

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**

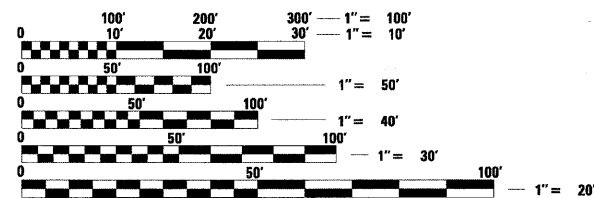
F.A.U. ROUTE 2503 / IL 25 (5TH AVE)  
OVER U.P.R.R.  
SECTION 39EXT-VB-1  
BRIDGE DECK OVERLAY & BRIDGE JOINT REPAIR  
STRUCTURE NO. 045-0061  
KANE COUNTY

C-91-238-10

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE  
CITY OF ST. CHARLES

ADT = 10,900 (2009)  
SPEED LIMIT = 30 MPH

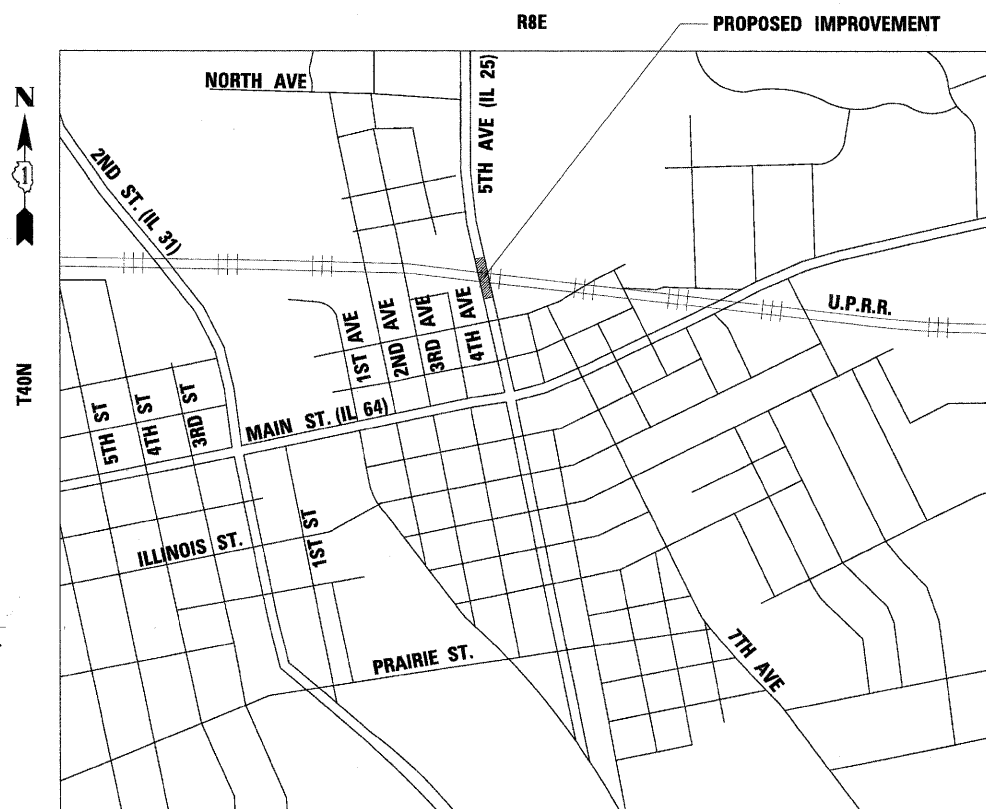


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811



B. ROBERT TESSIATORE, S.E., P.E.  
DATE: MARCH 1, 2010  
EXP: 11/30/2011  
SHEETS: 1-8  
WIGHT & COMPANY



ST. CHARLES TOWNSHIP

GROSS LENGTH = 217.83 FT. = 0.041 MILE  
NET LENGTH = 217.83 FT. = 0.041 MILE

PROJECT MANAGER: HELEN PAZON (847)705-4523

CONTRACT NO. 60J48

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	39EXT-VB-1	KANE	27	1
		ILLINOIS	CONTRACT NO. 60J48	

D-91-238-10



LOCATION OF SECTION INDICATED THUS: - [black box] -



B. ROBERT TESSIATORE, S.E., P.E.  
DATE: MARCH 1, 2010  
EXP: 11/30/2010  
SHEETS: 9-121  
WIGHT & COMPANY

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED MARCH 12, 2010

Diana M. O'Keefe  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 7 2010  
Scott E. Stett, P.E.  
ENGINEER OF DESIGN AND ENVIRONMENT

May 7 2010  
Christina M. Road  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER



PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

**INDEX OF SHEETS**

SHEET NO.	DESCRIPTION
1.	COVER SHEET
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8.	PAVEMENT MARKING PLAN
9-21.	STRUCTURE DRAWINGS
22.	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
23.	BUTT JOINT AND HMA TAPER DETAILS
24.	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
25.	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
26.	DETOUR SIGNING FOR CLOSING STATE HIGHWAYS
27.	ARTERIAL ROAD INFORMATION SIGNING

**STATE STANDARDS**

STANDARD NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
420401-08	BRIDGE APPROACH PAVEMENT CONNECTOR
701301-03	LANE CLOSURE, 2L, 2W SHORT TIME OPERATIONS
701901-01	TRAFFIC CONTROL DEVICES

**GENERAL NOTES - ROADWAY**

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED.)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE LOCAL MUNICIPALITY, AND THE UNION PACIFIC RAILROAD.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- ALL DAMAGE TO EXISTING PAVEMENT MARKING OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT.) IN ACCORDANCE WITH "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREA.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD (FOR FUTURE REFERENCES), ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- THE ENGINEER SHALL CONTACT DON CHIARUGI, TRAFFIC FIELD ENGINEER, AT (847) 741-9857 A MINIMUM OF TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE "ADVANCE WARNING SIGN DETAIL FOR ARTERIAL TRAFFIC" LOCATED IN THE SPECIAL PROVISIONS FOR "TEMPORARY INFORMATION SIGNING FOR LANE CLOSURES" IS APPLICABLE ONLY TO ARTERIAL ROADS.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM IDOT BRIDGE INSPECTORS IN OCTOBER 2009.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKINGS.
- REMOVAL OF RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE INCIDENTAL TO HOT-MIX ASPHALT SURFACE REMOVAL (DECK).

**GENERAL NOTES - TRAFFIC CONTROL & PROTECTION**

- STAGING PROCEDURES AND DETOUR PRESENTED ARE THE SUGGESTED SEQUENCE OF OPERATIONS. AT HIS OPTION, THE CONTRACTOR MAY SUBMIT AN ALTERNATIVE STAGING PROPOSAL TO THE ENGINEER FOR HIS APPROVAL
- TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY, OR REMOVE LANE CLOSURES OF CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION WITHIN TWO HOURS FROM THE TIME OF NOTIFICATION.

**GENERAL NOTES**

- PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. SUCH VARIATION SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED AT THE UNIT PRICE BID FOR THE WORK.
- POLLUTION CONTROL: THE CONTRACTOR SHALL BE REQUIRED TO COMPLY WITH STATE REGULATIONS REGARDING AIR, WATER, AND NOISE POLLUTION.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS, AS REQUIRED, PRIOR TO COMMENCING WITH CONSTRUCTION.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, STATE STANDARDS,  
& GENERAL NOTES**

SCALE: SHEET NO. \_\_\_ OF \_\_\_ SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	39-EXT-VB-1	KANE	27	2
<b>CONTRACT NO. 60J48</b>				
ILLINOIS FED. AID PROJECT				

## SUMMARY OF QUANTITIES

CODE NUMBER	ITEM	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION TYPE CODE	
				BRIDGE SN 045-0061	100% STATE
				SFTY-2A	
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	175	175	
<del>40600982</del>	<del>HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT</del>	<del>SQ YD</del>	<del>34</del>	<del>34</del>	
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	29.4	29.4	
42001300	PROTECTIVE COAT	SQ YD	705	705	
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	350	350	
44000915	HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ YD	478	478	
<del>44001100</del>	<del>PAVEMENT REMOVAL (SPECIAL)</del>	<del>SQ YD</del>	<del>66.7</del>	<del>66.7</del>	
44201794	CLASS D PATCH, TYPE III, 12 INCH	SQ YD	40	40	
<del>44000700</del>	<del>APPROACH SLAB REMOVAL</del>	<del>SQ YD</del>	<del>66.7</del>	<del>66.7</del>	
50102400	CONCRETE REMOVAL	CU YD	18.2	18.2	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	38.8	38.8	
50300260	BRIDGE DECK GROOVING	SQ YD	478	478	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	9450	9450	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	98	98	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4	4	
67100100	MOBILIZATION	L SUM	1	1	
70101900	TRAFFIC CONTROL AND PROTECTION (DETOUR 1)	L SUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	90	90	
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	60	60	
* 78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	3	3	
X0322185	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/4 INCHES	SQ YD	478	478	
<del>X0322256</del>	<del>TEMPORARY INFORMATION SIGNING</del>	<del>SQ FT</del>	<del>85</del>	<del>85</del>	
X0325303	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	14	14	
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH LESS THAN OR EQUAL TO 5 INCHES)	SQ FT	165	165	
X0325737	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	17	17	
<del>X0325864</del>	<del>BRIDGE APPROACH PAVEMENT REMOVAL</del>	<del>SQ YD</del>	<del>66.7</del>	<del>66.7</del>	
Z0006204	BRIDGE DECK HYRDO-SCARIFICATION 1/2"	SQ YD	478	478	
<del>Z0016002</del>	<del>DECK SLAB REPAIR (FULL DEPTH, TYPE II)</del>	<del>SQ YD</del>	<del>6</del>	<del>6</del>	
<del>Z0016000</del>	<del>DECK SLAB REPAIR (FULL)</del>	<del>SQ YD</del>	<del>6.0</del>	<del>6.0</del>	
Z0073300	TEMPORARY SHORING AND CRIBBING	L SUM	1.0	1.0	
<del>Z0010000</del>	<del>DRILL AND GROUT DOWEL BARS</del>	<del>EACH</del>	<del>24</del>	<del>24</del>	

\*SPECIALTY ITEMS

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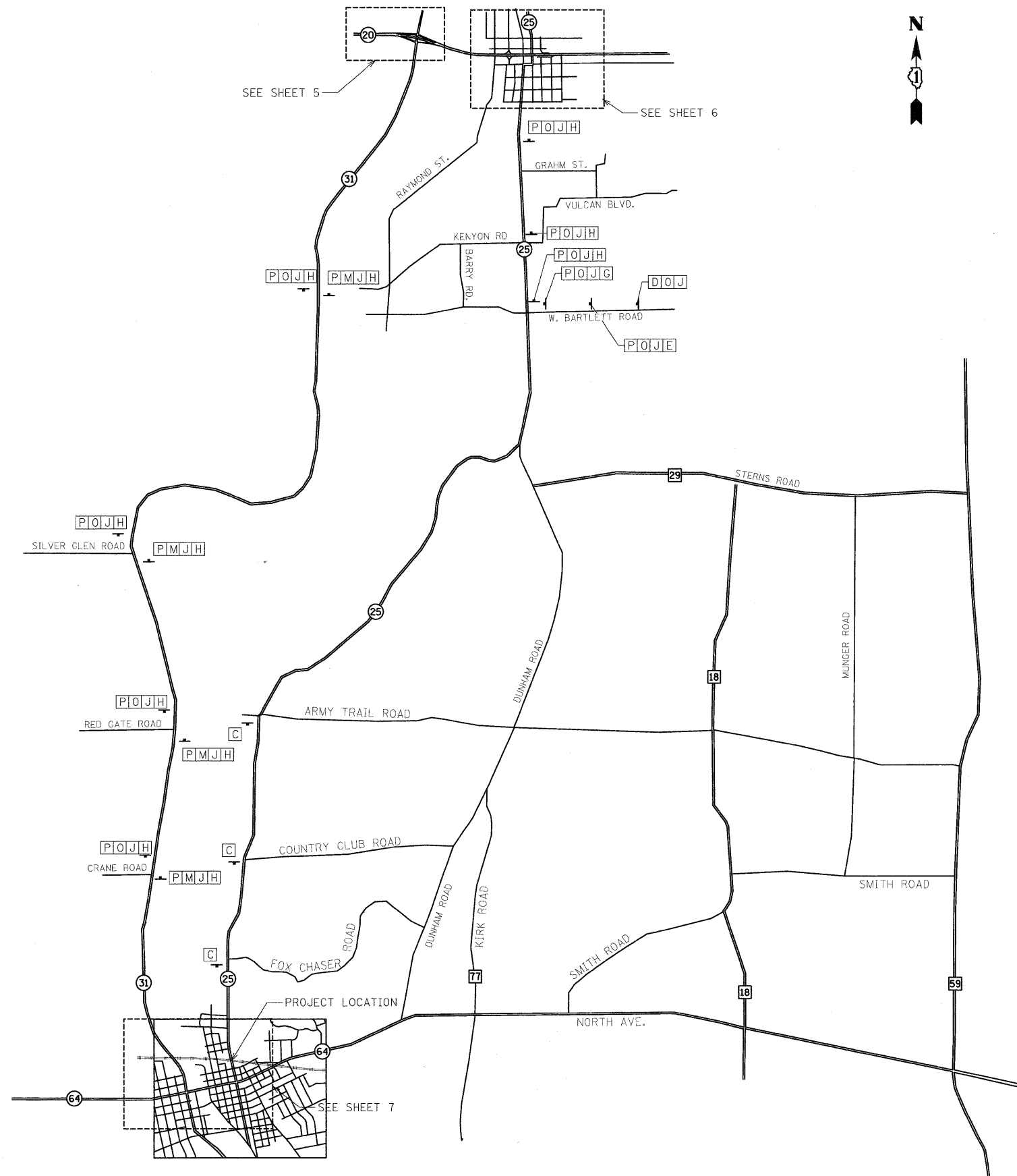
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

### SUMMARY OF QUANTITIES

SCALE: SHEET NO. \_\_\_ OF \_\_\_ SHEETS STA. TO STA.

F.A.U. RTE. 2503	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	39-EXT-VB-1	KANE	27	3
CONTRACT NO. 60J48				
ILLINOIS FED. AID PROJECT				

Rev.



**DETOUR PLAN LEGEND**

- SIGN
- [A] SIGN DESCRIPTION (SEE BELOW)
- ▬ TYPE III BARRICADE WITH 2 HIGH INTENSITY WARNING LIGHTS & SIGNS AS NOTED

<p><b>A</b> ROAD CLOSED R11-2 48"X30" (6 REQD.)</p> <p><b>B</b> ROAD CLOSED MILES AHEAD LOCAL TRAFFIC ONLY R11-3A 60"X30" (2 REQD.)</p> <p><b>C</b> ROAD CLOSED AHEAD W20-1 48"X48" LOW INTENSITY FLASHING LIGHT &amp; 18"X18" ORANGE FLAG (6 REQD.)</p> <p><b>D</b> DETOUR AHEAD W20-2 48"X48" LOW INTENSITY FLASHING LIGHT &amp; 18"X18" ORANGE FLAG (9 REQD.)</p> <p><b>E</b>  M5-1R 21"X15" (8 REQD.)</p> <p><b>F</b>  M4-10 48"X18" (2 REQD.)</p> <p><b>G</b>  M6-1 21"X15" (7 REQD.)</p> <p><b>H</b>  M6-3 21"X15" (22 REQD.)</p>	<p><b>I</b>  END DETOUR M4-8A 24"X18" (3 REQD.)</p> <p><b>J</b>  ILL M1-50 24"X24" (55 REQD.)</p> <p><b>K</b>  M5-1L 21"X15" (6 REQD.)</p> <p><b>L</b>  TO M4-5 24"X12" (0 REQD.)</p> <p><b>M</b>  NORTH M3-1 24"X12" (21 REQD.)</p> <p><b>N</b>  M6-1 21"X15" (7 REQD.)</p> <p><b>O</b>  SOUTH M3-3 24"X12" (32 REQD.)</p> <p><b>P</b>  DETOUR M4-8 24"X12" (52 REQD.)</p> <p><b>Q</b> ROAD CLOSED TO THRU TRAFFIC R11-4 60"X30" (2 REQD.)</p> <p><b>R</b>  M6-2 (R&amp;L) 21"X15" (3 REQD.)</p> <p><b>S</b>  ROAD CLOSED 500 FT W20-3 48"X48" LOW INTENSITY FLASHING LIGHT &amp; 18"X18" ORANGE FLAG (1 REQD.)</p>
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**


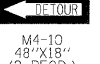
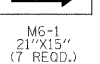





