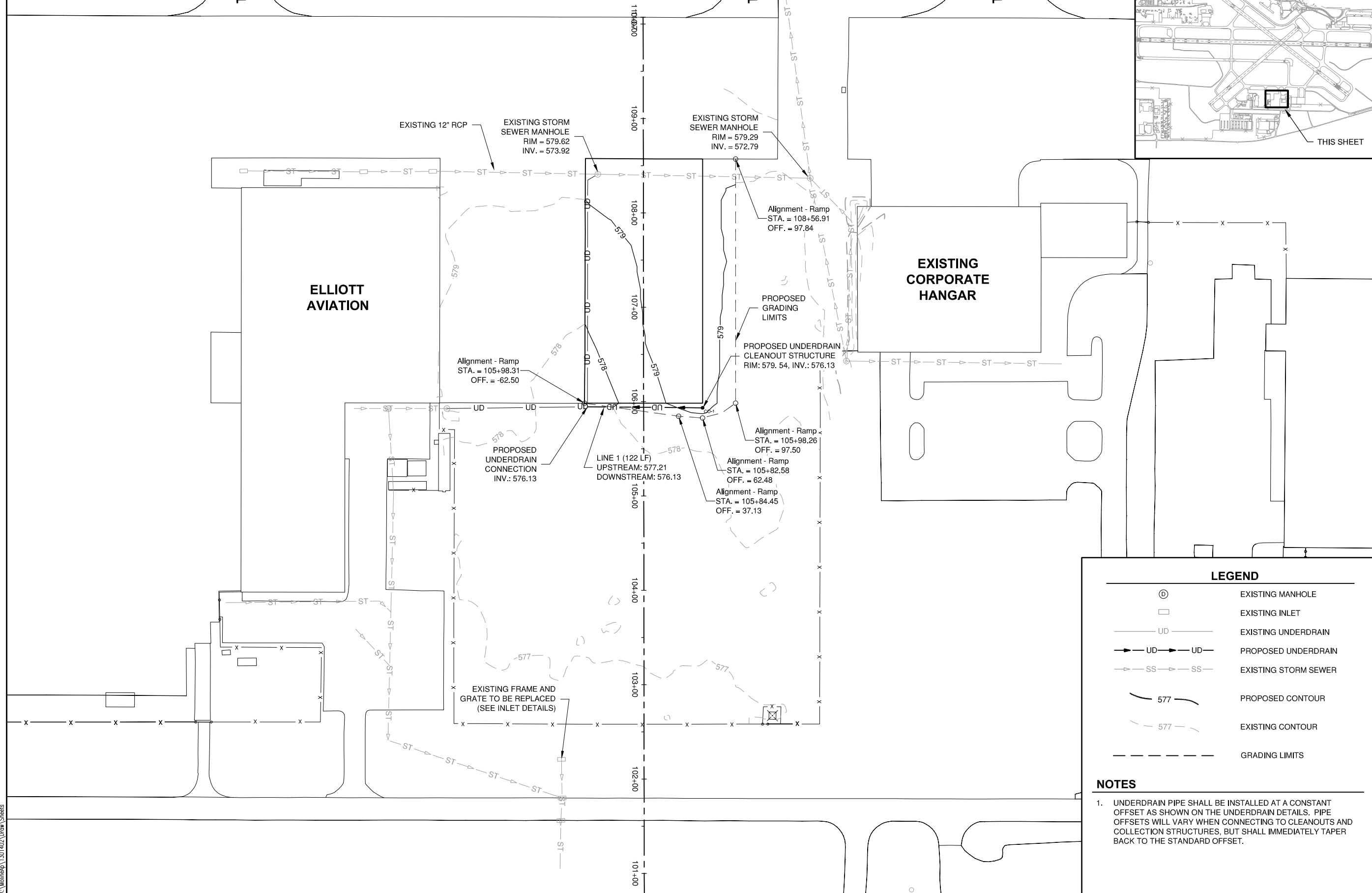
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 UPDATE BY: Andrew Bodine  
 PLOT DATE: 5/20/2013 9:55 AM

quad-plan  
 BASE PROP GEO  
 Keymap

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**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**

**SOUTH GA RAMP EXPANSION - PHASE 1  
 GRADING & DRAINAGE PLAN**

**LEGEND**

	EXISTING MANHOLE
	EXISTING INLET
	EXISTING UNDERDRAIN
	PROPOSED UNDERDRAIN
	EXISTING STORM SEWER
	PROPOSED CONTOUR
	EXISTING CONTOUR
	GRADING LIMITS

**NOTES**

1. UNDERDRAIN PIPE SHALL BE INSTALLED AT A CONSTANT OFFSET AS SHOWN ON THE UNDERDRAIN DETAILS. PIPE OFFSETS WILL VARY WHEN CONNECTING TO CLEANOUTS AND COLLECTION STRUCTURES, BUT SHALL IMMEDIATELY TAPER BACK TO THE STANDARD OFFSET.

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**QUAD CITY INTERNATIONAL AIRPORT**

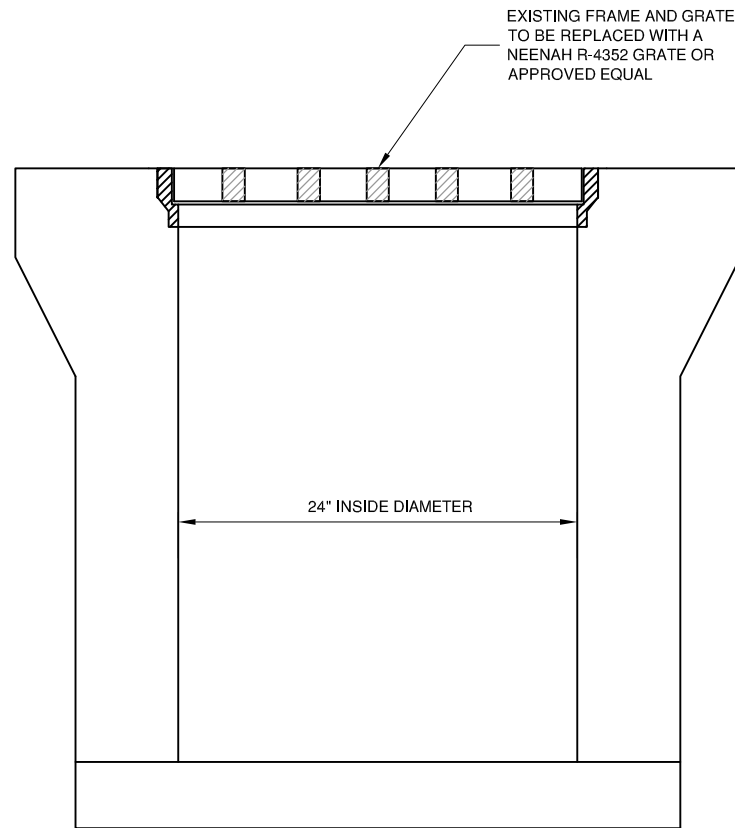
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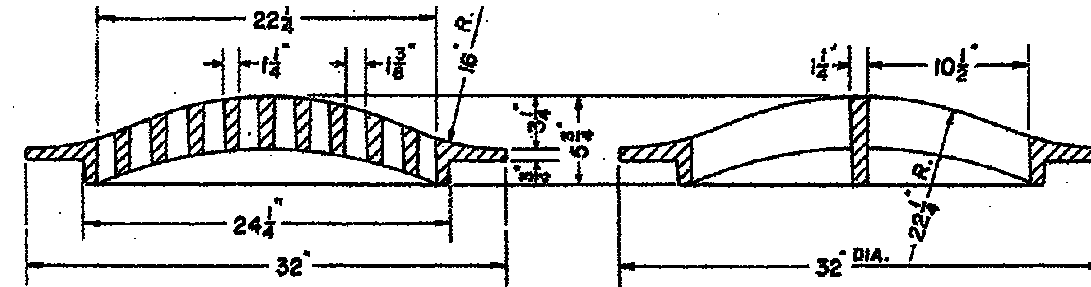
0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).



**REPLACE INLET FRAME AND GRATE**  
 N.T.S.

**NOTE:**

- ONCE REMOVED, THE EXISTING INLET FRAME AND GRATE SHALL BE TURNED OVER TO THE AIRPORT.
- THE NEW GRATE SHALL BE A NEENAH R-4352 BEEHIVE GRATE OR APPROVED EQUAL.



