

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION  
**PLANS FOR PROPOSED  
 FEDERAL AID HIGHWAY**

FAU 1381 (E FULLERTON AVENUE)  
 FAU 2690 (NORTH WOLF ROAD) TO CORPORATE LIMITS (GENEVA AVENUE)  
 SECTION NO.: 20-00097-00-RS  
 PROJECT NO.: LGC9(552)  
 RESURFACING  
 COOK COUNTY  
 CITY OF NORTHLAKE  
 JOB NO.: C-91-097-21

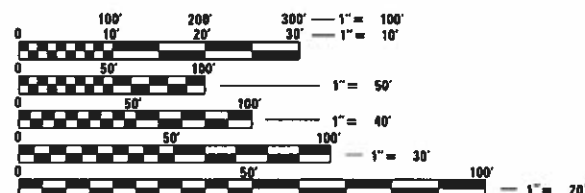
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1381	20-00097-00-RS	COOK	21	1
		ILLINOIS	CONTRACT NO. 61H11	

FOR INDEX OF SHEETS, SEE SHEET NO. 2  
 FOR LIST OF STATE STANDARDS, SEE SHEET NO. 2



DESIGN DESIGNATION ROUTE : MINOR COLLECTOR  
 DESIGN SPEED = 25 M.P.H.  
 POSTED SPEED = 25 M.P.H.

TRAFFIC DATA: 2014 2030  
 ADT = 1,225 1,225



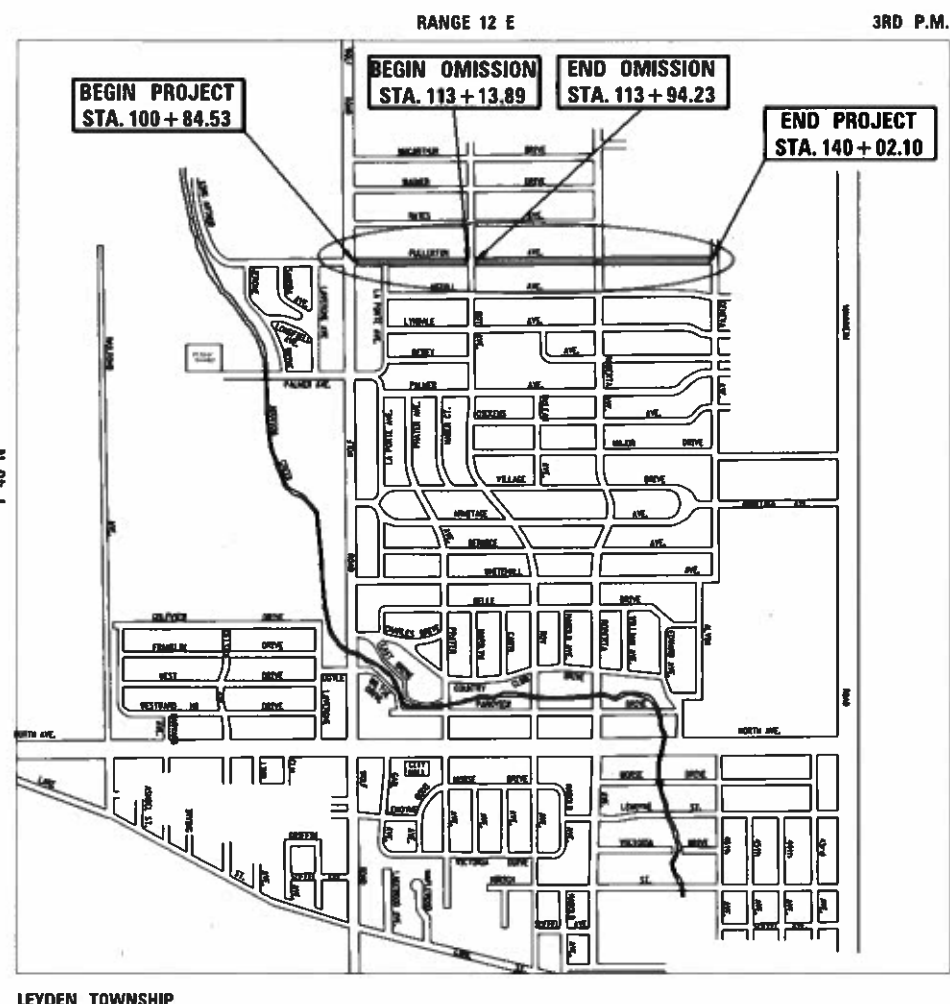
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

**CB** CHRISTOPHER B. BURKE ENGINEERING, LTD.  
 9575 W. Higgins Road, Suite 600  
 Rosemont, Illinois 60018  
 (847) 823-0500

PROFESSIONAL DESIGN FIRM NO.: 184-00175  
 EXPIRATION DATE: APRIL 30, 2023

**VAL M. RACICH**  
 REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS  
 062-049622  
 VAL M. RACICH, P.E.  
 ILLINOIS REGISTRATION No. 062-049622  
 EXPIRATION DATE: 11/2021



N  
 N.T.S.

CITY OF NORTHLAKE  
 APPROVED [Signature]  
 February 24, 2021  
 CITY OF NORTHLAKE, MAYOR  
 PASSED [Signature]  
 APRIL 2, 2021  
 DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS  
 RELEASED FOR BID BASED ON LIMITED REVIEW  
 APRIL 2, 2021  
 REGIONAL ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

LOCATION MAP  
 GROSS LENGTH OF PROJECT = 3,917.6 LINEAL FEET (0.74 MI.)  
 NET LENGTH OF PROJECT = 3,637.2 LINEAL FEET (0.73 MI.)

FEDERAL AID PROGRAM AND OFFICE ENGINEER: CARMEN E. RAMOS SCHAUMBURG, IL

CONTRACT NO. 61H11

# GENERAL NOTES

## INDEX OF SHEETS

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12 - 21	DISTRICT 1 DETAILS

## HIGHWAY STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS
424026-03	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C & D PATCHES
604001-05	FRAME AND LIDS TYPE 1
606001-07	CONC. CURB TYPE B AND COMB. CONC CURB AND GUTTER
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURES, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS

## SPECIFICATIONS, STANDARDS, SPECIAL PROVISIONS, AND PERMITS

- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2021; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (IMUTCOD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" 2020 EIGHTH EDITION; THE "ILLINOIS URBAN MANUAL " AND THE "ILLINOIS URBAN MANUAL FIELD MANUAL FOR INSPECTION OF EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES"; THE "AMERICANS WITH DISABILITIES ACT OF 1990 ACCESSIBILITY GUIDELINES"; THE "DRAFT" REHABILITATION ACT OF 1973 (SECTION 504); THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES; THE "DETAILS" IN THE PLANS; AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.
- ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT HIGHWAY STANDARD.
- CODES OF THE IEPA TITLE 35, AND O.S.H.A. SHALL BE ADHERED TO FOR THE CONSTRUCTION OF THIS PROJECT. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE ADHERENCE TO THESE (NOT THE CITY OF NORTHLAKE OR CHRISTOPHER B. BURKE ENGINEERING).
- ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.
- THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHEETING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE AGENCY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SIGNS AND WARNING DEVICES TO INFORM AND PROTECT THE PUBLIC.

## GENERAL NOTES

- THE CITY OF NORTHLAKE (OWNER) AND OWNER'S REPRESENTATIVE SHALL BE NOTIFIED IN WRITING A MINIMUM OF THREE (3) FULL BUSINESS DAYS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.
- THE LOCATION OF EXISTING DRAINAGE STRUCTURES, UNDERGROUND UTILITIES, SUCH AS WATER MAINS, SEWERS, GAS LINES, ETC., AS SHOWN ON THE PLANS, HAS BEEN DETERMINED FROM THE BEST AVAILABLE INFORMATION AND IS GIVEN FOR THE CONVENIENCE OF THE CONTRACTOR. HOWEVER, THE CITY AND THE ENGINEER DO NOT GUARANTEE THEIR ACCURACY AND DO NOT ASSUME RESPONSIBILITY IN THE EVENT THAT DURING CONSTRUCTION, UTILITIES OTHER THAN THOSE SHOWN MAY BE ENCOUNTERED, AND THAT THE ACTUAL LOCATION OF THOSE WHICH ARE SHOWN MAY BE DIFFERENT FROM THE LOCATION AS SHOWN ON THE DRAWINGS. THE EXACT HORIZONTAL AND VERTICAL LOCATIONS SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
- NOTIFY THE ENGINEER WITHOUT DELAY OF ANY DISCREPANCIES BETWEEN THE SURFACE AND/OR SUBSURFACE FEATURES INDICATED ON THE DRAWINGS AND EXISTING FIELD CONDITIONS.
- THE CONTRACTOR SHALL CALL "J.U.I.I.E." AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, CABLE AND GAS FACILITIES AND THE CITY OF NORTHLAKE FOR FIELD LOCATIONS OF BURIED WATER, SANITARY AND STORM FACILITIES. TWO (2) WORKING DAYS IN ADVANCE NOTICE IS REQUIRED.
- THE CONTRACT DOCUMENTS ARE NOT INTENDED TO SHOW EVERY AND ALL DETAILS OF WORK TO BE PERFORMED OR EQUIPMENT TO BE SUPPLIED. THE INTENT OF THE CONTRACT DOCUMENTS IS TO ILLUSTRATE THE CONCEPTUAL DESIGN AND LAYOUT. THE CONTRACTOR SHALL BE KNOWLEDGEABLE AND REGULARLY ENGAGED IN THE TYPE OF WORK DESCRIBED BY THESE CONTRACT DOCUMENTS AND SHALL BE RESPONSIBLE FOR UNDERSTANDING THEIR INTENT. ANY WORK TO BE PERFORMED OR ITEM OF EQUIPMENT TO BE SUPPLIED WHICH IS NOT SPECIFICALLY CALLED FOR BY THESE CONTRACT DOCUMENTS BUT WHICH IS NECESSARY TO PROVIDE A COMPLETE AND SUCCESSFUL WORKING SYSTEM SHALL BE INCLUDED IN THE CONTRACTOR'S SCOPE OF WORK.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ALL MATERIAL QUANTITIES AND APPRISE HIMSELF/HERSELF OF ALL CONDITIONS. THE CONTRACT PRICE SUBMITTED BY THE CONTRACTOR SHALL BE CONSIDERED AS THE TOTAL COST FOR THE COMPLETE PROJECT.
- THE WORK PERFORMED UNDER THIS CONTRACT SHALL IN NO WAY INTERFERE WITH THE NORMAL OPERATION OF ANY EXISTING UTILITY SERVICE. THE CONTRACTOR SHALL FURNISH ALL NECESSARY ITEMS OF EQUIPMENT REQUIRED TO MAINTAIN SUCH NORMAL OPERATION AT ALL TIMES.
- AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019. ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING.

## GENERAL NOTES (CONTINUED)

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEANS/METHODS AND TECHNIQUES OF CONSTRUCTION. OWNER'S REPRESENTATIVE/ENGINEER'S REVIEW OF SAME DOES NOT RELIEVE CONTRACTOR OF THIS RESPONSIBILITY. CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR SAFETY AND JOB SITE SAFETY.
- THE GENERAL OR SUB-CONTRACTOR SHALL LIMIT THEIR CONSTRUCTION AND STAGING ACTIVITIES TO THE AREAS DESIGNATED ON THE PLANS. ANY DAMAGE TO AREAS BEYOND THESE LIMITS SHALL BE RETURNED TO THE STATE IT WAS FOUND PRIOR TO NEW CONSTRUCTION, EXCEPT WHERE NEW WORK IS SHOWN, BY THE CONTRACTOR TO THE SATISFACTION OF THE OWNER AND ENGINEER.
- PRECONSTRUCTION MEETING WILL BE HELD AT LEAST 7 DAYS PRIOR TO START OF CONSTRUCTION.
- BEFORE INITIAL ACCEPTANCE BY THE CITY, ALL WORK SHALL BE INSPECTED AND APPROVED BY THE CITY OR ITS REPRESENTATIVE.

## DEMOLITION NOTES

- THE CONTRACTOR WILL BE REQUIRED TO RELOCATE OR REMOVE AND REINSTALL SIGNS WHICH INTERFERE WITH CONSTRUCTION OPERATIONS, AND TO TEMPORARILY RESET ALL SUCH SIGNS DURING CONSTRUCTION OPERATIONS. ANY EXISTING SIGN DAMAGED DURING THE REMOVAL AND REPLACEMENT PROCESS SHALL BE REPLACED BY THE CONTRACTOR.
- ALL SAWCUTTING SHALL BE FULL DEPTH TO PROVIDE A CLEAN EDGE TO MATCH NEW CONSTRUCTION. MATCH EXISTING ELEVATIONS AT POINTS OF CONNECTION FOR NEW AND EXISTING PAVEMENT, CURB, SIDEWALKS, ETC. ALL SAWCUT LOCATIONS INDICATED ON THE PLANS ARE APPROXIMATE AND MAY BE ADJUSTED IN THE FIELD TO ACCOMMODATE CONDITIONS, JOINTS, MATERIAL TYPE, ETC. REMOVE MINIMUM AMOUNT NECESSARY FOR INSTALLATION OF PROPOSED IMPROVEMENTS.
- ALL SAWCUTTING SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED. ALL CONCRETE SAWCUTTING SHALL UTILIZE A "WET CUTTING METHOD" AND SHALL BE THOROUGHLY CLEANED AT THE END OF EACH WORKING DAY.
- ALL SAWCUTTING SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL.
- ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENT IS TO BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER.
- KEEP ALL CITY STREETS FREE AND CLEAR OF CONSTRUCTION RELATED DIRT, DUST AND/OR DEBRIS.
- CONSTRUCTION SITE IS TO BE CLEANED BY THE CONTRACTOR AFTER EACH PHASE OF WORK (I.E.: SEWER, CONCRETE, PAVING, SODDING, ETC.)
- NO BURNING OR INCINERATION OF RUBBISH WILL BE PERMITTED ON SITE.

## UTILITY NOTES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.
- THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND STRUCTURES AND OTHER UTILITY LINES AND STRUCTURES ARE APPROXIMATE, AND THE CITY DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AS COORDINATED WITH THE CITY.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES (48 HOURS NOTIFICATION IS REQUIRED.)
- WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS.
- ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR.
- ADJUST MANHOLE OR VAULT RIM ELEVATIONS TO FINAL GRADE AT TIME OF FINAL PAVING OR LANDSCAPING.
- STRUCTURE ADJUSTMENTS: PRECAST CONCRETE ADJUSTMENT RINGS ARE NOT TO EXCEED 12 INCHES IN OVERALL HEIGHT AND SHALL BE USED IF AN ADJUSTMENT TO THE FINISHED GRADE ESTABLISHED BY THE RESIDENT ENGINEER IS NECESSARY. MAXIMUM TWO RINGS.
- THE FINAL RING (AND ALL RINGS UNDER 2") ON ALL NEW STRUCTURES AND STRUCTURE ADJUSTMENTS SHALL BE RUBBER. MASTIC SHALL BE PLACED BETWEEN EACH RING. NO EZ-STIK SHALL BE USED.
- THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT WATER DEPARTMENT FOR THEM TO TURN VALVES OR OPERATE HYDRANTS. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

## GRADING AND PAVING NOTES

- HOT-MIX ASPHALT BINDER COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACK-FILLED TO THE SATISFACTION OF THE ENGINEER.
- THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACE, BINDER, OR BASE UPON WHICH THE HOT-MIX ASPHALT MATERIALS ARE PLACED. THE THICKNESSES SHOWN ON THE PLANS ARE THE MINIMUM ACCEPTABLE THICKNESSES.
- CURB AND GUTTER TO BE REMOVED AT DRIVEWAYS SHALL BE REPLACED WITH DEPRESSED CURB AND GUTTER.

## GRADING AND PAVING NOTES (CONTINUED)

- THE GRADING AND CONSTRUCTION OF PROPOSED IMPROVEMENTS SHALL BE COMPLETED IN A MANNER WHICH WILL ALLOW FOR POSITIVE DRAINAGE, AND NOT CAUSE PONDING OF STORMWATER ON THE SURFACE OF PROPOSED IMPROVEMENTS.
- MAXIMUM SIDEWALK CROSS SLOPES SHALL NOT EXCEED 2.00%. MAXIMUM LONGITUDINAL SIDEWALK SLOPE SHALL NOT EXCEED 5.00%. MAXIMUM LONGITUDINAL CURB RAMP SLOPE SHALL NOT EXCEED 8.30% AND SHALL BE CONSTRUCTED IN CONFORMANCE WITH AMERICAN WITH DISABILITIES ACT (ADA) STANDARDS FOR ACCESSIBLE DESIGN, PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) REQUIREMENTS AND THE "DETAILS" PROVIDED UNLESS OTHERWISE NOTED.
- TWO (2) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTER IN THE FIELD UNLESS OTHERWISE SHOWN.

## LANDSCAPING NOTES

- ALL LANDSCAPED AREAS DISTURBED BY CONSTRUCTION ACTIVITY SHALL BE FINE GRADED, RESPREAD WITH 4 INCHES (MIN.) TO 12 INCHES (MAX.) TOPSOIL, AND SODDED UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- CONTRACTOR SHALL NOT PLACE SOD UNTIL THE TEMPERATURE IS 80° OR LESS AND THE FORECAST FOR THE NEXT 7 DAYS SHOWS TEMPERATURES OF 80° OR LESS. IF ALL OTHER PAY ITEMS ARE COMPLETED, THE CONTRACTOR WILL NOT BE CHARGED WORKING DAYS FOR DELAYS IN PARKWAY RESTORATION DUE TO TEMPERATURE.
- WHEN DIRECTED BY THE ENGINEER, SUPPLEMENTAL WATERING SHALL BE APPLIED TO ALL SODDED AREAS PRIOR TO FINAL ACCEPTANCE AT A RATE SPECIFIED BY THE ENGINEER.
- THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR SHALL BE RESTORED BY THE CONTRACTOR.
- CONTRACTOR SHALL TAKE PRECAUTION BY PRESERVING EXISTING TREES WITHIN THE RIGHT OF WAY. IF ANY DAMAGE OCCURS, TREES SHALL BE REPLACED IN KIND PER ARTICLE 201.07 REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL REQUIREMENTS STATED HEREIN.

## SOIL EROSION AND SEDIMENTATION CONTROL NOTES

- ILLINOIS URBAN MANUAL SHALL GOVERN ALL SOIL EROSION AND SEDIMENT CONTROL, AND RELATED WORK.
- SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF CONSTRUCTION ACTIVITY.
- ALL STORM SEWER STRUCTURES THAT ARE, OR WILL BE, FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED, FILTERED, OR OTHERWISE TREATED TO REMOVE SEDIMENT. INLET PROTECTORS, IN CONFORMANCE WITH CITY REQUIREMENTS SHALL BE USED IN PAVED AREAS TO PREVENT SILTATION AND DISCHARGE INTO WATERWAYS.
- TEMPORARY SOIL STABILIZATION SHALL BE APPLIED TO TOPSOIL STOCKPILES AND DISTURBED AREAS, WHERE CONSTRUCTION ACTIVITY WILL NOT OCCUR FOR A PERIOD OF MORE THAN 14 CALENDER DAYS. TEMPORARY MEASURES SHALL BE APPLIED WITHIN 7 CALENDER DAYS OF THE END OF ACTIVE HYDROLOGIC DISTURBANCE. THE SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED ON A CONTINUING BASIS UNTIL THE DISTURBED AREA(S) IS PERMANENTLY STABILIZED AND ALL INSPECTIONS ARE COMPLETE. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 14 DAYS AFTER COMPLETION OF FINAL GRADING OF SOIL.
- ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.
- FINAL SITE STABILIZATION IS DEFINED BY THE EPA GENERAL PERMIT AS MEANING THAT ALL SOIL DISTURBING ACTIVITIES AT THE SITE HAVE BEEN COMPLETED, AND THAT A UNIFORM PERENNIAL VEGETATIVE COVER WITH A DENSITY OF 70 PERCENT OF THE COVER FOR UNPAVED AREAS NOT COVERED BY PERMANENT STRUCTURES HAS BEEN ESTABLISHED OR EQUIVALENT PERMANENT STABILIZATION MEASURES HAVE BEEN EMPLOYED.
- ALL TEMPORARY AND PERMANENT SEDIMENT AND EROSION CONTROL MEASURES MUST BE MAINTAINED, REPAIRED, AND INSPECTED IN CONFORMANCE WITH ALL APPLICABLE IEPA-NPDES PHASE II REQUIREMENTS.
- REPORT RELEASES OF REPORTABLE QUANTITIES OF OIL OR HAZARDOUS MATERIALS IF THEY OCCUR IN ACCORDANCE WITH THE IEPA NPDES REQUIREMENTS.
- ALL CONCRETE WASHOUTS SHALL CONFORM TO THE "TEMPORARY CONCRETE WASHOUT FACILITY" STANDARDS (CODE 954) OF THE ILLINOIS URBAN MANUAL, LATEST EDITION.
- ANY AND ALL DEWATERING REQUIRED TO KEEP EXCAVATIONS DRY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- DEWATERING OF EXCAVATIONS SHALL BE PERFORMED IN A MANNER SUCH AS THROUGH THE USE OF FILTER BAGS OR POLYMER TREATED DEWATERING SWALES, SO AS TO NOT DISCHARGE SEDIMENT LADEN WATER INTO STORM SEWERS TRIBUTARY TO OPEN WATER.

## INTENDED SEQUENCE OF SEDIMENT AND EROSION CONTROL MEASURES

- INSTALL TEMPORARY INLET PROTECTORS BEFORE COMMENCEMENT OF ANY GROUND DISTURBING ACTIVITY.
- PROVIDE TREE PROTECTION FENCING PRIOR TO CONSTRUCTION OPERATIONS. MAINTAIN THROUGHOUT CONSTRUCTION ACTIVITY.
- WHEN PRACTICAL, SCHEDULE CONSTRUCTION ACTIVITIES TO LIMIT THE POTENTIAL IMPACT FROM SEASONAL CLIMATE CHANGES OR WEATHER EVENTS.
- REMOVE ANY ACCUMULATED SDIMENT AND/OR CONSTRUCTION DEBRIS FROM EXISTING INFRASTRUCTURE AND PUBLIC IMPROVEMENTS.
- REMOVE TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES FOLLOWING FINAL STABILIZATION OF UPSTREAM DRAINAGE AREAS

## MAINTENANCE OF TRAFFIC GENERAL NOTES

- CONTRACTOR IS RESPONSIBLE FOR TRAFFIC CONTROL WHEN THERE IS LESS THAN TWO LANES (ONE IN EACH DIRECTION) OPEN, INCLUDING DURING PAVEMENT PATCHING.
- THE SAFE AND ORDERLY PASSAGE OF TRAFFIC AND PEDESTRIANS SHALL BE PROVIDED WHERE OPERATIONS ABUT PUBLIC THOROUGHFARES AND ADJACENT PROPERTY.
- PROVIDE AND MAINTAIN ALL NECESSARY TRAFFIC CONTROL AND SAFETY MEASURES REEQUIRED DURING CONSTRUCTION OPERATIONS WITHIN OR NEAR PUBLIC ROADWAY.
- NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL ALL PROPER SIGNS AND BARRICADES HAVE BEEN INSTALLED.
- ONE LANE OF TRAFFIC SHALL BE MAINTAINED FOR EMERGENCY VEHICLES AT ALL TIMES.
- ADVANCE WARNING SIGNS NEED TO BE ERECTED AT LEAST 7 DAYS PRIOR TO START OF CONSTRUCTION.
- THE CONTRACTOR IS RESPONSIBLE TO POST TWO (2) TEMPORARY SIGNS NEXT TO FUL-ROK PARK IN THE NORTHWEST CORNER OF THE ROY AVE/E FULLERTON AVE INTERSECTION. PRIOR TO CLOSING THE PARKING STALLS NEXT TO THE PARK FOR PARKING PAVEMENT REHABILITATION AND RESTRIPING, THE CONTRACTOR SHALL FURNISH AND ERECT SIGNS WITH THE MESSAGE "PARKING TEMPORARILY CLOSED" OR SIMILAR MESSAGE APPROVED BY THE ENGINEER.
- ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.
- THE CONTRACTOR SHALL SUBMIT A STAGING SEQUENCE PLAN TO THE ENGINEER FOR APPROVAL PRIOR TO MOBILIZATION.
- THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTIES AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT. A QUANTITY OF "TEMPORARY ACCESS (PRIVATE ENTRANCE)" IS INCLUDED IN THE CONTRACT FOR THIS PURPOSE.

## STAKING NOTES

- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE ENGINEER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

## MISCELLANEOUS NOTES

- DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
- AGGREGATE BASE REPAIR, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, AND STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AND WILL NOT EXCEED THE PLAN QUANTITY.
- THE THICKNESS OF EXISTING PAVEMENT TO BE REMOVED HAS BEEN ESTIMATED FROM PAVEMENT CORES. THE PAVEMENT CORING GEOTECHNICAL REPORT IS ATTACHED TO THE PROJECT SPECIFICATIONS. THE CITY DOES NOT GUARANTEE ITS ACCURACY.
- THE CONTRACTOR SHALL MAINTAIN ACCESS TO RESIDENTIAL DRIVEWAYS AT ALL TIMES. THIS WORK SHALL BE COMPLETED AND PAID FOR IN ACCORDANCE WITH THE SPECIAL PROVISION FOR "AGGREGATE SURFACE COURSE FOR TEMPORARY ACCESS". AGGREGATE RAMPS SHALL BE CONSTRUCTED FROM THE EXISTING ROADWAY AGGREGATE BASE TO THE EXISTING CURB AND GUTTER. THE GRADE OF THE AGGREGATE RAMPS SHALL NOT EXCEED 12%.
- RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES.
- THE CONTRACTOR SHALL REMOVE FROM THE SITE ALL EXCESS UTILITY SPOIL AND DISPOSE OF IT LEGALLY.
- STOCKPILING OF EXCAVATED MATERIALS ON SITE SHALL BE MINIMIZED TO THE GREATEST EXTENT PRACTICABLE. THE CONTRACTOR SHALL PROCEED PROMPTLY WITH UNDERGROUND WORK IMMEDIATELY FOLLOWING EXCAVATION, AND SHALL BACKFILL ALL EXCAVATIONS IMMEDIATELY FOLLOWING APPROVAL OF COMPLETED WORK BY THE OWNER.

FILE NAME = N:\NORTHLAKE\940032\HR - Municipal Review Projects\940032\_HR\_302's\940032\HR327\NC\1\1\GEN\_01\_940032\HR327\_SHT

USER NAME = esidorozuk	DESIGNED - JC	REVISED - _____
	DRAWN - JC	REVISED - _____
PLOT SCALE = 20'	CHECKED - MW	REVISED - _____
PLOT DATE = 3/29/2021	DATE - 06/22/07	REVISED - _____

## STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

CITY OF NORTHLAKE E FULLERTON AVE INDEX OF SHEETS, HIGHWAY STANDARDS, GENERAL NOTES	
SCALE: _____	SHEET NO. ____ OF ____ SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1381	20-00097-00-RS	COOK	21	2
				CONTRACT NO. 61H11
ILLINOIS FED. AID PROJECT				

## SUMMARY OF QUANTITIES

S.I.	SP	CODE NO.	ITEM	UNIT	CODE 0005 QUANTITY
**		20101200	TREE ROOT PRUNING	EACH	10
		20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	230
		21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1165
		25200110	SODDING, SALT TOLERANT	SQ YD	1165
		25200200	SUPPLEMENTAL WATERING	UNIT	50
		28000200	EARTH EXCAVATION FOR EROSION CONTROL	CU YD	35
		28000510	INLET FILTERS	EACH	70
	*	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	230
		31101100	SUBBASE GRANULAR MATERIAL, TYPE B	CU YD	75
		40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	9500
		40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	1800
		40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50	TON	1200
		42400800	DETECTABLE WARNINGS	SQ FT	140
		44000163	HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/2"	SQ YD	13585
		44000600	SIDEWALK REMOVAL	SQ FT	11495
		44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	275
		44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	225
		44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	100
		44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	50
		44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	13585
		67100100	MOBILIZATION	L SUM	1
		70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
		70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1

S.I.	SP	CODE NO.	ITEM	UNIT	CODE 0005 QUANTITY
		70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
		70300100	SHORT TERM PAVEMENT MARKING	FOOT	1,030
		70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	350
**		72000100	SIGN PANEL - TYPE 1	SQ FT	51
**		72900100	METAL POST - TYPE A	FOOT	48
**		78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS	SQ FT	64
**		78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	829
**		78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	605
**		78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	270
**		78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	120
**	*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	200
	*	X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1
	*	X0326806	WASHOUT BASIN	L SUM	1
	*	X0326862	STRUCTURES TO BE ADJUSTED	EACH	46
	*	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	84
	*	X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	11,495
	*	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	7
	*	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	160
	*	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1812
	*	Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	3920
	*	XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT	SQ YD	37
	*	XX006947	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	1471

\* INDICATES SPECIAL PROVISION  
 \*\* INDICATES SPECIALTY ITEM

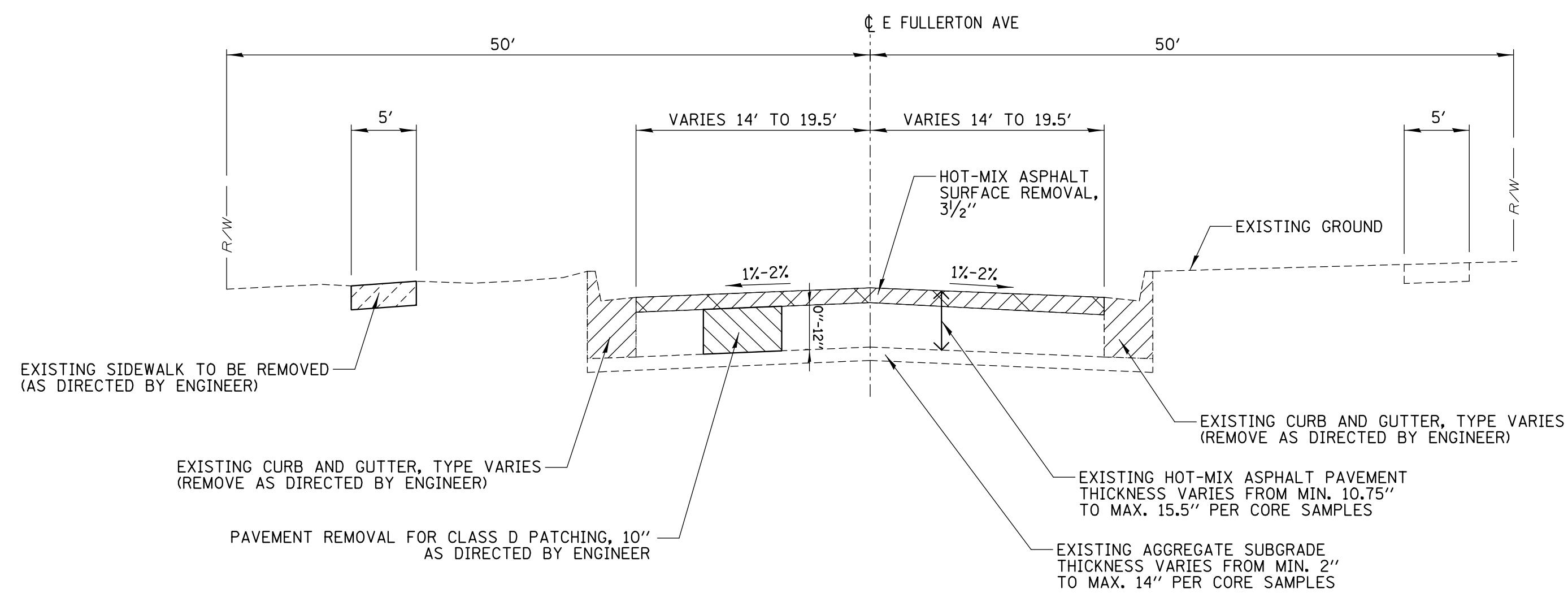
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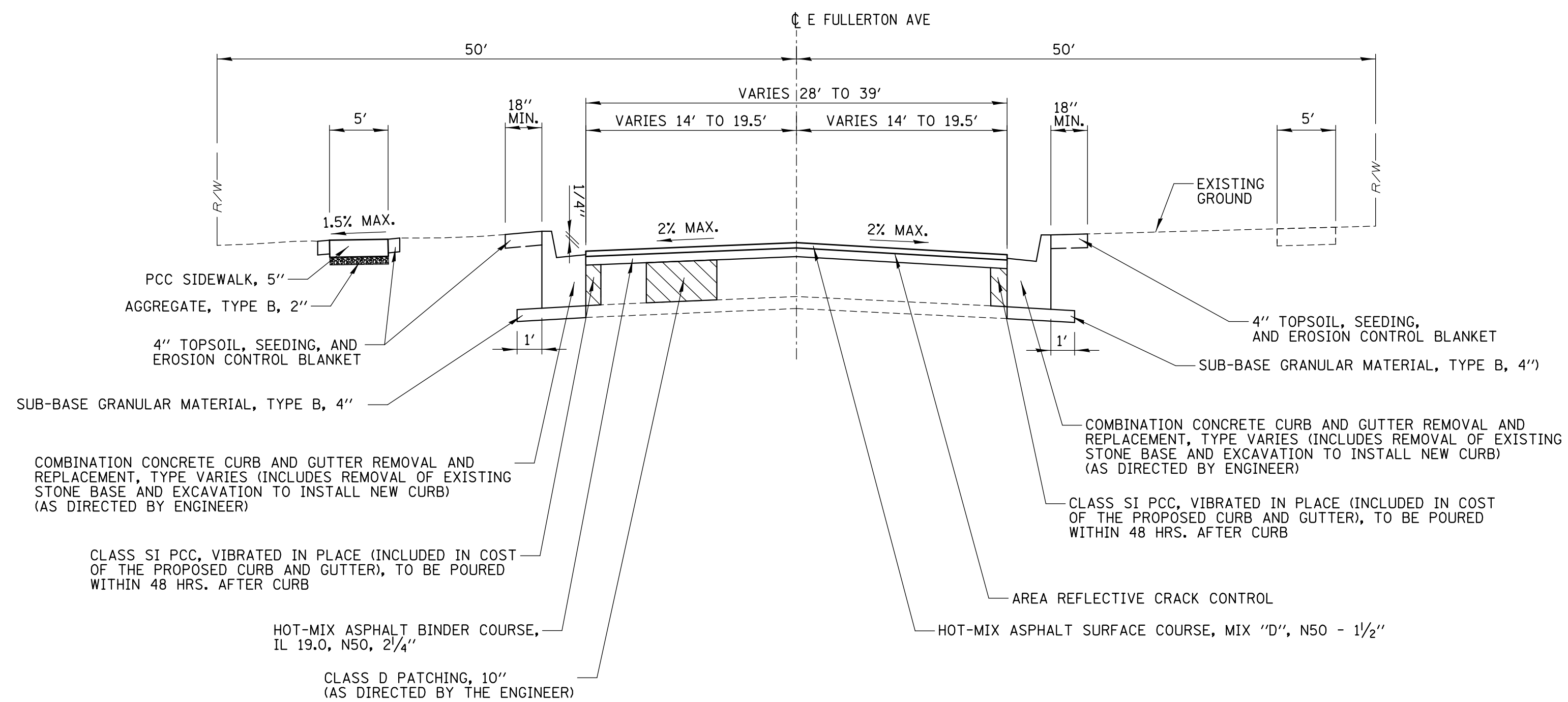
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>CITY OF NORTHLAKE E FULLERTON AVE SUMMARY OF QUANTITIES</b>		
SCALE: _____	SHEET NO. ___ OF ___ SHEETS	STA. _____ TO STA. _____

F.A.U. RTE. 1381	SECTION 20-00097-00-RS	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 3
CONTRACT NO. 61H11				
ILLINOIS FED. AID PROJECT				



**EXISTING TYPICAL SECTION**  
**STA. 100 + 84.53 TO STA. 140 + 02.10**  
**(OMISSION STA. 113 + 13.89 TO STA. 113 + 94.23)**



**PROPOSED TYPICAL SECTION**  
**STA. 100 + 84.53 TO STA. 140 + 02.10**  
**(OMISSION STA. 113 + 13.89 TO STA. 113 + 94.23)**

**NOTES:**

- SEE THE DISTRICT 1 DETAILS FOR NOTES AND INFORMATION REGARDING WORK INCLUDED IN THE COST OF "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".
- CONTRACTOR SHALL SAWCUT PAVEMENT PRIOR TO REMOVING THE CURB.
- PAVEMENT REMOVAL ADJACENT TO PROPOSED CURB AND GUTTER SHALL BE INCLUDED IN THE COST OF THE PROPOSED CURB AND GUTTER.
- ALL WORK INCLUDING LANDSCAPE RESTORATION MUST BE COMPLETED AND APPROVED BY ENGINEER PRIOR TO FINAL PAYOUT.
- CONTRACTOR SHALL MILL PRIOR TO PATCHING.
- EXISTING REFLECTIVE CRACK CONTROL FABRIC WAS OBSERVED IN CORE REPORT. REMOVAL OF PETROMAT (FABRIC) ENCOUNTERED DURING GRINDING OPERATIONS SHALL BE INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/2".

**LOCATIONS:**

TOTAL HOT-MIX ASPHALT TO BE REMOVED ( 3 1/2" ) & REPLACED ( 3 3/4" )

- E FULLERTON AVE  
N WOLF RD TO N GENEVA AVE

**LEGEND**

- REMOVAL OF EXISTING CURB AND GUTTER
- REMOVAL OF EXISTING HOT-MIX ASPHALT BASE COURSE
- REMOVAL OF EXISTING PCC SIDEWALK
- CLASS D PATCHING, 10"

NOTE: THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE HMA BINDER IL-19

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

	AIR VOIDS @Ndes
PAVEMENT RESURFACING HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 1-1/2" HMA BINDER COURSE, IL-19.0, N50; 2-1/4"	4% @ 50 GYR 4% @ 50 GYR
HMA DRIVEWAY REMOVAL AND REPLACEMENT HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 3"	4% @ 50 GYR
CLASS D PATCHES, 10" HMA BINDER COURSE IL-19.0, N50; 10"	4% @ 50 GYR

**NOTES:**

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA MIXTURE QUANTITIES IS 112 LB/SQ YD/IN.
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS.  
FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

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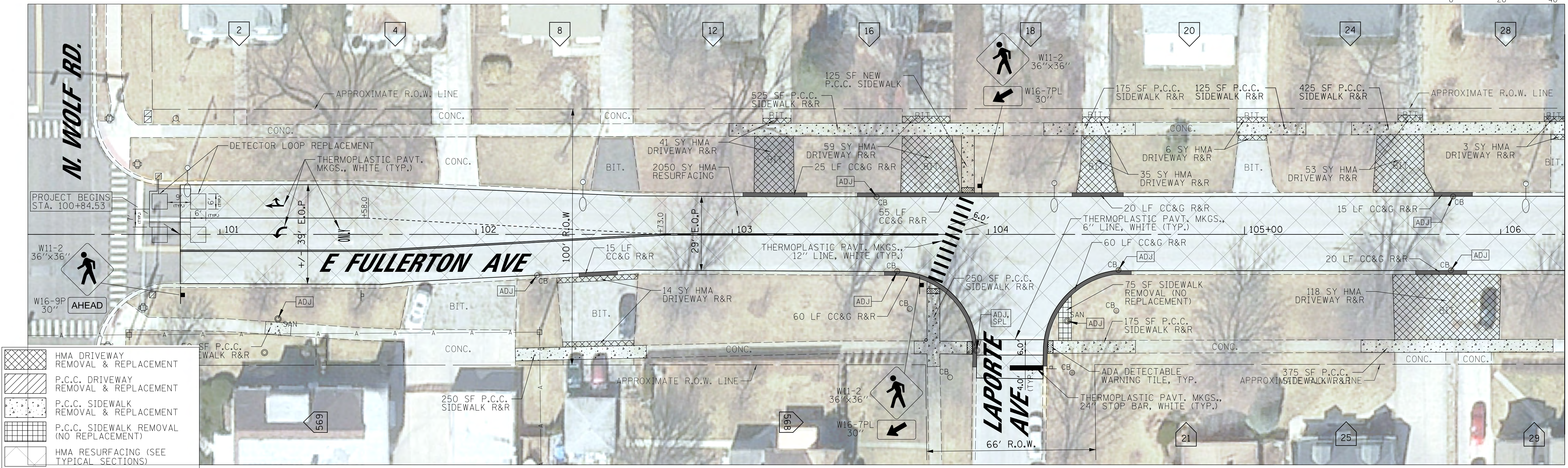
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

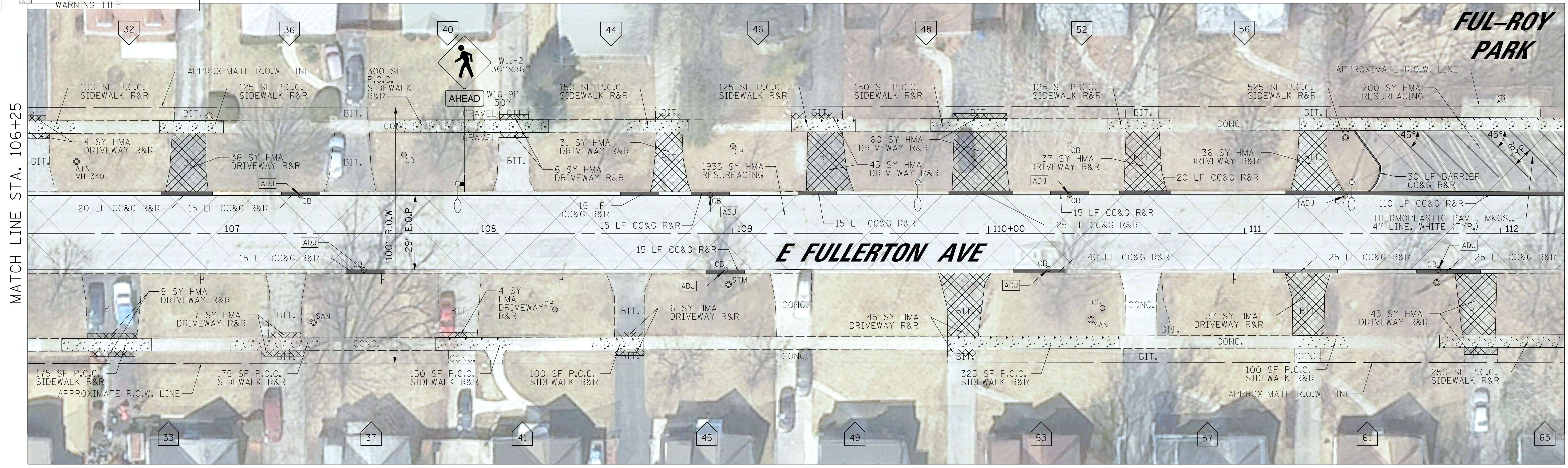
**CITY OF NORTHLAKE**  
**E FULLERTON AVE**  
**TYPICAL SECTIONS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1381	20-00097-00-RS	COOK	21	4
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61H11	



	HMA DRIVEWAY REMOVAL & REPLACEMENT
	P.C.C. DRIVEWAY REMOVAL & REPLACEMENT
	P.C.C. SIDEWALK REMOVAL & REPLACEMENT
	P.C.C. SIDEWALK REMOVAL (NO REPLACEMENT)
	HMA RESURFACING (SEE TYPICAL SECTIONS)
	CONCRETE CURB AND GUTTER REMOVAL & REPLACEMENT
	ADA DETECTABLE WARNING TILE



MATCH LINE STA. 106+25

MATCH LINE STA. 112+25

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

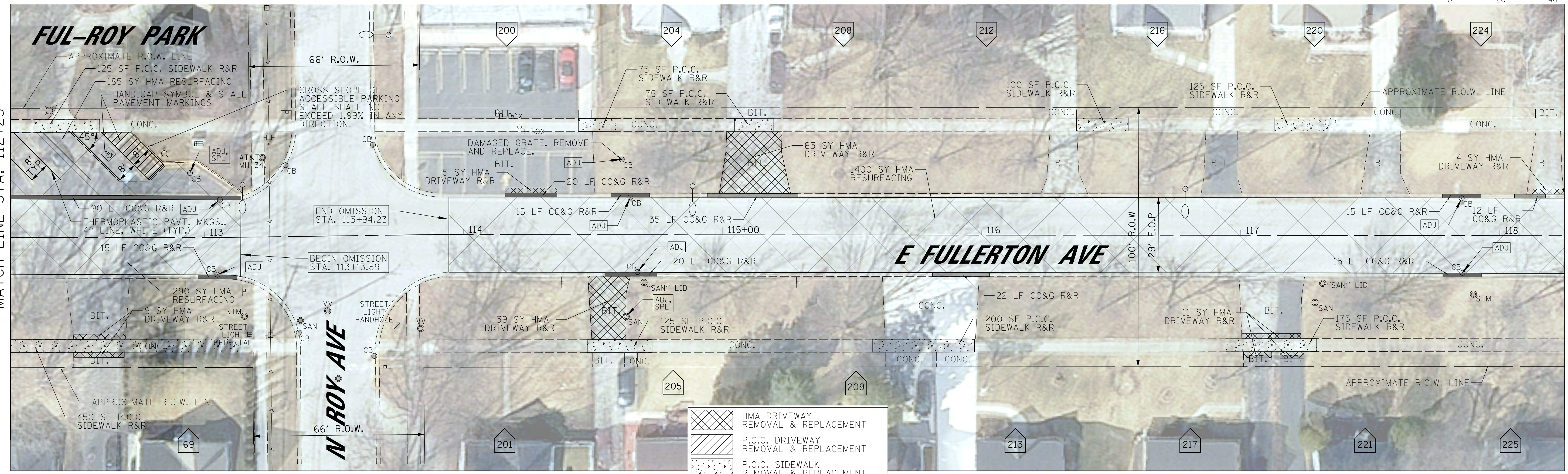
CITY OF NORTHALKE  
E FULLERTON AVE  
ROADWAY PLAN

SCALE: 1" = 20' SHEET NO. 1 OF 4 SHEETS STA. 100+84.53 TO STA. 112+25.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1381	20-00097-00-RS	COOK	21	5
CONTRACT NO. 61H11			ILLINOIS FED. AID PROJECT	

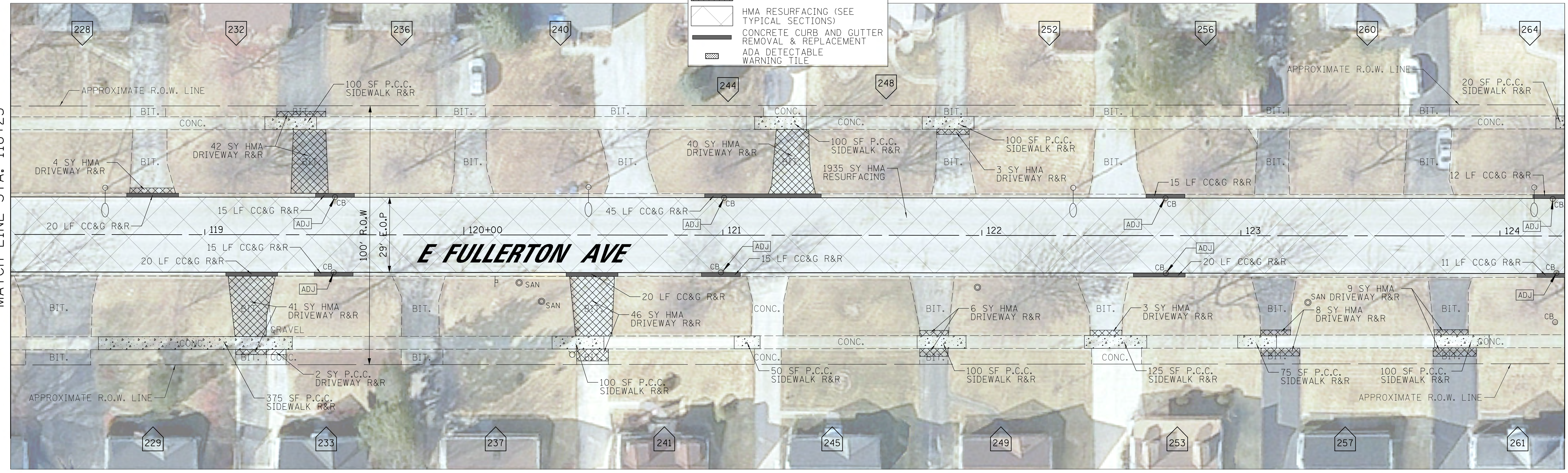


MATCH LINE STA. 112+25



MATCH LINE STA. 118+25

MATCH LINE STA. 118+25



MATCH LINE STA. 124+25

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

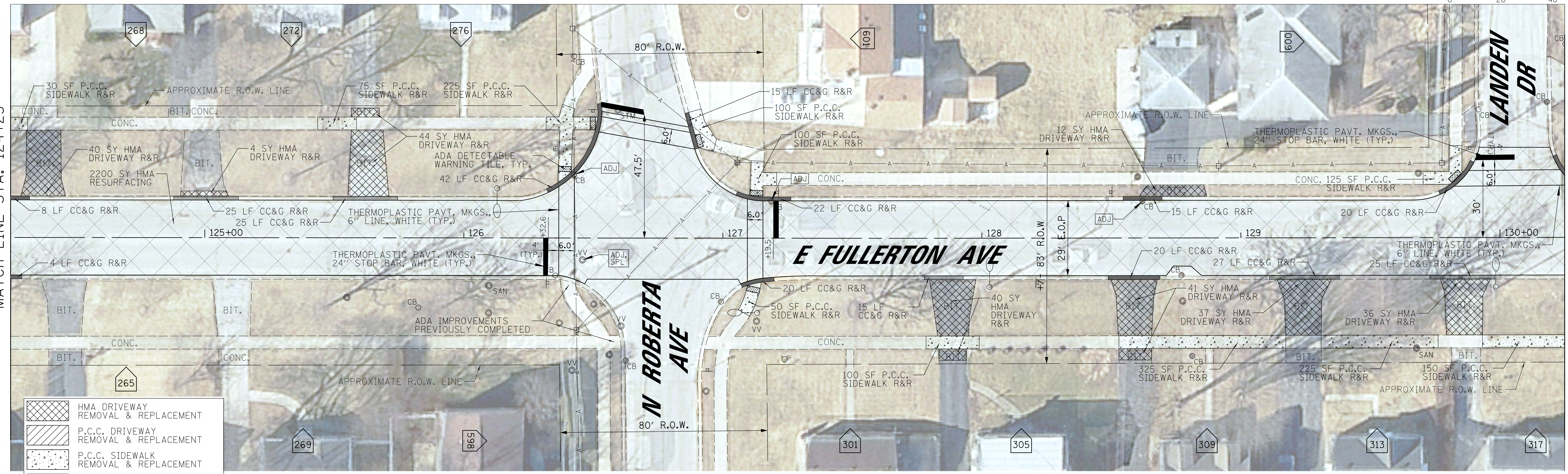
CITY OF NORTHLAKE E FULLERTON AVE ROADWAY PLAN	
SCALE: 1" = 20'	SHEET NO. 2 OF 4 SHEETS
STA. 112+25.00 TO STA. 124+25.00	

F.A.U. RTE. 1381	SECTION 20-00097-00-RS	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 6
CONTRACT NO. 61H11			ILLINOIS FED. AID PROJECT	



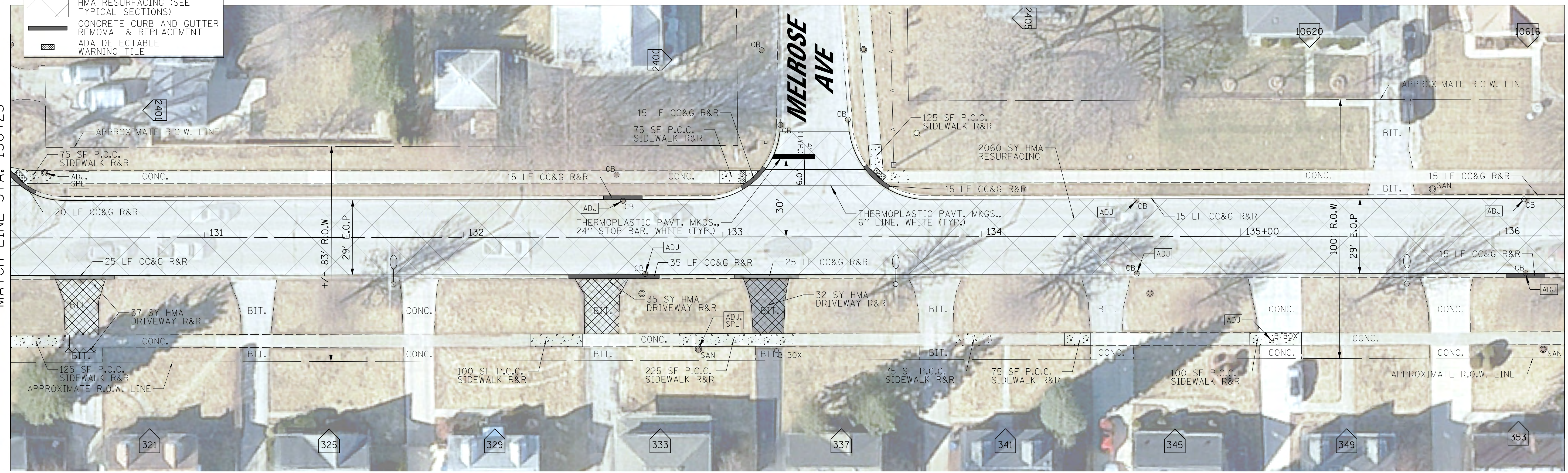
MATCH LINE STA. 124+25

MATCH LINE STA. 130+25



MATCH LINE STA. 130+25

MATCH LINE STA. 136+25



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

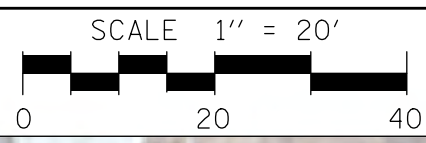
**CITY OF NORTHLAKE  
E FULLERTON AVE  
ROADWAY PLAN**

SCALE: 1" = 20' SHEET NO. 3 OF 4 SHEETS STA. 124+25.00 TO STA. 136+25.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1381	20-00097-00-RS	COOK	21	7
				CONTRACT NO. 61H11

ILLINOIS FED. AID PROJECT

MATCH LINE STA. 136+25



	HMA DRIVEWAY REMOVAL & REPLACEMENT
	P.C.C. DRIVEWAY REMOVAL & REPLACEMENT
	P.C.C. SIDEWALK REMOVAL & REPLACEMENT
	P.C.C. SIDEWALK REMOVAL (NO REPLACEMENT)
	HMA RESURFACING (SEE TYPICAL SECTIONS)
	CONCRETE CURB AND GUTTER REMOVAL & REPLACEMENT
	ADA DETECTABLE WARNING TILE

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

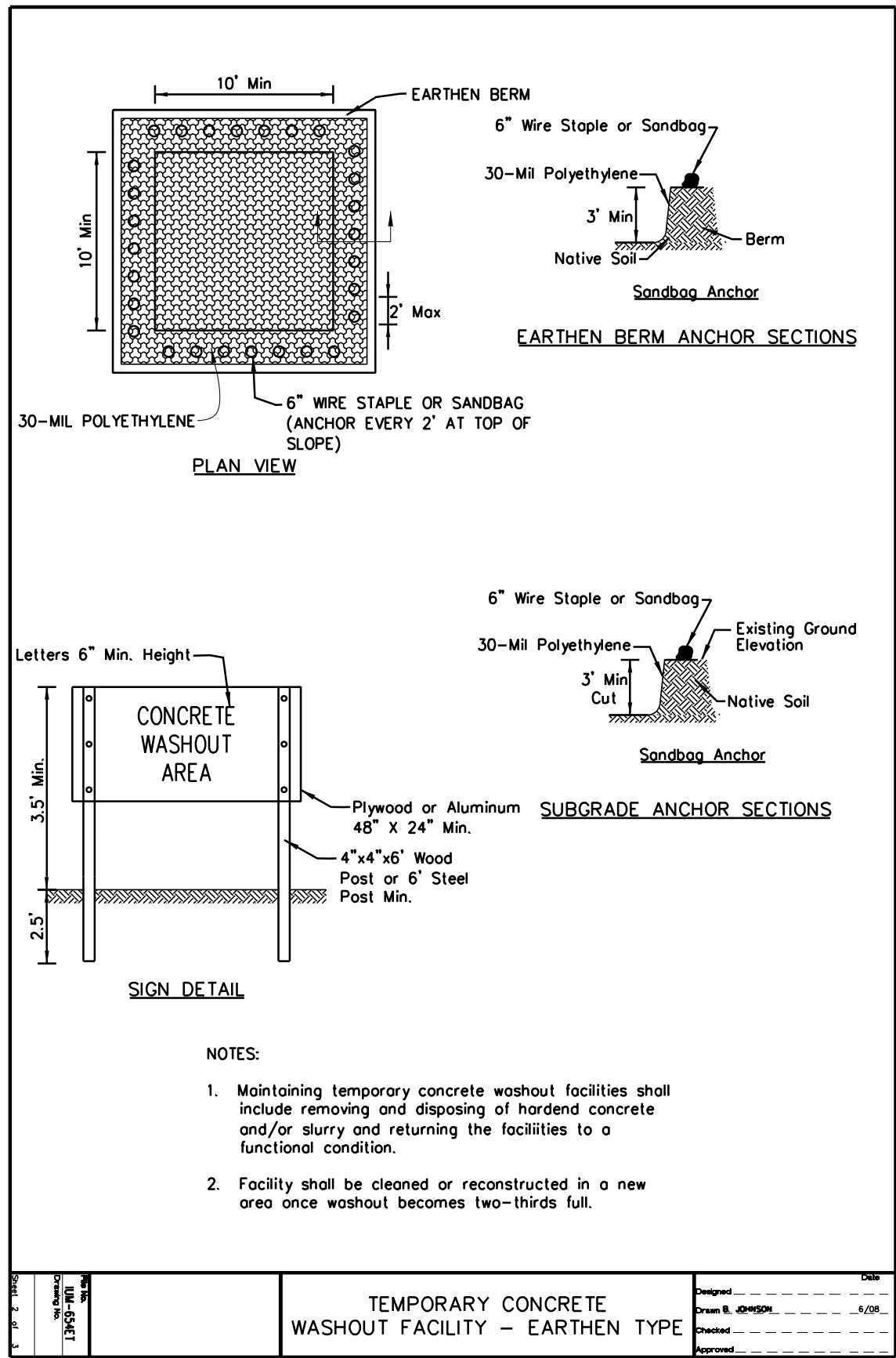
**CITY OF NORTHALKE  
E FULLERTON AVE  
ROADWAY PLAN**

SCALE: 1" = 20' SHEET NO. 4 OF 4 SHEETS STA. 136+25.00 TO STA. 140+02.10

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1381	20-00097-00-RS	COOK	21	8
				CONTRACT NO. 61H11
ILLINOIS FED. AID PROJECT				



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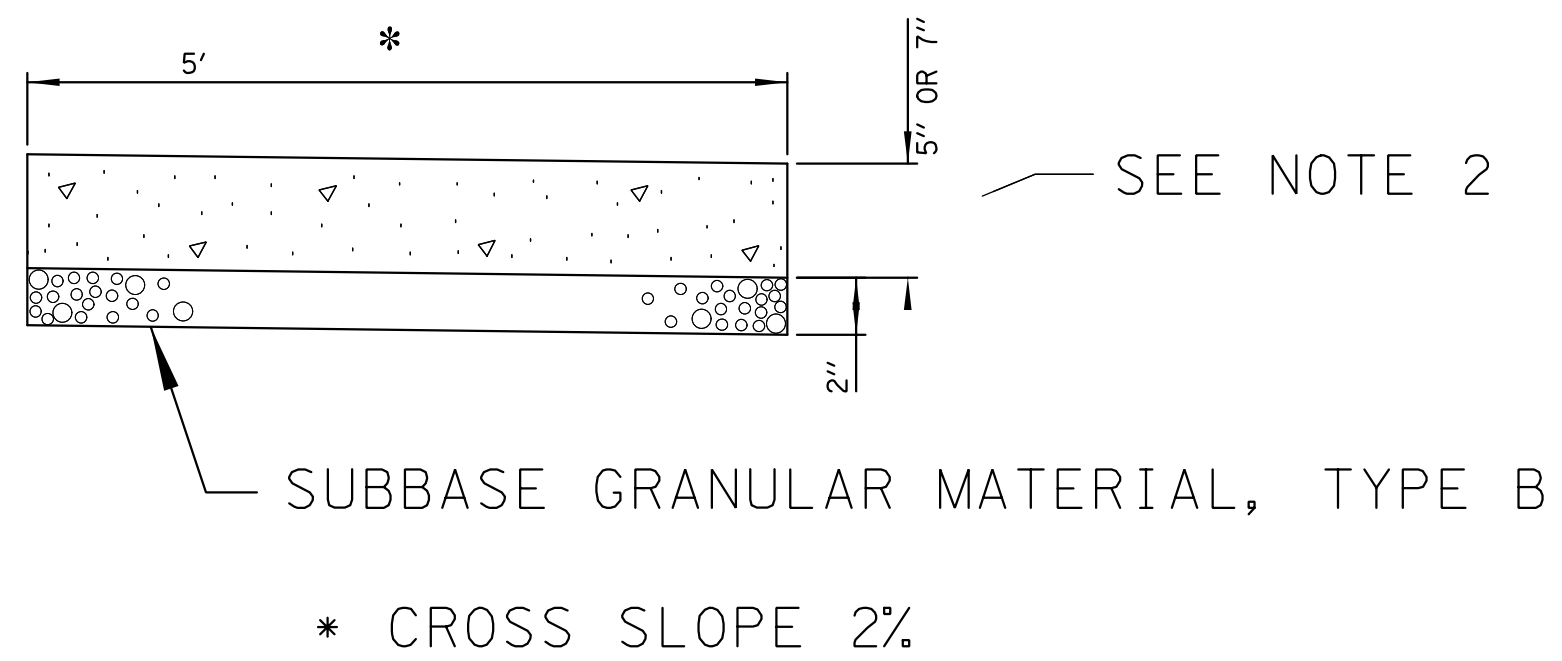
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CITY OF NORTHLAKE  
E FULLERTON AVE  
CONSTRUCTION DETAILS**

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_ OF \_\_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

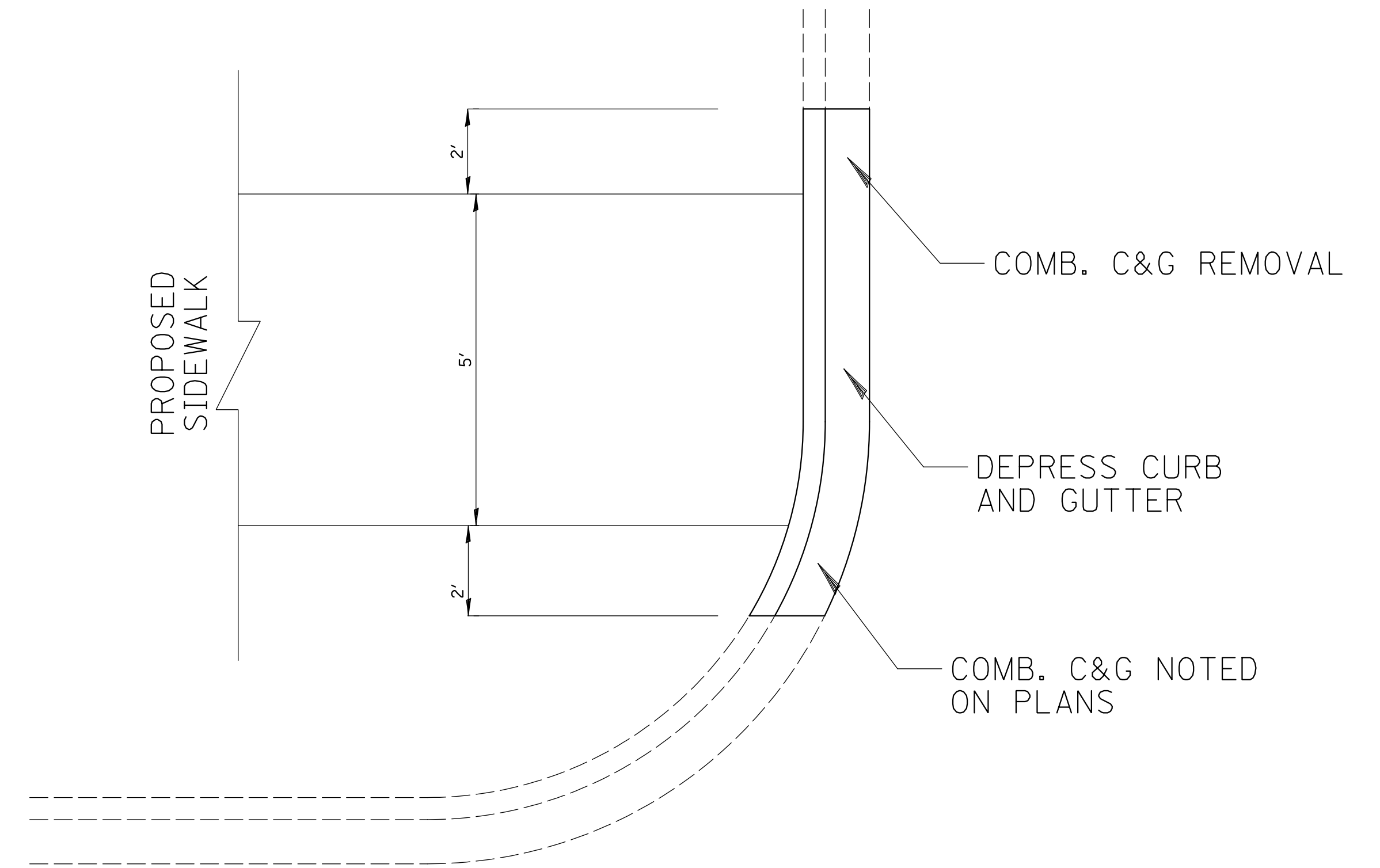
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1381	20-00097-00-R5	COOK	21	9
<b>CONTRACT NO. 61H11</b>				
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFILLED WITHIN 24 HOURS.
2. ALL LANDSCAPE RESTORATION (4" TOPSOIL, SALT TOLERANT SODDING) SHALL BE INCLUDED IN COST FOR P.C.C. SIDEWALK.

P. C. C. SIDEWALK DETAIL



PROPOSED DEPRESSED CURB & GUTTER  
EXISTING CURB & GUTTER NOT DEPRESSED  
NOT TO SCALE

P. C. C. SIDEWALK DETAIL

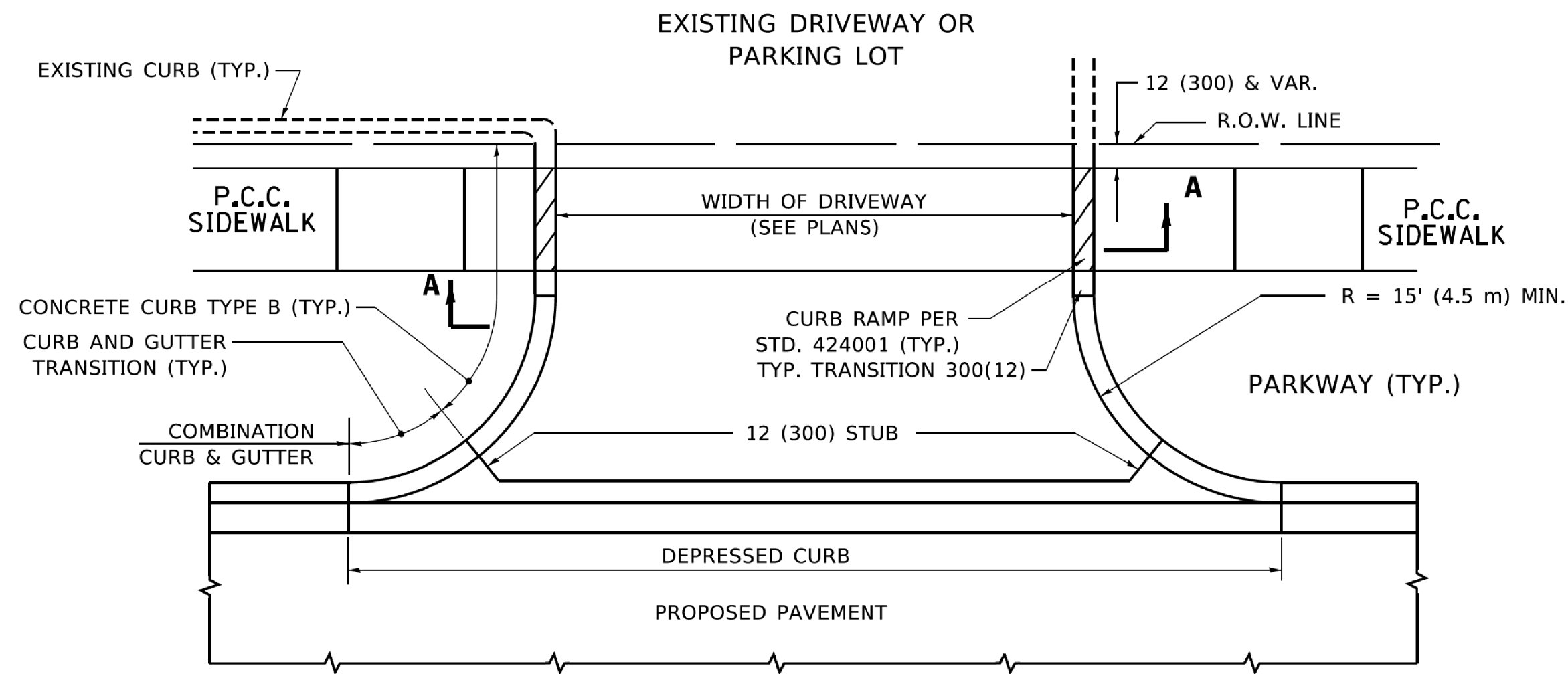
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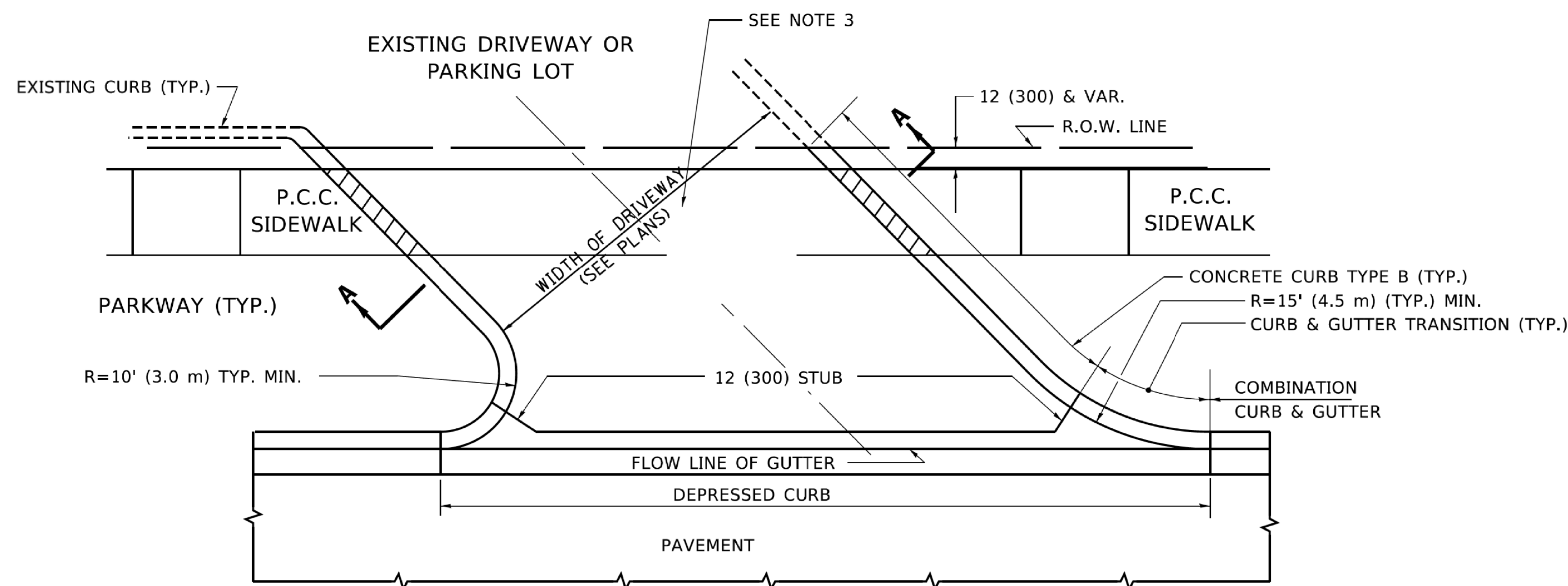
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>CITY OF NORTHLAKE</b>		
<b>E FULLERTON AVE</b>		
<b>CONSTRUCTION DETAILS</b>		
SCALE: _____	SHEET NO. ____ OF ____ SHEETS	STA. _____ TO STA. _____

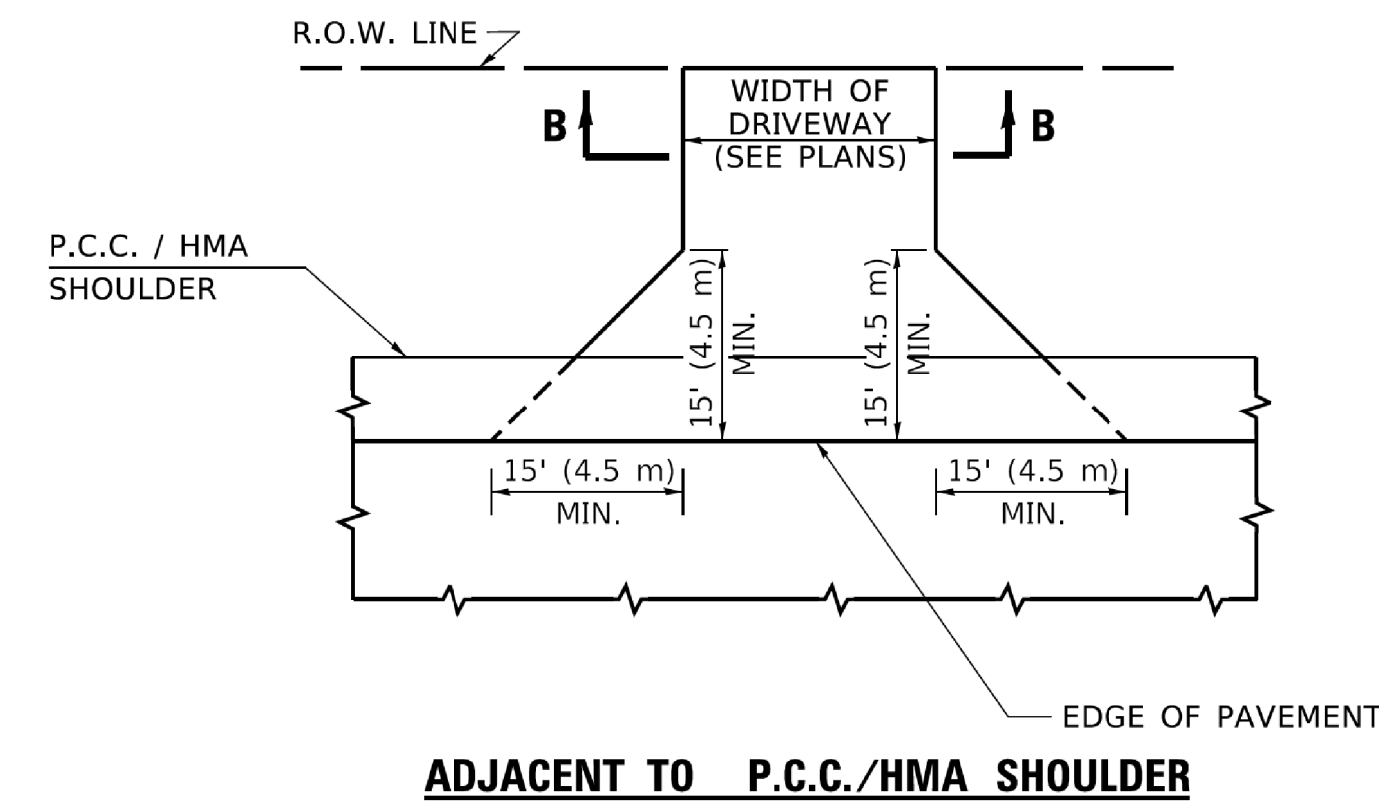
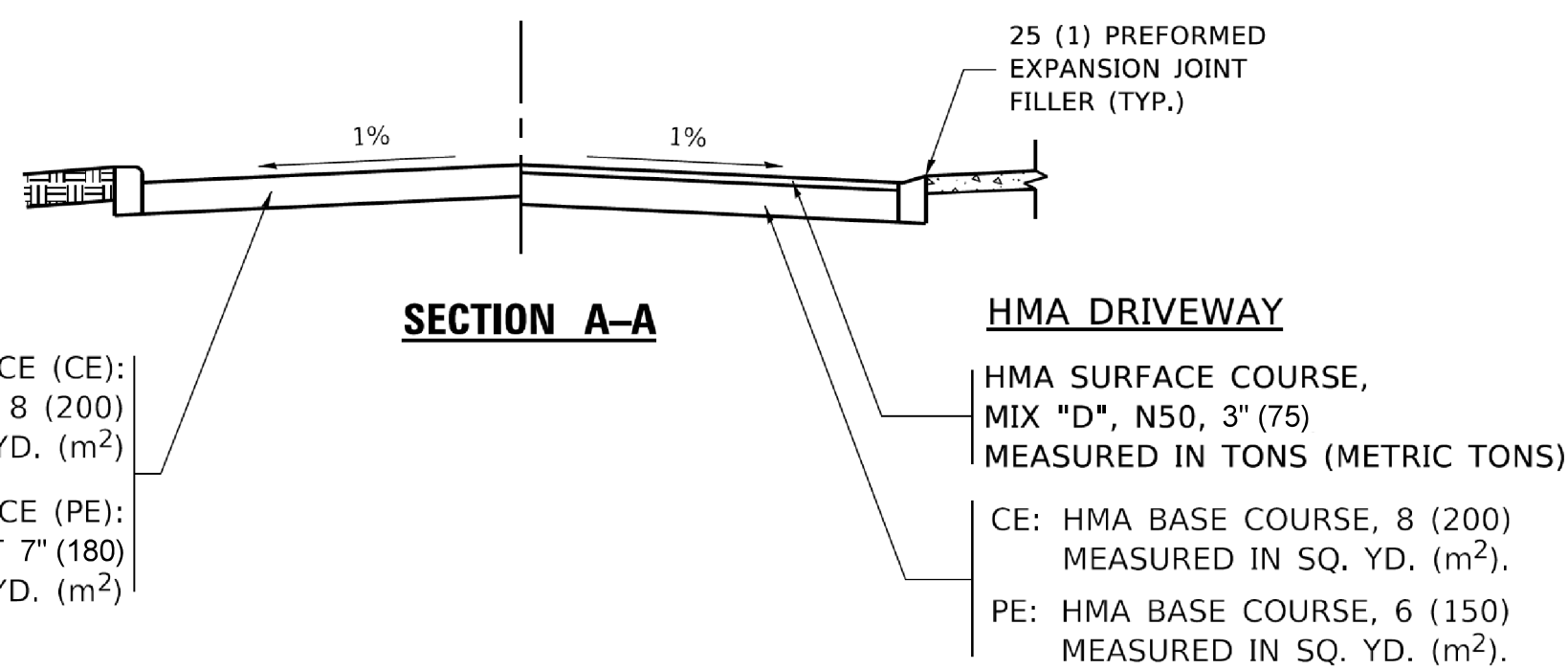
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1381	20-00097-00-RS	COOK	21	10
CONTRACT NO. 61H11				
ILLINOIS FED. AID PROJECT				



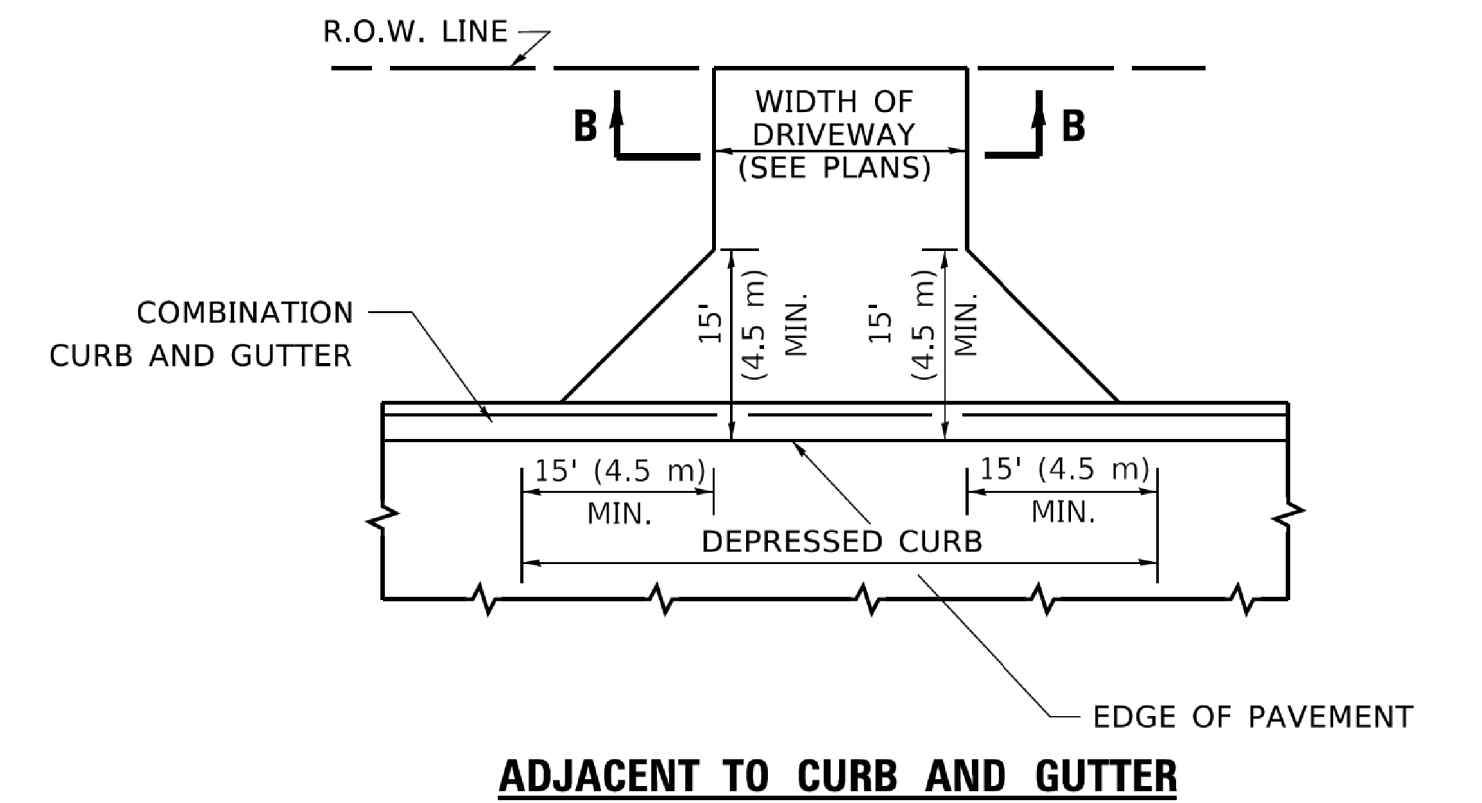
**WITH CONCRETE CURB, TYPE B**



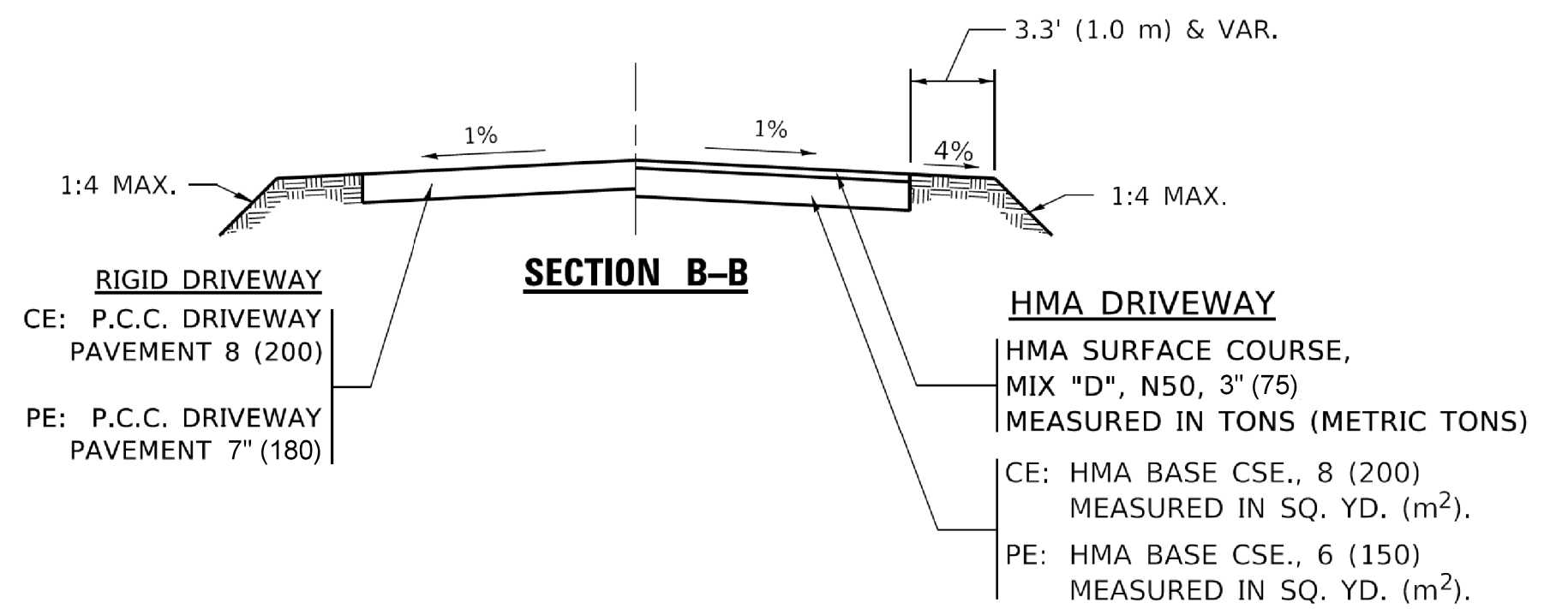
**WITH CONCRETE CURB, TYPE B**



**ADJACENT TO P.C.C./HMA SHOULDER**



**ADJACENT TO CURB AND GUTTER**



**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

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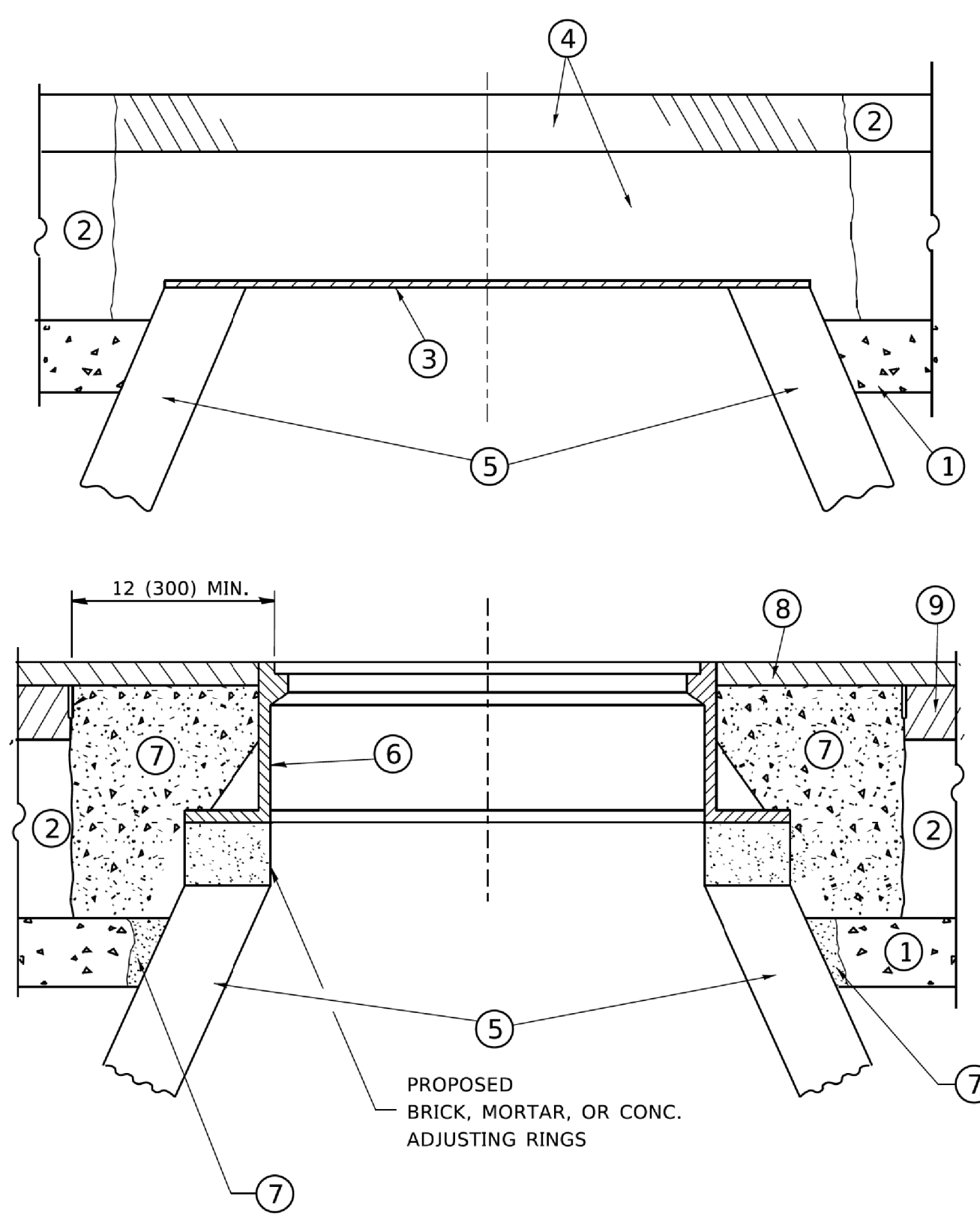
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>CITY OF NORTHLAKE E FULLERTON AVE CONSTRUCTION DETAILS</b>	
SCALE: _____	SHEET NO. ____ OF ____ SHEETS
STA. _____	TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1381	20-00097-00-RS	COOK	21	11
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61H11	

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**CONSTRUCTION PROCEDURES**

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
  - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
  - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
  - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 \* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT \*THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1 \*CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR \*FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

USER NAME = footemj	DESIGNED - R. SHAH	REVISED - R. WEDEMAN 05-14-04
DRAWN -	REVISED - R. BORO 01-01-07	
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11
PLOT DATE = 3/27/2019	DATE - 10-25-94	REVISED - R. BORO 12-06-11

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
BD600-03 (BD-8)			CONTRACT NO.	
ILLINOIS			FED. AID PROJECT	

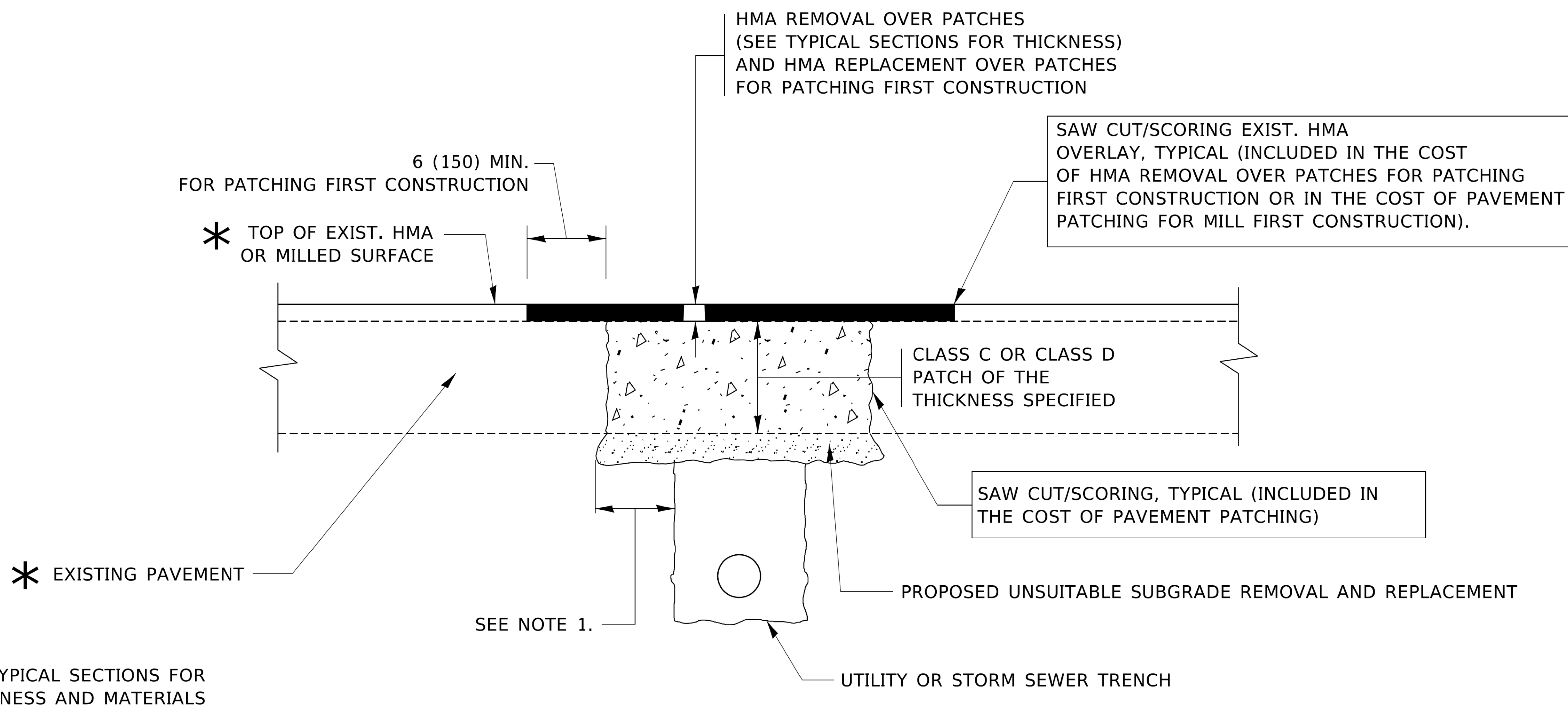
USER NAME = esidorozuk	DESIGNED - JC	REVISED -
DRAWN - JC	REVISED -	
PLOT SCALE = 20'	CHECKED - MW	REVISED -
PLOT DATE = 3/29/2021	DATE - 06/22/07	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 DETAILS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1381	20-00097-00-RS	COOK	21	12
			CONTRACT NO. 61H11	
ILLINOIS			FED. AID PROJECT	



**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

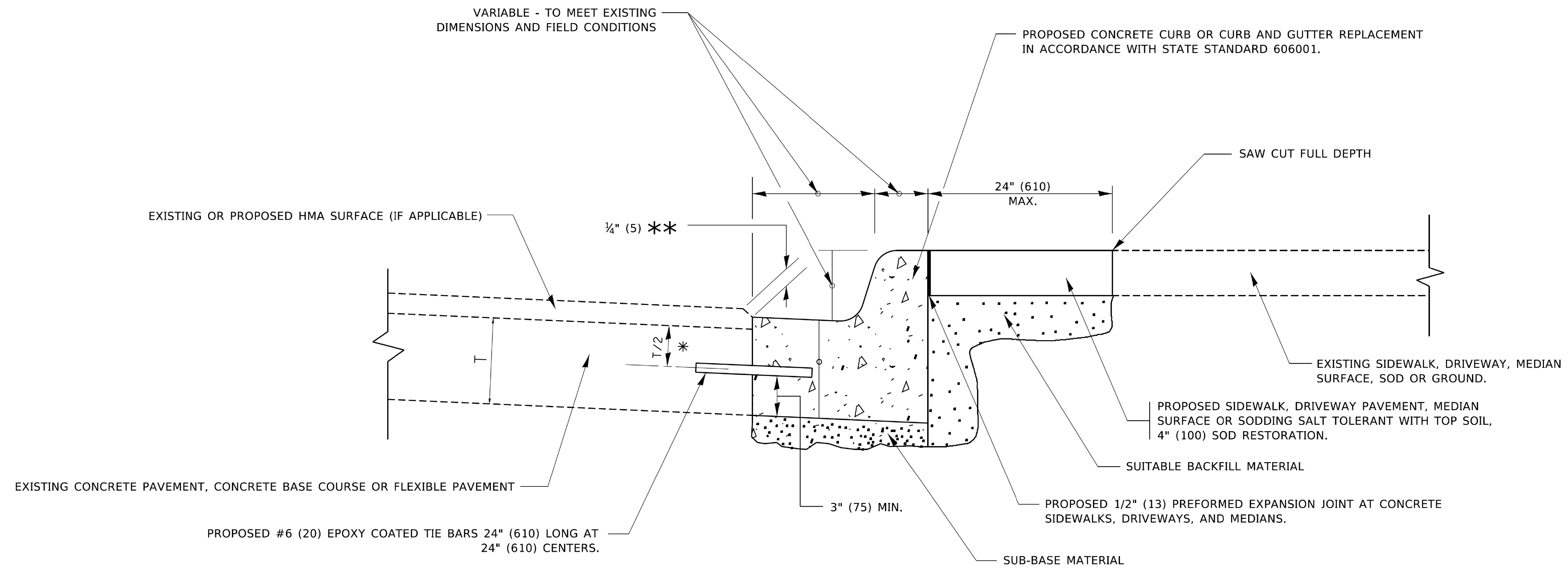
**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = footemj DESIGNED - R. SHAH DRAWN - PLOT SCALE = 50.0000' / in. PLOT DATE = 3/27/2019				DESIGNED - R. SHAH DRAWN - CHECKED - DATE - 10-25-94				REVISED - A. ABBAS 04-27-98 REVISED - R. BORO 01-01-07 REVISED - R. BORO 09-04-07 REVISED - K. ENG 10-27-08				<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>				<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>				F.A.U. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.	
												SCALE: NONE    SHEET 1 OF 1 SHEETS    STA.    TO STA.				1381    20-00097-00-RS    COOK    21    13					
USER NAME = esidorozuk DESIGNED - JC DRAWN - JC PLOT SCALE = 20' PLOT DATE = 3/29/2021				DESIGNED - JC DRAWN - JC CHECKED - MW DATE - 06/22/07								<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>				<b>DISTRICT 1 DETAILS</b>				ILLINOIS FED. AID PROJECT	
												SCALE:    SHEET NO.    OF    SHEETS    STA.    TO STA.				ILLINOIS FED. AID PROJECT					

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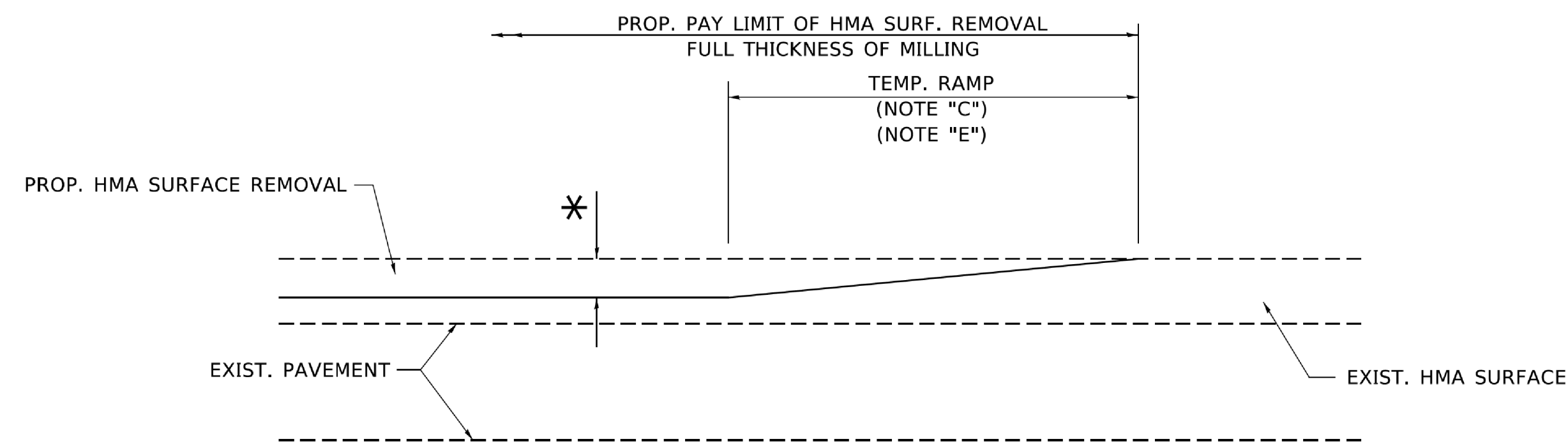
- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

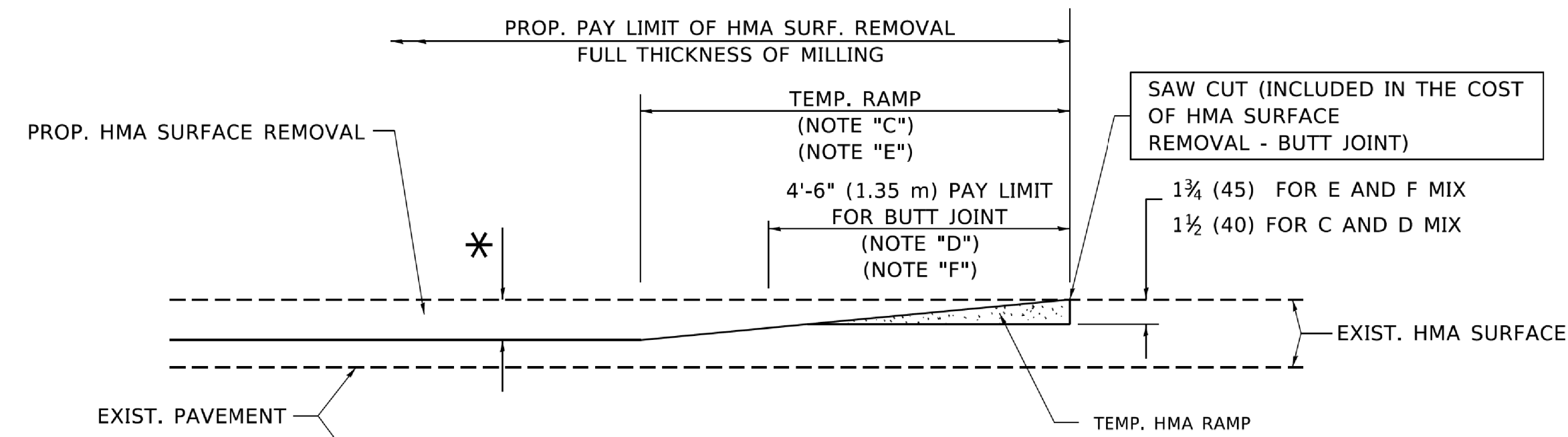
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	PLOT SCALE = 50,0000' / in.	CHECKED -	REVISED - M. GOMEZ 01-22-01			SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.	ILLINOIS	FED. AID PROJECT	
	PLOT DATE = 7/11/2019	DATE - 03-11-94	REVISED - R. BORO 12-15-09			<b>BD600-06 (BD-24)</b>						CONTRACT NO.
			REVISED - K. SMITH 07-11-19			ILLINOIS FED. AID PROJECT						
	USER NAME = esidorozuk	DESIGNED - JC	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 DETAILS</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
	PLOT SCALE = 20'	CHECKED - MW	REVISED -			SCALE: _____	SHEET NO. ___ OF ___ SHEETS	STA. _____	TO STA. _____	ILLINOIS	FED. AID PROJECT	
	PLOT DATE = 3/29/2021	DATE - 06/22/07	REVISED -			<b>20-00097-00-RS</b>						CONTRACT NO. 61H11
						ILLINOIS FED. AID PROJECT						

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**MILLED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

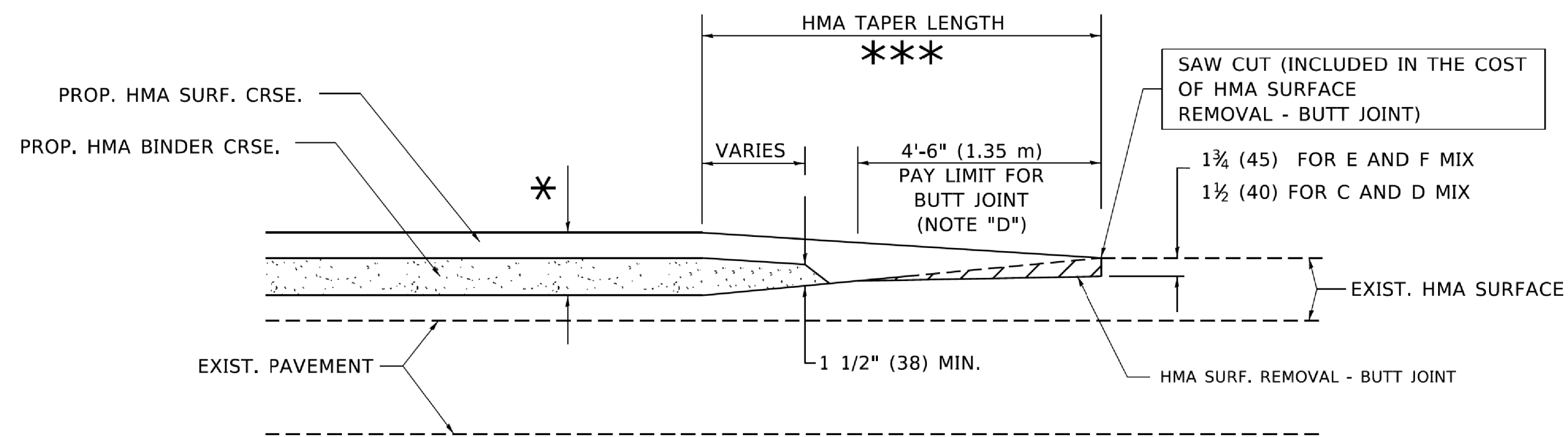
**OPTION 1**



**HMA CONSTRUCTED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

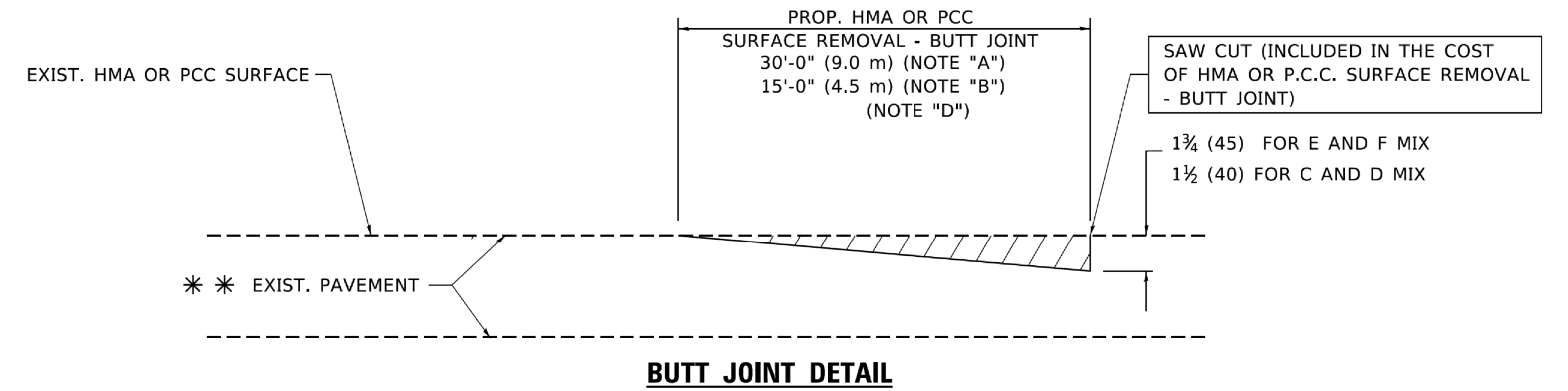
**OPTION 2**

**TYPICAL TEMPORARY RAMP**

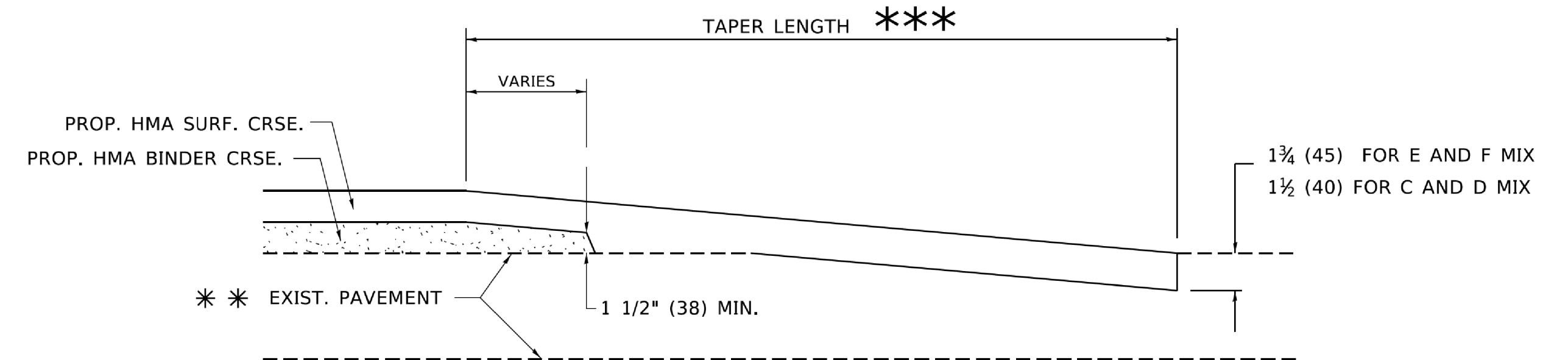


**BUTT JOINT AND HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = footemj	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - A. ABBAS 03-21-97
PLOT DATE = 3/27/2019	DATE - 06-13-90	REVISED - M. GOMEZ 04-06-01
		REVISED - R.BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.	CONTRACT NO.		
				ILLINOIS	FED. AID PROJECT	

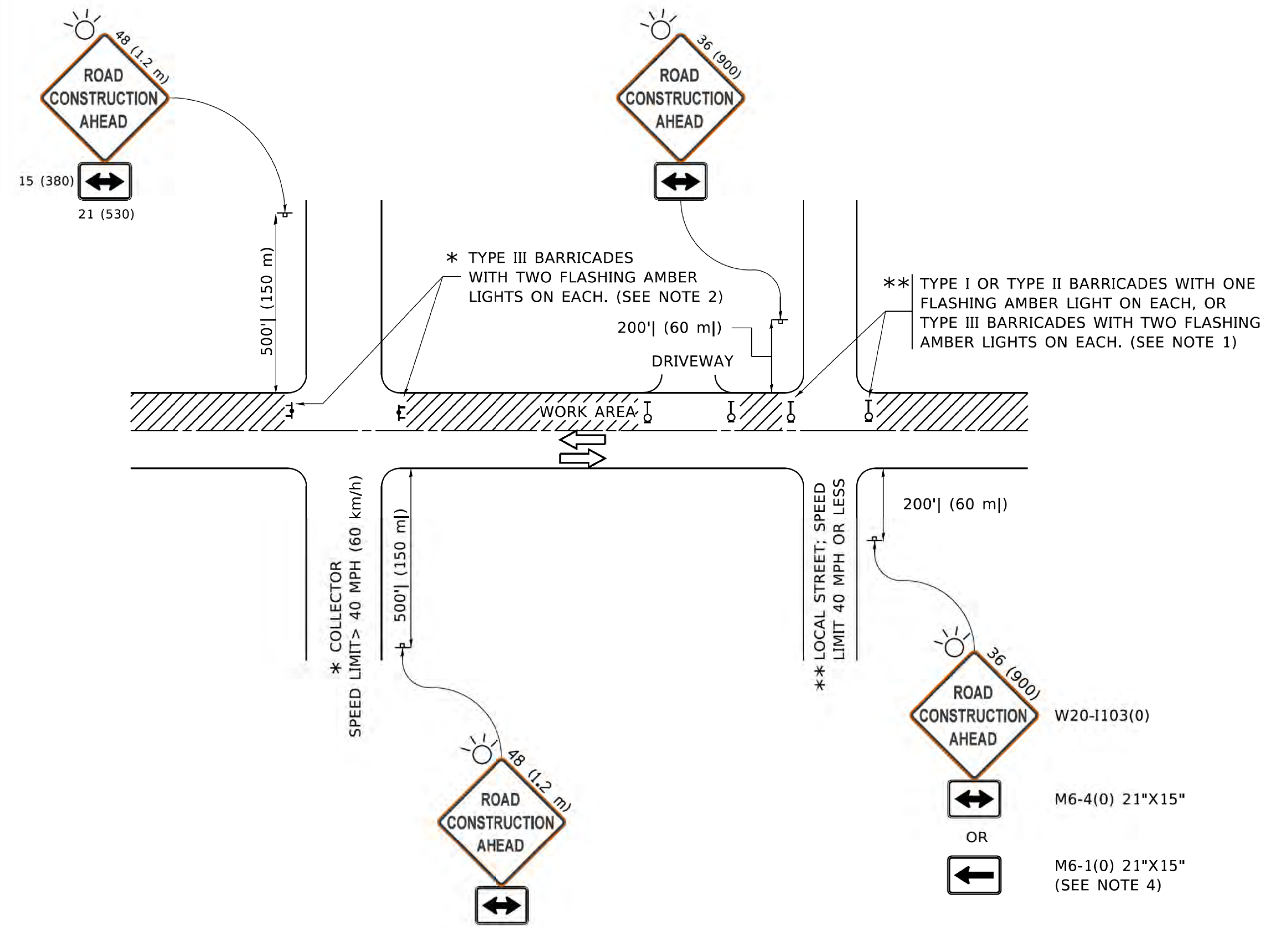
USER NAME = esidorozuk	DESIGNED - JC	REVISED -
PLOT SCALE = 20'	CHECKED - MW	REVISED -
PLOT DATE = 3/29/2021	DATE - 06/22/07	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT 1 DETAILS</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: _____	SHEET NO. ____ OF ____ SHEETS	STA. _____	TO STA. _____	CONTRACT NO. 61H11		
				ILLINOIS	FED. AID PROJECT	

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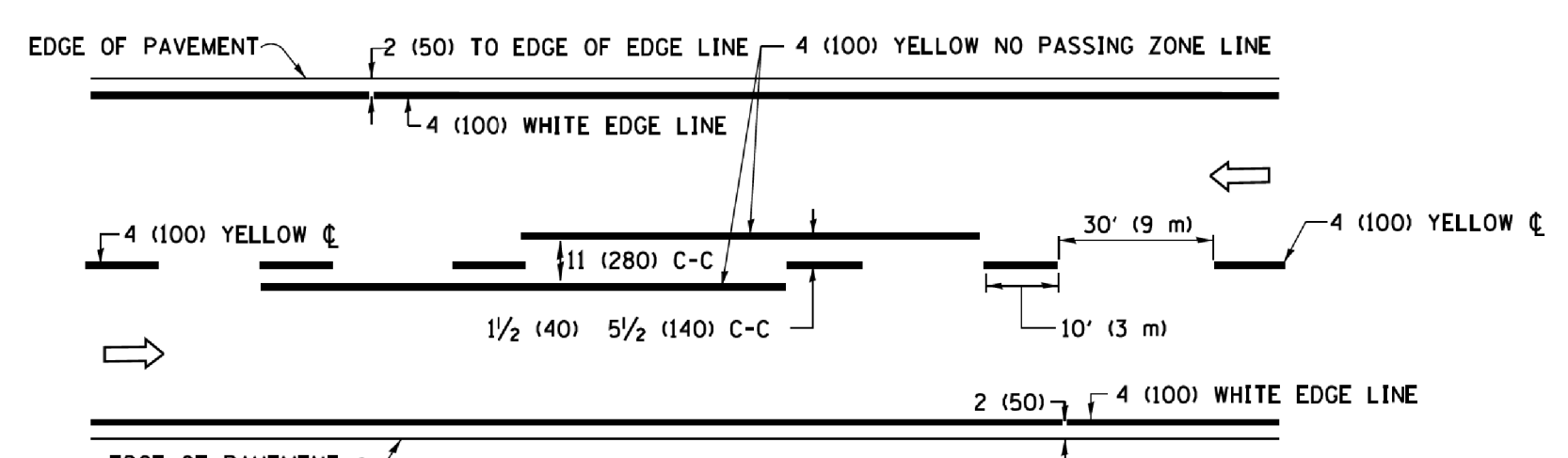
**NOTES:**

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

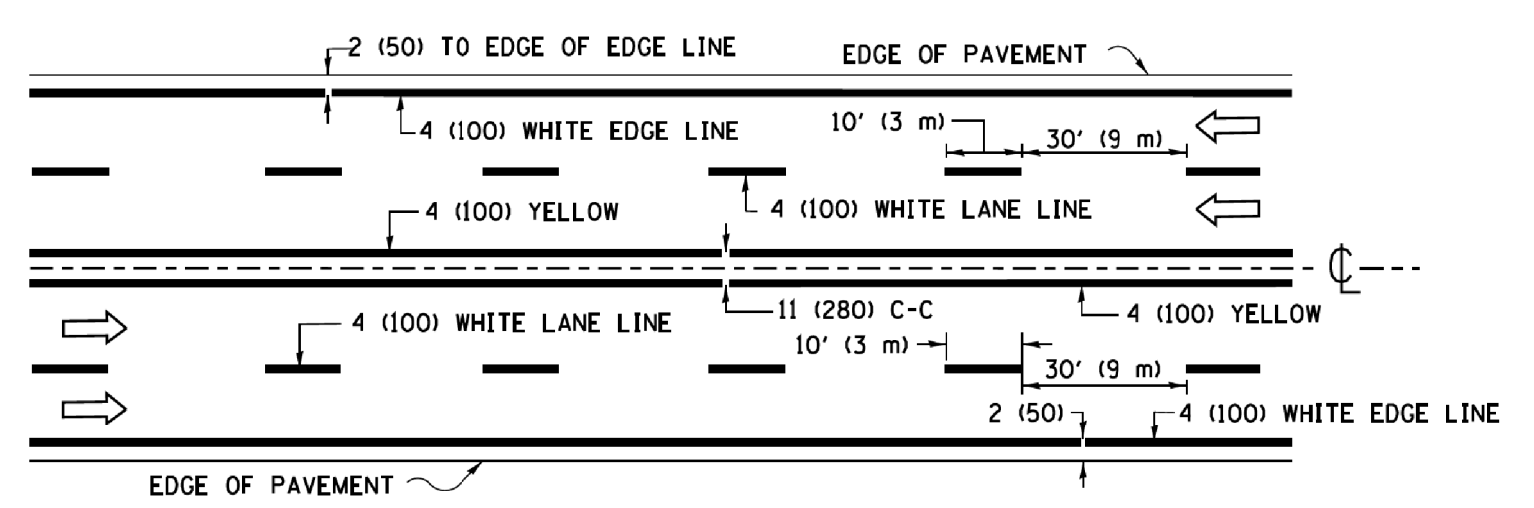
All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footemj DESIGNED - L.H.A. DRAWN - CHECKED - DATE - 06-89 PLOT SCALE = 50.0000' / in. PLOT DATE = 3/4/2019				DESIGNED - A. HOUSEH 10-15-96 DRAWN - T. RAMMACHER 01-06-00 CHECKED - DATE - REVISIONS: REVISED - A. SCHUETZE 07-01-13 REVISED - A. SCHUETZE 09-15-16				<b>STATE OF ILLINOIS          DEPARTMENT OF TRANSPORTATION</b>				<b>TRAFFIC CONTROL AND PROTECTION FOR          SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS</b>				F.A.U. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO. 1381 20-00097-00-RS COOK 21 16							
USER NAME = esidorozuk DESIGNED - JC DRAWN - JC CHECKED - MW DATE - 06/22/07 PLOT SCALE = 20' PLOT DATE = 3/29/2021				DESIGNED - JC DRAWN - JC CHECKED - MW DATE - 06/22/07 REVISIONS: REVISED - REVISED - REVISED - REVISED -				<b>STATE OF ILLINOIS          DEPARTMENT OF TRANSPORTATION</b>				<b>DISTRICT 1 DETAILS</b>				F.A.U. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO. 1381 20-00097-00-RS COOK 21 16							
SCALE: NONE				SHEET 1 OF 1 SHEETS				STA. TO STA.				SCALE: NONE				SHEET NO. OF SHEETS				STA. TO STA.			
ILLINOIS FED. AID PROJECT CONTRACT NO. 61H11																							

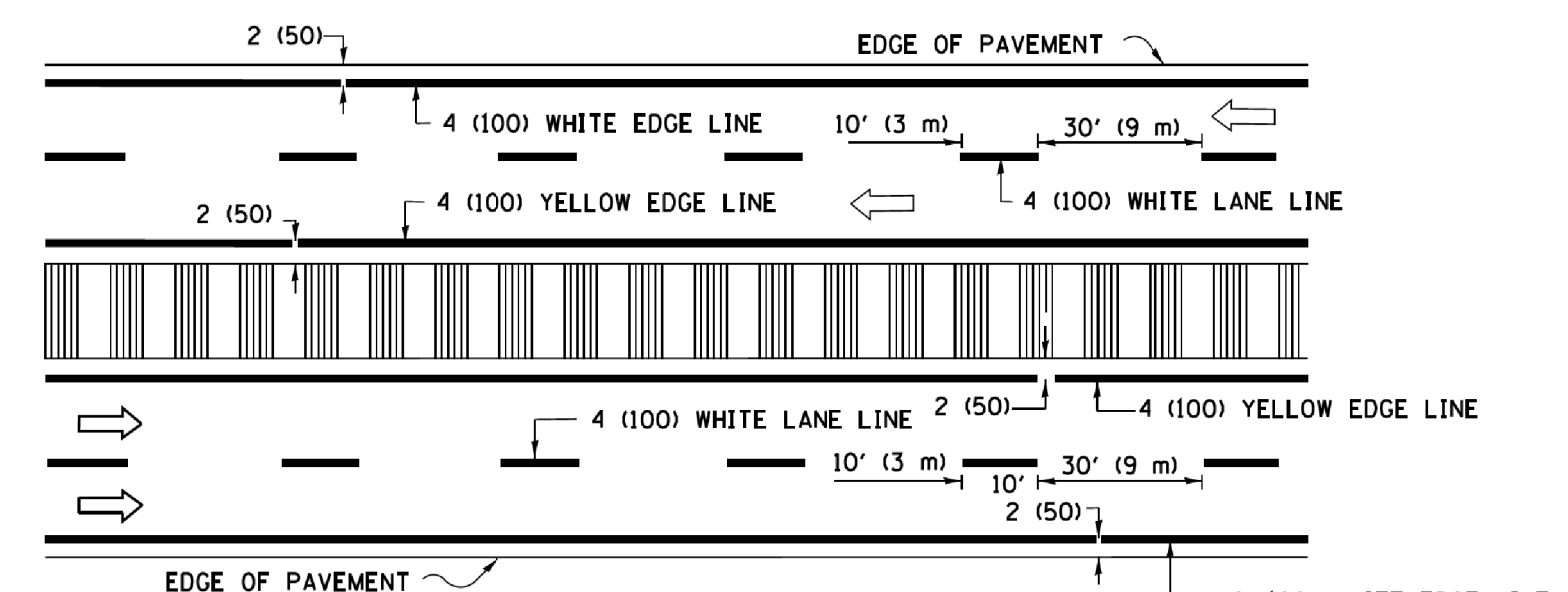




**2-LANE ROADWAY**

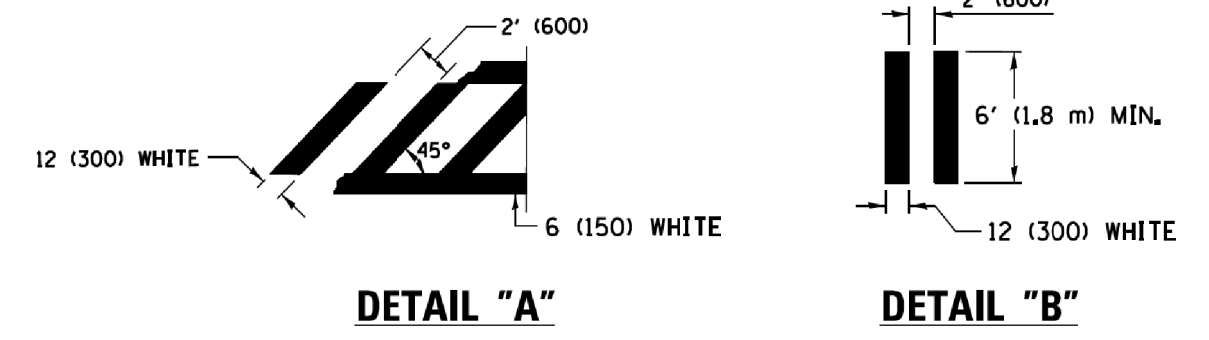
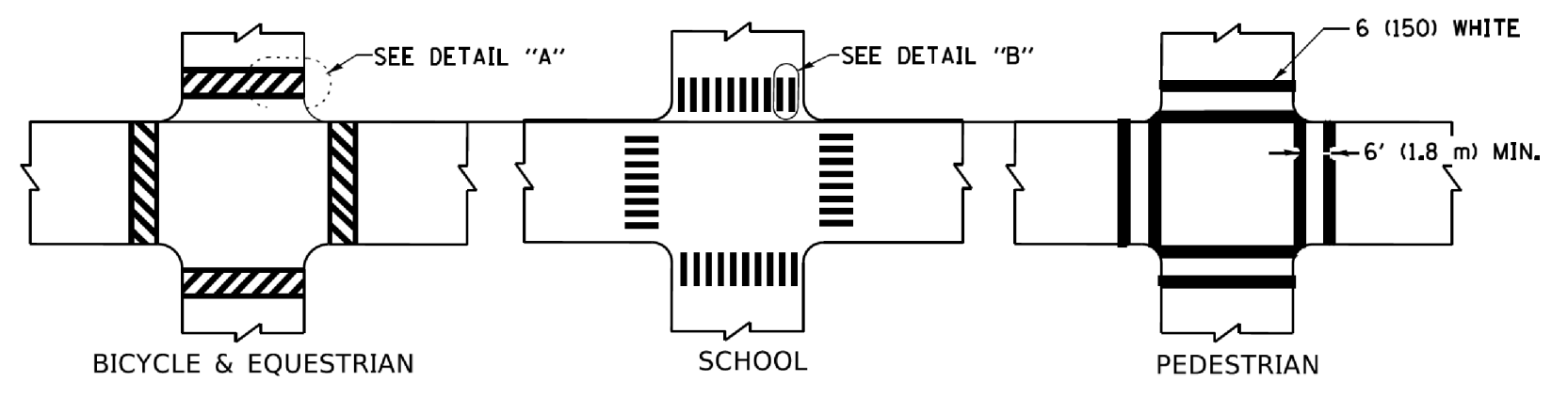


**MULTI-LANE UNDIVIDED**



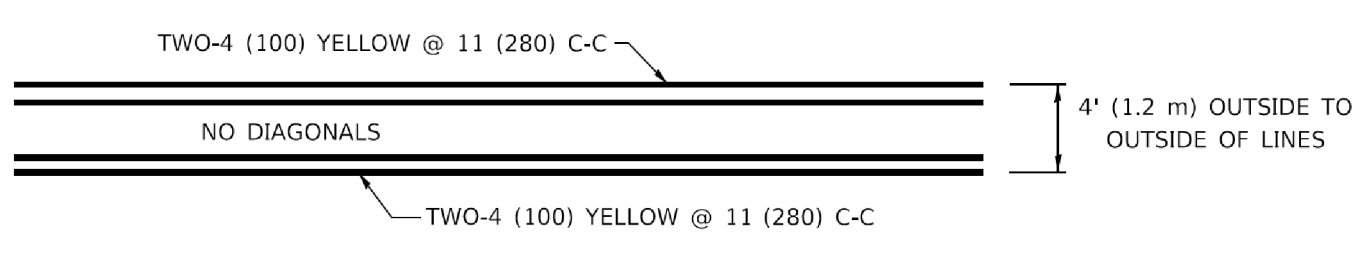
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

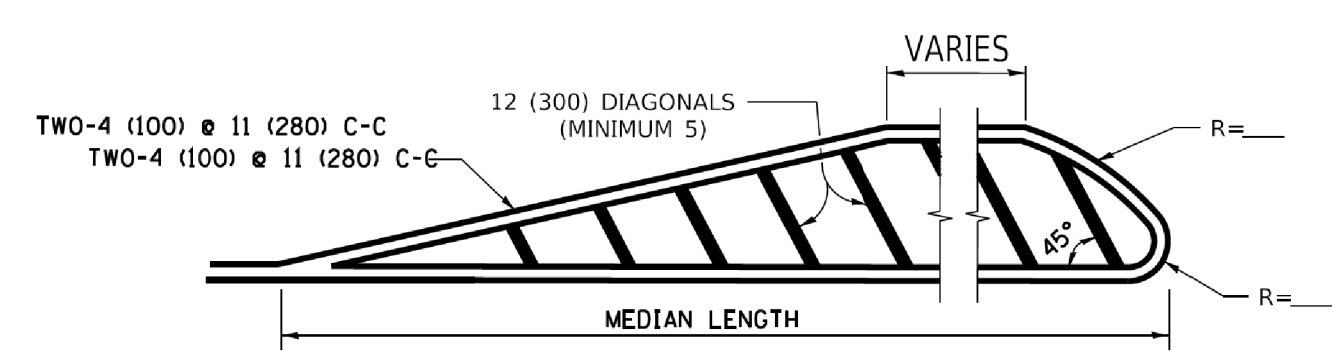


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

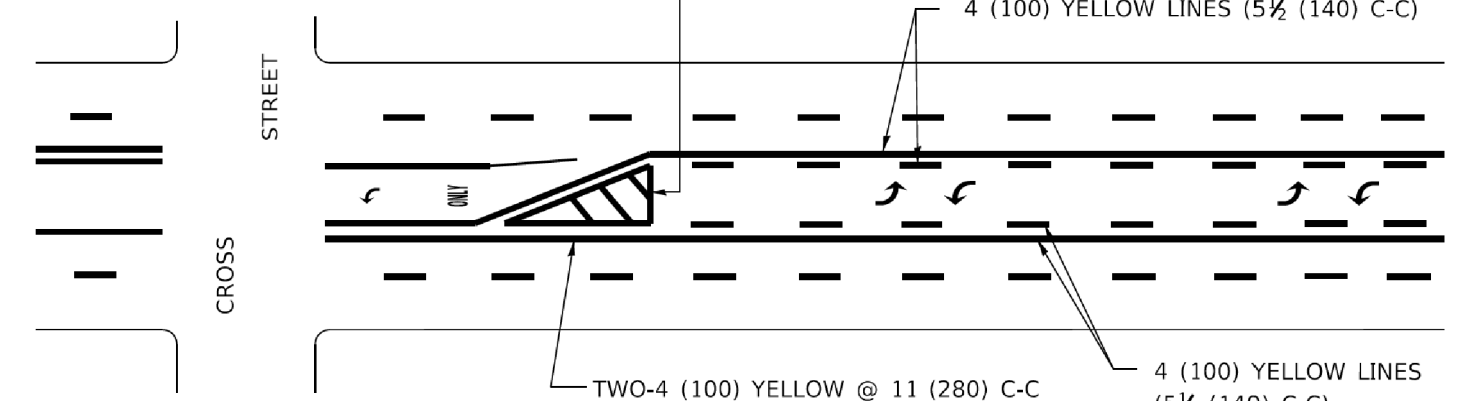


**4' (1.2 m) WIDE MEDIANS ONLY**

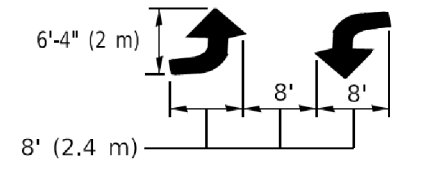


**MEDIANS OVER 4' (1.2 m) WIDE**

FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

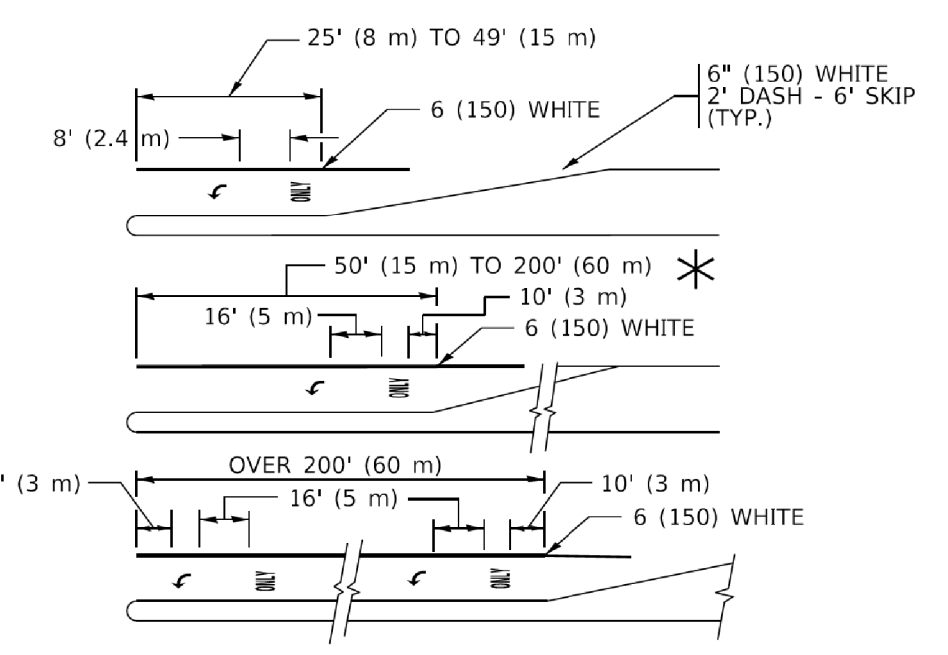


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

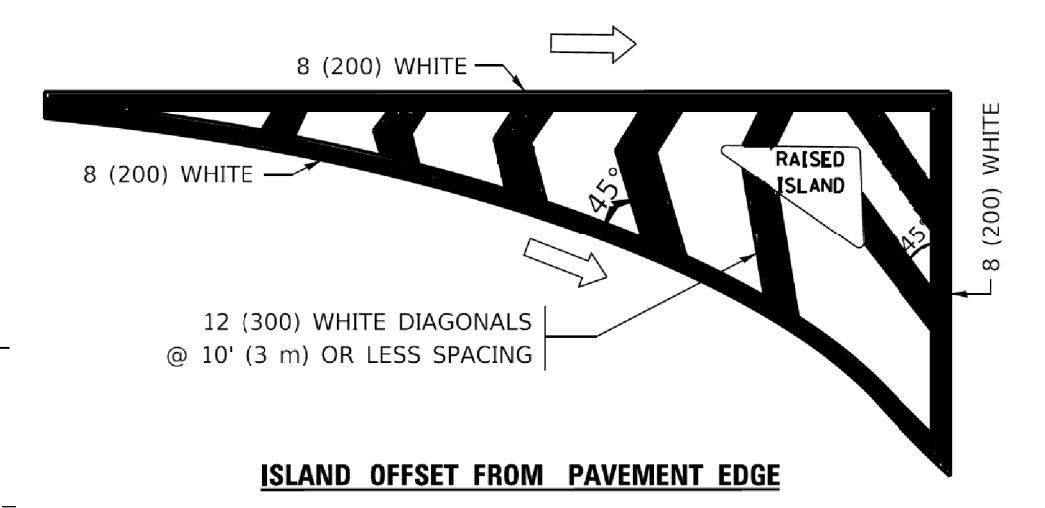
**TYPICAL PAINTED MEDIAN MARKING**



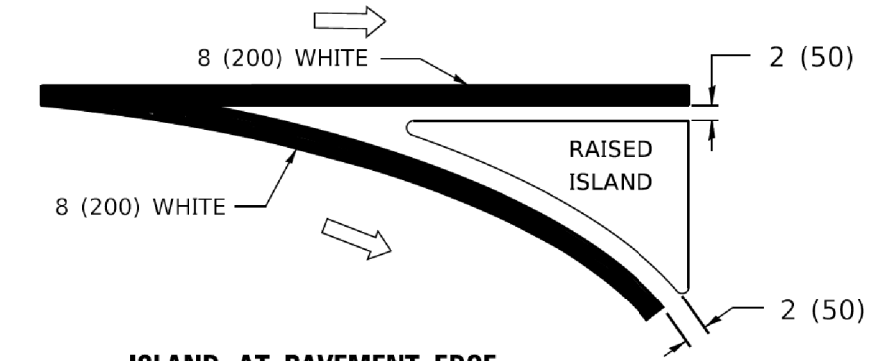
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) \* ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

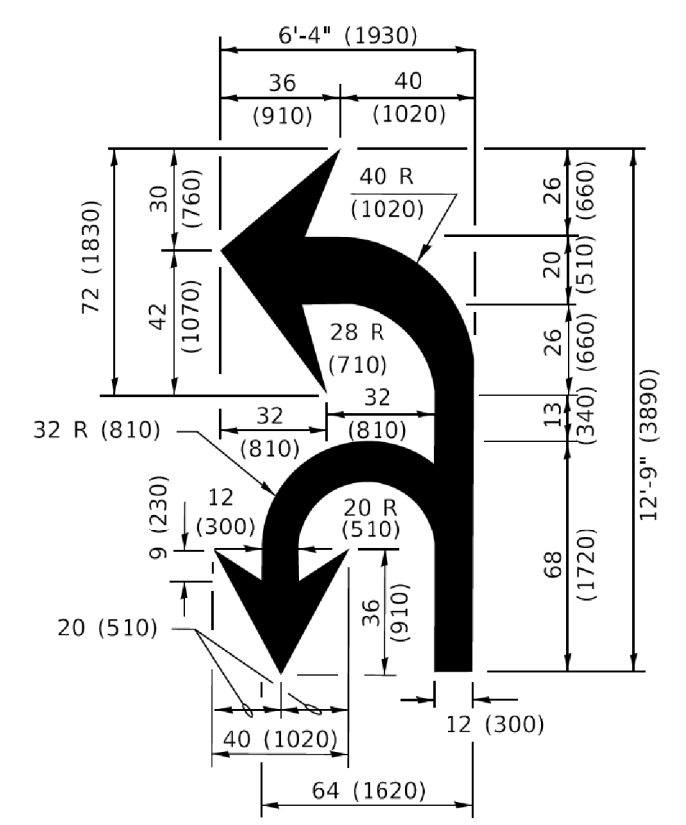


**ISLAND OFFSET FROM PAVEMENT EDGE**

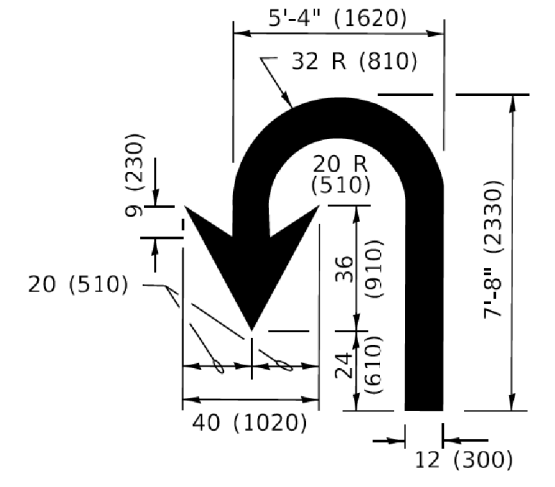


**ISLAND AT PAVEMENT EDGE**

**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footemj	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - C. JUCIUS 07-01-13
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
		REVISED - C. JUCIUS 04-12-16

<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	
<b>DISTRICT ONE TYPICAL PAVEMENT MARKINGS</b>	
SCALE: NONE	SHEET 1 OF 2 SHEETS STA. TO STA.

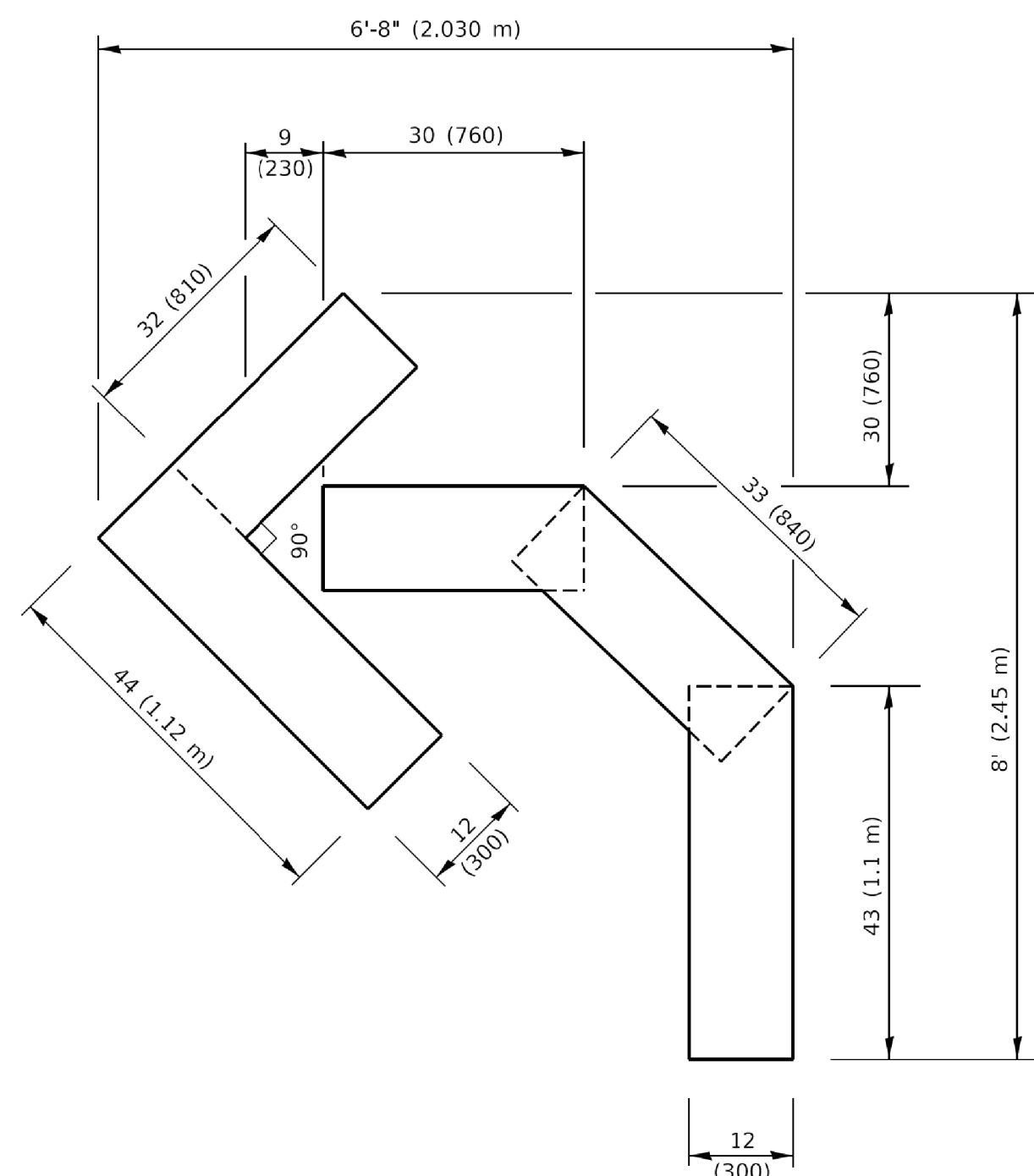
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	<b>TC-13</b>			
ILLINOIS		FED. AID PROJECT		

USER NAME = esidorozuk	DESIGNED - JC	REVISED -
PLOT SCALE = 20'	CHECKED - MW	REVISED -
PLOT DATE = 3/29/2021	DATE - 06/22/07	REVISED -

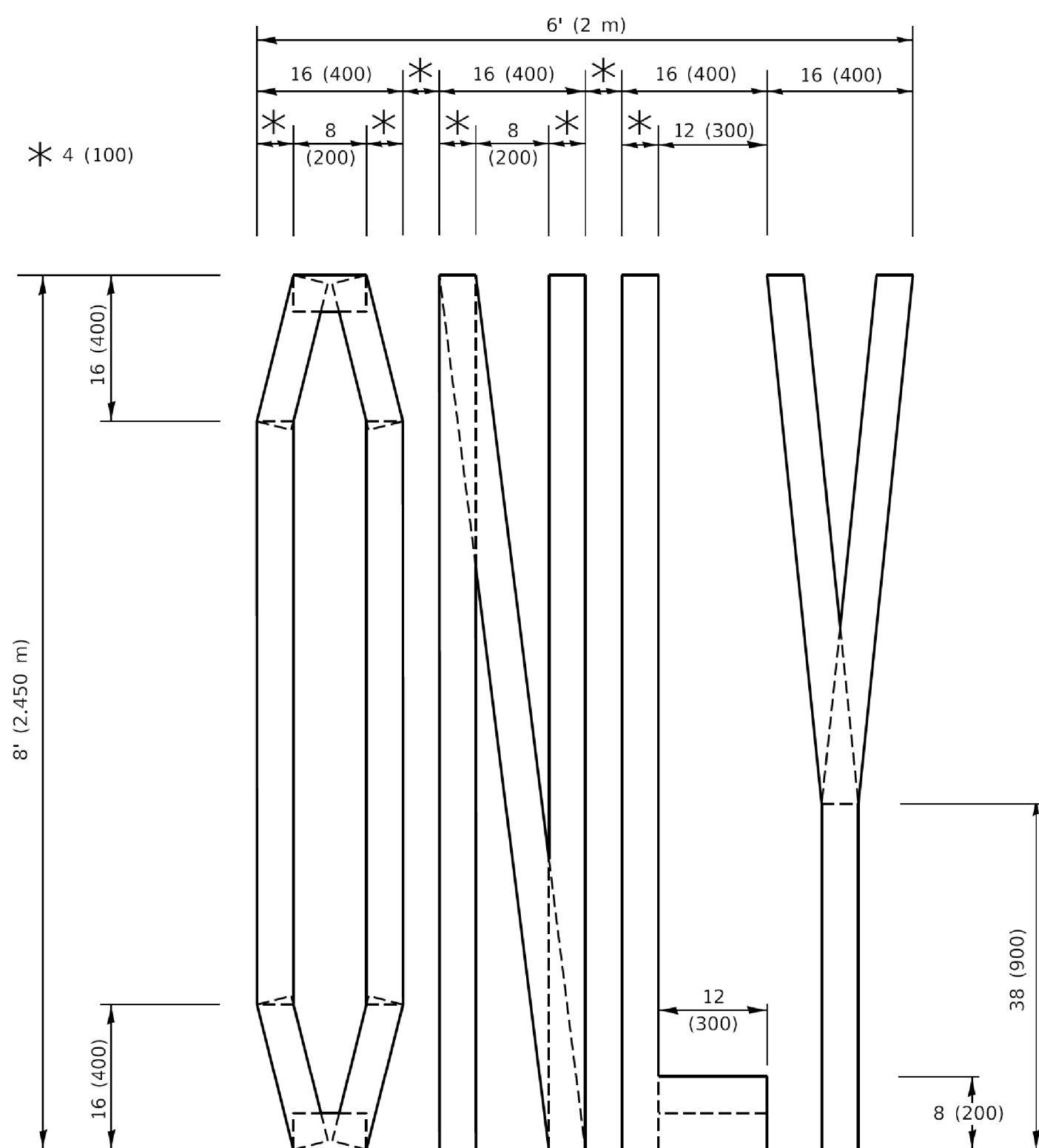
<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	
<b>DISTRICT 1 DETAILS</b>	
SCALE: _____	SHEET NO. ____ OF ____ SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1381	20-00097-00-RS	COOK	21	17
ILLINOIS		FED. AID PROJECT		
CONTRACT NO. 61H11				

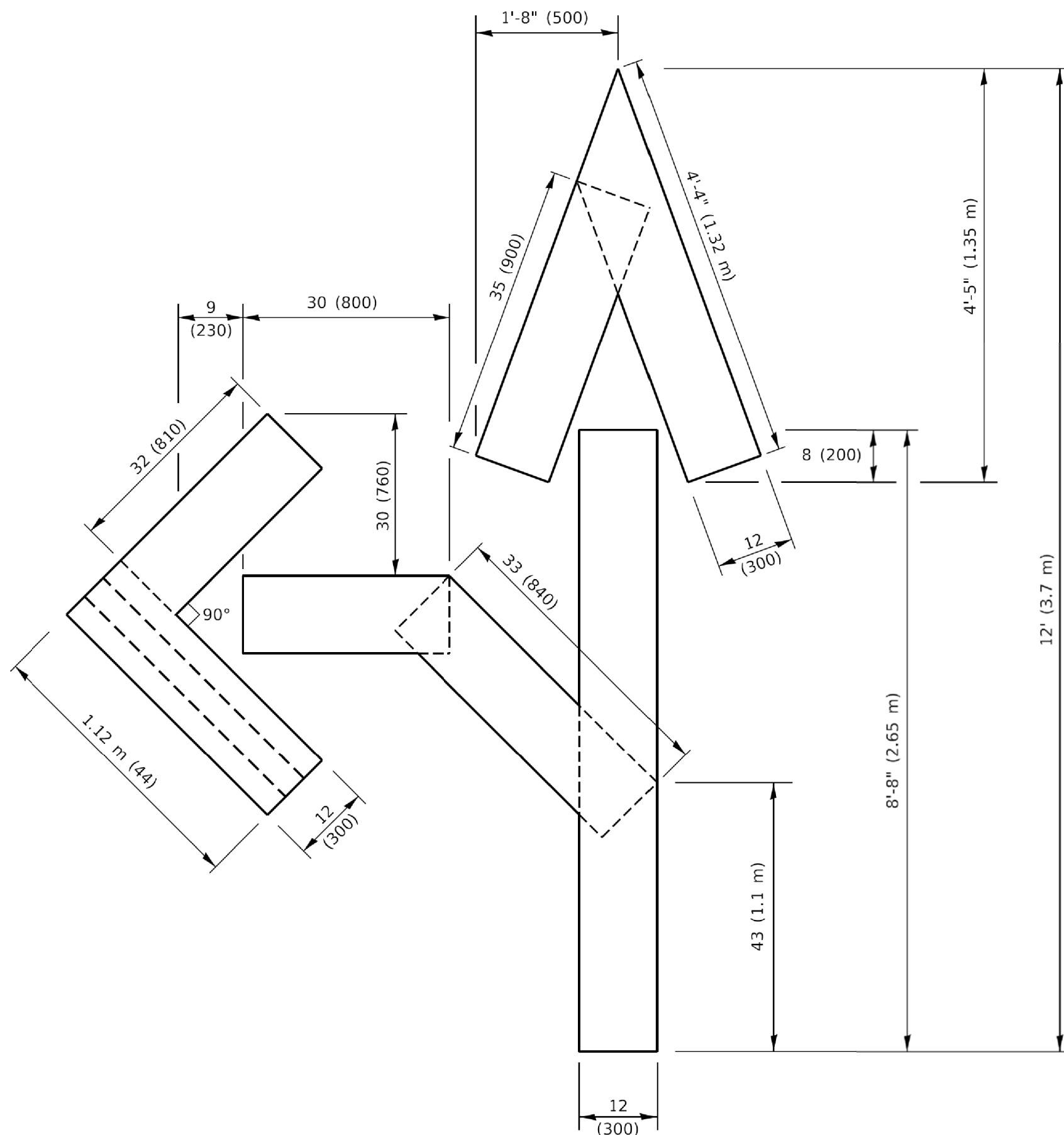
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**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

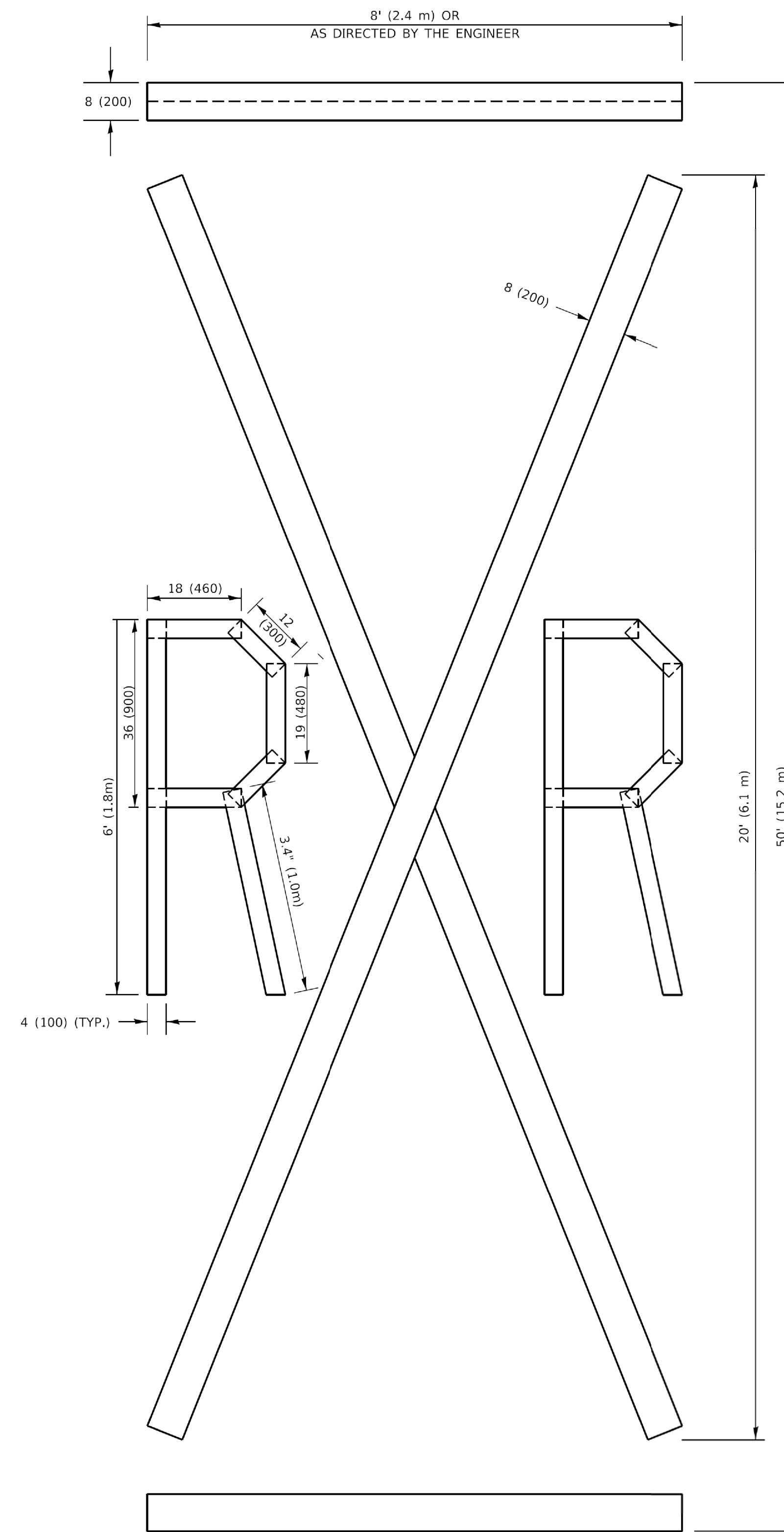


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED  
 IN LINEAR FEET OF 4" LINES TO MATCH THE  
 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS  
 THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters)  
 unless otherwise shown.

USER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
PLOT SCALE = 50.0068' / in.	DRAWN - JC	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 3/4/2019	CHECKED -	REVISED - E. GOMEZ 08-28-00
	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TC-16			CONTRACT NO.	
ILLINOIS			FED. AID PROJECT	

USER NAME = esidorozuk	DESIGNED - JC	REVISED -
PLOT SCALE = 20'	DRAWN - JC	REVISED -
PLOT DATE = 3/29/2021	CHECKED - MW	REVISED -
	DATE - 06/22/07	REVISED -

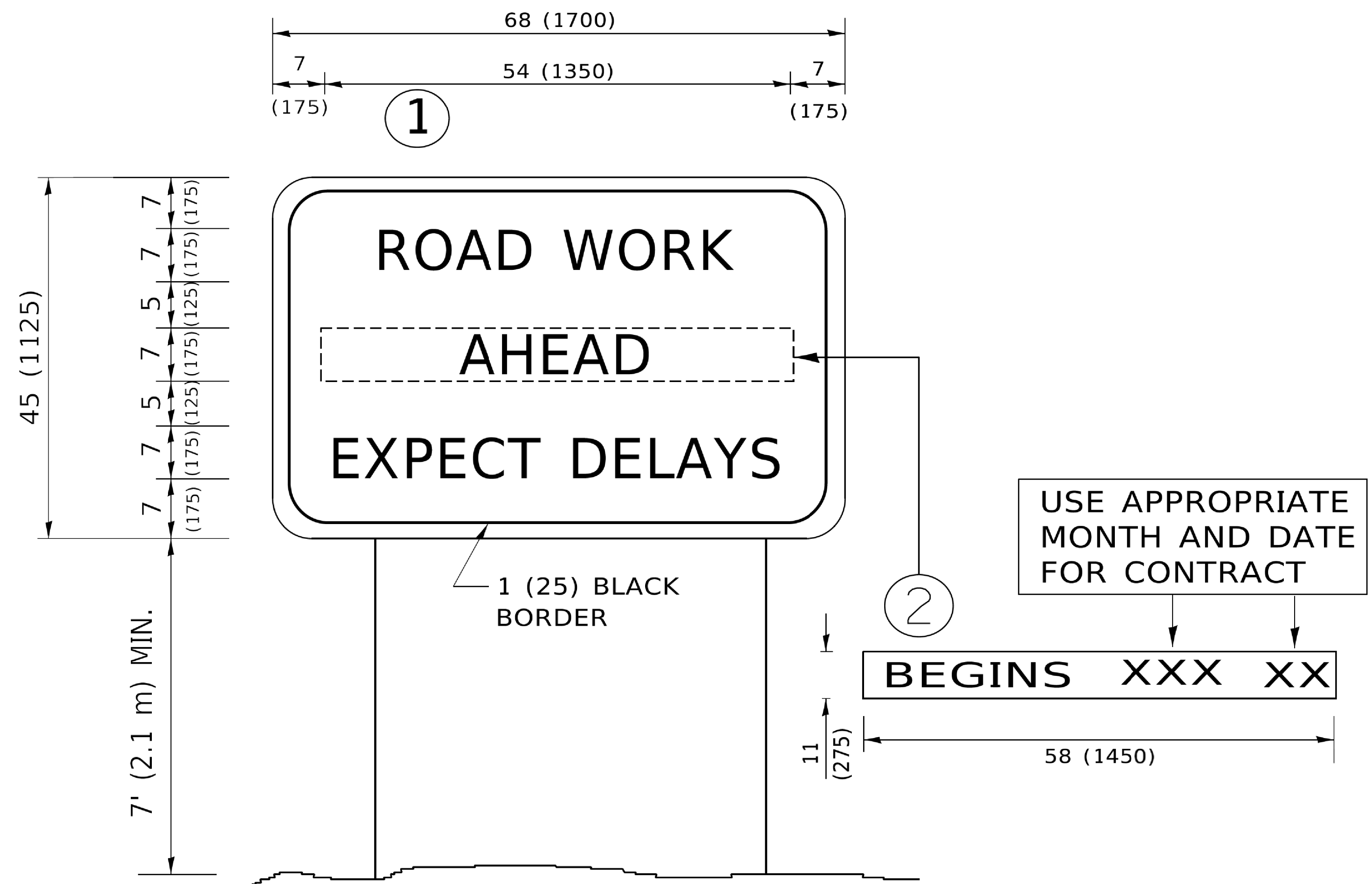
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 DETAILS**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1381	20-00097-00-RS	COOK	21	18
			CONTRACT NO. 61H11	
ILLINOIS			FED. AID PROJECT	

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 3/29/2021 10:27:36 AM User:footemj



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL Default  
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USER NAME = footemj	DESIGNED -	REVISED - R. MIRS 09-15-97
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07

<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	
<b>ARTERIAL ROAD INFORMATION SIGN</b>	
SCALE: NONE	SHEET 1 OF 1 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	<b>TC-22</b>			
		CONTRACT NO.		

ILLINOIS	FED. AID PROJECT
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USER NAME = esidorozuk	DESIGNED - JC	REVISED -
PLOT SCALE = 20'	CHECKED - MW	REVISED -
PLOT DATE = 3/29/2021	DATE - 06/22/07	REVISED -

<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	
<b>DISTRICT 1 DETAILS</b>	
SCALE: _____	SHEET NO. ___ OF ___ SHEETS

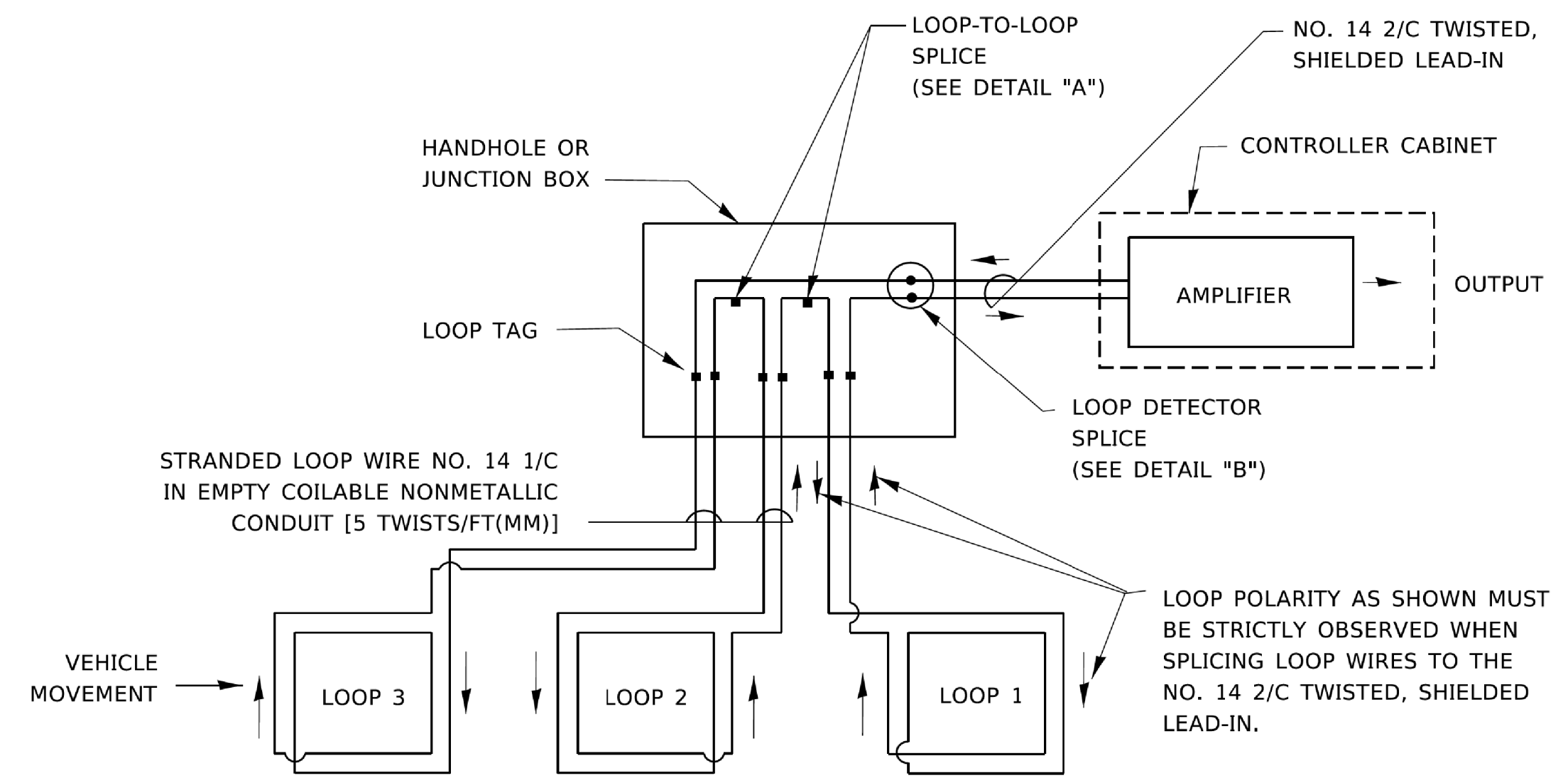
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1381	20-00097-00-RS	COOK	21	19
		CONTRACT NO. 61H11		

ILLINOIS	FED. AID PROJECT
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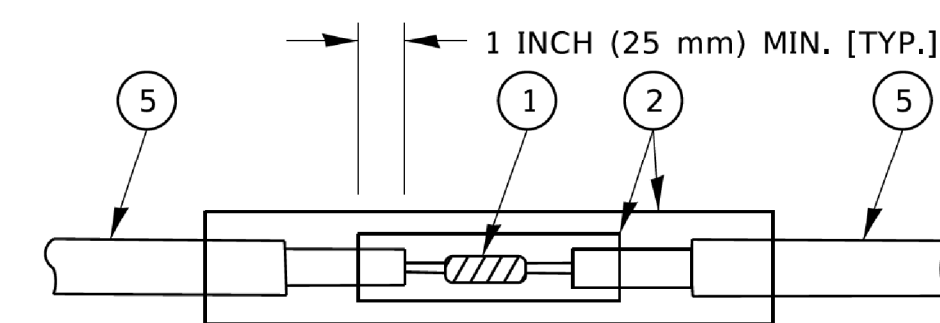
**LOOP DETECTOR NOTES**

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

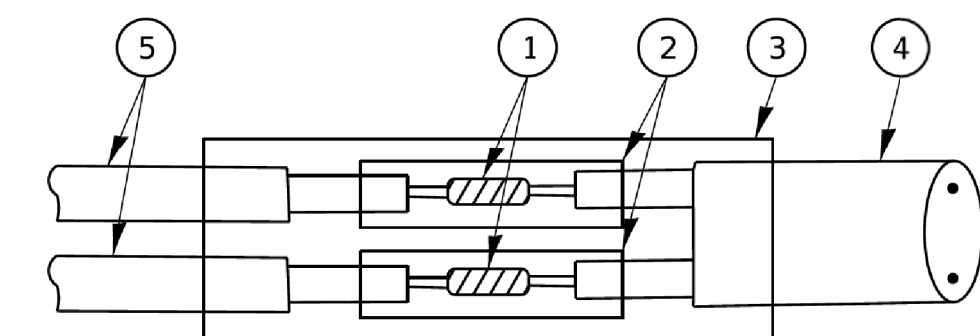


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

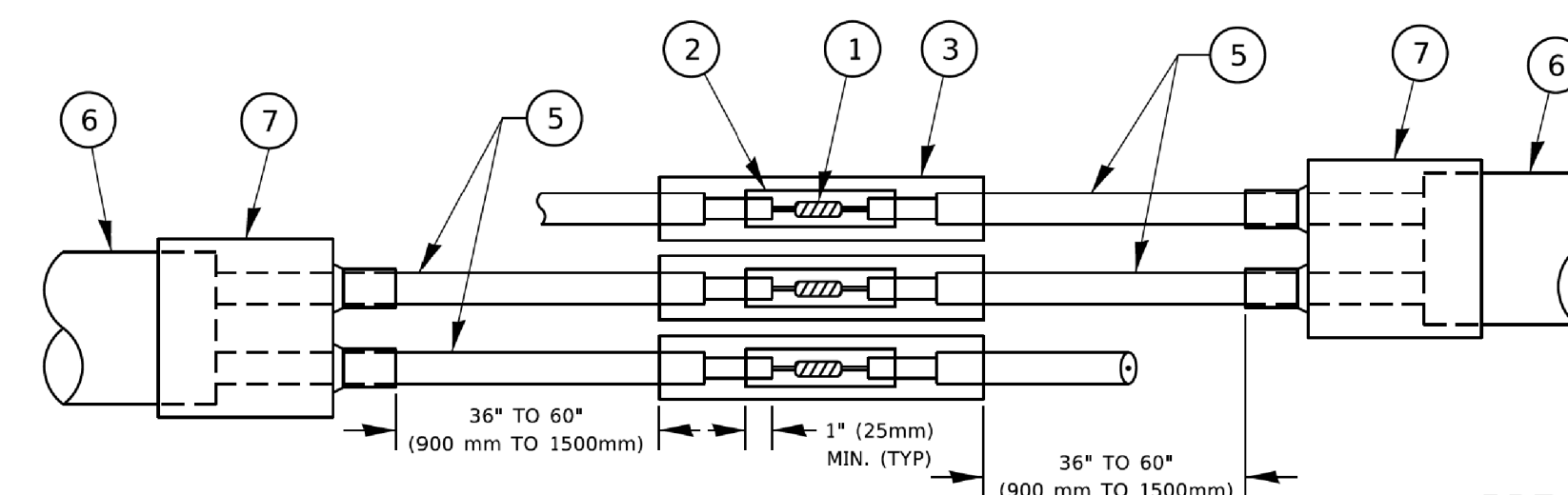


DETAIL "A"  
LOOP-TO-LOOP SPLICE

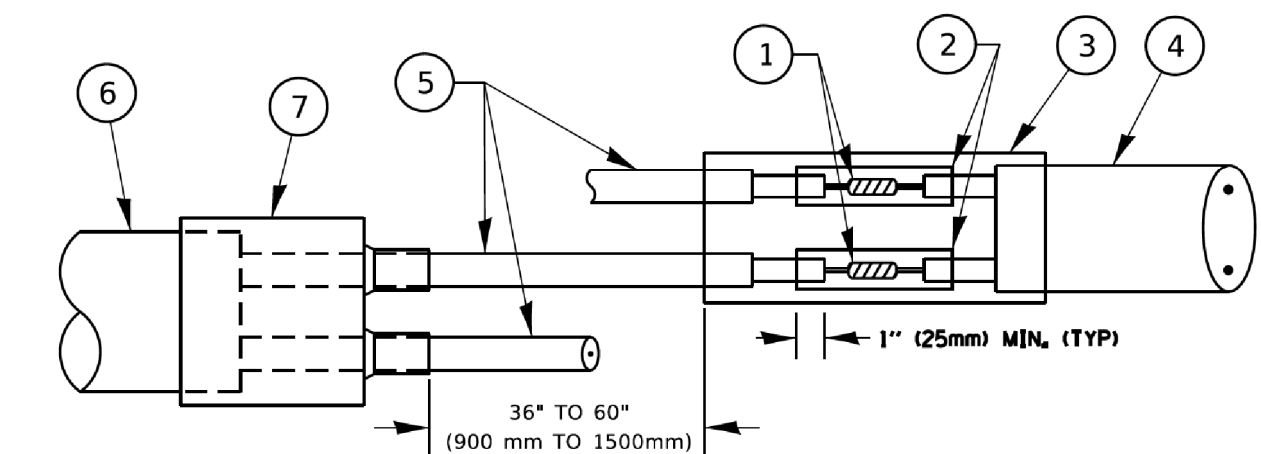


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**TYPE I LOOP**



DETAIL "A"  
LOOP-TO-LOOP SPLICE



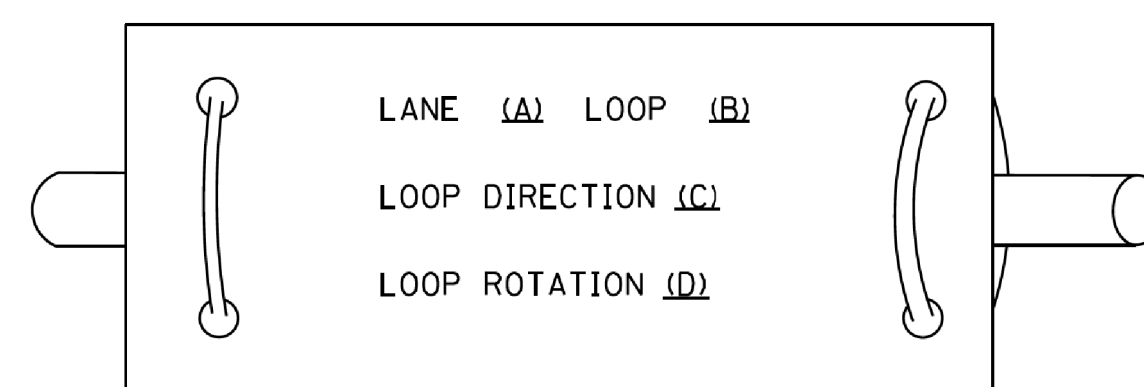
DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**PREFORMED LOOP**

**LOOP DETECTOR SPLICE**

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PREFORMED LOOP
- ⑥ XL POLYOLEFIN 2 CONDUCTOR
- ⑦ BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

**LOOP LEAD-IN CABLE TAG**



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

USER NAME = footemj	DESIGNED -	REVISED -
	DRAWN - JC	REVISED -
PLOT SCALE = 50,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 3/4/2019	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TS-05		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		

USER NAME = esidorozuk	DESIGNED - JC	REVISED -
	DRAWN - JC	REVISED -
PLOT SCALE = 20'	CHECKED - MW	REVISED -
PLOT DATE = 3/29/2021	DATE - 06/22/07	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

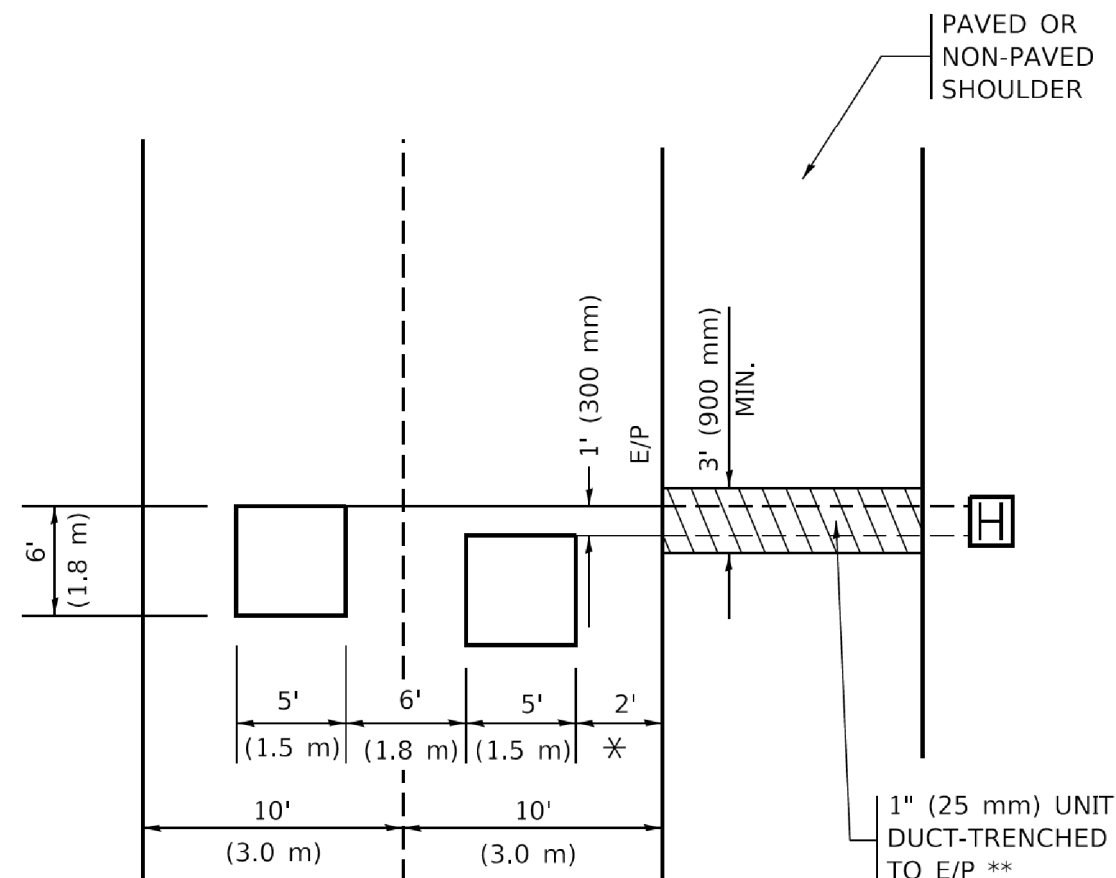
DISTRICT 1 DETAILS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1381	20-00097-00-RS	COOK	21	20
		CONTRACT NO. 61H11		
ILLINOIS		FED. AID PROJECT		

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 FILE NAME: P:\V\04\EBID\INTEC...

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

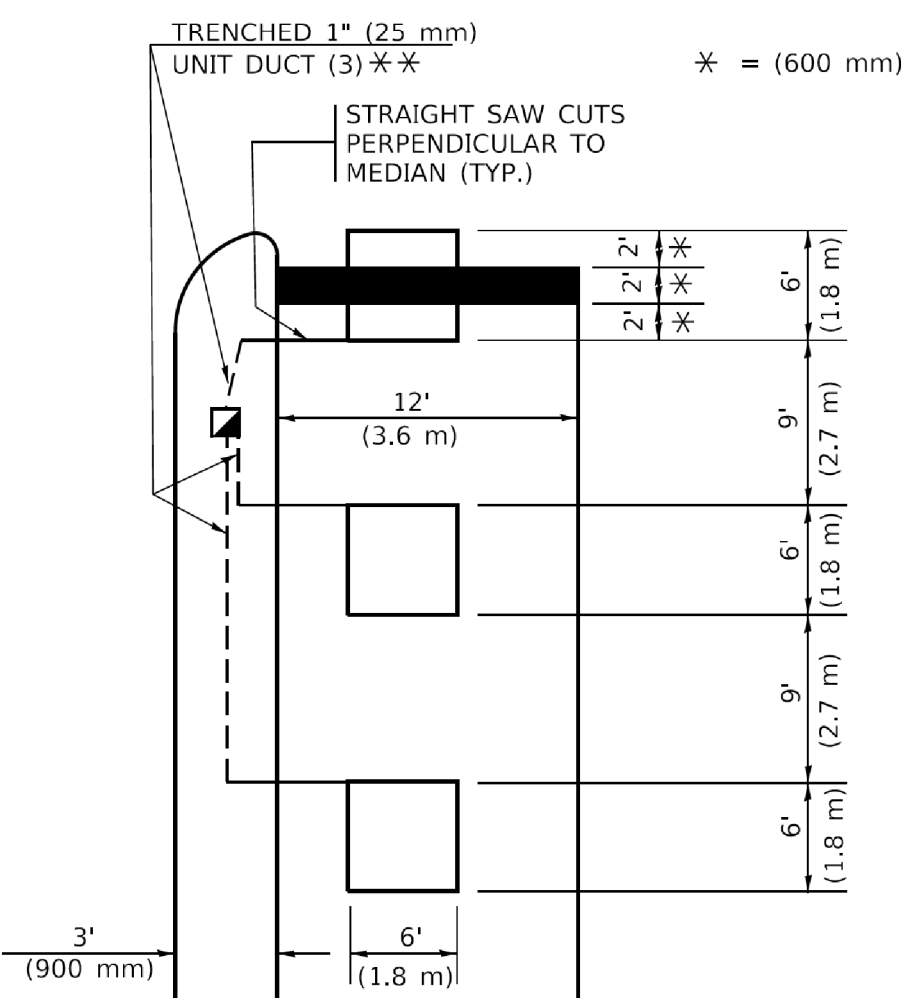


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS**  
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

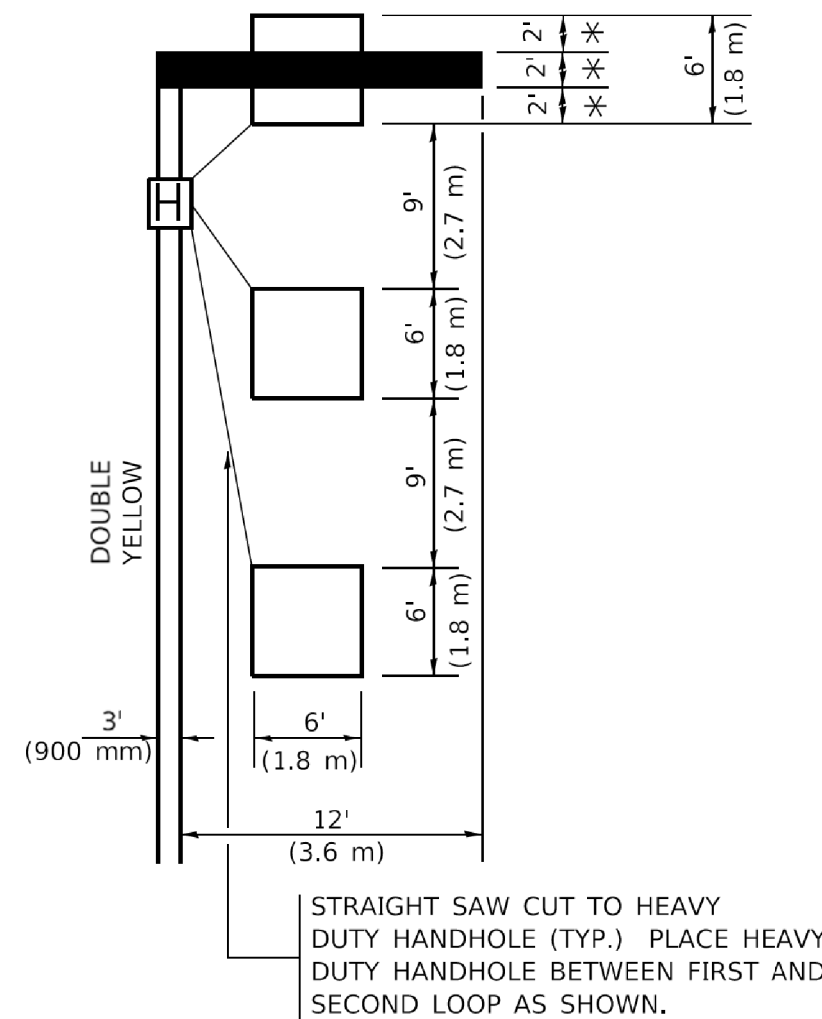


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS**  
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)

\* = (600 mm)



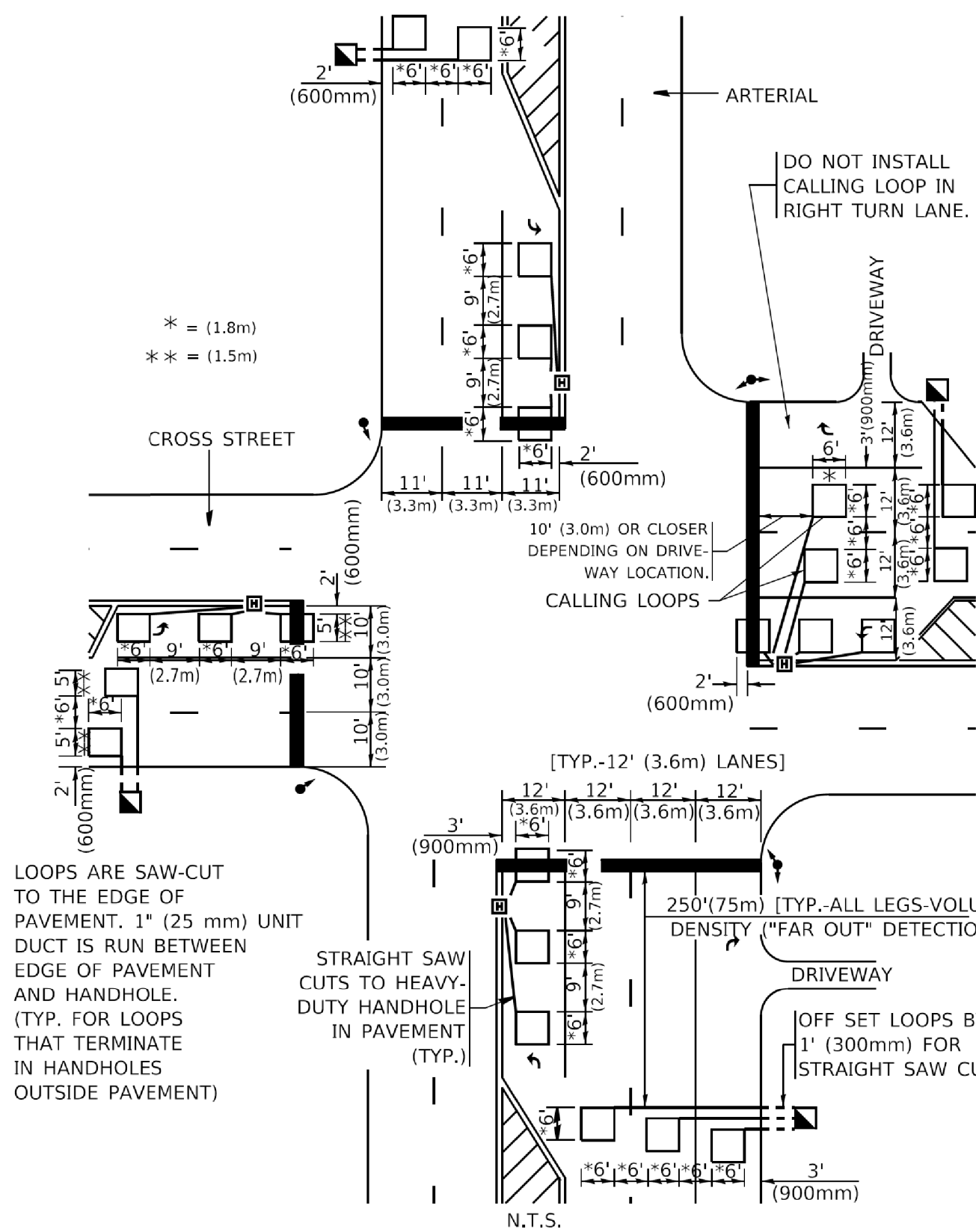
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

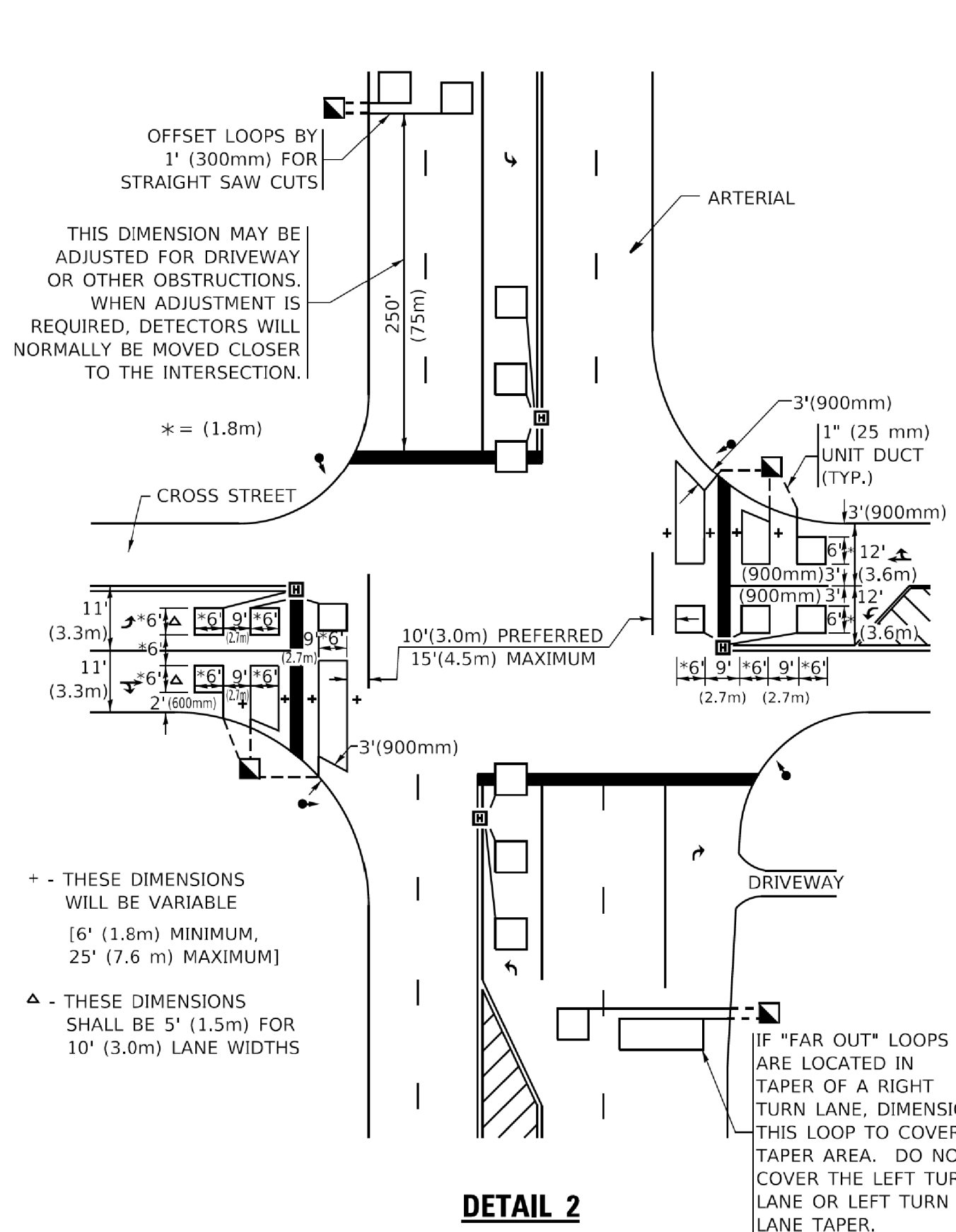
- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)**  
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)**  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



**DETAIL 2**  
N.T.S.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

USER NAME = footemj	DESIGNED -	REVISED -
PLOT SCALE = 50.0000' / in.	DRAWN - JC	REVISED -
PLOT DATE = 3/4/2019	CHECKED - R.K.F.	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION**  
**DETAILS FOR ROADWAY RESURFACING**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TS-07			CONTRACT NO.	

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

USER NAME = esidorozuk	DESIGNED - JC	REVISED -
	DRAWN - JC	REVISED -
PLOT SCALE = 20'	CHECKED - MW	REVISED -
PLOT DATE = 3/29/2021	DATE - 06/22/07	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 DETAILS**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1381	20-00097-00-RS	COOK	21	21
			CONTRACT NO. 61H11	

ILLINOIS FED. AID PROJECT