

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	1
		ILLINOIS	CONTRACT NO. 62B47	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

**DESIGN DESIGNATION**  
MINOR ARTERIAL  
ADT 19,100 (2014)  
SPEED LIMIT 30 MPH

# PROPOSED HIGHWAY PLANS

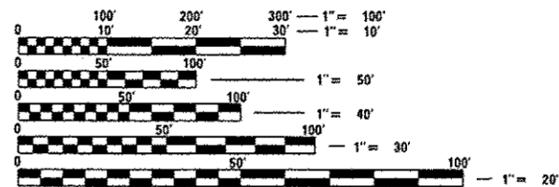
FAU ROUTE 3730: IL-1 (HALSTED ST.)  
SECTION K(B&C) 2-BR(15)  
OVER LITTLE CALUMET RIVER  
BRIDGE PAINTING & BRIDGE REPAIR  
COOK COUNTY  
ACSTP-3730(004)

IMPROVEMENT LOCATED IN  
THE CITY OF CHICAGO

C-91-047-16



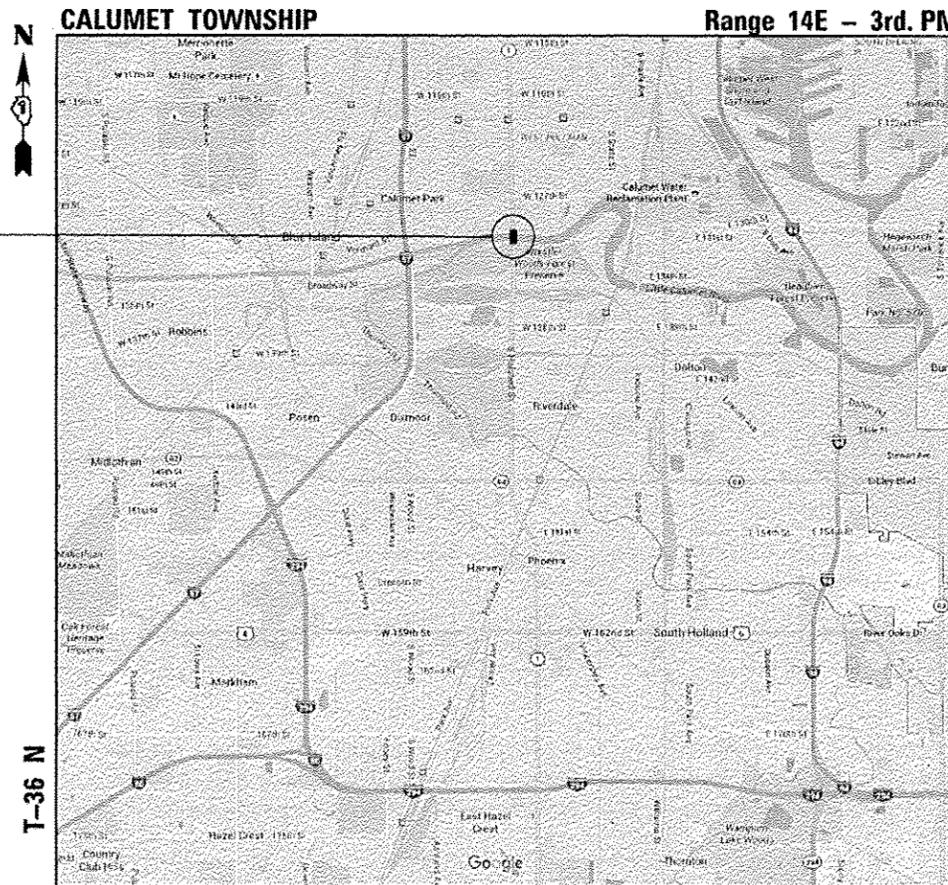
IMPROVEMENT LOCATION  
HALSTED STREET (IL-1)  
OVER LITTLE CALUMET RIVER  
(SN 016-0193)



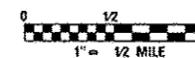
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N.  
CHICAGO UTILITY ALERT NETWORK  
1-312-744-7000

PROJECT ENGINEER: MS. RAGHAD ADEIS-DAHAN, P.E. (847) 705-518  
PROJECT MANAGER: MR. ISSAM RAYYAN, P.E. (847) 705-4178  
CONTRACT NO. 62B47



GROSS LENGTH = 420'-3" = 0.08 MILES  
NET LENGTH = 420'-3" = 0.08 MILES



3/23/16  
COLLINS ENGINEERS, INC.  
JASON M. SCHNEIDER  
NO. 81-7245  
EXPIRES 11-30-2016

3/23/16  
COLLINS ENGINEERS, INC.  
RYAN GALL  
NO. 062-064397  
EXPIRES 11-30-2017

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED 3-25 2016

*[Signature]* REGIONAL ENGINEER

*[Signature]* 2016  
ENGINEER OF DESIGN AND ENVIRONMENT

*[Signature]* 2016  
DIRECTOR OF PROGRAM DEVELOPMENT

**COLLINS ENGINEERS**  
123 N. WACKER DR., SUITE 900  
CHICAGO, IL 60606  
(312) 704-9300  
ILLINOIS PROFESSIONAL DESIGN FIRM  
LICENSE NO. 184-000993

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OF THE STATE OF ILLINOIS

**INDEX OF SHEETS**

- 1 TITLE SHEET
- 2 INDEX OF SHEETS, STATE STANDARDS, GENERAL NOTES AND PAINTING GENERAL NOTES
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- 4 DETOUR PLAN
- 5-24 EXISTING BRIDGE PLANS (SN. 099-0101)
- 25 TC21 - DETOUR SIGNING FOR CLOSING STATE HIGHWAYS
- 26 TC22 - ARTERIAL ROAD INFORMATION SIGN

**STATE STANDARDS**

- 701801- Sidewalk, Corner, or Crosswalk Closure
- 701901- Traffic Control Devices

**GENERAL NOTES**

1. BEFORE STARTING ANY EXCAVATION IN THE CITY OF CHICAGO, THE CONTRACTOR SHALL CALL "C.U.A.N" AT (312)744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES.
2. THE CONTRACTOR SHALL COORDINATE THE WORK WITH ALL AFFECTED UTILITIES AND THE CITY OF CHICAGO.
3. THE CONTRACTOR SHALL NOT BE ALLOWED TO SETUP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
4. PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION OR A CHANGE IN SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED UPON THE UNIT PRICE FOR THE WORK.
5. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 AT LEAST 72 HOURS IN ADVANCE OF BEGINNING WORK.
6. THE ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD ENGINEER, AT (708) 597-9800 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
7. EXISTING WATER MAINS ARE ATTACHED TO THE UNDERSIDE OF THE TRUSS SPAN ON THE EAST SIDE OF THE STRUCTURE. CARE SHALL BE TAKEN WHILE WORKING AROUND EXISTING WATER MAINS.
8. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THE PROJECT.
9. WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHTTIME OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJACENT RESIDENTIAL AREAS.
10. DO NOT SCALE THESE PLANS FOR CONSTRUCTION PURPOSES.
11. THE CONTRACTOR SHALL OBTAIN COAST GUARD APPROVAL FOR ANY WORK THAT MAY INTERFERE WITH NAVIGATIONAL OPERATIONS OF THE NAVIGABLE WATERS. A WORK PLAN SHALL BE PREPARED BY THE CONTRACTOR, REVIEWED AND APPROVED BY THE ENGINEER, AND SUBMITTED BY THE ENGINEER TO THE COAST GUARD AT THE ADDRESS LISTED BELOW FOR APPROVAL.  
  
BRIDGE ADMINISTRATOR  
US COAST GUARD  
EIGHTH COAST GUARD DISTRICT  
1222 SPRUCE STREET  
ST. LOUIS, MISSOURI 63103-2832  
(ERIC WASHBURN (314)269-2300 EXT. 2378)
12. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO ENSURE THAT NO DEBRIS WILL ENDANGER OR INTERFERE WITH RIVER TRAFFIC BENEATH THE BRIDGE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE APPROPRIATE PAY ITEM.
13. PRIOR TO THE START OF THE GUSSET PLATE RETROFITS, CONTRACTOR VEHICULAR TRAFFIC IS ALLOWED ACROSS THE BRIDGE. HOWEVER, MAXIMUM GROSS VEHICLE WEIGHT IS 12 TONS ACROSS THE BRIDGE AT ANY TIME. AFTER GUSSET PLATE RETROFITS HAVE COMMENCED, NO CONTRACTOR VEHICULAR TRAFFIC IS ALLOWED ON THE MAIN TRUSS SPAN UNTIL THE RETROFITS HAVE BEEN COMPLETED. AFTER GUSSET PLATE REPAIRS HAVE BEEN COMPLETED, LEGAL LOADS WILL BE ALLOWED ON THE BRIDGE.
14. PRIOR TO COMPLETION OF THE GUSSET PLATE RETROFITS, THE MAIN TRUSS SPAN SHALL NOT BE USED FOR CONSTRUCTION STAGING AND THE CONTRACTOR SHALL KEEP THE MAIN TRUSS SPAN CLEAR OF ALL MATERIALS, EQUIPMENT, AND PARKED VEHICLES.
15. ACCESS TO THE BRIDGE MAY BE OBTAINED THROUGH AN ACCESS HATCH IN THE SIDEWALK AT THE NORTHWEST AND SOUTHWEST CORNERS OF THE TRUSS SPAN. CONTRACTOR SHALL PROVIDE BARRIERS AROUND THESE ACCESS HATCHES, WHEN OPEN, TO PROTECT THE OPENING FROM PEDESTRIANS AND BICYCLISTS.
16. AN UNDERBRIDGE PLATFORM IS PRESENT ON THIS BRIDGE WITHIN THE TRUSS SPAN. THE PLATFORM WILL BE TURNED OVER TO THE CONTRACTOR AT THE BEGINNING OF THE WORK AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE AND REPAIR OF THE PLATFORM FOR THE DURATION OF THE WORK. THE ENGINEER SHALL INSPECT THE PLATFORM AT THE BEGINNING AND THE END OF THE WORK TO VERIFY IT IS BEING RETURNED TO THE DEPARTMENT IN SIMILAR CONDITION TO THE START OF THE WORK.
17. THE TOTAL WEIGHT ON THE UNDERBRIDGE PLATFORM SHALL BE LIMITED TO 750 POUNDS FOR PERSONNEL AND MATERIALS.
18. IF THE UNDERBRIDGE PLATFORM IS UTILIZED, THE CONTRACTOR SHALL NOT ENCROACH ON THE REQUIRED HORIZONTAL AND VERTICAL CLEARANCES AND SHALL HAVE RADIO CONTACT WITH THE COAST GUARD WHEN REPOSITIONING THE UNDERBRIDGE PLATFORM.
19. THE CONTRACTOR MUST NOTIFY ALL LOCAL MUNICIPALITIES WHEN BRIDGE IS CLOSED, SO THAT EMERGENCY SERVICES CAN BE REROUTED ACCORDINGLY.

**PAINTING GENERAL NOTES**

1. CLEANING AND PAINTING OF THE EXISTING STRUCTURAL STEEL SHALL BE AS SPECIFIED IN THE SPECIAL PROVISION "CLEANING AND PAINTING EXISTING STEEL STRUCTURES."
2. THE EXISTING STRUCTURAL STEEL COATING CONTAINS LEAD. THE CONTRACTOR SHALL TAKE APPROPRIATE PRECAUTIONS TO DEAL WITH THE PRESENCE OF LEAD ON THIS PROJECT.
3. THE CONTRACTOR SHALL SUBMIT CALCULATIONS AND DETAILS DEMONSTRATING THE STRUCTURAL INTEGRITY OF THE BRIDGE IS MAINTAINED UNDER THE ADDITIONAL IMPOSED LOADS OF THE CONTAINMENT SYSTEM. SEE SPECIAL PROVISIONS.
4. A MINIMUM OF FOUR (4) AIR MONITORS WILL BE REQUIRED TO MONITOR ABRASIVE BLASTING OPERATIONS. SEE SPECIAL PROVISION FOR "CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUE."
5. ALL ITEMS (SUCH AS, BUT NOT LIMITED TO, CONDUITS, BRACKETS, DECK DRAINS) ATTACHED TO THE STRUCTURE WITHIN THE LIMITS OF THE CLEANING AND PAINTING SHALL ALSO BE CLEANED AND PAINTED WITH THE EXCEPTION OF WATER MAINS, SIGNS, AND NAVIGATION LIGHTS. SIGNS SHALL BE REMOVED DURING PAINTING IN ORDER TO ADEQUATELY PAINT THE STEEL AND SHALL BE REPLACED UPON COMPLETION. THE EXISTING WATER MAIN AND NAVIGATION LIGHTS SHALL BE PROTECTED DURING PAINTING.
6. THE HANDRAILS AND STEEL BRIDGE RAILS ARE GALVANIZED AND SHALL NOT BE PAINTED. HANDRAILS AND BRIDGE RAILS WITHIN THE LIMITS OF THE CLEANING AND PAINTING SHALL BE PROTECTED DURING PAINTING.
7. UNLESS NOTED OTHERWISE, THE AREAS DESIGNATED TO BE CLEANED AND PAINTED SHALL BE CLEANED PER NEAR WHITE BLAST CLEANING SSPC-SP10 AND SHALL BE PAINTED ACCORDING TO THE REQUIREMENTS OF PAINT SYSTEM 1 OZE/U. THE COLOR OF THE FINAL FINISH COAT FOR ALL STEEL SURFACES SHALL BE BLUE MUNSELL NO. 10B 3/6.
8. WITHIN APPROACH SPANS, ALL EXISTING STEEL PORTIONS OF THE ELASTOMERIC BEARINGS AT THE NORTH AND SOUTH ABUTMENTS AND FIXED BEARINGS AT PIERS 1 AND 2 SHALL BE CLEANED AND PAINTED.
9. ALL EXISTING STEEL WITHIN THE TRUSS SPAN ABOVE AND BELOW THE DECK, EXCEPT AS NOTED ON THE PLANS, SHALL BE CLEANED AND PAINTED. TRUSS SPAN BEARING SHALL BE CLEANED AND PAINTED.
10. CLEANING AND PAINTING WITHIN AND ADJACENT TO THE PROPOSED STRUCTURAL STEEL REPAIRS SHALL BE COMPLETED PRIOR TO PERFORMING THE STRUCTURAL STEEL REPAIRS.



USER NAME : rgo11	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE : 2.0000' / 1" =	CHECKED -	REVISED -
PLOT DATE : 4/18/2016	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS, GENERAL NOTES  
AND GENERAL NOTES FOR PAINT  
STRUCTURE NUMBER: 016-0193

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	2
SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 62B47	
ILLINOIS FED. AID PROJECT				

REV.

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	URBAN
				CONSTR. CODE 80% FED 20% STATE BRIDGE 0014
67000400	ENGINEER'S FILED OFFICE, TYPE A	CAL MO	9	9
67100100	MOBILIZATION	L SUM	1	1
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	24	24
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1
Z0001905	STRUCTURAL STEEL REPAIR	POUND	2360	2360
Z0007101	CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES NO 1	L SUM	1	1
Z0010501	CLEANING AND PAINTING STEEL BRIDGE NO. 1	L SUM	1	1
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	55	55
Z0073100	TEMPORARY SHORING	EACH	4	4
Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	24	24

10

**COLLINS ENGINEERS**

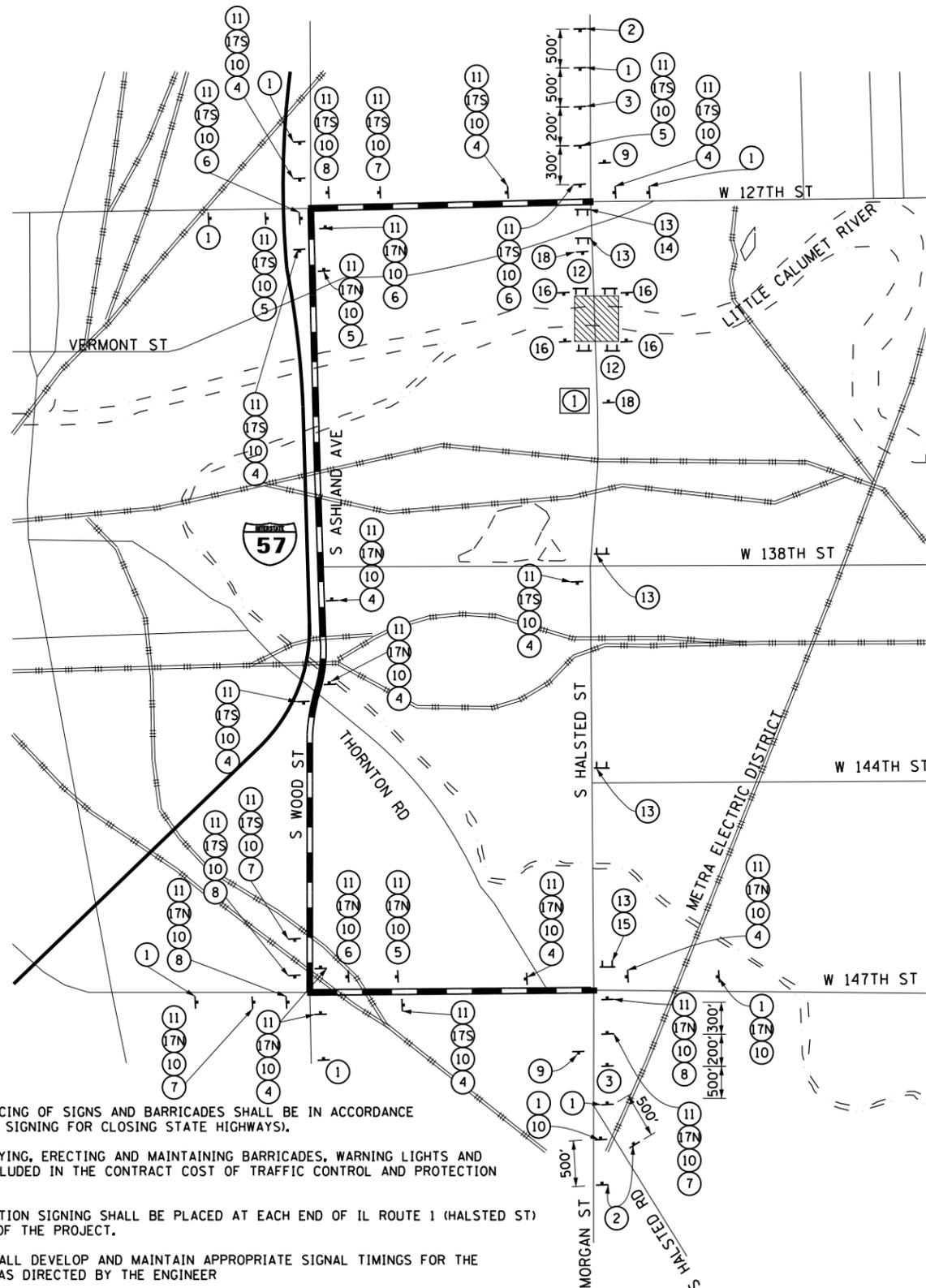
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	DRAWN -	REVISED -
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PLOT DATE = 3/24/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES  
STRUCTURE NUMBER: 016-0193**

SCALE: SHEET OF SHEETS STA. TO STA.