**PROPOSED FRAME AND GRATE DETAIL**  
 N.T.S.

**NOTE:**

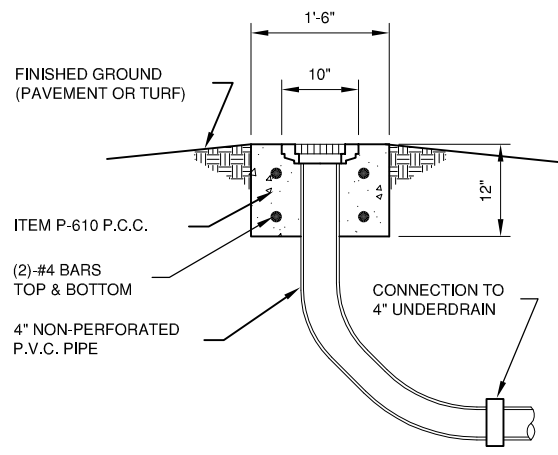
- THE NEW GRATE SHALL BE A NEENAH R-4352 BEEHIVE GRATE OR APPROVED EQUAL.
- THE PROPOSED GRATE SHALL BE SET DIRECTLY ON THE INLET

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SOUTH GA RAMP EXPANSION - PHASE 1  
 INLET DETAILS

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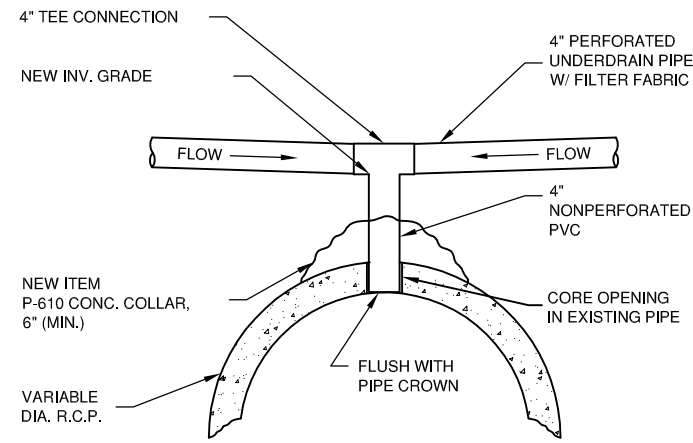
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**SIDE VIEW**

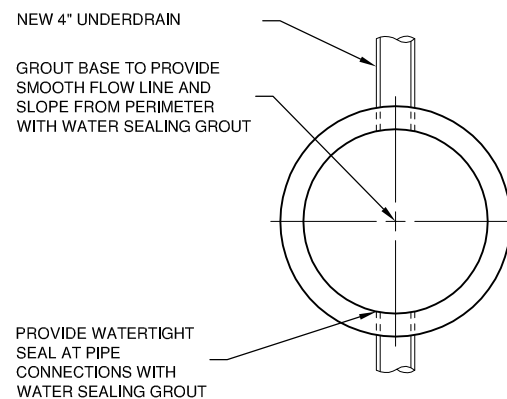
**UNDERDRAIN CLEAN-OUT DETAIL (TYPE 1)**

N.T.S.



**UNDERDRAIN DIRECT TOP CONNECTION DETAIL**

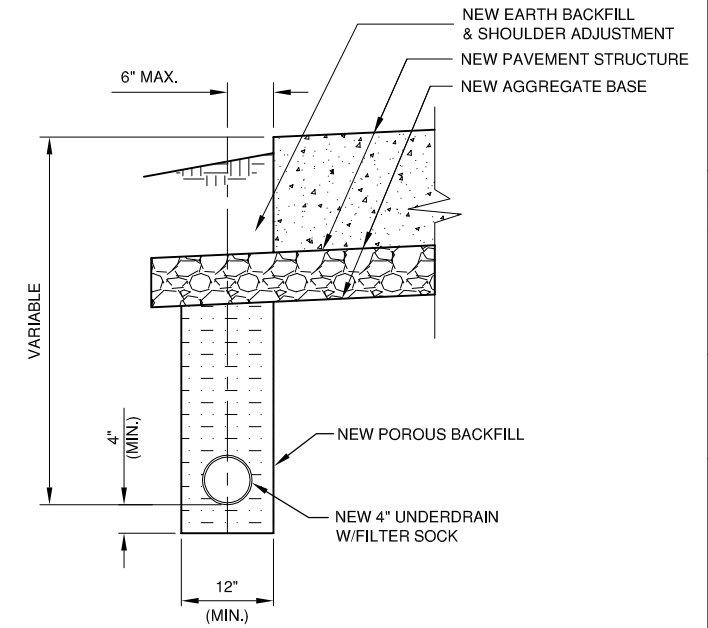
N.T.S.



**PLAN**

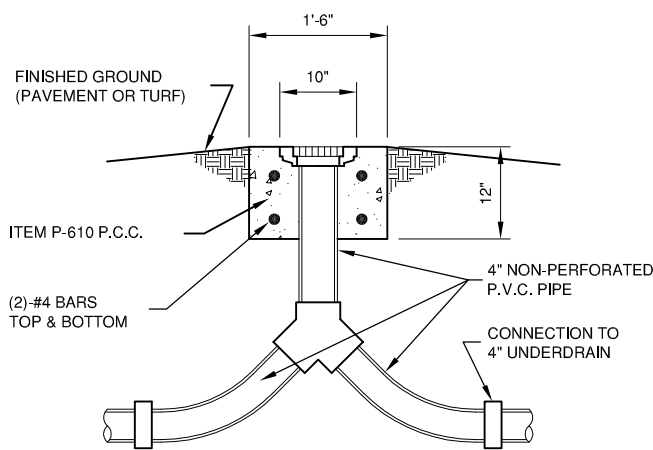
**UNDERDRAIN CLEAN-OUT DETAIL**

N.T.S.



**TYPICAL UNDERDRAIN DETAIL PAVEMENT EDGE**

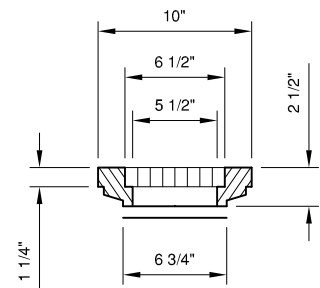
N.T.S.



**SIDE VIEW**

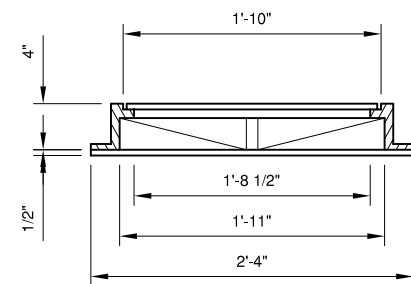
**UNDERDRAIN CLEAN-OUT DETAIL (TYPE 2)**

N.T.S.



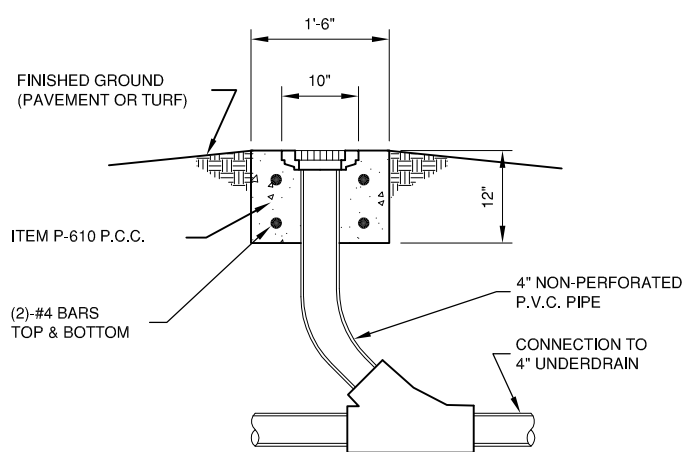
**FRAME & COVER**

CAST IRON FRAME & COVER (SIMILAR TO NEENAH R-6013 OR APPROVED EQUAL)



**FRAME & LID**

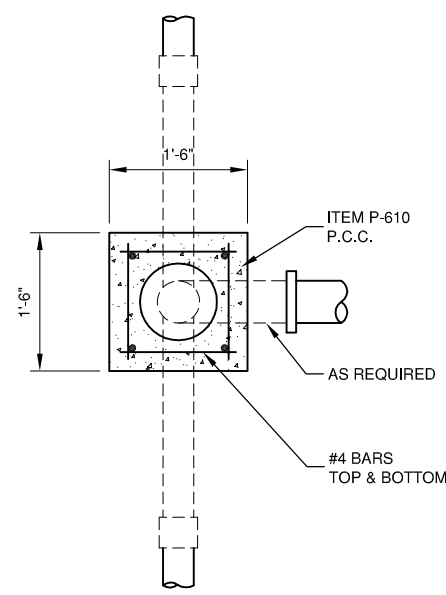
CAST IRON FRAME & LID (SIMILAR TO NEENAH R-1690-A HEAVY DUTY OR APPROVED EQUAL)



**SIDE VIEW**

**UNDERDRAIN CLEAN-OUT DETAIL (TYPE 3)**

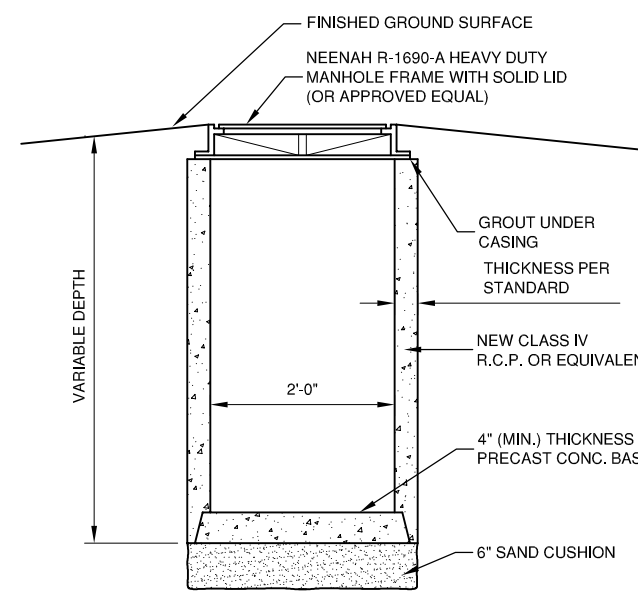
N.T.S.



**PLAN**

**UNDERDRAIN CLEAN-OUT DETAIL**

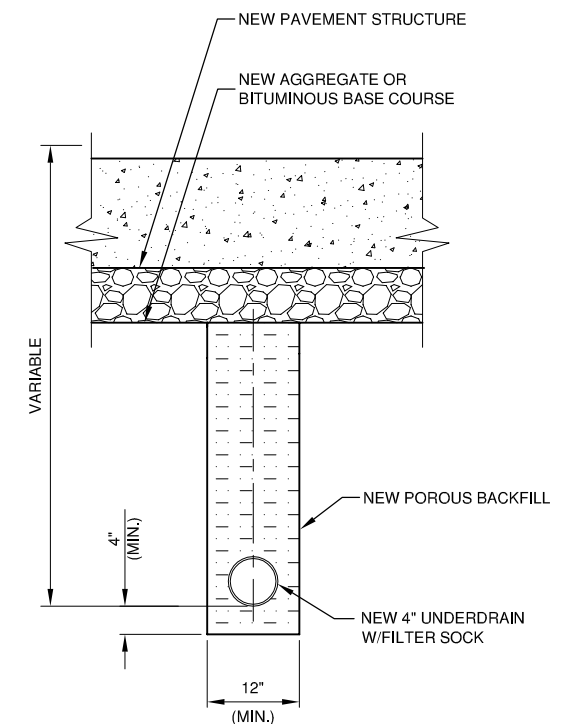
N.T.S.



**SIDE VIEW**

**UNDERDRAIN COLLECTION STRUCTURE DETAIL**

N.T.S.



**TYPICAL UNDERDRAIN DETAIL BELOW PAVEMENT**

N.T.S.

**UNDERDRAIN NOTE**

DURING UNDERDRAIN CONSTRUCTION, CARE SHALL BE TAKEN TO ENSURE GOOD DRAINAGE BETWEEN THE SUB-BASE AND THE POROUS BACKFILL MATERIAL WHICH WILL AFFECT DRAINAGE TO THE UNDERDRAIN SHALL BE REMOVED PRIOR TO THE PLACEMENT OF PROPOSED POROUS BACKFILL. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL.

**QU017**

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0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

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SOUTH GA RAMP EXPANSION - PHASE 1  
 UNDERDRAIN DETAILS

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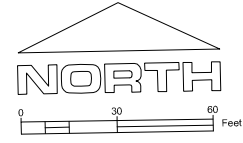
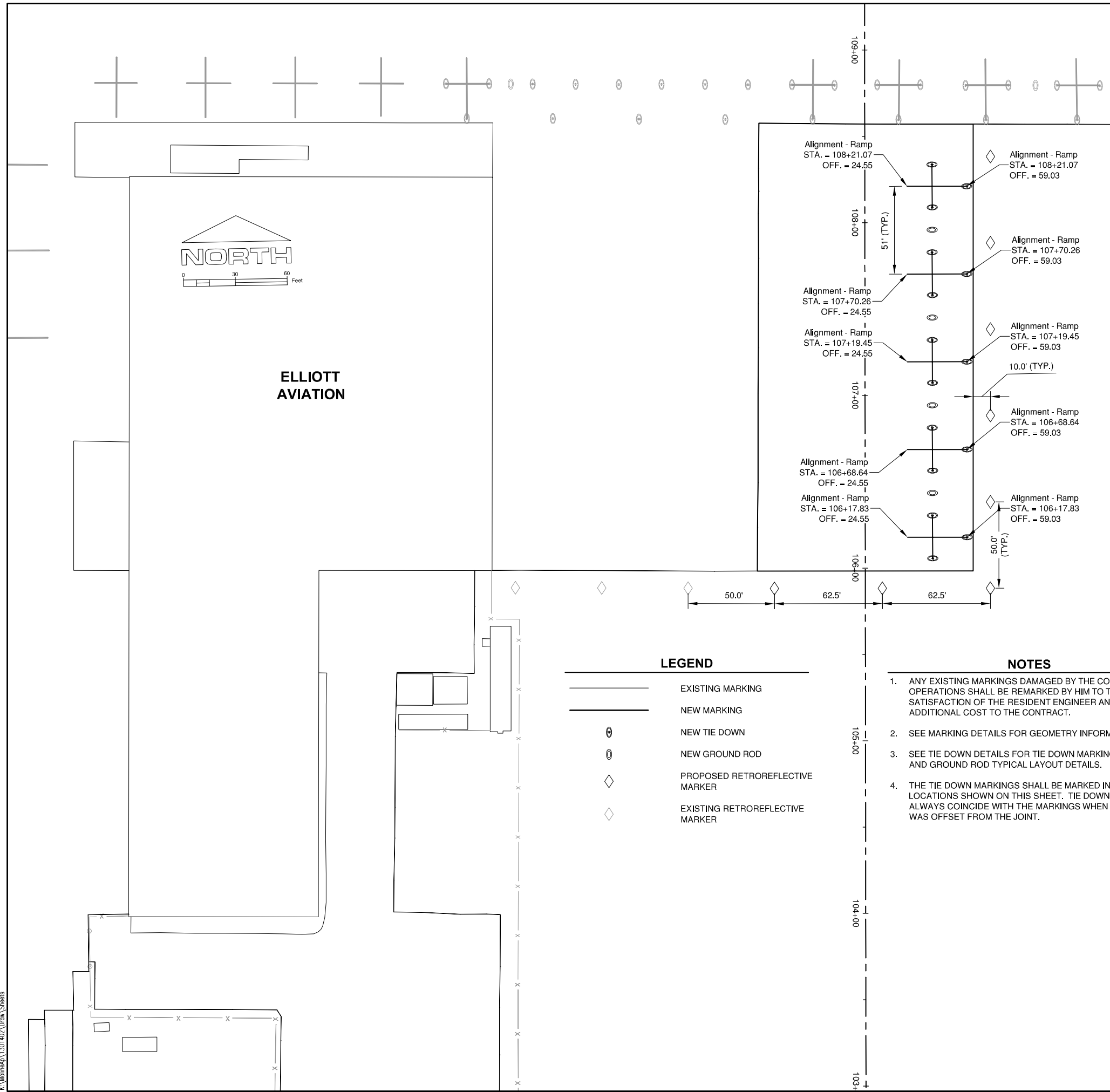
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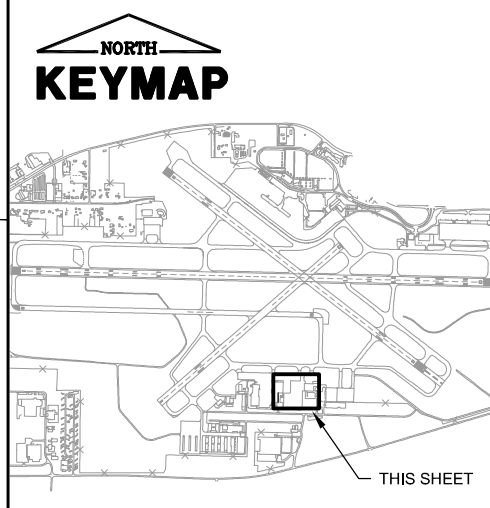
SHEET 16 OF 22 SHEETS



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**ELLIOTT AVIATION**



**NORTH**  
**KEYMAP**

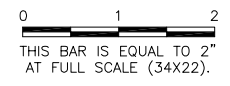
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UPDATE BY: Andrew Bodine  
PLOT DATE: 5/20/2013 9:56 AM

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**EXISTING CORPORATE HANGAR**

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MOLINE, ILLINOIS**

**SOUTH GA RAMP EXPANSION - PHASE 1  
MARKING PLAN**

**LEGEND**

- EXISTING MARKING
- NEW MARKING
- NEW TIE DOWN
- NEW GROUND ROD
- PROPOSED RETROREFLECTIVE MARKER
- EXISTING RETROREFLECTIVE MARKER

**NOTES**

1. ANY EXISTING MARKINGS DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REMARKED BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AT NO ADDITIONAL COST TO THE CONTRACT.
2. SEE MARKING DETAILS FOR GEOMETRY INFORMATION.
3. SEE TIE DOWN DETAILS FOR TIE DOWN MARKING, TIE DOWN, AND GROUND ROD TYPICAL LAYOUT DETAILS.
4. THE TIE DOWN MARKINGS SHALL BE MARKED IN THE LOCATIONS SHOWN ON THIS SHEET. TIE DOWNS MAY NOT ALWAYS COINCIDE WITH THE MARKINGS WHEN A TIE DOWN WAS OFFSET FROM THE JOINT.

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SHEET 17 OF 22 SHEETS	

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0 1 2  
 THIS BAR IS EQUAL TO 2"  
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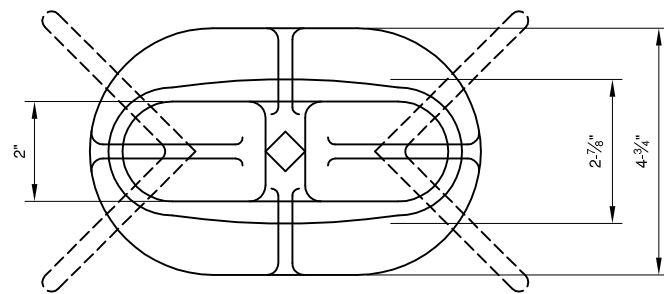
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**SOUTH GA RAMP EXPANSION - PHASE 1  
 MARKING & TIE DOWN DETAILS**

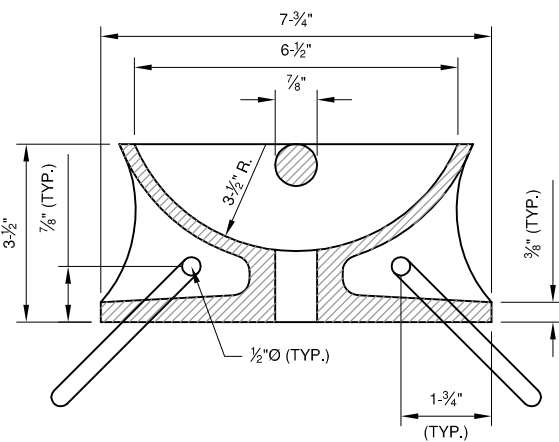
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SHEET 18 OF 22 SHEETS	



**PLAN**



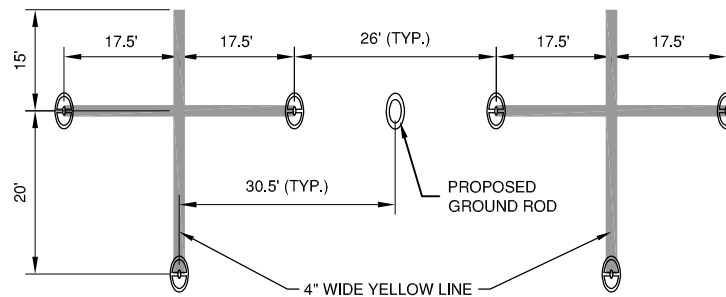
**SECTION**

**MOORING EYE/TIE DOWN DETAIL**

N.T.S.

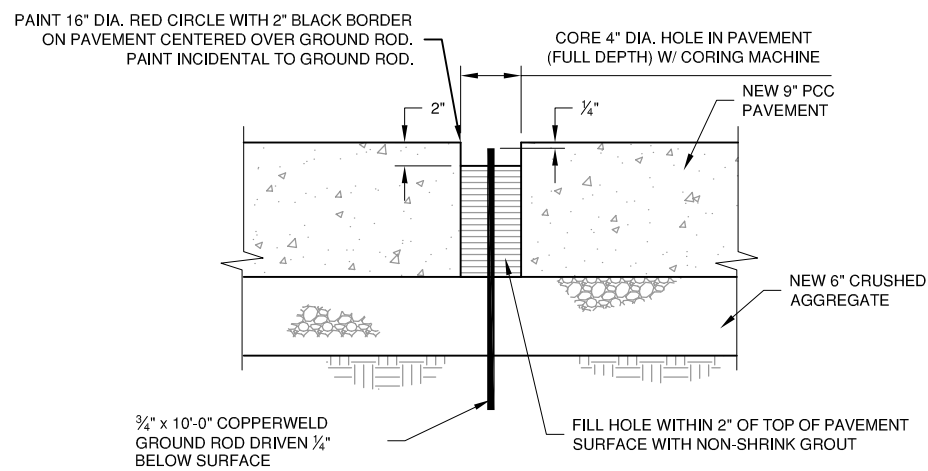
**NOTES:**

- MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. R3490 OR APPROVED EQUIVALENT.
- ANCHOR RODS SHALL BE #3 DEFORMED REBAR, 15" LONG AND SHALL BE BENT DOWNWARD AT 45°. TWO ANCHOR BARS PER MOORING EYE ARE REQUIRED.
- MOORING EYES SHALL BE INSTALLED DURING THE PAVING OPERATION AND SHALL NOT BE CORED AFTER PAVING.
- THE PLACEMENT OF THE TIE DOWNS SHALL BE AN INTEGRAL PART OF THE PAVING PROCESS. THE CONTRACTOR SHALL IDENTIFY ALL TIE DOWNS AND GROUND RODS THAT FALL WITHIN 2' OF A JOINT PRIOR TO BEGINNING PAVING OPERATIONS.



**TIE DOWN DETAIL**

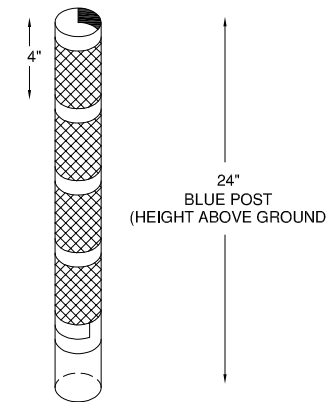
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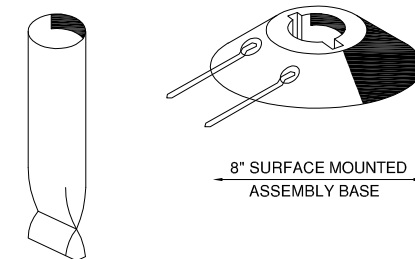
**GROUNDING POINT DETAIL**

N.T.S.

HIGH INTENSITY  
 BLUE REFLECTOR  
 OR WRAPS



18" SOIL  
 ANCHOR



**ELEVATED RETROREFLECTIVE**

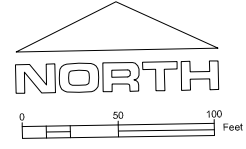
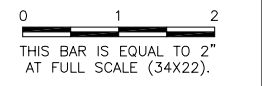
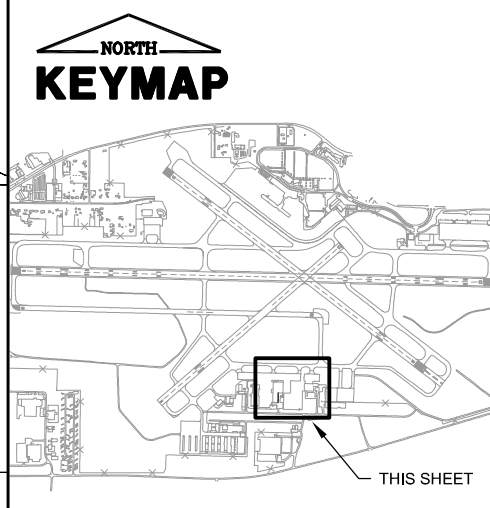
**MARKER DETAIL**

N.T.S.

NOTE: RETROREFLECTIVE MARKER SHALL BE SAFE-HIT OR APPROVED EQUAL.

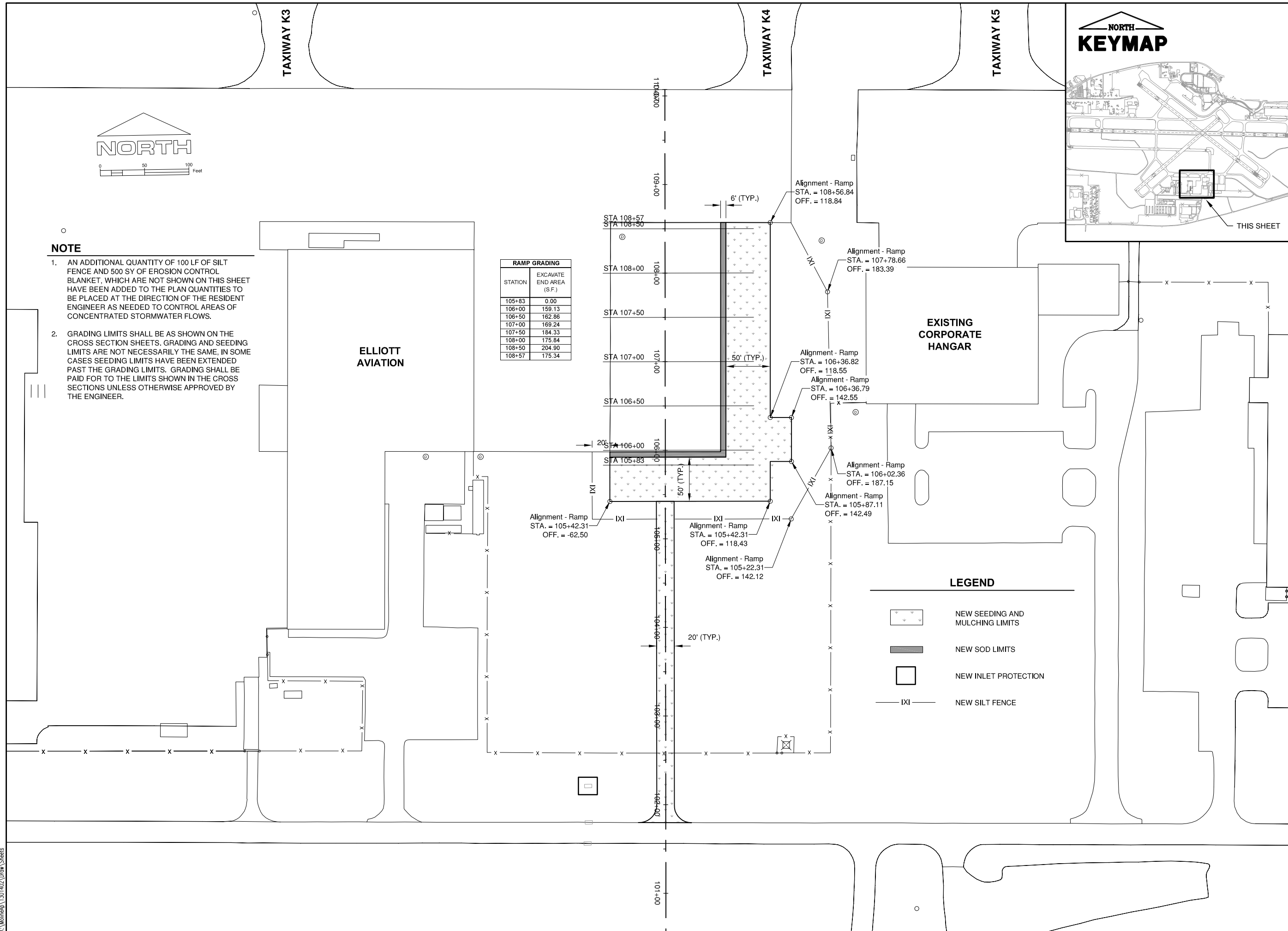
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NUMBER	BY	DATE



- NOTE**
1. AN ADDITIONAL QUANTITY OF 100 LF OF SILT FENCE AND 500 SY OF EROSION CONTROL BLANKET, WHICH ARE NOT SHOWN ON THIS SHEET HAVE BEEN ADDED TO THE PLAN QUANTITIES TO BE PLACED AT THE DIRECTION OF THE RESIDENT ENGINEER AS NEEDED TO CONTROL AREAS OF CONCENTRATED STORMWATER FLOWS.
  2. GRADING LIMITS SHALL BE AS SHOWN ON THE CROSS SECTION SHEETS. GRADING AND SEEDING LIMITS ARE NOT NECESSARILY THE SAME. IN SOME CASES SEEDING LIMITS HAVE BEEN EXTENDED PAST THE GRADING LIMITS. GRADING SHALL BE PAID FOR TO THE LIMITS SHOWN IN THE CROSS SECTIONS UNLESS OTHERWISE APPROVED BY THE ENGINEER.

RAMP GRADING	
STATION	EXCAVATE END AREA (S.F.)
105+83	0.00
106+00	159.13
106+50	162.86
107+00	169.24
107+50	184.33
108+00	175.84
108+50	204.90
108+57	175.34



**LEGEND**

- NEW SEEDING AND MULCHING LIMITS
- NEW SOD LIMITS
- NEW INLET PROTECTION
- NEW SILT FENCE

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**

**SOUTH GA RAMP EXPANSION - PHASE 1  
 TURFING PLAN & INDEX TO  
 CROSS SECTIONS**

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JOB No:	13014-02-00
IL. PROJ. NO. MLI-4275	
SHEET 19 OF 22 SHEETS	

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0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**

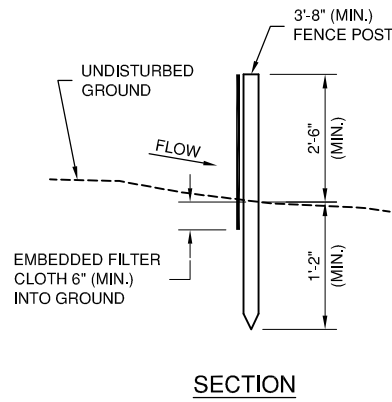
**SOUTH GA RAMP EXPANSION - PHASE 1  
 EROSION CONTROL DETAILS**

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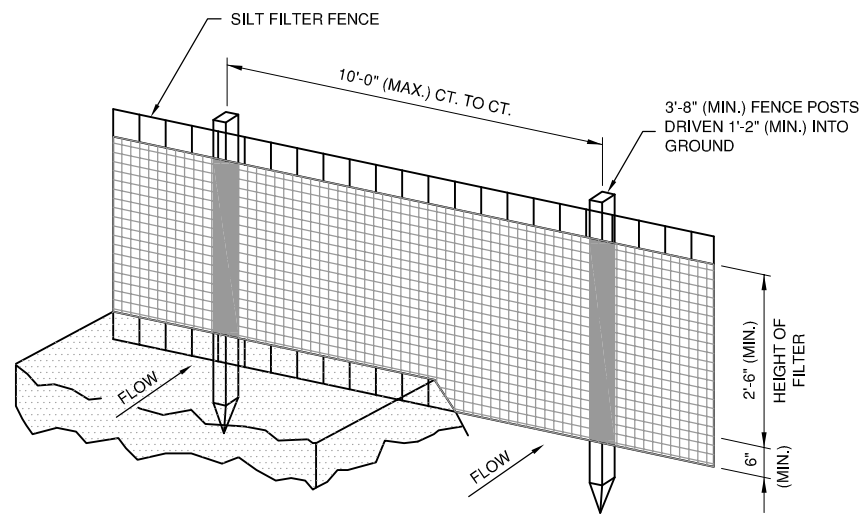


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**SECTION**

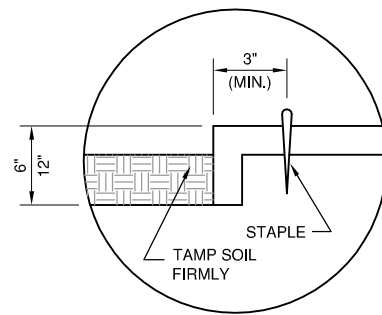


**PERSPECTIVE VIEW**

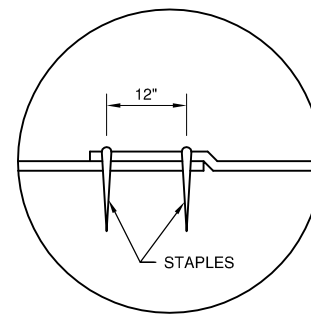
**EROSION CONTROL FABRIC FENCE DETAILS**  
 N.T.S.

**EROSION CONTROL FABRIC FENCE NOTES**

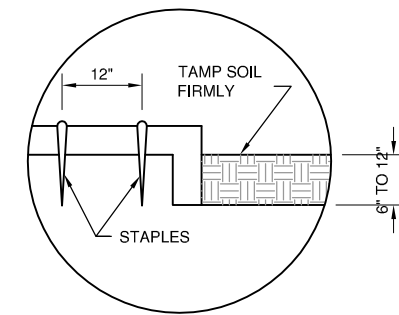
1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.



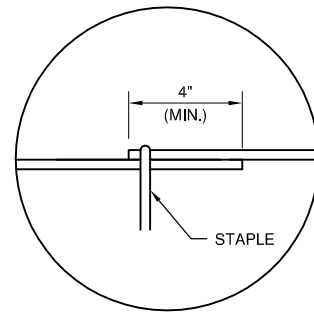
**DETAIL 1 - TERMINAL FOLD**



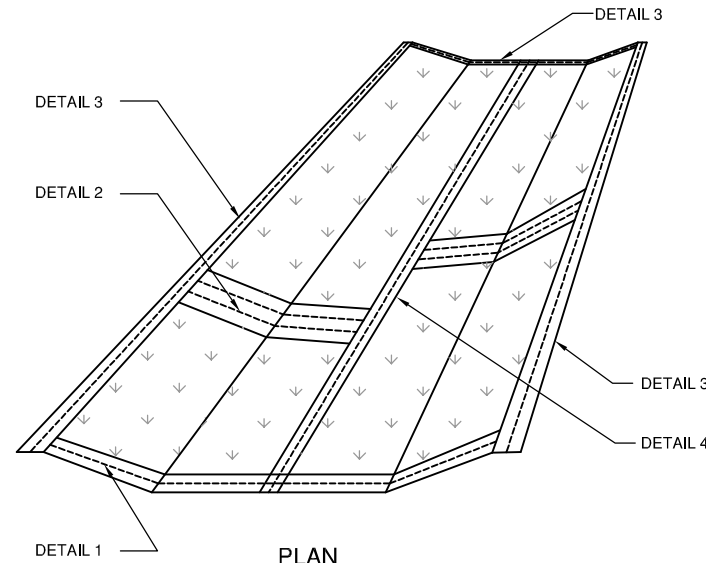
**DETAIL 2 - JUNCTION SLOT**



**DETAIL 3 - ANCHOR SLOT**



**DETAIL 4 - LAP JOINT**

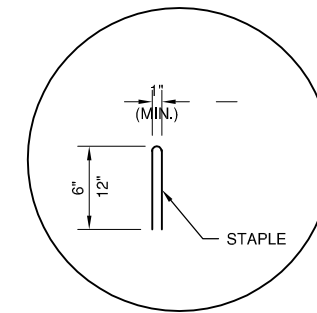


**PLAN**

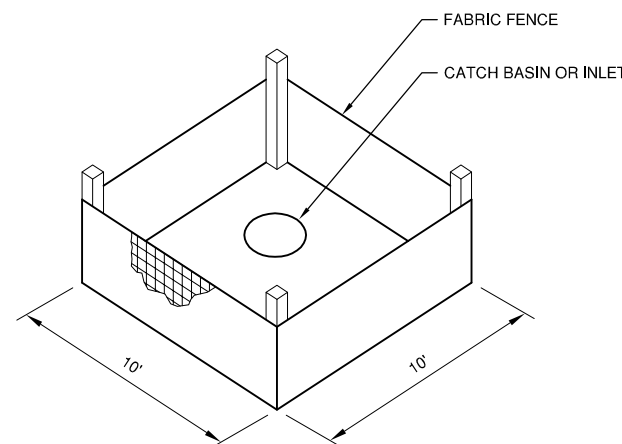
**EXCELSIOR BLANKET DETAILS**  
 N.T.S.

**EXCELSIOR BLANKET NOTES**

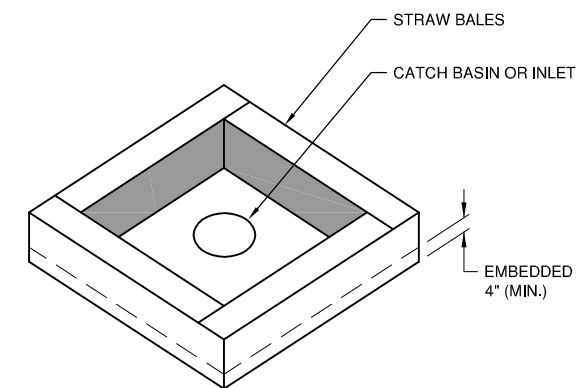
1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINAL ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.



