

JUNE 13, 2014 LETTING

CONSTRUCTION PLANS FOR ABRAHAM LINCOLN CAPITAL AIRPORT

SPRINGFIELD AIRPORT AUTHORITY
SPRINGFIELD, ILLINOIS

ILLINOIS PROJECT NO. SPI-4226
AIP PROJECT NO. 3-17-0096-XX

STORM SEWER REHABILITATION - PHASE 1

APRIL 18, 2014

SUMMARY OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1
AR151450	CLEARING AND GRUBBING	AC	4
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	5900
AR156510	SILT FENCE	L.F.	3590
AR156516	AGGREGATE DITCH CHECK	EA	4
AR156543	RIPRAP-GRADATION NO. 3	S.Y.	20
AR208612	12" AGGREGATE BASE COURSE	S.Y.	580
AR209606	CRUSHED AGG. BASE COURSE - 6"	S.Y.	580
AR701518	18" RCP, CLASS IV	L.F.	50
AR701821	TRENCH EXCAVATION	C.Y.	1225
AR701850	CONTROLLED LOW-STRENGTH MATERIAL	C.Y.	695
AR701900	REMOVE PIPE	L.F.	50
AR754610	PAVED DITCH	L.F.	800
AR901510	SEEDING	AC	4.5
AR908510	MULCHING	AC	1.9
AR908520	EXCELSIOR BLANKET	S.Y.	12500

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22	CROSS SECTIONS-PAVED DITCH STA. 2+00 TO STA. 3+50
23	CROSS SECTIONS-PAVED DITCH STA. 3+75 TO STA. 5+00
24	CROSS SECTIONS-PAVED DITCH STA. 5+25 TO STA. 6+50
25	CROSS SECTIONS-PAVED DITCH STA. 6+75 TO STA. 8+00

811 Know what's below. Call before you dig.

J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS
www.illinoiscall.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTORS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

DESIGN INFORMATION

GEOMETRIC CRITERIA

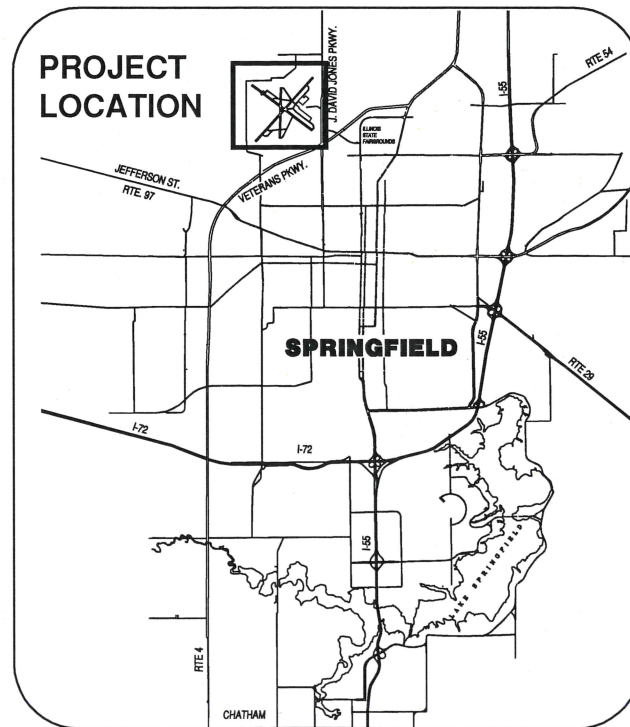
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PAVEMENT DESIGN CRITERIA

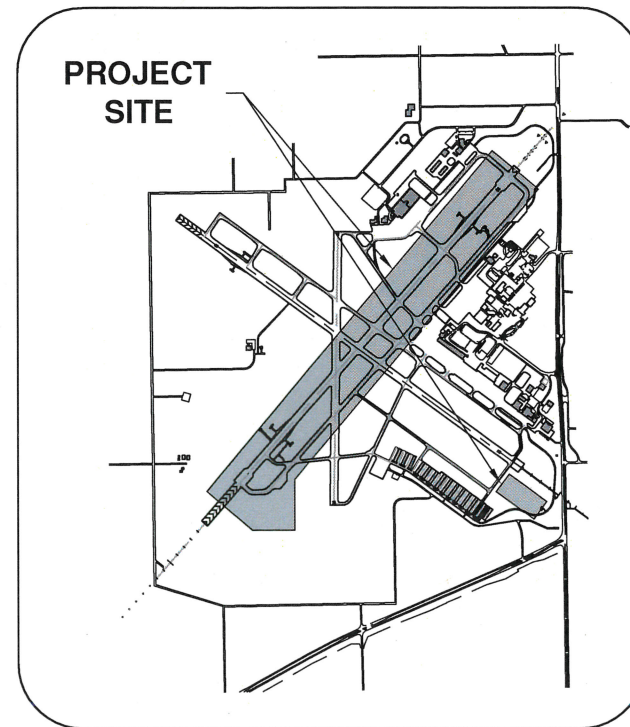
N/A

CALL J.U.L.I.E. BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: 16 NORTH
RANGE: 5 WEST OF THE 4TH P.M.
SECTION: 16
COUNTY: SANGAMON
CIVIL TOWNSHIP: CAPITAL



LOCATION MAP



SITE PLAN

5/9/14
ANDREW HUEBNER
062.065232
LICENSED PROFESSIONAL ENGINEER OF ILLINOIS
EXPIRES: 11/20/15

ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

APPROVED: *[Signature]*
EXECUTIVE DIRECTOR OF AVIATION

DATE: 4/16/2014

CMT
CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS
SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO

SUBMITTED BY: *[Signature]*
DATE: 4/22/14

CMT JOB NUMBER: 04035-09

GROUND CONTROL FREQUENCY - 121.90
 AIR CONTROL FREQUENCY - 121.30
 MAXIMUM HEIGHT OF EQUIPMENT - 25'



**CONTRACTOR
 ACCESS
 THROUGH
 EXISTING GATE**

**PROPOSED
 IMPROVEMENTS**

**CONTRACTOR
 STAGING AND
 STORAGE AREA**

**CONTRACTOR
 ACCESS
 THROUGH
 EXISTING GATE**

**CONTRACTOR
 STAGING AND
 STORAGE AREA**

GENERAL NOTES

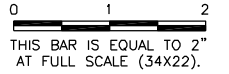
1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED SAA/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
3. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT DIRECTOR.
4. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
5. BROKEN ASPHALT AND OR CONCRETE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
6. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 95' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
7. ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND TO ORIGINAL CONDITION BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
8. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
9. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH ARE OR WILL BE OPENED TO AIR TRAFFIC.
10. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
11. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A. THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - B. THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR.
 - C. THE CONTRACTOR SHALL USE AN EXISTING GATE(S) FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER, S.A.A. MAINTENANCE SUPERVISOR, AND S.A.A. SECURITY CHIEF. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE RESIDENT ENGINEER.
 - D. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - E. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - F. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
 - G. THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - H. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - I. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
 - J. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES.
12. EXCESS EARTHWORK NOTES:
 - A. EXCESS EARTHWORK SHALL BE DISPOSED AT THE LOCATION SHOWN.
 - B. EXCESS EARTHWORK SHALL BE STOCKPILED IN MULTIPLE LAYERS AND SHAPED TO MINIMIZE AREA USED.

FILE: SITE PLAN.dwg
 UPDATE BY: Andrew Huebner
 PLOT DATE: 5/9/2014 8:05 PM
 SPI-BASE CURRENT GEOMETRY

CA017

REVISIONS

NUMBER	BY	DATE



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

**STORM SEWER REHABILITATION - PHASE 1
 AIRPORT SITE PLAN**

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DESIGN BY:	AJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	APRIL 18, 2014
JOB No:	04035-09

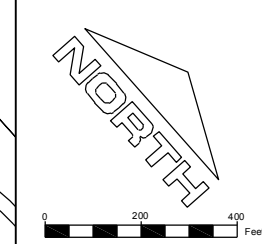
ILLINOIS PROJECT NO. SPI-4226
 AIP PROJECT NO. 3-17-0096-XX

K:\Springfield\0403509\Drawn\Sheets

CRITICAL POINT TABLE *			
POINT	LATITUDE	LONGITUDE	ELEVATION
1	N39° 50' 43.30"	W89° 40' 31.07"	586.00
2	N39° 50' 43.70"	W89° 40' 30.61"	586.00
3	N39° 50' 43.41"	W89° 40' 30.18"	586.00
4	N39° 50' 43.01"	W89° 40' 30.64"	586.00
5	N39° 50' 47.63"	W89° 40' 26.17"	585.00
6	N39° 50' 47.34"	W89° 40' 25.74"	585.00
7	N39° 50' 47.74"	W89° 40' 25.28"	585.00
8	N39° 50' 48.03"	W89° 40' 25.71"	585.00
9	N39° 50' 53.27"	W89° 40' 19.16"	584.00
10	N39° 50' 52.98"	W89° 40' 18.74"	584.00
11	N39° 50' 53.38"	W89° 40' 18.28"	584.00
12	N39° 50' 53.67"	W89° 40' 18.70"	584.00
13	N39° 50' 56.61"	W89° 40' 15.95"	582.00
14	N39° 50' 56.32"	W89° 40' 15.53"	582.00
15	N39° 50' 56.72"	W89° 40' 15.07"	582.00
16	N39° 50' 57.01"	W89° 40' 15.50"	582.00
17	N39° 51' 00.41"	W89° 40' 21.57"	584.00
18	N39° 51' 00.12"	W89° 40' 21.14"	584.00
19	N39° 51' 00.52"	W89° 40' 20.68"	584.00
20	N39° 51' 00.81"	W89° 40' 21.11"	584.00

CRITICAL POINT TABLE *			
POINT	LATITUDE	LONGITUDE	ELEVATION
21	N39° 51' 16.54"	W89° 39' 59.95"	591.00
22	N39° 51' 16.80"	W89° 39' 59.64"	591.00
23	N39° 51' 17.24"	W89° 40' 00.28"	591.00
24	N39° 51' 16.97"	W89° 40' 00.58"	591.00
25	N39° 51' 08.63"	W89° 39' 56.91"	593.00
26	N39° 51' 10.22"	W89° 39' 55.07"	593.00
27	N39° 51' 08.17"	W89° 39' 56.24"	593.00
28	N39° 51' 09.77"	W89° 39' 54.41"	593.00

* - THIS INFORMATION FOR FAA USE ONLY



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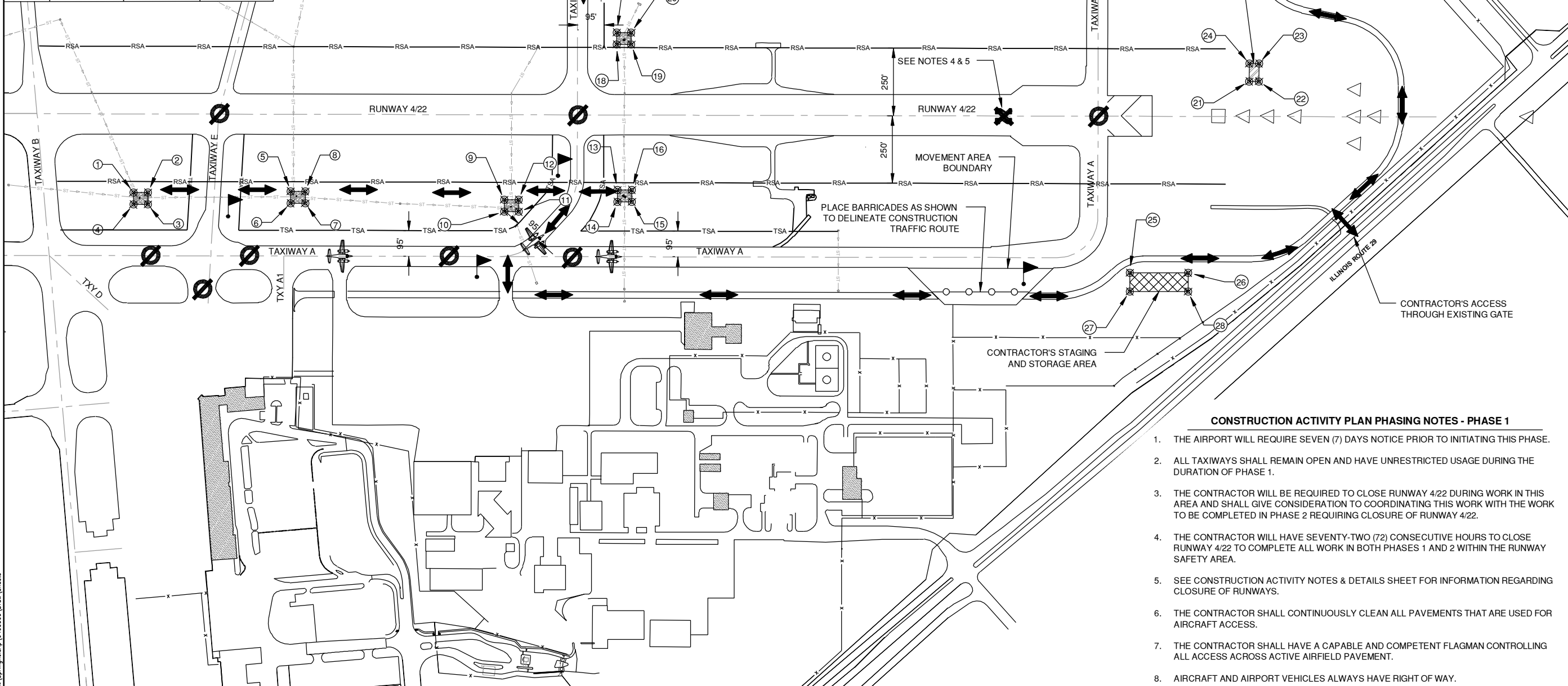
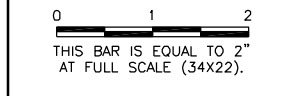
- SINK HOLE REPAIR WORK AREA
- REMOVE AND REPLACE PIPE WORK AREA
- CONTRACTOR'S STAGING AND STORAGE
- IDOT TYPE 1 BARRICADES W/FLASHING RED LIGHTS AND 20"x20" ORANGE FLAGS AT 10' SPACING
- LOW-PROFILE BARRICADES
- RUNWAY SAFETY AREA
- TAXIWAY SAFETY AREA
- CONTRACTOR'S ACCESS ROUTE - THIS PHASE
- FLAGMAN IN CONTACT WITH AIR TRAFFIC CONTROL TOWER (ATCT)
- PAVEMENT OFF LIMITS TO CONTRACTOR
- CRITICAL POINT - FOR FAA USE ONLY
- AIRCRAFT TRAFFIC

FILE: 0403509-C-CAP01.dwg
 UPDATE BY: Andrew Huebner
 PLOT DATE: 5/9/2014 8:05 PM

SPI-BASE-1
 0403509-C-UPDR
 BASE_EX-SINKHOLES
 SPI-BASE CURRENT GEOMETRY
 marking

CA017

REVISIONS		
NUMBER	BY	DATE



CONSTRUCTION ACTIVITY PLAN PHASING NOTES - PHASE 1

1. THE AIRPORT WILL REQUIRE SEVEN (7) DAYS NOTICE PRIOR TO INITIATING THIS PHASE.
2. ALL TAXIWAYS SHALL REMAIN OPEN AND HAVE UNRESTRICTED USAGE DURING THE DURATION OF PHASE 1.
3. THE CONTRACTOR WILL BE REQUIRED TO CLOSE RUNWAY 4/22 DURING WORK IN THIS AREA AND SHALL GIVE CONSIDERATION TO COORDINATING THIS WORK WITH THE WORK TO BE COMPLETED IN PHASE 2 REQUIRING CLOSURE OF RUNWAY 4/22.
4. THE CONTRACTOR WILL HAVE SEVENTY-TWO (72) CONSECUTIVE HOURS TO CLOSE RUNWAY 4/22 TO COMPLETE ALL WORK IN BOTH PHASES 1 AND 2 WITHIN THE RUNWAY SAFETY AREA.
5. SEE CONSTRUCTION ACTIVITY NOTES & DETAILS SHEET FOR INFORMATION REGARDING CLOSURE OF RUNWAYS.
6. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN ALL PAVEMENTS THAT ARE USED FOR AIRCRAFT ACCESS.
7. THE CONTRACTOR SHALL HAVE A CAPABLE AND COMPETENT FLAGMAN CONTROLLING ALL ACCESS ACROSS ACTIVE AIRFIELD PAVEMENT.
8. AIRCRAFT AND AIRPORT VEHICLES ALWAYS HAVE RIGHT OF WAY.

SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

STORM SEWER REHABILITATION - PHASE 1
CONSTRUCTION ACTIVITY PLAN 1

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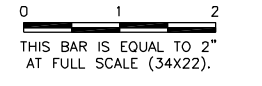
DESIGN BY:	AJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	APRIL 18, 2014
JOB No:	04035-09
ILLINOIS PROJECT NO. SPI-4226	
AIP PROJECT NO. 3-17-0096-XX	
SHEET 03 OF 25 SHEETS	

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REVISIONS

NUMBER	BY	DATE



**SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS**

**STORM SEWER REHABILITATION - PHASE 1
 CONSTRUCTION ACTIVITY PLAN 2**



DESIGN BY:	AJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	APRIL 18, 2014
JOB No:	04035-09
ILLINOIS PROJECT NO. SPI-4226 AIP PROJECT NO. 3-17-0096-XX	
SHEET 04 OF 25 SHEETS	

LEGEND

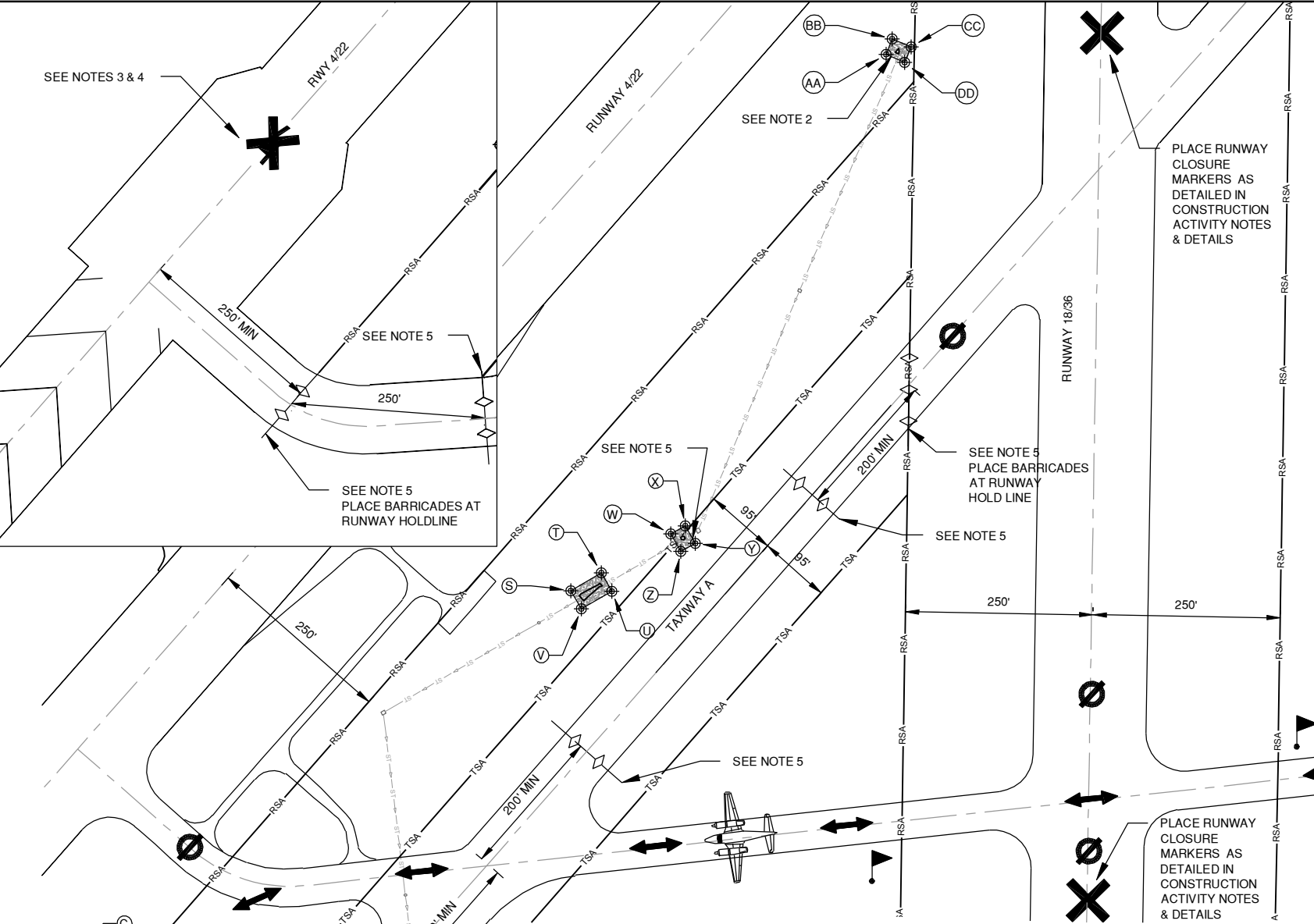
- SINK HOLE REPAIR WORK AREA
- DRAINAGE OUTFALL WORK AREA
- CONTRACTOR'S STAGING AND STORAGE
- IDOT TYPE 1 BARRICADES W/FLASHING RED LIGHTS AND 20"x20" ORANGE FLAGS AT 10' SPACING
- LOW PROFILE BARRICADES
- RUNWAY SAFETY AREA
- TAXIWAY SAFETY AREA
- CONTRACTOR'S ACCESS ROUTE - THIS PHASE
- FLAGMAN IN CONTACT WITH AIR TRAFFIC CONTROL TOWER (ATCT)
- PAVEMENT OFF LIMITS TO CONTRACTOR
- CRITICAL POINT - FOR FAA USE ONLY
- AIRCRAFT TRAFFIC

RUNWAY 18/36 NOTES

- RUNWAY 18/36 SHALL BE CLOSED DURING WORK HOURS WHILE PHASE 2 IS UNDER CONSTRUCTION TO ALLOW CONSTRUCTION TRAFFIC LESS IMPEDED ACCESS TO THE WORK AREAS.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO CLEAN AND RE-OPEN THE PAVEMENT EACH EVENING AT THE CLOSE OF WORK.
- THE CONTRACTOR SHALL PLACE BARRICADES AS SHOWN ONLY WHEN RUNWAY 18/36 IS CLOSED AND SHALL REMOVE THE BARRICADES FROM THE TAXIWAY SAFETY AREA WHEN NOT IN USE.
- THE CONTRACTOR SHALL FOLLOW THE CONSTRUCTION ACTIVITY NOTES & DETAILS REGARDING THE CLOSURE OF A RUNWAY.

CONSTRUCTION ACTIVITY PLAN PHASING NOTES - PHASE 2

- THE AIRPORT WILL REQUIRE 7 DAYS NOTICE PRIOR TO INITIATING THIS PHASE TO COORDINATE WITH TENANTS.
- THE CONTRACTOR WILL BE REQUIRED TO CLOSE RUNWAY 4/22 DURING WORK IN THESE TWO AREAS AND SHALL MAKE EVERY EFFORT TO COORDINATE THIS WORK WITH THE WORK REQUIRING CLOSURE OF RUNWAY 4/22 IN PHASE 1.
- THE CONTRACTOR WILL HAVE SEVENTY-TWO (72) CONSECUTIVE HOURS TO CLOSE RUNWAY 4/22 TO COMPLETE ALL WORK IN BOTH PHASES 1 AND 2 WITHIN THE RUNWAY SAFETY AREA.
- SEE CONSTRUCTION ACTIVITY NOTES & DETAILS SHEET FOR INFORMATION REGARDING CLOSURE OF RUNWAYS.
- TAXIWAY A SHALL BE CLOSED WHEN WORK IS OCCURRING IN THE TAXIWAY SAFETY AREA. BARRICADES SHALL BE PLACED AS SHOWN TO CLOSE THE TAXIWAY.
- TAXIWAY A MAY ONLY BE CLOSED **EITHER** NORTH OF TAXIWAY H **OR** SOUTH OF TAXIWAY H AND MAY NOT BE CLOSED IN BOTH AREAS AT THE SAME TIME.
- THE AIRPORT SHALL HAVE THE FINAL AUTHORITY ABOUT WHICH AREA IS CLOSED AND WHEN EACH AREA IS CLOSED.
- TAXIWAY A MUST BE CLOSED WHILE THERE ARE OPEN TRENCHES AND WORK BEING COMPLETED IN THE TAXIWAY SAFETY AREA.
- THE CONTRACTOR SHALL HAVE A SEVENTY-TWO (72) CONSECUTIVE HOUR PERIOD TO CLOSE TAXIWAY A NORTH OF TAXIWAY H AND A SEPARATE SEVENTY-TWO (72) CONSECUTIVE HOUR PERIOD TO CLOSE TAXIWAY A SOUTH OF TAXIWAY H. THE TWO PERIODS MAY NOT OVERLAP OR OCCUR CONCURRENTLY.
- TAXIWAY H WILL REMAIN OPEN TO AIRCRAFT AT ALL TIMES.
- CONTRACTOR SHALL CONTINUOUSLY CLEAN THIS AREA FOR AIRCRAFT ACCESS.
- AIRCRAFT AND AIRPORT VEHICLES ALWAYS HAVE RIGHT OF WAY.