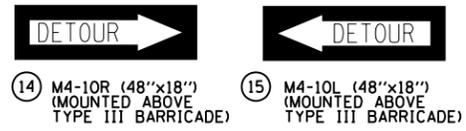
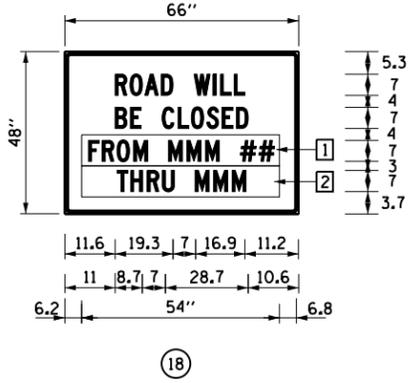
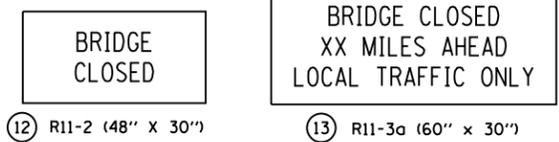
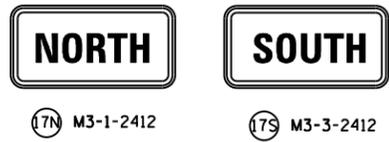
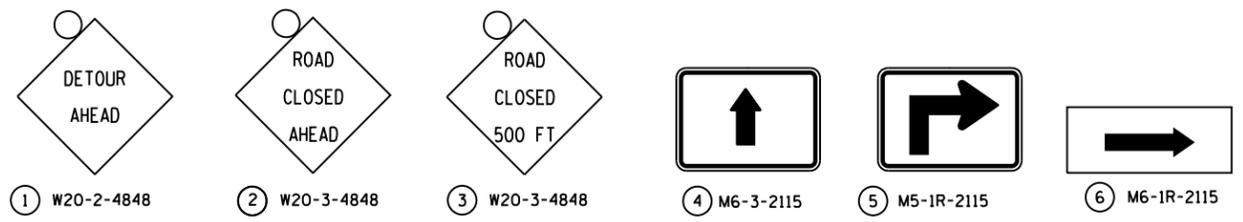
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	3
			CONTRACT NO. 62847	
ILLINOIS FED. AID PROJECT				



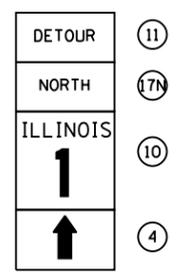
**NOTES:**

1. PLACEMENT AND SPACING OF SIGNS AND BARRICADES SHALL BE IN ACCORDANCE WITH TC-21 (DETOUR SIGNING FOR CLOSING STATE HIGHWAYS).
2. THE COST OF SUPPLYING, ERECTING AND MAINTAINING BARRICADES, WARNING LIGHTS AND SIGNS SHALL BE INCLUDED IN THE CONTRACT COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL).
3. TEMPORARY INFORMATION SIGNING SHALL BE PLACED AT EACH END OF IL ROUTE 1 (HALSTED ST) FOR THE DURATION OF THE PROJECT.
4. THE CONTRACTOR SHALL DEVELOP AND MAINTAIN APPROPRIATE SIGNAL TIMINGS FOR THE INTERSECTIONS, OR AS DIRECTED BY THE ENGINEER

- A) IL ROUTE 83 / IL ROUTE 1
- B) IL ROUTE 83 / BROADWAY ST
- C) IL ROUTE 83 / LOOMIS AVE
- D) IL ROUTE 83 / WOOD ST
- E) ASHLAND AVE / THORNTON RD
- F) ASHLAND AVE / W 138TH ST
- G) ASHLAND AVE / BROADWAY ST
- H) ASHLAND AVE / VERMONT ST
- I) ASHLAND AVE / W 127TH ST
- J) W 127TH / BISHOP ST
- K) W 127TH / S THROOP ST
- L) W 127TH / IL ROUTE 1



**TYPICAL DETOUR SIGN ASSEMBLY**



**LEGEND:**

- DETOUR ROUTE
- CONSTRUCTION ZONE
- TYPE III BARRICADE WITH 2 FLASHING LIGHTS
- AMBER FLASHING LIGHT

ARTERIAL ROAD INFORMATION SIGN FOR ROADS TO BE FULLY CLOSED AND DETOURED  
6'R, 1.3" BORDER, BLACK ON ORANGE

- ARTERIAL ROAD INFORMATION SIGN NOTES:**
1. SIGN SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING. ONE SIGN ASSEMBLY EQUALS 27.3 SQ FT.
  2. OVERLAY PANELS SHALL BE "HIGHWAY C" FONT
  3. OVERLAY PANEL 1 TO CONTAIN STARTING DATE OF FULL CLOSURE AND DETOUR IMPLEMENTATION
  4. OVERLAY PANEL 2 TO CONTAIN ENDING MONTH OF FULL CLOSURE AND DETOUR. OMIT THE DATE ON PANEL; MONTH ONLY.
  5. ERECT SIGN ASSEMBLY (POST-MOUNTED) WITH PANELS 1 AND 2 IN PLACE ON ROAD TO BE CLOSED IN EACH DIRECTION NEAR POINT OF CLOSURE OR WITHIN SECTION TO BE FULLY CLOSED TWO (2) WEEKS PRIOR TO START DATE OF FULL CLOSURE. REMOVE ASSEMBLY AFTER CLOSURE.

FILE NAME = I:\8274 - IDOT\_PTB\_168 - 07\_Verious\8274\_21 - Halsted over Cal Sag\CADD\CADD\_Sheets\0162847\_004-DeTour.dgn



USER NAME = rge11	DESIGNED - ZT	REVISED -
PLOT SCALE = 1/8" = 1'-0"	DRAWN - ZT	REVISED -
PLOT DATE = 4/18/2016	CHECKED - RAG	REVISED -
	DATE - MARCH, 2016	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>IL 1 (HALSTED ST.) OVER LITTLE CALUMET RIVER DETOUR PLAN</b>				
SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.

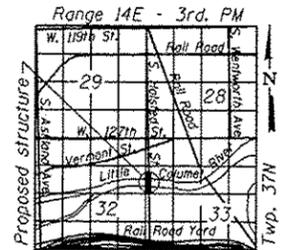
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	4
CONTRACT NO. 62B47				
ILLINOIS FED. AID PROJECT				

Bench Mark: "X" cut on west bolt on fire hydrant, northwest corner of Halsted Street and 129th Pl., Elevation 601.53.

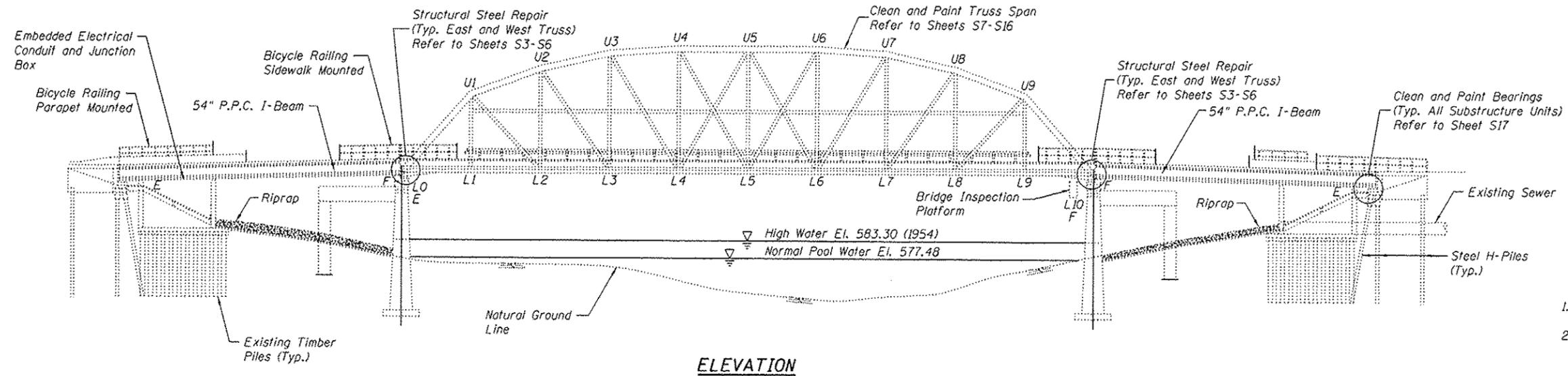
Existing Structure: The existing Structure consists of a main channel through truss span and a PPC I-Beam approach span at each end of the truss span. The original Structure was built in 1931 as Section Number K-B-2 and reconstructed in 1996.

An assigned detour route will be utilized to maintain vehicular traffic, but the channel will remain open to river traffic.

No Salvage.



LOCATION SKETCH



ELEVATION

**SCOPE OF WORK**

1. Structural steel repairs.
2. Cleaning and painting.

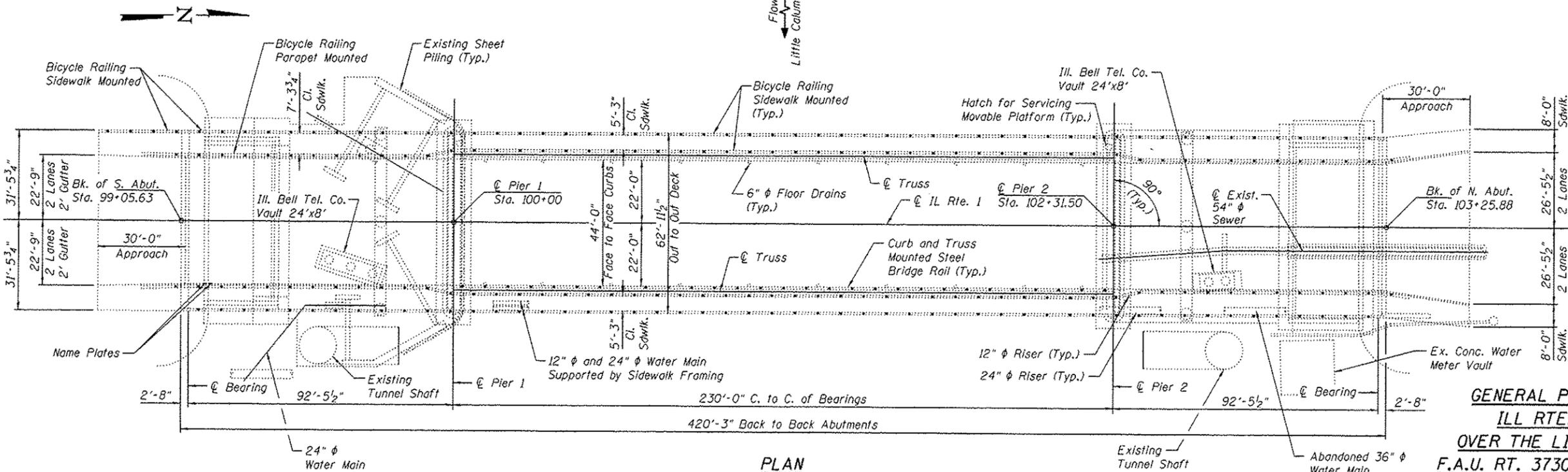
**DESIGN SPECIFICATIONS**

2012 AASHTO LRFD Bridge Design Specifications, 6th Edition

**LOADING HS20-44**

**DESIGN STRESSES**

Fy = 50,000 psi (Retrofit Plates)  
Fy = 36,000 psi (Fill Plates)



PLAN



COLLINS ENGINEERS, INC.  
JASON M. SCHNEIDER  
NO. 81-7245  
EXPIRES 11-30-2016

**GENERAL PLAN AND ELEVATION  
ILL RTE. 1 (HALSTED ST.)  
OVER THE LITTLE CALUMET RIVER  
F.A.U. RT. 3730 - SEC. K(B&C) 2-BR(15)  
COOK COUNTY  
STATION 101+15.80  
STRUCTURE NO. 016-0193**

**COLLINS ENGINEERS**

USER NAME	DESIGNED	REVISOR
PLOT SCALE	CHECKED	REVISION
PLOT DATE	DRAWN	REVISION
	CHECKED	REVISION

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN AND ELEVATION  
STRUCTURE NO. 016-0193**

SHEET NO. 51 OF 520 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	5

CONTRACT NO. 62B47

ILLINOIS FED. AID PROJECT

**Index of Sheets**

- S1 General Plan and Elevation
- S2 Index of Sheets, Repair Notes/Procedures, and Bill of Materials
- S3 Outside Gusset Plate Repair Details I
- S4 Outside Gusset Plate Repair Details II
- S5 Inside Gusset Plate Repair Details I
- S6 Inside Gusset Plate Repair Details II
- S7 Painting Plan, Elevation and Section
- S8 Truss Details I
- S9 Truss Details II
- S10 Truss Details III
- S11 Steel Bridge Rail (Special)
- S12 Bicycle Railing (Special)
- S13 Previous Deck Framing Repairs
- S14 Previous Truss Repairs
- S15 Expansion Bearing Details I
- S16 Expansion Bearing Details II
- S17 Bearing Details - Abutments
- S18 12" Water Main Support
- S19 24" Water Main Supports
- S20 Truss Span Platform

**STRUCTURAL STEEL REPAIR NOTES**

1. Gusset Plate L0 and L10 Load Rating (Load Factor Rating Method)  
Current Inv. 0.26 Retrofitted Inv. 1.13  
Current Op. 0.43 Retrofitted Op. 1.88
2. The retrofit and fill plates shown on Sheets S3 through S6 are based on the field measurements of one repair location. It is the nature of riveted steel of this vintage to have routine variations. Similarly, previous repairs may have modified original conditions such as the floor beam connection angles at L0 (South), which will require changes to the dimensions and or fill plate thicknesses. Retrofit plates may require minor field adjustment to fit actual as-built conditions. The Contractor shall field verify existing dimensions, rivet/bolt locations, and details affecting the proposed work and Notify Engineer of any discrepancies or conflicts with the dimensions shown on Sheets S3 through S6. Necessary approved adjustments shall be made prior to ordering of materials. Such variations or adjustments shall not be cause for additional compensation and cost shall be included in STRUCTURAL STEEL REPAIR.
3. Cost of rivet removal, trimming, reaming of bolt holes, and installation of all plates and bolts necessary to complete the work as shown on the plans shall be included in STRUCTURAL STEEL REPAIR.
4. Fasteners shall be galvanized, high strength AASHTO M 164 (ASTM A 325) bolts with threads excluded from the shear planes. Bolts having a diameter of 7/8 inch shall be used in existing rivet holes and in new 15/16 inch diameter open holes. Field verify existing rivet hole diameter after rivet removal. Holes that will not accommodate a new 7/8 inch diameter bolt shall be reamed to 15/16 inch diameter. Edge distances between center of the bolt hole and the edge of the new plates shall be 1-1/2 inches minimum, unless noted otherwise.
5. All steel retrofit plate material shall conform to AASHTO M270 Grade 50 (ASTM A709 Grade 50). Steel fill plates may be Grade 36 (ASTM A36).
6. All structural steel plate and shapes shall meet Zone 2 Charpy V-notch toughness requirements of 25 ft-lbs. at 40 deg. F.
7. All fabricated steel to be hot-dipped galvanized in conformance with AASHTO M111 (ASTM A123) and ASTM A385. All high-strength bolts, nuts, and washers shall be galvanized in accordance with AASHTO M232 (ASTM A153).
8. One outside gusset plate repair and one inside gusset plate repair are required per location as shown on Sheets S3 through S6. Four such locations (L0-E, 10-E, L0-W, and L10-W) require these repairs.
9. Fill plates are shown at a reduced scale to show position on sheets S3 and S5. Actual fill plate edges shall be tight against each other, the existing plates, and existing members, and shall extend to the edges of repair plate.
10. It is intended that the new retrofit plate bear flush on the web cover plate of the existing diagonal member.
11. When retrofitting the inside gusset plate, shoring shall be used to support the floor beam end, as shown on Sheet S5. The shoring shall be supported on the existing concrete pier and be capable of supporting the following service loads: DL = 60 kips. The shoring shall be positioned directly beneath the floor beam stiffeners. See Special Provisions.
12. Should the Contractor elect to disconnect members for his/her convenience, the Contractor shall submit calculations demonstrating stability of the structure during all stages of the work, signed and sealed by a Licensed Illinois Structural Engineer.
13. Contractor shall complete the inside gusset plate retrofit before beginning the outside gusset plate retrofit. Only one gusset plate on each truss shall be worked on at any time.
14. Contractor shall remove rivets and replace with high-strength bolts one-at-a-time. Flame-cutting of rivets is not permitted. Newly installed bolts shall have the bolt heads located between the two gusset plates to facilitate installation of the retrofit and fill plates. Nuts and washers shall be installed snug tight on all new bolts until the installation of the new fill plates or retrofit plate, unless noted otherwise.
15. Trimming of end diagonal flange angle, on member L0-U1 or U9-L10, to accommodate new retrofit plate shall occur by saw cutting or grinding. Flame-cutting is not permitted. All cut edges shall be smoothed to a surface roughness average of 250 micro-inches.
16. Upon completion of retrofits and coating touch-up, joint sealant shall be installed along perimeter of plys between steel plate as designated on the drawings and as directed by the Engineer. Cost for sealant is included in STRUCTURAL STEEL REPAIR. Estimated length of sealant is 51 feet.
17. Sealant shall be Dow Corning 795 Silicone Building Sealant or approved equal with color to match new coating.
18. Costs incurred for repainting or touch-up to address damaged coating (existing, new, or galvanizing) shall be included in STRUCTURAL STEEL REPAIR.

**PROPOSED STRUCTURAL STEEL REPAIR PROCEDURE**

1. Trim existing top flange angle leg on diagonal member, as shown on Sheets S3 and S5.
2. Grind area of bottom flange angle leg on diagonal member flush with the web plate, as shown on Sheets S3 and S5.
3. Clean and paint interior surfaces and isolated locations on exterior surfaces at repair locations (L0-E, L10-E, L0-W, and L10-W).
4. Wash remaining outside gusset surfaces.
5. Verify retrofit plate dimensions.
6. Erect approved temporary shoring below floor beam stiffeners in work area, as shown on Sheet S5.
7. Remove rivets and install new permanent bolts (fully pretensioned) on the inside gusset plate at the four locations, as shown on Sheet S5.
8. Remove rivets and install new bolts one-by-one in the outstanding leg of the floor beam connection angle, snug tight if required for clearances, as shown on Sheet S5.
9. Remove remaining rivets on inside gusset plate one-by-one and replace with new high strength bolts, snug tight.
10. Field drill holes in new retrofit and fill plates to match existing holes in the gusset plate.
11. Clean all surfaces of all cutting oils and debris.
12. Touch up galvanized coating in the retrofit plate and fill plates.
13. With all bolts installed in the gusset plate, remove the nuts/washers and install new fill and retrofit plates, do not remove nuts from bolts installed during Step 7.
14. Reinstall all nuts and washers and pretension all bolts in the retrofit plate and floor beam connection angles.
15. Install outside gusset plate retrofits following Steps 7 through 14, refer to Sheet S3.
16. Remove temporary shoring.
17. Touch-up damage to painted and galvanized coatings.

**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Cleaning and Painting Steel Bridge No. 1	L. Sum	1
Containment and Disposal of Lead Paint Cleaning Residues No 1	L. Sum	1
Structural Steel Repair	Pounds	2,360
Temporary Shoring	Each	4



USER NAME =	DESIGNED -	REVISED
	CHECKED -	REVISED
PLOT SCALE =	DRAWN -	REVISED
PLOT DATE =	CHECKED -	REVISED

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

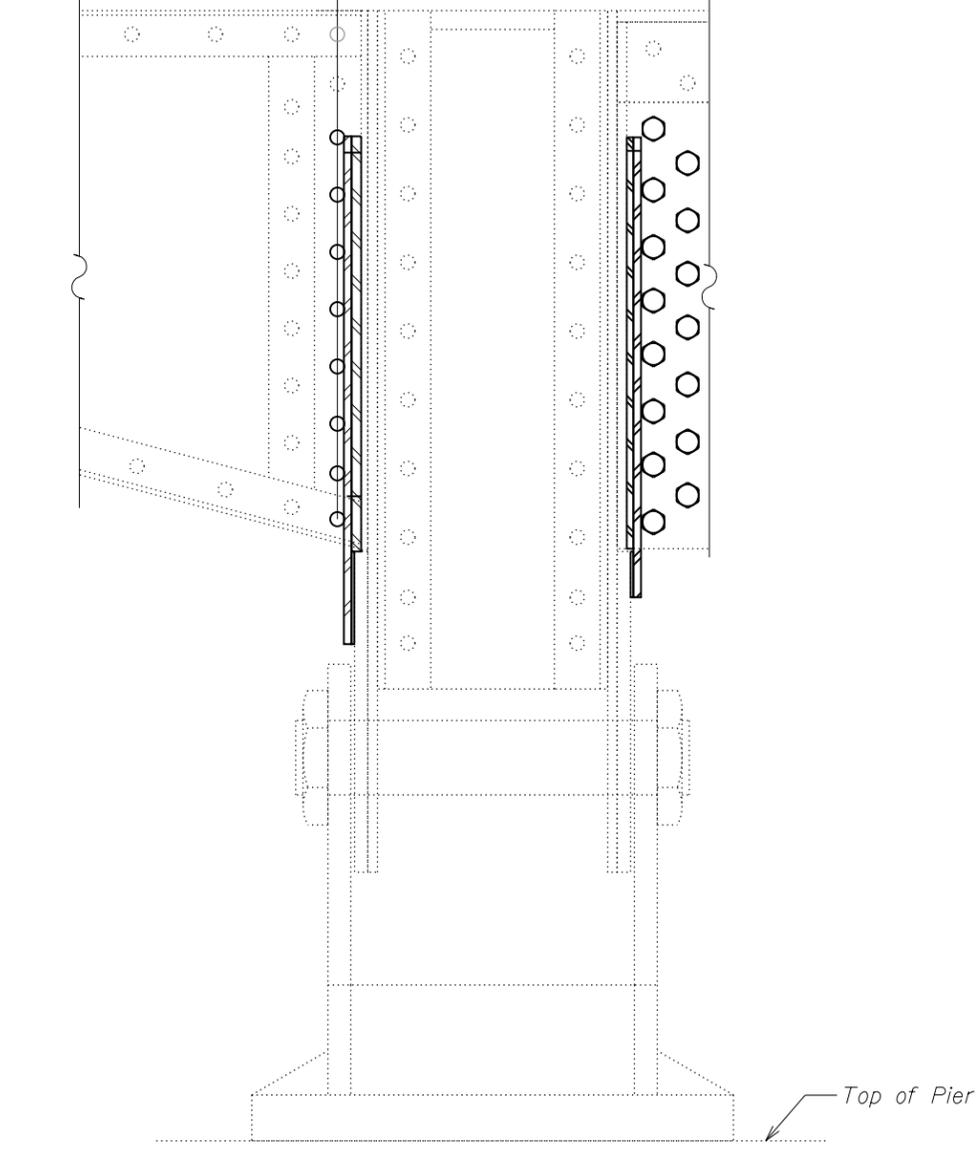
**INDEX OF SHEETS, REPAIR NOTES/PROCEDURES, AND BILL OF MATERIALS  
STRUCTURE NUMBER: 016-0193**

SHEET NO. S2 OF S20 SHEETS

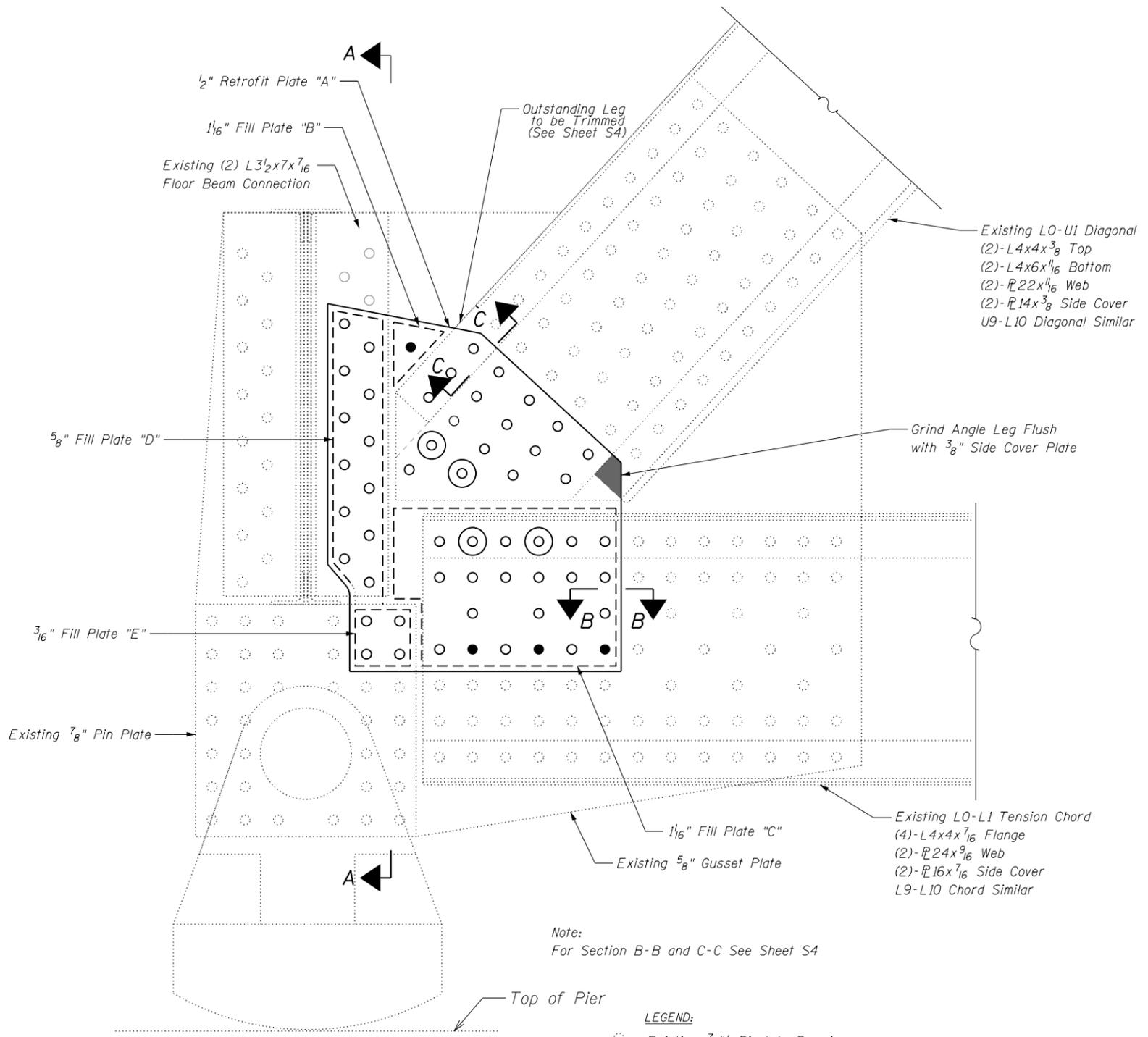
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	6
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B47	

Remove Rivets one-by-one and Replace with High Strength Bolts if Required for Plate Clearances

Refer to Inside Gusset Plate Repair Details Sheet S5 to S6



SECTION A-A



OUTSIDE ELEVATION

Note:  
For Section B-B and C-C See Sheet S4

- LEGEND:**
- Existing 7/8"φ Rivet to Remain
  - ⊙ Replace Existing 7/8"φ Rivet with 7/8"φ A325 Bolt, Installed and Pretensioned Prior to Installing Fill/Retrofit Plates
  - Replace Existing 7/8"φ Rivet with 7/8"φ A325 Bolt
  - New 7/8"φ A325 Bolt
  - New 7/8"φ A325 Bolt Installed During Repair of the Inside Gusset Plate (Sheet S5)

**COLLINS ENGINEERS**

USER NAME =	DESIGNED -	REVISED
	CHECKED -	REVISED
PLOT SCALE =	DRAWN -	REVISED
PLOT DATE =	CHECKED -	REVISED

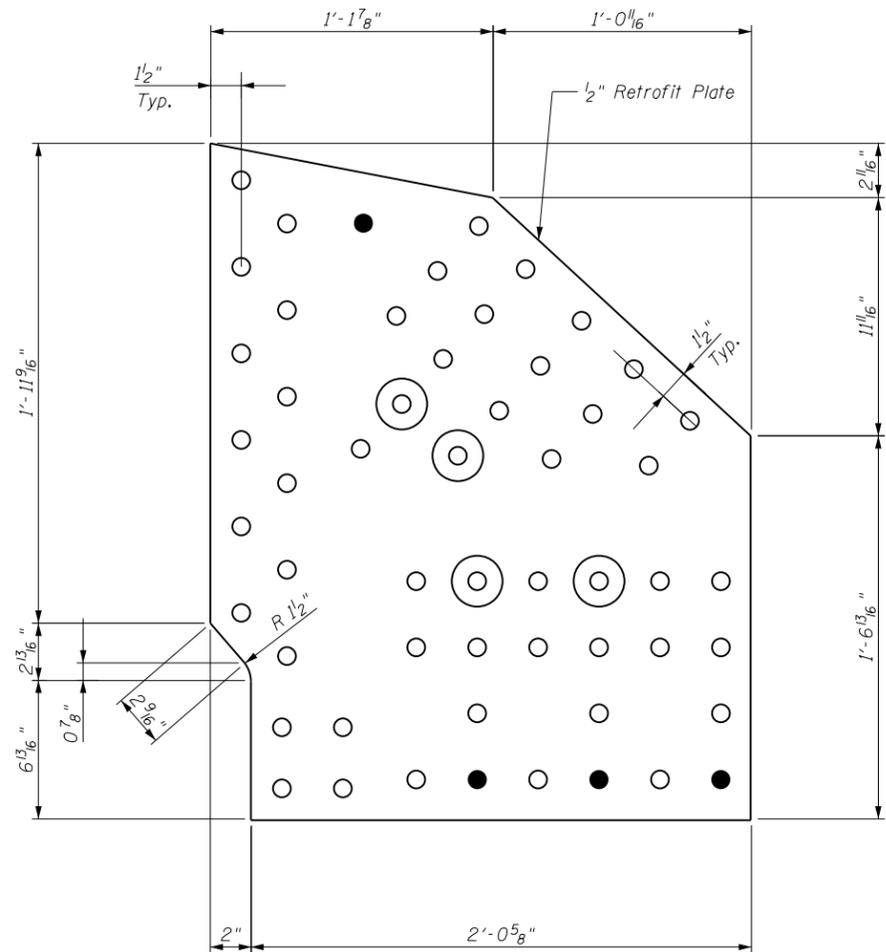
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**OUTSIDE GUSSET PLATE REPAIR DETAILS I  
STRUCTURE NUMBER: 016-0193**

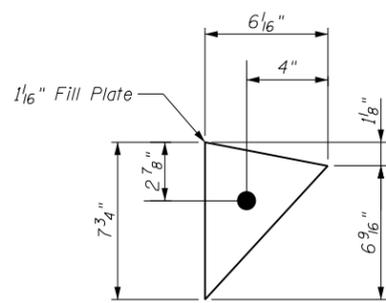
SHEET NO. S3 OF S20 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	KIB&C 2-BR(15)	COOK	26	7
CONTRACT NO. 62B47				

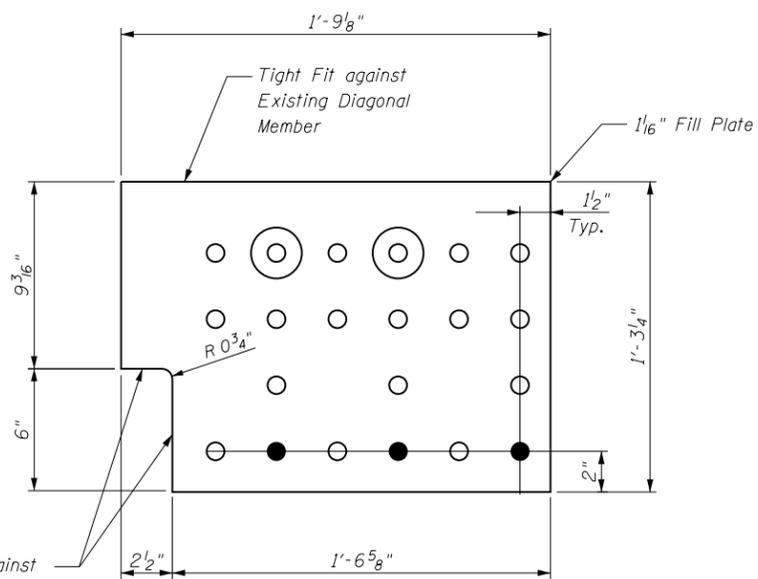
ILLINOIS FED. AID PROJECT



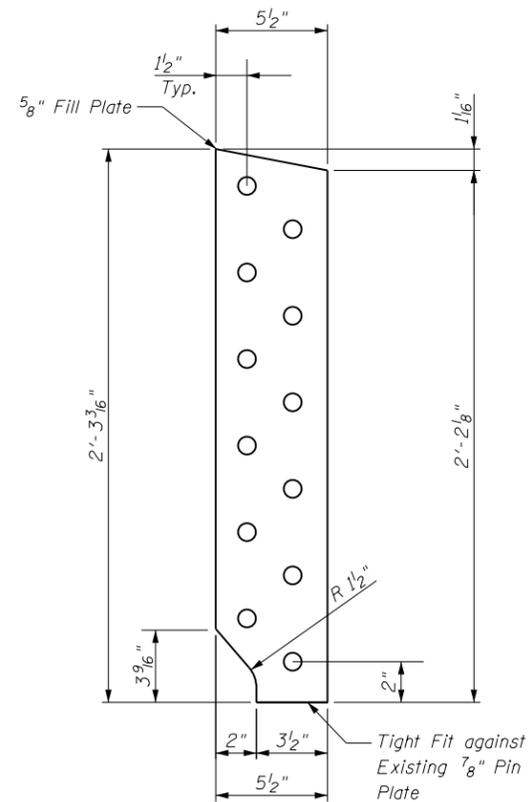
**OUTSIDE GUSSET RETROFIT PLATE "A"**  
(1-Plate Required)



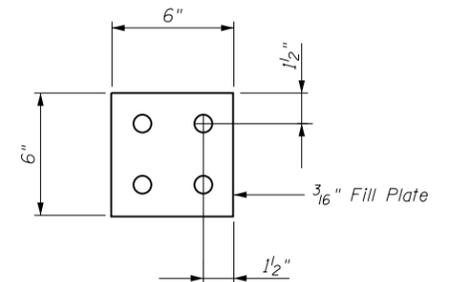
**FILL PLATE "B"**  
(1-Plate Required)



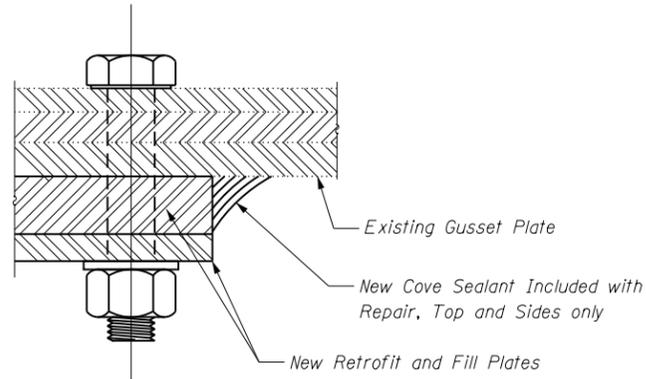
**FILL PLATE "C"**  
(1-Plate Required)



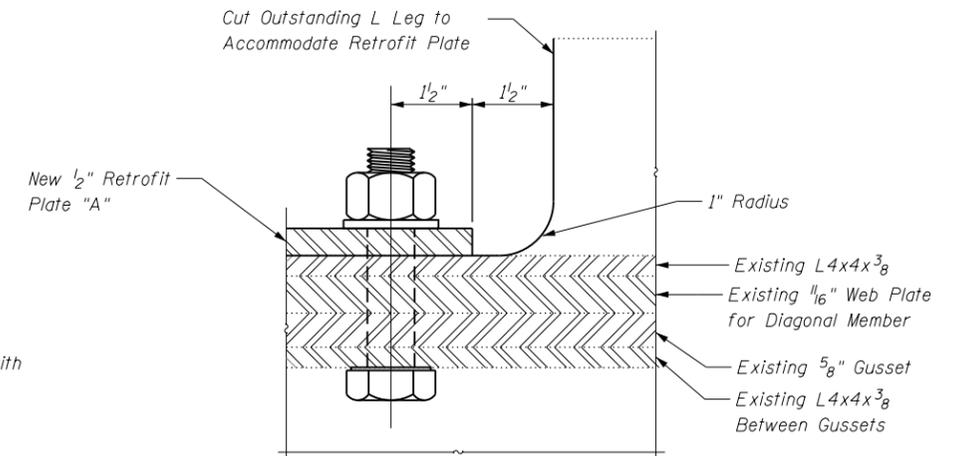
**FILL PLATE "D"**  
(1-Plate Required)



**FILL PLATE "E"**  
(1-Plate Required)



**SECTION B-B**  
For Location of Section B-B  
See Sheet S3



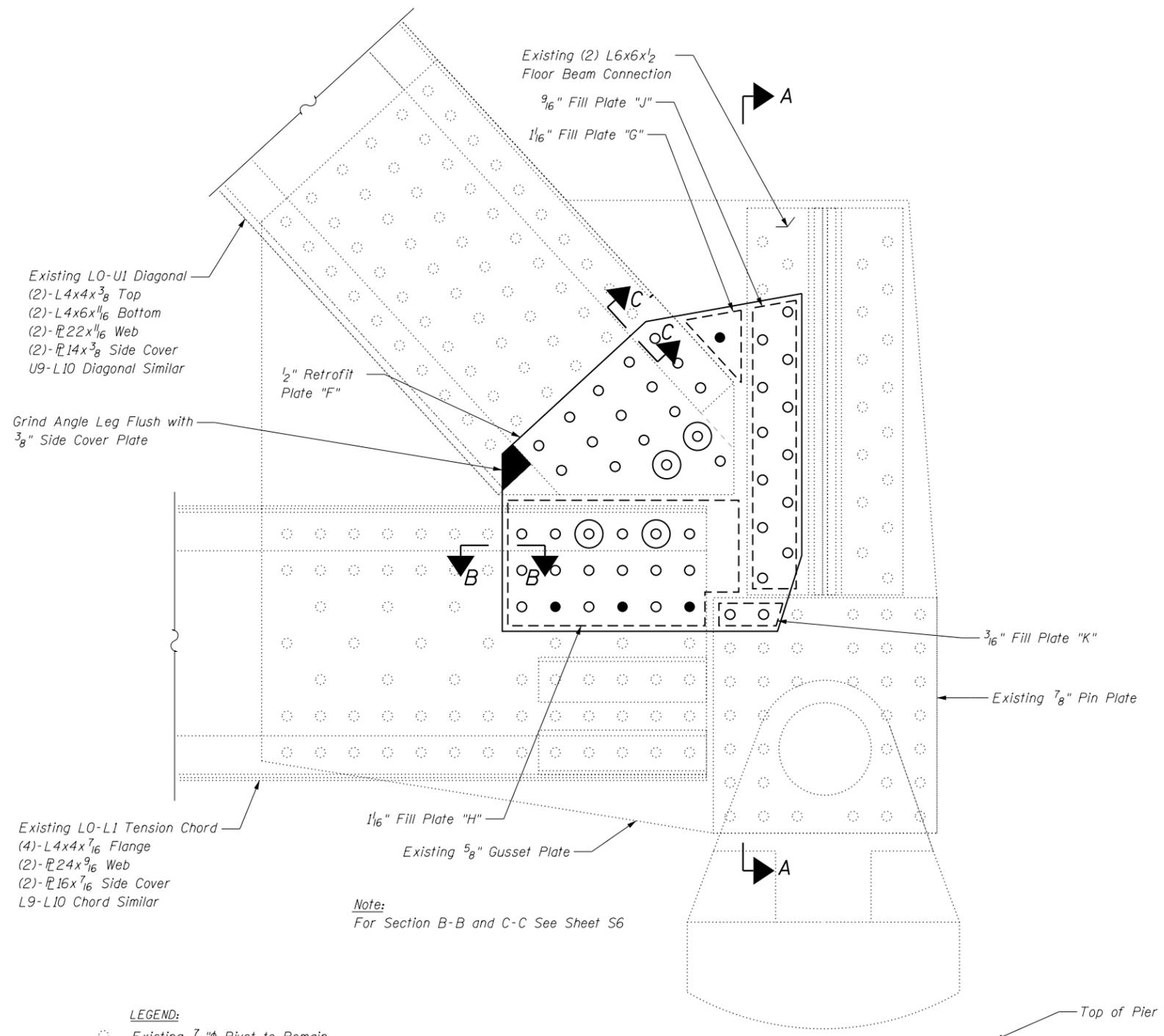
**SECTION C-C**  
For Location of Section C-C  
See Sheet S3

**LEGEND:**

- New Field-drilled  $15/16$ "  $\phi$  Hole to Match Existing Rivet Holes
- New  $15/16$ "  $\phi$  Hole for New  $7/8$ "  $\phi$  Bolt
- ⊙ New  $2 1/2$ "  $\phi$  Hole to Encompass New  $7/8$ "  $\phi$  Bolt

USER NAME =	DESIGNED -	REVISED
	CHECKED -	REVISED
PLOT SCALE =	DRAWN -	REVISED
PLOT DATE =	CHECKED -	REVISED

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	8
CONTRACT NO. 62B47				



Existing L0-U1 Diagonal  
 (2)-L4x4x3/8 Top  
 (2)-L4x6x1/16 Bottom  
 (2)-P22x1/16 Web  
 (2)-P14x3/8 Side Cover  
 U9-L10 Diagonal Similar

Grind Angle Leg Flush with  
 3/8\" Side Cover Plate

Existing L0-L1 Tension Chord  
 (4)-L4x4x7/16 Flange  
 (2)-P24x9/16 Web  
 (2)-P16x7/16 Side Cover  
 L9-L10 Chord Similar

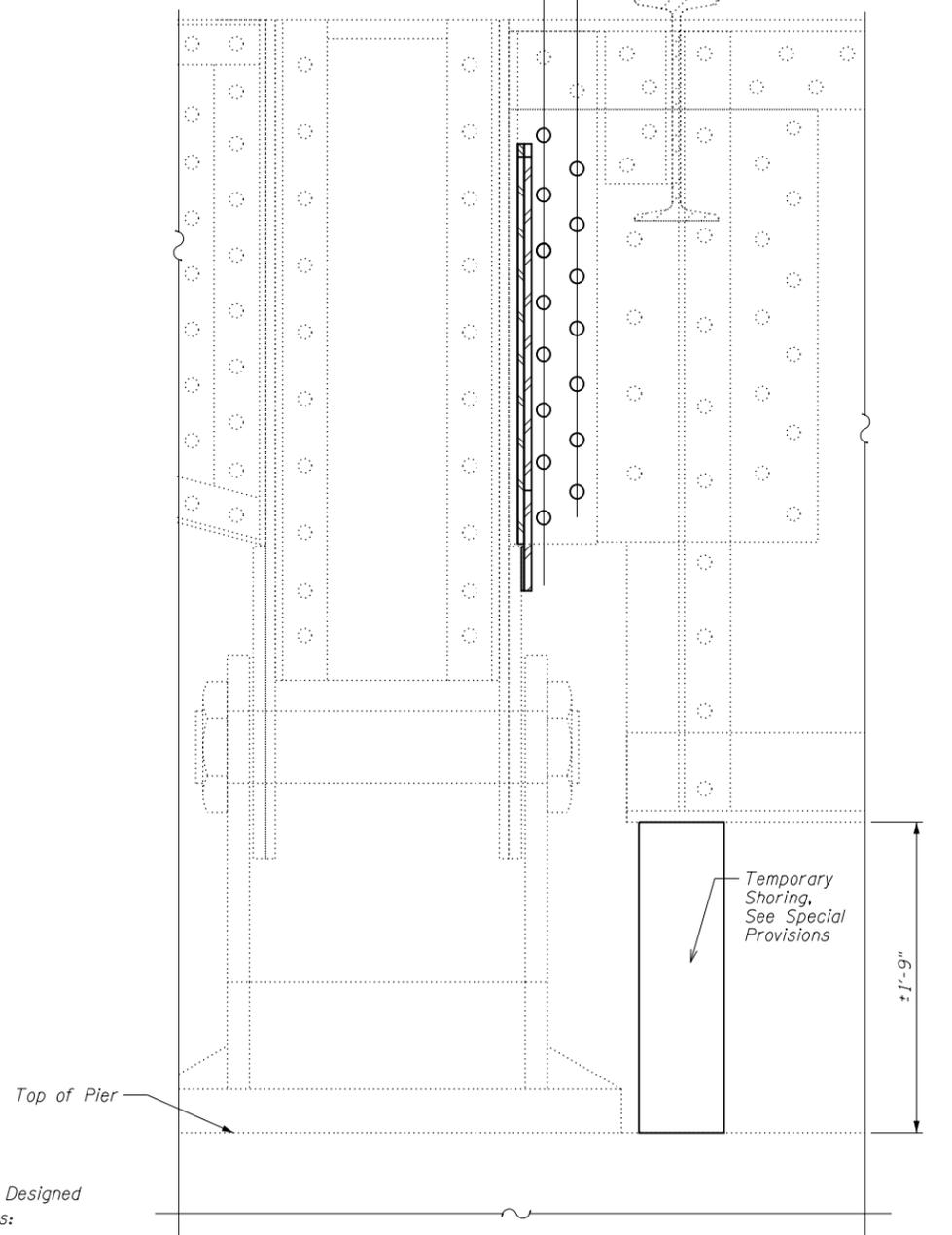
Note:  
 For Section B-B and C-C See Sheet S6

- LEGEND:**
- ⊙ Existing 7/8"φ Rivet to Remain
  - ⊕ Replace Existing 7/8"φ Rivet with 7/8"φ A325 Bolt, Installed and Pretensioned Prior to Installing Fill/Retrofit Plates
  - Replace Existing 7/8"φ Rivet with 7/8"φ A325 Bolt
  - New 7/8"φ A325 Bolt

**INSIDE ELEVATION**

Remove Rivets one-by-one and Replace with High Strength Bolts if Required for Plate Clearances

Longitudinal Slotted Holes (15/16" x 1/8") Permitted in this Row if Required for Floor Beam Bolt Installation



**SECTION A-A**

\*Temporary Shoring Should be Designed for the Following Service Loads:  
 DL = 60 k

**COLLINS ENGINEERS**

USER NAME =	DESIGNED -	REVISED
	CHECKED -	REVISED
PLOT SCALE =	DRAWN -	REVISED
PLOT DATE =	CHECKED -	REVISED

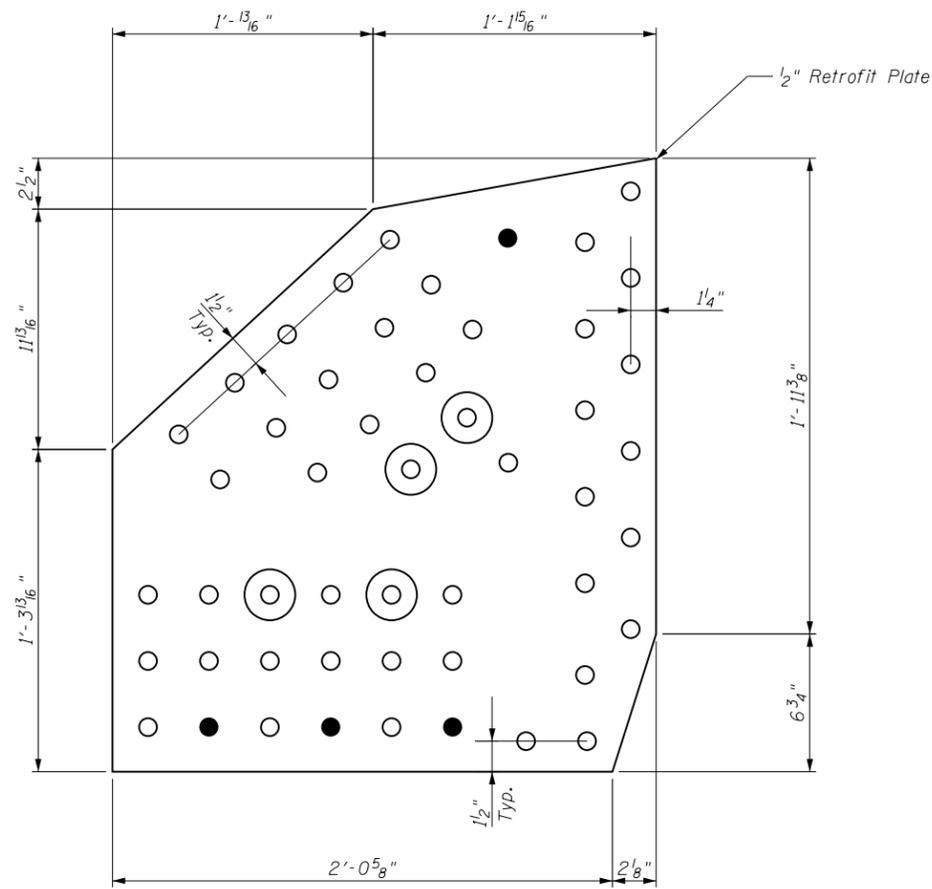
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**INSIDE GUSSET PLATE REPAIR DETAILS I  
 STRUCTURE NUMBER: 016-0193**

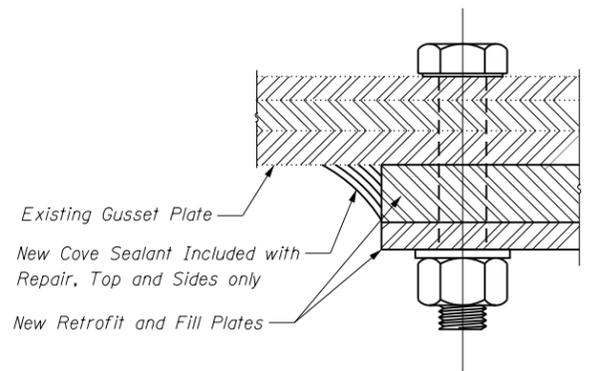
SHEET NO. S5 OF S20 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	9
CONTRACT NO. 62B47				

ILLINOIS FED. AID PROJECT

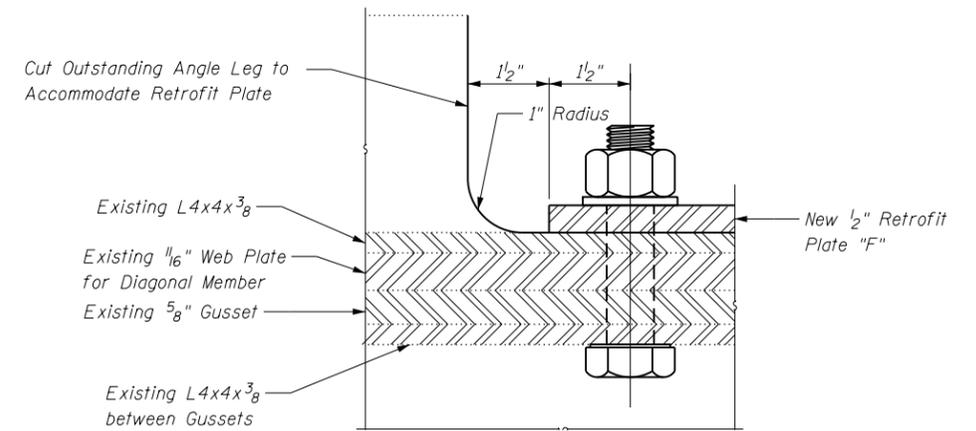


**INSIDE GUSSET RETROFIT PLATE "F"**  
(1-Plate Required)



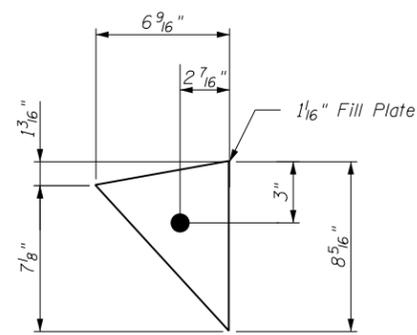
**SECTION B-B**

For Location of Section B-B  
See Sheet S5



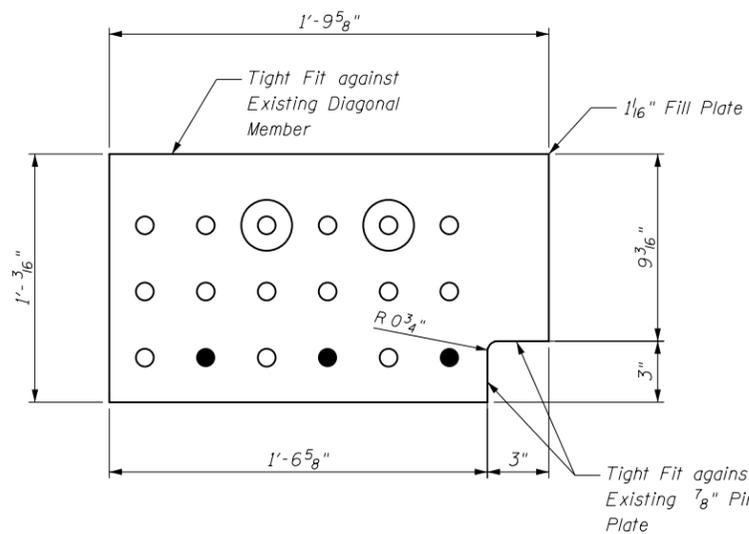
**SECTION C-C**

For Location of Section C-C  
See Sheet S5



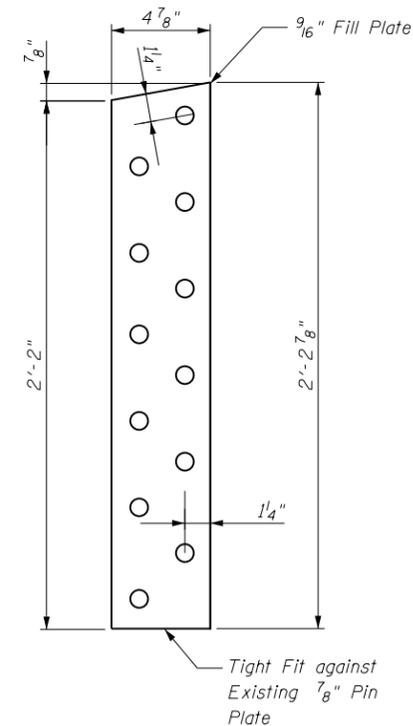
**FILL PLATE "G"**

(1-Plate Required)



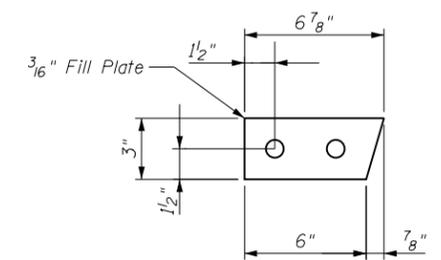
**FILL PLATE "H"**

(1-Plate Required)



**FILL PLATE "J"**

(1-Plate Required)



**FILL PLATE "K"**

(1-Plate Required)

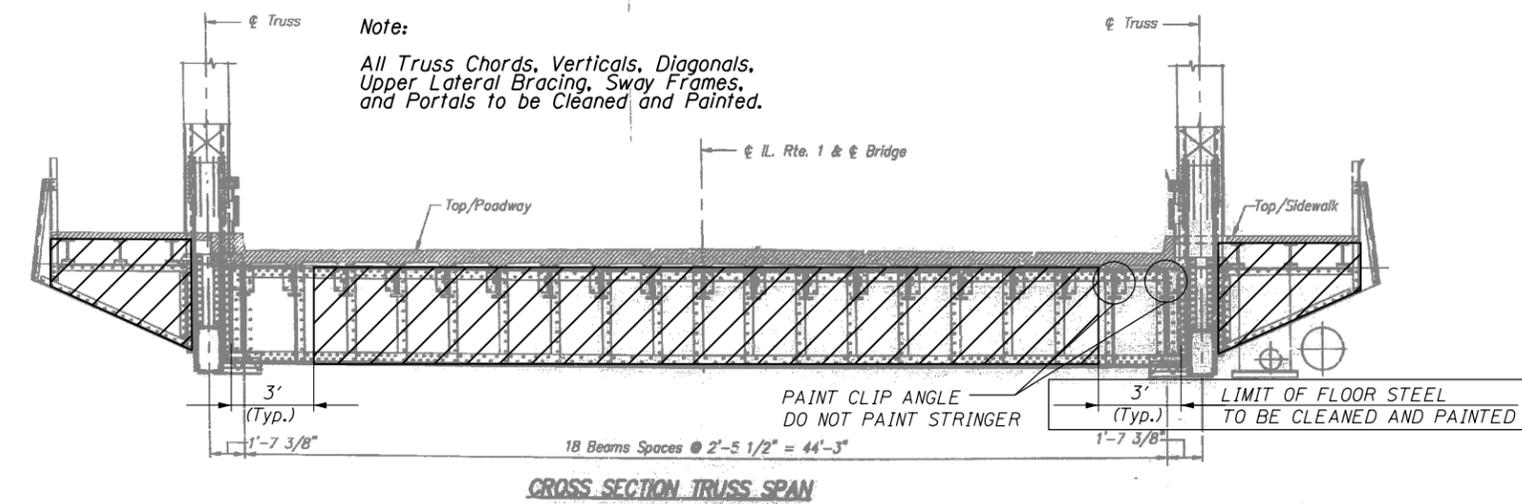
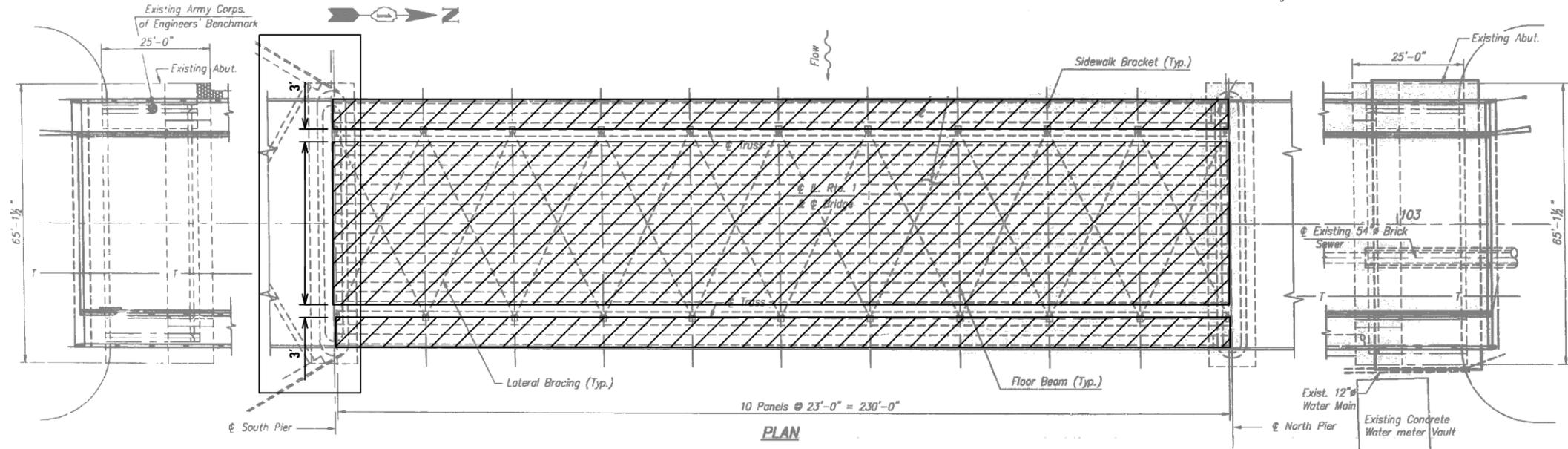
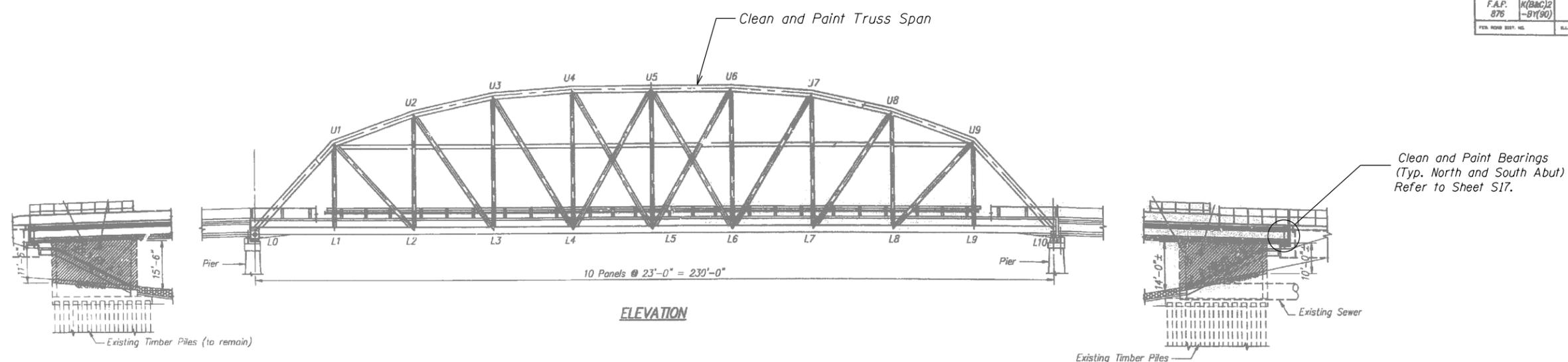
**LEGEND:**

- New Field-drilled  $\frac{15}{16}$ "  $\phi$  Hole to Match Existing Rivet Holes
- New  $\frac{15}{16}$ "  $\phi$  Hole for New  $\frac{7}{8}$ "  $\phi$  Bolt
- ⊙ New  $2\frac{1}{2}$ "  $\phi$  Hole to Encompass New  $\frac{7}{8}$ "  $\phi$  Bolt

USER NAME =	DESIGNED -	REVISED -
PLOT SCALE =	CHECKED -	REVISED -
PLOT DATE =	DRAWN -	REVISED -
	CHECKED -	REVISED -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	10
CONTRACT NO. 62B47				

ROUTE NO.	SECTION	COUNTY	SHEETS	SHEET NO.	SHEET NO.
F.A.P. 876	K(B&C)2 -B(90)	COOK	60	19	43 SHEETS
FED. ROAD DIST. NO.	BLANKED	FED. AID PROJECT NO.			



**LEGEND**

HATCHED AREA DESIGNATES PORTION OF FLOOR STEEL TO BE OMITTED FROM CLEANING AND PAINTING.

**COLLINS ENGINEERS**

USER NAME =	DESIGNED -	REVISOR
PLOT SCALE =	CHECKED -	REVISOR
PLOT DATE =	DRAWN -	REVISOR
	CHECKED -	REVISOR

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAINTING PLAN, ELEVATION, AND SECTION  
STRUCTURE NUMBER: 016-0193**

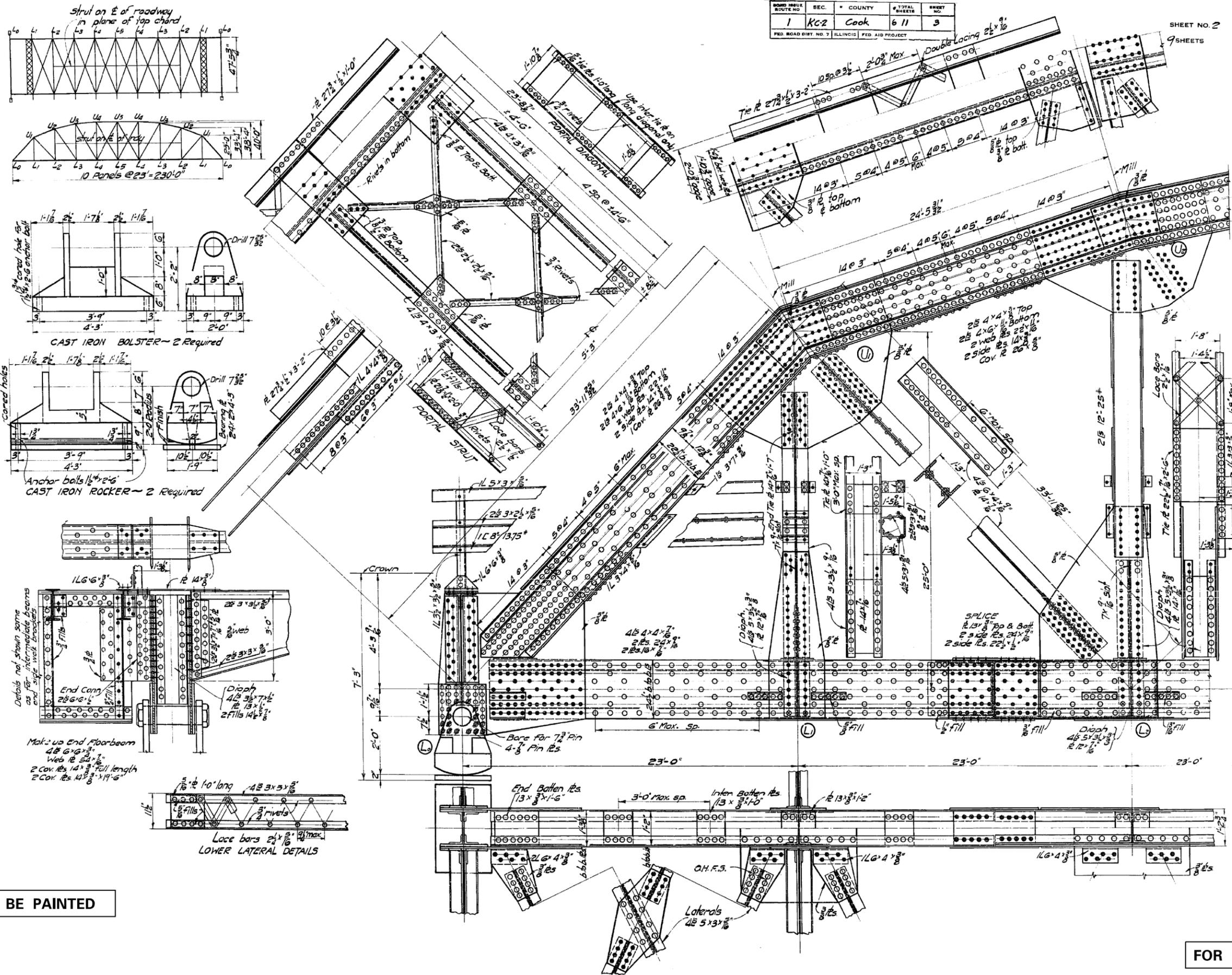
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	11
CONTRACT NO. 62B47				

SHEET NO. S7 OF S20 SHEETS

ILLINOIS FED. AID PROJECT

ROAD DIST. NO.	SEC.	COUNTY	# TOTAL SHEETS	SHEET NO.
1 Kc2	Cook	611	3	
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

SHEET NO. 2  
9 SHEETS



**COLLINS ENGINEERS**

USER NAME =	DESIGNED -	REVISED -
PLOT SCALE =	CHECKED -	REVISED -
PLOT DATE	DRAWN -	REVISED -
	CHECKED -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRUSS DETAILS I  
STRUCTURE NUMBER: 016-0193

SHEET NO. 58 OF 520 SHEETS

F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	12
CONTRACT NO. 62B47				

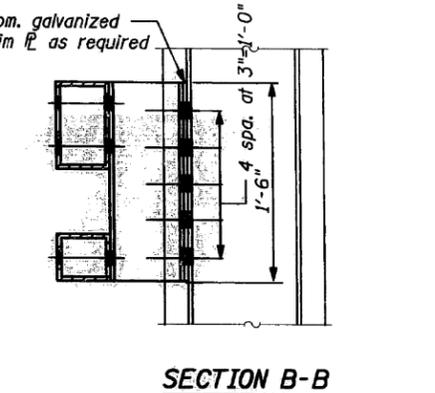
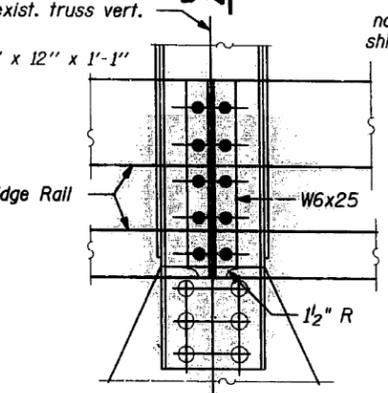
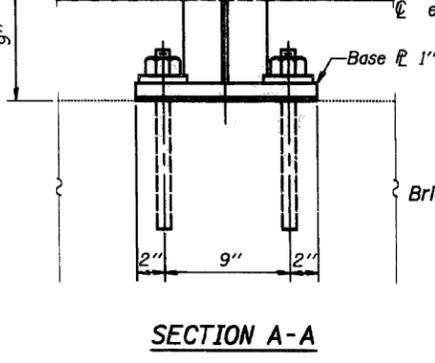
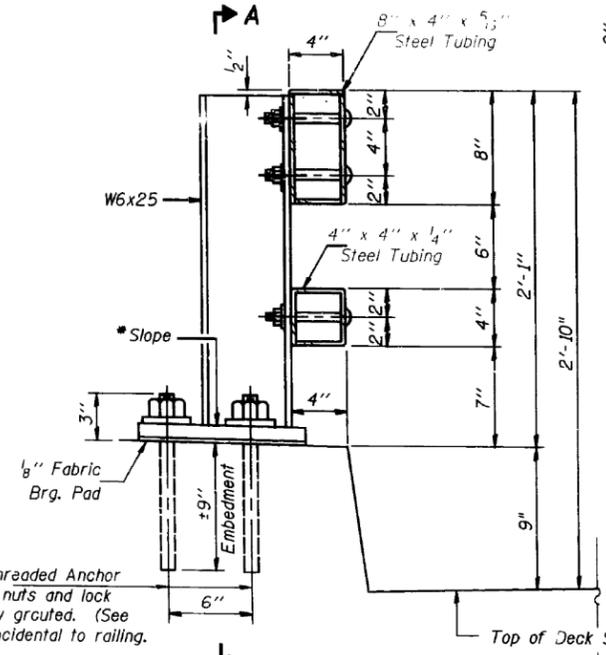
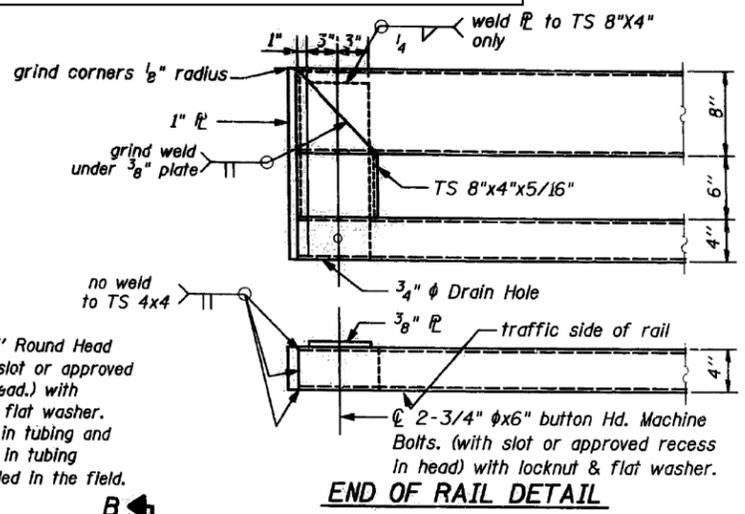
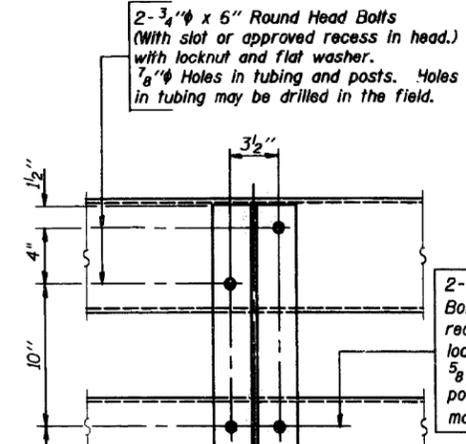
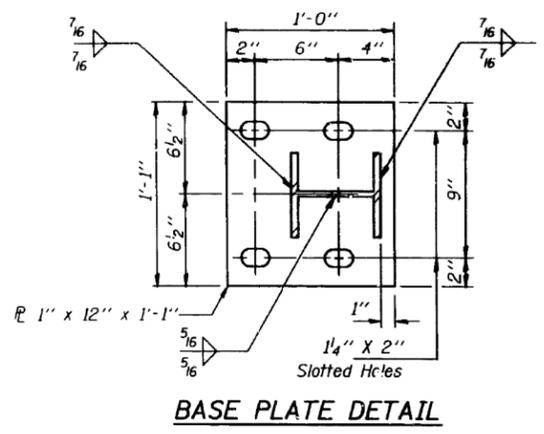
ILLINOIS FED. AID PROJECT



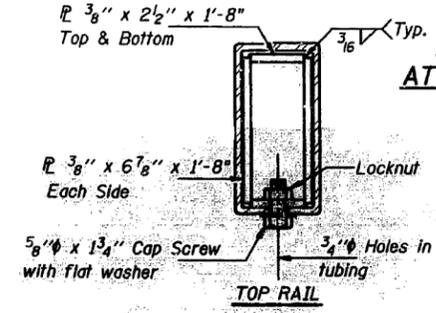
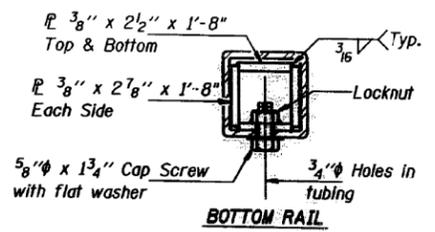
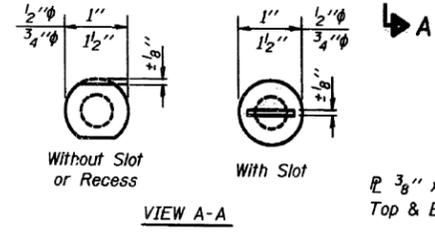
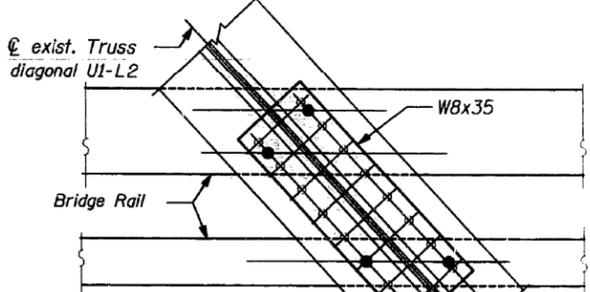
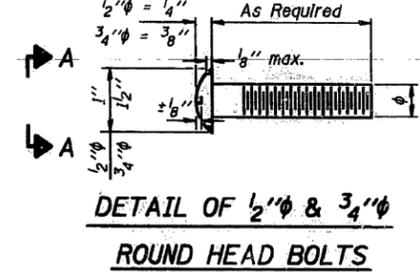


# GALVANIZED BRIDGE RAIL NOT TO BE PAINTED

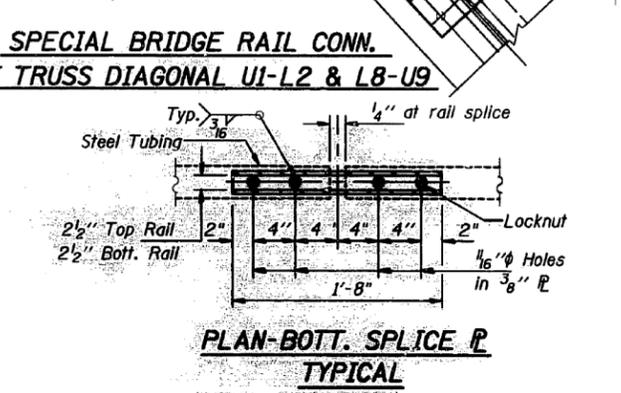
ROUTE NO.	SECTION	COUNTY	DATE	SHEET NO.
F.A.P. 876	KIB&C 2-BR150	COOK	60 31	15
FED. ROAD DIST. NO.		BARRETS	FED. AID PROJECT	43 SHEETS



**SPECIAL BRIDGE RAIL CONNECTION AT TRUSS VERT. AT L2, L3, L4, L5, L6, L7 & L8**  
(ref. to sht. 22 for Truss panel point locations)



**SECTIONS AT RAIL SPLICE**

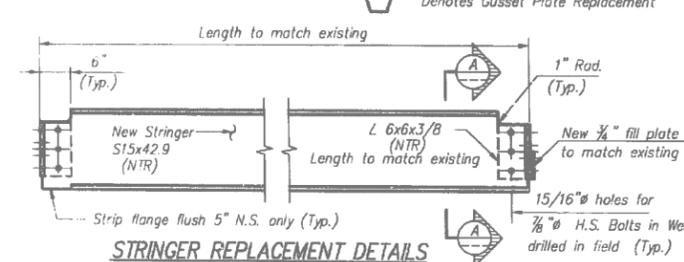
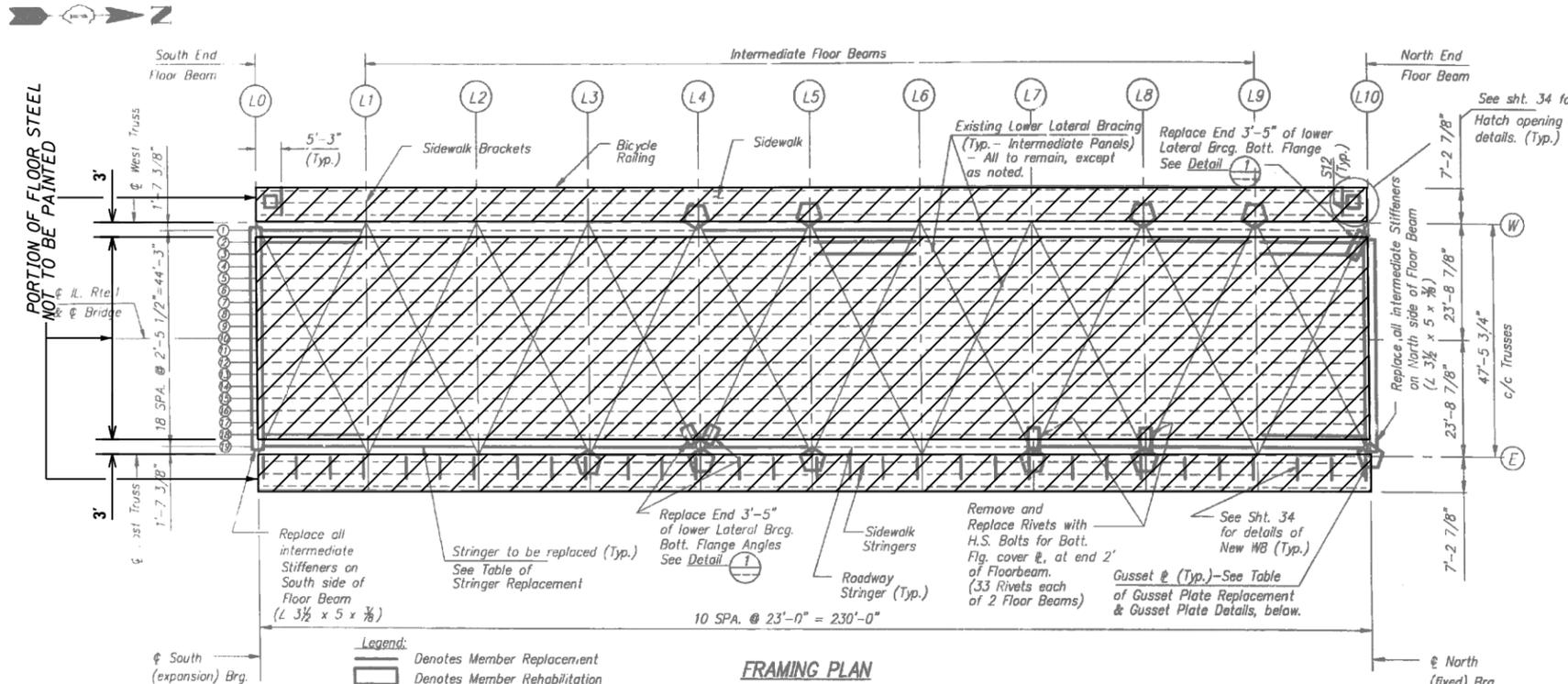


**FOR INFORMATION ONLY**

USER NAME =	DESIGNED -	REVISED
PLOT SCALE =	CHECKED -	REVISED
PLOT DATE =	DRAWN -	REVISED
	CHECKED -	REVISED

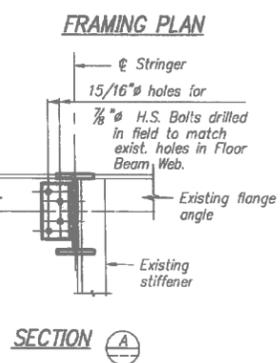
F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	KIB&C 2-BR150	COOK	26	15
				CONTRACT NO. 62B47



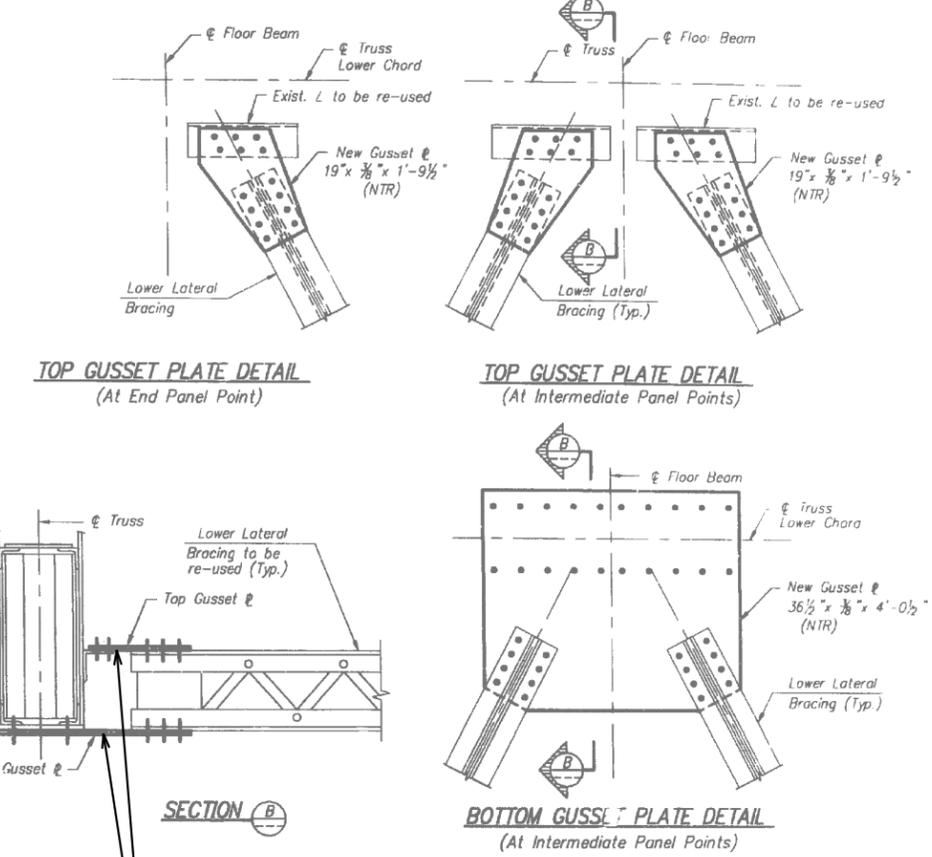
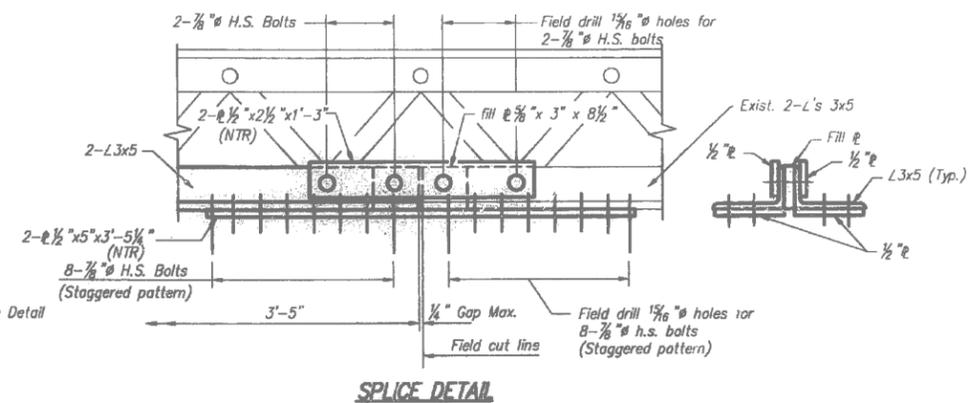
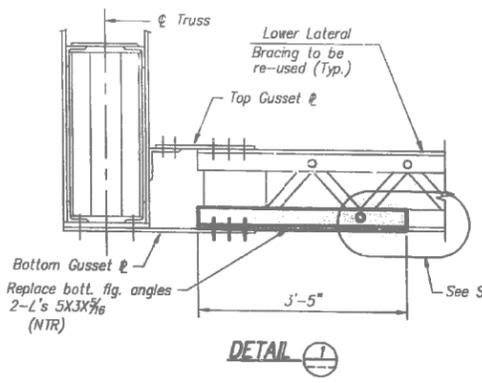


PANEL	STRINGER
L0-L1	1,2,18,19
L1-L2	19
L2-L3	19
L3-L4	19
L4-L5	1, 19
L5-L6	1,2,3
L6-L7	-
L7-L8	19
L8-L9	2,19
L9-L10	2,3,18,19

In addition to replacement Stringers shown in the Table above, provide 10 additional Stringers for replacement of unforeseen deteriorated Stringers, as directed by the Engineer. Weight of these Stringers is included in the Quantity for "Furnishing and Erecting Structural Steel". If not used, these Stringers are salvage and become the property of the State of Illinois.



Panel/Location	Gusset @ Top	Gusset @ Bottom
L3/East	X (South)	X
L4/East		X
L4/East	X (North)	
L4/West	X (South)	
L5/East	X (South)	
L5/East	X (North)	
L5/West	X (South)	
L5/West	X (North)	
L7/East		X
L7/East	X (North)	
L8/East	X (South)	X
L8/West	X (North)	
L9/West	X (South)	
L10/East	X (South)	



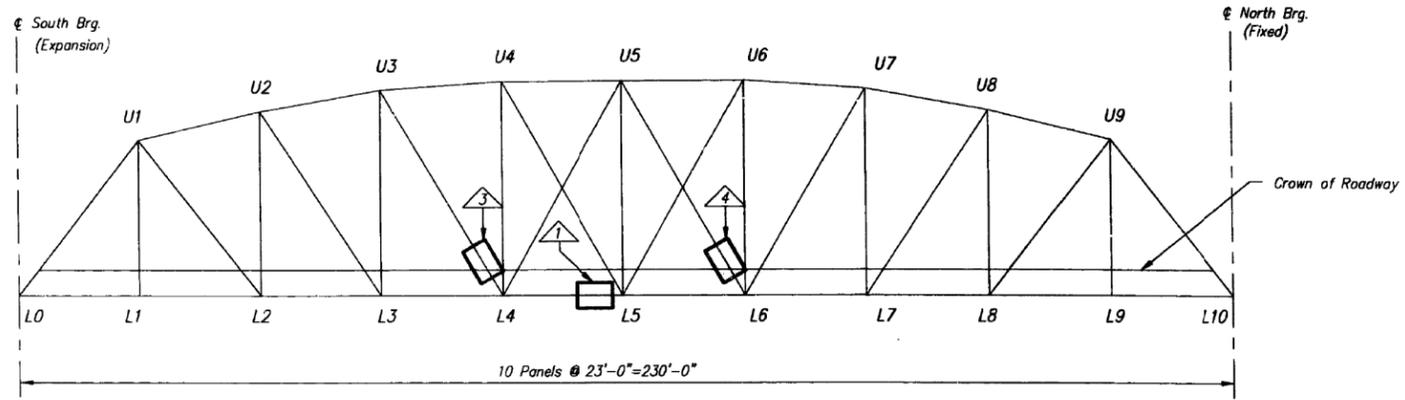
**LOWER LATERAL BRACING-GUSSET PLATE DETAILS**  
(New replacement gusset plates to match existing plates in shape, size and hole location. See Table of Gusset Plate Replacement.)

**LATERAL GUSSET PLATE CONNECTING TRUSS TO LOWER LATERAL BRACING TO BE PAINTED**

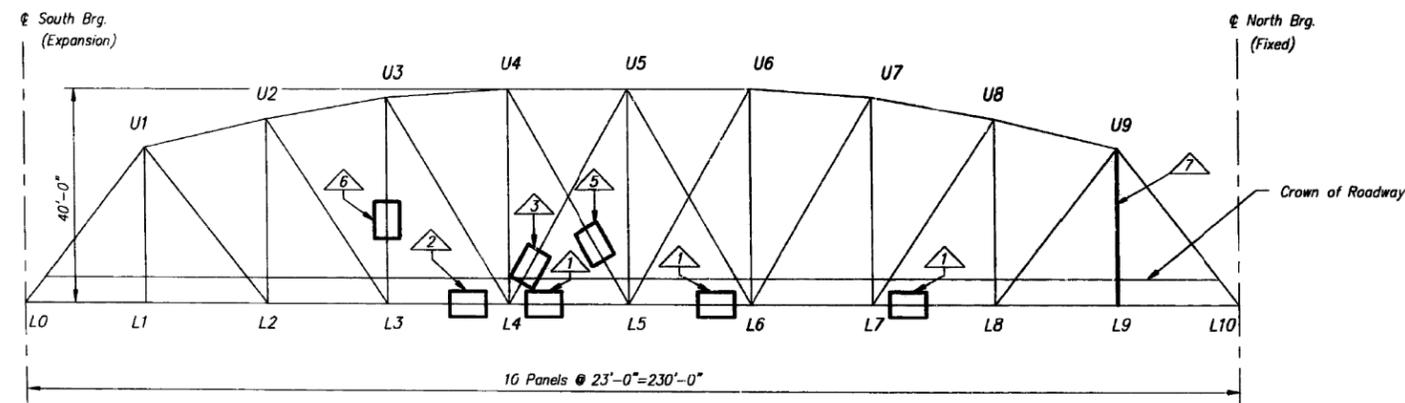
**LEGEND**

HATCHED AREA DESIGNATES PORTION OF FLOOR STEEL TO BE OMITTED FROM CLEANING AND PAINTING.

**FOR INFORMATION ONLY**



**EAST TRUSS**



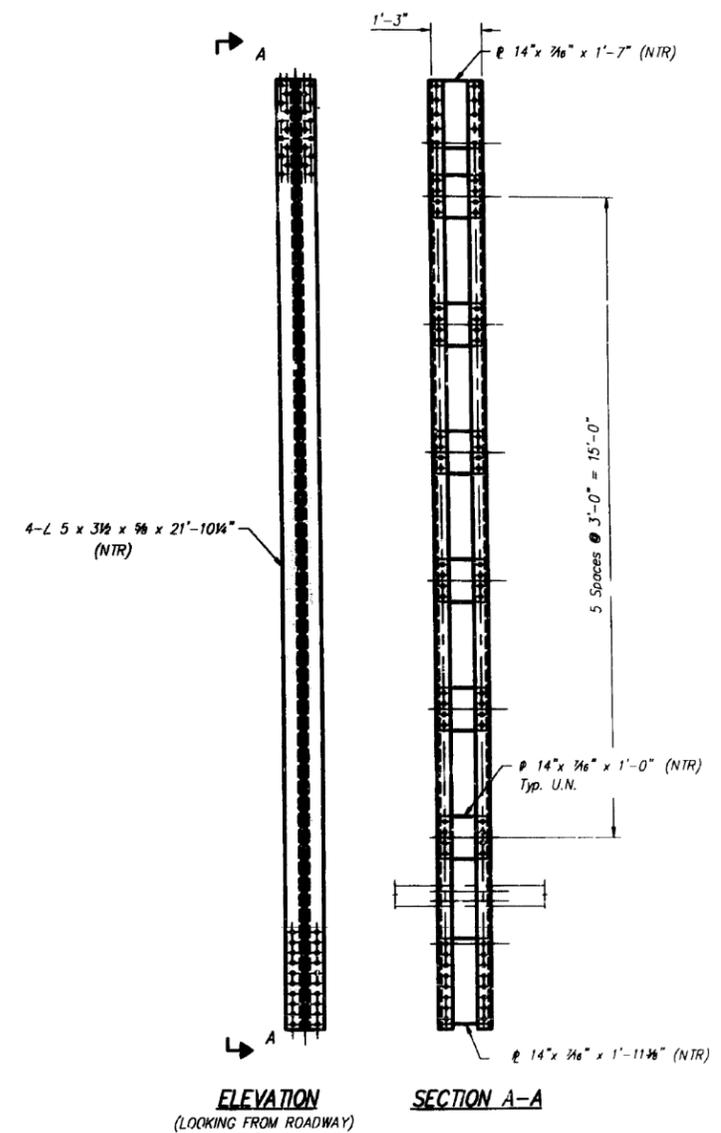
**WEST TRUSS**

**MEMBER REHABILITATION:**

- | ITEM | DESCRIPTION  |
|------|--|
| 1    | REPLACE FIRST STAY $\phi$ ON BOTTOM FLANGE, $\phi$ 13" x $\frac{3}{8}$ " x 1'-0"                                 |
| 2    | REPLACE SPLICE $\phi$ ON BOTTOM FLANGE, $\phi$ 13" x $\frac{3}{8}$ " x 3'-4"                                     |
| 3    | REPLACE LOWERMOST WEB BATTEN $\phi$ , $\phi$ 18" x $\frac{3}{8}$ " x 1'-1", $\phi$ 18" x $\frac{3}{8}$ " x 1'-2" |
| 4    | REPLACE 2nd FROM BOTTOM WEB BATTEN $\phi$ , $\phi$ 12" x $\frac{3}{8}$ " x 1'-2"                                 |
| 5    | REPLACE 3rd FROM BOTTOM WEB BATTEN $\phi$ , $\phi$ 12" x $\frac{3}{8}$ " x 1'-2"                                 |
| 6    | REPLACE 2 LACING BARS, Bars 2 $\frac{1}{2}$ " x $\frac{3}{8}$ " x 2'-4"  |
| 7    | REPLACE TRUSS VERTICAL   |

**LEGEND:**

- Member Replacement
- Member Rehabilitation



**ELEVATION**  
(LOOKING FROM ROADWAY)

**SECTION A-A**  
**VERTICAL U9-L9**  
**WEST TRUSS**

"NTR" denotes members to which Supplemental requirements for notch toughness ZONE 2 are applicable.

**AREA TO BE PAINTED**

**FOR INFORMATION ONLY**

**COLLINS ENGINEERS**

USER NAME =	DESIGNED -	REVISED
	CHECKED -	REVISED
PLOT SCALE =	DRAWN -	REVISED
PLOT DATE =	CHECKED -	REVISED

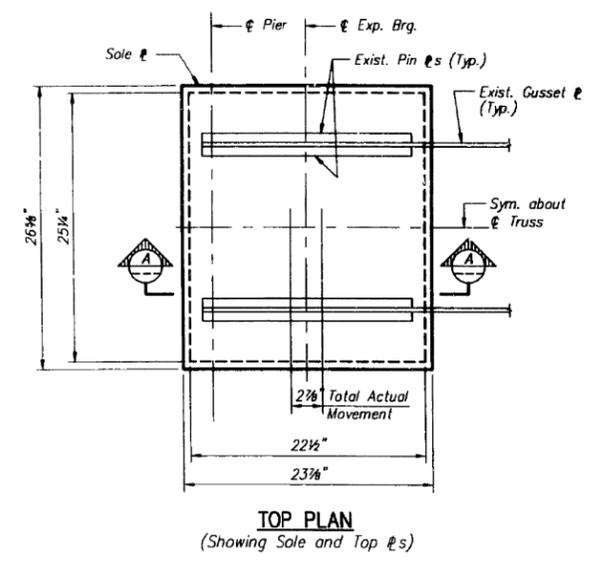
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PREVIOUS TRUSS REPAIRS**  
**STRUCTURE NUMBER: 016-0193**

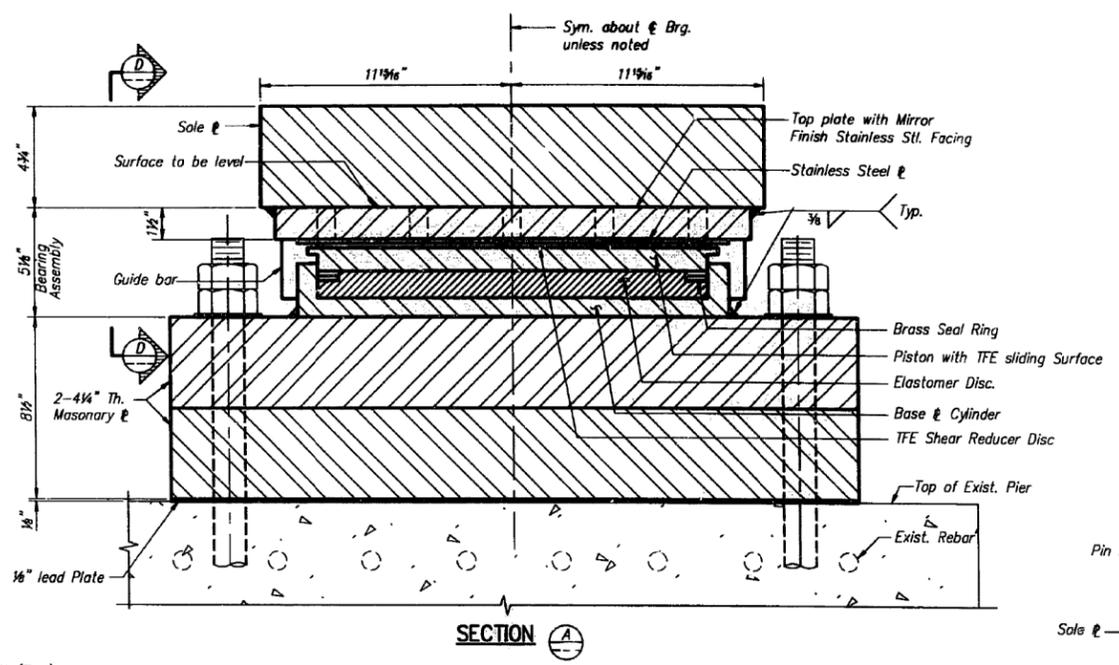
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	18
CONTRACT NO. 62B47				

SHEET NO. S14 OF S20 SHEETS

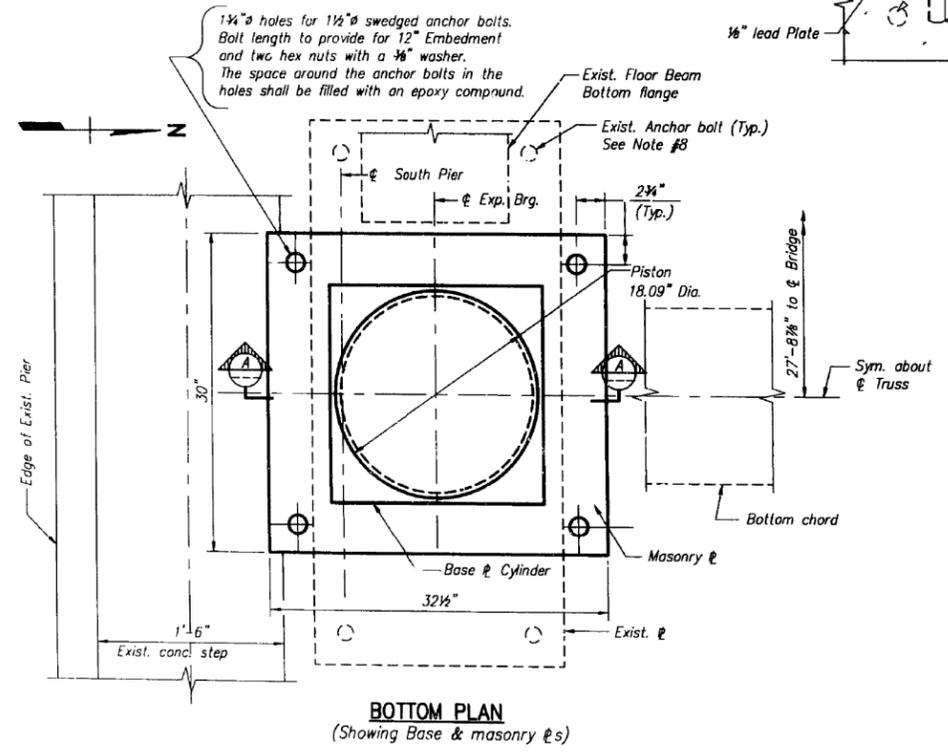
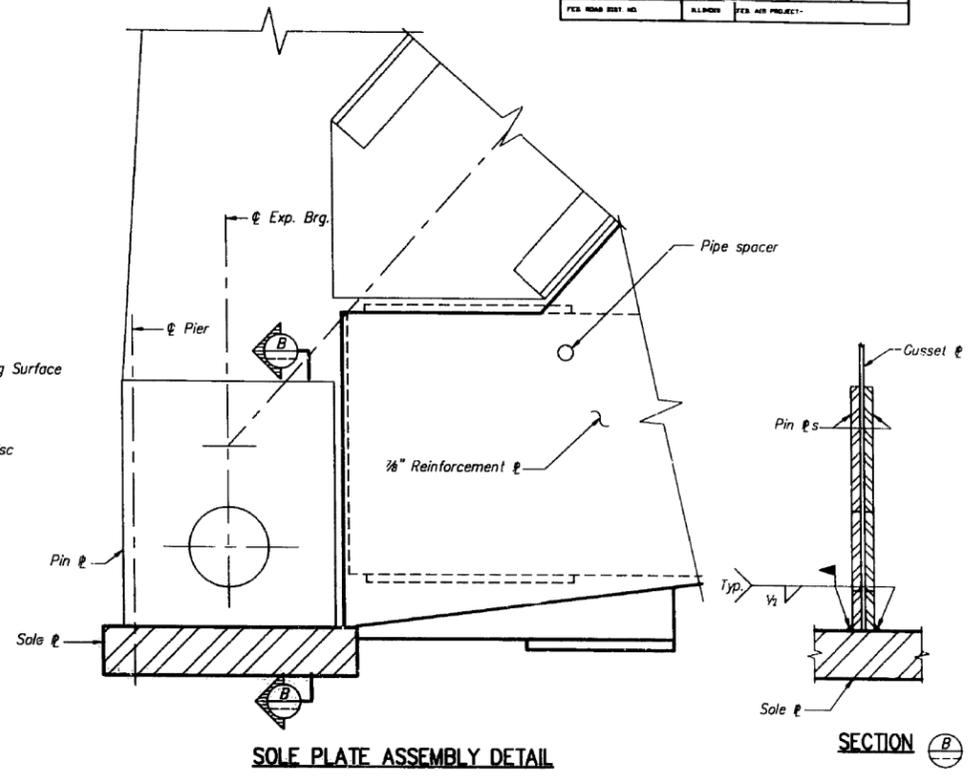
ILLINOIS FED. AID PROJECT



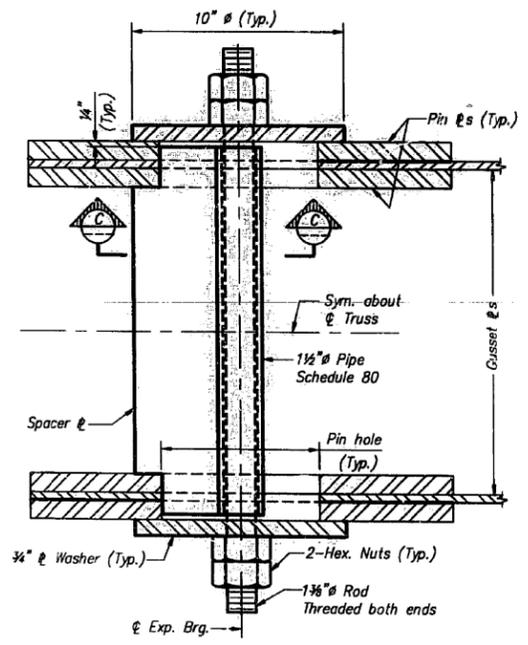
**TOP PLAN**  
(Showing Sole and Top Pls)



**SECTION A**

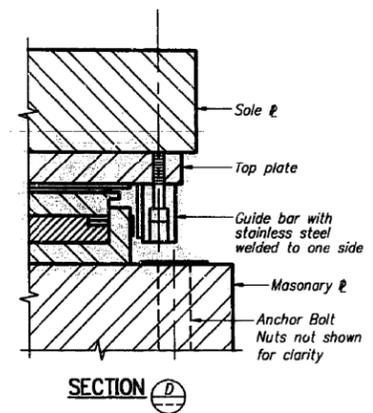
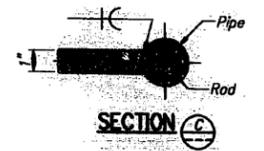


**BOTTOM PLAN**  
(Showing Base & masonry Pls)



**PLAN DETAIL PERMANENT SPACER**

1 Required for each new floating bearing.



**BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Floating Bearings, Guided Expansion, 900K.	Each	2
Removal of Existing Bearings	Each	2

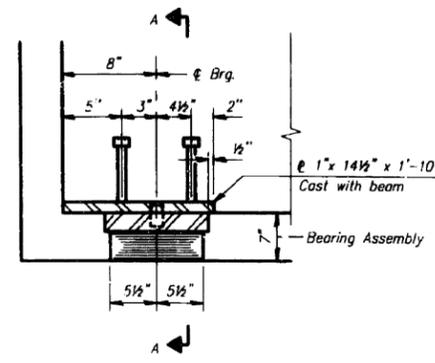
**PERMANENT SPACER NOTES:**  
A Permanent spacer shall be installed at each expansion bearing. Each spacer shall be installed before the sole plate for the new bearing has been welded to the existing gusset & pin plates. The spacer shall be inserted into the existing pin hole and the rod installed and tightened with the inside faces of the pin plates bearing against the spacer plate & the washer plates bearing against the outer faces of the pin plates. The cost of these spacers shall be included in the cost of furnishing and erecting structural steel.

**AREA TO BE PAINTED**

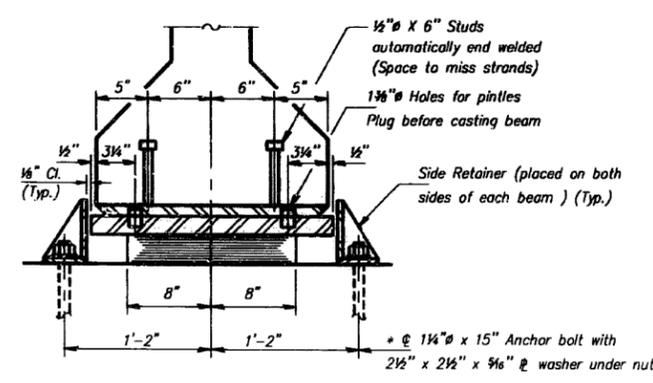
**FOR INFORMATION ONLY**



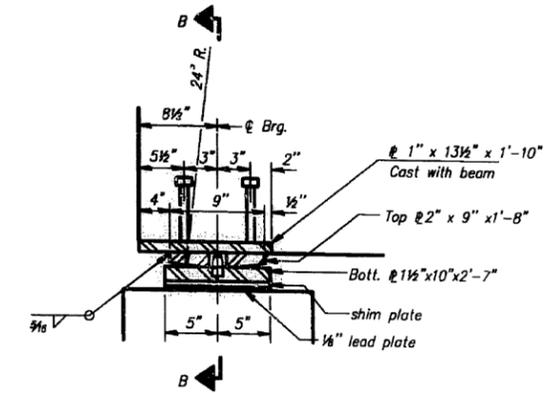
ROUTE NO.	SECTION	COUNTY	SHEETS	SHEET NO.	SHEET NO. 23 43 SHEETS
F.A.P. 876	K(B&C)2 -BR(80)	COOK	60	39	
FED. HIGHWAY DIST. NO.	ILLINOIS		FED. AID PROJECT		



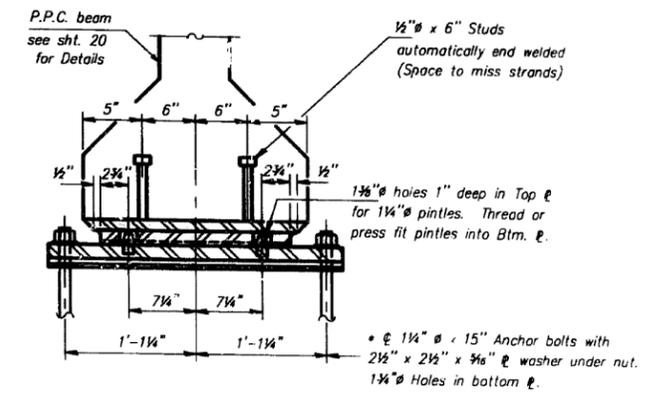
**SECTION AT ABUT.**



**SECTION A-A**



**SECTION AT PIER.**



**SECTION B-B**

**TYPE I ELASTOMERIC EXP. BRG. AT ABUTMENTS**

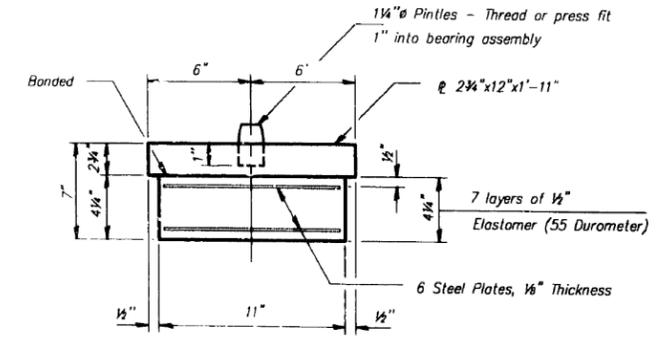
\* Notes: After beams have been erected, holes at expansion bearings shall be drilled and anchor bolts grouted in place.

See sheet 32 for anchor bolt installation.

The cost of side retainers shall be included in the cost of Furnishing and Erecting Structural Steel.

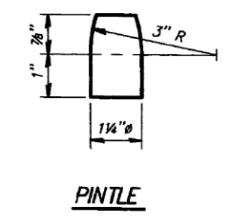
**FIXED BEARING**

\* Anchor bolts may be built into the masonry or drilled and grouted in place after all beams have been erected. See sheet 32 for anchor bolt installation.

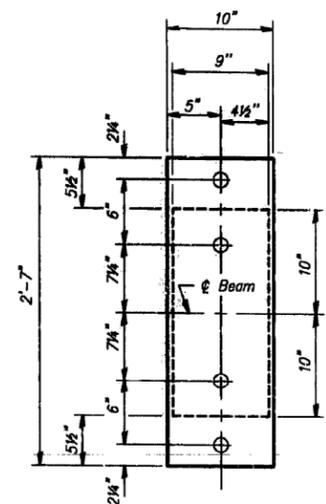


**BEARING ASSEMBLY**

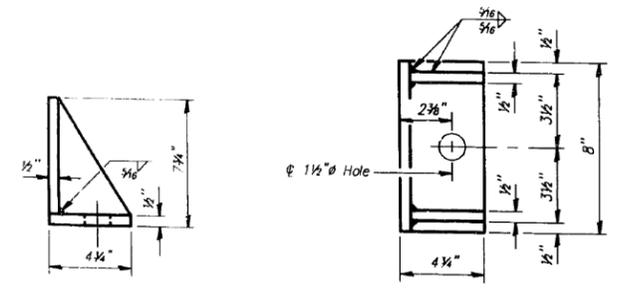
Note: Shim plates shall not be placed under Bearing Assembly.



**PINTLE**



**PLAN OF TOP & BOTTOM PLATES**



**SIDE RETAINER**

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates. Weight included with Structural Steel.

**AREA TO BE PAINTED**

**FOR INFORMATION ONLY**

**COLLINS ENGINEERS INC.**

USER NAME =	DESIGNED -	REVISED
PLOT SCALE =	CHECKED -	REVISED
PLOT DATE =	DRAWN -	REVISED
	CHECKED -	REVISED

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

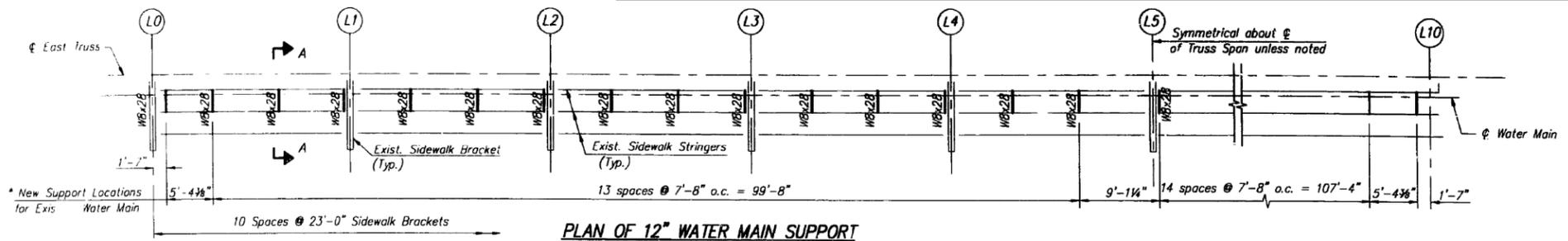
**BEARING DETAILS ABUTMENTS  
STRUCTURE NUMBER: 016-0193**

SHEET NO. S17 OF S20 SHEETS

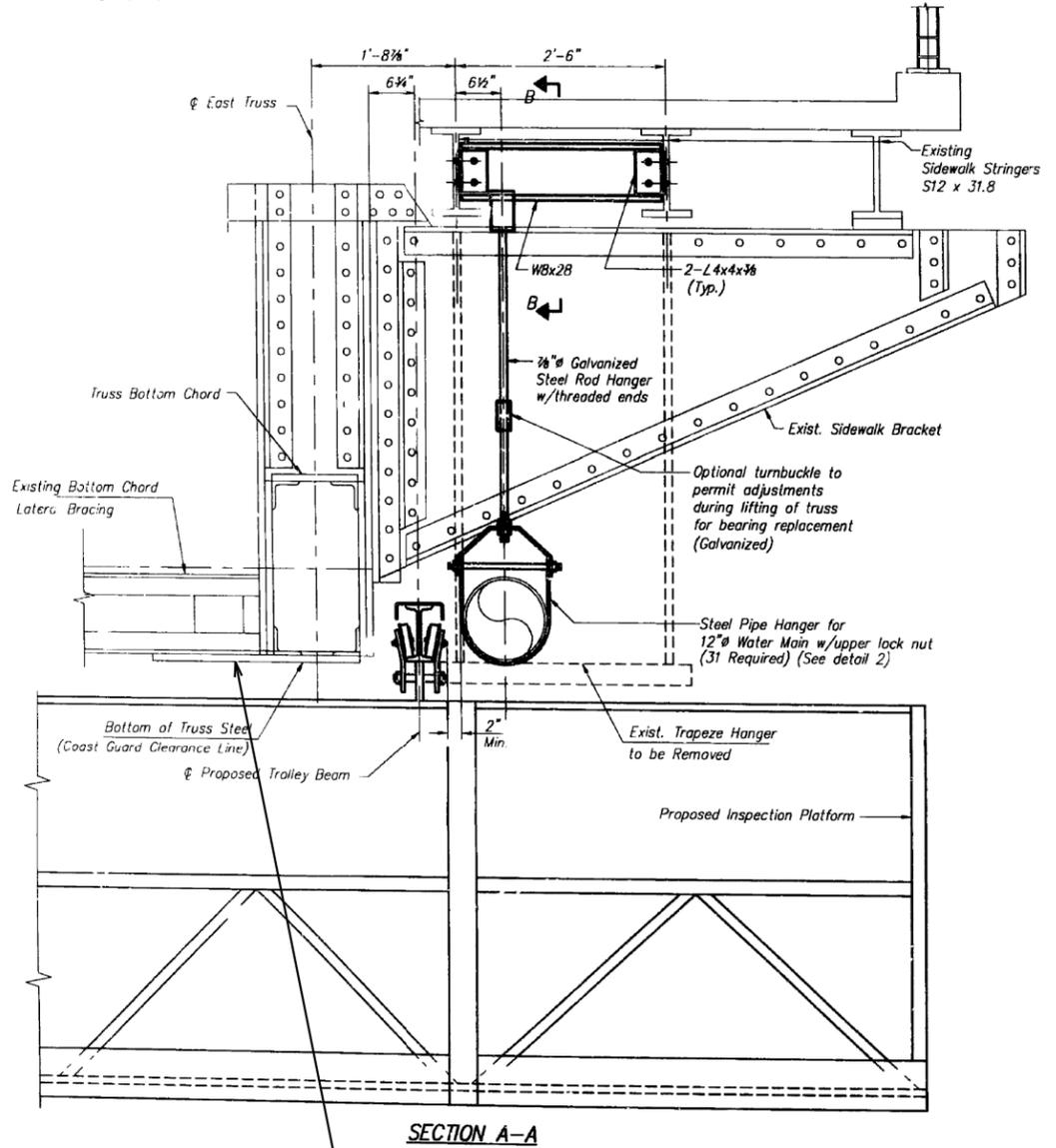
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	21
CONTRACT NO. 62B47				
ILLINOIS FED. AID PROJECT				

# WATER MAIN AND WATER MAIN SUPPORT NOT TO BE PAINTED

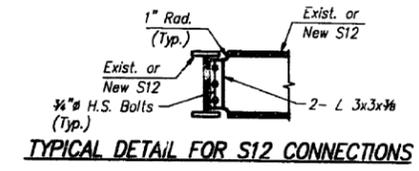
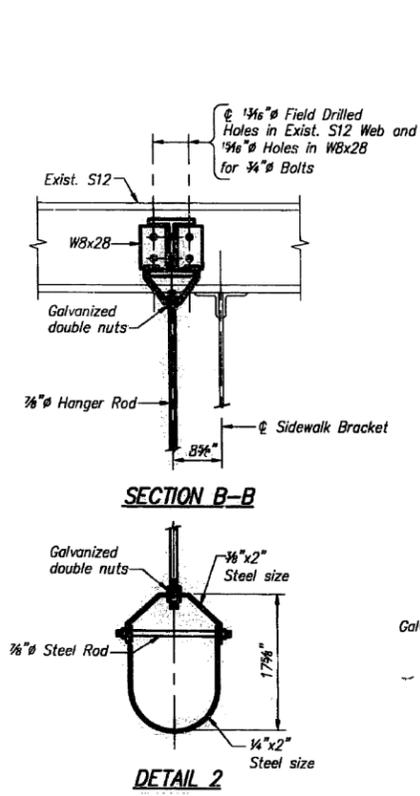
REVISED	REVISION	DATE	BY	CHKD	SHEET NO.
876	K(B&C)2-BY(90)	COOK	60	5	43 SHEETS
SHEET NO. 34					



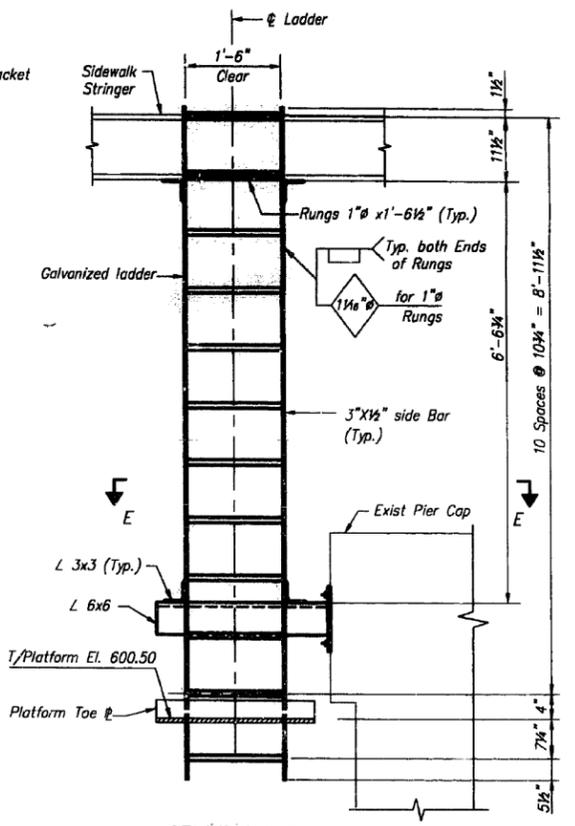
\* The Contractor shall check these locations for any interferences and coordinate with Engineer before making any adjustments to them.



LATERAL GUSSET PLATE CONNECTING TRUSS TO LOWER LATERAL BRACING TO BE PAINTED (TYP.)

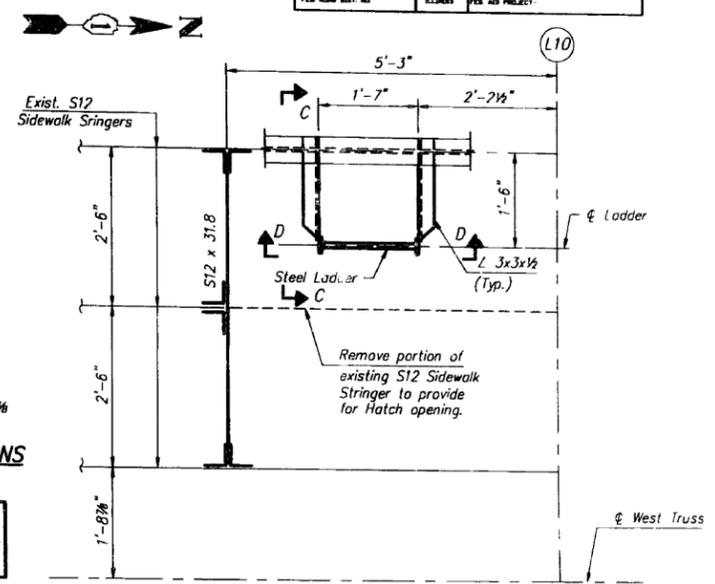


The cost of modifying existing sidewalk stringers and furnishing and installing new s12x31.8 for sidewalk hatch opening is included in the cost of "Furnishing and Erecting Structural Steel".

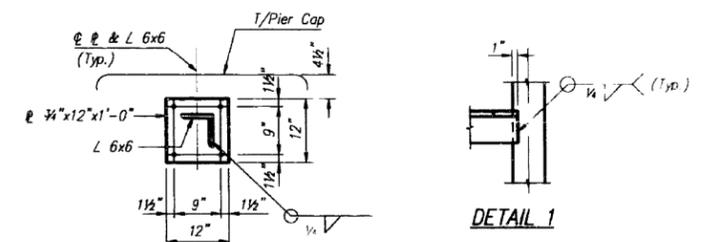
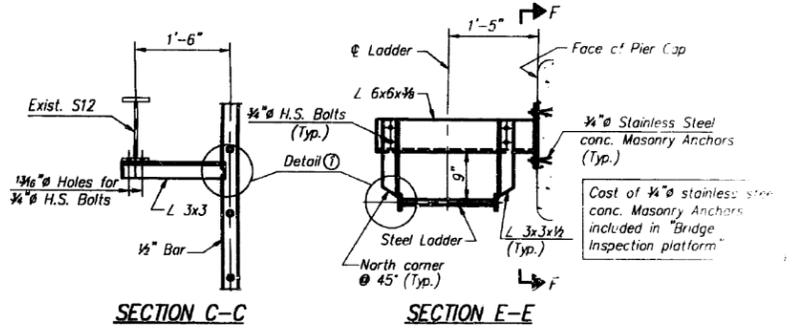


All parts of ladder including supports for ladder shall be galvanized.  
(1 ladder required, only at north Pier)

The cost of furnishing and installing the Platform Access Ladder is included in the cost of the pay item "Bridge Inspection Platform".



Note: Prior to removing portion of existing stringer, the stringer must be supported until new connections are made. Ladder and its attachment required only at North Pier.



Work Sheets 21 and 34 together  
The cost of furnishing and installing new supports for 12" water main including beams, pipe hanger, camps, fasteners and new transverse sidewalk framing members shall be included in cost of pay item "Water Main Hanger Assembly".

**FOR INFORMATION ONLY**

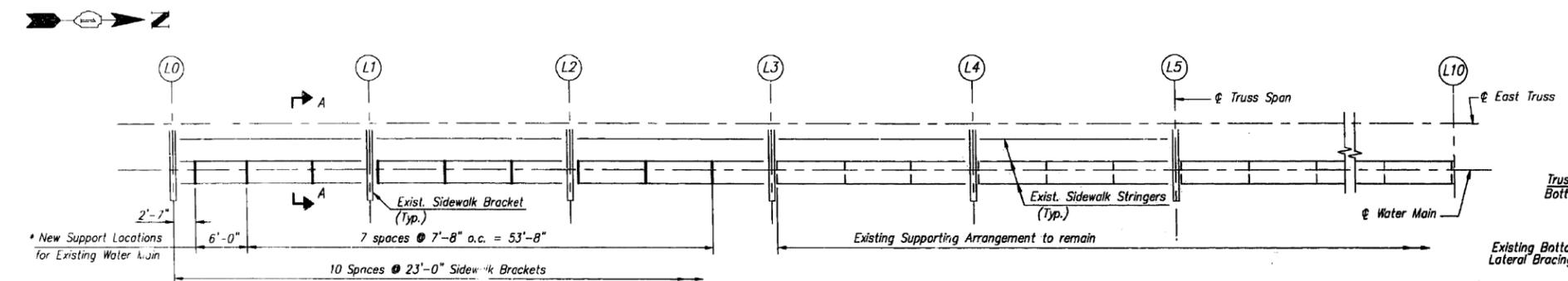
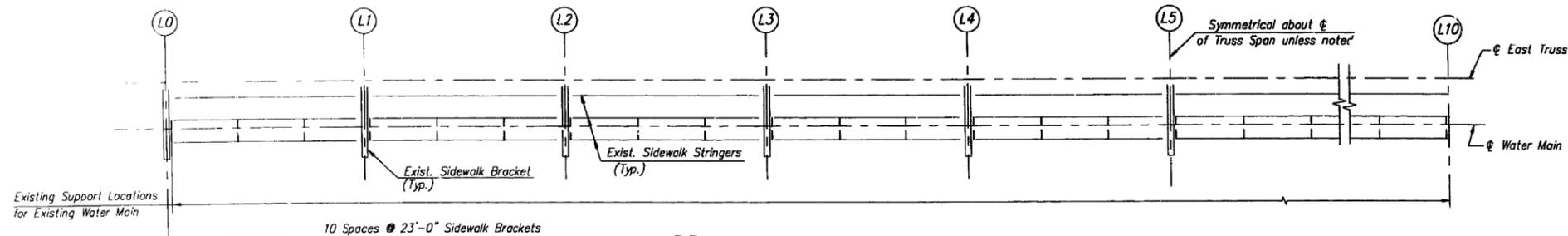
USER NAME =	DESIGNED -	REVISIONS
PLOT SCALE =	CHECKED -	REVISIONS
PLOT DATE	DRAWN -	REVISIONS
	CHECKED -	REVISIONS

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	22
CONTRACT NO. 62B47				
ILLINOIS FED. AID PROJECT				

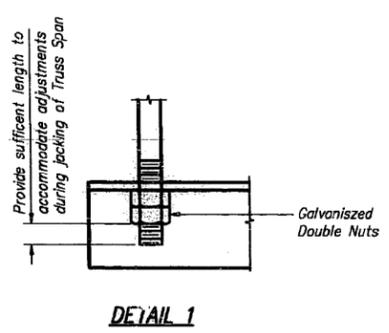
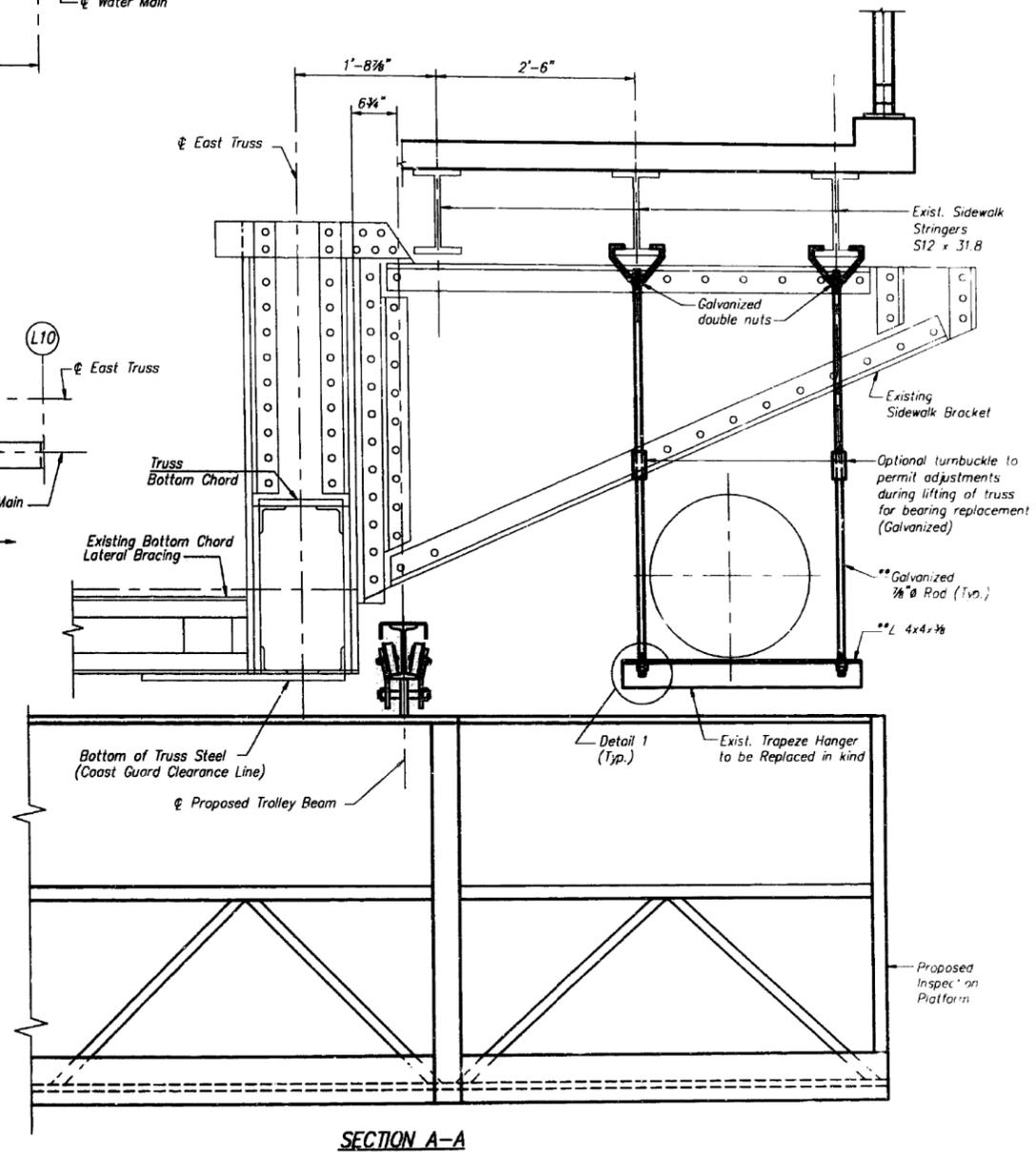
# WATER MAIN AND WATER MAIN SUPPORT NOT TO BE PAINTED

ROUTE NO.	SECTION	COUNTY	JOB NO.	SHEET NO.
F.A.P. 876	K(B&C)2-BY(90)	COOK	60	52
FED. ROAD DIST. NO.	BLANKETS	PER. AND PROJECT		

SHEET NO. 56  
43 SHEETS



\* The Contractor shall check these locations for any interferences and coordinate with Engineer before making any adjustments to them.



FOR INFORMATION ONLY

**COLLINS ENGINEERS**

USER NAME =	DESIGNED -	REVISED
	CHECKED -	REVISED
PLOT SCALE =	DRAWN -	REVISED
PLOT DATE =	CHECKED -	REVISED

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**24" WATER MAIN SUPPORTS  
STRUCTURE NUMBER: 016-0193**

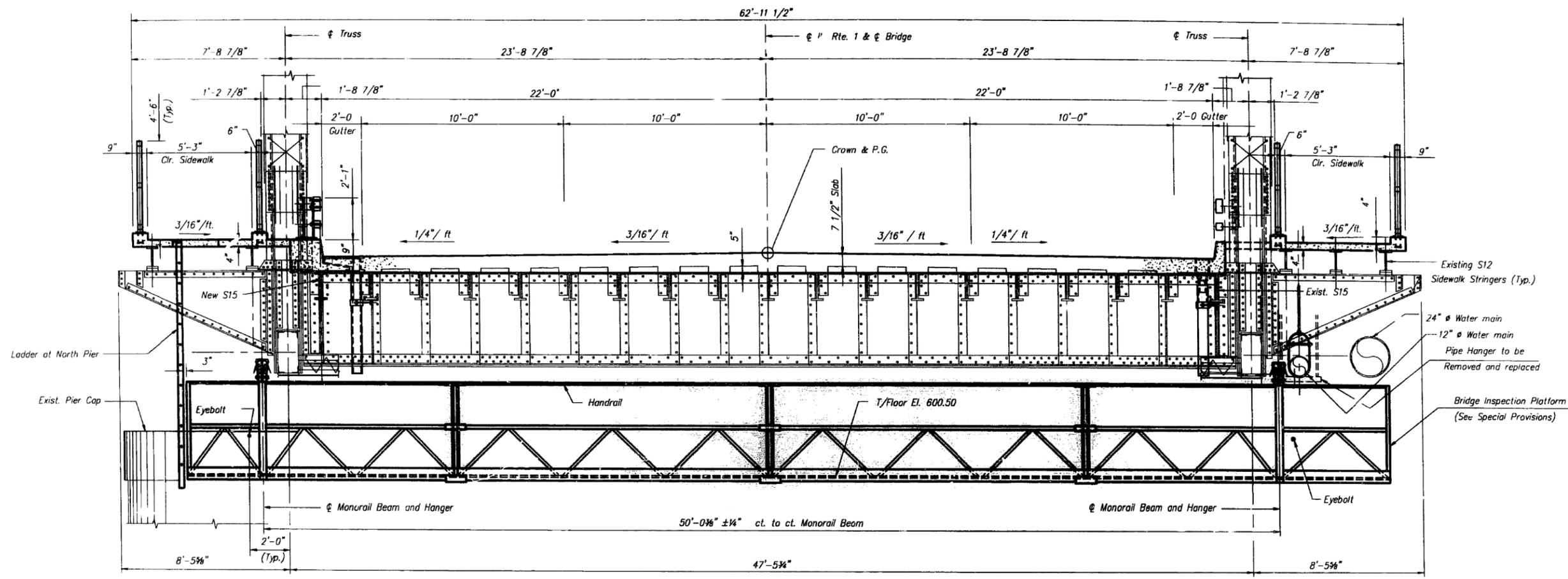
SHEET NO. S19 OF S20 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	23
CONTRACT NO. 62B47				

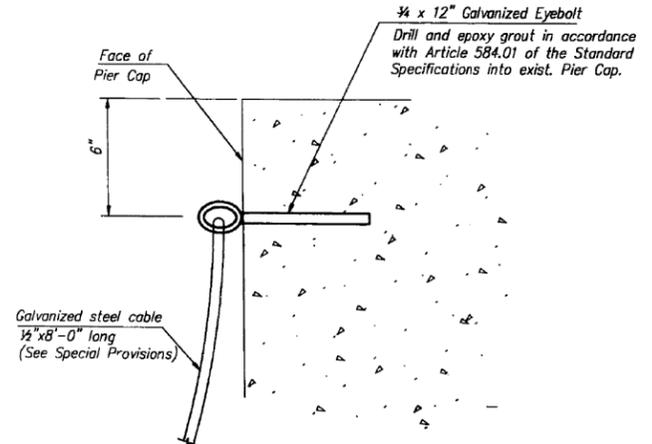
ILLINOIS FED. AID PROJECT

# TRUSS SPAN PLATFORM NOT TO BE PAINTED

ROUTE NO.	SECTION	COUNTY	SHEET	SHEET NO.
F.A.P. 876	K(B&C)2-BY(90)	COOK	60	51
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**GENERAL ELEVATION OF PLATFORM**  
(LOCKING NORTH)



FOR INFORMATION ONLY

**COLLINS ENGINEERS**

USER NAME =	DESIGNED -	REVISED
	CHECKED -	REVISED
PLOT SCALE =	DRAWN -	REVISED
PLOT DATE =	CHECKED -	REVISED

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**GENERAL ELEVATION OF PLATFORM**  
**STRUCTURE NUMBER: 016-0193**

SHEET NO. S20 OF S20 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	24
CONTRACT NO. 62B47				

ILLINOIS FED. AID PROJECT

**ROUTE MARKERS**

 FOR U.S. ROUTES  
M1-40-2424

 FOR ILLINOIS ROUTES  
M1-50-2424

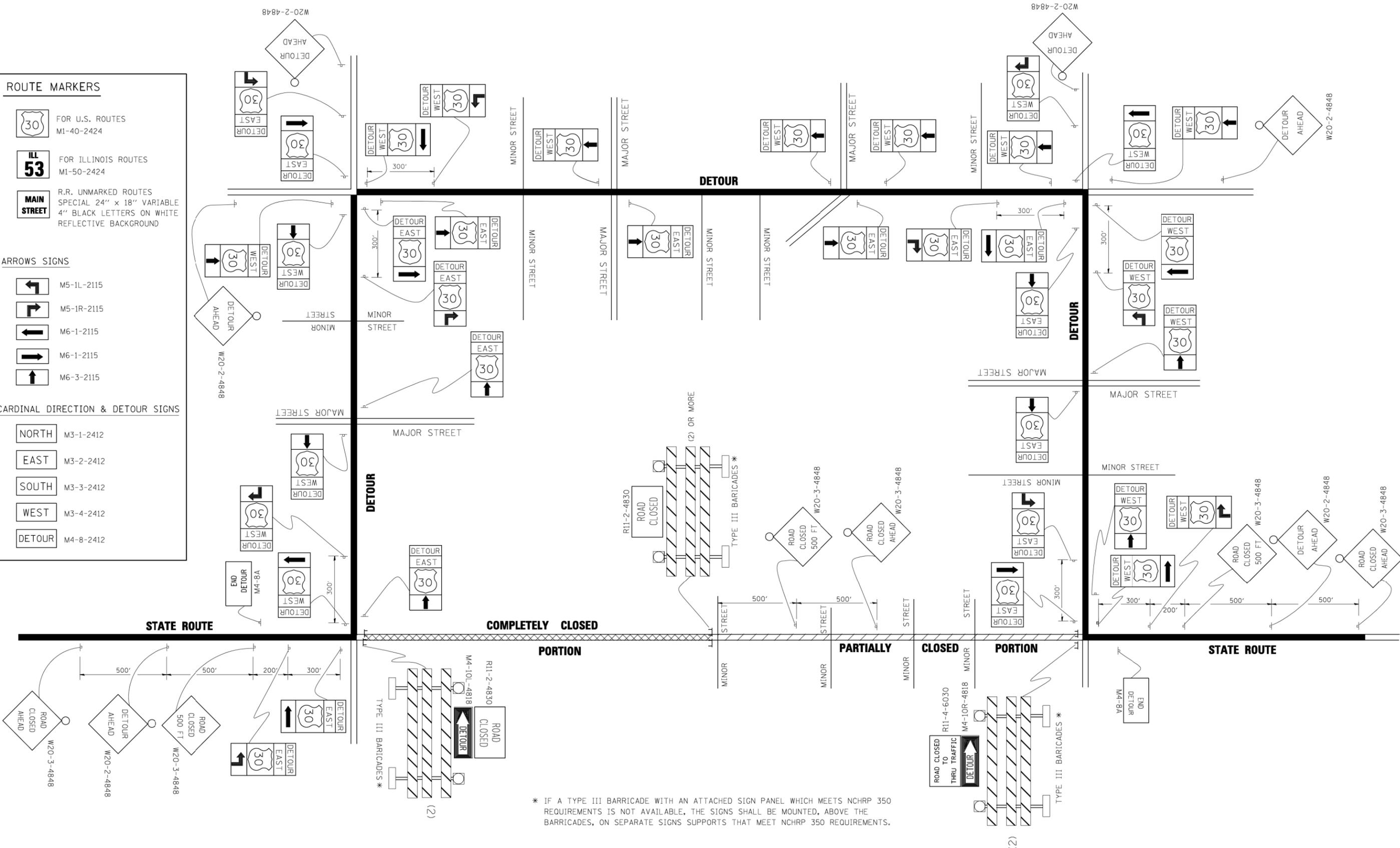
 R.R. UNMARKED ROUTES  
SPECIAL 24" x 18" VARIABLE  
4" BLACK LETTERS ON WHITE  
REFLECTIVE BACKGROUND

**ARROWS SIGNS**

-  M5-1L-2115
-  M5-1R-2115
-  M6-1-2115
-  M6-1-2115
-  M6-3-2115

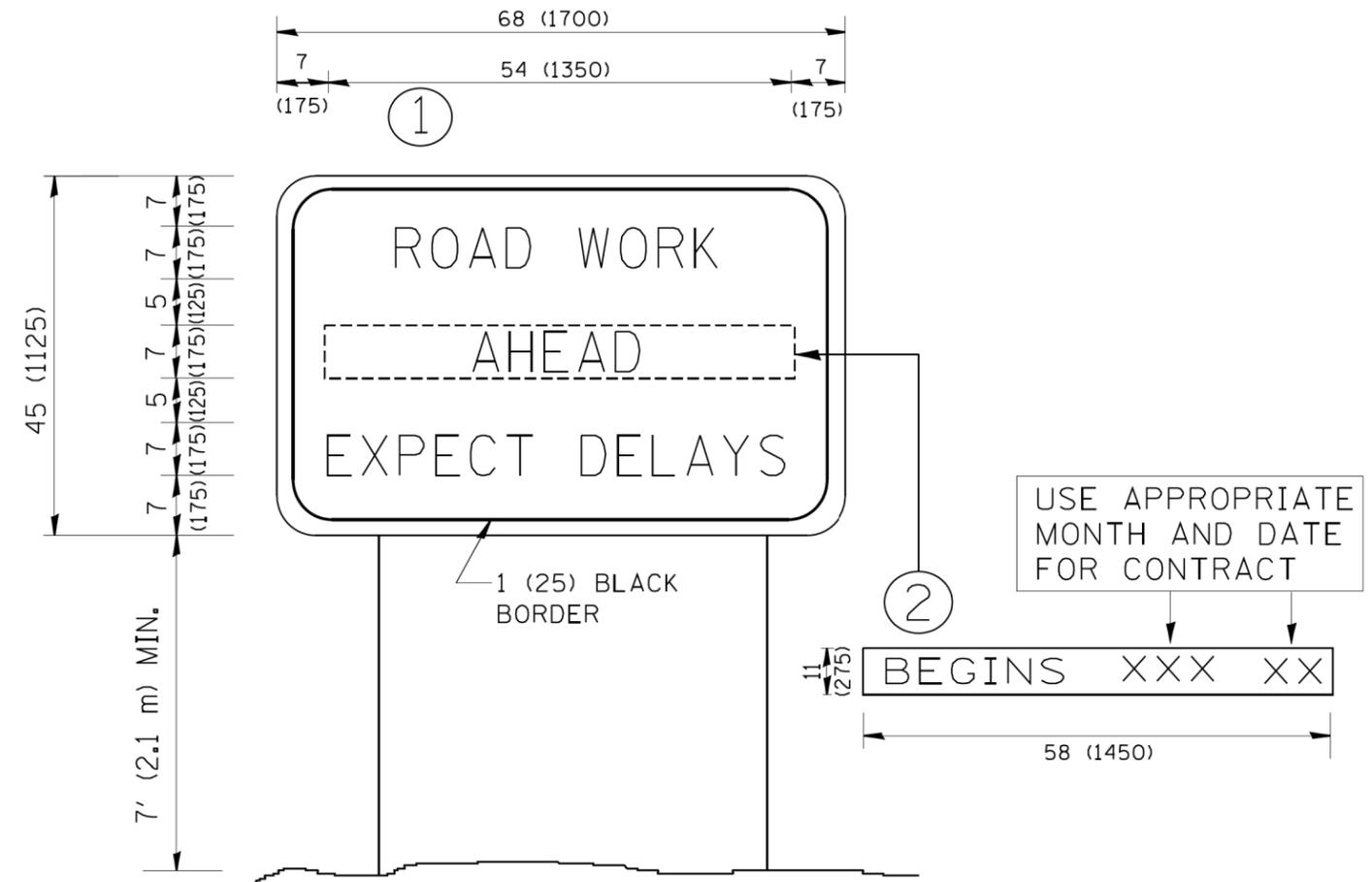
**CARDINAL DIRECTION & DETOUR SIGNS**

-  NORTH M3-1-2412
-  EAST M3-2-2412
-  SOUTH M3-3-2412
-  WEST M3-4-2412
-  DETOUR M4-8-2412



\* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

FILE NAME = c:\p\work\p\WIDOT\DRIVAKOSGN\d0108315\1421.dgn	USER NAME = d-ivakosgn	DESIGNED - DRAWN -	REVISED - 10-18-02 REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETOUR SIGNING FOR CLOSING STATE HIGHWAYS</b>			F.A.U. RTE. 3730	SECTION KIB&C 2-BR(15)	COUNTY COOK	TOTAL SHEETS 26	SHEET NO. 25
PLOT SCALE = 49.9999 / IN.	CHECKED -	REVISED -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	<b>TC-21</b>		CONTRACT NO. 62B47	
PLOT DATE = 9/14/2009	DATE -	REVISED -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gegl.enobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	K(B&C) 2-BR(15)	COOK	26	26
TC-22			CONTRACT NO. 62B47	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				