**DETAIL 5 - STAPLE DETAIL**



**INLET PROTECTION WITH FABRIC**  
 N.T.S.

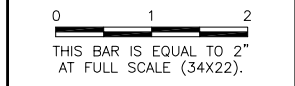


**INLET PROTECTION WITH STRAW BALES**  
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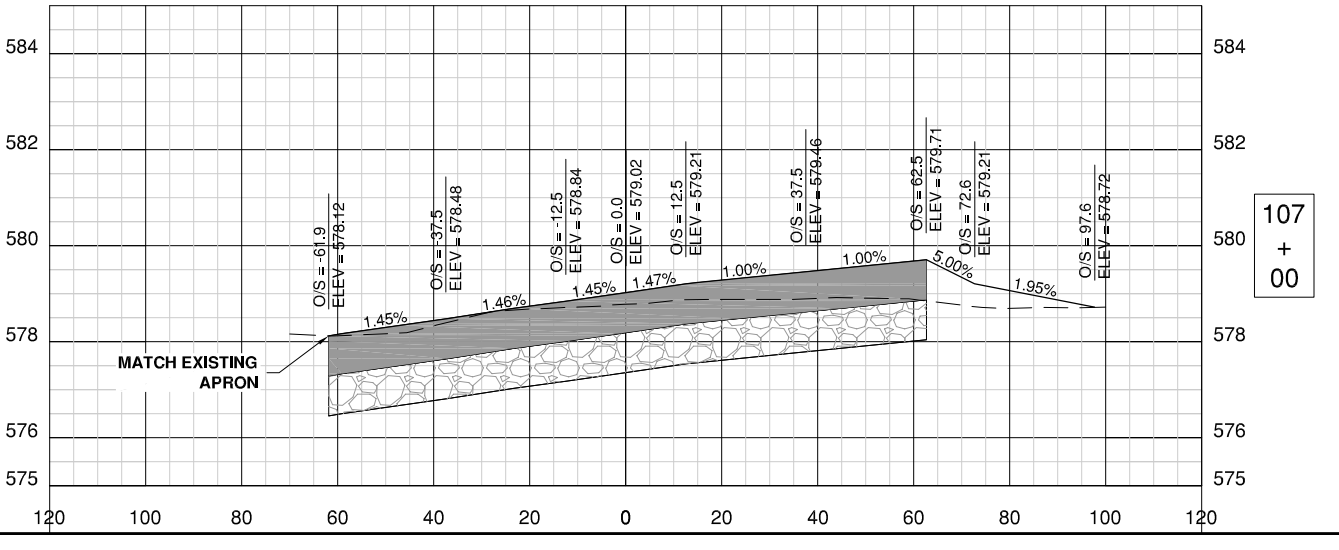
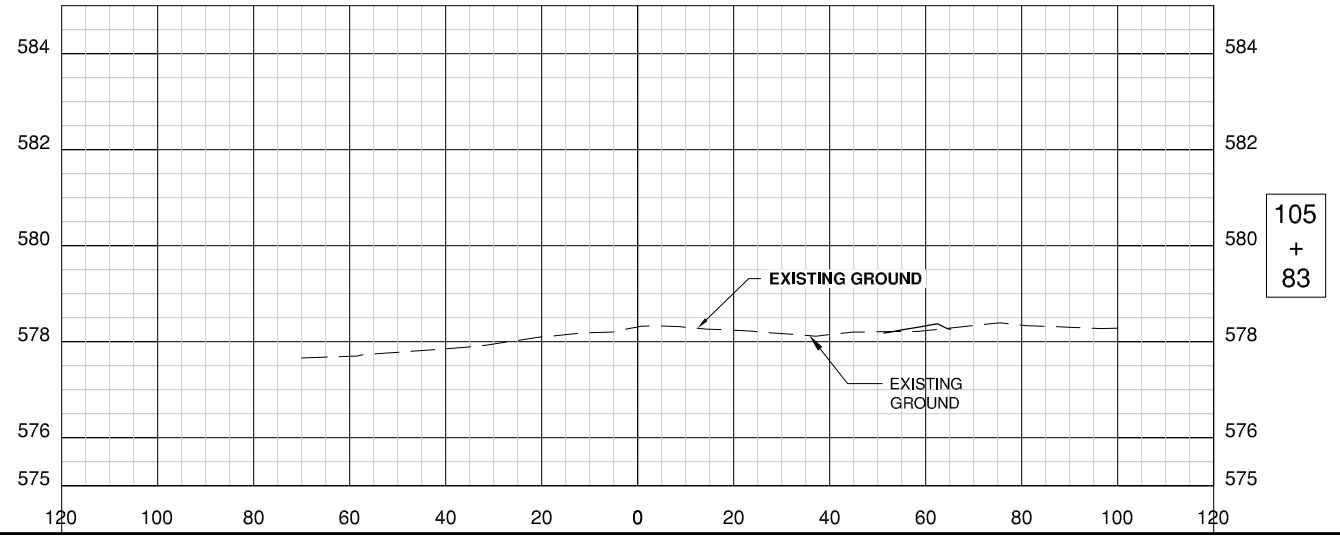
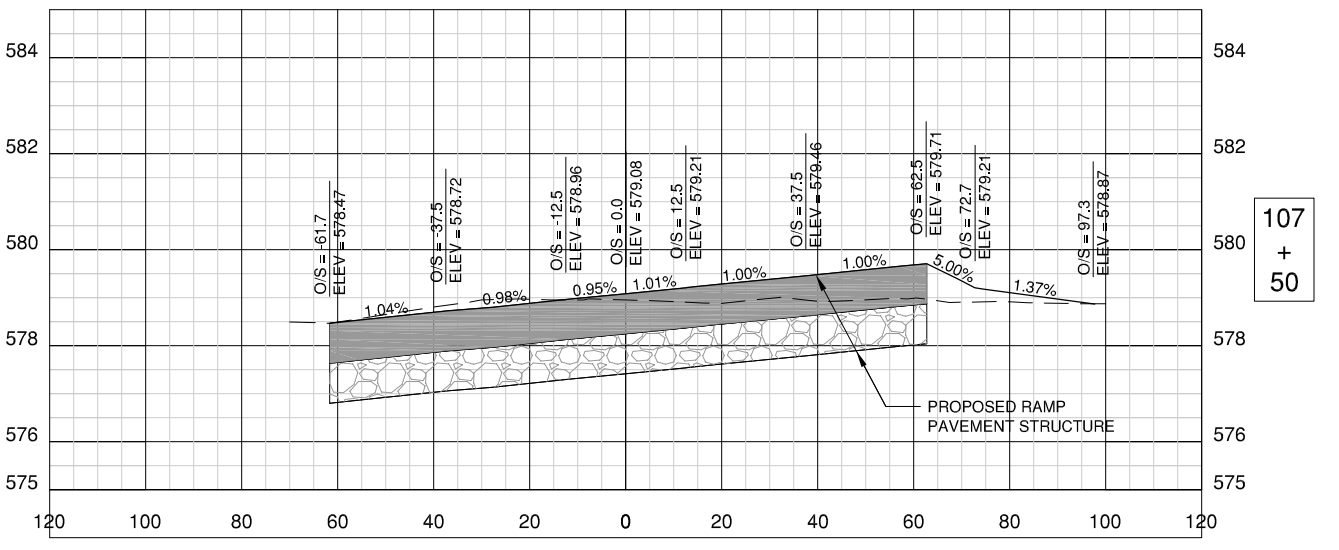
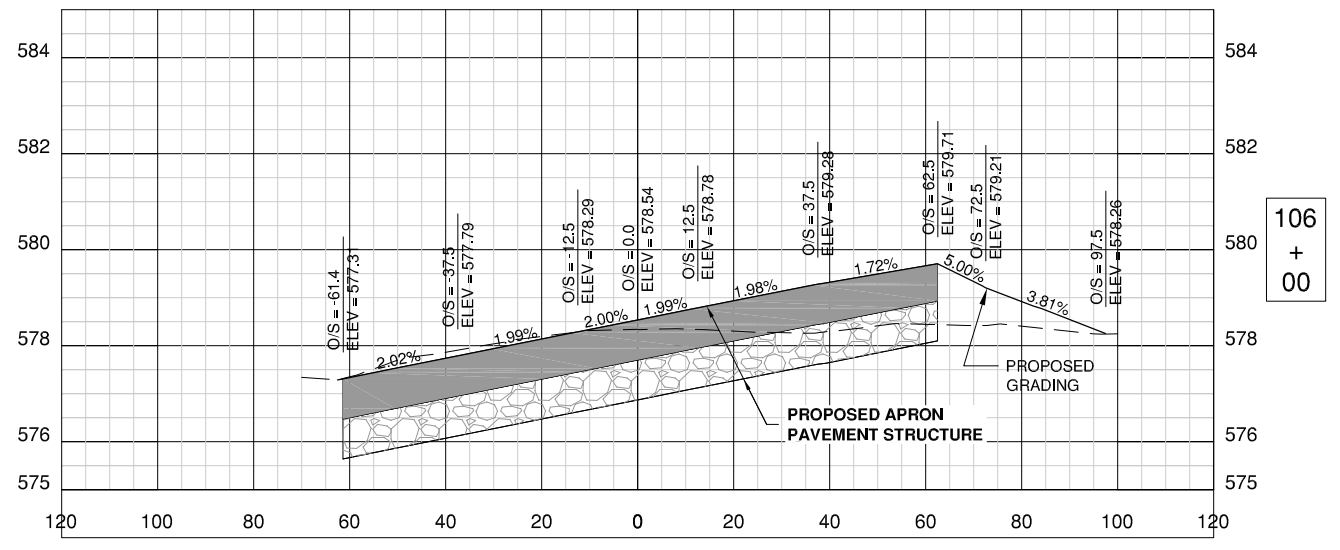
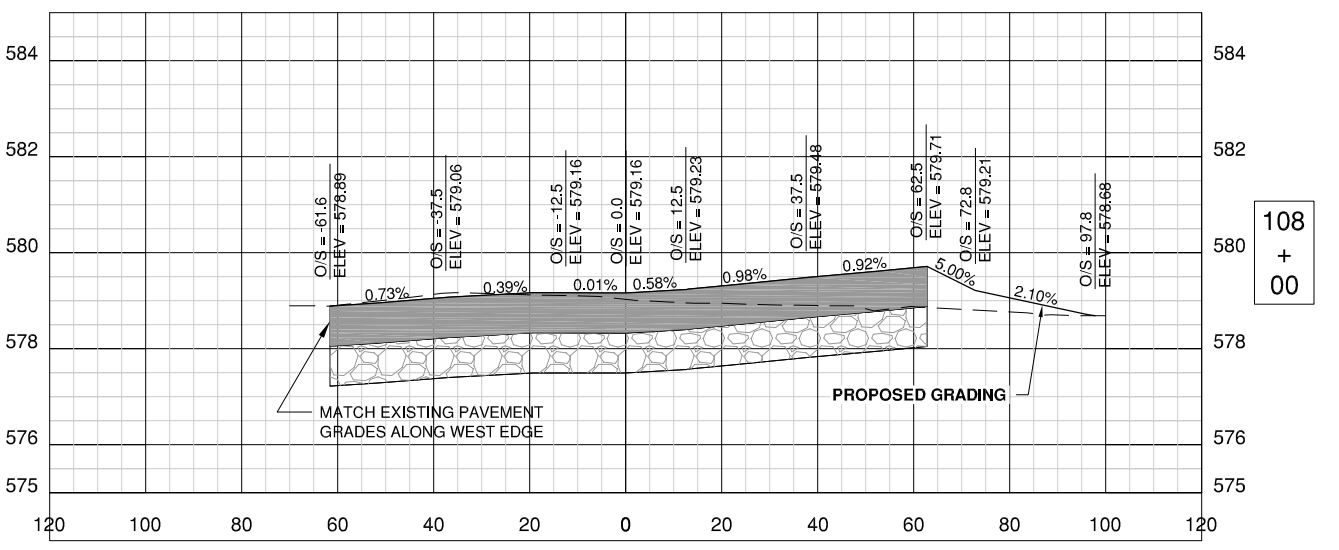
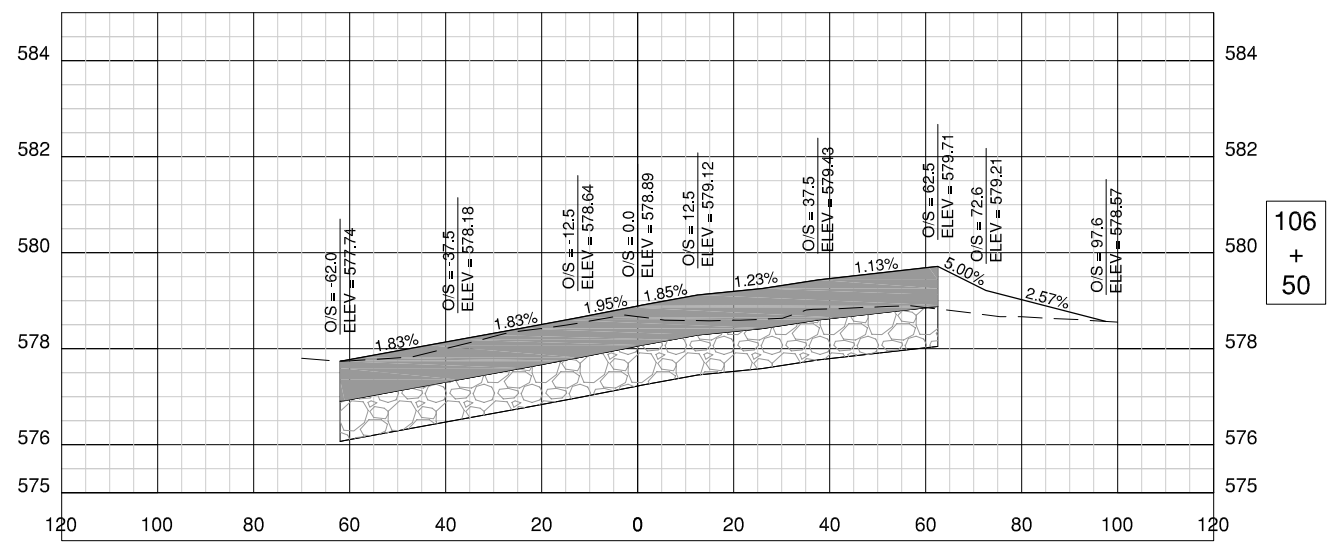
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**SOUTH GA RAMP EXPANSION - PHASE 1  
 GA RAMP CROSS SECTIONS 1**

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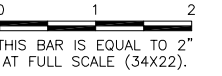
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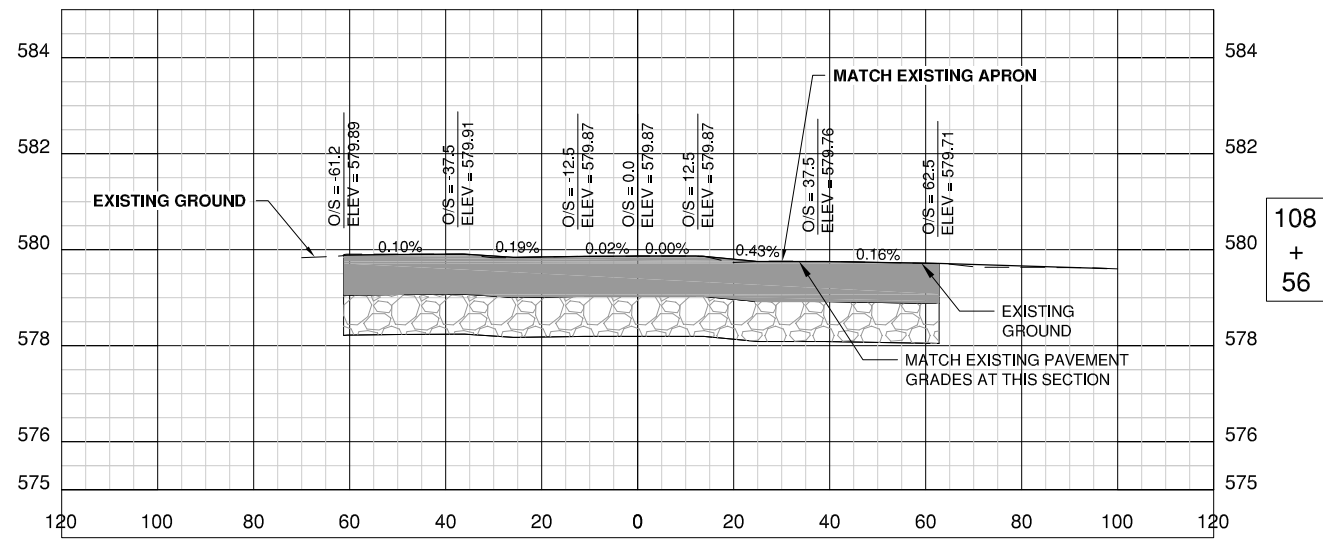
**SOUTH GA RAMP EXPANSION - PHASE 1  
 GA RAMP CROSS SECTIONS 2**

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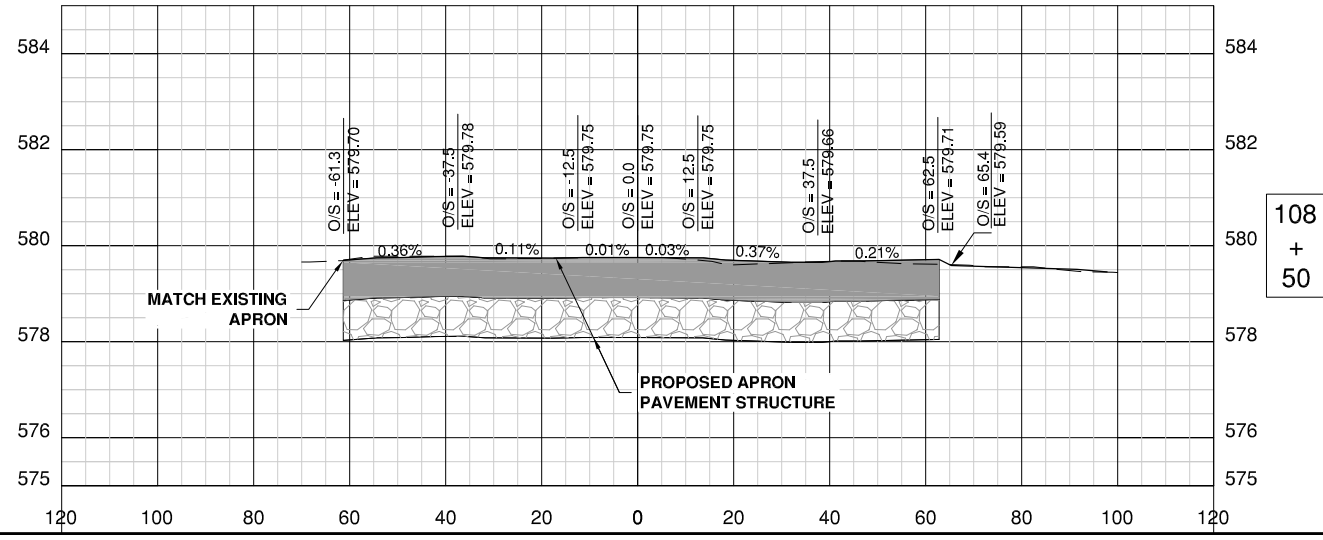


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108  
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