

**SCOPE OF WORK**

THE PROPOSED IMPROVEMENTS INCLUDE: REHABILITATION OF THE PAVED AREAS ADJACENT TO THE EXISTING T-HANGARS. THE REHABILITATION CONSISTS OF THE REMOVAL OF DISINTEGRATED (PCI OF ZERO) BITUMINOUS PAVEMENTS AND RECONSTRUCTING THEM IN ACCORDANCE WITH THE CONSTRUCTION PLANS. ASSOCIATED WORK INCLUDES EARTHWORK, DRAINAGE IMPROVEMENTS, AND INCIDENTALS.

BASE BID: INCLUDES PAVEMENT REMOVAL, SOIL STABILIZATION FABRIC DRAINAGE IMPROVEMENTS, EROSION CONTROL, TIEDOWN INSTALLATION, MARKING, SEEDING AND INCIDENTALS.

ALTERNATE BID A - PAVEMENT STRUCTURE SHALL CONSIST OF PORTLAND CEMENT CONCRETE (6" DEPTH) SURFACE ON A 4" CRUSHED AGGREGATE SUB-BASE.

ALTERNATE BID B - PAVEMENT STRUCTURE SHALL CONSIST OF 4" BITUMINOUS (SURFACE AND BASE COURSES) ON AN 8" CRUSHED AGGREGATE SUB-BASE.

**BITUMINOUS MATERIAL**

BITUMINOUS PAVEMENTS SHALL COMPLY WITH THE SPECIAL PROVISIONS FOR AIRCRAFT WEIGHING LESS THAN 60,000 POUNDS.

**EARTHWORK**

FOLLOWING REMOVAL OF THE EXISTING PAVEMENT, THE CONTRACTOR SHALL THEN BE REQUIRED TO EXCAVATE, REWORK AND COMPACT THE SUBGRADE PRIOR TO FILL PLACEMENT OR AGGREGATE BASE PLACEMENT IN ACCORDANCE WITH ITEM 152 OF THE SPECIFICATIONS AND TO THE SATISFACTION OF THE RESIDENT ENGINEER. EXCAVATION, REWORK, AND COMPACTION OF THE EXISTING EARTH SUBGRADE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION ALLOWED.

AREAS FOUND TO BE SOFT OR OTHERWISE UNSUITABLE FOR PLACEMENT OF SPECIFIED MATERIALS AND UNABLE TO REACH COMPACTION REQUIREMENTS SHALL BE UNDERCUT AND BACKFILLED WITH APPROVED MATERIAL TO THE SATISFACTION OF THE RESIDENT ENGINEER.

ONCE THE SUBGRADE IS ACCEPTED BY THE RESIDENT ENGINEER, THE CONTRACTOR SHALL PLACE SOIL STABILIZATION FABRIC AND CONSTRUCT THE PAVEMENT SECTION IN ACCORDANCE WITH THESE CONSTRUCTION PLANS, THE SPECIFICATIONS, AND THE SPECIAL PROVISIONS.

ALL EARTHWORK WILL BE PAID FOR UNDER ITEM AR152410 UNCLASSIFIED EXCAVATION, PER CUBIC YARD.

ALL DISTURBED AREAS OUTSIDE OF PAVEMENT WILL BE SEEDED AND MULCHED.

**CATCH BASIN**

THE PROPOSED CATCH BASIN SHALL BE IDOT TYPE A (STANDARD 602001-01) AND SHALL BE PRECAST REINFORCED CONCRETE. THE PRECAST REINFORCED CONCRETE FLAT SLAB TOP (STANDARD 602601-02) SHALL BE USED IN LIEU OF THE TAPERED TOP. THE CASTING SHALL BE NEENAH R-3492 OR APPROVED EQUAL. THE CATCH BASIN SHALL BE PAID FOR UNDER ITEM AR751600 CATCH BASIN, PER EACH AND SHALL INCLUDE ALL WORK NECESSARY TO COMPLETE THE ITEM.

**EROSION CONTROL BLANKET NOTES**

AN EROSION CONTROL MATERIAL (EXCELSIOR BLANKET) WILL BE INSTALLED AT THE LOCATIONS SHOWN ON THESE CONSTRUCTION PLANS AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM AR156531 EROSION CONTROL BLANKET, PER SQUARE YARD.

**BUTT JOINT CONSTRUCTION (ALTERNATE BID B)**

THE PROPOSED BUTT JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ITEM AR401655 BUTT JOINT CONSTRUCTION AS STATED IN THE SUPPLEMENTAL SPECIFICATION AND RECURRING SPECIAL PROVISIONS.

THIS ITEM SHALL CONSIST OF REMOVING THE EXISTING BITUMINOUS PAVEMENT SURFACE BY MILLING IN ORDER TO CONSTRUCT A BUTT JOINT FOR THE TRANSITION OF THE PROPOSED BITUMINOUS PAVEMENT AT THE LOCATIONS AS SHOWN ON THE PLANS.

THE PAVEMENT SURFACE WILL BE MILLED TO ACCOMMODATE A MINIMUM DEPTH OF 1.5 INCHES OF BITUMINOUS SURFACE AT THE BUTT JOINT. ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

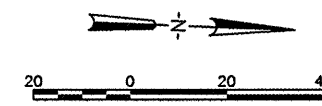
IF A TRUE VERTICAL FACE IS NOT ESTABLISHED ALONG THE BUTT JOINT FROM THE MILLING OPERATIONS, THE CONTRACTOR IS REQUIRED TO SAWCUT THE JOINT. THE SAWING, IF NECESSARY, SHALL BE CONSIDERED INCIDENTAL TO THE BUTT JOINT CONSTRUCTION AND NO ADDITIONAL COMPENSATION ALLOWED.

PRIOR TO APPLYING THE BITUMINOUS MATERIAL ALL MILLED MATERIAL WILL BE BROOMED AND BLOWN CLEAN AND A BITUMINOUS TACK COAT APPLIED. THE VERTICAL FACE WILL BE PAINTED WITH A LIQUID ASPHALT.

ALL WORK RELATED TO THE CONSTRUCTION OF THE BUTT JOINT WILL BE PAID FOR UNDER ITEM AR401655 BUTT JOINT CONSTRUCTION, PER SQUARE YARD.

**LEGEND**

- EXISTING PAVEMENT
- PROPOSED IMPROVEMENTS
- PROPOSED EROSION CONTROL BLANKET
- EXISTING BUILDINGS
- EXISTING FENCE



COORDINATE DATA TABLE					
NO.	STATION	OFFSET	NORTHING	EASTING	DESCRIPTION
①	10+00.00	℄	1104049.0959	1169159.8447	BEGIN WEST ALIGNMENT
②	13+99.09	℄	1104447.9063	1169144.8889	WEST ALIGNMENT/PAVEMENT EDGE
③	10+00.00	℄	1104053.0677	1169265.7553	BEGIN CENTER ALIGNMENT
④	13+99.09	℄	1104451.8780	1169250.7996	CENTER ALIGNMENT/PAVEMENT EDGE
⑤	10+00.00	℄	1104057.1661	1169375.0439	BEGIN EAST ALIGNMENT
⑥	13+69.76	℄	1104426.6694	1169361.1872	EAST ALIGNMENT/PAVEMENT EDGE
⑦	13+69.76	11.62' RT.**	1104423.0064	1169263.5081	PAVEMENT EDGE
⑧	13+99.09	11.62' RT.**	1104452.3134	1169262.4091	PAVEMENT EDGE

\*\*OFFSET REFERENCED FROM CENTER ALIGNMENT

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. AT 1-800-892-0123 FOR UTILITY INFORMATION.

BY	
REVISION	
DATE	
<b>EDGAR COUNTY AIRPORT PARIS, ILLINOIS</b>	
A.I.P. PROJ.: 3-17-0077-B11	
IL PROJ.: PRG-3889	
TEL. Project No. 0840209_0800	
Filename R-121CON.DWG	
Scale 1"=20'	
Date 04/17/09	
LAYOUT 03/05/09	
DRAWN MLH 03/06/09	
REVIEWED JDW 03/18/09	
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REMOVE AND RECONSTRUCT T-HANGAR TAXIWAYS	PROPOSED CONSTRUCTION PLAN
<b>6</b> 6 of 18 sheets	