CRITICAL POINT TABLE *

POINT	LATITUDE	LONGITUDE	ELEVATION
A	N39° 50' 11.92"	W89° 40' 59.71"	590.00
AA	N39° 50' 27.65"	W89° 40' 50.70"	588.00
B	N39° 50' 11.82"	W89° 40' 57.79"	584.00
BB	N39° 50' 27.85"	W89° 40' 50.59"	588.00
C	N39° 50' 14.88"	W89° 41' 04.85"	582.00
CC	N39° 50' 27.74"	W89° 40' 50.27"	588.00
D	N39° 50' 15.17"	W89° 41' 04.54"	582.00
DD	N39° 50' 27.54"	W89° 40' 50.38"	588.00
E	N39° 50' 14.97"	W89° 41' 04.21"	582.00
EE	N39° 50' 10.96"	W89° 40' 31.78"	583.00
F	N39° 50' 14.67"	W89° 41' 04.51"	582.00
FF	N39° 50' 10.95"	W89° 40' 29.01"	583.00
G	N39° 50' 13.82"	W89° 41' 03.08"	585.00
GG	N39° 50' 10.26"	W89° 40' 29.01"	583.00
H	N39° 50' 14.03"	W89° 41' 02.86"	585.00
HH	N39° 50' 10.27"	W89° 40' 31.78"	583.00
I	N39° 50' 13.89"	W89° 41' 02.64"	585.00
J	N39° 50' 13.68"	W89° 41' 02.86"	585.00
K	N39° 50' 13.68"	W89° 40' 58.82"	586.00
L	N39° 50' 13.66"	W89° 40' 58.48"	586.00

* - THIS INFORMATION FOR FAA USE ONLY

CRITICAL POINT TABLE *

POINT	LATITUDE	LONGITUDE	ELEVATION
M	N39° 50' 13.44"	W89° 40' 58.50"	586.00
N	N39° 50' 13.46"	W89° 40' 58.84"	586.00
O	N39° 50' 14.21"	W89° 40' 58.77"	584.00
P	N39° 50' 14.19"	W89° 40' 58.42"	584.00
Q	N39° 50' 13.97"	W89° 40' 58.44"	584.00
R	N39° 50' 13.99"	W89° 40' 58.79"	584.00
S	N39° 50' 20.60"	W89° 40' 56.14"	585.00
T	N39° 50' 20.83"	W89° 40' 55.61"	585.00
U	N39° 50' 20.59"	W89° 40' 55.44"	585.00
V	N39° 50' 20.36"	W89° 40' 55.96"	585.00
W	N39° 50' 21.34"	W89° 40' 54.42"	585.00
X	N39° 50' 21.45"	W89° 40' 54.18"	585.00
Y	N39° 50' 21.22"	W89° 40' 54.00"	585.00
Z	N39° 50' 21.11"	W89° 40' 54.25"	585.00

CA017

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

STORM SEWER REHABILITATION - PHASE 1
CONSTRUCTION ACTIVITY PLAN 3









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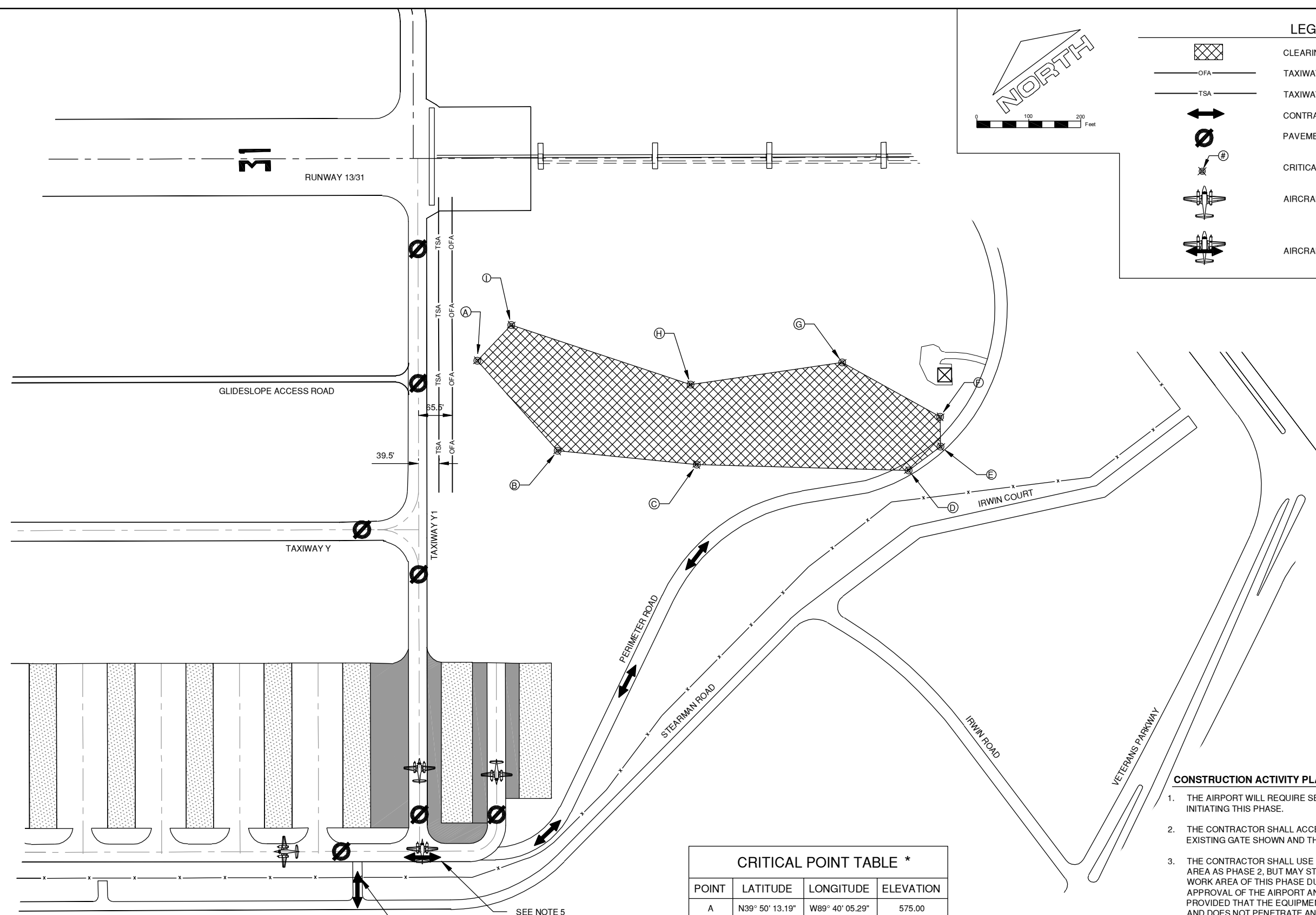
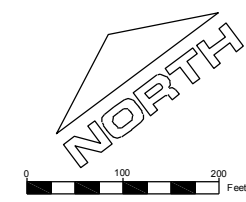
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DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	APRIL 18, 2014
JOB No:	04035-09
ILLINOIS PROJECT NO. SPI-4226	
AIP PROJECT NO. 3-17-0096-XX	
SHEET 05 OF 25 SHEETS	

LEGEND

-  CLEARING AND GRUBBING WORK AREA
-  TAXIWAY OBJECT FREE AREA
-  TAXIWAY SAFETY AREA
-  CONTRACTOR'S ACCESS ROUTE - THIS PHASE
-  PAVEMENT OFF LIMITS TO CONTRACTOR
-  CRITICAL POINT - FOR FAA USE ONLY
-  AIRCRAFT TRAFFIC
-  AIRCRAFT & CONTRACTOR TRAFFIC AREA



CRITICAL POINT TABLE *

POINT	LATITUDE	LONGITUDE	ELEVATION
A	N39° 50' 13.19"	W89° 40' 05.29"	575.00
B	N39° 50' 10.90"	W89° 40' 05.03"	561.00
C	N39° 50' 09.12"	W89° 40' 02.48"	572.00
D	N39° 50' 06.62"	W89° 39' 58.38"	541.00
E	N39° 50' 06.61"	W89° 39' 57.39"	543.00
F	N39° 50' 07.07"	W89° 39' 56.97"	556.00
G	N39° 50' 09.02"	W89° 39' 58.10"	547.00
H	N39° 50' 10.40"	W89° 40' 01.42"	561.00
I	N39° 50' 13.35"	W89° 40' 04.09"	569.00

* - THIS INFORMATION FOR FAA USE ONLY

CONSTRUCTION ACTIVITY PLAN PHASING NOTES - PHASE 3

1. THE AIRPORT WILL REQUIRE SEVEN (7) DAYS NOTICE PRIOR TO INITIATING THIS PHASE.
2. THE CONTRACTOR SHALL ACCESS THE WORK AREA VIA THE EXISTING GATE SHOWN AND THE PERIMETER ROAD.
3. THE CONTRACTOR SHALL USE THE SAME STAGING AND STORAGE AREA AS PHASE 2, BUT MAY STORE SOME EQUIPMENT NEAR THE WORK AREA OF THIS PHASE DURING CONSTRUCTION ONLY UPON APPROVAL OF THE AIRPORT AND THE RESIDENT ENGINEER PROVIDED THAT THE EQUIPMENT IS FULLY LOWERED BELOW 15' AND DOES NOT PENETRATE ANY PART 77 SURFACES.
4. THE CONTRACTOR SHALL NOT USE ANY AIRFIELD PAVEMENTS FOR ACCESS AND SHALL ONLY ACCESS THE SITE VIA THE PERIMETER ROAD.
5. THE CONTRACTOR WILL BE FORCED TO SHARE USABLE PAVEMENT WITH AIRCRAFT FROM THE ACCESS GATE TO THE PERIMETER ROAD. EXTREME CAUTION SHALL BE USED IN THIS AREA TO AVOID ANY INCIDENTS.
6. AIRCRAFT AND AIRPORT VEHICLE TRAFFIC WILL ALWAYS TAKE PRECEDENCE AND SHALL GOVERN AND HAVE THE RIGHT-OF-WAY.
7. CONTRACTOR SHALL CONTINUOUSLY CLEAN THE AREA BETWEEN THE ACCESS GATE AND THE PERIMETER ROAD FOR POTENTIAL AIRCRAFT USAGE.

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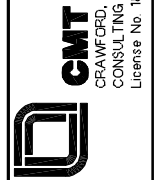
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 AT FULL SCALE (34x22).



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

**STORM SEWER REHABILITATION - PHASE 1
 CSPP - CONSTRUCTION SAFETY PHASING
 PLAN NOTES**

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SHEET	06 OF 25 SHEETS

16. PROTECTION

- ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 45' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

17. OTHER LIMITATIONS ON CONSTRUCTION

- IF DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

9. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

10. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT 7 DAYS BEFORE STARTING WORK IN EACH PHASE. THIS WILL ENSURE THAT THE AIRPORT CAN CONTACT TENANTS ABOUT MOVING AIRCRAFT DURING THE TIME OF CONSTRUCTION.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911 AND CONTACT THE AIRPORT.
- CONTACTS FOR THIS PROJECT ARE AS DISCUSSED IN THE PRE-CONSTRUCTION MEETING.

11. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR AND AIRPORT SHALL ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

12. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

13. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

14. SPECIAL CONDITIONS

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

15. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE AIRPORT.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

6. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN.
- CONTRACTOR EMPLOYEES MAY BE REQUIRED TO OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
- ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATIONS AREA (AOA) SHALL ALSO ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
- DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, ASPHALT, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE.
- CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS. BE AWARE OF TENANT AIRCRAFT MOVEMENT NEAR THE WORK AREAS.
- THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF THE AIRPORT. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.

7. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY THE AIRPORT OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

8. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

1. GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL.
- A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED, THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

2. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

3. PHASING

- PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

4. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

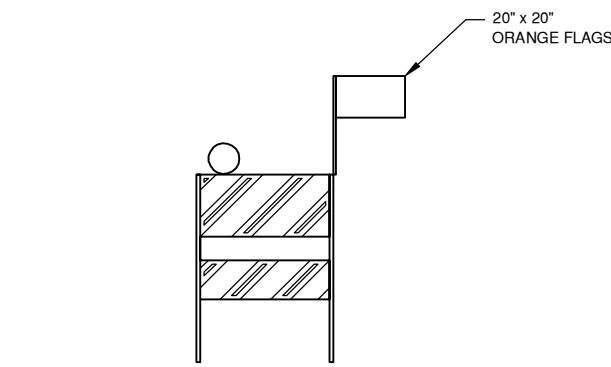
5. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.
- COORDINATION WILL BE REQUIRED FOR THE SHUTDOWN OF THE RUNWAY 4/22 NAVAIDS AND THE LOCKOUT OF THE RUNWAY 4/22 CIRCUIT DURING RUNWAY CLOSURES.
- COORDINATION WILL BE REQUIRED FOR THE SHUTDOWN OF THE GLIDESLOPE ON RUNWAY 31 DURING WORK WITHIN THE ILS CRITICAL AREA.

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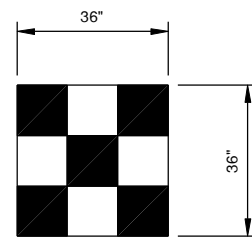
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FLASHER BARRICADE DETAIL-IDOT TYPE 1
 N.T.S.

FLASHER BARRICADE NOTES

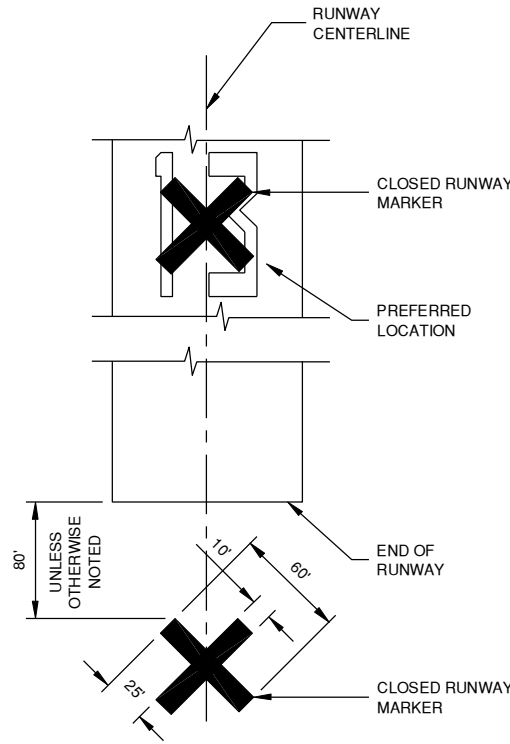
1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 10' INTERVALS.



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
 N.T.S.

PHASING GENERAL NOTES

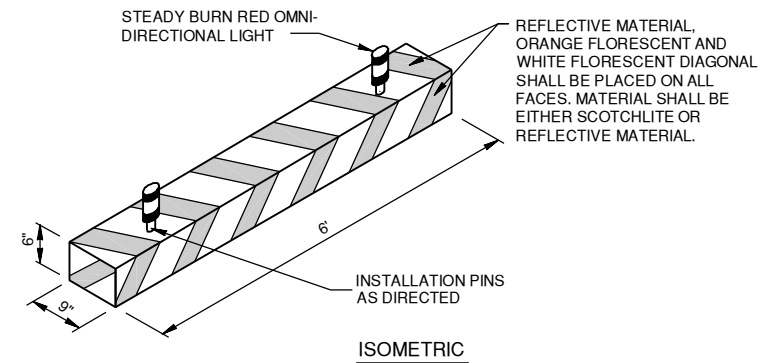
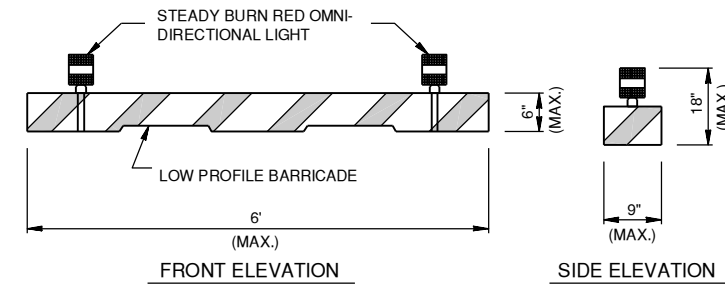
1. ALL CONSTRUCTION BARRICADES SHALL BE IN PLACE PRIOR TO INITIATING EACH PHASE.
2. THE CONTRACTOR SHALL GIVE THE AIRPORT A MINIMUM 7 DAYS NOTIFICATION PRIOR TO THE INITIATION OF WORK IN EACH PHASE.
3. THE CONTRACTOR SHALL MAINTAIN ALL MINIMUM DISTANCES SHOWN IN THE CONSTRUCTION ACTIVITY PLANS OR AS REQUIRED FOR THE CRITICAL OPERATING AIRCRAFT THROUGH THESE AREAS.
4. ALL PAVEMENTS OR TURF AREAS UTILIZED BY THE CONTRACTOR FOR AN ACCESS ROUTE, STAGING, OR STORAGE SHALL BE REPAIRED AND RESTORED TO THEIR ORIGINAL CONDITION TO THE SATISFACTION OF THE AIRPORT. NO ADDITIONAL COMPENSATION TO PROTECT, REPAIR, OR RESTORE THESE AREAS SHALL BE MADE.
5. WORK AREA ACCESS MAINTENANCE SHALL BE INCIDENTAL TO THE CONTRACT. MAINTENANCE SHALL CONTINUE AS REQUIRED SUCH THAT DURING ADVERSE WEATHER THE CONTRACTOR SHALL BE ABLE TO MAINTAIN ACCESS TO THE WORK AND NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITES.
6. FOR PHASES INVOLVING CLOSURE OF A TAXIWAY OR RUNWAY FOR A RESTRICTED NUMBER OF CALENDAR DAYS, THE CONTRACTOR SHALL TAKE INTO ACCOUNT CERTAIN SCHEDULING CONSIDERATIONS, SUCH AS, SUFFICIENT NUMBER OF PERSONNEL AND EQUIPMENT AVAILABLE AND THE WORKING OF SUFFICIENT HOURS TO COMPLETE CONSTRUCTION WITHIN THESE PHASES AND TIME RESTRAINTS.
7. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT TO SCHEDULE TAXIWAY AND RUNWAY CLOSURES. ITEMS SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT DEPENDABILITY, & MANPOWER AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THE CRITICAL CLOSURE. THE AIRPORT AND THE CONTRACTOR SHALL MUTUALLY AGREE ON THE ACTUAL DAYS FOR ANY CLOSURES.



PORTABLE CLOSED RUNWAY MARKER DETAIL & NOTES
 N.T.S.

NOTES

1. THE AIRPORT WILL PROVIDE PORTABLE LIGHTED CLOSURE MARKERS FOR THE CONTRACTOR'S USE.
2. THE CONTRACTOR WILL BE REQUIRED TO TOW AND SET UP THE MARKERS AT THE ENDS OF THE RUNWAY WHEN RUNWAY CLOSURES ARE REQUIRED
3. THE CONTRACTOR WILL BE REQUIRED TO PROVIDE REPLACEMENT BULBS, FUEL, LUBRICANTS, AND DAILY/PERIODIC MAINTENANCE INSPECTIONS AS REQUIRED BY THE AIRPORT.
4. THE AIRPORT WILL PERFORM PERIODIC MAINTENANCE, PROVIDE REPLACEMENT PARTS AND PERFORM REPAIRS AS REQUIRED TO KEEP THE EQUIPMENT OPERATIONAL.
5. THE COST OF SET-UP, FUELING, BULBS, INSPECTION, AND REMOVAL OF THE MARKERS SHALL BE INCIDENTAL TO THE CONTRACT.
6. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS.
7. UPON COMPLETION OF THE RUNWAY CLOSURE, THE CONTRACTOR WILL BE REQUIRED TO TAKE DOWN & RETURN THE MARKERS TO THEIR STORAGE LOCATION.



BEAM BARRICADE DETAILS
 N.T.S.

