

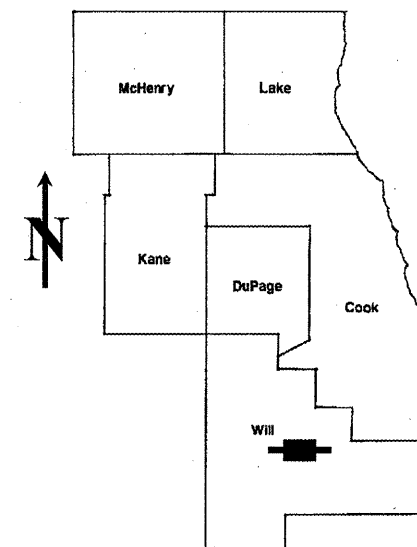
| ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------|------------|--------|--------------|--------------|
| VARIOUS | 2010-016PP | WILL | 19* | 1 |

* 19 + 3 = 22

CONTRACT NO. 60K35

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
DISTRICT ONE
PROPOSED HIGHWAY PLANS

D-91-474-10



LOCATION OF IMPROVEMENT INDICATED THUS: 

FOR INDEX OF SHEETS SEE SHEET 2

VARIOUS ROUTES
SECTION: 2010-016PP
VARIOUS LOCATIONS IN WILL COUNTY
PCC PAVEMENT PATCHING
WILL COUNTY
C-91-474-10

CONTRACT NO. 60K35

DISTRICT ONE - DESIGN - PLAN PREPARATION ENGINEER:
KEN ENG / (847) 705-4247

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED: March 22 2010

Diane Sklar
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 7 2010
Scott E. Stitt P.E. J.E.
acting ENGINEER OF DESIGN AND ENVIRONMENT

May 7 2010
Christine M. Reed J.E.
DIRECTOR, DIVISION OF HIGHWAYS

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

J.U.L.I.E.: JOINT UTILITY LOCATION
INFORMATION FOR EXCAVATION
(312) 744-7000

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

| <u>SHEET NO.</u> | <u>DESCRIPTION</u> | <u>STANDARD NO.</u> | <u>DESCRIPTION</u> |
|------------------|--|---------------------|--|
| 1 | TITLE SHEET | 000001-05 | TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 2 | INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES | 420001-07 | PAVEMENT JOINTS |
| 3 | SUMMARY OF QUANTITIES | 420701-02 | PAVEMENT FABRIC |
| 4 | GENERAL LOCATION MAP | 421001-02 | BAR REINFORCEMENT FOR CRC PAVEMENT |
| 5 | SUMMARY OF PATCHING SCHEDULE | 442001-04 | CLASS A PATCHES |
| 6-8 | PATCHING SCHEDULE | 442101-07 | CLASS B PATCHES |
| 9-9A | EXISTING 26 FT. - CONTINUOUSLY REINFORCED PCC PAVEMENT (WITH LUG SYSTEM) DETAIL (FOR INFORMATION ONLY) | 701400-04 | APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY |
| 10 | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT | 701401-05 | LANE CLOSURE, FREEWAY/EXPRESSWAY |
| 11 | FREEWAY SINGLE AND MULTI-LANE WEAVE | 701411-06 | LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP |
| 12 | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS | 701426-03 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS |
| 13 | TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) | 701601-06 | URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN |
| 14-15 | MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS | 701701-06 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 16 | DISTRICT ONE TYPICAL PAVEMENT MARKINGS | 701901-01 | TRAFFIC CONTROL DEVICES |
| 17 | TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC) | | |
| 18 | ARTERIAL ROAD INFORMATION SIGN | | |
| 19 | STANDARD TRAFFIC SIGNAL DESIGN DETAILS | | |
| 19A | FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS | | |
| 19B | TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES | | |

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (OR ISTHA)

WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT MS. CORA MATHIS, AREA TRAFFIC FIELD ENGINEER AT (815) 485-6475 MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

THE EXISTING ROADWAY TYPICAL SECTION IS ASSUMED TO BE 10 INCHES OF PORTLAND CEMENT CONCRETE (PCC) PAVEMENT.

NO PATCHING IS TO BE DONE WITH IN 50 FEET OF ANY RAILROAD.

THE MINIMUM CLASS A PATCH DIMENSIONS SHALL BE A LENGTH OF 4.5 FEET AND A WIDTH THAT INCLUDES HALF THE WIDTH OF THE TRAVEL WAY. THE MINIMUM CLASS B PATCH DIMENSIONS SHALL BE A LENGTH OF 6 FEET AND A WIDTH THAT INCLUDES THE FULL WIDTH OF THE TRAVEL WAY.

FOR ALL PATCHING ON I-80 AT HOUBOLT RD, CLASS PP-2 PORTLAND CEMENT CONCRETE IS TO BE USED.

JOINT SEALING FOR CLASS B PATCHES IS TO BE REPLACED WITH A SOLID PLASTIC BOND BREAKER (1/8" X 1/3"; WHERE T IS EQUAL TO THE THICKNESS OF THE PATCH). THE COST OF THE SOLID PLASTIC BOND BREAKER IS TO BE INCLUDED IN THE COST OF THE CLASS B PATCH.

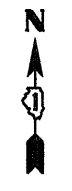
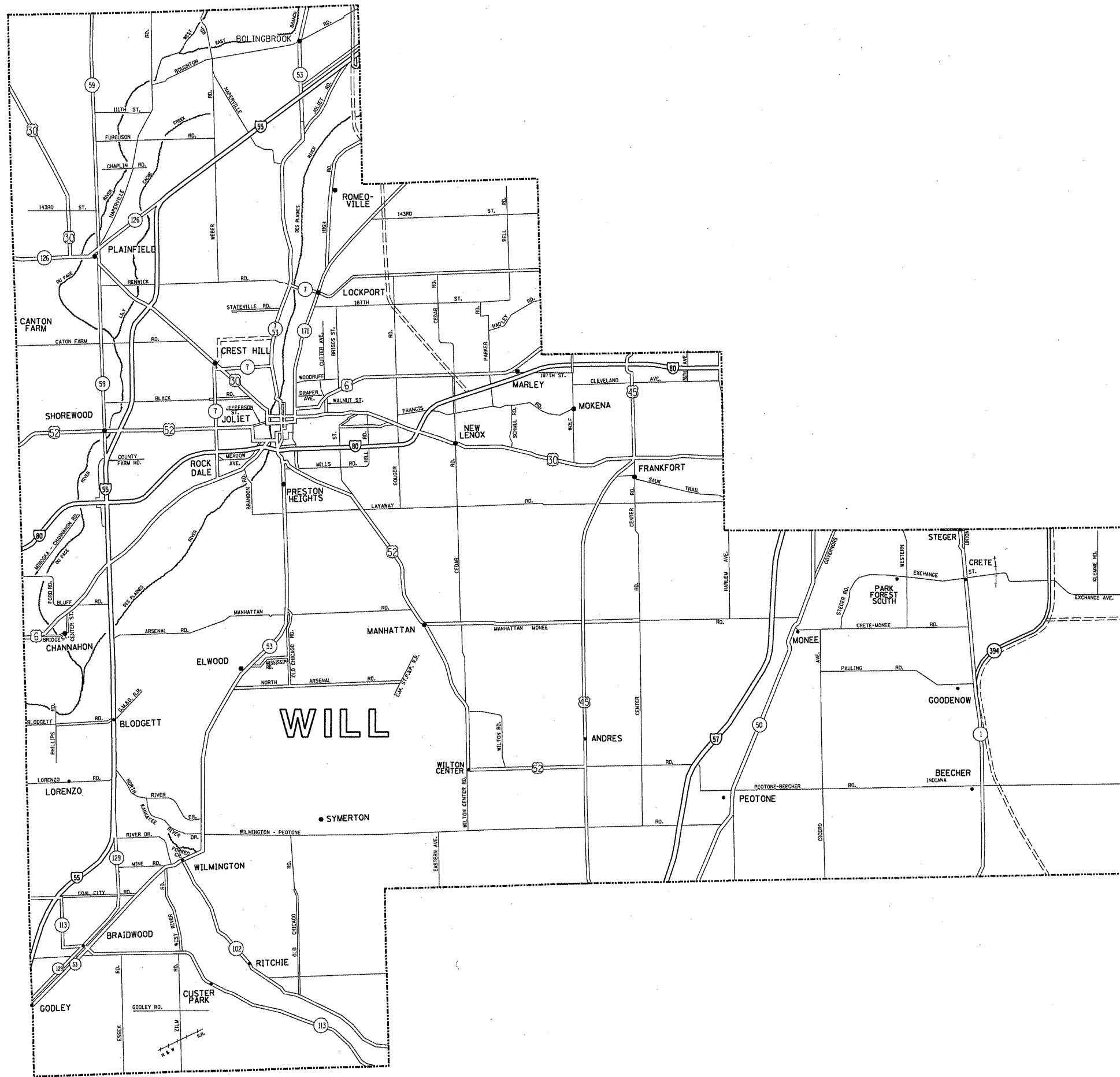
| | | | | | | | | | | |
|---|-----------------------|------------|---|---|---|------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = wlgreendp | DESIGNED - | REVISED - 10/28/10 - DW | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwork\pwork\wlgreendp\0183633\Design.dgn | DRAWN - | REVISED - | VAR. | | | 2010-016PP | WILL | 19 | 2 | |
| PLOT SCALE = 100.0000 / IN. | CHECKED - | REVISED - | CONTRACT NO. 60K35 | | | | | | | |
| PLOT DATE = 4/29/2010 | DATE - | REVISED - | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | |
| SCALE: | | | | | | SHEET NO. | OF | SHEETS | STA. | TO STA. |

| SUMMARY OF QUANTITIES | | | URBAN 100% STATE TOTAL QUANTITIES | | CONSTRUCTION TYPE CODE | | | | | SUMMARY OF QUANTITIES | | | URBAN 100% STATE TOTAL QUANTITIES | | CONSTRUCTION TYPE CODE | | | | |
|-----------------------|--|--------|---|--------------|------------------------|--|--|--|--|-----------------------|-------------------|------|---|------|------------------------|--|--|--|--|
| CODE NO | ITEM | UNIT | | 1000 | | | | | | CODE NO | ITEM | UNIT | | 1000 | | | | | |
| 42101300 | PROTECTIVE COAT | SO YD | 1824 | 1824 | | | | | | Z0017202 | DOWEL BARS 1 1/2" | EACH | 3720 | 3720 | | | | | |
| 44001700 | COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT | FOOT | 100 | 100 | | | | | | Z0075310 | TIE BARS 3/4" | EACH | 100 | 100 | | | | | |
| 44200577 | CLASS A PATCHES, TYPE II, 12 INCH | SO YD | 32 | 32 | | | | | | | | | | | | | | | |
| 44200970 | CLASS B PATCHES, TYPE II, 10 INCH | SO YD | 1418 | 1418 | | | | | | | | | | | | | | | |
| 44200974 | CLASS B PATCHES, TYPE III, 10 INCH | SO YD | 220 | 220 | | | | | | | | | | | | | | | |
| 44200976 | CLASS B PATCHES, TYPE IV, 10 INCH | SO YD | 132 | 132 | | | | | | | | | | | | | | | |
| 44213000 | PATCHING REINFORCEMENT | SO YD | 32 | 32 | | | | | | | | | | | | | | | |
| 44213100 | PAVEMENT FABRIC | SO YD | 352 | 352 | | | | | | | | | | | | | | | |
| 44213200 | SAW CUTS | FOOT | 10,887 | 10,887 | | | | | | | | | | | | | | | |
| 60300305 | FRAMES AND LIDS TO BE ADJUSTED | EACH | 5 | 5 | | | | | | | | | | | | | | | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 6 | 6 | | | | | | | | | | | | | | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | | | | | | | | | | | | | | | |
| 70102625 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701606 | L SUM | 1 | 1 | | | | | | | | | | | | | | | |
| 70102630 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701601 | L SUM | 1 | 1 | | | | | | | | | | | | | | | |
| 70102635 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701701 | L SUM | 1 | 1 | | | | | | | | | | | | | | | |
| 70103815 | TRAFFIC CONTROL SURVEILLANCE | CAL DA | 6 | 6 | | | | | | | | | | | | | | | |
| 70106800 | CHANGEABLE MESSAGE SIGN | CAL MO | 1 | 1 | | | | | | | | | | | | | | | |
| *78008200 | POLYUREA PAVEMENT MARKING TYPE I - LETTERS AND SYMBOLS | SO FT | 50 | 50 | | | | | | | | | | | | | | | |
| *78008210 | POLYUREA PAVEMENT MARKING TYPE I - LINE 4" | FOOT | 450 | 450 | | | | | | | | | | | | | | | |
| *78008220 | POLYUREA PAVEMENT MARKING TYPE I - LINE 5" | FOOT | 24 | 24 | | | | | | | | | | | | | | | |
| *78008230 | POLYUREA PAVEMENT MARKING TYPE I - LINE 6" | FOOT | 50 | 50 | | | | | | | | | | | | | | | |
| *78008270 | POLYUREA PAVEMENT MARKING TYPE I - LINE 24" | FOOT | 24 | 24 | | | | | | | | | | | | | | | |
| *78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 50 | 50 | | | | | | | | | | | | | | | |
| *88600600 | DETECTOR LOOP REPLACEMENT | FOOT | 100 | 100 | | | | | | | | | | | | | | | |
| X0322256 | TEMPORARY INFORMATION SIGNING | SO FT | 51.4 | 51.4 | | | | | | | | | | | | | | | |
| X0325702 | NIGHTTIME WORK ZONE LIGHTING | L SUM | 1 | 1 | | | | | | | | | | | | | | | |
| X7011015 | TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS) | L SUM | 1 | 1 | | | | | | | | | | | | | | | |
| X7013820 | TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS | CAL DA | 4 | 4 | | | | | | | | | | | | | | | |

* SPECIALTY ITEM

Rev.

| | | | | | | | | | | | | | | | | |
|---|-----------------------|------------|-----------|---|--|--|--|-----------------------|--|--|--|--------------------|---------------------|--------------|---|-----------|
| FILE NAME = | USER NAME = wlgreendp | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | | | | SUMMARY OF QUANTITIES | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwwork\pwwork\wlgreendp\0183633\Design.dgn | | DRAWN - | REVISED - | | | | | | | | | VAR. | 2010-016PP | WILL | 19 | 3 |
| PLOT SCALE = 1000000' / IN. | | CHECKED - | REVISED - | | | | | | | | | CONTRACT NO. 60K35 | | | | |
| PLOT DATE = 3/30/2010 | | DATE - | REVISED - | | | | | | | | | SCALE: | SHEET NO. OF SHEETS | STA. TO STA. | FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT | |



FILE NAME =
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USER NAME = wilgreendp
 PLOT SCALE = 100.0000' / IN.
 PLOT DATE = 3/30/2010

| | |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

GENERAL LOCATION MAP - WILL COUNTY

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---|------------|--------|--------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2010-016PP | WILL | 19 | 4 |
| FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT | | | CONTRACT NO. 60K35 | |

| PCC PATCHING SUMMARY - WILL COUNTY | CLASS A PATCHES, 12" TYPE II (SY) | CLASS B PATCHES, 10" TYPE II (SY) | CLASS B PATCHES, 10" TYPE III (SY) | CLASS B PATCHES, 10" TYPE IV (SY) |
|--|---|---|--|---|
| RICHARDS ST. (5TH AVE. TO SOUTH OF COLBURN AVE.) | | 1418 | 220 | 132 |
| INTERSTATE 80 AT HOUBOLT RD. | 32 | | | |
| SUMMARY TOTALS: | 32 (SY) | 1418 (SY) | 220 (SY) | 132 (SY) |

| | | | |
|--|-------------------------------|------------|------------------------|
| FILE NAME = | USER NAME = wilgreendp | DESIGNED - | REVISED - 4/28/10 - DW |
| c:\pi_work\PWIDOT\WILGREENDP\d0183633\ | Design.dgn | DRAWN - | REVISED - |
| | PLOT SCALE = 100.0000 ' / IN. | CHECKED - | REVISED - |
| | PLOT DATE = 4/28/2010 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF PATCHING SCHEDULE
WILL COUNTY**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---|------------|--------|--------------------|--------------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2010-016PP | WILL | 19 | 5 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 60K35 | |

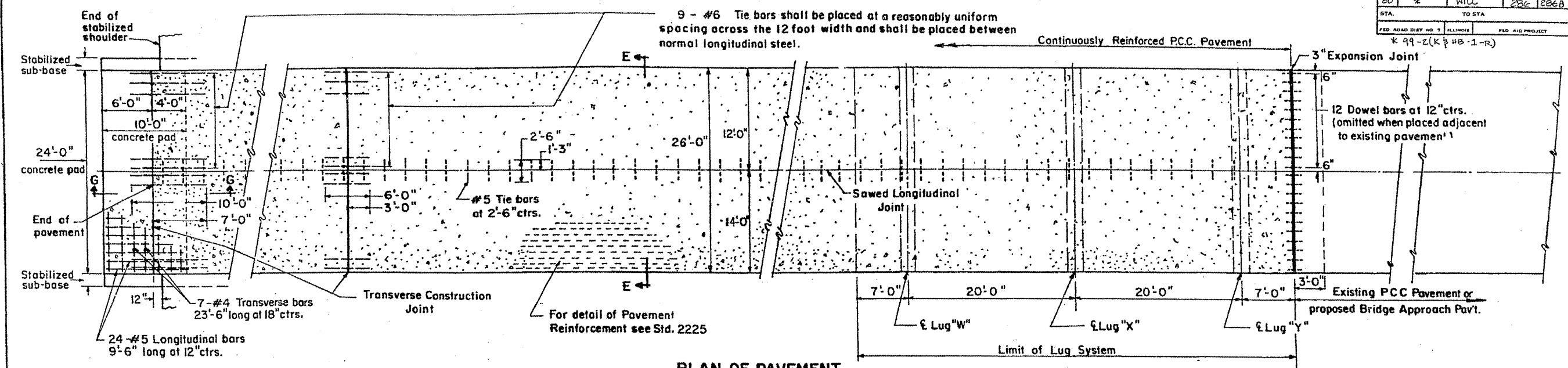
| ROUTE: I-80 at Houbolt Rd. - CRC Pavement Section | | | | PATCHING = CLASS A | | | |
|---|----|-----------|-----------|--------------------|-----------|---------|-----------|
| CROSS STREETS | | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| 50' East of East Approach Slab | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| 50' West of West Approach Slab | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| TOTALS: | | | | | 24 | | 32 |
| | | | | | FT | | SY |

NOTE:

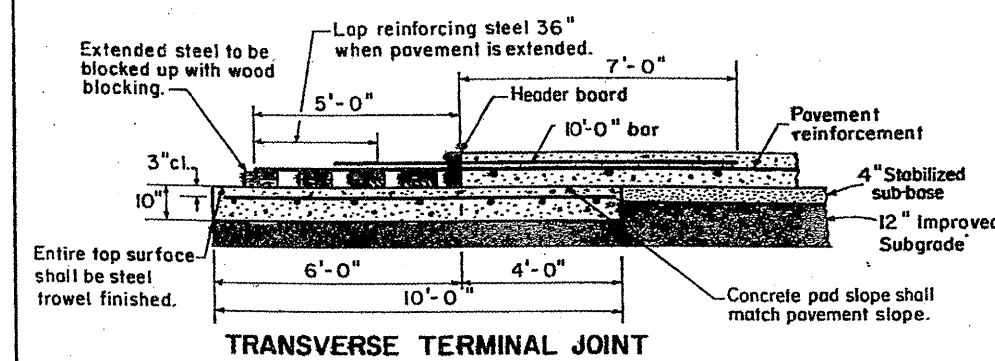
PATCHES ON I-80 AT HOUBOLT RD. MAY BE LOCATED WITHIN THE LIMITS OF EXISTING LUG SYSTEM. THE CONTRACTOR SHALL TAKE EXTRA PRECAUTION WHEN REMOVING THE PATCHES, INCLUDING REMOVAL BY HAND METHODS IF NECESSARY, TO ENSURE NO DAMAGE IS CAUSED TO THE EXISTING LUG SYSTEM AND LUG REINFORCEMENT.

| | | | | | | | | | | | |
|--|------------------------|------------|------------------------|---|--|---|------------|--------|--------------|-----------|--|
| FILE NAME = | USER NAME = wilgreendp | DESIGNED - | REVISED - 4/28/10 - DW | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PATCHING SCHEDULE I-80 AT HOUBOLT RD. | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ca\pki_work\PWIDOT\MILGREENDP\0183633\Design.dgn | | DRAWN - | REVISED - | | | VAR. | 2010-016PP | WILL | 19 | 8 | |
| PLOT SCALE = 100.0000' / IN. | | CHECKED - | REVISED - | | | CONTRACT NO. 60K35 | | | | | |
| PLOT DATE = 4/28/2010 | | DATE - | REVISED - | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | |
| | | | | SCALE: | SHEET NO. OF SHEETS | STA. | TO STA. | | | | |

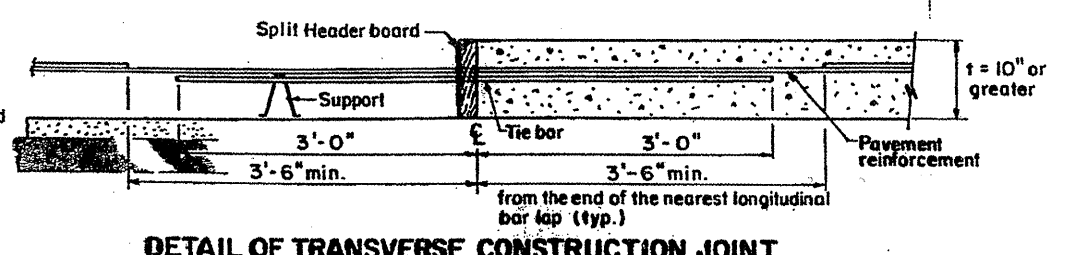
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|-----------------------|---------|----------|------------------|-----------|
| F.A. SHEET | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 80 | * | WILL | 226 | 226A |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |
| * 99-Z(K) #8-1-R | | | | |



PLAN OF PAVEMENT



TRANSVERSE TERMINAL JOINT SECTION G-G



DETAIL OF TRANSVERSE CONSTRUCTION JOINT

GENERAL NOTES

Details shown in Section G-G shall apply only at the end of the construction section. The 10-inch reinforced concrete pad, header board, wood blocking and the 5-feet of extended pavement reinforcement shall be included in the unit price bid each for the TRANSVERSE TERMINAL JOINT COMPLETE of the pavement width as shown on the plans.

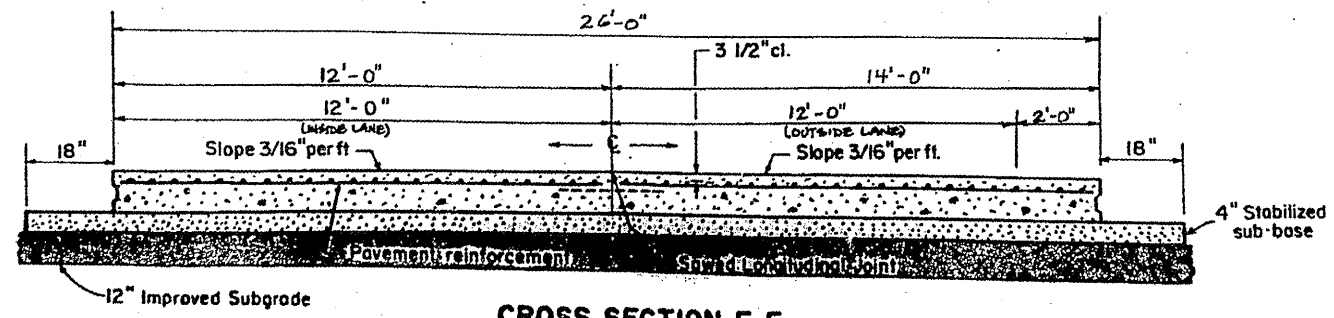
Expansion joints and extra reinforcement in the pavement over concrete pads and at transverse construction joints shall be included in the price bid for C.R.P.C.C. PAVEMENT.

When the Contractor places the reinforcement using a method which requires the tie bars for the longitudinal joint to be placed above the longitudinal reinforcement bars, the first three longitudinal bars on either side of the joint shall be placed such that the tie bars will be at the neutral axis.

Concrete for the Lugs shall be either Class X or Portland Cement Concrete Pavement. It shall be placed in trench to the neat lines as shown. Forms will not be permitted. The concrete in the lugs concrete pad shall be cured in accordance with the methods specified for footings in Section 625 of the Standard Specifications, except that membrane curing will not be permitted for the lugs.

See Standard 2323 for details of joints and tie bars not shown.

FOR INFORMATION ONLY



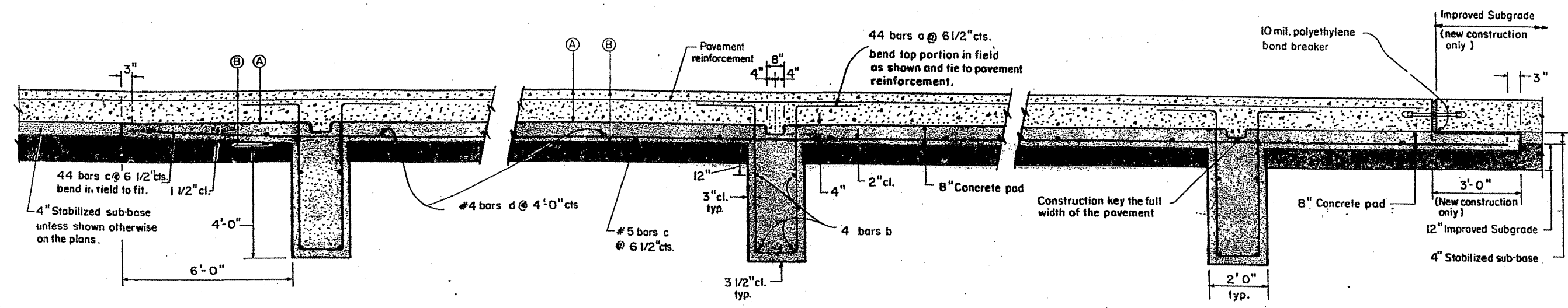
CROSS SECTION E-E

26 FT. - CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (WITH LUG SYSTEM) (Sheet 1 of 2) EXPRESSWAY UNIT DETAIL (FULL SIZE) D.W.W.SR

| | | | | | | | | | | |
|---|------------------------|----------------------------------|---|---|--|------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = wilgreendp | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | 26 FT. - CRC PAVEMENT (WITH LUG SYSTEM) DETAIL | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ca:\pwwork\pwwid01\WILGREENDP\d0183633\design.dgn | DRAWN - | REVISED - | VAR. | | | 2010-016PP | WILL | 19 | 9 | |
| PLOT SCALE = 1/8" = 1' / IN. | CHECKED - | REVISED - | CONTRACT NO. 60K35 | | | | | | | |
| PLOT DATE = 4/28/2010 | DATE - | REVISED - | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | |
| SCALE: | | SHEET NO. OF SHEETS STA. TO STA. | | | | | | | | |

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|-----------------------|----------|------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 80 | * | WILL | 286 | 286C |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT | | |

* 99-2(K#B-1-R)

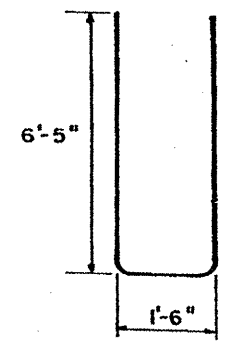


SECTION AT LUG W

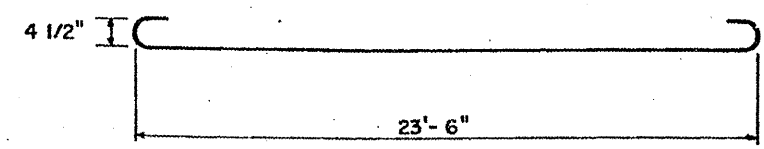
SECTION AT LUG X

SECTION AT LUG Y

| Pav't. Thk's | (A) | (B) |
|--------------|-----|-----|
| 10"-11 3/4" | 18" | |
| 12"-12 3/4" | 20" | |
| ≥ 13" | 21" | |



Bar a



Bar b

*** MATERIALS REQUIRED FOR ONE (1) LUG SYSTEM (Excluding pavement concrete and pavement reinforcement)**

| Bar | No. | Size | Length | Shape |
|-----|-----|------|--------|-------|
| a | 132 | 7 | 14'-0" | U |
| b | 12 | 5 | 24'-9" | — |
| c | 132 | 5 | 20'-0" | — |
| d | 28 | 4 | 11'-9" | — |

| | | |
|-------------------|----------|-------|
| Class X Concrete | Cu. Yds. | 20.85 |
| Reinforcing Bars | Lbs. | 7062 |
| Concrete pad | Sq.Yds. | 146 |
| Improved Subgrade | Sq.Yds. | 146 |

* NOTE: TABLE APPLICABLE ONLY FOR LUG SYSTEM COMPLETE, 24'.
TABLE SHALL BE ADJUSTED BY THE ENGINEER TO BE APPLICABLE FOR LUG SYSTEM COMPLETE, 26'.

FOR INFORMATION ONLY

26 FT. - CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (WITH LUG SYSTEM) (Sheet 2 of 2)

| | | | | | | | | | | |
|---|------------------------|------------|-----------|---|---|---|------------|--------|--------------|-----------|
| FILE NAME = | USER NAME = wilgreendp | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | 26 FT. - CRC PAVEMENT (WITH LUG SYSTEM) DETAIL | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| os:\p\work\p\WIDOT\WILGREENDP\d0183633\ | design.dgn | DRAWN - | REVISED - | | | VAR. | 2010-016PP | WILL | 19 | 9A |
| PLOT SCALE = 100.0000' / IN. | CHECKED - | REVISED - | REVISED - | | | CONTRACT NO. 60K35 | | | | |
| PLOT DATE = 4/28/2010 | DATE - | REVISED - | REVISED - | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

18" (450) MAX.

T/2 *

3" (75) MIN.

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

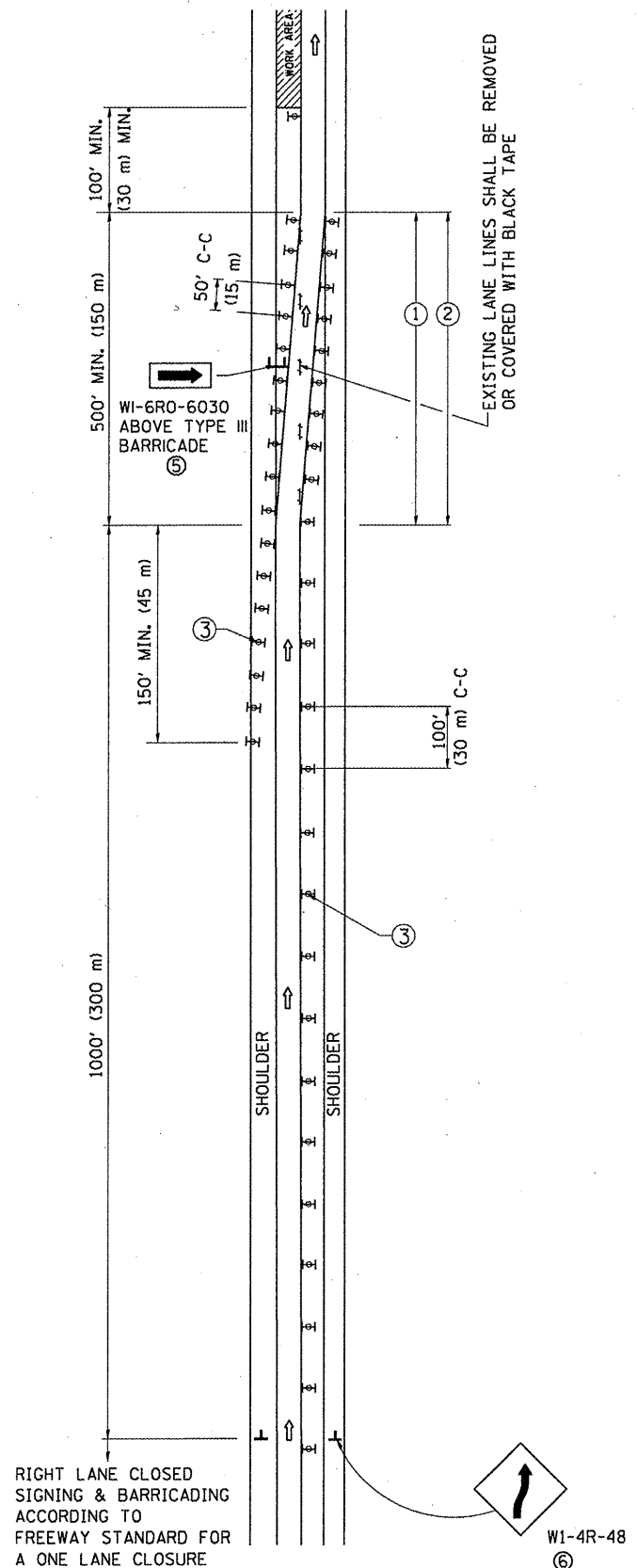
⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

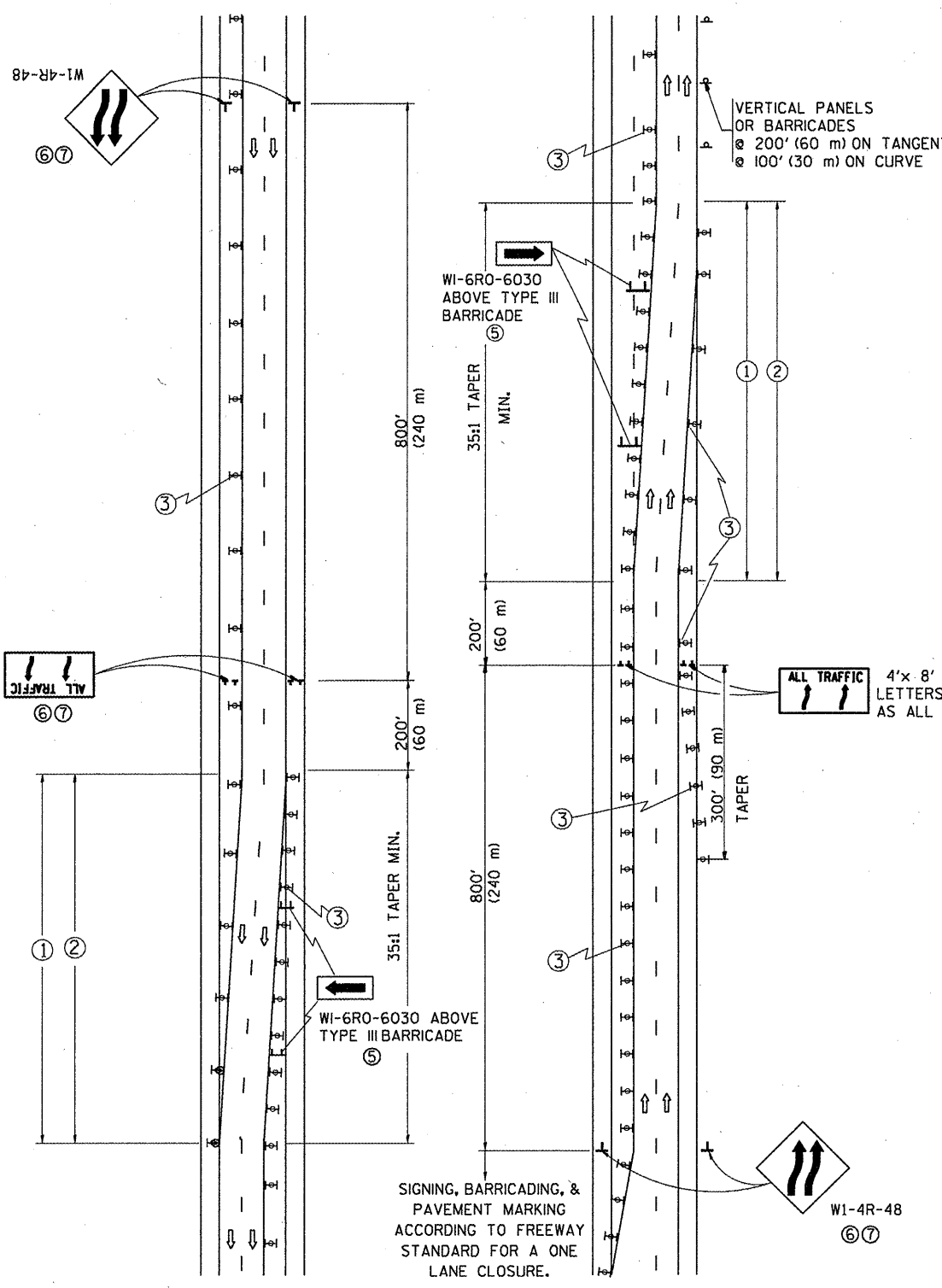
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | |
|--|------------------------------|-----------------------------|-----------------------------|---|--|---|------------|--------------------|--------------|-----------|
| FILE NAME = | USER NAME = wjgreendp | DESIGNED - A. HOUSEH | REVISED - R. SHAH 10-03-96 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ci:\pwork\PWIDOT\WILGREENDP\0183633\d01std.dgn | PLOT SCALE = 100.0000' / IN. | CHECKED - | REVISED - A. ABBAS 03-21-97 | | | VAR. | 2010-016PP | WILL | 19 | 10 |
| PLOT DATE = 3/30/2010 | DATE - 03-11-94 | REVISED - M. GOMEZ 01-22-01 | REVISED - R. BORO 12-15-09 | | | BD600-06 (BD-24) | | CONTRACT NO. 60K35 | | |
| | | | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |
| | | | | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | | STA. | TO STA. | |

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

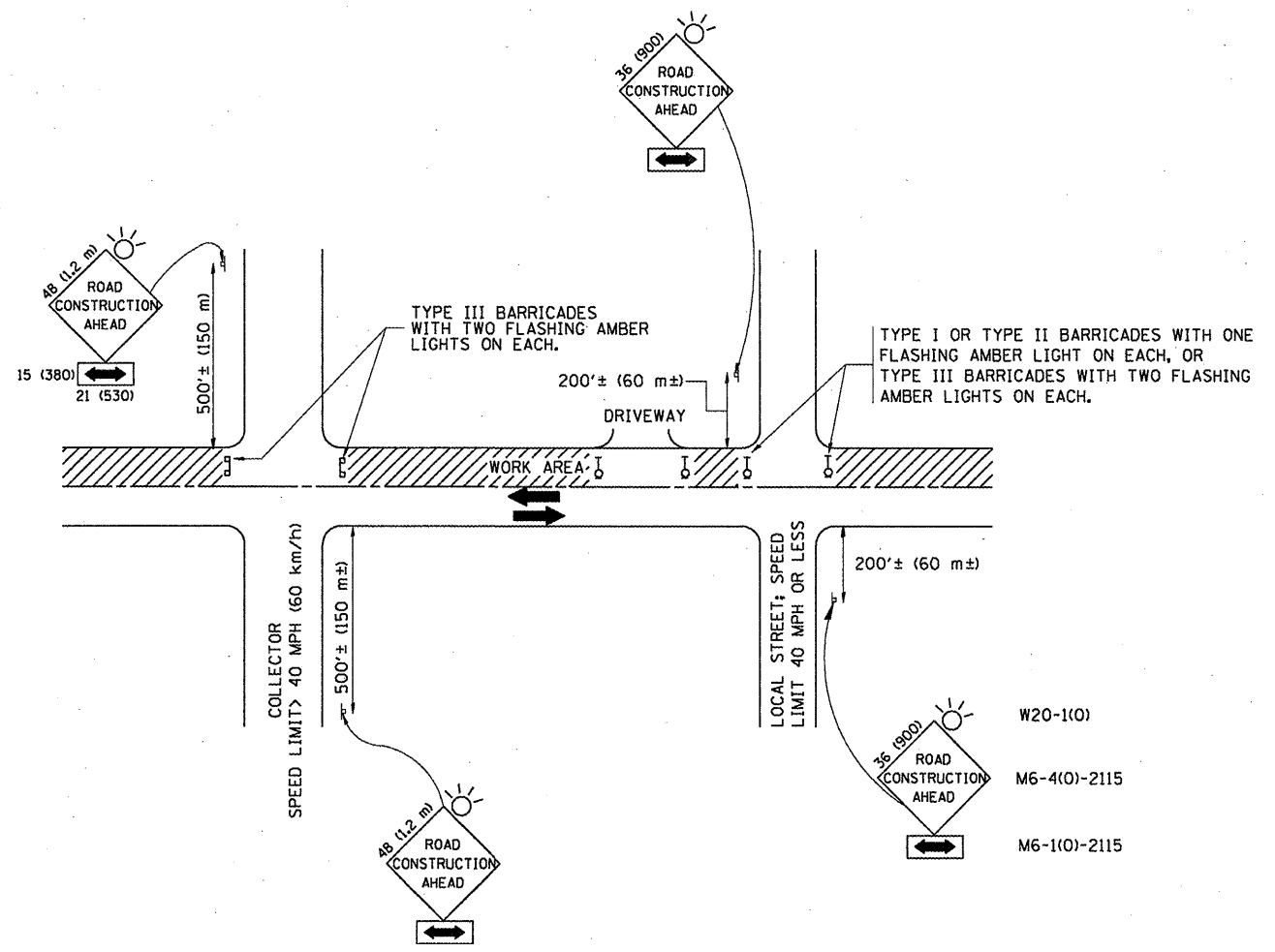
- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 24 HOURS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS. TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- ↑ DIRECTION OF TRAFFIC
 - ▨ WORK AREA
 - ⊥ SIGN ON PORTABLE OR PERMANENT SUPPORT
 - ⊥ TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

| | | | | | | | | | | | | | |
|--|------------------------|----------------|---------------------|---|--|-------------------------|------|-----------|---------|---|--------------|-----------|--------------------|
| FILE NAME = | USER NAME = wilgreendp | DESIGNED - DWS | REVISED - JAF 01-03 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ca:\p\work\WIDDOT\WILGREENDP\d0183633\d0183633.dgn | | DRAWN - | REVISED - JAF 02-06 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | VAR. | 2010-016PP | WILL | 19 | 11 |
| | | CHECKED - | REVISED - SPB 01-07 | | | | | | | TC-09 | | | CONTRACT NO. 60K35 |
| | | DATE - 02-87 | REVISED - SPB 12-09 | | | | | | | FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT | | | |



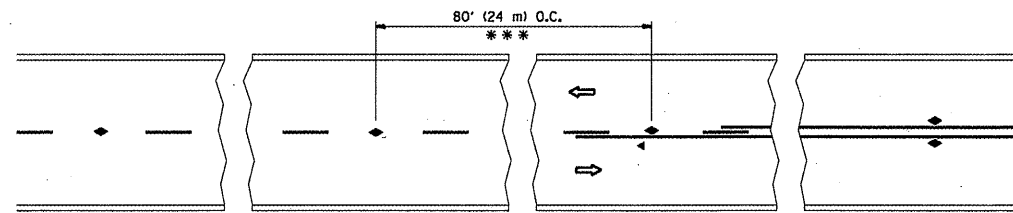
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

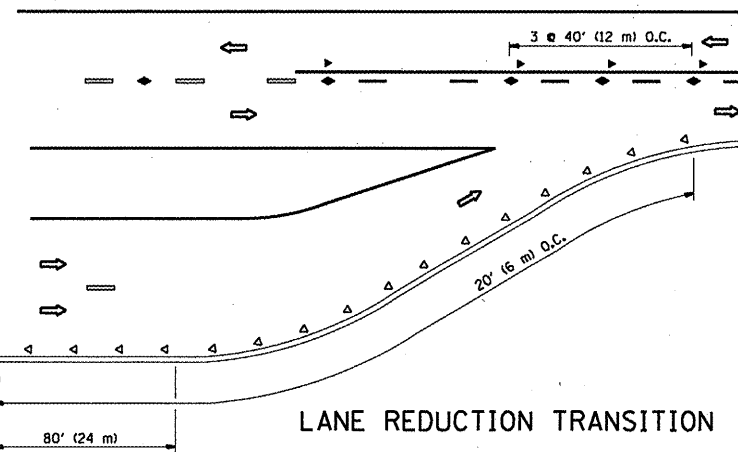
- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

All dimensions are in millimeters (inches) unless otherwise shown.

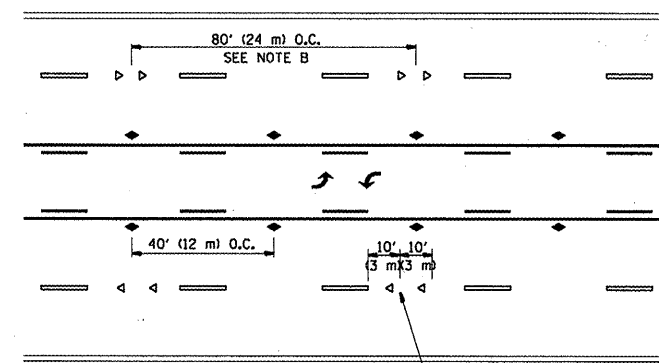
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|---|------------------------|---------------------------------|---------------------------------|---|--|-------------------------|------------|---------------------------|-----------------------|---------------------------|
| FILE NAME = | USER NAME = wilgreendp | DESIGNED - LHA | REVISED - J. OBERLE 10-18-95 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS | F.A. RTE. * | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| o:\pwork\FW1007\MILGREENDP\d0183633\DistStd.dgn | DRAWN - | REVISED - A. HOUSEH 03-06-96 | REVISED - A. HOUSEH 10-15-96 | | | VAR. | 2010-016PP | WILL | 19 | 12 |
| PLOT SCALE = 100.0000" / IN. | CHECKED - | REVISED - A. HOUSEH 10-15-96 | REVISED - T. RAMMACHER 01-06-00 | | | TC-10 | | CONTRACT NO. 60K35 | | |
| PLOT DATE = 3/30/2010 | DATE - 06-89 | REVISED - T. RAMMACHER 01-06-00 | SCALE: NONE | | | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. 1 | ILLINOIS FED. AID PROJECT |



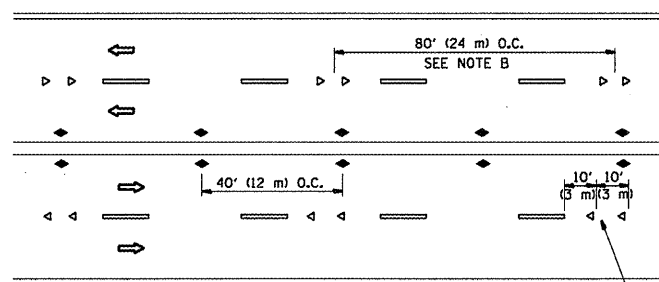
TWO-LANE/TWO-WAY



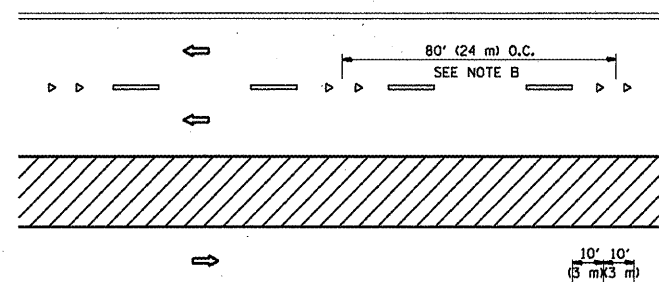
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

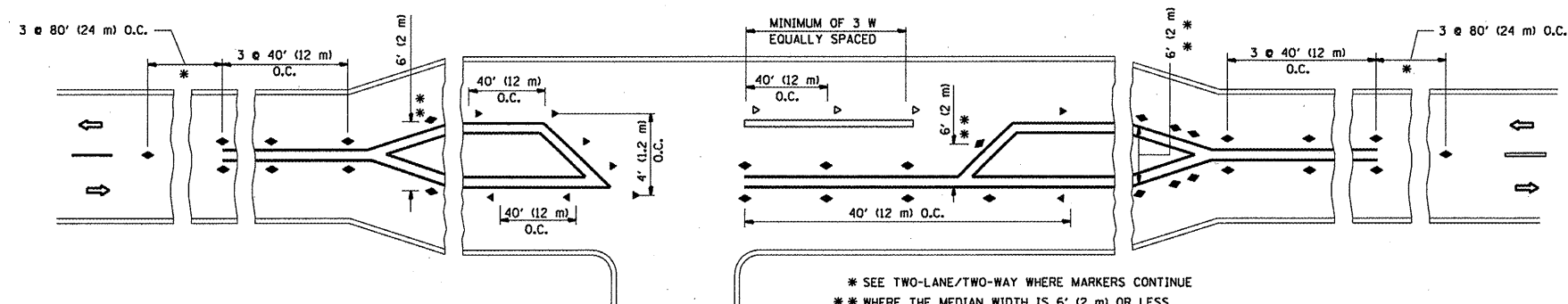
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

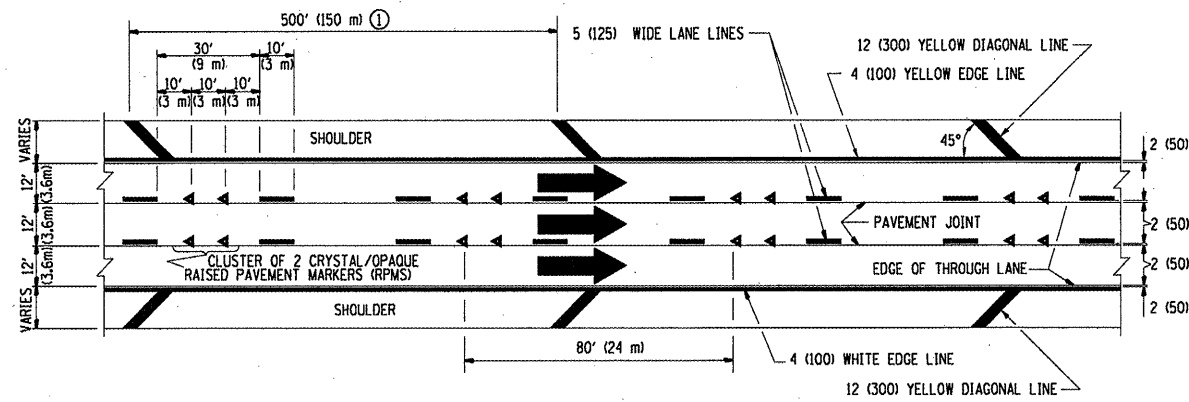
1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

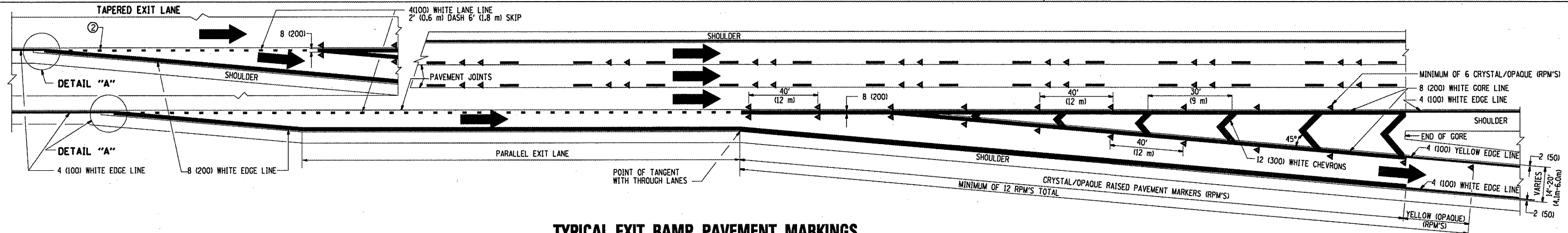
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| FILE NAME = | USER NAME = wilgreendp | DESIGNED - | REVISED - T. RAMMACHER 09-19-94 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ct:\pwork\PW100T\WILGREENDP\80183633\st5std.dgn | | DRAWN - | REVISED - T. RAMMACHER 03-12-99 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | VAR. | 2010-01GPP | WILL | 19 | 13 |
| PLOT SCALE = 100.0000 / IN. | | CHECKED - | REVISED - T. RAMMACHER 01-06-00 | | TC-11 | | | CONTRACT NO. 60K35 | | | | | |
| PLOT DATE = 3/30/2010 | | DATE - | REVISED - C. JUCIUS 09-09-09 | | FED. ROAD DIST. NO. 1 | | | ILLINOIS FED. AID PROJECT | | | | | |



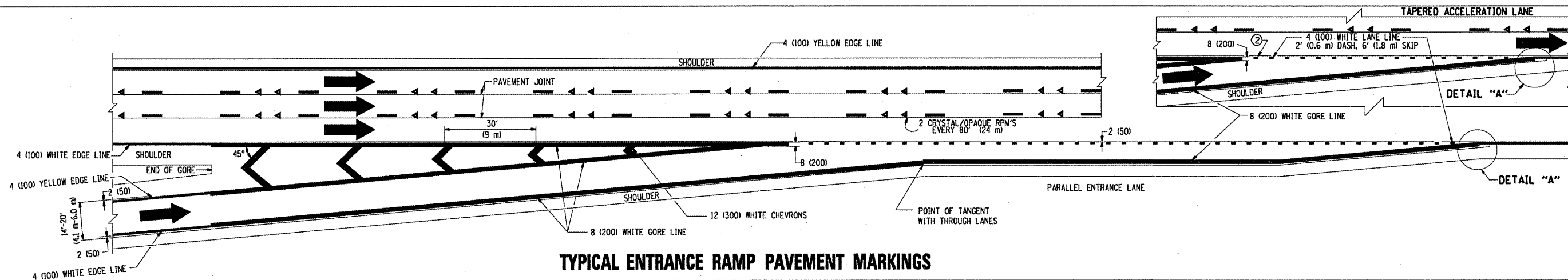
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

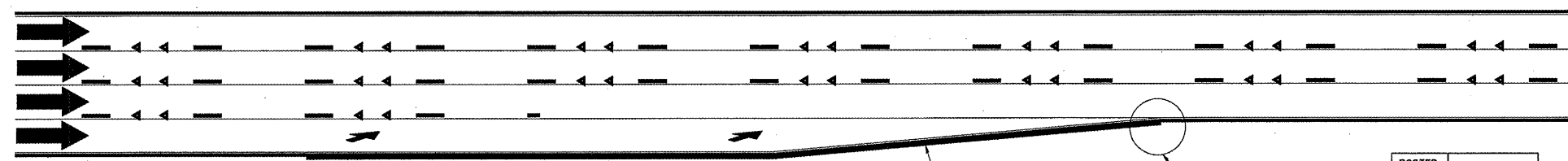
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES ON BITUMINOUS PAVEMENT.
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC.



TYPICAL EXIT RAMP PAVEMENT MARKINGS

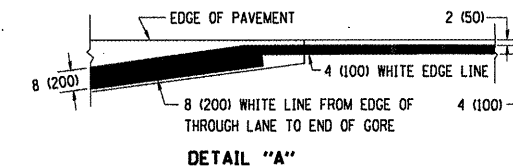


TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



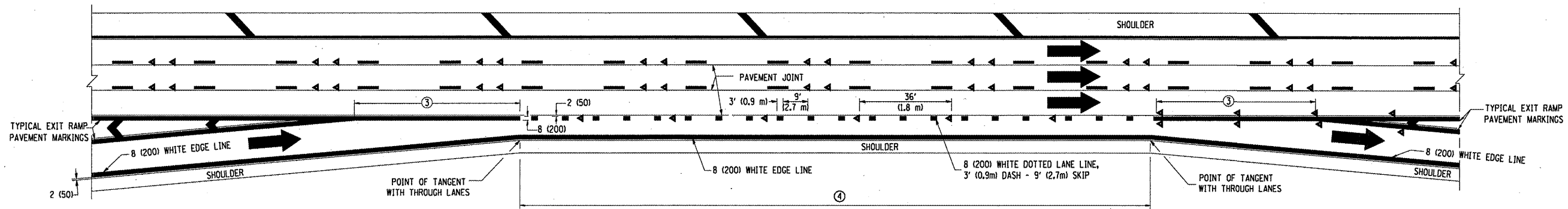
LANE REDUCTION PAVEMENT MARKINGS

| POSTED SPEED LIMIT | D DISTANCE |
|--------------------|---------------|
| 45 MPH | 750' (230 m) |
| 55 MPH | 950' (290 m) |
| 65 MPH | 1200' (365 m) |

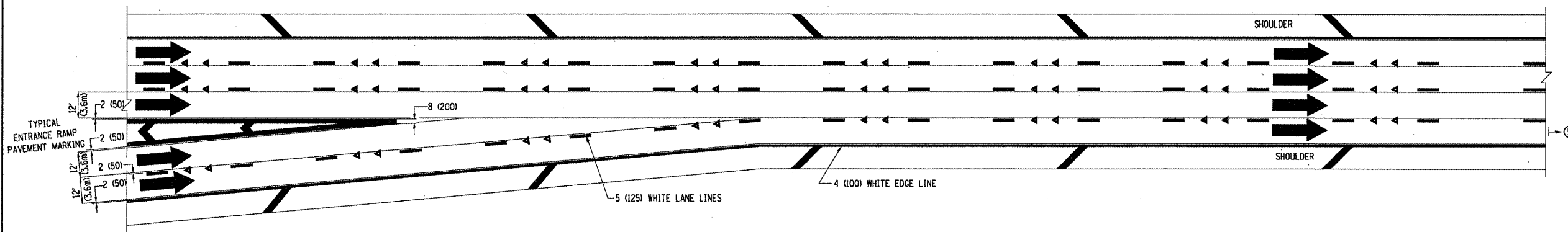


NOTES:

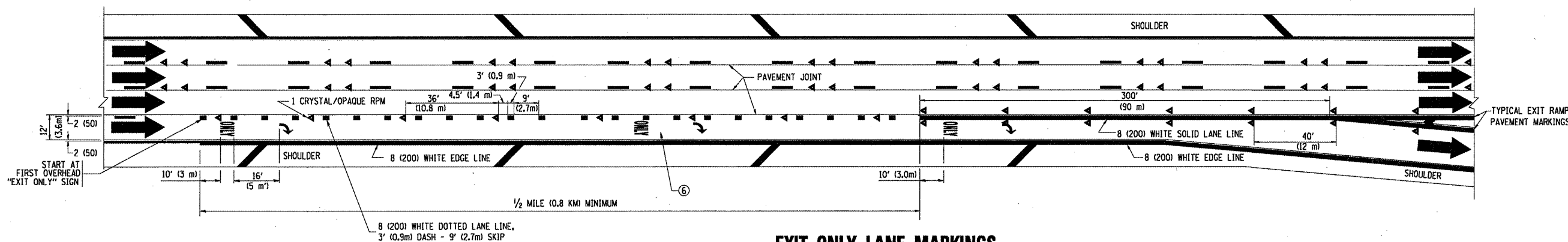
- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
- ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.



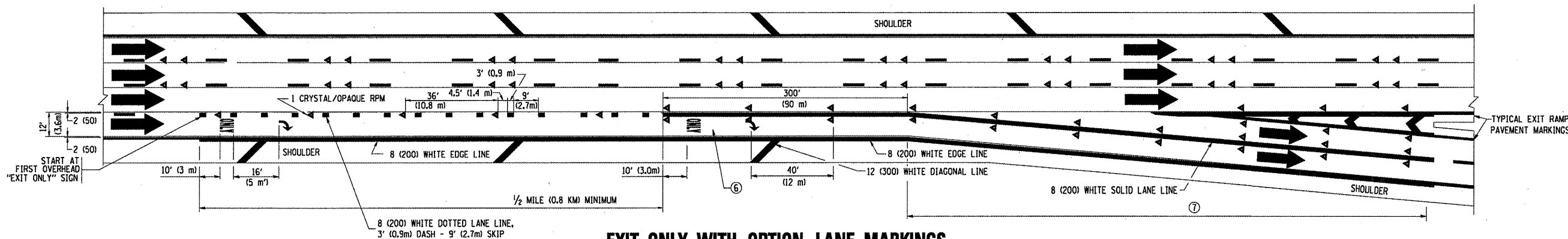
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED CORE.

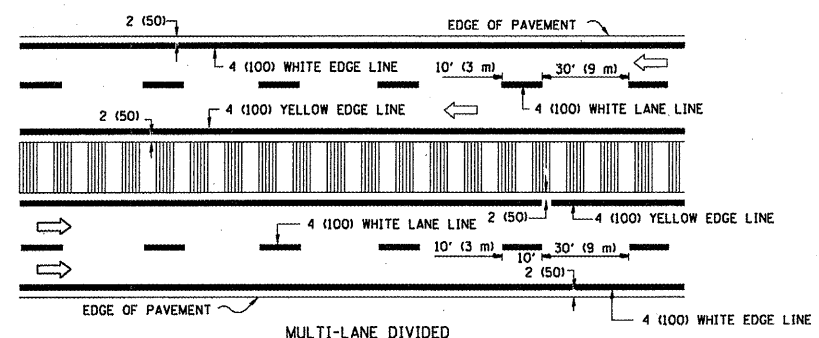
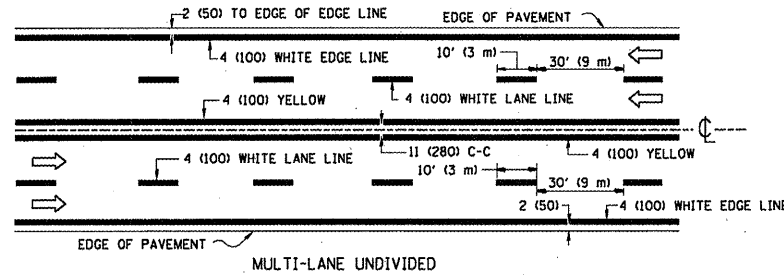
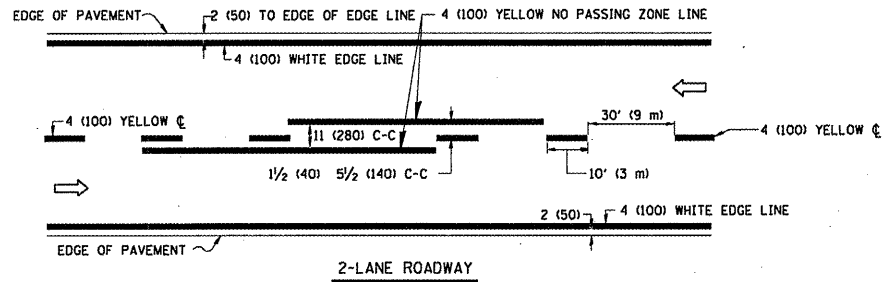
| | | | |
|---|----------------------------|-------------------|------------------------|
| FILE NAME = | USER NAME = wilgreendp | DESIGNED - D.W.S. | REVISED - D.W.S. 07-96 |
| at\p\work\PM\DOT\WILGREENDP\0183633\01st5td.dgn | | DRAWN - | REVISED - J.A.F. 02-06 |
| | PLOT SCALE = 1/8" = 1' IN. | CHECKED - | REVISED - S.P.B. 01-07 |
| | PLOT DATE = 3/30/2010 | DATE - 01-90 | REVISED - S.P.B. 01-10 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS**

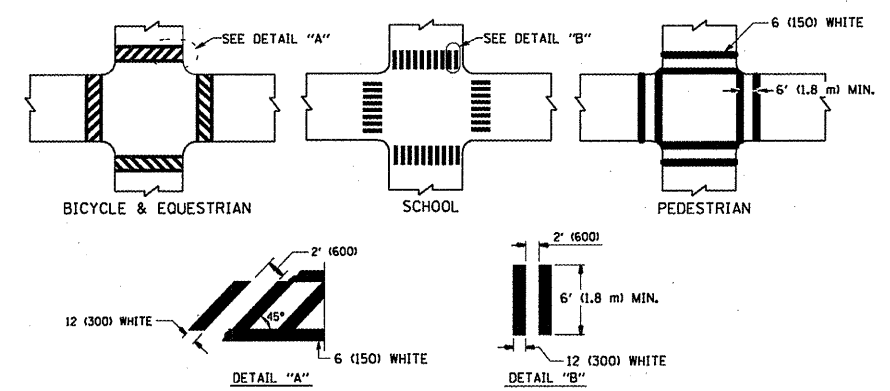
SCALE: NONE SHEET NO. 2 OF 2 SHEETS STA. TO STA.

| | | | | |
|---|------------|--------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2010-016PP | WILL | 19 | 15 |
| TC-12 | | CONTRACT NO. 60K35 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

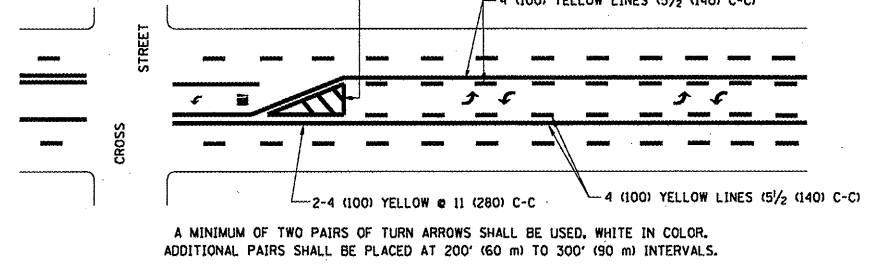
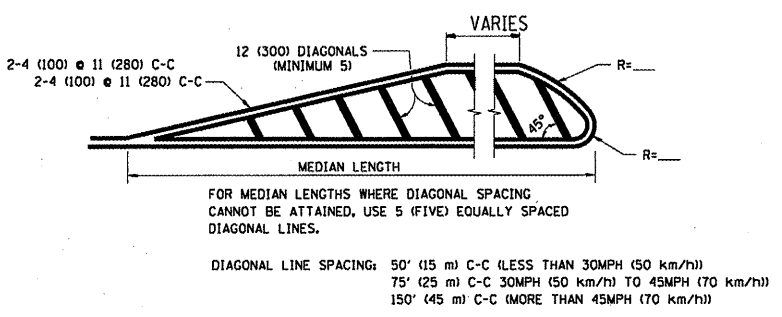
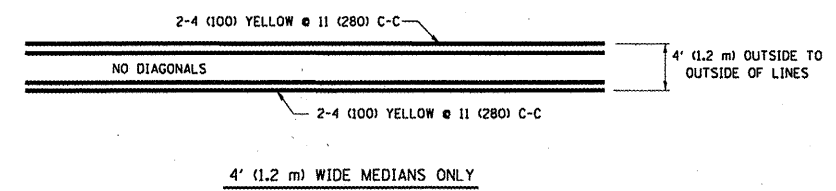


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

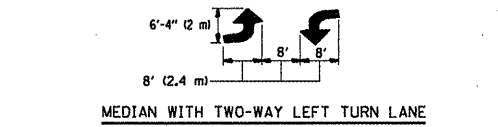
TYPICAL LANE AND EDGE LINE MARKING



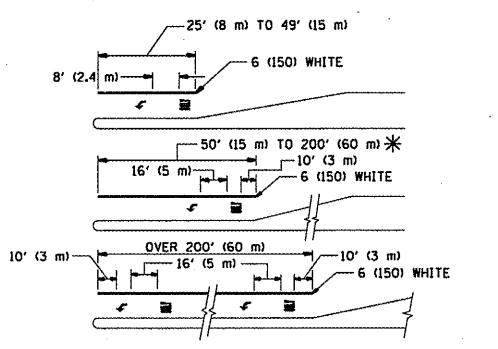
TYPICAL CROSSWALK MARKING



TYPICAL PAINTED MEDIAN MARKING



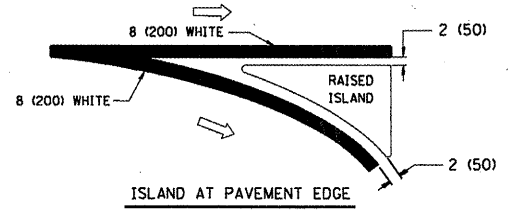
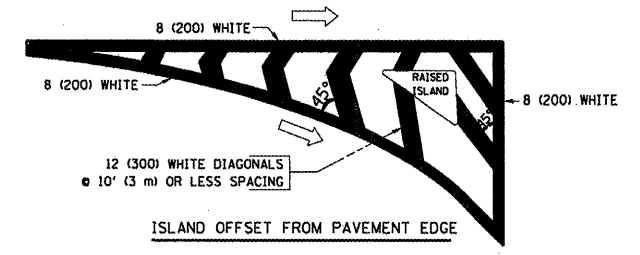
TYPICAL LEFT (OR RIGHT) TURN LANE



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING

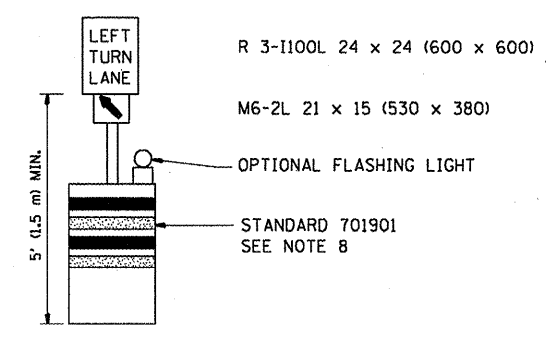
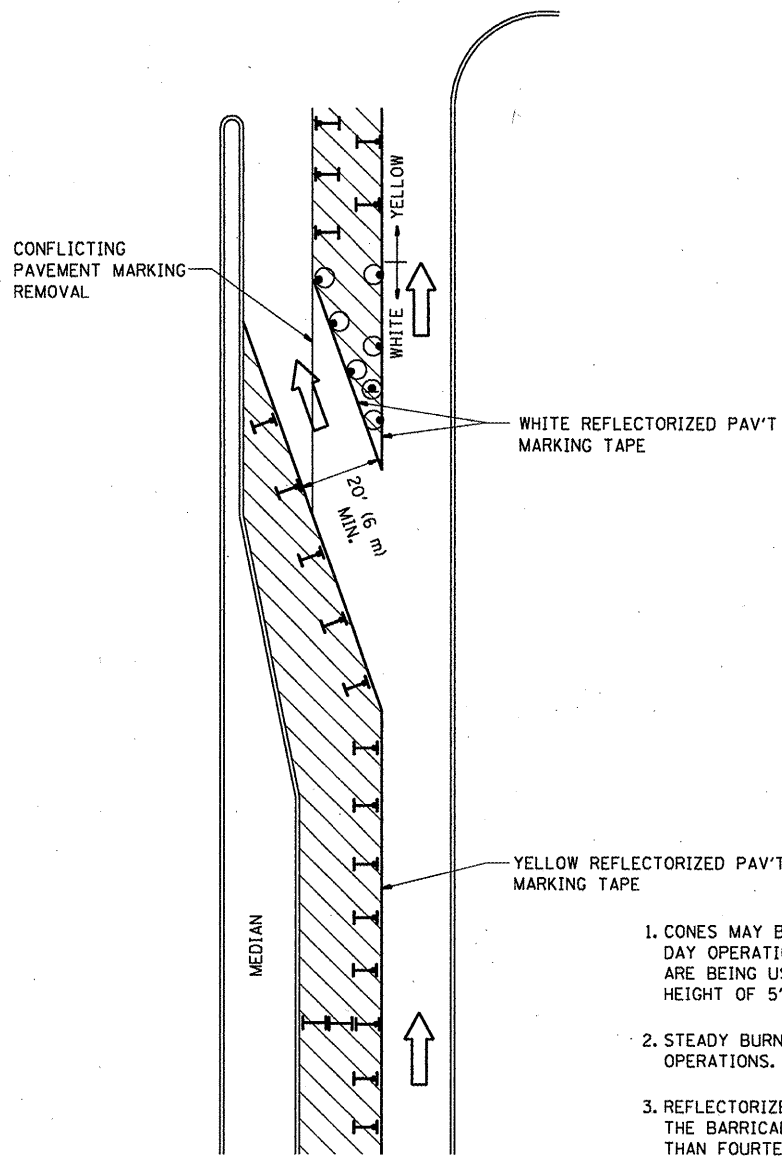


TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|---|------------------------------|--|--|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION | 4 (100) | SOLID | YELLOW | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE |
| FOR BOTH DIRECTIONS | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF "R" = 3.6 SQ. FT. (0.33 m ²) EACH "X" = 54.0 SQ. FT. (5.0 m ²) |
| SHOULDER DIAGONALS | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

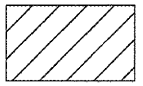
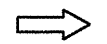






GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

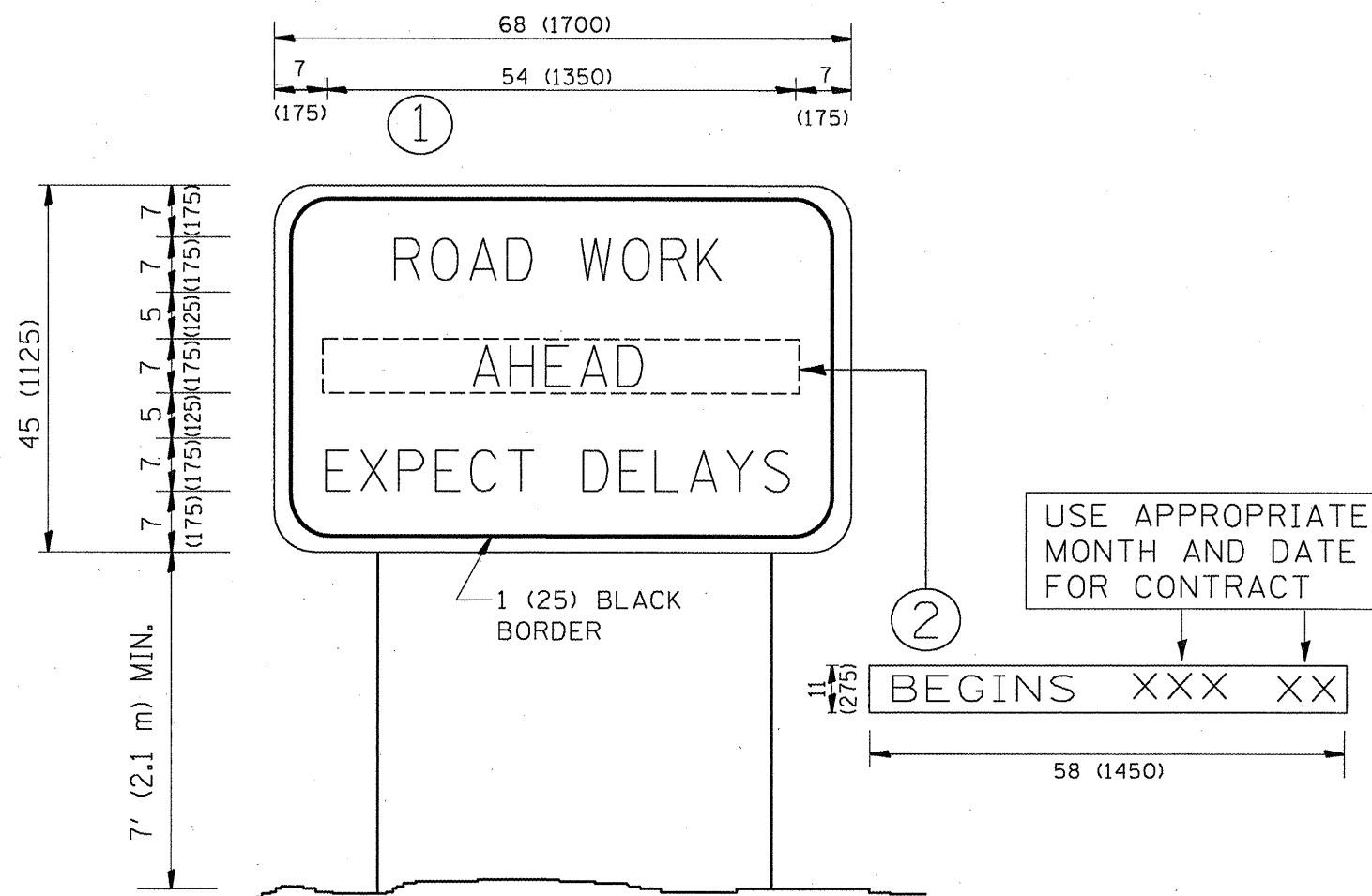
-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

| | | | |
|---|------------------------|--------------------------------|----------------------------|
| FILE NAME = | USER NAME = wilgreendp | REVISED -T. RAMMACHER 09-08-94 | REVISED - R. BORO 09-14-09 |
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| PLOT SCALE = 100.0000 ' / IN. | | REVISED - A. HOUSEH 10-12-96 | REVISED - |
| PLOT DATE = 3/30/2010 | | REVISED -T. RAMMACHER 01-06-00 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) | | | |
|--|-------------------------|------|---------|
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

| | | | | |
|---|------------|--------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2010-016PP | WILL | 19 | 17 |
| TC-14 | | CONTRACT NO. 60K35 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | |
|---|------------------------------|------------|---------------------------------|
| FILE NAME = | USER NAME = wilgreendp | DESIGNED - | REVISED - R. MIRS 09-15-97 |
| at\p\work\PWIDOT\WILGREENDP\d0183633\01 | wt5td.dgn | DRAWN - | REVISED - R. MIRS 12-11-97 |
| | PLOT SCALE = 100.0000' / IN. | CHECKED - | REVISED - T. RAMMACHER 02-02-99 |
| | PLOT DATE = 3/30/2010 | DATE - | REVISED - C. JUCIUS 01-31-07 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

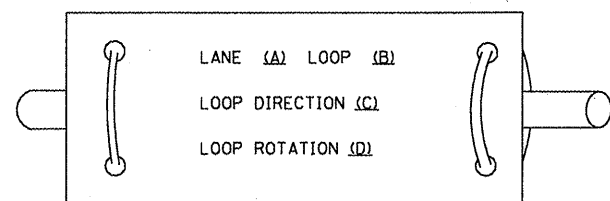
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

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|---|------------|--------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2010-016PP | WILL | 19 | 18 |
| TC-22 | | CONTRACT NO. 60K35 | | |
| FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT | | | | |

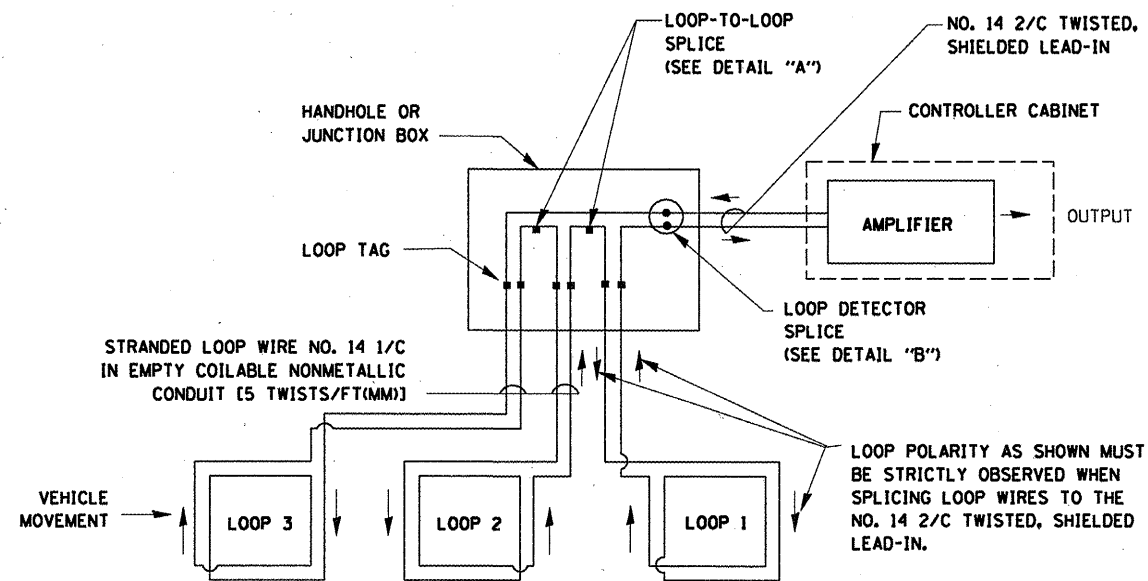
LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

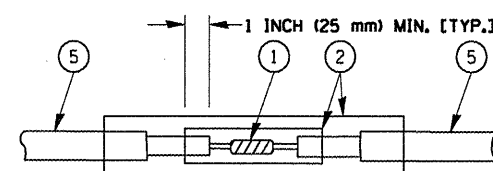


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

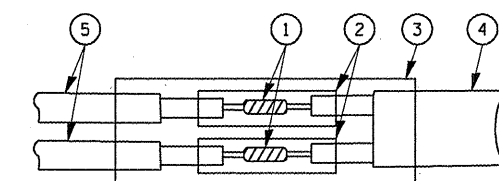


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE. THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

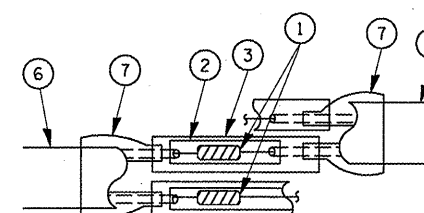


DETAIL "A"
LOOP-TO-LOOP SPLICE

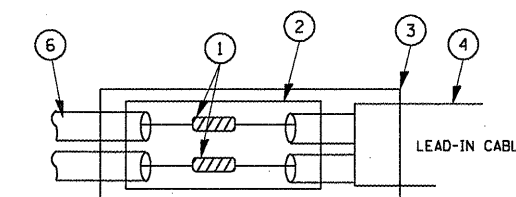


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PREFORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

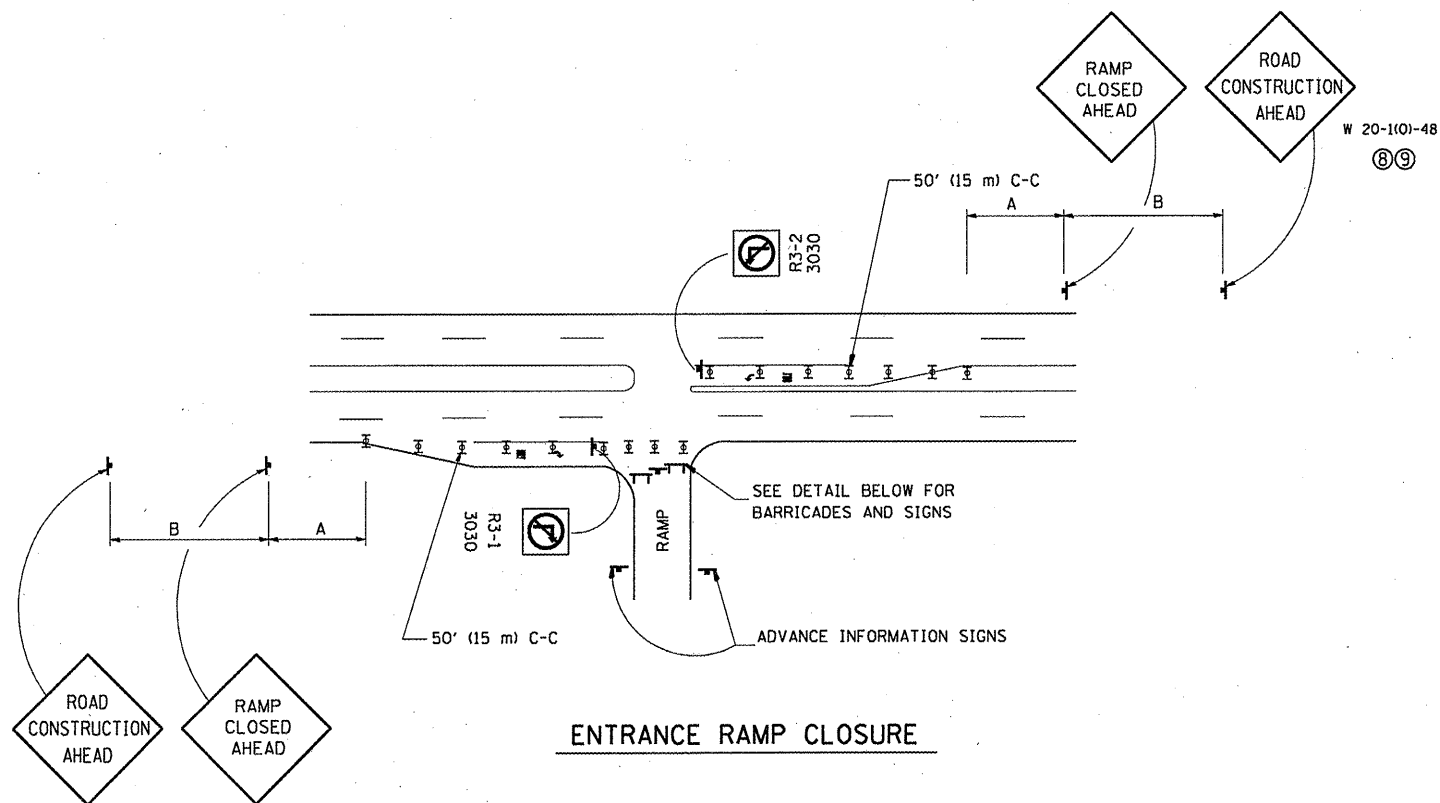
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| PLOT SCALE = 100.0000' / IN. | CHECKED - DAD | REVISIONS | REVISED - |
| PLOT DATE = 3/30/2010 | DATE - 10-28-09 | REVISED - | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET NO. 1 OF 6 SHEETS STA. TO STA.

| | | | | |
|---|------------|--------|--------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2010-016PP | WILL | 19 | 19 |
| TS-05 | | | CONTRACT NO. 60K35 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



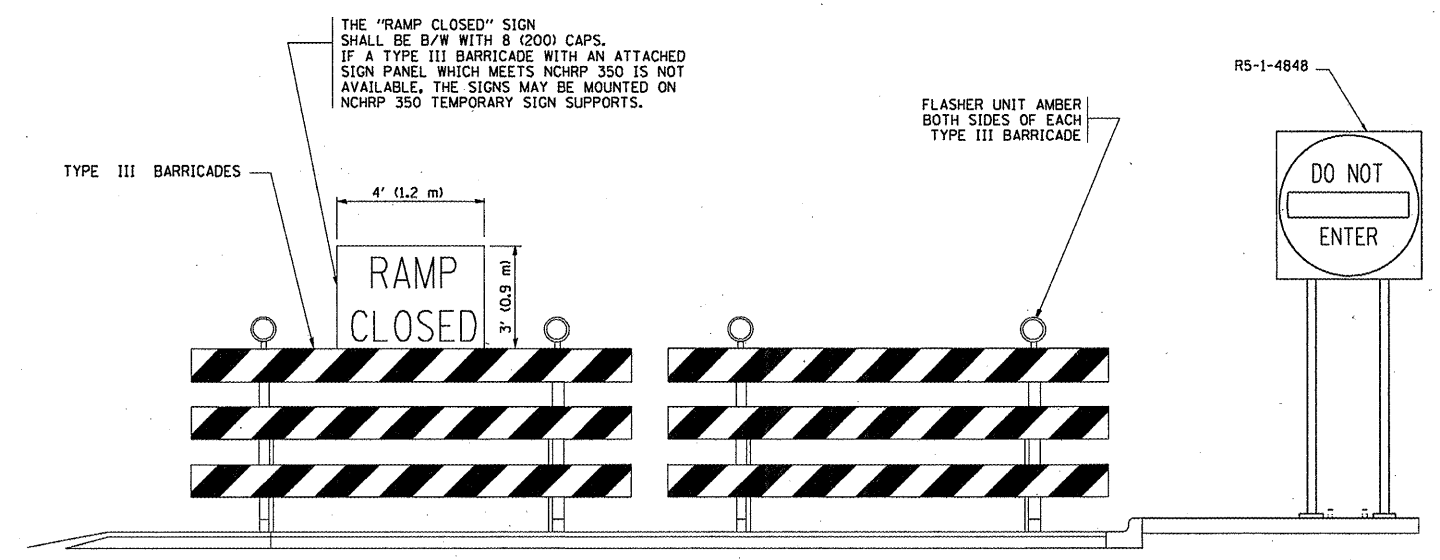
ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

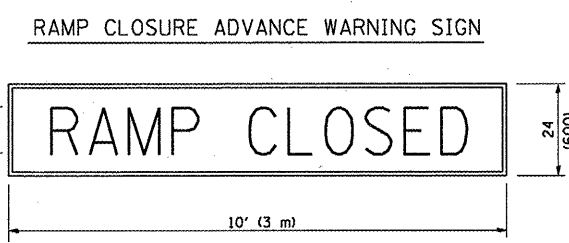
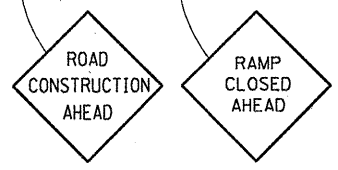
| FACILITY | DISTANCE BETWEEN SIGNS | |
|----------------------|------------------------|---------------|
| | A | B |
| EXPRESSWAY >24 HOURS | 1000' (300 m) | 1500' (450 m) |
| EXPRESSWAY ≤24 HOURS | 500' (150 m) | 500' (150 m) |
| ARTERIAL ≥45 MPH | 350' (100 m) | 350' (100 m) |
| ARTERIAL <45 MPH | 150' (45 m) | 150' (45 m) |

DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.

W 20-101-48
⑧⑨

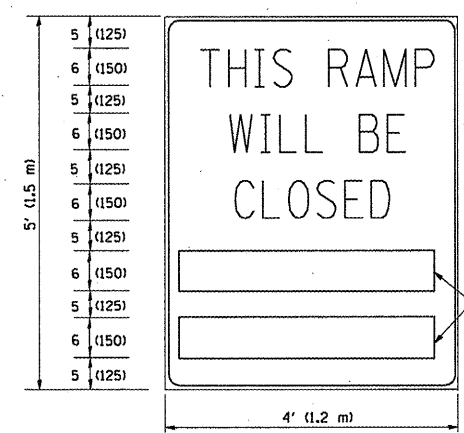


DETAIL FOR REQUIRED BARRICADES & SIGNS



BLACK LEGEND ON ORANGE REFLECTORIZED BACKGROUND
1 (25) BORDER
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR THE CLOSED EXIT RAMPS.

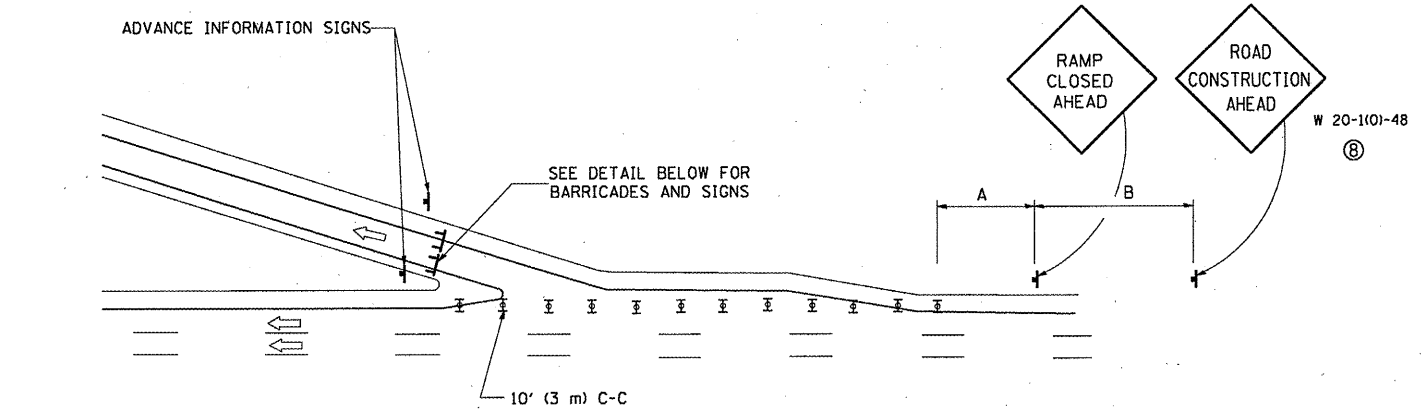
RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE REFLECTORIZED BACKGROUND
1/2 (12) BORDER

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.



EXIT RAMP CLOSURE

- SYMBOLS**
- ▬ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
 - ▬ TYPE III BARRICADE WITH FLASHING LIGHT

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY-FOUR (24) HOURS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED ON CLOSURES LESS THAN 24 HOURS IN DURATION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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| PLOT SCALE = 100.0000' / IN. | | CHECKED - | REVISED - SPB 01-07 |
| PLOT DATE = 4/29/2010 | | DATE - 02-83 | REVISED - SPB 12-09 |

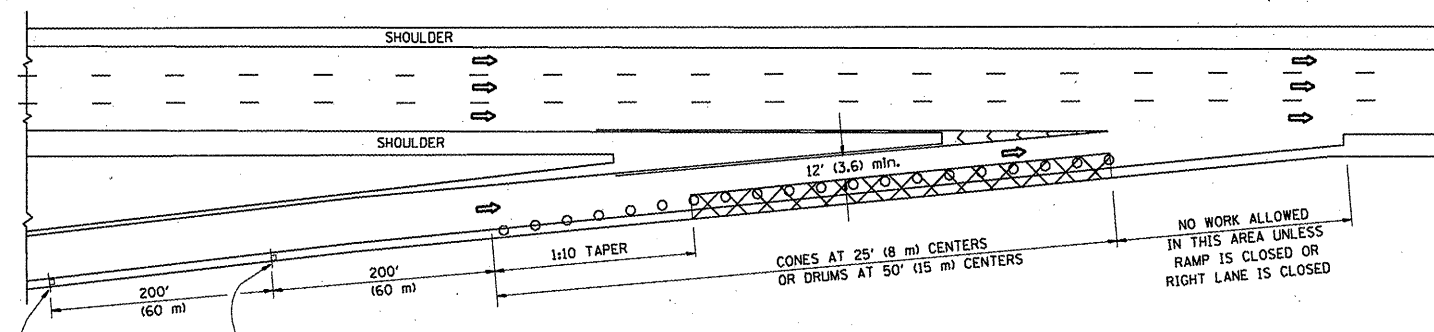
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FREWAY ENTRANCE AND EXIST RAMP
CLOSURE DETAILS**

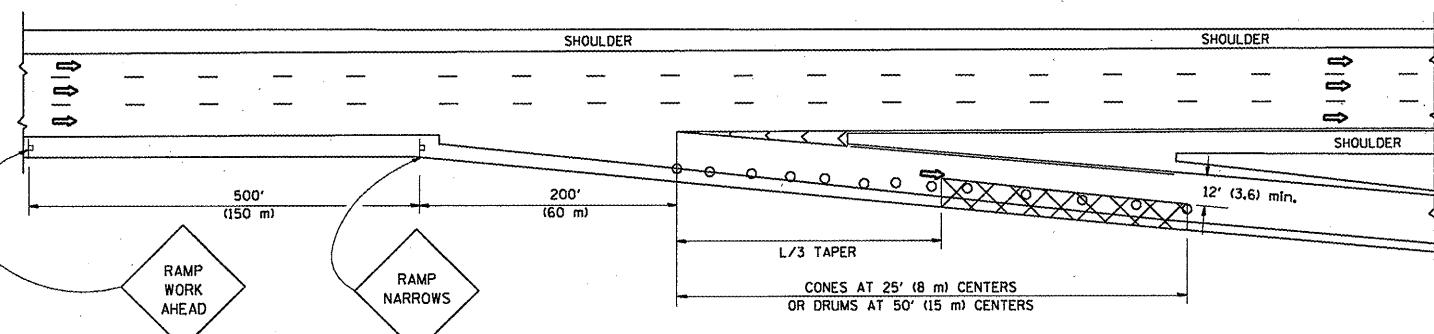
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

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|---|------------|--------|--------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR. | 2010-016PP | WILL | 19 | 194 |
| TC-08 | | | CONTRACT NO. 60K35 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

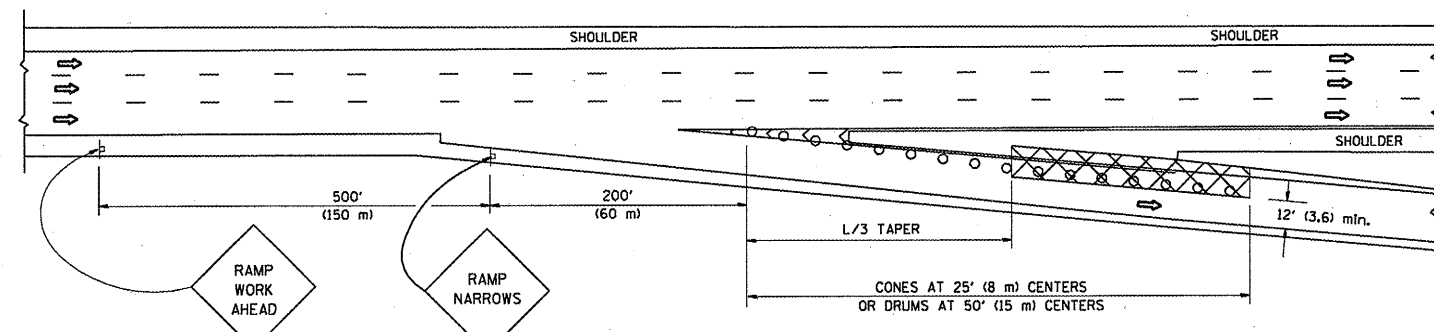
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

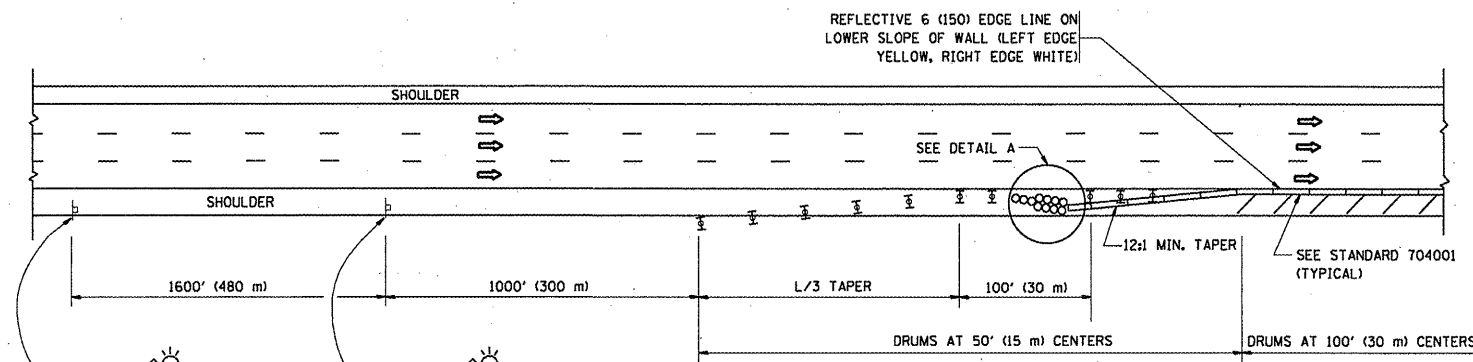
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

GENERAL NOTES

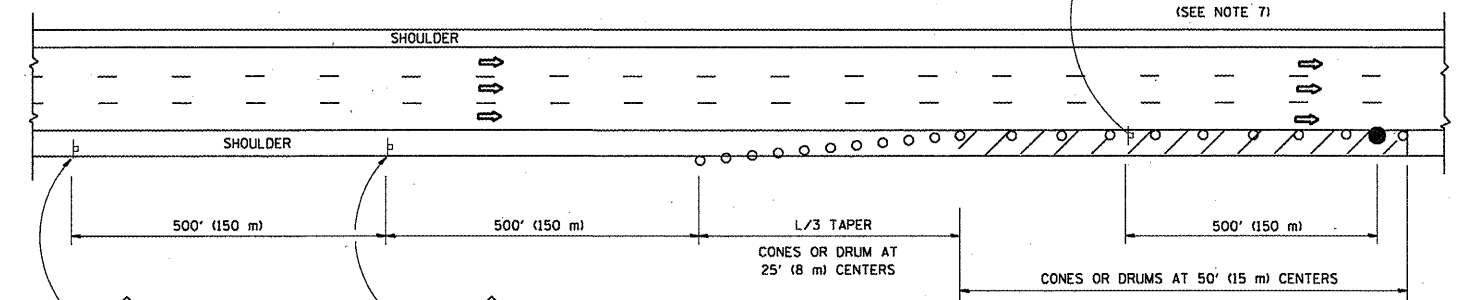
1. THE "L" DISTANCE EQUALS:

| | |
|--------------------------------------|---------------------------------------|
| SPEED LIMIT | FORMULAS |
| 45 mph (80 km/h) OR GREATER: | METRIC ENGLISH L=0.65(W/S) L=(W/S) |
| W = WIDTH OF OFFSET IN FEET (METERS) | |
| S = NORMAL POSTED SPEED MPH (KM/H) | |
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

SHOULDER CLOSURE DETAILS



PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
 1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.

ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350 COMPLIANT.

DETAIL "A"
 IMPACT ATTENUATOR, TEMPORARY
 (SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | | | |
|---|-----------------------|------------------------|------------------------|---|--|-------------------------|------|---|---------|---------------------------|--------------|-----------|-----|
| FILE NAME = | USER NAME = wlgreendp | DESIGNED - | REVISED - 04-03 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES | | | F.A. RTE. * | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ca:\pki_work\WIDOT\WILGREENDP\d0183633\d0183633.dgn | DRAWN - D.W.S. | REVISED - J.A.F. 12-06 | REVISED - S.P.B. 01-07 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | VAR. | 2010-016PP | WILL | 19 | 198 |
| PLOT SCALE = 100.0000' / IN. | CHECKED - | REVISED - S.P.B. 01-07 | REVISED - S.P.B. 12-09 | | | | | TC-17 | | CONTRACT NO. 60K35 | | | |
| PLOT DATE = 4/29/2010 | DATE - 11-96 | REVISED - S.P.B. 12-09 | | | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | |