CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS



CONSTRUCTION PLANS FOR

CHICAGO EXECUTIVE AIRPORT

REHABILITATE EAST QUADRANT APRON, PHASE 2

ILLINOIS PROJECT: PWK-4427

S.B.G. PROJECT: 3-17-SBGP-XX

DATE: APRIL 17. 2015

JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE—CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

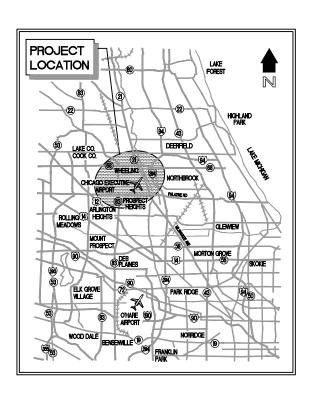
CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811

CHICAGO EXECUTIVE AIRPORT

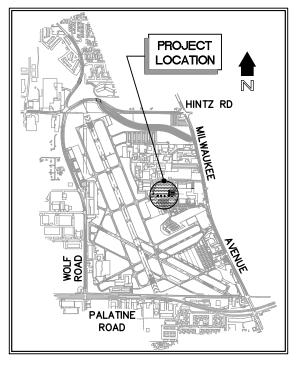
TOWNSHIP: 42 NORTH RANGE: 11 EAST

14290-04 27 February 2015

CHICAGO EXECUTIVE AIRPORT EXECUTIVE DIRECTOR DATE 25 Februar 2015



LOCATION MAP



SITE PLAN

PROJECT INFORMATION

CONTRACTOR: RESIDENT ENGINEER: ORIGINAL CONTRACT AMOUNT: FINAL CONSTRUCTION COST: IDOT LETTING DATE: IDOT AWARD DATE: NOTICE TO PROCEED: START OF CONSTRUCTION: SUBSTANTIAL COMPLETION:

LOCAL AGENCY CONTACT INFORMATION

VILLAGE OF WHEELING - 847.459.2600 CITY OF PROSPECT HEIGHTS - 847.398.6070

ENGINEER'S PROJECT PERMIT LOG

NPDES # FAA AIRSPACE # CCDD LPC-663 DATED 11/16/12 MWRDGC PERMIT # 03-246 & RL 09-063 VILLAGE APP FOR CONSTRUCTION PERMIT # VILLAGE FLOODPLAIN PERMIT #
CONTRACTORS REGISTRATION WITH VILLAGE VILLAGE SITE ALTERATION PERMIT # CITY APPLICATION FOR PERMIT # CITY FLOODPLAIN PERMIT # CITY SITE GRADING PERMIT # CONTRACTORS REGISTRATION WITH CITY

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SUMMARY OF QUANTITIES ITEM NO. DESCRIPTION UNIT QUANTITY QUANTITY AR150510 ENGINEER'S FIELD OFFICE AR152410 UNCLASSIFIED EXCAVATION CY 570 AR156520 INLET PROTECTION EACH CY 85 AR208515 POROUS GRANULAR EMBANKMENT AR209606 CRUSHED AGG. BASE COURSE - 6" SY 920 SY 1,760 AR209650 AGGREGATE BASE PREPARATION AR401610 BITUMINOUS SURFACE COURSE TON 20 SY BITUMINOUS PAVEMENT MILLING 130 AR401650 SY BITUMINOUS PAVEMENT GRINDING 1,715 AR401910 REMOVE & REPLACE BIT. PAVEMENT SY AR501509 9" PCC PAVEMENT SY 2,520 AR501530 PCC TEST BATCH EACH AR501900 REMOVE PCC PAVEMENT SY GAL BITUMINOUS TACK COAT 30 AR603510 SF AR620520 PAVEMENT MARKING - WATERBORNE 9,000 SF PAVEMENT MARKING - BLACK BORDER 1,380 AR620525 SF AR620900 PAVEMENT MARKING REMOVAL 900 LF 275 AR705506 6" PERFORATED UNDERDRAIN AR751920 REPLACE INLET EACH CONTROLLED LOW STRENGTH MATERIAL CY AR800035 AR800194 REMOVE ELEVATED RETROREFLECTIVE MARKER EACH AR910420 EACH ADDITIVE ALTERNATE #1 CONSTRUCTION - PAVEMENT AND INLET REPAIRS AS209650 AGGREGATE BASE PREPARATION 240 9" PCC PAVEMENT SY 240 AS501509 SY 240 AS501900 REMOVE PCC PAVEMENT AS751980 RECONSTRUCT INLET EACH

IL. CONTRACT: PA058

IL. LETTING ITEM: 11A

IL. PROJECT: PWK-4427 S.B.G. PROJECT: 3-17-SBGP-XX

SURVEY BOOK # 1279

REVISIONS		
NUMBER	BY	DATE

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS REHABILITATE EAST QUADRANT APRON, PHASE

SUMMARY OF QUANTITIES

MURPHY & T ENGINEERS 184-00061.3

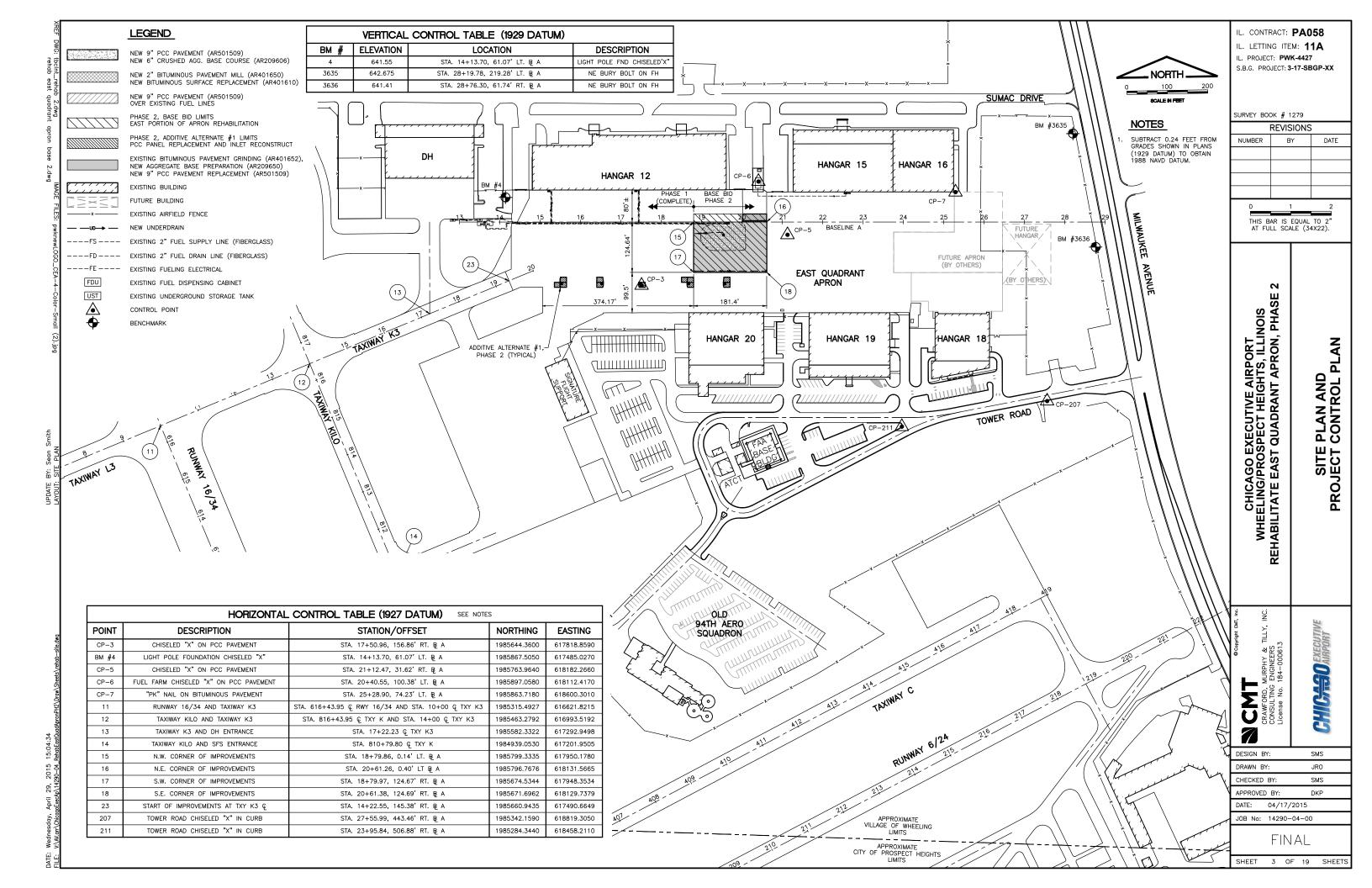
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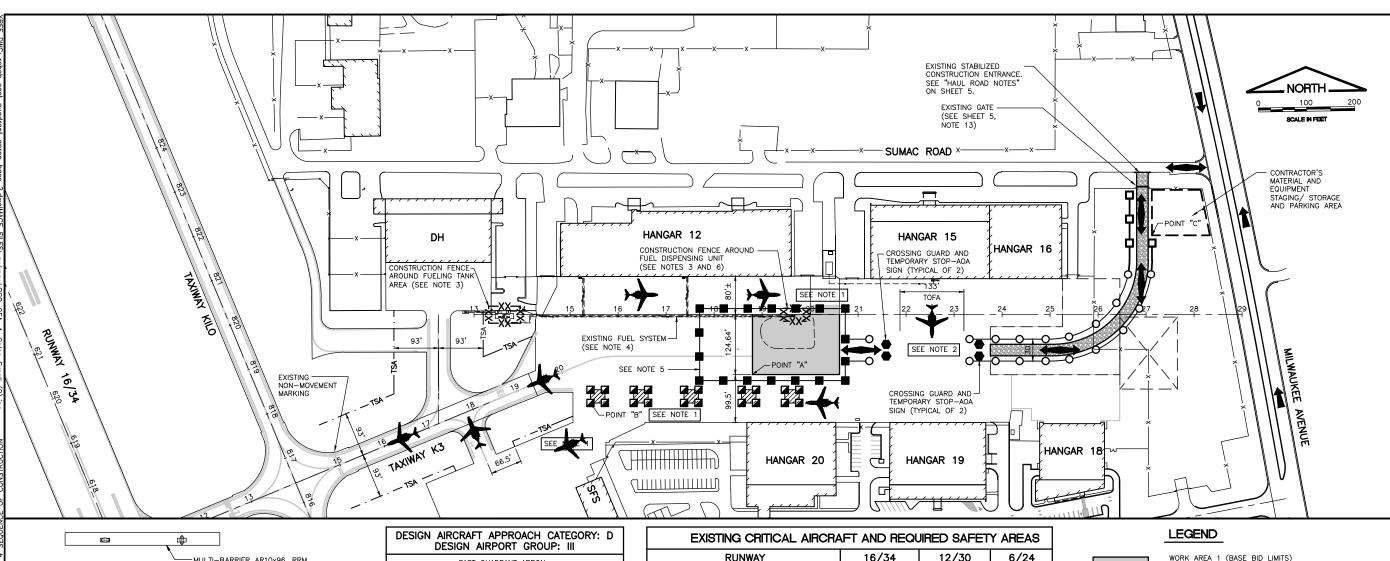
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CHICA-30 EXECUTIVE

FINAL

SHEET 2 OF 19 SHEETS





MULTI-BARRIER AR10x96, RRM AIRPORT BARRIER 4202-25, WATERCADE WCO1-10, NEUBERT AERO CORP. NAC-PC9642, OR APPROVED EQUAL (SEE NOTE 6) <u>PLAN</u> RED STEADY - 20" X 20" NYLON FLAG WITH 12 GA. DIAGONAL WIRE STIFFENER. TWO FLAGS TOTAL (ONE ORANGE FLAG AND ONE WHITE FLAG). FLAGS SHALL BE REMOVABLE. " TO 12" WIDE

LOW PROFILE LIGHTED BARRICADE

- BARRICADE NOTES: FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OR SOLAR POWER OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
- FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
- 3. BARRICADES TO BE PLACED END TO END AS INDICATED AT THE LOCATIONS SHOWN ON THE PLANS ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION OR AS DIRECTED BY THE RESIDENT ENGINEER OR AIRPORT. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- 4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
- 5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
- 6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN.

