

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

| | | | | |
|-----------|------------|--------|--------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030RS | DUPAGE | 44 | 1 |
| ILLINOIS | | | CONTRACT NO. 62A84 | |

D-91-330-15

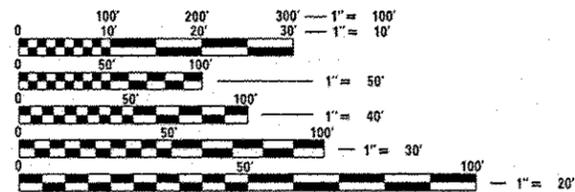


LOCATION OF SECTION INDICATED THUS: - [rectangle] -

VARIOUS ROUTES
SECTION: 2015-030RS
VARIOUS LOCATIONS IN DUPAGE COUNTY
INTERMITTENT RESURFACING
DUPAGE COUNTY
C-91-330-15

FOR GENERAL LOCATION MAP, SEE SHEET NO. 4

- THIS PROJECT IS LOCATED IN:
- THE VILLAGE OF ADDISON
 - THE CITY OF AURORA
 - THE VILLAGE OF BENSENVILLE
 - THE VILLAGE OF CLARENDON HILLS
 - THE VILLAGE OF DOWNERS GROVE
 - THE CITY OF ELMHURST
 - THE VILLAGE OF HANOVER PARK
 - THE VILLAGE OF HINSDALE
 - THE VILLAGE OF ITASCA
 - THE VILLAGE OF LOMBARD
 - THE VILLAGE OF OAK BROOK
 - THE CITY OF OAKBROOK TERRACE
 - THE VILLAGE OF ROSELLE
 - THE VILLAGE OF SCHAUMBURG
 - THE VILLAGE OF VILLA PARK
 - THE VILLAGE OF WESTMONT
 - THE CITY OF WOOD DALE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: DANIEL WILGREEN (847) 705-4240
PROJECT MANAGER: KEN ENG

CONTRACT NO. 62A84

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED April 8 2015
John F. Adams
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 18 2015
John D. Baranzoni, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

May 8 2015
Omor Osman, P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

| SHEET NO. | DESCRIPTION | STANDARD NO. | DESCRIPTION |
|-----------|--|--------------|--|
| 1 | COVER SHEET | 000001-00 | TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 2 | INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES | 701011-04 | OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY |
| 3 | SUMMARY OF QUANTITIES | 701301-04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS |
| 4 | GENERAL LOCATION MAP | 701306-03 | LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS - DAY ONLY |
| 5 | ROUTE INFORMATION | 701311-03 | LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY |
| 6 | SUMMARY OF INTERMITTENT RESURFACING SCHEDULE | 701336-06 | LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES |
| 7-35 | INTERMITTENT RESURFACING SCHEDULE | 701421-07 | LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS \geq 45 MPH TO 55 MPH |
| 36 | BUTT JOINT AND HMA TAPER DETAILS (BD-32) | 701426-07 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS \geq 45 MPH |
| 37 | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10) | 701427-03 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS \leq 40 MPH |
| 38 | TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11) | 701502-06 | URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE |
| 39 | DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13) | 701601-09 | URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN |
| 40 | TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14) | 701602-07 | URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE |
| 41 | PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16) | 701606-10 | URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN |
| 42 | ARTERIAL ROAD INFORMATION SIGN (TC-22) | 701701-09 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 43 | STANDARD TRAFFIC SIGNAL DESIGN DETAILS (TS-05, SHEET 2 OF 7) | 701901-04 | TRAFFIC CONTROL DEVICES |
| 44 | DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING (TS-07) | | |

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (OR ISTHA)

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL INTERMITTENT RESURFACING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT DON CHIARUCI, AREA TRAFFIC FIELD ENGINEER AT (847) 741-9857 MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

THE EXISTING ROADWAY TYPICAL SECTION IS ASSUMED TO HAVE A 3 INCH HOT-MIX ASPHALT OVERLAY ON TOP OF A TEN INCH CONCRETE BASE.

ALL INTERMITTENT RESURFACING LOCATIONS SHOWN IN THE PLANS ARE TWO (2) INCH MILL AND RESURFACE ONLY. THE MINIMUM WIDTH FOR INTERMITTENT RESURFACING SHALL BE THREE (3) FEET.

NO PATCHING OR RESURFACING IS TO BE DONE WITHIN FIFTY (50) FEET OF ANY RAILROAD CROSSING.

THE COST OF ANY PARTIAL OR FULL DEPTH PATCHING REQUIRED AFTER THE REMOVAL OF THE EXISTING 2 INCH HOT-MIX ASPHALT SURFACE SHALL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

ANY DETECTOR LOOPS DAMAGED BY MILLING SHALL BE REPLACED IN KIND. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO QUANTIFY LOOP REPLACEMENTS NEEDED AND PROVIDE THE RESIDENT ENGINEER THIS INFORMATION PRIOR TO GRINDING OR REMOVAL.

ALL LOOP DETECTOR LOCATIONS SHALL BE CURB MARKED BY THE CONTRACTOR PRIOR TO MILLING FOR THE PURPOSE OF REESTABLISHING DETECTOR LOOP LAYOUT AFTER THE RESURFACING IS COMPLETED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).

OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS. ANY MILLED PAVEMENT IS TO BE RESURFACED BY THE END OF EACH DAY AND OPEN TO TRAFFIC.

| HOT-MIX ASPHALT MIXTURE REQUIREMENTS | | QUALITY MANAGEMENT PROGRAM (QMP) |
|---|-----------------------------------|----------------------------------|
| MIXTURE TYPE | AIR VOIDS (%) @ N _{DES.} | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5MM), 2" | 4% @ TO GYR | QC/ QA |
| QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA) | | |

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

| | | | | | | | | | | | |
|-------------|-----------------------------|------------|------------|---|---|--------------------|---------------------|--------|--------------|---------------------------|--|
| FILE NAME : | USER NAME : barson | DESIGNED : | REVISOR : | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ... | DuPage.dgn | DRAWN : | REVISION : | | | VAR. | 2015-03ORS | DUPAGE | 44 | 2 | |
| ... | PLOT SCALE = 100.0000' / 1" | CHECKED : | REVISION : | | | CONTRACT NO. 62A84 | | | | | |
| ... | PLOT DATE = 4/7/2015 | DATE : | REVISION : | | | SCALE: | SHEET 1 OF 1 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT | |

URBAN

URBAN

| SUMMARY OF QUANTITIES | | | CONSTRUCTION TYPE CODE | | | | |
|-----------------------|--|--------|------------------------|-----------------|--|--|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | 100% STATE 0005 | | | |
| 40600275 | BITUMINOUS MATERIALS (PRIME COAT) | POUND | 15937 | 15937 | | | |
| 40600400 | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS | TON | 54 | 54 | | | |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 1063 | 1063 | | | |
| 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | TON | 3967 | 3967 | | | |
| 44000157 | HOT-MIX ASPHALT SURFACE REMOVAL, 2" | SQ YD | 35415 | 35415 | | | |
| 60300305 | FRAMES AND LIDS TO BE ADJUSTED | EACH | 10 | 10 | | | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 6 | 6 | | | |
| 67100100 | MOBILIZATION | LSUM | 1 | 1 | | | |
| 70300520 | PAVEMENT MARKING TAPE, TYPE III 4" | FOOT | 2676 | 2676 | | | |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 892 | 892 | | | |
| * 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 1038 | 1038 | | | |
| * 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 26112 | 26112 | | | |
| * 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 3322 | 3322 | | | |

| SUMMARY OF QUANTITIES | | | CONSTRUCTION TYPE CODE | | | | |
|-----------------------|---|-------|------------------------|-----------------|--|--|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | 100% STATE 0005 | | | |
| * 78000500 | THERMOPLASTIC PAVEMENT MARKING - LINE 8" | FOOT | 159 | 159 | | | |
| * 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 88 | 88 | | | |
| * 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 130 | 130 | | | |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 1112 | 1112 | | | |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 1112 | 1112 | | | |
| * 88600600 | DETECTOR LOOP REPLACEMENT | FOOT | 1405 | 1405 | | | |
| 20030850 | TEMPORARY INFORMATION SIGNING | SQ FT | 1208 | 1208 | | | |

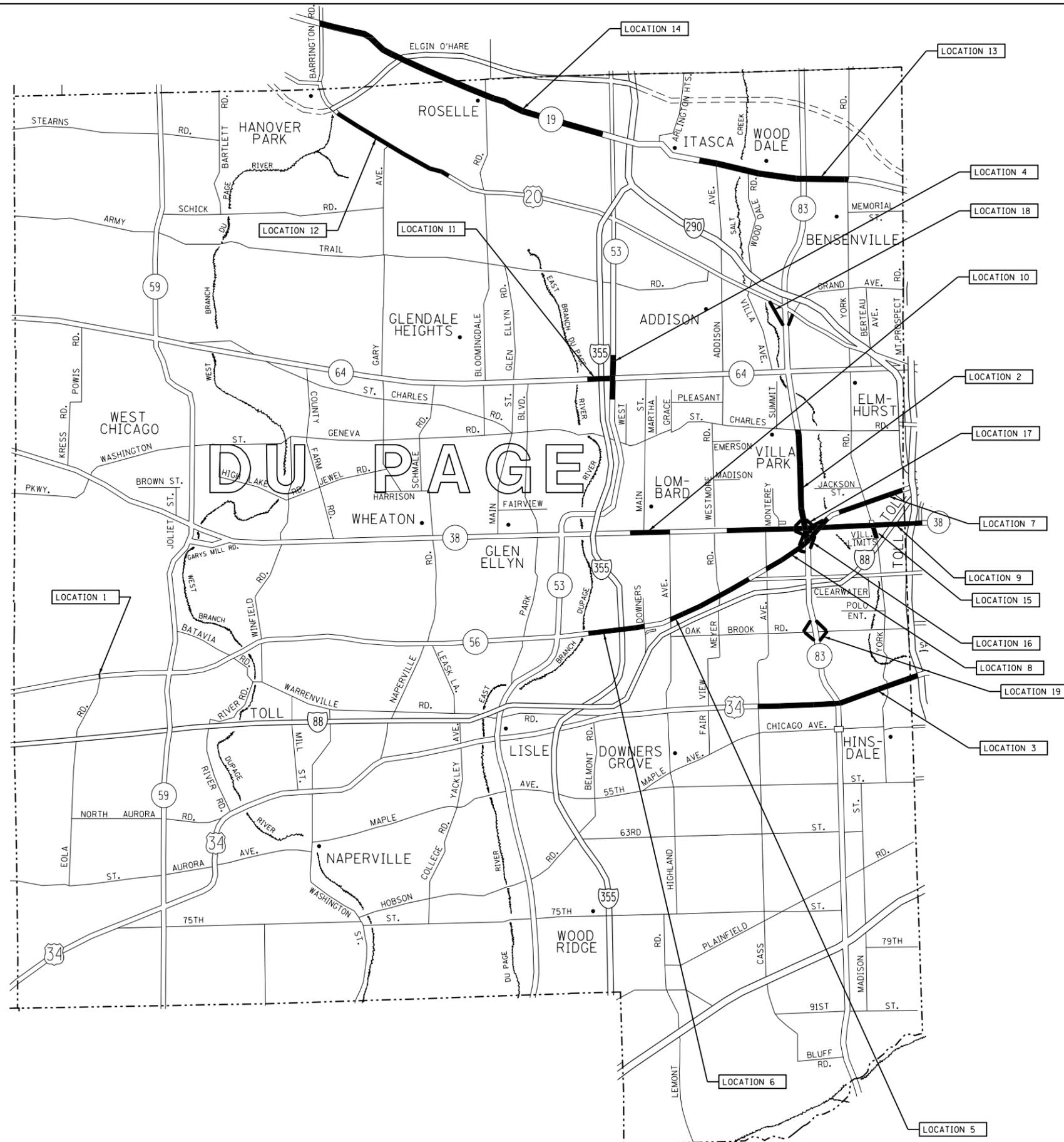
* SPECIALTY ITEM

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| | PLOT SCALE = 100.0000' / 1" | CHECKED - | REVISED - |
| DeFault | PLOT DATE = 4/7/2015 | DATE - 4/3/2015 | REVISED - |

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

| | | | | | | | |
|---------------------------|---------------------|--------------|----------------|--------------------|--------|--------------------|-------------|
| SCALE: | SHEET 1 OF 1 SHEETS | STA. TO STA. | F.A. RTE. VAR. | SECTION 2015-030RS | COUNTY | TOTAL SHEETS 44 | SHEET NO. 3 |
| | | | | | | CONTRACT NO. 62A84 | |
| ILLINOIS FED. AID PROJECT | | | | | | | |



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| | PLOT DATE = 4/7/2015 | | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | | |
|--|---------|-------------|--------------|
| GENERAL LOCATION MAP VARIOUS LOCATIONS IN DUPAGE COUNTY | | | |
| SCALE: NTS | SHEET 1 | OF 1 SHEETS | STA. TO STA. |

| | | | | |
|---------------------------|------------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DUPAGE | 44 | 4 |
| CONTRACT NO. 62A84 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | SUMMARY - DUPAGE COUNTY ARTERIAL ROUTES | CITIES/VILLAGES | TOWNSHIPS | SPEED LIMIT | EXISTING ADT (YEAR) |
|--------|---|---|-------------------------------------|-------------|---------------------|
| LOC.1 | WB IL 56 (500' WEST OF EOLA RD.) | AURORA | WINFIELD | 55 MPH | 17,500 (2013) |
| LOC.2 | IL 83 (IL 56 TO ST. CHARLES RD.) | ELMHURST, OAKBROOK TERRACE, VILLA PARK | YORK | 45-50 MPH | 71,800 (2013) |
| LOC.3 | US 34 (CASS AVE. TO I-294) | CLARENDON HILLS, HINSDALE, WESTMONT | DOWNERS GROVE, LYONS | 35 MPH | 35,600 (2013) |
| LOC.4 | IL 53 (PLEASANT LN. TO SIDNEY AVE.) | ADDISON, LOMBARD | ADDISON, BLOOMINGDALE, MILTON, YORK | 40 MPH | 21,300 (2013) |
| LOC.5 | IL 56 (22ND ST. TO HIGHLAND AVE.) | LOMBARD, OAK BROOK | YORK | 45 MPH | 46,800 (2013) |
| LOC.6 | IL 56 (DOWNERS DR. TO GRAY AVE.) | DOWNERS GROVE | MILTON, YORK | 45-50 MPH | 54,900 (2013) |
| LOC.7 | IL 56 (PROSPECT AVE. TO CALDWELL AVE.) | ELMHURST | YORK | 35 MPH | 17,500 (2013) |
| LOC.8 | IL 56 (SUMMIT AVE. TO COMMONWEALTH LN.) | ELMHURST, OAKBROOK TERRACE | YORK | 35-45 MPH | 22,600 (2013) |
| LOC.9 | IL 38 (WISCONSIN AVE. TO HARRISON ST.) | ELMHURST, OAK BROOK, OAKBROOK TERRACE, VILLA PARK | PROVISO, YORK | 35-55 MPH | 54,700 (2013) |
| LOC.10 | IL 38 (HIGHLAND AVE. TO FINLEY RD.) | LOMBARD | YORK | 35 MPH | 41,600 (2013) |
| LOC.11 | IL 64 (IL 53 TO SWIFT RD.) | LOMBARD | BLOOMINGDALE, MILTON | 45 MPH | 51,600 (2013) |
| LOC.12 | US 20 (SUMMERFIELD DR. TO GREENBROOK BLVD.) | HANOVER PARK, ROSELLE | BLOOMINGDALE | 40 MPH | 34,200 (2013) |
| LOC.13 | IL 19 (PROSPECT AVE. TO YORK RD.) | BENSENVILLE, WOOD DALE | ADDISON | 30-35 MPH | 26,200 (2013) |
| LOC.14 | IL 19 (IL 53 TO BARRINGTON RD.) | HANOVER PARK, ITASCA, ROSELLE, SCHAUMBURG | BLOOMINGDALE, SCHAUMBURG | 30-45 MPH | 34,600 (2013) |
| LOC.15 | YORK RD. (IL 38 TO NORTH OF I-88) | ELMHURST | YORK | 45 MPH | 14,300 (2008) |
| LOC.16 | IL 56 (RAMPS AT IL 83) | OAK BROOK, OAKBROOK TERRACE | YORK | 25-30 MPH | 3,400 (2012) |
| LOC.17 | IL 38 (RAMPS AT IL 83 AND WB IL 56) | ELMHURST, OAKBROOK TERRACE | YORK | 25-30 MPH | 2,850 (2012) |
| LOC.18 | US 20 (RAMPS AT IL 83) | ADDISON, ELMHURST | ADDISON | 25-45 MPH | 24,300 (2005) |
| LOC.19 | 31ST ST. (RAMPS AT IL 83) | OAK BROOK | YORK | N/A | 3,450 (2012) |

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| | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROUTE INFORMATION
VARIOUS LOCATIONS IN DUPAGE COUNTY**

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DUPAGE | 44 | 5 |
| | | | CONTRACT NO. 62A84 | |
| ILLINOIS FED. AID PROJECT | | | | |

| | SUMMARY - DUPAGE COUNTY ARTERIAL ROUTES | HMA 2" MILL & RESURFACE (SY) |
|--------|---|------------------------------------|
| LOC.1 | WB IL 56 (500' WEST OF EOLA RD.) | 67 |
| LOC.2 | IL 83 (IL 56 TO ST. CHARLES RD.) | 107 |
| LOC.3 | US 34 (CASS AVE. TO I-294) | 1,293 |
| LOC.4 | IL 53 (PLEASANT LN. TO SIDNEY AVE.) | 60 |
| LOC.5 | IL 56 (22ND ST. TO HIGHLAND AVE.) | 1,160 |
| LOC.6 | IL 56 (DOWNERS DR. TO GRAY AVE.) | 723 |
| LOC.7 | IL 56 (PROSPECT AVE. TO CALDWELL AVE.) | 957 |
| LOC.8 | IL 56 (SUMMIT AVE. TO COMMONWEALTH LN.) | 561 |
| LOC.9 | IL 38 (WISCONSIN AVE. TO HARRISON ST.) | 5,334 |
| LOC.10 | IL 38 (HIGHLAND AVE. TO FINLEY RD.) | 413 |
| LOC.11 | IL 64 (IL 53 TO SWIFT RD.) | 176 |
| LOC.12 | US 20 (SUMMERFIELD DR. TO GREENBROOK BLVD.) | 3,893 |
| LOC.13 | IL 19 (PROSPECT AVE. TO YORK RD.) | 8,714 |
| LOC.14 | IL 19 (IL 53 TO BARRINGTON RD.) | 7,873 |
| LOC.15 | YORK RD. (IL 38 TO NORTH OF I-88) | 293 |
| LOC.16 | IL 56 (RAMPS AT IL 83) | 1,171 |
| LOC.17 | IL 38 (RAMPS AT IL 83 AND WB IL 56) | 1,849 |
| LOC.18 | US 20 (RAMPS AT IL 83) | 435 |
| LOC.19 | 31ST ST. (RAMPS AT IL 83) | 336 |
| | DUPAGE COUNTY ARTERIAL TOTAL = | 35,415 SY |