**DETOUR PLAN**

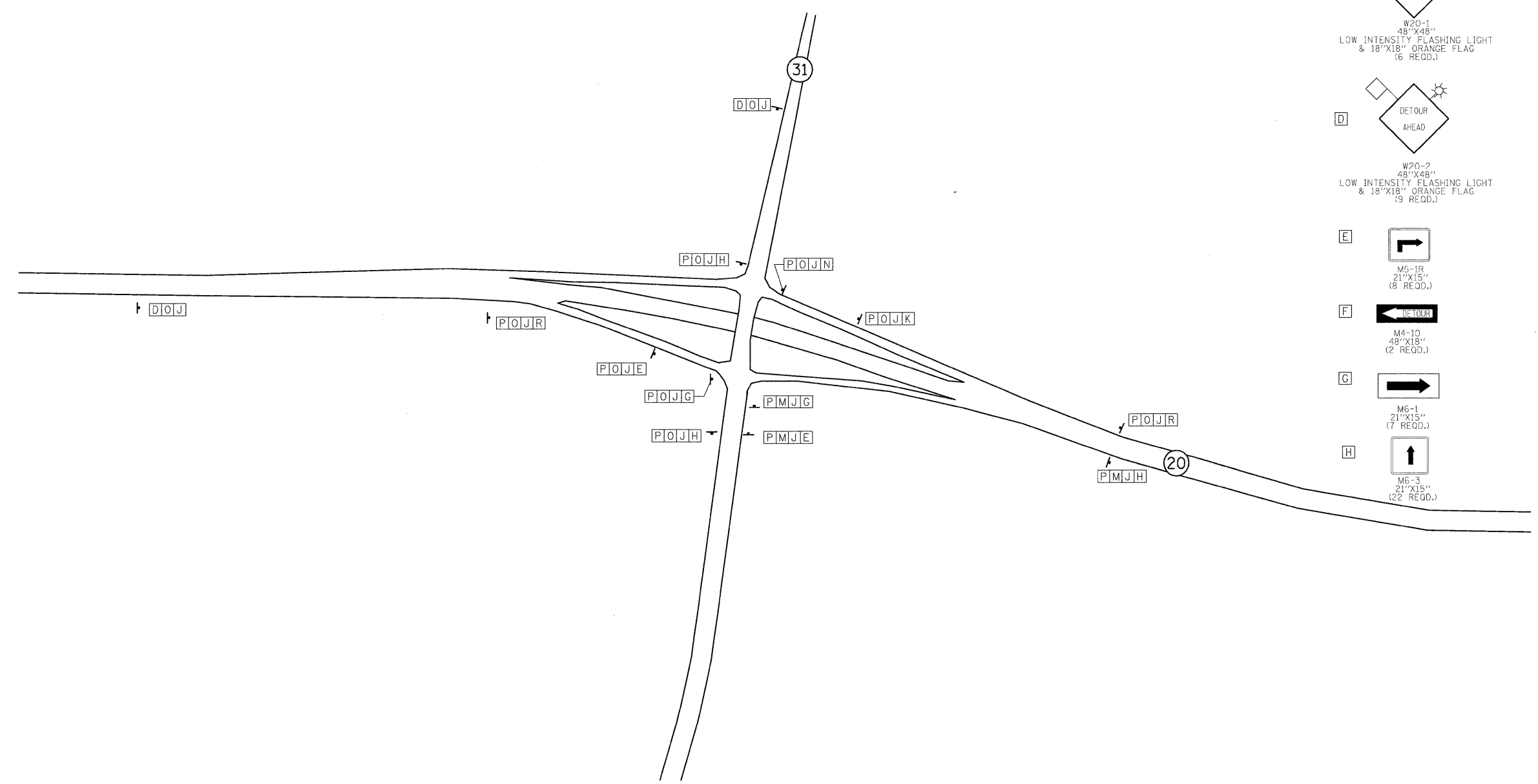
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	39-EXT-VB-1	KANE	27	4
CONTRACT NO. 60J48				
ILLINOIS FED. AID PROJECT				



### DETOUR PLAN LEGEND

- SIGN
  - [A] SIGN DESCRIPTION (SEE BELOW)
  - TT TYPE III BARRICADE WITH 2 HIGH INTENSITY WARNING LIGHTS & SIGNS AS NOTED
- |   |   |
|---|---|
| <p>[A] ROAD CLOSED<br/>R11-2<br/>48"X30"<br/>(6 REQD.)</p> <p>[B] ROAD CLOSED MILES AHEAD LOCAL TRAFFIC ONLY<br/>R11-3A<br/>60"X30"<br/>(2 REQD.)</p> <p>[C] ROAD CLOSED AHEAD<br/>W20-1<br/>48"X48"<br/>LOW INTENSITY FLASHING LIGHT &amp; 18"X18" ORANGE FLAG<br/>(6 REQD.)</p> <p>[D] DETOUR AHEAD<br/>W20-2<br/>48"X48"<br/>LOW INTENSITY FLASHING LIGHT &amp; 18"X18" ORANGE FLAG<br/>(9 REQD.)</p> <p>[E] <br/>M5-1R<br/>21"X15"<br/>(8 REQD.)</p> <p>[F] <br/>M4-10<br/>48"X18"<br/>(2 REQD.)</p> <p>[G] <br/>M6-1<br/>21"X15"<br/>(7 REQD.)</p> <p>[H] <br/>M6-3<br/>21"X15"<br/>(22 REQD.)</p> | <p>[I] END DETOUR<br/>M4-8A<br/>24"X18"<br/>(5 REQD.)</p> <p>[J] ILL 25<br/>M1-50<br/>24"X24"<br/>(55 REQD.)</p> <p>[K] <br/>M5-1L<br/>21"X15"<br/>(6 REQD.)</p> <p>[L] TO<br/>M4-5<br/>24"X12"<br/>(6 REQD.)</p> <p>[M] NORTH<br/>M3-1<br/>24"X12"<br/>(21 REQD.)</p> <p>[N] <br/>M6-1<br/>21"X15"<br/>(7 REQD.)</p> <p>[O] SOUTH<br/>M3-3<br/>24"X12"<br/>(32 REQD.)</p> <p>[P] DETOUR<br/>M4-8<br/>24"X12"<br/>(52 REQD.)</p> <p>[Q] ROAD CLOSED TO THRU TRAFFIC<br/>R11-4<br/>60"X30"<br/>(2 REQD.)</p> <p>[R] <br/>M6-2 (R&amp;L)<br/>21"X15"<br/>(3 REQD.)</p> <p>[S] <br/>W20-3<br/>48"X48"<br/>LOW INTENSITY FLASHING LIGHT &amp; 18"X18" ORANGE FLAG<br/>(1 REQD.)</p> |
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	DATE -	REVISED -	

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETOUR PLAN**

SCALE: 1"=200' SHEET NO. 5 OF 5 SHEETS STA. TO STA.

F.A.U. RTE. 2503	SECTION 39-EXT-VB-1	COUNTY KANE	TOTAL SHEETS 27	SHEET NO. 5
<b>CONTRACT NO. 60J48</b>				
ILLINOIS FED. AID PROJECT				

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**DETOUR PLAN LEGEND**

- SIGN
- [A] SIGN DESCRIPTION (SEE BELOW)
- TT TYPE III BARRICADE WITH 2 HIGH INTENSITY WARNING LIGHTS & SIGNS AS NOTED

<p><b>A</b> ROAD CLOSED R11-2 48"X30" (6 REQD.)</p> <p><b>B</b> ROAD CLOSED MILES AHEAD LOCAL TRAFFIC ONLY R11-3A 60"X30" (2 REQD.)</p> <p><b>C</b> ROAD CLOSED AHEAD W20-1 48"X48" LOW INTENSITY FLASHING LIGHT &amp; 18"X18" ORANGE FLAG (6 REQD.)</p> <p><b>D</b> DETOUR AHEAD W20-2 48"X48" LOW INTENSITY FLASHING LIGHT &amp; 18"X18" ORANGE FLAG (9 REQD.)</p> <p><b>E</b>  M5-1R 21"X15" (8 REQD.)</p> <p><b>F</b>  M4-10 48"X18" (2 REQD.)</p> <p><b>G</b>  M6-1 21"X15" (7 REQD.)</p> <p><b>H</b>  M6-3 21"X15" (22 REQD.)</p>	<p><b>I</b>  END DETOUR M4-8A 24"X18" (3 REQD.)</p> <p><b>J</b>  ILL 25 M1-50 24"X24" (55 REQD.)</p> <p><b>K</b>  M5-1L 21"X15" (6 REQD.)</p> <p><b>L</b>  TO M4-5 24"X12" (6 REQD.)</p> <p><b>M</b>  NORTH M3-1 24"X12" (21 REQD.)</p> <p><b>N</b>  M6-1 21"X15" (7 REQD.)</p> <p><b>O</b>  SOUTH M3-3 24"X12" (32 REQD.)</p> <p><b>P</b>  DETOUR M4-8 24"X12" (52 REQD.)</p> <p><b>Q</b>  ROAD CLOSED TO THRU TRAFFIC R11-4 60"X30" (2 REQD.)</p> <p><b>R</b>  M6-2 (R&amp;L) 21"X15" (3 REQD.)</p> <p><b>S</b>  ROAD CLOSED 500 FT W20-3 48"X48" LOW INTENSITY FLASHING LIGHT &amp; 18"X18" ORANGE FLAG (1 REQD.)</p>
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETOUR PLAN**

SCALE: 1"=200' SHEET NO. 6 OF 6 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	39-EXT-VB-1	KANE	27	6
CONTRACT NO. 60J48				
ILLINOIS FED. AID PROJECT				



**DETOUR PLAN LEGEND**

- SIGN
  - [A] SIGN DESCRIPTION (SEE BELOW)
  - TYPE III BARRICADE WITH 2 HIGH INTENSITY WARNING LIGHTS & SIGNS AS NOTED
- |   |   |
|---|---|
| <p>[A] ROAD CLOSED<br/>R11-2<br/>48"X30"<br/>(6 REQD.)</p> <p>[B] ROAD CLOSED<br/>MILES AHEAD<br/>LOCAL TRAFFIC ONLY<br/>R11-3A<br/>60"X30"<br/>(2 REQD.)</p> <p>[C] ROAD CLOSED<br/>AHEAD<br/>W20-1<br/>48"X48"<br/>LOW INTENSITY FLASHING LIGHT<br/>&amp; 18"X18" ORANGE FLAG<br/>(6 REQD.)</p> <p>[D] DETOUR<br/>AHEAD<br/>W20-2<br/>48"X48"<br/>LOW INTENSITY FLASHING LIGHT<br/>&amp; 18"X18" ORANGE FLAG<br/>(9 REQD.)</p> <p>[E] M5-1R<br/>21"X15"<br/>(6 REQD.)</p> <p>[F] M4-1C<br/>48"X18"<br/>(2 REQD.)</p> <p>[G] M6-1<br/>21"X15"<br/>(7 REQD.)</p> <p>[H] M6-3<br/>21"X15"<br/>(22 REQD.)</p> | <p>[I] END<br/>DETOUR<br/>M4-8A<br/>24"X18"<br/>(3 REQD.)</p> <p>[J] ILL<br/>25<br/>M1-50<br/>24"X24"<br/>(55 REQD.)</p> <p>[K] M5-1L<br/>21"X15"<br/>(6 REQD.)</p> <p>[L] TO<br/>M4-5<br/>24"X12"<br/>(0 REQD.)</p> <p>[M] NORTH<br/>M3-1<br/>24"X12"<br/>(21 REQD.)</p> <p>[N] M6-1<br/>21"X15"<br/>(7 REQD.)</p> <p>[O] SOUTH<br/>M3-3<br/>24"X12"<br/>(32 REQD.)</p> <p>[P] DETOUR<br/>M4-8<br/>24"X12"<br/>(52 REQD.)</p> <p>[Q] ROAD CLOSED<br/>TO<br/>THRU TRAFFIC<br/>R11-4<br/>60"X30"<br/>(2 REQD.)</p> <p>[R] M6-2 (R&amp;L)<br/>21"X15"<br/>(3 REQD.)</p> <p>[S] ROAD CLOSED<br/>500 FT<br/>W20-3<br/>48"X48"<br/>LOW INTENSITY FLASHING<br/>&amp; 18"X18" ORANGE<br/>(1 REQD.)</p> |
|---|---|

FILE NAME = G:\ENR\96\5329-02\_01\_Verlow\_Structura\CAD\045006\Sheet\01\XXXXX-2.dgn



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PLOT SCALE = #SCALE#	DRAWN - ---	REVISED - ---
PLOT DATE = 4/28/2010	CHECKED - ---	REVISED - ---
	DATE - ---	REVISED - ---

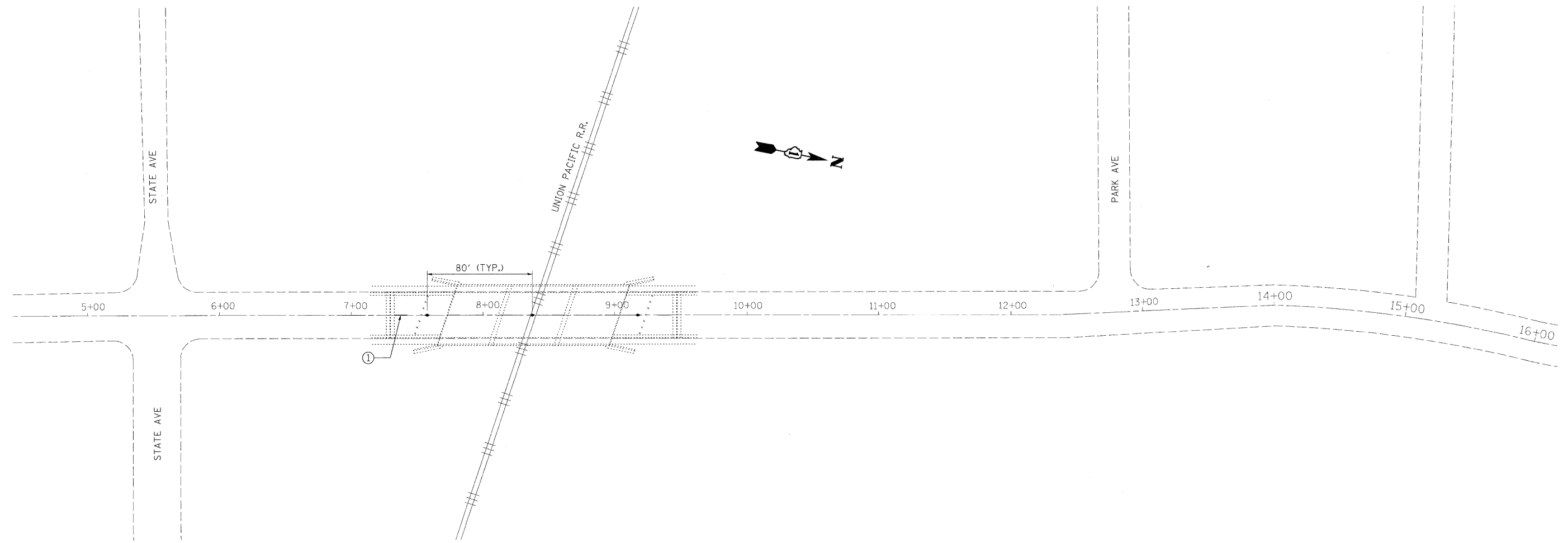
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETOUR PLAN**

SCALE: 1"=200' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2503	SECTION 39-EXT-VB-1	COUNTY KANE	TOTAL SHEETS 27	SHEET NO. 7
CONTRACT NO. 60J48				
ILLINOIS FED. AID PROJECT				

FILE NAME = G:\ENGV06-53291-02 D1 Various Structures\CD\43006E1\SH\A\11XXXXX-shvz.pmk.dgn



**LEGEND**

- ① POLYUREA PAVEMENT MARKING  
- LINE 4" (YELLOW)  
10' DASH - 30' SKIP
- ◆ TWO WAY AMBER RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)



USER NAME = *USER*	DESIGNED -	REVISED -
PLOT SCALE = *SCALE*	DRAWN -	REVISED -
PLOT DATE = 4/28/2010	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

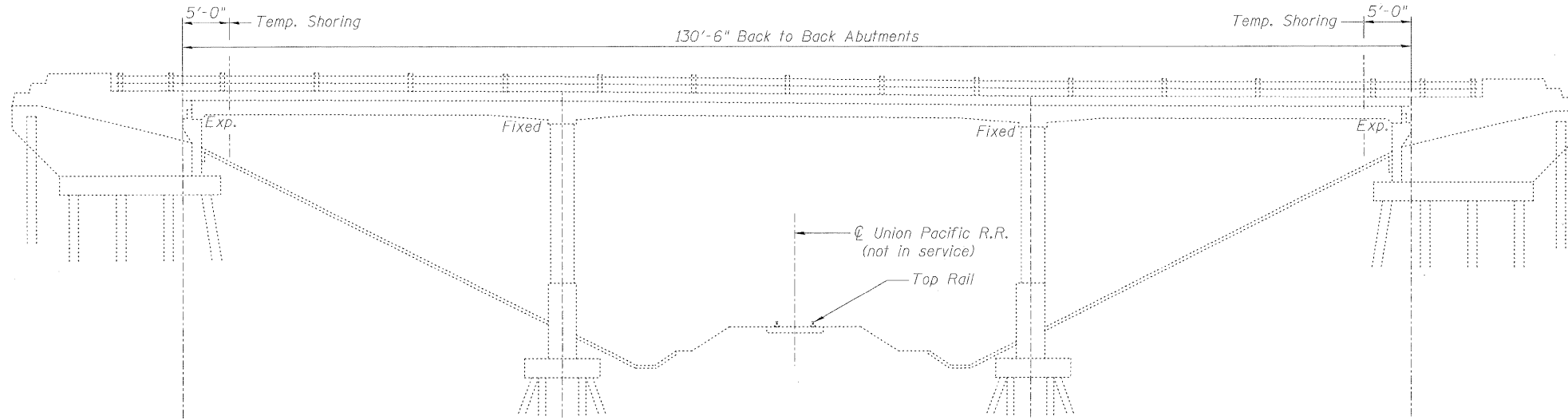
**PROPOSED PAVEMENT MARKING PLAN**

SCALE: SHEET NO. \_\_ OF \_\_\_ SHEETS STA. TO STA.

F.A.J. RTE. 2503	SECTION 39-EXT-VB-1	COUNTY KANE	TOTAL SHEETS 27	SHEET NO. 8
<b>CONTRACT NO. 60J48</b>				
ILLINOIS FED. AID PROJECT				



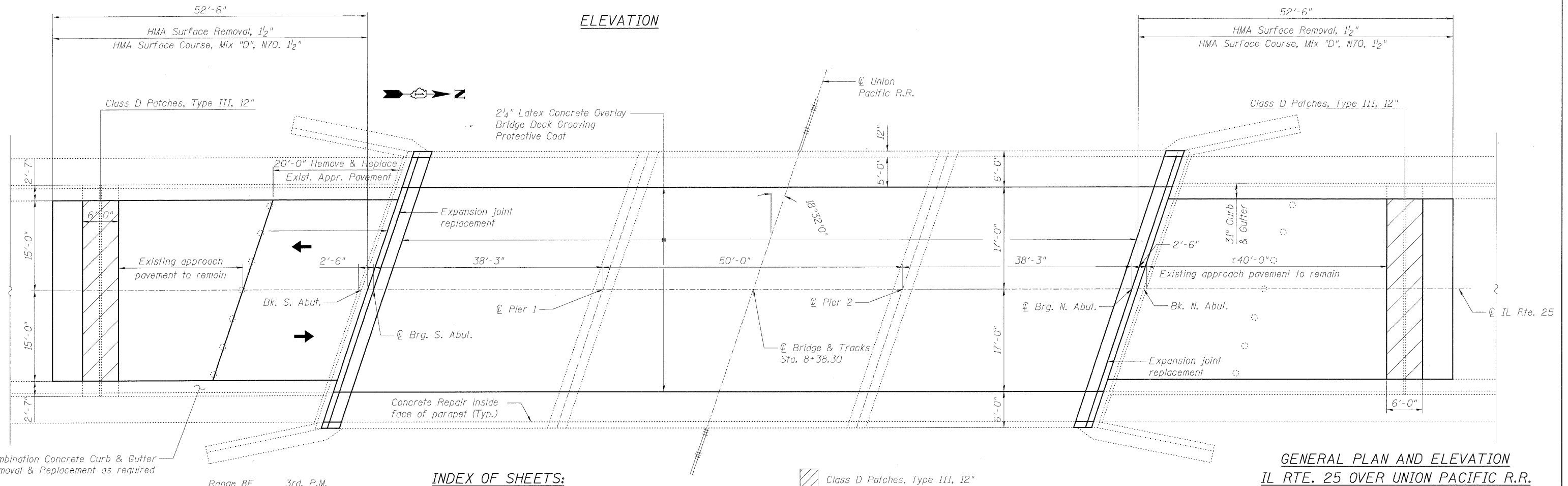
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



R DL	5.0 kip/ft
Rs DL	0.5 kip/ft
R TOT	5.5 kip/ft

Reactions are unfactored dead load reactions per unit width of bridge deck. Since the road is to be closed and traffic detoured, live load is not included in the reactions shown above. The Contractor shall adjust the reactions as required to account for actual conditions and anticipated construction live load. The Contractor shall submit details and calculations for approval per the Special Provision "Temporary Shoring and Cribbing".

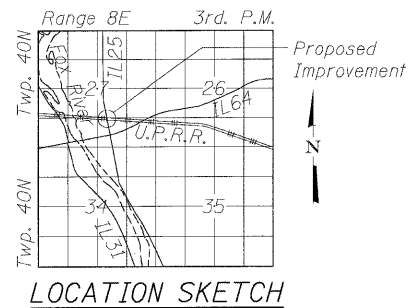
**ELEVATION**



**PLAN**

**INDEX OF SHEETS:**

1. General Plan and Elevation
2. General Notes and Bill of Material
3. Cross Section
4. Deck Repair Plan
5. Expansion Joint Removal
6. Expansion Joint Replacement
7. Preformed Joint Strip Seal
8. Rail Post Details
9. Parapet Repair
10. Substructure Repair
11. Bridge Approach Slab Details
12. Bridge Approach Slab Details
13. Approach Pavement Repair



DESIGNED	BRT
CHECKED	BJM
DRAWN	JNH
CHECKED	BJM

**GENERAL PLAN AND ELEVATION  
IL RTE. 25 OVER UNION PACIFIC R.R.  
F.A.U RTE. 2503 - SEC. 32-CS  
KANE COUNTY  
STATION 8+38.30  
STRUCTURE NO. 045-0061**



**Wight & Company**  
2500 North Frontage Road . Darien . IL 60561  
630.969.7000 630.969.7979 fax  
Design Firm Registration 184-000451

SHEET NO. 1 13 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2503	39 EXT-VB-1	KANE	27	9
CONTRACT NO. 60J48					
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT					

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOTAL BILL OF MATERIAL

**NOTES:**

- The contractor shall provide support and/or shoring systems for the end of deck during joint reconstruction. See Special Provision "Temporary Shoring and Cribbing".
- Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60.
- Reinforcement bars designated (E) shall be epoxy coated.
- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- Areas of deck repairs shown are estimated. The engineer shall show actual locations of deck repairs on as-built plans.
- Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. When the deck is poured at an ambient temperature other than 50° F.
- The road is to be closed and traffic detoured during construction.
- Utility lines and underground structures as shown shall be considered approximate only. It is the Contractor's responsibility to determine the existence of all utilities, whether shown or not shown on these plans, prior to commencing any work. The Contractor shall contact all utility companies to field locate all utilities prior to grading or construction of improvements.

ITEM	UNIT	SUPER	SUB	TOTAL
Hot-Mix Asphalt Surface, Course, Mix "D", N70	Ton	29.4		29.4
Bituminous Materials (Prime Coat)	Gal.	175		175
Protective Coat	Sq. Yd.	705		705
Hot-Mix Asphalt Surface Removal, 1 1/2"	Sq. Yd.	350.0		350.0
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	478		478
Concrete Removal	Cu. Yd.	18.2		18.2
Concrete Superstructure	Cu. Yd.	38.8		38.8
Bridge Deck Grooving	Sq. Yd.	478		478
Reinforcement Bars, Epoxy Coated	Pound	9450		9450
Preformed Joint Strip Seal	Foot	98		98
Bridge Deck Hydro-scarification, 1/2"	Sq. Yd.	478		478
Deck Slab Repair (Full-Depth, Type II)	Sq. Yd.	6		6
Class D Patches, Type III, 12 Inch	Sq. Yd.	40		40
Temporary Shoring and Cribbing	L. Sum	1		1
Bridge Deck Latex Concrete Overlay, 2 1/4"	Sq. Yd.	478		478
Structural Repair of Concrete (Depth equal to or less than 5")	Sq. Ft.	117	48	165
Structural Repair of Concrete (Depth Greater than 5")	Sq. Ft.	14		14
<b>Approach Slab Removal</b>	Sq. Yd.	66.7		66.7

**NOTE:**

Cost of Deck Slab Repair (Partial) is included with Bridge Deck Latex Concrete Overlay, 2 1/4"

GENERAL NOTES AND BILL OF MATERIAL  
STRUCTURE NO. 045-0061

DESIGNED	BRT
CHECKED	BJM
DRAWN	JNH
CHECKED	BJM

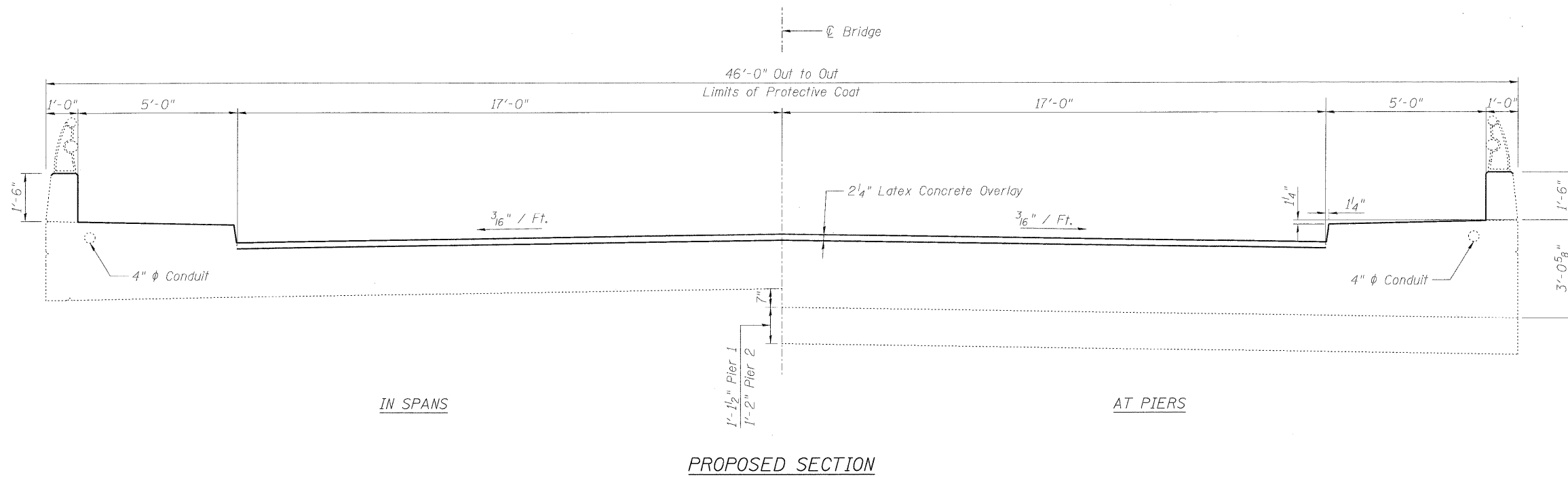
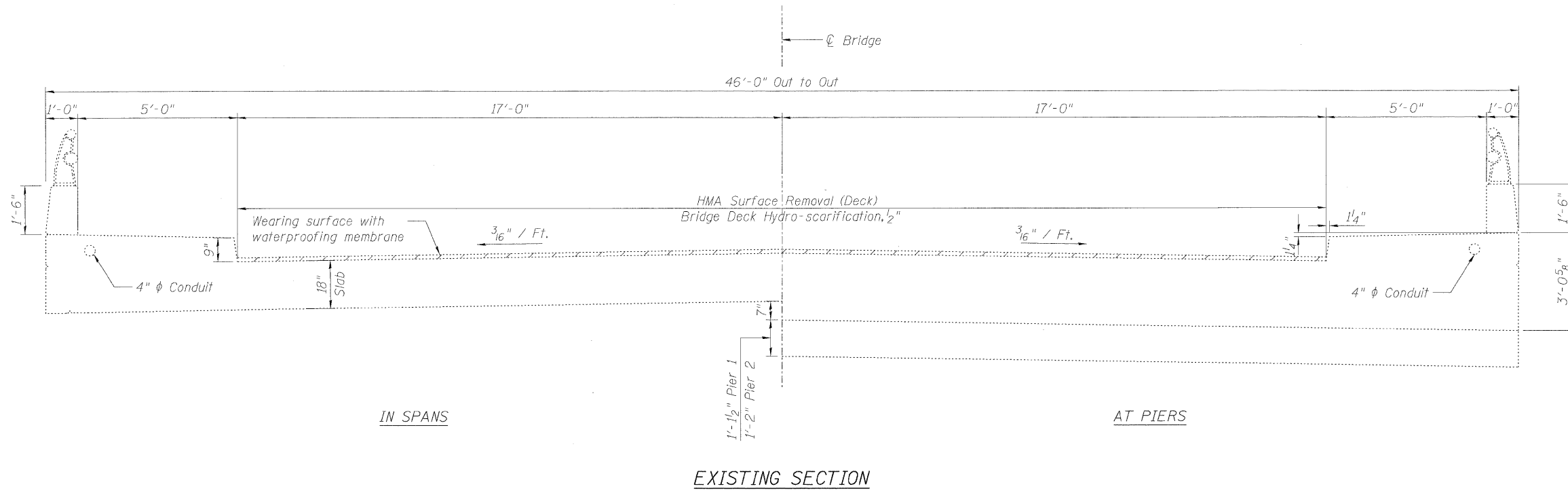


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SHEET NO. 2 13 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2503	39 EXT-VB-1	KANE	27	10
			CONTRACT NO. 60J48		
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT					

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



CROSS SECTION  
STRUCTURE NO. 045-0061

DESIGNED	BRT
CHECKED	BJM
DRAWN	JNH
CHECKED	BJM



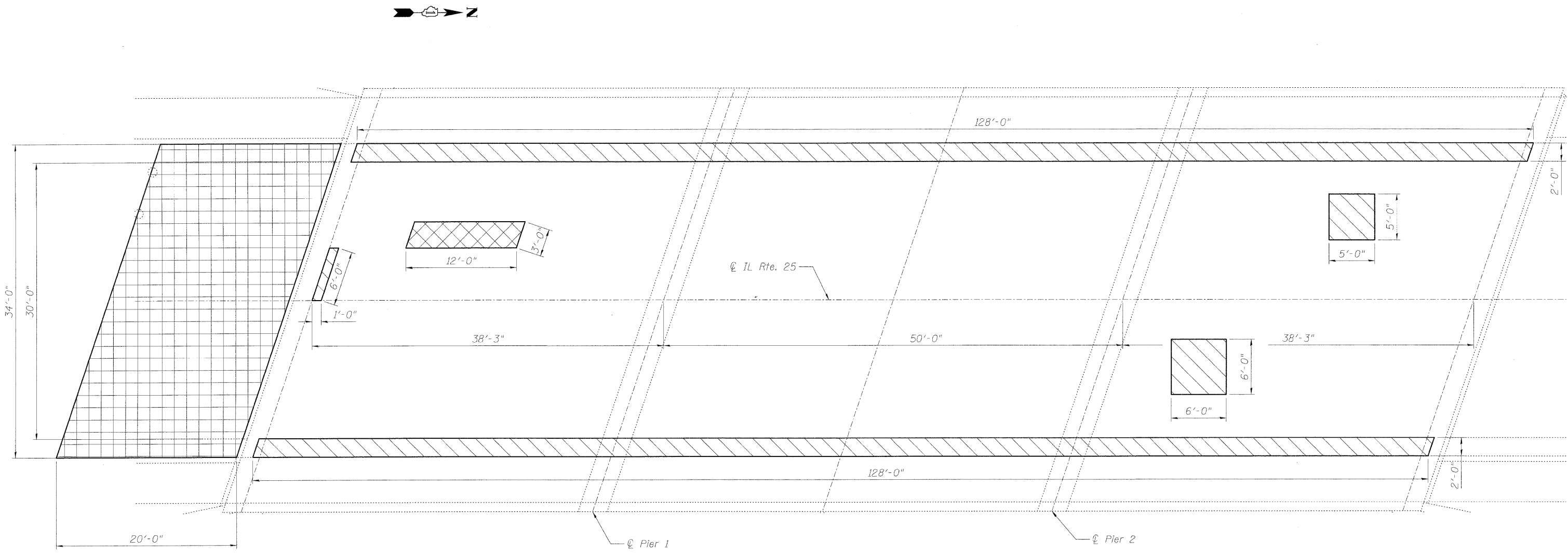
Wight & Company  
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Design Firm Registration 184-000451

SHEET NO. 3 13 SHEETS	F.A.U. RTE. 2503	SECTION 39 EXT-VB-1	COUNTY KANE	TOTAL SHEETS 27	SHEET NO. 11
	CONTRACT NO. 60J48			FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**NOTE:**

The limits and locations of repairs shown on the Deck Plan are taken from the most recent Survey performed by the District in October 2009.



PLAN

**LEGEND:**



Deck Slab Repair (Full-Depth, Type II)



Deck Slab Repair (Partial) (For information only)



Pavement Removal (Special)  
Bridge Approach Slab (Cost included with Concrete Superstructure and Reinforcement Bars, Epoxy Coated)

DESIGNED	BRT
CHECKED	BJM
DRAWN	JNH
CHECKED	BJM

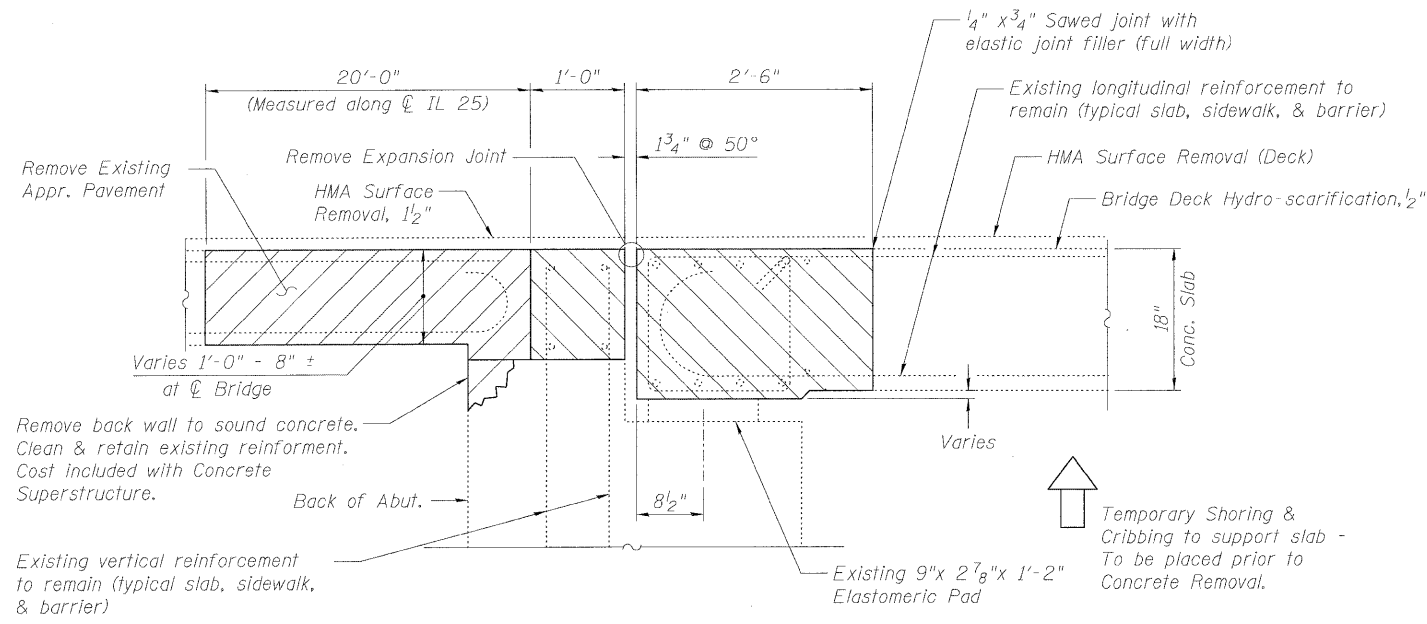
**DECK REPAIR PLAN  
STRUCTURE NO. 045-0061**



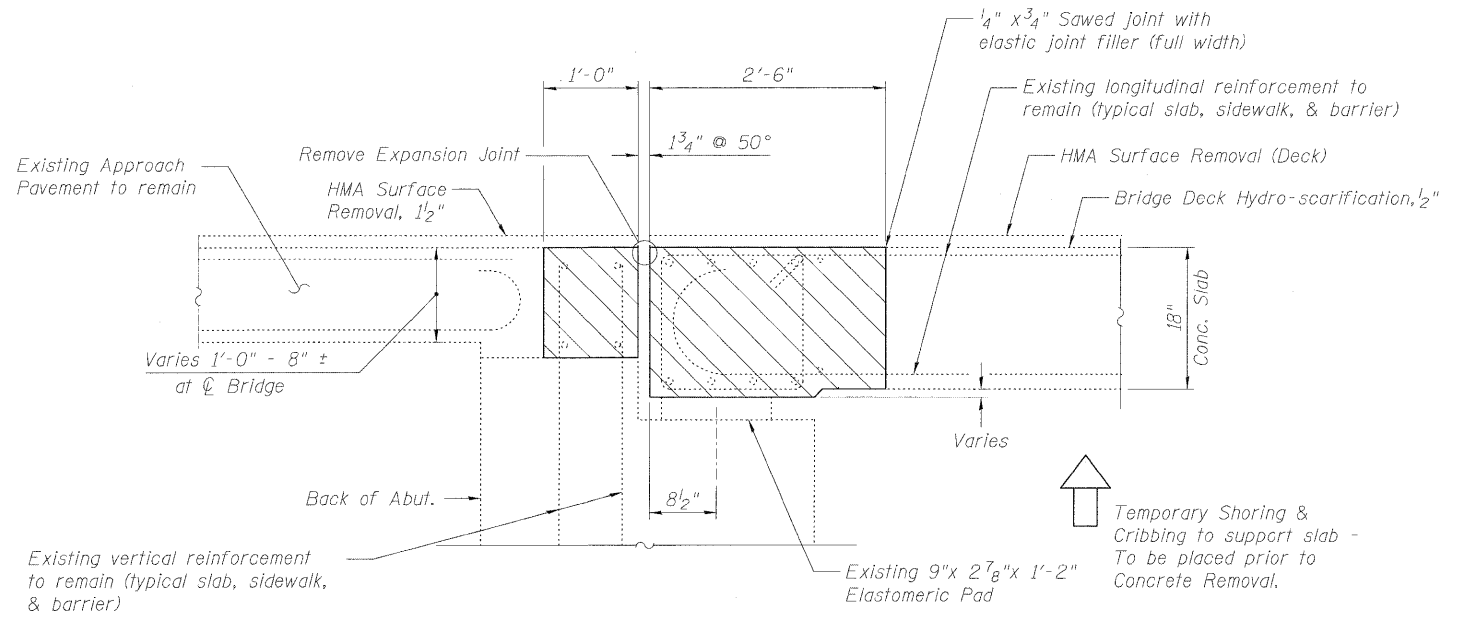
**Wight & Company**  
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630.969.7000 630.969.7979 fax  
Design Firm Registration 184-000451

SHEET NO. 4 13 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60J48					
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

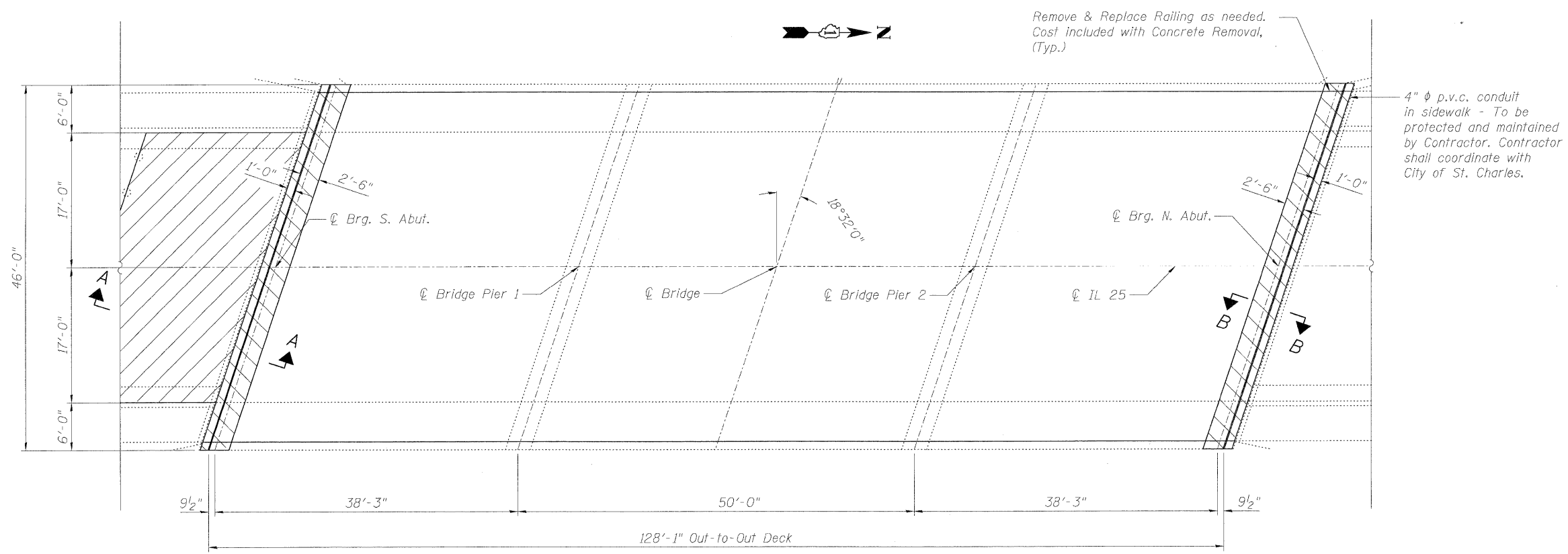
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



SECTION A-A  
South Abutment



SECTION B-B  
North Abutment



PLAN

DESIGNED	BRT
CHECKED	BJM
DRAWN	JNH
CHECKED	BJM



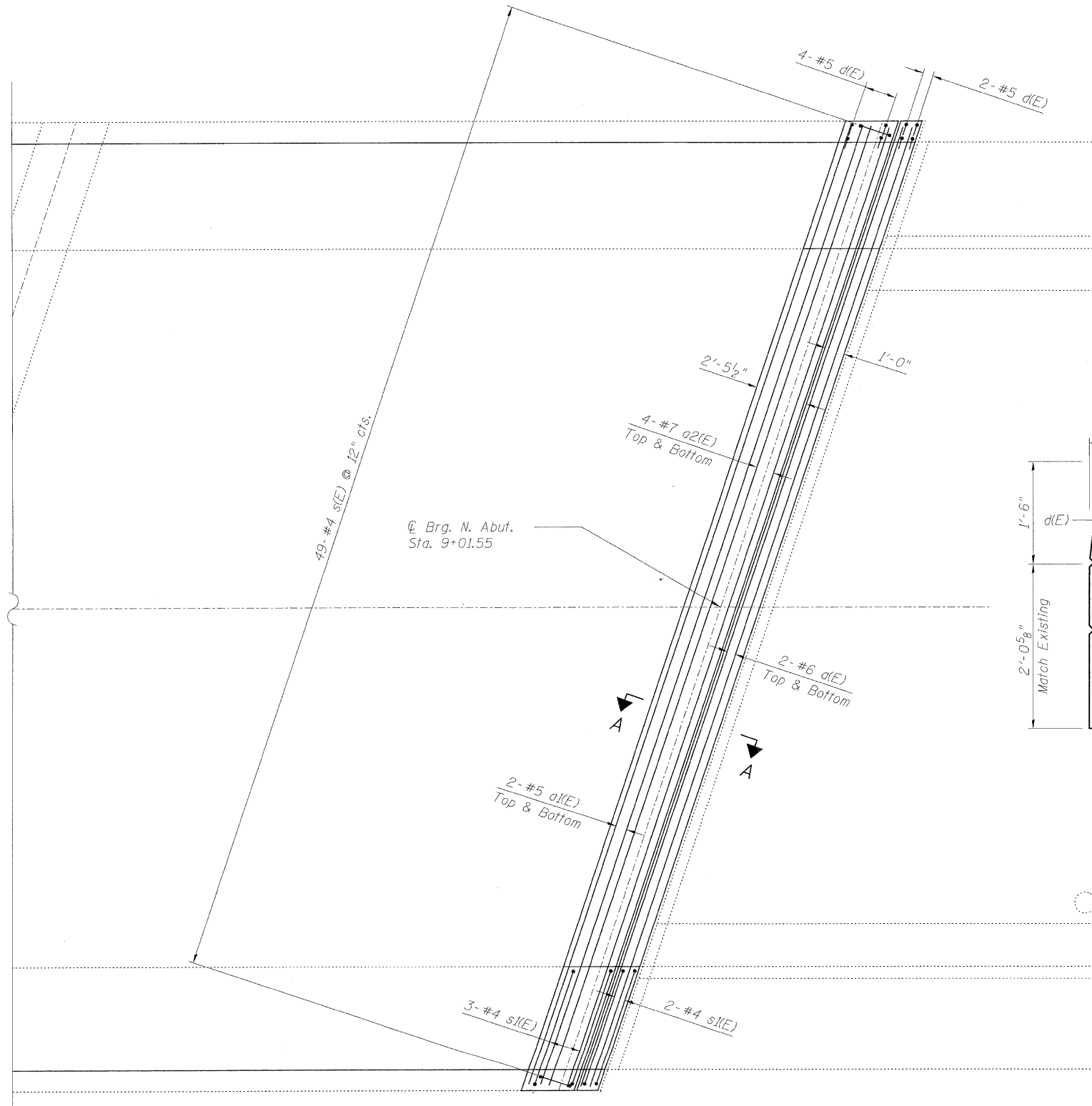
**Wight & Company**  
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630.969.7000 630.969.7979 fax  
Design Firm Registration 184-000451

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			CONTRACT NO. 60J48		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

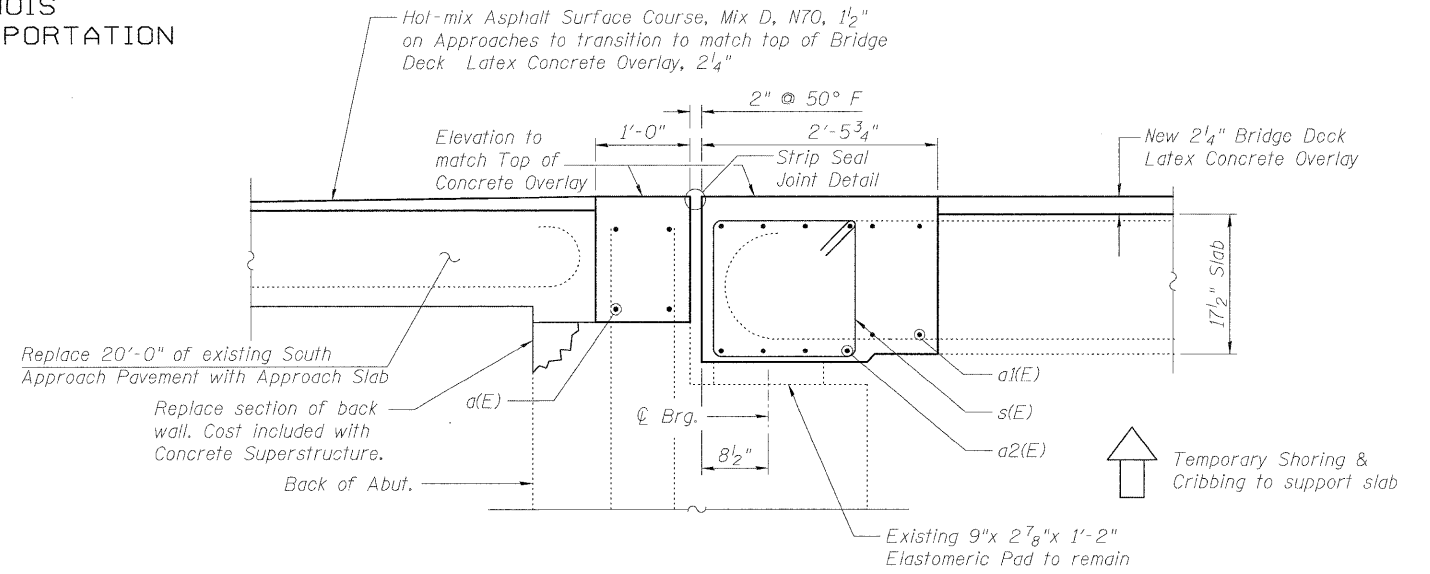
EXPANSION JOINT REMOVAL  
STRUCTURE NO. 045-0061

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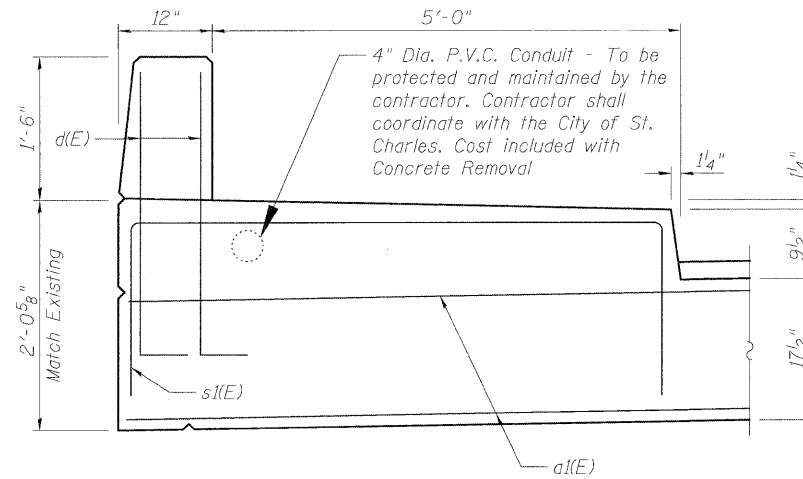
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



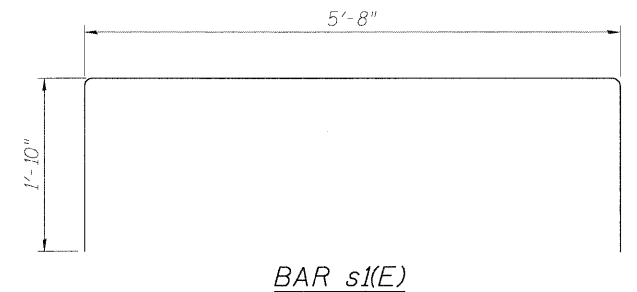
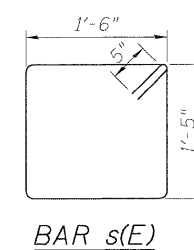
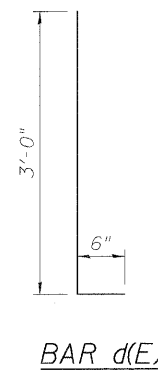
**EXPANSION JOINT PLAN**  
(North Abutment shown -  
South Abutment Similar)



**SECTION A-A**  
**SOUTH**  
(North similar, except no new Approach Slab.)



**SECTION THRU SIDEWALK**



**BILL OF MATERIAL**  
North & South Abutments

No.	Size	Length	Shape
a(E)	#6	48'-0"	—
a1(E)	#5	48'-0"	—
a2(E)	#7	48'-0"	—
d(E)	#5	3'-6"	┌
s(E)	#4	5'-11"	□
s1(E)	#4	9'-4"	└
Concrete Removal		Cu. Yd.	18.2
Preformed Joint Strip Seal		Foot	98
Reinforcement Bars, Epoxy Coated		Pound	2950
Concrete Superstructure		Cu. Yd.	20.0

DESIGNED	BRT
CHECKED	BJM
DRAWN	JNH
CHECKED	BJM

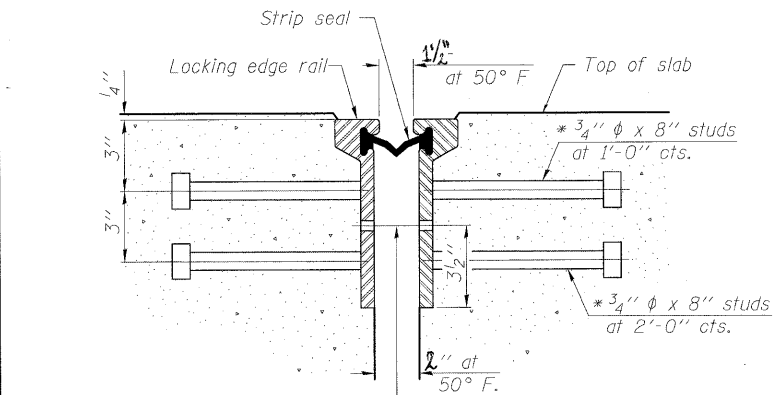


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Design Firm Registration 184-000451

SHEET NO. 6 13 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 60J48		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

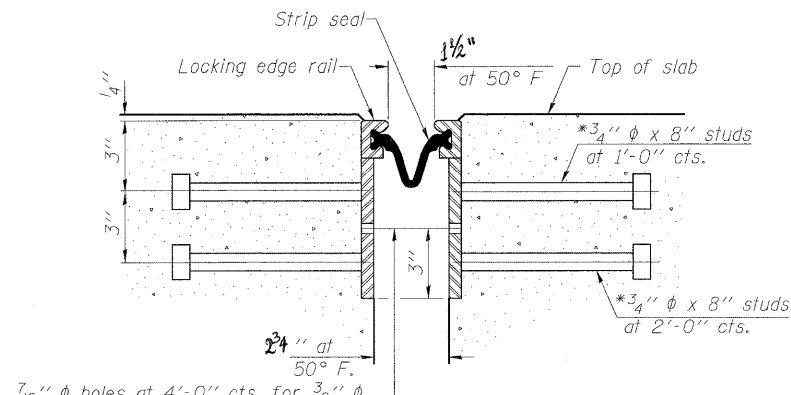
\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



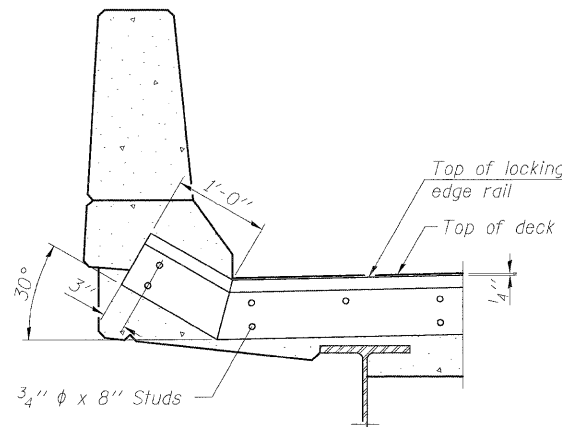
7/16"  $\phi$  holes at 4'-0" cts. for 3/8"  $\phi$  bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU  
ROLLED RAIL JOINT

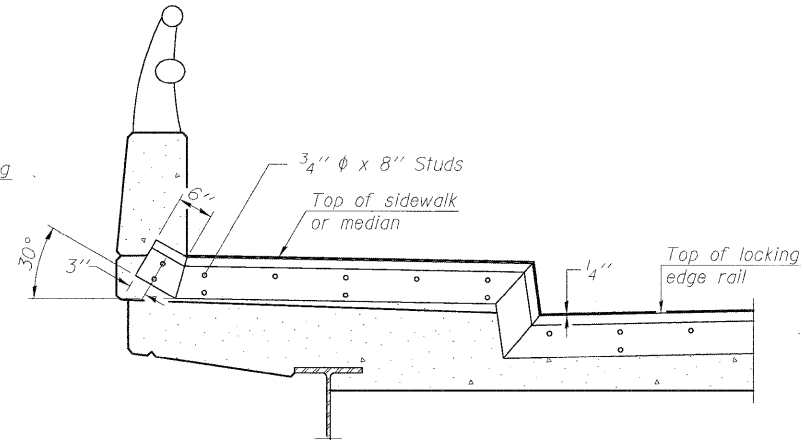


7/16"  $\phi$  holes at 4'-0" cts. for 3/8"  $\phi$  bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU  
WELDED RAIL JOINT



AT PARAPET  
See Section A-A for end treatment of skews > 30°.

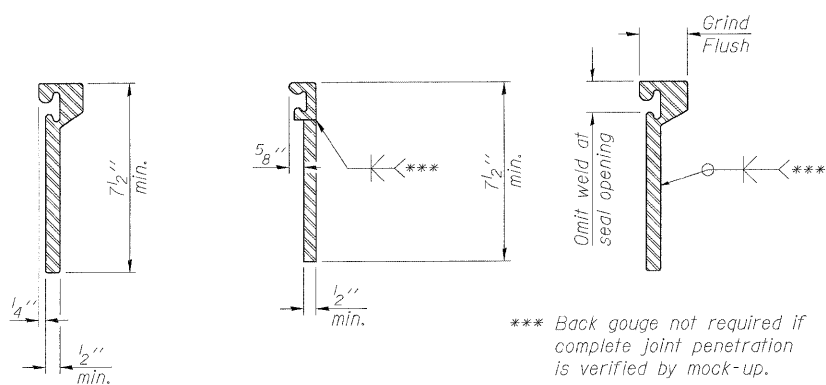


AT SIDEWALK OR MEDIAN  
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

TYPICAL END TREATMENTS

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches. The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities. The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State. All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications. Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

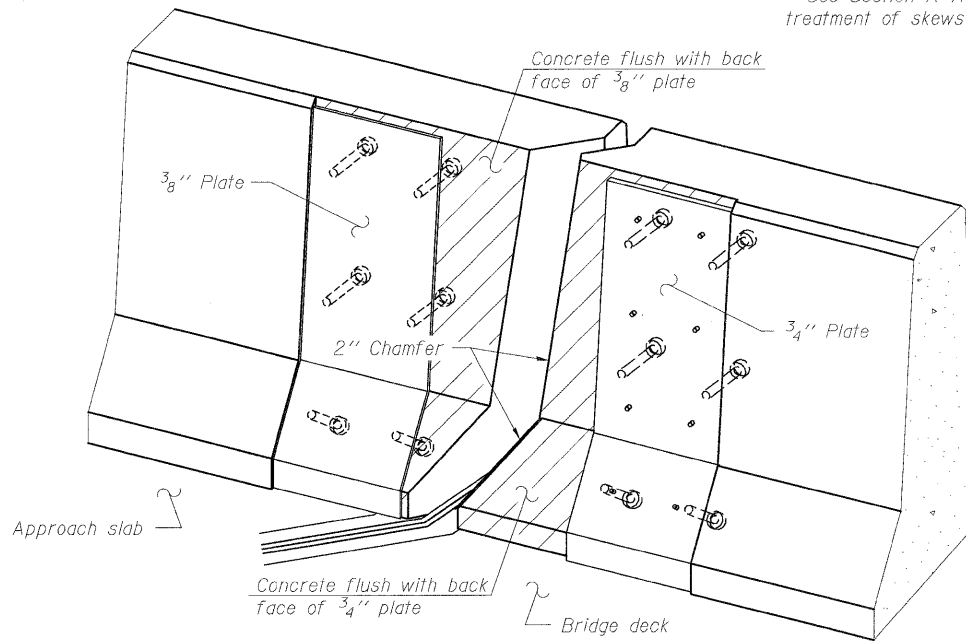


ROLLED EXTRUDED RAIL  
WELDED RAIL

\*\*\* Back gouge not required if complete joint penetration is verified by mock-up.

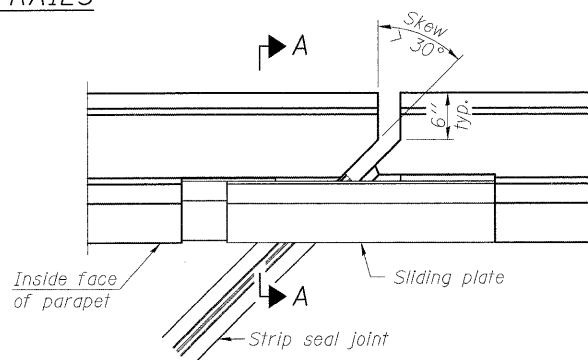
LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

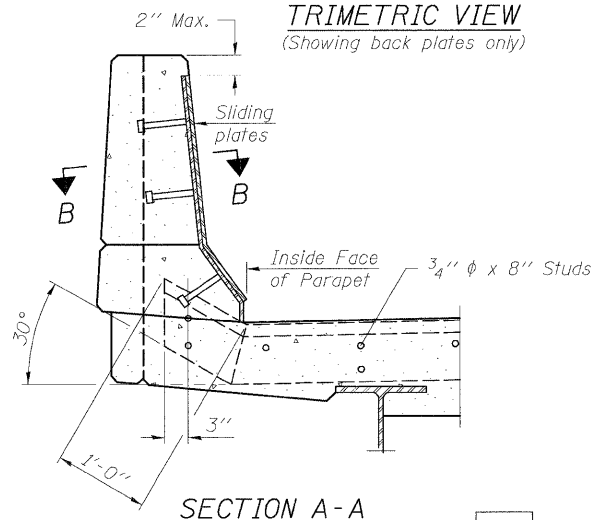


TRIMETRIC VIEW  
(Showing back plates only)

LOCKING EDGE RAILS

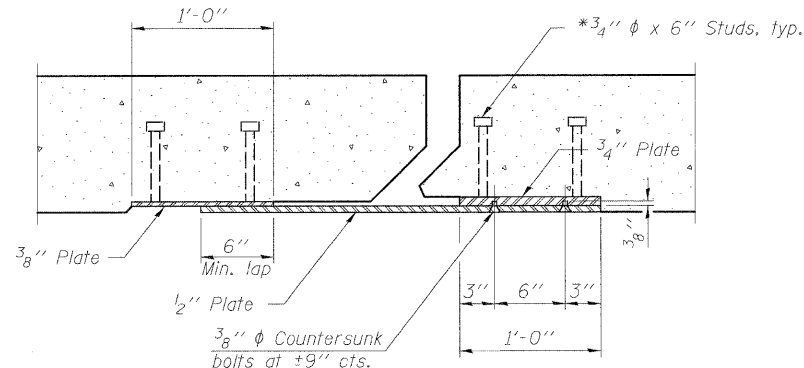


PLAN



SECTION A-A

POINT BLOCK DETAILS  
(for skews > 30°)



SECTION B-B

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	98

PREFORMED JOINT STRIP SEAL  
STRUCTURE NO. 045-0061

DESIGNED	BRT
CHECKED	BJM
DRAWN	JNH
CHECKED	BJM



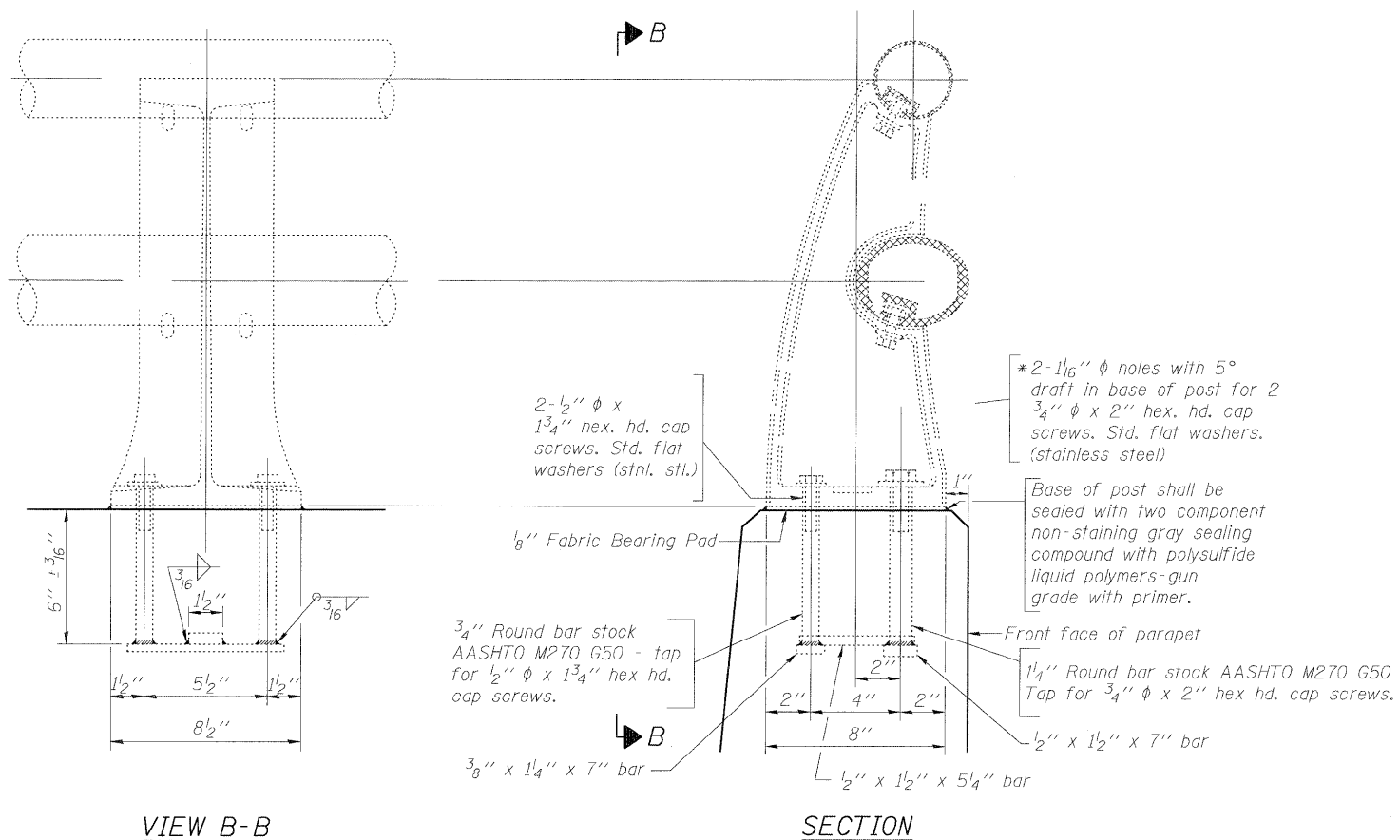
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630.969.7000 630.969.7979 fax  
Design Firm Registration 184-000451

SHEET NO. 7 13 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2503	39 EXT-VB-1	KANE	27	15
CONTRACT NO. 60J48					
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

Notes:

1. All posts shall be normal to parapet.
2. In lieu of the cast-in-place anchor device shown, the contractor has the option of drilling and setting stainless steel anchor rods of the same diameter and grade as the specified cap screws according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.
3. Removal and re-erection of the existing aluminum handrail, rail post, and all new applicable hardware, including labor and installation shall be included in the cost of Concrete Removal.



RAIL POST DETAILS  
STRUCTURE NO. 045-0061

DESIGNED	BRT
CHECKED	BJM
DRAWN	JNH
CHECKED	BJM



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SHEET NO. 8 13 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2503	39 EXT-VB-1	KANE	27	16
			CONTRACT NO. 60J48		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

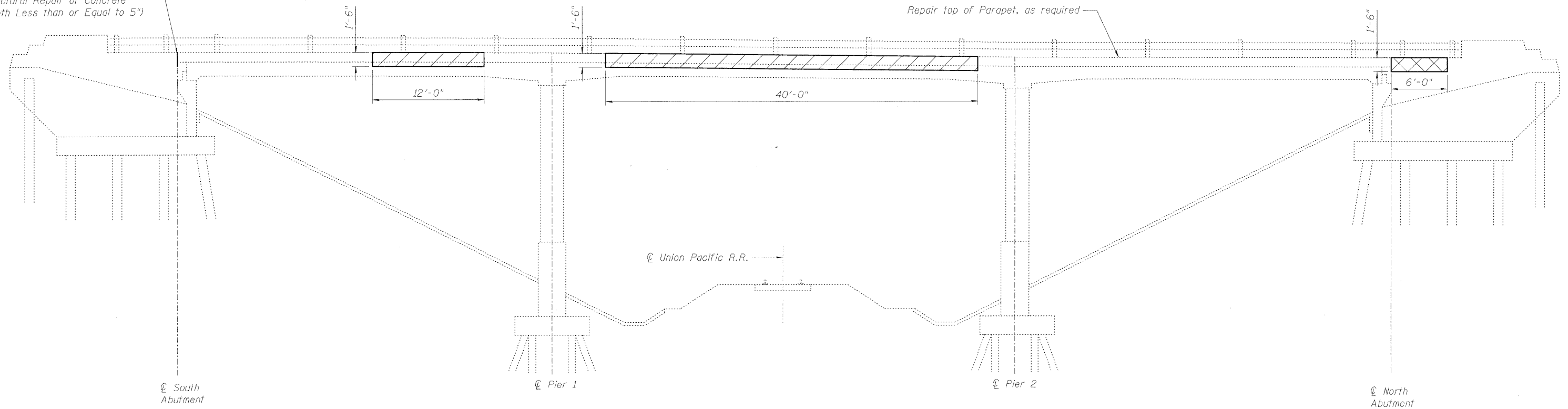


STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**NOTE:**  
Structural Repair of Concrete (Depth Less than or Equal to 5") shown on this sheet is along the inside face of the East parapet wall.

The limits and locations of repair shown on this sheet are taken from the most recent survey performed by the District in October 2009.

Southwest  
Corner of S. Abut. 16 Sq. Ft.  
Structural Repair of Concrete  
(Depth Less than or Equal to 5")



ELEVATION

**LEGEND:**



Structural Repair of Concrete  
(Depth Less than or Equal to 5")



Structural Repair of Concrete  
(Depth Greater than or Equal to 5")

DESIGNED	BRT
CHECKED	BJM
DRAWN	JNH
CHECKED	BJM

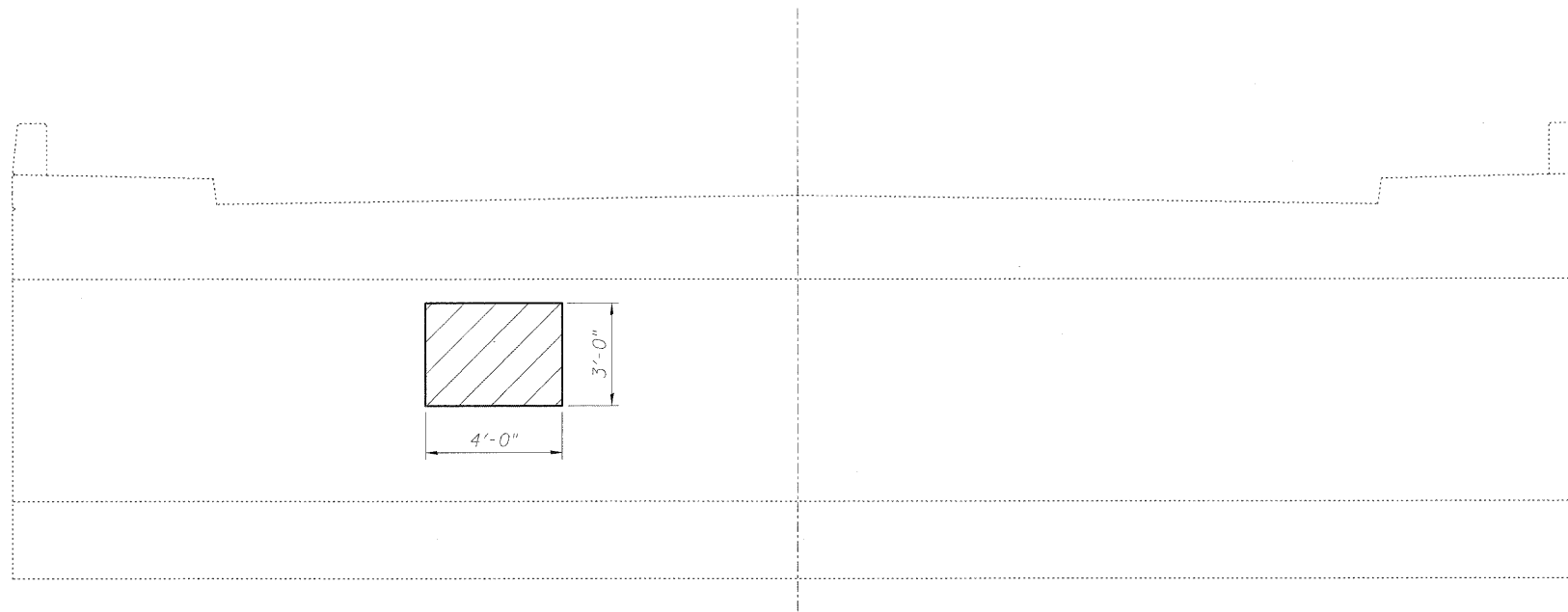
**PARAPET REPAIR  
STRUCTURE NO. 045-0061**



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2500 North Frontage Road . Darien . IL 60561  
630.969.7000 630.969.7979 fax  
Design Firm Registration 184-000451

SHEET NO. 9 13 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2503	39 EXT-VB-1	KANE	27	17
			CONTRACT NO. 60J48		
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT					

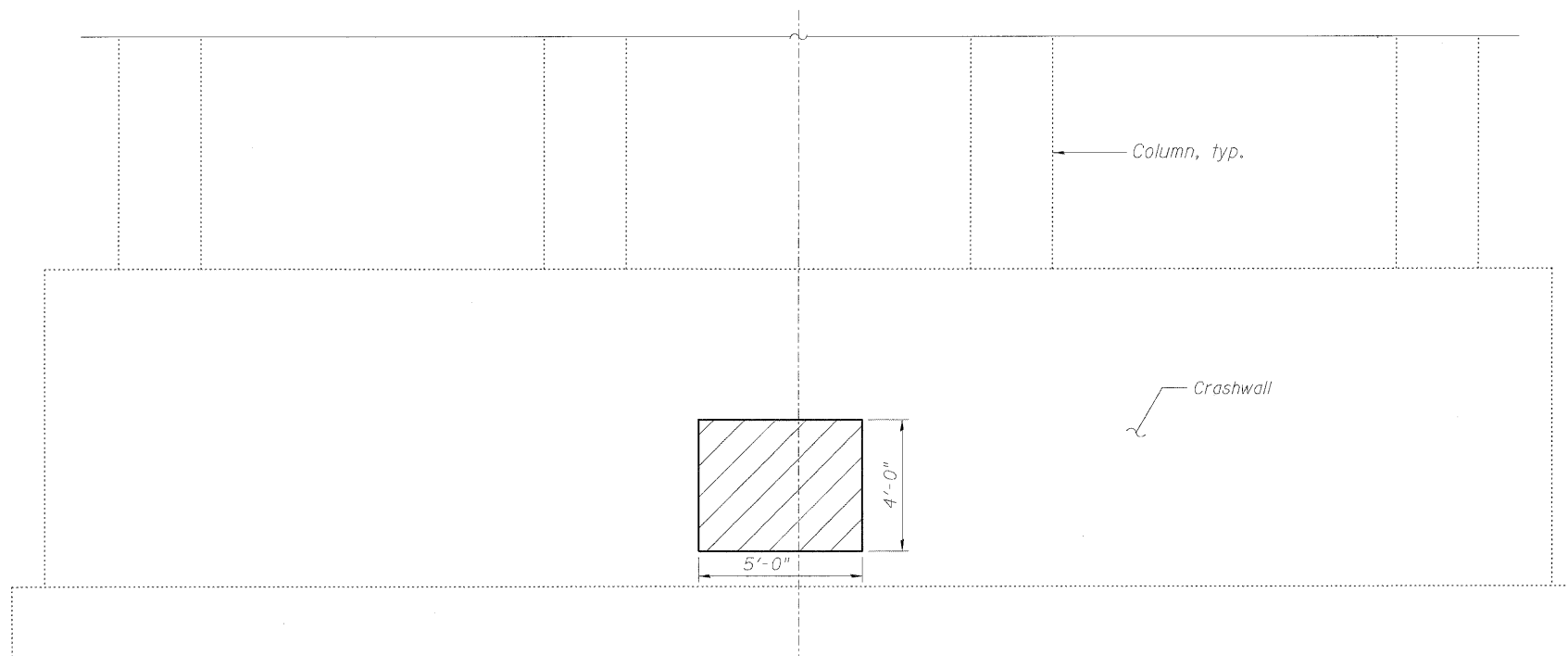
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



NORTH ABUTMENT ELEVATION  
(LOOKING NORTH)

**NOTE:**

The limits and locations of repair shown on this sheet are taken from the most recent survey performed by the District in October 2009.



PIER 2 ELEVATION  
NORTH FACE

**LEGEND:**

 Structural Repair of Concrete  
(Depth Less than or Equal to 5")

SUBSTRUCTURE REPAIR  
STRUCTURE NO. 045-0061

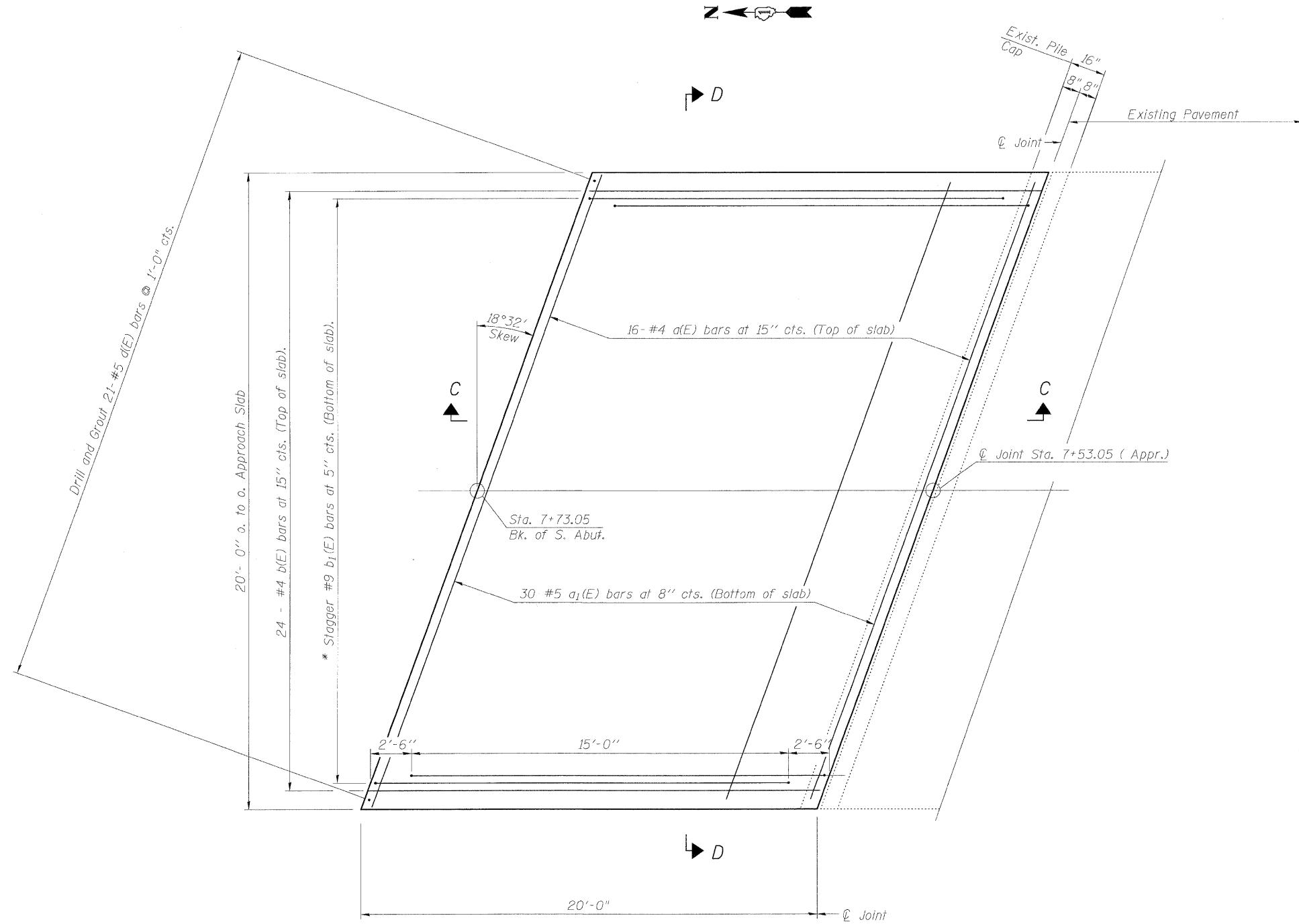
DESIGNED	BRT
CHECKED	BJM
DRAWN	JNH
CHECKED	BJM



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630.969.7000 630.969.7979 fax  
Design Firm Registration 184-000451

SHEET NO. 10 13 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2503	39 EXT-VB-1	KANE	27	18
			CONTRACT NO. 60J48		
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

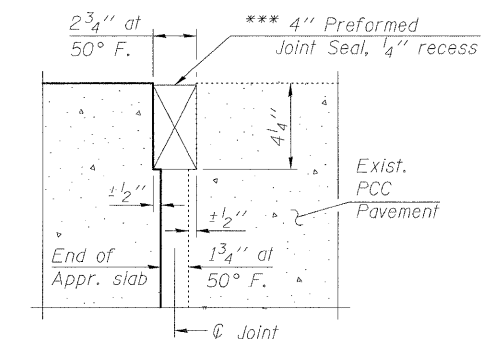


PLAN

\* Tilt #9 b<sub>1</sub>(E) bars as required to maintain clearance.

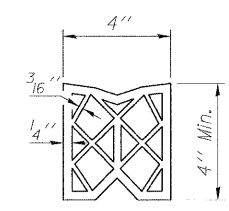
Notes:  
See sheet 20 of 27 for Sections C-C & D-D.  
a(E) and a<sub>1</sub>(E) bar spacings measured along  $\phi$  Rdwy.

\*\*\* Cost included with Concrete Superstructure.



RIGID PAVEMENT

DETAIL A



PREFORMED JOINT SEAL

BRIDGE APPROACH SLAB DETAILS  
STRUCTURE NO. 045-0061

DESIGNED	BRT
CHECKED	BJM
DRAWN	JNH
CHECKED	BJM



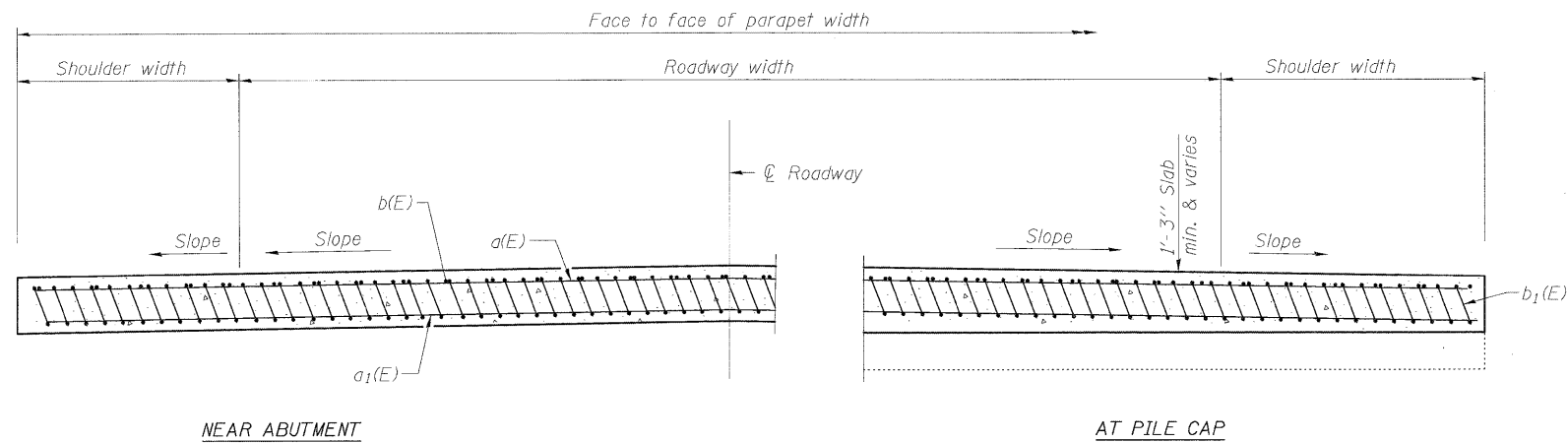
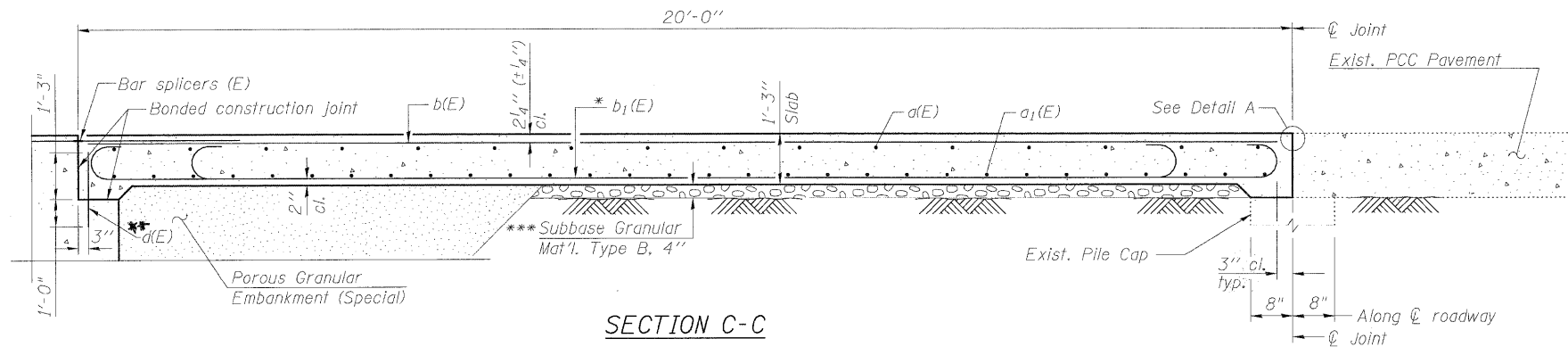
Wight & Company  
2500 North Frontage Road . Darien . IL 60561  
630.969.7000 630.969.7979 fax  
Design Firm Registration 184-000451

SHEET NO. 11 13 SHEETS	F.A.U. RTE. 2503	SECTION 39 EXT-VB-1	COUNTY KANE	TOTAL SHEETS 27	SHEET NO. 19
	CONTRACT NO. 60J48			FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT	

G:\ENG\06-5328-02.D1 Various Structure\CAD\0450061\Sh1\0450061\_ApproachSlab\_Details.dgn 4/28/2010 1:30:38 PM Default

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

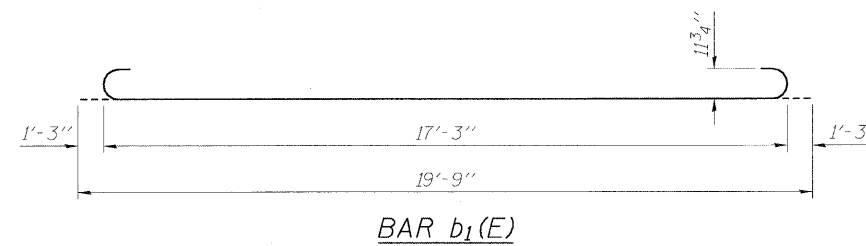
Notes:  
See sheet 19 of 27 for Detail A.  
Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.  
Approach Slab concrete shall be paid for as Concrete Superstructure.



\* Tilt #9  $b_1(E)$  bars as required to maintain clearance.  
\*\*\* Cost included with Concrete Superstructure.  
\*\*  $d(E)$  bars to be epoxy grouted in accordance with Art. 584 of the Standard Specifications. Cost included with Reinforcement Bars, Epoxy Coated.

APPROACH  
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
$a(E)$	16	#4	31'-0"	—
$a_1(E)$	30	#5	31'-0"	—
$b(E)$	24	#4	19'-8"	—
$b_1(E)$	72	#9	19'-9"	⌋
$d(E)$	21	#5	2'-3"	—
Concrete Superstructure			Cu. Yd.	18.8
Reinforcement Bars, Epoxy Coated			Pound	6500



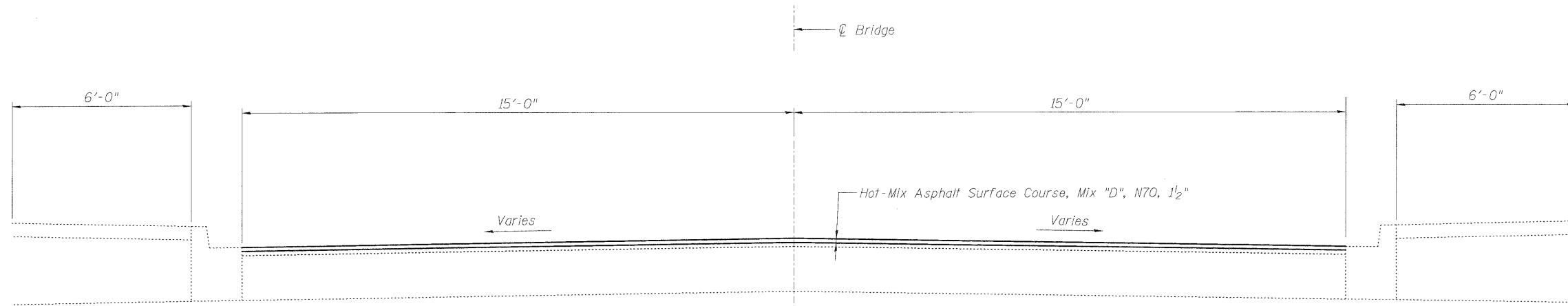
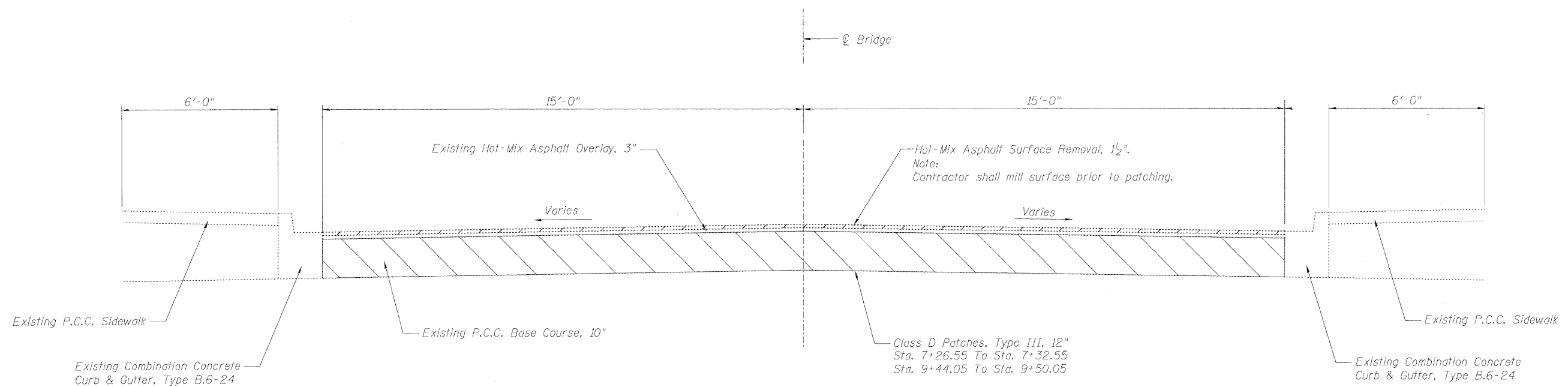
BRIDGE APPROACH SLAB DETAILS  
STRUCTURE NO. 045-0061

DESIGNED	BRT
CHECKED	BJM
DRAWN	JNH
CHECKED	BJM

**Wight**  
Wight & Company  
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SHEET NO. 12 13 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2503	39 EXT-VB-1	KANE	27	20
			CONTRACT NO. 60J48		
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
Resurfacing	
Hot-mix Asphalt Surface Course, Mix "D", N70 (IL-9.5mm)	4% @ 70 Gyr.
Patching	
Class D patches (HMA binder IL-19mm)	4% @ 70 Gyr.

The unit weight used to calculate all HMA surface mixture quantities is 112 lbs/sq yd/ in.

The "ac type" for polymerized hma mixes shall be "sbs/sbr pg 70-22" and for non-polymerized HMA the "ac type" shall be "pg 64-22" unless modified by district one special provisions. For "percent of rap" see district one special provisions.

Contractor shall mill surface prior to patching.

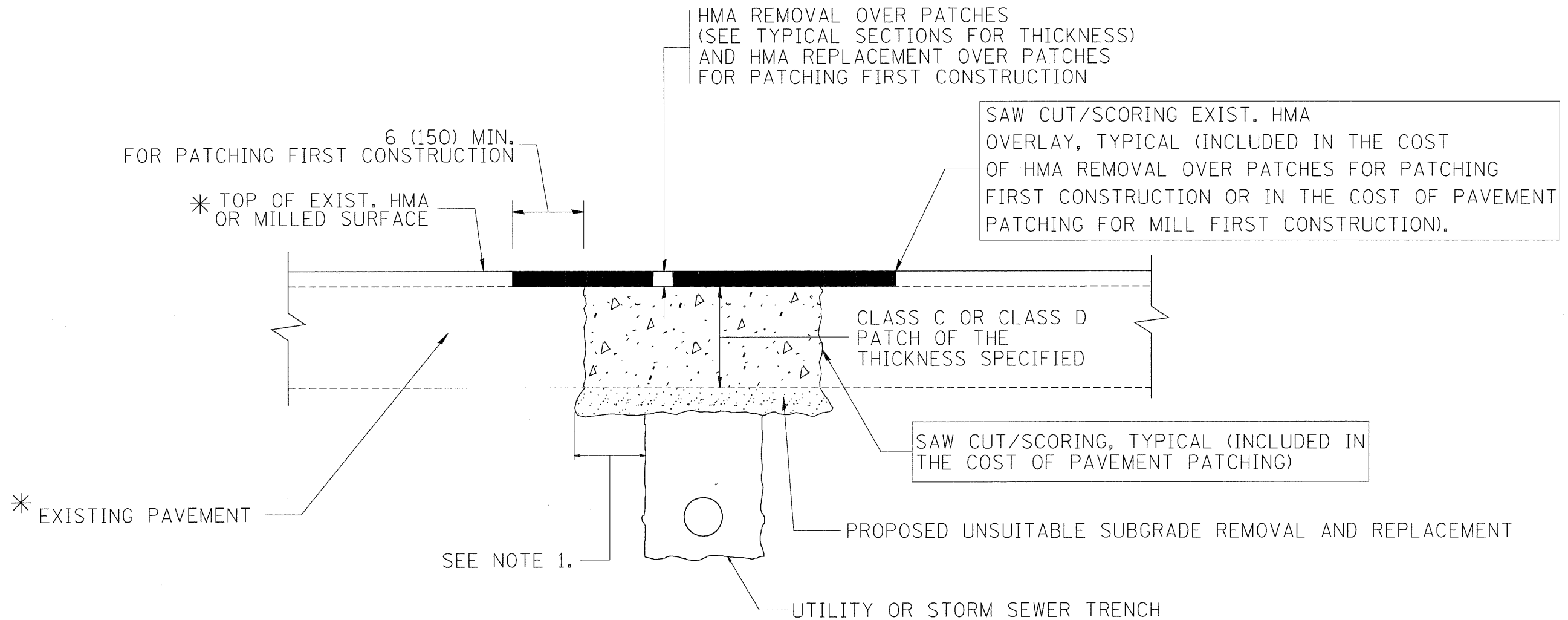
DESIGNED	BRT
CHECKED	BJM
DRAWN	JNH
CHECKED	BJM



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630.969.7000 630.969.7979 fax  
Design Firm Registration 184-000451

APPROACH PAVEMENT REPAIR  
STRUCTURE NO. 045-0061

SHEET NO. 13 13 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2503	39 EXT-VB-1	KANE	27	21
CONTRACT NO. 60J48					
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT					



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

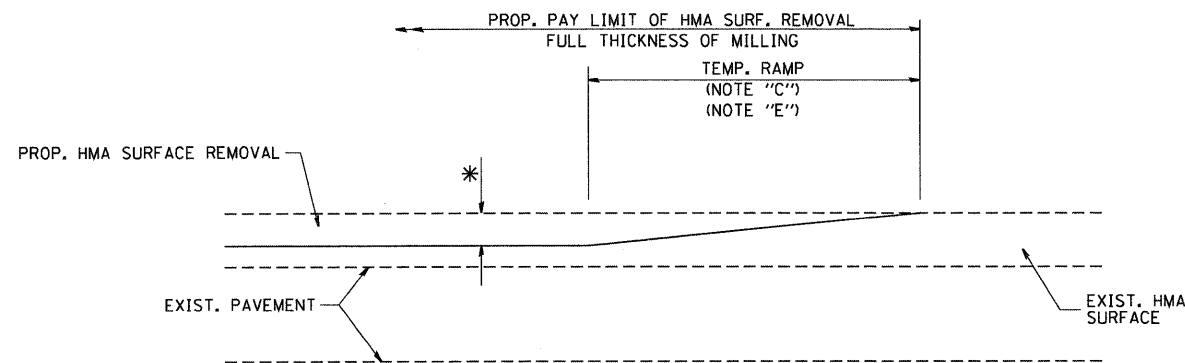
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - R. BORO 01-01-07
	PLLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

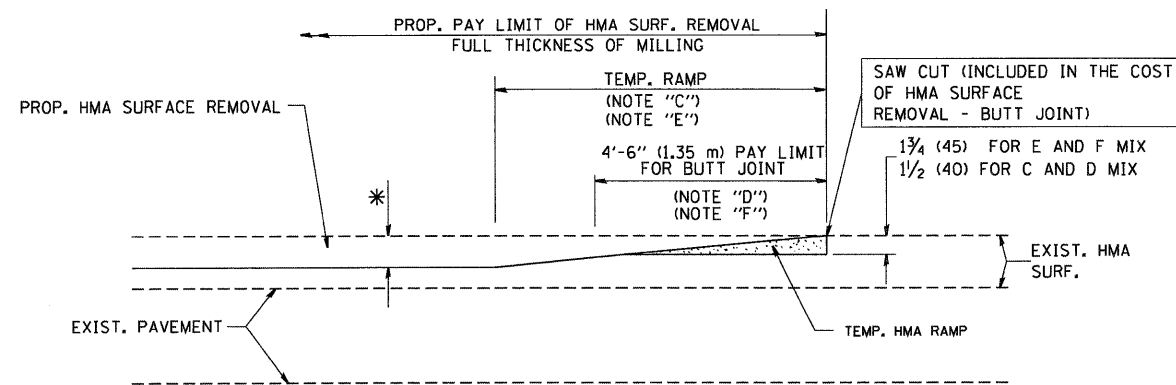
<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	39-EXT-VB-1	KANE	27	22
<b>BD400-04 (BD-22)</b>			CONTRACT NO. 60J48	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

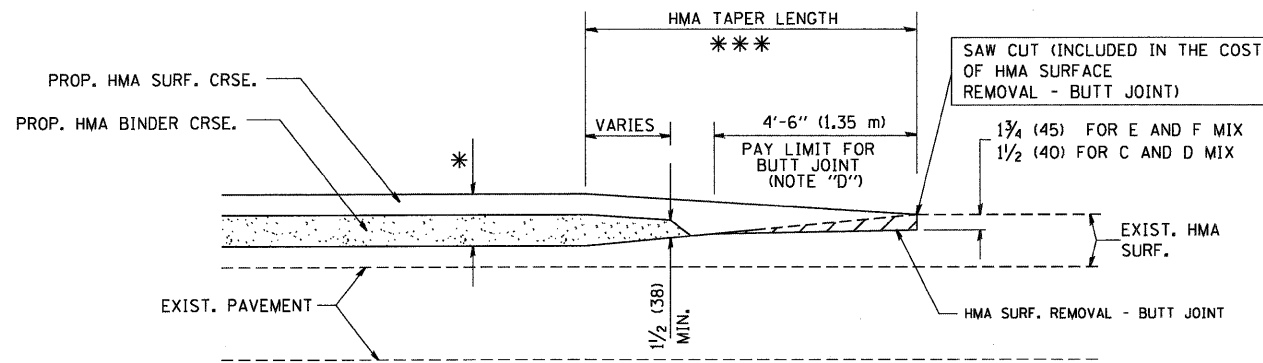
**OPTION 1**



HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

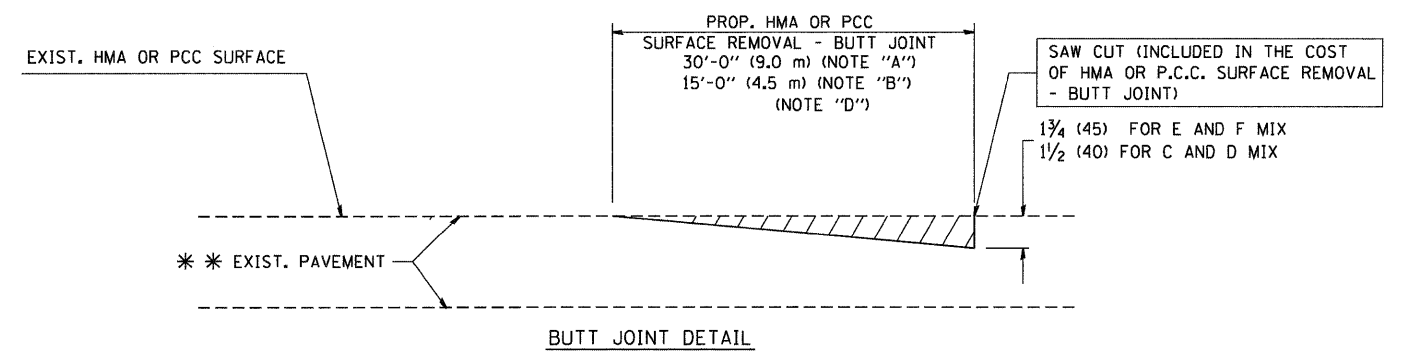
**OPTION 2**

**TYPICAL TEMPORARY RAMP**

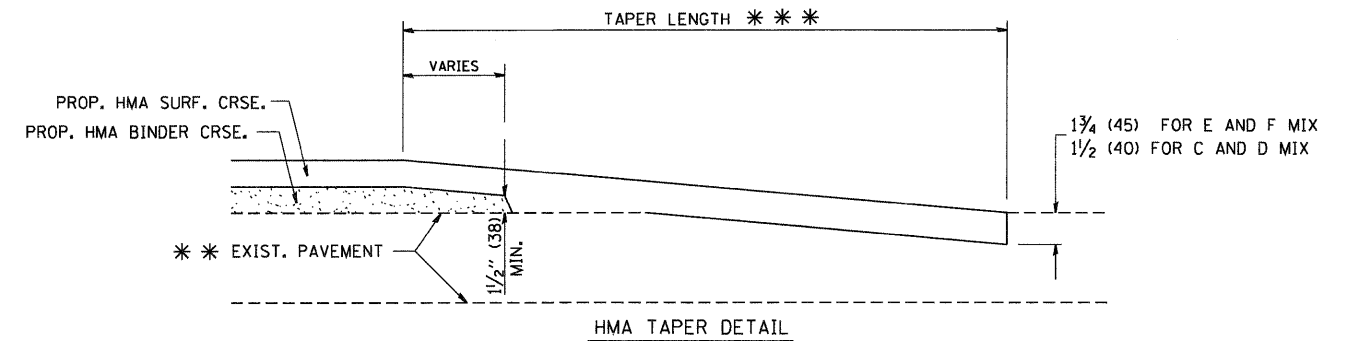


BUTT JOINT AND  
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



**TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

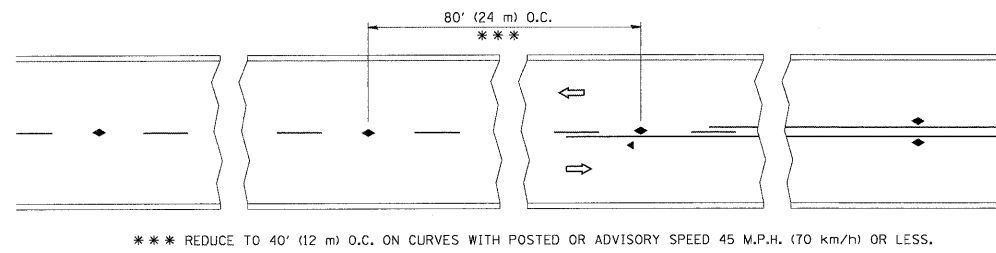
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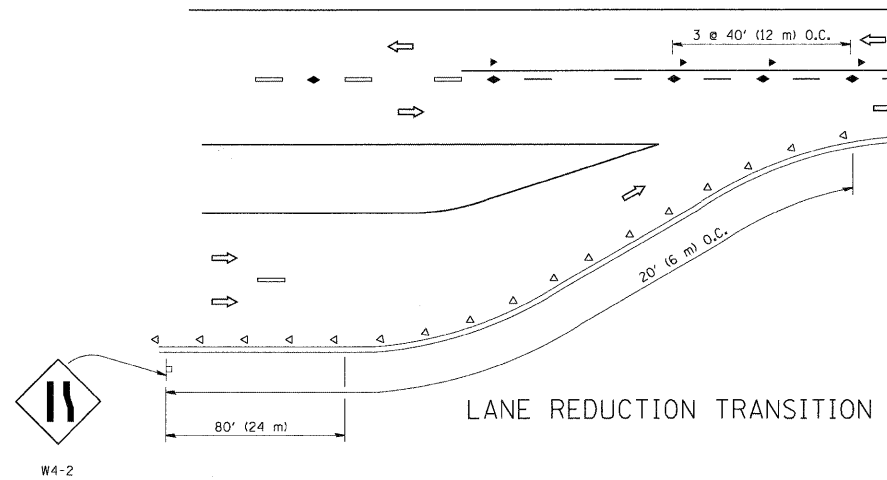
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

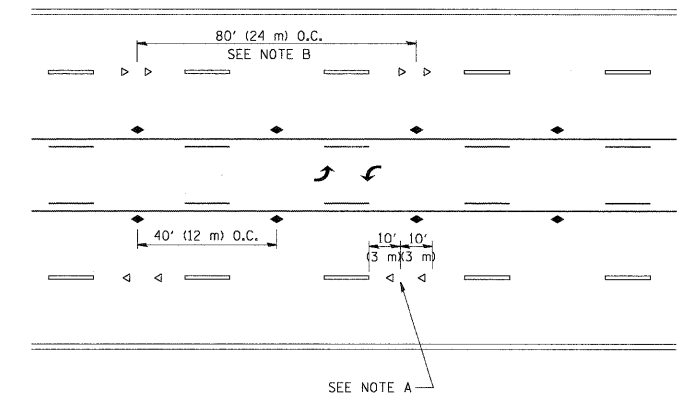
F.A.J. RTE. 2503	SECTION 39-EXT-VB-1	COUNTY KANE	TOTAL SHEETS 27	SHEET NO. 23
BD400-05 BD32			CONTRACT NO. 60J48	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



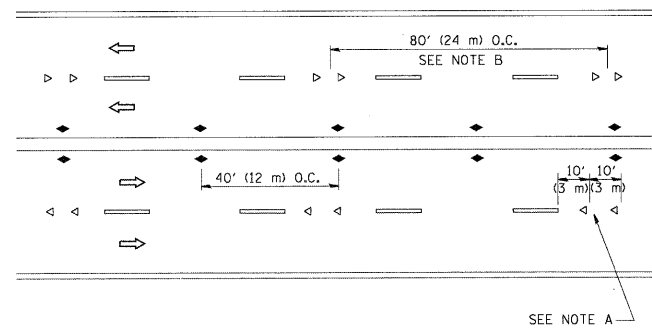
TWO-LANE/TWO-WAY



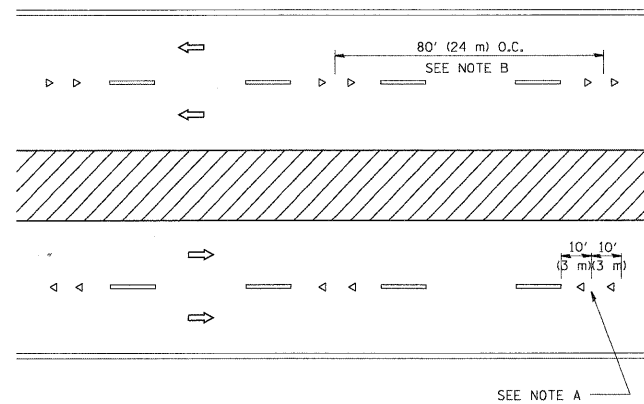
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

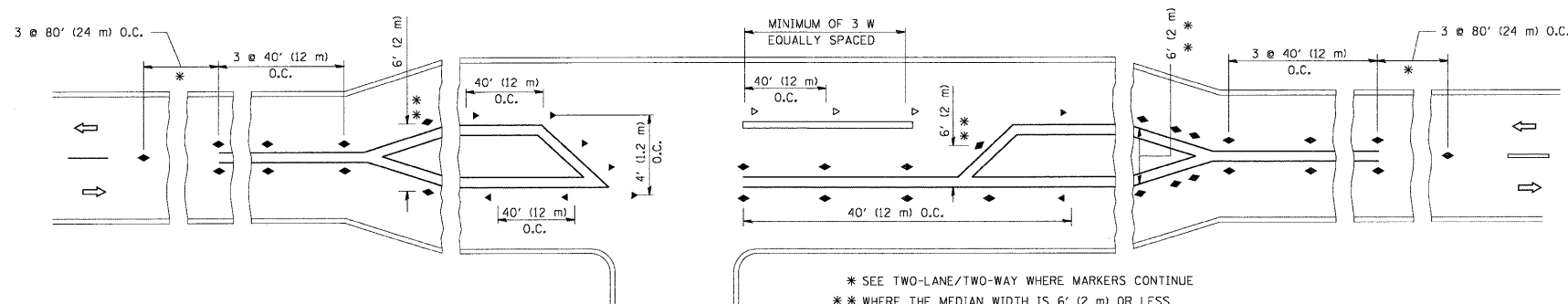
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 9/9/2009	DATE -	REVISED - C. JUCIUS 09-09-09

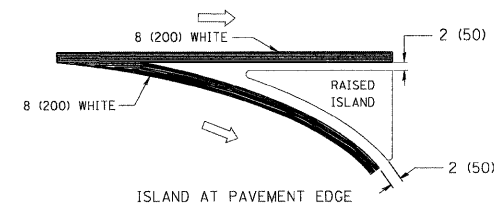
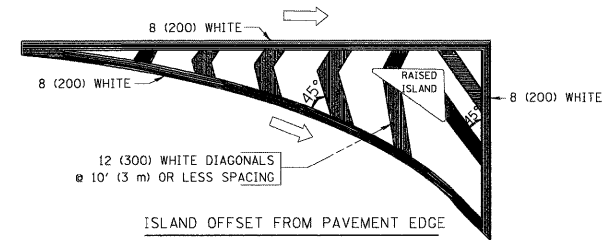
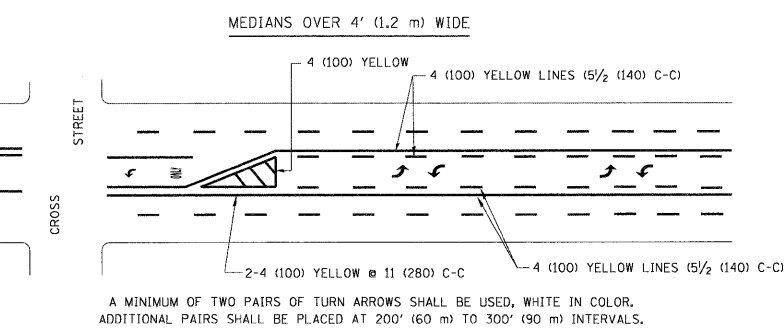
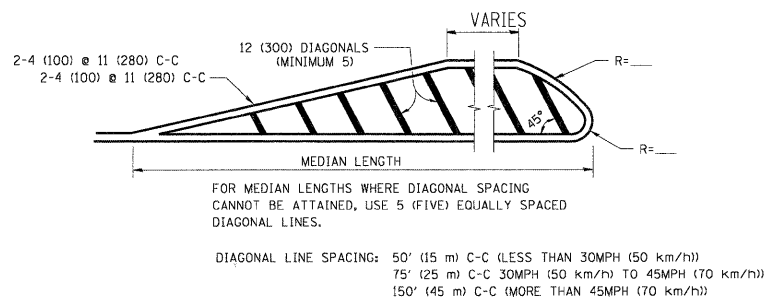
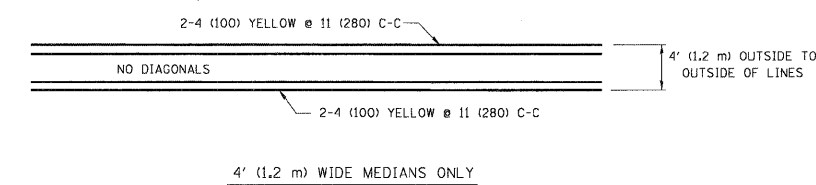
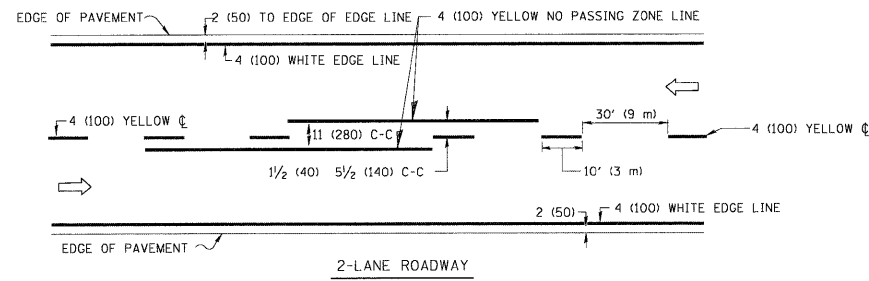
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS  
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

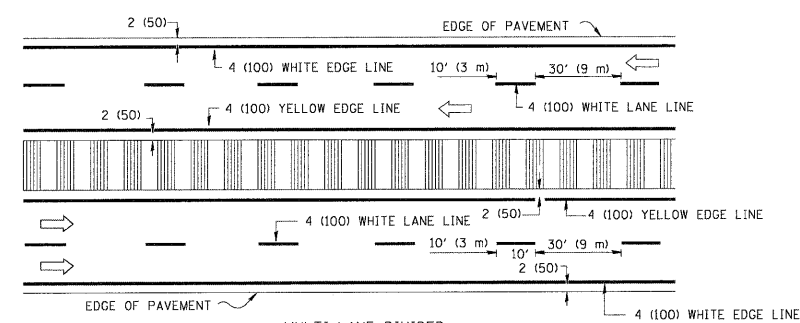
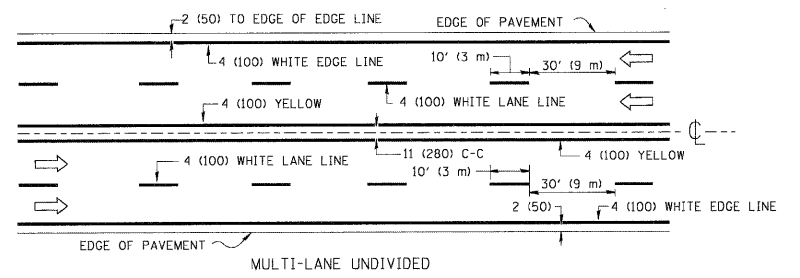
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	39-EXT-VB-1	KANE	27	24
TC-11		CONTRACT NO. 60J48		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



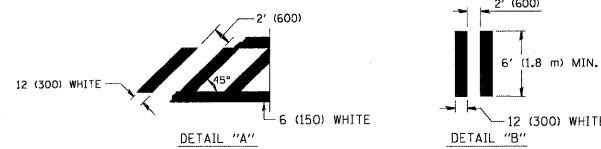
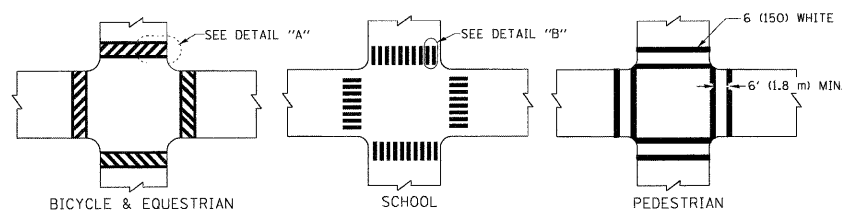


TYPICAL ISLAND MARKING

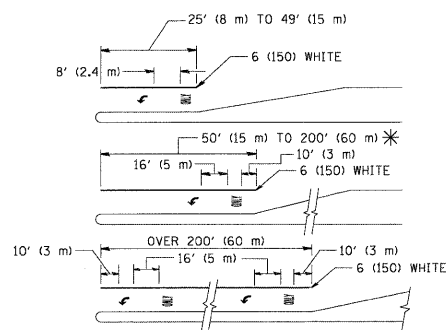


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES; FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drvakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
es:\pwwork\pwwork\drvakosgn\0108315\to3.dgn		DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50,000 "/ IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	39-EXT-VB-1	KANE	27	25
TC-13		CONTRACT NO. 60J48		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

**ROUTE MARKERS**

FOR U.S. ROUTES  
MI-40-2424

FOR ILLINOIS ROUTES  
MI-50-2424

R.R. UNMARKED ROUTES  
SPECIAL 24" x 18" VARIABLE  
4" BLACK LETTERS ON WHITE  
REFLECTIVE BACKGROUND

**ARROWS SIGNS**

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-1-2115

M6-3-2115

M6-3-2115

**CARDINAL DIRECTION & DETOUR SIGNS**

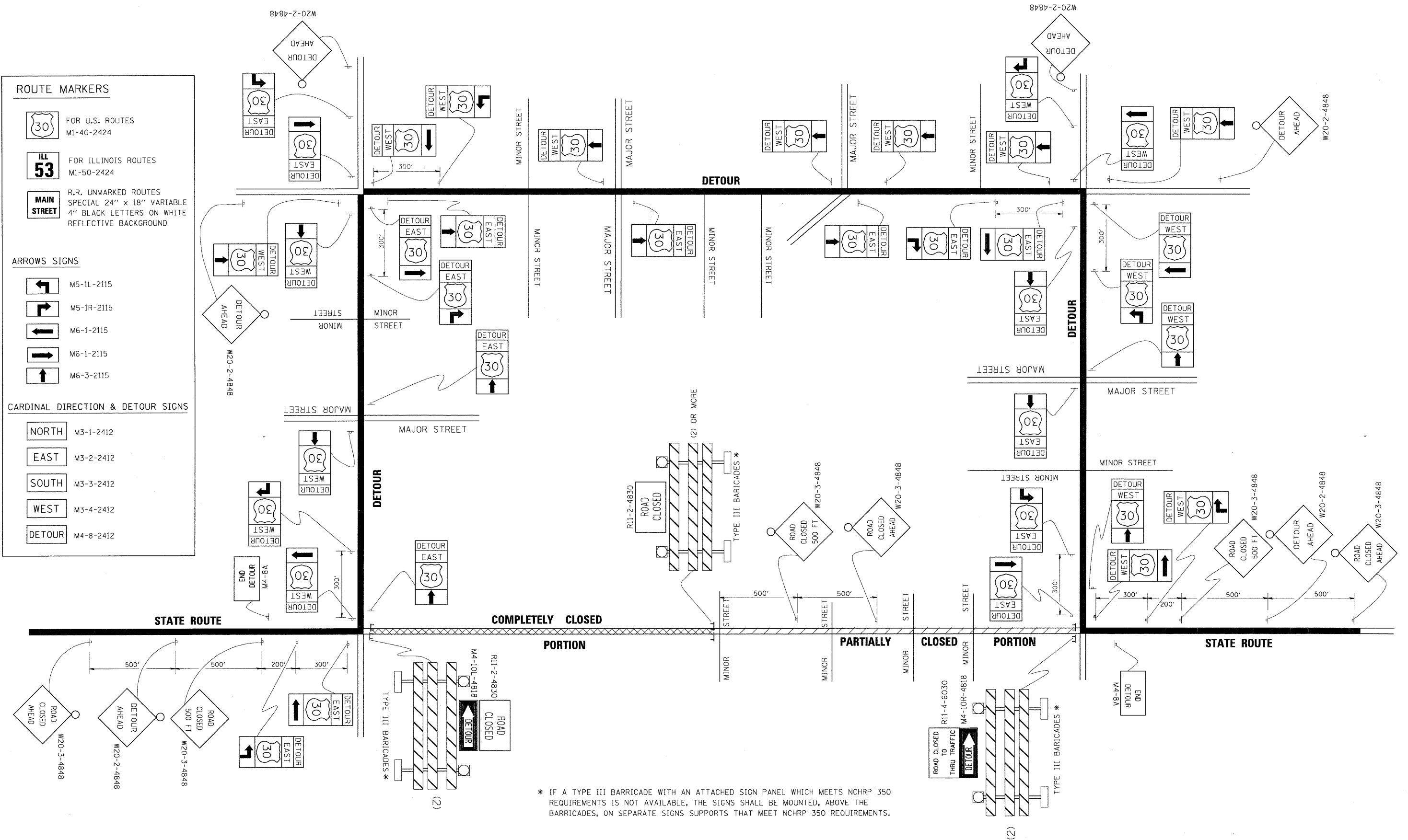
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-8-2412



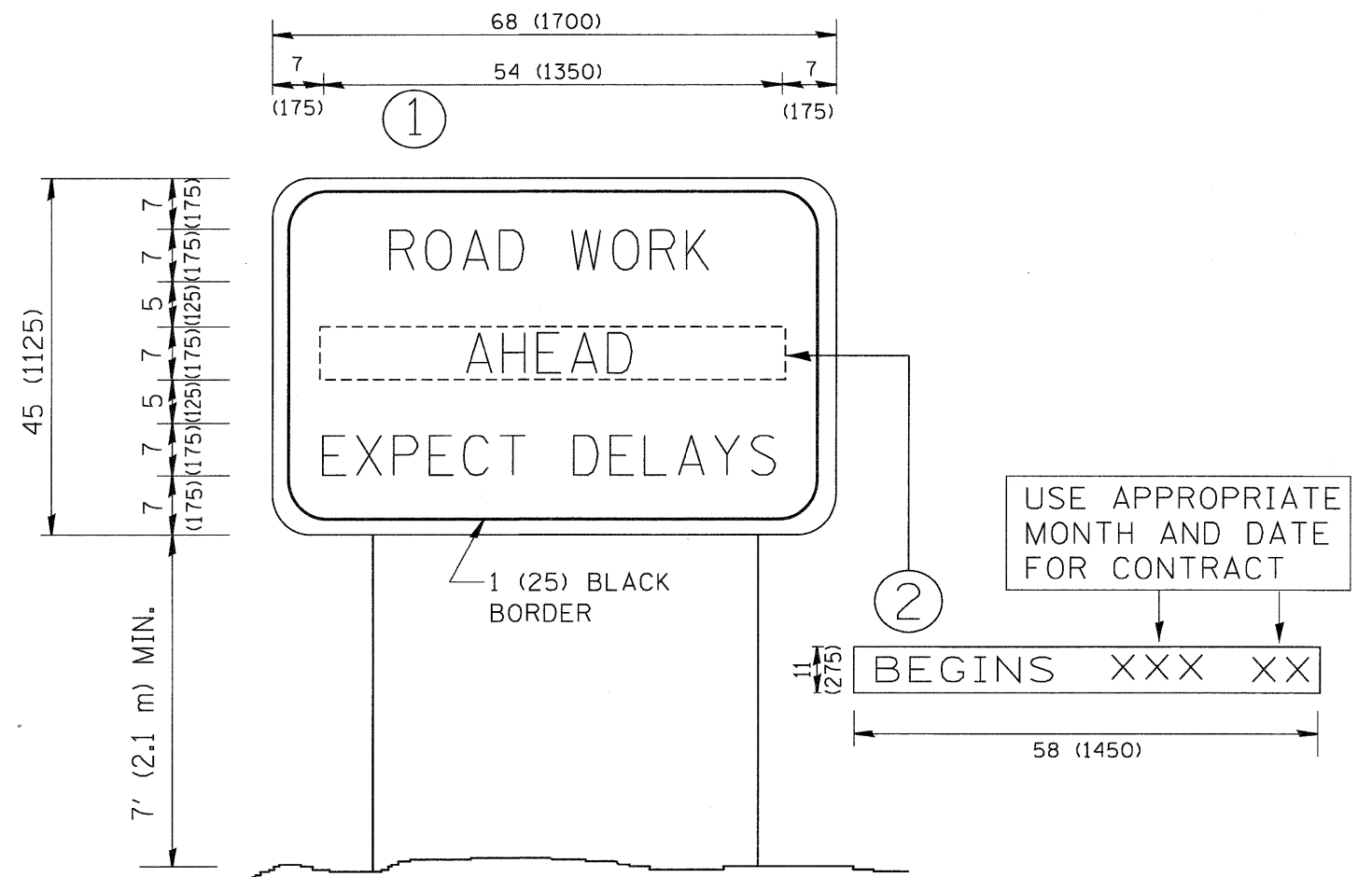
\* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - 10-18-02
c:\pwork\pwi001\DRIVAKOSGN\d2108315\21.dgn		DRAWN -	REVISED - R. BORO 09-14-09
	PLOT SCALE = 49.9999" / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/14/2009	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETOUR SIGNING FOR CLOSING STATE HIGHWAYS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE. -	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	39-EXT-VB-1	KANE	27	26
<b>TC-21</b>			CONTRACT NO. 60J48	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gaglianobt	DESIGNED - DRAWN -	REVISED - REVISED -
		REVISOR - CHECKED -	REVISOR - REVISOR -
	PLOT SCALE = 50,000 / IN.	DATE -	REVISOR -
	PLOT DATE = 1/4/2008		

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>ARTERIAL ROAD INFORMATION SIGN</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	39-EXT-VB-1	KANE	27	27
TC-22			CONTRACT NO. 60J48	
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				