GENERAL NOTES

- 1. NO AIRCRAFT WILL BE ALLOWED TO PARK IN THIS AREA TO ALLOW FOR AIRCRAFT TRAFFIC.
- 2. CONTRACTOR WILL BE REQUIRED TO HAVE A CROSSING GUARD FOR CONTRACTOR'S VEHICLES AND EQUIPMENT CROSSING
- TEMPORARY FENCE SHALL BE INSTALLED PRIOR TO THE BEGINNING OF CONSTRUCTION OPERATIONS. INITIAL INSTALLATION REMOVAL AND REPLACEMENT DUE TO SEQUENCING AND CONSTRUCTION OPERATIONS SHALL BE INCLUDED IN THE CONTRACT.
- CONTRACTOR SHALL INSTALL BARRICADES ALONG FUEL LINE SYSTEM LIMITS (BOTH SIDES). THE CONTRACTOR SHALL NOT CROSS OVER FUEL LINES DURING HAULING OPERATIONS OR CONSTRUCTION OPERATIONS.

EAST QUADRANT APRON
MAXIMUM ANTICIPATED WINGSPAN OF ADG III
GULFSTREAM G500 — WINGSPAN = 93.5'
COMPUTED TAXILANE CENTERLINE TO OBJECT
SEPARATION (TOFA) = 66.1'

GROUND CONTROL FREQUENCY: 121.7 AIR CONTROL FREQUENCY: 119.9 MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 25'

IN THE EVENT THE CONTRACTOR PROPOSES TO UTILIZE AN ON-SITE CONCRETE BATCH PLANT, LOCATION TO BE COORDINATED WITH RESIDENT ENGINEER AND AIRPORT MANAGER TO ALLOW FOR APPROPRIATE AIRSPACE CLEARANCE, THE CONTRACTOR WILL BE RESPONSIBLE TO SUBMIT FAA FORM 7460 FOR AIRSPACE APPROVAL. THE RESIDENT ENGINEER WILL PROVIDE BASE AIRPORT INFORMATION FOR THE CONTRACTOR'S USE.

POINT "A", WORK AREA 1 CLOSEST CONSTRUCTION POINT TO RUNWAY 16/34 AND RUNWAY 12/30 ELEVATION: 640.2 LATITUDE: 42'07'03.14" (NAD83) LONGITUDE: 87'53'55.25" (NAD83)

POINT "B", WORK AREA 2 CLOSEST CONSTRUCTION POINT TO RUNWAY 16/34 AND RUNWAY 12/30 ELEVATION: 640.2 LATITUDE: 42'07'02.84" (NAD83) LONGITUDE: 87'53'59.84" (NAD83)

POINT "C", CLOSEST STAGING AREA LOCATION TO RUNWAY 16/34, RUNWAY 12/30 AND RUNWAY 6/24 ELEVATION: 641.8 LATITUDE: 42'07'05.83" (NAD83) LONGITUDE: 87'53'44.13" (NAD83)

NOTE — ALL WORK AREAS ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

RUNWAY 16/34 12/30 6/24 APPROACH CATEGORY DESIGN GROUP GULESTREAM 550 KING AIR B200 DESIGN AIRCRAFT CESSNA 421 APPROACH SPEED 141 KNOTS 103 KNOTS 96 KNOTS WINGSPAN 94 FEET 55 FEET 42 FEET TAIL HEIGHT 25.8 FEET 15.0 FEET 11.6 FEET STRENGTH (MGTW) 90.500 LBS 12.500 LBS. 7,450 LBS LENGTH 97 FEET 44 FEET 37 FEET AOA • RUNWAY SAFETY AREA WIDTH (RSA) 150 120 400 RUNWAY OBJECT FREE AREA WIDTH (ROFA) 500 49 TAXIWAY SAFETY AREA WIDTH (TSA) AOA • TAXIWAY OBJECT FREE AREA WIDTH (TOFA) 186 131 89

AOA = AIRCRAFT OPERATIONS AREA

DATA FROM 2009 CEA APPROVED ALP

WORK AREA	OPERATIONAL STATUS/ RESTRICTIONS
WORK AREA 1 EAST QUADRANT APRON REHAB	TAXIWAY K3 ENTRANCE TO EAST QUADRANT APRON — OPEN
	EAST QUADRANT APRON — AIRCRAFT OPERATIONS RESTRICTED TO OWNER'S DESIGNATED AVIATION PERSONNEL TO "WING WALK" AIRCRAFT AT ALL TIMES. CONTRACTOR'S FLAGGERS TO DIRECT AND MOVE THEIR MEN AND EQUIPMENT TO BE CLEAR TO ALLOW PASSAGE OF AIRCRAFT VIA "WING WALKERS". 10 FEET WINGTIP CLEARANCE WILL BE REQUIRED ON BOTH SIDES OF AIRCRAFT.
WORK AREA 2 EAST QUADRANT APRON PANEL REPLACEMENT	EAST QUADRANT APRON — PANEL REPLACEMENT WORK SHALL ONLY BE STARTED ONCE PHASE 1 WORK IS COMPLETED AND OPEN TO AIRCRAFT TRAFFIC ON PHASE 1 PAVEMENT

GENERAL NOTES, CONT.

- A MUTUALLY AGREED UPON LOCATION SHALL BE COORDINATED WITH HANGAR TENANTS PRIOR TO ESTABLISHING THE WESTERNMOST BARRICADE LINE IN PHASE 1.
- 6. WORK AREAS 1 AND 2 SHALL NOT BE CONCURRENT.

CONTRACTOR'S WORK AREA

WORK AREA 2 (ADDITIVE ALTERNATE #1 LIMITS) CONTRACTOR'S WORK AREA

AIRCRAFT MOVEMENT AREA

WORK AREA 1 LOW PROFILE BARRICADES

WORK AREA

LOW PROFILE BARRICADES ALL WORK AREAS

LOW PROFILE BARRICADES FOR HAUL ROUTE ___

IDOT TYPE II BARRICADES (20' SPACING) WITH STEADY OR FLASHING RED LIGHTS FOR HAUL ROUTE DESIGNATION OUTSIDE AIRCRAFT MOVEMENT AREA

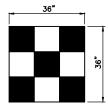
CROSSING GUARD AND TEMPORARY "STOP" AND



"AIRCRAFT MOVEMENT AREA" SIGN AIR OPERATIONS AREA (AOA)

CONTRACTOR'S ACCESS/HAUL ROUTE EXISTING FENCE

4' TALL CONSTRUCTION FENCE (INCIDENTAL)



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG

NOT TO SCALE

IL. CONTRACT: PA058 IL. LETTING ITEM: 11A IL. PROJECT: PWK-4427

S.B.G. PROJECT: 3-17-SBGP-XX

URVEY BOOK # 1279 REVISIONS

BY DATE NUMBER

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

ONSTRUCTION (LATEST EDITION) CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS EHABILITATE EAST QUADRANT APRON, PHAS OF C CE (SEQUENC R AC 150/

HY & LEERS

CHICAGO EXECUT

DESIGN BY: SMS DRAWN BY JRO CHECKED BY SMS APPROVED BY 04/17/2015 JOB No: 14290-04-00 FINAL

SHEET 4 OF 19 SHEETS

- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2F (LATEST EDITION) OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE
- 8. THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT
- 10. MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY. UNLESS NOTED OTHERWISE.
- . PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT MARKERS, AIR OPERATIONS AREA (A.C.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PARAITS. SEPARAITELT, BOT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES WITH TWO ORANGE FLAG (20" x 20") BETWEEN EACH SET OF BARRICADES OR 4' HIGH ORANGE SNOW FENCE SECURELY ATTACHED TO EACH BARRICADE SHALL BE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER BARRICADES SHALL HAVE A STEADY BURN OR FLASHING RED LIGHT. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TABLES. TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. IN CONJUNCTION WITH IDOT TYPE II BARRICADES, THE CONTRACTOR SHALL SUPPLY AND USE AS DIRECTED BY THE AIRPORT, REFLECTIVE LOW PROFILE TYPE BARRICADES. ALL BARRICADES SHALL BE PLACED OUTSIDE OF ACTIVE SAFETY AREAS.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER TEN (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- 13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON-WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE READING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON
 LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD,
 ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- 14. CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT, EXCEPT HAUL VEHICLES, AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL
- 15. IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT
- 16. DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- 17. THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN ASPHALT/STONE TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEFT IN A DUMP POSITION.
- 18. WHEN APPLICABLE, IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
- 19. CHICAGO EXECUTIVE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT
- 20. APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE SEQUENCING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF—SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON—SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON—SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER, FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK, ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES
- 21. MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.

6

- 22. LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO NEXT EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
- 23. COORDINATION MEETINGS THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE RESIDENT ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE
- 24. THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY, PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- 25. DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO
- 26. CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS AND THE RUNWAY SAFETY AREA (RSA) OF
- 27. CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 IMAGINARY SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
- 28. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS. FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 29. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAVE BEEN OBTAINED FROM EXISTING RECORDS. NETHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES, PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- 30. ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER. ANY DEFECIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.
- 31. ORANGE CONES SHALL BE PLACED AT 25' CENTERS ALONG THE PAVEMENT EDGE DURING CONCRETE POURING OPERATIONS OF THE CLOSURE LANES TO PREVENT VEHICLES FROM ENTERING PLASTIC CONCRETE. IN THE EVENT A VEHICLE ENTERS THE CONCRETE BEFORE A MINIMUM COMPRESSIVE STRENGTH OF 3,500 PSI HAS BEEN OBTAINED, SAID PAVEMENT SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.

CONTRACTOR CROSSING RUNWAY SAFETY AREAS (RSA) AND TAXIWAY OBJECT FREE AREA (TOFA)

- 32 ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTROL WITH THE CONTROL TOWER SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
- 33. ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEPT TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE

MUNICIPALITIES GENERAL NOTES

- 1. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH VILLAGE/CITY CODES, ORDINANCES AND STANDARDS AS
- 2. ALL ELEVATIONS SHOWN ON PLANS ARE IN 1929 DATUM, SUBTRACT 0.24 FEET FROM ELEVATIONS SHOWN TO OBTAIN 1988 NAVD
- 3. ALL CONTRACTORS AND SUBCONTRACTORS SHALL BE REGISTERED WITH THE VILLAGE/CITY PRIOR TO THE NOTICE TO PROCEED. ALL REGISTRATION FEES SHALL BE INCIDENTAL TO THE CONTRACT
- THE CONTRACTOR SHALL WORK WITH THE AIRPORT AND ENGINEER TO SECURE THE REQUIRED VILLAGE AND CITY LOCAL CONSTRUCTION PERMITS PRIOR TO THE NOTICE TO PROCEED.
- ALL STORM SEWERS ON THE AIRPORT SITE ARE OWNED, OPERATED AND MAINTAINED BY THE CHICAGO EXECUTIVE AIRPORT.
- 6. THE CONTRACTOR SHALL COORDINATE WITH THE VILLAGE/CITY AT THE WEEKLY PROGRESS MEETINGS AND SHALL NOTIFY THE CITY OF PROSPECT HEIGHTS (847.398.6700) AND THE VILLAGE OF WHEELING (847.459.2600) A MINIMUM OF 48 HOURS PRIOR TO ANY REQUIRED VILLAGE/CITY INSPECTIONS.

LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY SAFETY AREA (RSA) AND TAXIWAY OBJECT FREE AREA (TOFA)

RUNWAYS:

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. ANY WORK WITHIN THE RUNWAY SAFETY AREA WILL REQUIRE A RUNWAY CLOSURE. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING PERIOD THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED PER FAA REQUIREMENTS. AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES. IF NECESSARY, STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE RSA AT NO ADDITIONAL COST TO THE CONTRACT ON OMATERIAL SHALL BE STOCKEUED WITHIN THE PSA AT NO ADDITIONAL COST TO THE CONTRACT. NO MATERIAL SHALL BE STOCKPILED WITHIN THE RSA. IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN, EQUIPMENT AND BARRICADES.

TAXIWAYS:

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE FOR WORK WITHIN THE TAXIWAY OBJECT FREE AREA. ANY WORK WITHIN THE TAXIWAY OBJECT FREE AREA WILL REQUIRE A TAXIWAY CLOSURE. CONSTRUCTION MAY BE ALLOWED UP TO THE EDGE OF THE TAXIWAY PAVEMENTS WITHOUT CLOSURE ON A LIMITED BASIS AS DETERMINED BY THE AIRPORT MANAGER AND THE CONTRACTOR WILL BE REQUIRED TO HAVE WING WALKERS AND FLAGGERS AT HIS OWN COST. WORK WITHIN THE TAXIWAY OBJECT FREE AREA SHALL BE EXPEDITED AND AT THE END OF EACH WORKING PERIOD THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE TAXIWAY TO BE REOPENED PER FAA REQUIREMENTS. IF NECESSARY, STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE TOFA AT NO ADDITIONAL COST TO THE CONTRACT. NO MATERIAL SHALL BE STOCKPILED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT.

> CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS
> IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND ACCESS THE WORK SITE DUE TO

RUNWAY 16 EMAS REHABILITATION OF RUNWAY 16/34

HAUL ROAD NOTES

- THE HAUL ROUTE FROM THE PHASE 1 PROJECT WAS LEFT IN-PLACE AT THE COMPLETION OF CONSTRUCTION AND SHALL BE UTILIZED FOR THIS PROJECT. THE CONTRACTOR SHALL INVESTIGATE THE SUITABILITY OF THE HAUL ROAD FOR HIS OWN OPERATIONS. LACK OF ACCESS DUE TO POOR HAUL ROAD CONDITIONS SHALL NOT BE CONSIDERED A WARRANTABLE REASON FOR CONTRACT OVERRUN OR DELAY.
- 2. THE CONTRACTOR SHALL REMOVE THE HAUL ROAD AT THE COMPLETION OF ALL CONSTRUCTION OPERATIONS. THE AREA SHALL BE REGRADED TO DRAIN WITH TOPSOIL SUITABLE TO ESTABLISH A STAND OF GRASS, SEEDED AND MULCHED, AT NO ADDITIONAL COST TO THE CONTRACT.

IL. CONTRACT: PA058

IL. LETTING ITEM: 11A

IL. PROJECT: PWK-4427 S.B.G. PROJECT: 3-17-SBGP-XX

SURVEY BOOK # 1279

REVISIONS			
NUMBER	BY	DATE	

THIS BAR IS FOUAL TO 2 AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT EELING/PROSPECT HEIGHTS, ILLINOIS ILITATE EAST QUADRANT APRON, PHAS

ONSTRUCTION NOTES Ž OF CC ERAL ШΖ C III QUENC ш WHI S

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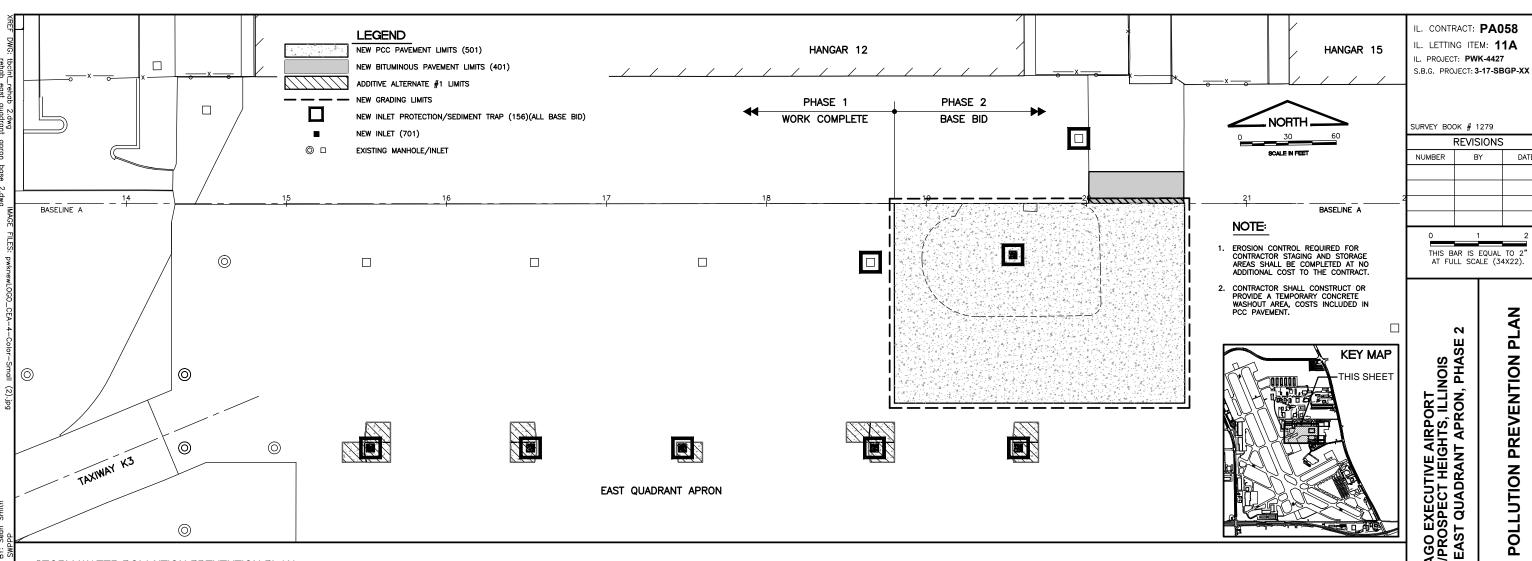
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SHEET 5 OF 19 SHEETS



STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION

THE PURPOSE OF THIS PLAN IS TO MINIMIZE FROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATION OF A PORTION OF THE EAST QUADRANT GENERAL AVIATION APRON AT THE CHICAGO EXECUTIVE AIRPORT. THE PROJECT INCLUDES EARTH EXCAVATION, VARIOUS PAVEMENT ITEMS AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

- 1. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL. SUCH
- 2. PAVEMENT MILLING, PAVEMENT REMOVAL AND REPLACEMENT.
- 3. EXCAVATION AND EMBANKMENT WILL BE COMPLETED WITHIN THE PROJECT LIMITS TO GRADE OUT FOR THE PROPOSED PAVEMENT IMPROVEMENTS.
- PAVEMENT CONSTRUCTION.
- PAVEMENT MARKING AND OTHER MISCELLANEOUS ITEMS
- PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND EROSION CONTROL BLANKET IN AREAS DISTURBED BY CONTRACTOR STAGING, STORAGE AND HAUL OPERATIONS.

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 1 ACRE OF WHICH 1 ACRE WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS. STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS;

- 1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION
- 2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE DES PLAINES RIVER THROUGH A STORM SEWER SYSTEM

CONTROLS-EROSION CONTROLS AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

- THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTEL CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
- 2. AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, TEMPORARY DITCH CHECKS, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLAN AND DIRECTED BY THE ENGINEER.
- 3. THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

- WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
- 2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
- 3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE

A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

B. CONSTRUCT DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.

- C. BUILD NECESSARY EMBANKMENT AT CULVERT/STORM SEWER LOCATIONS AND THEN EXCAVATE AND
- D. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
- CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR DELIVERY OF THE CONTROL OF
- 5. THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT WEEKLY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE AFTER RAINS OF 1/2-INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD.
- SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND
- 7. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

- TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.
- 2. ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

IL. CONTRACT: PA058 IL. LETTING ITEM: 11A

IL. PROJECT: PWK-4427

URVEY BOOK # 1279

REVISIONS NUMBER BY DATE

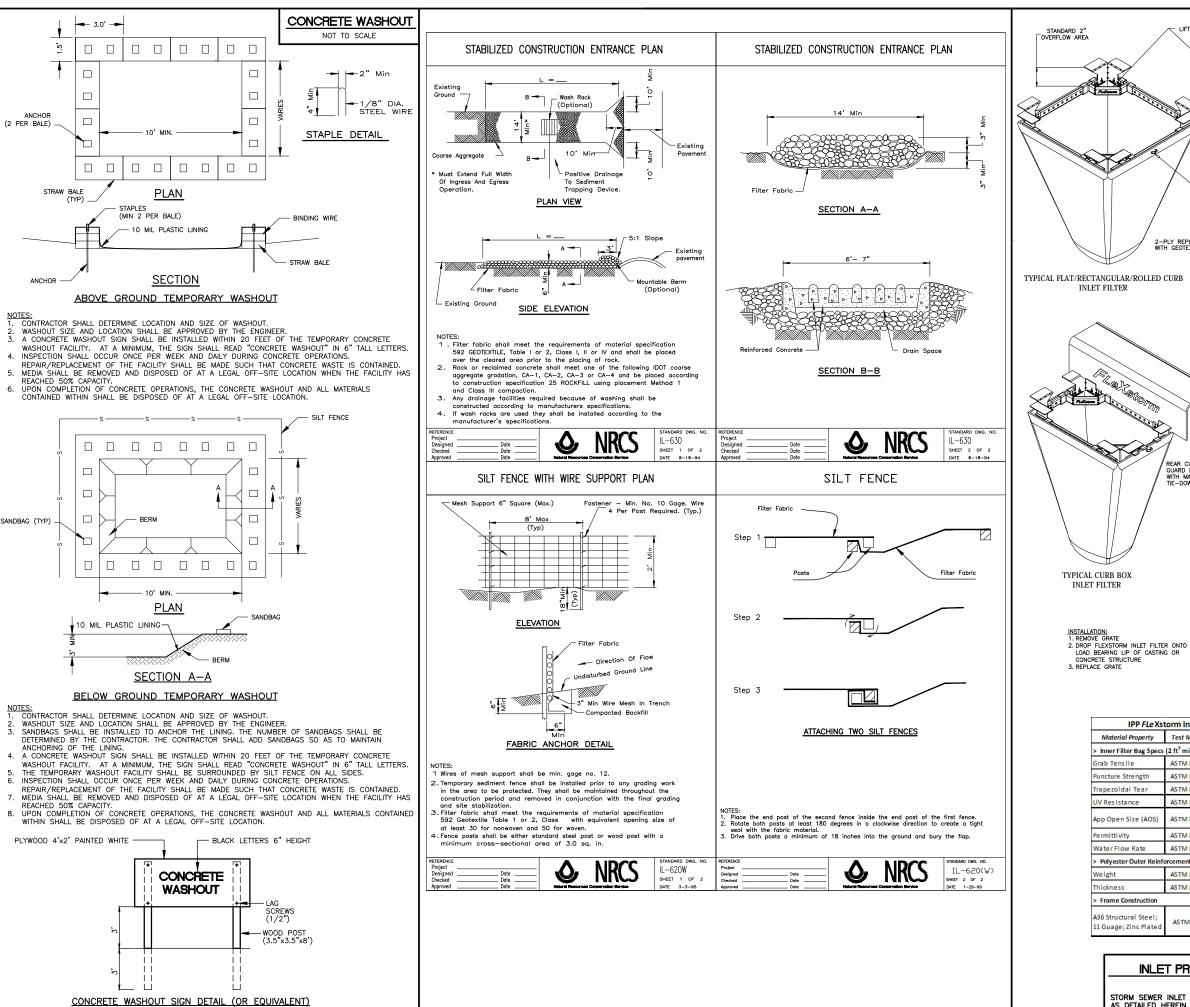
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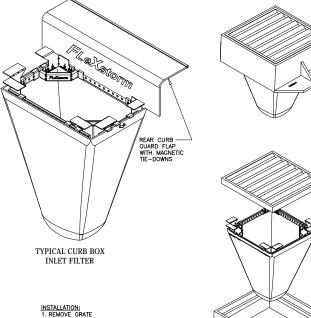
SHEET 6 OF 19 SHEETS