NOTES

- 1.) BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
- 2.) BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
- 3.) PLACE AT 10' INTERVALS.
- 4.) NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

STORM SEWER REHABILITATION - PHASE 1
 CONSTRUCTION ACTIVITY PLAN
 NOTES & DETAILS

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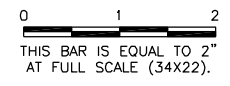


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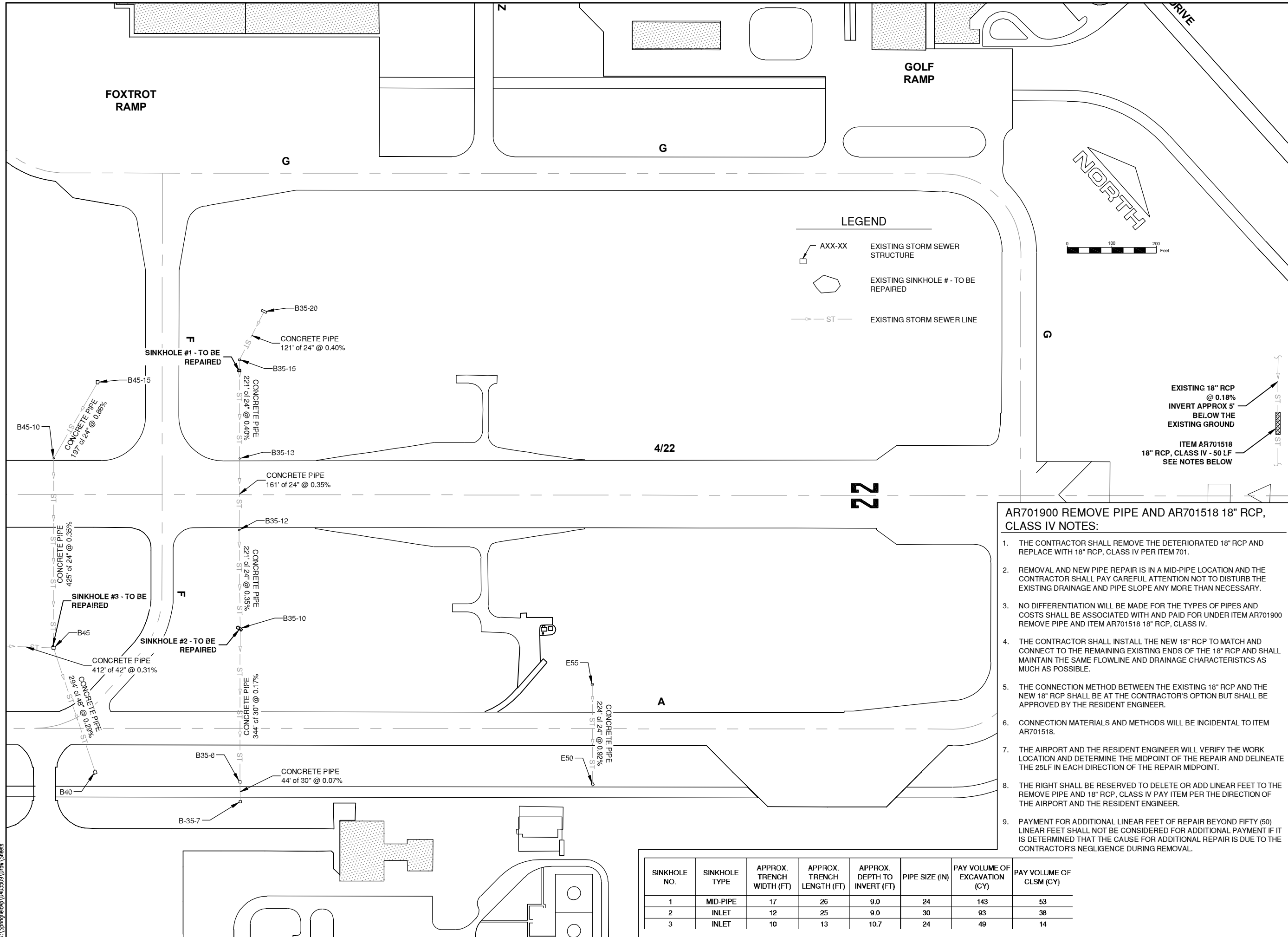


**SPRINGFIELD AIRPORT AUTHORITY
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 SPRINGFIELD, ILLINOIS**

**STORM SEWER REHABILITATION - PHASE 1
 PROPOSED IMPROVEMENTS 1**

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SHEET 08 OF 25 SHEETS	



LEGEND

- AXX-XX EXISTING STORM SEWER STRUCTURE
- EXISTING SINKHOLE # - TO BE REPAIRED
- EXISTING STORM SEWER LINE

AR701900 REMOVE PIPE AND AR701518 18" RCP, CLASS IV NOTES:

1. THE CONTRACTOR SHALL REMOVE THE DETERIORATED 18" RCP AND REPLACE WITH 18" RCP, CLASS IV PER ITEM 701.
2. REMOVAL AND NEW PIPE REPAIR IS IN A MID-PIPE LOCATION AND THE CONTRACTOR SHALL PAY CAREFUL ATTENTION NOT TO DISTURB THE EXISTING DRAINAGE AND PIPE SLOPE ANY MORE THAN NECESSARY.
3. NO DIFFERENTIATION WILL BE MADE FOR THE TYPES OF PIPES AND COSTS SHALL BE ASSOCIATED WITH AND PAID FOR UNDER ITEM AR701900 REMOVE PIPE AND ITEM AR701518 18" RCP, CLASS IV.
4. THE CONTRACTOR SHALL INSTALL THE NEW 18" RCP TO MATCH AND CONNECT TO THE REMAINING EXISTING ENDS OF THE 18" RCP AND SHALL MAINTAIN THE SAME FLOWLINE AND DRAINAGE CHARACTERISTICS AS MUCH AS POSSIBLE.
5. THE CONNECTION METHOD BETWEEN THE EXISTING 18" RCP AND THE NEW 18" RCP SHALL BE AT THE CONTRACTOR'S OPTION BUT SHALL BE APPROVED BY THE RESIDENT ENGINEER.
6. CONNECTION MATERIALS AND METHODS WILL BE INCIDENTAL TO ITEM AR701518.
7. THE AIRPORT AND THE RESIDENT ENGINEER WILL VERIFY THE WORK LOCATION AND DETERMINE THE MIDPOINT OF THE REPAIR AND DELINEATE THE 25LF IN EACH DIRECTION OF THE REPAIR MIDPOINT.
8. THE RIGHT SHALL BE RESERVED TO DELETE OR ADD LINEAR FEET TO THE REMOVE PIPE AND 18" RCP, CLASS IV PAY ITEM PER THE DIRECTION OF THE AIRPORT AND THE RESIDENT ENGINEER.
9. PAYMENT FOR ADDITIONAL LINEAR FEET OF REPAIR BEYOND FIFTY (50) LINEAR FEET SHALL NOT BE CONSIDERED FOR ADDITIONAL PAYMENT IF IT IS DETERMINED THAT THE CAUSE FOR ADDITIONAL REPAIR IS DUE TO THE CONTRACTOR'S NEGLIGENCE DURING REMOVAL.

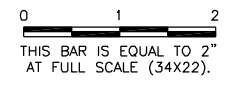
SINKHOLE NO.	SINKHOLE TYPE	APPROX. TRENCH WIDTH (FT)	APPROX. TRENCH LENGTH (FT)	APPROX. DEPTH TO INVERT (FT)	PIPE SIZE (IN)	PAY VOLUME OF EXCAVATION (CY)	PAY VOLUME OF CLSM (CY)
1	MID-PIPE	17	26	9.0	24	143	53
2	INLET	12	25	9.0	30	93	38
3	INLET	10	13	10.7	24	49	14

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**STORM SEWER REHABILITATION - PHASE 1
 PROPOSED IMPROVEMENTS 2**

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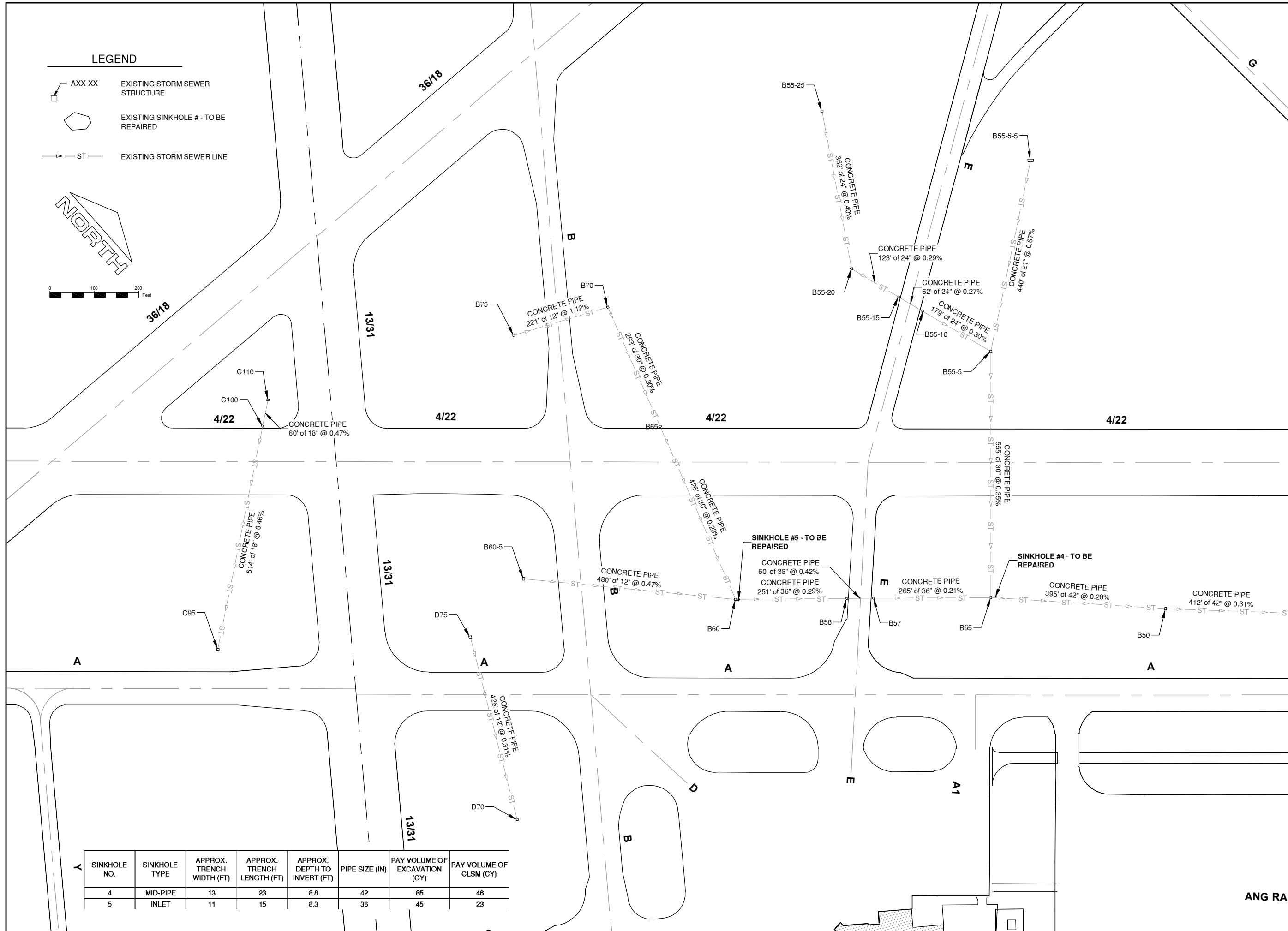
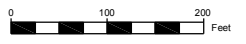
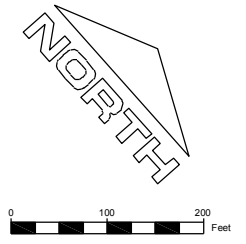


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DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	APRIL 18, 2014
JOB No:	04035-09

ILLINOIS PROJECT NO. SPI-4226
 AIP PROJECT NO. 3-17-0096-XX

LEGEND

- AXX-XX EXISTING STORM SEWER STRUCTURE
- EXISTING SINKHOLE # - TO BE REPAIRED
- ST EXISTING STORM SEWER LINE



SINKHOLE NO.	SINKHOLE TYPE	APPROX. TRENCH WIDTH (FT)	APPROX. TRENCH LENGTH (FT)	APPROX. DEPTH TO INVERT (FT)	PIPE SIZE (IN)	PAY VOLUME OF EXCAVATION (CY)	PAY VOLUME OF CLSM (CY)
4	MID-PIPE	13	23	8.8	42	85	46
5	INLET	11	15	8.3	36	45	23

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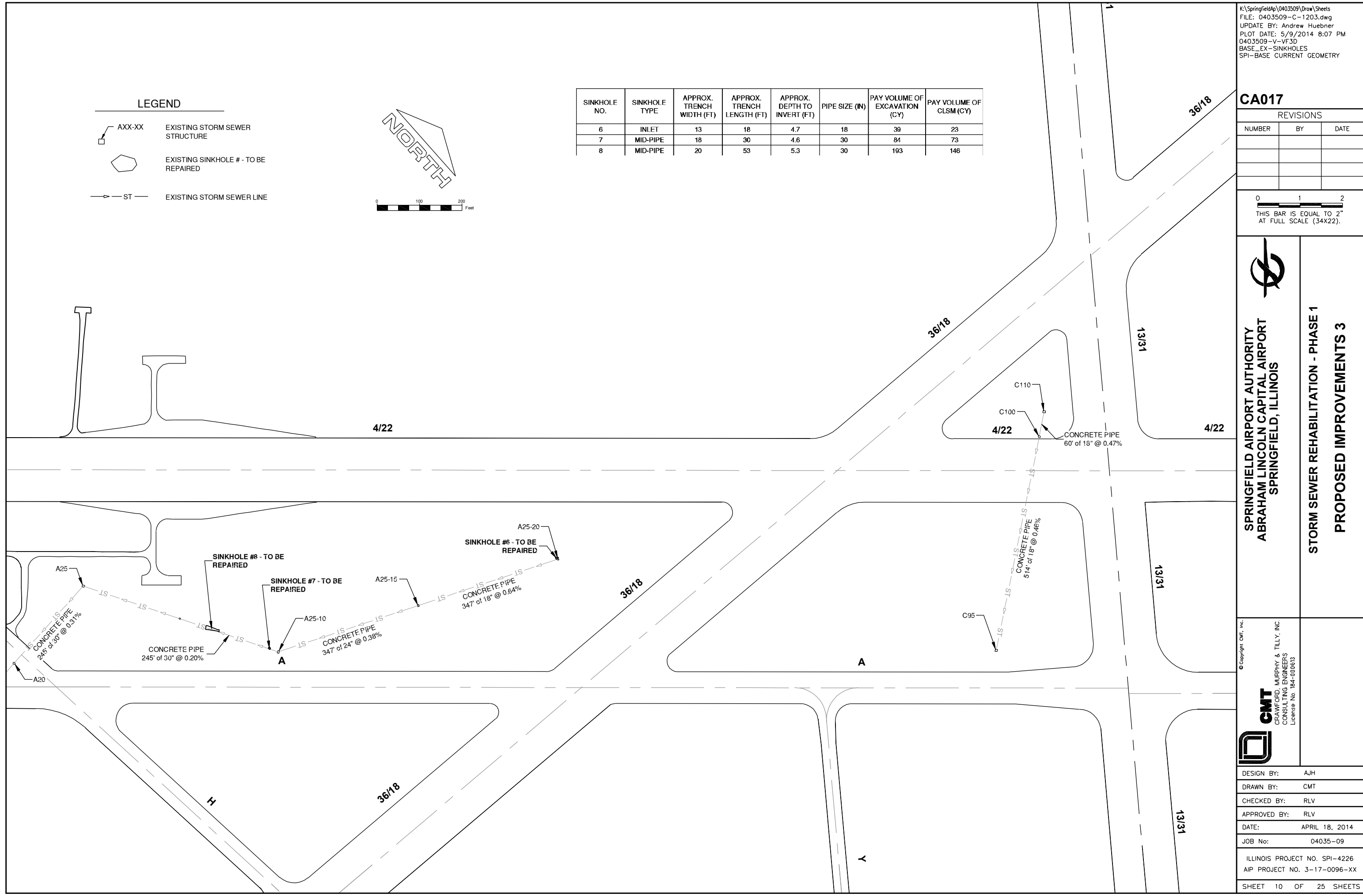
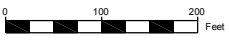
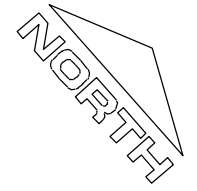

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

SINKHOLE NO.	SINKHOLE TYPE	APPROX. TRENCH WIDTH (FT)	APPROX. TRENCH LENGTH (FT)	APPROX. DEPTH TO INVERT (FT)	PIPE SIZE (IN)	PAY VOLUME OF EXCAVATION (CY)	PAY VOLUME OF CLSM (CY)
6	INLET	13	18	4.7	18	39	23
7	MID-PIPE	18	30	4.6	30	84	73
8	MID-PIPE	20	53	5.3	30	193	146

LEGEND


- AXX-XX EXISTING STORM SEWER STRUCTURE
- EXISTING SINKHOLE # - TO BE REPAIRED
- ST EXISTING STORM SEWER LINE

SPRINGFIELD AIRPORT AUTHORITY
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SPRINGFIELD, ILLINOIS

STORM SEWER REHABILITATION - PHASE 1
PROPOSED IMPROVEMENTS 3

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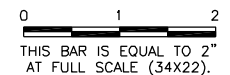


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AIP PROJECT NO. 3-17-0096-XX	
SHEET 10 OF 25 SHEETS	

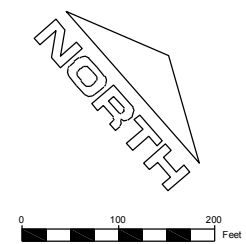
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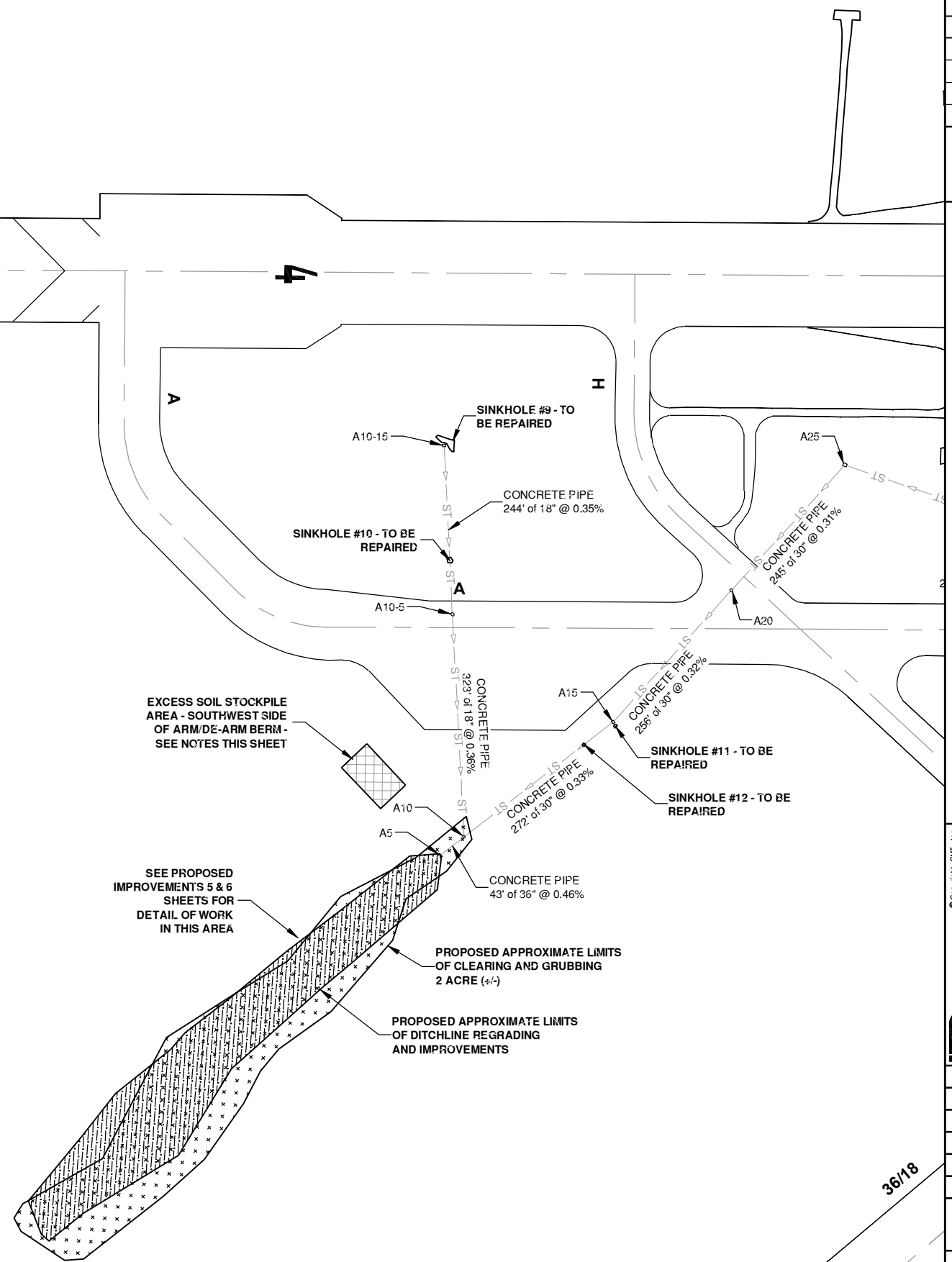
LEGEND

- AXX-XX** EXISTING STORM SEWER STRUCTURE
- EXISTING SINKHOLE # - TO BE REPAIRED
- ST** EXISTING STORM SEWER LINE
- PROPOSED CLEARING & GRUBBING AREA
- PROPOSED GRADING LIMITS
- APPROXIMATE PROPOSED EXCESS SOIL STOCKPILE AREA



SINKHOLE NO.	SINKHOLE TYPE	APPROX. TRENCH WIDTH (FT)	APPROX. TRENCH LENGTH (FT)	APPROX. DEPTH TO INVERT (FT)	PIPE SIZE (IN)	PAY VOLUME OF EXCAVATION (CY)	PAY VOLUME OF CLSM (CY)
9	INLET	21	46	4.7	18	163	99
10	MID-PIPE	20	30	7.8	18	170	62
11	INLET	13	17	6.8	30	51	29
12	MID-PIPE	16	26	7.4	30	107	56

- EXCESS SOIL STOCKPILE NOTES:**
- ANY EXCESS SOIL FROM THE DITCHLINE IMPROVEMENTS CONSTRUCTION SHALL FIRST BE HAULED AND USED IN THE SINKHOLE REPAIR AREAS AS NEEDED.
 - ANY EXCESS SOIL REMAINING AFTER COMPLETION OF THE SINKHOLE REPAIR AREAS AND PIPE REMOVAL AND REPLACEMENT AREA SHALL BE WASTED ON THE ARM / DE-ARM BERM NEAREST THE DITCH IMPROVEMENT AREA AND APPROXIMATELY AS SHOWN ON THIS SHEET.
 - SOIL WASTED ON THE ARM / DE-ARM BERM SHALL BE PLACED AND COMPACTED TO THE ENGINEER'S AND THE AIRPORT'S SATISFACTION.
 - THE AREA SHALL BE GRADED SIMILARLY TO THE EXISTING ARM / DE-ARM BERM AND SHALL MAINTAIN EXISTING DRAINAGE PATTERNS.
 - AFTER GRADING THIS AREA SMOOTHLY, ALL DISTURBED AREAS SHALL BE SEEDED AND COVERED WITH EXCELSIOR BLANKET SIMILARLY TO THE DITCHLINE IMPROVEMENTS AREA AND IN ACCORDANCE WITH THE ITEM 901 AND ITEM 908 SPECIFICATIONS.
 - THIS SEEDING AND EXCELSIOR BLANKET FOR THE EXCESS SOIL STOCKPILE AREA SHALL BE INCIDENTAL TO THE CONTRACT.



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 ABRAHAM LINCOLN CAPITAL AIRPORT
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**STORM SEWER REHABILITATION - PHASE 1
 PROPOSED IMPROVEMENTS 4**

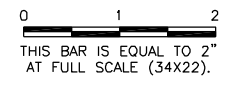
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 AIP PROJECT NO. 3-17-0096-XX
 SHEET 11 OF 25 SHEETS

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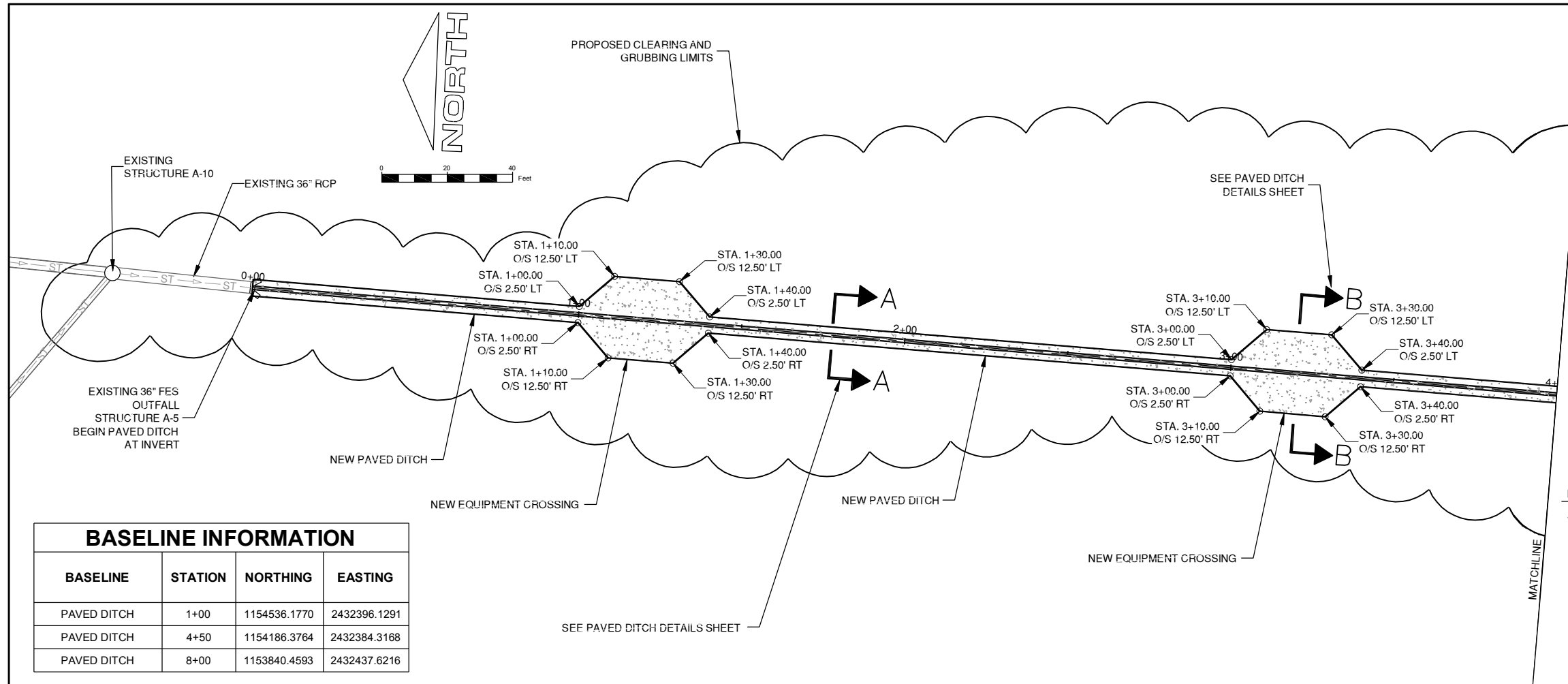


LEGEND

-  NEW PCC PAVED DITCH
-  NEW RIP RAP
-  PROPOSED CLEARING AND GRUBBING LIMITS

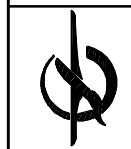
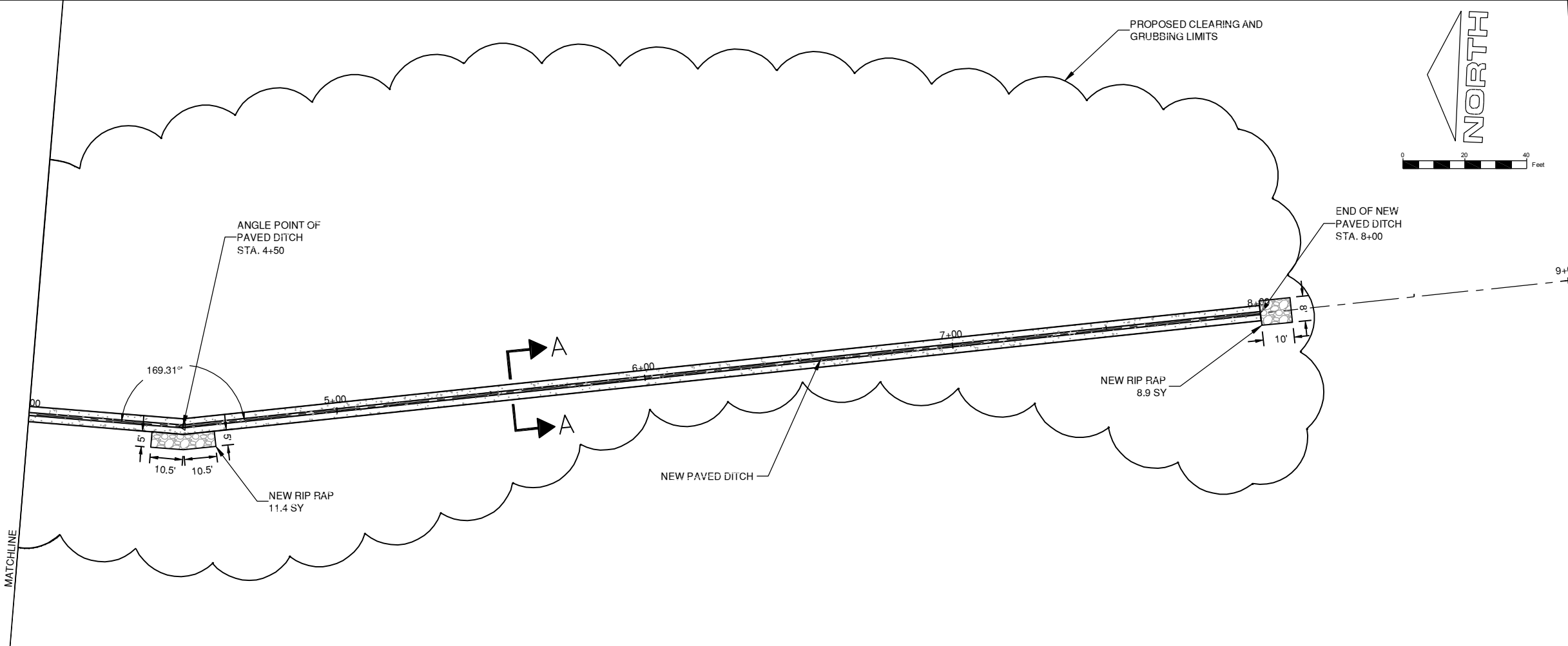
NOTES:

1. RIP RAP SHALL BE INSTALLED AS SHOWN AND AS DIRECTED BY THE RESIDENT ENGINEER. THE DEPTH OF THE RIP RAP SHALL BE TWO (2') FEET AND SHALL BE INSTALLED ON FILTER FABRIC.



BASELINE INFORMATION

BASELINE	STATION	NORTHING	EASTING
PAVED DITCH	1+00	1154536.1770	2432396.1291
PAVED DITCH	4+50	1154186.3764	2432384.3168
PAVED DITCH	8+00	1153840.4593	2432437.6216



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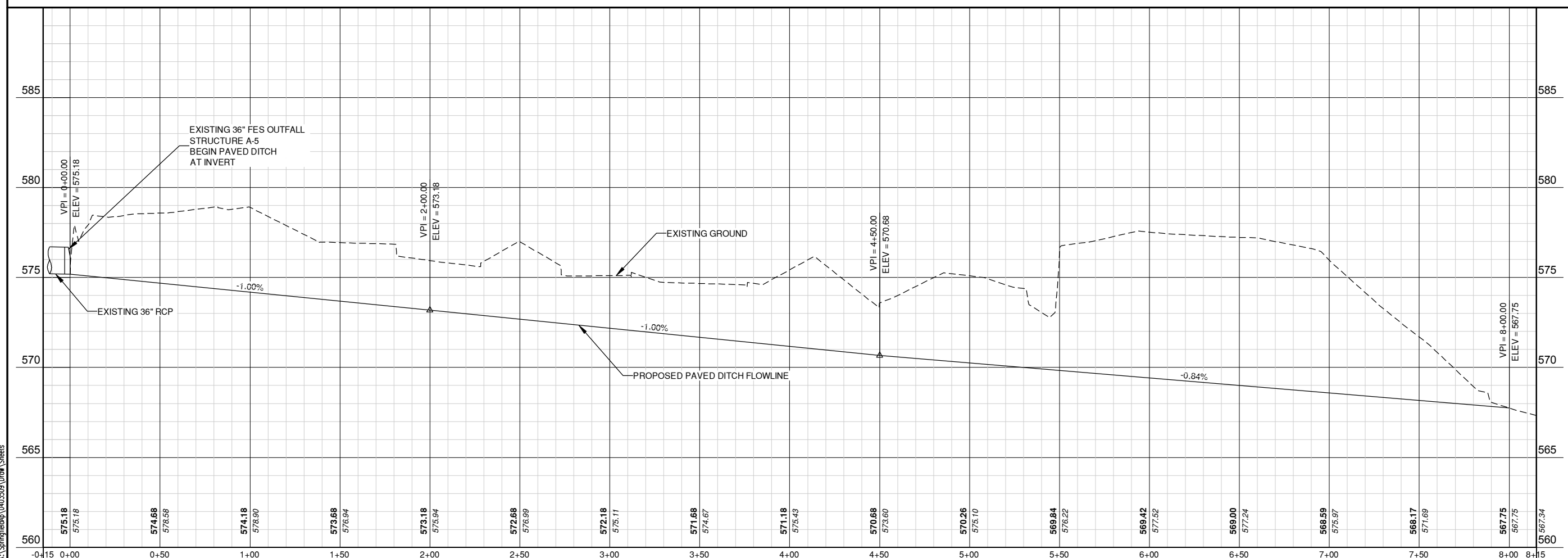
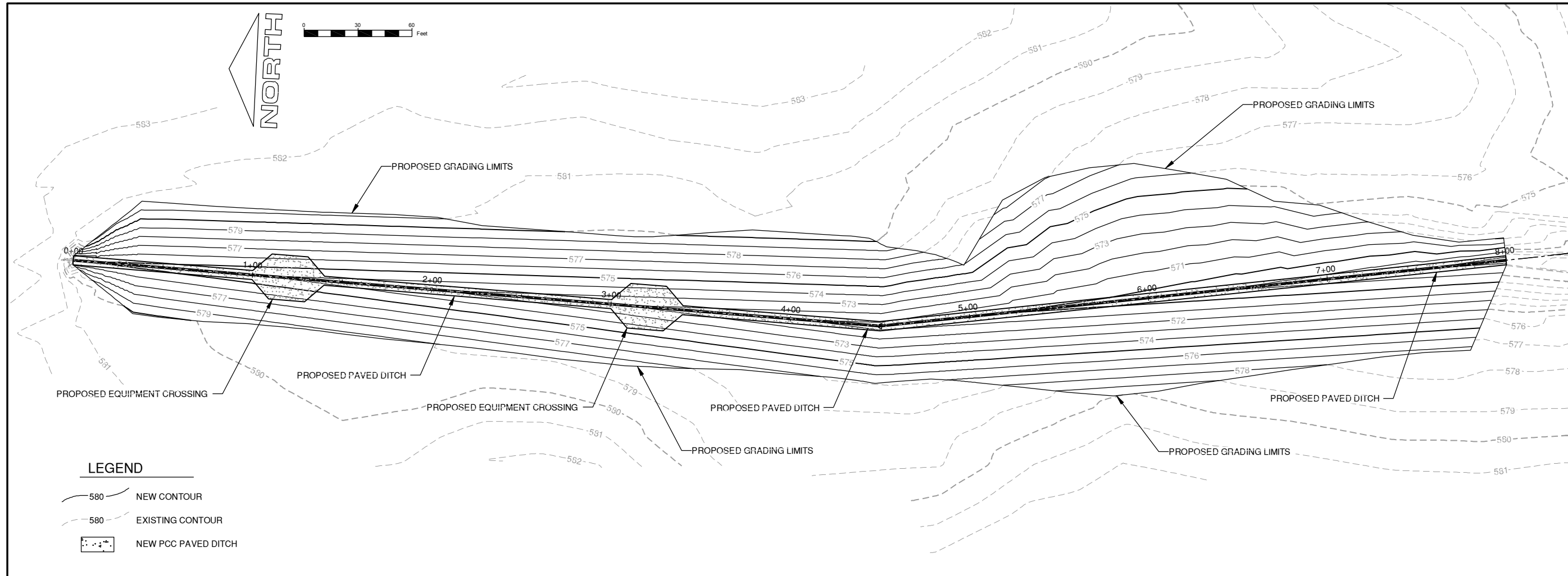
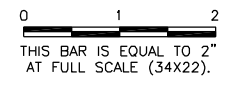
**STORM SEWER REHABILITATION - PHASE 1
 PROPOSED IMPROVEMENTS 5**

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ILLINOIS PROJECT NO. SPI-4226 AIP PROJECT NO. 3-17-0096-XX	
SHEET 12 OF 25 SHEETS	

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STORM SEWER REHABILITATION - PHASE 1
PROPOSED IMPROVEMENTS 6 - PLAN & PROFILE

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
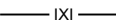

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DATE:	APRIL 18, 2014
JOB No:	04035-09
ILLINOIS PROJECT NO. SPI-4226 AIP PROJECT NO. 3-17-0096-XX	
SHEET 13 OF 25 SHEETS	

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RUNWAY 13/31

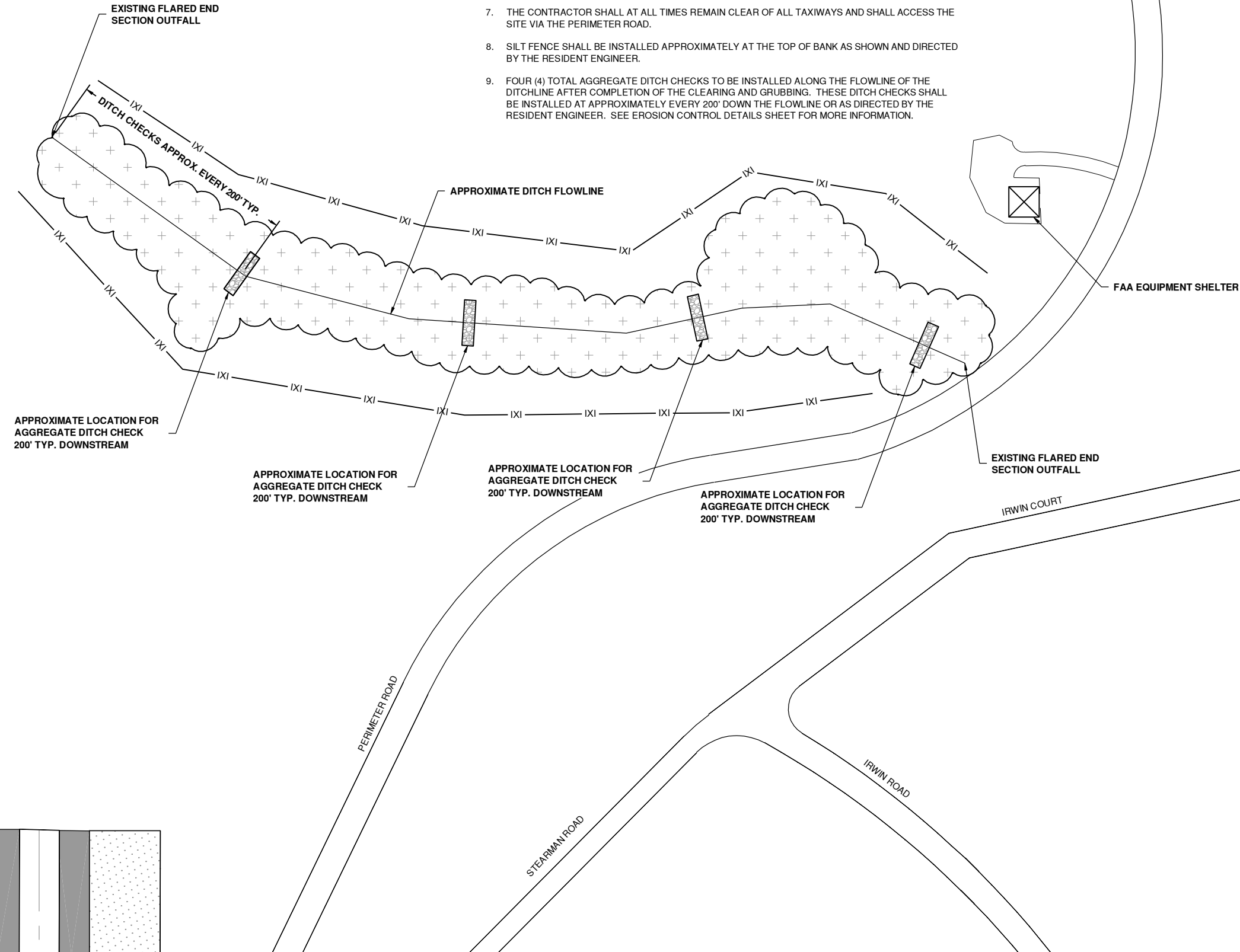


LEGEND

-  PROPOSED CLEARING & GRUBBING AREA
-  PROPOSED SILT FENCE
-  PROPOSED AGGREGATE DITCH CHECK

NOTES:

1. THE CONTRACTOR SHALL CLEAR AND GRUB THE AREA AS SHOWN AND AS PER ITEM 151.
2. THE CLEARING AND GRUBBING AREA SHOWN IS APPROXIMATE, RESIDENT ENGINEER AND THE AIRPORT SHALL HAVE THE FINAL AUTHORITY ON THE CLEARING AND GRUBBING LIMITS.
3. THE AREA SHALL BE GRADED SIMILARLY TO THE EXISTING DITCHLINE AND SHALL MAINTAIN EXISTING DRAINAGE PATTERNS.
4. RECENT SURVEYS OF THIS AREA ARE NOT AVAILABLE. THE CONTRACTOR SHOULD CONTACT THE AIRPORT IF THEY FEEL THEY NEED TO CONDUCT A SITE VISIT.
5. AFTER COMPLETION OF THE CLEARING AND GRUBBING, THE CONTRACTOR SHALL GRADE THIS AREA SMOOTHLY, ALL DISTURBED AREAS SHALL BE SEEDED AND MULCHED IN ACCORDANCE WITH THE ITEM 901 AND ITEM 908 SPECIFICATIONS.
6. THIS SEEDING AND MULCHING SHALL BE PAID FOR UNDER THEIR RESPECTIVE ITEMS AND THE AREA FOR PAYMENT SHALL BE CONSIDERED THE SAME AS THE AREA FOR CLEARING AND GRUBBING.
7. THE CONTRACTOR SHALL AT ALL TIMES REMAIN CLEAR OF ALL TAXIWAYS AND SHALL ACCESS THE SITE VIA THE PERIMETER ROAD.
8. SILT FENCE SHALL BE INSTALLED APPROXIMATELY AT THE TOP OF BANK AS SHOWN AND DIRECTED BY THE RESIDENT ENGINEER.
9. FOUR (4) TOTAL AGGREGATE DITCH CHECKS TO BE INSTALLED ALONG THE FLOWLINE OF THE DITCHLINE AFTER COMPLETION OF THE CLEARING AND GRUBBING. THESE DITCH CHECKS SHALL BE INSTALLED AT APPROXIMATELY EVERY 200' DOWN THE FLOWLINE OR AS DIRECTED BY THE RESIDENT ENGINEER. SEE EROSION CONTROL DETAILS SHEET FOR MORE INFORMATION.

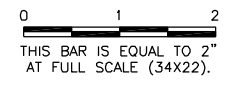


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 PLOT DATE: 5/9/2014 8:09 PM
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 BASE_EX-SINKHOLES
 SPI-BASE CURRENT GEOMETRY

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**STORM SEWER REHABILITATION - PHASE 1
 PROPOSED IMPROVEMENTS 7**

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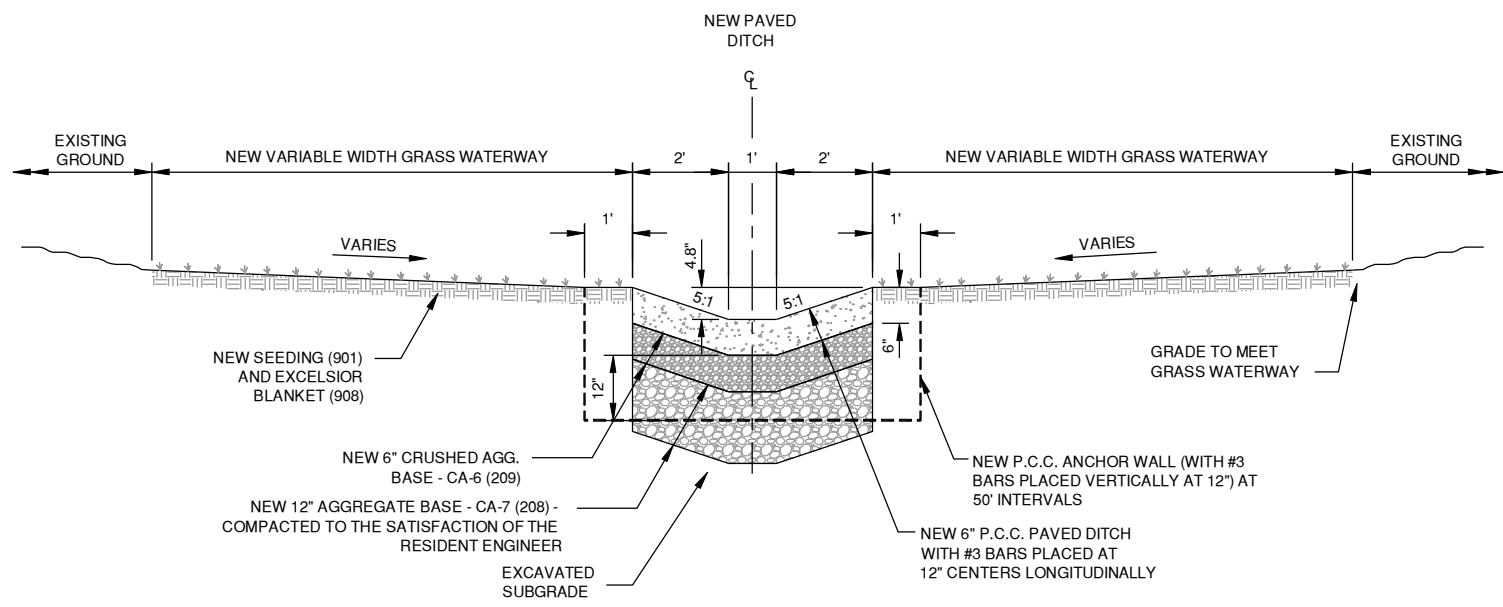
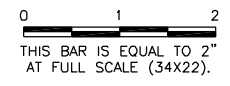


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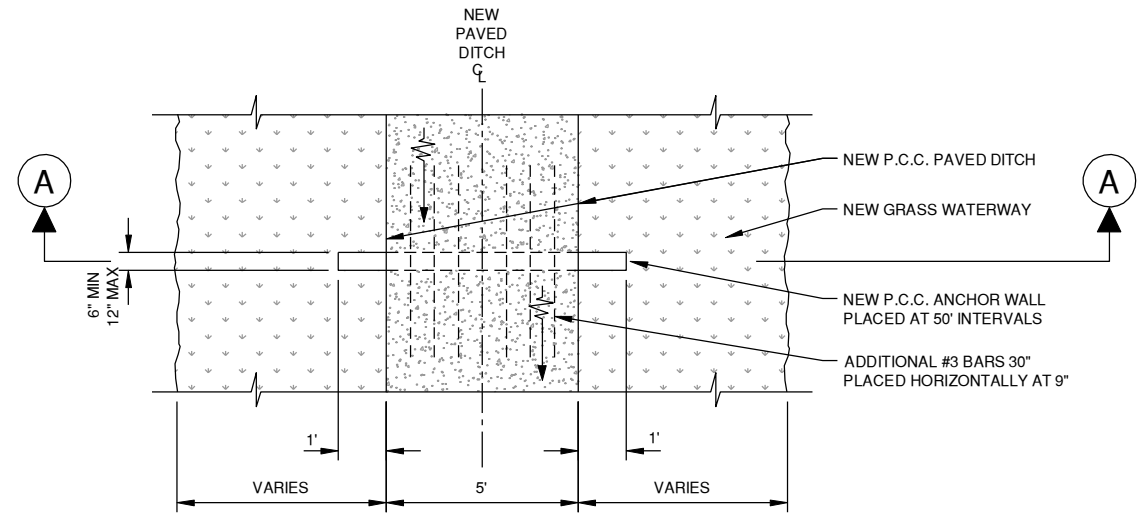
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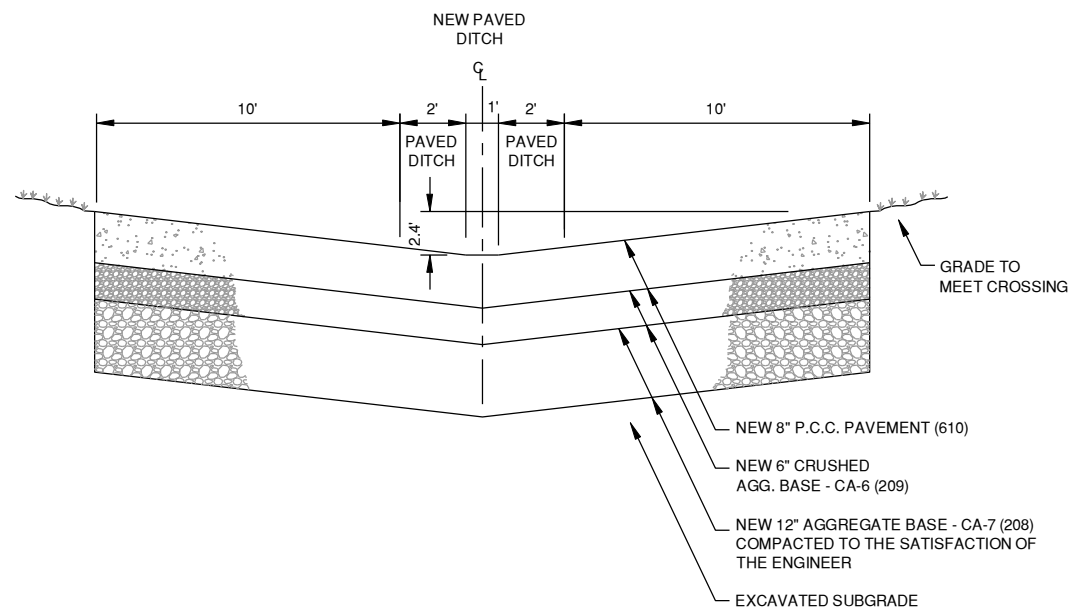
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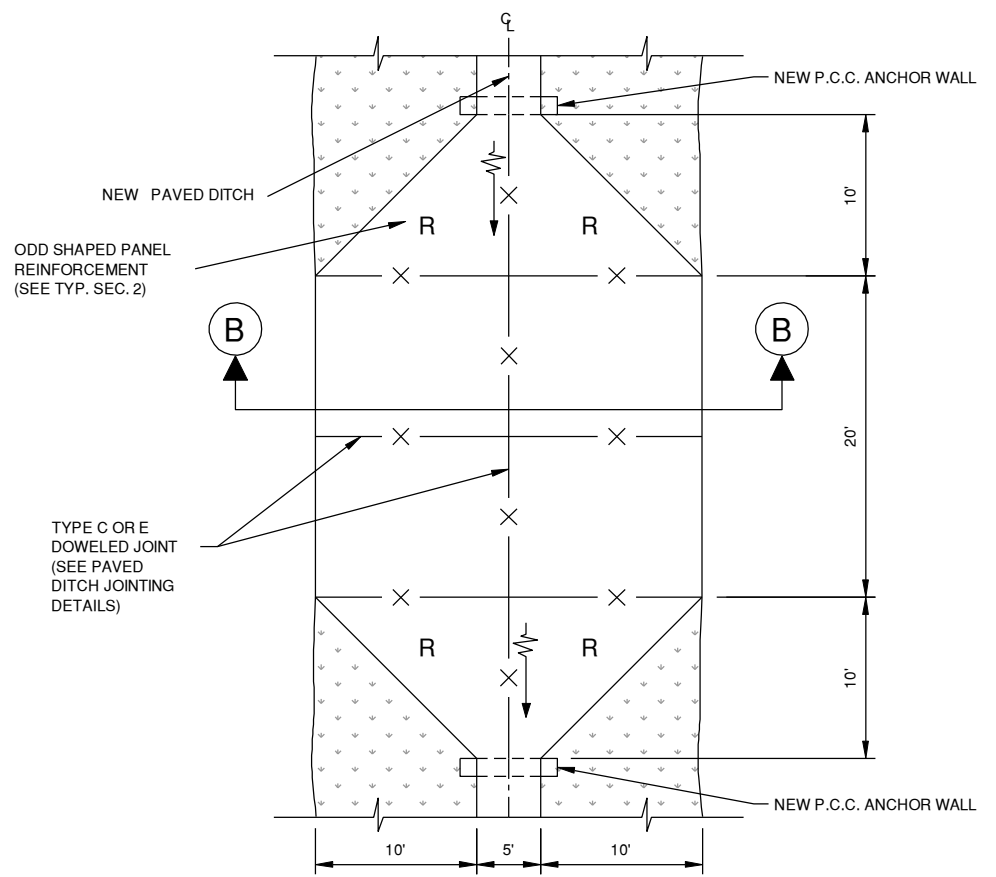
PAVED DITCH AND GRASS WATERWAY TYPICAL SECTION
 N.T.S.



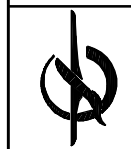
PAVED DITCH AND GRASS WATERWAY PLAN VIEW
 N.T.S.



PAVED DITCH EQUIPMENT CROSSING SECTION
 N.T.S.



PAVED DITCH EQUIPMENT CROSSING
 N.T.S.



SPRINGFIELD AIRPORT AUTHORITY
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 SPRINGFIELD, ILLINOIS

STORM SEWER REHABILITATION - PHASE 1
 TYPICAL SECTIONS-PAVED DITCH
 DETAILS

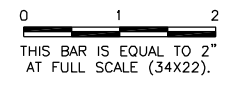
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 SHEET 15 OF 25 SHEETS

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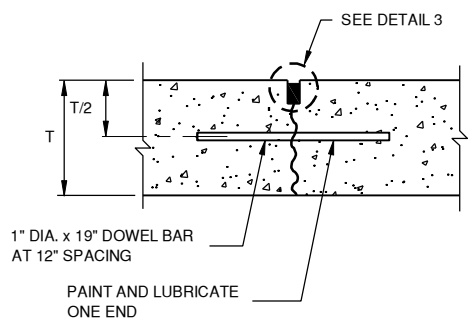


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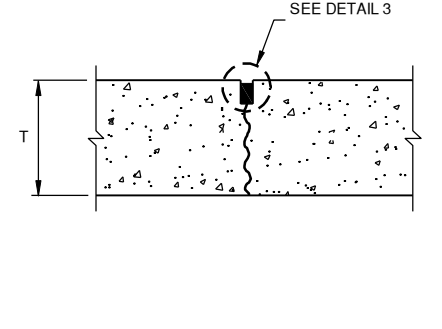
**STORM SEWER REHABILITATION - PHASE 1
 PAVED DITCH JOINTING DETAILS**

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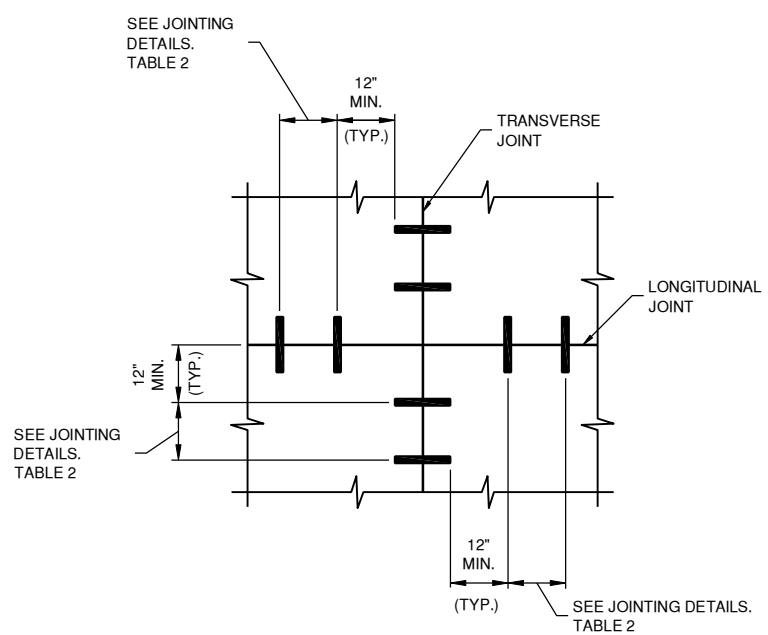
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 SHEET 16 OF 25 SHEETS



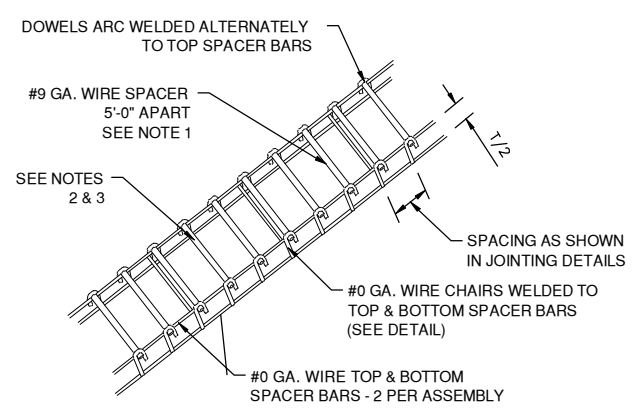
TYPE C DOWELED CONTRACTION
 SYMBOL — C —



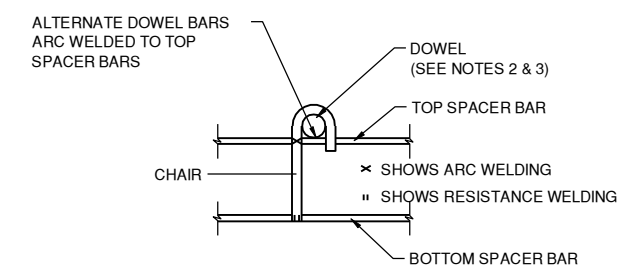
TYPE D DUMMY CONTRACTION
 SYMBOL — D —



DOWEL PLACEMENT DETAIL
 N.T.S.



DOWEL BASKET ASSEMBLY DETAIL
 N.T.S.

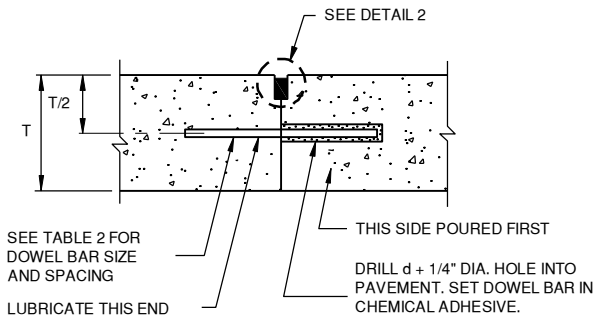


TYPICAL DOWEL BASKET ELEVATION DETAIL SHOWING CHAIR

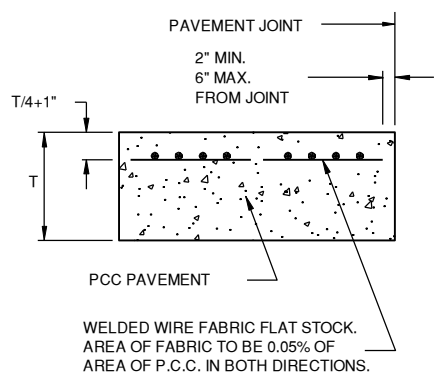
DOWEL BASKET DETAILS
 N.T.S.

DOWEL BASKET NOTES

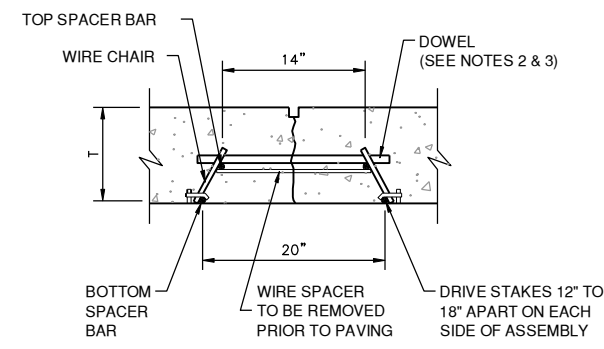
- #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
- DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 2.
- DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.



TYPE E DOWELED CONSTRUCTION
 SYMBOL — E —



ODD SHAPED PANEL REINFORCEMENT
 SYMBOL — R —

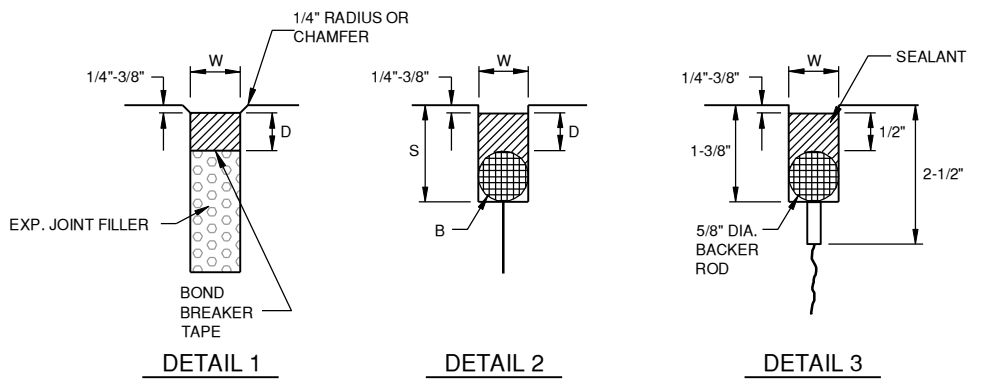


DOWEL BAR INSTALLATION DETAIL
 N.T.S.

JOINT NOTES

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- ALL JOINTS SHALL BE SEALED WITH HOT/COLD POUR SEALANT PER ITEM 605 OF THE STANDARD SPECIFICATIONS.

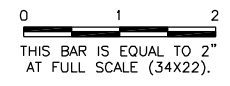
JOINT SEALING DIMENSIONS			
	DETAIL 1	DETAIL 2	DETAIL 3
W=WIDTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/2
D=DEPTH OF SEALANT RESERVOIR (IN.)	3/8	1/4	1/4
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-1/8	1-1/8



JOINT SEALING DETAILS

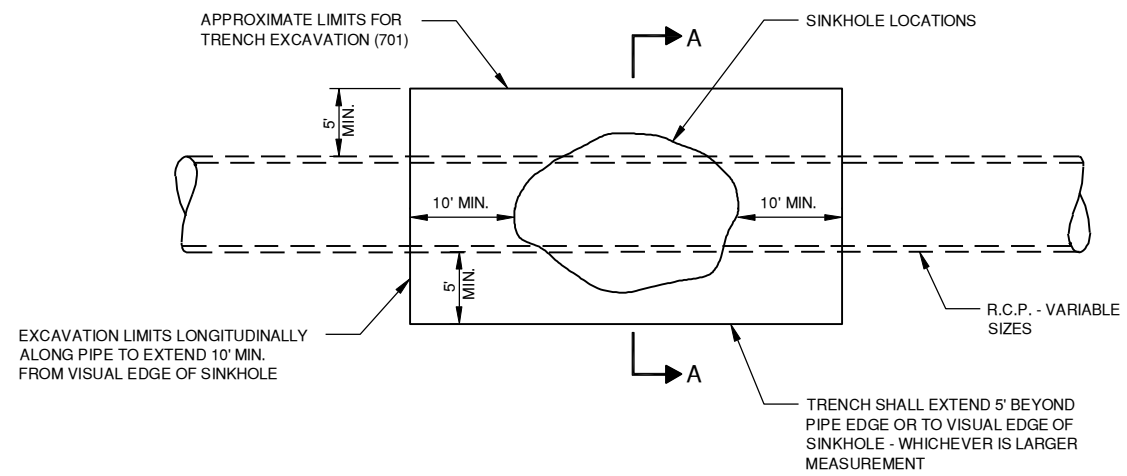
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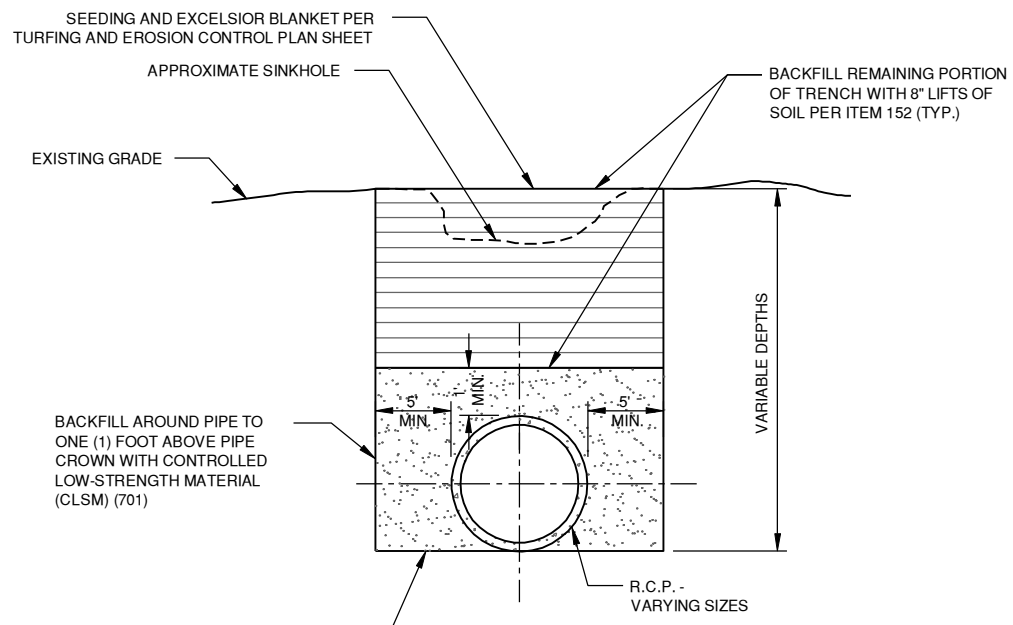


LEGEND

- EXISTING INLET STRUCTURE
- CONSOLIDATED LOW-STRENGTH MATERIAL (701)
- COMPACTED SOIL BACKFILL (152)
- EXISTING R.C.P.

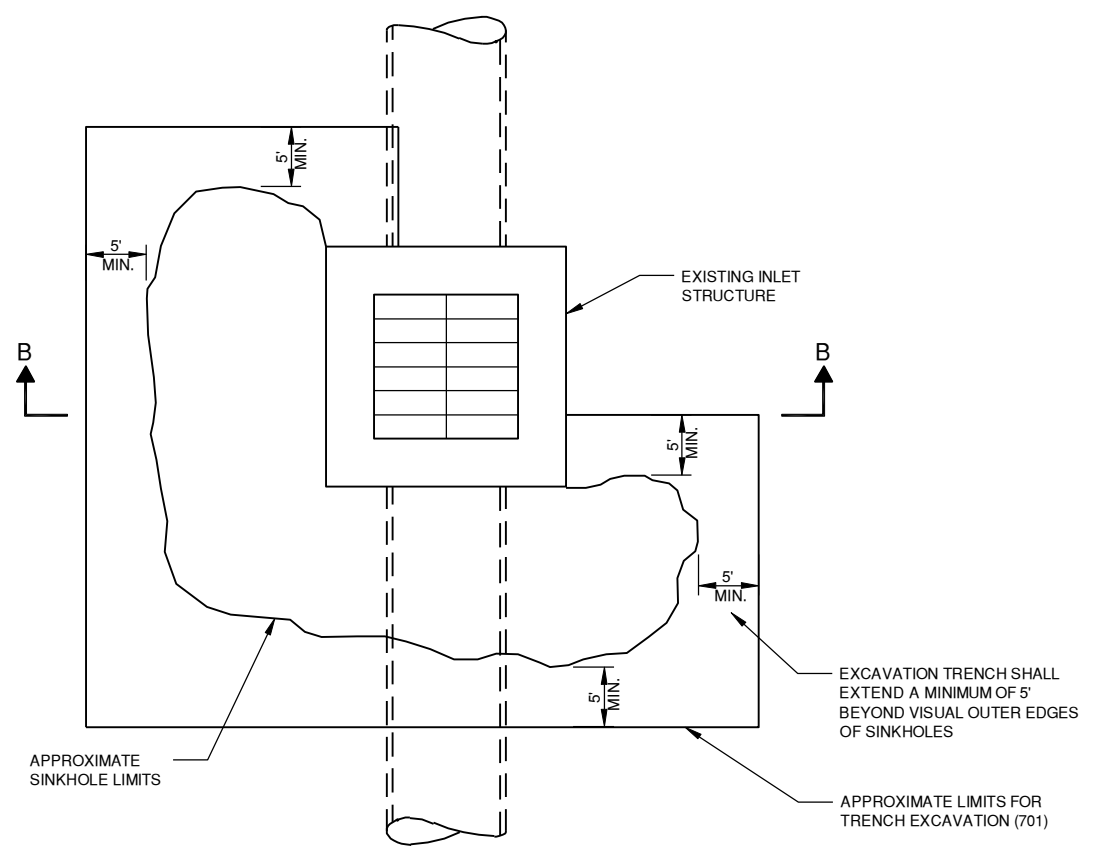


PLAN VIEW

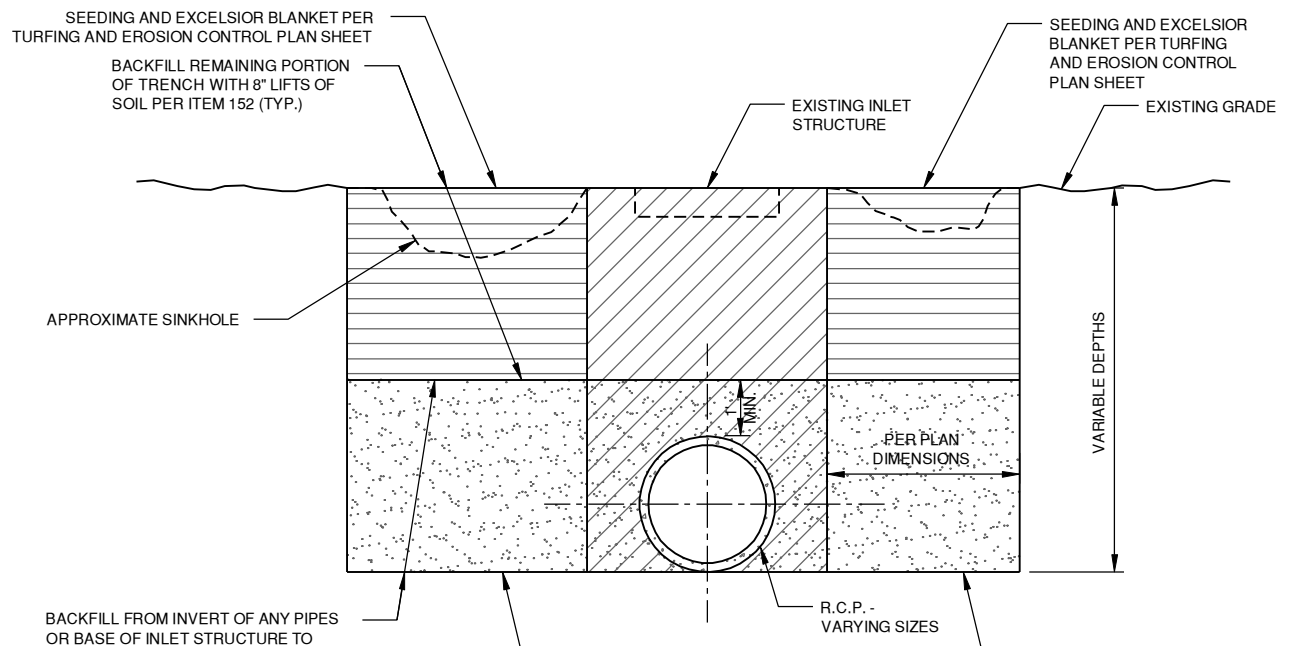


SECTION A-A

SINKHOLE MID PIPE REPAIR DETAIL
N.T.S.



PLAN VIEW



SECTION B-B

SINKHOLE NEAR INLET STRUCTURE REPAIR DETAIL
N.T.S.



