06-13-14 LETTING ITEM 222

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE VILLAGE OF RIVERSIDE

TRAFFIC DATA:

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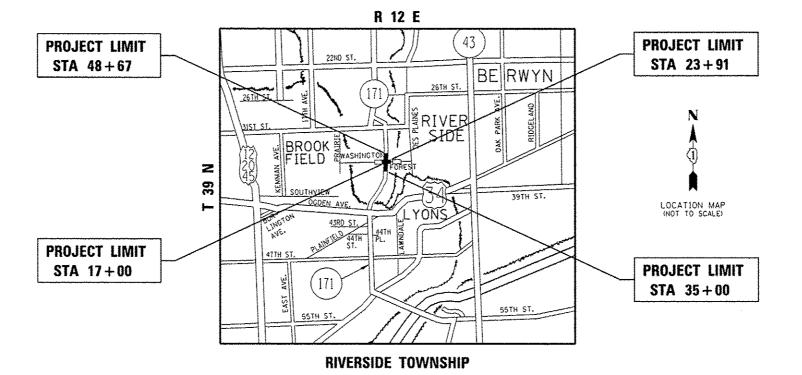
IL RTE. 171 2013 ADT = 42,700SPEED LIMIT = 35 MPH

FOREST AVE./RIDGEWOOD RD.

2010 ADT = 6.300SPEED LIMIT = 25 MPH

PROPOSED HIGHWAY PLANS

F.A.P. RTE. 372 (IL RTE. 171) AT FOREST AVE./RIDGEWOOD RD. SECTION 0303S-TS PROJECT: ACHSIP-0372 (011) TRAFFIC SIGNAL MODERNIZATION AND CHANNELIZATION **COOK COUNTY** C-91-436-13



GROSS AND NET LENGTH = 2058 FT. = 0.390 MILE

03035-TS COOK ILLINOIS CONTRACT NO. 60X08

D-91-436-13

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION SUBMITTED More H 21 20 14 DEPUTY/DIRECTOR OF HIGHWAYS, REGION ENGINEER

LOCATION OF SECTION INDICATED THUS: - -

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

FILL SIZE PLANS HAVE REEN PREPARED LISING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION 1-800-892-0123

PROJECT ENGINEER: DAN WILGREEN (847) 705-4240 PROJECT MANAGER: KEN ENG (847) 705-4247

CONTRACT NO. 60X08

	INDEX OF SHEETS		STATE STANDARDS
SHEET NO.	DESCRIPTION	STANDARD NO.	DESCRIPTION
1	COVER SHEET	000001-06	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
2	INDEX OF SHEETS, STATE STANDARDS, GENERAL NOTES, AND COMMITMENTS	280001-07	TEMPORARY EROSION CONTROL SYSTEMS
3-15	SUMMARY OF QUANTITIES	424001-07	PERPENDICULAR CURP RAMPS FOR SIDEWALKS
16-20	EXISTING AND PROPOSED TYPICAL SECTIONS	424011-01	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
21	SCHEDULE OF QUANTITIES	424021-02	DEPRESSED CORNER FOR SIDEWALKS
22-23	ALICNMENT, TIES. AND BENCHMARKS	442201-03	CLASS C AND D PATCHES
24-26	EXISTING AND PROPOSED ROADWAY PLANS AND PROFILE	601001-04	SUB-SURFACE DRAINS
27-31	SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL	601101-01	CONCRETE HEADWALL FOR PIPE DRAIN
32	EROSION AND SEDIMENT CONTROL PLANS	602001-02	CATCH BASIN TYPE A
33-36	EXISTING AND PROPOSED DRAINAGE AND UTILITIES PLANS	602011-02	CATCH BASIN TYPE C
37-38	SUE PLANS	602401-03	MANHOLE TYPE A
39-40	RIGHT-OF-WAY SHEETS	602601-03	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
41	PAVEMENT MARKING PLANS	604001-03	FRAME AND LIDS, TYPE 1
42	LANDSCAPING PLANS	604061-02	FRAME AND GRATE TYPE 12
43-49	DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS	604086-02	FRAME AND GRATE TYPE 23
50-58	TRAFFIC SIGNAL PLANS	606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
59	MAST ARM MOUNTED STREET NAME SIGNS	635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
60	COMBINATION LIGHTING PLAN	635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
61-62	DISTRICT 1 STANDARD COMBINATION LIGHTING DESIGN DETAILS	701101-04	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
63-65	SIGNING PLANS	701301-04	LANE CLOSURE, 2L, 2W, SHORT TME OPERATIONS
66-67	SIDEWALK DETAIL PLANS	701311-03	LANE CLOSURE, 2L. ZW. MOVING OPERATIONS - DAY ONLY
68	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB	701427-02	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS <= 40 MPH
60	AND EDGE OF SHOULDER >= 15' (4.5 m) (80-01)	701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
69 70	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8) MANHOLE WITH RESTRICTOR PLATE (BD-12)	701602-07	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
71	PAYEMENT PATCHING FOR HMA SURFACED PAYEMENT (BD-22)	701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
72	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)	701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
73	BUTT JOINT AND HMA TAPER DETAILS (BD-32)	701901-03	TRAFFIC CONTROL DEVICES
	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS.	720001-01	SIGN PANEL MOUNTING DETAILS
74	INTERSECTIONS AND DRIVEWAYS (TC-10)	720006-04	SIGN PANEL ERECTION DETAILS
75	TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)	728001-01	TELESCOPING STEEL SIGN SUPPORT
76	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)	731001-01	BASE FOR TELESCOPING STEEL SIGN SUPPORT
77	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)	780001-04	TYPICAL PAVEMENT MARKINGS
78	ARTERIAL ROAD INFORMATION SIGN (TC-22)	814001-02	HANDHOLES
79	DRIVEWAY ENTRANCE SIGNING (TC-26)	814006-02	DOUBLE HANDHOLES
80	DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING (TS-07)	857001-01	STANDARD SIGNAL DESIGNATION DIAGRAMS AND PHASE SEQUENCES
81-87	CROSS SECTIONS	862001-01	UNINTERRUPTABLE POWER SUPPLY (UPS)
01-01	Chops Secritors	877011-05	STEEL COMB. MAST ARM ASSEMBLY AND POLE 16' THOUGH 55'.
		878001-09	CONCRETE FOUNDATION DETAILS
			GENERAL NOTES
		"JULIE" A TELEPHON	TARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL IT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, E, AND GAS FACILITIES. (48 HOUR NOTIFICATION REDUIRED)
	•		RACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY S AND THE VILLAGE OF RIVERSIDE.

- COMPANIES AND THE VILLAGE OF RIVERSIDE.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

- TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- TOP OF FRAME ("RIM") ELEVATIONS CIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF EACH STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATIONS OF THE AREAS IN WHICH THEY ARE LOCATED, AS PART OF THE STRUCTURE COST. TOP OF FRAME ("RIM") ELEVATIONS SHOWN ON THE PLANS FOR STRUCTURES LOCATED IN THE CURB LINE ARE GIVEN AT THE EDGE OF PAVEMENT.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER SHALL CONTACT JOE ECKERT, AREA TRAFFIC FIELD TECHNICIAN, AT (224) 217-8632 A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)".
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH, WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM CRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE
- LINEESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION, THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARMS LENGTHS.
- AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH AGGREGATE SUBGRADE IMPROVEMENT (CU YD) WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 (01/01/2012) AND THE IDOT SUBGRADE STABILITY MANUAL (05/01/2005). IF UNSTABLE AND/OR UNSUITABLE SOILS IS NOT ENCOUNTERED. THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
- ALL PIPE UNDERDRAINS SHALL BE PLACED AT A DEPTH OF 30" BELOW THE TOP OF THE PROPOSED PAVEMENT OR AS DEEP AS POSSIBLE AND IN ACCORDANCE WITH CHECK SHEET *19 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.

COMMITMENTS

THE MONUMENTS AND ATTACHED HANGING SIGNS IN THE NORTHEAST AND SOUTHEAST CORNERS OF IL ROUTE 171 AND FOREST AVE. WILL NOT BE IMPACTED.

FILE NAME :	USER NAME = Torigin	DESIGNED -	REVISED -		IL ROUTE 171 AT FOREST AVE./RIDGEWOOD RD.	F.A.P. SECTION	COUNTY TOTAL S	SHEET NO.
si/pr.nork/pridat/tariqfm/d8315748/PI144	2-Design.dgn	DRAWN -	REVISED -	STATE OF ILLINOIS	INDEX OF SHEETS, STATE STANDARDS, GEN. NOTES, AND COMMITMENTS	312 0303S-TS	COOK 87	2
	PLOT SCALE < 100.000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION			CONTRACT NO. 60	JX08
Cafault	PLOT DOTE 1 3/31/2014	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.	ILLINOIS FED. AIF	ID PROJECT	

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COUNTY TOTAL SHEET NO.
COOK 87 3
CONTRACT NO. 60X08

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44000600	SIDEWALK REMOVAL	SQ FT	4095	4095								Barrare announce des	-	Andreas de la constitución de la	-							and part our management and programmers.	
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44002210	HOT-MIX ASPHALT REMOVAL OVER PATCHES. 2 1/2"	SO YO	51	51		and the second of the second o	pursers or resignature reverse reference	e againmeach ann agus an againte ad gceann an amagan			The second secon	100 - 100 -		and apart of the	Marithmen, I be not the HAT THE NATION TO SHE			and the state of t		and the second s		a familia grande e familiar e m. promete e spike mijde.	woodeness and the second second
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		1000-10-7 and 1-40-11-11-11-11-11-11-11-11-11-11-11-11-11												<u> </u>									
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	31	31		factorism to spin population () of stilling	Angle or communicative and communications and		And the second s	,							the second second second	Andrew Section (W. / Section and Section 2011)	al a contratorio de la contratorio del contratorio de la contratorio del contratorio de la contratorio				
		ng mangang dag salaningang dipikalan ladi seji dilipikalanin kadise	**************************************			rajan syyanya arka angan jaranan digi kamaninan digini didanti	-2			and the second production of the second seco		yang mananaya mangaya (mangaba biba bira bir maha) (ba		A markent-marks consist the strongstrand decide		The same of the sa	fagos po for contentant total tables			and colored (in the colored and physicistics are seen	a gales madd a dwarffer o bh b' cannad i a a fhaileid fi a a m a d	Symposytes to hoodeen the term on the best feel	***************************************
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SO YO	8	8		gy a nghungan san san giri fina jambanin da ba' ka' ka' ka											· ····································		and the section of th	- Japangan mgapan (hanna andangha		sso see as more and more seemed	
			mundarer Adules version		-					-	-	***											
44201747	CLASS D PATCHES, TYPE IV. 8 INCH	S0 Y0	13	13		aranningaranan an more arang		The second secon											********				
magaliminimi manaman kantalaha pekanang belah kan		and a " Just have a " A - A - A - A - A - A - A - A - A - A		***************************************	Angel Angel Marie Control of the Con		a fyriaetriaum method firfantundt aktyleum)					and the second second second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the second section in the second section is a second section in the section in the second section is a section section in the section is a section section in the section in the section is a section section in the section is a section section in the section in the section is a section section in the section is a section section in the section section in the section is a section section in the section section in the section section is a section section section in the section sect	y nigoralis na spanjije na fantus yn an gogan ambo				and photosystems to an extension of the second						and the second s
44201827	CLASS D PATCHES, TYPE II. 15 INCH	SO YD	102	102											,		. , , ="declare \1/kmdl till 1/menn/s 1\\tilde{\text{constraint}		the first transferred to strate and white and part of the an	n,	ni opysyttenystyyvingysiainisika	autoraninarigaanininterianari kaariinin	
41201021								<u></u>									and the state of t	enamentamantenamento consumera		en e man havenganaragengan nhhu men	· · · · · · · · · · · · · · · · · · ·		ALTONO THE STREET, STR
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44201831	CLASS D PATCHES, TYPE III. 15 INCH	SO YD	26	26		an annualeus jarillillig (Million) frantskrivister				Albando de maria de la constitución de la constituc				and the second of the second s	e barrenne i marineraria estrumentaria bar		- hayanin - garinan yangara		A magazini in				
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44201833	CLASS D PATCHES, TYPE IV. 15 INCH	SO YD	43	43													go, come (pagas mas) sigle a ty " (pass) N/A " (paga	and the second s	nong tigan on agrangan kyamanda jid kanandan kabinda		an garage de la company de la	atan'i paograma di Palgama di Anglinda manani padah	- Series and an artist of the series of the
Annual Control of State			ine disdress of the second second	A Control of the Cont							andra de la constante de la co											over an environment was more to a normal	(1
44201851	CLASS D PATCHES. TYPE II, 17 INCH	sa yo	12	12								PRO DE MENTE AN BESTERNING AND AN ANAMAS								Annany transfers t			·
		ennegara a faratifulga kikasin taati jidiliidad iyoti								g agrapan, a sa dhannan di James Pjeler kundije den aljel e së		marina di marina manggan dan marina paga manggan manggan dan manggan manggan manggan manggan manggan manggan m	4		6y dan 16,000 - 19 g69 - 10 f a by hat 1 d87-187-187-187-187-187-1		v- v	an garanatan ayan ayan da an da an da	**************************************	(a. andressyan's assert the state of a set of the set o			laggeorite get i moon koon op mit van branke i kan be
44201855	CLASS D PATCHES, TYPE III, 17 INCH	SO YD	21	21											and the contract profession and profession of the state o		***************************************		nga Pamananan at titik tang nantatah di nam	ong magamanag yi hangan) jamat di tibuyg a hyu ingan	the annual population of the transfer that the second of the transfer that	And a major material from Post and Post and Post annual from an article from a	
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44201857	CLASS & PATCHES, TYPE IV. 17 INCH	SQ YD	253	253				and Salandare (part of the late of the state				on to projugiou, como tijidad hilidai ji shiabili i			gy professionales tolle materials (see 1 to 1 to 1 de 1 d		, and the first of the control of th	A STATE OF A PERSON SEC. STATE OF SECRET SECRET SEC.		agge yez mi'ne englerenne habbenberkant word			
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550A0050	STORM SEWERS. CLASS A. TYPE 1 12"	FOOT	27	27																nga ang Salah ang Malanda da Malanda da Malanda			
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550A0110	STORM SEWERS. CLASS A. TYPE 1 21"	FOOT	325	325	h hagagagan menenbagai menbanganya sebira s				en genego, - magamatala hafumantan salumata	And the state of t	***************************************	gangan gamagalan a a siyaaan 1968 fi Albahyaa 1867 ilahari	* SPE	CIALTY	ITEMS		g at a december of a section of the						
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	SUMMARY OF QUANTITIES				,,		ON TYPE CO		r			T			· · · · · · · · · · · · · · · · · · ·					,			
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CODE NO	ITEM	UNIT	QUANTITIES		TR. SIGNALS ODZ1	E.V.P. 0021	COMBINATION LIGHT(NG 0021	CHARLO DEC	INTERCONNECT 0021	UTILITIES 0043		A Company of the Comp											
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	38	38				V4.0															
	окустиция и окуму потвородно притер и оттем у се от столо в верхного пответ в пответо досей в верхного техного пответо досей в верхного пответо до	g Tarken Bergere Medical Angele Medical Angele Medical Angele Medical Angele Angele Angele Angele Angele Angel	AT A THE OWNER OF THE OWNER OF THE OWNER OF THE OWNER	a.) Taker too katoo aaareerina too ka	And the second s	and the state of t	er mengen pengen kanangan pengen pengen baha	ig in far of graphen and it the desirement apply and in the public fall of the		One control of the second of t				anne e de la companya e que distribuir de la Madei, la companya e que distribuir de la Madei, la companya e qu	The state of the s	THE STATE OF THE S							
S50A0400	STORM SEWERS, CLASS A, TYPE 2 21"	FOOT	26	26										genggyami'nangga to getting o a ti nagga									
11 William of Political Code Politicam Code (Code Code Code Code Code Code Code Code		gan a recommenda a construction, engly gap an antiferror and	ang gang kalang ang ang kalang ka ng Palanggan Panasan ang Pandhahay						na yakamalajimalaylanlad (g)ilday	00000000000000000000000000000000000000						omnymitation o'r monor on th' jobblyne the orbo	o a magazije do a na po na ponapa na pomo nama do a nambo do d		and the third the transfer find to be the transfer to the transfer find to be the transfer to	sverilled higaen steets successive		, or for a serious from the contract of the co	yn an mae ar felwyd y fan yl yr mae menedd "ea
55100300	STORM SEWER REMOVAL 8"	FOOT	52	52		***************************************							A CANADA	***************************************				******************************			- mandrady's Mandradd Mandradous Andreadous (* 1810)	er f efe efe efe en en efe efter fre e fellesten en en fle en en eks	a sighad dika) mangalama ma hiya ay gi sa shishaghay me, sa
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55100500	STORM SEWER REMOVAL 12"	FOOT	56	56		hande and definition and the same of the s	, quantifficantiformação (manusque "Spande") fra hac					er gewein i had bengulaging trouggnous statistism on					and the state of t					r annual for all forms had being the error of	m. march Processor for States Accessors of the
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PO101600	PIPE UNDERDRAINS 4"	FOOT	350	350																An had radious Management gas a basis rigerous			
60201330	CATCH BASINS, TYPE A. 4'-DIAMETER, TYPE	EACH	1	1			eg lagelig, en la se skaallige betyddigidda					-	and the second s						<u> </u>				
	23 FRAME AND GRATE	and a photosoft continues of fifteenhave equipment with		The state of the s		and manufactured of the manufactured and the second	ан разанда жоо 38 онд должна должна фол Фол Фол Фил	ge dejuggener grænnen mensynner men er er er								etama galjumituma kai kamanija aktiva ameterbikamete	ganga ar agharbhaadhladhlaa garar ta ad aarabharbra	Annual England and Annual England and Annual England	o pro-recent lenocorrecentaries			rapagamenta dan katamatan kari matangan fasikatan	enemental facilità de la cicloffe de la finital de la fini
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60208105	CATCH BASINS, TYPE C. TYPE 12 FRAME AND	EACH	1	ı																		,,,,	
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60208230	CATCH BASINS, TYPE C. TYPE 23 FRAME AND	EACH	4	4					and an internal contract of the principle (
	GRATE			para antidoprincipativa e realizarment de criminos de co			and the therefore, and the think and an extending a second of the tensor											3-10-10-10-10-10-10-10-10-10-10-10-10-10-		phonogram (c) togganger (milipsycal continue **			
60218400	MANHOLES, TYPE A. 4'-DIAMETER, TYPE 1	EACH	1	1			engada angapaga pangapah tegapapan ng									etransparaceljene i ngrupene fyzicji of nagdigo	ta/shac (arkanghayan cantro an Fatigrita)		and seen and the seen of the s	Amorphism beautiful for the second section of the section of the section of the second section of the s			
	FRAME, CLOSED LID														ne, market free free free and grade and grade and free an			Annalish again ini ayan ing Péra mgayan				manus (, n° a a g) a shad ga chad ga cand a a a c'aga, g	Arthur Magazza (annuar) (Arta Jackson)
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60221100	MANHOLES, TYPE A, 5' -DIAMETER. TYPE 1	EACH	1	l																			
	FRAME, CLOSED LID	C-1-1														- One of a contract of the forecast on the contract of the con	ty demonstration of the second section of the section of the second section of the section		pr		~~~	er er konsernoviska sammerski Sjakassica kadalada (je	whop (Mah) Mikhari wa James Wilaniga wilan
				and the state of t																			
60250500	CATCH BASINS TO BE ADJUSTED WITH NEW	EACH	gyard aylard aganggagd magarinan daylarda da'ba'	Towns and the second		ing the second of the second o											**************************************	and the same of th			haline de l'annancia de l'angle an l'angle a	of a the other states and the states	nother thanks the best and section of the section o
	TYPE 1 FRAME, CLOSED LID	Children and programme and programme and the contract of the c				المراوات المساورة والمساورة والمساور				distribution of the state of th				n maganilyang di malja majana aya 1 yang at iyan da 18km 19m a		haddirkaga inga ga gha Nada ahang ak ga siy si			and an analysis of the second			***************************************	, 1 (100 to
60251730	CATCH BASINS TO BE ADJUSTED WITH NEW	EACH					Vera community of the c				,												
	TYPE 23 FRAME AND GRATE		3000			and a second thing for the formula of the second	San Andrews																
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······		ATE -	·····	REVISED			<u>'</u>		<i></i>				SCALE:	SHEET N	10. OF	SHEETS STA	i.,	TO STA.	FEO.	ROAD DIST. NO. 1	ILLINOIS FED. A	O PROJECT	

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	SUMMARY OF QUANTITIES						ON TYPE CO				1		I						T				
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CODE NO	МЭТІ	UNIT	QUANTITIES URBAN	ROADWAY	TR. SIGNALS 0021	E.V.P. 0021	COMBINATION LIGHTING 0021	SHARED-USE PATH AND SIDEWALK 0028	INTERCONNECT 0021	UTILITIES 0043			Management de										
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE I	EACH	1					1										hamad and the control of the Control		one grant, and a second of the Color of the state state of the state of	entire de l'entre l'annoire de l'entre	propriedure (Specifical 1 of Specifical 1 of Specifical 1 of Specifical Speci	noune Communicate I (alle de la communicate de l
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		***************************************	a promote to the second		and the second of the second o			julganganagaj jakat 1980) tad ta Warter M ^{ara} nda.	anggamaghalan pangaranan persapakan sebes			ether properties and departs that are and	Maringapagagapa Apadina (J.M. E. yin'i 1 Ani				***************************************			Anna Anna Anna Anna Anna Anna Anna Anna	wherethe		
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60260400		EACH														ang ang magang maganah menandi di kedikan di Malami			gan and a second	essananianianianianianianianianianianianiah		engay y gayararan yangagi i, in yangbali dibi sabari a	era i è commende che che commende de des
	FRAME. CLOSED LID	a, a																gangama (garantaga sakkama an 1979) kilada (A			.,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,		
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60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	5	5												andelester et année de le constitution (1744)		***************************************					ng taon 19 ₀ 00000000000000000000000000000000000
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60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	12	10		and the second s		2					Annual control of the				***************************************						
					and for eather the Remark and and the Parkers and					April and a second seco		hayanday garithidaka promponyonyi (interna											
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60500050	REMOVING CATCH BASINS	EACH	1	1					and the second s				***************************************					lamma padyana katanjat i tekstot teoriki ti			A		
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60500060	REMOVING INLETS	EACH	4	4			to the second of	and the second s				parties, s., S., estare seri-metralesses											on, or, managed and provide VP to
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60600605	CONCRETE CURB, TYPE B	FOOT	108	108			-		***************************************		A de la constanta de la consta				and the second second				4	Season Se			And the first constitution and the second
a					and the same of th		_												***************************************				
	COMBINATION CONCRETE CURB AND GUTTER,	FOOT	205	205														omazenen omazen oraz				2500 11000 11000	
60603800		1 7001	404						ar valleyanasa ara gijasana jakir virisis, ta kapitanin sar		gan sanganagan militara ika * **** sis ***** ik ***	Augusta on Pales Adv. Adv. Apr. March 1811 1811 1811		my agraphy and the or the or the order of th				en and an enterior of the first over the second of the					
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60604400	COMBINATION CONCRETE CURB AND GUTTER.	FOOT	1060	1060		alija kanaagan ka kantaa kiintaa in lii da makana										<u> </u>							
	TYPE 8-6.18										- Principle of the Prin							one or the second statement of the second se		,,,			
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63200310	GUARORAIL REMOVAL	FOOT	131	131										And the second s						***************************************		maas sum vers erbreib	
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70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	1 001	1 0320					and the state of t								arana aran aran aran aran da aran aran a		###Johnson Lifeting Architecture Architectur		and an anti-original principles and market and market	AMA*Au/(un/4)*15***********************************	American W. (1) Laboratory (1)	(10)(m)(m)(10)(10)(10)(10)(10)(10)(10)(10)(10)(10
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70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	773	773									As a second seco							- gragating and grand property and the best of the bes		Continues on Children de Braches, y anticipal y	
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81028:	200 UNDERGROUND C	ONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	815		740	reme Average and A		errerennishtend visual d	75				* SPE	CIALTY	ITEMS	**************************************			Terren militari de serviciones de se	The state of the s	***		:
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	SUMMARY OF QUANTITIES		TOTAL	90% FED 10% STATE	90% FED 5% STATE 5% RIVERSIDE	,	ON TYPE CO 90% FED 10% RIVERSIDE		90% FED 10% STATE	100% RIVERSIDE	-		Annual An		· ·				ATTICOPPOSE TO A STATE OF THE S			
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81028220	INDERGROUND CONDUIT, GALVANIZED STEEL. 3" DIA.	FOOT	70	***************************************	70		The state of the s	\$***LETE ALT E POACHA "ALL"*********************************	- an e-bare-hare by en system en bre e-bare		And a column of the foreign of the first triber of			offer on facilities become the section of the facilities of the fa			r many haganaji diriramagagayang pengsama pemen	gang gangaritin hing ing kanaman maka santukaya a magamat san			Albahi, madagan kanan kan 156 kg ma, an	
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81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL. 4" DIA.	FOOT	600		600			,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,													ر د د د د د د د د د د د د د د د د د د د	
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81400200	HEAVY-OUTY HANDHOLE	EACH	4		4								And the second s									
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81400300	DOUBLE HANDHOLE	EACH	4							ann an dear ann an air an Aighrich an Mhair Sphaead bail thiù	n Magaziniya dadi ilinda Madigida dadida da da			magnanananan garana (pajna	erioripijaseraonaereoministrae socior				THE RESIDENCE OF THE PROPERTY		Shander to the construction of the state of	
81702110	ELECTRIC CABLE IN CONDUIT, 600V	FOOT	1600				1600			المنافظة المنافظة المنافظة (المنافظة المنافظة المنافظة المنافظة المنافظة المنافظة المنافظة المنافظة المنافظة ا									ACTION AND ADDRESS OF THE ACTION AND ACTION			
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81702120	ELECTRIC CABLE IN CONDUIT, 600V	FOOT	275				275														- 1511/00/F14000141 F741/00/F1400140PF44	
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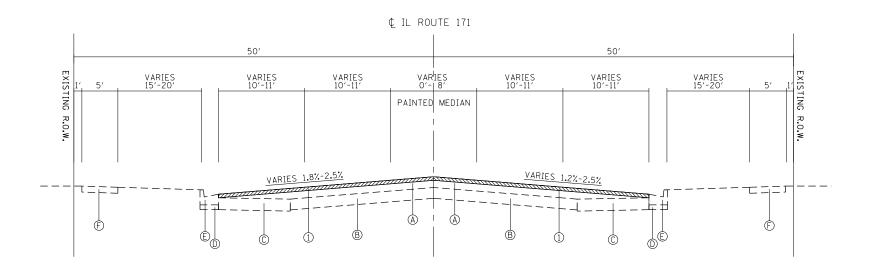
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87702880	STEEL COMBINATION MAST ARM ASSEMBLY AND	EACH	1		1	en agamagilar) Salam salam Pangangang Ing Nobel San San S		and the second s													and the state of t		-
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87900200	DRILL EXISTING HANDHOLE	EACH							<u>t</u>								***************************************		***************************************		andranitad metikuskad kanadangan kanbu meditan
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88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION,	EACH	4		4												4		2004		
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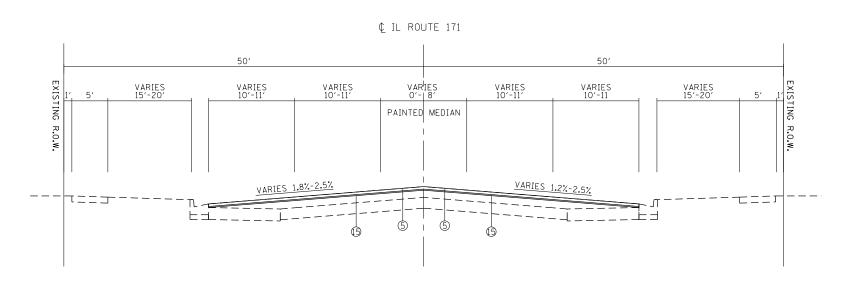
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F	0322918	PROPOSED MANHOLE/CATCH BASIN CONNECTION	EACH	4	4									-									ł	İ
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*	0324085	EMERGENCY VEHICLE PRIORITY SYSTEM LINE	FOOT	320			320				and the state of t		gaga gangan da da da gala gan da ada ma da							440-				
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-	x6020095	MANHOLES. TYPE A, 4' DIAMETER, TYPE 1	EACH	1	international desired		devere direct of debt			1				AND THE PROPERTY AND TH										
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*	x8620200	UNINTERRUPTABLE POWER SUPPLY, SPECIAL	LACT	<u> </u>											o diglampodas sel i dasslindrindridd 1927 och 188	managapan agany sahanan at garbanten at term	***************************************	nat i gallannannal lastafytri kindyai iskyamat		A THE STREET AND THE PROPERTY OF THE PROPERTY				mi mi mi wa mana i wa mana wa
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*	x8710024	FIBER OPTIC CABLE IN CONDUIT, NO.	FOOT	2950		and the fact of the forest of the first of the fact of				2950			annash nagara kanggianish biranis Marik										A Treasure treasure at the second and second at the second	
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		SUMMARY OF QUANTITIES	,					ON TYPE C	····	······································			<u>-</u>				····	·				······		1
-				TOTAL	90% FED 10% STATE	90% FED 5% STATE 5% RIVERSIDE	100% RIVERSIDE	90% FED 10% RIVERSID	80% STATE 20% RIVERSIDE	90% FEB 10% STATE	100% RIVERSIDE	The second secon		***	**************************************	nyesyania nasana na		**************************************	# * * * * * * * * * * * * * * * * * * *		méreretementanteré-très		and the second s	
CODE	NO	Mati	UNIT	OUANTITIES URBAN		TR. SIGNALS 0021		COMBINATION LIGHTING 0021		INTERCONNECT	UTILITIES 0043				***************************************			And the second s	***	***************************************	Adventibility of the currentifier	or decounterer control of the contro		
20004	530	HOT-MIX ASPHALT ORIVEWAY PAVEMENT, 8"	SQ YD	27	27				VV28														a description of the following of the control of the control of	
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Z0004	538	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 10"	SQ YO	76	76	g a selection gales quantities gelt and efficiency and decreases and a	and the state of t		e e numero essensial de l'assaule no essensial no essensial no essensial no essensial no essensial no essensia				ing and graph (in terms of the second order of the second order of the second order of the second order of the											
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EXISTING TYPICAL SECTION
STA. 35+00 TO 37+40



PROPOSED TYPICAL SECTION
STA. 35+00 TO 37+40

EXISTING LEGEND:

- A HMA SURFACE & BINDER COURSE, ± 10"
- B PCC PAVEMENT, ± 9"
- C HMA BASE COURSE, ± 10"
- D SUBBASE GRANULAR MATERIAL, 4"
- © COMB. CONC. CURB & GUTTER, TYPE B-6.24
- F PCC SIDEWALK

PROPOSED LEGEND:

- 1 HMA SURFACE REMOVAL, 21/2"
- 2 PAVEMENT REMOVAL
- 3 COMB. CONC. CURB & GUTTER REMOVAL
- 4 SIDEWALK REMOVAL
- 5 POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 13/4"
- 6 HMA BASE COURSE WIDENING, 91/2"
- 7 AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (8) COMB. CONC. CURB & GUTTER, TYPE B-6.18
- 9 SUBBASE GRANULAR MATERIAL, TYPE B, 6"
- () HMA SURFACE COURSE, MIX "D", N50, (IL 9.5 mm), 2"
- ① AGGREGATE BASE COURSE, TYPE B, 6"
- 2 PCC SIDEWALK, 5"
- (3) SODDING & TOPSOIL 4"
- (4) PIPE UNDERDRAIN, 4"
- (5) POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"

NOTE: THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

	MIXTURE TYPE	AIR VOIDS @ N _{DES}	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT	POLYMERIZED HMA SURFACE COURSE, MIX "F", N90 (IL-9.5 mm)	4% @ 90 GYR	OCP
RESURFACING	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% @ 50 GYR	QC/QA
	POLYMERIZED HMA SURFACE COURSE, MIX "F", N90 (IL-9.5 mm)	4% @ 90 GYR	QCP
PAVEMENT WIDENING	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% @ 50 GYR	QC/QA
	HMA BASE COURSE WIDENING, 5" & 912" (HMA BINDER IL-19.0)	4% @ 70 GYR	QC/ QA
HMA DRIVEWAY	HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2"	4% @ 50 GYR	QC/ QA
HWA DRIVEWAT	HMA BASE COURSE (HMA BINDER IL-19 mm); PE - 6", CE - 8"	4% ⊚ 50 GYR	QC/ QA
SHARED-USE PATH	HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2"	4% @ 50 GYR	QC/QA
PATCHING	CLASS D PATCHES, (HMA BINDER IL-19 mm)	4% @ 70 GYR	QC/QA
FAICHING	HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR	QC/ QA
QMP DESIGNATION:	QUALITY CONTROL/QUALITY ASSURANCE (QC/QA);	QUALITY CONTROL FO	OR PERFORMANCE (OCP)

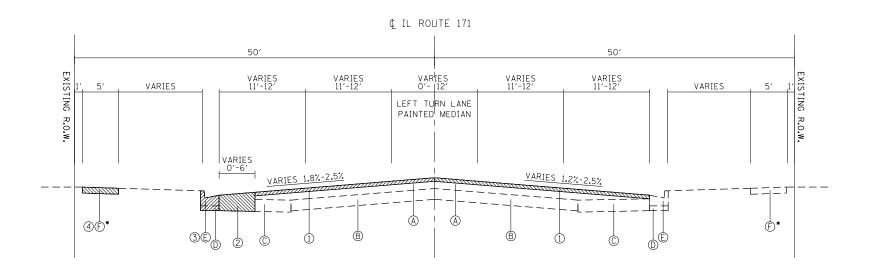
NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA MIXTURES IS 112 LBS/SQ YD/IN.

NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22"
AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22"
UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.
OUALITY MANAGEMENT PROGRAM (QCP) IDENTIFIES THE PARTICULAR
OUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

SCALE:

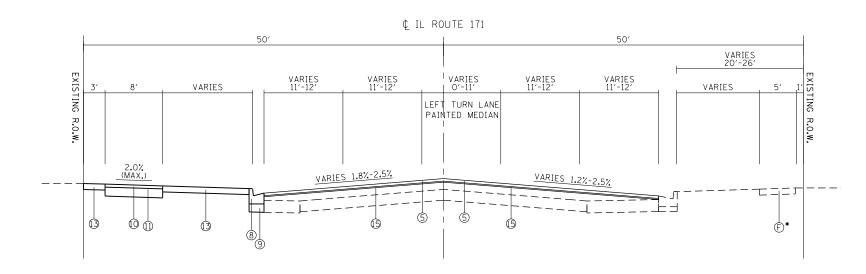
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IL.	ROUTE 171	AT FO	DREST AV	E./RIDGEV	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	TVDICA	I SEC.	TIONS _	II ROLITE	372	0303S-TS	COOK	87	16	
TYPICAL SECTIONS – IL. ROUTE 171					171			CONTRACT	NO. 6	0X08
	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FED. AI	D PROJECT		



EXISTING TYPICAL SECTION

STA. 37+40 TO 42+00 STA. 45+47 TO 48+67



PROPOSED TYPICAL SECTION

STA. 37+40 TO 42+00 STA. 45+47 TO 48+67

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Default	PLOT DATE = 3/31/2014	DATE -	REVISED -	

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

SCALE:

IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD. TYPICAL SECTIONS – IL. ROUTE 171 SHEET OF SHEETS STA. TO STA.

EXISTING LEGEND:

- A HMA SURFACE & BINDER COURSE, ± 10"
- B PCC PAVEMENT, ± 9"
- C HMA BASE COURSE, ± 10"
- D SUBBASE GRANULAR MATERIAL, 4"
- © COMB. CONC. CURB & GUTTER, TYPE B-6.24
- F PCC SIDEWALK

PROPOSED LEGEND:

- 1 HMA SURFACE REMOVAL, 21/2"
- 2 PAVEMENT REMOVAL
- 3 COMB. CONC. CURB & GUTTER REMOVAL
- 4 SIDEWALK REMOVAL
- 5 POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 13/4"
- 6 HMA BASE COURSE WIDENING, 91/2"
- 7 AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (8) COMB. CONC. CURB & GUTTER, TYPE B-6.18
- 9 SUBBASE GRANULAR MATERIAL, TYPE B, 6"
- () HMA SURFACE COURSE, MIX "D", N50, (IL 9.5 mm), 2"
- ① AGGREGATE BASE COURSE, TYPE B, 6"
- 2 PCC SIDEWALK, 5"
- (3) SODDING & TOPSOIL 4"
- (4) PIPE UNDERDRAIN, 4"
- (5) POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"

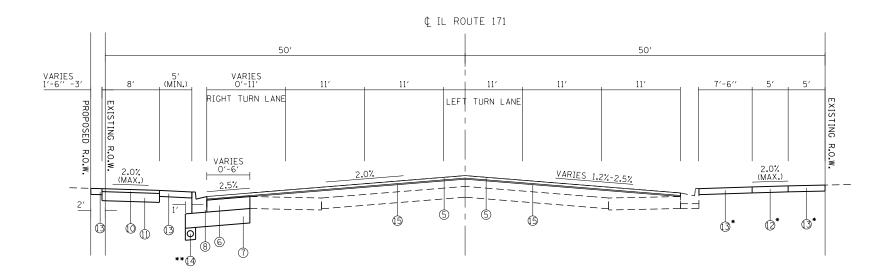
NOTE: THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.

* EXISTING PCC SIDEWALK FROM STA. 37+40 TO 41+30 ONLY

1 FOUNTE 171

| 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' | 12' |

EXISTING TYPICAL SECTION STA. 42+00 TO 45+47



PROPOSED TYPICAL SECTION STA. 42+00 TO 45+47

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

SCALE:

IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD. TYPICAL SECTIONS – IL. ROUTE 171 SHEET OF SHEETS STA. TO STA.

EXISTING LEGEND:

- A HMA SURFACE & BINDER COURSE, ± 10"
- B PCC PAVEMENT, ± 9"
- C HMA BASE COURSE, ± 10"
- D SUBBASE GRANULAR MATERIAL, 4"
- © COMB. CONC. CURB & GUTTER, TYPE B-6.24
- F PCC SIDEWALK

PROPOSED LEGEND:

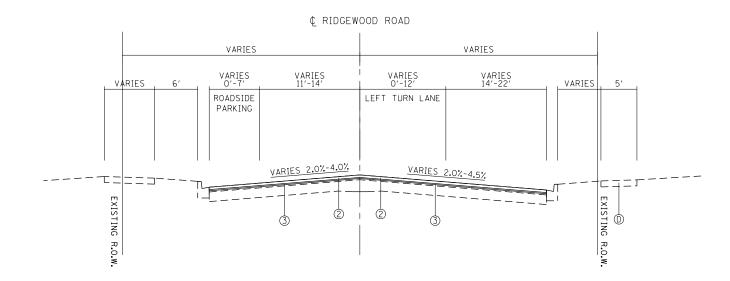
- 1 HMA SURFACE REMOVAL, 21/2"
- 2 PAVEMENT REMOVAL
- 3 COMB. CONC. CURB & GUTTER REMOVAL
- 4 SIDEWALK REMOVAL
- 5 POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 13/4"
- 6 HMA BASE COURSE WIDENING, 91/2"
- 7 AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (8) COMB. CONC. CURB & GUTTER, TYPE B-6.18
- 9 SUBBASE GRANULAR MATERIAL, TYPE B, 6"
- () HMA SURFACE COURSE, MIX "D", N50, (IL 9.5 mm), 2"
- ① AGGREGATE BASE COURSE, TYPE B, 6"
- 2 PCC SIDEWALK, 5"
- (3) SODDING & TOPSOIL 4"
- (4) PIPE UNDERDRAIN, 4"
- (5) POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, $\frac{3}{4}$ "

NOTE: THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.

- * PCC SIDEWALK, 5" FROM STA. 42+37 TO 43+10 ONLY. SEE ROADWAY PLAN.
- * * LONGITUDINAL PIPE UNDERDRAIN FROM STA. 42+50 TO STA. 44+50.

VARIES VARIES VARIES VARIES VARIES VARIES VARIES O'-12' 14'-22' VARIES 5' ROADSIDE PARKING VARIES 2.07.-4.07. VARIES 2.07.-4.57. EXISTING ROADS. WARIES 2.07.-4.57.

EXISTING TYPICAL SECTION
STA. 17+00 TO 19+25



PROPOSED TYPICAL SECTION STA. 17+00 TO 19+25

EXISTING LEGEND:

- riangle HMA SURFACE & BINDER COURSE, $\pm \ 2^{1}\!/_{2}{}^{\prime\prime}$
- B PCC PAVEMENT, ± 8"
- © COMB. CONC. CURB & GUTTER, TYPE B-6.12
- D PCC SIDEWALK

PROPOSED LEGEND:

- 1) HMA SURFACE REMOVAL, 21/2"
- ② POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, $1\frac{3}{4}$ "
- 3 POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, $\frac{3}{4}$ "

NOTE: THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.

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STATI	OF ILLINOIS	
DEPARTMENT	OF TRANSPORTAT	ΓΙΟΝ

SCALE:

IL.	ROUTE 17	AT FO	DREST AV	E./RIDGE	WOOD RD.	F.A.P. RTE.	SECTION	COUNTY
	TVDICAL	SECTIO	ONS – RII	ncewoni	n RN	372	0303S-TS	СООК
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	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FED. AI	D PROJECT

COUNTY TOTAL SHEET NO.

COOK 87 19

CONTRACT NO. 60X08

VARIES 70'6"- 73'

VARIES 70'6"- 73'

VAR. 13'

LEFT TURN LANE PAINTED MEDIAN

VARIES 2.27-4.0X

VARIES 3.5X-4.7X

VARIES 3.5X-4.7X

EXISTING TYPICAL SECTION
STA. 20+54 TO 23+91

VARIES 70'6"- 73'

VARIES VARIES VARIES VARIES VARIES II'-13'

11'-13'

VARIES VARIES VARIES VARIES II'-13'

VARIES VARIES VARIES VARIES II'-13'

VARIES VARIES VARIES VARIES II'-13'

VARIES VARIES VARIES VARIES VARIES VARIES II'-13'

VARIES VARIES VARIES VARIES VARIES VARIES II'-13'

VARIES VARIES VARIES VARIES VARIES VARIES VARIES II'-13'

VARIES VA

PROPOSED TYPICAL SECTION
STA. 20+54 TO 23+91

EXISTING LEGEND:

- A HMA SURFACE & BINDER COURSE, ± 21/2"
- B PCC PAVEMENT, ± 8"
- © COMB. CONC. CURB & GUTTER, TYPE B-6.12
- D PCC SIDEWALK

PROPOSED LEGEND:

- 1 HMA SURFACE REMOVAL, 21/2"
- ② COMB. CONC. CURB & GUTTER REMOVAL
- 3 SIDEWALK REMOVAL
- 4 POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 13/4"
- ⑤ HMA BASE COURSE WIDENING, 5"
- 6 AGGREGATE SUBGRADE IMPROVEMENT, 12"
- 7 COMB. CONC. CURB & GUTTER, TYPE B-6.12
- 8 PCC SIDEWALK, 5"
- SODDING & TOPSOIL, 4"
- (0 6" UNDERCUT (STA. 21+00 TO STA. 22+00, WIDTH OF WIDENING)
- ① PIPE UNDERDRAIN, 4"
- POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"

NOTE: THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.

- * PCC SIDEWALK REMOVAL AND PROPOSED PCC SIDEWALK, 5" FROM STA. 20+54 TO 22+06 ONLY. CURB & GUTTER REMOVAL AND PROPOSED COMB. CURB & GUTTER, TYPE B6.12 FROM 20+54 TO 22+22 ONLY.
- ** LONGITUDINAL PIPE UNDERDRAIN FROM STA. 20+50 TO STA. 22+00.

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Default	PLOT DATE = 3/31/2014	DATE -	REVISED -		SCALE:	SHEET OF SHEETS STA. TO STA.		ILLINOIS FED	ald PROJECT

	EARTHWORK												
1	2	3	4	5	6	7							
IL. ROUTE 171 AT FOREST AVE. / RIDGEWOOD RD.	EARTH EXCAVATION (CU YD)	EMBANKMENT (CU YD)	ADJUSTMENT FOR SHRINKAGE (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)	UNSUITABLE MATERIAL (CU YD)	TOPSOIL EXCAVATION AND PLACEMENT (CU YD)							
IL. ROUTE 171 (SOUTH LEG) - STA. 35+00 TO STA. 42+00	17	211	15	-196	96	69							
IL. ROUTE 171 (NORTH LEG) - STA. 42+00 TO STA. 48+67	88	284	75	-209	302	102							
FOREST AVE STA. 20+29 TO STA. 23+91	58	14	49	35	63	15							
TOTAL	163	509	139	-370	461	186							

COLUMN 1: LOCATION FROM PLANS

COLUMN 2: CUT QUANTITIES FROM CROSS SECTIONS, WHICH DOES NOT INCLUDE UNSUITABLE MATERIAL

COLUMN 3: QUANTITIES FROM CROSS SECTIONS (FILL)
COLUMN 4: EARTH EXCAVATION THAT IS TO BE USED AS FILL

MATERIAL IN THE EMBANKMENT, SHRINKAGE FACTOR IS 15%

COLUMN 5: COLUMN 4 - COLUMN 3

POSITIVE QUANTITY = EXTRA EXCAVATION

NEGATIVE QUANTITY = FURNISHED EXCAVATION NEEDED COLUMN 6: CUT MATERIAL THAT IS DETERMINED TO BE EITHER

UNSTABLE OR UNSUITABLE FOR USE IN EMBANKMENT

COLUMN 7: TOPSOIL EXCAVATION AND PLACEMENT = AREA OF SOD AND TOPSOIL

NOTES:

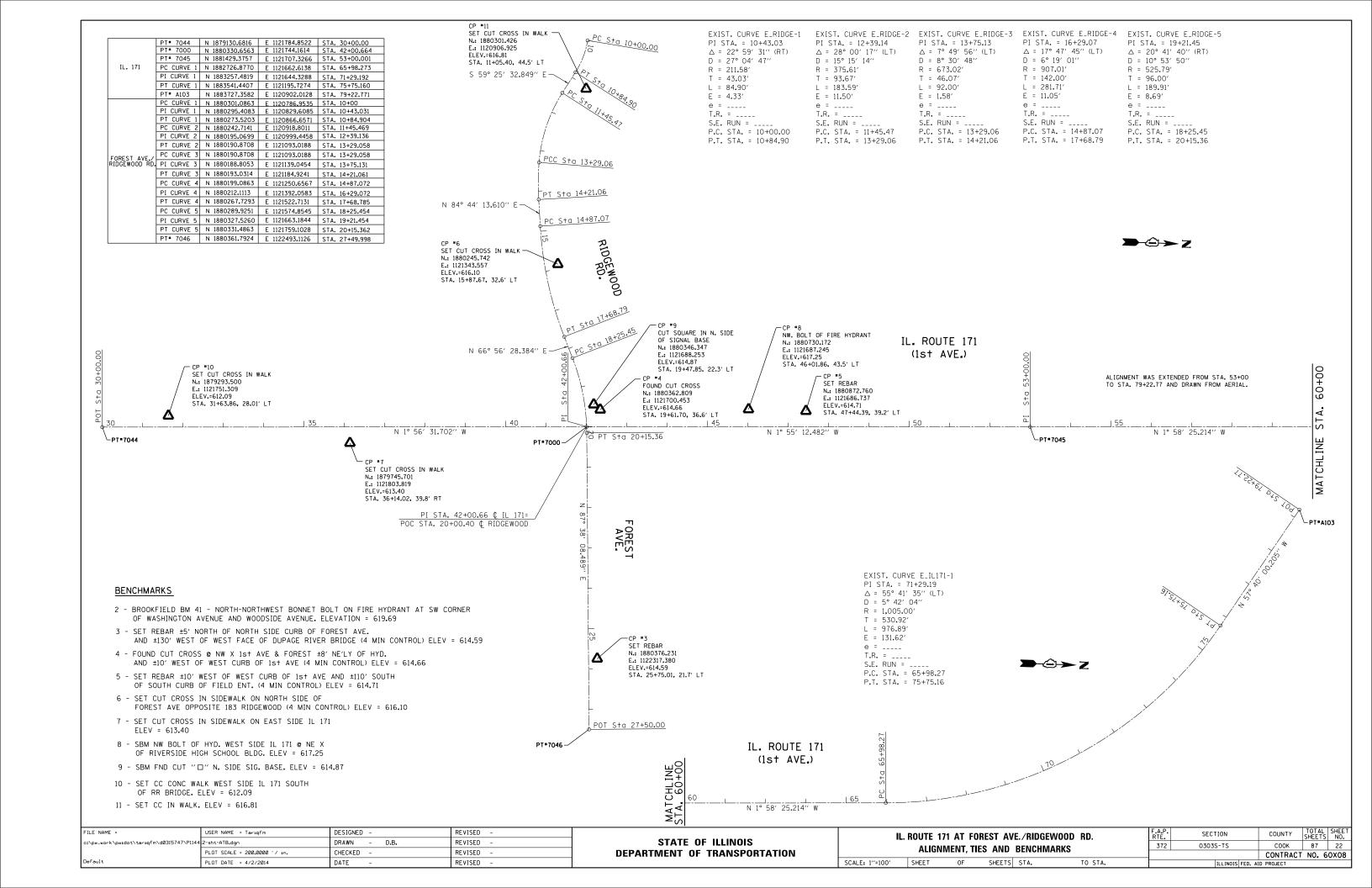
1. TOPSOIL SHALL BE EXCAVATED TO A DEPTH OF 12" THROUGHOUT THE PROJECT LIMITS.

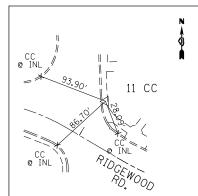
SCALE:

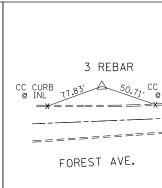
- 2. EXCAVATED TOPSOIL REQUIRED AT LOCATIONS OF NEW SOD AS SHOWN ON THE LANDSCAPING PLAN SHALL BE PLACED AT A DEPTH OF 4" AND PAID FOR AS TOPSOIL EXCAVATION AND PLACEMENT.
- 3. EXCAVATED TOPSOIL NOT REQUIRED ON THE PROJECT SHALL BE CONSIDERED UNSUITABLE MATERIAL AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL.

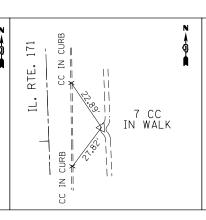
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c:\pw_work\pwidot\tariqfm\d0315748\P1144	2-Design.dgn	DRAWN -	REVISED -	STATE OF ILLINOIS
	PLOT SCALE = 100.000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION
Default	PLOT DATE = 3/31/2014	DATE -	REVISED -	

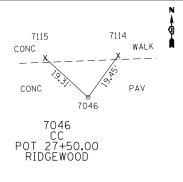
IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD				E./RIDGE	WOOD RD.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCHEDULE OF QUANTITIES						372	0303S-TS	соок	87	21
		JUILDULL	. 01 40/	AIVIIIILO				CONTRACT	NO. 6	80X08
	SHEET	OF	SHEETS	STA.	TO STA.		TILINOIS EED A	ID PROJECT		

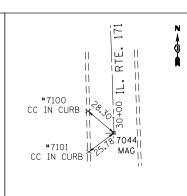


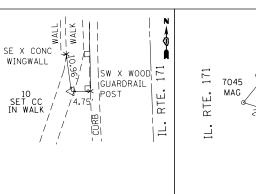


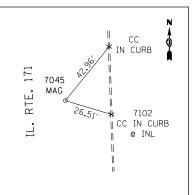












POINT #11

SET CC IN WALK N 1880301.4260 E 1120906.9250

POINT #3

REBAR N 1880376.2310 E 1122317.3800

POINT #4

CC IN WALK N 1880362.8090 E 1121700.4530

POINT #7046

POT 27+50.00 RIDGEWOOD N 1880361.7924 E 1122493.1126

POINT #7044

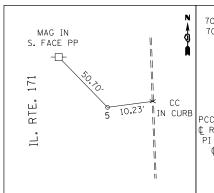
SET MAG IN ¢ POT 30+00 ¢ IL 171 N 1879130.6816 E 1121784.8522

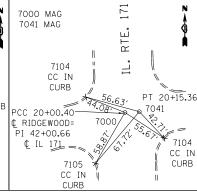
POINT #10

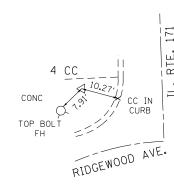
SET CC IN WALK N 1879293.5000 E 1121751.3090

POINT #7045

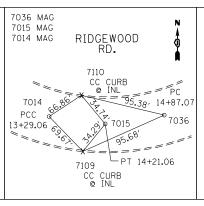
SET MAG IN ¢ POT 53+00 ¢ IL 171 N 1881429.3757 E 1121707.3266

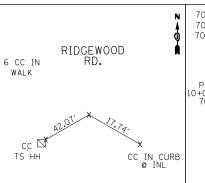


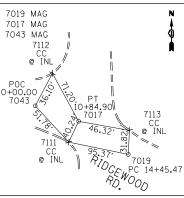




7038 MAG 7039 MAG RIDGEWOOD PC 18+25.45 7038 7108







POINT #5

REBAR N 1880872.7600 E 1121686.7370

POINT #7000

SET MAG IN POC 20+00.40 ¢ RIDGEWOOD= PI 42+00.66 ¢ IL 171 N 1880330.6563 E 1121744.1614

POINT #7041

SET MAG IN

PT 20+15.36 ¢ FOREST/

RIDGEWOOD=

N 1880331.4863

E 1121759.1028

CONTROL POINT #4

CC IN WALK N 1880362.8090 E 1121700.4530

POINT #7038

SET MAG IN PT 17+68.785 RIDGEWOOD N 1880267.7293 E 1121522.7131

POINT #7039

SET MAG IN PC 18+25.454 RIDGEWOOD N 1880289.9252 E 1121574.8546

POINT #7014

SET MAG IN PCC 13+29.06 ¢ RIDGEWOOD N 1880190.8708 E 1121093.0188

POINT #7015

SET MAG IN PT 14+21.06 ¢ RIDGEWOOD N 1880193.0314 E 1121184.9243

POINT #7036

SET MAG IN PC 14+87.07¢ RIDGEWOOD N 1880199.0863 E 1121250.6569

POINT #6

SET CC IN WALK N 1880245.7420 E 1121343.5570

POINT #7019

SET MAG IN PC 11+45.47 ¢ RIDGEWOOD N 1880242.7140 E 1121093.0188

POINT #7017

SET MAG IN PT 10+84.90 ¢ RIDGEWOOD N 1880273.5204 E 1120866.6570

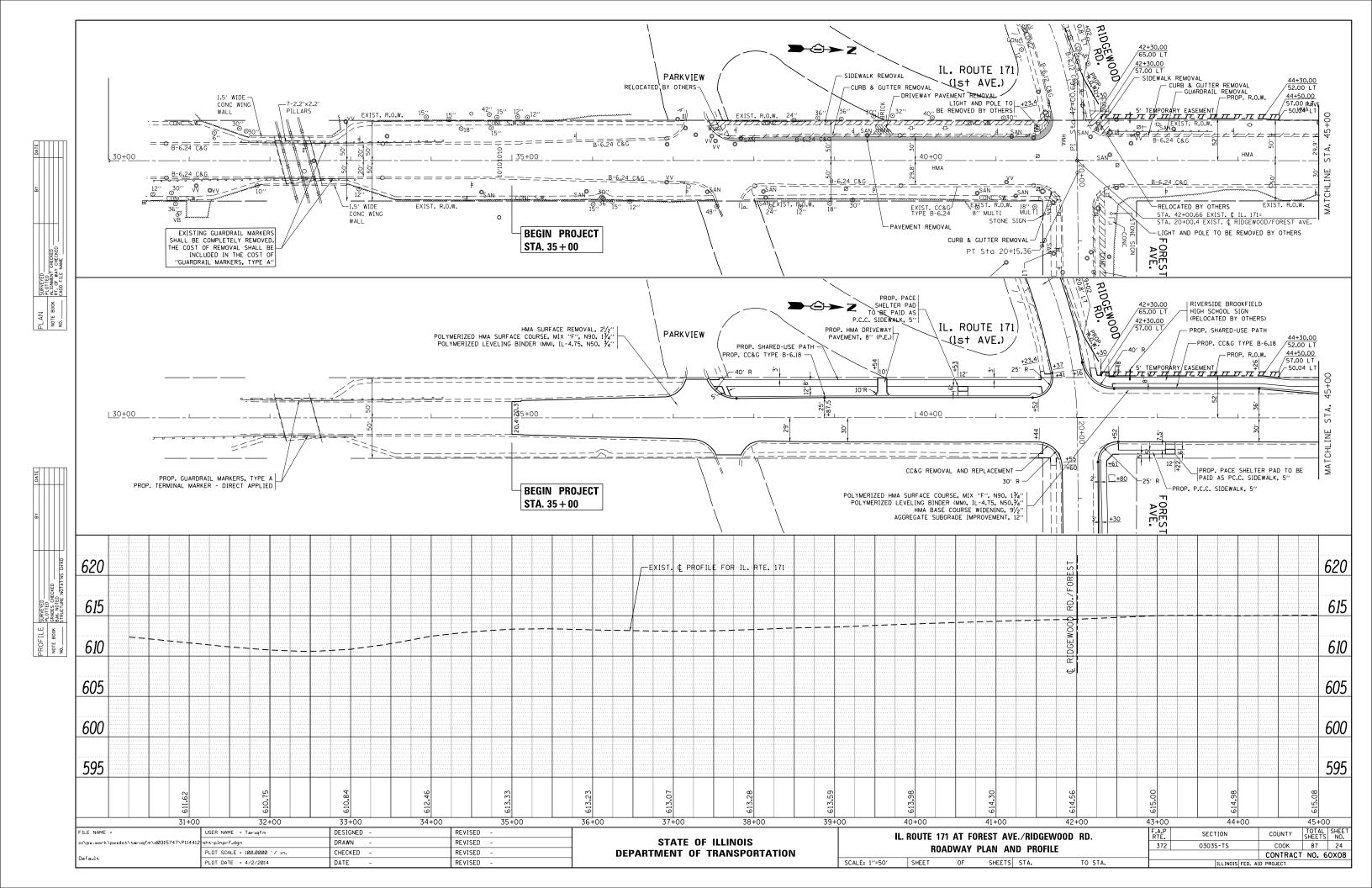
POINT #7043

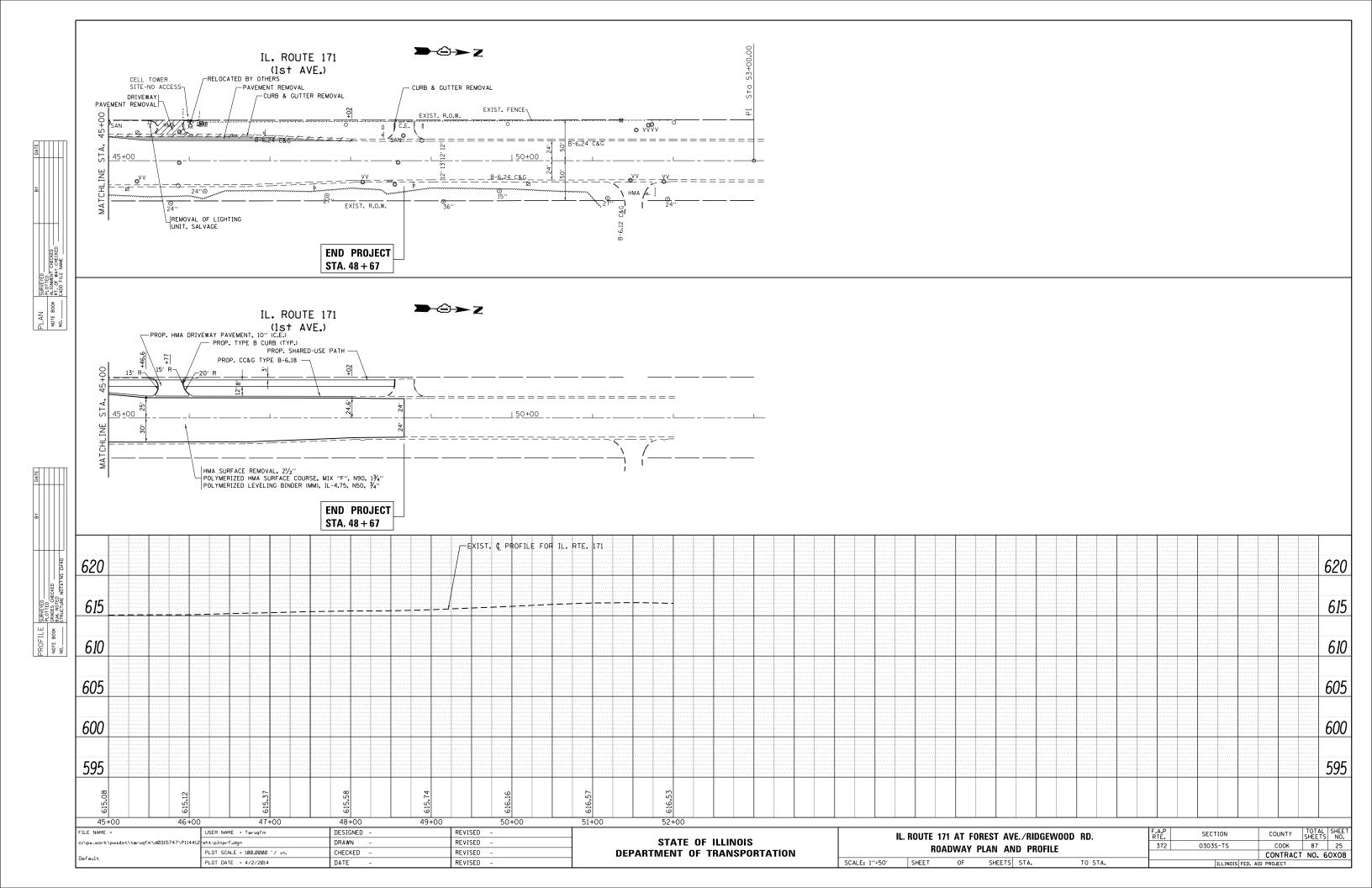
SET MAG IN POC 10+00 ¢ RIDGEWOOD N 1880301.0863 E 1120786.9535

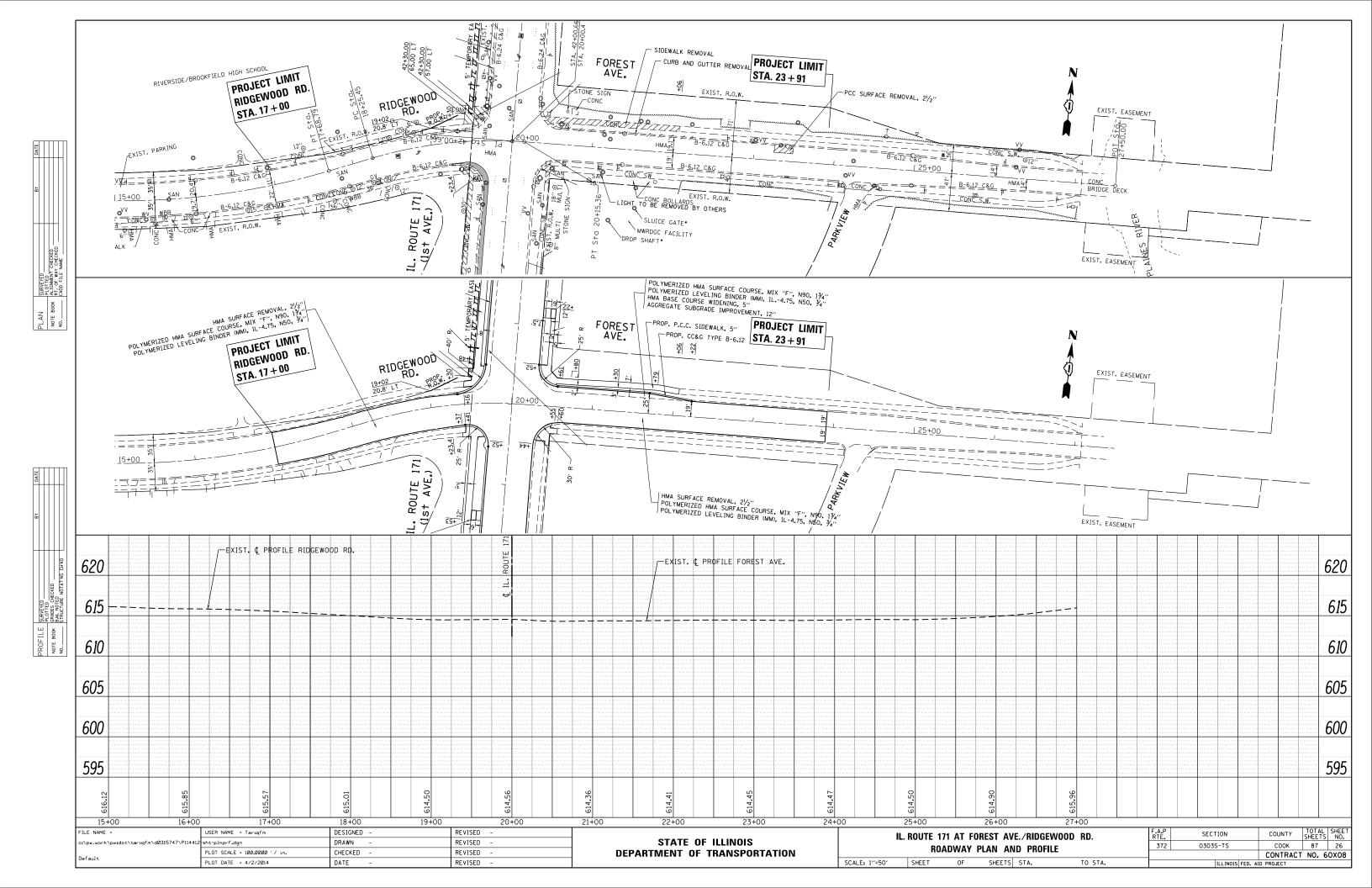
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Default PLOT DATE = 4/2/2014 DATE - REVISED -	Default	PLOT DATE = 4/2/2014	DATE -	REVISED -

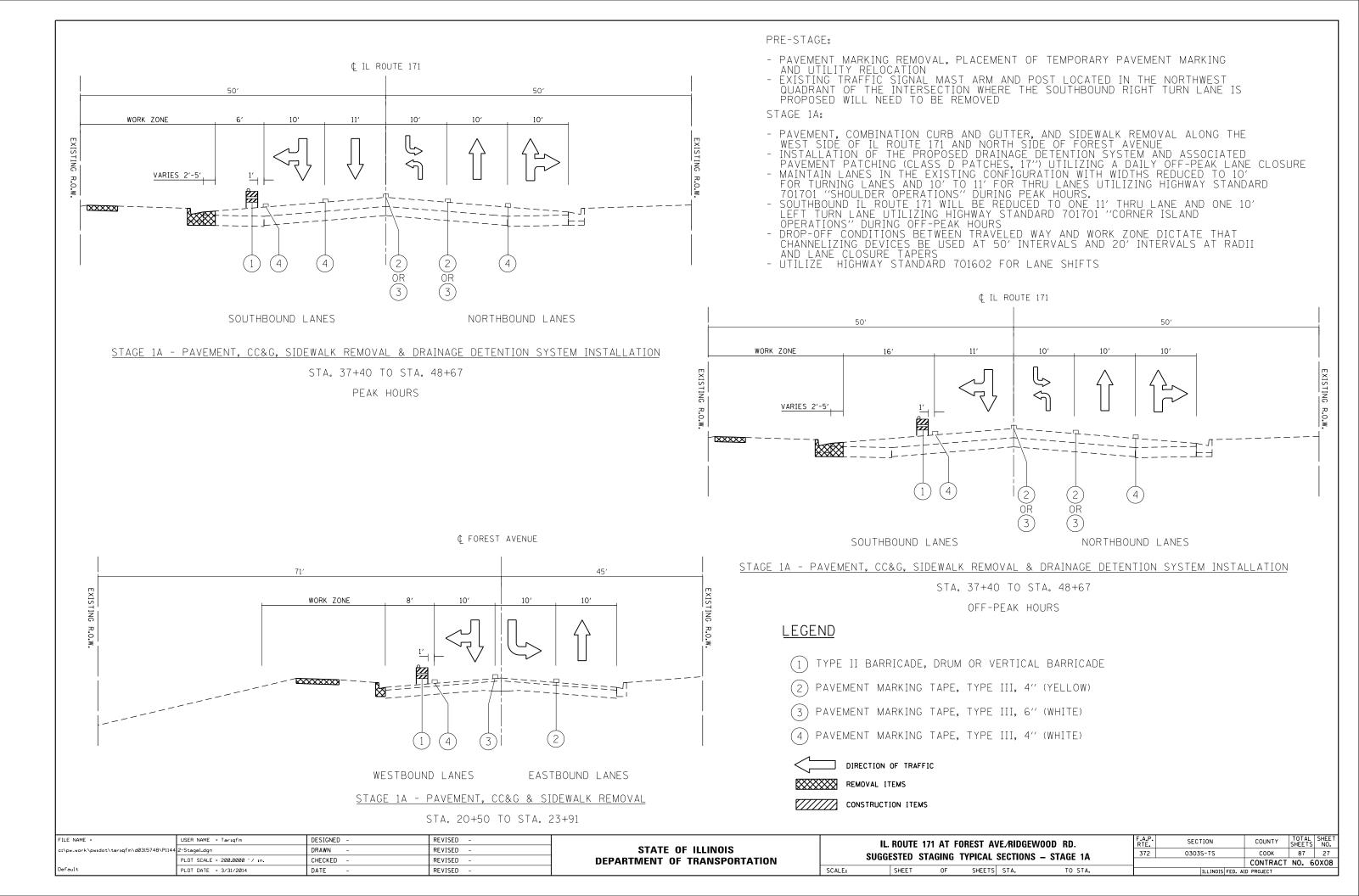
STATE OF ILLINOIS	
DEPARTMENT OF TRANSPORTATION	

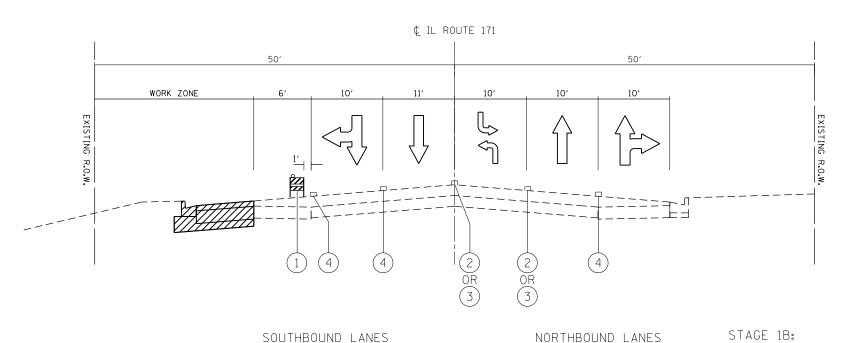
IL.	ROUTE 1	71 AT F0	REST AV	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
TIES							0303S-TS	соок	87	23
			IILU			CONTRACT	NO. 6	80X0		
SCALE: 1"=100"	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FED. A	ID PROJECT		







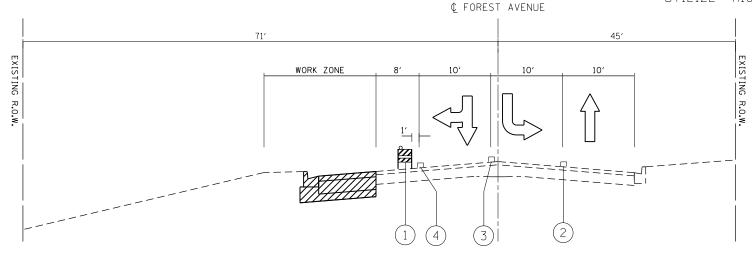




STAGE 1B - PAVEMENT WIDENING & CC&G INSTALLATION STA. 37+40 TO STA. 48+67

- PAVEMENT WIDENING AND COMBINATION CURB AND GUTTER INSTALLATION
- ALL HOT-MIX ASPHALT SHALL BE INSTALLED TO THE BINDER ONLY; THE
FINAL SURFACE COURSE AND LEVELING BINDER IS TO BE INSTALLED IN POST-STAGE
- MAINTAIN LANES IN THE EXISTING CONFIGURATION WITH WIDTHS REDUCED TO
10' TO 11' FOR THRU LANES AND 10' FOR TURNING LANES UTILIZING HIGHWAY
STANDARD 701701 "SHOULDER OPERATIONS"
- DROP-OFF CONDITIONS BETWEEN TRAVELED WAY AND WORK ZONE DICTATE THAT
CHANNELIZING DEVICES BE USED AT 50' INTERVALS AND 20' INTERVALS AT RADII
AND LANE CLOSURE TAPERS
- LITTLIZE HIGHWAY STANDARD 701602 FOR LANE SHIFTS

- UTILIZE HIGHWAY STANDARD 701602 FOR LANE SHIFTS



WESTBOUND LANES

EASTBOUND LANES

STAGE 1B - PAVEMENT WIDENING & CC&G INSTALLATION

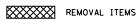
STA. 20+50 TO STA. 23+91

LEGEND

- (1) TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE
- (2) PAVEMENT MARKING TAPE, TYPE III, 4" (YELLOW)
- (3) PAVEMENT MARKING TAPE, TYPE III, 6" (WHITE)
- (4) PAVEMENT MARKING TAPE, TYPE III, 4" (WHITE)



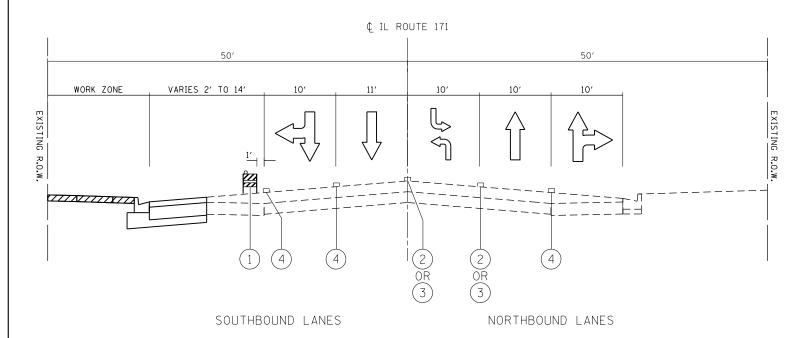
DIRECTION OF TRAFFIC



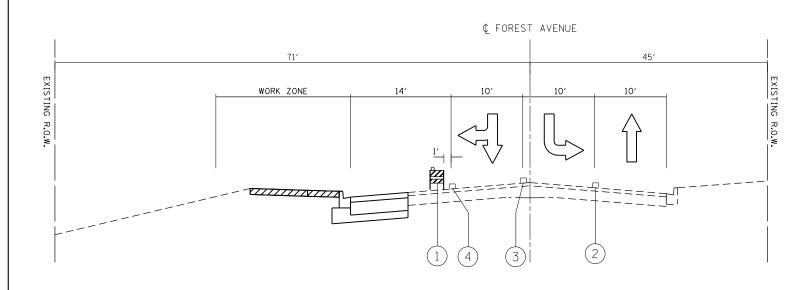


CONSTRUCTION ITEMS

FILE NAME =	USER NAME = Tariqfm	DESIGNED -	REVISED -		li li	ROUTE	171 AT F	OREST	AVF /RI	IDGEWOOD RD.		F.A.P.	SECTION	COUNTY	TOTAL	SHEET S NO.
c:\pw_work\pwidot\tariqfm\d0315748\P1144	2-Stagel.dgn	DRAWN -	REVISED -	STATE OF ILLINOIS	SUGGESTED STAGING TYPICAL SECTIONS - STAGE 1B		372	0303S-TS	соок	87	28					
	PLOT SCALE = 200.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION			ID	i		CONTRAC	T NO.	60X08				
Default	PLOT DATE = 3/31/2014	DATE -	REVISED -	SCALE: SHEET OF SHEETS STA. TO STA.		TA.		ILLINOIS FEF	ILLINOIS FED. AID PROJECT							



STAGE 1C - SIDEWALK INSTALLATION & GRADING STA. 37+40 TO STA. 48+67



WESTBOUND LANES

STAGE 1C - SIDEWALK INSTALLATION & GRADING

EASTBOUND LANES

STA. 20+50 TO STA. 23+91

STAGE 1C:

- INSTALLATION OF SIDEWALK/SHARED-USE PATHS AND TRAFFIC SIGNALS
 MAINTAIN LANES IN THE EXISTING CONFIGURATION WITH WIDTHS REDUCED TO
 10' TO 11' FOR THRU LANES AND 10' FOR TURNING LANES UTILIZING HIGHWAY STANDARD
 701701 "SHOULDER OPERATIONS"
 DROP-OFF CONDITIONS BETWEEN TRAVELED WAY AND WORK ZONE DICTATE THAT
 CHANNELIZING DEVICES BE USED AT 50' INTERVALS AND 20' INTERVALS AT RADII
- AND LANE CLOSURE TAPERS
 UTILIZE HIGHWAY STANDARD 701602 FOR LANE SHIFTS

POST-STAGE:

- RESURFACING OF ALL LEGS WITHIN THE PROJECT LIMITS
 PAVEMENT MARKING PLACEMENT UTILIZING THE PROPOSED TRAFFIC PATTERN
 HIGHWAY STANDARD 701801 WILL BE UTILIZED TO CLOSE THE SHARED-USE PATH ON THE NORTH SIDE OF FOREST AVENUE, BETWEEN IL ROUTE 171 AND GROVELAND AVENUE TO THE EAST, AND THE SIDEWALK ON THE WEST SIDE OF THE IL ROUTE 171, BETWEEN PARKVIEW AVENUE AND FOREST AVENUE, DURING CONSTRUCTION. WESTBOUND PEDESTRIAN AND BICYCLE TRAFFIC ALONG FOREST AVENUE WILL BE DETOURED AT GROVELAND AVENUE TO THE EXISTING SHARED-USE PATH ON THE SOUTH SIDE OF FOREST AVENUE. NORTHBOUND PEDESTRIAN AND BICYCLE TRAFFIC ALONG IL ROUTE 171 WILL BE DETOURED AT PARKVIEW AVENUE TO THE EXISTING SIDEWALK ON THE EAST SIDE OF IL ROUTE 171.

LEGEND

- (1) TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE
- (2) PAVEMENT MARKING TAPE, TYPE III, 4" (YELLOW)
- (3) PAVEMENT MARKING TAPE, TYPE III, 6" (WHITE)
- (4) PAVEMENT MARKING TAPE, TYPE III, 4" (WHITE)



DIRECTION OF TRAFFIC



REMOVAL ITEMS



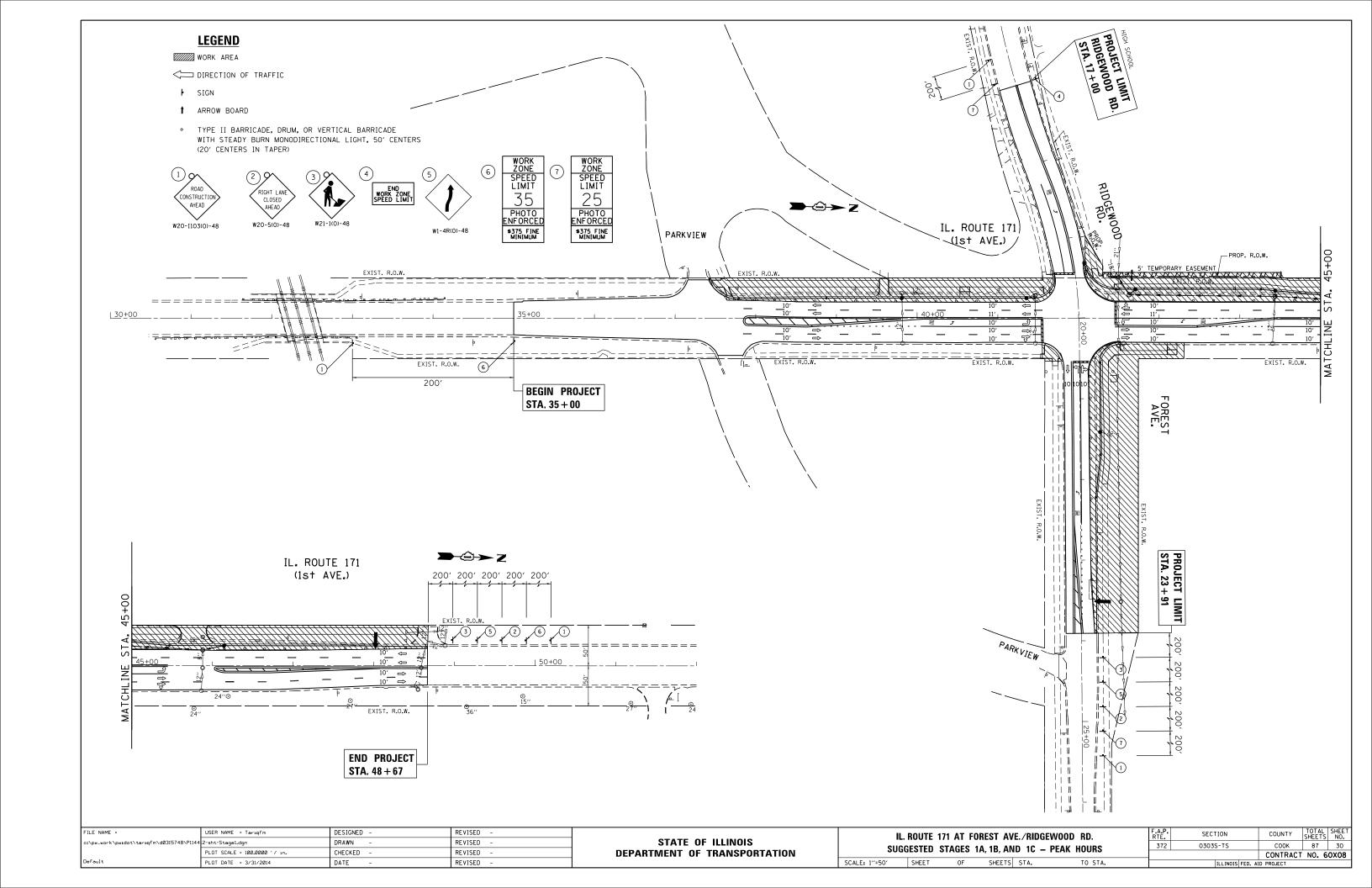
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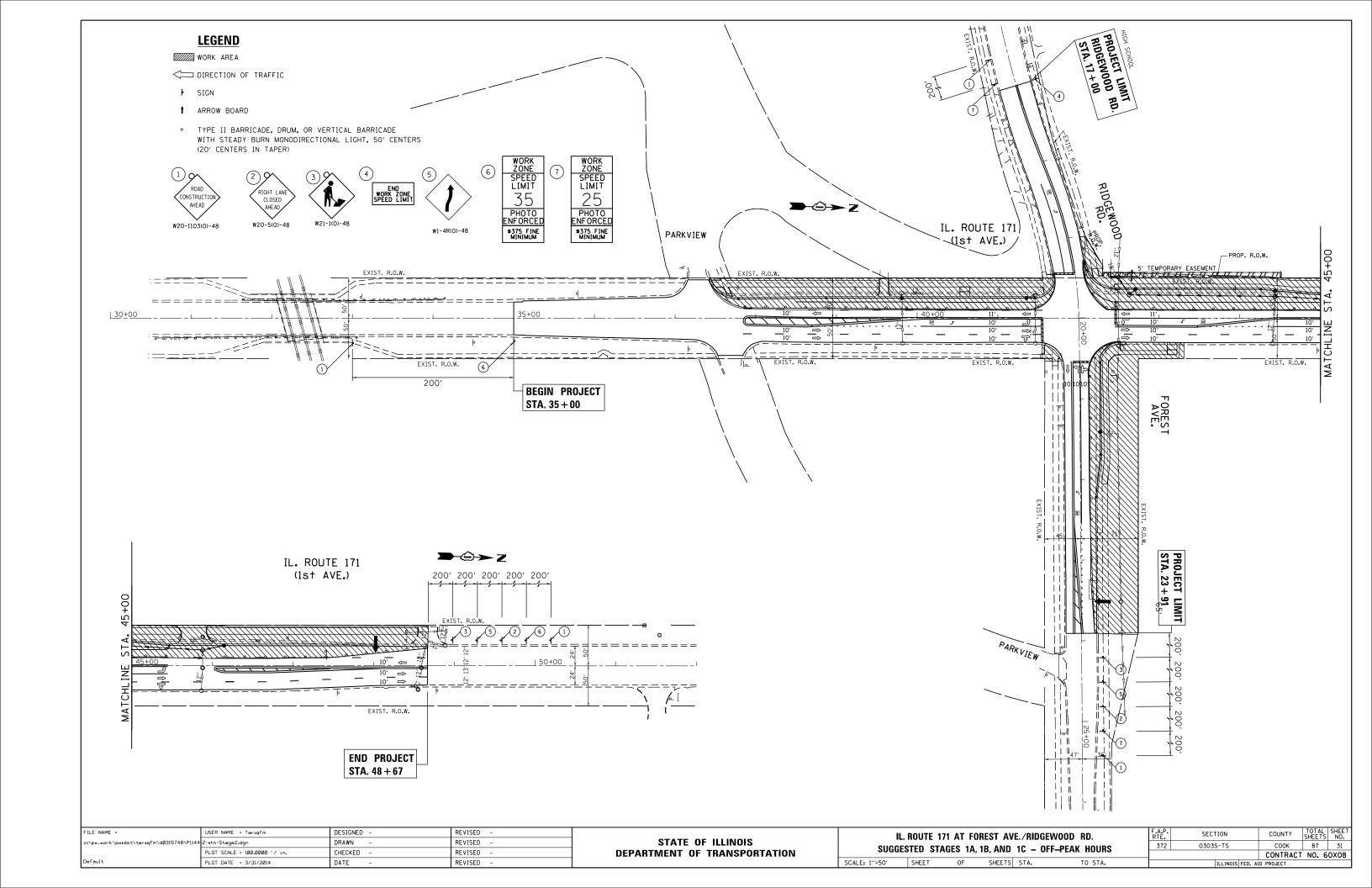
CONSTRUCTION ITEMS

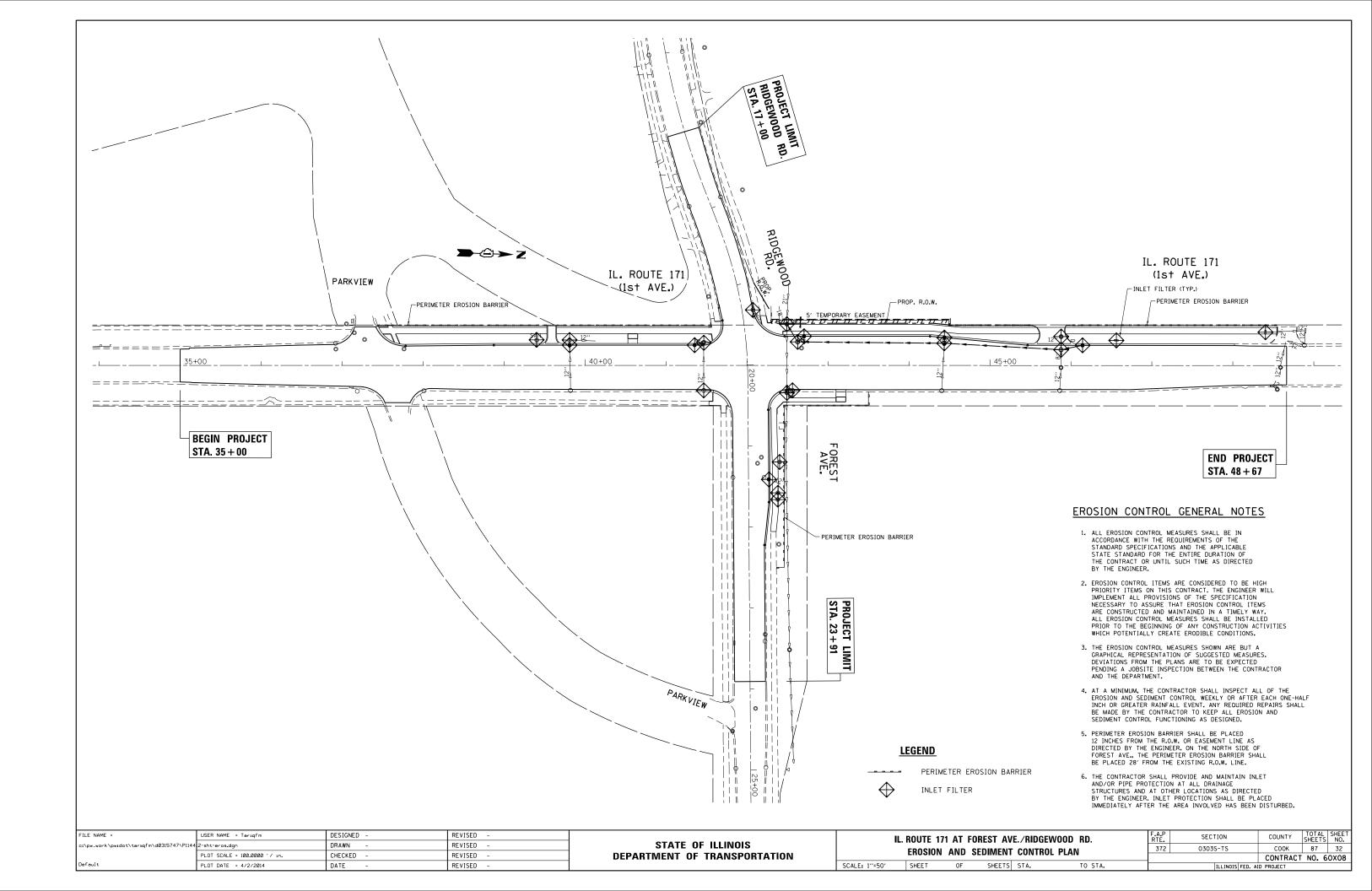
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Default	PLOT DATE = 3/31/2014	DATE -	REVISED -

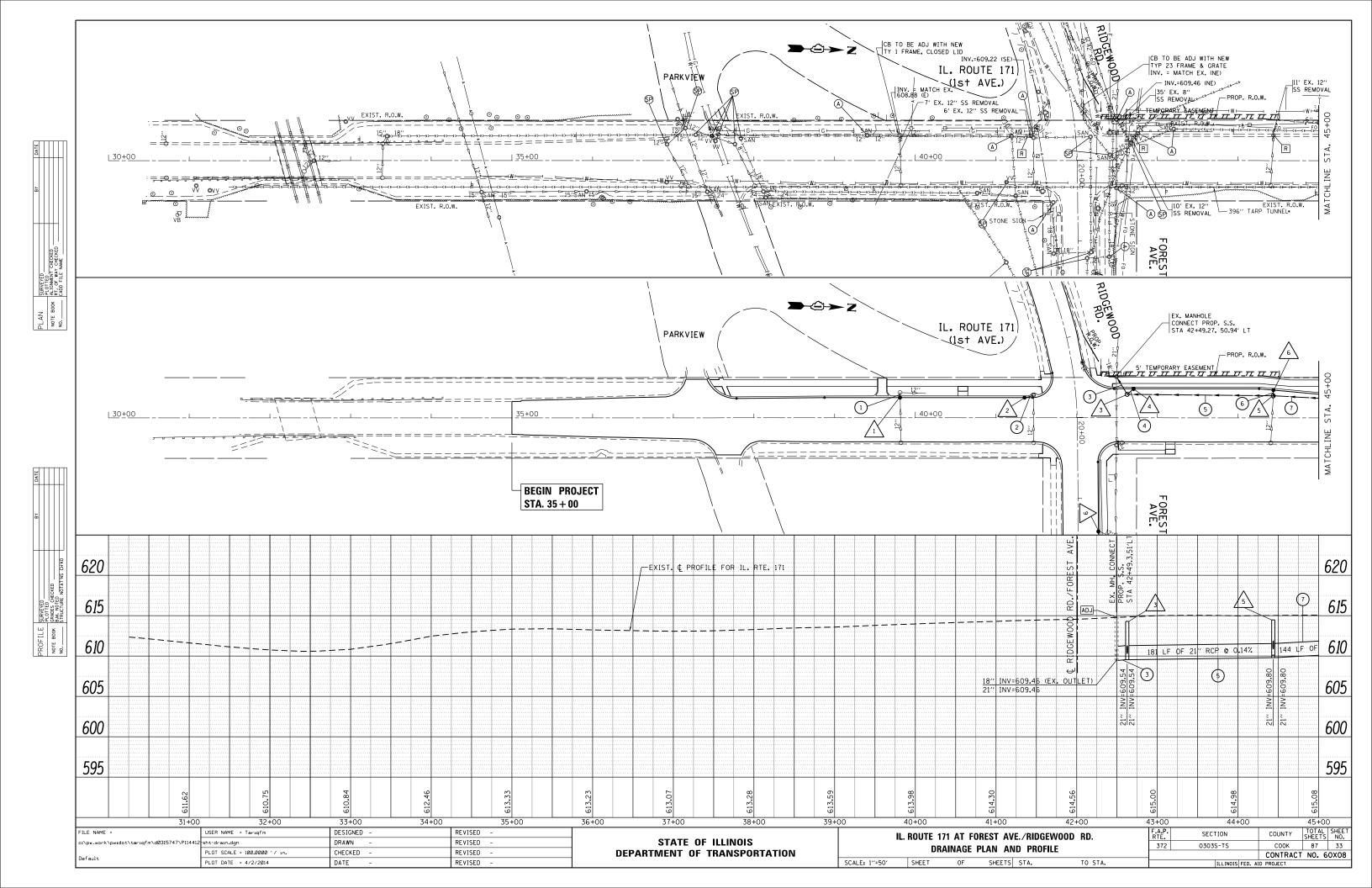
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

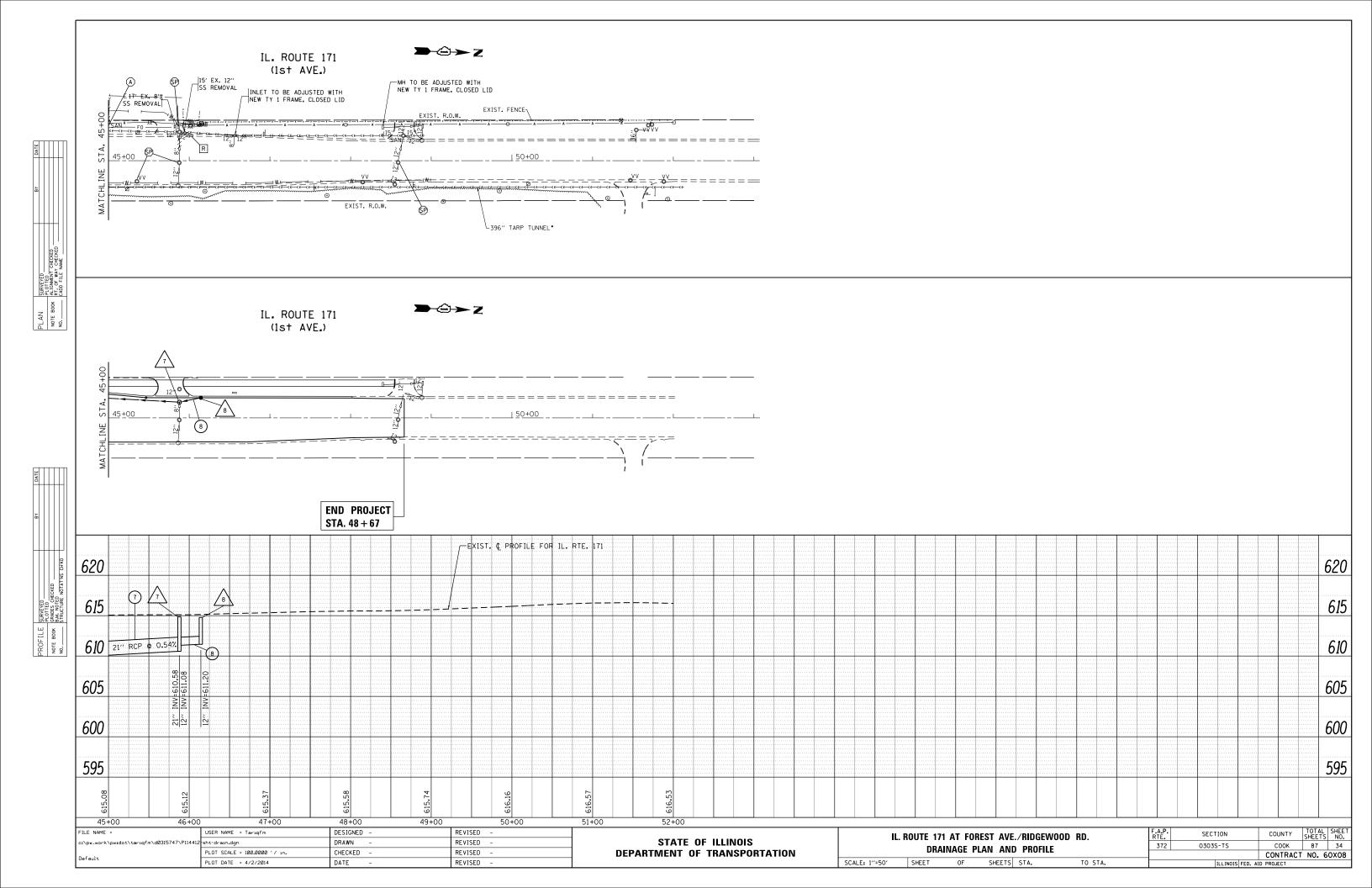
IL. ROUTE 171 AT FOREST AVE / RIDGEWOOD RD.							SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SUGGESTED STAGING TYPICAL SECTIONS - STAGE 1C						372	0303S-TS	COOK	87	29
Jour	Oddisted Stading Tifical Scotlons - Stade to							CONTRACT	NO. 6	80X0
	SHEET	OF	SHEETS	STA.	TO STA.		TILINOIS EED AT	n ppn iect		

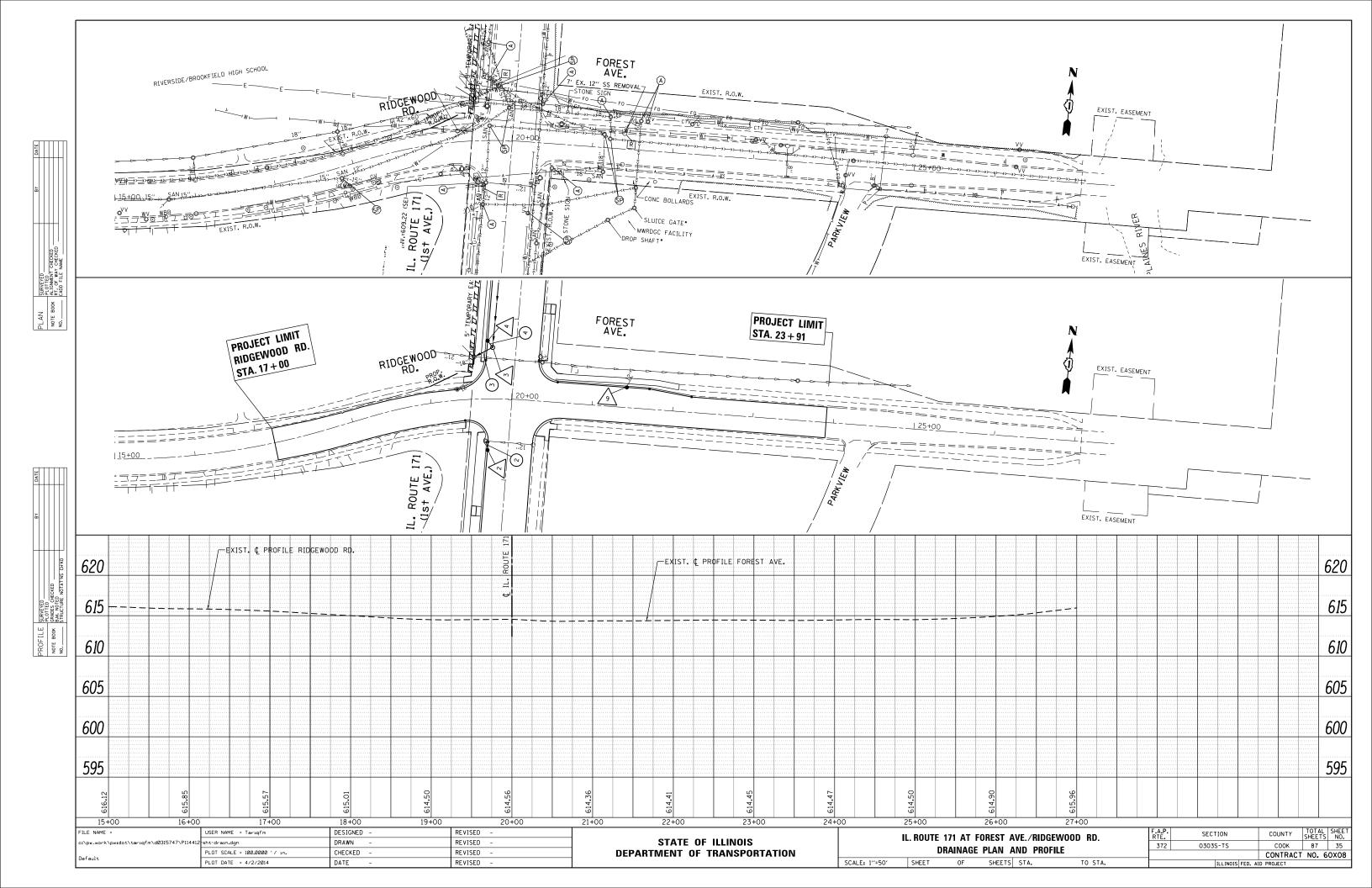












DRAINAGE STRUCTURES:

FLAT TOP

CB, TY-A, 4' DIA W/ TY-23 F&G STA 39+81.2 (IL 171), 25.8' LT RIM 613.35 INV MATCH EX 608.94 (E) INV 608.94 (W)



CB, TY-C W/ TY-23 F&G STA 41+35.4 (IL 171), 25.8' LT RIM 613.56 INV 609.50 (NW)



MH, TY-A, 4' DIA
W/ 9" RESTRICTOR
W/ TY-1 FR & CLID
STA 42+62.6 (IL 171), 29.0' LT
RIM 614.30
INV 609.54 (N)
INV 609.54 (SW)
INV 609.54 (RW)
INV 609.54 (RESTRICTOR)
OVERFLOW ELEVATION 613.11
• NOTE: NOTE *3 IN DISTRICT DETAIL BD-12 SHALL BE DISREGARDED.