LIFT HANDLES 11 GAUGE STEEL SUSPENSION SYSTEM STAINLESS STEEL CLAMPING 2-PLY REPLACEABLE SEDIMENT BAGS

TYPICAL FLAT/RECTANGULAR/ROLLED CURB INLET FILTER





IPP FLe Xs	torm Inlet Filt	er Specificatio	ns	
Material Property Test Method		Value (n	Value (min ave)	
> Inner Filter Bag Specs	(2 ft ³ min vol)	Non-Woven	Woven Mono	
Grab Tensile	ASTM D 4632	100 lbs	200 lbs	
Puncture Strength	ASTM D 4833	65 lbs	90 lbs	
Trapezoidal Tear	ASTM D 4533	45 lbs	75 lbs	
UV Resistance	ASTM D 4355	70% at 500 hrs	90%	
App Open Size (AOS)	ASTM D 4751	70 sieve (.212 mm)	40 s ie ve (.425 mm)	
Permittivity	ASTM D 4491	2.0 /s ec	2.1/sec	
Water Flow Rate	ASTM D 4491	145 gpm/sqft 145 gpm/sqft		
> Polyester Outer Reinfe	orcement Bag Sp	ecifications		
Weight	ASTM D 3776	4.55 oz/sqyd +/- 15%		
Thickness	ASTM D 1777	.040 +/005		
> Frame Construction				
A36 Structural Steel; 11 Guage; Zinc Plated	ASTM A 576	Tensile Strength > 58,000 psi; Yield Strength > 36,000 psi		

INLET PROTECTION / SEDIMENT TRAP

NOT TO SCALE

STORM SEWER INLET PROTECTION SHALL BE FLEXSTORM INLET FILTERS AS DETAILED HEREIN OR APPROVED EQUAL

IL. CONTRACT: PA058

IL. LETTING ITEM: 11A IL. PROJECT: PWK-4427 S.B.G. PROJECT: 3-17-SBGP-XX

URVEY BOOK # 1279

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PLAN CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS REHABILITATE EAST QUADRANT APRON, PHASE EVENT S POLLUTION PRI ES AND DETAIL

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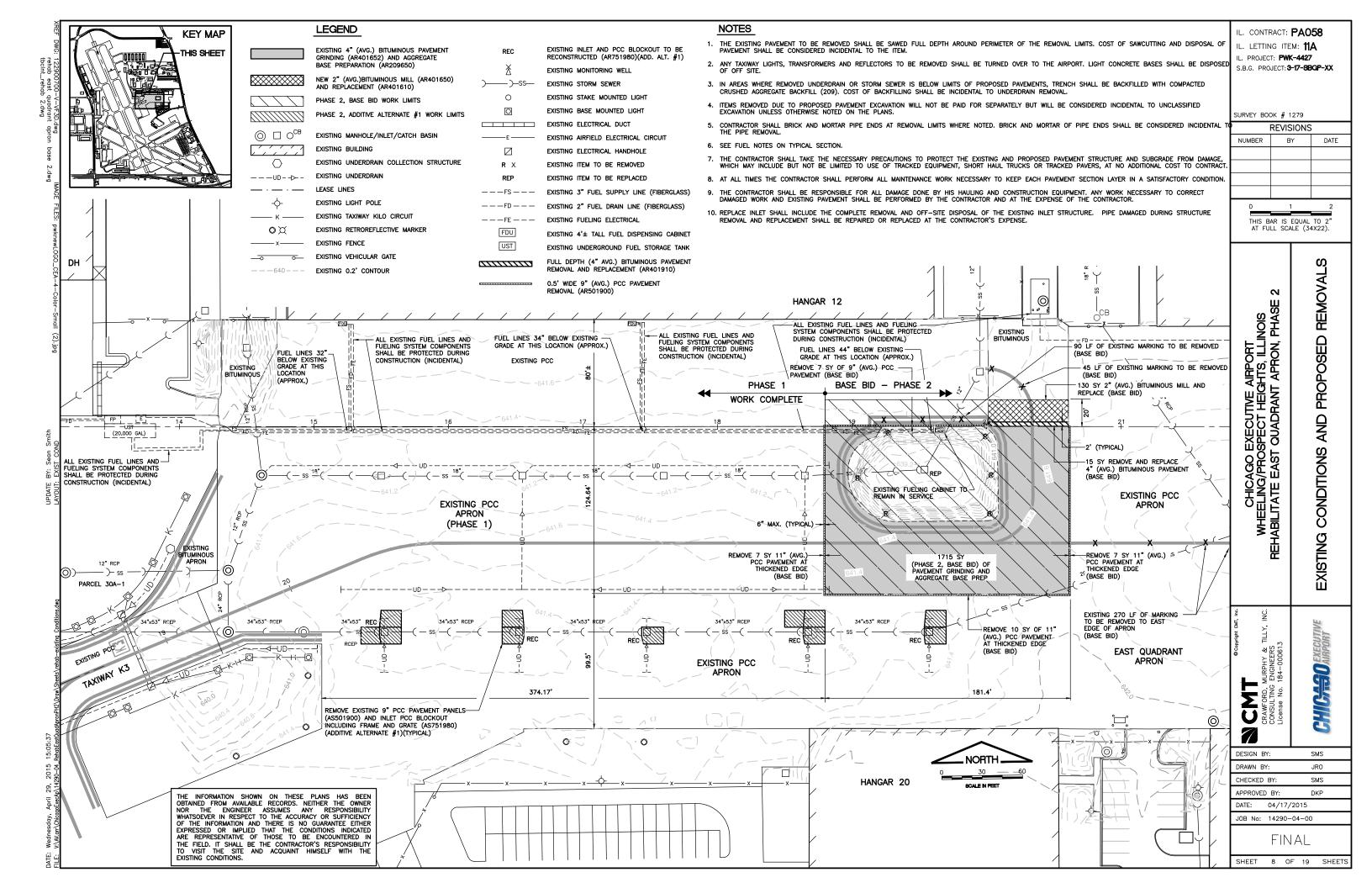
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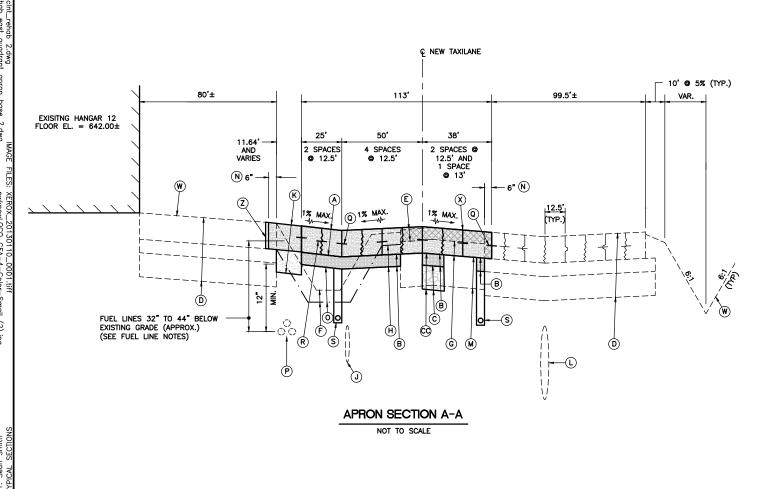
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SHEET 7 OF 19 SHEETS





-1 SPACE @ 14.17'

77.75

5 SPACES @ 15.55'

(T)

GENERAL NOTES

- 1. MILLINGS RESULTING FROM AR401652 BITUMINOUS PAVEMENT GRINDING SHALL BE USED FOR EMBANKMENT FILL (PAID UNDER
- 2. IT IS ANTICIPATED THAT THERE WILL BE MILLINGS REMAINING AFTER ALL EMBANKMENT FILL IS COMPLETED. THE REMAINING MILLINGS SHALL BE USED FOR BASE COURSE STABILIZATION IN ADDITIONAL UNDERCUT AREAS (AR152410) AS DETERMINED BY
- 3. ANY EXCESS MATERIAL INCLUDING CLAY, EXISTING AGGREGATE BASE, MILLINGS, AND TOPSOIL SHALL BE HAULED OFF AND DISPOSED OF BY THE CONTRACTOR.
- 4. LOCATION OF REMOVAL LIMITS TO BE COORDINATED IN THE FIELD WITH THE RESIDENT ENGINEER PRIOR TO BEGINNING WORK.
- 5. NO THICKENED EDGE AT THIS LOCATION. DOWEL IN EXISTING PAVEMENT SHALL BE INSTALLED AT THE DEPTH FOR 9" PCC PAVEMENT. SEE JOINTING DETAILS.