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| | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF INTERMITTENT RESURFACING SCHEDULE
VARIOUS LOCATIONS IN DUPAGE COUNTY**

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------------|--------------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DUPAGE | 44 | 6 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 62A84 | |

ROUTE: IL 56 (500' West of Eola Road)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|------------------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| Eola Road | 500' West of Eola Road | WB | 1 | 12 | 50 | 600 | 67 |

TOTALS: **50 FT** **67 SY**

ROUTE: IL 83 (IL 56 to St. Charles Road)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|------------------|------------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| IL 56 | | NB | 1 | 12 | 5 | 60 | 7 |
| | | NB | 1 | 12 | 5 | 60 | 7 |
| | | NB | 1 | 12 | 3 | 36 | 4 |
| | | NB | 2 | 12 | 5 | 60 | 7 |
| | | NB | 2 | 12 | 5 | 60 | 7 |
| | | NB | 2 | 12 | 3 | 36 | 4 |
| | | NB | 3 | 12 | 5 | 60 | 7 |
| | | NB | 3 | 12 | 5 | 60 | 7 |
| | St. Charles Road | NB | 3 | 12 | 3 | 36 | 4 |
| St. Charles Road | | SB | 1 | 12 | 3 | 36 | 4 |
| | | SB | 1 | 12 | 3 | 36 | 4 |
| | | SB | 1 | 12 | 3 | 36 | 4 |
| | | SB | 1 | 12 | 3 | 36 | 4 |
| | | SB | 2 | 12 | 3 | 36 | 4 |
| | | SB | 2 | 12 | 3 | 36 | 4 |
| | | SB | 2 | 12 | 3 | 36 | 4 |
| | | SB | 2 | 12 | 3 | 36 | 4 |
| | | SB | 2 | 6 | 5 | 30 | 3 |
| | | SB | 2 | 6 | 5 | 30 | 3 |
| | | SB | 3 | 12 | 3 | 36 | 4 |
| | | SB | 3 | 12 | 3 | 36 | 4 |
| | | SB | 3 | 12 | 3 | 36 | 4 |
| | IL 56 | SB | 3 | 12 | 3 | 36 | 4 |

TOTALS: **85 FT** **107 SY**

ROUTE: US 34 (Cass Avenue to I-294)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|----|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| I-294 | | WB | LT | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 6 | 10 | 60 | 7 |
| | | WB | LT | 3 | 200 | 600 | 67 |

ROUTE: US 34 (Cass Avenue to I-294) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|-------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 3 | 100 | 300 | 33 |
| | | WB | 1 | 6 | 5 | 30 | 3 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 3 | 30 | 90 | 10 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 6 | 5 | 30 | 3 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 3 | 50 | 150 | 17 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 3 | 100 | 300 | 33 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | CL | 3 | 50 | 150 | 17 |
| | | WB | 1 | 3 | 100 | 300 | 33 |
| | | WB | LT | 3 | 100 | 300 | 33 |
| | | WB | CL | 6 | 30 | 180 | 20 |
| | | WB | CL | 6 | 50 | 300 | 33 |
| | | WB | 1 | 6 | 30 | 180 | 20 |
| | | WB | LT | 3 | 50 | 150 | 17 |
| | | WB | 1 | 3 | 40 | 120 | 13 |
| | | WB | CL | 6 | 20 | 120 | 13 |
| | | WB | 1 | 6 | 20 | 120 | 13 |
| | | WB | CL | 12 | 20 | 240 | 27 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | CL | 3 | 20 | 60 | 7 |
| I-294 | Cass Avenue | WB | CL | 3 | 20 | 60 | 7 |
| | | WB | 2 | 3 | 10 | 30 | 3 |
| | | WB | 2 | 6 | 20 | 120 | 13 |
| | | WB | 2 | 6 | 20 | 120 | 13 |
| | | WB | 2 | 3 | 10 | 30 | 3 |
| | | WB | 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 6 | 10 | 60 | 7 |
| | | WB | 2 | 6 | 5 | 30 | 3 |
| | | WB | 2 | 6 | 10 | 60 | 7 |
| | | WB | 2 | 3 | 10 | 30 | 3 |
| | | WB | 2 | 6 | 10 | 60 | 7 |
| | | WB | 2 | 6 | 10 | 60 | 7 |
| | | WB | 2 | 6 | 10 | 60 | 7 |
| | | WB | 2 | 6 | 10 | 60 | 7 |
| | | WB | 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 3 | 30 | 90 | 10 |
| | | WB | 2 | 3 | 10 | 30 | 3 |
| | | WB | 2 | 6 | 10 | 60 | 7 |
| | | WB | 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 3 | 10 | 30 | 3 |
| | | WB | 2 | 6 | 10 | 60 | 7 |
| | | WB | 2 | 12 | 10 | 120 | 13 |
| Cass Avenue | Cass Avenue | WB | 2 | 3 | 10 | 30 | 3 |
| | | EB | LT | 3 | 50 | 150 | 17 |
| | | EB | LT | 3 | 50 | 150 | 17 |
| | | EB | LT | 3 | 50 | 150 | 17 |

CONTINUED ON NEXT SHEET

ROUTE: US 34 (Cass Avenue to I-294) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|-------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | LT | 3 | 200 | 600 | 67 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 6 | 10 | 60 | 7 |
| | | EB | 1 | 6 | 5 | 30 | 3 |
| | | EB | LT | 3 | 100 | 300 | 33 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 6 | 10 | 60 | 7 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | | EB | 1 | 6 | 20 | 120 | 13 |
| | | EB | 1 | 3 | 200 | 600 | 67 |
| | | EB | 1 | 6 | 40 | 240 | 27 |
| | | EB | 1 | 6 | 10 | 60 | 7 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 6 | 10 | 60 | 7 |
| | | EB | 1 | 12 | 20 | 240 | 27 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | I-294 | EB | 1 | 3 | 20 | 60 | 7 |
| Cass Avenue | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 6 | 10 | 60 | 7 |
| | | EB | 2 | 6 | 10 | 60 | 7 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 6 | 10 | 60 | 7 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 6 | 10 | 60 | 7 |
| | | EB | 2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 6 | 10 | 60 | 7 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 10 | 30 | 3 |
| | | EB | 2 | 6 | 20 | 120 | 13 |
| | | EB | 2 | 3 | 30 | 90 | 10 |
| | I-294 | EB | 2 | 6 | 20 | 120 | 13 |

TOTALS: 2880 FT 1293 SY

ROUTE: IL 53 (Pleasant Lane to Sidney Avenue)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|---------------|---------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| Sidney Avenue | | SB | 1 | 3 | 30 | 90 | 10 |
| | | SB | 1 | 3 | 30 | 90 | 10 |
| | Pleasant Lane | SB | 1 | 3 | 10 | 30 | 3 |
| Pleasant Lane | | NB | 1 | 3 | 20 | 60 | 7 |
| | | NB | 1 | 6 | 20 | 120 | 13 |
| | | NB | RT | 12 | 5 | 60 | 7 |
| | | NB | RT | 12 | 5 | 60 | 7 |
| | Sidney Avenue | NB | 1 | 3 | 10 | 30 | 3 |

TOTALS: 130 FT 60 SY

ROUTE: IL 56 (22nd Street to Highland Avenue)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| 22nd Street | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | LT | 3 | 40 | 120 | 13 |
| | | WB | LT | 3 | 20 | 60 | 7 |
| | | WB | 1 | 3 | 30 | 90 | 10 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 3 | 100 | 300 | 33 |
| | | WB | 1 | 3 | 100 | 300 | 33 |
| | | WB | LT | 3 | 200 | 600 | 67 |
| | | WB | 1 | 3 | 30 | 90 | 10 |
| | | WB | 1 | 6 | 10 | 60 | 7 |
| | Highland Avenue | WB | 1 | 6 | 10 | 60 | 7 |
| 22nd Street | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 3 | 100 | 300 | 33 |
| | | WB | 2 | 3 | 100 | 300 | 33 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 3 | 100 | 300 | 33 |
| | | WB | 2 | 3 | 100 | 300 | 33 |
| | | WB | 2 | 3 | 100 | 300 | 33 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 3 | 30 | 90 | 10 |
| | | WB | 2 | 3 | 100 | 300 | 33 |
| | Highland Avenue | WB | 2 | 3 | 20 | 60 | 7 |

CONTINUED ON NEXT SHEET

ROUTE: IL 56 (Prospect Avenue to Caldwell Avenue)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-----------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| Caldwell Avenue | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 3 | 40 | 120 | 13 |
| | | WB | 1 | 3 | 100 | 300 | 33 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 3 | 100 | 300 | 33 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | LT1 | 3 | 50 | 150 | 17 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 3 | 40 | 120 | 13 |
| | | WB | 1 | 3 | 200 | 600 | 67 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | CL | 12 | 20 | 240 | 27 |
| | | WB | 1 | 3 | 30 | 90 | 10 |
| | | WB | 1 | 3 | 40 | 120 | 13 |
| | Prospect Avenue | WB | 1 | 6 | 30 | 180 | 20 |
| Caldwell Avenue | | WB | 2 | 3 | 30 | 90 | 10 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 20 | 240 | 27 |
| | | WB | RT | 3 | 40 | 120 | 13 |
| | | WB | 2 | 12 | 30 | 360 | 40 |
| | | WB | 2 | 12 | 30 | 360 | 40 |
| | Prospect Avenue | WB | 2 | 12 | 20 | 240 | 27 |
| Prospect Avenue | | EB | 1 | 3 | 100 | 300 | 33 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 3 | 50 | 150 | 17 |
| | | EB | LT1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 3 | 100 | 300 | 33 |
| | | EB | 1 | 3 | 100 | 300 | 33 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | | EB | 1 | 12 | 20 | 240 | 27 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 3 | 50 | 150 | 17 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 12 | 30 | 360 | 40 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | Caldwell Avenue | EB | 1 | 3 | 30 | 90 | 10 |
| Prospect Avenue | | EB | 2 | 12 | 20 | 240 | 27 |
| | | EB | 2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 6 | 20 | 120 | 13 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | RT | 3 | 50 | 150 | 17 |
| | | EB | 2 | 3 | 50 | 150 | 17 |
| | | EB | 2 | 3 | 20 | 60 | 7 |

ROUTE: IL 56 (Prospect Avenue to Caldwell Avenue) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 2 | 6 | 20 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | Caldwell Avenue | EB | 2 | 3 | 30 | 90 | 10 |

TOTALS: 1810 FT 957 SY

ROUTE: IL 56 (Summit Avenue to Commonwealth Lane)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-------------------|---------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| Commonwealth Lane | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 40 | 480 | 53 |
| | | WB | 2 | 12 | 40 | 480 | 53 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 2 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| Summit Avenue | Summit Avenue | WB | 3 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 6 | 6 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |

CONTINUED ON NEXT SHEET

ROUTE: IL 56 (Summit Avenue to Commonwealth Lane) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|-------------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 20 | 240 | 27 |
| | | EB | 2 | 12 | 20 | 240 | 27 |
| | | EB | 3 | 12 | 20 | 240 | 27 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 24 | 288 | 32 |
| | Commonwealth Lane | EB | 1 | 12 | 4 | 48 | 5 |

TOTALS: 424 FT 561 SY

ROUTE: IL 38 (Wisconsin Avenue to Harrison Street)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-----------------|-------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| Harrison Street | | WB | 1 | 24 | 100 | 2400 | 267 |
| | | WB | 2 | 24 | 100 | 2400 | 267 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | Ramp | 18 | 6 | 108 | 12 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | Ramp | 12 | 6 | 72 | 8 |
| | | WB | Ramp | 18 | 5 | 90 | 10 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| I-294 | I-294 | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 8 | 96 | 11 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 3 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 8 | 96 | 11 |
| | | WB | 2 | 12 | 8 | 96 | 11 |
| | | WB | 3 | 12 | 8 | 96 | 11 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 12 | 144 | 16 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 12 | 144 | 16 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 6 | 72 | 8 |

ROUTE: IL 38 (Wisconsin Avenue to Harrison Street) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|----|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 8 | 96 | 11 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 12 | 144 | 16 |
| | | WB | 3 | 12 | 12 | 144 | 16 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | Ramp | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 12 | 144 | 16 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 12 | 144 | 16 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 2 | 12 | 10 | 120 | 13 |
| | | WB | Ramp | 18 | 10 | 180 | 20 |
| | | WB | Ramp | 18 | 10 | 180 | 20 |
| | | WB | 3 | 12 | 12 | 144 | 16 |
| | | WB | Ramp | 18 | 6 | 108 | 12 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | Ramp | 18 | 5 | 90 | 10 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 10 | 120 | 13 |
| | | WB | 3 | 12 | 6 | 72 | 8 |
| | | WB | Ramp | 18 | 5 | 90 | 10 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | Ramp | 18 | 5 | 90 | 10 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 3 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |

CONTINUED ON NEXT SHEET

| | | | | | |
|---|------------------------------|------------|----------|-----------|--|
| FILE NAME = | USER NAME = bartonw | DESIGNED - | RWB | REVISED - | |
| es:\pw\work\p\midot\bar tonw\10427922\HMA-DupPage.dgn | | DRAWN - | RWB | REVISED - | |
| Default | PLOT SCALE = 100.0000' / in. | CHECKED - | | REVISED - | |
| | PLOT DATE = 4/7/2015 | DATE - | 4/6/2015 | REVISED - | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INTERMITTENT RESURFACING SCHEDULE
IL 56 / IL 38**

SCALE: SHEET 5 OF 29 SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DUPAGE | 44 | 11 |
| | | | CONTRACT NO. 62A84 | |
| ILLINOIS FED. AID PROJECT | | | | |

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|----|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 2 | 18 | 5 | 90 | 10 |
| | | EB | 1 | 18 | 40 | 720 | 80 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | Joint | 3 | 200 | 600 | 67 |
| | | EB | Ramp | 18 | 5 | 90 | 10 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 20 | 240 | 27 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 20 | 240 | 27 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | Joint | 3 | 50 | 150 | 17 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | Joint | 3 | 30 | 90 | 10 |
| | | EB | Joint | 3 | 200 | 600 | 67 |
| | | EB | 1 | 12 | 15 | 180 | 20 |
| | | EB | 2 | 12 | 20 | 240 | 27 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | Joint | 3 | 100 | 300 | 33 |
| | | EB | Joint | 3 | 30 | 90 | 10 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | Joint | 3 | 20 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | Ramp | 18 | 10 | 180 | 20 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|----|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | Joint | 3 | 100 | 300 | 33 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | Ramp | 18 | 5 | 90 | 10 |
| | | EB | 3 | 12 | 10 | 120 | 13 |

CONTINUED ON NEXT SHEET

ROUTE: IL 38 (Wisconsin Avenue to Harrison Street) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | Ramp | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | LT1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | Harrison Street | EB | 3 | 12 | 10 | 120 | 13 |

TOTALS: 4943 FT 5334 SY

ROUTE: IL 38 (Highland Avenue to Finley Road)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-----------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| Highland Avenue | | WB | Joint | 3 | 100 | 300 | 33 |
| | | WB | Joint | 3 | 200 | 600 | 67 |
| | | WB | 1 | 12 | 60 | 720 | 80 |
| | | WB | 2 | 12 | 60 | 720 | 80 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| Finley Road | Finley Road | WB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | Highland Avenue | EB | RT | 12 | 60 | 720 | 80 |

TOTALS: 535 FT 413 SY

ROUTE: IL 64 (IL 53 to Swift Road)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| IL 53 | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 3 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 12 | 144 | 16 |
| | | WB | 2 | 12 | 12 | 144 | 16 |
| | Swift Road | WB | 3 | 12 | 12 | 144 | 16 |
| Swift Road | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 3 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 3 | 12 | 6 | 72 | 8 |
| | | EB | 4 | 12 | 6 | 72 | 8 |
| | | EB | LT1 | 12 | 24 | 288 | 32 |
| | IL 53 | EB | LT2 | 12 | 24 | 288 | 32 |

TOTALS: 132 FT 176 SY

ROUTE: US 20 (Summerfield Drive to Greenbrook Boulevard)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|----------------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| Greenbrook Boulevard | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 6 | 12 | 72 | 8 |
| | | EB | 2 | 12 | 13 | 156 | 17 |
| | | EB | 1 | 16 | 14 | 224 | 25 |
| | | EB | 1 | 20 | 6 | 120 | 13 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | LT | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 35 | 420 | 47 |
| | | EB | RT | 12 | 10 | 120 | 13 |
| | | EB | 2, RT | 3 | 40 | 120 | 13 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | Bartels Road | EB | RT | 12 | 3 | 36 | 4 |
| Bartels Road | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | INT | 4 | 20 | 80 | 9 |
| | | EB | EOP | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 50 | 600 | 67 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 35 | 420 | 47 |
| | | EB | 1 | 10 | 100 | 1000 | 111 |
| | | EB | 2 | 10 | 6 | 60 | 7 |
| | | EB | 2 | 10 | 3 | 30 | 3 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 3 | 13 | 39 | 4 |
| | | EB | 1 | 12 | 70 | 840 | 93 |
| | | EB | 2 | 12 | 70 | 840 | 93 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 90 | 1080 | 120 |
| | | EB | 1 | 12 | 90 | 1080 | 120 |
| | | EB | 1 | 10 | 60 | 600 | 67 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 20 | 3 | 60 | 7 |
| | | EB | LT | 12 | 10 | 120 | 13 |
| | | EB | 2 | 3 | 40 | 120 | 13 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 200 | 2400 | 267 |
| | | EB | 1 | 6 | 10 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 6 | 30 | 180 | 20 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | Cloverdale Road | EB | 1 | 12 | 3 | 36 | 4 |
| Cloverdale Road | | EB | 2 | 12 | 20 | 240 | 27 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 6 | 3 | 18 | 2 |

ROUTE: US 20 (Summerfield Drive to Greenbrook Boulevard) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|---------------|---------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 1 | 6 | 20 | 120 | 13 |
| | | EB | 1 | 6 | 3 | 18 | 2 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 60 | 720 | 80 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 40 | 480 | 53 |
| | | EB | 2 | 12 | 60 | 720 | 80 |
| | Thorn Road | EB | 2 | 12 | 6 | 72 | 8 |
| Thorn Road | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | EOP | 3 | 80 | 240 | 27 |
| | | EB | 2 | 3 | 10 | 30 | 3 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 3 | 10 | 30 | 3 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | RT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 6 | 6 | 36 | 4 |
| | | EB | 1 | 6 | 6 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | RT | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | Gary Avenue | EB | 2 | 12 | 10 | 120 | 13 |
| Gary Avenue | | EB | EOP | 3 | 60 | 180 | 20 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | RT | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | Gary Avenue | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | EOP | 3 | 60 | 180 | 20 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 8 | 96 | 11 |
| | Virginia Road | EB | 2 | 12 | 8 | 96 | 11 |
| Virginia Road | | EB | 1 | 12 | 35 | 420 | 47 |
| | | EB | 1 | 12 | 14 | 168 | 19 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | Keeney Road | EB | Median | 12 | 6 | 72 | 8 |
| Keeney Road | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 20 | 3 | 60 | 7 |
| | | EB | 2 | 12 | 13 | 156 | 17 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 20 | 6 | 120 | 13 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 20 | 3 | 60 | 7 |
| | | EB | LT | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |

CONTINUED ON NEXT SHEET

| | | | |
|---|------------------------------|-----------------|-----------|
| FILE NAME = | USER NAME = bartonw | DESIGNED - RWB | REVISED - |
| et:\pw\work\p\id\bar tonw\0427922\HMA-DupPage.dgn | | DRAWN - RWB | REVISED - |
| Default | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INTERMITTENT RESURFACING SCHEDULE
US 20**

SCALE: SHEET 10 OF 29 SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DUPAGE | 44 | 16 |
| | | | CONTRACT NO. 62A84 | |
| ILLINOIS FED. AID PROJECT | | | | |

ROUTE: US 20 (Summerfield Drive to Greenbrook Boulevard) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|------------------|------------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | Bryn Mawr Avenue | EB | 2 | 12 | 3 | 36 | 4 |
| Bryn Mawr Avenue | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | EOP | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | 2 | 12 | 9 | 108 | 12 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 20 | 3 | 60 | 7 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 30 | 360 | 40 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 6 | 65 | 390 | 43 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Papworth Street | EB | EOP | 3 | 80 | 240 | 27 |
| Papworth Street | | EB | EOP | 3 | 90 | 270 | 30 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 8 | 96 | 11 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 15 | 180 | 20 |
| | | EB | 2 | 2 | 3 | 6 | 1 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Wheaton Road | EB | 1 | 6 | 30 | 180 | 20 |
| Wheaton Road | | EB | 1 | 3 | 10 | 30 | 3 |
| | | EB | 1 | 12 | 25 | 300 | 33 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 35 | 105 | 12 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 3 | 10 | 30 | 3 |
| | | EB | 1, 2 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 10 | 30 | 3 |
| | | EB | 1 | 12 | 3 | 36 | 4 |

ROUTE: US 20 (Summerfield Drive to Greenbrook Boulevard) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|----------------|----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 52 | 156 | 17 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | EOP | 3 | 27 | 81 | 9 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 20 | 3 | 60 | 7 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | Rodenburg Road | EB | LT | 20 | 3 | 60 | 7 |
| Rodenburg Road | | EB | 1, 2 | 3 | 80 | 240 | 27 |
| | | EB | 2 | 6 | 12 | 72 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 70 | 210 | 23 |
| | | EB | EOP | 3 | 70 | 210 | 23 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 20 | 240 | 27 |
| | | EB | EOP | 6 | 65 | 390 | 43 |
| | | EB | EOP | 3 | 60 | 180 | 20 |
| | | EB | 1, 2 | 3 | 80 | 240 | 27 |
| | | EB | 1, 2 | 3 | 35 | 105 | 12 |
| | | EB | 1, 2 | 3 | 30 | 90 | 10 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | Garden Avenue | EB | 2 | 12 | 6 | 72 | 8 |
| Garden Avenue | | EB | EOP | 3 | 70 | 210 | 23 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 40 | 120 | 13 |
| | | EB | 1, Median | 3 | 40 | 120 | 13 |
| | | EB | 2 | 6 | 20 | 120 | 13 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | EOP | 3 | 140 | 420 | 47 |
| | | EB | 1 | 12 | 8 | 96 | 11 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 15 | 3 | 45 | 5 |
| | | EB | 2 | 12 | 3 | 36 | 4 |

CONTINUED ON NEXT SHEET

| | | | | | |
|---|------------------------------|------------|----------|-----------|--|
| FILE NAME = | USER NAME = bartonw | DESIGNED - | RWB | REVISED - | |
| et:\pw\work\p\id\bar tonw\id0427922\HMA-DupPage.dgn | | DRAWN - | RWB | REVISED - | |
| Default | | CHECKED - | | REVISED - | |
| | PLOT SCALE = 100.0000' / in. | DATE - | 4/6/2015 | REVISED - | |
| | PLOT DATE = 4/7/2015 | | | | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INTERMITTENT RESURFACING SCHEDULE
US 20**

SCALE: SHEET 11 OF 29 SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DUPAGE | 44 | 17 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 62A84 | |

ROUTE: US 20 (Summerfield Drive to Greenbrook Boulevard) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-------------------|----------------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 1, 2 | 3 | 17 | 51 | 6 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | EOP | 3 | 80 | 240 | 27 |
| | | EB | 2 | 6 | 65 | 390 | 43 |
| | | EB | 2 | 10 | 6 | 60 | 7 |
| | | EB | Median | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Summerfield Drive | EB | 2 | 12 | 3 | 36 | 4 |
| Summerfield Drive | | WB | 1, 2 | 3 | 130 | 390 | 43 |
| | | WB | 1 | 12 | 40 | 480 | 53 |
| | | WB | 2 | 12 | 50 | 600 | 67 |
| | | WB | 1 | 15 | 110 | 1650 | 183 |
| | | WB | 1, 2 | 3 | 30 | 90 | 10 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1, 2 | 3 | 15 | 45 | 5 |
| | | WB | 1, 2 | 3 | 130 | 390 | 43 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 20 | 240 | 27 |
| | | WB | 1 | 6 | 3 | 18 | 2 |
| | | WB | 1 | 6 | 3 | 18 | 2 |
| | | WB | 1, 2 | 3 | 35 | 105 | 12 |
| | | WB | 2 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | Garden Avenue | WB | 1, 2 | 3 | 25 | 75 | 8 |
| Garden Avenue | | WB | 1, 2 | 3 | 120 | 360 | 40 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | Greenbrook Boulevard | WB | 1, 2 | 3 | 50 | 150 | 17 |

TOTALS: 4584 FT 3893 SY

ROUTE: IL 19 (Prospect Avenue to York Road)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|----|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| York Road | | WB | 2 | 22 | 15 | 330 | 37 |
| | | WB | 2 | 5 | 5 | 25 | 3 |
| | | WB | 1, 2 | 3 | 12 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |

ROUTE: IL 19 (Prospect Avenue to York Road) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|----|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | WB | 2 | 3 | 15 | 45 | 5 |
| | | WB | 1, 2 | 3 | 5 | 15 | 2 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 57 | 171 | 19 |
| | | WB | 2 | 3 | 27 | 81 | 9 |
| | | WB | 1, Median | 3 | 90 | 270 | 30 |
| | | WB | 2 | 3 | 8 | 24 | 3 |
| | | WB | 1, 2 | 3 | 110 | 330 | 37 |
| | | WB | 1, Median | 3 | 25 | 75 | 8 |
| | | WB | 1 | 15 | 3 | 45 | 5 |
| | | WB | 1, Median | 15 | 3 | 45 | 5 |
| | | WB | 1, 2 | 3 | 35 | 105 | 12 |
| | | WB | EOP | 3 | 25 | 75 | 8 |
| | | WB | EOP | 3 | 10 | 30 | 3 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | EOP | 3 | 30 | 90 | 10 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1, 2 | 3 | 10 | 30 | 3 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 10 | 28 | 280 | 31 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 2 | 5 | 5 | 25 | 3 |
| | | WB | EOP | 3 | 15 | 45 | 5 |
| | | WB | 1, 2 | 3 | 26 | 78 | 9 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 6 | 5 | 30 | 3 |
| | | WB | 1, 2 | 3 | 75 | 225 | 25 |
| | | WB | 1, 2 | 3 | 75 | 225 | 25 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 89 | 267 | 30 |
| | | WB | EOP | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 12 | 7 | 84 | 9 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 10 | 3 | 30 | 3 |
| | | WB | 2 | 10 | 3 | 30 | 3 |
| | | WB | 2 | 10 | 3 | 30 | 3 |
| | | WB | 2 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 1, 2 | 3 | 130 | 390 | 43 |
| | | WB | 2 | 12 | 8 | 96 | 11 |
| | | WB | 2 | 12 | 3 | 36 | 4 |

CONTINUED ON NEXT SHEET

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|-------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | EOP | 3 | 65 | 195 | 22 |
| | | WB | 1 | 12 | 8 | 96 | 11 |
| | | WB | 2 | 6 | 12 | 72 | 8 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 8 | 20 | 160 | 18 |
| | | WB | 1, 2 | 3 | 10 | 30 | 3 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 1, 2 | 3 | 50 | 150 | 17 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 7 | 84 | 9 |
| | | WB | 2 | 12 | 13 | 156 | 17 |
| | | WB | 1, 2 | 3 | 16 | 48 | 5 |
| | | WB | 1 | 12 | 15 | 180 | 20 |
| | | WB | 1, 2 | 3 | 22 | 66 | 7 |
| | | WB | 2 | 12 | 14 | 168 | 19 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | EOP | 4 | 30 | 120 | 13 |
| | | WB | 1 | 6 | 10 | 60 | 7 |
| | | WB | 1, 2 | 3 | 30 | 90 | 10 |
| Church Road | Church Road | WB | EOP | 6 | 10 | 60 | 7 |
| | | WB | INT | 3 | 80 | 240 | 27 |
| | | WB | INT | 3 | 40 | 120 | 13 |
| | | WB | INT | 12 | 12 | 144 | 16 |
| | | WB | INT | 12 | 12 | 144 | 16 |
| | | WB | INT | 12 | 12 | 144 | 16 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | CL | 3 | 12 | 36 | 4 |
| | | WB | CL | 3 | 90 | 270 | 30 |
| | | WB | 1, 2 | 3 | 90 | 270 | 30 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 18 | 216 | 24 |
| | | WB | Median | 3 | 38 | 114 | 13 |
| | | WB | 2 | 12 | 8 | 96 | 11 |
| | | WB | 1, 2 | 3 | 60 | 180 | 20 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 3 | 13 | 39 | 4 |
| | | WB | 2 | 10 | 3 | 30 | 3 |
| | | WB | 2 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 2 | 12 | 10 | 120 | 13 |
| | | WB | 2 | 6 | 30 | 180 | 20 |
| | | WB | EOP | 6 | 6 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 45 | 135 | 15 |

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-----------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | WB | 2 | 12 | 10 | 120 | 13 |
| | | WB | Median | 3 | 12 | 36 | 4 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 60 | 180 | 20 |
| | | WB | 1 | 12 | 8 | 96 | 11 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | EOP | 3 | 18 | 54 | 6 |
| | | WB | 2 | 12 | 9 | 108 | 12 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| Eastview Avenue | Eastview Avenue | WB | 2 | 12 | 40 | 480 | 53 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 30 | 90 | 10 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 60 | 180 | 20 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 25 | 75 | 8 |
| | | WB | EOP | 6 | 23 | 138 | 15 |
| | | WB | 1, 2 | 3 | 100 | 300 | 33 |
| | | WB | EOP | 8 | 8 | 64 | 7 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | EOP | 3 | 30 | 90 | 10 |
| Franzen Street | Franzen Street | WB | 1, 2 | 3 | 40 | 120 | 13 |
| | | WB | EOP | 3 | 13 | 39 | 4 |
| | | WB | 1, 2 | 3 | 35 | 105 | 12 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 8 | 24 | 3 |
| | | WB | 1 | 12 | 8 | 96 | 11 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| Marshall Road | Marshall Road | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | INT | 6 | 20 | 120 | 13 |
| | | WB | INT | 3 | 50 | 150 | 17 |
| | | WB | 1, 2 | 3 | 90 | 270 | 30 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 3 | 30 | 90 | 10 |
| | | WB | 1, Median | 3 | 40 | 120 | 13 |
| | | WB | 1, 2 | 3 | 60 | 180 | 20 |
| | | WB | 1, Median | 3 | 30 | 90 | 10 |

CONTINUED ON NEXT SHEET

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-----------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | WB | 1, Median | 3 | 20 | 60 | 7 |
| | | WB | 1, 2 | 3 | 35 | 105 | 12 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 6 | 3 | 18 | 2 |
| | | WB | 2 | 6 | 3 | 18 | 2 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 6 | 3 | 18 | 2 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | LT | 15 | 3 | 45 | 5 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | Spruce Avenue | WB | 1 | 12 | 3 | 36 | 4 |
| Spruce Avenue | | WB | INT | 6 | 6 | 36 | 4 |
| | | WB | INT | 6 | 110 | 660 | 73 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | | WB | EOP | 3 | 15 | 45 | 5 |
| | | WB | 2 | 12 | 8 | 96 | 11 |
| | | WB | EOP | 3 | 12 | 36 | 4 |
| | | WB | 1, 2 | 3 | 100 | 300 | 33 |
| | Pine Lane | WB | 1, Median | 3 | 60 | 180 | 20 |
| Pine Lane | | WB | 1, 2 | 3 | 120 | 360 | 40 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | | WB | EOP | 3 | 20 | 60 | 7 |
| | Edgewood Avenue | WB | 1, 2 | 3 | 25 | 75 | 8 |
| Edgewood Avenue | | WB | 1, 2 | 3 | 130 | 390 | 43 |
| | | WB | EOP | 4 | 6 | 24 | 3 |
| | | WB | 2 | 6 | 3 | 18 | 2 |
| | | WB | 1, 2 | 3 | 105 | 315 | 35 |
| | | WB | 1, 2 | 3 | 50 | 150 | 17 |
| | Ash Avenue | WB | 1 | 6 | 50 | 300 | 33 |
| Ash Avenue | | WB | INT | 6 | 6 | 36 | 4 |
| | | WB | EOP | 3 | 70 | 210 | 23 |
| | | WB | 1, 2 | 3 | 90 | 270 | 30 |
| | | WB | 1 | 12 | 17 | 204 | 23 |
| | | WB | 1, 2 | 3 | 22 | 66 | 7 |
| | Hemlock Avenue | WB | 1, 2 | 3 | 90 | 270 | 30 |
| Hemlock Avenue | | WB | EOP | 3 | 60 | 180 | 20 |
| | | WB | 1, 2 | 3 | 30 | 90 | 10 |
| | | WB | 1, 2 | 3 | 70 | 210 | 23 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | Central Avenue | WB | 1, 2 | 3 | 87 | 261 | 29 |
| Central Avenue | | WB | EOP | 3 | 87 | 261 | 29 |
| | | WB | 1, 2 | 3 | 100 | 300 | 33 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 30 | 90 | 10 |
| | Catalpa Avenue | WB | LT | 12 | 3 | 36 | 4 |
| Catalpa Avenue | | WB | 1, 2 | 3 | 40 | 120 | 13 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | Maple Avenue | WB | 1, 2 | 3 | 25 | 75 | 8 |
| Maple Avenue | | WB | 1, Median | 6 | 20 | 120 | 13 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | Cedar Avenue | WB | 1 | 12 | 35 | 420 | 47 |

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------------|--------------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| Cedar Avenue | | WB | 1, 2 | 3 | 30 | 90 | 10 |
| | | WB | 1, 2 | 3 | 27 | 81 | 9 |
| | | WB | 2 | 12 | 20 | 240 | 27 |
| | Oak Avenue | WB | 1, 2 | 3 | 40 | 120 | 13 |
| Oak Avenue | | WB | Median | 4 | 15 | 60 | 7 |
| | Elmwood Avenue | WB | Median | 15 | 3 | 45 | 5 |
| Elmwood Avenue | | WB | 1, 2 | 3 | 15 | 45 | 5 |
| | | WB | EOP | 3 | 58 | 174 | 19 |
| | | WB | EOP | 6 | 30 | 180 | 20 |
| | | WB | EOP | 3 | 50 | 150 | 17 |
| | | WB | LT | 12 | 8 | 96 | 11 |
| | Wood Dale Road | WB | 1 | 12 | 25 | 300 | 33 |
| Wood Dale Road | | WB | EOP | 3 | 80 | 240 | 27 |
| | | WB | 2 | 8 | 8 | 64 | 7 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 1, 2 | 3 | 10 | 30 | 3 |
| | | WB | 1, 2 | 3 | 70 | 210 | 23 |
| | | WB | EOP | 3 | 40 | 120 | 13 |
| | | WB | 1, Median | 4 | 4 | 16 | 2 |
| | | WB | Median | 14 | 10 | 140 | 16 |
| | | WB | 2 | 12 | 35 | 420 | 47 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 15 | 45 | 5 |
| | | WB | 1, Median | 6 | 65 | 390 | 43 |
| | | WB | 2 | 6 | 4 | 24 | 3 |
| | | WB | 1, 2 | 6 | 20 | 120 | 13 |
| | | WB | 1, 2 | 3 | 65 | 195 | 22 |
| | | WB | 1, Median | 3 | 30 | 90 | 10 |
| | Grove Avenue | WB | 1, 2 | 3 | 25 | 75 | 8 |
| Grove Avenue | | WB | EOP | 3 | 70 | 210 | 23 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | Bridge Omission | WB | 2 | 12 | 3 | 36 | 4 |
| Bridge Omission | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | Forest View Avenue | WB | EOP | 3 | 10 | 30 | 3 |
| Forest View Avenue | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1, 2 | 3 | 14 | 42 | 5 |
| | | WB | 1 | 4 | 30 | 120 | 13 |
| | | WB | 1, Median | 3 | 30 | 90 | 10 |
| | | WB | 2 | 12 | 24 | 288 | 32 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | Harvey Avenue | WB | 2 | 12 | 25 | 300 | 33 |
| Harvey Avenue | | WB | EOP | 3 | 15 | 45 | 5 |
| | | WB | 1, Median | 3 | 50 | 150 | 17 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 1, Median | 6 | 18 | 108 | 12 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 3 | 45 | 135 | 15 |
| | | WB | 1 | 15 | 4 | 60 | 7 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 15 | 3 | 45 | 5 |

CONTINUED ON NEXT SHEET

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-----------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | WB | 1 | 15 | 3 | 45 | 5 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 16 | 192 | 21 |
| | | WB | 2 | 6 | 17 | 102 | 11 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | Addison Road | WB | 2 | 12 | 3 | 36 | 4 |
| Addison Road | | WB | EOP | 6 | 80 | 480 | 53 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 2 | 25 | 8 | 200 | 22 |
| | | WB | 2 | 5 | 30 | 150 | 17 |
| | | WB | 2 | 5 | 30 | 150 | 17 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 11 | 132 | 15 |
| | | WB | 2 | 12 | 35 | 420 | 47 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 6 | 18 | 108 | 12 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2, RT | 3 | 120 | 360 | 40 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 15 | 3 | 45 | 5 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | Dalewood Avenue | WB | 2 | 12 | 3 | 36 | 4 |
| Dalewood Avenue | | WB | INT | 8 | 8 | 64 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 19 | 228 | 25 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 16 | 48 | 5 |
| | | WB | 1, Median | 15 | 5 | 75 | 8 |
| | | WB | 1 | 16 | 3 | 48 | 5 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1, 2 | 3 | 15 | 45 | 5 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 16 | 192 | 21 |
| | | WB | LT | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 12 | 144 | 16 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 14 | 4 | 56 | 6 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1, Median | 15 | 5 | 75 | 8 |

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-----------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | LT | 15 | 5 | 75 | 8 |
| | | WB | 1, LT | 6 | 40 | 240 | 27 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 16 | 48 | 5 |
| | | WB | 1, Median | 3 | 22 | 66 | 7 |
| | | WB | 1 | 6 | 15 | 90 | 10 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 50 | 150 | 17 |
| | | WB | EOP | 3 | 25 | 75 | 8 |
| | | WB | EOP | 5 | 35 | 175 | 19 |
| | | WB | 1, 2 | 3 | 260 | 780 | 87 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | EOP | 3 | 10 | 30 | 3 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | Prospect Avenue | WB | 1 | 12 | 3 | 36 | 4 |
| Prospect Avenue | | EB | EOP | 6 | 23 | 138 | 15 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | EOP | 3 | 26 | 78 | 9 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 15 | 45 | 5 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, Median | 3 | 10 | 30 | 3 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | Station Drive | EB | 2 | 12 | 3 | 36 | 4 |
| Station Drive | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | Miller Lane | EB | 2 | 12 | 6 | 72 | 8 |

CONTINUED ON NEXT SHEET

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-----------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| Miller Lane | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | EOP | 3 | 50 | 150 | 17 |
| | | EB | 2 | 12 | 9 | 108 | 12 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 3 | 25 | 75 | 8 |
| | | EB | 2 | 6 | 6 | 36 | 4 |
| | | EB | 1 | 16 | 4 | 64 | 7 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 8 | 12 | 96 | 11 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 10 | 3 | 30 | 3 |
| | Dalewood Avenue | EB | 2 | 6 | 20 | 120 | 13 |
| Dalewood Avenue | | EB | EOP | 3 | 90 | 270 | 30 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 22 | 66 | 7 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | RT | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | 1 | 6 | 6 | 36 | 4 |
| | | EB | RT | 12 | 6 | 72 | 8 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | RT | 12 | 6 | 72 | 8 |
| | Addison Road | EB | EOP | 3 | 23 | 69 | 8 |
| Addison Road | | EB | INT | 14 | 20 | 280 | 31 |
| | | EB | EOP | 3 | 60 | 180 | 20 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 6 | 40 | 240 | 27 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | EOP | 3 | 25 | 75 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 8 | 96 | 11 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 20 | 240 | 27 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 15 | 180 | 20 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 40 | 480 | 53 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | 1 | 12 | 35 | 420 | 47 |

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------------|--------------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | LT | 6 | 40 | 240 | 27 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1, 2 | 3 | 40 | 120 | 13 |
| | | EB | LT | 12 | 25 | 300 | 33 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Harvey Avenue | EB | 2 | 12 | 6 | 72 | 8 |
| Harvey Avenue | | EB | EOP | 6 | 50 | 300 | 33 |
| | | EB | 2 | 12 | 26 | 312 | 35 |
| | | EB | 1 | 12 | 35 | 420 | 47 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, Median | 6 | 60 | 360 | 40 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | 1 | 12 | 15 | 180 | 20 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Forest View Avenue | EB | 2 | 12 | 3 | 36 | 4 |
| Forest View Avenue | | EB | 1, 2 | 3 | 35 | 105 | 12 |
| | | EB | EOP | 3 | 60 | 180 | 20 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | Bridge Omission | EB | 2 | 12 | 3 | 36 | 4 |
| Bridge Omission | | EB | LT | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1, 2 | 3 | 17 | 51 | 6 |
| | | EB | 1, 2 | 3 | 12 | 36 | 4 |
| | | EB | 1, Median | 3 | 17 | 51 | 6 |
| | Edgebrook Road | EB | 2 | 12 | 40 | 480 | 53 |
| Edgebrook Road | | EB | INT | 6 | 60 | 360 | 40 |
| | | EB | INT | 12 | 10 | 120 | 13 |
| | | EB | 1, 2 | 3 | 35 | 105 | 12 |
| | | EB | 1, 2 | 3 | 35 | 105 | 12 |

CONTINUED ON NEXT SHEET

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-------------------|-------------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 1, 2 | 6 | 20 | 120 | 13 |
| | | EB | 1, 2 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, Median | 6 | 50 | 300 | 33 |
| | | EB | 2 | 6 | 30 | 180 | 20 |
| | Oakwood Drive | EB | 1, 2 | 3 | 22 | 66 | 7 |
| Oakwood Drive | | EB | EOP | 6 | 60 | 360 | 40 |
| | | EB | 1, 2 | 3 | 90 | 270 | 30 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1, 2 | 3 | 40 | 120 | 13 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | 1, LT | 3 | 45 | 135 | 15 |
| | | EB | 1, 2 | 3 | 60 | 180 | 20 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1, 2 | 3 | 40 | 120 | 13 |
| | Wood Dale Road | EB | 1 | 12 | 3 | 36 | 4 |
| Wood Dale Road | | EB | INT | 12 | 20 | 240 | 27 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1, 2 | 6 | 33 | 198 | 22 |
| | | EB | 1 | 3 | 6 | 18 | 2 |
| | Railroad Omission | EB | 1, 2 | 6 | 30 | 180 | 20 |
| Railroad Omission | | EB | 1, 2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 12 | 25 | 300 | 33 |
| | | EB | 1, 2 | 3 | 30 | 90 | 10 |
| | | EB | 1, Median | 15 | 15 | 225 | 25 |
| | | EB | 1, 2 | 3 | 10 | 30 | 3 |
| | | EB | 1, 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 6 | 25 | 150 | 17 |
| | | EB | 2 | 12 | 15 | 180 | 20 |
| | | EB | 1, 2 | 3 | 23 | 69 | 8 |
| | | EB | 1, 2 | 3 | 30 | 90 | 10 |
| | | EB | LT | 8 | 10 | 80 | 9 |
| | | EB | LT | 3 | 16 | 48 | 5 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | LT | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 1, 2 | 3 | 130 | 390 | 43 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | EOP | 3 | 10 | 30 | 3 |
| | | EB | 1, 2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 12 | 20 | 240 | 27 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | 1, 2 | 3 | 50 | 150 | 17 |
| | | EB | 1, 2 | 3 | 60 | 180 | 20 |
| | | EB | 1, 2 | 3 | 60 | 180 | 20 |

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-----------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 12 | 36 | 4 |
| | | EB | 1 | 20 | 6 | 120 | 13 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, LT | 3 | 15 | 45 | 5 |
| | | EB | LT | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 125 | 375 | 42 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 20 | 6 | 120 | 13 |
| | | EB | LT | 20 | 5 | 100 | 11 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | LT | 12 | 15 | 180 | 20 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 80 | 960 | 107 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | LT | 12 | 15 | 180 | 20 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | Hemlock Avenue | EB | 2 | 12 | 10 | 120 | 13 |
| Hemlock Avenue | | EB | EOP | 3 | 60 | 180 | 20 |
| | | EB | 1 | 12 | 25 | 300 | 33 |
| | | EB | 1, 2 | 3 | 23 | 69 | 8 |
| | | EB | 1 | 12 | 65 | 780 | 87 |
| | Ash Avenue | EB | 1, 2 | 3 | 80 | 240 | 27 |
| Ash Avenue | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | EOP | 3 | 50 | 150 | 17 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 12 | 8 | 96 | 11 |
| | | EB | Median | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1, 2 | 3 | 18 | 54 | 6 |
| | | EB | Median | 12 | 6 | 72 | 8 |
| | | EB | Median | 12 | 20 | 240 | 27 |
| | | EB | Median | 12 | 4 | 48 | 5 |
| | | EB | 1, 2 | 3 | 18 | 54 | 6 |
| | Edgewood Avenue | EB | 1, 2 | 3 | 16 | 48 | 5 |
| Edgewood Avenue | | EB | EOP | 3 | 60 | 180 | 20 |
| | | EB | 1,2 | 3 | 40 | 120 | 13 |
| | | EB | 1 | 12 | 25 | 300 | 33 |
| | | EB | Median | 12 | 3 | 36 | 4 |
| | | EB | Median | 12 | 23 | 276 | 31 |
| | | EB | 1, 2 | 3 | 34 | 102 | 11 |
| | | EB | 1, Median | 3 | 30 | 90 | 10 |
| | | EB | 1 | 12 | 14 | 168 | 19 |
| | | EB | 1, 2 | 3 | 50 | 150 | 17 |

CONTINUED ON NEXT SHEET

| | | | | | |
|---|------------------------------|------------|----------|-----------|--|
| FILE NAME = | USER NAME = bartonw | DESIGNED - | RWB | REVISED - | |
| et:\pw\work\p\dot\bar tonw\10427922\HMA-DupPage.dgn | | DRAWN - | RWB | REVISED - | |
| Default | PLOT SCALE = 100.0000' / in. | CHECKED - | | REVISED - | |
| | PLOT DATE = 4/7/2015 | DATE - | 4/7/2015 | REVISED - | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INTERMITTENT RESURFACING SCHEDULE
IL 19**

SCALE: SHEET 17 OF 29 SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DUPAGE | 44 | 23 |
| | | | CONTRACT NO. 62A84 | |
| ILLINOIS FED. AID PROJECT | | | | |

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|---------------|---------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 1, Median | 3 | 86 | 258 | 29 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | Median | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 33 | 99 | 11 |
| | | EB | 1, Median | 3 | 33 | 99 | 11 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | Pine Lane | EB | 1 | 12 | 3 | 36 | 4 |
| Pine Lane | | EB | EOP | 3 | 30 | 90 | 10 |
| | | EB | 2 | 3 | 5 | 15 | 2 |
| | | EB | 1, LT | 3 | 40 | 120 | 13 |
| | | EB | 1, 2 | 3 | 15 | 45 | 5 |
| | | EB | 1 | 20 | 6 | 120 | 13 |
| | | EB | LT | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 20 | 240 | 27 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 55 | 165 | 18 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1, LT | 3 | 10 | 30 | 3 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | Spruce Avenue | EB | 1 | 12 | 3 | 36 | 4 |
| Spruce Avenue | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 15 | 180 | 20 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | EOP | 3 | 26 | 78 | 9 |
| | | EB | 1 | 6 | 3 | 18 | 2 |
| | | EB | 1 | 12 | 45 | 540 | 60 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 6 | 3 | 18 | 2 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 5 | 12 | 60 | 7 |
| | | EB | EOP | 3 | 15 | 45 | 5 |
| | | EB | EOP | 3 | 75 | 225 | 25 |
| | | EB | EOP | 3 | 50 | 150 | 17 |
| | | EB | EOP | 3 | 20 | 60 | 7 |
| | | EB | 1, 2 | 12 | 8 | 96 | 11 |
| | | EB | 1, 2 | 6 | 30 | 180 | 20 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-----------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Marshall Road | EB | 2 | 12 | 3 | 36 | 4 |
| Marshall Road | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | EOP | 3 | 15 | 45 | 5 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | EOP | 3 | 25 | 75 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | EOP | 3 | 20 | 60 | 7 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 6 | 90 | 540 | 60 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 130 | 390 | 43 |
| | | EB | EOP | 3 | 40 | 120 | 13 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | 2 | 12 | 55 | 660 | 73 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 1, 2 | 3 | 18 | 54 | 6 |
| | | EB | 1, 2 | 3 | 35 | 105 | 12 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 26 | 312 | 35 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 5 | 15 | 2 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 7 | 5 | 35 | 4 |
| | | EB | 1, 2 | 3 | 65 | 195 | 22 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | EOP | 3 | 35 | 105 | 12 |
| | | EB | 1, 2 | 3 | 40 | 120 | 13 |
| | | EB | 1, 2 | 3 | 40 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1, 2 | 3 | 25 | 75 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 30 | 360 | 40 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | Eastview Avenue | EB | 2 | 12 | 6 | 72 | 8 |
| Eastview Avenue | | EB | 2 | 12 | 20 | 240 | 27 |
| | | EB | 1, 2 | 3 | 45 | 135 | 15 |
| | | EB | EOP | 3 | 45 | 135 | 15 |
| | | EB | 2 | 12 | 40 | 480 | 53 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 45 | 135 | 15 |
| | | EB | 1, 2 | 3 | 50 | 150 | 17 |
| | | EB | 2 | 12 | 3 | 36 | 4 |

CONTINUED ON NEXT SHEET

ROUTE: IL 19 (Prospect Avenue to York Road) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|---------------|---------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 110 | 330 | 37 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 10 | 3 | 30 | 3 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 22 | 66 | 7 |
| | | EB | 2 | 12 | 15 | 180 | 20 |
| | | EB | 2 | 12 | 50 | 600 | 67 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 20 | 240 | 27 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 6 | 25 | 150 | 17 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 60 | 180 | 20 |
| Parkside Lane | Parkside Lane | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 50 | 600 | 67 |
| | | EB | 2 | 12 | 35 | 420 | 47 |
| | | EB | EOP | 3 | 12 | 36 | 4 |
| | | EB | 1, 2 | 3 | 25 | 75 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 20 | 5 | 100 | 11 |
| | | EB | RT | 12 | 22 | 264 | 29 |
| | | EB | 2 | 10 | 3 | 30 | 3 |
| | | EB | 1 | 10 | 3 | 30 | 3 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 35 | 420 | 47 |
| | Church Road | EB | EOP | 6 | 20 | 120 | 13 |

TOTALS: 13920 FT 8714 SY

ROUTE: IL 19 (IL 53 to Barrington Road)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|------------------|------------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| Barrington Road | | EB | 2 | 12 | 40 | 480 | 53 |
| | | EB | 2 | 3 | 90 | 270 | 30 |
| | | EB | 1,2 | 3 | 20 | 60 | 7 |
| | | EB | 1,2 | 3 | 5 | 15 | 2 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1,2 | 3 | 60 | 180 | 20 |
| | | EB | 2 | 3 | 45 | 135 | 15 |
| | | EB | 2 | 3 | 35 | 105 | 12 |
| | | EB | 1,2 | 3 | 15 | 45 | 5 |
| | | EB | 2 | 4 | 12 | 48 | 5 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 3 | 35 | 105 | 12 |
| | | EB | 2 | 3 | 45 | 135 | 15 |
| | | EB | 12 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 45 | 135 | 15 |
| | | EB | 2 | 3 | 100 | 300 | 33 |
| | | EB | 2 | 9 | 3 | 27 | 3 |
| Kingsbury Drive | Kingsbury Drive | EB | 2 | 9 | 3 | 27 | 3 |
| | | EB | 2 | 5 | 12 | 60 | 7 |
| | | EB | 2 | 3 | 15 | 45 | 5 |
| | | EB | 2 | 3 | 30 | 90 | 10 |
| | | EB | 1,2 | 3 | 5 | 15 | 2 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| Cumberland Drive | Cumberland Drive | EB | LT,1 | 3 | 65 | 195 | 22 |
| Longmeadow Lane | Longmeadow Lane | EB | CL | 6 | 10 | 60 | 7 |
| Edgebrook Lane | Edgebrook Lane | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1,2 | 3 | 55 | 165 | 18 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | LT | 10 | 3 | 30 | 3 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| Northway Drive | Northway Drive | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| Olde Salem Road | Olde Salem Road | EB | CL,1 | 3 | 55 | 165 | 18 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | LT,1 | 3 | 40 | 120 | 13 |
| | | EB | 2 | 6 | 4 | 24 | 3 |
| | | EB | LT,1 | 3 | 65 | 195 | 22 |
| | | EB | 2 | 3 | 45 | 135 | 15 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| Orchard Lane | Orchard Lane | EB | 2 | 13 | 3 | 39 | 4 |
| | | EB | CL | 3 | 6 | 18 | 2 |
| | | EB | 2 | 14 | 4 | 56 | 6 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 14 | 5 | 70 | 8 |

CONTINUED ON NEXT SHEET

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------------|--------------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1,2 | 3 | 160 | 480 | 53 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 3 | 15 | 45 | 5 |
| | | EB | 1 | 9 | 3 | 27 | 3 |
| | | EB | 2 | 9 | 3 | 27 | 3 |
| | | EB | LT | 10 | 3 | 30 | 3 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 9 | 3 | 27 | 3 |
| | Olde Salem Circle | EB | RT | 9 | 3 | 27 | 3 |
| Olde Salem Circle | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2,RT | 3 | 15 | 45 | 5 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | RT | 12 | 3 | 36 | 4 |
| | | EB | 2,RT | 3 | 10 | 30 | 3 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2,RT | 3 | 100 | 300 | 33 |
| | Wise Road | EB | RT | 9 | 3 | 27 | 3 |
| Wise Road | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 4 | 10 | 40 | 4 |
| | | EB | 2 | 3 | 75 | 225 | 25 |
| | | EB | 2 | 3 | 50 | 150 | 17 |
| | | EB | 1,2 | 3 | 50 | 150 | 17 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 3 | 40 | 120 | 13 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1,2 | 3 | 240 | 720 | 80 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | LT | 6 | 3 | 18 | 2 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | Mercury Drive | EB | LT,1 | 3 | 100 | 300 | 33 |
| Wright Boulevard | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 4 | 30 | 120 | 13 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | Dupage County Line | EB | 1 | 14 | 3 | 42 | 5 |
| Dupage County Line | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 12 | 168 | 19 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 5 | 70 | 8 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | LT,1 | 14 | 3 | 42 | 5 |
| | Mitchell Boulevard | EB | LT | 12 | 3 | 36 | 4 |
| Mitchell Boulevard | | EB | 1 | 16 | 3 | 48 | 5 |
| | RR Tracks | EB | 1 | 14 | 3 | 42 | 5 |

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-----------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| RR Tracks | | EB | 1 | 14 | 4 | 56 | 6 |
| | | EB | 1 | 14 | 6 | 84 | 9 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 4 | 56 | 6 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | Williams Street | EB | 1 | 14 | 3 | 42 | 5 |
| Williams Street | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 8 | 3 | 24 | 3 |
| | | EB | 1 | 14 | 12 | 168 | 19 |
| | | EB | 1 | 14 | 4 | 56 | 6 |
| | | EB | 1 | 6 | 3 | 18 | 2 |
| | | EB | 1 | 6 | 4 | 24 | 3 |
| | | EB | 1 | 6 | 55 | 330 | 37 |
| | | EB | 1 | 3 | 85 | 255 | 28 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 6 | 20 | 120 | 13 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | Lincoln Street | EB | 1 | 16 | 3 | 48 | 5 |
| Lincoln Street | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 13 | 5 | 65 | 7 |
| | | EB | 1 | 10 | 3 | 30 | 3 |
| | | EB | 1 | 9 | 5 | 45 | 5 |
| | | EB | 1 | 19 | 3 | 57 | 6 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 9 | 20 | 180 | 20 |
| | | EB | 1 | 19 | 4 | 76 | 8 |
| | | EB | 1 | 9 | 9 | 81 | 9 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | Marion Street | EB | 1 | 19 | 6 | 114 | 13 |
| Marion Street | | EB | 1 | 19 | 4 | 76 | 8 |
| | | EB | 1 | 9 | 6 | 54 | 6 |
| | | EB | 1 | 9 | 35 | 315 | 35 |
| | | EB | 1 | 10 | 3 | 30 | 3 |
| | | EB | 1 | 10 | 3 | 30 | 3 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | Roselle Road | EB | LT | 12 | 3 | 36 | 4 |
| Roselle Road | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 6 | 4 | 24 | 3 |
| | | EB | 1 | 8 | 4 | 32 | 4 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | LT | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 6 | 10 | 60 | 7 |
| | | EB | 1 | 12 | 4 | 48 | 5 |

CONTINUED ON NEXT SHEET

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-----------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 1 | 4 | 4 | 16 | 2 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | Prospect Street | EB | 2 | 12 | 4 | 48 | 5 |
| Prospect Street | | EB | 2 | 13 | 4 | 52 | 6 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 13 | 10 | 130 | 14 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | Park Street | EB | 2 | 12 | 4 | 48 | 5 |
| Park Street | | EB | 1,2 | 6 | 20 | 120 | 13 |
| | | EB | 2 | 13 | 35 | 455 | 51 |
| | | EB | 1 | 11 | 4 | 44 | 5 |
| | | EB | 2 | 13 | 250 | 3250 | 361 |
| | | EB | 1 | 11 | 6 | 66 | 7 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 11 | 4 | 44 | 5 |
| | | EB | 2 | 13 | 60 | 780 | 87 |
| | | EB | 1,2 | 4 | 45 | 180 | 20 |
| | | EB | 2 | 13 | 65 | 845 | 94 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 4 | 20 | 80 | 9 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 4 | 70 | 280 | 31 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 4 | 20 | 80 | 9 |
| | | EB | 1,2 | 4 | 40 | 160 | 18 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 4 | 30 | 120 | 13 |
| | | EB | LT | 13 | 4 | 52 | 6 |
| | | EB | LT | 12 | 4 | 48 | 5 |
| | Lawrence Avenue | EB | 1 | 12 | 4 | 48 | 5 |
| Lawrence Avenue | | EB | 2 | 4 | 6 | 24 | 3 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 4 | 55 | 220 | 24 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 8 | 96 | 11 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1,2 | 4 | 100 | 400 | 44 |
| | | EB | 2 | 12 | 4 | 48 | 5 |

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-----------------|------------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | LT | 12 | 4 | 48 | 5 |
| | | EB | LT | 12 | 4 | 48 | 5 |
| | Maple Avenue | EB | 1 | 12 | 4 | 48 | 5 |
| Maple Avenue | | EB | LT | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | Maple Avenue | EB | 2 | 4 | 45 | 180 | 20 |
| Maple Avenue | Ardmore Street | EB | 1 | 12 | 6 | 72 | 8 |
| Ardmore Street | | EB | 2 | 4 | 15 | 60 | 7 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 1,2 | 4 | 40 | 160 | 18 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 4 | 70 | 280 | 31 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 13 | 60 | 780 | 87 |
| | | EB | 2 | 4 | 20 | 80 | 9 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 6 | 10 | 60 | 7 |
| | | EB | 1 | 11 | 4 | 44 | 5 |
| | | EB | 2 | 13 | 55 | 715 | 79 |
| | | EB | 1 | 11 | 4 | 44 | 5 |
| | | EB | 1,2 | 4 | 15 | 60 | 7 |
| | | EB | 2 | 13 | 10 | 130 | 14 |
| | | EB | 1,2 | 4 | 15 | 60 | 7 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 45 | 540 | 60 |
| | | EB | 1,2 | 4 | 15 | 60 | 7 |
| | | EB | 1 | 12 | 15 | 180 | 20 |
| | Pinecroft Drive | EB | 2 | 12 | 30 | 360 | 40 |
| Pinecroft Drive | | EB | 1 | 12 | 4 | 48 | 5 |
| | Pinecroft Drive | EB | 2 | 4 | 12 | 48 | 5 |
| Pinecroft Drive | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 4 | 50 | 200 | 22 |
| | | EB | 1,2 | 4 | 30 | 120 | 13 |
| | | EB | 2 | 12 | 40 | 480 | 53 |
| | | EB | 2 | 4 | 30 | 120 | 13 |
| | | EB | 2 | 4 | 10 | 40 | 4 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 11 | 4 | 44 | 5 |
| | | EB | 2 | 4 | 80 | 320 | 36 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 13 | 60 | 780 | 87 |
| | | EB | 1 | 11 | 4 | 44 | 5 |
| | | EB | 1,RT | 4 | 30 | 120 | 13 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | RT | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | Springhill Drive | EB | RT | 12 | 4 | 48 | 5 |

CONTINUED ON NEXT SHEET

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|------------------|------------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Baker Drive | EB | 1 | 12 | 3 | 36 | 4 |
| Baker Drive | | EB | 1 | 10 | 3 | 30 | 3 |
| | | EB | 1,2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 8 | 3 | 24 | 3 |
| | Andrene Lane | EB | 2 | 8 | 30 | 240 | 27 |
| Andrene Lane | | WB | 1 | 6 | 20 | 120 | 13 |
| | | WB | 2 | 4 | 15 | 60 | 7 |
| | | WB | 2 | 6 | 30 | 180 | 20 |
| | | WB | 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | Baker Drive | WB | 1 | 12 | 3 | 36 | 4 |
| Baker Drive | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 6 | 20 | 120 | 13 |
| | Hillcrest Avenue | WB | 1 | 12 | 3 | 36 | 4 |
| Hillcrest Avenue | | WB | 1 | 12 | 7 | 84 | 9 |
| | | WB | 1 | 3 | 70 | 210 | 23 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 3 | 15 | 45 | 5 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 4 | 30 | 120 | 13 |
| | | WB | 1 | 8 | 4 | 32 | 4 |
| | Gates Avenue | WB | 1 | 3 | 40 | 120 | 13 |
| Gates Avenue | | WB | 1 | 6 | 20 | 120 | 13 |
| | | WB | 1 | 8 | 3 | 24 | 3 |
| | | WB | 1 | 8 | 3 | 24 | 3 |
| | | WB | 1 | 8 | 10 | 80 | 9 |
| | | WB | 1 | 8 | 3 | 24 | 3 |
| | | WB | 1 | 8 | 3 | 24 | 3 |
| | | WB | 1 | 3 | 25 | 75 | 8 |
| | Newland Place | WB | 1 | 9 | 3 | 27 | 3 |
| Newland Place | | WB | 1 | 4 | 30 | 120 | 13 |
| | | WB | 1 | 13 | 6 | 78 | 9 |
| | | WB | 1 | 13 | 3 | 39 | 4 |
| | | WB | 1 | 13 | 5 | 65 | 7 |
| | | WB | 1 | 13 | 3 | 39 | 4 |
| | | WB | 1 | 13 | 3 | 39 | 4 |
| | | WB | 1 | 3 | 100 | 300 | 33 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | Manor Road | WB | 1 | 10 | 3 | 30 | 3 |
| Manor Road | | WB | 1,2 | 3 | 55 | 165 | 18 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | Valley Lane | WB | 1 | 12 | 3 | 36 | 4 |

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|--------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | Valley Lane | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 13 | 4 | 52 | 6 |
| | | WB | 1 | 3 | 45 | 135 | 15 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 6 | 3 | 18 | 2 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 10 | 120 | 13 |
| | | WB | 1,2 | 3 | 12 | 36 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 6 | 3 | 18 | 2 |
| | | WB | 1 | 6 | 3 | 18 | 2 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 14 | 3 | 42 | 5 |
| | Medinah Road | WB | 2 | 13 | 3 | 39 | 4 |
| Medinah Road | | WB | 2 | 16 | 3 | 48 | 5 |
| | | WB | 1 | 7 | 3 | 21 | 2 |
| | | WB | 2 | 13 | 3 | 39 | 4 |
| | | WB | 2 | 13 | 3 | 39 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 8 | 4 | 32 | 4 |
| | | WB | 1,2 | 3 | 12 | 36 | 4 |
| | | WB | 2 | 7 | 4 | 28 | 3 |
| | | WB | 1,2 | 3 | 40 | 120 | 13 |
| | | WB | 2 | 6 | 4 | 24 | 3 |
| | | WB | 2 | 5 | 3 | 15 | 2 |
| | | WB | 1,2 | 3 | 50 | 150 | 17 |
| | | WB | 1 | 13 | 3 | 39 | 4 |
| | | WB | 1 | 14 | 6 | 84 | 9 |
| | Harvey Road | WB | 1 | 13 | 3 | 39 | 4 |
| Harvey Road | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 6 | 10 | 60 | 7 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 3 | 15 | 45 | 5 |
| | | WB | 1 | 4 | 40 | 160 | 18 |
| | | WB | 1 | 12 | 5 | 60 | 7 |

CONTINUED ON NEXT SHEET

ROUTE: IL 19 (IL 53 to Barrington Road) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|------------------|------------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | WB | 1 | 3 | 35 | 105 | 12 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | Sycamore Avenue | WB | 1 | 12 | 3 | 36 | 4 |
| Sycamore Avenue | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 4 | 270 | 1080 | 120 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1,2 | 4 | 15 | 60 | 7 |
| | Springhill Drive | WB | 1,2 | 4 | 100 | 400 | 44 |
| Springhill Drive | | WB | 2 | 4 | 30 | 120 | 13 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 40 | 160 | 18 |
| | | WB | 2 | 12 | 8 | 96 | 11 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 13 | 35 | 455 | 51 |
| | | WB | 1,2 | 4 | 15 | 60 | 7 |
| | | WB | 2 | 12 | 200 | 2400 | 267 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 30 | 120 | 13 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 15 | 60 | 7 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 50 | 200 | 22 |
| | | WB | 2 | 4 | 50 | 200 | 22 |
| | Pinecroft Drive | WB | 1,2 | 4 | 45 | 180 | 20 |
| Pinecroft Drive | | WB | 1,2 | 4 | 95 | 380 | 42 |
| | | WB | 2 | 4 | 50 | 200 | 22 |
| | | WB | 2 | 12 | 45 | 540 | 60 |
| | | WB | 2 | 4 | 25 | 100 | 11 |
| | | WB | 2 | 12 | 30 | 360 | 40 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 13 | 70 | 910 | 101 |
| | | WB | 2 | 13 | 20 | 260 | 29 |
| | | WB | 1 | 11 | 4 | 44 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 1,2 | 4 | 10 | 40 | 4 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1,2 | 4 | 15 | 60 | 7 |
| | | WB | 2 | 10 | 4 | 40 | 4 |
| | | WB | 2 | 4 | 6 | 24 | 3 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1,2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 12 | 4 | 48 | 5 |

ROUTE: IL 19 (IL 53 to Barrington Road) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------|--------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | WB | 2 | 4 | 15 | 60 | 7 |
| | | WB | LT | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | LT | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 75 | 300 | 33 |
| | | WB | 2 | 4 | 55 | 220 | 24 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | Maple Avenue | WB | 2 | 12 | 4 | 48 | 5 |
| Maple Avenue | Maple Avenue | WB | 2 | 4 | 20 | 80 | 9 |
| Maple Avenue | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 140 | 560 | 62 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 75 | 300 | 33 |
| | | WB | 1 | 12 | 40 | 480 | 53 |
| | | WB | 2 | 12 | 40 | 480 | 53 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 13 | 25 | 325 | 36 |
| | | WB | 1,2 | 4 | 10 | 40 | 4 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 20 | 240 | 27 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 45 | 180 | 20 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 8 | 32 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 65 | 260 | 29 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 25 | 100 | 11 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 4 | 10 | 40 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 8 | 96 | 11 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 10 | 40 | 4 |
| | | WB | 2 | 13 | 6 | 78 | 9 |

CONTINUED ON NEXT SHEET

| | | | | | |
|---|------------------------------|------------|----------|-----------|--|
| FILE NAME = | USER NAME = bartonw | DESIGNED - | RWB | REVISED - | |
| et:\pw\work\p\id\bar tonw\0427922\HMA-DupPage.dgn | | DRAWN - | RWB | REVISED - | |
| Default | PLOT SCALE = 100.0000' / in. | CHECKED - | | REVISED - | |
| | PLOT DATE = 4/7/2015 | DATE - | 4/7/2015 | REVISED - | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INTERMITTENT RESURFACING SCHEDULE
IL 19**

SCALE: SHEET 24 OF 29 SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------------|-----------|
| F.A. RTÉ. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DUPAGE | 44 | 30 |
| | | | CONTRACT NO. 62A84 | |
| ILLINOIS FED. AID PROJECT | | | | |

ROUTE: IL 19 (IL 53 to Barrington Road) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|-----------------|-----------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | WB | 1,2 | 6 | 30 | 180 | 20 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1,2 | 4 | 8 | 32 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 4 | 15 | 60 | 7 |
| | | WB | 2 | 4 | 10 | 40 | 4 |
| | | WB | 1,2 | 4 | 10 | 40 | 4 |
| | Park Street | WB | 1,2 | 4 | 25 | 100 | 11 |
| Park Street | | WB | 2 | 12 | 15 | 180 | 20 |
| | Park Street | WB | 2 | 12 | 4 | 48 | 5 |
| Park Street | | WB | 1 | 14 | 6 | 84 | 9 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | Prospect Street | WB | 2 | 12 | 4 | 48 | 5 |
| Prospect Street | | WB | 1 | 12 | 4 | 48 | 5 |
| | Prospect Street | WB | 2 | 12 | 4 | 48 | 5 |
| Prospect Street | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | LT | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | Bokelman Street | WB | 2 | 12 | 4 | 48 | 5 |
| Bokelman Street | Bokelman Street | WB | 1 | 13 | 4 | 52 | 6 |
| Bokelman Street | | WB | 1 | 11 | 4 | 44 | 5 |
| | | WB | 2 | 13 | 4 | 52 | 6 |
| | | WB | 2 | 4 | 10 | 40 | 4 |
| | | WB | LT,1 | 18 | 4 | 72 | 8 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | LT | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | Roselle Road | WB | 2 | 12 | 4 | 48 | 5 |
| Roselle Road | Roselle Road | WB | 1 | 12 | 12 | 144 | 16 |
| Roselle Road | | WB | 1 | 6 | 3 | 18 | 2 |
| | | WB | 1 | 19 | 3 | 57 | 6 |
| | | WB | 1 | 3 | 25 | 75 | 8 |
| | | WB | 1 | 19 | 3 | 57 | 6 |
| | | WB | 1 | 19 | 3 | 57 | 6 |
| | | WB | 1 | 3 | 8 | 24 | 3 |
| | | WB | 1 | 6 | 3 | 18 | 2 |
| | Marion Street | WB | 1 | 3 | 20 | 60 | 7 |
| Marion Street | | WB | 1 | 7 | 3 | 21 | 2 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 3 | 6 | 18 | 2 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | Lincoln Street | WB | 1 | 6 | 6 | 36 | 4 |
| Lincoln Street | Lincoln Street | NB | 1 | 15 | 3 | 45 | 5 |
| Lincoln Street | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 14 | 3 | 42 | 5 |
| | Williams Street | WB | 1 | 14 | 3 | 42 | 5 |
| Williams Street | | WB | 1 | 14 | 3 | 42 | 5 |
| | | WB | 1 | 14 | 3 | 42 | 5 |

ROUTE: IL 19 (IL 53 to Barrington Road) (Continued)

| CROSS STREET | | DIRECTION (EB/WB) (NB/SB) | LANE NO. (1, 2, 3) | PAVEMENT PATCH WIDTH | PAVEMENT PATCH LENGTH | REPAIR AREA (SQ FT) | REPAIR AREA (SQ YD) |
|--------------------|--------------------|---------------------------------|--------------------------|----------------------------|-----------------------------|---------------------------|---------------------------|
| FROM | TO | | | | | | |
| | | WB | 1 | 14 | 4 | 56 | 6 |
| | | WB | 1 | 14 | 3 | 42 | 5 |
| | | WB | 1 | 14 | 3 | 42 | 5 |
| | RR Tracks | WB | 1 | 14 | 10 | 140 | 16 |
| RR Tracks | Mitchell Boulevard | WB | 1 | 12 | 3 | 36 | 4 |
| Mitchell Boulevard | | WB | 1 | 14 | 3 | 42 | 5 |
| | | WB | 1 | 14 | 3 | 42 | 5 |
| | | WB | 1 | 14 | 3 | 42 | 5 |
| | Cook County Line | WB | 1 | 14 | 3 | 42 | 5 |
| Cook County Line | | WB | 1 | 14 | 4 | 56 | 6 |
| | | WB | 1 | 3 | 40 | 120 | 13 |
| | | WB | 1 | 3 | 25 | 75 | 8 |
| | | WB | 1 | 8 | 3 | 24 | 3 |
| | | WB | 1 | 14 | 3 | 42 | 5 |
| | | WB | 1 | 14 | 3 | 42 | 5 |
| | | WB | 1 | 6 | 10 | 60 | 7 |
| | Wright Boulevard | WB | 1 | 6 | 30 | 180 | 20 |
| Mercury Drive | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | LT,1 | 3 | 65 | 195 | 22 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | Wise Road | WB | 1,2 | 3 | 35 | 105 | 12 |
| Wise Road | | WB | RAD | 9 | 3 | 27 | 3 |
| | | WB | 2 | 3 | 10 | 30 | 3 |
| | | WB | 1,2 | 3 | 10 | 30 | 3 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | Olde Salem Circle | WB | 2 | 12 | 3 | 36 | 4 |
| Olde Salem Circle | | WB | 1,2 | 3 | 15 | 45 | 5 |
| | | WB | LT | 8 | 3 | 24 | 3 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | LT | 10 | 3 | 30 | 3 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | LT,1 | 3 | 80 | 240 | 27 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 9 | 3 | 27 | 3 |
| | Orchard Lane | WB | 2 | 9 | 3 | 27 | 3 |
| Orchard Lane | | WB | 2 | 6 | 4 | 24 | 3 |
| | | WB | 2 | 9 | 3 | 27 | 3 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 6 | 3 | 18 | 2 |
| | | WB | 2 | 6 | 4 | 24 | 3 |
| | | WB | 1 | 12 | 7 | 84 | 9 |
| | | WB | 2 | 12 | 7 | 84 | 9 |
| | | WB | 1 | 9 | 3 | 27 | 3 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 3 | 55 | 165 | 18 |
| | Olde Salem Road | WB | 2 | 12 | 3 | 36 | 4 |
| Olde Salem Road | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 2 | 13 | 4 | 52 | 6 |
| | | WB | 2 | 12 | 7 | 84 | 9 |
| | | WB | 2 | 3 | 40 | 120 | 13 |
| | | WB | 12 | 12 | 4 | 48 | 5 |
| | Northway Drive | WB | 2 | 3 | 10 | 30 | 3 |
| Northway Drive | | NB | 1 | 15 | 50 | 750 | 83 |
| | Northway Drive | SB | 1 | 15 | 50 | 750 | 83 |

CONTINUED ON NEXT SHEET

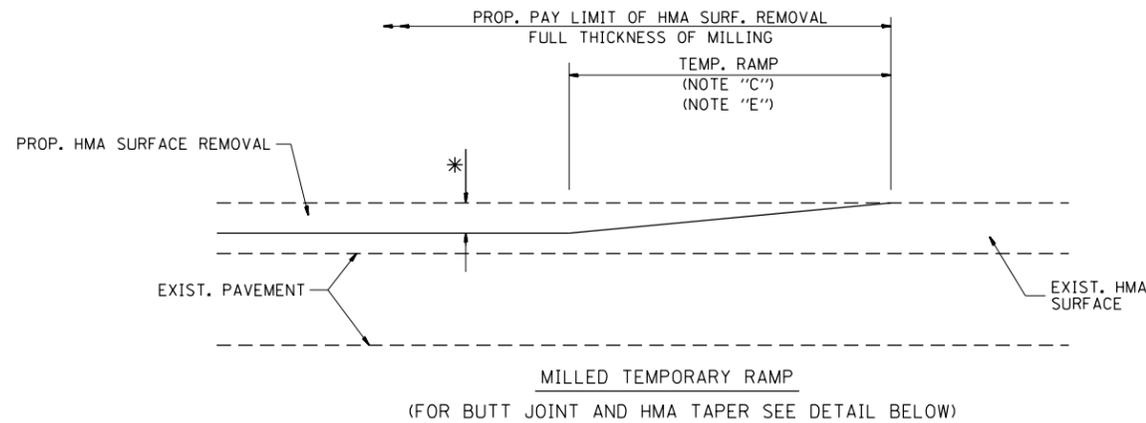
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|--|-------------------------------|------------|----------|-----------|--|
| FILE NAME = | USER NAME = bartonw | DESIGNED - | RWB | REVISED - | |
| c:\pwork\pwork\bar tonw\10427922\HMA-DupPage.dgn | | DRAWN - | RWB | REVISED - | |
| Default | | CHECKED - | | REVISED - | |
| | PLOT SCALE = 100.0000' / 1in. | DATE - | 4/7/2015 | REVISED - | |
| | PLOT DATE = 4/7/2015 | | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

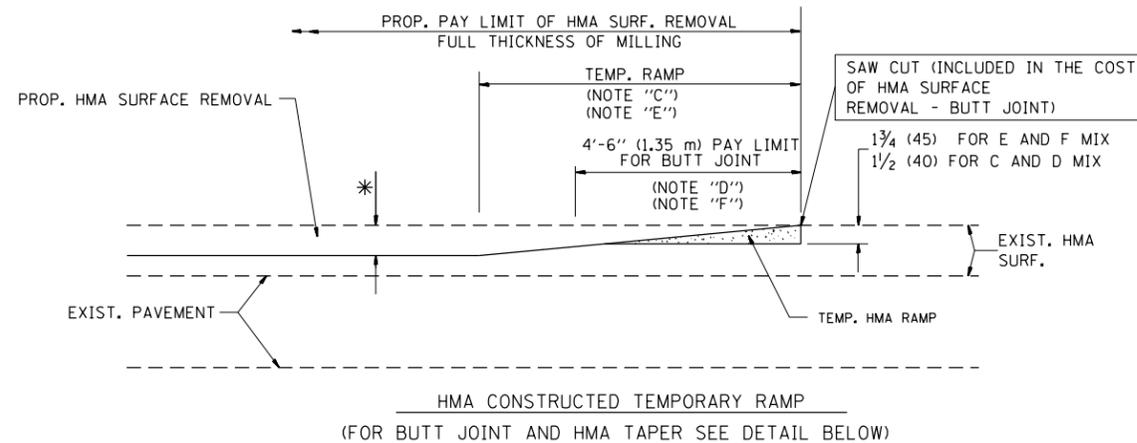
INTERMITTENT RESURFACING SCHEDULE
IL 19

SCALE: SHEET 25 OF 29 SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DUPAGE | 44 | 31 |
| CONTRACT NO. 62A84 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

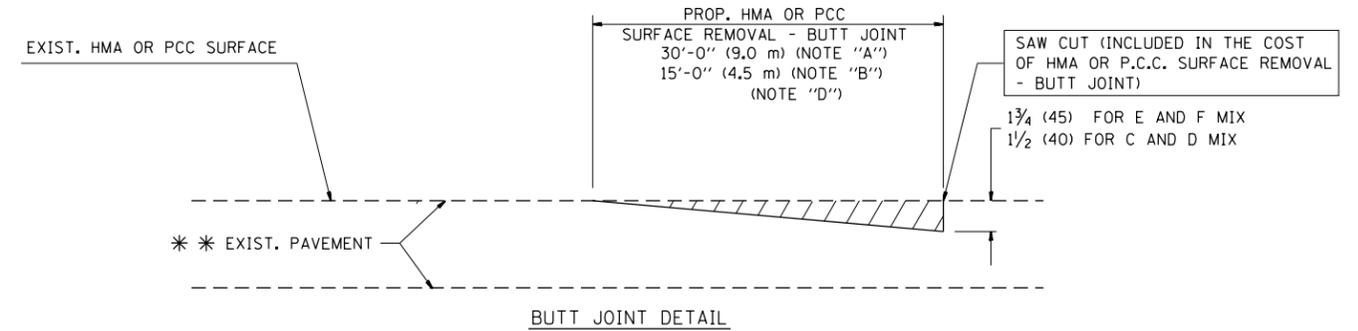


OPTION 1

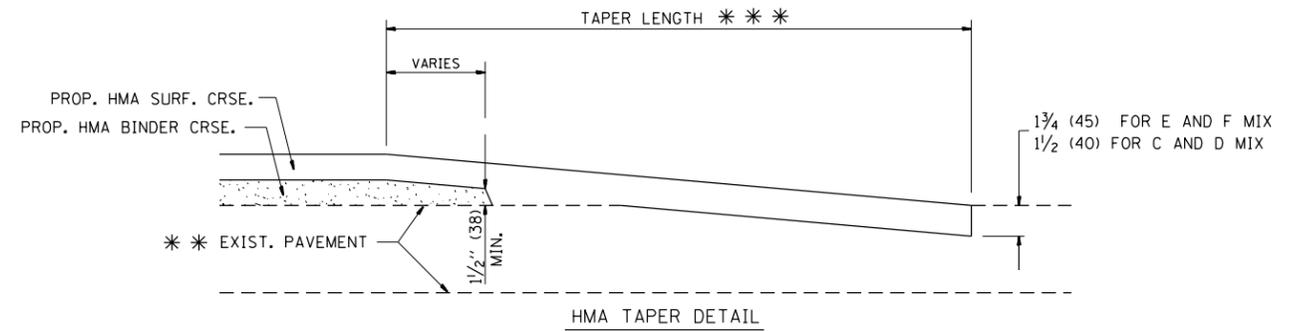


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

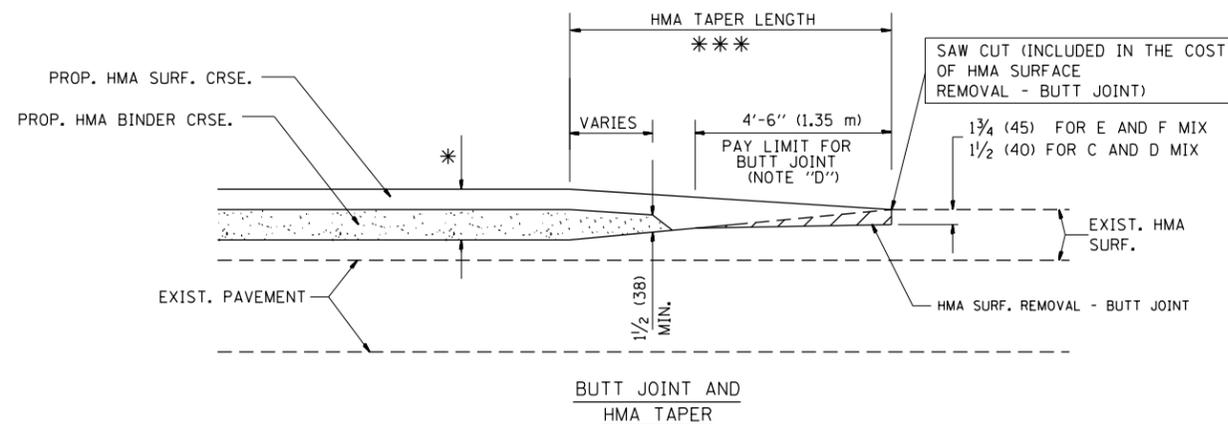
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



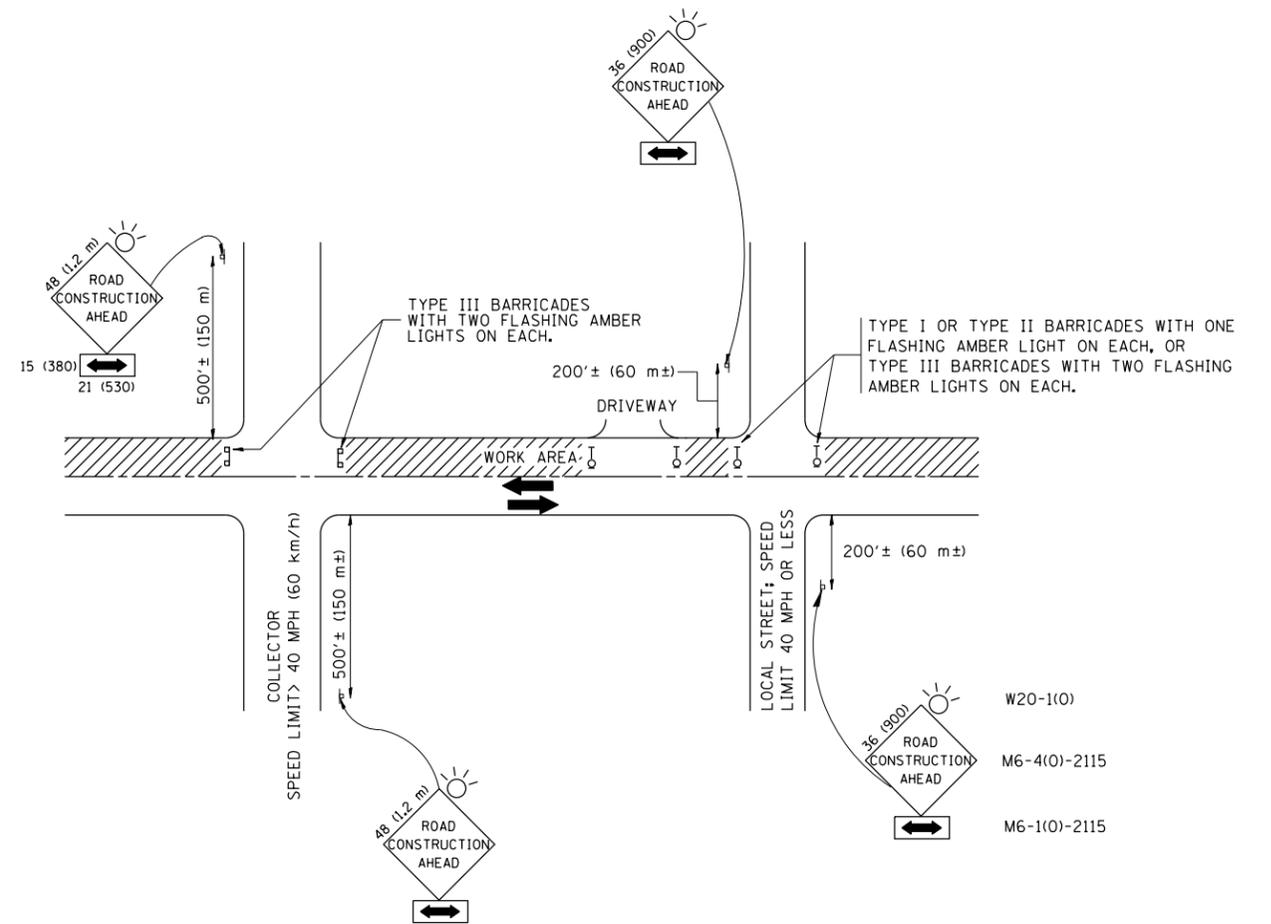
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

| | | | |
|--|-----------------------------|-----------------------|-----------------------------|
| FILE NAME = | USER NAME = bartonw | DESIGNED - M. DE YONG | REVISED - R. SHAH 10-25-94 |
| et:\pw\work\p1dot\bar tonw\10427922\HMA-Dupage-DistStd.dgn | | DRAWN - | REVISED - A. ABBAS 03-21-97 |
| | PLOT SCALE = 100.0000' / 1" | CHECKED - | REVISED - M. GOMEZ 04-06-01 |
| | PLOT DATE = 4/7/2015 | DATE - 06-13-90 | REVISED - R. BORO 01-01-07 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | |
|---|-------------------------|
| BUTT JOINT AND HMA TAPER DETAILS | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS |
| STA. | TO STA. |

| | | | | |
|---|------------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DUAPGE | 44 | 36 |
| BD400-05 BD32 | | CONTRACT NO. 62A84 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

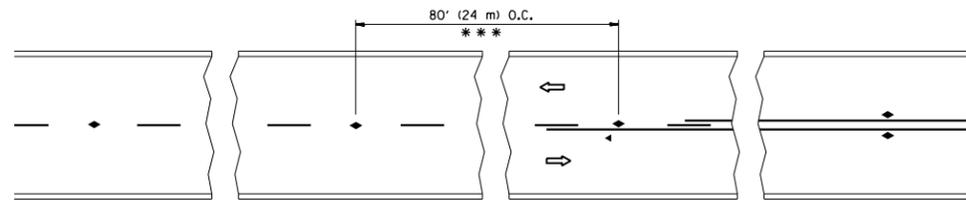
| | | | |
|--|-------------------------------|----------------|-------------------------------|
| FILE NAME = | USER NAME = bartonw | DESIGNED - LHA | REVISED - J. OBERLE 10-18-95 |
| c:\pwwork\pwwork\bartonw\d0427922\HMA-Dupage-DistStd.dgn | | DRAWN - | REVISED - A. HOUSEH 03-06-96 |
| | PLOT SCALE = 100.0000' / 1in. | CHECKED - | REVISED - A. HOUSEH 10-15-96 |
| | PLOT DATE = 4/7/2015 | DATE - 06-89 | REVISED - T. RAMMACH 01-06-00 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

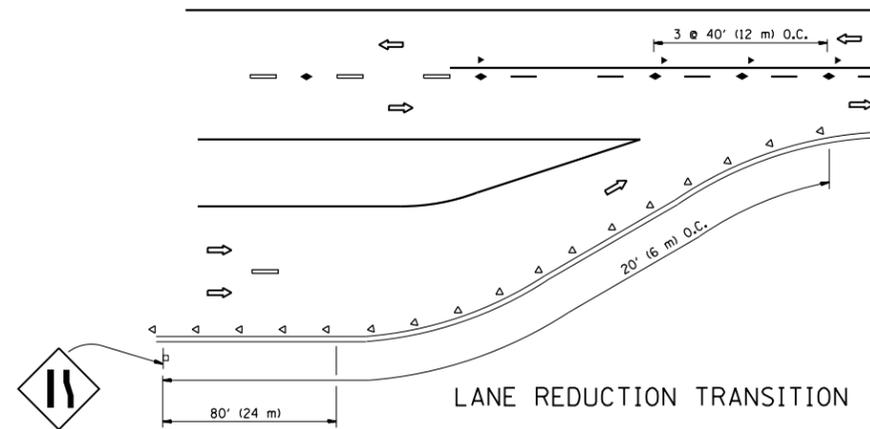
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|------------|--------|--------------------|-----------|
| VAR. | 2015-030R5 | DUAPGE | 44 | 37 |
| TC-10 | | | CONTRACT NO. 62A84 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