CB, TY-C W/ TY-23 F&G STA 42+70.6 (IL 171), 36.9' LT RIM 614.15 INV 609.75 (SE)



MH, TY-A, 5' DIA W/ TY-1 FR & CLID STA 44+43.6 (IL 171), 27.4' LT RIM 614.48 INV 609.80 (N) INV 609.80 (S) INV MATCH EX 609.84 (E) INV 610.30 (W)



CB, TY-C W/ TY-23 F&G STA 44+43.6 (IL 171), 35.5' LT RIM 614.32 INV 610.45 (E)

STORM SEWERS:

1 12" DIA SS, CL A, TY 2 LENGTH = 7' SLOPE = 0.9% TBF = 2.4 CU. YD.

2 | 12" DIA SS, CL A, TY 2 LENGTH = 12' SLOPE = 2.3% TBF = 3.1 CU. YD.

3 21" DIA SS, CL A, TY 2 LENGTH = 26' SLOPE = 0.3% TBF = 10.8 CU. YD.

4 | 12" DIA SS, CL A, TY 2 LENGTH = 11' SLOPE = 1.9% TBF = 3.3 CU. YD.

5 21" DIA SS, CL A, TY 1 LENGTH = 181' SLOPE = 0.1% TBF = 47.5 CU. YD.

6 | 12" DIA SS, CL A, TY 2 LENGTH = 8' SLOPE = 1.9% TBF = 2.1 CU. YD.

7 21" DIA SS, CL A, TY 1 LENGTH = 144" SLOPE = 0.5% TBF = 31.7 CU. YD.

8 | 12" DIA SS, CL A, TY 1 LENGTH = 27' SLOPE = 0.4% TBF = 5.0 CU. YD.

FLAT	TOP	

MH, TY-A, 4' DIA
W/ TY-1 FR & CLID
STA 45+87.9 (IL 171), 19.4' LT
RIM 614.85
INV 611.08 (NW)
INV 610.58 (S)
INV MATCH EX (E)
• NOTE: MANHOLE SHALL BE CONSTRUCTED TO A DEPTH OF 10'
(TOP OF MASONRY TO TOP OF BOTTOM SLAB)



CB, TY-C W/ TY-23 F&G STA 46+14.6 (IL 171), 25.7' LT RIM 614.78 INV 611.20 (SE)

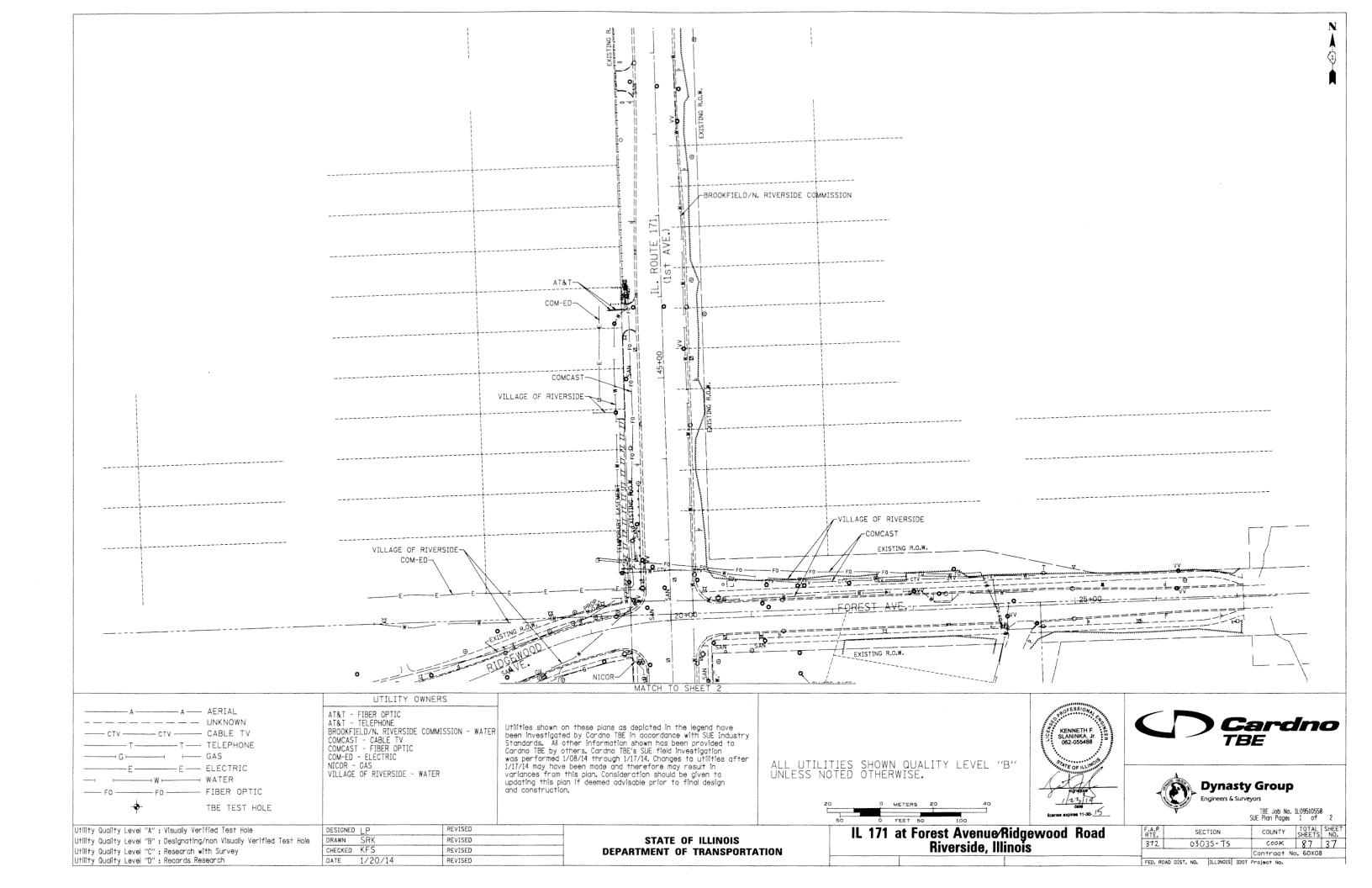


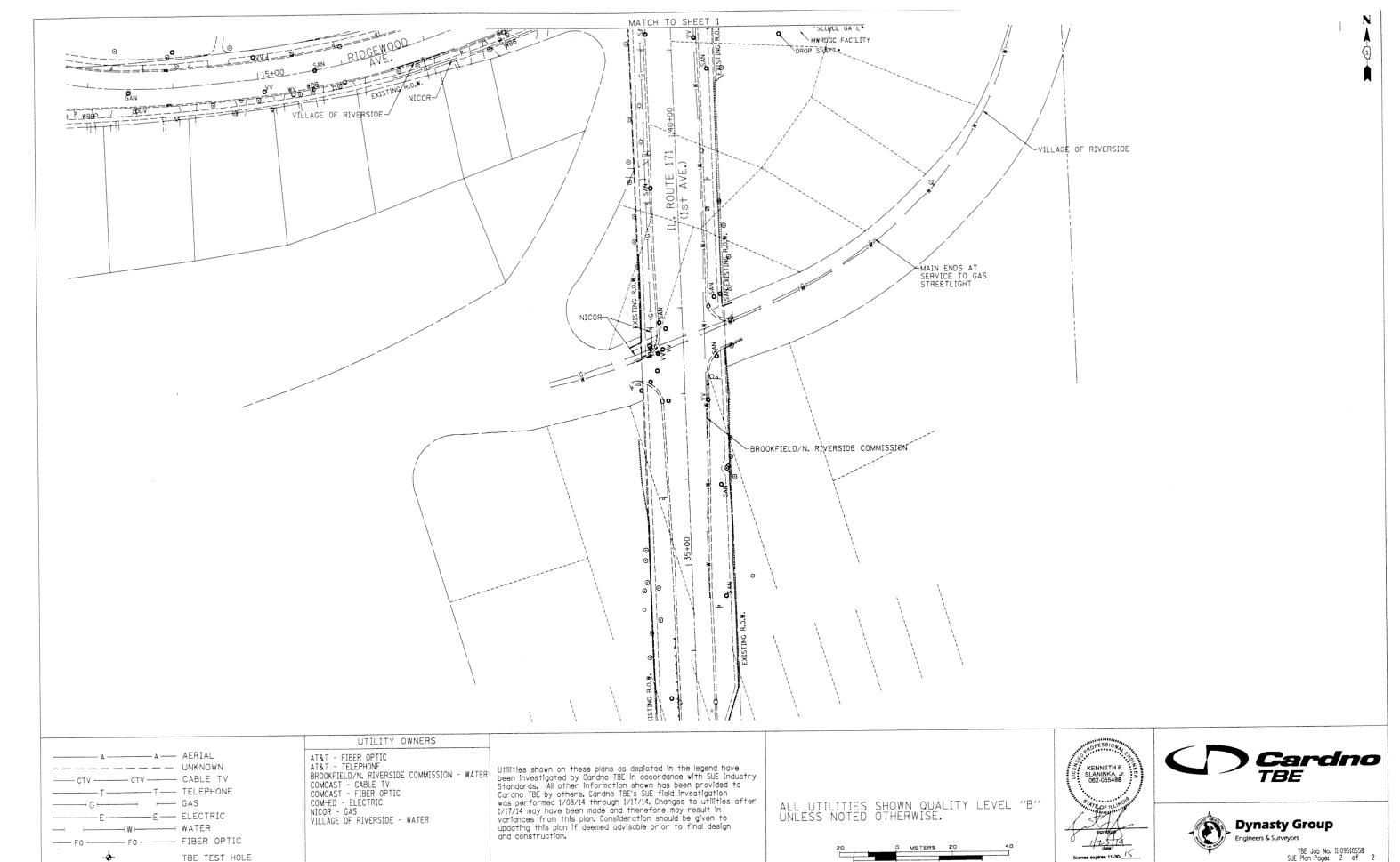
CB, TY-C W/ TY-12 F&G STA 21+41.2 (FOREST AVE), 25.9' LT RIM 613.53 INV MATCH EX (NE)



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:





STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

REVISED

REVISED

REVISED

DRAWN SRK

CHECKED KFS

DATE 1/20/14

Utility Quality Level "A" : Visually Verified Test Hole

Utility Quality Level "C" : Research with Survey Utility Quality Level "D" : Records Research

Utility Quality Level "B": Designating/non Visually Verified Test Hole

IL 171 at Forest Avenue/Ridgewood Road
Riverside, Illinois

FED. ROAD DIST. NO. | ILLINOIS | IDOT Project No. |

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS

PLAT OF HIGHWAYS

PARCEL NUMBER	OWNER	SHEET NUMBER	PROPERTY ACQUIRED BY
OK90001 OK90001TE	RIVERSIDE BROOKFIELD HIGH SCHOOL DISTRICT NO. 208 OF COOK COUNTY, ILLINOIS WHICH ACQUIRED TITLE AS THE TRUSTEES OF SCHOOLS OF TOWNSHIP 39 NORTH, RANGE 12 EAST OF THE 3RD PRINCIPAL MERIDIAN, COOK COUNTY, ILLINOIS IN TRUST FOR THE RIVERSIDE BROOKFIELD HIGH SCHOOL, BEING SCHOOL DISTRICT NO. 208	3	

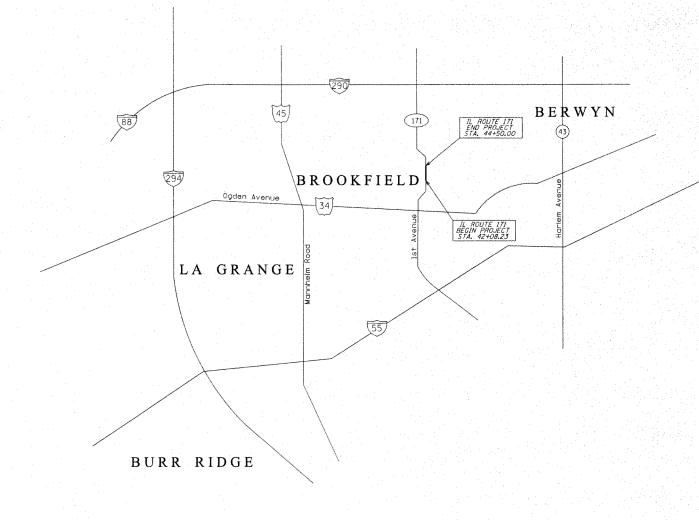
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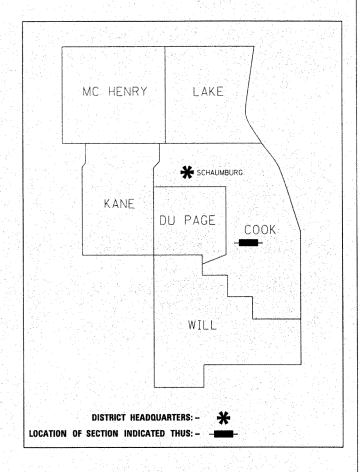
IL ROUTE	171 () () () () () () () () () (
SECTION	
COOK COL	UNTY I THE REPORT OF THE PARTY
LIMITS	Forest Ave./Ridgewood Ave
JOB NO.:	R-90-024-13

llinois State	Plane, East 2	one, NAD	83 (200	07)	
JS ROUTE 17	1 CENTERLIN	E			
Pt. Number	Station	Offset		Northing	Easting
503	42+08.24	102.04	LT.	1880334.806	1121641.928
504	42+30.00	64.94	LT.	1880357.801	1121678.279
505	42+30.00	56.94	LT.	1880358.068	1121686.274
506	42+30.00	51.94	LT.	1880358.235	1121691.271
507	44+30.00	52.03	LT.	1880558.120	1121684.479
508	44+50.00	50.04	LT.	1880578.175	1121685.799
509	44+50.00	57.04	LT.	1880577.941	1121678.803
2138	42+08.23	49.93	LT.	1880336.548	1121694.010
7000	42+00.66	0.00	LT.	1880330.656	1121744.161
		-			
RIDGEWOOD	AVE. CENTE	RIINE			
		1161146		i	
Pt. Number	Station	Offset		Northing	Easting
Pt. Number 503			LT.	Northing 1880334.806	Easting 1121641.928
	Station	Offset	LT.		
503	Station 19+02.00	Offset 20.81		1880334.806	1121641.928
503 504	Station 19+02.00 19+40.45	Offset 20.81 35.31	LT.	1880334.806 1880357.801	1121641.928 1121678.279
503 504 505	\$tation 19+02.00 19+40.45 19+47.88	Offset 20.81 35.31 34.17	LT. LT.	1880334.806 1880357.801 1880358.068	1121641.928 1121678.279 1121686.274 1121691.271
503 504 505 506	Station 19+02.00 19+40.45 19+47.88 19+52.54	Offset 20.81 35.31 34.17 33.51	LT. LT. LT.	1880334.806 1880357.801 1880358.068 1880358.235	1121641.928 1121678.279 1121686.274
503 504 505 506 507	Station 19+02.00 19+40.45 19+47.88 19+52.54 19+70.08	Offset 20.81 35.31 34.17 33.51 232.33	Г. Г. Г.	1880334.806 1880357.801 1880358.068 1880358.235 1880558.120	1121641.928 1121678.279 1121686.274 1121691.271 1121684.479 1121685.799
503 504 505 506 507 508	\$tation 19+02.00 19+40.45 19+47.88 19+52.54 19+70.08 19+72.69	Offset 20.81 35.31 34.17 33.51 232.33 525.06	LT. LT. LT. LT.	1880334.806 1880357.801 1880358.068 1880358.235 1880558.120 1880578.175	1121641.928 1121678.279 1121686.274 1121691.271 1121684.479 1121685.799 1121678.803
503 504 505 506 507 508 509	Station 19+02.00 19+40.45 19+47.88 19+52.54 19+70.08 19+72.69 19+67.98	Offset 20.81 35.31 34.17 33.51 232.33 525.06 252.72	LT. LT. LT. LT. LT.	1880334.806 1880357.801 1880358.068 1880358.235 1880558.120 1880578.175 1880577.941	1121641.928 1121678.279 1121686.274 1121691.271 1121684.479 1121685.799 1121678.803 1120876.603
503 504 505 506 507 508 509 2009	\$\text{Station}\$ 19+02.00 19+40.45 19+47.88 19+52.54 19+70.08 19+72.69 19+67.98 10+76.70	Offset 20.81 35.31 34.17 33.51 232.33 525.06 252.72 35.97	LT. LT. LT. LT. LT. LT.	1880334.806 1880357.801 1880358.068 1880358.235 1880558.120 1880578.175 1880577.941 1880309.212	1121641.928 1121678.279 1121686.274 1121691.271 1121684.479 1121685.799 1121678.803 1120876.603
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503 504 505 506 507 508 509 2009 2138 7000	\$tation 19+02.00 19+40.45 19+47.88 19+52.54 19+70.08 19+72.69 19+67.98 10+76.70 19+51.79 20+00.40	Offset 20.81 35.31 34.17 33.51 232.33 525.06 252.72 35.97 11.67 0.00	T.	1880334.806 1880357.801 1880358.068 1880358.235 1880558.125 1880578.175 1880577.941 1880309.212 1880336.548 1880330.656	1121641.928 1121678.279 1121686.274 1121691.271 1121684.479 1121685.799 1121678.803 1120876.603 1121694.010 1121744.161 1121093.019
503 504 505 506 507 508 509 2009 2138 7000 7014	\$tation 19+02.00 19+40.45 19+47.88 19+52.54 19+70.08 19+72.69 19+67.98 10+76.70 19+51.79 20+00.40 13+29.06	Offset 20.81 35.31 34.17 33.51 232.33 525.06 252.72 35.97 11.67 0.00 0.00	LT.	1880334.806 1880357.801 1880358.068 1880358.235 1880558.120 1880578.175 1880577.941 1880309.212 1880336.548 1880330.656 1880190.871	1121641.928 1121678.279 1121686.274 1121691.271 1121684.479 1121678.803 112087.6603 1121694.010 1121744.161 1121093.019
503 504 505 506 507 508 509 2009 2138 7000 7014 7015	\$tation 19+02.00 19+40.45 19+47.88 19+52.54 19+70.08 19+72.69 19+67.98 10+76.70 19+51.79 20+00.40 13+29.06 14+21.06	0ffset 20.81 35.31 34.17 33.51 232.33 525.06 252.72 35.97 11.67 0.00 0.00	LT.	1880334.806 1880357.801 1880358.068 1880358.235 1880558.120 1880577.941 1880309.212 1880336.548 1880330.656 1880190.871	1121641.928 1121678.279 1121686.274 1121691.271 1121684.479 1121684.479 11216876.603 1120876.603 1121694.010 1121093.019 1121184.924 1120866.657
503 504 505 506 507 508 509 2009 2138 7000 7014 7015	\$tation 19+02.00 19+40.45 19+47.88 19+52.54 19+70.08 19+72.69 19+67.98 10+76.70 19+51.79 20+00.40 13+29.06 14+21.06 10+84.90	0ffset 20.81 35.31 34.17 33.51 232.33 525.06 252.72 35.97 0.00 0.00 0.00	LT.	1880334.806 1880357.801 1880358.068 1880358.235 1880558.120 1880578.175 1880577.941 1880336.548 1880330.656 1880190.871 1880193.031 1880273.520	1121641.928 1121678.279 1121686.274 1121691.271 1121684.479



LOCATION MAP

GROSS LENGTH = 241.77 FT. = 0.046 MILE



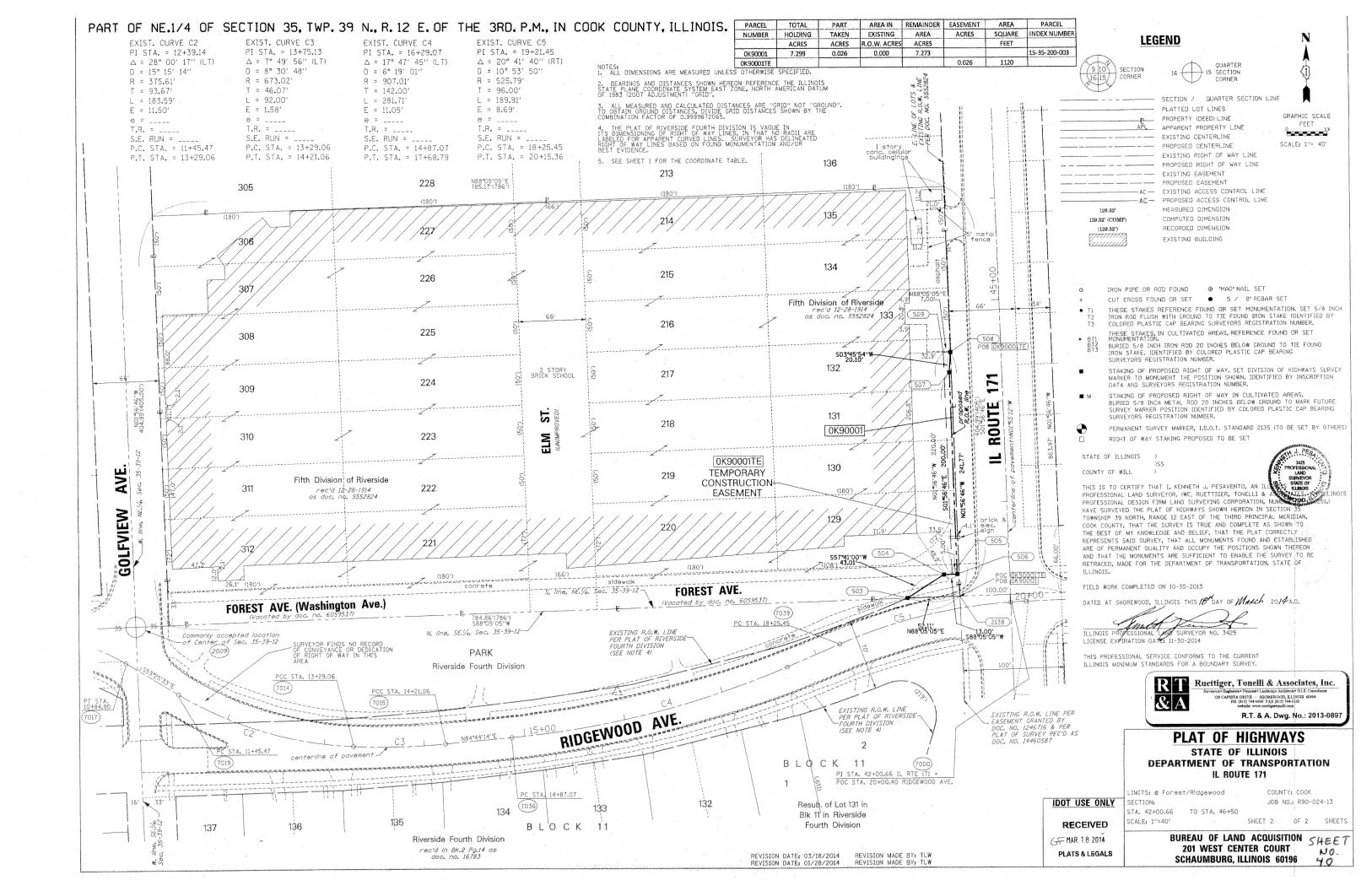
PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

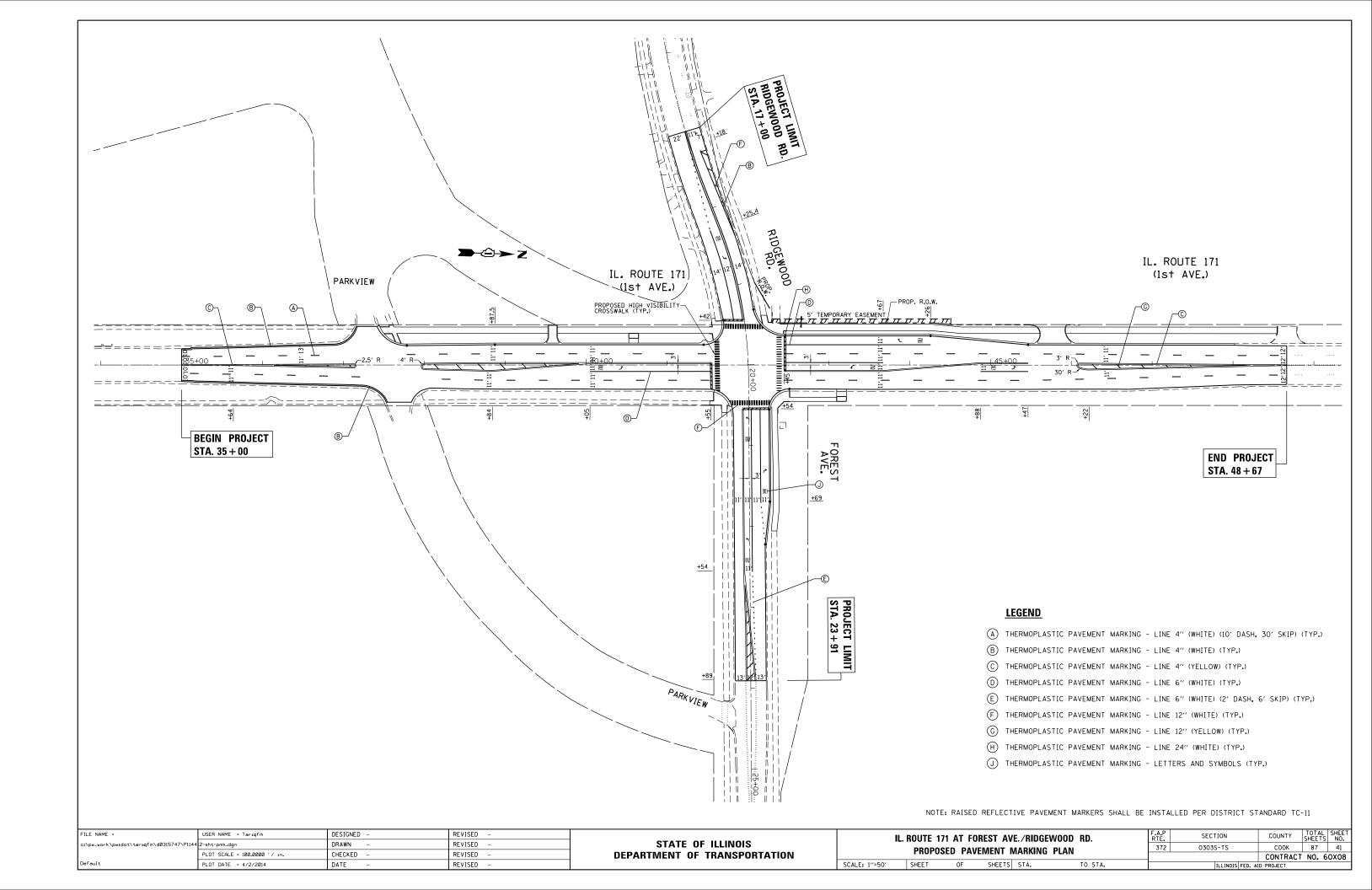


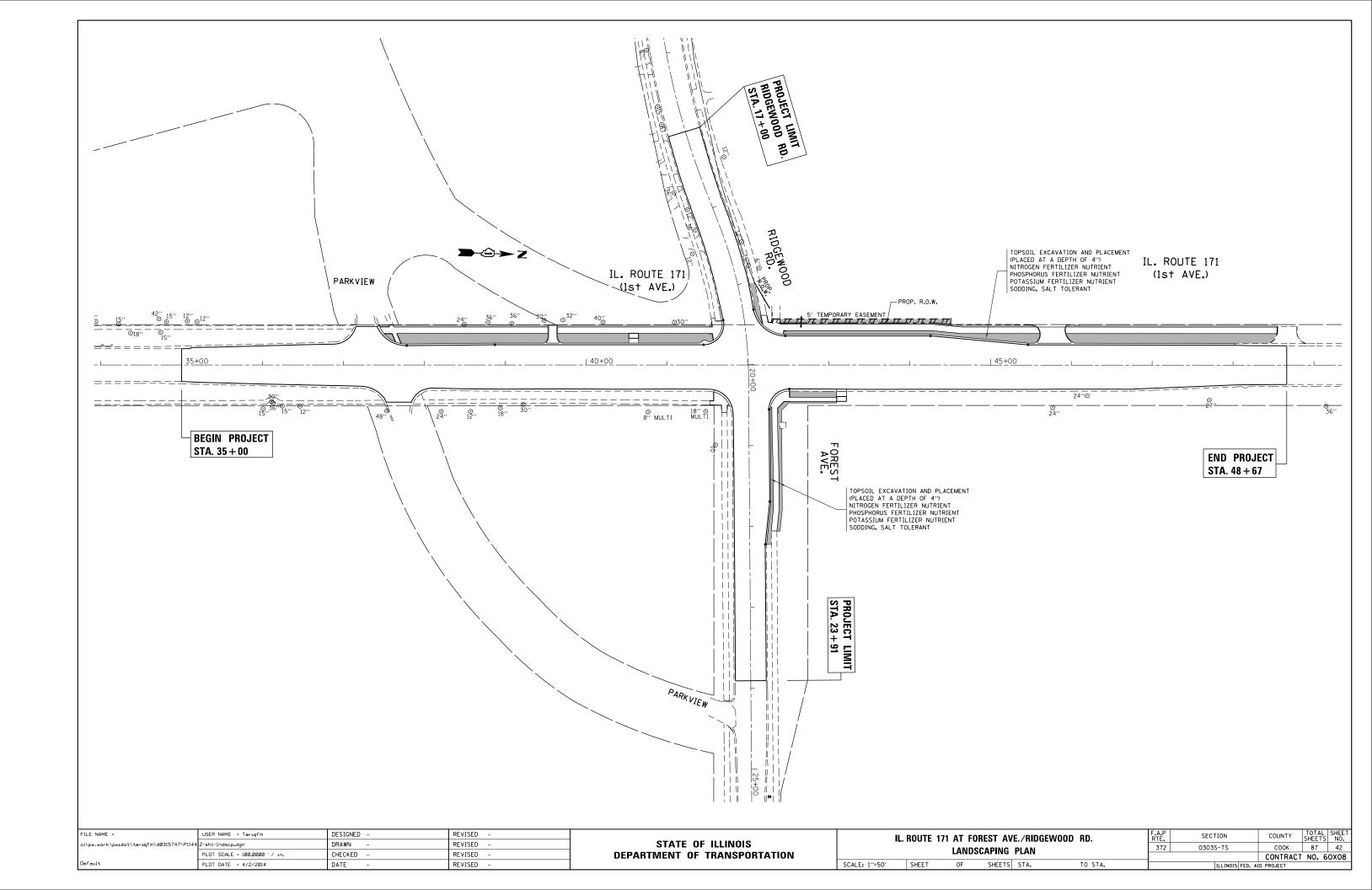
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PLATS & LEGALS







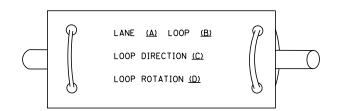
TRAFFIC SIGNAL LEGEND

				IIIAIIIO	JIJIM	CLI					
<u>ITEM</u>	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET	R	\boxtimes		EMERGENCY VEHICLE LIGHT DETECTOR	R ⊗<	———	~	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			<u> </u>
RAILROAD CONTROL CABINET				CONFIRMATION BEACON	R_{o-1}	0—()	←	NO. 11 17 G. GREESS HOTED STREETINGS			
COMMUNICATIONS CABINET	C C	ECC	СС	HANDHOLE	R □			COAXIAL CABLE		<u> </u>	<u> </u>
MASTER CONTROLLER		EMC	MC	HANDIOLE				VENDOD CARLE FOR CAMERA		α	
MASTER MASTER CONTROLLER	R	EMMC	MMC	HEAVY DUTY HANDHOLE	RH	Н	H	VENDOR CABLE FOR CAMERA			
UNINTERRUPTABLE POWER SUPPLY	UPS	EUPS	UPS	DOUBLE HANDHOLE	R D			COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED		<u> </u>	<u>—6</u> —
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT	- <u></u> -R	- <u></u> -	- ■ P	JUNCTION BOX UNDERGROUND CONDUIT,		0	0	FIBER OPTIC CABLE NO. 62.5/125, MM12F		—(12F)—	
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT	R	P	P	GALVANIZED STEEL (UC) TEMPORARY SPAN WIRE, TETHER WIRE,	P			FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F		—24F)—	—24F—
STEEL MAST ARM ASSEMBLY AND POLE	R	0	•	AND CABLE				NO. 62.37123, MIMIZE SMIZE		<i>y</i> -	
ALUMINUM MAST ARM ASSEMBLY AND POLE	R	0		COMMON TRENCH			СТ	FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F		—36F—	—36F)—
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE	R _O → ∞ ——	O-X	• ×	COILABLE NONMETALLIC CONDUIT (EMPTY)			CNC				
STEEL COMBINATION MAST ARM	R _O	Q	•	SYSTEM ITEM		S	S IP	GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE		c	ç⊩⊢
ASSEMBLY AND POLE WITH PTZ CAMERA	r Pizl	PTZ	PIZ	INTERSECTION ITEM REMOVE ITEM	R	1	IF	CONTROLLER CABINET AND	RCF		
SIGNAL POST	R _O	0	•	RELOCATE ITEM	RL			FOUNDATION TO BE REMOVED			
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM	$\overset{R}{\otimes}$	\otimes	•	ABANDON ITEM	А			STEEL MAST ARM POLE AND	ORMF		
GUY WIRE	>R	>	>	12" (300mm) TRAFFIC SIGNAL SECTION		R	R	FOUNDATION TO BE REMOVED ALUMINUM MAST ARM POLE AND	RMF		
SIGNAL HEAD	R →	\rightarrow	-	12" (300mm) RED WITH 8" (200mm)		R		FOUNDATION TO BE REMOVED	0		
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)			→ ²	YELLOW AND GREEN TRAFFIC SIGNAL FACE				STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND	RMF O - X		
SIGNAL HEAD WITH BACKPLATE	+₽ ^R	+	+-			R	R	FOUNDATION TO BE REMOVED	0 /4		
SIGNAL HEAD OPTICALLY PROGRAMMED	R →	>′′P′′	-> "P"	SIGNAL FACE			G 4 Y	SIGNAL POST AND FOUNDATION TO BE REMOVED	RPF O		
FLASHER INSTALLATION (S DENOTES SOLAR POWER)	R ○-'>''F''	O-t>"F"	● ►"F"			↔ S	♣Υ ◆G	INTERSECTION & SAMPLING (SYSTEM) DETECTOR			IS
PEDESTRIAN SIGNAL HEAD	R -	-0	-1			R	R	SAMPLING (SYSTEM) DETECTOR			S
PEDESTRIAN PUSHBUTTON DETECTOR	R (10)	©	©	SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD			Y G	OUEUE DETECTOR			0
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR	R APS	@APS		"RB" INDICATES REFLECTIVE BACKPLATE		₹Ŷ € 0	← Y ← G				
ILLUMINATED SIGN "NO LEFT TURN"	R		9			"P"	"P"	PREFORMED QUEUE DETECTOR		ÎPOÎ	PO
ILLUMINATED SIGN				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL		(DW) W		PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		PIS	PIS
"NO RIGHT TURN"	R			12" (300mm) PEDESTRIAN SIGNAL HEAD				PREFORMED SAMPLING (SYSTEM) DETECTOR		PS	PS
DETECTOR LOOP, TYPE I		[]]		INTERNATIONAL SYMBOL, OUTLINED						↓ →	→
PREFORMED DETECTOR LOOP		94 P 6	P	12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID		(₽ ☆	RAILROAD	SYMBO	OLS	
MICROWAVE VEHICLE SENSOR	R M 1	r(M)	M	PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER		● C ⑤ D	₽ C ☆ D			<u>EXISTING</u>	PROPOSED
VIDEO DETECTION CAMERA	R	(V)	V	RADIO INTERCONNECT	- - R	##+0		RAILROAD CONTROL CABINET			R►∢R
VIDEO DETECTION ZONE								RAILROAD CANTILEVER MAST ARM		X OX X X	X CIXX
	R			RADIO REPEATER	RERR	ERR	RR	FLASHING SIGNAL		⊠o ⊠	X⊕X
PAN, TILT, ZOOM CAMERA	PTZ]		₽TZ I	DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED			_5_	CROSSING GATE		\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\	X⊙X ►
WIRELESS DETECTOR SENSOR WIRELESS ACCESS POINT	R R		W	GROUND CABLE IN CONDUIT		1	(1)	CROSSBUCK		*	*
FILE NAME = USER NAME = plascencial		DESIGNED - DAG/BCK	REVISED	NO. 6 SOLID COPPER (GREEN) - DAG 1-1-14		~			IF.A.P.	CECTION	COUNTY TOTAL SHE
c:\pw.work\pwidot\plascenciai\d0370834\F114412-TS.dgn PLOT SCALE = 100.0000 '/		DRAWN - BCK CHECKED - DAD	REVISED REVISED		E OF ILLINOI OF TRANSP			DISTRICT ONE Standard traffic signal design details	F.A.P. RTE. 372	SECTION 0303S-TS TS-05	COUNTY TOTAL SHEETS NO COOK 87 4:
PLOT DATE = 3/28/2014		DATE - 10-28-09	REVISED	-			SCALE: NO	NE SHEET NO. 1 OF 7 SHEETS STA. TO STA	A. FED. RO	AD DIST. NO. 1 ILLINOIS FE	

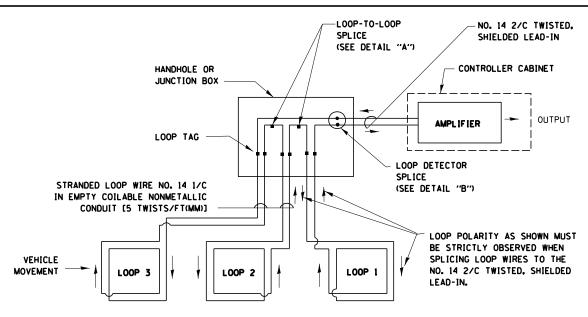
LOOP DETECTOR NOTES

- 1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- 2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER.
 ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- 3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- 4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- 5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- 6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- 7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT I SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

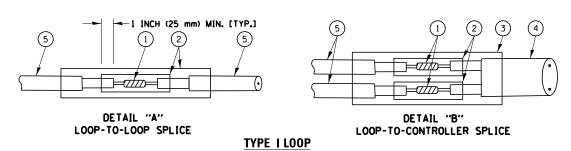


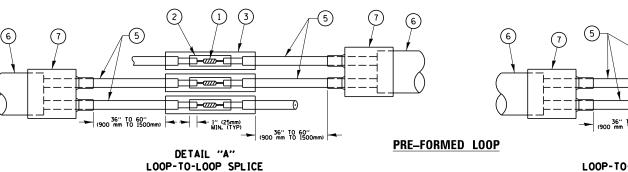
- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP "1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETECTOR LOOP WIRING SCHEMATIC

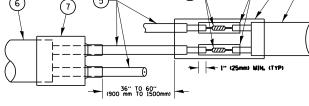
- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.





LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- (2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- (3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.

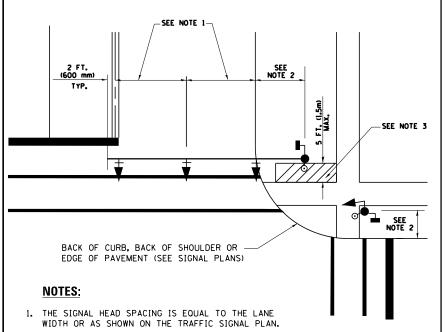


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

- 5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

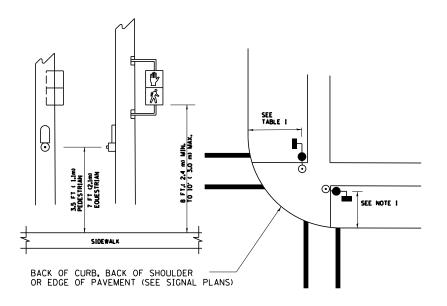
FILE	E NAME =	USER NAME = plascenciai	DESIGNED - DAD	REVISED - DAG 1-1-14		DISTRICT ONE	F.A.P.	SECTION	COUNTY	TOTAL	SHEET NO.
c:/t	pw_work\pwidot\plascenciai\d0370834\F	114412-TS.dgn	DRAWN - BCK	REVISED -	STATE OF ILLINOIS		372	0303S-TS	соок	87	44
		PLOT SCALE = 100.0000 ' / in.	CHECKED - DAD	REVISED -	DEPARTMENT OF TRANSPORTATION	STANDARD TRAFFIC SIGNAL DESIGN DETAILS		TS-05	CONTRACT	r NO. F	0X08
		PLOT DATE = 3/28/2014	DATE - 10-28-09	REVISED -		SCALE: NONE SHEET NO. 2 OF 7 SHEETS STA. TO STA.	FED. ROAD				

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALKBICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



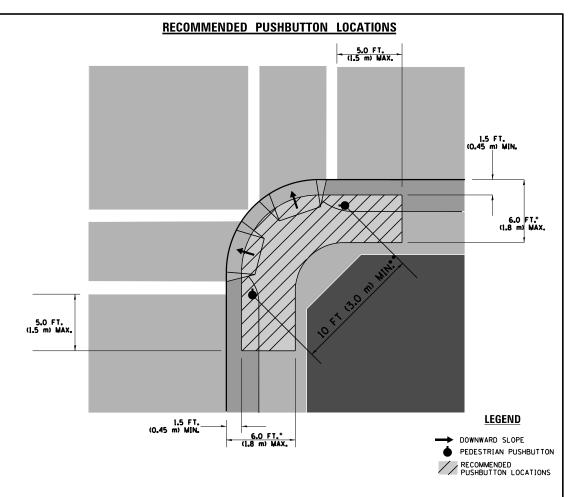
- 2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
- 3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST
- 4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
- 5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

<u>Pedestrian Signal Post</u> <u>and</u> Pedestrian Push Button Post



NOTES:

- 1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
- 2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
- 3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
- 4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."



- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- •• WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

- PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
- 2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
- 3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
- 4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
- THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAYEMENT.

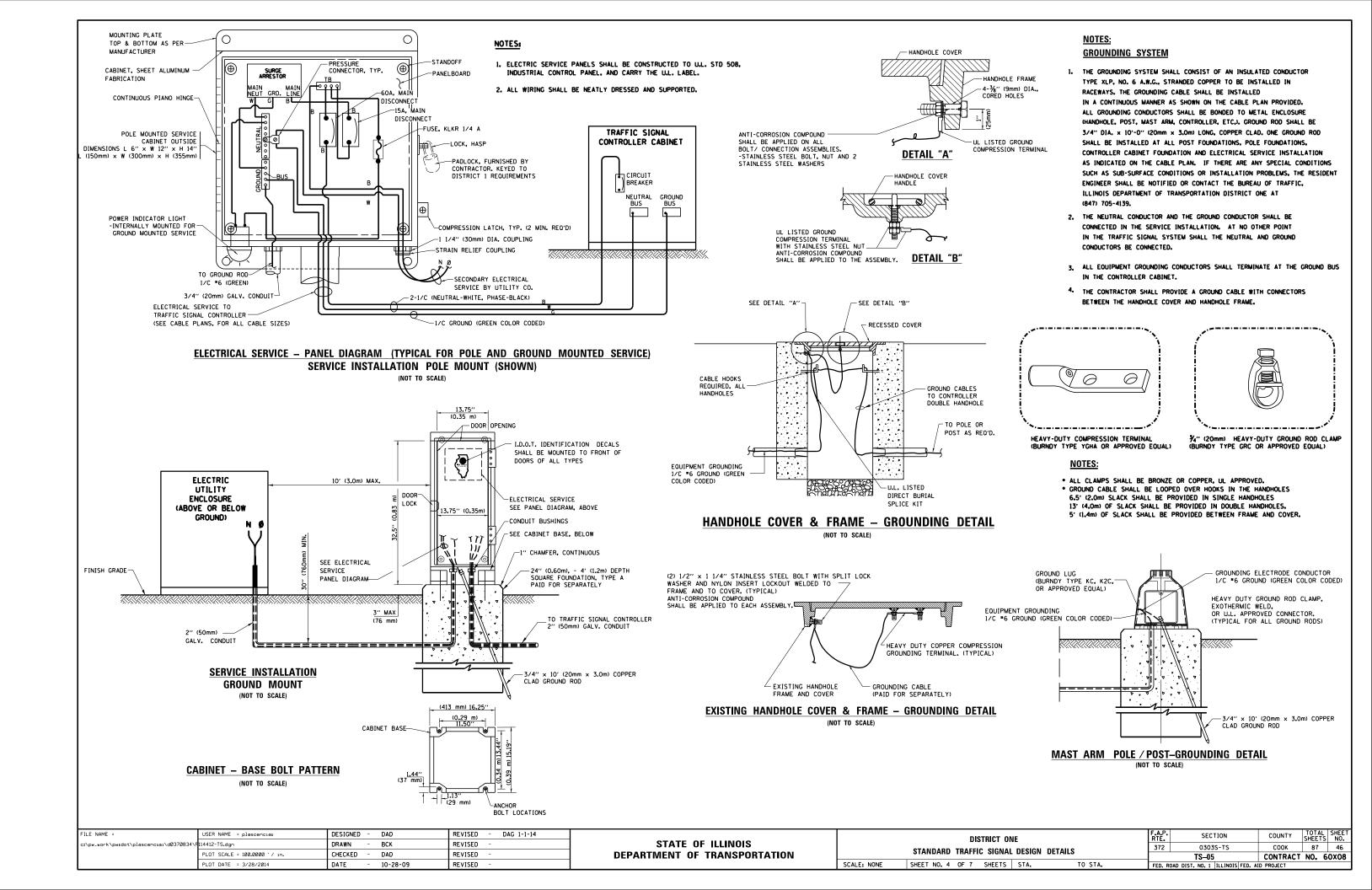
TRAFFIC SIGNAL EQUIPMENT OFFSET

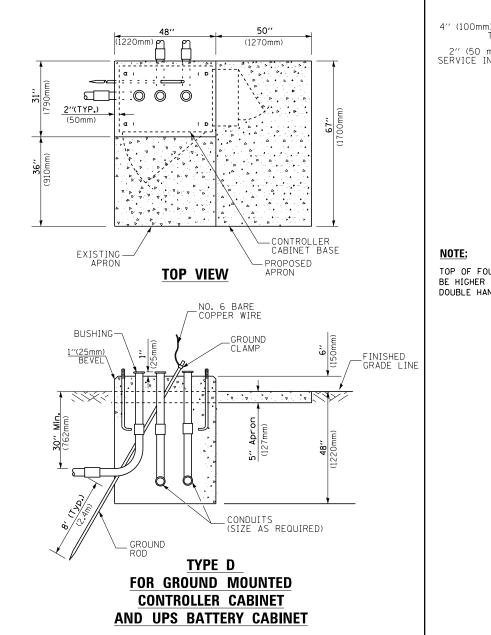
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

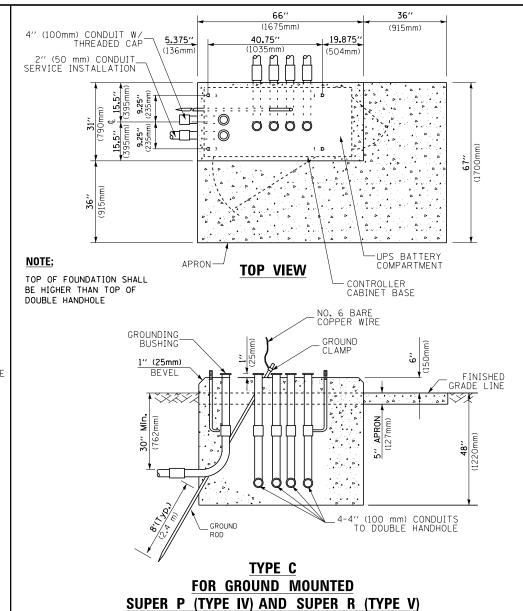
NOTES:

- 1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
- 2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
- 3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
- 4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

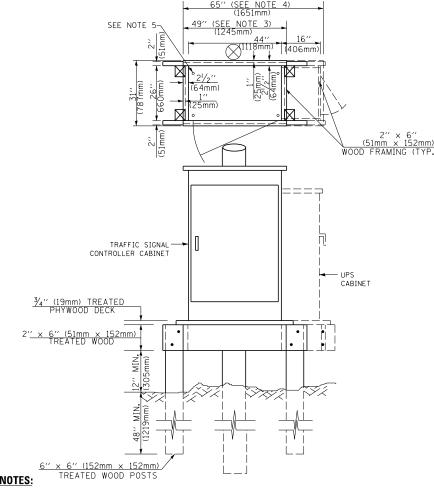
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c:\pw_work\pwidot\plascenciai\d0370834\F	114412-TS.dgn	DRAWN - BCK	REVISED -	STATE OF ILLINOIS	STANDARD TRAFFIC SIGNAL DESIGN DETAILS		372	0303S-TS	соок	87 45		
	PLOT SCALE = 100.0000 '/ in.	CHECKED - DAD	REVISED -	DEPARTMENT OF TRANSPORTATION				TS-05		T NO. 60X08		
	PLOT DATE = 3/28/2014	DATE - 10-28-09	REVISED -		SCALE: NONE SHEET NO.	3 OF 7 SHEETS	STA.	TO STA.	FED. ROAD	DIST. NO. 1 ILLINOIS FED.		1100 001100







CONTROLLER CABINETS



- BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm).
 ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED
- 2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" \times 25" (406mm \times 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
- 3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
- 4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
- 5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
- 6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

TEMPORARY SIGNAL CONTROLLER **WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD)		
(L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL	CABLE	LENGTH
----------	-------	--------

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0'' (1.2m)
SERVICE INSTALLATION. GROUND MOUNT. TYPE A - SOUARE	4'-0'' (1.2m)

DEPTH OF FOUNDATION

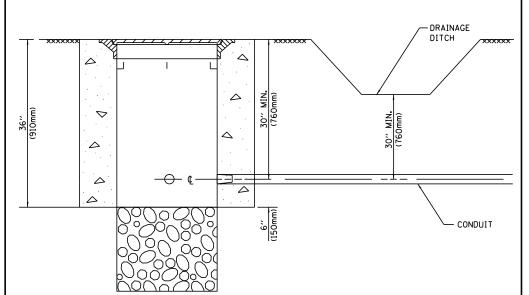
Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
30' (9.1 m) and less than 40' (12.2 m)	11'-0'' (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0'' (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0'' (4.6 m)	36" (900mm)	30'' (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0'' (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

- These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along
 the length of the shaft, with an average Unconfined Compressive Strength (0u) > 1.0 tsf (100 kpa).
 This strength shall be verified by boring data prior to construction or with testing by the Engineer
 during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised
 design if other conditions are encountered.
- 2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
- 3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
- 4. For mast arm assemblies with dual arms refer to state standard 878001...

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

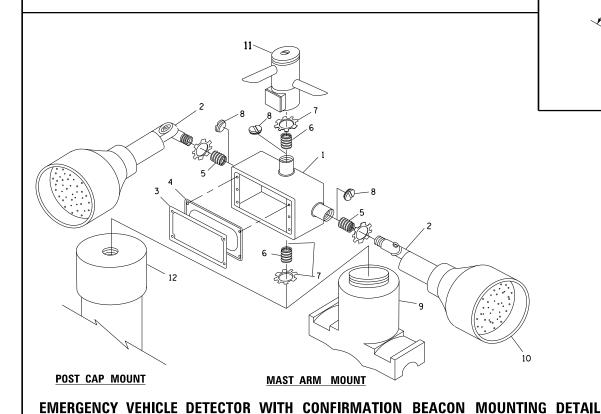
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c:\pw_work\pwidot\plascenciai\d0370834\	F114412-TS.dgn	DRAWN - BCK	REVISED -	STATE OF ILLINOIS				372	0303S-TS	соок	87 47	
	PLOT SCALE = 100.0000 ' / in.	CHECKED - DAD	REVISED -	DEPARTMENT OF TRANSPORTATION	STANDARD TRAFFIC SIGNAL DESIGN DETAILS		ILS		TS-05	CONTRACT	T NO. 60X08	
	PLOT DATE = 3/28/2014	DATE - 10-28-09	REVISED -		SCALE: NONE	SHEET NO. 5 OF 7 SHEETS	STA.	TO STA.	FED. ROAD		ED. AID PROJECT	



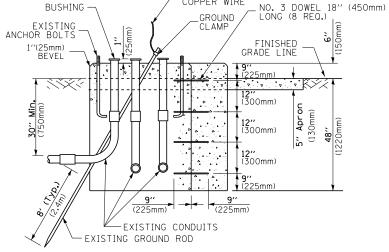
NOTES:

- CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
- 2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
- 3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

HANDHOLE WITH MINIMUM CONDUIT DEPTH (NOT TO SCALE)



66" 36" (1675mm) (915mm) 19.875" (136mm) (504mm) (1035mm) (504mm)



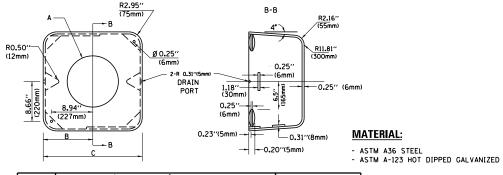
MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION

(NOT TO SCALE)

ITEM NO. IDENTIFICATION 1 OUTLET BOX- GALV. 21 CU.IN. (0,000344 CU-M) 2 LAMP HOLDER AND COVER 3 OUTLET BOX COVER 4 RUBBER COVER GASKET 5 REDUCING BUSHING 6 ½/119 mm) CLOSE NIPPLE 7 ½/119 mm) LOCKNUT 8 ½/119 mm) HOLE PLUG 9 SADDLE BRACKET - GALV. 10 6 WATT PAR 38 LED FLOOD LAMP 11 DETECTOR UNIT 12 POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS *2 AND *11 SHALL BE ALUMINUM OR GALVANIZED
- 2. ITEM *1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
 ITEM *2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
 ITEM *9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- 3. WHEN POST MOUNTING IS SPECIFIED, ITEM *9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 "(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

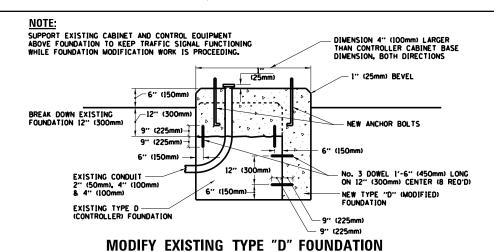


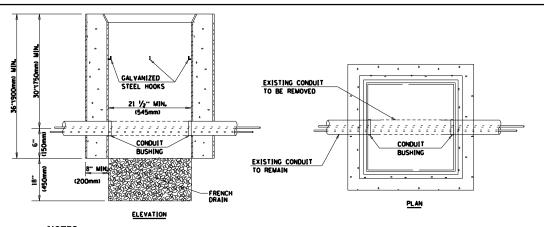
A	В	С	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19''(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37''(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

SHROUD

NOTES:

- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD.
 THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
- 2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
- 3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.





NOTES:

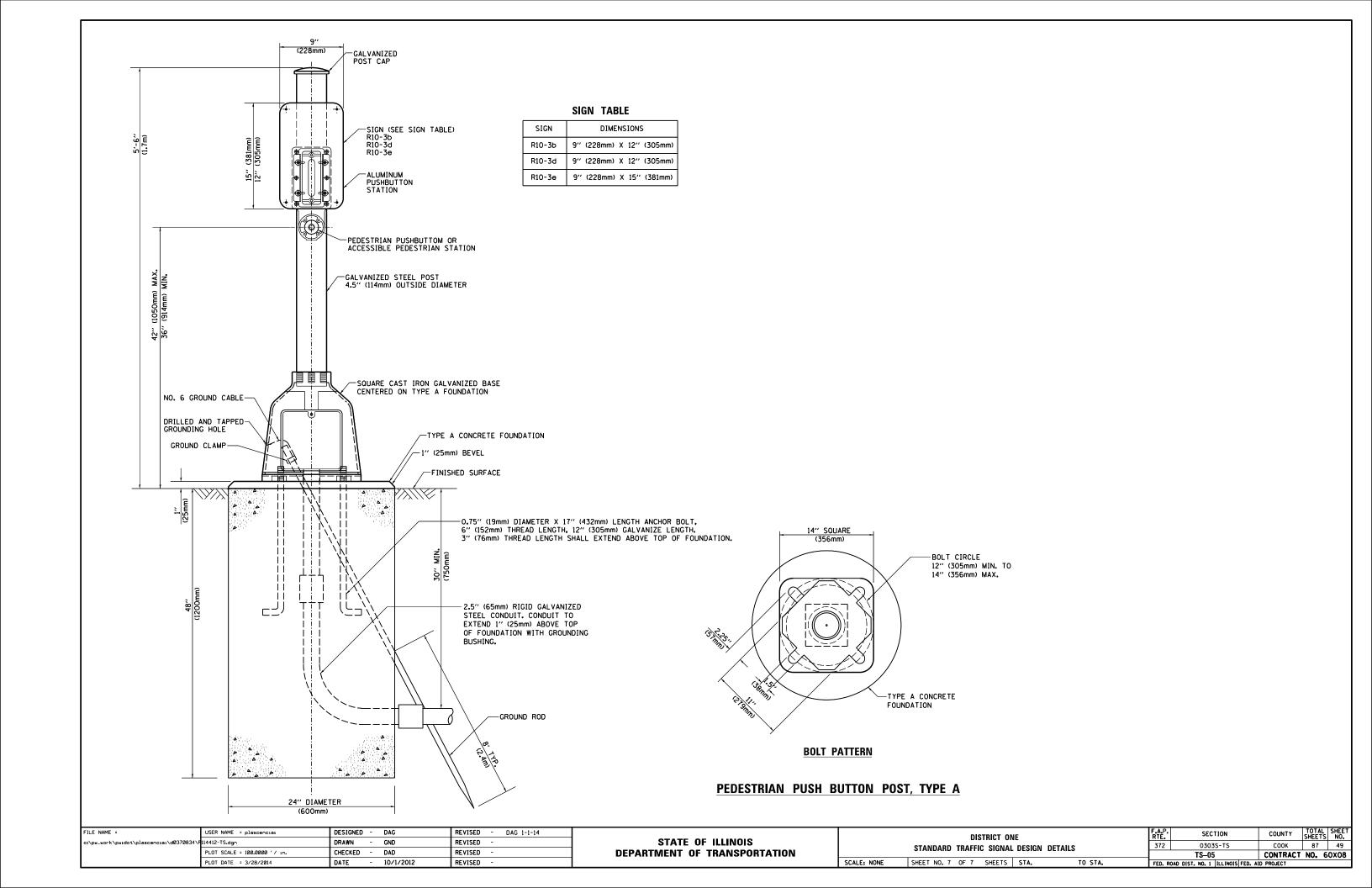
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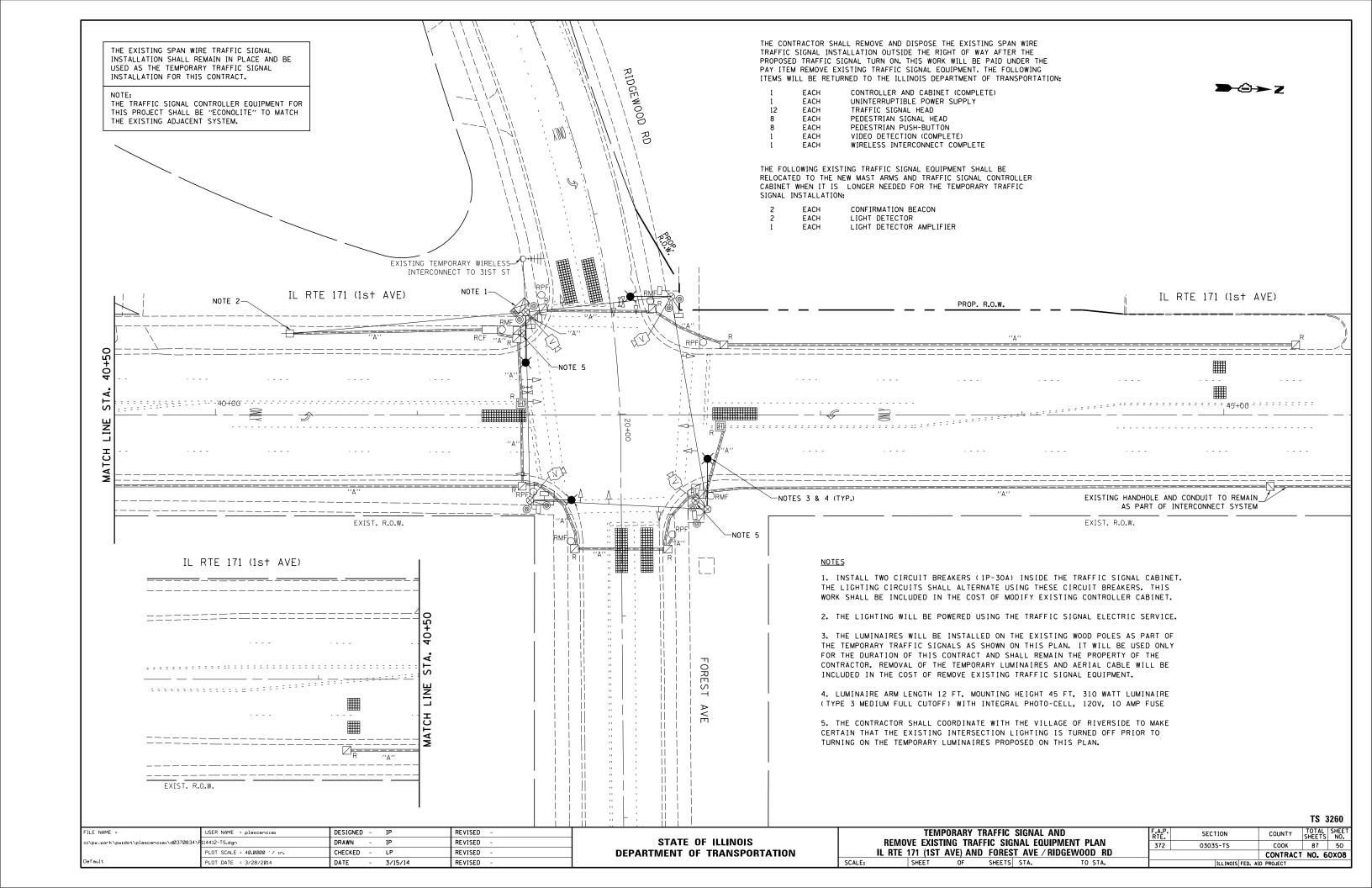
- 1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
- 2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	
STANDARD TRAFFIC SIGNAL DESIGN DETAILS	372	0303S-TS	COOK	87	48
STANDARD TRAFFIC SIGNAL DESIGN DETAILS		TS-05	CONTRACT	NO. 6	80x0
SHEET NO. 6 OF 7 SHEETS STA. TO STA.	FED. R	DAD DIST. NO. 1 ILLINOIS FED. AI	D PROJECT		



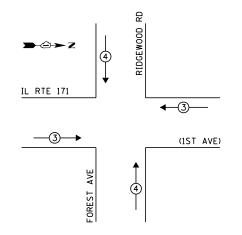


IL RTE 171

LEGEND:

- **★** SINGLE ENTRY PHASE
- ◆ * DUAL ENTRY PHASE
- **←**(*) → PEDESTRIAN PHASE

PHASE DESIGNATION DIAGRAM



EMERGENCY VEHICLE PREEMPTION SEQUENCE

EXISTING EMERGENCY VEHICLE PREEMPTORS								
EMERGENCY VEHICLE PREEMPTOR	3	4						
MOVEMENT	_	1						

	I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS										
	WATTAGE										
TYPE		LAMPS	INCAND.	LED	% OPERATION						
SIGNAL	(RED)	12		17	0.50	102.0					
(YE	ELLOW)	12		25	0.25	75.0					
(1	GREEN)	12		15	0.25	45.0					
ARROW		16		12	0.10	19.2					
PED. SIG	GNAL	8		25	1.00	200.0					
CONTROL	LER	1		100	1.00	100.0					
ILLUM. S	SIGN	-		25	0.05	-					
VIDEO S	YSTEM	1		150	1.00	150.0					
LUMINAI	RE	4	310		0.50	620.0					
FLASHER	1				0.50						
ENERGY	COSTS	TO:	-		TOTAL =	1311.2					

ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAY/DISTRICT 1
201 WEST CENTER COURT/SCHAUMBURG, ILLINOIS 60196-1096

ENERGY SUPPLY: CONTACT: JOE STACHO

PHONE: (630) 424-5704 COMPANY: COMMONWEALTH EDISON

ITEM DESCRIPTION	UNITS	TOTAL QTY.
AERIAL CABLE, 2-1/C NO. 8 WITH MESSENGER WIRE	FOOT	1425
LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, PHOTO-CELL	EACH	4
CONTROL, 310 WATT		
MODIFY EXISTING CONTROLLER CABINET	EACH	1
		-

(3) (3+20) (7) (1ST AVE) -1)---(1 IL RTE 171 FOREST __2-1/C NO. 8 (TYP.) **SCHEDULE OF QUANTITIES** EX. SPAN WIRE CABLE PLAN (NOT TO SCALE)

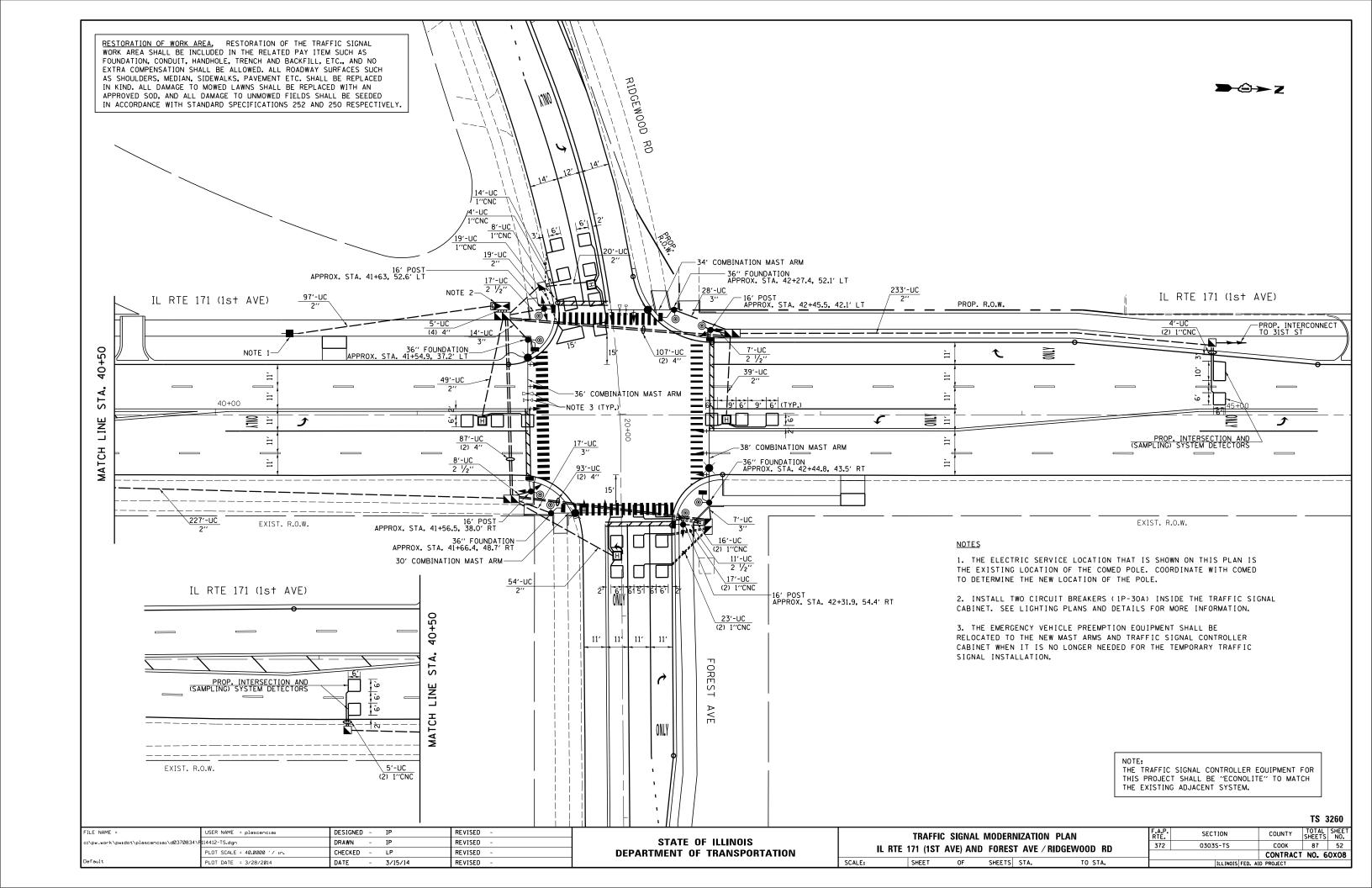
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

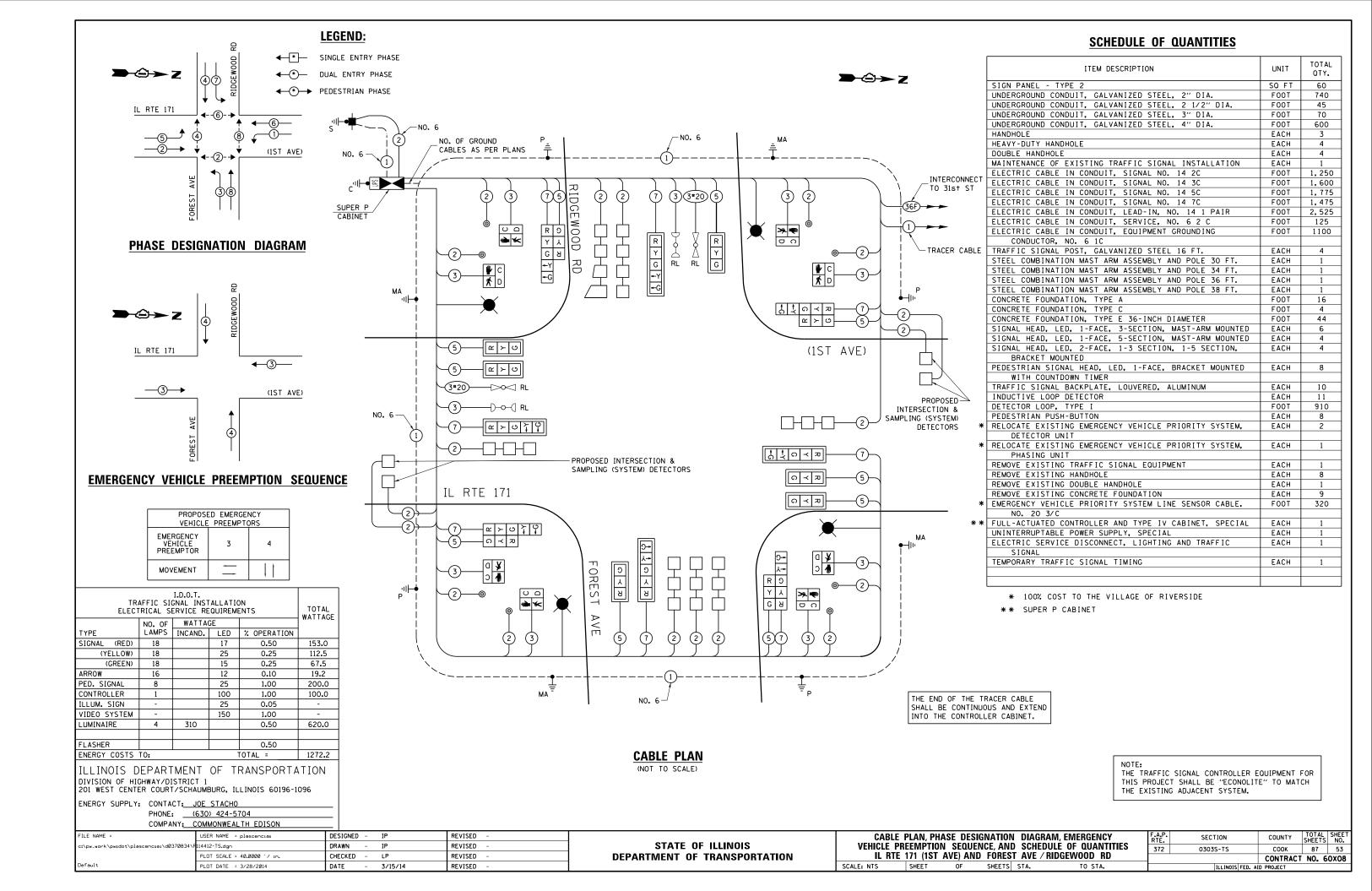
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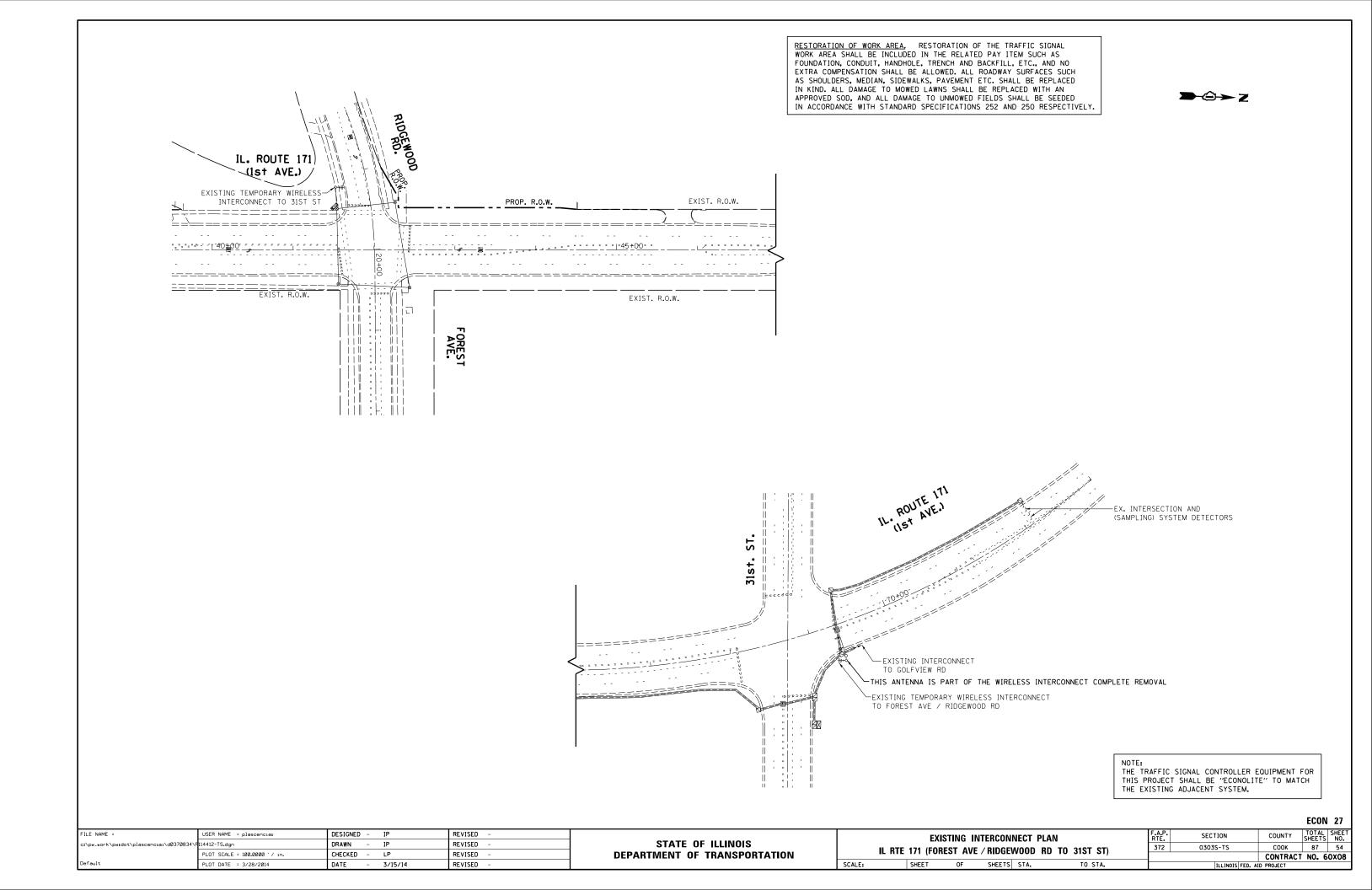
TS 3260

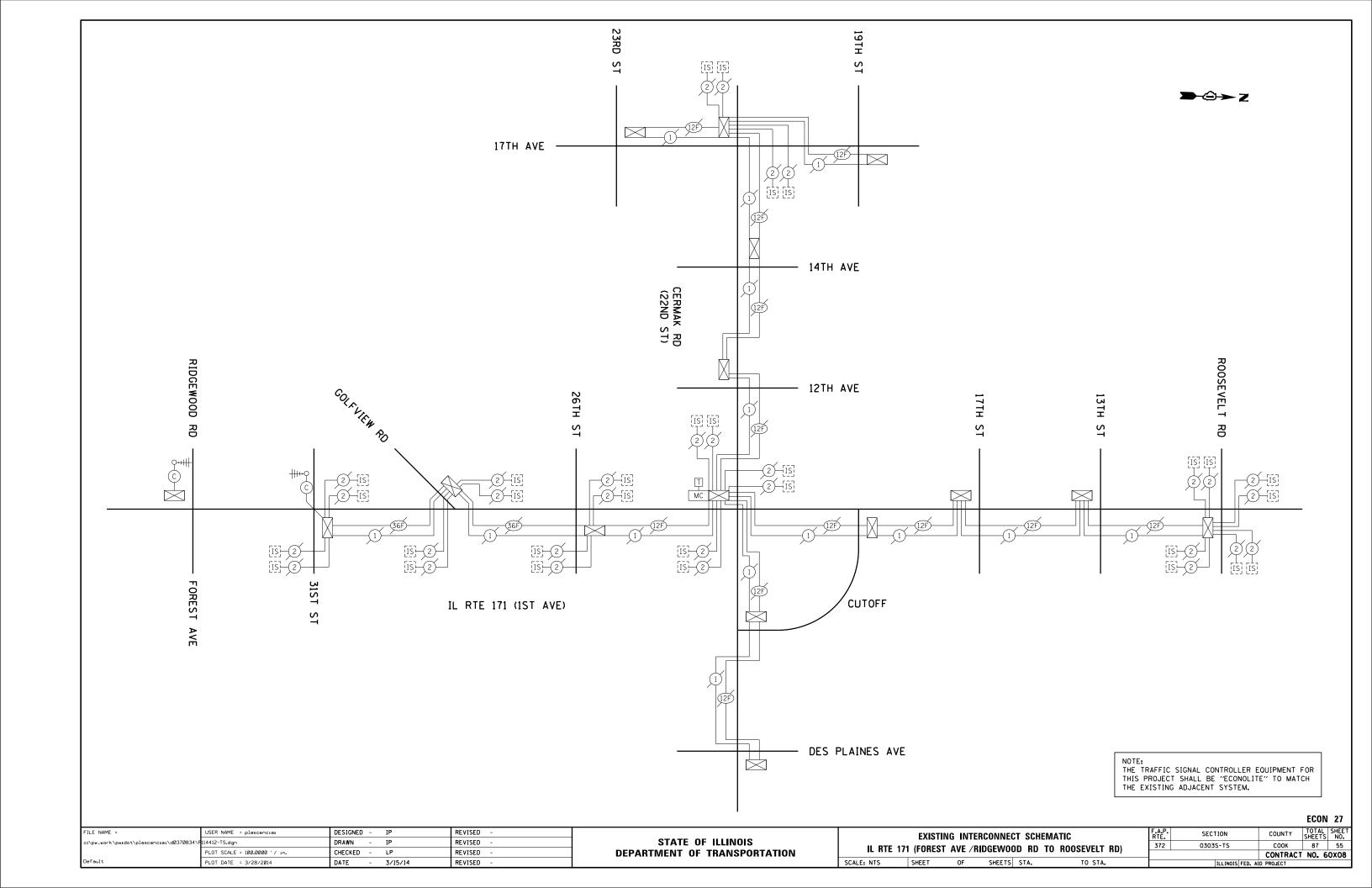
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	c:\pw_work\pwidot\plascenciai\d0370834\F	114412-TS.dgn	DRAWN -	IP	REVISED -	STATE OF ILLINOIS						EMPTION SEQUENCE	372	0303S-TS	соок	87	51
		PLOT SCALE = 40.0000 ' / 10.	CHECKED -	LP	REVISED -	DEPARTMENT OF TRANSPORTATION	IL RTE	171 (1ST A	(VE) AND	FORES	T AVE /	RIDGEWOOD RD			CONTRAC	T NO.	60x08
	Default	PLOT DATE = 3/28/2014	DATE -	3/15/14	REVISED -		SCALE: NTS	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS F	ED. AID PROJECT		
ı		7207 51112 572572511	DAIL	37 137 1 1	NETISES		SCALL! NIS	J.ILL.	<u> </u>	SHEETS	316	10 518		ILLINOI3 F	ED. AID PROJECT		

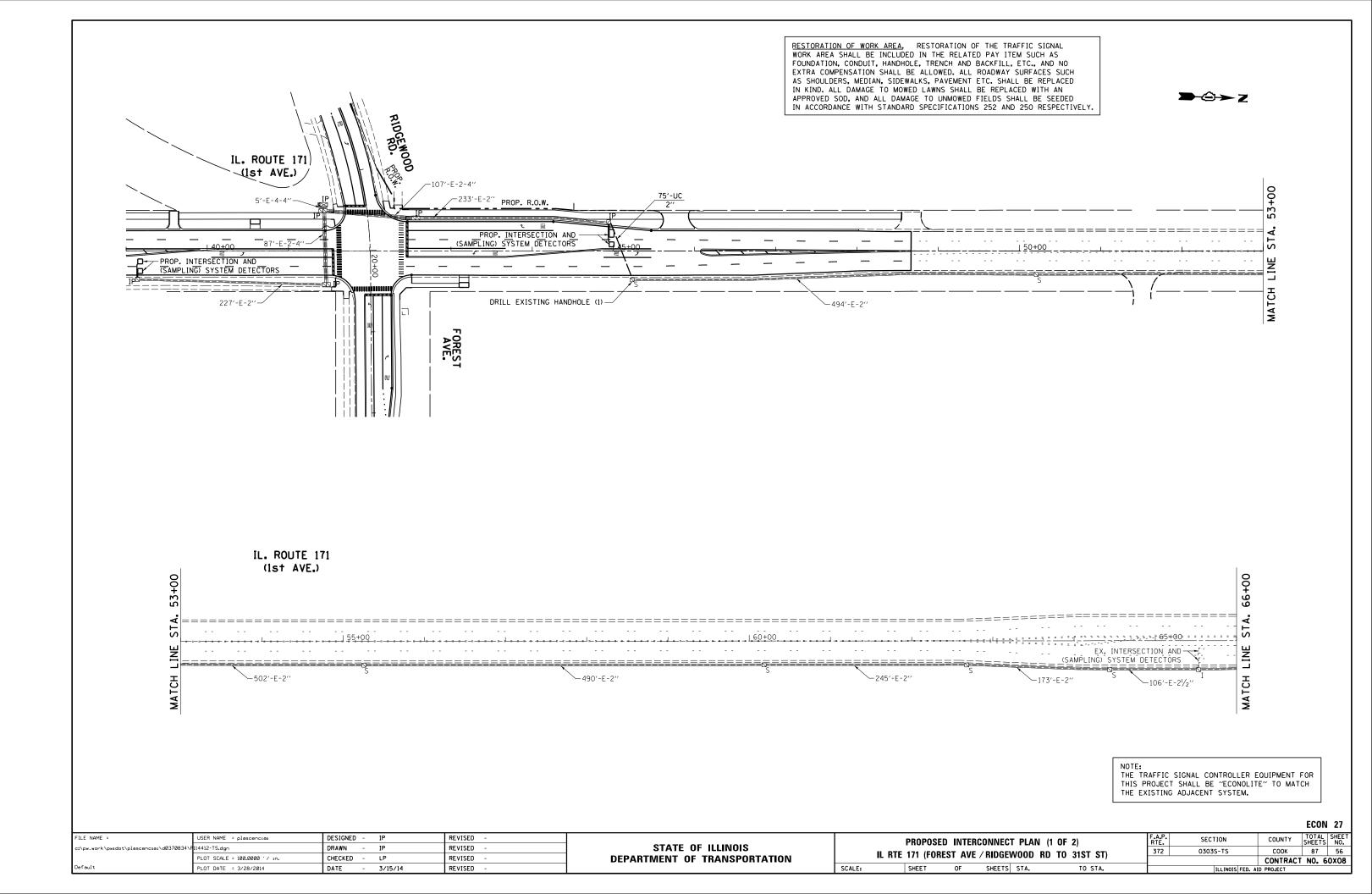
EXISTING TEMPORARY WIRELESS— INTERCONNECT TO 31ST ST





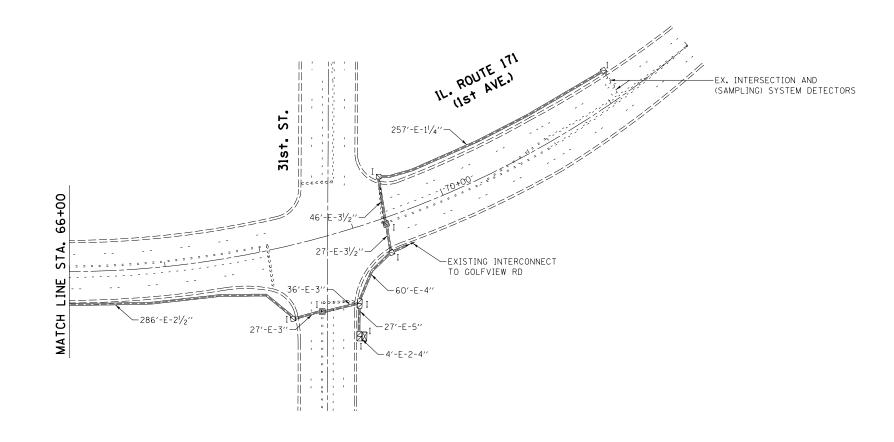






RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAYMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



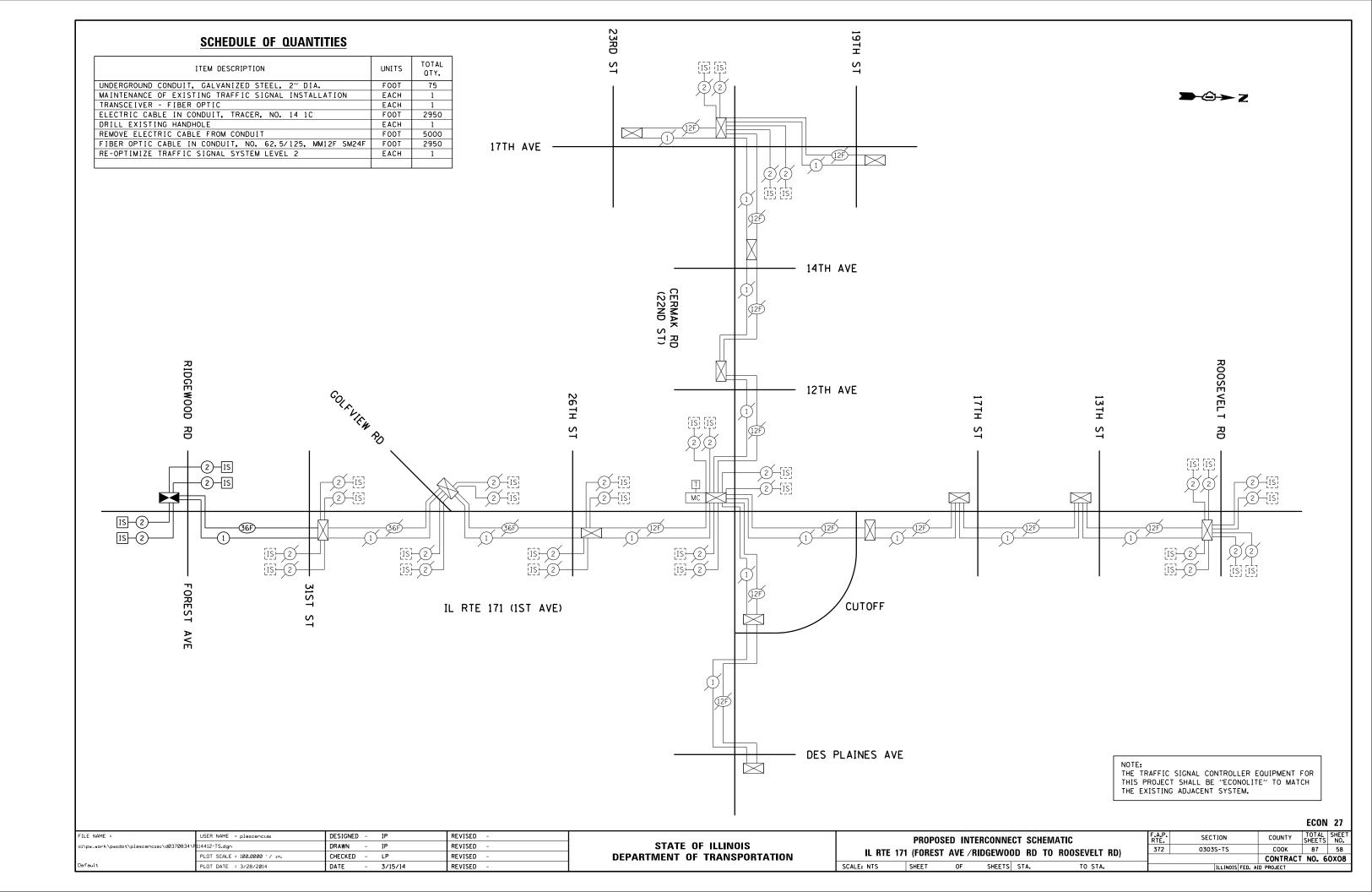


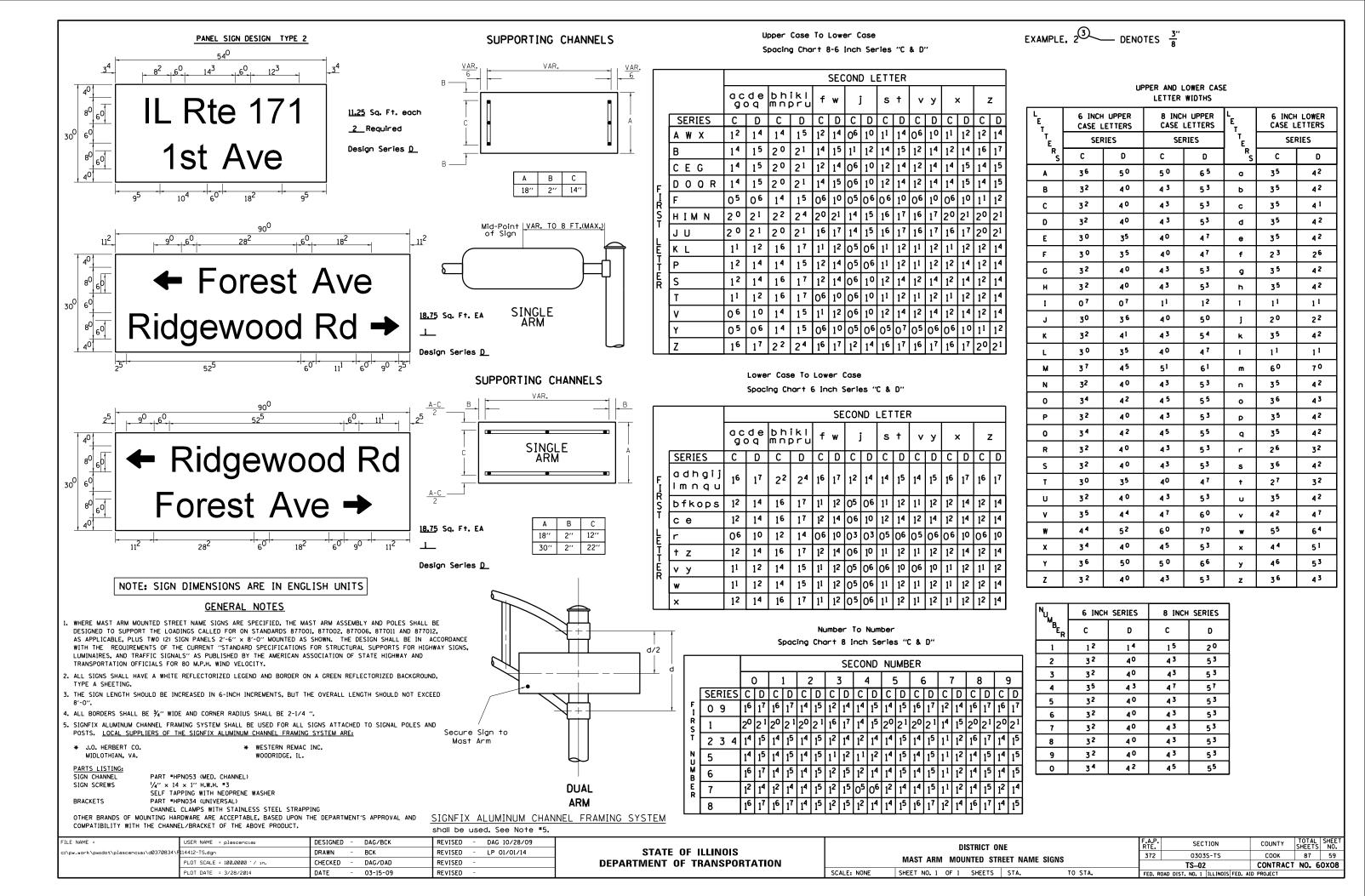
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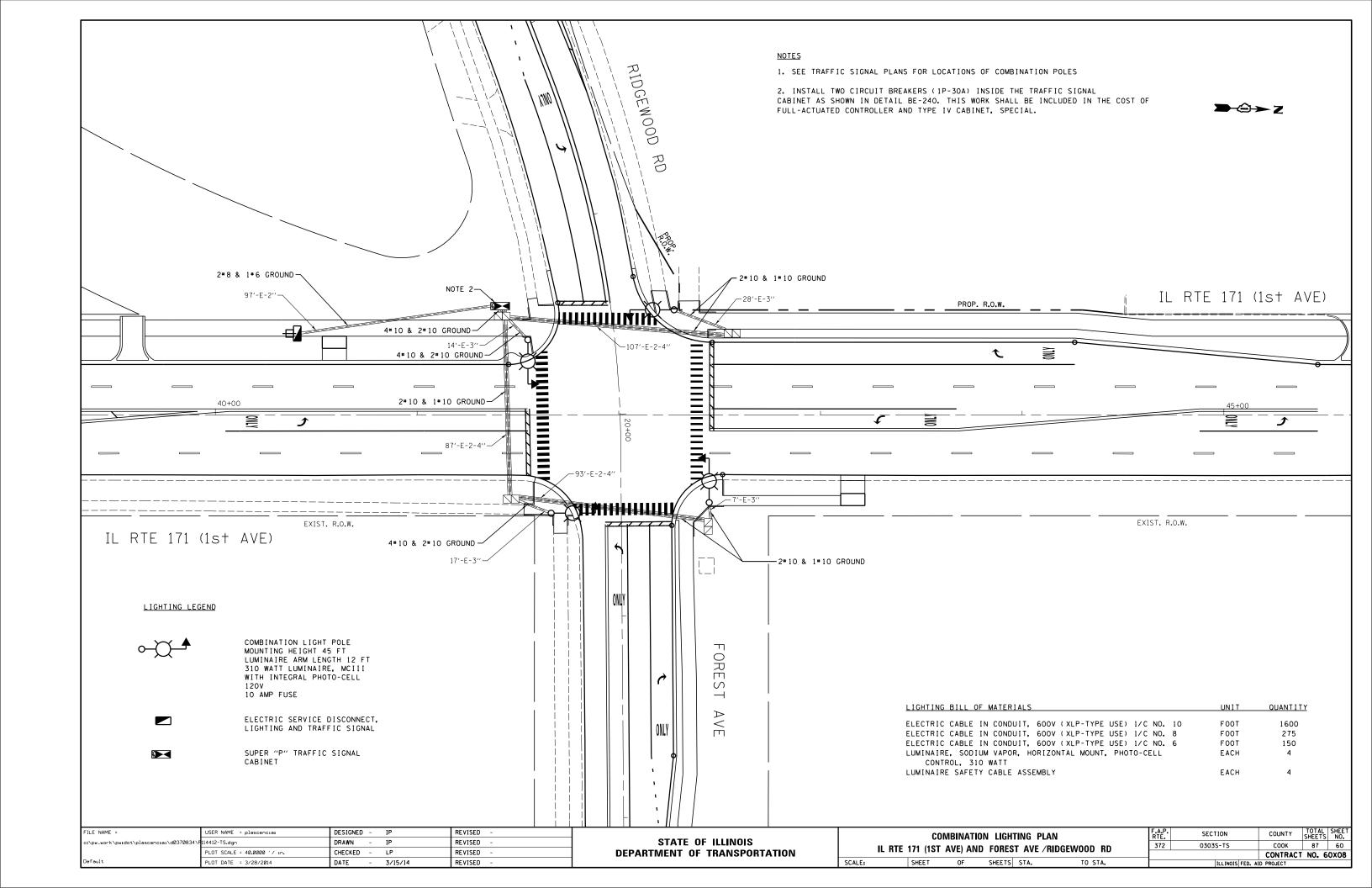
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

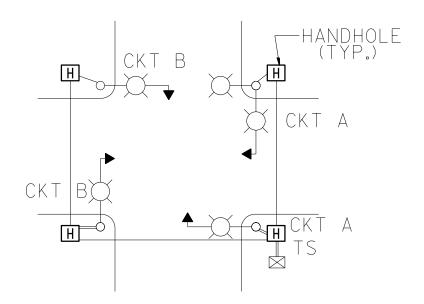
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	PLOT SCALE = 100.0000 ' / 10.	CHECKED - LP	REVISED -	DEPARTMENT OF TRANSPORTATION	IL R	IE I/I (FUKI	E91 AVE	: / KIDGE	י עטטאי	RD TO 31ST ST)			CONTRAC	T NO. 60	0X08
Default	PLOT DATE = 3/28/2014	DATE - 3/15/14	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FE	D. AID PROJECT		

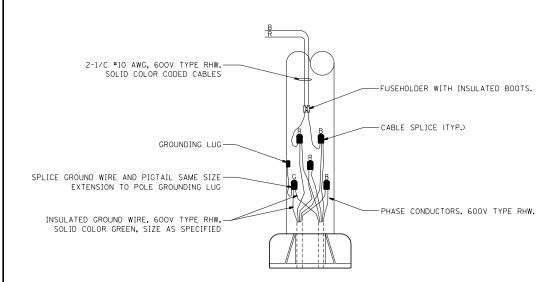






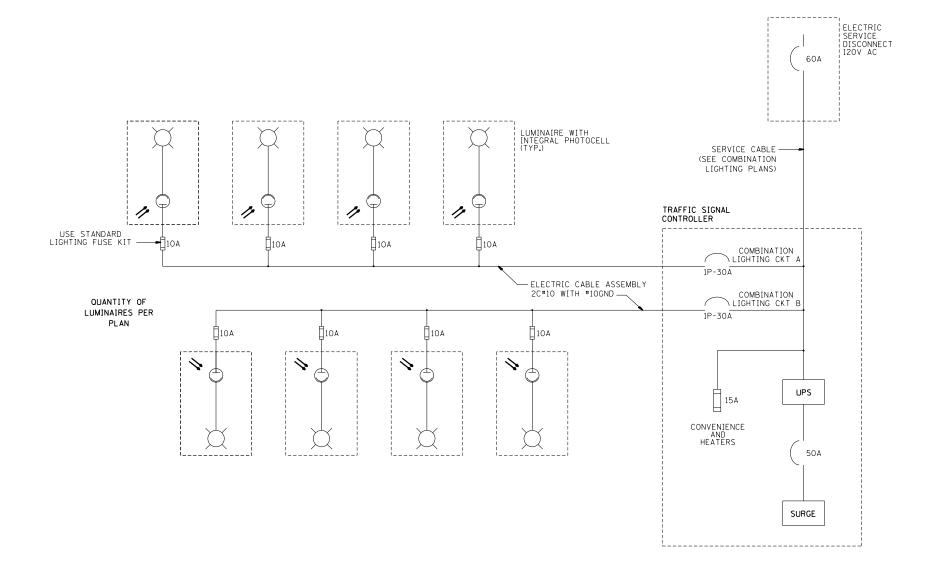


TYPICAL LIGHTING LAYOUT
(NOT TO SCALE)



COMBINATION POLE WIRING DETAIL

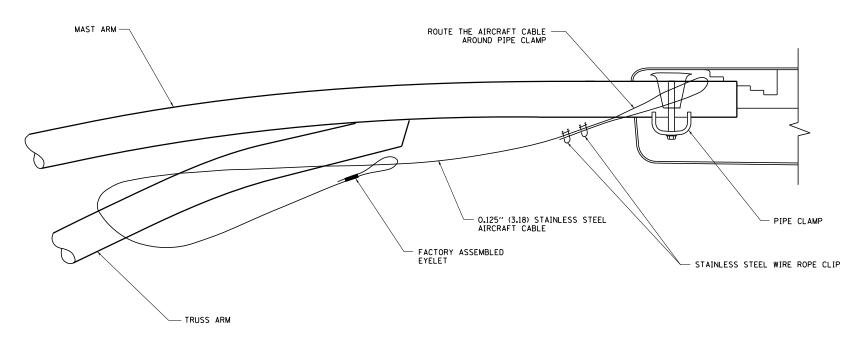
(NOT TO SCALE)



NOTES:

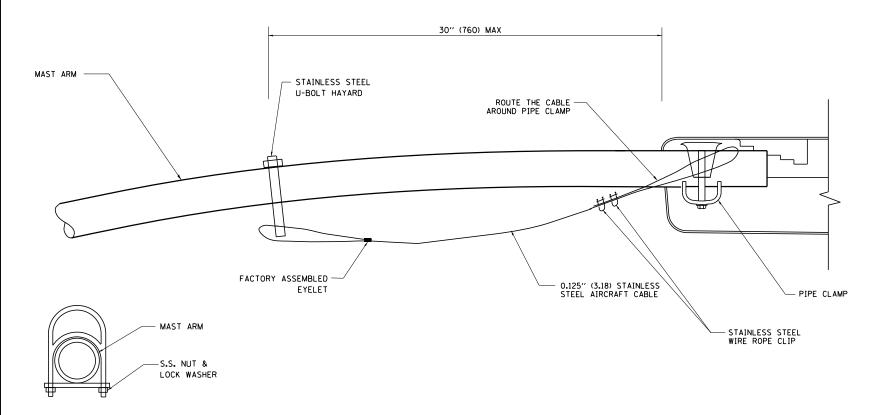
- 1.- 4 LUMINAIRES PER CIRCUIT, MAXIMUM.
- 2.- EACH LUMINAIRE IS PHOTOCELL CONTROLLED.
- 3.- MULTI-CONDUCTOR CABLE ASSEMBLY FOR LIGHTING CIRCUITS.
- 4.- ROUTE LIGHTING CIRCUITS IN TRAFFIC SIGNAL CONDUIT SYSTEM.
- 5.- ALL CONTROLLERS TO HAVE TWO LIGHTING BRANCH BREAKERS.
- 6.- ALL WIRING SHALL BE NEATLY DRESSED, IDENTIFIED BY TAGS, AND SUPPORTED.
- 7.- ALL SPLICES AND CONNECTIONS FOR ROADWAY LIGHTING SHALL BE AT POLE BASE ONLY. UNDERGROUND SPLICING OF LIGHTING CONDUCTORS IS NOT PERMITTED.

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Default	PLOT DATE = 3/28/2014	DATE - 03/14/2014	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FED.	AID PROJECT		



SIDE VIEW (TRUSS ARM)

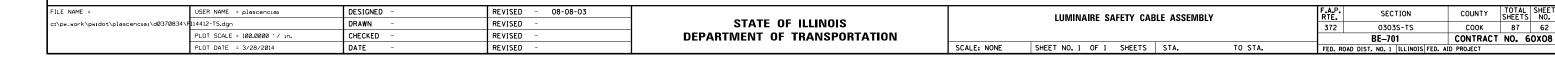
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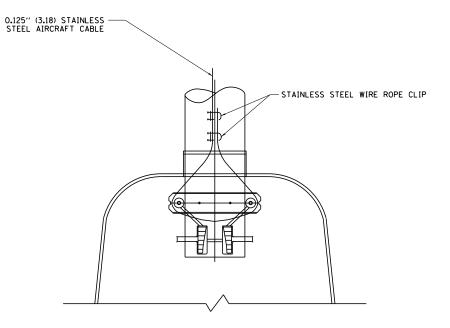


SIDE VIEW (SINGLE MEMBER OR DAVIT ARM)

STAINLESS STEEL U-BOLT HAYARD

N.T.S.



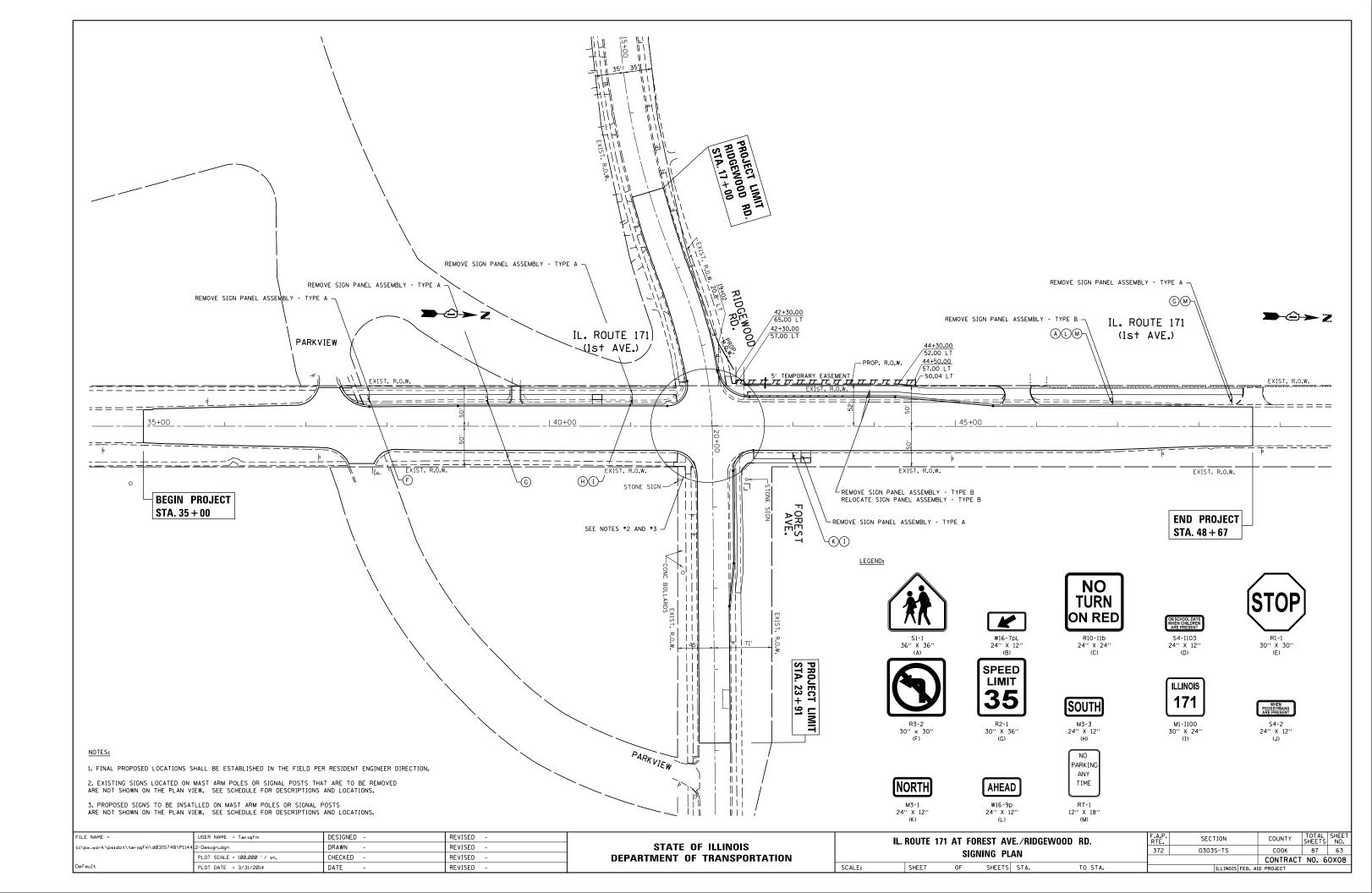


BOTTOM VIEW

N.T.S.

NOTES:

- 1. ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.
- 2. CONTRACTOR SHALL ADJUST THE WIRE CLIP TO ELIMINATE ANY SLACK FROM THE WIRE ROPE.
- 3. THE 0.125" (3.18) STAINLESS STEEL AIRCRAFT CABLE SHALL REMAIN VISIBLE FROM THE GROUND LEVEL.
- 4. THE BREAKING STRENGTH OF THE CABLE SHALL BE 1700 LBS. MIN.



RIDGEWO	OD ROAD /	FOREST AVE	NUE SIGNS TO BE REMOVED:					
		FACING		REMOVE S	IGN PANEL	REMOVE SIGN PANEL ASSEMBLY		
STATION	OFFSET	FACING	PANEL DESCRIPTION	TYPE 1	TYPE 2	TYPE A	TYPE B	
		TRAFFIC		(SQ FT)	(SQ FT)	(EACH)	(EACH)	
19+35.4	33.8' RT	EB	NO TURN ON RED	4				
19+35.4	33.8' RT	EB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				
19+35.4	33.8' RT	EB	STOP (FOLDED)	6.25				
19+48.7	21.0' LT	WB	NO TURN ON RED	4				
19+48.7	21.0' LT	WB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				
20+59.9	25.2' LT	WB	SCHOOL	9				
20+59.9	25.2' LT	WB	LEFT DIAGONAL ARROW	2.2				
20+59.9	25.2' LT	WB	NO TURN ON RED	4				
20+59.9	25.2' LT	WB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				
20+59.9	25.2' LT	WB	STOP (FOLDED)	6.25				
20+60.6	25.4' RT	EB	NO TURN ON RED	4				
20+60.6	25.4' RT	EB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				

		FACING		REMOVE S	IGN PANEL	REMOVE SIGN P	ANEL ASSEMBLY
STATION	OFFSET	TRAFFIC	PANEL DESCRIPTION	TYPE 1 (SQ FT)	TYPE 2 (SQ FT)	TYPE A (EACH)	TYPE B (EACH)
37+78.1	37.9' LT	SB	LEFT TURN PROHIBITION		,	1	, ,
39+23.9	36.3' LT	SB	SPEED LIMIT (35)			1	
41+2.0	35.8' LT	SB	CARDINAL DIRECTION (SOUTH)			1	
41+2.0	35.8 LI	SB	ILLINOIS ROUTE 171			1	
41+51.6	40.5' LT	SB	NO TURN ON RED	4			
41+51.6	40.5' LT	SB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2			
41+57.7	37.9' RT	NB	SCHOOL CROSSING	9			
41+57.7	37.9' RT	NB	NO TURN ON RED	4			
41+57.7	37.9' RT	NB	WHEN PEDESTRIANS ARE PRESENT	2			
41+57.7	37.9' RT	NB	STOP (FOLDED)	6.25			
42+40.9	34.6' LT	SB	SCHOOL CROSSING	9			
42+40.9	34.6' LT	SB	NO TURN ON RED	4			
42+40.9	34.6' LT	SB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2			
42+40.9	34.6' LT	SB	STOP (FOLDED)	6.25			
42+47.3	38.7' RT	NB	NO TURN ON RED	4			
42+47.3	38.7' RT	NB	WHEN PEDESTRIANS ARE PRESENT	2			
43+10.1	37.9' RT	NB	CARDINAL DIRECTION (NORTH)			1	
45+10.1	37.5 KI	NB	ILLINOIS ROUTE 171			1	
43+94.5 *	36.7' LT	SB	BROOKFIELD ZOO, OPEN EVERY DAY				1
45+54.5	30.7 LI	SB	SERVICE & SOUTH GATE				1
46+93.9	34.2' LT	SB	SCHOOL				1
TU1 JJ.J	J4.∠ LI	SB	NO PARKING				_
48+40.7	32.0' LT	SB	SPEED LIMIT (35)			1	
40740./	32.U LI	SB	NO PARKING			1	

^{*} EXISTING SIGN POST TO BE REUSED WHEN SIGN PANEL ASSEMBLY IS RELOCATED.

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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

SCALE:

IL 171 SIGN	IS TO BE R	ELOCATED:						
		- A GINI G		RELOCATE SIGN PANEL ASSEMBLY				
STATION	OFFSET	FACING	PANEL DESCRIPTION	TYPE A	TYPE B			
		TRAFFIC		(EACH)	(EACH)			
12+04 E *	40 2' LT	SB	BROOKFIELD ZOO, OPEN EVERY DAY		1			
43+94.5	3+94.5 * 40.3' LT		SERVICE & SOUTH GATE		1			

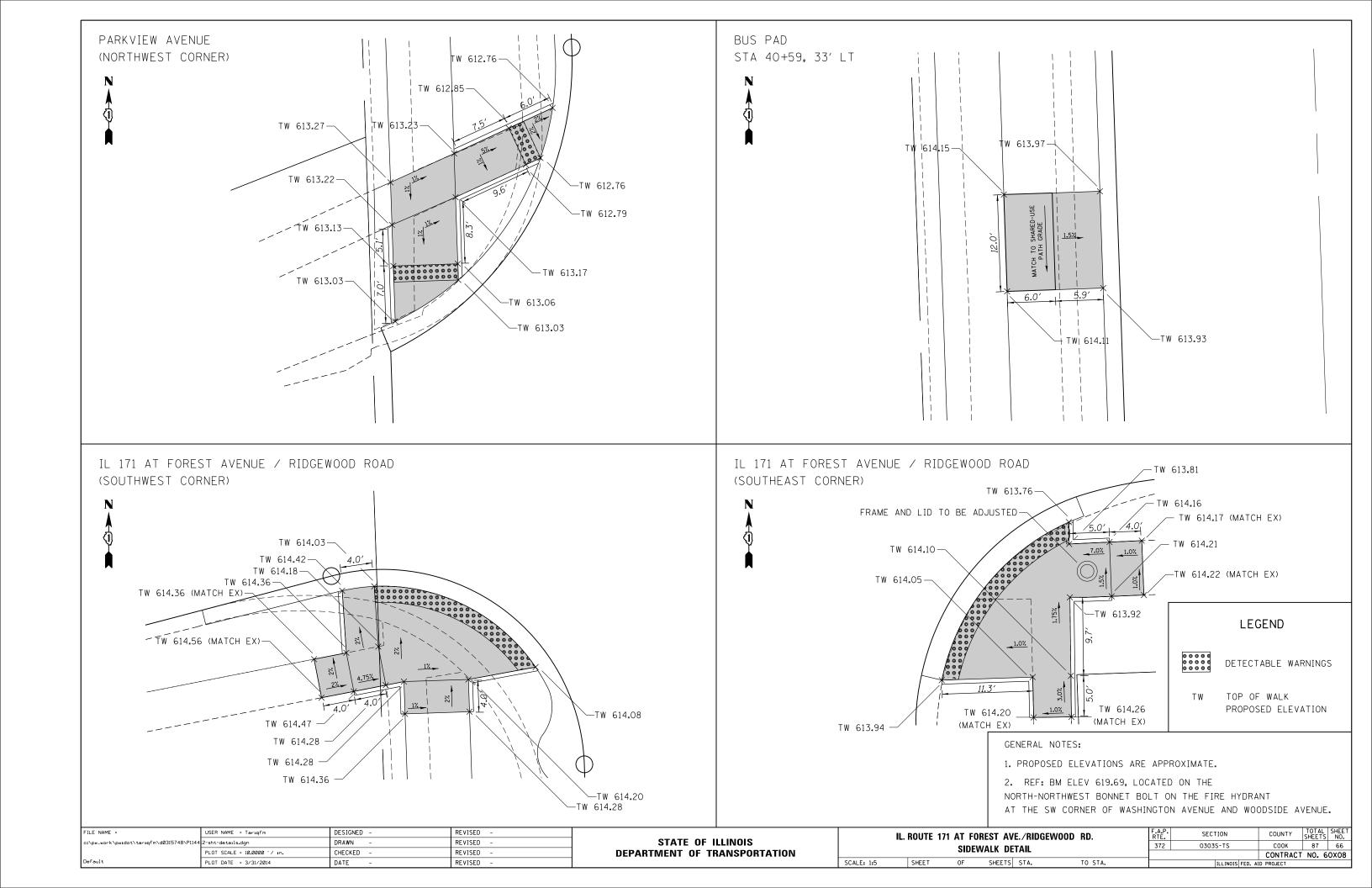
^{*} EXISTING SIGN POST TO BE REUSED WHEN SIGN PANEL ASSEMBLY IS RELOCATED.

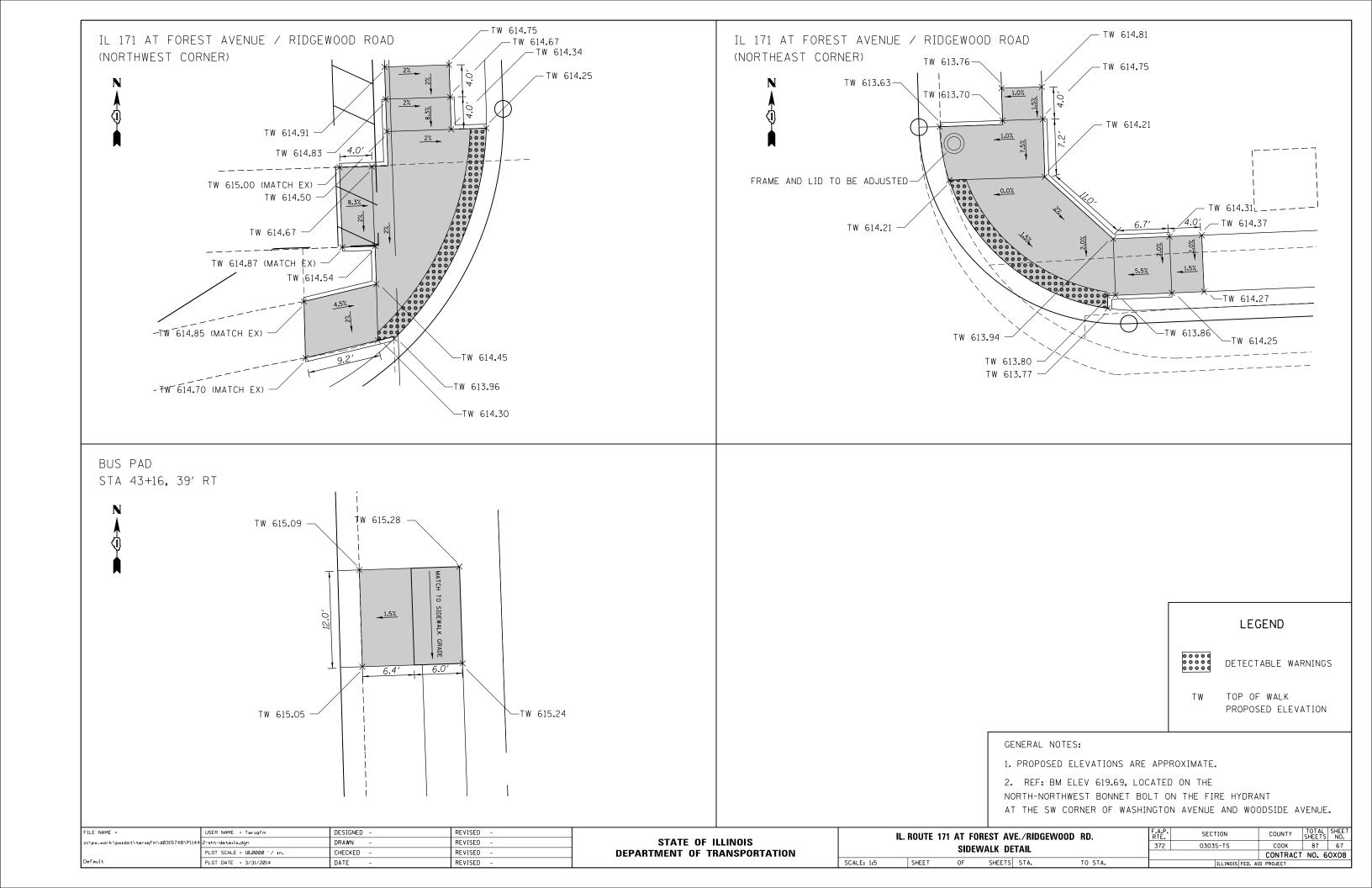
IL.	IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD.					F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	
SIGNING SCHEDULE				DILLE		372	0303S-TS	соок	87	64
		Jidivi	ING SUIL	DOLL				CONTRACT	NO. 6	80X08
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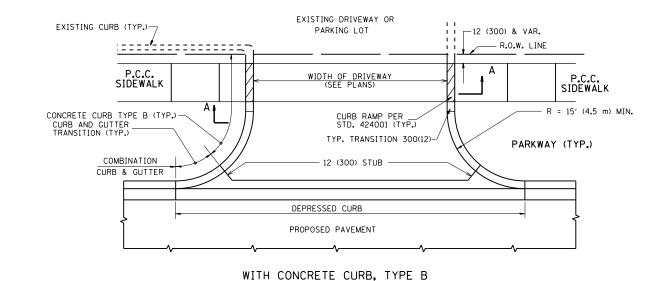
RIDGEWOOD ROA	RIDGEWOOD ROAD / FOREST AVENUE PROPOSED SIGNS:											
SIGN SYMBOL	SIGN CODE	STATION	OFFSET	FACING TRAFFIC	PANEL DESCRIPTION	SIGN TYPE 1 (SQ FT)	TYPE 2 (SQ FT)	TELESCOPING STEEL SIGN SUPPORT (FT)	BASE FOR TELESCOPING STEEL SIGN SUPPORT (EA)	NOTES		
Α	S1-1	19+42.8	32.8' RT	EB	SCHOOL	9				PLACED ON SIGNAL POST		
В	W16-7pL	19+42.8	32.8' RT	EB	LEFT DIAGONAL ARROW	2				PLACED ON SIGNAL POST		
С	R10-11b	19+42.8	32.8' RT	EB	NO TURN ON RED	4				PLACED ON SIGNAL POST		
D	S4-I103	19+42.8	32.8' RT	EB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				PLACED ON SIGNAL POST		
Е	R1-1	19+42.8	32.8' RT	EB	STOP	6.25				THIS IS A TRI-FOLD SIGN, PLACED ON SIGNAL POST		
С	R10-11b	19+52.1	31.0' LT	WB	NO TURN ON RED	4				PLACED ON MAST ARM POLE		
D	S4-I103	19+52.1	31.0' LT	WB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				PLACED ON MAST ARM POLE		
С	R10-11b	20+48.9	34.8' RT	EB	NO TURN ON RED	4				PLACED ON MAST ARM POLE		
D	S4-I103	20+48.9	34.8' RT	EB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				PLACED ON MAST ARM POLE		
Α	S1-1	20+55.0	30.6" LT	WB	SCHOOL	9				PLACED ON SIGNAL POST		
В	W16-7pL	20+55.0	30.6" LT	WB	LEFT DIAGONAL ARROW	2				PLACED ON SIGNAL POST		
С	R10-11b	20+55.0	30.6" LT	WB	NO TURN ON RED					PLACED ON SIGNAL POST		
D	S4-I103	20+55.0	30.6" LT	WB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				PLACED ON SIGNAL POST		
E R1-1 20+55.0 30.6" LT WB STOP						6.25				THIS IS A TRI-FOLD SIGN, PLACED ON SIGNAL POST		

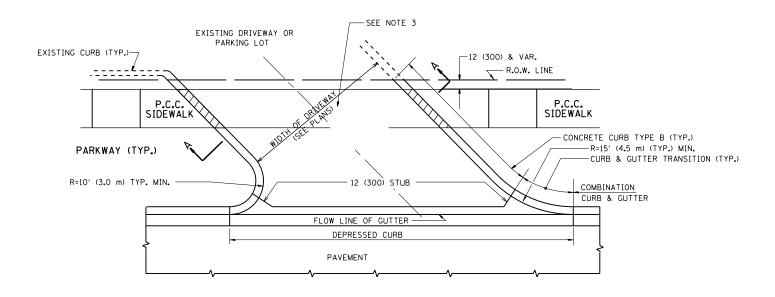
IL 171 PROPOSED	SIGNS:									
SIGN SYMBOL	SIGN CODE	STATION	OFFSET	FACING TRAFFIC	PANEL DESCRIPTION	SIGN TYPE 1 (SQ FT)	PANEL TYPE 2 (SQ FT)	TELESCOPING STEEL SIGN SUPPORT (FT)	BASE FOR TELESCOPING STEEL SIGN SUPPORT (EA)	NOTES
F	R3-2	37+78.1	33.7' LT	SB	LEFT TURN PROHIBITION	6.25		14.5	1	
G	R2-1	39+23.9	31.1' LT	SB	SPEED LIMIT (35)	7.5		15	1	
Н	M3-3	41+2.0	30.7' LT	SB	CARDINAL DIRECTION (SOUTH)	2		15	1	
1	M1-I100	41+2.0	30.7 L1	JD	ILLINOIS ROUTE 171	5		15	1	
С	R10-11b	41+54.9	37.2' LT	SB	NO TURN ON RED	4				PLACED ON MAST ARM POLE
D	S4-I103	41+54.9	37.2' LT	SB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				PLACED ON MAST ARM POLE
Α	S1-1	41+56.5	38.0' RT	NB	SCHOOL	9				PLACED ON SIGNAL POST
В	W16-7pL	41+56.5	38.0' RT	NB	LEFT DIAGONAL ARROW	2				PLACED ON SIGNAL POST
С	R10-11b	41+56.5	38.0' RT	NB	NO TURN ON RED	4				PLACED ON SIGNAL POST
J	S4-2	41+56.5	38.0' RT	NB	WHEN PEDESTRIANS ARE PRESENT	2				PLACED ON SIGNAL POST
E	R1-1	41+56.5	38.0' RT	NB	STOP	6.25				THIS IS A TRI-FOLD SIGN, PLACED ON SIGNAL POST
С	R10-11b	42+44.8	43.5' RT	NB	NO TURN ON RED	4				PLACED ON MAST ARM POLE
J	S4-2	42+44.8	43.5' RT	NB	WHEN PEDESTRIANS ARE PRESENT	2				PLACED ON MAST ARM POLE
Α	S1-1	42+46.2	42.1' LT	SB	SCHOOL	9				PLACED ON SIGNAL POST
В	W16-7pL	42+46.2	42.1' LT	SB	LEFT DIAGONAL ARROW	2				PLACED ON SIGNAL POST
С	R10-11b	42+46.2	42.1' LT	SB	NO TURN ON RED	4				PLACED ON SIGNAL POST
D	S4-I103	42+46.2	42.1' LT	SB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				PLACED ON SIGNAL POST
E	R1-1	42+46.2	42.1' LT	SB	STOP	6.25				THIS IS A TRI-FOLD SIGN, PLACED ON SIGNAL POST
K	M3-1	43+00.0	36.3' RT	NB	CARDINAL DIRECTION (NORTH)	2		15	1	
1	M1-I100	45100.0	30.3 1(1		ILLINOIS ROUTE 171	5		15	-	
Α	S1-1				SCHOOL	9				
L	W16-9p	46+93.9	30.4' LT	SB	AHEAD	2		17.5	1	
M	R7-1				NO PARKING ANYTIME	1.5				
G	R2-1	48+40.7	32.0' LT	SB	SPEED LIMIT (35)	7.5		16.5	1	
М	R7-1	40140.7	J2.0 L1	JU	NO PARKING ANYTIME	1.5		10.5	1	

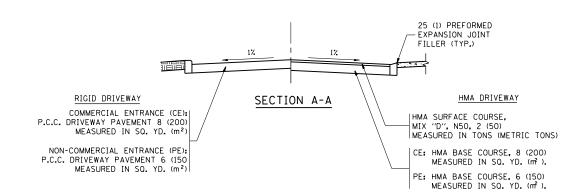
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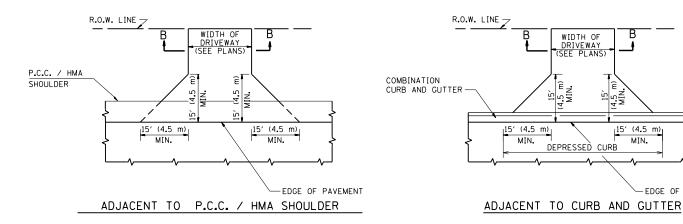


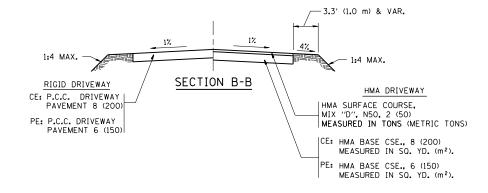






WITH CONCRETE CURB, TYPE B





RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m²).

15' (4.5 m)

MIN.

EDGE OF PAVEMENT

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY OUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

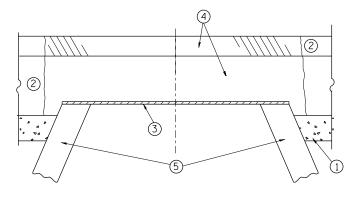
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

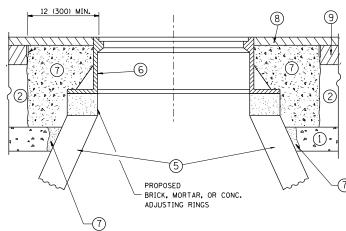
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	PLOT DATE = 3/31/2014	DATE - 11-04-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
AND FACE OF CURB & EDGE OF SHOULDER > = 15' (4.5 m)	372	0303S-TS	соок	87	68
AND TACE OF CORD & EDGE OF SHOOLDER >= 15 (4.5 III)		BD0156-07 (BD-01)	CONTRACT	NO. 6	80X08
IE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FFD. R	OAD DIST. NO. 1 THE INDIS FED. A	D PROJECT		





EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM
- AROUND THE STRUCTURE.

 B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE. D) BACKFILL WITH CRUSHED STONE AND A MINIMUM $1\frac{1}{2}$ (40)
- THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE

LEGEND

- 1 SUB-BASE GRANULAR MATERIAL
- (6) FRAME AND LID (SEE NOTES)
- 2 EXISTING PAVEMENT

(5) EXISTING STRUCTURE

- (7) CLASS PP-1* CONCRETE
- 3 36 (900) DIAMETER METAL PLATE
- (8) PROPOSED HMA SURFACE COURSE
- PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- (9) PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK. THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

COUNTY

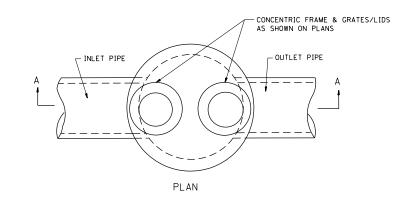
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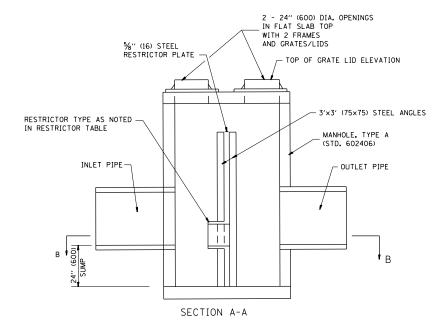
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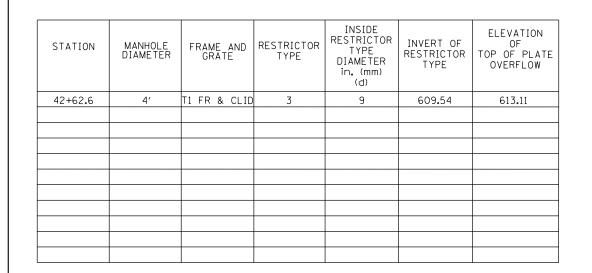
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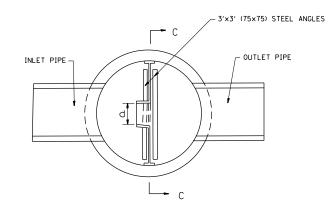
STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

		F.A.P. RTE.	SECTION	COUNTY				
	FRAMES AND	372	0303S-TS	соок				
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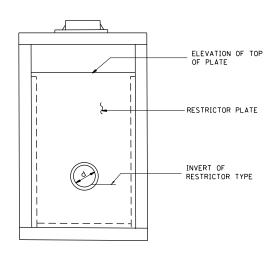




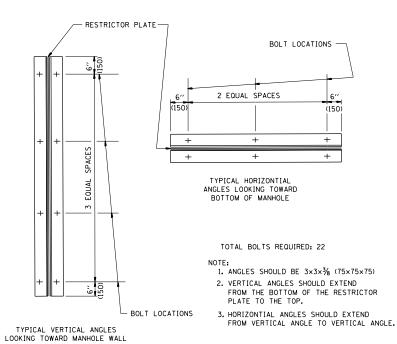


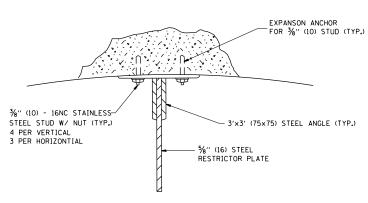


SECTION B-B



SECTION C-C

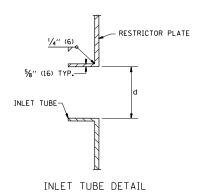




ANGLE FASTENER DETAIL

NOTES:

- 1. ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION.
- 2. ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.
- 3. BASIS OF PAYMENT: "MANHOLES TYPE A, 6 FT. (1.8 m)-DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE" EACH



RESTRICTOR TYPE RE-ENTRANT RE-ENTRANT SHARP EDGED SQUARE EDGED SQUARE EDGED ROUNDED TUBE TUBE - ENGTH: 2-1/2 DI LENGTH: 1/2 TO 1 DI

VALUES OF "C" FOR CIRCULAR AND SQUARE ORIFICES

C=.61

STEEL ANGLE BOLTING DETAILS

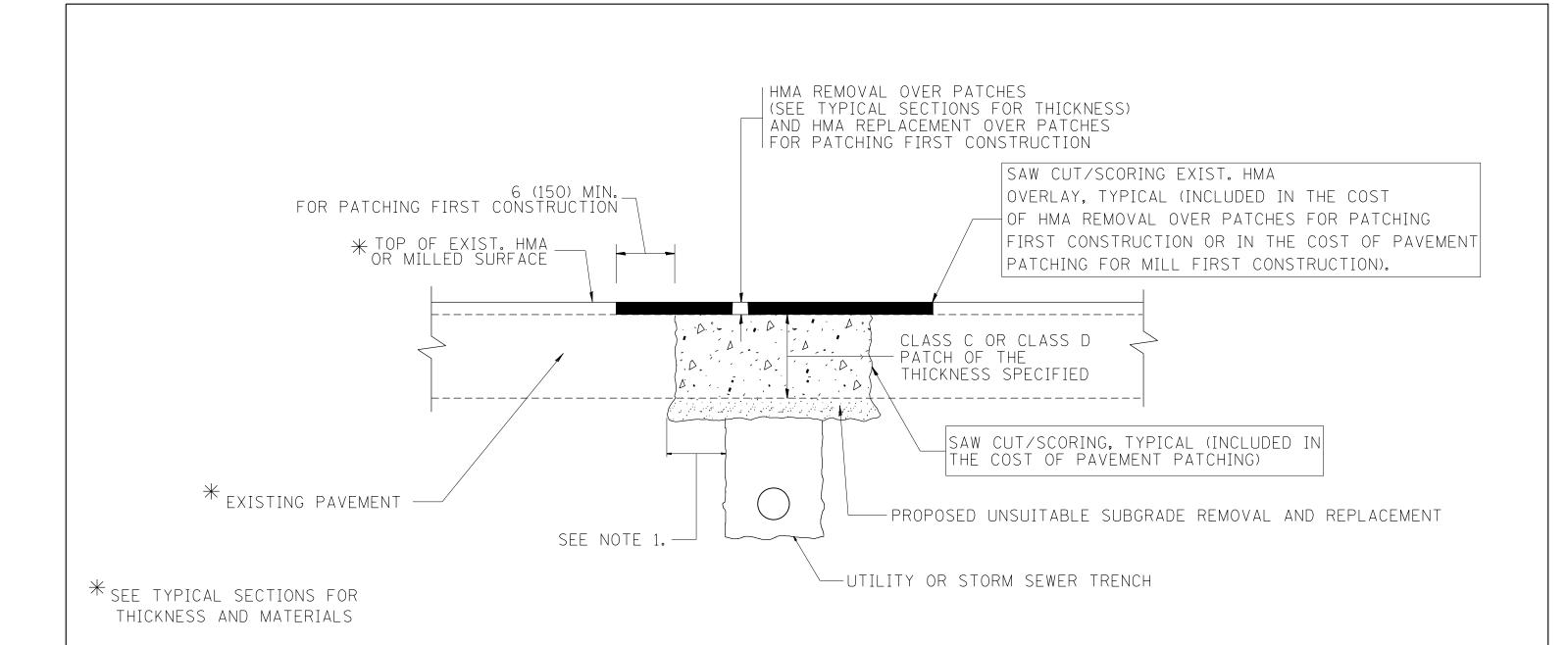
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

C=.98

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STATE OF ILLINOIS	
DEPARTMENT OF TRANSPORTATION	N

MANHOLE WITH							SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		372	0303S-TS	COOK	87	70				
		nLo	TRICTOR PI	ВІ	D600-04 (BD-12)	CONTRACT	NO. 6	80X08		
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NOTES:

- 1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
- 2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

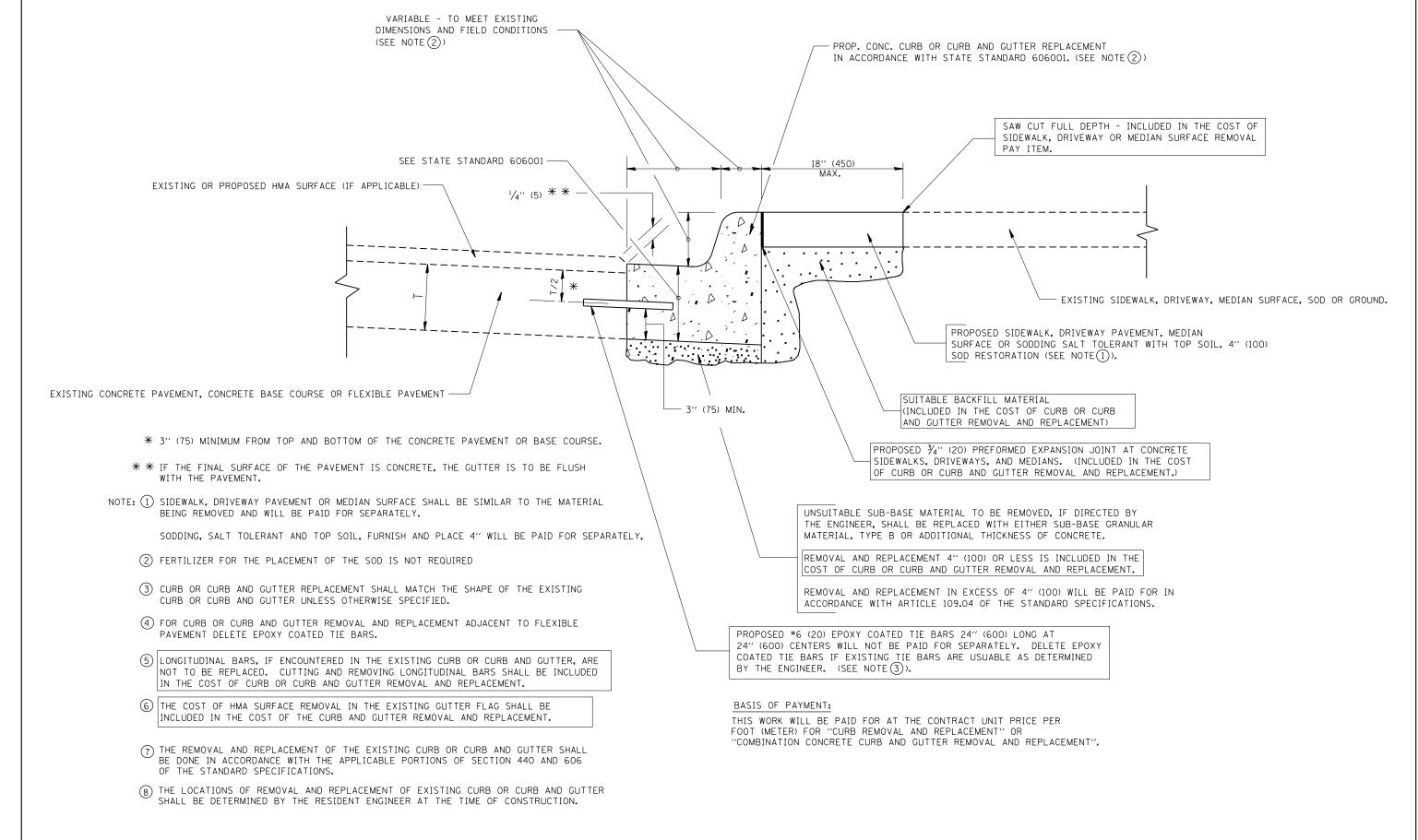
- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 41/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

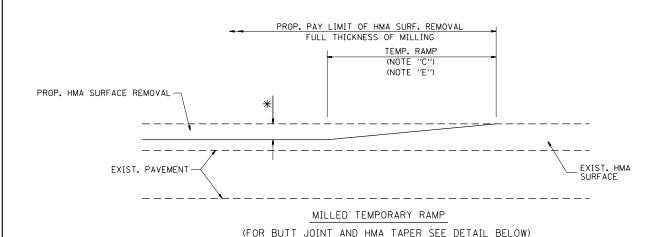
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c:\pw_work\pwidot\tariqfm\d0315748\DistS	td.dgn	DRAWN -	REVISED - R. BORO 01-01-07	STATE OF ILLINOIS		372 0303S-TS	COOK 87 71
	PLOT SCALE = 100.0002 '/ in.	CHECKED -	REVISED - R. BORO 09-04-07	DEPARTMENT OF TRANSPORTATION	HMA SURFACED PAVEMENT	BD400-04 (BD-22)	CONTRACT NO. 60X08
	PLOT DATE = 3/31/2014	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AI	



CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

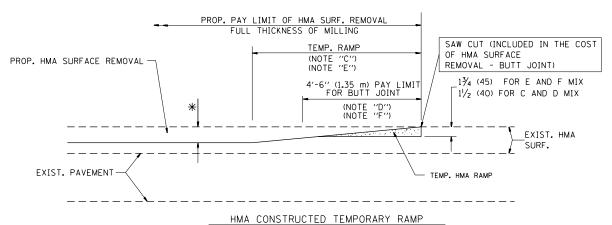
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

F	FILE NAME =	USER NAME = Tariqfm	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS	CURB OR CURB AND GUTTER			P SEC		SHEET	L SHEET
С	c:\pw_work\pwidot\tariqfm\d03l5748\DistS	td.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97					2 0303	S-TS COOK	87	72
		PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - M. GOMEZ 01-22-01	DEPARTMENT OF TRANSPORTATION		REMOVAL AND REPLACEMENT	F	BD600-06 (E		CT NO.	60X08
L		PLOT DATE = 3/31/2014	DATE - 03-11-94	REVISED - R. BORO 12-15-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO ST	A. FE	D. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT		



BOTT SOINT AND TIMA TAI ER SEE

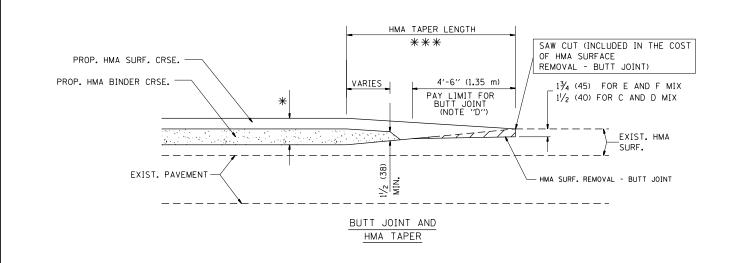
OPTION 1



(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

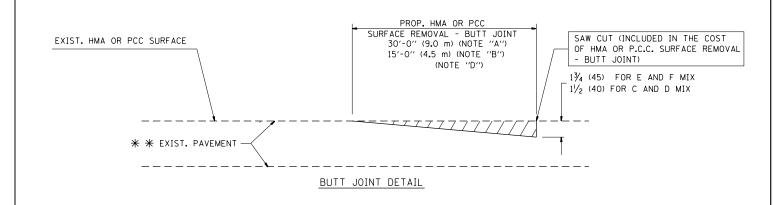
OPTION 2

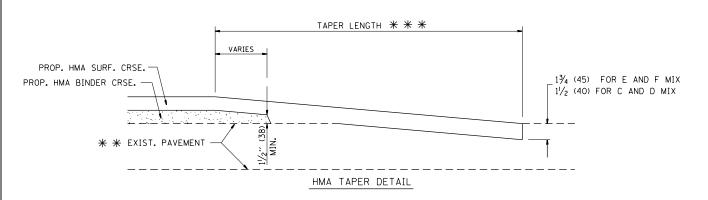
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION





TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

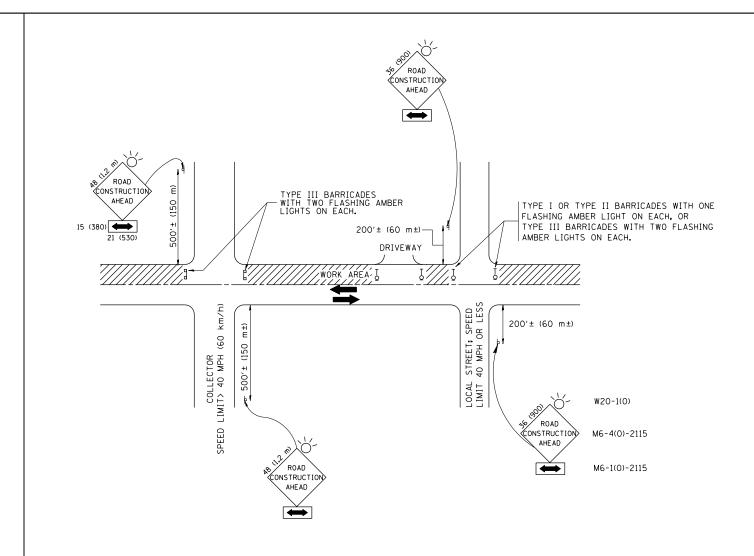
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- Q) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h)
 AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- d) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1,2 m x 1,2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROLLTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (MG-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (MG-4).

SCALE: NONE

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

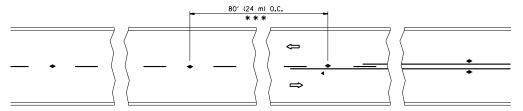
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

	FILE NAME =	USER NAME = Tariqfm	DESIGNED -	LHA	REVISED	- J. OBERLE 10-18-95
FILE NAME = USER NAME = Tariqfm c:\pw_work\pwidot\tariqfm\d0315748\DistStd.dgn PLOT SCALE = 100.0002 ' / in. PLOT DATE = 3/31/2014	DRAWN -		REVISED	- A. HOUSEH 03-06-96		
	c:\pw.work\pwidot\tariqfm\d0315748\DistStd.dgn PLOT SCALE = 100.0002 '/ in.	CHECKED -		REVISED	- A. HOUSEH 10-15-96	
		PLOT DATE = 3/31/2014	DATE -	06-89	REVISED	-T. RAMMACHER 01-06-00

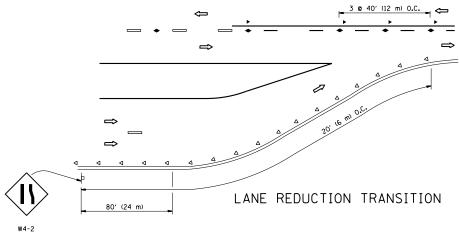
STATI	E OF	: ILLINOIS
DEPARTMENT	0F	TRANSPORTATION

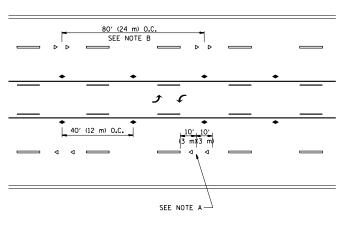
TRAFFIC CONTROL AND PROTECTION FOR	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	372	0303S-TS	COOK	87	74	
SIDE RUADS, INTERSECTIONS, AND DRIVEWATS		TC-10	CONTRACT NO. 60X08			
SHEFT NO. 1 OF 1 SHEFTS STA. TO STA.	EED D	OAD DICT NO 1 THE INDIC EED AT	D DDO IECT			



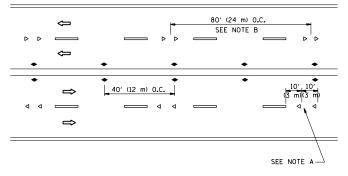
*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO-LANE/TWO-WAY

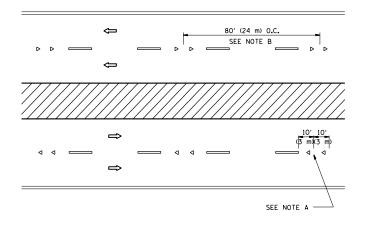




TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

- 1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- 2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- 3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

---- YELLOW STRIPE

---- WHITE STRIPE

- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (₩/O)
- ◆ TWO-WAY AMBER MARKER

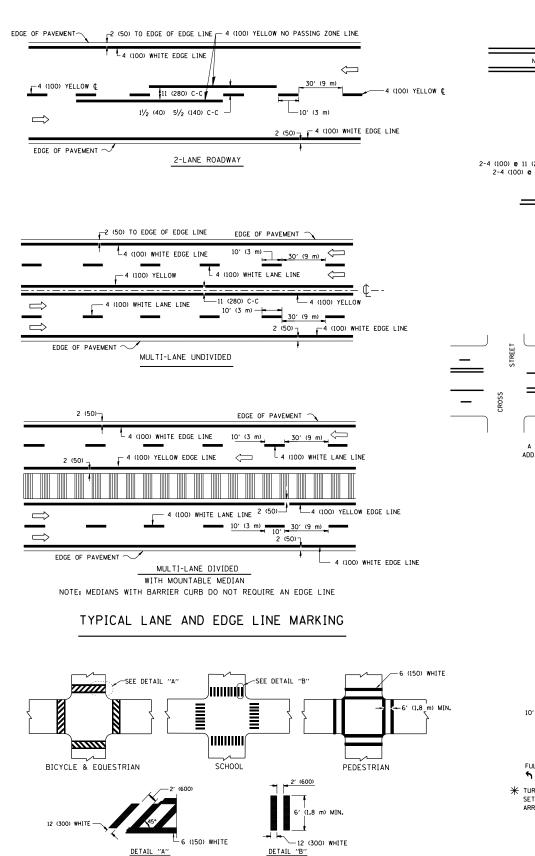
DESIGN NOTES

- 1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- 2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- 3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- 4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

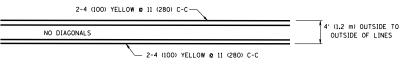
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

FILE	NAME =	USER NAME = Tariqfm	DESIGNED -	REVISED - T. RAMMACHER 09-19-94			TYPICAL APPLICATION	INC	RTF.	SECTION	COUNTY	SHEETS NO.
c:\p	ow_work\pwidot\tariqfm\d03l5748\DistS	:d.dgn	DRAWN -	REVISED -T. RAMMACHER 03-12-99	STATE OF ILLINOIS				372	0303S-TS	соок	87 75
		PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED -T. RAMMACHER 01-06-00	DEPARTMENT OF TRANSPORTATION	KAISED	REFLECTIVE PAVEMENT MARKERS	(SNOW-PLOW RESISTANT)		TC-11	CONTRACT	T NO. 60X08
		PLOT DATE = 3/31/2014	DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS S	TA. TO STA.	FED. ROAD I	DIST. NO. 1 ILLINOIS FE	ED. AID PROJECT	



TYPICAL CROSSWALK MARKING

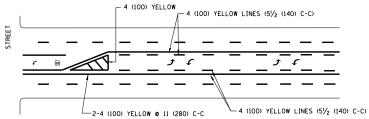


4' (1.2 m) WIDE MEDIANS ONLY

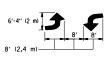


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

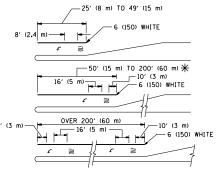


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

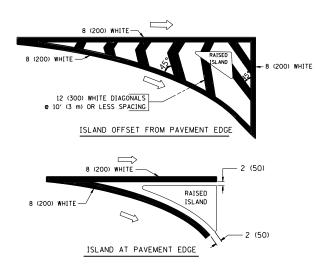


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. \P AREA = 15.6 SO. FT. (1.5 m²) \P AREA = 20.8 SO. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

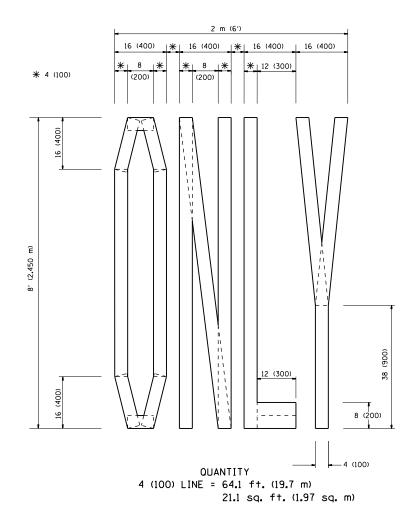
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOL ID SOL ID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 1280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW: EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART 5EE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SO. FT. (0.33 m²) EACH "X"=54.0 SO. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (0VER 45MPH (70 km/h))

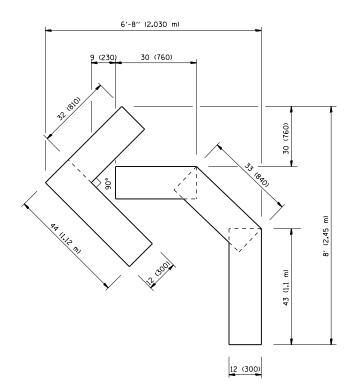
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

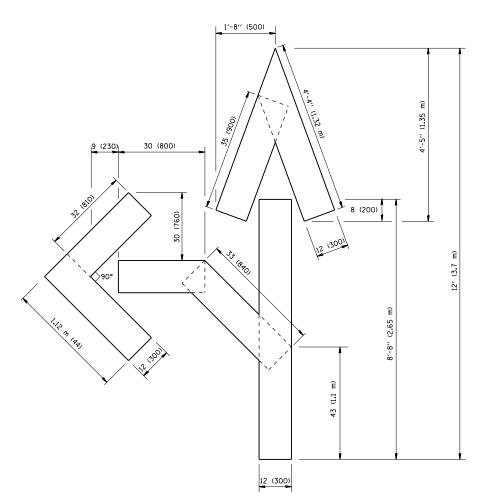
- 1							
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	c:\pw_work\pwidot\tariqfm\d0315748\DistS	td.dgn	DRAWN -	REVISED -C. JUCIUS 09-09-09	STATE OF ILLINOIS		
		PLOT SCALE = 100.0002 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION		
- 1		DLOT DATE - 2/21/2014	DATE - 03-19-90	DEVISED -	1	SCALE NONE	СП

	DI	STRICT ON	IE		F.A.P. RTE.	SECTION	COUNTY	TOTAL	SHEET NO.
	TYPICAL PAVEMENT MARKINGS	372	0303S-TS	соок	87	76			
					TC-13	CONTRACT	NO.	80X08	
CALE: NONE	SHEET NO. 1 OF 1	SHEETS	STA.	TO STA.	FED. F	ROAD DIST. NO. 1 ILLINOIS FED. A	ID PROJECT		





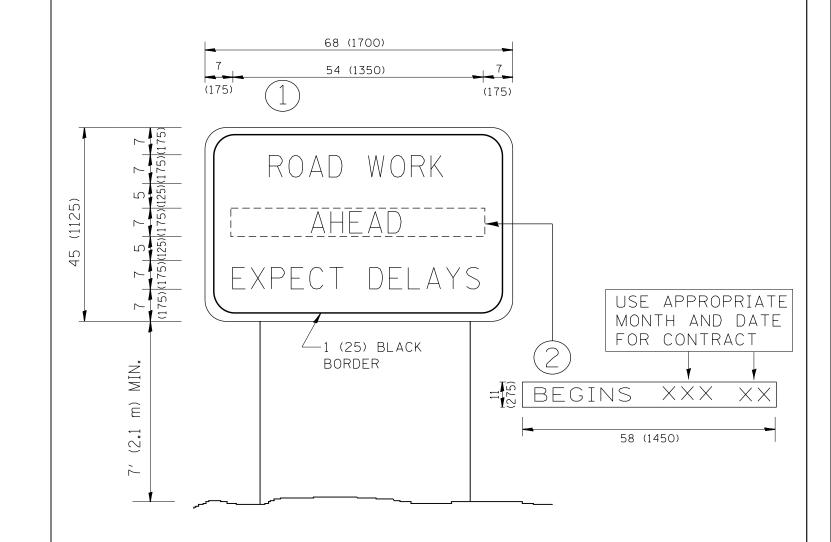
OUANTITY 4 (100) LINE = 45.5 ft. (13.9 m) 15.2 sq. ft. (1.39 sq. m)



QUANTITY 4 (100) LINE = 82.5 ft. (25.3 m) 27.5 sq. ft. (2.53 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME : USER NAME = Tariqfim DESIGNED - REVISED -T. RAMMACHER 06-05-96	AVEMENT MARKING LETTERS AND SYMBOLS	RTF.	SECTION	COUNTY	TOTAL SHEET SHEETS NO.
ct/pw.work/pwidot/tariqfm/d0315748/DistSkd.dgn DRAWN - REVISED -T. RAMMACHER 11-04-97 STATE OF ILLINOIS		372	0303S-TS	соок	87 77
PLOT SCALE = 100.0002 '/ in. CHECKED - REVISED -T. RAMMACHER 03-02-98 DEPARTMENT OF TRANSPORTATION	FOR TRAFFIC STAGING		TC-16	CONTRACT	NO. 60X08
PLOT DATE = 3/31/2014 DATE - 09-18-94 REVISED -E. GOMEZ 08-28-00 SCALE: NONE SHE	HEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROA		AID PROJECT	

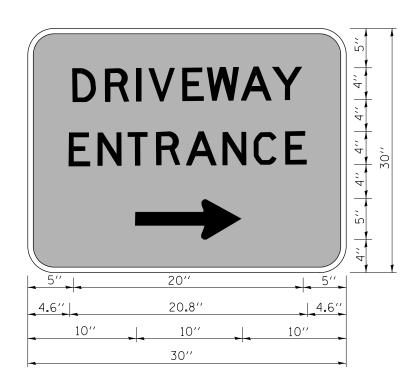


NOTES:

- 1. USE BLACK LETTERING ON ORANGE BACKGROUND.
- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

Γ	FILE NAME =	USER NAME = Tariqfm	DESIGNED -	REVISED - R. MIRS 09-15-97	·		ARTERIAL ROAD		F.A.P.	SECTION	COUNTY	TOTAL S	EET
	c:\pw_work\pwidot\tariqfm\d0315748\DistS	td.dgn	DRAWN -	REVISED - R. MIRS 12-11-97	STATE OF ILLINOIS				372	0303S-TS	соок	87	78
		PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED -T. RAMMACHER 02-02-99			INFORMATION SIGN			TC-22	CONTRACT	NO. 60	.08
- 1		PLOT DATE = 3/31/2014	DATE -	REVISED - C. JUCIUS 01-31-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA.	TO STA.	FED. ROAD I		D PROJECT		



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

- 1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
- 2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
- 3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = Tariqfm	DESIGNED -	REVISED - C. JUCIUS 02-15-07
c:\pw_work\pwidot\tariqfm\d0315748\DistS	td.dgn	DRAWN -	REVISED -
	PLOT SCALE = 100.0002 '/ in.	CHECKED -	REVISED -
	PLOT DATE = 3/31/2014	DATE -	REVISED -

STATE OF ILLINOIS	
DEPARTMENT OF TRANSPORTATION	

	DRIVEWAY ENTRANCE SIGNING				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	
	3					0303S-TS	СООК	87	79
							CONTRACT	NO. 6	80X08
ı	SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA. FED. ROAD DIST. NO. 1 LLINDIS FED. AID PROJECT								

LOOPS NEXT TO SHOULDERS PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER. PAVED OR NON-PAVED SHOULDER Ê (1.5 m) (1.8 m) (1.5 m) 1" (25 mm) UNI DUCT-TRENCHED TO E/P •• (3.0 m) (3.0 m) * = (600 mm)* * UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS. ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)

VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING) HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD B14001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN. TRENCHED 1" (25 mm) UNIT DUCT (3) ** * = (600 mm) * = (600 mm) * | 12' | (3.6 m) 12' | (3.6 m) 12' | (6.0 mm) * | 12' | (6.0 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO

BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

VOLUME DENSITY ("FAR OUT" DETECTION)

ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

* = (600 mm)

* = (600 mm)

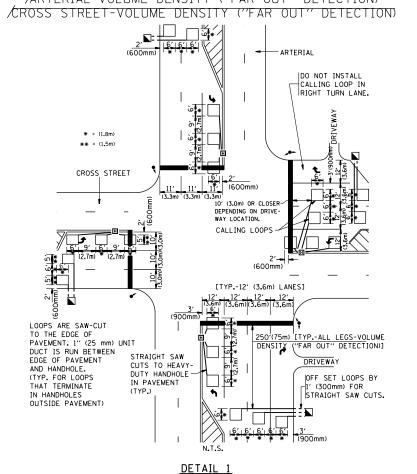
* = (600 mm)

* = (600 mm)

STRAIGHT SAW CUT TO HEAVY DUTY HANDHOLE (TYP.) PLACE HEAVY DUTY HANDHOLE BETWEEN FIRST AND SECOND LOOP AS SHOWN.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



N.T.S.

DESIGNED

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R.K.F.

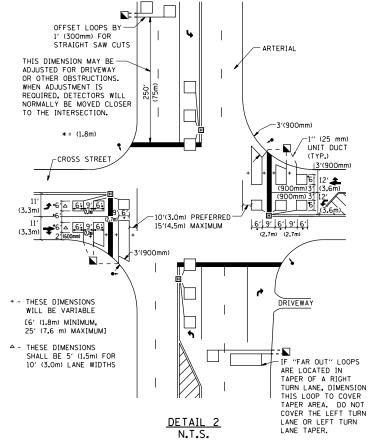
USER NAME = Tarigfm

PLOT DATE = 3/31/2014

PLOT SCALE = 100.0002 '/ in.

FILE NAME :

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SCALE: NONE

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIFLDED
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX, EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF <u>ALL</u> DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE
 THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR
 (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

JOTE.

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.				
DETAILS FOR ROADWAY RESURFACING				372	0303S-TS	COOK	87	80			
				TS-07		CONTRACT	NO. 6	80X0			
	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