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STORM SEWER REHABILITATION - PHASE 1
SINKHOLE REPAIR DETAILS

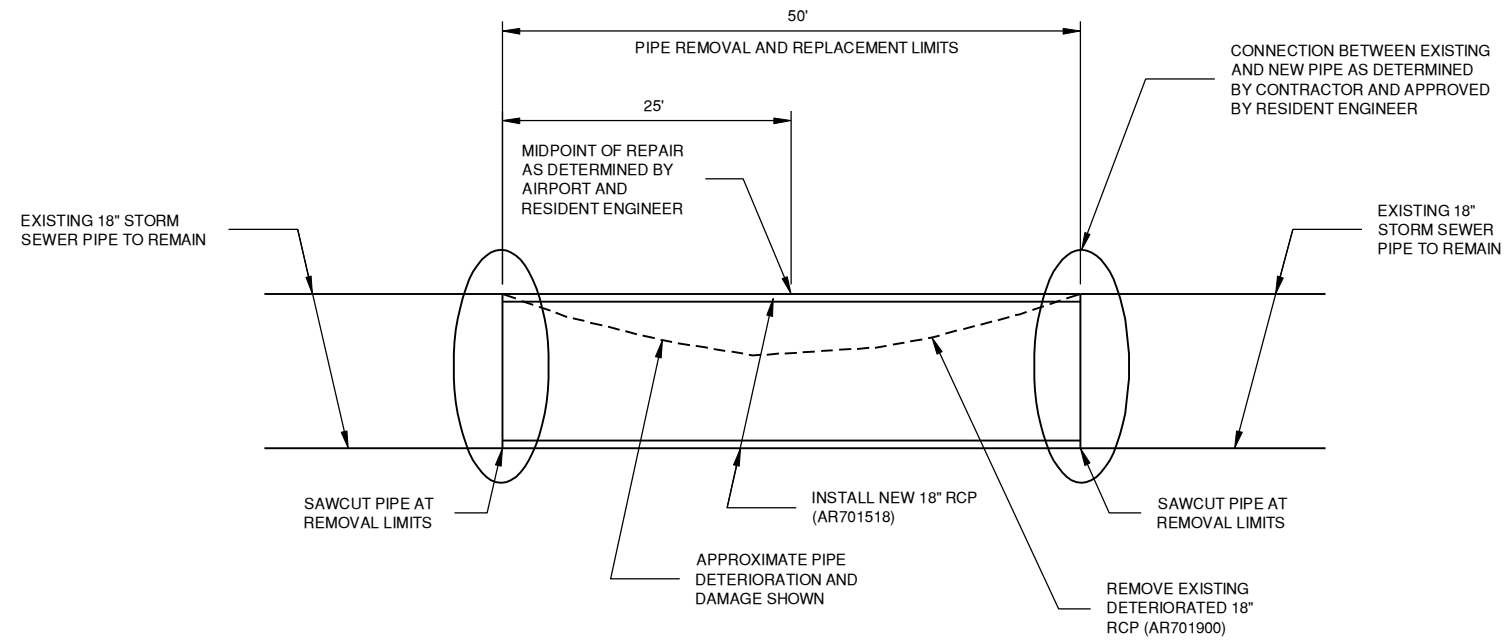
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ILLINOIS PROJECT NO. SPI-4226	
AIP PROJECT NO. 3-17-0096-XX	
SHEET 17 OF 25 SHEETS	

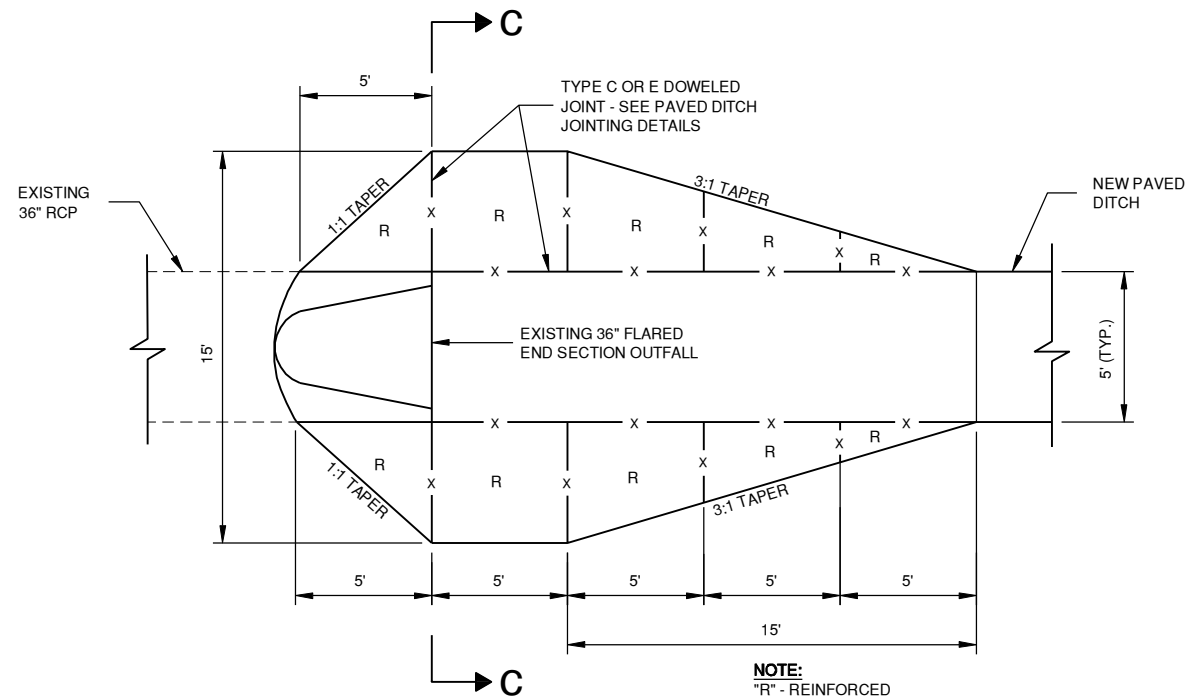
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NUMBER	BY	DATE

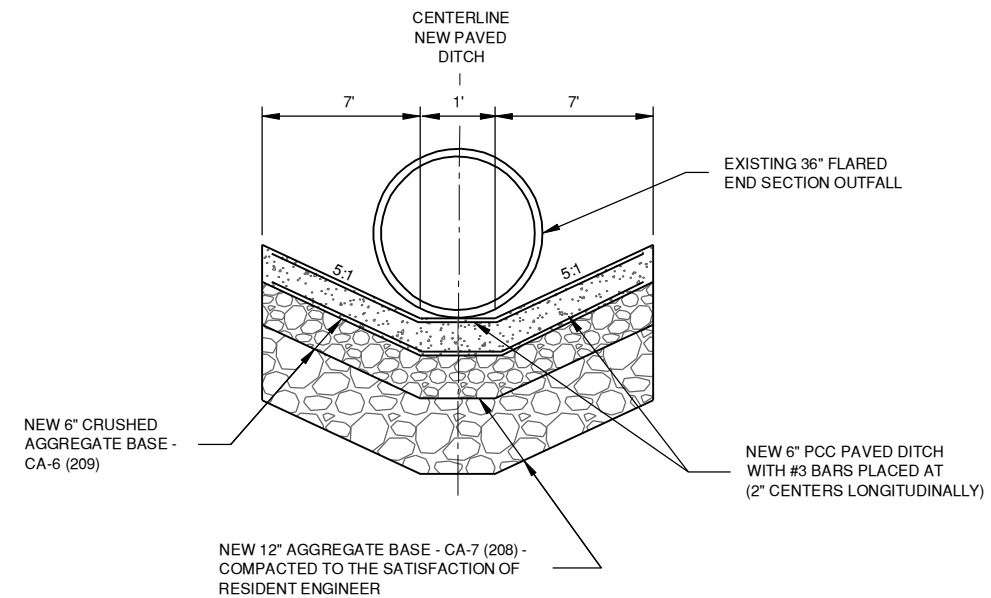
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



PIPE REMOVAL AND REPLACEMENT DETAIL
 NTS



PAVED DITCH AT OUTFALL PLAN VIEW
 NTS



PAVED DITCH OUTFALL SECTION VIEW
 NTS



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STORM SEWER REHABILITATION - PHASE 1
 MISCELLANEOUS DETAILS

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SHEET 18 OF 25 SHEETS	

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0 1 2
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 SPRINGFIELD, ILLINOIS

**STORM SEWER REHABILITATION - PHASE 1
 TURFING AND EROSION CONTROL
 PLAN & INDEX TO CROSS SECTIONS**

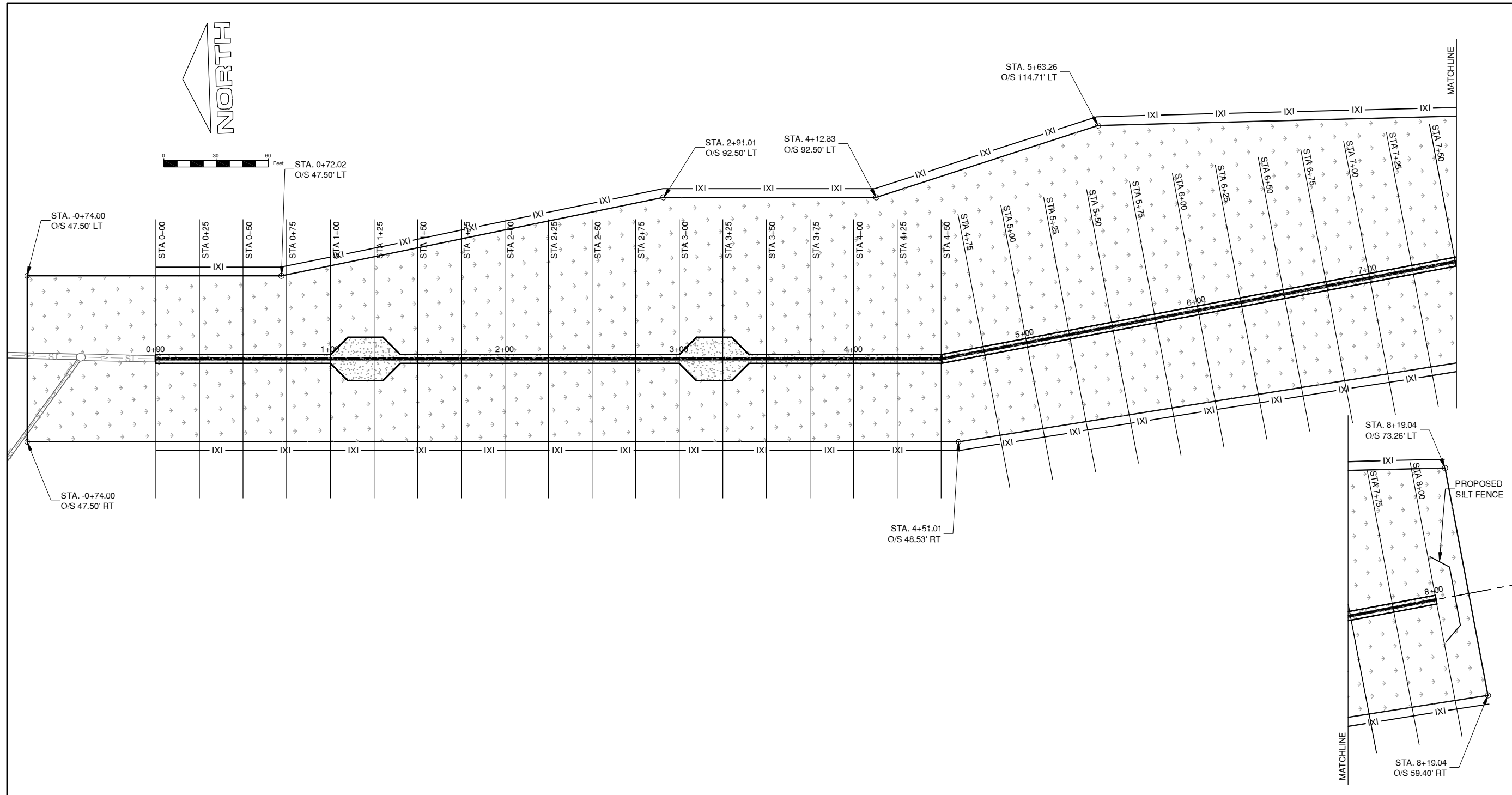
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JOB No:	04035-09


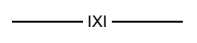
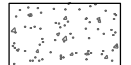
ILLINOIS PROJECT NO. SPI-4226
 AIP PROJECT NO. 3-17-0096-XX



TURFING AND EROSION CONTROL NOTES:

- ALL AREAS DISTURBED BY THE CONTRACTOR WILL BE SEEDING AND COVERED WITH EXCELSIOR BLANKET PER ITEM AR901510 AND ITEM AR908520 UNLESS NOTED OTHERWISE. USE OF OTHER METHODS OF MULCHING AS DETAILED IN THE ITEM 908 SPECIFICATIONS WILL NOT BE ALLOWED WITHIN THE PAVED DITCH WORK AREA OR THE SINKHOLE REPAIR AREAS.
- ONLY SEEDING AND EXCELSIOR BLANKET IN THE AREA OF THE PAVED DITCH WORK AND AS SHOWN ON THIS SHEET WILL BE MEASURED AND PAID FOR UNDER THEIR RESPECTIVE PAY ITEMS.
- ALL SEEDING AND EXCELSIOR BLANKET IN THE SINKHOLE REPAIR AREAS SHALL BE INCIDENTAL TO THE CONTRACT AND SHALL BE IN ACCORDANCE WITH ITEM 901 AND ITEM 908. SEE NOTE 2.
- ALL STAGING AND STORAGE AREAS SHALL BE REPAIRED TO THEIR PRE-CONSTRUCTION CONDITIONS AND TO THE APPROVAL OF THE AIRPORT WITH SEEDING AND EXCELSIOR BLANKET IN ACCORDANCE WITH ITEM 901 AND ITEM 908. THIS WILL BE INCIDENTAL TO THE CONTRACT AND NO MEASUREMENTS WILL BE MADE FOR PAYMENT FOR REPAIR OF THESE AREAS.
- THE SILT FENCE SHALL BE PLACED AT GENERALLY THE LIMITS OF THE SEEDING AND EXCELSIOR BLANKET AREA AS SHOWN OR AS DIRECTED BY THE RESIDENT ENGINEER.
- AN ADDITIONAL QUANTITY OF 200LF SILT FENCE WHICH ARE NOT SHOWN ON THIS SHEET HAS BEEN INCLUDED IN THE PLAN QUANTITY TO BE PLACED AT THE DIRECTION OF THE RESIDENT ENGINEER AS NEEDED. THIS MAY INCLUDE PROTECTION OF THE EXCESS SOIL STOCKPILE AREA.

LEGEND

-  SEEDING AND EXCELSIOR BLANKET AREA
-  PROPOSED SILT FENCE
-  PROPOSED PAVED DITCH

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 AT FULL SCALE (34X22).

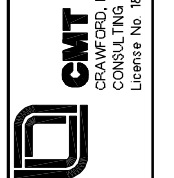


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 SPRINGFIELD, ILLINOIS**

STORM SEWER REHABILITATION - PHASE 1

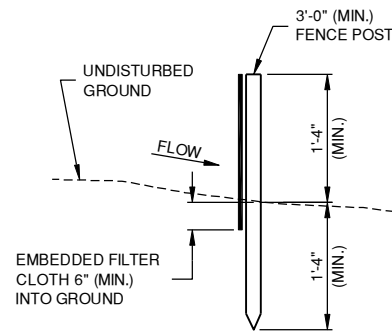
EROSION CONTROL DETAILS

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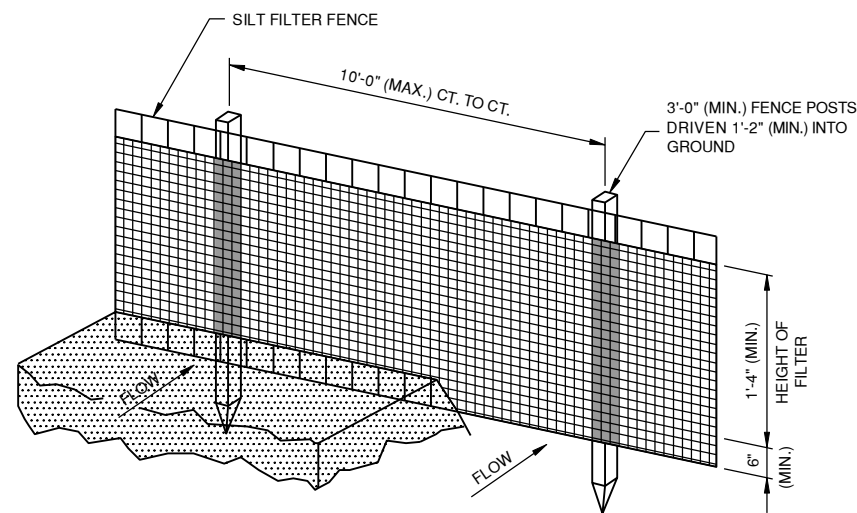


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ILLINOIS PROJECT NO. SPI-4226 AIP PROJECT NO. 3-17-0096-XX	
SHEET	20 OF 25 SHEETS



SECTION



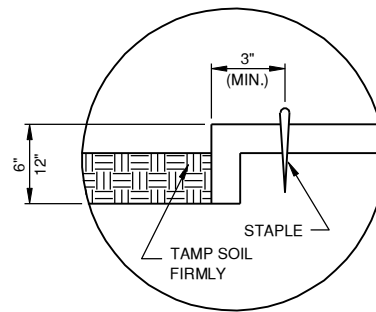
PERSPECTIVE VIEW

EROSION CONTROL FABRIC FENCE DETAILS

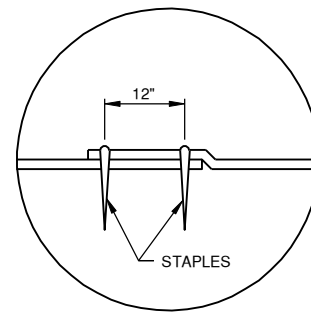
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EROSION CONTROL FABRIC FENCE NOTES

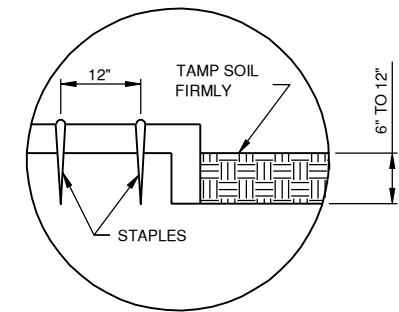
1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.



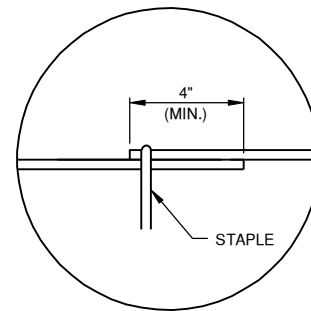
DETAIL 1 - TERMINAL FOLD



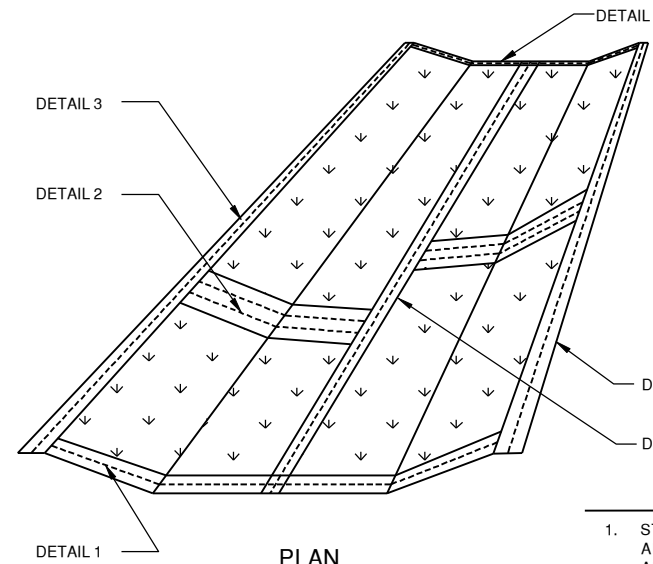
DETAIL 2 - JUNCTION SLOT



DETAIL 3 - ANCHOR SLOT



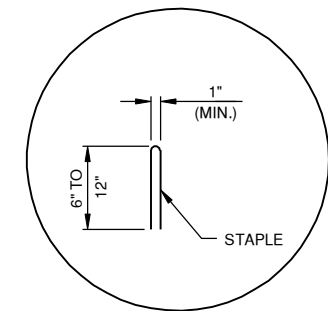
DETAIL 4 - LAP JOINT



PLAN

EXCELSIOR BLANKET DETAILS

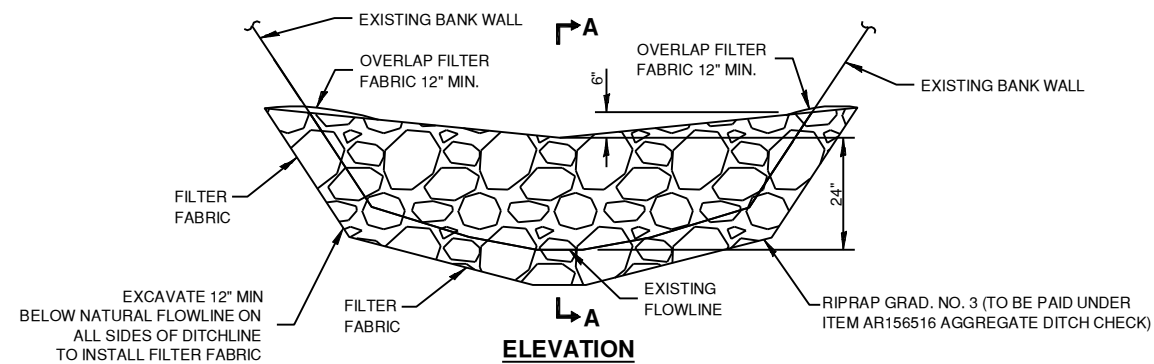
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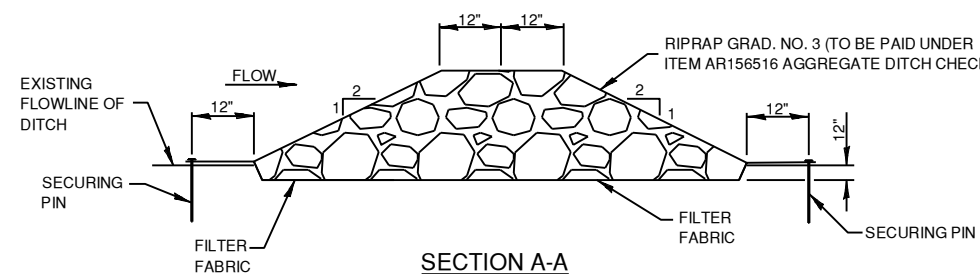
DETAIL 5 - STAPLE DETAIL

EXCELSIOR BLANKET NOTES

1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINALS ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.



ELEVATION



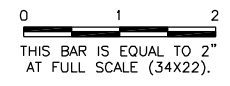
SECTION A-A

AGGREGATE DITCH CHECK

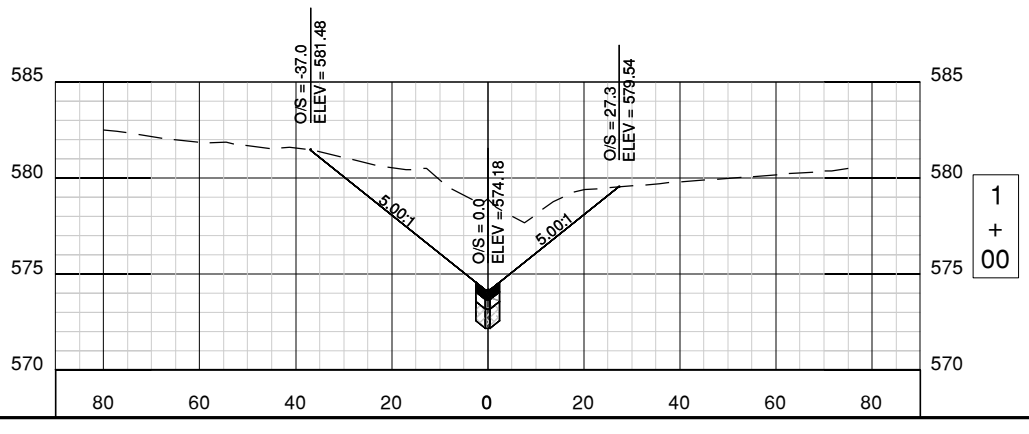
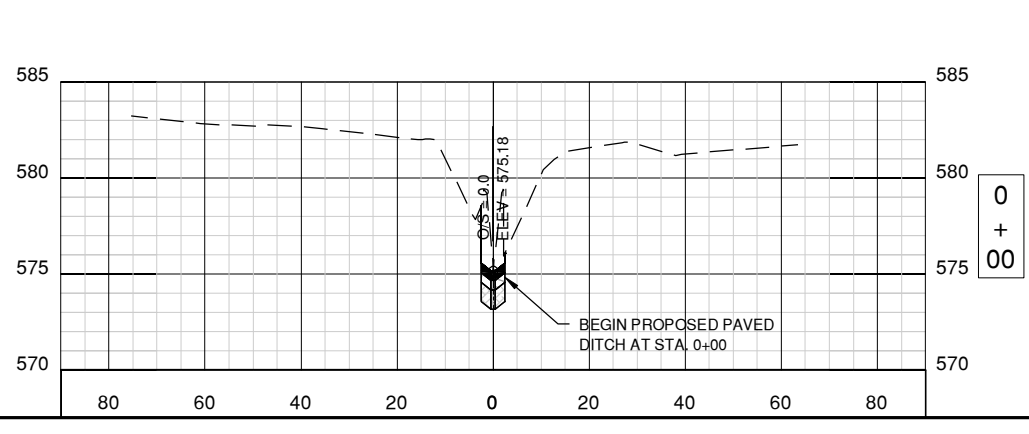
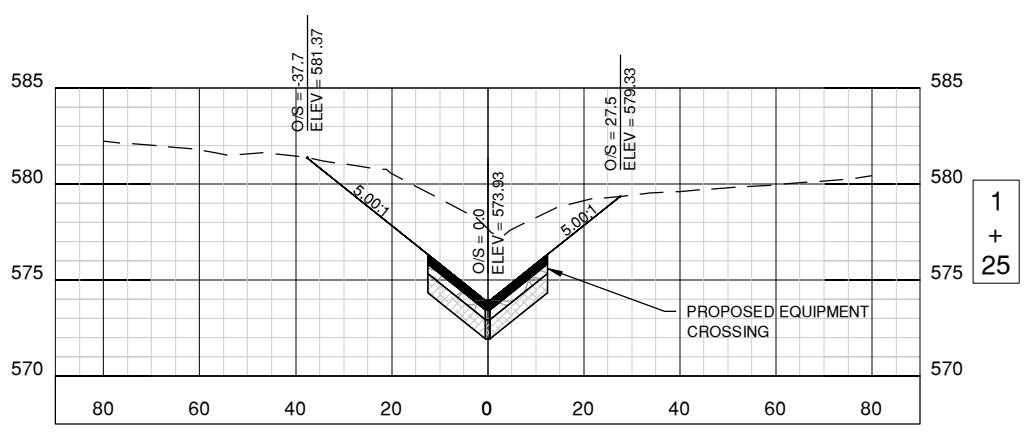
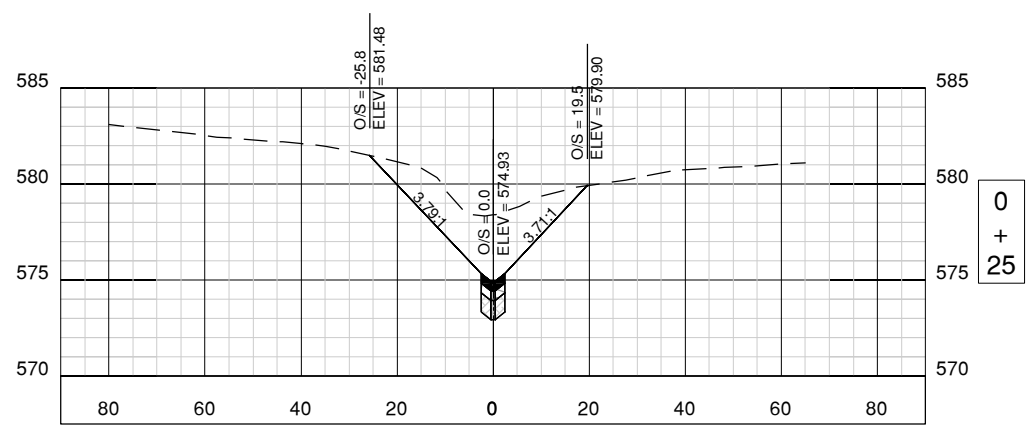
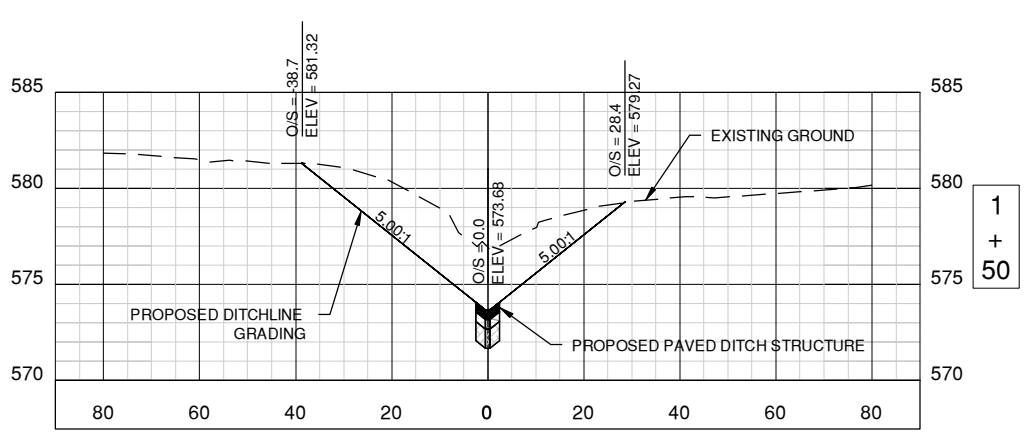
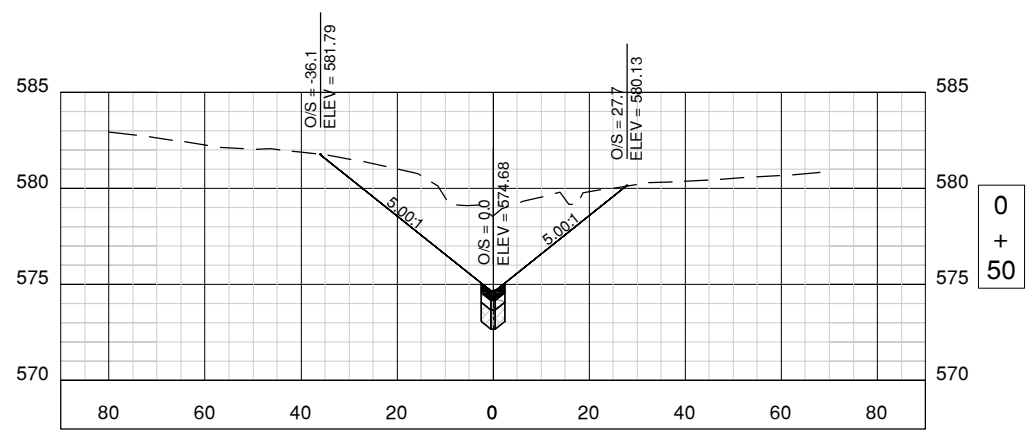
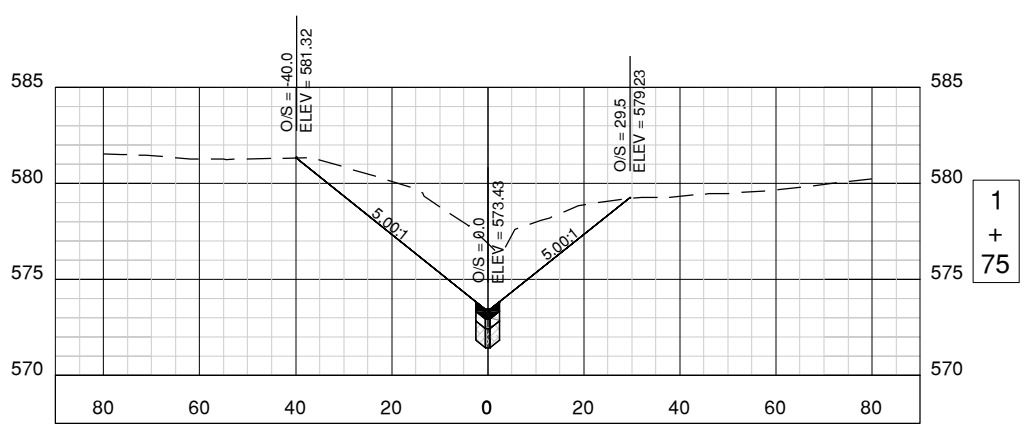
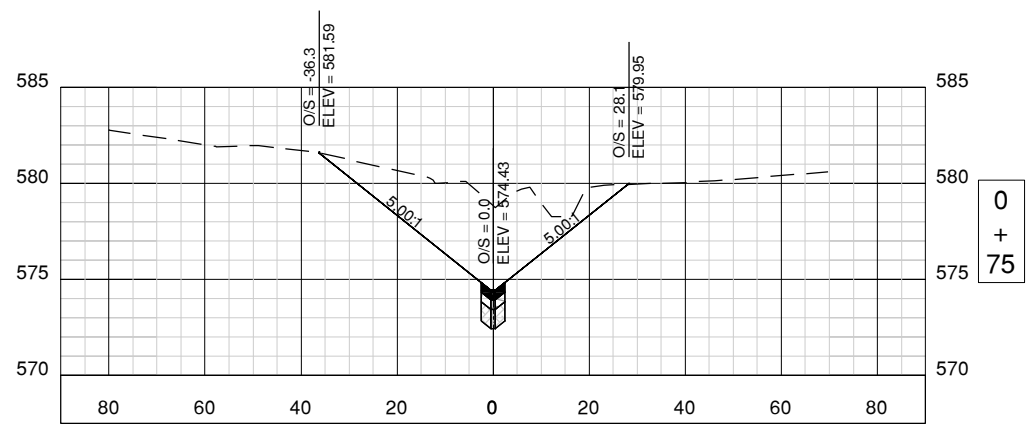
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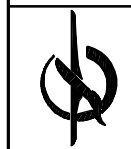
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NUMBER	BY	DATE



PAVED DITCH - SHEET 21		
STATION	EXCAVATE END AREA (S.F.)	EMBANKMENT END AREA (S.F.)
0+00	22.0	0.0
0+25	99.3	0.0
0+50	155.4	0.0
0+75	162.4	0.0
1+00	160.0	0.0
1+25	193.9	0.0
1+50	154.7	0.0
1+75	166.3	0.0



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**STORM SEWER REHABILITATION - PHASE 1
 CROSS SECTIONS-PAVED DITCH STA. 0+00 TO
 STA. 1+75**

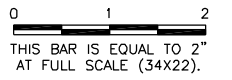
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APPROVED BY:	RLV
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JOB No:	04035-09
ILLINOIS PROJECT NO.	SPI-4226
AIP PROJECT NO.	3-17-0096-XX
SHEET	21 OF 25 SHEETS

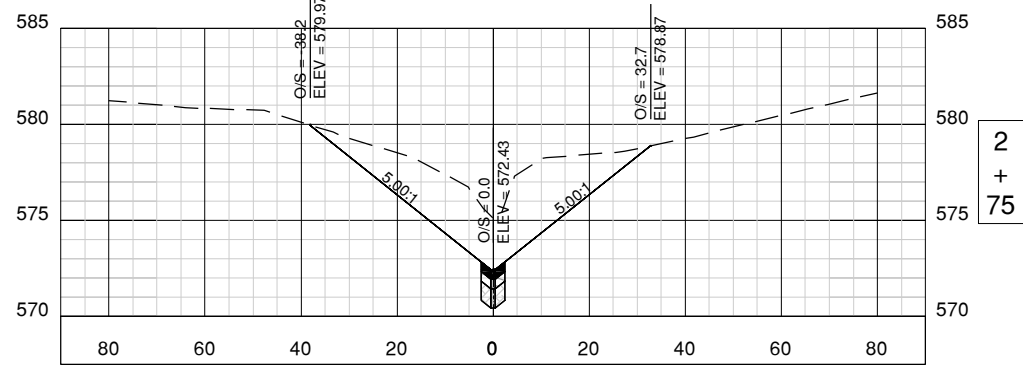
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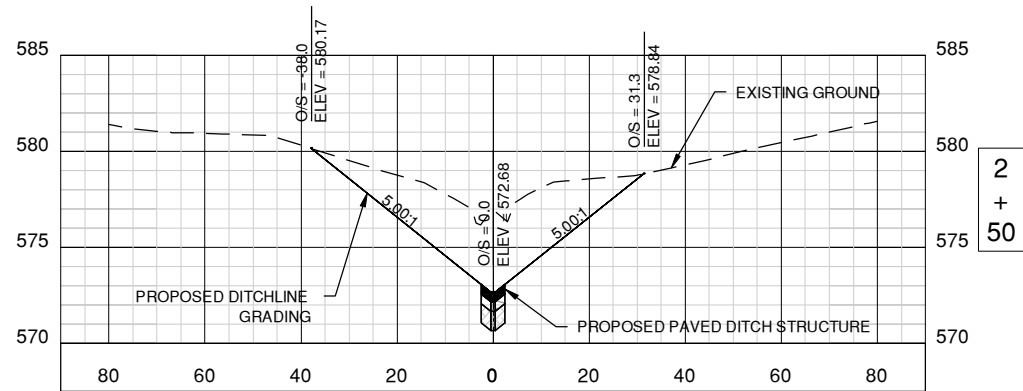
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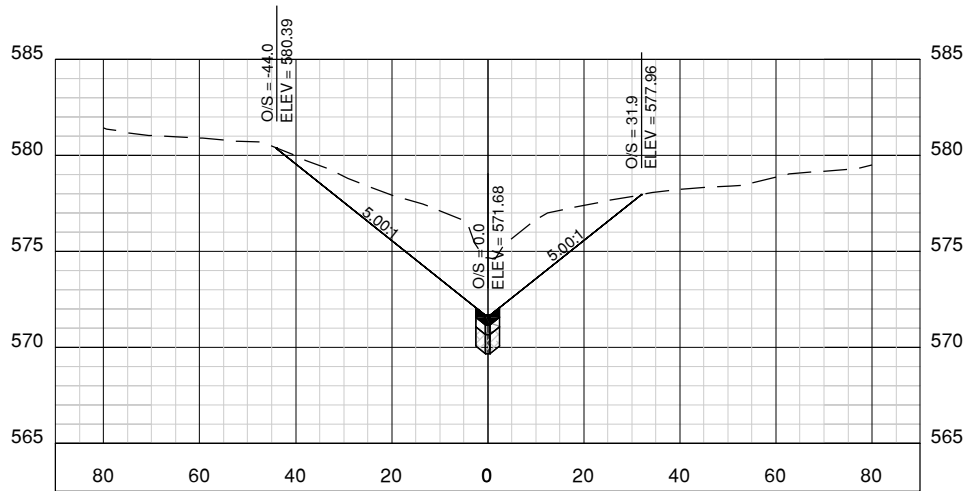
PAVED DITCH - SHEET 22		
STATION	EXCAVATE END AREA (S.F.)	EMBANKMENT END AREA (S.F.)
2+00	162.2	0.0
2+25	155.6	0.0
2+50	168.0	0.0
2+75	166.8	0.0
3+00	168.2	0.0
3+25	204.6	0.0
3+50	170.4	0.0



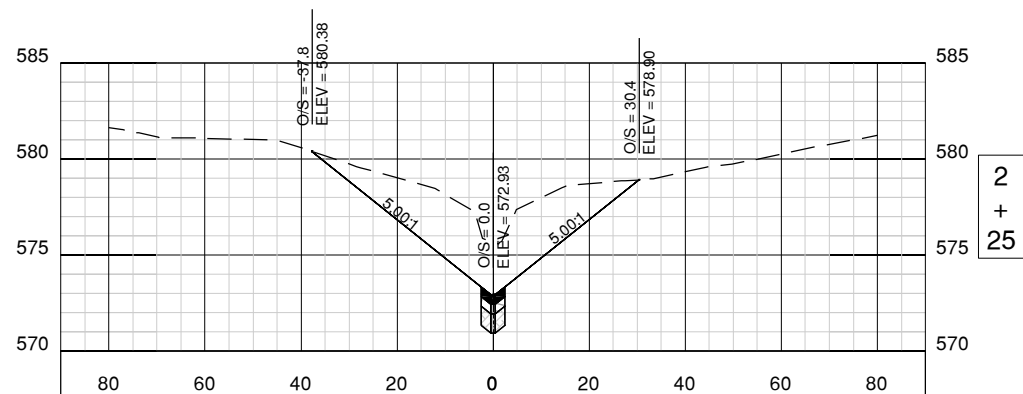
2
+
75



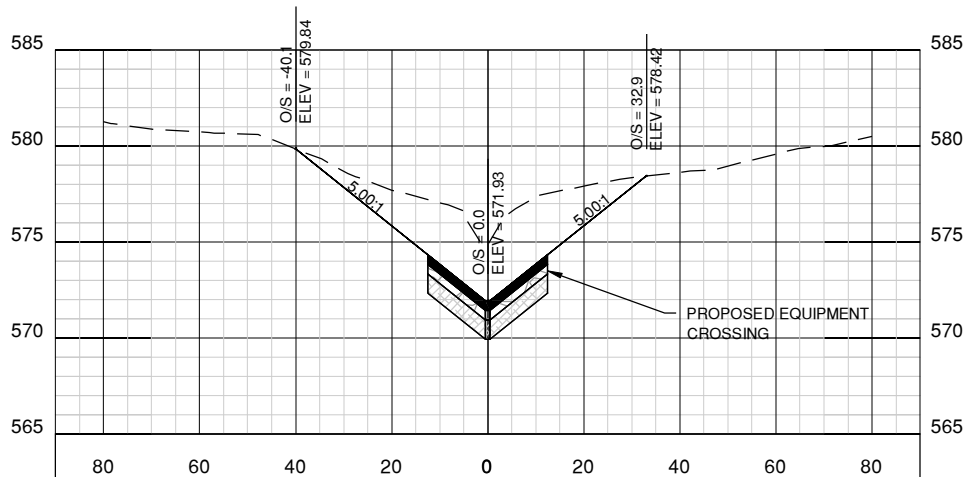
2
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50



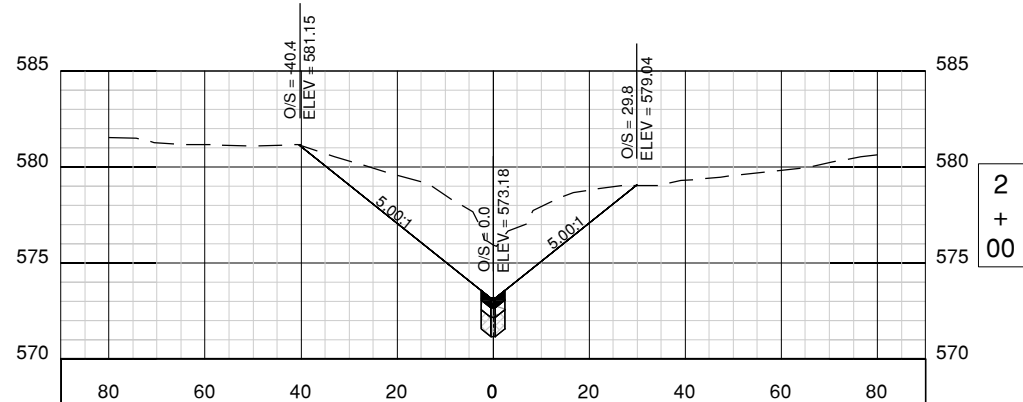
3
+
50



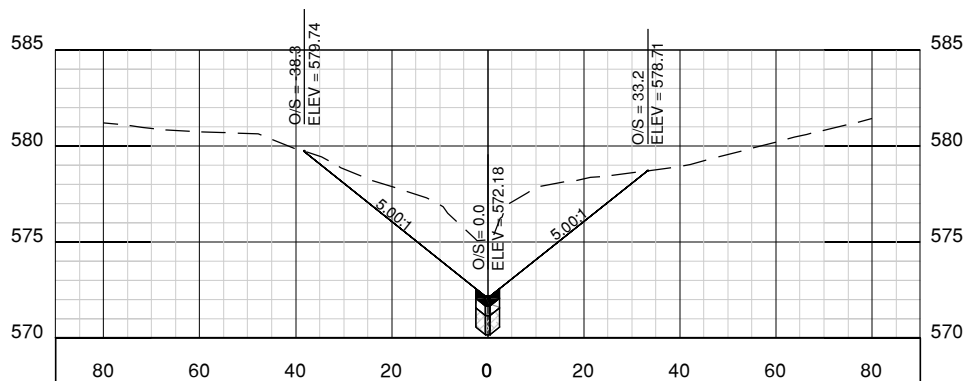
2
+
25



3
+
25



2
+
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3
+
00



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STORM SEWER REHABILITATION - PHASE 1
 CROSS SECTIONS-PAVED DITCH STA. 2+00 TO
 STA. 3+50

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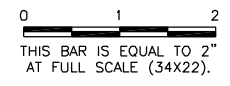


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ILLINOIS PROJECT NO. SPI-4226 AIP PROJECT NO. 3-17-0096-XX	
SHEET 22	OF 25 SHEETS

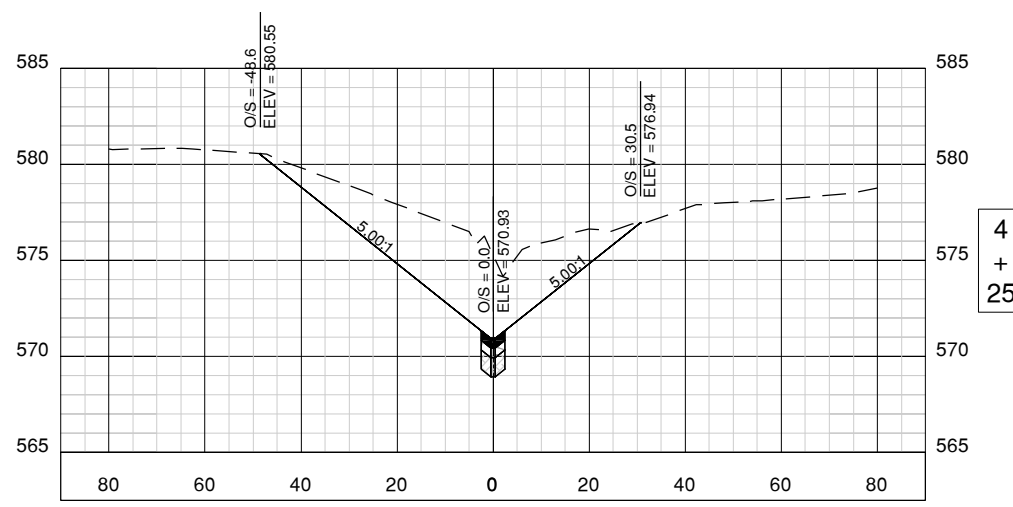
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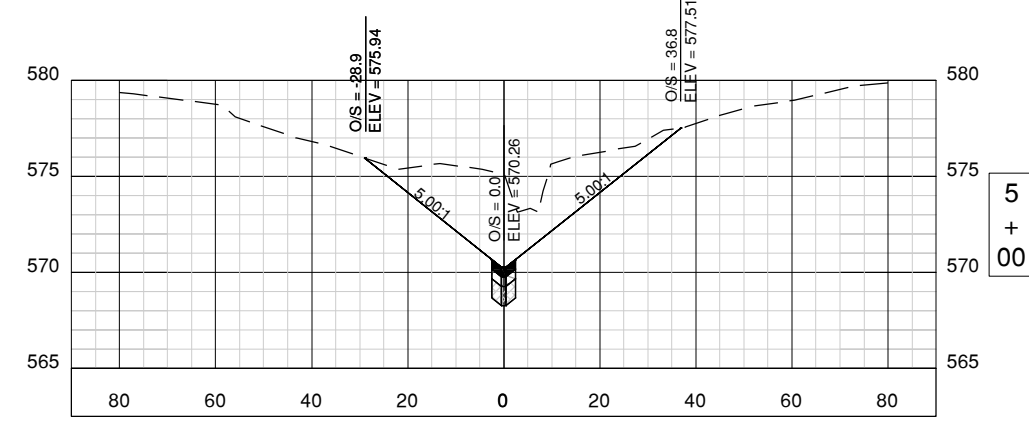
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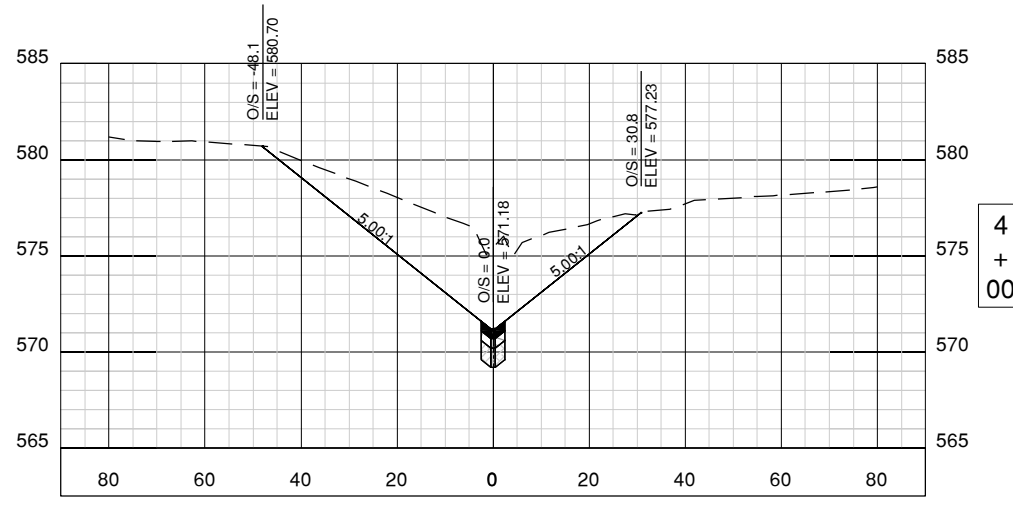
PAVED DITCH - SHEET 23		
STATION	EXCAVATE END AREA(S.F.)	EMBANKMENT END AREA(S.F.)
3+75	178.5	0.0
4+00	198.9	0.0
4+25	204.1	0.0
4+50	179.2	0.0
4+75	158.0	0.0
5+00	153.9	0.0



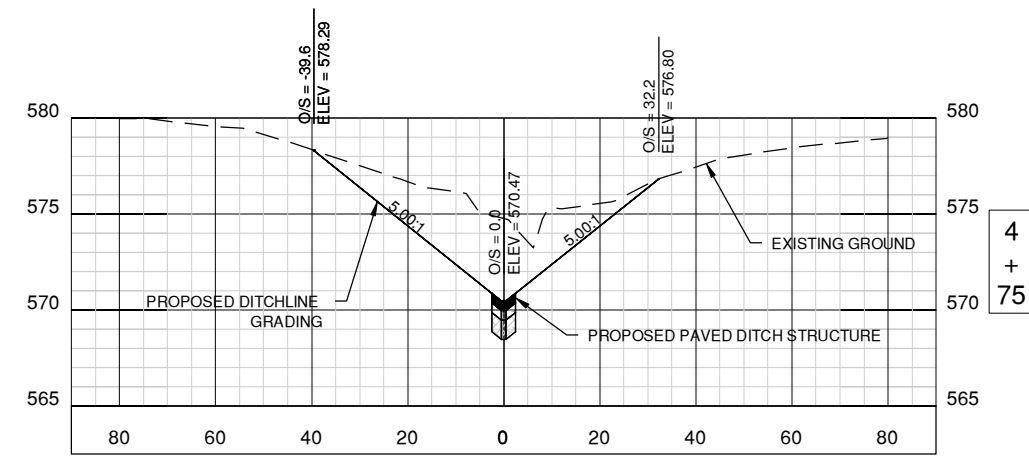
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+
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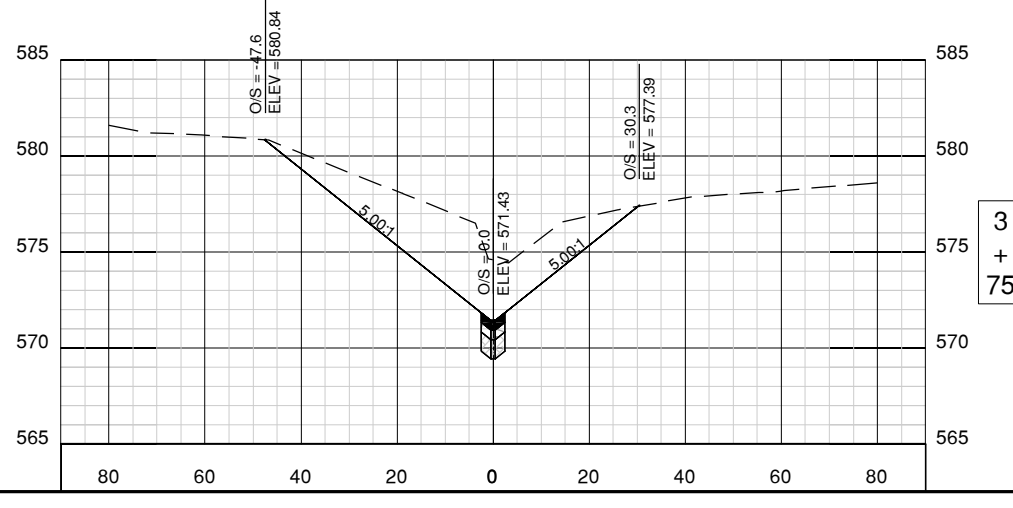
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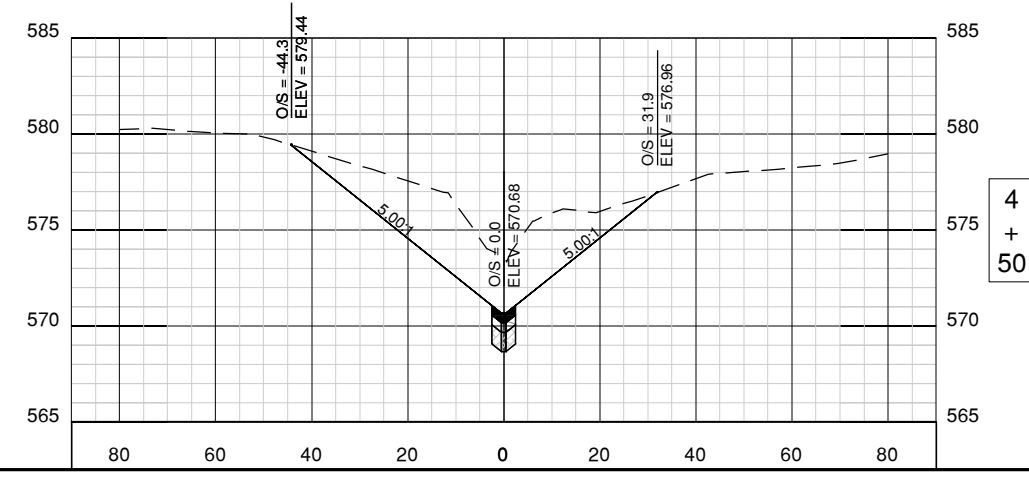
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75



3
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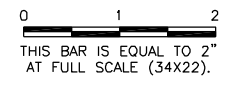
**STORM SEWER REHABILITATION - PHASE 1
 CROSS SECTIONS-PAVED DITCH STA. 3+75 TO
 STA. 5+00**



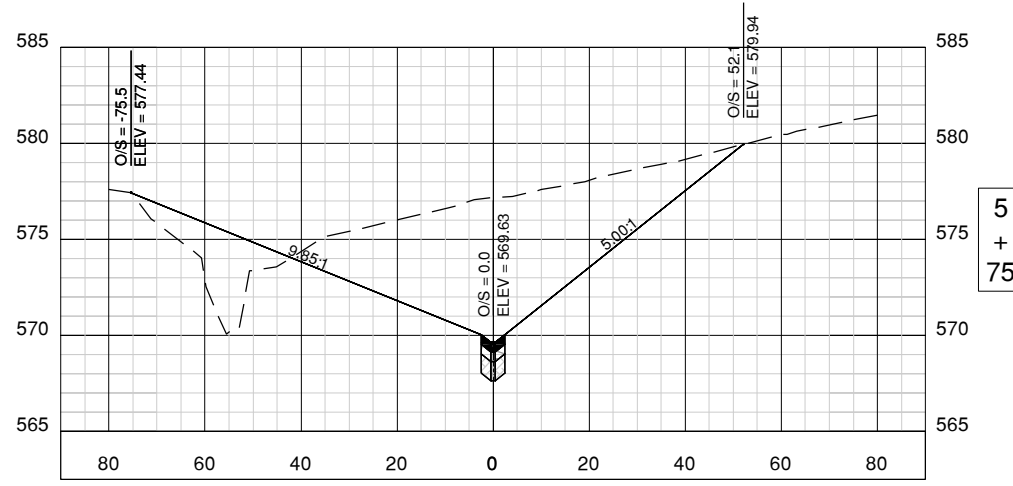
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ILLINOIS PROJECT NO.	SPI-4226
AIP PROJECT NO.	3-17-0096-XX
SHEET	23 OF 25 SHEETS

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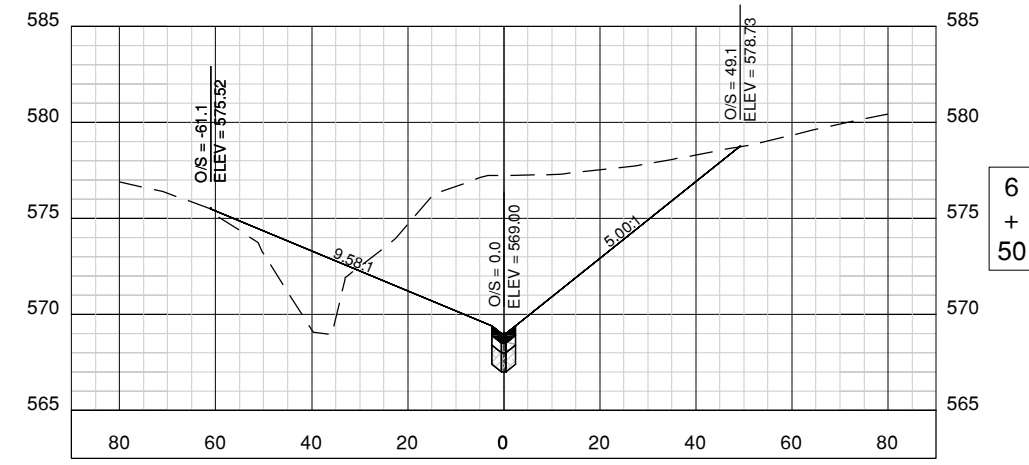
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NUMBER	BY	DATE



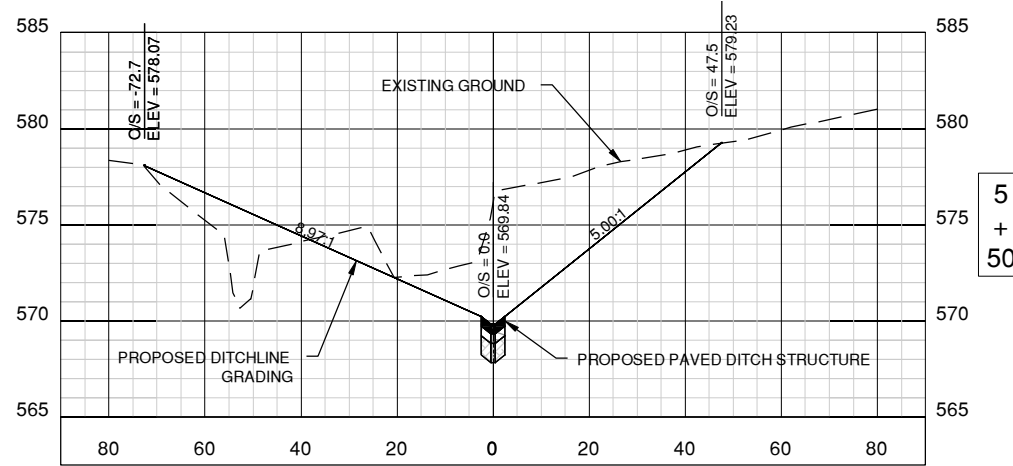
PAVED DITCH - SHEET 24		
STATION	EXCAVATE END AREA(S.F.)	EMBANKMENT END AREA(S.F.)
5+25	188.6	20.1
5+50	231.2	55.7
5+75	372.3	65.8
6+00	404.7	73.2
6+25	377.8	66.3
6+50	348.3	58.5



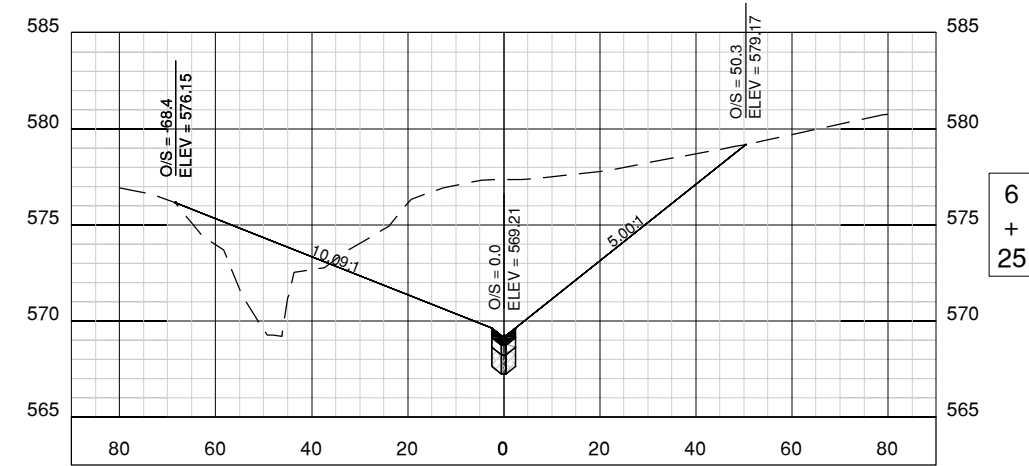
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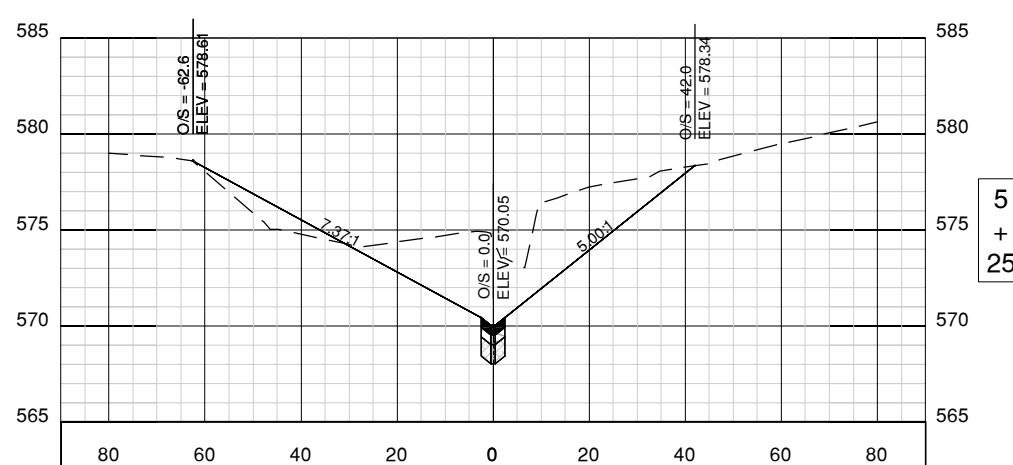
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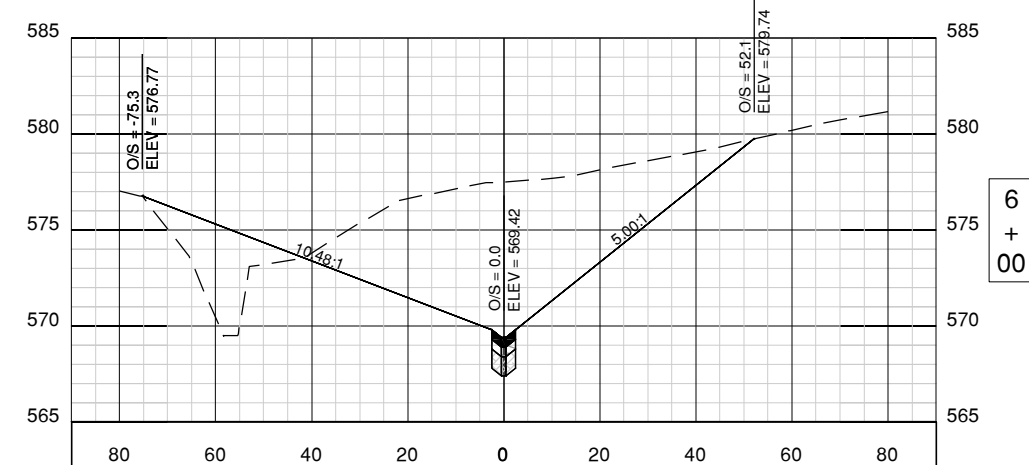
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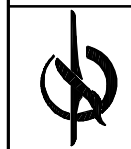


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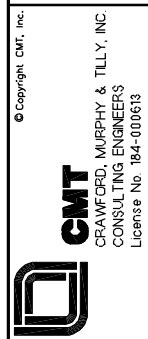
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STORM SEWER REHABILITATION - PHASE 1
 CROSS SECTIONS-PAVED DITCH STA. 5+25 TO
 STA. 6+50



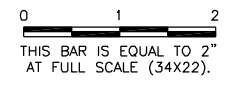
CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	AJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	APRIL 18, 2014
JOB No:	04035-09
ILLINOIS PROJECT NO. SPI-4226	
AIP PROJECT NO. 3-17-0096-XX	
SHEET 24 OF 25 SHEETS	

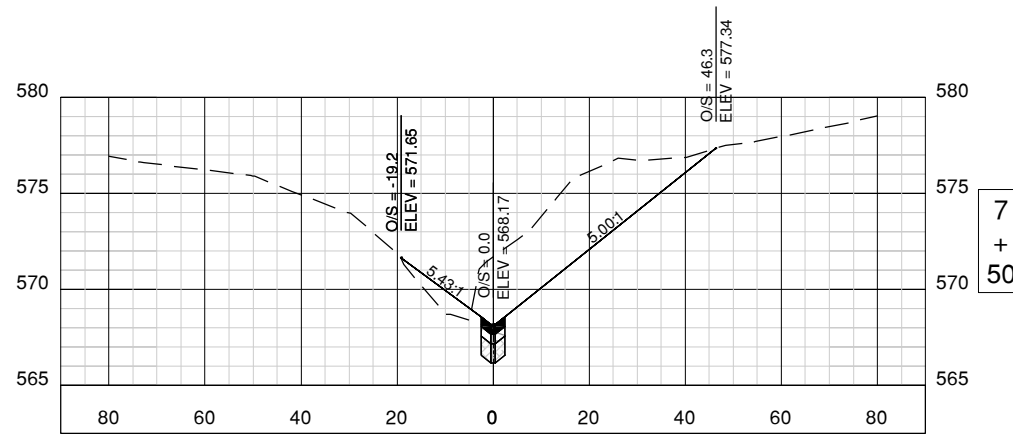
CA017

REVISIONS

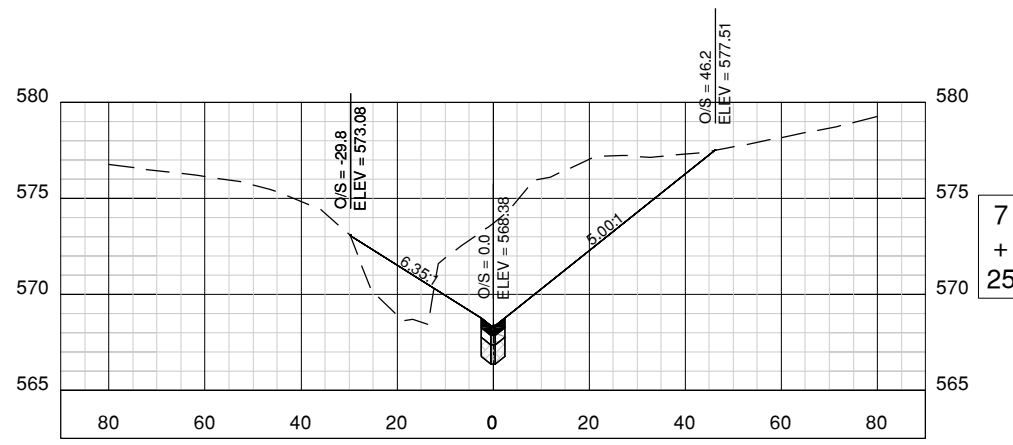
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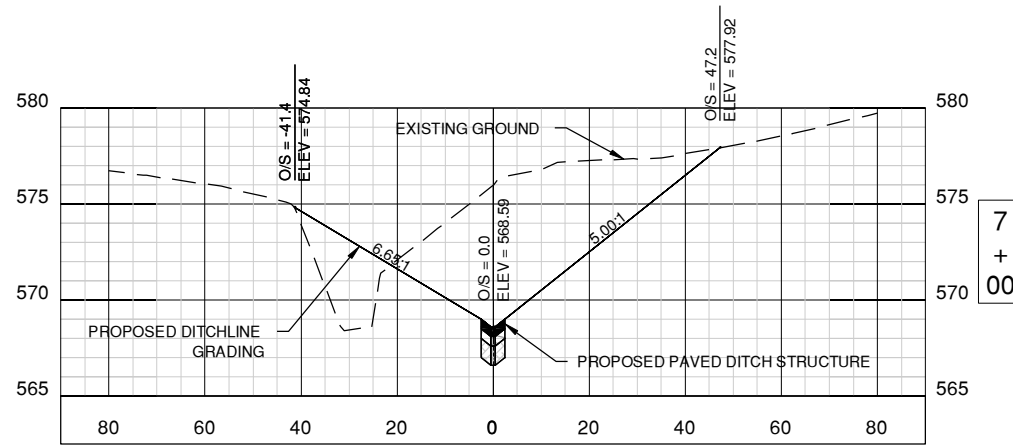
PAVED DITCH - SHEET 25		
STATION	EXCAVATE END AREA (S.F.)	EMBANKMENT END AREA (S.F.)
6+75	316.6	51.2
7+00	276.9	56.8
7+25	220.2	34.3
7+50	152.7	10.8
7+75	93.0	7.9
8+00	11.8	0.9



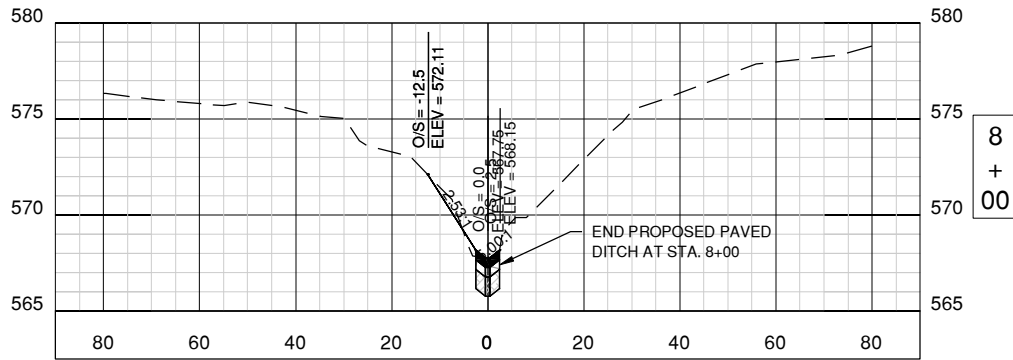
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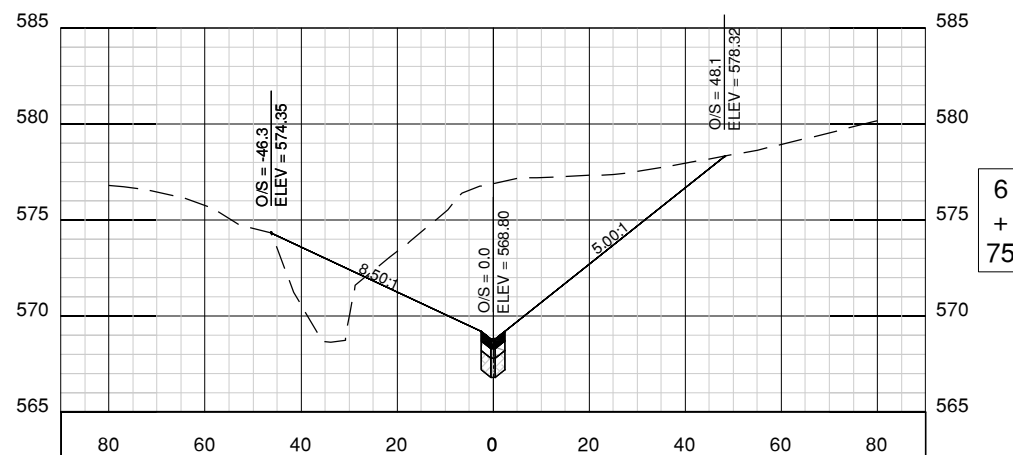
7
+
25



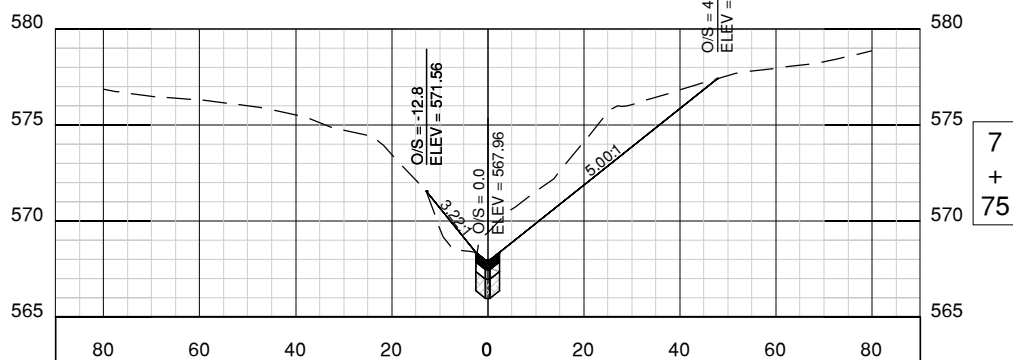
7
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8
+
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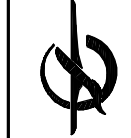


6
+
75



7
+
75

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SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

STORM SEWER REHABILITATION - PHASE 1
 CROSS SECTIONS-PAVED DITCH STA. 6+75 TO
 STA. 8+00

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ILLINOIS PROJECT NO.	SPI-4226
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SHEET	25 OF 25 SHEETS