GENERAL NOTES FOR WORK OVER **EXISTING FUEL LINES**

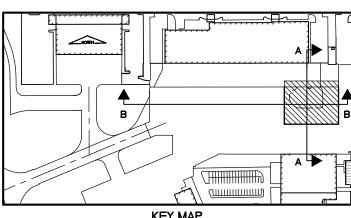
- 1. THE CONTRACTOR SHALL NOT USE HEAVY CONSTRUCTION EQUIPMENT OVER THE FUEL LINES. MEANS, METHODS AND EQUIPMENT TO BE USED OVER AND AROUND THE FUEL LINES SHALL BE APPROVED BY THE OWNER AND ENGINEER PRIOR TO STARTING WORK IN THE FUEL LINE LIMITS.
- CONTRACTOR SHALL INSTALL BARRICADES ALONG FUEL LINE SYSTEM LIMITS (BOTH SIDES). THE CONTRACTOR SHALL NOT CROSS OVER FUEL LINES DURING HAULING OPERATIONS OR
- 3. THE CONTRACTOR SHALL HAND DIG TO VALIDATE DEPTHS OF THE FUEL LINES PRIOR TO EXCAVATION OPERATIONS AROUND AND OVER THE FUEL LINES. (COST INCIDENTAL)
- 4. THE EXISTING DEPTH OF COVER OVER THE FUEL LINES IS APPROXIMATE AND BASED ON OWNER PROVIDED DATA.
- 5. BOTH PRIOR TO THE START OF CONSTRUCTION AND AFTER CONSTRUCTION OF THE PROJECT, THE CONTRACTOR SHALL
 PERFORM THE APPROPRIATE PRESSURE AND LEAKAGE TESTS ON
 THE FUEL LINE SYSTEM TO ENSURE THAT THE FUEL LINES WERE NOT DAMAGED DURING CONSTRUCTION ACTIVITIES. THESE TESTS NOT DAMAGED DURING CONSTRUCTION ACTIVITIES. THESE TESTS SHALL BE PERFORMED BY A CERTIFIED FUEL LINE TESTING COMPANY WITH A MINIMUM OF 5 YEARS EXPERIENCE. RESULTS SHALL BE SUBMITTED TO THE OWNER AND ENGINEER FOR REVIEW AND APPROVAL. ALL COSTS ASSOCIATED WITH THE TESTING, REPORTING AND COORDINATION SHALL BE INCIDENTAL
- 6. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING AND/OR REPLACING ANY FUEL SYSTEM COMPONENTS THAT ARE DAMAGED AS A RESULT OF CONSTRUCTION, ALL FUEL LINE TESTING, EVALUATION AND REPAIR COSTS SHALL BE AT THE CONTRACTOR'S EXPENSE.

- 6" (N

1 SPACE @ 16.4'-

LEGEND

- (A) NEW 9" PCC PAVEMENT (AR501509)
- NEW 6" CRUSHED AGGREGATE BASE COURSE (AR209606)
- (C) NEW 12" POROUS GRANULAR EMBANKMENT (AR208515)
- **(D)** EXISTING PAVEMENT STRUCTURES
 - 9" PCC PAVEMENT 4" ASPHALT TREATED PERMEABLE BASE COURSE (ATPB) 12" LIME MODIFIED SUBGRADE
- EXISTING ±4" BITUMINOUS PAVEMENT GRINDING (AR401652)(SEE NOTES)
- AVERAGE 6" TO 12" TOPSOIL STRIPPING (AR152410)
- EXISTING VARIABLE DEPTH CRUSHED AGGREGATE BASE COURSE TO BE REMOVED (PAID FOR AS AR152410 UNCLASSIFIED EXCAVATION)
- (H) UNCLASSIFIED EXCAVATION (AR152410)
- NOT USED (1)
- EXISTING 18" RCP STORM SEWER (J)
- NEW PCC PAVEMENT SECTION OVER FUEL LINES: 9" PCC PAVEMENT (AR501509)
 9" CONTROLLED LOW STRENGTH MATERIAL (CLSM)(AR800035)
- (L) EXISTING 34"x53" RCEP STORM SEWER
- EXISTING ±9" CRUSHED AGGREGATE BASE COURSE TO REMAIN AS BASE. CONTRACTOR TO REGRADE AND RECOMPACT BASE AS REQUIRED. PAID FOR AS AGGREGATE BASE PREPARATION (AR209650)
- (N) 6" WIDE PCC PAVEMENT REMOVAL (FULL DEPTH 11" AVG.)(AR501900)
- NEW EMBANKMENT FILL (PAID AS PART OF AR401652)(SEE NOTES)
- EXISTING 2" AND 3" FIBERGLASS FUEL LINES AND FUEL DISPENSER P ELECTRICAL CABLE TO REMAIN UNDISTURBED (SEE FUEL NOTES)
- NEW DOWEL BAR
- R NEW TIE BAR
- (S) NEW 6" PERFORATED UNDERDRAIN, TRENCH ENVELOPE AND CA-7 BACKFILL (AR705506)(SEE DETAIL ON SHEET 14)
- T EXISTING 12" RCP
- U 2" BITUMINOUS PAVEMENT TO BE MILLED (AR401650) AND REPLACED (AR401610)
- NOT USED \mathbf{v}
- EXISTING GROUNDLINE (W)
- NEW GROUNDLINE
- 2' WIDE BITUMINOUS PAVEMENT REMUVAL AND THE COURSE AT BITUMINOUS SURFACE COURSE PLACED IN TWO-2" LIFTS WIDE BITUMINOUS PAVEMENT REMOVAL AND REPLACEMENT (AR401910)
- (Z) NEW TYPE A ISOLATION JOINT
- NEW PRIME COAT (AR602510)
- **BB** NEW TACK COAT (AR603510)
- (CQ) NEW UNDERCUT AREAS (AR152410) AS FIELD CONDITIONS NECESSITATE
- EXISTING PAVEMENT STRUCTURE: 9" PCC PAVEMENT 6" CRUSHED AGGREGATE BASE



KEY MAP

PHASE 2 BASE BID WORK LIMITS

CHICAGO EXECUTIVE AIRPORT EELING/PROSPECT HEIGHTS, ILLINOIS ILITATE EAST QUADRANT APRON, PHASI

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IL. CONTRACT: PA058 IL. LETTING ITEM: 11A IL. PROJECT: PWK-4427

S.B.G. PROJECT: 3-17-SBGP-XX

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FINAL

SHEET 9 OF 19 SHEETS

APRON SECTION B-B NOT TO SCALE

633.32

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360

24 SPACES @ 15'

15' (TYPICAL)

PHASE 1

WORK

COMPLETE

(N) 6" MAX.

15.5

PHASE 2, BASE BID

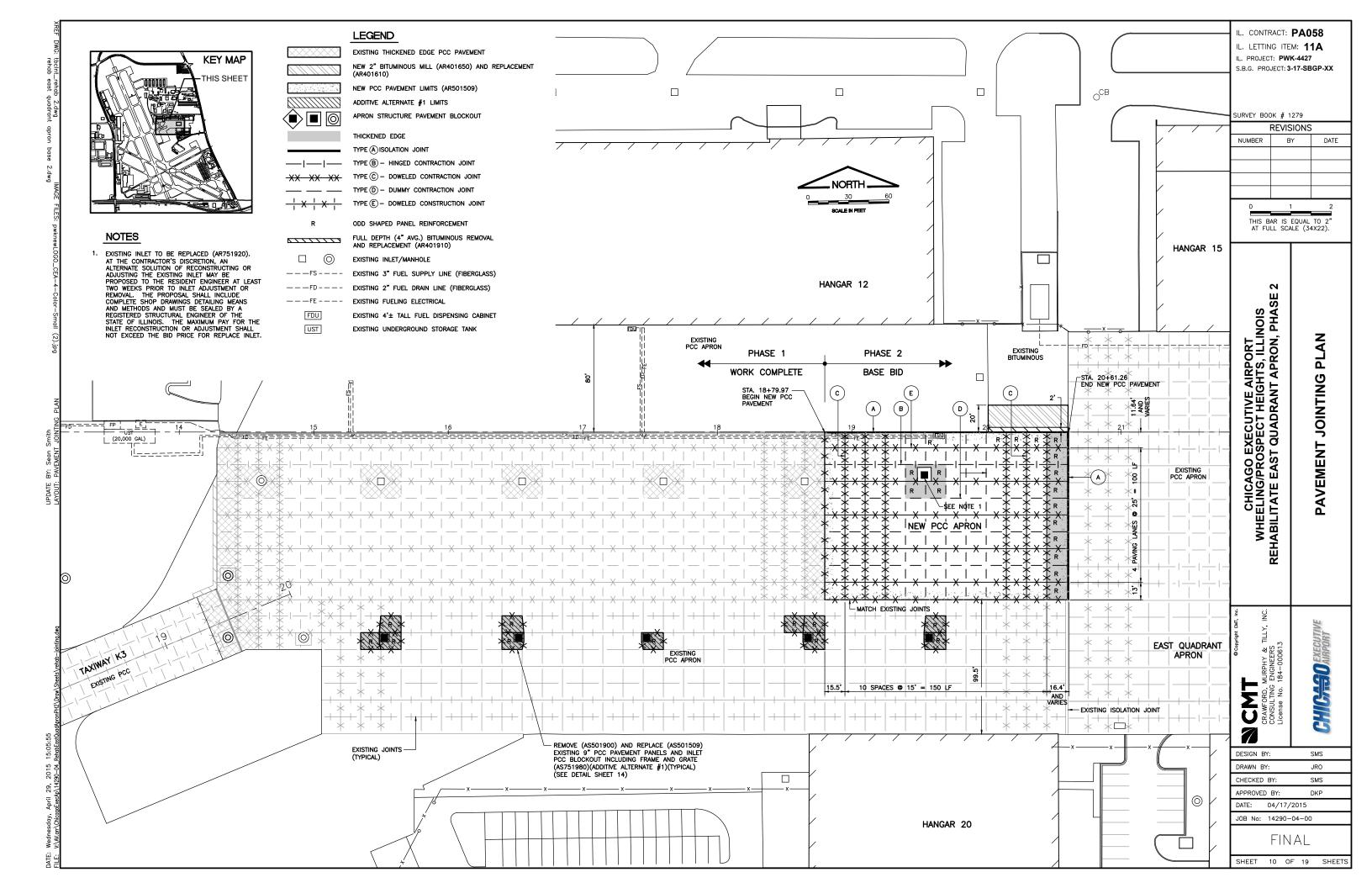
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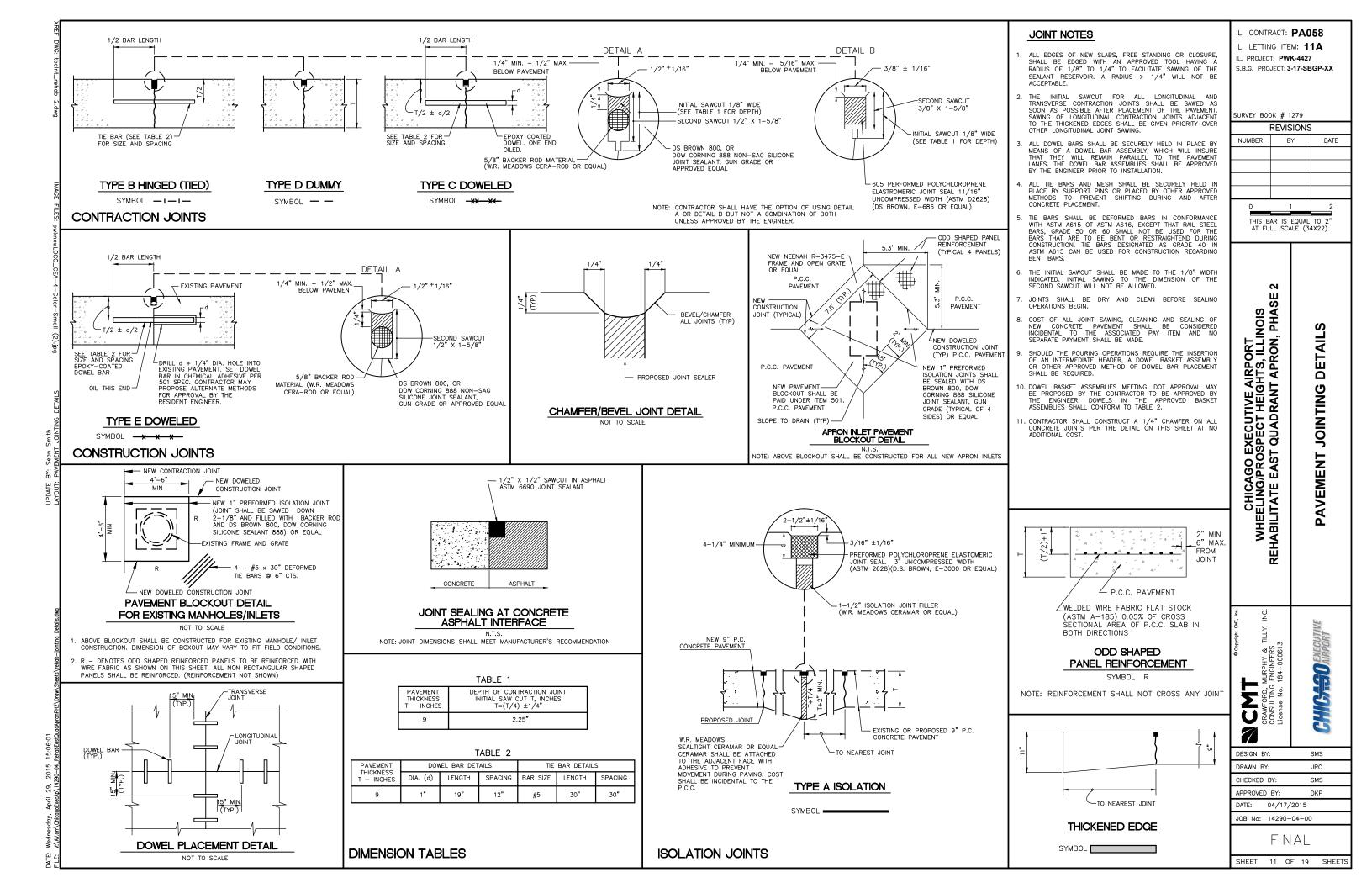
10 SPACES @ 15'

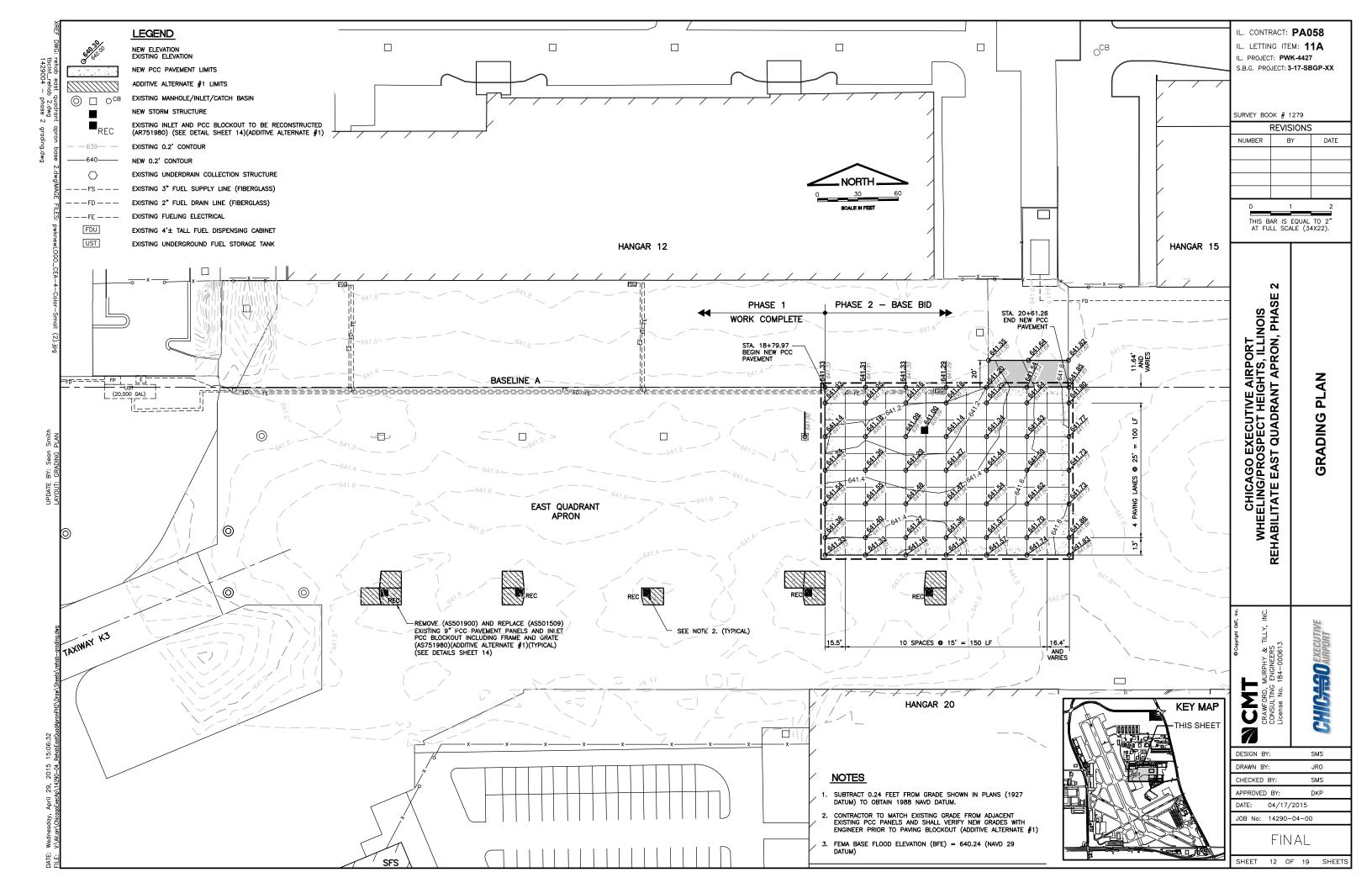
SEE GENERAL NOTE 4

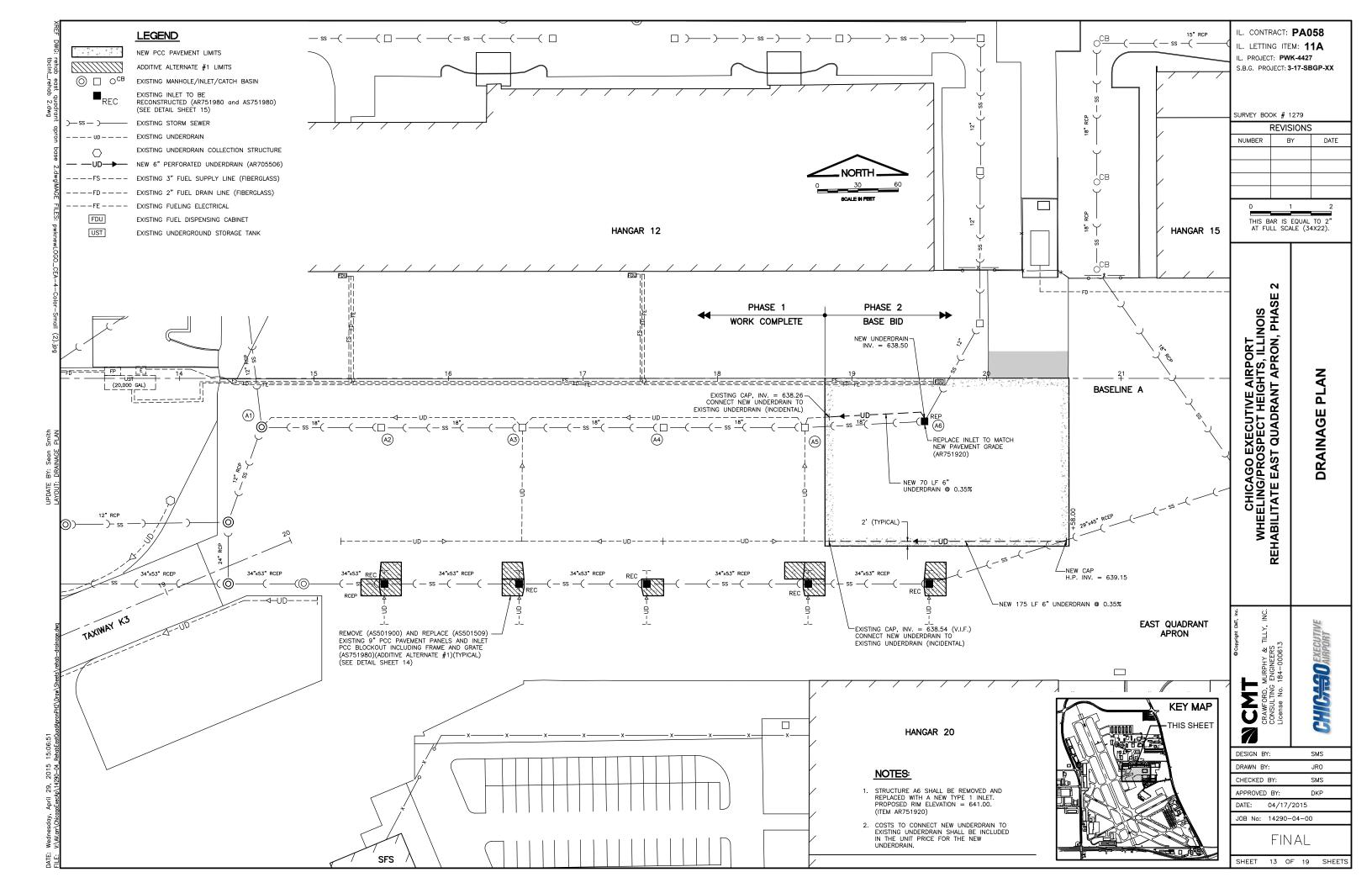
(G) (M)

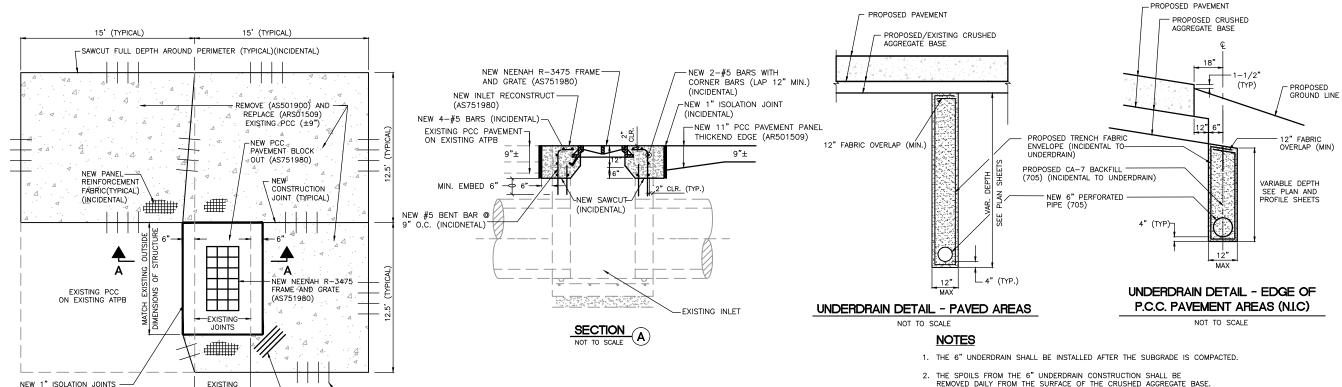
SEE GENERAL NOTE 5.











TIE BARS @ 6" CENTERS (INCIDENTAL) RECONSTRUCT INLET - TYPICAL (ADDITIVE ALTERNATE #1, ITEM AS751980)

NEW 19" DOWEL BAR 1"Ø @ 12" CENTERS

(TYP. ALL SIDES)(INCIDENTAL)

-NEW 4-#5x30" DEFORMED

4" EXTRA STRONG PIPE TUBING (ASTM A500)

PAINT YELLOW (2 COATS)

-EXISTING CONCRETE FUEL PAD

-3/8"× 10"× 10" (WELD TO 4" PIPE ALL AROUND)

CONCRETE ENCASEMENT

— 4" GRSC

— IDOT CLASS 610 CONCRETE ENCASEMENT

SEE PAVEMENT JOINTING PLAN FOR LOCATIONS

" DIA. GALVANIZED IRON BAR LADDER В < RUNGS 12" TO 15" O.C. TO BE INSTALLED WHEN Y IS GREATER THAN 5. COST TO BE INCLUDED IN THE CONTRACT UNIT PRICE FOR REPLACE INLET REINFORCING BAR SCHEDULE DIMENSIONS A B SIZE APPROX.WT.OF PER INLET A B 2 3'4" 2'4" #5 16.7 REINFORCING STEEL BARS TYPES NEENAH R-3475-E FRAME & GRATE OR APPROVED EQUAL В <-REINFORCING BAR TYPE "A' <u>PLAN</u> -REINFORCING BAR TYPE A 7 1/2" 3' PCC CONTOURED FLOWLINE L_{1 5/8"} - 6" GRANULAR BACKFILL LPIPE THICKNESS KEYWAY — SECTION A-A SECTION B-B

CONTRACTOR SHALL FIELD VERIFY EXISTING STORM SEWER/UNDERDRAIN INVERTS BEFORE INSTALLING PROPOSED PIPE, CONNECTIONS AND ORDERING MATERIALS

ALL UNDERDRAIN CONNECTIONS, CORING INTO STRUCTURES, CAPS, TEES, BENDS, STORM SEWER ETC. SHALL BE CONSIDERED INCLUDED IN THE COST OF THE

UNDERDRAIN SLOPES FOLLOW EDGE OF PAVEMENT SLOPE UNLESS OTHERWISE NOTED

STORM SEWER/UNDERDRAIN NOTES

INSTALL PROPOSED ELECTRICAL DUCTS/CONDUITS TO BE CLEAR OF UNDERDRAIN, COSTS INCLUDED.

UNDERDRAIN CONFLICTS WITH EXISTING CONDITIONS SHALL BE RESOLVED AND COST SHALL BE INCIDENTAL TO UNDERDRAIN.

PRIOR TO ORDERING AND INSTALLING ALL FIELD TILE REPLACEMENT PIPE, THE CONTRACTOR SHALL FIELD VERIFY THE LOCATION AND INVERTS OF EXISTING FIELD TILE CONNECTIONS. ADJUSTMENTS SHALL BE MADE AS NECESSARY AT NO ADDITIONAL COST TO THE CONTRACT.

CORING OF DRAINAGE STRUCTURE AND REMOVAL OF EXISTING STORM SEWER MANHOLE/INLET CONCRETE BENCHES TO FACILITATE CONNECTIONS OF PROPOSED STORM SEWER AND UNDERDRAIN PIPE SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PIPE.

STRUCTURE TYPE RIM INVERT STATION/OFFSET EXISTING 4' MH 640.93 IN 635.62 (E) STA. 14+61.12, 36.18' RT. R A IN 635.83 (E) A2 EX. TYPE 1 INLET 640.91 STA. 15+49.97, 36.64' RT. R OUT 635.83 (W IN 636.08 (E) EX. TYPE 1 INLET 640.96 STA. 16+54.97, 36.64' RT. B OUT 636.08 (W IN 636.28 (E) FX. TYPE 1 INLET 640.96 STA. 17+59.95, 36.64' RT, B OUT 636.28 (W IN 636.62 (F) A5 EX. TYPE 1 INLET 640.90 STA. 18+64.97, 36.64' RT. R A EX. TYPE 1 INLET SEE NOTE 3 OUT 636.78 (W 639.00± STA. 19+54.02, 31.96' RT. R A

STRUCTURE SCHEDULE

NOTE: 1. THE STATION AND OFFSET IS MEASURED TO THE CENTER OF THE STRUCTURE.

2. ALL ELEVATIONS ARE IN 1929 DATUM

STRUCTURE A6 TO BE REPLACED WITH A NEW TYPE 1 INLET (SEE DETAIL). RIM ELEVATION TO MATCH THE PROPOSED PCC APRON PAVEMENT AT ELEVATION 641.00±. SEE GRADING PLAN FOR EXACT ELEVATION. TO BE PAID UNDER ITEM AR751920.

IN 636.78 (NE)

4. FEMA BASE FLOOD ELEVATION (BFE) = 640.24 (NAVD 29 DATUM).

NOTES:

NEW 1" ISOLATION JOINTS

SHALL BE SEALED WITH DOW CORNING 888 SILICONE JOINT SEALANT

FILL W/CONCRETE AND ROUND OFF TOP

MAX

8" (TYP.) -►

NEW 1" PREFORMED —— ISOLATION JOINTS SHALL BE SEALED WITH DS

NON-SAG DOW CORNING 888 SILICONE JOINT

SEALANT, GUN GRADE OR

NEW PCC PAVEMENT ON -

CLSM OVER EXISTING FUEL LINES

BROWN 800 OR

EQUAL (TYPICAL)

(INCIDENTAL)

LOCATION OF NEW BOLLARDS SHALL BE COORDINATED WITH FUEL LINES, UTILITIES AND TAXILANE OBJECT FREE AREA TO AVOID ANY CONFLICTS.

BOLLARD DETAIL

NOT TO SCALE

FXISTING

OUTSIDE DIMENSIONS

PLAN

NOT TO SCALE

TYPE 1 INLET FOR REPLACE INLET (ITEM AR751920)

N.T.S.

1. 1/2" CHAMFER TO BE USED ON ALL EXPOSED CORNERS ON INLETS. BARS TO BE INSTALLED 2" FROM FACE OF WALL.

REPLACE INLET NOTES

2. INLET TO BE CONSTRUCTED OF STRUCTURAL P.C. CONCRETE. THE CONTRACT UNIT PRICE FOR REPLACE INLET SHALL INCLUDE THE GRATE AND FRAME, NEENAH R-3475 OR APPROVED EQUAL. 3. REPLACE INLET SHALL INCLUDE THE COMPLETE REMOVAL AND DISPOSAL OF EXISTING STRUCTURE.
PIPE DAMAGED DURING STRUCTURE REMOVAL SHALL BE REPAIRED OR REPLACED AT CONTRACTOR'S

IL.	CONTRACT: PA058	
11	LETTING ITEM: 11A	

LETTING ITEM: 11A IL. PROJECT: PWK-4427 S.B.G. PROJECT: 3-17-SBGP-XX

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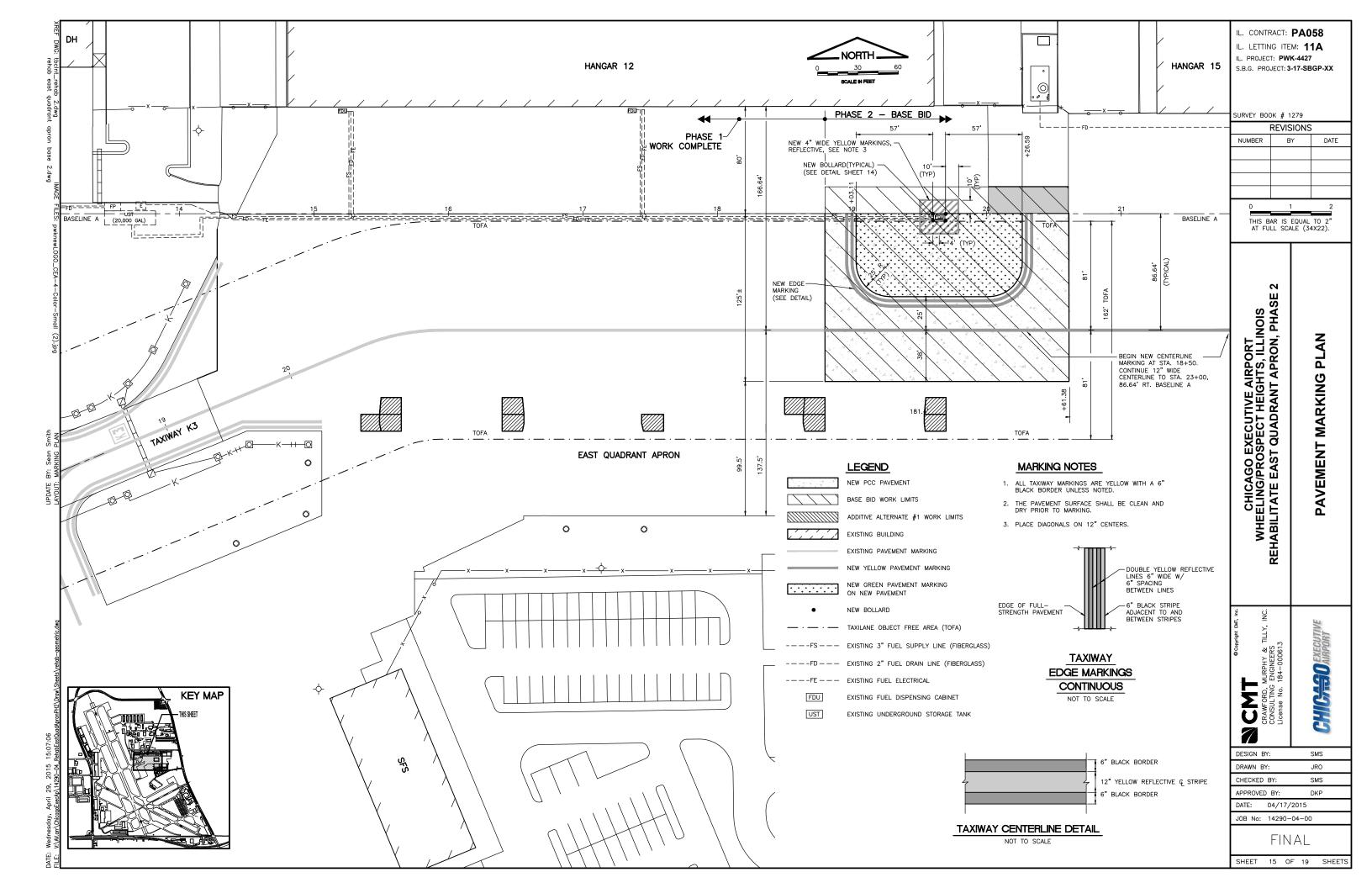
CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS REHABILITATE EAST QUADRANT APRON, PHASE S LLANEOU Ш MISC AND DRAINAGE

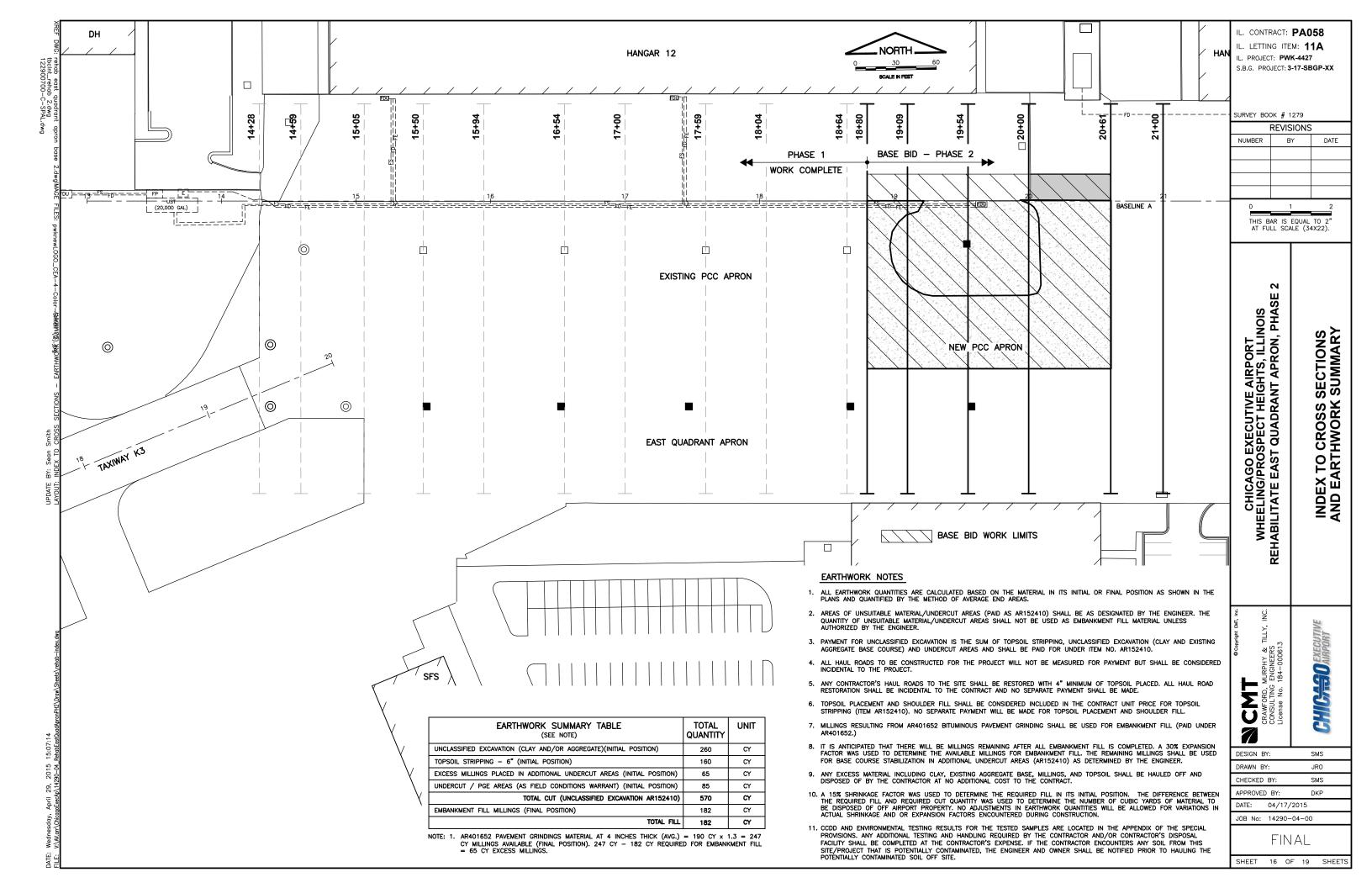
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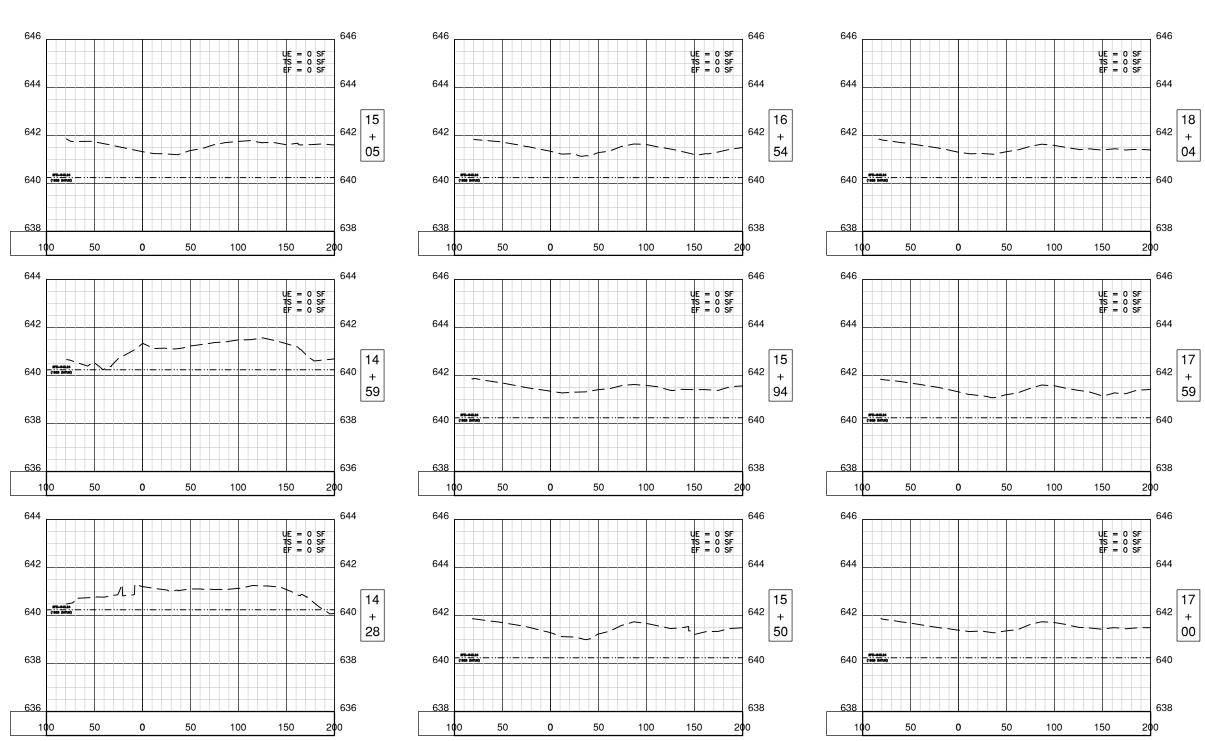
SHEET 14 OF 19 SHEETS





NOTES:

- 1. SUBTRACT 0.24 FEET FROM GRADES SHOWN IN PLANS (1929 DATUM) TO OBTAIN 1988 NAVD.
- 2. SEE GRADING PLAN FOR ELEVATIONS.
- CROSS SECTIONS SHOWN FOR STATION 14+28 THROUGH STATION 18+64 WERE COMPLETED DURING THE PHASE 1 PROJECT AND ARE SHOWN FOR INFORMATION ONLY.
- 4. SEE INDEX TO CROSS SECTIONS FOR EARTHWORK SUMMARY TABLE.
- 5. EXISTING AND NEW UTILITIES ARE NOT SHOWN FOR CLARITY. SEE EXISTING CONDITIONS AND REMOVALS FOR APPROXIMATE UTILITY LOCATIONS.
- 6. BFE = BASE FLOOD ELEVATION = 640.24 (1929 DATUM).



IL. CONTRACT: PA058

IL. LETTING ITEM: 11A IL. PROJECT: PWK-4427

S.B.G. PROJECT: 3-17-SBGP-XX

SURVEY BOOK # 1279

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NUMBER	BY	DATE

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS REHABILITATE EAST QUADRANT APRON, PHASE - SHEET SECTIONS

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NOTES:

- 1. SUBTRACT 0.24 FEET FROM GRADES SHOWN IN PLANS (1929 DATUM) TO OBTAIN 1988 NAVD.
- 2. SEE GRADING PLAN FOR ELEVATIONS.

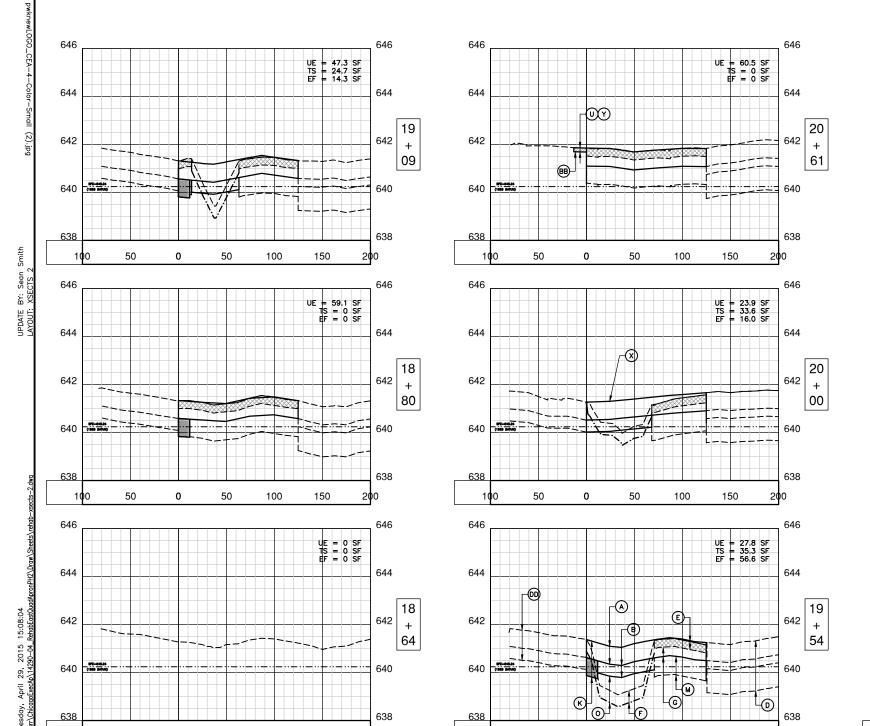
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LEGEND

- NEW 9" PCC PAVEMENT (AR501509)
- NEW 6" CRUSHED AGGREGATE BASE COURSE (AR209606)
- **D** EXISTING PAVEMENT STRUCTURE: 4" ASPHALT TREATED PERMEABLE BASE COURSE (ATPB) 12" LIME MODIFIED SUBGRADE
- E EXISTING ±4" BITUMINOUS PAVEMENT GRINDING (AR401652)(SEE NOTES)
- F) AVERAGE 6" TO 12" TOPSOIL STRIPPING (AR152410)
- EXISTING VARIABLE DEPTH CRUSHED AGGREGATE BASE COURSE TO BE REMOVED (PAID FOR AS AR152410 UNCLASSIFIED EXCAVATION) ©
- H UNCLASSIFIED EXCAVATION (AR152410)
- NEW PCC PAVEMENT SECTION OVER FUEL LINES: 9" PCC PAVEMENT (AR501509) 9" CONTROLLED LOW STRENGTH MATERIAL (CLSM)(AR800035)
- EXISTING ±9" CRUSHED AGGREGATE BASE COURSE TO REMAIN AS BASE. CONTRACTOR TO REGRADE AND RECOMPACT BASE AS REQUIRED. PAID FOR AS AGGREGATE BASE PREPARATION (AR209650)
- NEW EMBANKMENT FILL (PAID AS PART OF AR401652)(SEE NOTES)
- Ü 2" BITUMINOUS PAVEMENT TO BE MILLED (AR401650) AND REPLACED
- W EXISTING GROUNDLINE
- 2' WIDE BITUMINOUS PAVEMENT REMOVAL AND REPLACEMENT (AR401910) 2" BITUMINOUS SURFACE COURSE ON 2" BITUMINOUS BASE COURSE
- NEW TACK COAT (AR603510)
- **(D)** EXISTING PAVEMENT STRUCTURE: 9" PCC PAVEMENT 6" CRUSHED AGGREGATE BASE

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CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS REHABILITATE EAST QUADRANT APRON, PHAS SHEE. SECTIONS

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