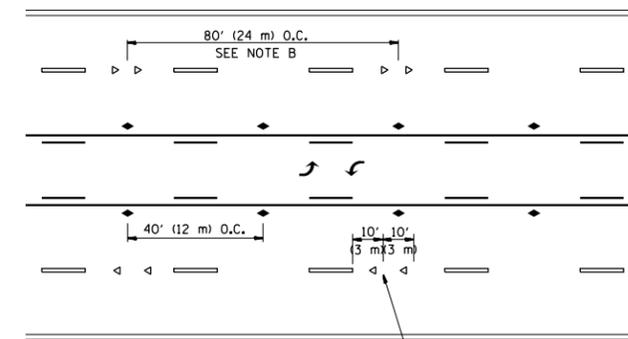


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

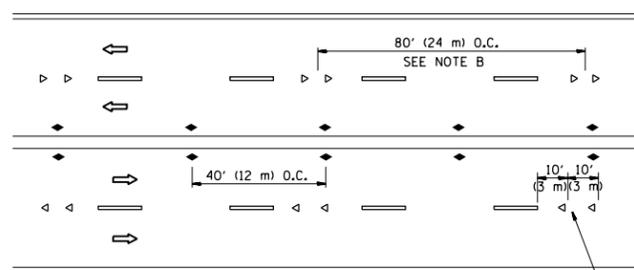
TWO-LANE/TWO-WAY



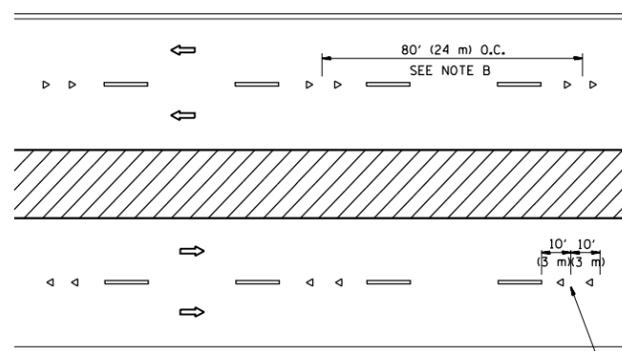
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

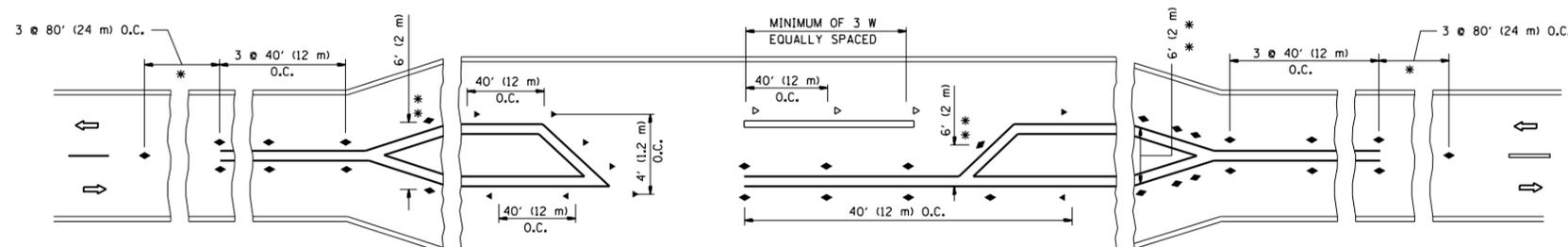
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

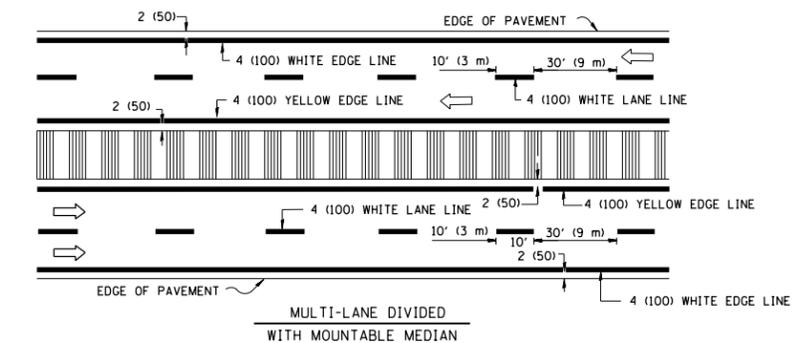
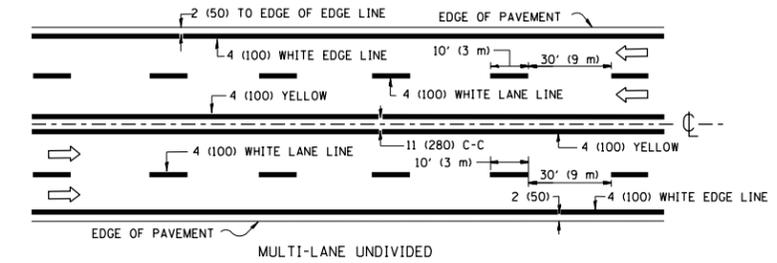
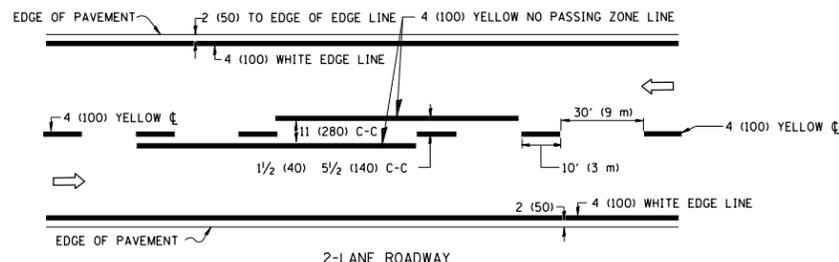
All dimensions are in inches (millimeters) unless otherwise shown.

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| | PLOT SCALE = 100.0000' / 1" = | CHECKED - | REVISED - T. RAMMACHER 01-06-00 |
| | PLOT DATE = 4/7/2015 | DATE - | REVISED - C. JUCIUS 09-09-09 |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

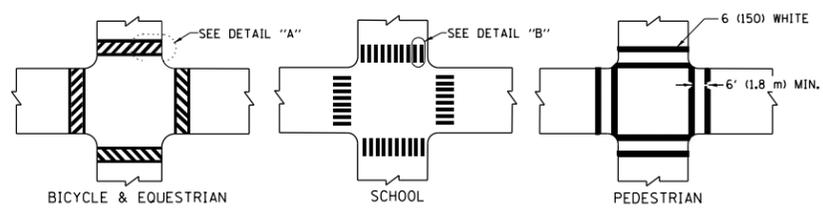
| | | | |
|--|-------------------------|------|---------|
| TYPICAL APPLICATIONS | | | |
| RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) | | | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

| | | | | |
|---|------------|--------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030RS | DUAPGE | 44 | 38 |
| TC-11 | | CONTRACT NO. 62A84 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

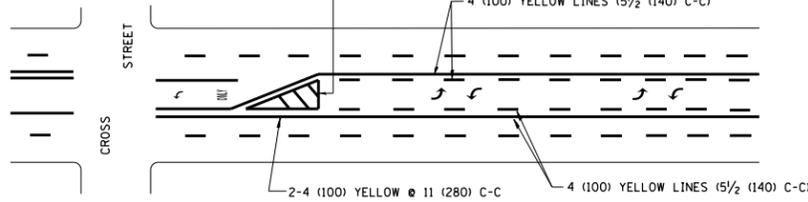
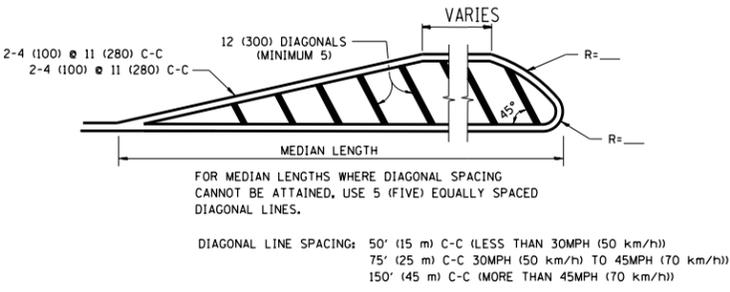
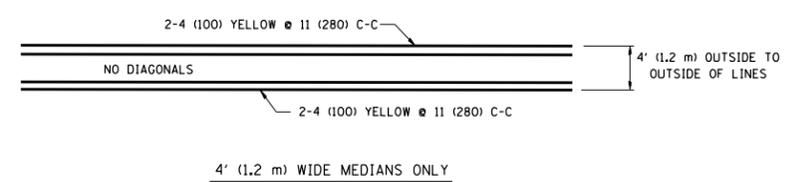


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

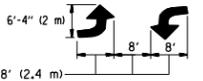
TYPICAL LANE AND EDGE LINE MARKING



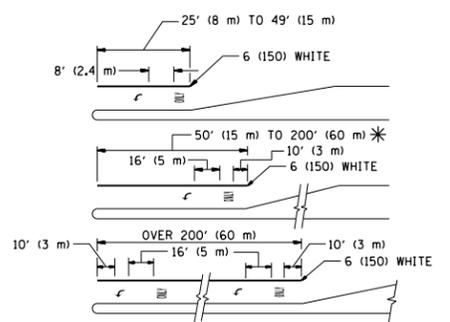
TYPICAL CROSSWALK MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



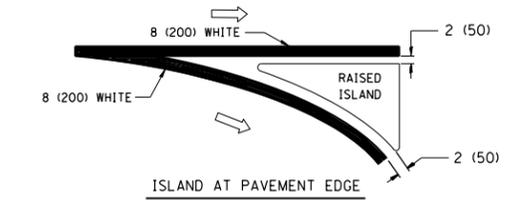
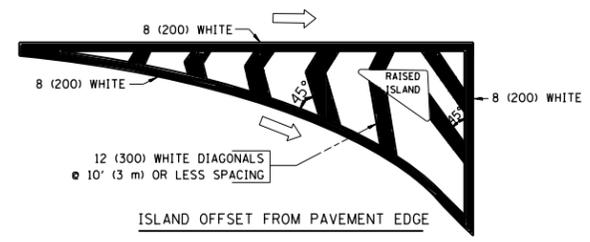
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|--|------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION | 4 (100) | SOLID | YELLOW | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE |
| NO PASSING ZONE LINES: FOR BOTH DIRECTIONS | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) MIN. LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²) EACH |
| SHOULDER DIAGONALS | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

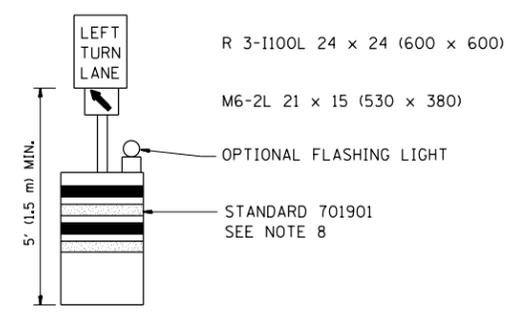
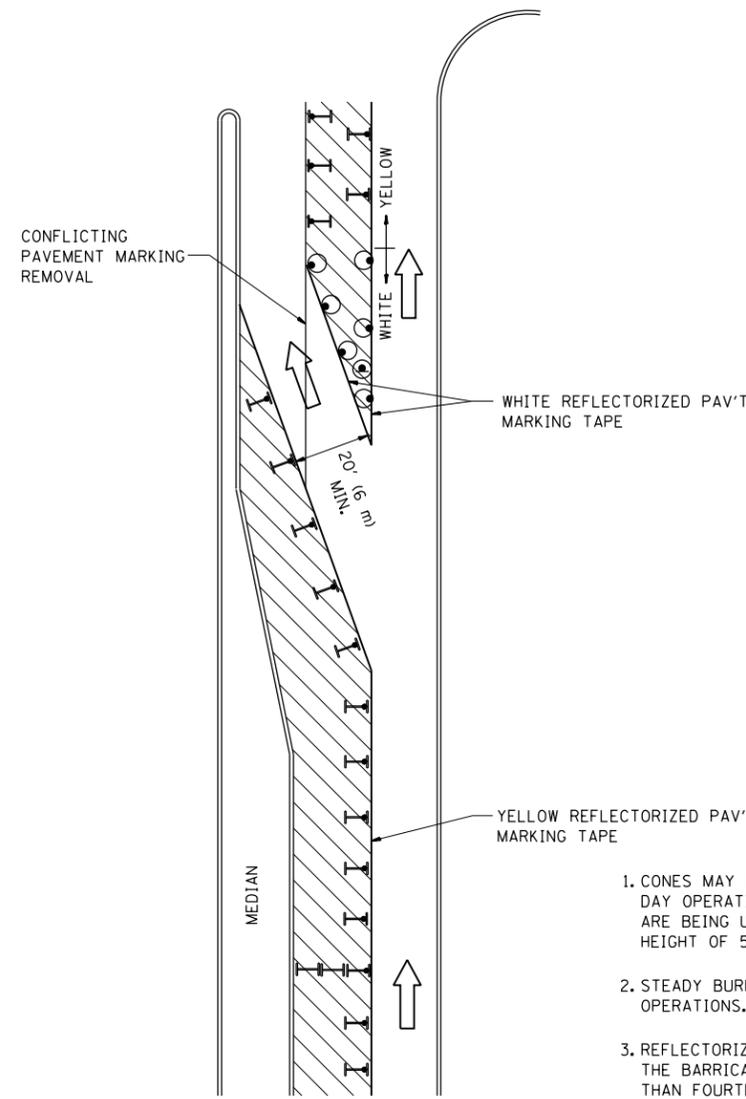
All dimensions are in inches (millimeters) unless otherwise shown.

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| es:\pw\work\p\dtd\bar tonw\10427922\HMA-Dupage-DistStd.dgn | | DRAWN - | REVISED -C, JUCIUS 09-09-09 |
| | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 4/7/2015 | DATE - 03-19-90 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | | |
|---------------------------|-------------------------|------|---------|
| DISTRICT ONE | | | |
| TYPICAL PAVEMENT MARKINGS | | | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

| | | | | |
|---|------------|--------------------|--------------|-----------|
| F.A. RT.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DUAPGE | 44 | 39 |
| TC-13 | | CONTRACT NO. 62A84 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

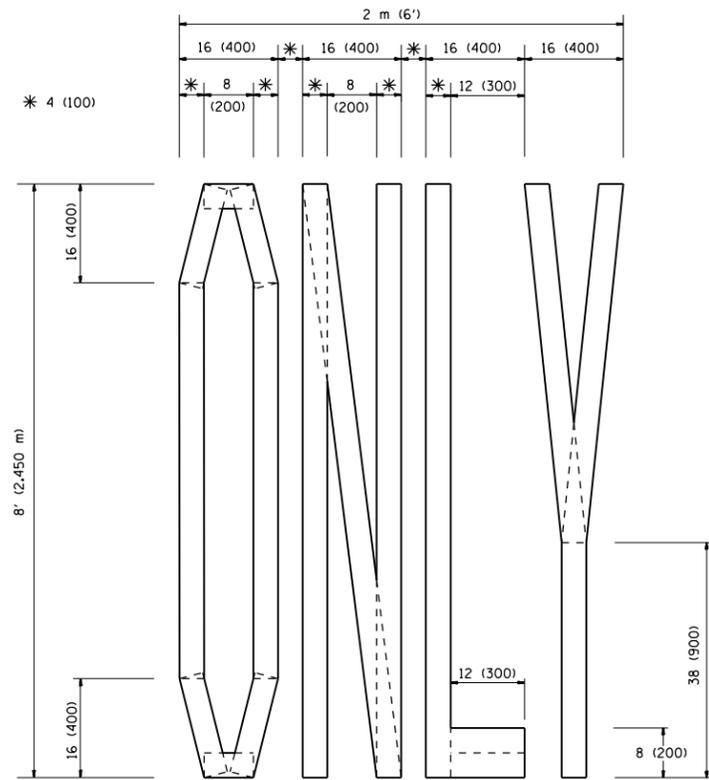
- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

| | | | |
|---|-------------------------------|--------------------------------|----------------------------|
| FILE NAME = | USER NAME = bartonw | REVISED -T. RAMMACHER 09-08-94 | REVISED - R. BORO 09-14-09 |
| et:\pw\work\p\dot\bar tonw\d0427922\HMA-Duppage-DistStd.dgn | | REVISED - A. HOUSEH 11-07-95 | REVISED - |
| | PLOT SCALE = 100.0000' / 1in. | REVISED - A. HOUSEH 10-12-96 | REVISED - |
| | PLOT DATE = 4/7/2015 | REVISED -T. RAMMACHER 01-06-00 | REVISED - |

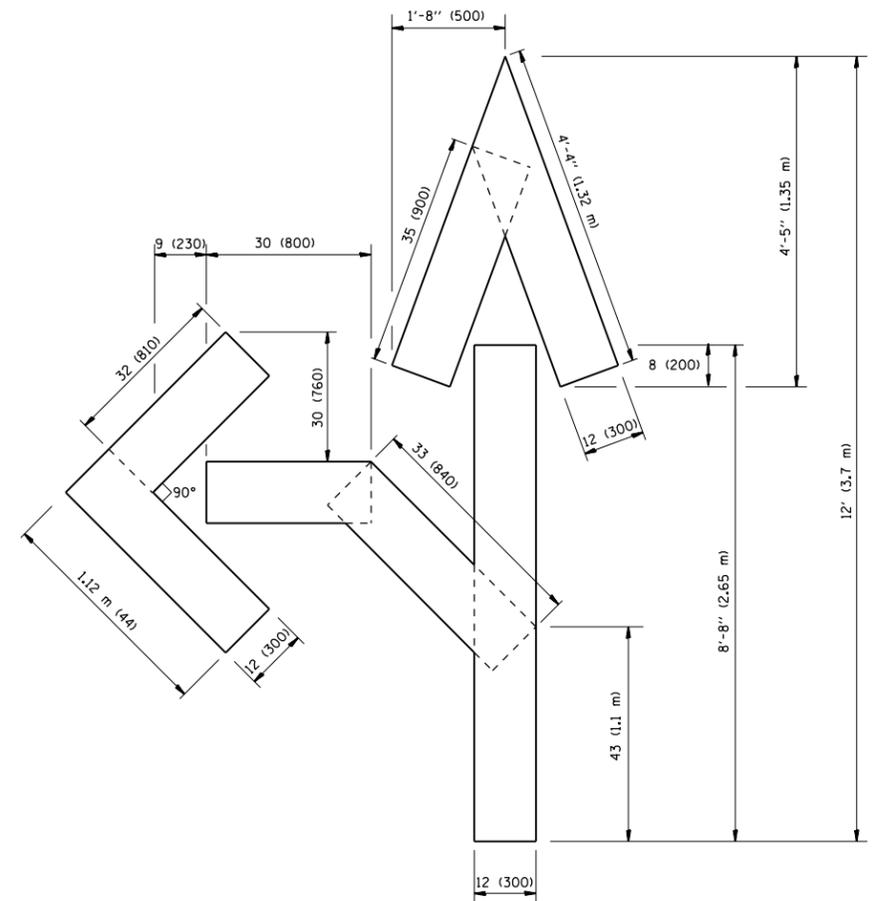
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) | | | |
|--|-------------------------|------|---------|
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

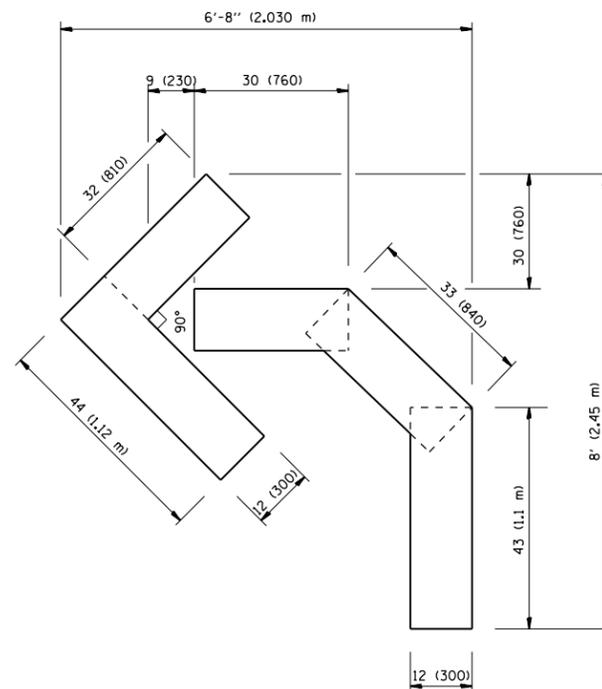
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| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DJAPGE | 44 | 40 |
| TC-14 | | CONTRACT NO. 62A84 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

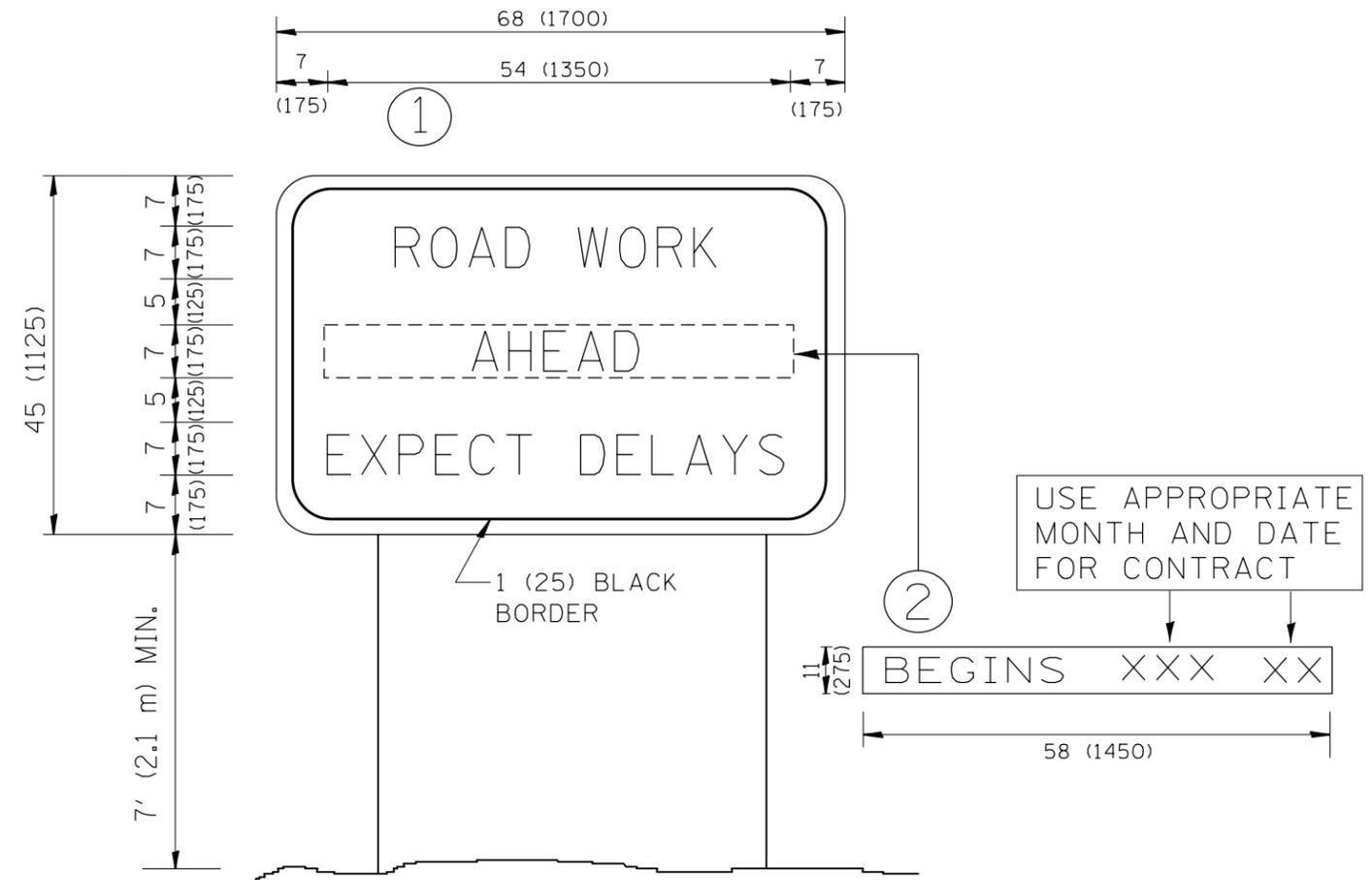
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| | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED -T. RAMMACHER 03-02-98 |
| | PLOT DATE = 4/7/2015 | DATE - 09-18-94 | REVISED -E. GOMEZ 08-28-00 |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---|------------|--------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DUPAGE | 44 | 41 |
| TC-16 | | CONTRACT NO. 62A84 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | |
|---|------------------------------|------------|---------------------------------|
| FILE NAME = | USER NAME = bartonw | DESIGNED - | REVISED - R. MIRS 09-15-97 |
| ct:\pwork\pwork\bartonw\0427922\HMA-Duplicate-DistStd.dgn | | DRAWN - | REVISED - R. MIRS 12-11-97 |
| | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - T. RAMMACHER 02-02-99 |
| | PLOT DATE = 4/7/2015 | DATE - | REVISED - C. JUCIUS 01-31-07 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

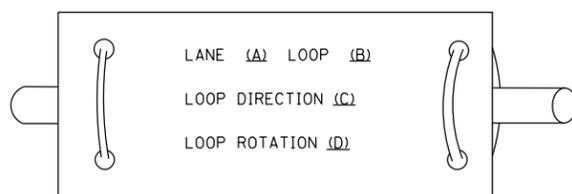
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---|------------|--------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DJAPGE | 44 | 42 |
| TC-22 | | CONTRACT NO. 62A84 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

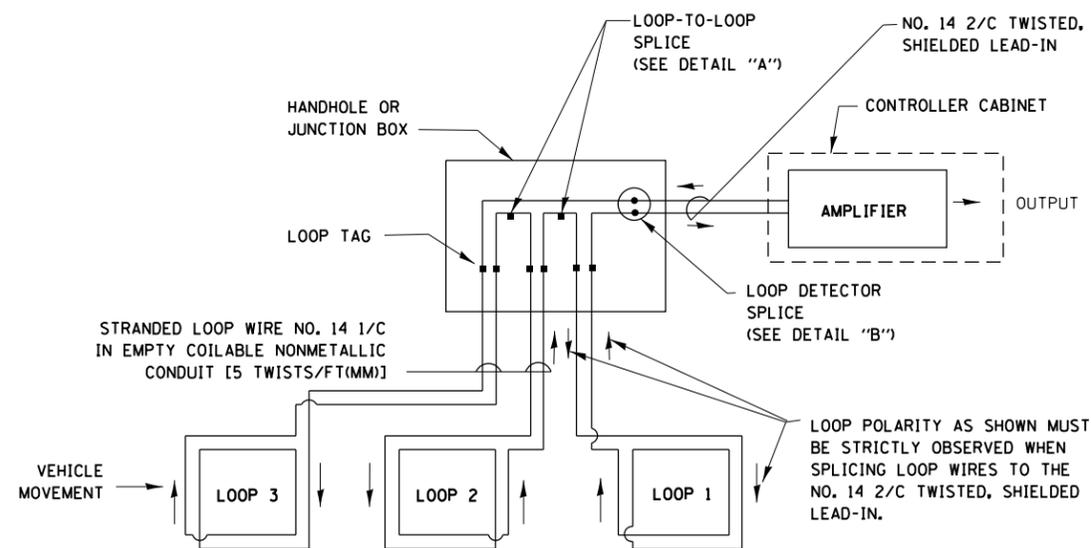
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

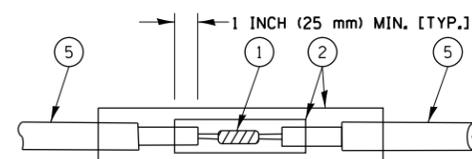


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

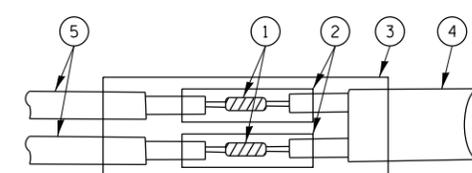


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

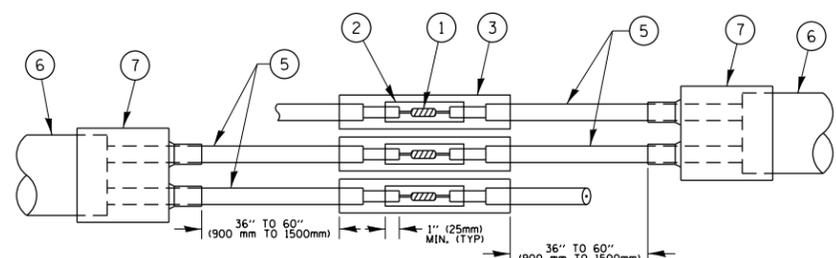


DETAIL "A"
LOOP-TO-LOOP SPLICE

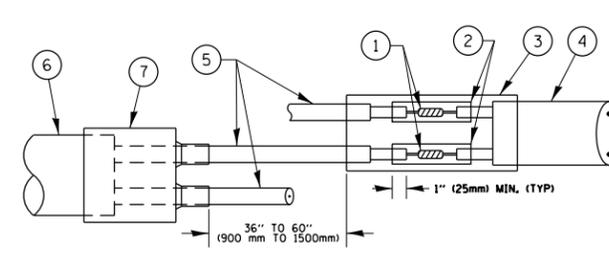


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

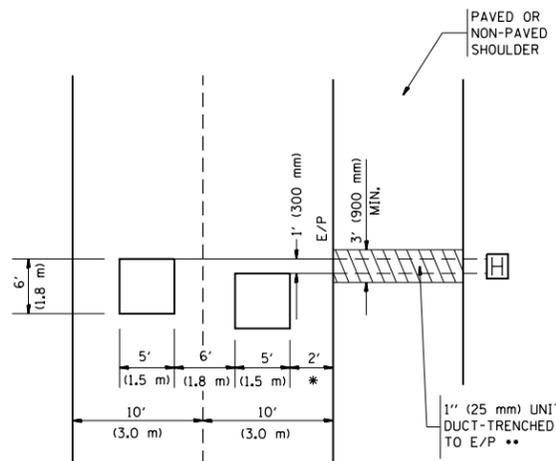
LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH, THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

| | | | | | | | | | | | | | |
|---|---------------------|-----------------|----------------------|---|--|-------------------------|------|-----------|--|------------|---------------------------|-----------|----|
| FILE NAME = | USER NAME = bartonw | DESIGNED - DAD | REVISED - DAG 1-1-14 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| c:\pwwork\pwwork\bar tonw\d0427922\HMA-Dupage-DistStd.dgn | | DRAWN - BCK | REVISED - | | SCALE: NONE | SHEET NO. 2 OF 7 SHEETS | STA. | TO STA. | VAR. | 2015-030R5 | DUAPGE | 44 | 43 |
| | | CHECKED - DAD | REVISED - | | | | | | TS-05 | | CONTRACT NO. 62A84 | | |
| | | DATE - 10-28-09 | REVISED - | | | | | | <small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small> | | | | |

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



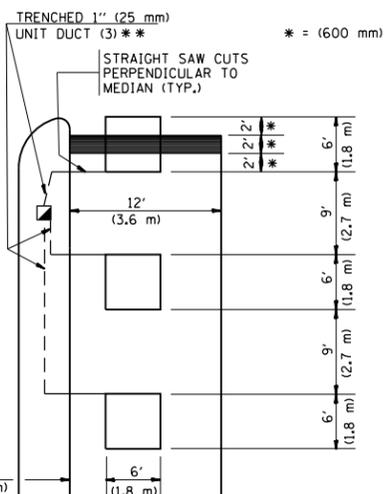
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



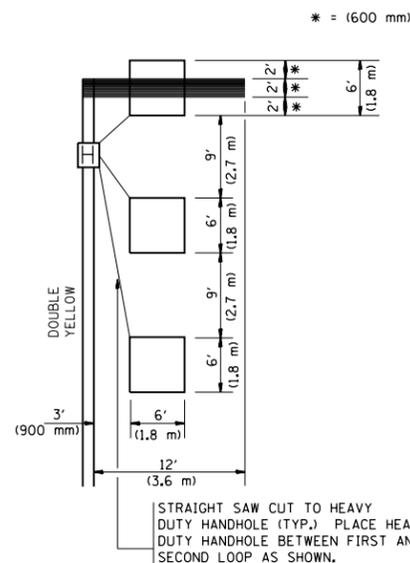
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

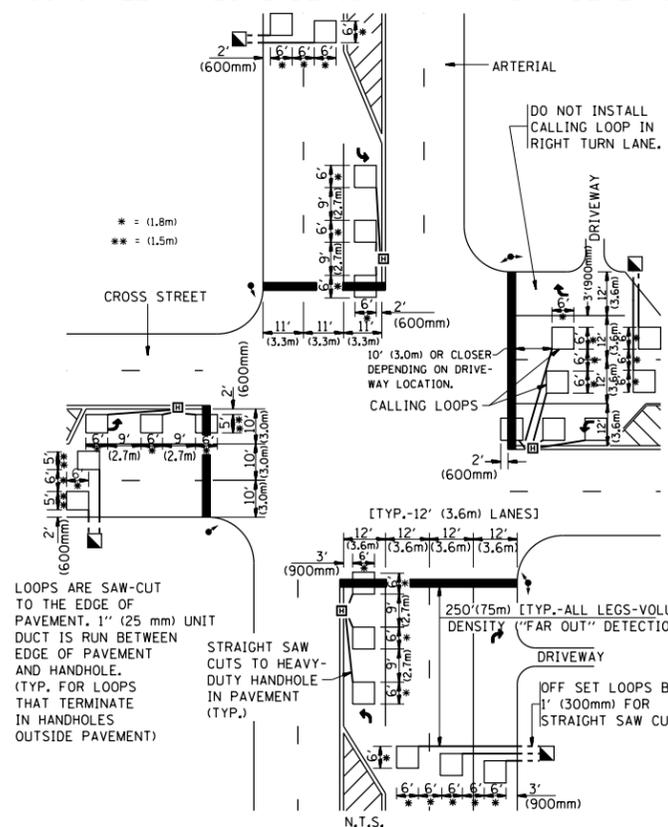
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)

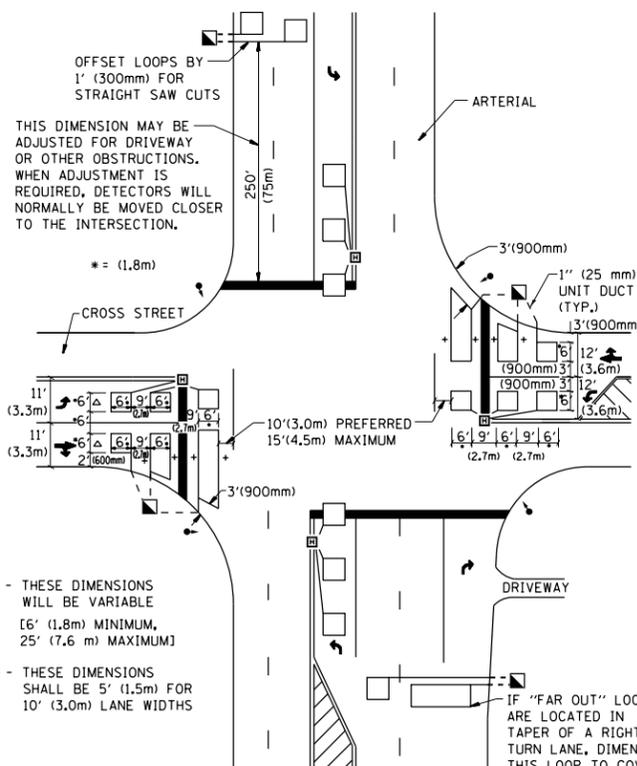


LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

△ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2
N.T.S.

| | | | |
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| FILE NAME = | USER NAME = bartonw | DESIGNED - | REVISED - |
| et:\pw\work\p1dot\bar tonw\10427922\HMA-Dupage-DistStd.dgn | | DRAWN - | REVISED - |
| | | CHECKED - R.K.F. | REVISED - |
| | | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---|------------|--------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2015-030R5 | DUAPGE | 44 | 44 |
| TS-07 | | CONTRACT NO. 62A84 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |