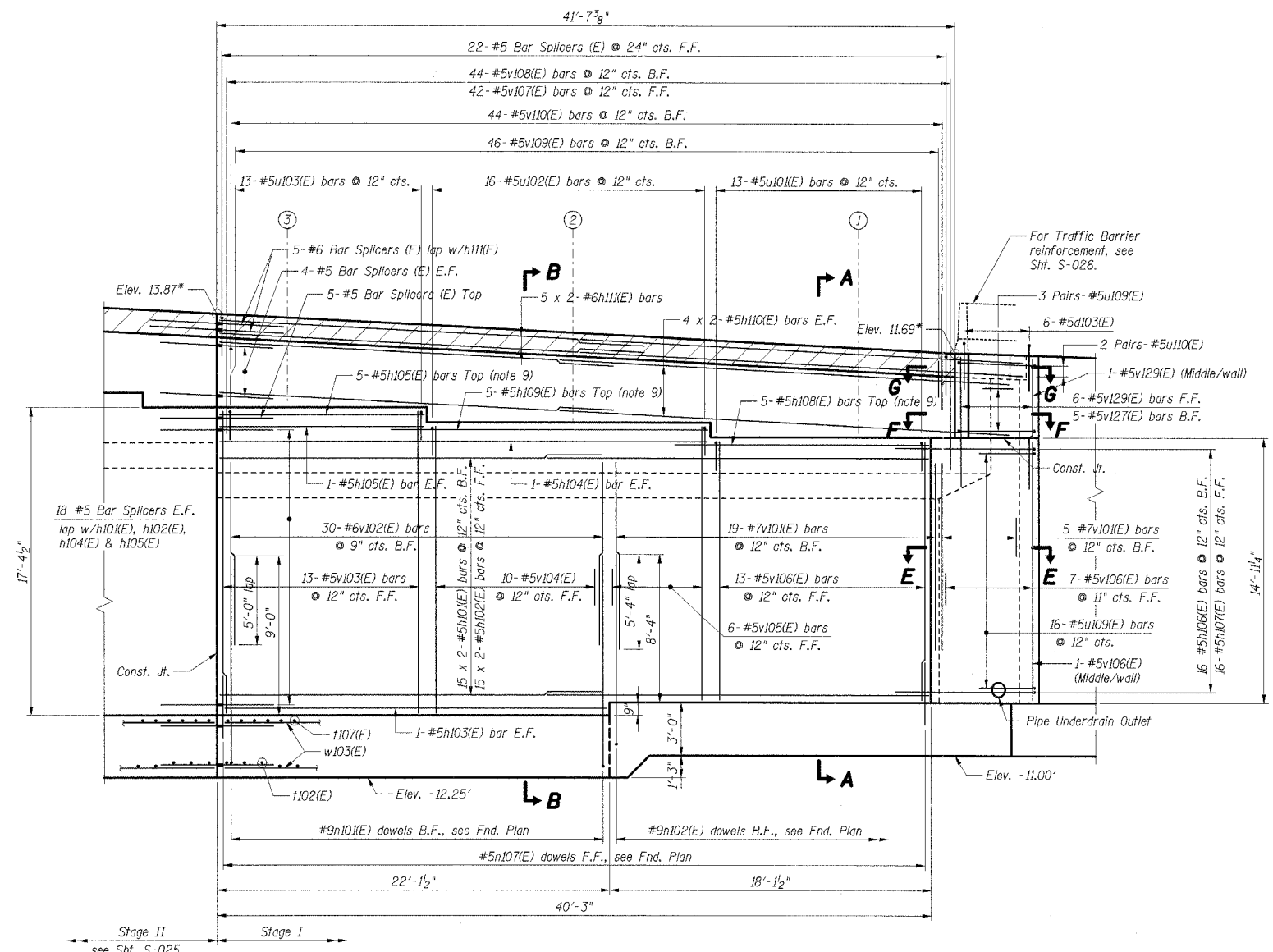


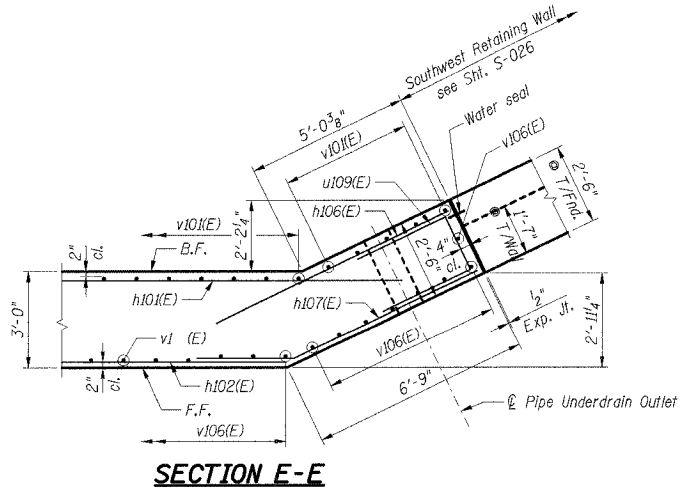
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	*	COOK	916	601
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		CONTRACT NO. 62304		
		*(1516.1, 1717, & 1818) R-4		



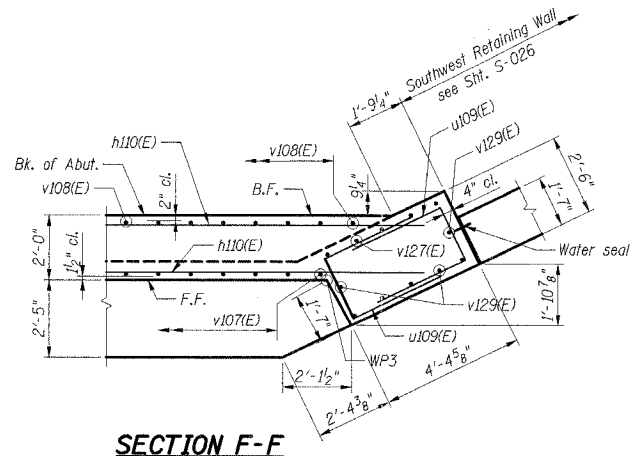
Bar	Min. Vert. Lap	Min. Hor. Lap
#5	2'-2"	3'-0"
#6	2'-7"	3'-7"
#7	3'-5"	-
#8	4'-6"	4'-6"
#9	5'-9"	-

ELEVATION - SOUTH ABUTMENT WALL I

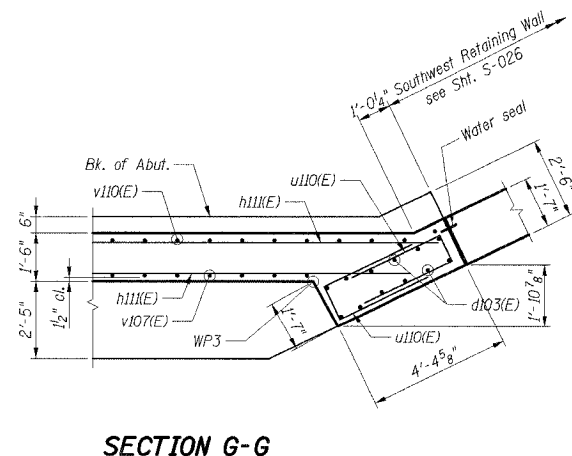
- Notes:**
1. Reinforcement bars designated (E) shall be epoxy coated.
 2. Bars indicates thus 4 x 2-#5 etc. Indicates 4 lines of bars with 2 lengths per line.
 3. Work this sheet with Shts. S-021 thru S-029.
 4. For Work Points, see Sht. S-021.
 5. For Sections A-A & B-B, see Sht. S-028.
 6. For Bar List & Bill of Material, see Sht. S-029.
 7. For typical wall details, see Sht. S-040.
 8. B.F. denotes Back Face.
 9. Space reinforcement to miss anchor bolts.



SECTION E-E



SECTION F-F



SECTION G-G

SHT. S-024

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 I-57 NB OVER WB CONNECTOR
 SN 016-0072 OLD, SN 016-2852 NEW
 STA. 238+73.54
 COOK COUNTY, SECTION (1516.1, 1717, & 1818) R-4

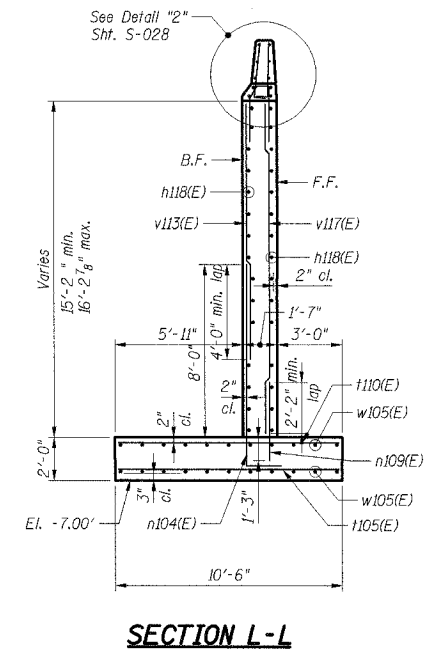
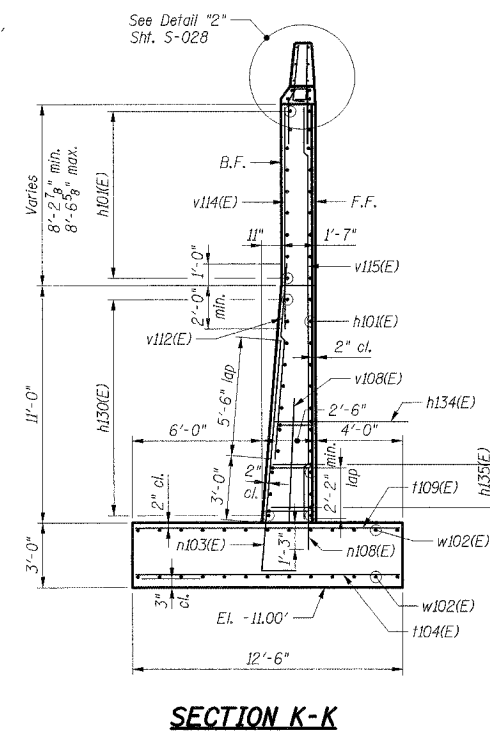
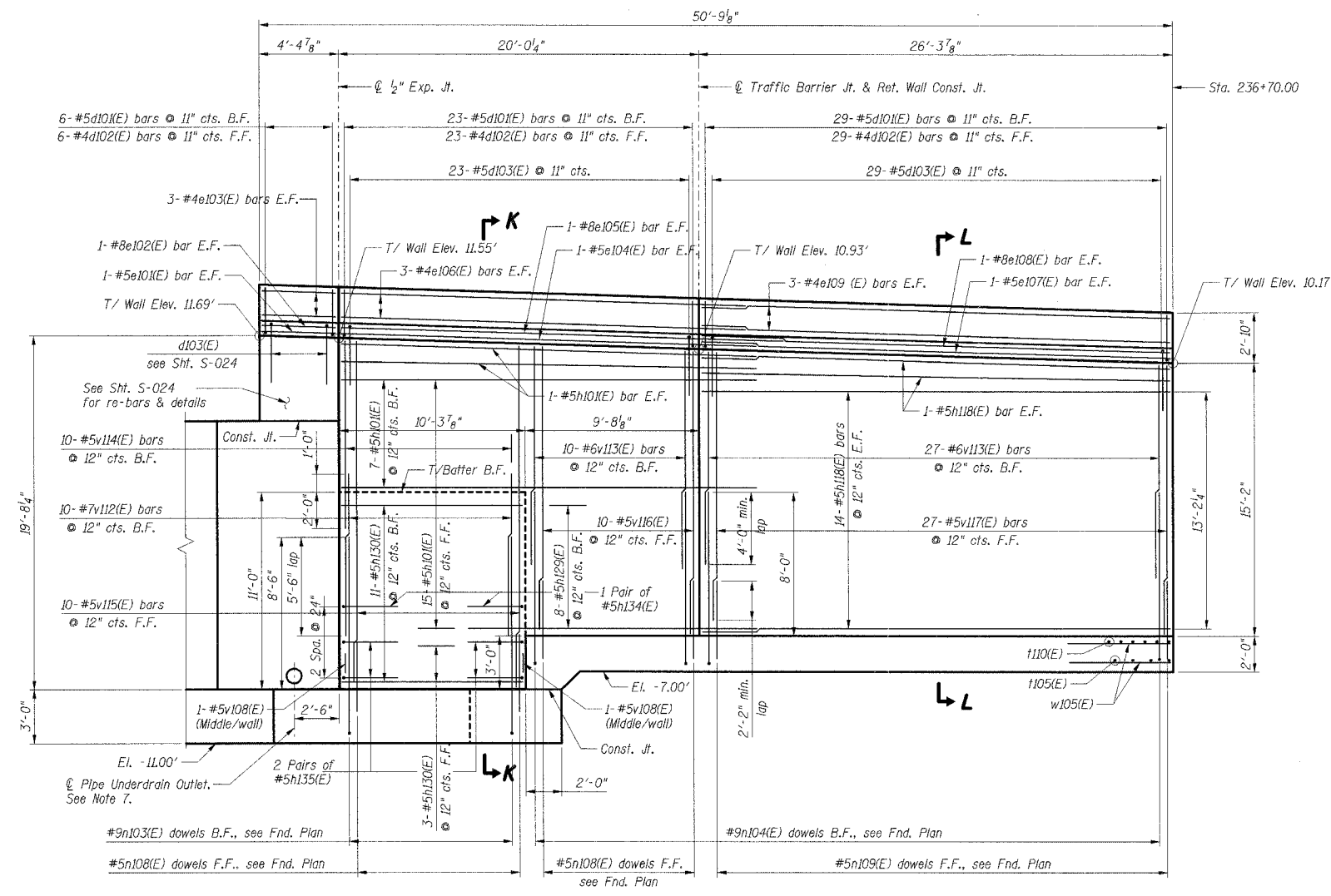
SOUTH ABUTMENT WALL I

DATE: 03/07/06
 DRAWN BY: HBJ
 CHECKED BY: TCU

TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312.616.0000

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 3-01-2006 15:58:55

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	*	COOK	916	603
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 62304				
*(1516.1, 1717, & 1818) R-4				



SOUTHWEST RETAINING WALL ELEVATION
(STAGE I)

Lap Lengths			
Bar	Min. Vert. Lap	Min. Hor. Lap	
#5	2'-2"	3'-0"	
#6	2'-7"	3'-7"	
#7	3'-5"	-	
#8	4'-6"	4'-6"	
#9	5'-9"	-	

- Notes:**
- Reinforcement bars designated (E) shall be epoxy coated.
 - Bars indicates thus 5 x 2-#5 etc. Indicates 5 lines of bars with 2 lengths per line.
 - Work this sheet with Shis. S-021 to S-029.
 - For Bar List & Bill of Material, see Shit. S-029.
 - For typical wall details, see Shit. S-040.
 - B.F. denotes Back Face.
 - F.F. denotes Front Face.
 - Tie pipe underdrain outlet to Structure number 5816 (see Drainage Plans). For typical underdrain details, see Shit. S-040.

SHT. S-026

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. ROUTE 57 (INTERSTATE 57)
I-57 NB OVER WB CONNECTOR
SN 016-0072 OLD. SN 016-2852 NEW
STA. 238+73.54
COOK COUNTY, SECTION (1516.1, 1717, & 1818) R-4

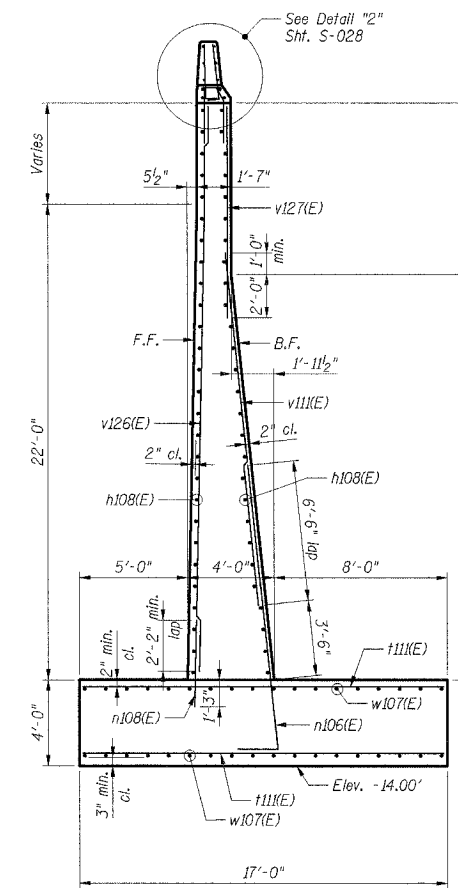
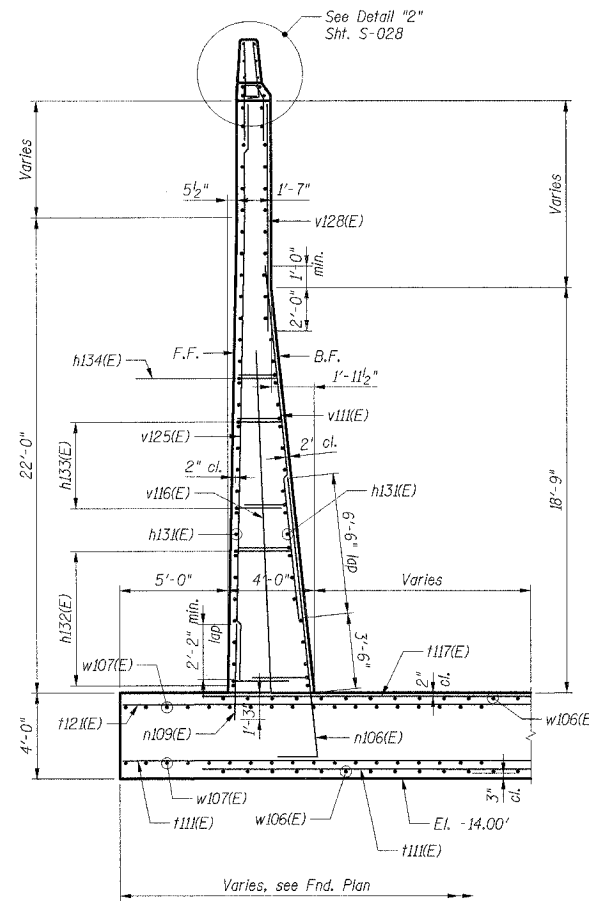
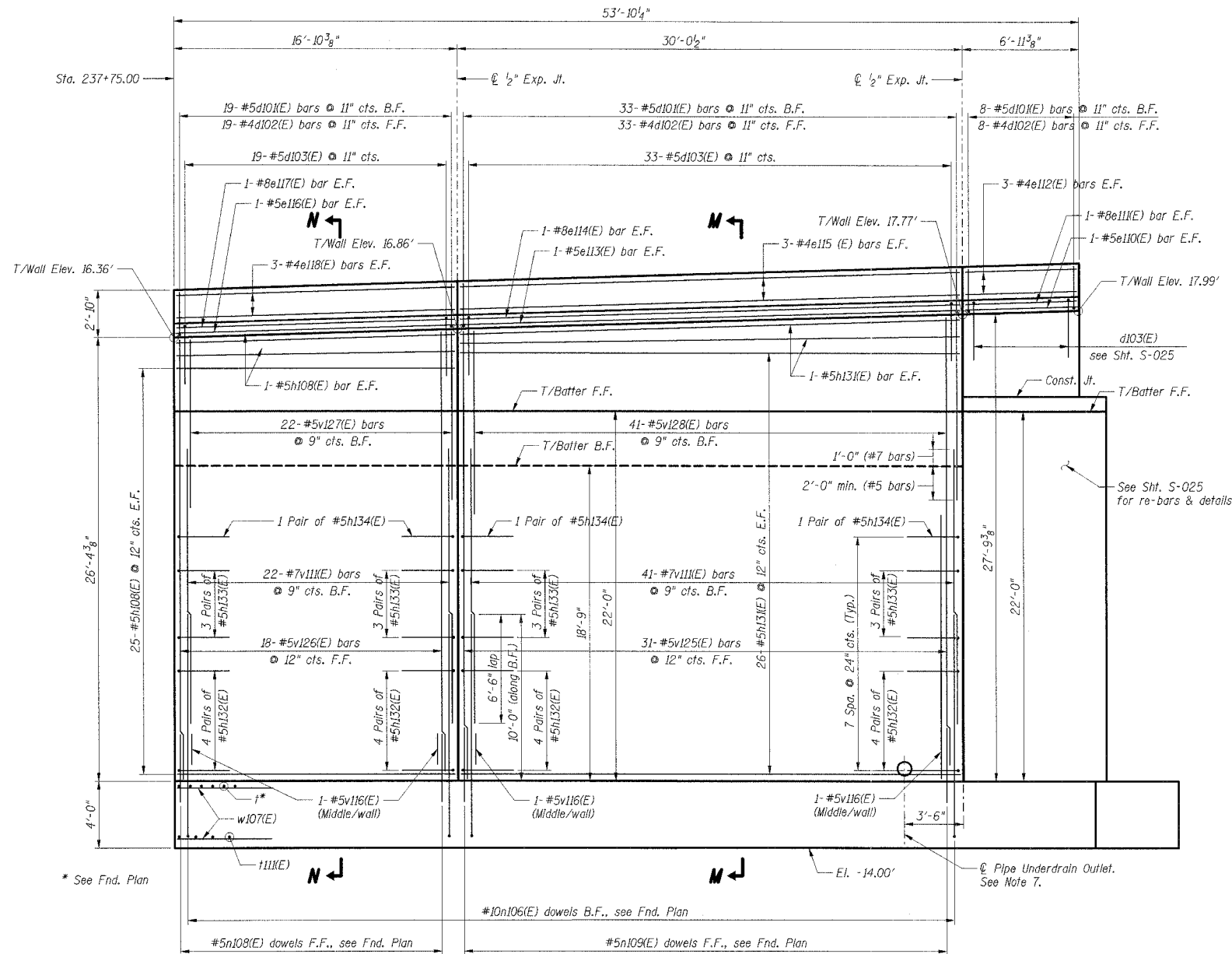
SOUTHWEST RETAINING WALL

DATE: 03/07/06

DRAWN BY: HBJ
CHECKED BY: TCU

TENG & ASSOCIATES, INC.
ENGINEERS ARCHITECTS & PLANNERS
205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312.541.0000

\\ARBORER.DGN, \\FINDOY88.DGN, \\ABT00Y30.DGN, \\GPE00Q1.DGN, \\ABT00REA.DGN
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SOUTHEAST RETAINING WALL ELEVATION
(STAGE II)

Bar	Lap Lengths	
	Min. Vert. Lap	Min. Hor. Lap
#5	2'-2"	3'-0"
#6	2'-7"	3'-7"
#7	3'-5"	-
#8	4'-6"	4'-6"
#9	5'-9"	-

Notes:

- Reinforcement bars designated (E) shall be epoxy coated.
- Bars indicates thus 5 x 2-#5 etc. indicates 5 lines of bars with 2 lengths per line.
- Work this sheet with Shts. S-021 to S-029.
- For Bar List & Bill of Material, see Sht. S-029.
- For typical wall details, see Sht. S-040.
- B.F. denotes Back Face.
F.F. denotes Front Face.
- Tie pipe underdrain outlet to existing structure @ 336+00.
38' LT Prop WB Connector (see Drainage Plans).
For typical underdrain details, see Sht. S-040.

SHT. S-027

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. ROUTE 57 (INTERSTATE 57)
I-57 NB OVER WB CONNECTOR
SN 016-0072 OLD, SN 016-2852 NEW
STA. 238+73.54
COOK COUNTY, SECTION (1516.1, 1717, & 1818) R-4

SOUTHEAST RETAINING WALL

DATE: 03/07/06
DRAWN BY: HBJ
CHECKED BY: TCU

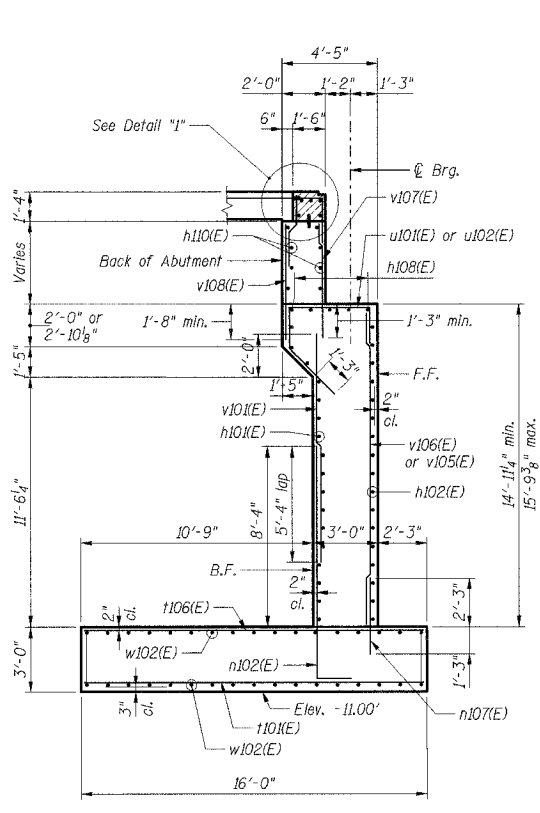
TENG

TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312.467.0000

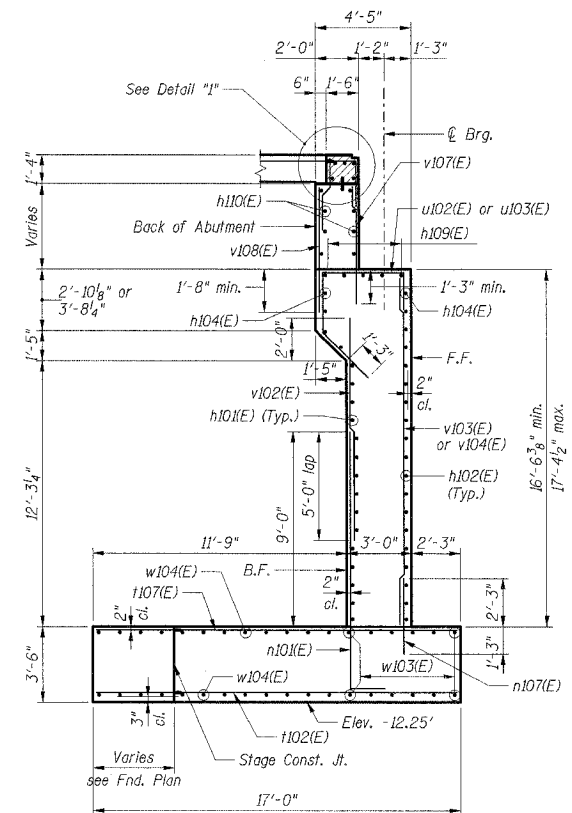
LDCTC

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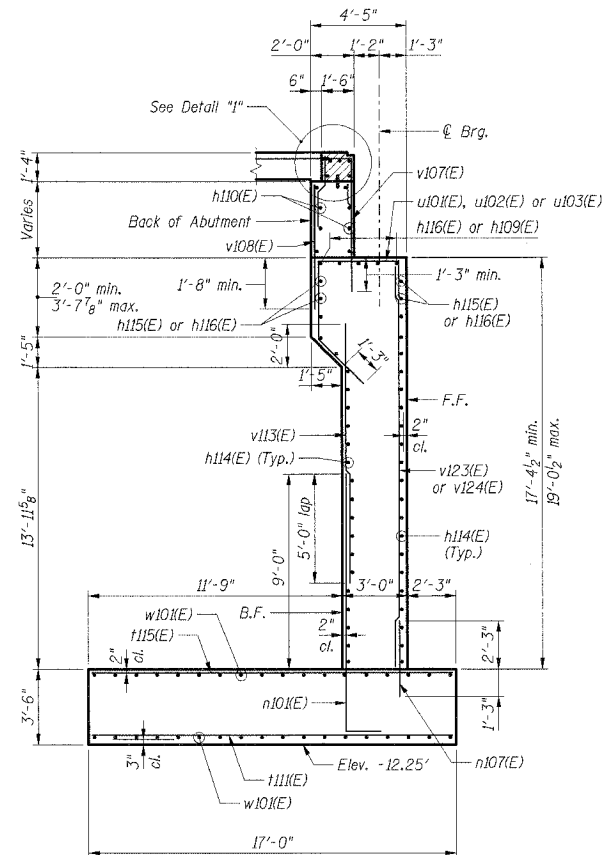
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57		COOK	916	605
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 62304				
*(1516.1, 1717, & 1818) R-4				



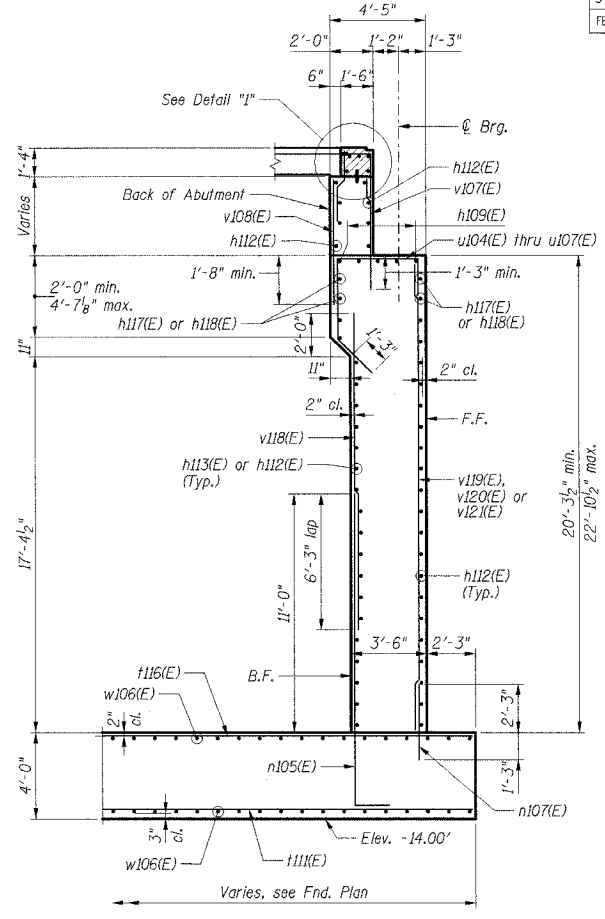
SECTION A-A



SECTION B-B

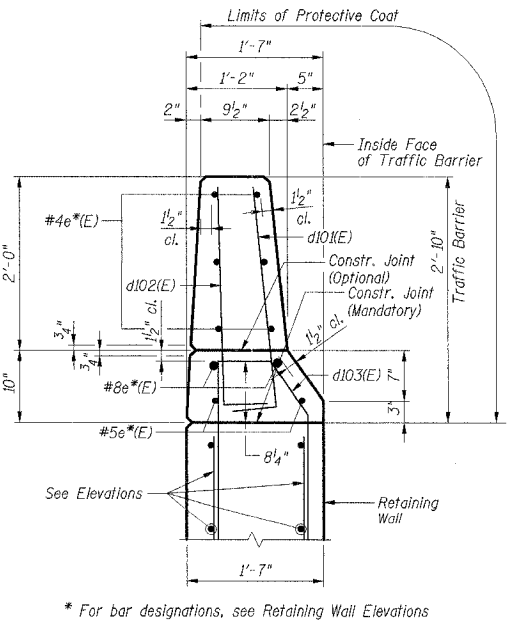
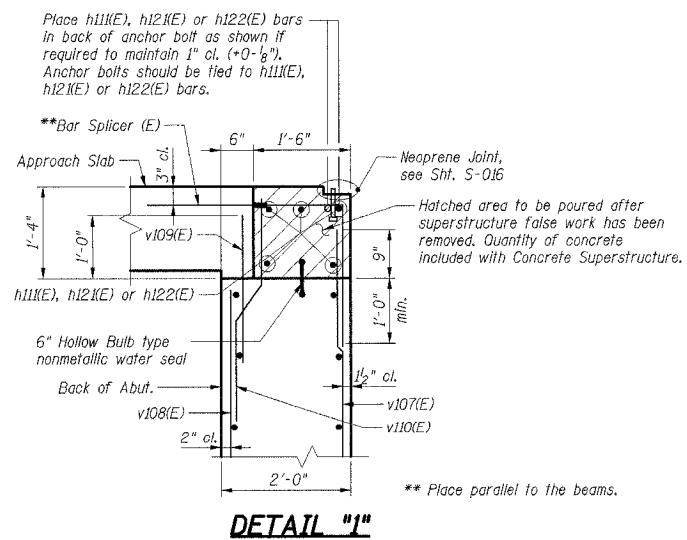


SECTION C-C



SECTION D-D

- Notes:**
1. Reinforcement bars designated (E) shall be epoxy coated.
 2. Work this sheet with Shts. S-021 to S-029.
 3. For Bar List & Bill of Materials, see Sht. S-029.
 4. B.F. denotes Back Face.
F.F. denotes Front Face.
 5. Space reinforcement to miss anchor bolts.
 6. For typical wall details, see Sht. S-040.



SHT. S-028

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 I-57 NB OVER WB CONNECTOR
 SN 016-0072 OLD, SN 016-2852 NEW
 STA. 238+73.54
 COOK COUNTY, SECTION (1516.1, 1717, & 1818) R-4

SOUTH ABUTMENT DETAILS I
 DATE: 03/07/06
 DRAWN BY: HBJ
 CHECKED BY: TCU

TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 208 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE 312.461.2000

UDETC
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SOUTH ABUTMENT - BAR LIST

Bar	No.		Size	Length	Shape
	Stage I	Stage II			
d101(E)	58	60	#5	3'-0"	[Shape]
d102(E)	58	60	#4	3'-0"	[Shape]
d103(E)	58	60	#5	7'-5"	[Shape]
e101(E)	2		#5	4'-4"	[Shape]
e102(E)	2		#8	4'-4"	[Shape]
e103(E)	6		#4	4'-4"	[Shape]
e104(E)	2		#5	23'-0"	[Shape]
e105(E)	2		#8	24'-6"	[Shape]
e106(E)	6		#4	22'-5"	[Shape]
e107(E)	2		#5	26'-0"	[Shape]
e108(E)	2		#8	26'-0"	[Shape]
e109(E)	6		#4	26'-0"	[Shape]
e110(E)		2	#5	6'-7"	[Shape]
e111(E)		2	#8	6'-7"	[Shape]
e112(E)	6		#4	6'-7"	[Shape]
e113(E)	2		#5	29'-8"	[Shape]
e114(E)	2		#8	29'-8"	[Shape]
e115(E)	6		#4	29'-8"	[Shape]
e116(E)	2		#5	16'-6"	[Shape]
e117(E)	2		#8	16'-6"	[Shape]
e118(E)		6	#4	16'-6"	[Shape]
h101(E)	56		#5	23'-6"	[Shape]
h102(E)	30		#5	21'-3"	[Shape]
h103(E)	2		#5	21'-10"	[Shape]
h104(E)	2		#5	27'-6"	[Shape]
h105(E)	7		#5	11'-6"	[Shape]
h106(E)	16		#5	8'-0"	[Shape]
h107(E)	16		#5	9'-6"	[Shape]
h108(E)	5	54	#5	16'-6"	[Shape]
h109(E)	5		#5	17'-9"	[Shape]
h110(E)	16		#5	24'-0"	[Shape]
h111(E)	20		#6	24'-6"	[Shape]
h112(E)		76	#5	24'-9"	[Shape]
h113(E)		20	#5	19'-9"	[Shape]
h114(E)	42		#5	31'-10"	[Shape]
h115(E)	2		#5	28'-0"	[Shape]
h116(E)	7		#5	11'-8"	[Shape]
h117(E)	4		#5	22'-9"	[Shape]
h118(E)	2		#5	26'-0"	[Shape]
h119(E)	7		#5	10'-2"	[Shape]
h120(E)	10		#5	6'-0"	[Shape]
h121(E)	5		#6	31'-10"	[Shape]
h122(E)	10		#6	24'-0"	[Shape]
h123(E)	24		#5	6'-0"	[Shape]
h124(E)	24		#5	17'-6"	[Shape]
h125(E)	4		#5	12'-4"	[Shape]
h126(E)	6		#5	4'-3"	[Shape]
h127(E)	2		#5	3'-4"	[Shape]
h128(E)	4		#5	6'-7"	[Shape]
h129(E)	8		#5	16'-0"	[Shape]
h130(E)	14		#5	10'-0"	[Shape]
h131(E)		56	#5	29'-8"	[Shape]
h132(E)		32	#5	5'-7"	[Shape]
h133(E)		24	#5	5'-0"	[Shape]
h134(E)	4		#5	4'-9"	[Shape]
h135(E)	8		#5	4'-11"	[Shape]
n101(E)	30	43	#9	13'-10"	[Shape]
n102(E)	24		#9	12'-8"	[Shape]
n103(E)	11		#9	12'-10"	[Shape]
n104(E)	37		#9	11'-4"	[Shape]
n105(E)		52	#10	16'-7"	[Shape]
n106(E)		63	#10	15'-7"	[Shape]
n107(E)	52	102	#5	3'-6"	[Shape]
n108(E)	21	18	#5	4'-0"	[Shape]
n109(E)	27	31	#5	4'-6"	[Shape]

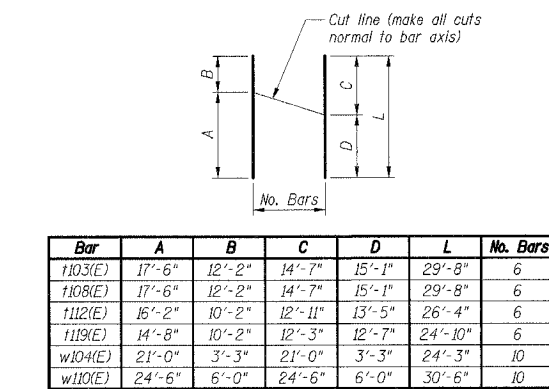
SOUTH ABUTMENT - BAR LIST

Bar	No.		Size	Length	Shape
	Stage I	Stage II			
v101(E)	23		#8	15'-8"	[Shape]
v103(E)	6		#6	29'-8"	[Shape]
v104(E)	5		#6	12'-2"	[Shape]
v105(E)	37		#6	10'-2"	[Shape]
v106(E)	23		#9	15'-8"	[Shape]
v108(E)	6		#7	29'-8"	[Shape]
v109(E)	5		#7	12'-2"	[Shape]
v110(E)	37		#8	10'-2"	[Shape]
v111(E)		125	#8	16'-8"	[Shape]
v112(E)	6		#8	26'-4"	[Shape]
v115(E)	43		#9	16'-8"	[Shape]
v116(E)	22		#10	24'-0"	[Shape]
v117(E)	22		#10	19'-0"	[Shape]
v118(E)	12		#10	14'-0"	[Shape]
v119(E)	6		#10	24'-10"	[Shape]
v120(E)	12		#10	17'-0"	[Shape]
v121(E)	14		#10	19'-6"	[Shape]
v122(E)	14		#10	21'-6"	[Shape]
v123(E)	14		#8	24'-0"	[Shape]
u101(E)	13	4	#5	11'-3"	[Shape]
u102(E)	16	16	#5	12'-1"	[Shape]
u103(E)	13	13	#5	12'-11"	[Shape]
u104(E)	4		#5	10'-6"	[Shape]
u105(E)		16	#5	11'-4"	[Shape]
u106(E)		16	#5	12'-3"	[Shape]
u107(E)		4	#5	13'-1"	[Shape]
u108(E)		11	#5	10'-0"	[Shape]
u109(E)	22		#5	8'-1"	[Shape]
u110(E)	4		#5	7'-2"	[Shape]
u111(E)		9	#5	9'-1"	[Shape]
u112(E)		8	#5	8'-7"	[Shape]
u113(E)		4	#5	7'-7"	[Shape]
v101(E)	24		#7	10'-7"	[Shape]
v102(E)	30		#6	10'-4"	[Shape]
v103(E)	13	4	#5	17'-2"	[Shape]
v104(E)	10		#5	16'-4"	[Shape]
v105(E)	6		#5	15'-7"	[Shape]
v106(E)	21		#5	14'-9"	[Shape]
v107(E)	42	80	#5	6'-6"	[Shape]
v108(E)	46	73	#5	5'-8"	[Shape]
v109(E)	46	75	#5	2'-6"	[Shape]
v110(E)	44	78	#5	3'-8"	[Shape]
v111(E)		70	#7	16'-6"	[Shape]
v112(E)	10		#7	9'-0"	[Shape]
v113(E)	37	43	#6	12'-0"	[Shape]
v114(E)	10		#5	10'-6"	[Shape]
v115(E)	10		#5	19'-0"	[Shape]
v116(E)	10	4	#5	15'-9"	[Shape]
v117(E)	27		#5	15'-0"	[Shape]
v118(E)		52	#7	14'-9"	[Shape]
v119(E)	27		#5	22'-8"	[Shape]
v120(E)	16		#5	21'-10"	[Shape]
v121(E)	16		#5	20'-11"	[Shape]
v122(E)	5		#5	20'-1"	[Shape]
v123(E)	14		#5	18'-10"	[Shape]
v124(E)	16		#5	18'-0"	[Shape]
v125(E)	31		#5	26'-9"	[Shape]
v126(E)	18		#5	26'-2"	[Shape]
v127(E)	5	23	#5	10'-0"	[Shape]
v128(E)		41	#5	11'-0"	[Shape]
v129(E)	7	8	#5	7'-0"	[Shape]

SOUTH ABUTMENT - BAR LIST

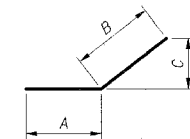
Bar	No.		Size	Length	Shape
	Stage I	Stage II			
w101(E)	22	36	#5	31'-10"	[Shape]
w102(E)	50		#5	16'-0"	[Shape]
w103(E)	12		#5	22'-10"	[Shape]
w104(E)	10		#5	24'-3"	[Shape]
w105(E)	44		#5	19'-6"	[Shape]
w106(E)		68	#5	26'-0"	[Shape]
w107(E)		72	#5	29'-0"	[Shape]
w108(E)	4		#5	33'-0"	[Shape]
w109(E)	10		#5	17'-0"	[Shape]
w110(E)	10		#5	30'-6"	[Shape]
w111(E)	2		#5	25'-3"	[Shape]
w112(E)	26		#5	11'-9"	[Shape]
w113(E)	18	18	#5	6'-0"	[Shape]
w114(E)		18	#5	6'-0"	[Shape]

BAR CUTTING DIAGRAM



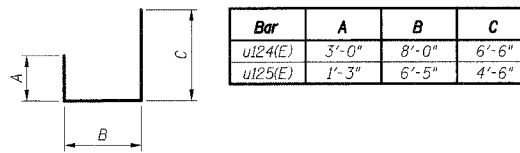
Bar	A	B
d101(E)	2'-6"	6"
d102(E)	2'-6"	6"
h127(E)	2'-6"	10"
n101(E)	12'-3"	1'-7"
n102(E)	11'-1"	1'-7"
n104(E)	9'-9"	1'-7"
n105(E)	14'-9"	1'-10"
w114(E)	3'-0"	3'-0"

Bars d101(E), d102(E), h127(E), n101(E), n102(E), n104(E), n105(E) & w114(E)

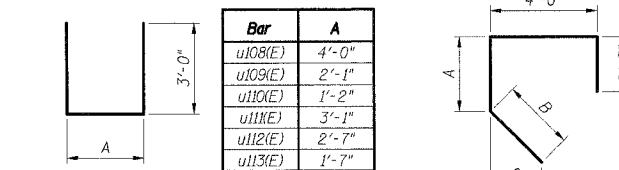


Bar	A	B	C
h107(E)	6'-6"	3'-0"	1'-3 1/4"
h123(E)	3'-0"	3'-0"	2'-7 3/4"
h126(E)	3'-0"	1'-3"	1'-1"
w113(E)	3'-0"	3'-0"	2'-1 1/2"

Bars h107(E), h123(E), h126 & w113(E)

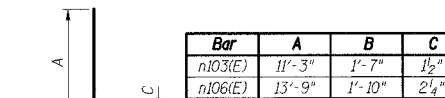


Bars u124(E) & u125(E)

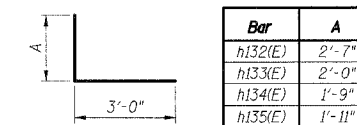


Bars u108(E) thru u113(E)

Bars u101(E) thru u107(E)



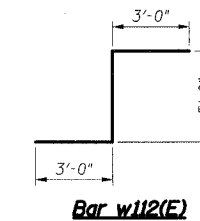
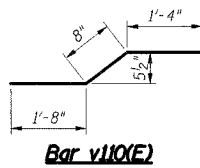
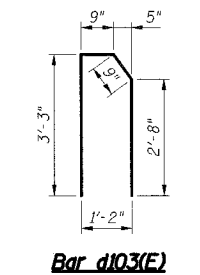
Bars n103(E) & n106(E)



Bars h132(E) thru h135(E)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	*	COOK	916	606

CONTRACT NO. 62304
*(1516.1, 1717, & 1818) R-4



BILL OF MATERIAL

Item	Unit	Stage I	Stage II	Total
Concrete Structures	Cu yd	279	651	930
Reinforcement Bars, Epoxy Coated	lb	23,250	56,670	79,920
Protective Coat	Sq yd			393
Bridge Seat Sealer	Sq ft			308
Structure Excavation	Cu yd			1,987
Geocomposite Wall Drain	Sq yd			529
Porous Granular Embankment (Special)	Cu yd			614
Pipe Underdrains For Structure 6"	Ft			289
Sub-base Granular Material, Type C 6"	Sq yd			361

SHT. S-029

REVISIONS	NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. ROUTE 57 (INTERSTATE 57)
I-57 NB OVER WB CONNECTOR
SN 016-0072 OLD, SN 016-2852 NEW
STA. 238+73.54
COOK COUNTY, SECTION (1516.1, 1717, & 1818) R-4
SOUTH ABUTMENT DETAILS II
& BILL OF MATERIAL

DATE: 03/07/06

DRAWN BY: HBJ
CHECKED BY: TCU

TENG

TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
209 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312.641.8000

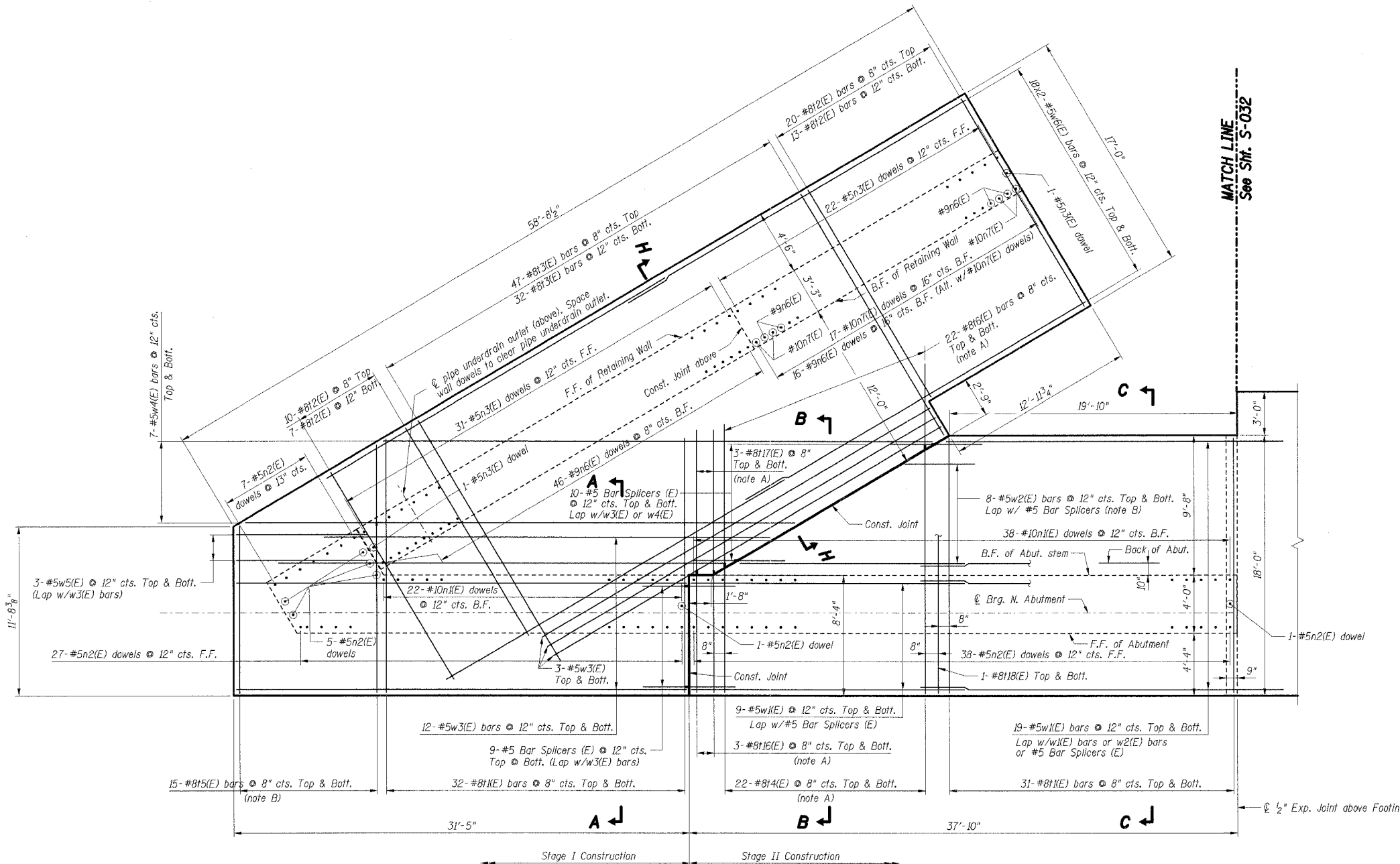
Notes:

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Work this Sht. with Shts. S-021 thru S-028.

UD/ETC

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57		COOK	916	608
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 62304				
*(1516.1, 1717, & 1818) R-4				



Re-bar Notes:

A. For variable length Bar Splicers, see Sht. S-043. Use half of cut bars in bottom of footing and remainder in top of footing.

B. Cut bars according to Cutting Diagram, Sht. S-039. Use half of bars in Bottom of Footing & remainder in Top of Footing.

Notes:

1. For Footing layout, see Sht. S-005.
2. Reinforcement bars designated (E) shall be epoxy coated.
3. Bars indicated thus 4 x 2-#5 etc. indicates 4 lines of bars with 2 lengths per line.
4. B.F. denotes Back Face, F.F. denotes Front Face.
5. For Section A-A, see Sht. S-033, for Sections B-B and C-C, see Sht. S-038.
6. For Bar List & Bill of Material, see Sht. S-039.
7. Work this sheet with Shts. S-030, S-032 & S-043.
8. For typical wall details, see Sht. S-040.
9. Maximum applied bearing pressure: 4.7 ksf @ Elev. -12.00.
10. For buried utilities, coordinate with Drainage and Utility plans in this Contract and in IDOT Contract No. 62593.

FOUNDATION PLAN I - NORTH ABUTMENT

SHT. S-031

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 I-57 NB OVER WB CONNECTOR
 SN 016-0072 OLD, SN 016-2852 NEW
 STA. 238+73.54
 COOK COUNTY, SECTION (1516.1, 1717, & 1818) R-4
 NORTH ABUTMENT
 FOUNDATION PLAN I

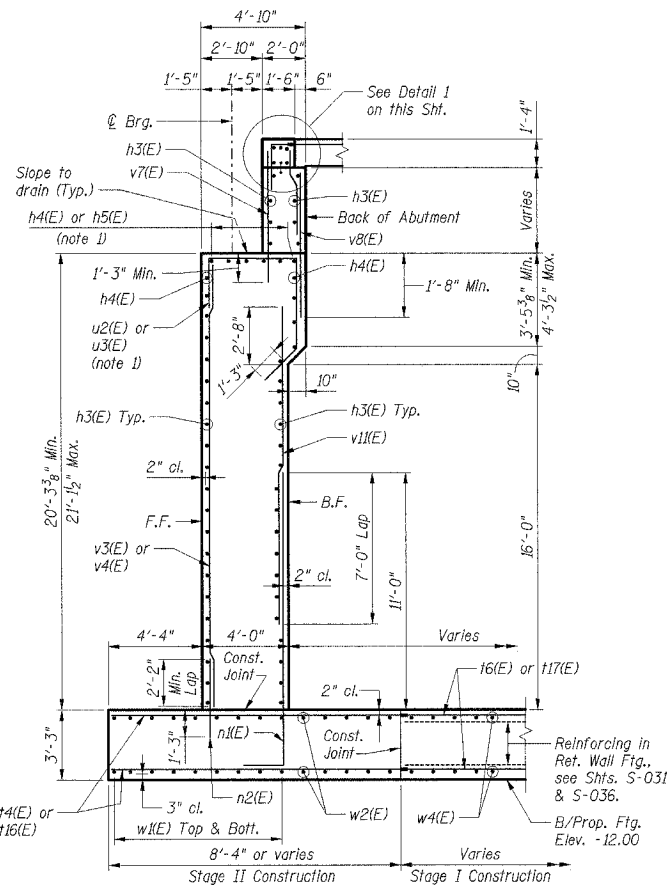
DATE: 03/07/06

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 CHECKED BY: TCU

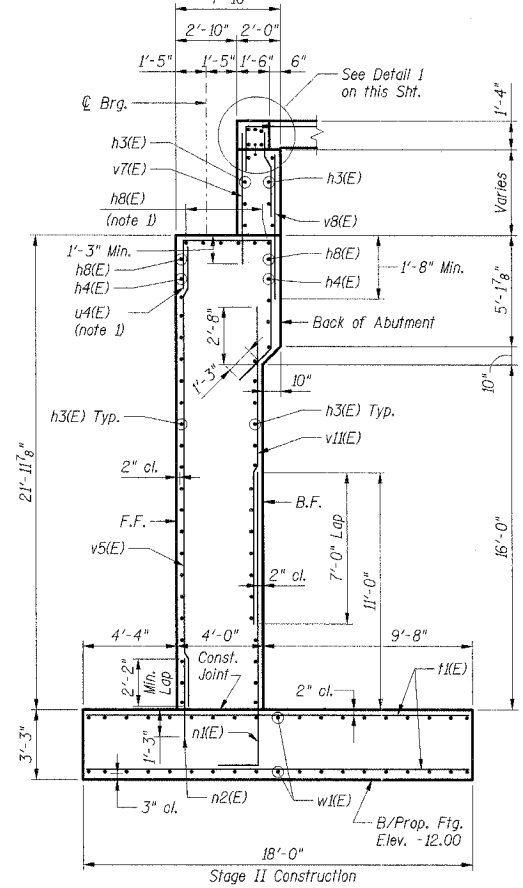
TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 202 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312.642.0000

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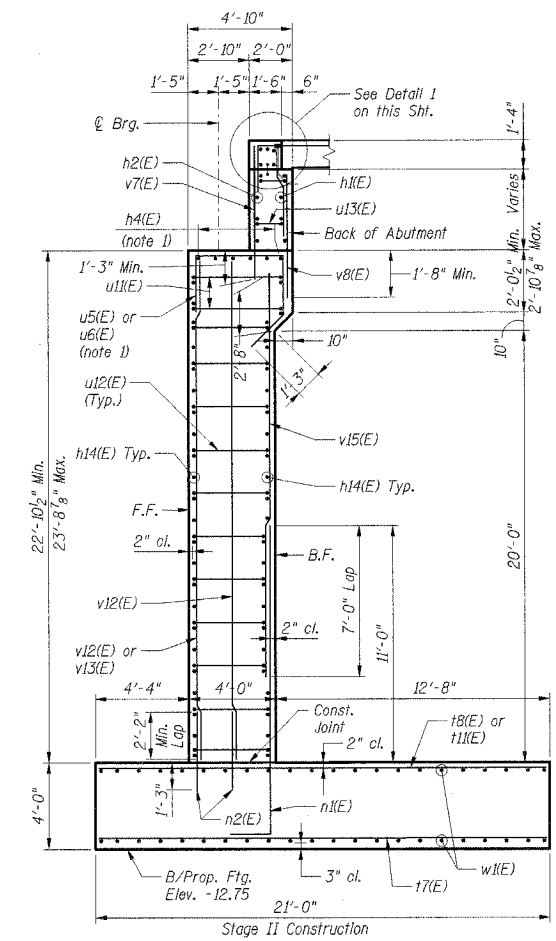
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	*	COOK	916	615
STA. TO STA.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
CONTRACT NO. 62304				
*(1516.1, 1717, & 1818) R-4				



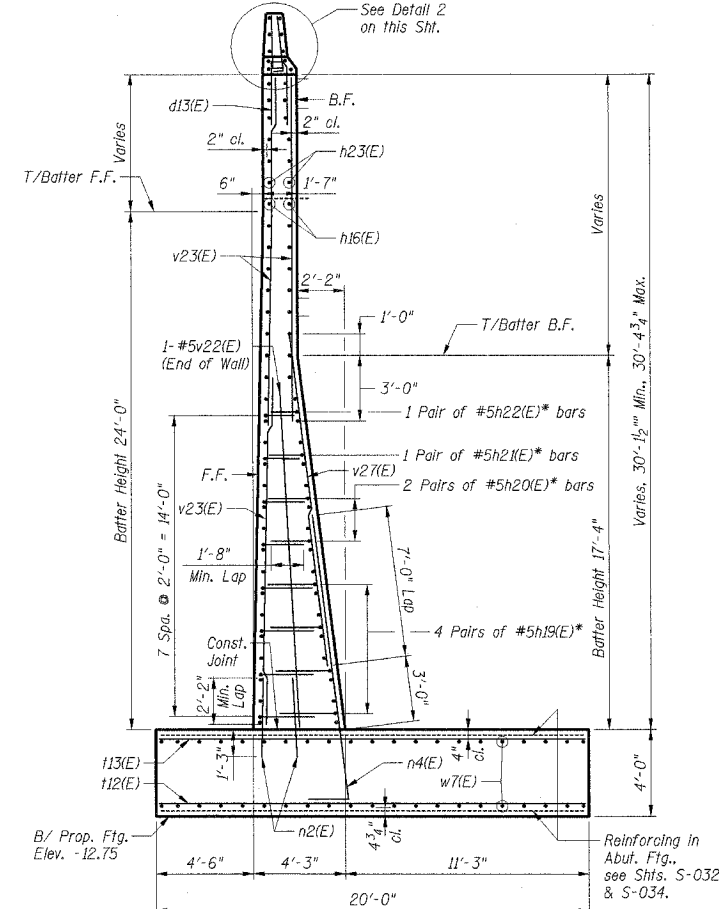
SECTION B-B



SECTION C-C



SECTION D-D



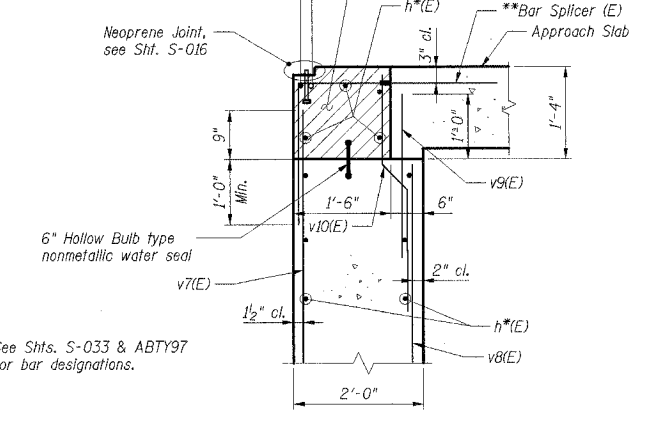
SECTION F-F

* Lap w/h16(E) bars (End of Wall)

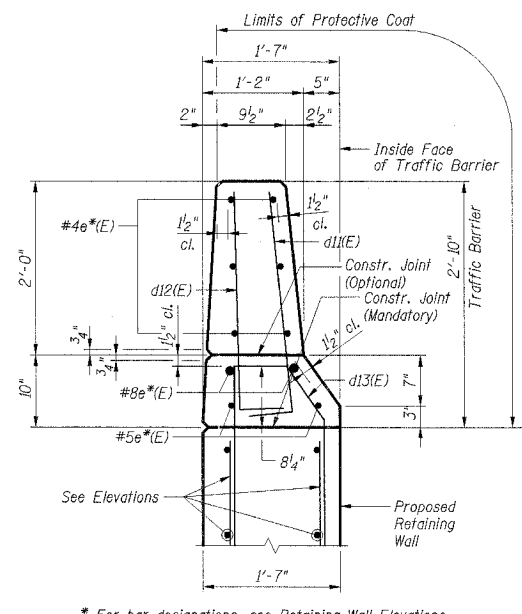
- Notes:**
- 1. Space reinforcement to miss anchor bolts.
 - 2. Reinforcement bars designated (E) shall be epoxy coated.
 - 3. F.F. denotes Front Face, B.F. denotes Back Face.
 - 4. For Bar List & Bill of Material, see Sht. S-039.
 - 5. Work this sheet with Shts. S-030 to S-039.
 - 6. For additional reinforcement, see Sht. S-037.
 - 7. For typical wall details, see Sht. S-040.

Place h11(E), h12(E) or h13(E) bars in back of anchor bolt as shown if required to maintain 1" cl. (+0-1/8"). Anchor bolts should be tied to h11(E), h12(E) or h13(E) bars.

Hatched area to be poured after superstructure false work has been removed. Quantity of concrete included with Concrete Superstructure.



DETAIL 1



DETAIL 2

* For bar designations, see Retaining Wall Elevations

SHT. S-038

REVISIONS	NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 I-57 NB OVER WB CONNECTOR
 SN 016-0072 OLD, SN 016-2852 NEW
 STA. 238+73.54
 COOK COUNTY, SECTION (1516.1, 1717, & 1818) R-4
 NORTH ABUTMENT DETAILS II

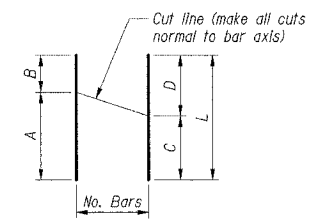
DATE: 03/07/06

DRAWN BY: FD
 CHECKED BY: TCU

TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312.467.0000

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BAR CUTTING DIAGRAM

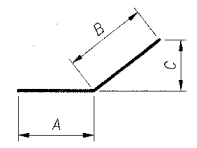


Bar	A	B	C	D	L	N bars
h17(E)	12'-9"	18'-8"	15'-1 1/2"	16'-3 1/2"	31'-5"	3
h18(E)	18'-6"	15'-8"	15'-8"	18'-6"	34'-2"	7
h15(E)	11'-6"	17'-0"	17'-0"	11'-6"	28'-6"	15
w2(E)	16'-6"	4'-6"	4'-6"	16'-6"	21'-0"	8

Bars h17(E), h18(E), h15(E) & w2(E)

Bar	A	B
d11(E)	0'-6"	2'-6"
d12(E)	0'-6"	2'-6"
h19(E)	2'-9"	3'-0"
h20(E)	2'-2"	3'-0"
h21(E)	1'-11"	3'-0"
h22(E)	1'-9"	3'-0"
h30(E)	0'-10"	3'-2"
h33(E)	2'-11"	2'-11"
n1(E)	1'-10"	14'-0"
w10(E)	2'-3"	5'-6"

Bars d11(E), d12(E), h19(E) thru h22(E), h30(E), h33(E), n1(E), w10(E)



Bar	A	B	C
h24(E)	6'-0"	3'-0"	1'-8 1/2"
h26(E)	3'-5"	3'-5"	2'-11"
h32(E)	4'-0"	4'-0"	2'-3 1/2"
v28(E)	4'-2"	2'-5"	1'-8"

Bars h24(E), h26(E), h32(E) & v28(E)

Bar	A
u8(E)	4'-0"
u9(E)	3'-3"
u10(E)	2'-8"
u11(E)	4'-5"
u12(E)	3'-7"
u13(E)	1'-7"

Bars u8(E) thru u13(E)

Bar	A	B	C
n4(E)	13'-7"	1'-10"	2 7/8"
n5(E)	12'-6"	1'-10"	1 1/8"
n6(E)	13'-0"	1'-7"	1 5/8"
n7(E)	13'-0"	1'-10"	1 1/8"

Bars n4(E), n5(E), n6(E) & n7(E)

Bar	A
u1(E)	2'-4"
u2(E)	3'-2"
u3(E)	4'-0"
u4(E)	4'-11"
u5(E)	1'-9"
u6(E)	2'-8"
u7(E)	3'-6"

Bars u1(E) thru u7(E)

NORTH ABUTMENT BAR LIST

Bar	No.		Size	Length	Shape
	Stage I	Stage II			
d11(E)	66	80	#5	3'-0"	
d12(E)	66	80	#4	3'-0"	
d13(E)	66	80	#5	7'-5"	
e11(E)	6	6	#4	31'-9"	
e12(E)	2	2	#8	34'-7"	
e13(E)	2	2	#5	32'-3"	
e14(E)		6	#4	11'-0"	
e15(E)		2	#8	11'-0"	
e16(E)		2	#5	11'-0"	
e17(E)	6		#4	21'-2"	
e18(E)	2		#8	21'-2"	
e19(E)	2		#5	21'-2"	
e20(E)	6		#4	4'-9"	
e21(E)	2		#8	4'-9"	
e22(E)	2		#5	4'-9"	
e23(E)		6	#4	29'-8"	
e24(E)		2	#8	29'-8"	
e25(E)		2	#5	29'-8"	
h1(E)	36	8	#5	26'-9"	
h2(E)	10	8	#5	25'-11"	
h3(E)		98	#5	20'-2"	
h4(E)		30	#5	17'-7"	
h5(E)	6	6	#5	8'-4"	
h6(E)	6		#5	13'-4"	
h7(E)	8		#5	9'-10"	
h8(E)		31	#5	15'-8"	
h9(E)	2		#6	23'-8"	
h10(E)	1		#6	24'-7"	
h11(E)	2		#6	25'-7"	
h12(E)		10	#6	20'-7"	
h13(E)		10	#6	25'-7"	
h14(E)	52	100	#5	33'-1"	
h15(E)		54	#5	29'-8"	
h16(E)		51	#5	13'-0"	
h17(E)		3	#5	31'-5"	
h18(E)		7	#5	34'-2"	
h19(E)	4	12	#5	5'-9"	
h20(E)	8	12	#5	5'-2"	
h21(E)	8	10	#5	4'-11"	
h22(E)	4	6	#5	4'-9"	
h23(E)		12	#5	11'-0"	
h24(E)		13	#5	9'-0"	
h25(E)	52		#5	21'-2"	
h26(E)	19		#5	6'-10"	
h27(E)	19		#5	15'-6"	
h28(E)	6		#5	9'-3"	
h29(E)	4		#5	7'-11"	
h30(E)	2		#5	4'-0"	
h31(E)	2		#5	4'-10"	
h32(E)		34	#5	8'-0"	
h33(E)		6	#5	5'-10"	
n1(E)	22	108	#10	15'-10"	
n2(E)	40	97	#5	3'-6"	
n3(E)	55	44	#5	4'-4"	
n4(E)		18	#10	15'-5"	
n5(E)		92	#10	14'-4"	
n6(E)	62		#9	14'-7"	
n7(E)	17		#10	14'-10"	
v1(E)	64	62	#8	17'-8"	
v2(E)	50	61	#8	16'-8"	
v3(E)	79		#8	19'-5"	
v4(E)	15		#8	28'-6"	
v5(E)		23	#8	20'-8"	
v6(E)		17	#10	20'-8"	
v7(E)		25	#8	22'-8"	
v8(E)		19	#10	22'-8"	
v9(E)		35	#10	17'-0"	
v10(E)		15	#8	19'-8"	
v11(E)		22	#9	19'-8"	
v12(E)		91	#9	16'-8"	
v13(E)		12	#6	16'-8"	
w1(E)		148	#5	25'-0"	
w2(E)		8	#5	21'-0"	
w3(E)	30		#5	31'-1"	
w4(E)	14		#5	38'-2"	
w5(E)	6		#5	9'-1"	
w6(E)	72		#5	27'-3"	
w7(E)		42	#5	15'-5"	
w8(E)		72	#5	31'-5"	
w9(E)		18	#5	15'-2"	
w10(E)		18	#5	7'-9"	

NORTH ABUTMENT BAR LIST (continued)

Bar	No.		Size	Length	Shape
	Stage I	Stage II			
h18(E)		2	#8	17'-1"	
u1(E)	12		#5	11'-6"	
u2(E)	10	6	#5	12'-4"	
u3(E)		16	#5	13'-2"	
u4(E)		16	#5	14'-1"	
u5(E)		16	#5	10'-11"	
u6(E)		16	#5	11'-10"	
u7(E)		13	#5	12'-8"	
u8(E)	1	1	#5	10'-0"	
u9(E)		1	#5	9'-3"	
u10(E)	1	1	#5	8'-8"	
u11(E)	6	5	#5	10'-5"	
u12(E)	9	20	#5	9'-7"	
u13(E)	2	4	#5	7'-7"	
v1(E)	10		#5	18'-10"	
v2(E)	12		#5	19'-3"	
v3(E)	11	6	#5	20'-1"	
v4(E)		16	#5	20'-11"	
v5(E)		17	#5	21'-10"	
v6(E)	8		#5	24'-0"	
v7(E)	30	85	#5	6'-6"	
v8(E)	22	87	#5	5'-9"	
v9(E)	22	87	#5	2'-6"	
v10(E)	22	87	#5	4'-7"	
v11(E)	22	38	#8	14'-8"	
v12(E)		17	#5	22'-8"	
v13(E)		16	#5	23'-7"	
v14(E)		15	#5	24'-5"	
v15(E)		70	#8	19'-8"	
v16(E)	79		#7	12'-3"	
v17(E)	48	2	#5	12'-10"	
v18(E)	33		#5	13'-6"	
v19(E)	31		#5	24'-2"	
v20(E)	22		#5	25'-1"	
v21(E)		46	#5	14'-4"	
v22(E)		47	#5	15'-2"	
v23(E)		35	#5	16'-3"	
v24(E)		21	#5	26'-0"	
v25(E)		21	#5	26'-8"	
v26(E)		92	#8	13'-4"	
v27(E)		17	#8	15'-5"	
v28(E)		3	#5	6'-7"	
w1(E)		148	#5	25'-0"	
w2(E)		8	#5	21'-0"	
w3(E)	30		#5	31'-1"	
w4(E)	14		#5	38'-2"	
w5(E)	6		#5	9'-1"	
w6(E)	72		#5	27'-3"	
w7(E)		42	#5	15'-5"	
w8(E)		72	#5	31'-5"	
w9(E)		18	#5	15'-2"	
w10(E)		18	#5	7'-9"	

- Notes:**
- Reinforcement bars designated (E) shall be epoxy coated.
 - For Retaining Wall Elevations and Sections, see Shts. S-035 & S-036.
 - For Abutment Wall Elevations, Sections and Details see Shts. S-033, S-034, S-037 & S-038.
 - For bars 14(E), 16(E), 116(E) & 117(E), see Sht. S-043.

BILL OF MATERIAL

Item	Unit	Stage I	Stage II	Total
Concrete Structures	Cu yd	353	862	1215
Reinforcement Bars, Epoxy Coated	lb	34,120	75,140	109,260
Protective Coat	Sq yd			458
Bridge Seat Sealer	Sq ft			335
Structure Excavation	Cu yd			2486
Geocomposite Wall Drain	Sq yd			634
Porous Granular Embankment (Special)	Cu yd			849
Pipe Underdrains For Structures 6"	Ft			307
Sub-base Granular Material, Type C 6"	Sq yd			467

SHT. S-039

REVISIONS	
NAME	DATE

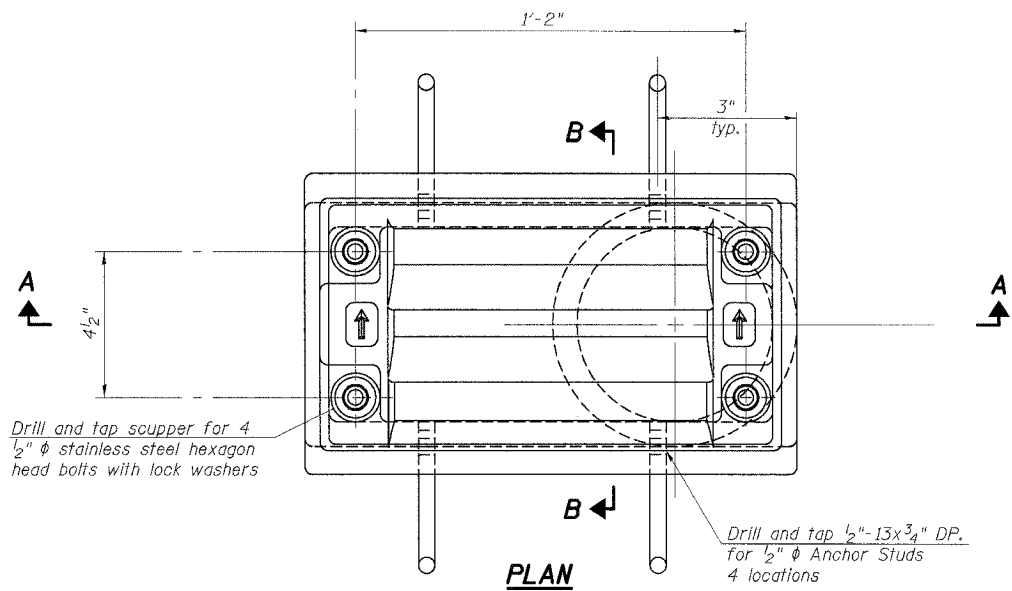
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 I-57 NB OVER WB CONNECTOR
 SN 016-0072 OLD, SN 016-2852 NEW
 STA. 238+73.54
 COOK COUNTY, SECTION (1516.1, 1717, & 1818) R-4
 NORTH ABUTMENT DETAILS III
 & BILL OF MATERIAL

DATE: 03/07/06
 DRAWN BY: FD
 CHECKED BY: TCU

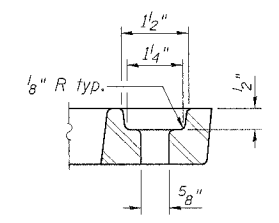
TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE 312.416.0000

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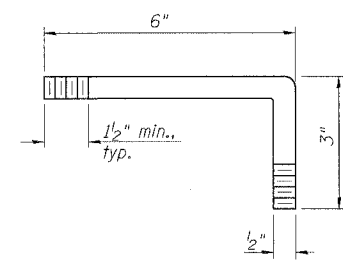
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	*	COOK	916	618
STA.	TO STA.			
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62304				
*(1516.1, 1717, & 1818) R-4				



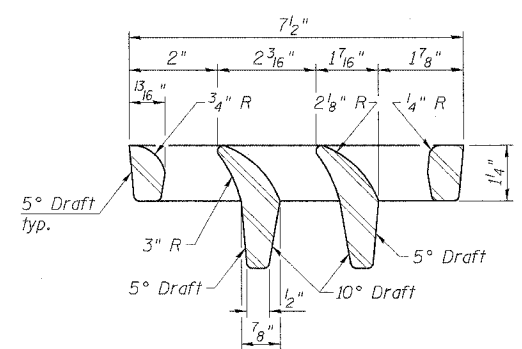
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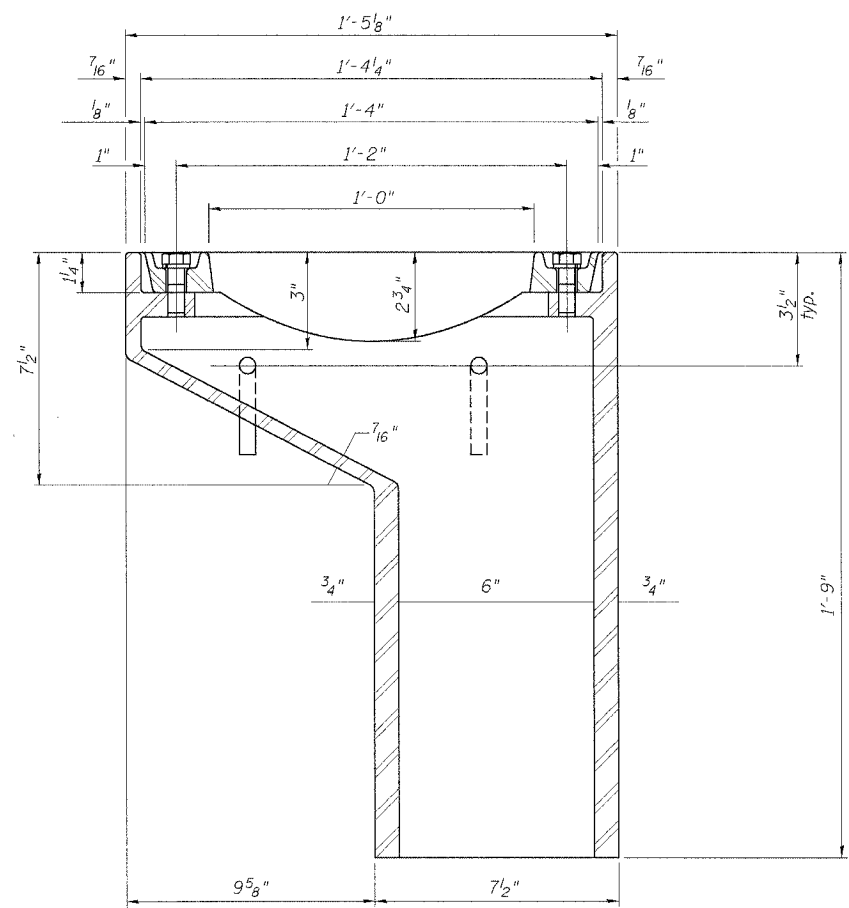
BOLT HOLE DETAIL



ANCHOR STUD DETAIL

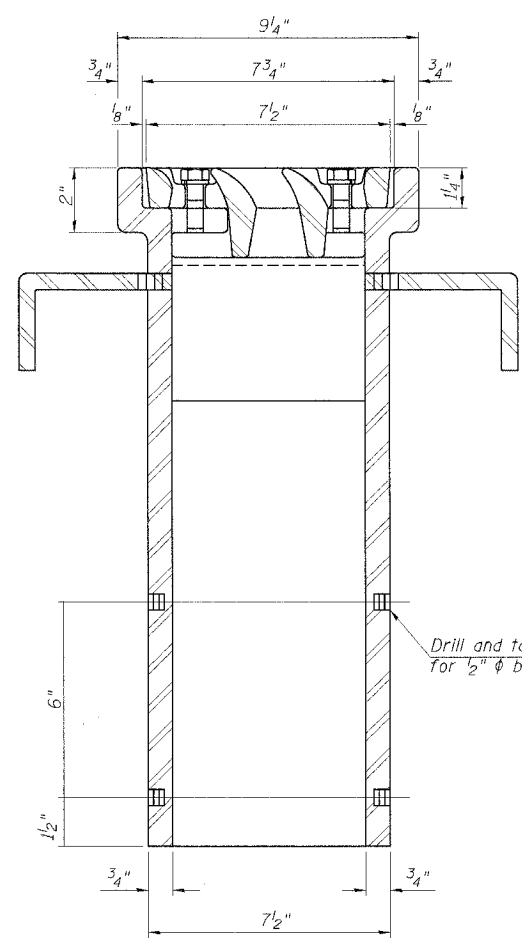


VANE GRATE DETAIL



SECTION A-A

See sheet of for scupper location relative to parapet.



SECTION B-B

Notes: All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 35B.
 Bolts, anchor studs, washers and nuts shall conform to the requirements of ASTM A 307 and shall be galvanized according to AASHTO M 232.
 The grate, frame and downspout shall be galvanized according to AASHTO M 111 and ASTM A 385. Downspouts located on the exterior side of a painted steel fascia beam shall be painted with the finish coat specified for the exterior side of the fascia beam.
 As an alternate, bolts, anchor studs, washers and nuts may be stainless steel according to Article 1006.29(d) of the Standard Specifications.
 Structural steel weldments of equal sections and of the same configuration may be substituted for cast iron. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval.
 The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.
 Cost of the Grate, Frame, Downspout, Anchor Studs, Bolts, Washers and Nuts including complete installation of the scupper shall be paid for at the contract unit price each for Drainage Scupper, DS-11.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Drainage Scupper, DS-11	Each	1

SHT. S-041

REVISIONS	
NAME	DATE

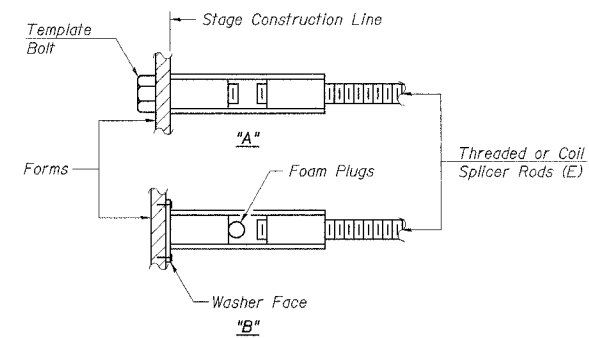
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 I-57 NB OVER WB CONNECTOR
 SN 016-0072 OLD, SN 016-2852 NEW
 STA. 238+73.54
 COOK COUNTY, SECTION (1516.1, 1717, & 1818) R-4
 SCUPPER DETAILS

DATE: 03/07/06

DRAWN BY: HJ
 CHECKED BY: RDS

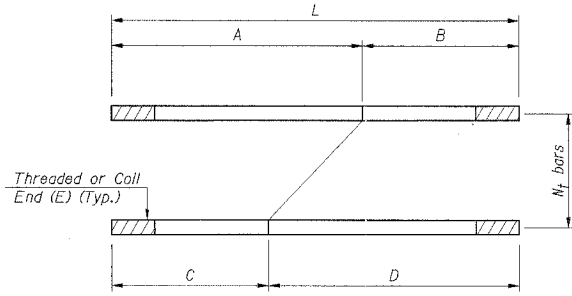
TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS ARCHITECTS PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE 326.1000

UBTC
 \FS-000\PROJ\CIVIL\DOCUMENT\193086\STRUCT\CONV\SCDOW#_DON
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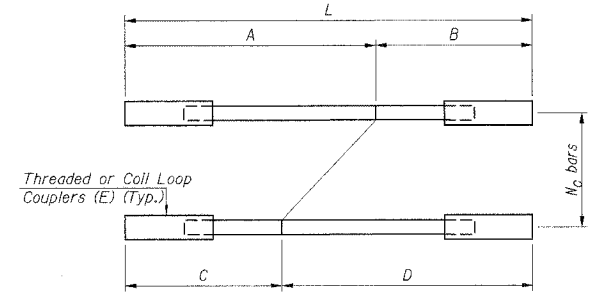
INSTALLATION AND SETTING METHODS

"A": Set bar splicer assembly by means of a template bolt.
 "B": Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



VARIABLE LENGTH THREADED TAIL ASSEMBLIES
 For use in Stage II Construction

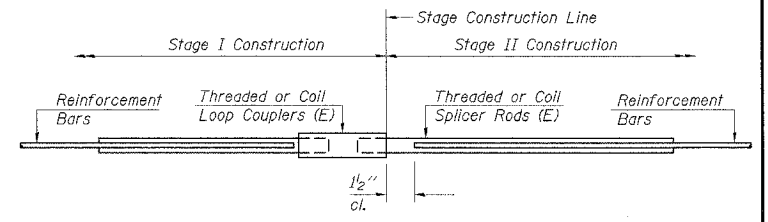
Location	Bar Mark	Size	A	B	C	D	L	N _b
S. Abut	113(E)	#8	11'-4"	1'-2"	6'-0"	6'-6"	12'-6"	11
S. Abut	114(E)	#9	11'-7 1/2"	1'-1 1/2"	6'-6 1/2"	6'-6 1/2"	12'-9"	15
N. Abut	14(E)	#8	8'-10"	17'-1"	17'-1"	8'-10"	25'-11"	22
N. Abut	116(E)	#8	8'-4"	8'-4"	8'-4"	8'-4"	16'-8"	3



VARIABLE LENGTH COUPLER TAIL ASSEMBLIES
 For use in Stage I Construction

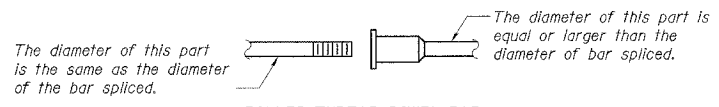
Location	Bar Mark	Size	A	B	C	D	L	N _c
S. Abut	1102(E)	#8	15'-5"	5'-3"	10'-1"	10'-7"	20'-8"	11
S. Abut	1107(E)	#9	15'-6"	5'-0"	10'-1"	10'-5"	20'-6"	15
N. Abut	16(E)	#8	10'-7"	5'-7"	5'-7"	10'-7"	16'-2"	22
N. Abut	117(E)	#8	10'-2"	10'-2"	10'-2"	10'-2"	20'-4"	3

* The total number of variable length bar splicer assemblies required shall be N_c + N_t, paid for as BAR SPLICERS



STANDARD LENGTH ASSEMBLIES

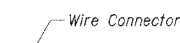
Bar Size	South Abutment	North Abutment	Superstructure
#5	83	92	447
#6	5	5	
#7			
#8			
#9			



ROLLED THREAD DOWEL BAR



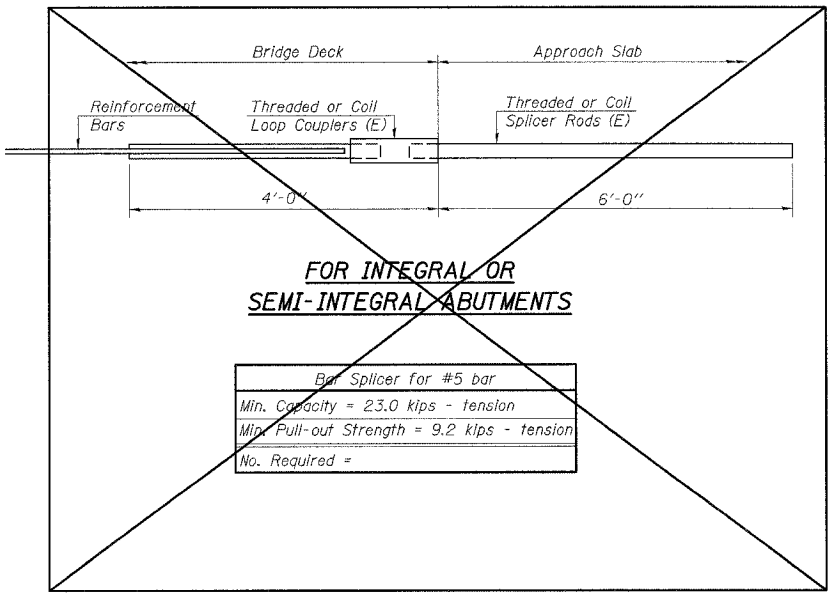
** ONE PIECE



WELDED SECTIONS

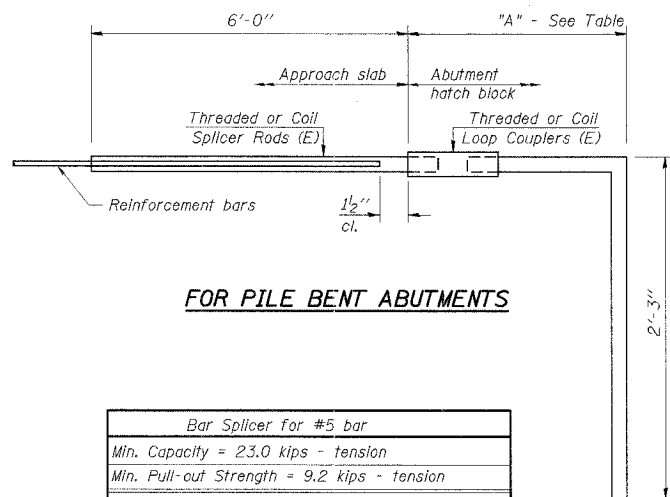
BAR SPLICER ASSEMBLY ALTERNATIVES

** Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

Bar Splicer for #5 bar		
Min. Capacity =	23.0 kips - tension	
Min. Pull-out Strength =	9.2 kips - tension	
No. Required =		



FOR PILE BENT ABUTMENTS

Bar Splicer for #5 bar		
Min. Capacity =	23.0 kips - tension	
Min. Pull-out Strength =	9.2 kips - tension	
Location	No. Req'd	"A" Dim.
South Abutment, Stage I	22	3'-2"
South Abutment, Stage II	38	2'-11"
North Abutment, Stage I	15	2'-9"
North Abutment, Stage II	50	2'-5"

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:
 ① Minimum Capacity (Tension in kips) = 1.25 x f_y x A₁
 ② Minimum *Pull-out Strength (Tension in kips) = 1.25 x f_{s,allow} x A₁
 Where f_y = Yield strength of lapped reinforcement bars in ksi.
 f_{s,allow} = Allowable tensile stress in lapped reinforcement bars in ksi (Service Load)
 A₁ = Tensile stress area of lapped reinforcement bars.
 * = 28 day concrete
 Bar splicer assemblies shall be according to Section 508 of the Standard Specifications, except as noted. The furnishing and installation of bar splicer assemblies will be measured and paid for at the contract unit price each for "BAR SPLICERS."

Bar Size to be Spliced	Splicer Rod or Dowel Bar Standard Lengths	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	5.9
#5	2'-0"	23.0	9.2
#6	2'-7"	33.1	13.3
#7	3'-5"	45.1	18.0
#8	4'-6"	58.9	23.6
#9	5'-9"	75.0	30.0
#10	7'-3"	95.0	38.0
#11	9'-0"	117.4	46.8

SHT. S-043

REVISIONS	
NAME	DATE

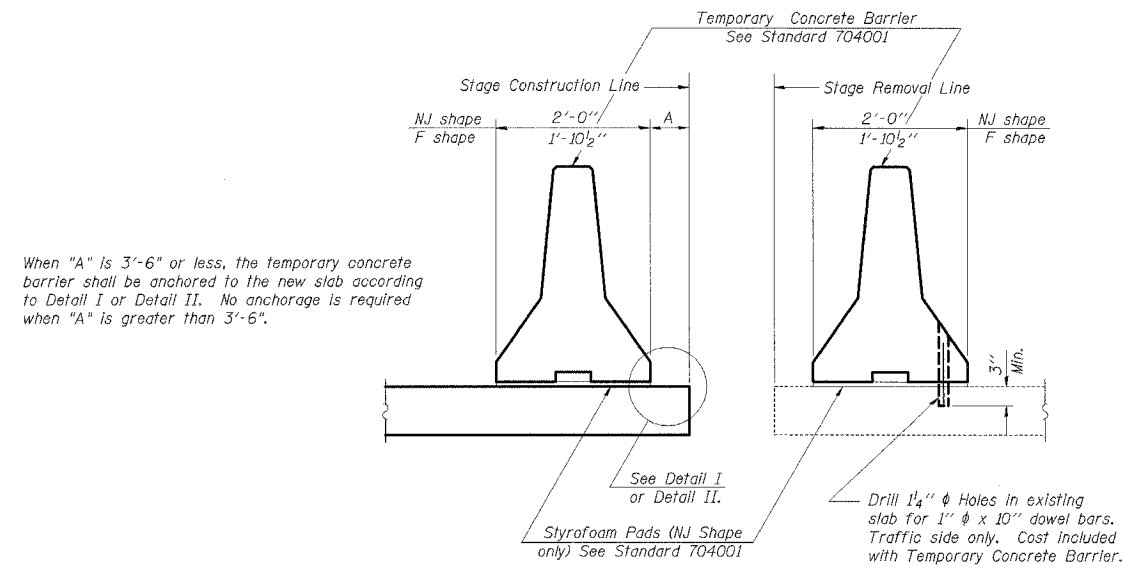
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 I-57 NB OVER WB CONNECTOR
 SN 016-0072 OLD, SN 016-2852 NEW
 STA. 238+73.54
 COOK COUNTY, SECTION (1516.1, 1717, & 1818) R-4
BAR SPLICER DETAILS

DRAWN BY: VV
 CHECKED BY: RDS
 DATE: 03/07/06

TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS ARCHITECTS PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312.467.6000

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 I\DETC

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	*	COOK	916	621
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 62304				
•(1516.1, 1717, & 1818) R-4				



When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".

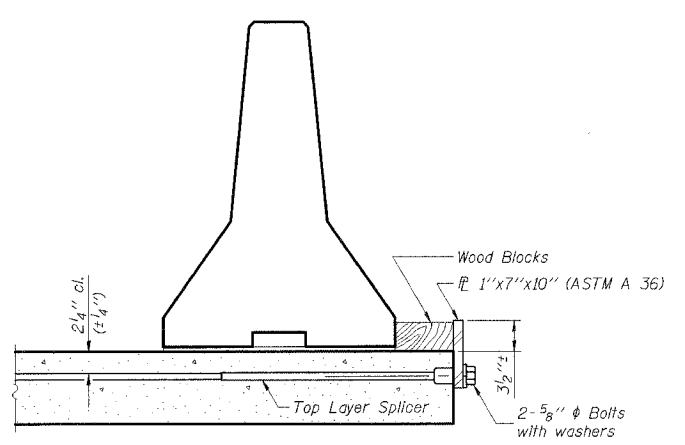
NOTES

Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel \bar{L} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.

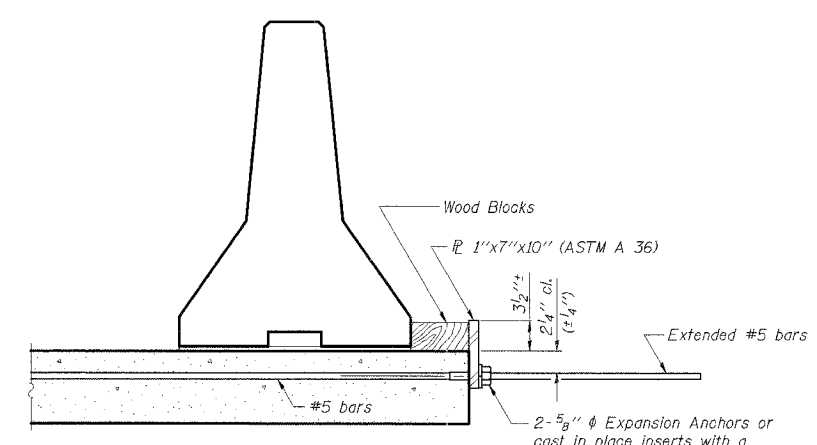
Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x10" steel \bar{L} to the concrete slab with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.

Cost of anchorage is included with Temporary Concrete Barrier.

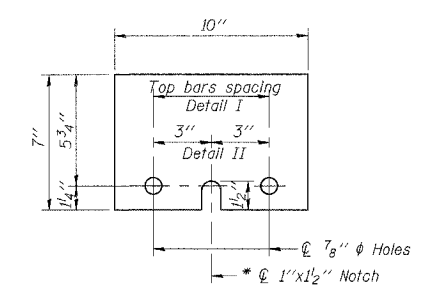
SECTIONS THRU SLAB



DETAIL I
The 1"x7"x10" Plate shall not be removed until Stage II Construction forms and reinforcement bars are in place.



DETAIL II
The 1"x7"x10" Plate shall not be removed until Stage II Construction forms and all reinforcement bars are in place and the concrete is ready to be placed.



P 1'x7'x10"
* Required only with Detail II

SHT. S-044

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. ROUTE 57 (INTERSTATE 57)
I-57 NB OVER WB CONNECTOR
SN 016-0072 OLD, SN 016-2852 NEW
STA. 238+73.54
COOK COUNTY, SECTION (1516.1, 1717, & 1818) R-4
TEMPORARY CONCRETE BARRIER

DATE: 03/07/06

DRAWN BY: HJ
CHECKED BY: RDS

TENG
TENG & ASSOCIATES, INC.
ENGINEERS ARCHITECTS PLANNERS
305 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312.461.0000

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Boring No. 1, Page 1 of 2

SOIL BORING LOG											
PAGE 1 of 2											
DATE 1-21-2004											
LOGGED BY RH											
OBA JOB No. 03365											
ROUTE Interstate 57 DESCRIPTION I-57 northbound over I-94 westbound ramp bridge improvements.											
SECTION I-80 to I-94 LOCATION Northbound entrance ramp to I-94 at I-57 termination.											
COUNTY Cook DRILLING METHOD 3.25" Hollow Stem Auger HAMMER TYPE CME Automatic											
STRUCT. NO. 018-0072 SURFACE WATER ELEV. <i>n/a</i>											
STATION 238-00 STREAM BED ELEV. <i>n/a</i>											
BORING NO. S-1 GROUNDWATER ELEVATION											
STATION 238-77 FIRST ENCOUNTER <i>-34.000</i>											
OFFSET 40' Left UPON COMPLETION <i>-306.000</i>											
GROUND SURFACE ELEV. <i>-94.000</i>											
CLAY LDM-trace gravel & brick-dark gray (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-stiff (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											

Boring No. 2, Page 1 of 1

SOIL BORING LOG											
PAGE 1 of 1											
DATE 2/8-9/04											
LOGGED BY RH											
OBA JOB No. 03365											
ROUTE Interstate 57 DESCRIPTION I-57 northbound over I-94 westbound ramp bridge improvements.											
SECTION I-80 to I-94 LOCATION Northbound entrance ramp to I-94 at I-57 termination.											
COUNTY Cook DRILLING METHOD 3.25" Hollow Stem Auger HAMMER TYPE CME Automatic											
STRUCT. NO. 018-0072 SURFACE WATER ELEV. <i>n/a</i>											
STATION 238-00 STREAM BED ELEV. <i>n/a</i>											
BORING NO. S-2 GROUNDWATER ELEVATION											
STATION 237-52 FIRST ENCOUNTER <i>n/a</i>											
OFFSET 47' Left UPON COMPLETION <i>n/a</i>											
GROUND SURFACE ELEV. <i>-51.000</i>											
10.0' CONCRETE, 8.0" CRUSHED STONE											
CLAY-gray-very stiff (A-6)											
CLAY-gray-hard (A-6)											
CLAY-gray-stiff (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											

Boring No. 4, Page 1 of 2

SOIL BORING LOG											
PAGE 1 of 2											
DATE 2/10-11/04											
LOGGED BY RH											
OBA JOB No. 03365											
ROUTE Interstate 57 DESCRIPTION I-57 northbound over I-94 westbound ramp bridge improvements.											
SECTION I-80 to I-94 LOCATION Northbound entrance ramp to I-94 at I-57 termination.											
COUNTY Cook DRILLING METHOD 3.25" Hollow Stem Auger HAMMER TYPE CME Automatic											
STRUCT. NO. 018-0072 SURFACE WATER ELEV. <i>n/a</i>											
STATION 238-00 STREAM BED ELEV. <i>n/a</i>											
BORING NO. S-4 GROUNDWATER ELEVATION											
STATION 238-43 FIRST ENCOUNTER <i>-37.000</i>											
OFFSET 12' Left UPON COMPLETION <i>n/a</i>											
GROUND SURFACE ELEV. <i>-98.100</i>											
2.5" ASPHALT, 14.5" CONCRETE											
CRUSHED STONE (FB)											
SAND-brown-medium dense (A-3) FILL											
CLAY-gray-very stiff to hard (A-6)											
SILTY LOAM-brown-very loose to medium dense (A-4) Apparent Fill											

Boring No. 1, Page 2 of 2

SOIL BORING LOG											
PAGE 2 of 2											
DATE 1-21-2004											
LOGGED BY RH											
OBA JOB No. 03365											
ROUTE Interstate 57 DESCRIPTION I-57 northbound over I-94 westbound ramp bridge improvements.											
SECTION I-80 to I-94 LOCATION Northbound entrance ramp to I-94 at I-57 termination.											
COUNTY Cook DRILLING METHOD 3.25" Hollow Stem Auger HAMMER TYPE CME Automatic											
STRUCT. NO. 018-0072 SURFACE WATER ELEV. <i>n/a</i>											
STATION 238-00 STREAM BED ELEV. <i>n/a</i>											
BORING NO. S-1 GROUNDWATER ELEVATION											
STATION 238-77 FIRST ENCOUNTER <i>-34.000</i>											
OFFSET 40' Left UPON COMPLETION <i>-306.000</i>											
GROUND SURFACE ELEV. <i>-94.000</i>											
CLAY-gray-very stiff to hard (A-6)											
FRACTURED STONE with Clay-gray-very dense (A-2)											
Driers Observation: Apparent Bedrock											
End of Boring @ -45.0' Hollow Stem Augers CME Automatic Hammer											

Boring No. 3, Page 1 of 1

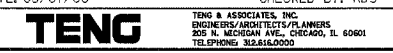
SOIL BORING LOG											
PAGE 1 of 1											
DATE 12-28-2003											
LOGGED BY RH											
OBA JOB No. 03365											
ROUTE Interstate 57 DESCRIPTION I-57 northbound over I-94 westbound ramp bridge improvements.											
SECTION I-80 to I-94 LOCATION Northbound entrance ramp to I-94 at I-57 termination.											
COUNTY Cook DRILLING METHOD 3.25" Hollow Stem Auger HAMMER TYPE CME Automatic											
STRUCT. NO. 018-0072 SURFACE WATER ELEV. <i>n/a</i>											
STATION 238-00 STREAM BED ELEV. <i>n/a</i>											
BORING NO. S-3 GROUNDWATER ELEVATION											
STATION 238-60 FIRST ENCOUNTER <i>n/a</i>											
OFFSET 39' Left UPON COMPLETION <i>n/a</i>											
GROUND SURFACE ELEV. <i>-49.000</i>											
TOPSOIL-block (A-7) FILL											
CLAY to CLAY LDM-brown & gray-hard (A-5) FILL											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-some fractured stone-gray-very dense (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											

Boring No. 4, Page 2 of 2

SOIL BORING LOG											
PAGE 2 of 2											
DATE 2/10-11/04											
LOGGED BY RH											
OBA JOB No. 03365											
ROUTE Interstate 57 DESCRIPTION I-57 northbound over I-94 westbound ramp bridge improvements.											
SECTION I-80 to I-94 LOCATION Northbound entrance ramp to I-94 at I-57 termination.											
COUNTY Cook DRILLING METHOD 3.25" Hollow Stem Auger HAMMER TYPE CME Automatic											
STRUCT. NO. 018-0072 SURFACE WATER ELEV. <i>n/a</i>											
STATION 238-00 STREAM BED ELEV. <i>n/a</i>											
BORING NO. S-4 GROUNDWATER ELEVATION											
STATION 238-43 FIRST ENCOUNTER <i>-37.000</i>											
OFFSET 12' Left UPON COMPLETION <i>n/a</i>											
GROUND SURFACE ELEV. <i>-98.100</i>											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											
CLAY-gray-very stiff to hard (A-6)											

REVISIONS		
NO.	NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 I-57 NB OVER WB CONNECTOR
 SN 016-0072 OLD, SN 016-2852 NEW
 STA. 238+73.54
 COOK COUNTY, SECTION (1516.1, 1717, & 1818) R-4
 SOIL BORINGS I
 DRAWN BY: VV
 CHECKED BY: RDS
 DATE: 03/07/06



Boring No. 5, Page 1 of 2

SOIL BORING LOG		PAGE 1 of 2	
DATE 1-16-2004		DATE 1-16-2004	
LOGGED BY RH		LOGGED BY RH	
OBA JOB No. 03365		OBA JOB No. 03365	
ROUTE Interstate 57 DESCRIPTION I-57 northbound over I-94 westbound ramp bridge improvements.			
SECTION I-80 to I-94 LOCATION Northbound entrance ramp to I-94 at I-57 termination.			
COUNTY Cook DRILLING METHOD 3.25" Hollow Stem Auger HAMMER TYPE CME Automatic			
STRUCT. NO. 018-0072 SURFACE WATER ELEV. 0.0' STREAM BED ELEV. 0.0'			
BORING NO. S-5 STATION 238-00 GROUNDWATER ELEVATION: 0.0' FIRST ENCOUNTER 0.0' UPON COMPLETION 0.0' OFFSET 39.1' RIGHT			
Ground Surface Elev. -20.1' (20)			
CLAY LOAM-cork gray (F8)			
CLAY to CLAY LOAM-brown & gray-hard (A-6) Fill			
CLAY-brown & gray-stiff to very stiff (A-6)			
CLAY-gray-stiff (A-6)			

Boring No. 6, Page 1 of 2

SOIL BORING LOG		PAGE 1 of 2	
DATE 12-29-2003		DATE 12-29-2003	
LOGGED BY RH		LOGGED BY RH	
OBA JOB No. 03365		OBA JOB No. 03365	
ROUTE Interstate 57 DESCRIPTION I-57 northbound over I-94 westbound ramp bridge improvements.			
SECTION I-80 to I-94 LOCATION Northbound entrance ramp to I-94 at I-57 termination.			
COUNTY Cook DRILLING METHOD 3.25" Hollow Stem Auger HAMMER TYPE CME Automatic			
STRUCT. NO. 018-0072 SURFACE WATER ELEV. 0.0' STREAM BED ELEV. 0.0'			
BORING NO. S-6 STATION 236-78 GROUNDWATER ELEVATION: 0.0' FIRST ENCOUNTER 0.0' UPON COMPLETION 0.0' OFFSET 9.0' RIGHT			
Ground Surface Elev. -14.4' (20)			
TOPSOIL-block (A-7) Fill			
CLAY to CLAY LOAM-brown & gray-very stiff to hard (A-6) Fill			
CLAY-gray-stiff to very stiff (A-6)			
CLAY-gray-medium stiff to stiff (A-6)			
SILTY CLAY LOAM-gray-dense (A-4)(A-5)			
CLAY-gray-hard (A-6)			
CLAY-gray-stiff to very stiff (A-6)			

Boring No. 7, Page 1 of 1

SOIL BORING LOG		PAGE 2 of 2	
DATE 2-2-2004		DATE 2-2-2004	
LOGGED BY RH		LOGGED BY RH	
OBA JOB No. 03365		OBA JOB No. 03365	
ROUTE Interstate 57 DESCRIPTION I-57 northbound over I-94 westbound ramp bridge improvements.			
SECTION I-80 to I-94 LOCATION Northbound entrance ramp to I-94 at I-57 termination.			
COUNTY Cook DRILLING METHOD 3.25" Hollow Stem Auger HAMMER TYPE CME Automatic			
STRUCT. NO. 018-0072 SURFACE WATER ELEV. 0.0' STREAM BED ELEV. 0.0'			
BORING NO. S-7 STATION 237-44 GROUNDWATER ELEVATION: 0.0' FIRST ENCOUNTER 0.0' UPON COMPLETION 0.0' OFFSET 9.0' RIGHT			
Ground Surface Elev. -20.1' (20)			
CLAY-gray-very stiff to hard (A-6)			
Run 1 (-41.0' to -46.0')			
Run 2 (-46.0' to -51.0')			
End of Boring @ -51.0'			

Boring No. 5, Page 2 of 2

SOIL BORING LOG		PAGE 2 of 2	
DATE 1-16-2004		DATE 1-16-2004	
LOGGED BY RH		LOGGED BY RH	
OBA JOB No. 03365		OBA JOB No. 03365	
ROUTE Interstate 57 DESCRIPTION I-57 northbound over I-94 westbound ramp bridge improvements.			
SECTION I-80 to I-94 LOCATION Northbound entrance ramp to I-94 at I-57 termination.			
COUNTY Cook DRILLING METHOD 3.25" Hollow Stem Auger HAMMER TYPE CME Automatic			
STRUCT. NO. 018-0072 SURFACE WATER ELEV. 0.0' STREAM BED ELEV. 0.0'			
BORING NO. S-5 STATION 240-17 GROUNDWATER ELEVATION: 0.0' FIRST ENCOUNTER 0.0' UPON COMPLETION 0.0' OFFSET 39.1' RIGHT			
Ground Surface Elev. -20.1' (20)			
CLAY-gray-very stiff to hard (A-6)			
CLAY-gray-medium stiff (A-6)			
Run 1 (-49.0' to -54.0')			
Run 2 (-54.0' to -59.0')			
End of Boring @ -58.0'			

Boring No. 6, Page 2 of 2

SOIL BORING LOG		PAGE 2 of 2	
DATE 12-29-2003		DATE 12-29-2003	
LOGGED BY RH		LOGGED BY RH	
OBA JOB No. 03365		OBA JOB No. 03365	
ROUTE Interstate 57 DESCRIPTION I-57 northbound over I-94 westbound ramp bridge improvements.			
SECTION I-80 to I-94 LOCATION Northbound entrance ramp to I-94 at I-57 termination.			
COUNTY Cook DRILLING METHOD 3.25" Hollow Stem Auger HAMMER TYPE CME Automatic			
STRUCT. NO. 018-0072 SURFACE WATER ELEV. 0.0' STREAM BED ELEV. 0.0'			
BORING NO. S-6 STATION 236-78 GROUNDWATER ELEVATION: 0.0' FIRST ENCOUNTER 0.0' UPON COMPLETION 0.0' OFFSET 9.0' RIGHT			
Ground Surface Elev. -14.4' (20)			
CLAY-gray-very stiff to hard (A-6)			
CLAY-gray-medium stiff (A-6)			
Run 1 (-49.0' to -54.0')			
Run 2 (-54.0' to -59.0')			
End of Boring @ -59.0'			

Boring No. 8, Page 1 of 1

SOIL BORING LOG		PAGE 1 of 1	
DATE 1-21-2004		DATE 1-21-2004	
LOGGED BY RH		LOGGED BY RH	
OBA JOB No. 03365		OBA JOB No. 03365	
ROUTE Interstate 57 DESCRIPTION I-57 northbound over I-94 westbound ramp bridge improvements.			
SECTION I-80 to I-94 LOCATION Northbound entrance ramp to I-94 at I-57 termination.			
COUNTY Cook DRILLING METHOD 3.25" Hollow Stem Auger HAMMER TYPE CME Automatic			
STRUCT. NO. 018-0072 SURFACE WATER ELEV. 0.0' STREAM BED ELEV. 0.0'			
BORING NO. S-8 STATION 238-21 GROUNDWATER ELEVATION: 0.0' FIRST ENCOUNTER 0.0' UPON COMPLETION 0.0' OFFSET 7.0' RIGHT			
Ground Surface Elev. -62.0' (20)			
TOPSOIL with Gravel-block (F8)			
SAND-brown-loose (A-3) Fill			
CLAY-gray-very stiff to hard (A-6)			
CLAY-gray-very stiff to hard (A-6)			
Run 1 (-29.0' to -34.0')			
Run 2 (-34.0' to -39.0')			
End of Boring @ -39.0'			

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 I-57 NB OVER WB CONNECTOR
 SN 016-0072 OLD, SN 016-2852 NEW
 STA. 238+73.54
 COOK COUNTY, SECTION (1516.1, 1717, & 1818) R-4
 SOIL BORINGS II

DATE: 03/07/06

DRAWN BY: VV
 CHECKED BY: RDS

TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 308 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312.467.0000

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Boring No. 9, Page 1 of 1

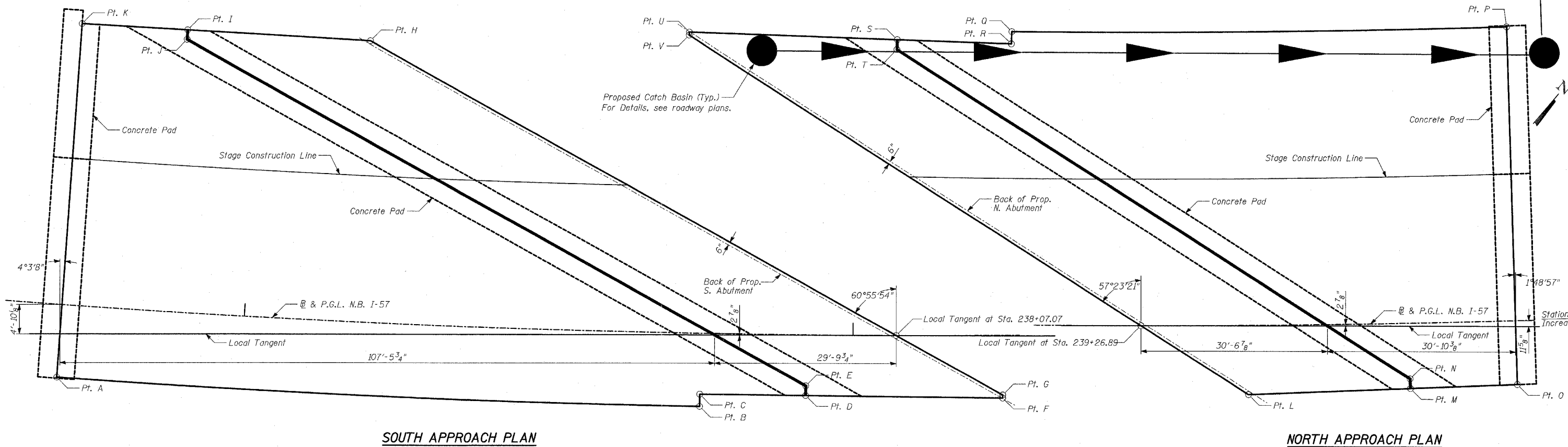
O'Brien & Associates, Inc. CONSULTING ENGINEERS 100 S. Wacker Drive, Suite 200 Chicago, IL 60606		SOIL BORING LOG		PAGE 1 of 1 DATE 1-19-2004 LOGGED BY BH OBA JOB No. 03365																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																					
ROUTE Interstate 57		DESCRIPTION I-57 northbound over I-94 westbound ramp bridge improvements.																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																							
SECTION I-80 to I-94		LOCATION Northbound entrance ramp to I-94 at I-57 termination.																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																							
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SOUTH APPROACH PLAN

NORTH APPROACH PLAN

WORKING POINTS
(South Approach Pavement)

Point	Distance along Local Tangent at Sta. 238+07.07	Offset from Local Tangent at Sta. 238+07.07
A	137'-9 ⁵ / ₈ "	7'-1 ¹ / ₂ " Rt.
B	32'-3"	11'-8 ³ / ₈ " Rt.
C	32'-2 ³ / ₄ "	9'-8 ³ / ₄ " Rt.
D*	14'-10"	9'-10 ⁷ / ₈ " Rt.
E*	14'-9 ⁷ / ₈ "	8'-4" Rt.
F	-17'-5 ¹ / ₈ "	10'-2 ⁵ / ₈ " Rt.
G	-17'-5 ⁷ / ₈ "	9'-8 ⁵ / ₈ " Rt.
H	86'-4 ⁵ / ₈ "	48'-0 ¹ / ₄ " Lt.
I*	116'-4 ⁵ / ₈ "	49'-8 ⁷ / ₈ " Lt.
J*	116'-5 ⁵ / ₈ "	48'-2" Lt.
K	133'-8 ³ / ₈ "	50'-8 ³ / ₄ " Lt.

WORKING POINTS
(North Approach Pavement)

Point	Distance along Local Tangent at Sta. 239+26.89	Offset from Local Tangent at Sta. 239+26.89
L	17'-7 ⁷ / ₈ "	11'-3 ¹ / ₂ " Rt.
M*	44'-2 ⁵ / ₈ "	10'-3" Rt.
N*	44'-1 ⁷ / ₈ "	8'-8 ¹ / ₄ " Rt.
O	61'-8 ¹ / ₈ "	9'-6 ⁵ / ₈ " Rt.
P	59'-10 ⁵ / ₈ "	48'-11 ³ / ₈ " Lt.
Q	-21'-2 ¹ / ₈ "	48'-1 ³ / ₈ " Lt.
R	-21'-3 ³ / ₄ "	46'-1 ¹ / ₂ " Lt.
S*	-40'-0 ³ / ₈ "	46'-9 ¹ / ₄ " Lt.
T*	-40'-1"	45'-2 ¹ / ₂ " Lt.
U	-74'-1 ⁷ / ₈ "	47'-11 ¹ / ₂ " Lt.
V	-74'-2"	47'-5 ¹ / ₂ " Lt.

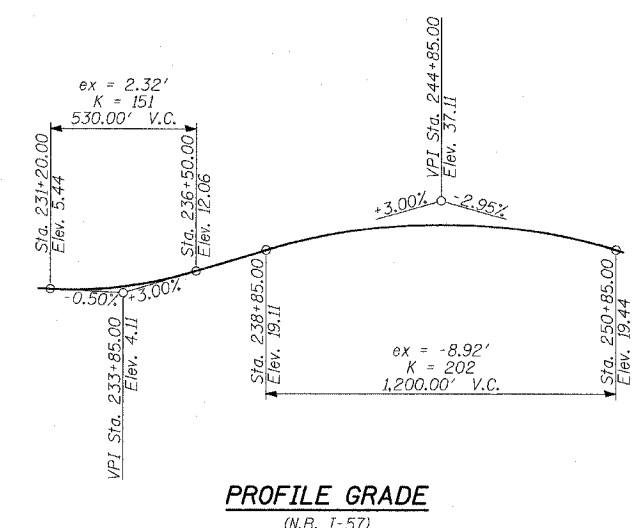
* @ of 4" Preformed Joint Seal

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Bridge Approach Pavement (Special)	SQ YD	391
Protective Coat	SQ YD	1,389
Bar Splicers	EACH	169
Bridge Approach Pavement Connector (PCC), Special	SQ YD	849

GENERAL NOTES:

- Construction of Bridge Approach Pavement (Special) and Bridge Approach Pavement Connector (PCC), Special shall conform to Sections 420, 1000 and 1100 of IDOT Standard Specifications for Road and Bridge Construction.
- This work will be paid for at the contract unit price per Square Yards for Bridge Approach Pavement (Special) or per Square Yards for Bridge Approach Pavement Connector (PCC), Special.
- The unit bid price shall include tie bars, preformed joint seal, polyethylene bond breaker, granular sub-base, concrete structures, reinforcement bars (epoxy coated), protective coat, concrete pads (including reinforcement and excavation) and all other items necessary to complete this item of work.



Signed *Phillip D. Frey*
 Phillip D. Frey, P.E., Ill. Lic. No. 081-004826
 CHICAGO, ILLINOIS
 Expires 11-30-2006.
 Date 3/7/06
 For drawings 1 thru 8 of 8

REVISIONS	
NAME	DATE

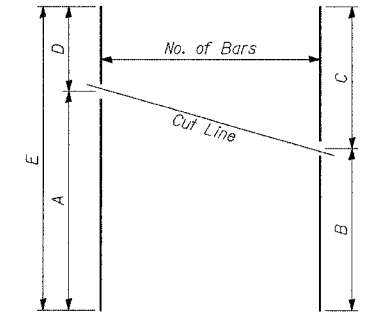
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 BRIDGE APPROACH PAVEMENT (SPECIAL)
 GENERAL PLAN AND GENERAL NOTES

S.N. _____ DESIGNED BY: MAF
 SCALE: _____ DRAWN BY: MAF
 DATE: MARCH 7, 2006 CHECKED BY: MI

TYLIN INTERNATIONAL

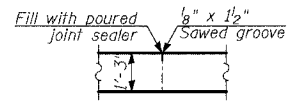
BAR TABLE SCHEDULE

Bar	No. of Sets Req'd	No. of Bars Per Set	A	D	B	C	E
a ₃ (E)	1	11	19'-10"	2'-7"	11'-7"	10'-10"	22'-5"
a ₄ (E)	1	13	29'-4"	2'-7"	16'-6"	15'-5"	31'-11"
a ₅ (E)	1	9	29'-4"	2'-7"	16'-9"	15'-2"	31'-11"
a ₇ (E)	1	14	30'-6"	2'-7"	17'-1"	16'-0"	33'-1"
a ₉ (E)	1	5	14'-0"	2'-7"	8'-11"	7'-8"	16'-7"
b ₁ (E)	1	6	35'-8"	14'-6"	26'-1"	24'-1"	50'-2"
b ₂ (E)	1	2	10'-4"	4'-0"	8'-3"	6'-1"	14'-4"
b ₄ (E)	1	5	20'-7"	3'-2"	12'-10"	10'-11"	23'-9"

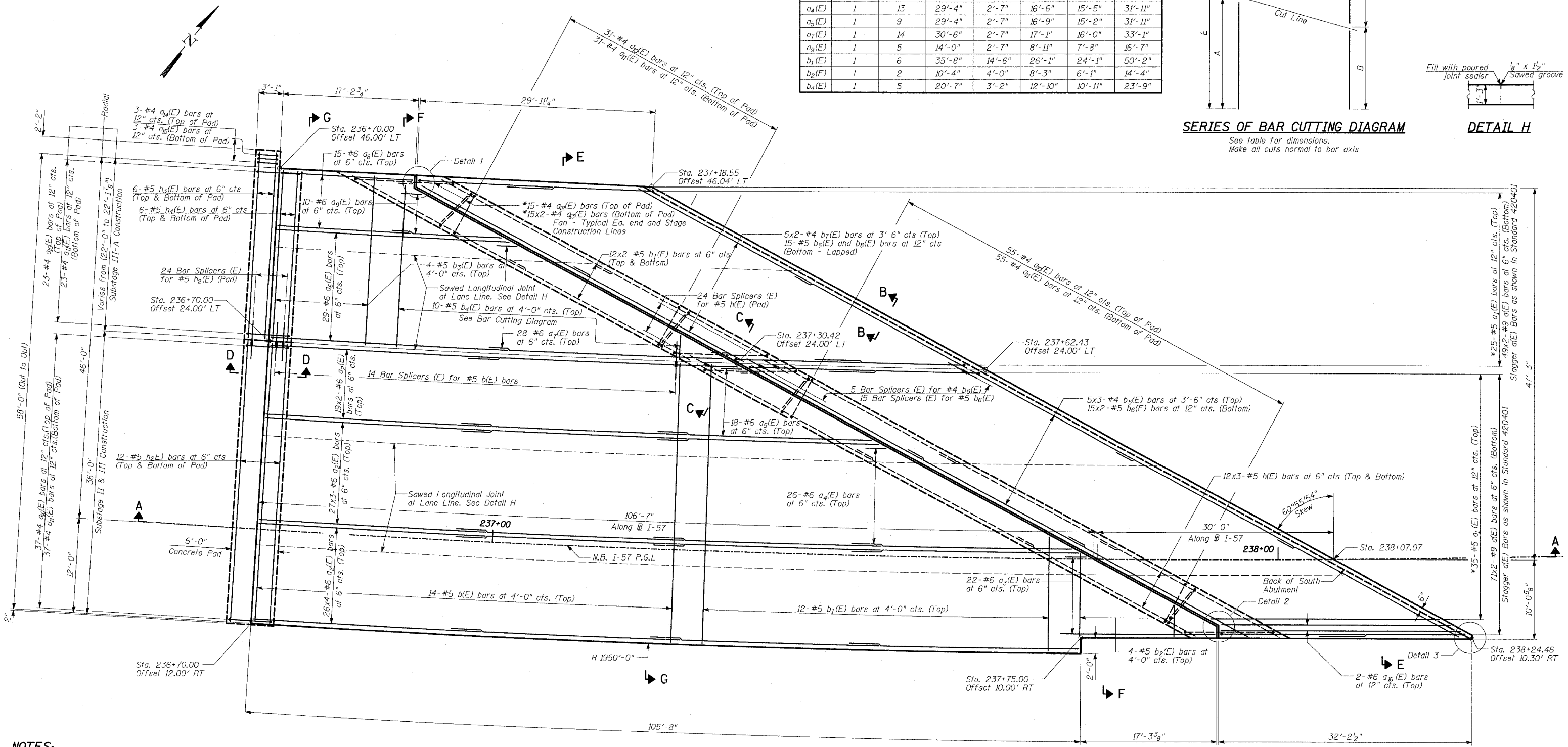


SERIES OF BAR CUTTING DIAGRAM

See table for dimensions.
Make all cuts normal to bar axis



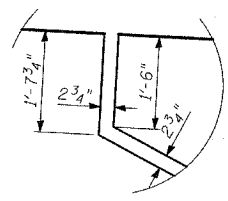
DETAIL H



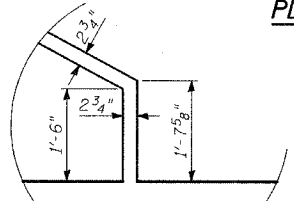
NOTES:
1. Bars indicated thus 26x4-#6 etc. indicates 26 lines of bars with 4 lengths per line.

MINIMUM LAPS

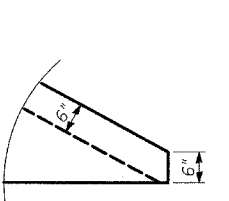
Bar Size	Min. Lap
#4	1'-8"
#5	2'-2"
#6	2'-7"
#8	4'-6"
#9	5'-9"



DETAIL 1



DETAIL 2



DETAIL 3

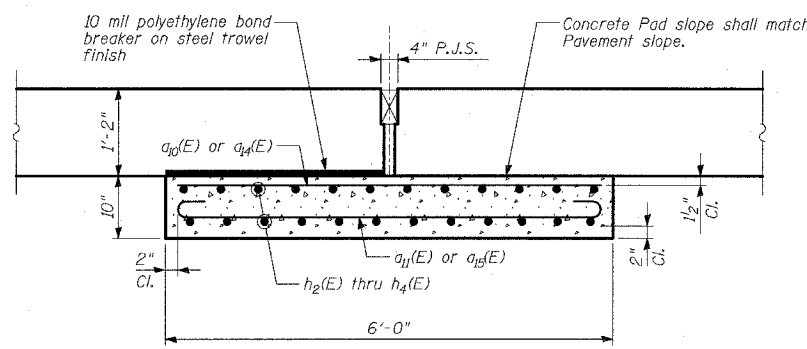
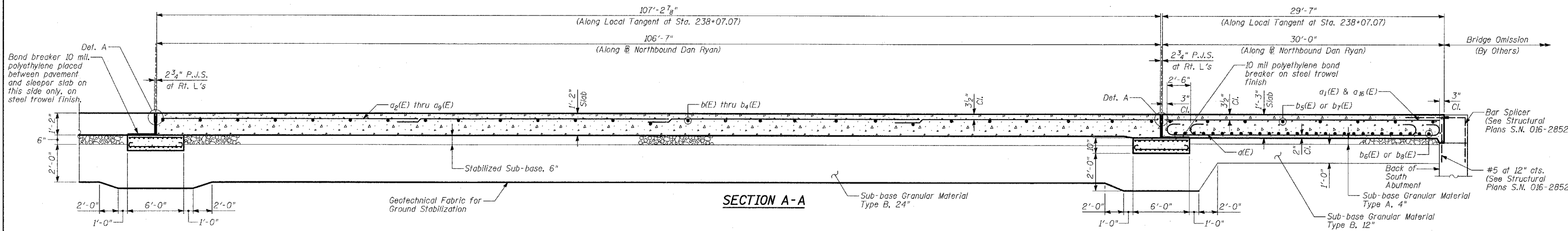
* Cut bars to fit in field.

TYLIN INTERNATIONAL

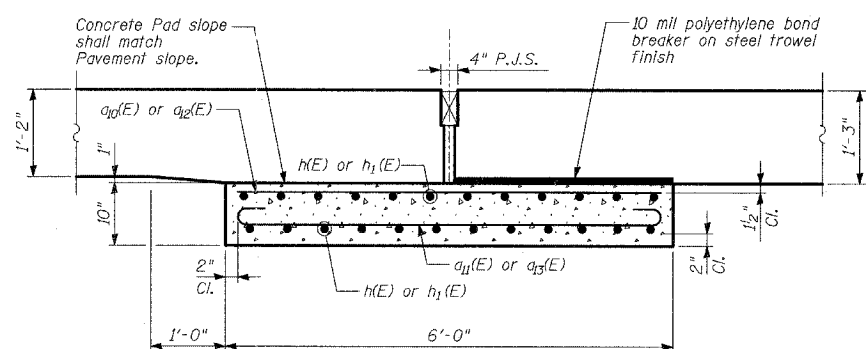
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
BRIDGE APPROACH PAVEMENT (SPECIAL)
SOUTH

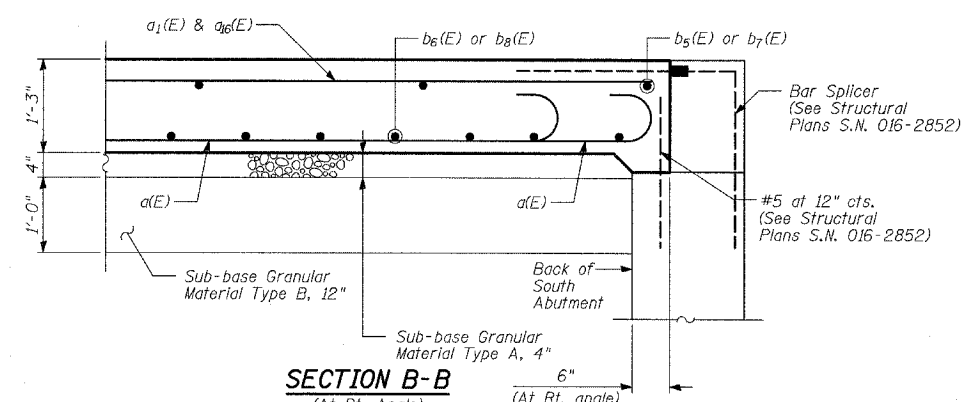
S.N.
SCALE: NTS
DATE: MARCH 7, 2006
DESIGNED BY: MAF
DRAWN BY: MAF
CHECKED BY: MI



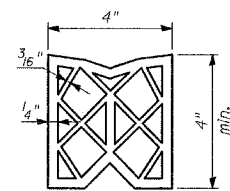
SECTION D-D



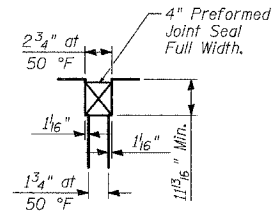
SECTION C-C



SECTION B-B
(At Rt. Angle)



PREFORMED JOINT SEAL



DETAIL A

DESIGN STRESSES

$f_y = 60,000 \text{ psi}$
 $f'_c = 3,500 \text{ psi}$
 $n = 8.5$

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 BRIDGE APPROACH PAVEMENT (SPECIAL)
 SOUTH - DETAIL 1

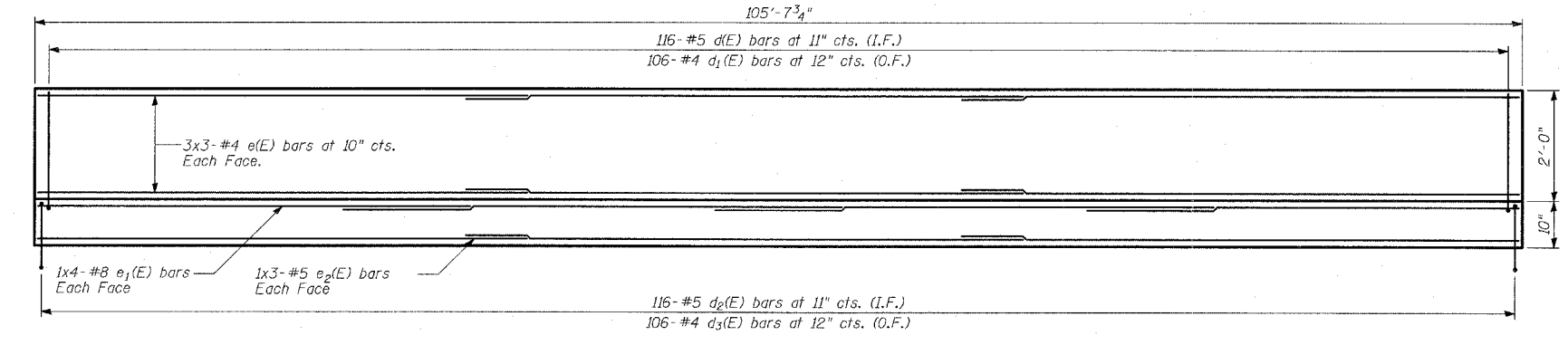
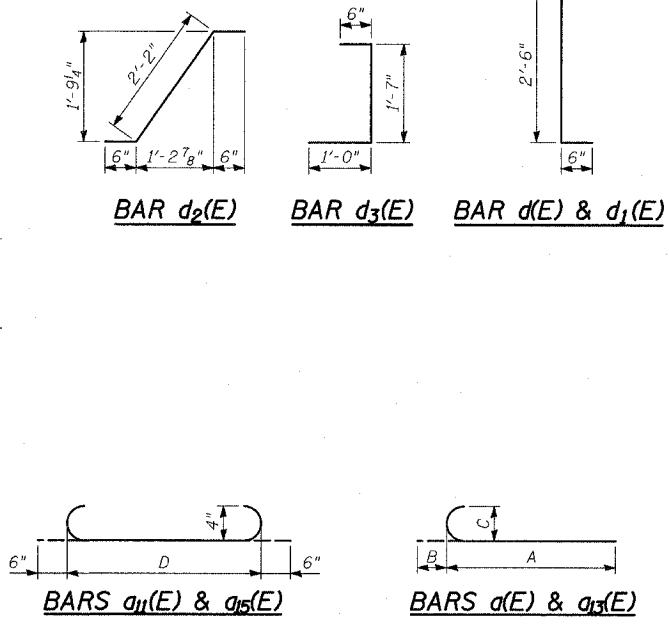
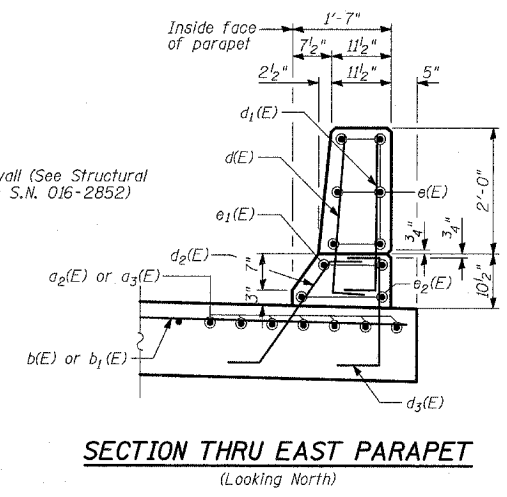
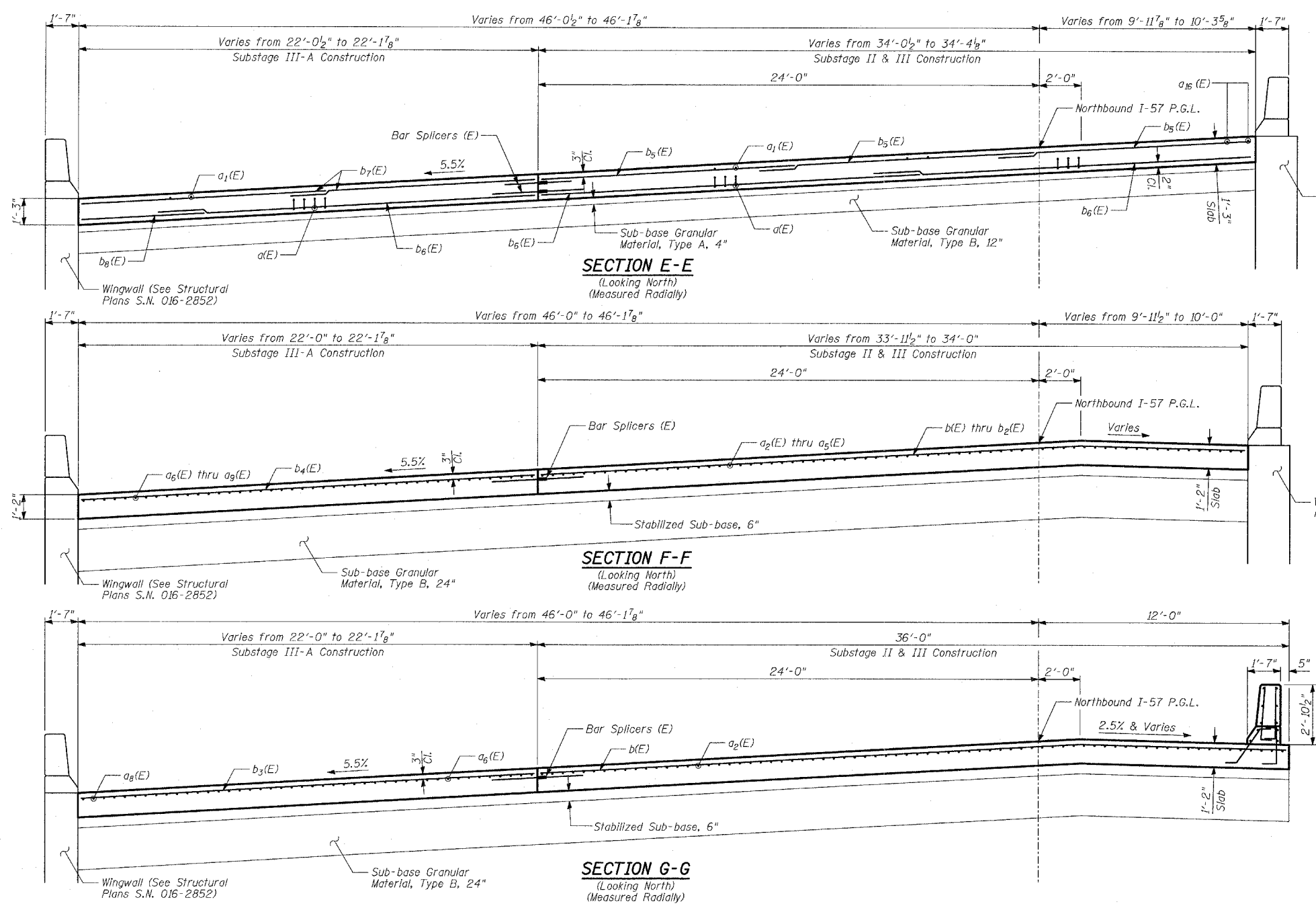
S.N. DESIGNED BY: MAF
 SCALE: NTS DRAWN BY: MAF
 DATE: MARCH 7, 2006 CHECKED BY: MI

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BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	240	#9	18'-9"	—
a ₁ (E)	60	#5	29'-9"	—
a ₂ (E)	223	#6	29'-4"	—
a ₃ (E)	11	#6	22'-5"	—
a ₄ (E)	13	#6	31'-11"	—
a ₅ (E)	9	#6	31'-11"	—
a ₆ (E)	29	#6	30'-6"	—
a ₇ (E)	14	#6	33'-1"	—
a ₈ (E)	15	#6	17'-2"	—
a ₉ (E)	5	#6	16'-7"	—
a ₁₀ (E)	146	#4	5'-8"	—
a ₁₁ (E)	146	#4	6'-8"	—
a ₁₂ (E)	60	#4	13'-2"	—
a ₁₃ (E)	120	#4	7'-11"	—
a ₁₄ (E)	3	#4	2'-9"	—
a ₁₅ (E)	3	#4	3'-9"	—
a ₁₆ (E)	2	#6	30'-1"	—
b(E)	14	#5	35'-8"	—
b ₁ (E)	6	#5	50'-2"	—
b ₂ (E)	2	#5	14'-4"	—
b ₃ (E)	4	#5	21'-8"	—
b ₄ (E)	5	#5	23'-9"	—
b ₅ (E)	15	#4	25'-3"	—
b ₆ (E)	45	#5	37'-3"	—
b ₇ (E)	10	#4	25'-9"	—
b ₈ (E)	15	#5	13'-11"	—
d(E)	116	#5	3'-0"	—
d ₁ (E)	106	#4	3'-0"	—
d ₂ (E)	116	#5	3'-2"	—
d ₃ (E)	106	#4	3'-1"	—
e(E)	18	#4	36'-3"	—
e ₁ (E)	8	#8	29'-9"	—
e ₂ (E)	6	#5	36'-7"	—
h(E)	72	#5	25'-10"	—
h ₁ (E)	48	#5	25'-11"	—
h ₂ (E)	24	#5	35'-10"	—
h ₃ (E)	12	#5	23'-10"	—
h ₄ (E)	12	#5	21'-8"	—
Reinforcement Bars, Epoxy Coated		POUND	44,070	
Bridge Approach Pavement, Special		SQ YD	194	
Bridge Approach Pavement Connector, Special		SQ YD	465	
Bar Splicers		EACH	82	
Protective Coat		SQ YD	744	

Reinforcement bars designated (E) shall be epoxy coated.



***INSIDE ELEVATION OF EAST PARAPET**

TYLIN INTERNATIONAL

* Parapet dimensions are measured at the inside face of parapet

NOTES:
 1. O.F. - denotes Outside Face
 2. I.F. - denotes Inside Face

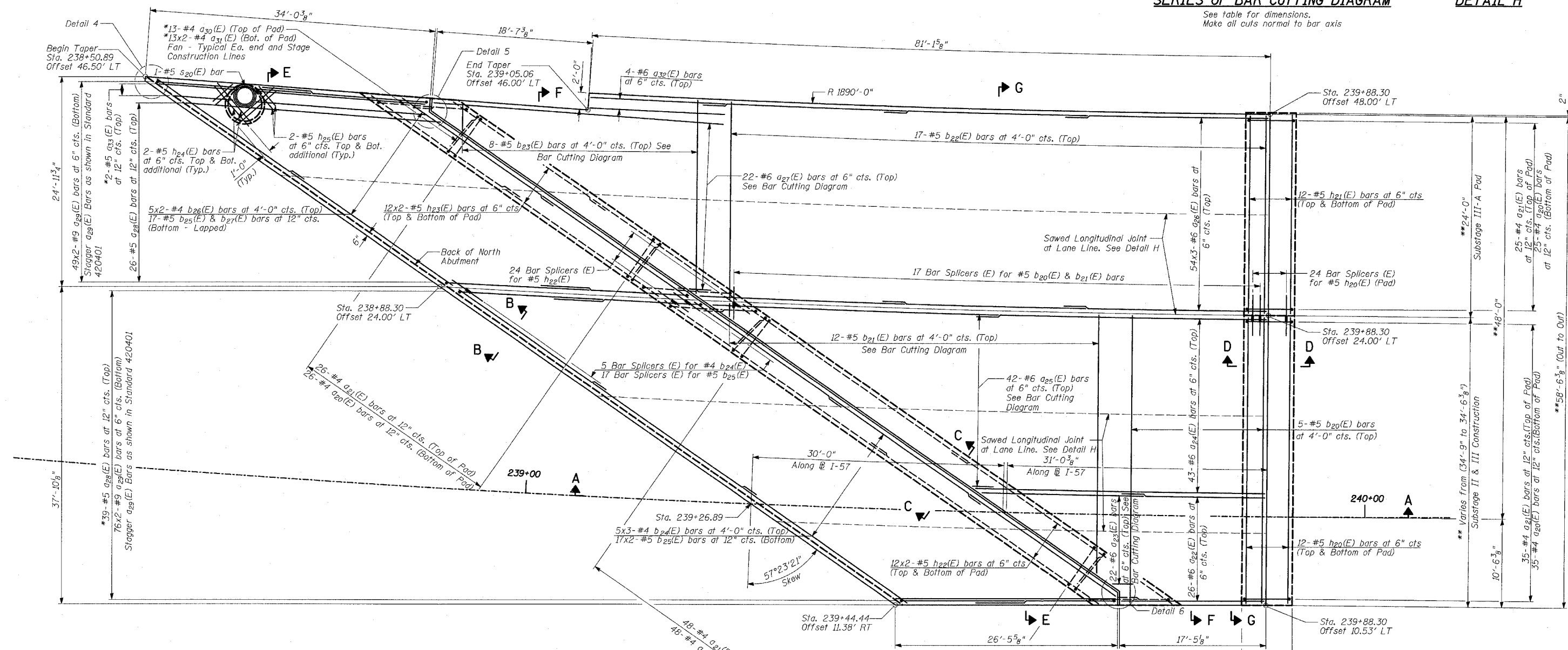
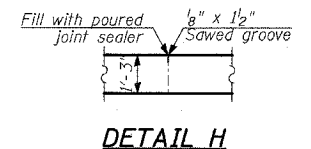
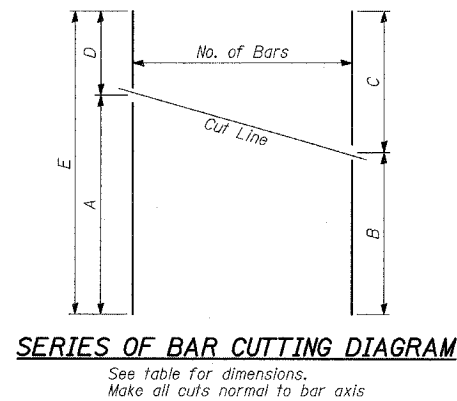
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
BRIDGE APPROACH PAVEMENT (SPECIAL)
SOUTH - DETAIL 2

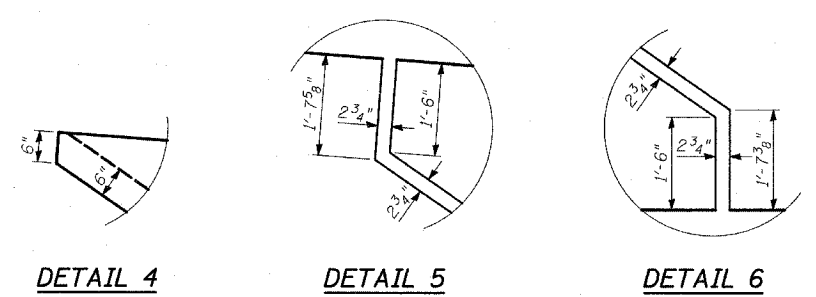
S.N. DESIGNED BY: MAF
 SCALE: NTS DRAWN BY: MAF
 DATE: MARCH 7, 2006 CHECKED BY: MI

BAR TABLE SCHEDULE

Bar	No. of Sets Req'd	No. of Bars Per Set	A	D	B	C	E
a ₂₃ (E)	1	11	19'-4"	2'-7"	11'-4"	10'-7"	21'-11"
a ₂₅ (E)	1	21	34'-10"	2'-7"	19'-1"	18'-4"	37'-5"
a ₂₇ (E)	1	11	34'-7"	2'-7"	19'-4"	17'-10"	37'-2"
b ₂₁ (E)	1	6	30'-9"	1'-7"	17'-6"	14'-10"	32'-4"
b ₂₃ (E)	1	4	23'-0"	3'-4"	14'-7"	11'-9"	26'-4"



NOTES:
 1. Bars indicated thus 5x2-#4 etc. indicates 5 lines of bars with 2 lengths per line.



PLAN

MINIMUM LAPS

Bar Size	Min. Lap
#4	1'-8"
#5	2'-2"
#6	2'-7"
#8	4'-6"
#9	5'-9"

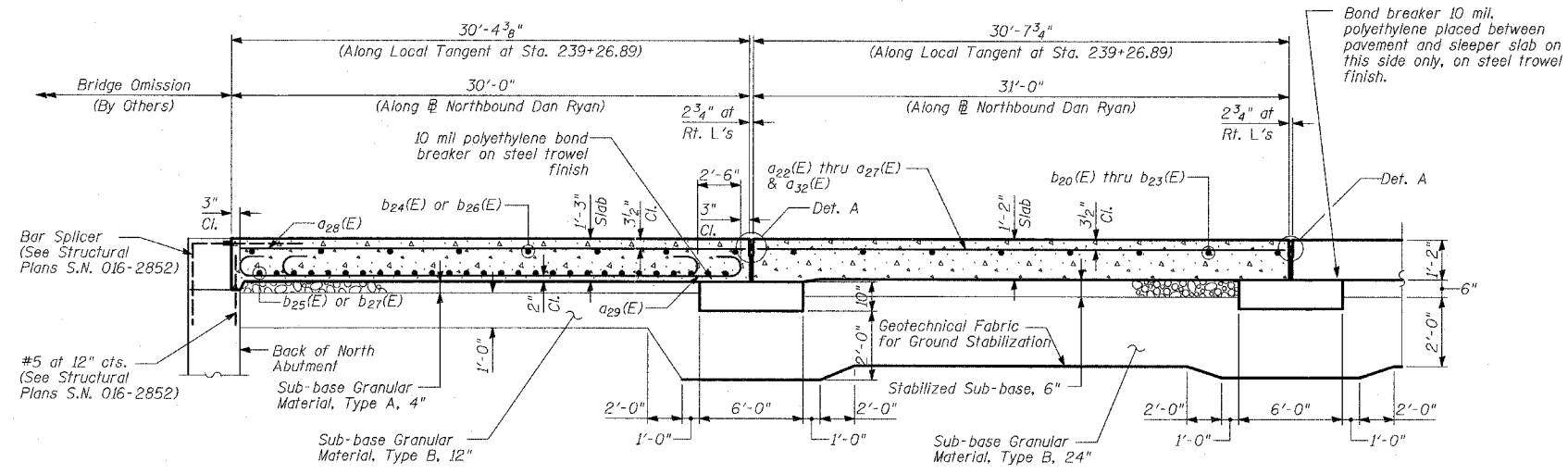
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 BRIDGE APPROACH PAVEMENT (SPECIAL) NORTH

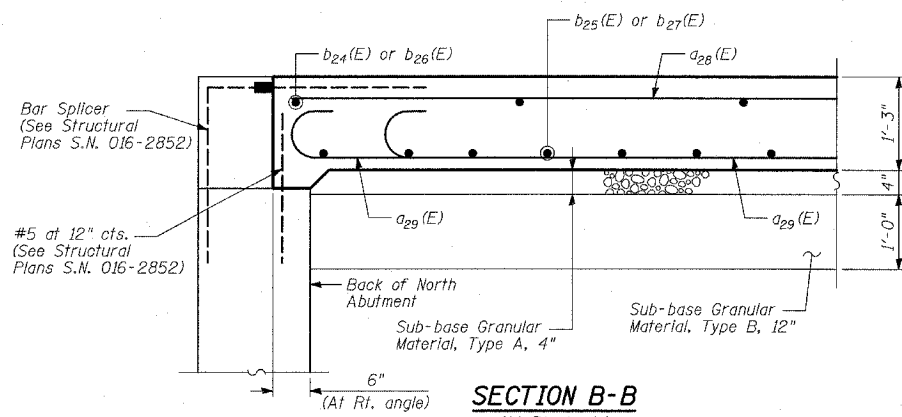
S.N.
 SCALE: NTS
 DATE: MARCH 7, 2006

DESIGNED BY: MAF
 DRAWN BY: MAF
 CHECKED BY: MI

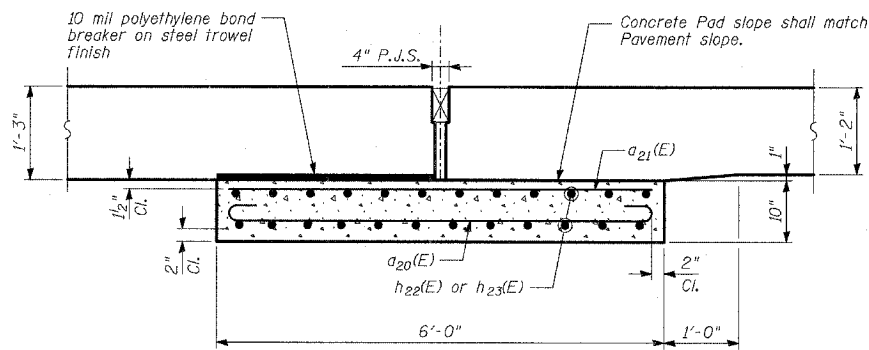
TYLIN INTERNATIONAL



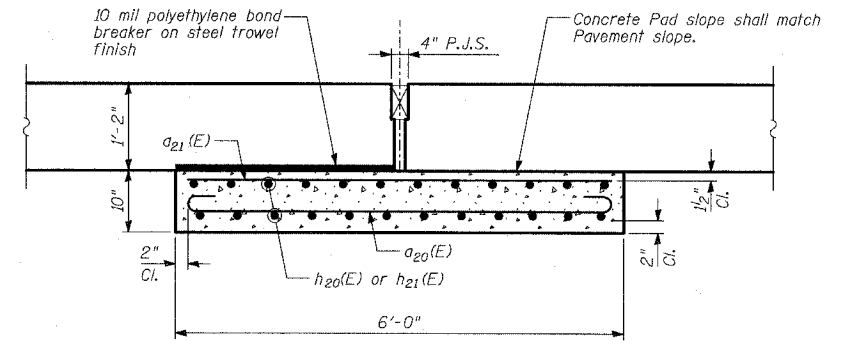
SECTION A-A



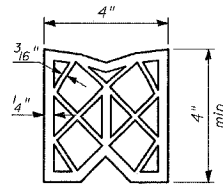
SECTION B-B
(At Rt. Angle)



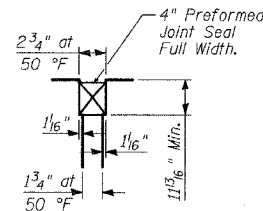
SECTION C-C



SECTION D-D



PREFORMED JOINT SEAL



DETAIL A

DESIGN STRESSES

$f_y = 60,000 \text{ psi}$
 $f'_c = 3,500 \text{ psi}$
 $n = 8.5$

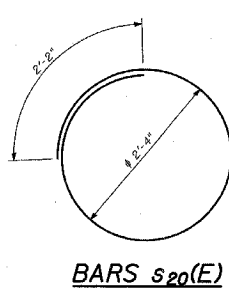
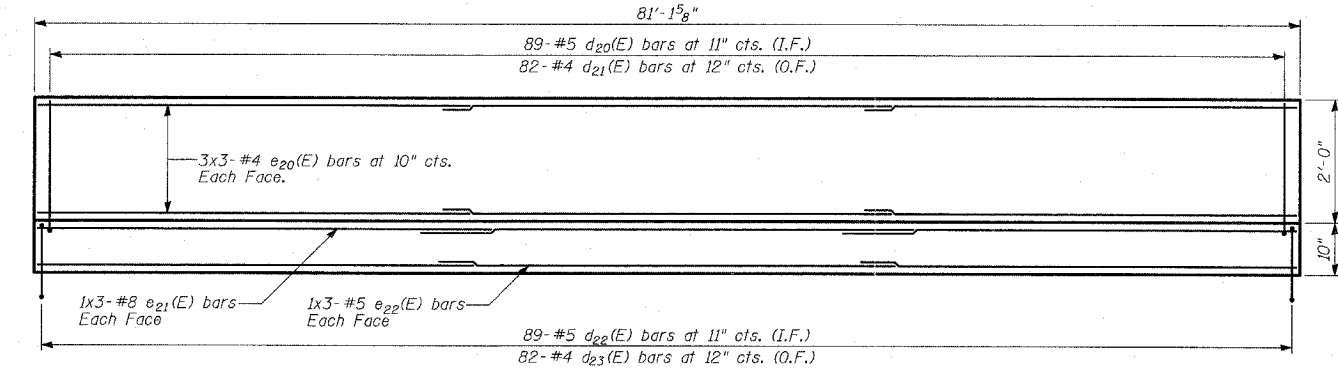
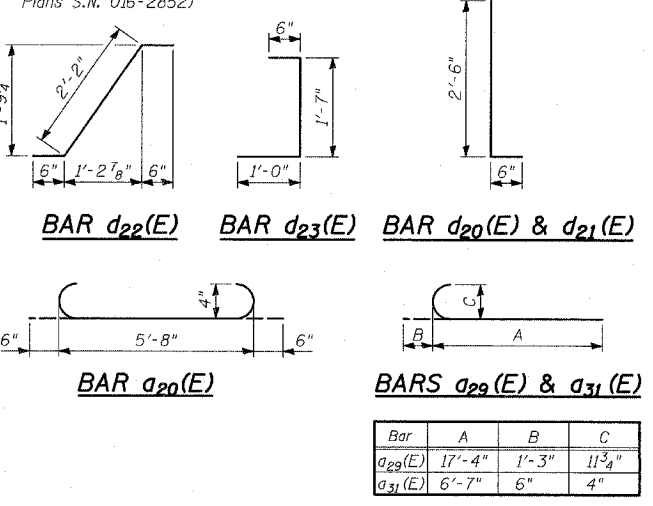
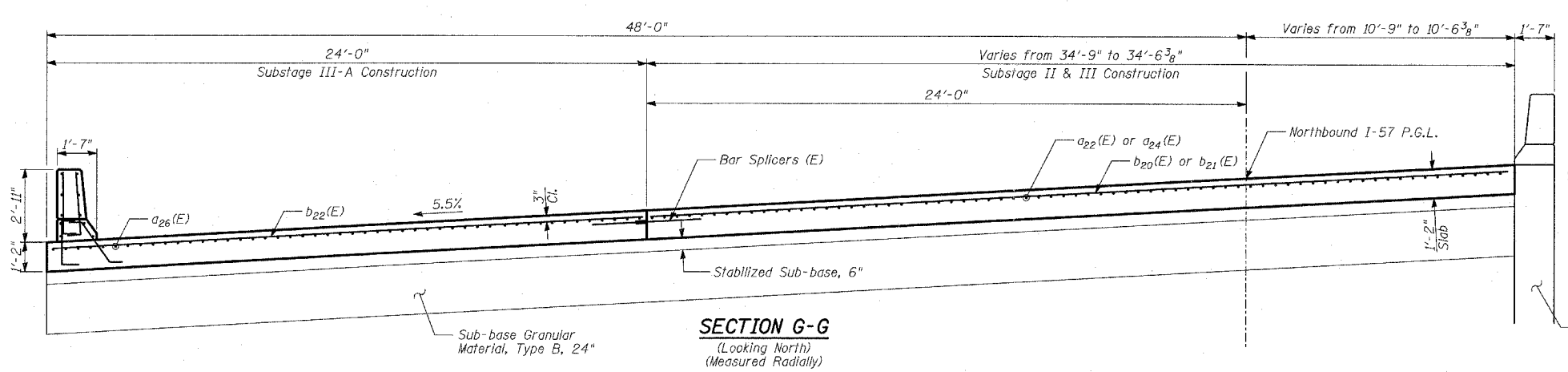
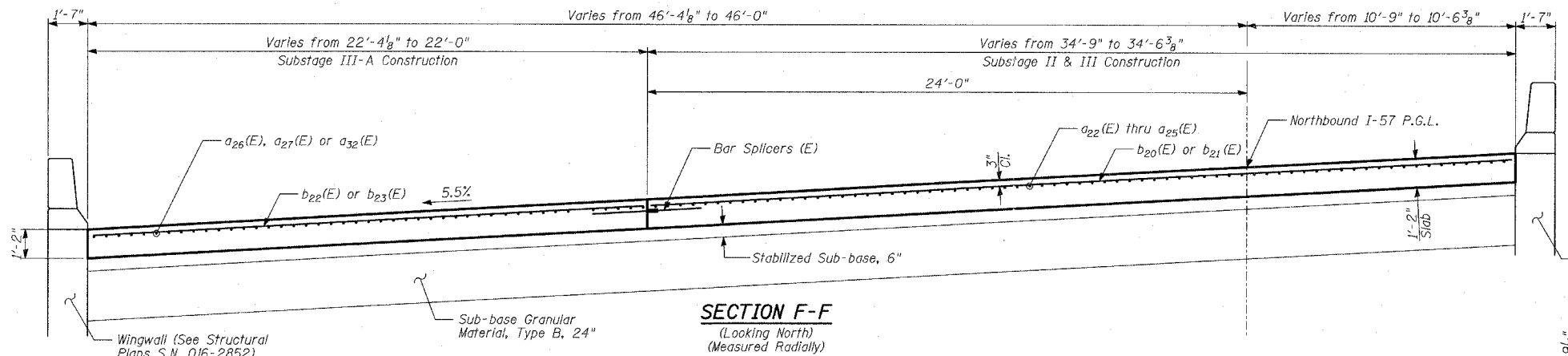
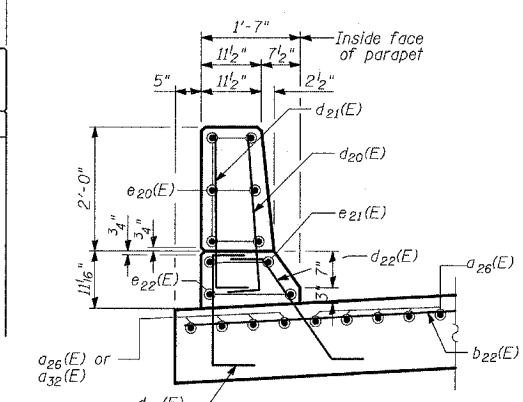
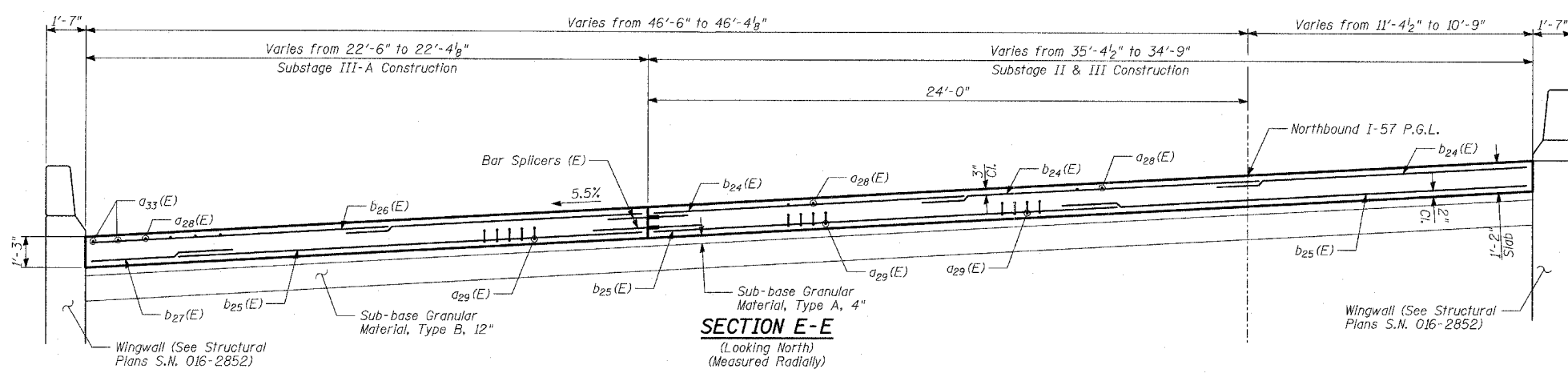
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
BRIDGE APPROACH PAVEMENT (SPECIAL)
 NORTH - DETAIL 1

S.N.
 SCALE: NTS
 DATE: MARCH 7, 2006
 DESIGNED BY: MAF
 DRAWN BY: MAF
 CHECKED BY: MI

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a ₂₀ (E)	134	#4	6'-8"	U
a ₂₁ (E)	134	#4	5'-8"	U
a ₂₂ (E)	26	#6	17'-1"	U
a ₂₃ (E)	11	#6	21'-11"	U
a ₂₄ (E)	43	#6	34'-10"	U
a ₂₅ (E)	21	#6	37'-5"	U
a ₂₆ (E)	162	#6	24'-2"	U
a ₂₇ (E)	11	#6	37'-2"	U
a ₂₈ (E)	65	#5	31'-5"	U
a ₂₉ (E)	250	#9	18'-7"	U
a ₃₀ (E)	52	#4	11'-1"	U
a ₃₁ (E)	104	#4	6'-11"	U
a ₃₂ (E)	4	#6	16'-0"	U
a ₃₃ (E)	2	#5	33'-6"	U
b ₂₀ (E)	5	#5	34'-2"	U
b ₂₁ (E)	6	#5	32'-4"	U
b ₂₂ (E)	17	#5	23'-8"	U
b ₂₃ (E)	4	#5	26'-4"	U
b ₂₄ (E)	15	#4	23'-0"	U
b ₂₅ (E)	51	#5	33'-11"	U
b ₂₆ (E)	10	#4	22'-2"	U
b ₂₇ (E)	17	#5	10'-11"	U
d ₂₀ (E)	89	#5	3'-0"	U
d ₂₁ (E)	82	#4	3'-0"	U
d ₂₂ (E)	89	#5	3'-2"	U
d ₂₃ (E)	82	#4	3'-1"	U
e ₂₀ (E)	18	#4	28'-1"	U
e ₂₁ (E)	6	#8	29'-11"	U
e ₂₂ (E)	6	#5	28'-5"	U
h ₂₀ (E)	24	#5	34'-2"	U
h ₂₁ (E)	24	#5	23'-10"	U
h ₂₂ (E)	48	#5	32'-8"	U
h ₂₃ (E)	48	#5	22'-1"	U
h ₂₄ (E)	8	#5	6'-3"	U
h ₂₅ (E)	8	#5	3'-0"	U
s ₂₀ (E)	1	#5	9'-6"	U
Reinforcement Bars, Epoxy Coated	POUND	40,490		
Bridge Approach Pavement, Special	SQ YD	197		
Bridge Approach Pavement Connector, Special	SQ YD	384		
Bar Splicers	EACH	87		
Protective Coat	SQ YD	645		



TYLIN INTERNATIONAL

***INSIDE ELEVATION OF WEST PARAPET**

* Parapet dimensions are measured at the inside face of parapet

NOTES:

- O.F. - denotes Outside Face
- I.F. - denotes Inside Face

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
BRIDGE APPROACH PAVEMENT (SPECIAL)
NORTH - DETAIL 2

S.N. DESIGNED BY: MAF
SCALE: NTS DRAWN BY: MAF
DATE: MARCH 7, 2006 CHECKED BY: MI

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity = $1.25 \times f_y \times A_t$
 (Tension in kips)
- ② Minimum *Pull-out Strength = $1.25 \times f_{s_{allow}} \times A_t$
 (Tension in kips)

Where f_y = Yield strength of lapped reinforcement bars in ksi.
 $f_{s_{allow}}$ = Allowable tensile stress in lapped reinforcement bars in ksi (Service Load)
 A_t = Tensile stress area of lapped reinforcement bars.
 * = 28 day concrete

BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	5.9
#5	2'-0"	23.0	9.2
#6	2'-7"	33.1	13.3
#7	3'-5"	45.1	18.0
#8	4'-6"	58.9	23.6
#9	5'-9"	75.0	30.0
#10	7'-3"	95.0	38.0
#11	9'-0"	117.4	46.8

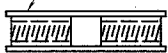
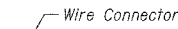
Bar splicer assemblies shall be according to Section 508 of the Standard Specifications, except as noted. The furnishing and installation of bar splicer assemblies will be measured and paid for at the contract unit price each for "BAR SPLICERS."

The diameter of this part is the same as the diameter of the bar spliced.

ROLLED THREAD DOWEL BAR



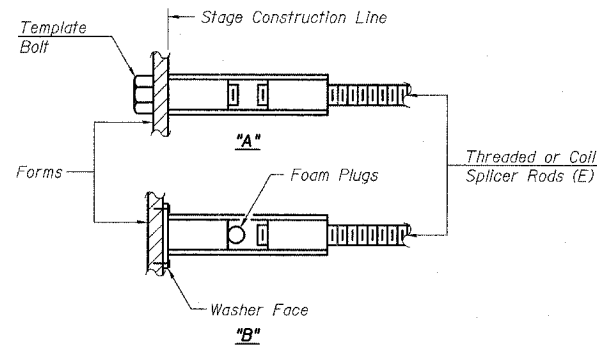
**** ONE PIECE**



WELDED SECTIONS

BAR SPLICER ASSEMBLY ALTERNATIVES

** Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.

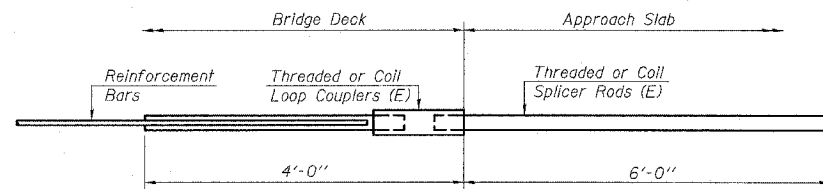


INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.

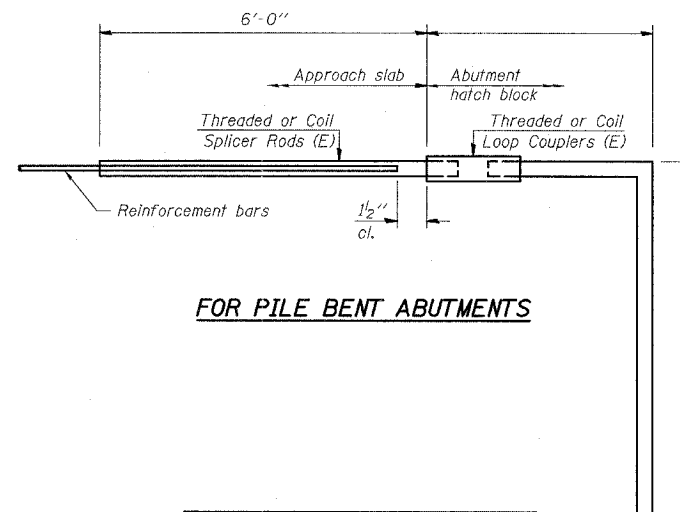
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



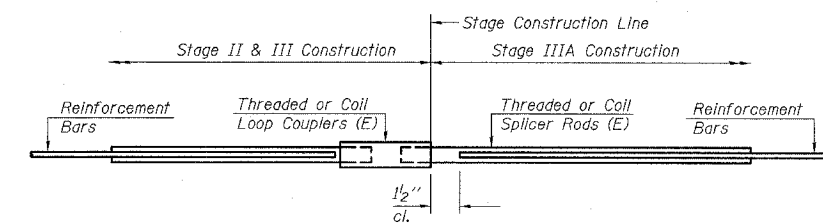
FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 9.2 kips - tension
No. Required =



FOR PILE BENT ABUTMENTS

Bar Splicer for #5 bar
Min. Pull-out Strength = 9.2 kips - tension
No. Required =



STANDARD

Bar Size	No. Assemblies Required	Location
#4	5	S. Appr. Pavement
#5	77	S. Appr. Pavement
#4	5	N. Appr. Pavement
#5	82	N. Appr. Pavement

BSD-1 9-01-03

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

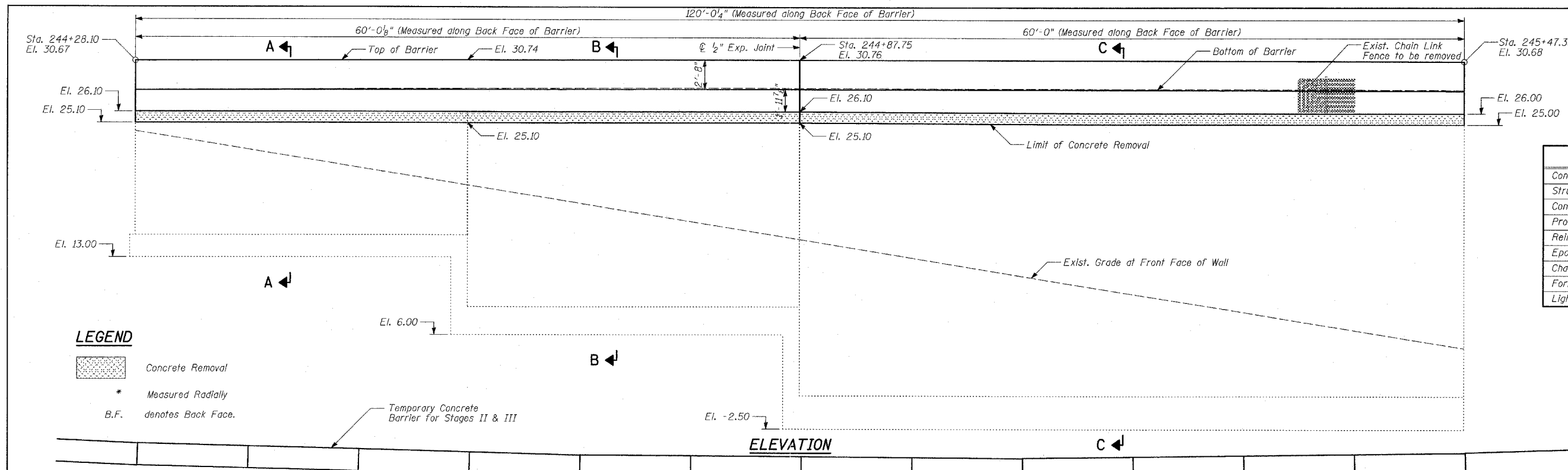
BAR SPLICER ASSEMBLY DETAILS

S.N. DESIGNED BY: MAF
 SCALE: DRAWN BY: MAF
 DATE: MARCH 7, 2006 CHECKED BY: MI

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	633
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-4 62304				

TOTAL BILL OF MATERIAL

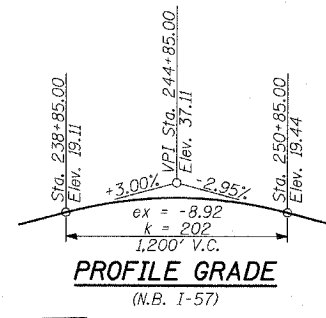
ITEM	UNIT	TOTAL
Concrete Removal	CU YD	5
Structure Excavation	CU YD	821
Concrete Structures	CU YD	85
Protective Coat	SQ YD	241
Reinforcement Bars, Epoxy Coated	FOUND	16,800
Epoxy Crack Sealing	FOOT	100
Chain Link Fence Removal	FOOT	120
Formed Concrete Repair (Depth < 5")	SQ FT	10
Lightweight Cellular Concrete Fill	CU YD	987



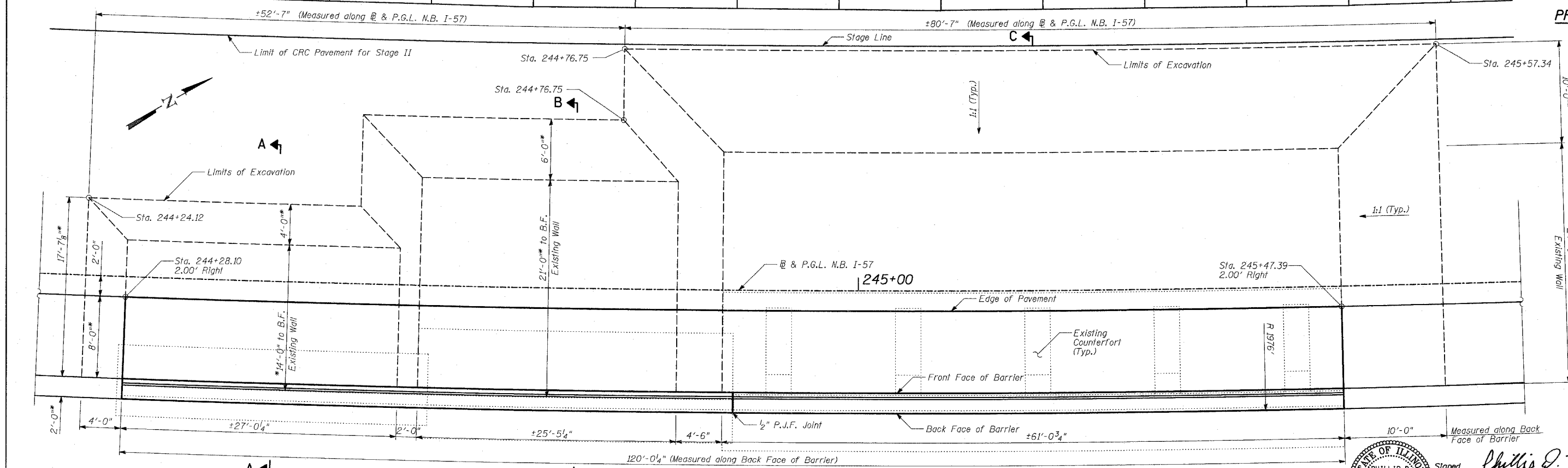
LEGEND

- Concrete Removal
- Measured Radially
- B.F. denotes Back Face.
- Temporary Concrete Barrier for Stages II & III

ELEVATION



PROFILE GRADE
(N.B. I-57)



PLAN

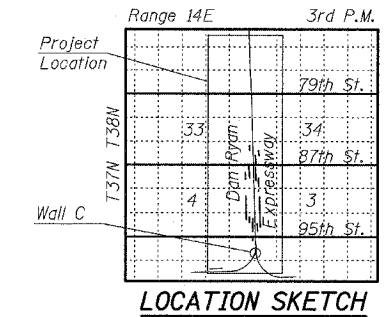
GENERAL NOTES:

- Reinforcement bars shall conform to the requirements of AASHTO M31 or M322 Grade 60.
- Protective Coat shall be applied to exposed surfaces of the concrete facing. See Section on D-D Sheet 3 of 4.
- Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work; however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Existing utilities in conflict with retaining wall reconstruction shall be abandoned or relocated according to directions given on the roadway plans.
- All elevations shown are based on the Chicago City Datum of 0.00, which is 579.19 feet above mean tide New York. (NAVD 88)
- It is the Contractor's responsibility to locate the existing Counterforts. Any repairs due to the damage of the Counterforts during excavation shall be the Contractor's responsibility.

- Place Lightweight Cellular Concrete Fill in lifts not exceeding 2 feet. The material shall be placed to prevent segregation.
- The finished surface of the Lightweight Cellular Concrete Fill shall be primed with an bituminous primer. Cost included with "Lightweight Cellular Concrete Fill".
- Coordinate staging with civil plans.

DESIGN SPECIFICATION
AASHTO 2002 Standard Specifications for Highway Bridges

DESIGN STRESSES
FIELD UNITS
f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)



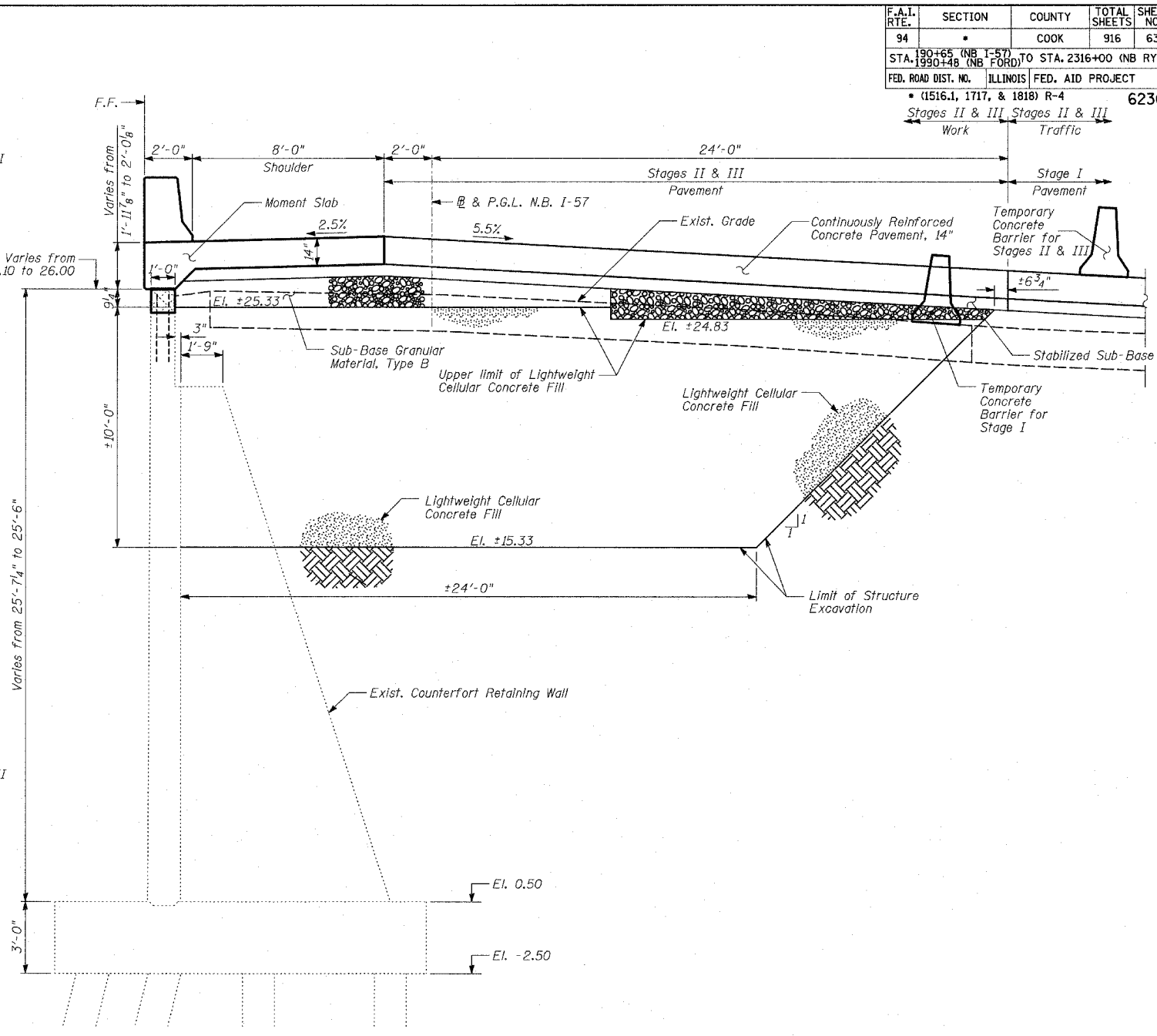
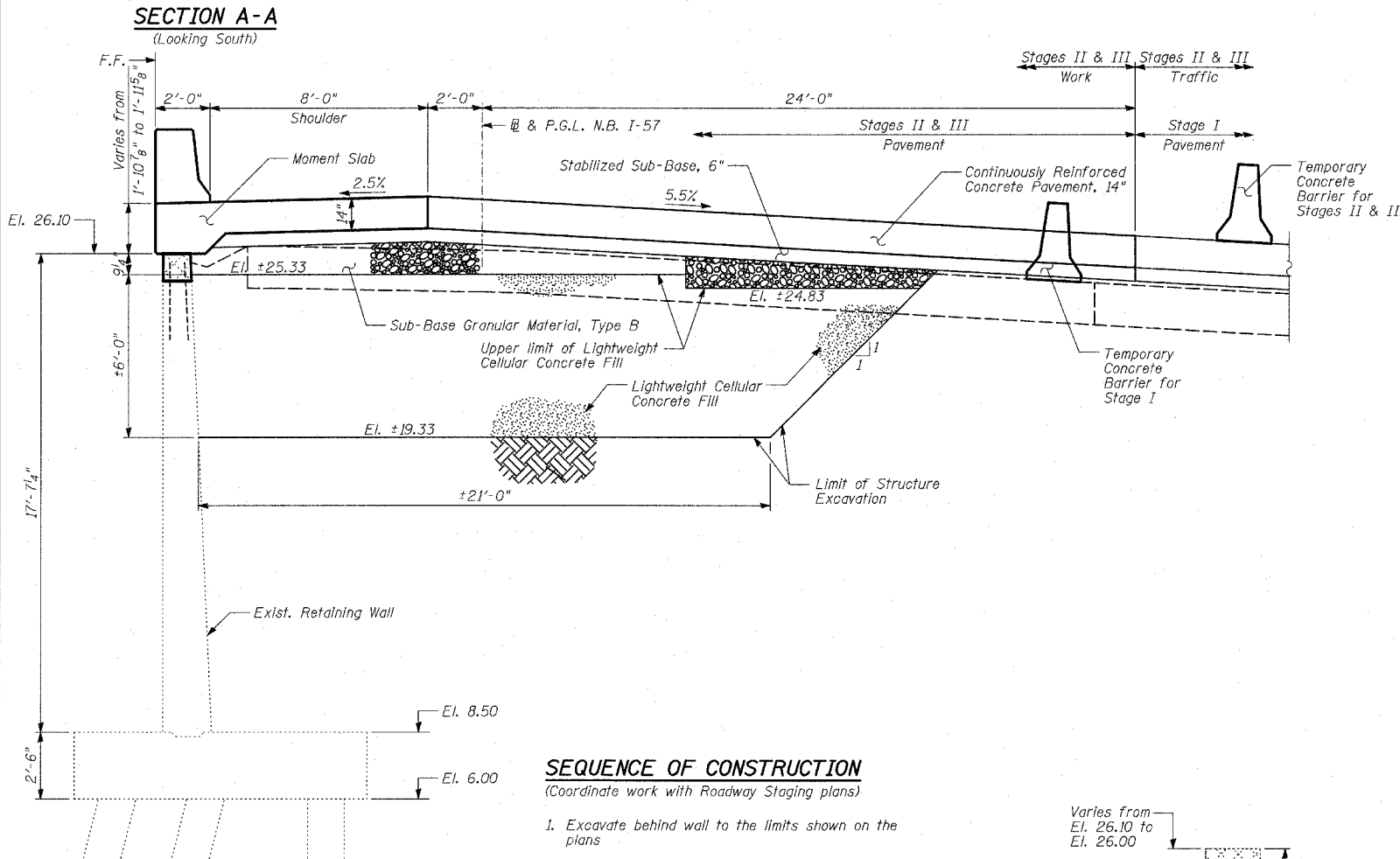
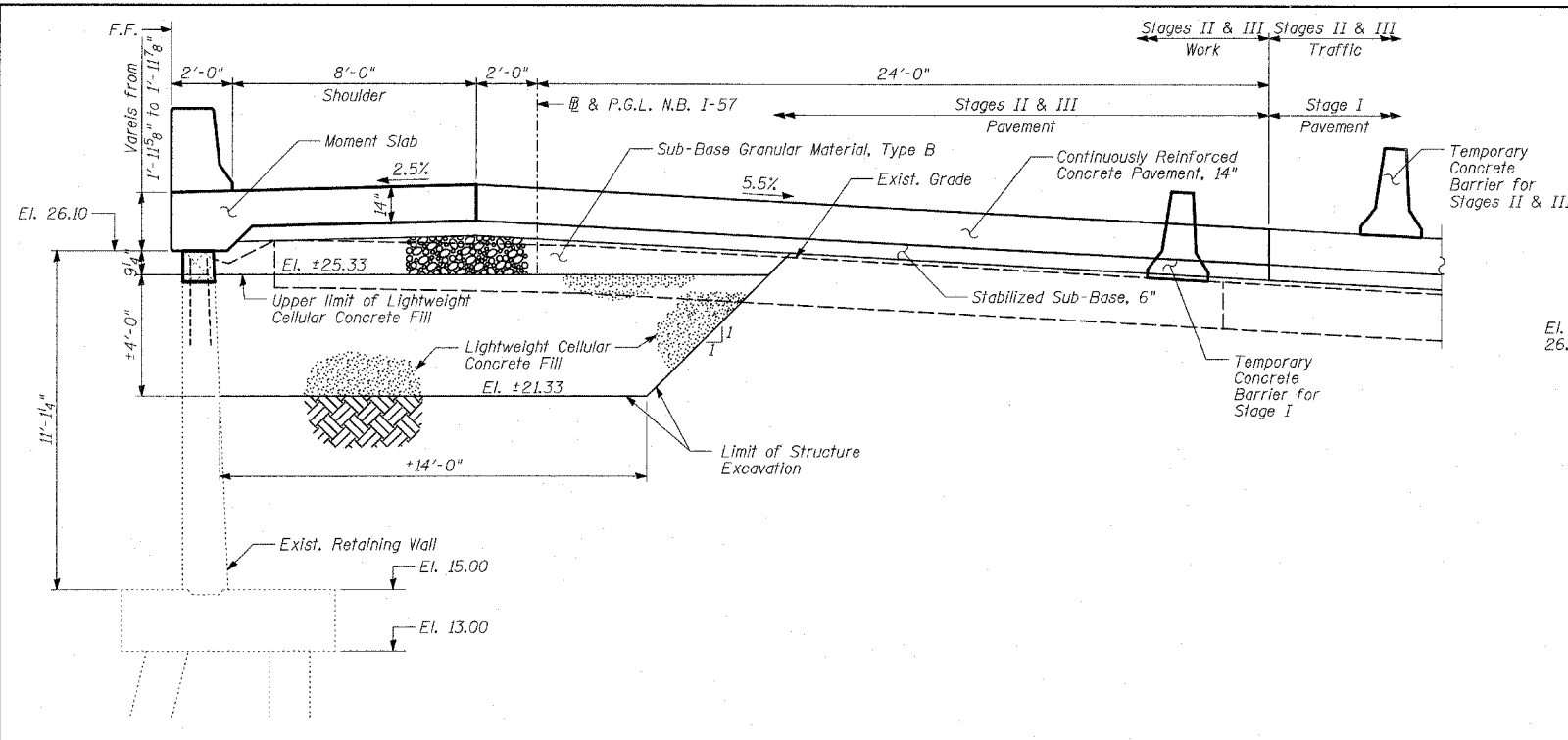
PHILLIP D. FREY
CHICAGO, ILLINOIS
REGISTERED PROFESSIONAL ENGINEER
No. 081-004826
Expires 11-30-2006

Signed *Phillip D. Frey*
Philip D. Frey, P.E., Ill. Lic. No. 081-004826
Date 3/7/06
For drawings 1 thru 4 of 4

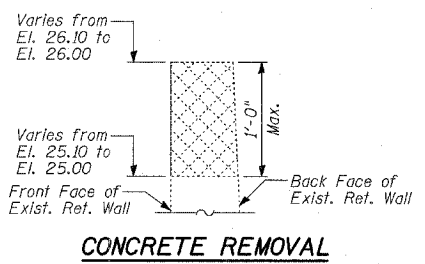
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
WALL C MODIFICATIONS
GENERAL PLAN AND ELEVATION
STA. 244+28.10 TO STA. 245+47.39
S.N. DESIGNED BY: TD, DJR
SCALE: DRAWN BY: DJR
DATE: MARCH 7, 2006 CHECKED BY: MI

TYLIN INTERNATIONAL



- SEQUENCE OF CONSTRUCTION**
(Coordinate work with Roadway Staging plans)
- Excavate behind wall to the limits shown on the plans
 - Remove and reconstruct top of existing Retaining Wall.
 - Backfill with approved Lightweight Cellular Concrete Fill in lifts not exceeding two (2) feet.
 - Place Sub-Base Granular Material, Type B, Stabilized Sub-Base and construct Moment Slab and Parapet.
 - Construct Pavement.



BILL OF MATERIAL

ITEM	UNIT	TOTAL
Concrete Removal	CU YD	5
Structure Excavation	CU YD	821
Lightweight Cellular Concrete Fill	CU YD	987

- NOTES:**
- Lightweight Cellular Concrete Fill shall be placed in layers not exceeding 2 feet in thickness.

LEGEND

- Concrete Removal
- F.F. denotes Front Face of Wall.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

WALL C MODIFICATIONS SECTIONS AND DETAILS

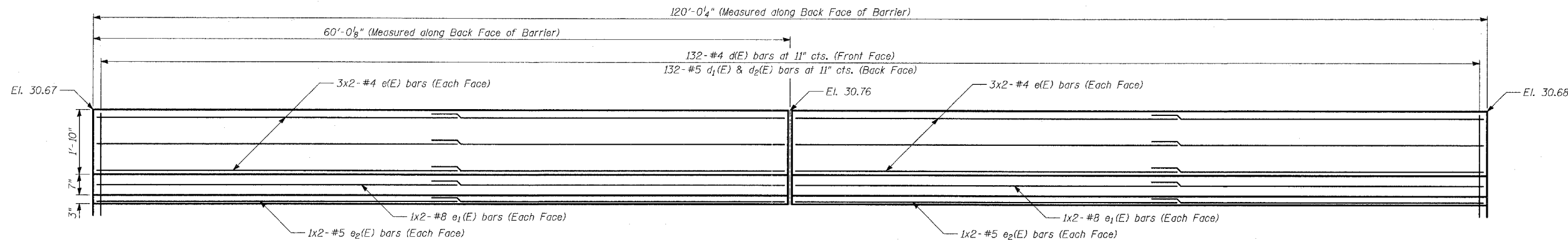
S.N. DESIGNED BY: TD, DJR
SCALE: DRAWN BY: DJR
DATE: MARCH 7, 2006 CHECKED BY: MI

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	635
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 62304				

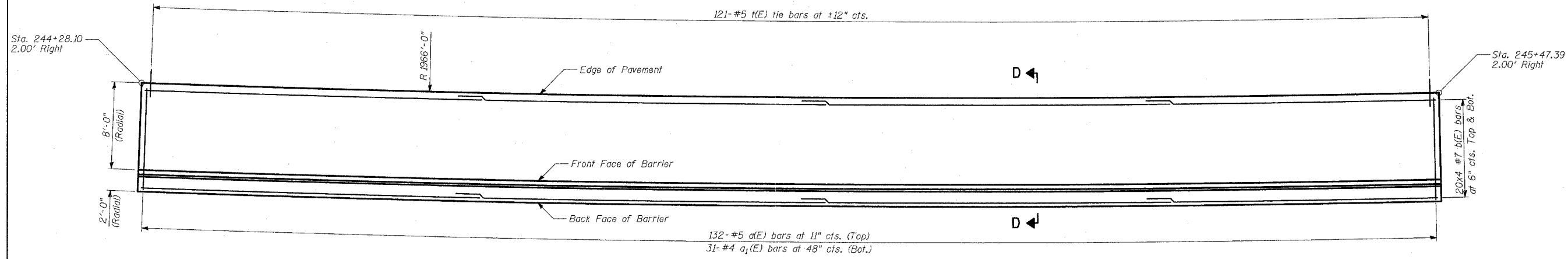
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	132	#5	11'-4"	┌
a ₁ (E)	31	#4	9'-8"	┌
b(E)	160	#7	32'-6"	┌
b ₁ (E)	8	#5	31'-7"	┌
d(E)	132	#4	8'-8"	└
d ₁ (E)	132	#5	5'-4"	└
d ₂ (E)	132	#5	4'-4"	└
e(E)	24	#4	30'-8"	┌
e ₁ (E)	8	#8	32'-1"	┌
e ₂ (E)	8	#5	30'-11"	┌
s(E)	121	#5	2'-4"	└
t(E)	121	#5	2'-6"	└
Reinforcement Bars, Epoxy Coated			POUND	16,800
Concrete Structures			CU YD	85
Protective Coat			SQ YD	241

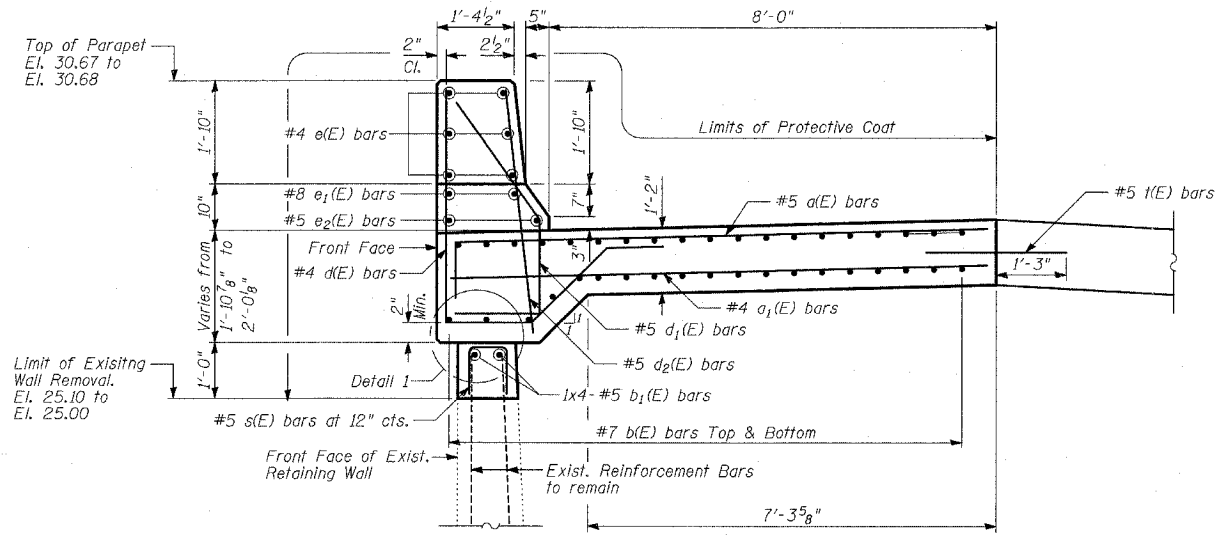
Reinforcement bars designated (E) shall be epoxy coated.



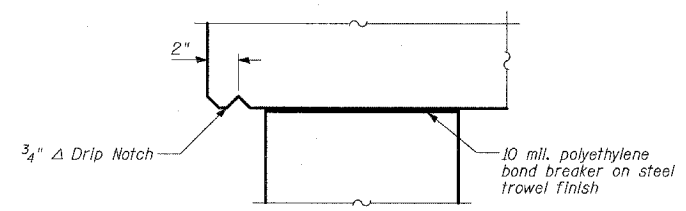
INSIDE ELEVATION OF PARAPET



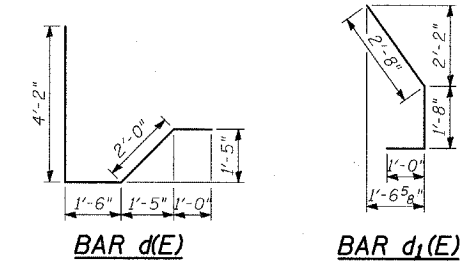
PLAN



SECTION D-D

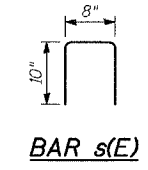


DETAIL 1

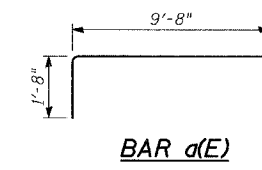


BAR d(E)

BAR d₁(E)



BAR s(E)



BAR a(E)

NOTES:

- Any reinforcement bars that are damaged during concrete removal operations of the retaining wall shall be repaired or replaced using approved bar splicer or anchorage system. Cost included with "Concrete Removal".
- Sand blast clean existing reinforcing bars to be incorporated into new construction. After cleaning, bars shall be evaluated to determine if additional reinforcing bars are required. Damaged or cut bars, or bars that have lost 25% or more of their original cross sectional area shall be supplemented by new in-kind reinforcement bars, to the approval of the Engineer. Cost included with "Concrete Removal".
- Bars indicated thus 3x2-#4 etc. indicates 3 lines of bars with 2 lengths per line.
- All exposed concrete edges shall be chamfered 3/4" except as noted.

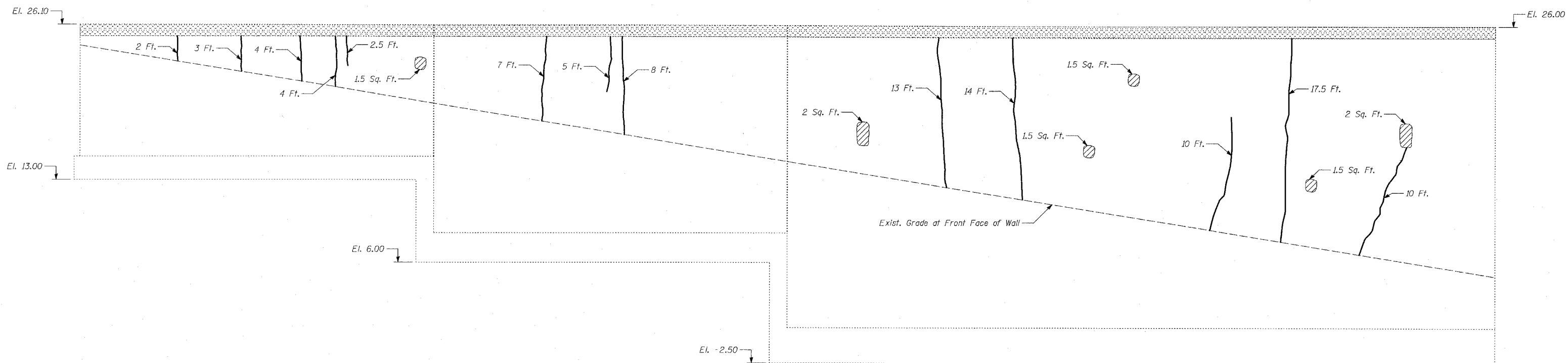
LAP SPLICES

Bar	Lap
#4	1'-8"
#5	2'-2"
#7	3'-5"
#8	4'-6"

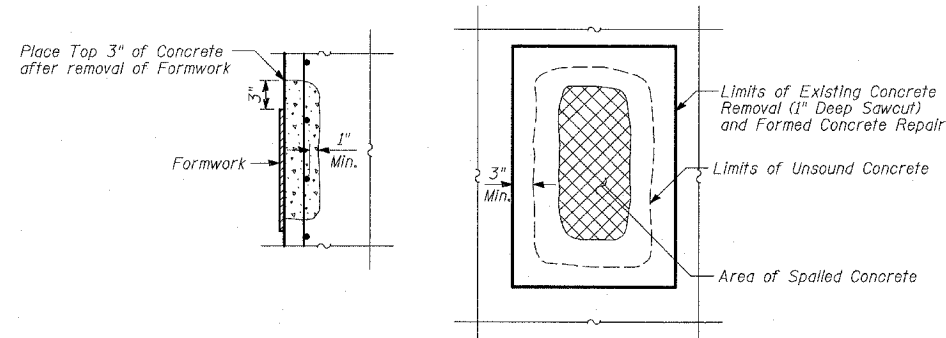
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
WALL C MODIFICATIONS
DETAILS AND BILL OF MATERIAL
 STA. 244+28.10 TO STA. 245+47.39
 S.N. DESIGNED BY: TD, DJR
 SCALE: DRAWN BY: DJR
 DATE: MARCH 7, 2006 CHECKED BY: MI

TYLIN INTERNATIONAL



ELEVATION



FORMED CONCRETE REPAIR

LEGEND

- Indicates Cracks (In feet)
- Formed Concrete Repair
- Concrete Removal

BILL OF MATERIAL

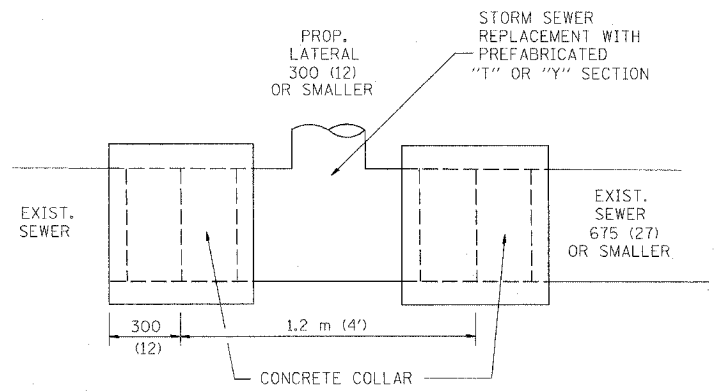
ITEM	UNIT	TOTAL
Epoxy Crack Sealing	FOOT	100
Formed Concrete Repair (Depth < 5")	SQ FT	10

TYLIN INTERNATIONAL

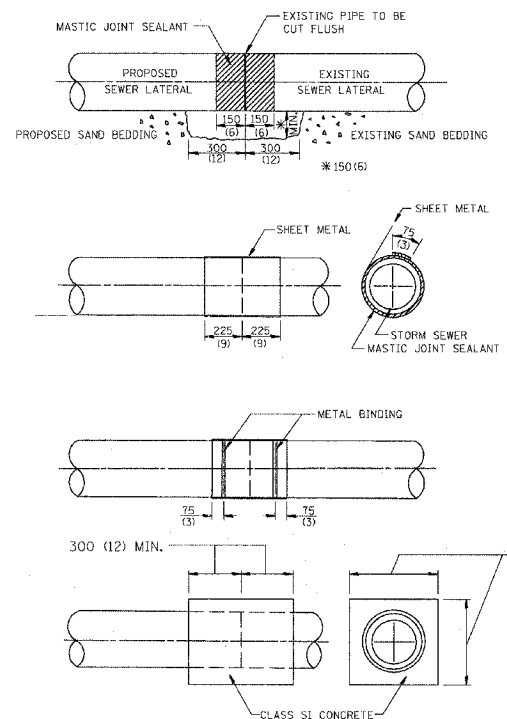
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
WALL C MODIFICATIONS
EXISTING RETAINING WALL REPAIRS

S.N. DESIGNED BY: DJR
 SCALE: DRAWN BY: DJR
 DATE: MARCH 7, 2006 CHECKED BY: MI



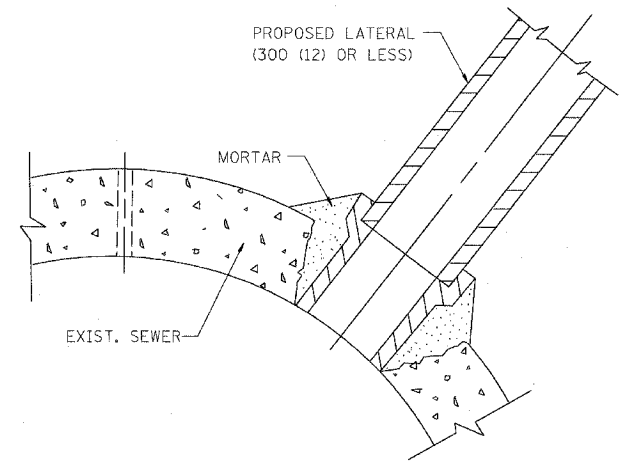
DETAIL "A"
LATERAL CONNECTION TO EXISTING SEWER
OF 675 (27) OR SMALLER



DETAIL "B"
CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 150 (6) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 300 x 150 (12 x 6) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 450 (18) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 75 (3) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 225 (9) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 75 (3) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"
PROPOSED LATERAL
CONNECTION TO EXISTING SEWER
OF 750 (30) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 675 (27) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 750 (30) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER

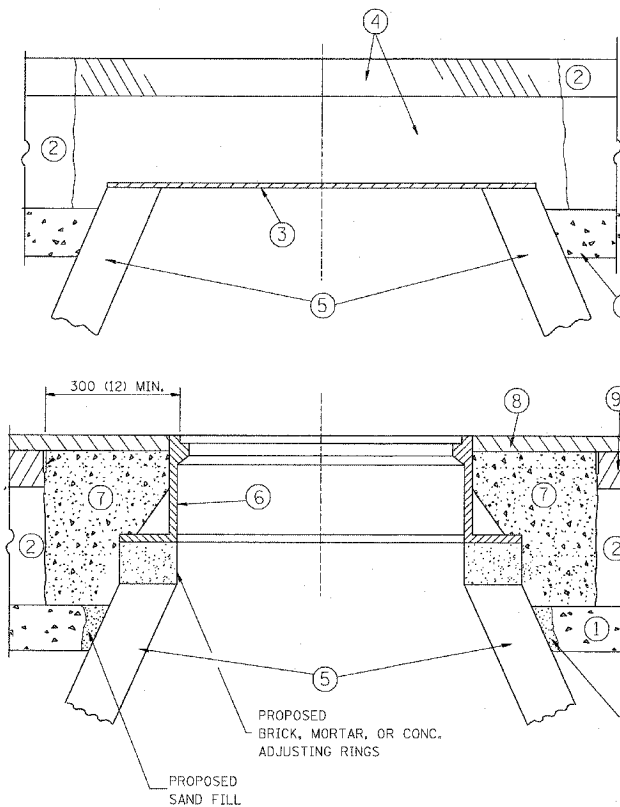
REVISIONS	
NAME	DATE
M. DE YONG	07/25/90
M. DE YONG	02/05/92
M. DE YONG	05/08/92
R. SHAH	09/09/94
R. SHAH	10/25/94
R. SHAH	06/12/96

SCALE: NONE
DATE: 10/18/2002

DRAWN BY CADD
CHECKED BY

BD500-01 (BD-7)
REVISION DATE: 06/12/96

F.A. SITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			916	638
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 300 (12) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 900 (36) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 40 (1 1/2) THICK BITUMINOUS MATERIAL APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE BITUMINOUS MATERIAL AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 900 (36) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND BITUMINOUS MATERIAL
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL
- ⑧ PROPOSED BITUMINOUS CONCRETE SURFACE COURSE
- ⑨ PROPOSED BITUMINOUS CONCRETE BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: FRAMES AND LIDS TO BE ADJUSTED, SPECIAL EACH

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN

ILLINOIS DEPARTMENT OF TRANSPORTATION

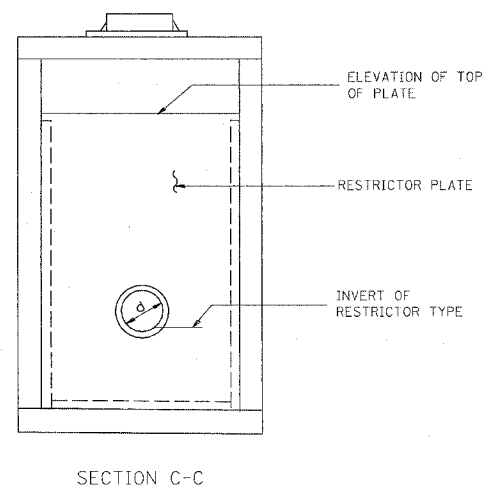
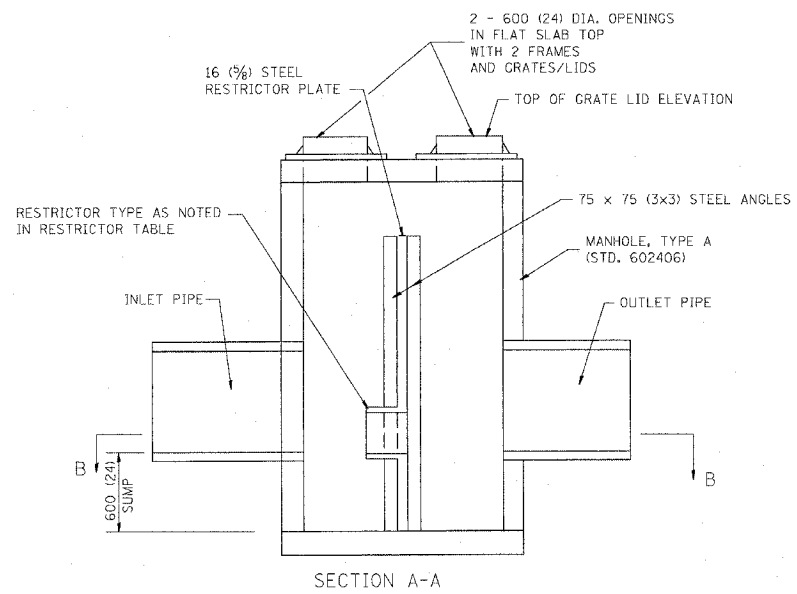
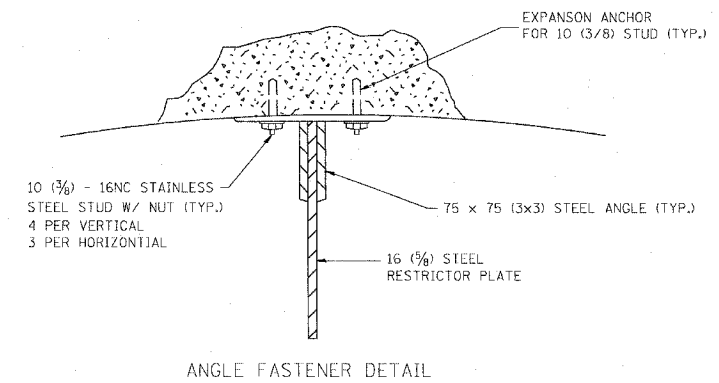
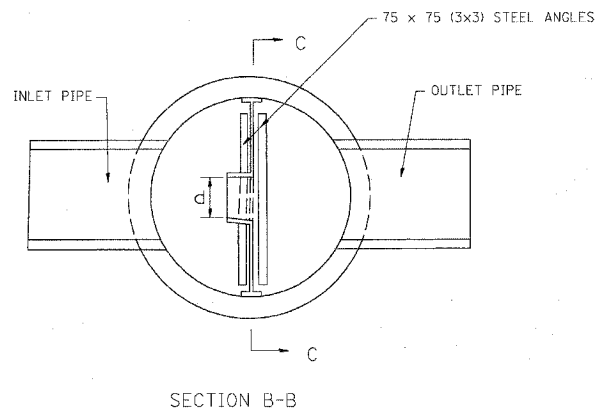
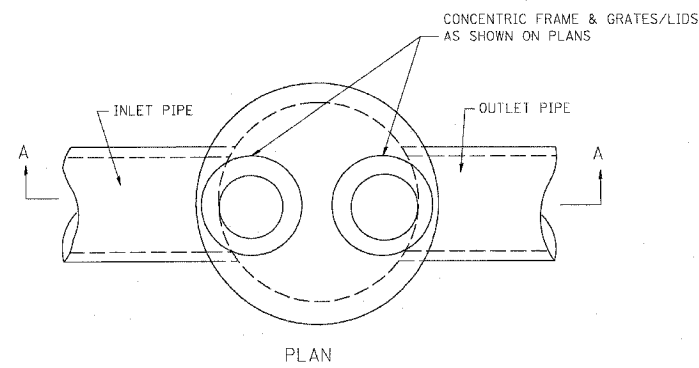
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04

SCALE: NONE
DATE: 05/17/2004

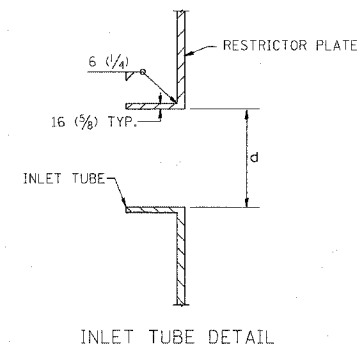
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CHECKED BY

BD600-03 (BD-8)
REVISION DATE: 05/17/04

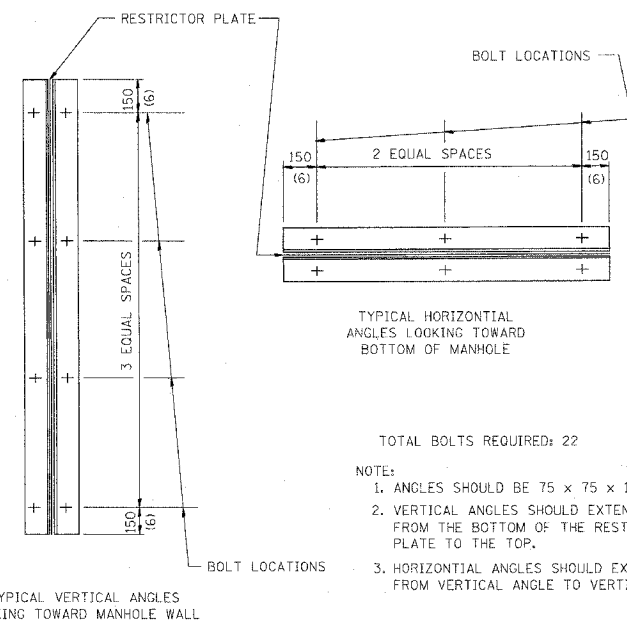


NOTES:

1. ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION.
2. ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.
3. BASIS OF PAYMENT: "MANHOLES, TYPE A, 1.8m (6FT.) DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE" EACH



STATION	MANHOLE DIAMETER	FRAME AND GRATE	RESTRICTOR TYPE	INSIDE RESTRICTOR TYPE DIAMETER mm (in.) (d)	INVERT OF RESTRICTOR TYPE	ELEVATION OF TOP OF PLATE OVERFLOW
233+41.77	6'	TIF CL	2	15"	-8.18	-2.18
408+76.91	6'	TIF CL	2	12"	-4.40	1.60
249+61.25	6'	TIF CL	2	15"	4.66	10.66
2040+22.06	6'	TIF CL	2	36"	-11.38	-5.38
2003+27.99	6'	TIF CL	2	15"	-8.70	-2.70



- TOTAL BOLTS REQUIRED: 22
- NOTE:
1. ANGLES SHOULD BE 75 x 75 x 10 (3x3x3/8)
 2. VERTICAL ANGLES SHOULD EXTEND FROM THE BOTTOM OF THE RESTRICTOR PLATE TO THE TOP.
 3. HORIZONTAL ANGLES SHOULD EXTEND FROM VERTICAL ANGLE TO VERTICAL ANGLE.

RESTRICTOR TYPE					
1	2	3	4	5	6
RE-ENTRANT TUBE	SHARP EDGED	SQUARE EDGED	RE-ENTRANT TUBE	SQUARE EDGED	ROUNDED
LENGTH: 1/2 TO 1 DIA.		STREAM CLEARS SIDES	LENGTH: 2-1/2 DIA.	LENGTH: 2-1/2 DIA.	
C=.52	C=.61	C=.61	C=.73	C=.82	C=.98

VALUES OF "C" FOR CIRCULAR AND SQUARE ORIFICES

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

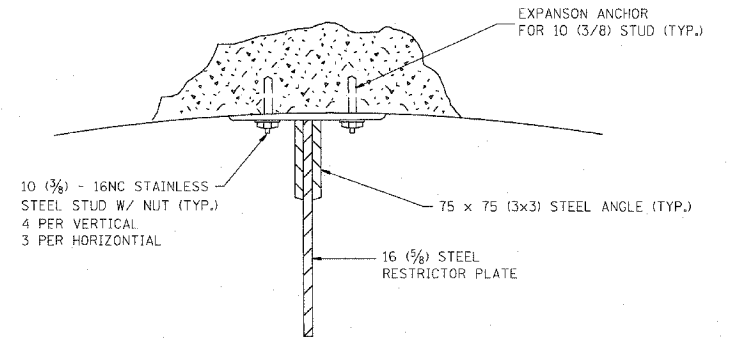
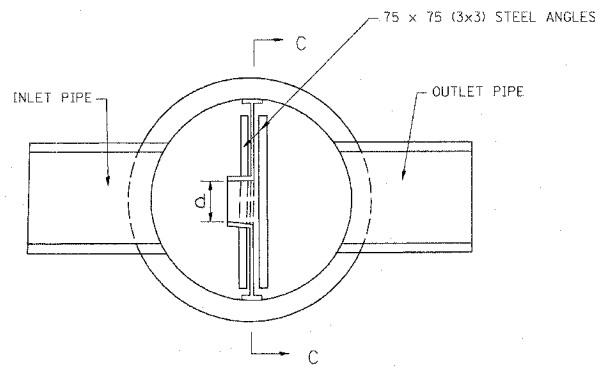
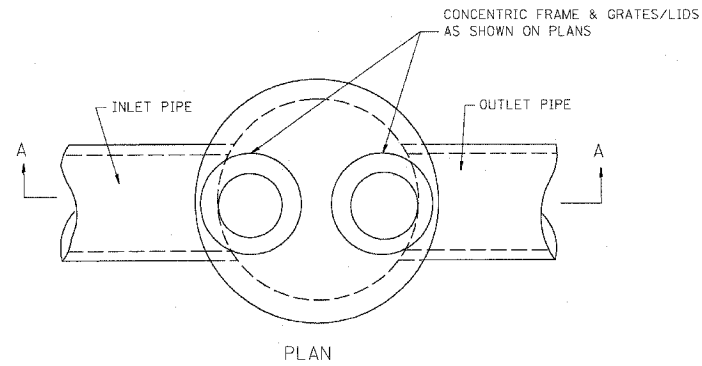
MANHOLE WITH RESTRICTOR PLATE

REVISIONS		
NAME	DATE	
R. SHAH	09/09/94	
R. SHAH	10/25/94	
F. GOMEZ	08/28/00	
M. GOMEZ	01/08/01	

SCALE: NONE
DATE 10/18/2002

DRAWN BY
CHECKED BY
BD600-04 (BD-12)

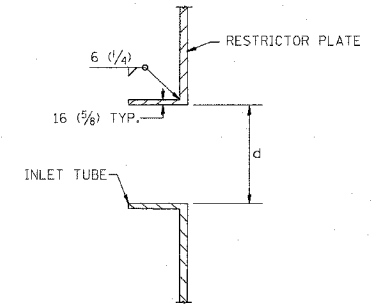
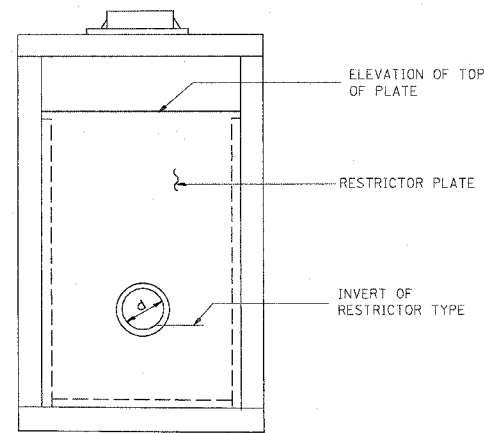
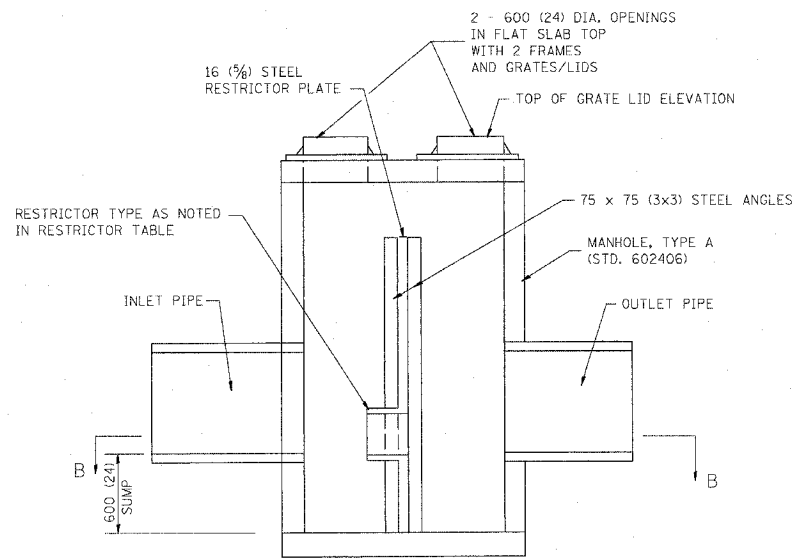
62304



ANGLE FASTENER DETAIL

NOTES:

1. ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION.
2. ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.
3. BASIS OF PAYMENT: "MANHOLES, TYPE A, 1.8m (6FT.) DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE" EACH



INLET TUBE DETAIL

493

496

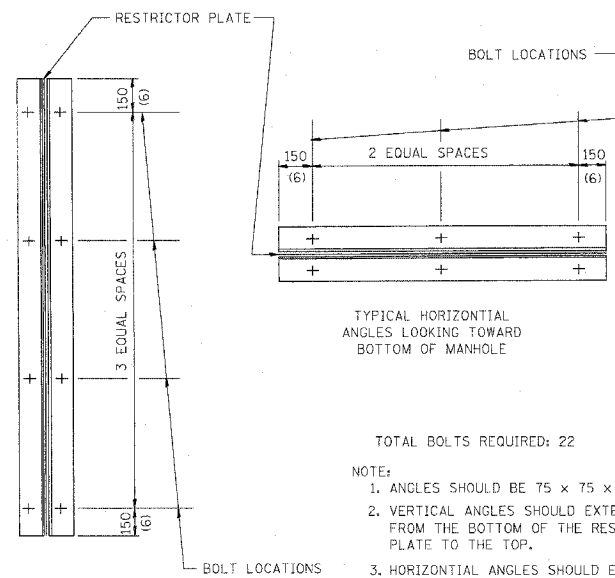
501

542

551

582

STATION	MANHOLE DIAMETER	FRAME AND GRATE	RESTRICTOR TYPE	INSIDE RESTRICTOR TYPE DIAMETER mm (In.) (d)	INVERT OF RESTRICTOR TYPE	ELEVATION OF TOP OF PLATE OVERFLOW
2017+17.21	6'	TIF CL	2	24"	-8.18	-2.33
2017+44.82	6'	TIF CL	2	15"	-8.33	-2.82
2021+06.35	6'	TIF CL	2	30"	-18.54	-12.54
2036+24.12	6'	TIF CL	2	12"	-6.53	-1.10
329+76.99	6'	TIF CL	2	21"	-26.43	-20.43
238+38.68	6'	TIF CL	2	15"	-16.22	-10.22



TOTAL BOLTS REQUIRED: 22

NOTE:

1. ANGLES SHOULD BE 75 x 75 x 10 (3x3x3/8)
2. VERTICAL ANGLES SHOULD EXTEND FROM THE BOTTOM OF THE RESTRICTOR PLATE TO THE TOP.
3. HORIZONTAL ANGLES SHOULD EXTEND FROM VERTICAL ANGLE TO VERTICAL ANGLE.

RESTRICTOR TYPE					
1	2	3	4	5	6
RE-ENTRANT TUBE	SHARP EDGED	SQUARE EDGED	RE-ENTRANT TUBE	SQUARE EDGED	ROUNDED
LENGTH: 1/2 TO 1 DIA.		STREAM CLEARS SIDES	LENGTH: 2-1/2 DIA.	LENGTH: 2-1/2 DIA.	
C=.52	C=.61	C=.61	C=.73	C=.82	C=.98

VALUES OF "C" FOR CIRCULAR AND SQUARE ORIFICES

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

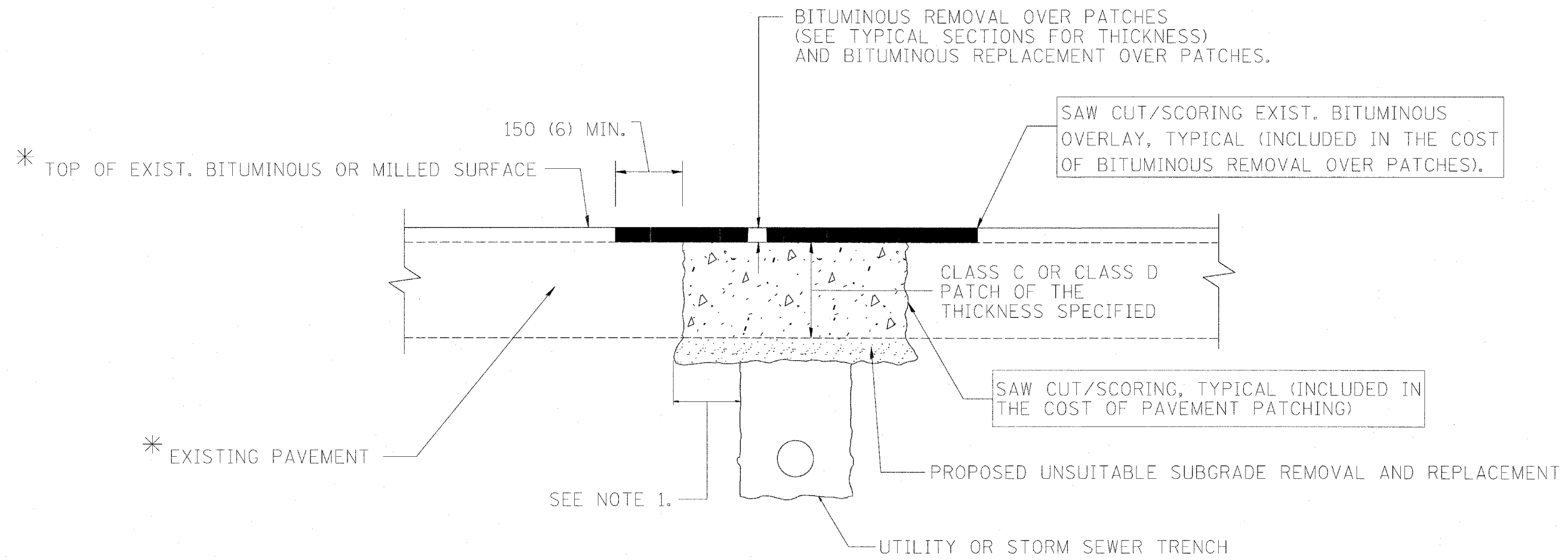
MANHOLE WITH RESTRICTOR PLATE

REVISIONS	
NAME	DATE
R. SHAH	09/09/94
R. SHAH	10/25/94
E. GOMEZ	08/28/00
M. GOMEZ	01/08/01

SCALE: NONE
DATE 10/18/2002

DRAWN BY
CHECKED BY
BD600-04 (BD-12)

SHEET 2 OF 2



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 300 (12) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE SPECIAL PROVISION "PATCHING WITH BITUMINOUS OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

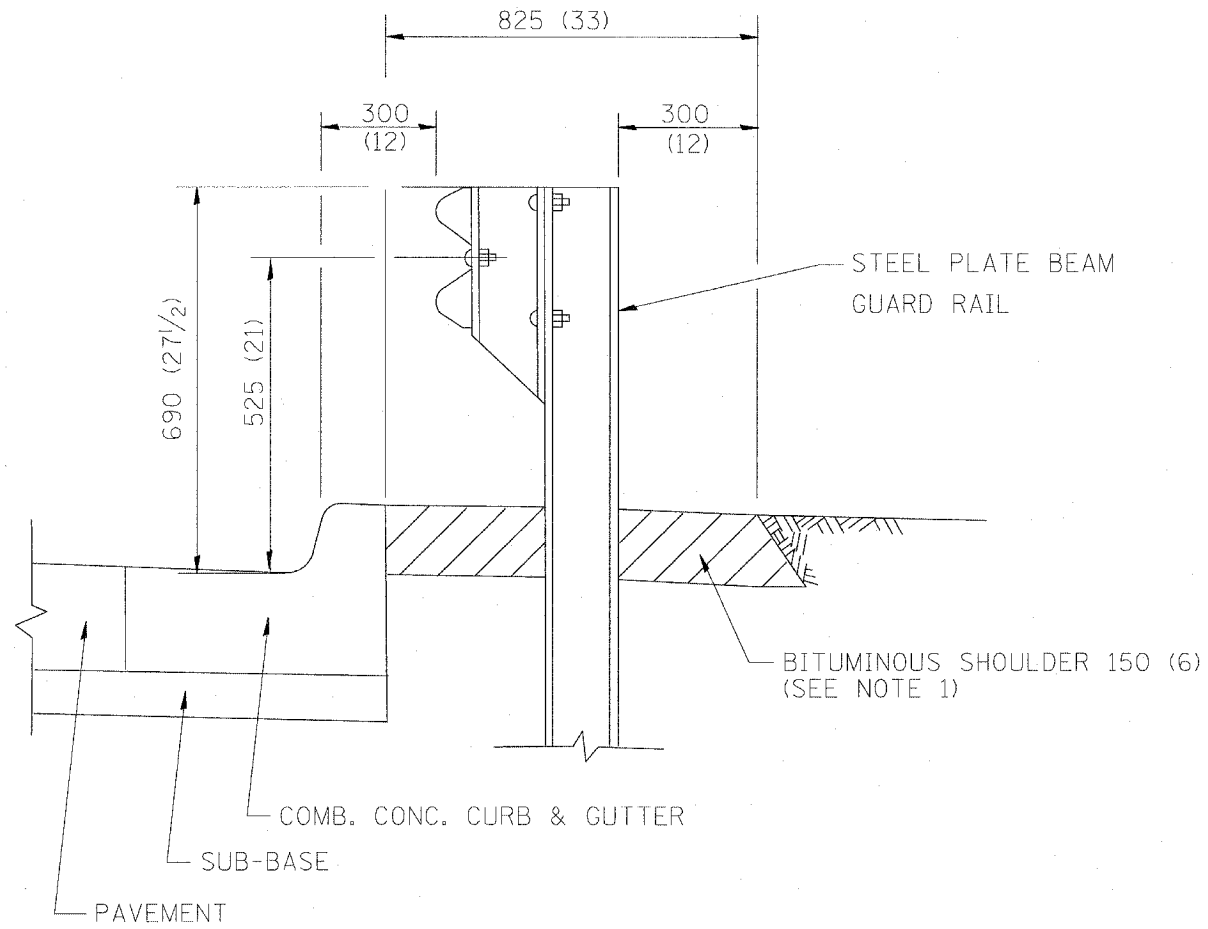
PAVEMENT PATCHING FOR BITUMINOUS SURFACED PAVEMENT

REVISIONS		REVISIONS	
NAME	DATE	NAME	DATE
R. SHAH	10/25/94	ART ABBAS	04/27/98
R. SHAH	01/14/95		
R. SHAH	03/23/95		
R. SHAH	04/24/95		
A. HOUSEH	03/15/96		
A. ABBAS	03/21/97		
A. ABBAS	01/20/98		

SCALE: NONE
DATE 10/18/2002

DRAWN BY
CHECKED BY
BD400-04 (BD-22)
REVISION DATE: 04/27/98

F. A. RTG.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			916	642
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

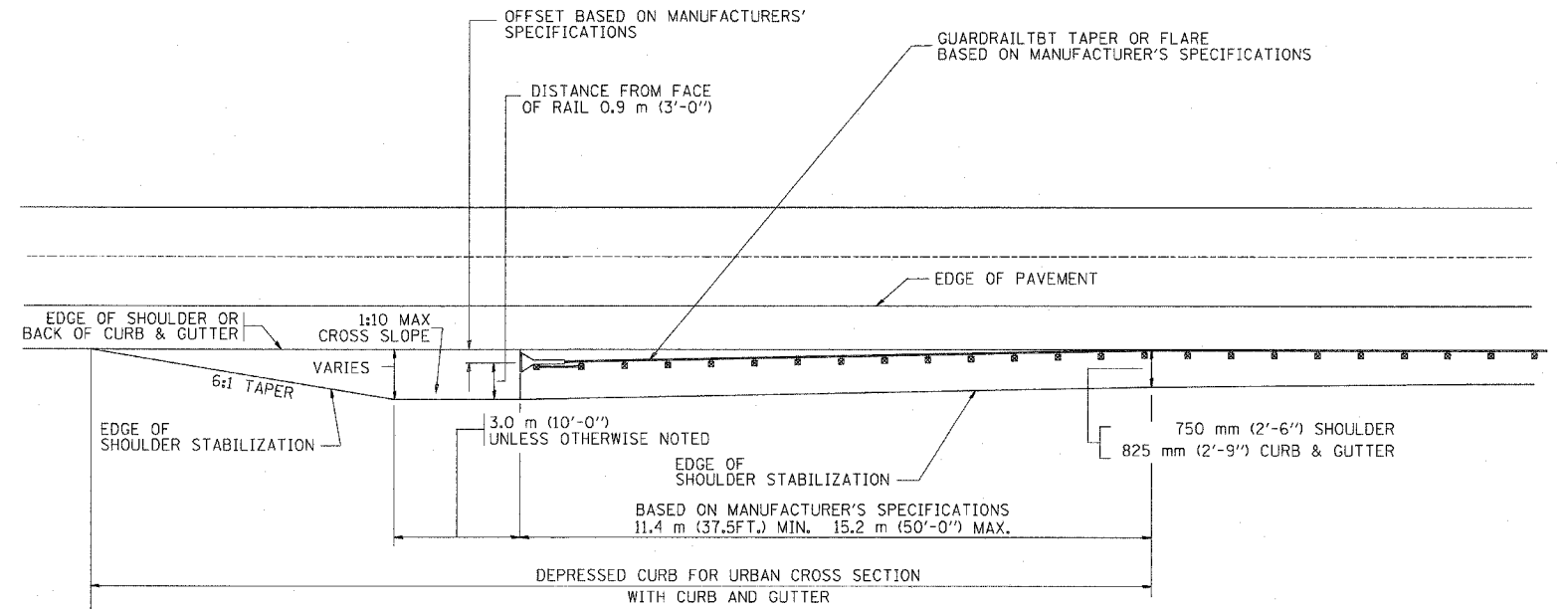


- NOTES: 1. THE BITUMINOUS SHOULDER SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL
2. GUARD RAIL MAY BE PLACED AT THE BACK OF CURB WHEN DIRECTED BY THE ENGINEER.

BASIS OF PAYMENT: BITUMINOUS SHOULDER 150 (6) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER m² (sq. yd.) AS "BITUMINOUS SHOULDER 150 (6)."

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER
[FOR ROADWAY SPEED 60 kmh (35 MPH) TO 70 kmh (45 MPH)]



STABILIZATION AT TBT TY. 1 SPL.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER
STABILIZATION AT TBT TY 1 SPL.

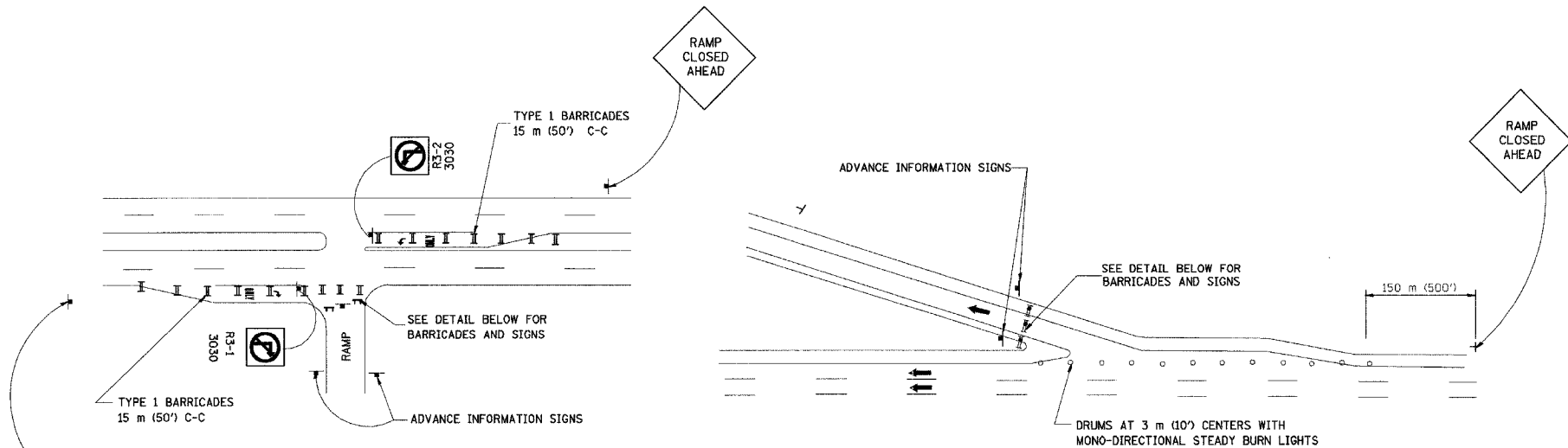
REVISIONS	
NAME	DATE
M. DE YONG	09-22-90
M. DE YONG	07-14-92
R. SHAH	09/09/94
R. SHAH	10/25/94
R. SHAH	02/23/95
A. ABBAS	03/21/97
E. GOMEZ	08/28/00

SCALE: NONE
DATE 10/18/2002

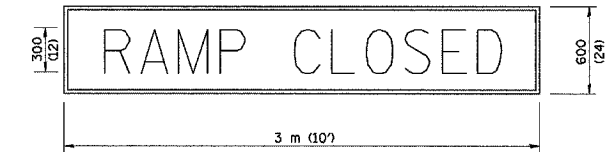
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CHECKED BY

BD600-10 (BD 34)
REVISION DATE: 08/28/00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			916	613
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



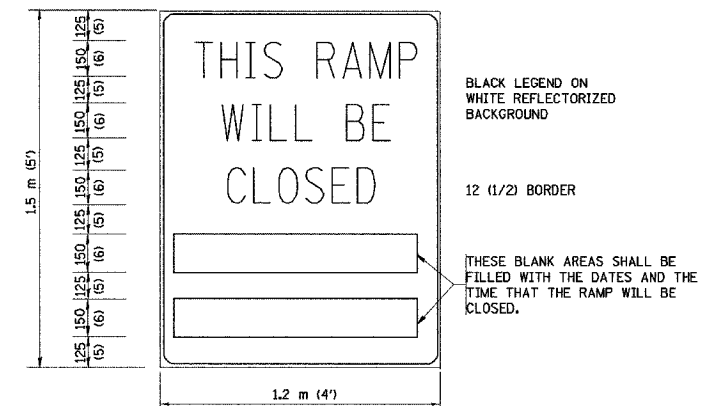
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE REFLECTORIZED BACKGROUND
25 (1) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR THE CLOSED EXIT RAMP.

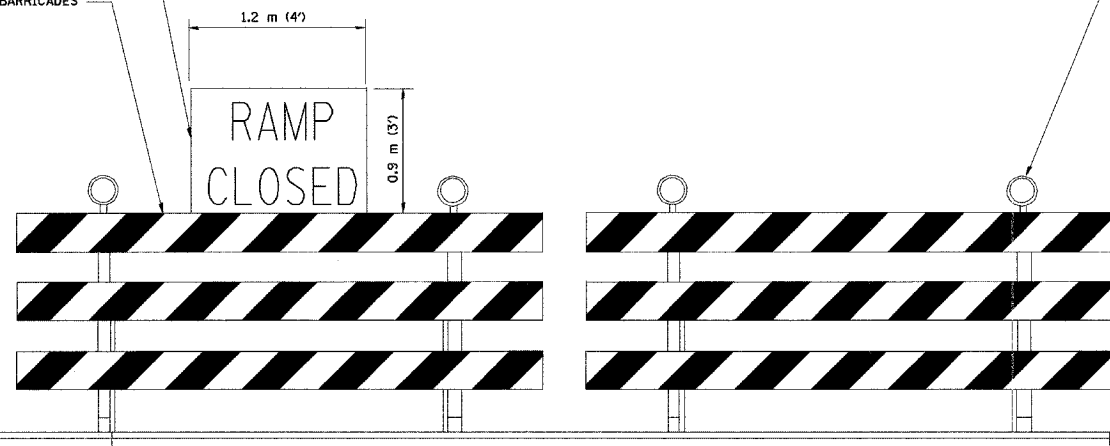
RAMP CLOSURE ADVANCE INFORMATION SIGN



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, 4 MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THE "RAMP CLOSED" SIGN SHALL BE B/W WITH 200 (8) CAPS. IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS DIRECTLY IN FRONT OF THE BARRICADE.

TYPE III BARRICADES



DETAIL FOR REQUIRED BARRICADES & SIGNS

GENERAL NOTES:

1. CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 700 (28) HIGH.
2. STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
3. A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
4. FOR DAYTIME RAMP CLOSURES, LASTING 6 HOURS OR LESS, THE CONTRACTOR MAY ELIMINATE THE ADVANCE WARNING SIGNS ON THE EXIT GUIDE SIGNS.
5. ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
6. THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE CONSIDERED INCIDENTAL TO TRAFFIC CONTROL AND PROTECTION.
7. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.

NOTES:

1. CONES MAY BE SUBSTITUTED FOR TYPE I AND TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28" IN HEIGHT.
2. STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
3. THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS.
4. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY FOUR (24) HOURS IN LENGTH.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

FREWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS

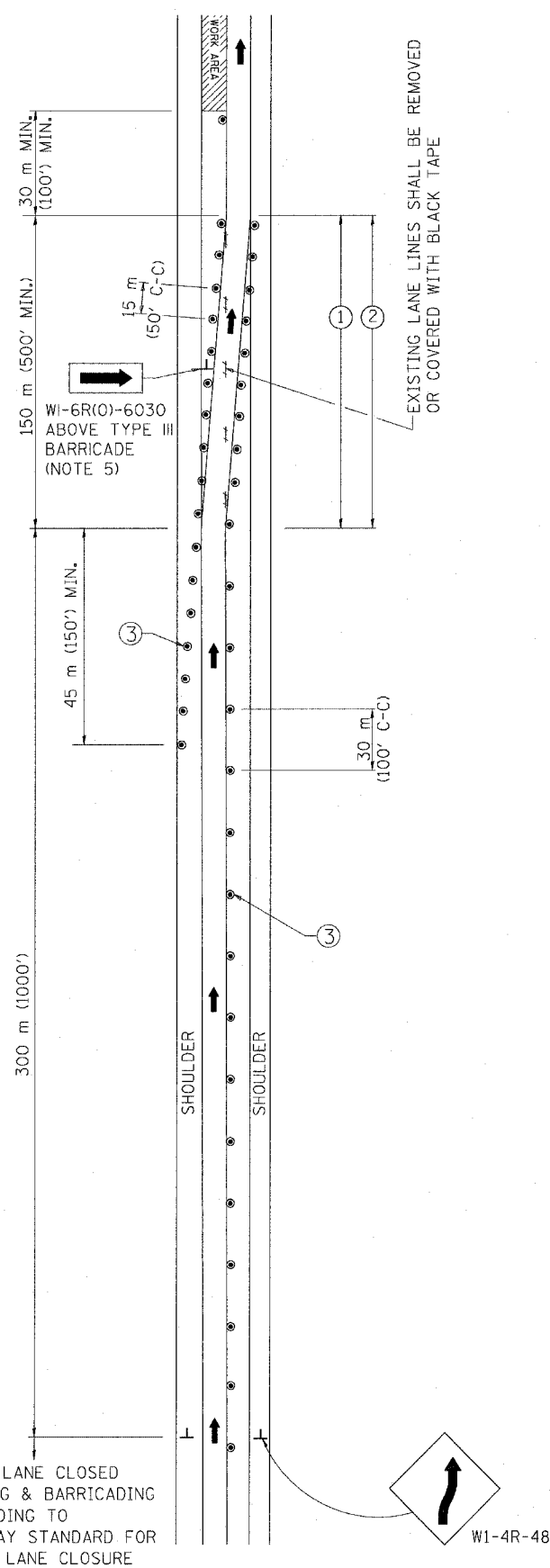
REVISIONS	
NAME	DATE
DWS	2-83
DWS	1/90
DWS	9/94
DWS	12/94
DWS/JAF	12/02
Revise devices to meet NCHRP 350	4/03

SCALE: NONE
DATE 05/06/2003

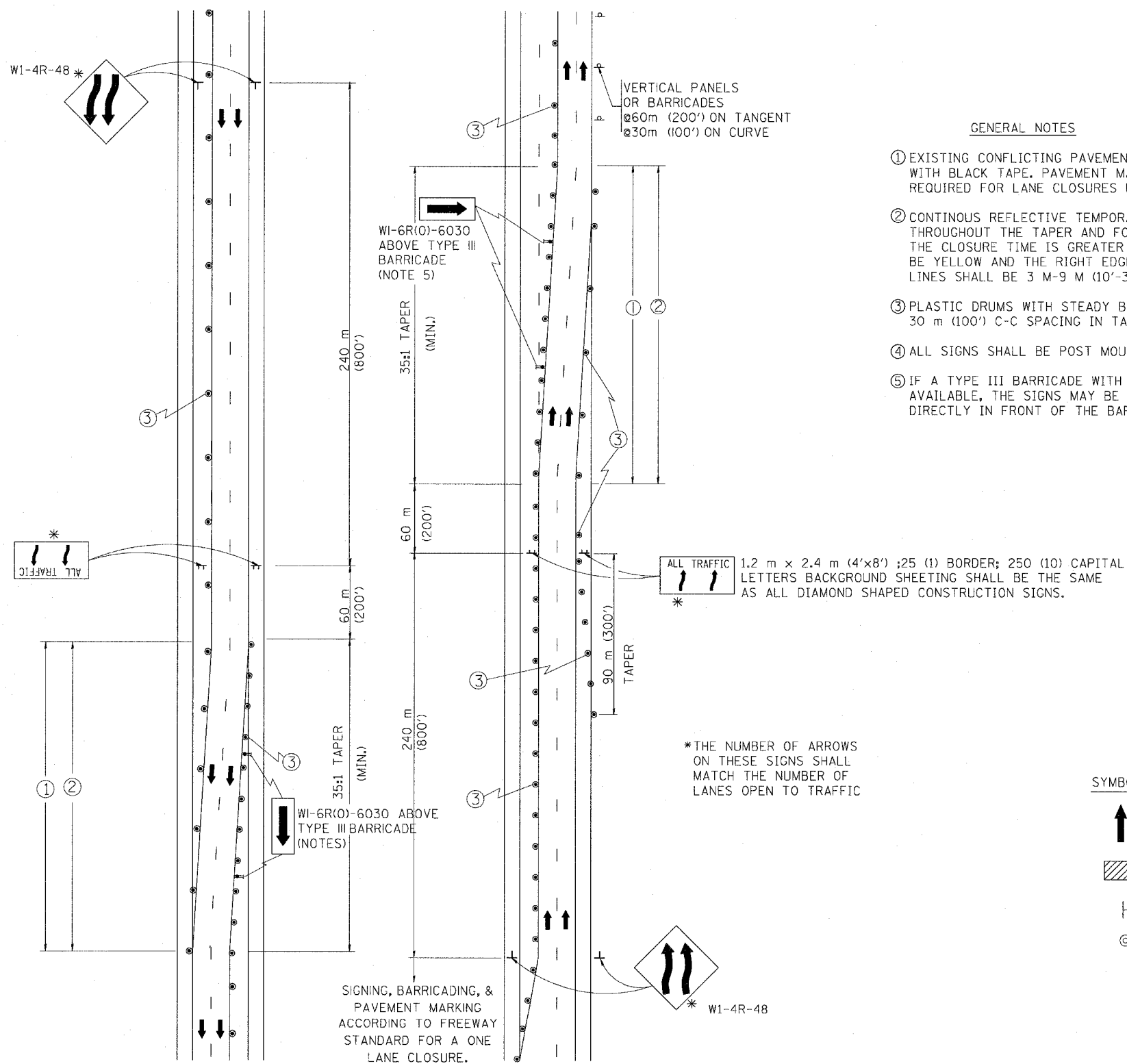
DRAWN BY
CHECKED BY
TC-8

REVISION DATE: 04/03/03

SINGLE LANE WEAVE



MULTI-LANE WEAVE



F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			916	644
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

GENERAL NOTES

- EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED OR COVERED WITH BLACK TAPE. PAVEMENT MARKING REMOVAL OR BLACK TAPE SHALL NOT BE REQUIRED FOR LANE CLOSURES UNDER 24 HOURS IN DURATION.
- CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 90 m (300') ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVE LANE LINES SHALL BE 3 M-9 M (10'-30') SKIP DASH, WHITE.
- PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 15 m (50') C-C SPACING IN TAPERS AND 30 m (100') C-C SPACING IN TANGENTS.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS DIRECTLY IN FRONT OF THE BARRICADE.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL DETAILS
 FOR FREEWAY
 SINGLE & MULTI-LANE WEAVE

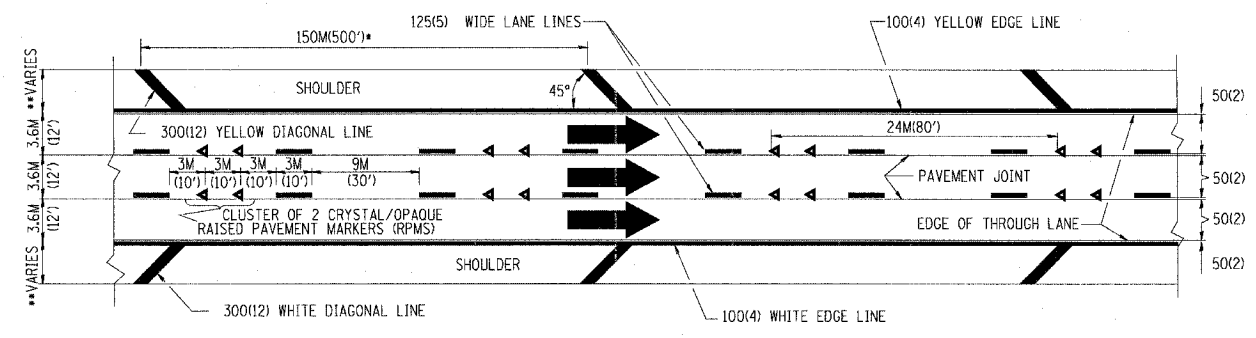
REVISIONS	
NAME	DATE
DWS	2/87
DWS	1/90
DWS	12/27/94
DWS	11/96
JAF	4/03

SCALE: NONE
 DATE: 05/12/2003

DRAWN BY R.H.
 CHECKED BY TC-9

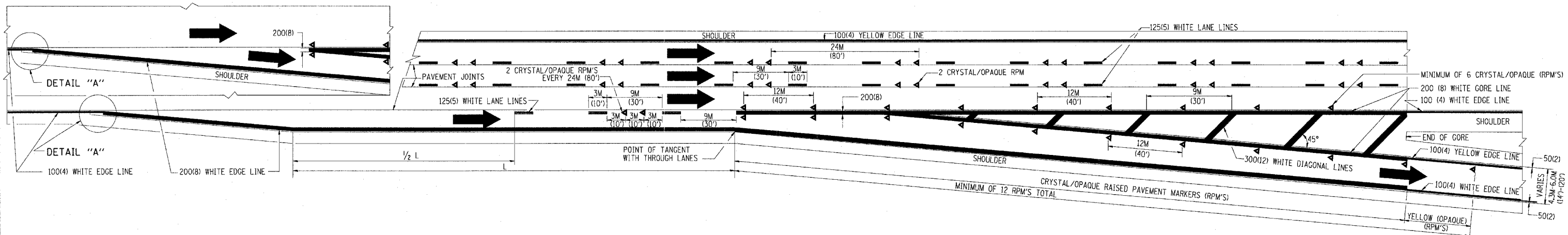
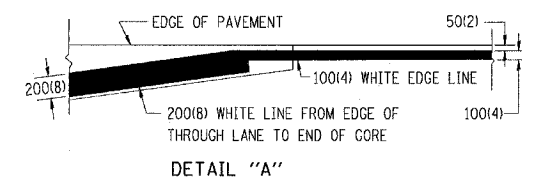
F. A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			916	645
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

- THE DIAGONAL LINES SHALL BE SPACED AT 12M (40') C-C ACROSS ALL STRUCTURES WHICH ARE 150M (500') OR LESS IN LENGTH
- THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 1.8M (6') OR LESS IN WIDTH

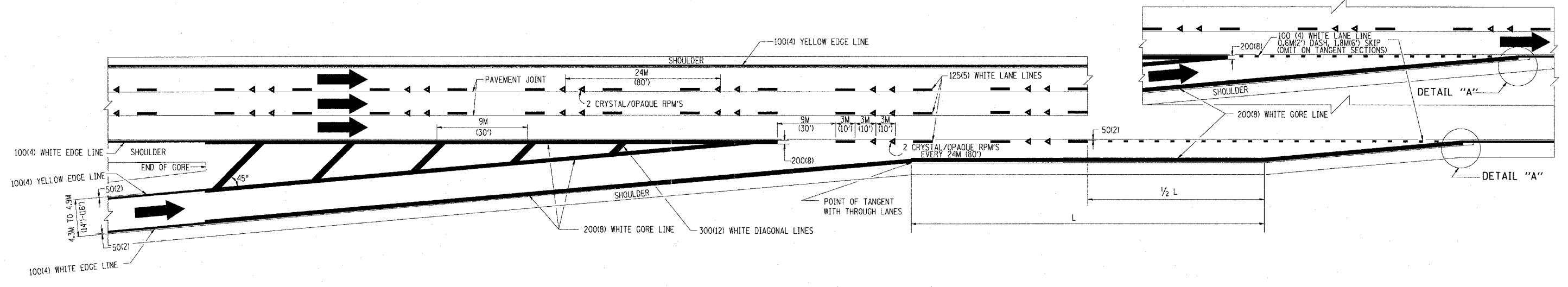


TYPICAL EDGE LINES & LANE LINES

- NOTES:
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
 2. PREFORMED PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES
 3. PREFORMED PLASTIC PAVEMENT MARKING LINE SHALL BE USED ON PCC PAVEMENT.



TYPICAL EXIT RAMP PAVEMENT MARKINGS



TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS

REVISIONS	
NAME	DATE
DWS	1/90
DWS	5/91
AH	3/96
DWS	7/96

ILLINOIS DEPARTMENT OF TRANSPORTATION

MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS

SCALE: NONE

DATE: 10/18/2002

DRAWN BY C.A.D.D.

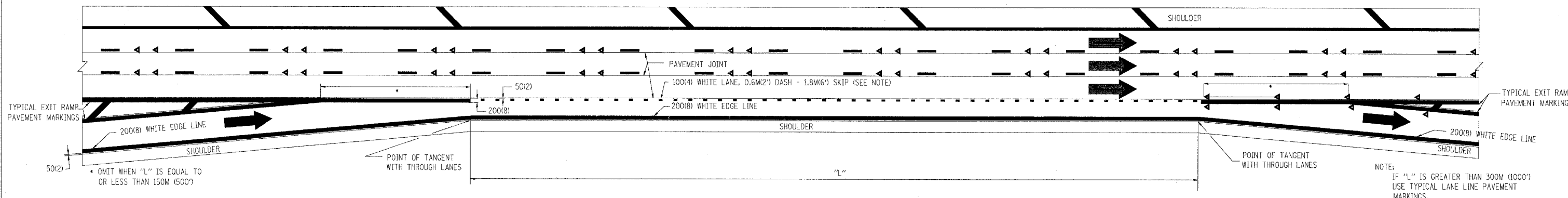
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TC12 SHEET 1 OF 2

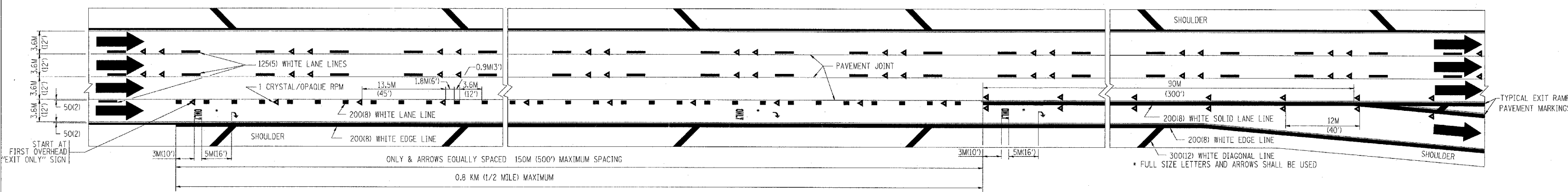
REVISION DATE: 01/01/96

DATE-TIME
DCM-SPEC
VI-TIC12

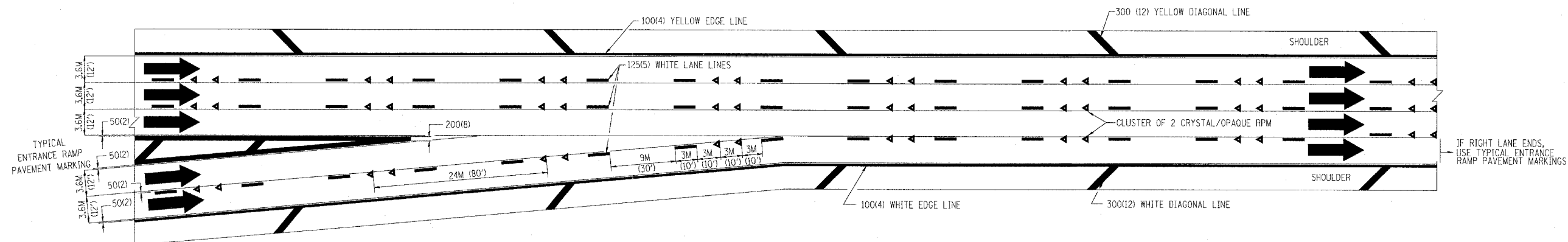
F. A. REG.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			916	696
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS		FED. AID PROJECT



TYPICAL ENTRANCE/EXIT RAMP COMBINATION PAVEMENT MARKINGS



TYPICAL EXIT ONLY LANE PAVEMENT MARKINGS



TYPICAL TWO LANE ENTRANCE RAMP PAVEMENT MARKINGS

DATE-TIME
DGN-SPEC
VTC12

REVISIONS	
NAME	DATE
DWS	1/90
DWS	5/91

ILLINOIS DEPARTMENT OF TRANSPORTATION

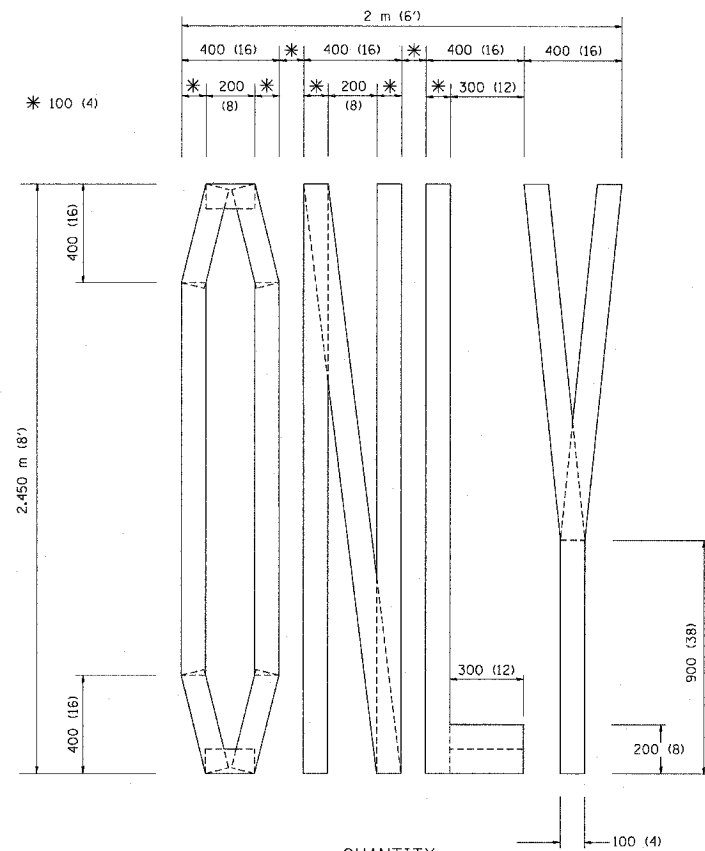
**MULTI-LANE FREEWAY
PAVEMENT MARKING
DETAILS**

SCALE: NONE
DATE: 10/18/2002

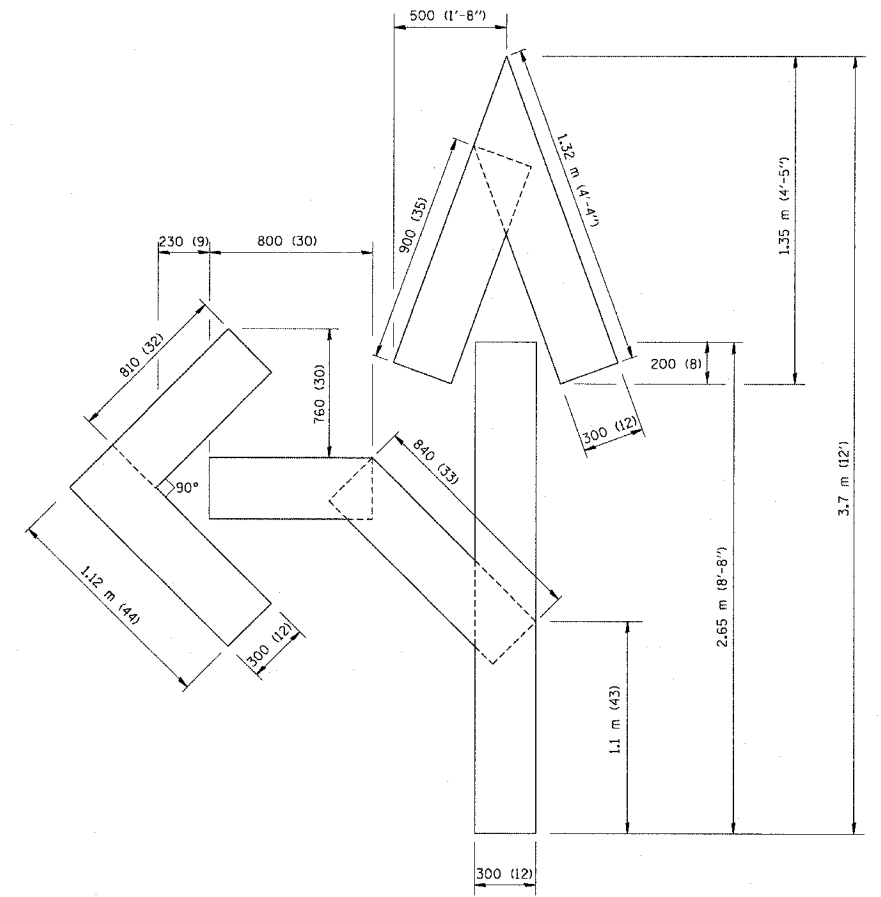
DRAWN BY C.A.D.D.
CHECKED BY

TC12 SHEET 2 OF 2
REVISION DATE: 01/01/96

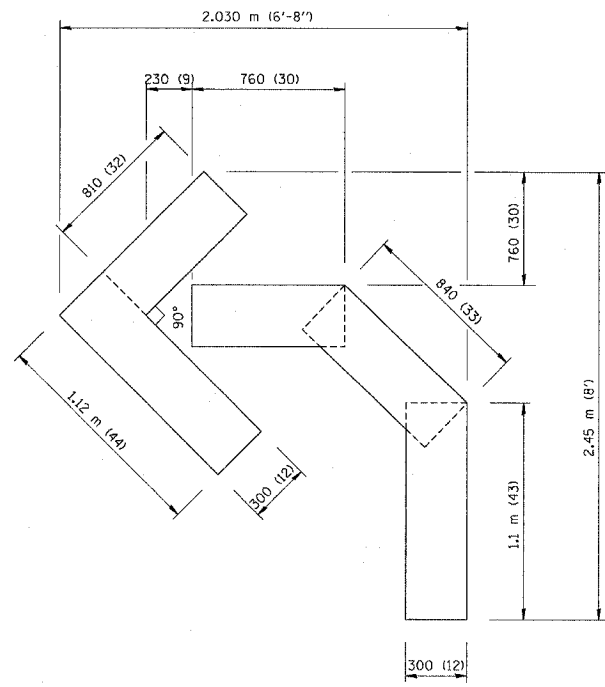
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			916	647
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



QUANTITY
 100 (4) LINE = 19.7 m (64.1 ft.)
 1.97 sq. m (21.1 sq. ft.)



QUANTITY
 100 (4) LINE = 25.3 m (82.5 ft.)
 2.53 sq. m (27.5 sq. ft.)



QUANTITY
 100 (4) LINE = 13.9 m (45.5 ft.)
 1.39 sq. m (15.2 sq. ft.)

All dimensions are in millimeters (Inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
 LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

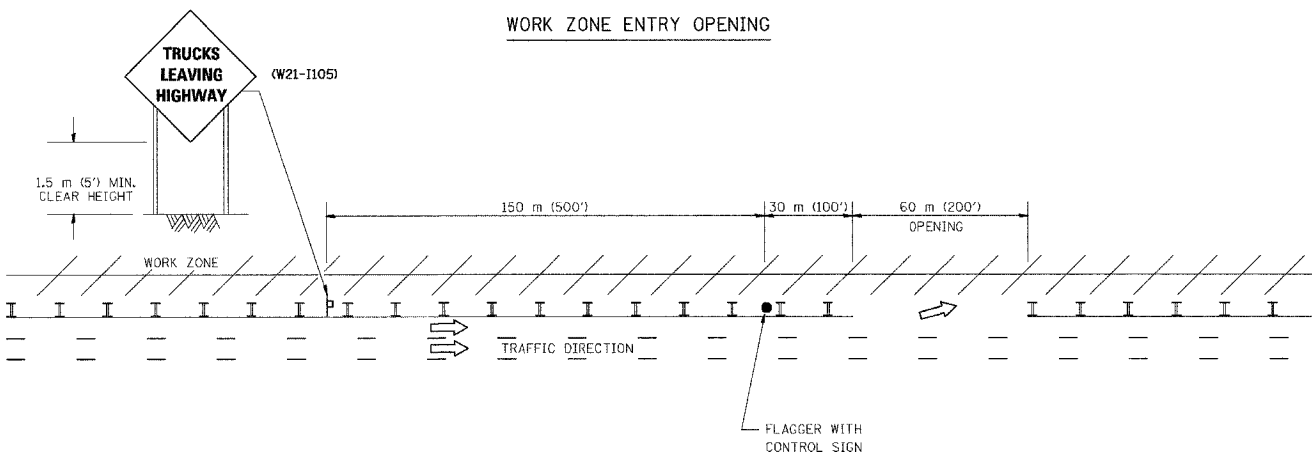
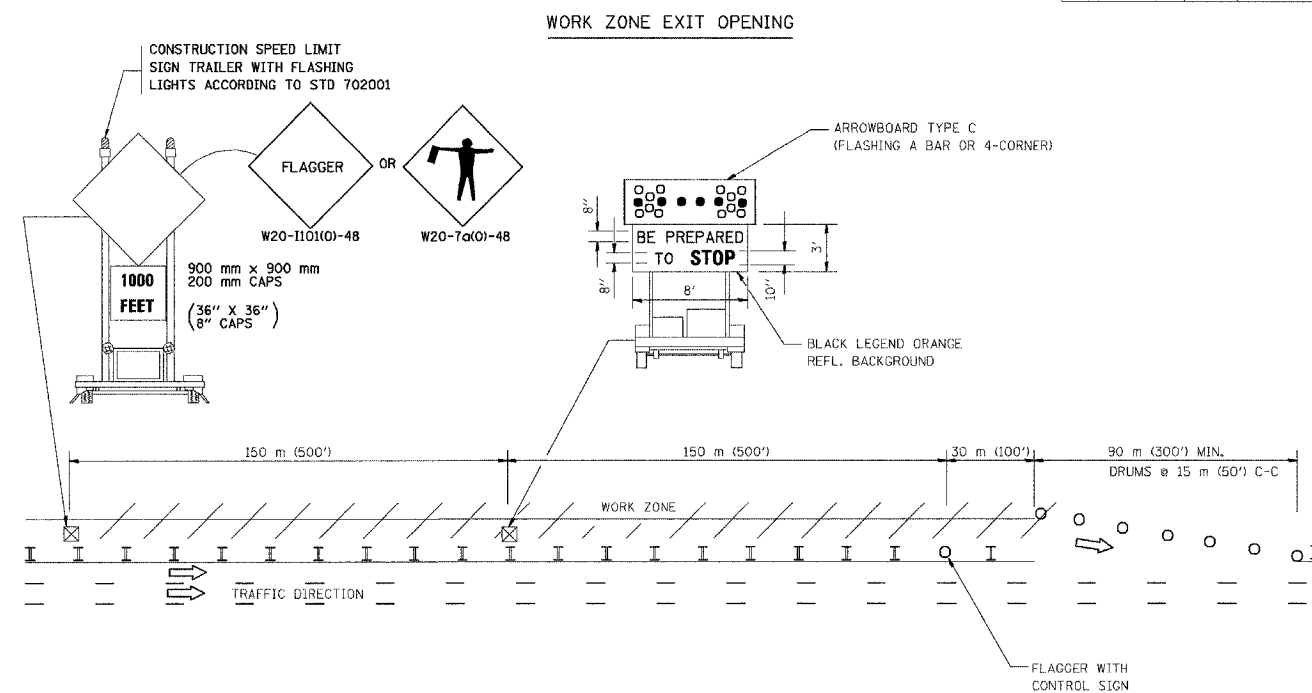
SCALE: NONE
 DATE 10/18/2002

DRAWN BY CADD
 CHECKED BY TC-16

REVISION DATE: 08/28/00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			916	549
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS



NOTES:

1. The Arrowboard, the Flagger Ahead trailer mounted sign, and the Trucks Leaving Highway sign shall be removed or turned away from traffic and the exit and entry openings shall be closed when the flagging operation ceases.
2. Work Zone Exit Openings should be a minimum of one half mile apart.
3. Exiting the work zone at any place other than at a Work Zone Exit Opening will be prohibited.
4. All vehicles shall enter the work zone at entry openings, using their turn signals to warn motorists

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN

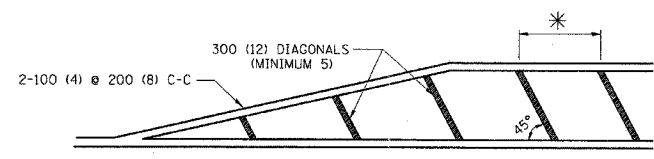
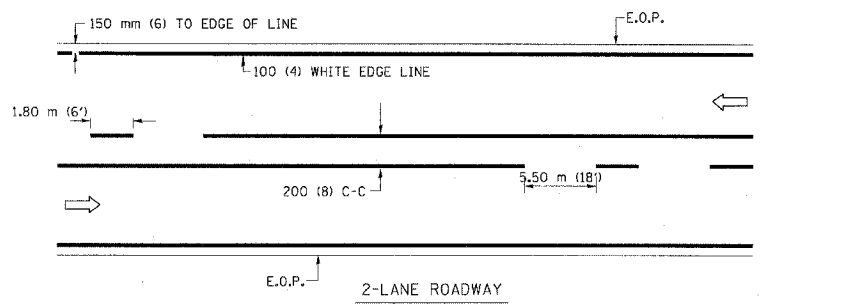
ILLINOIS DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

REVISIONS	
NAME	DATE
DWS	8/98
JAF	4/03

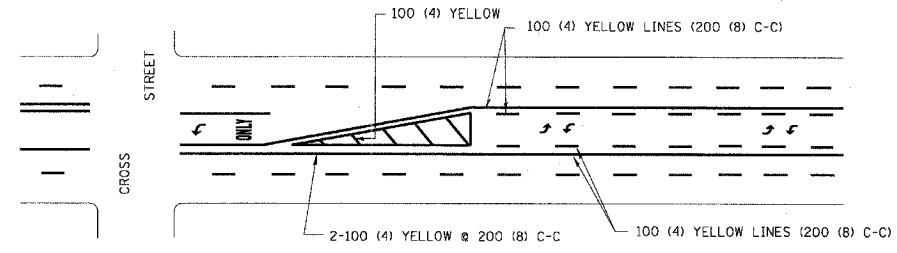
SCALE: NONE
DATE 05/06/2003

DRAWN BY CADD
CHECKED BY TC-18



* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 * DIAGONAL LINE SPACING: 6.1 m (20') C-C

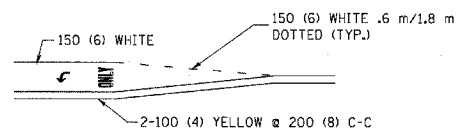
PAINTED MEDIANS



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

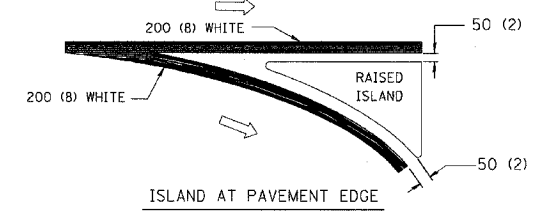
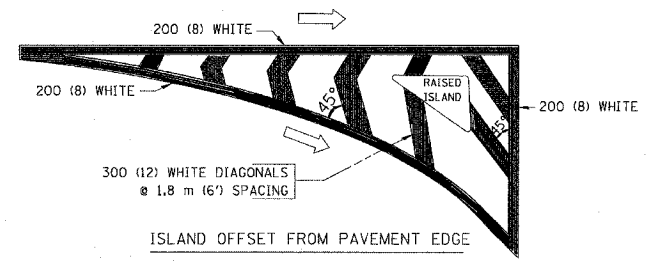


FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.
 AREA = 1.47 m² (15.8 SQ. FT.) ONLY AREA = 2.13 m² (22.9 SQ. FT.)

* TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	1.80 m (6') LINE WITH 5.50 m (18') SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	200 (8) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	200 (8) C-C
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	1.80 m (6') LINE WITH 5.50 m (18') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	1.8 m (6') LINE WITH 5.50 m (18') SPACE FOR SKIP-DASH; 200 (8) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 200 (8) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART 100 (2'-4") APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	200 (8) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 6.1 m (20') (LESS THAN 50 km/h (30 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=0.33m ² (3.6 SQ. FT.) EACH "X"=5.0 m ² (54.0 SQ. FT.)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

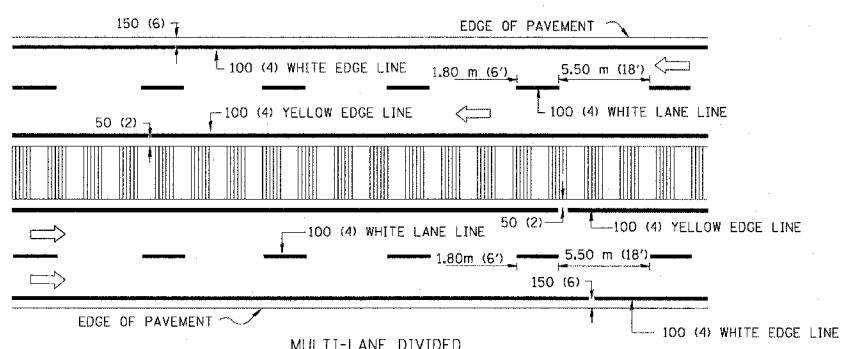
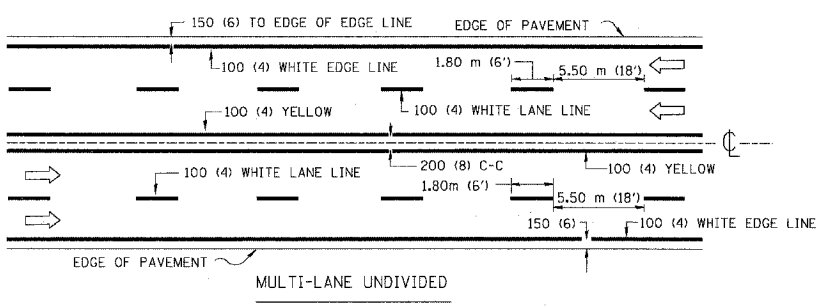
CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

REVISIONS	
NAME	DATE
T. RAMMACHER	12/07/00

SCALE: NONE
DATE: 10/18/2002

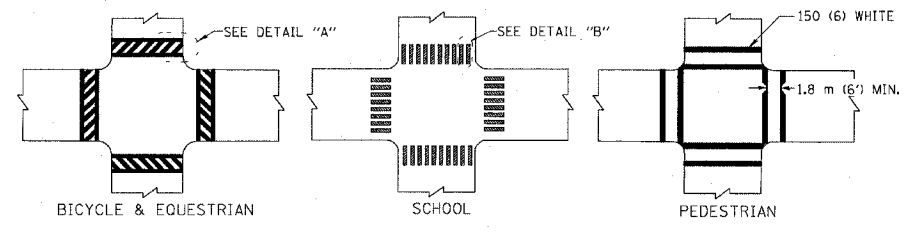
DRAWN BY: CADD
CHECKED BY:
TC-24

REVISION DATE: 12/07/00

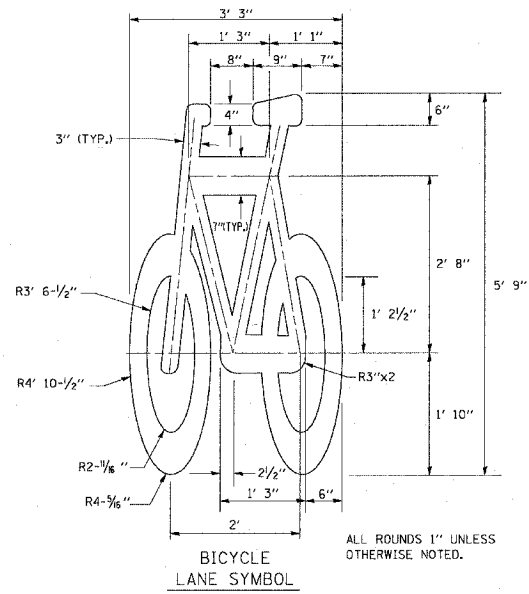
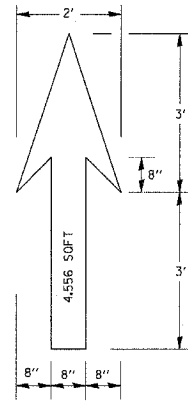


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



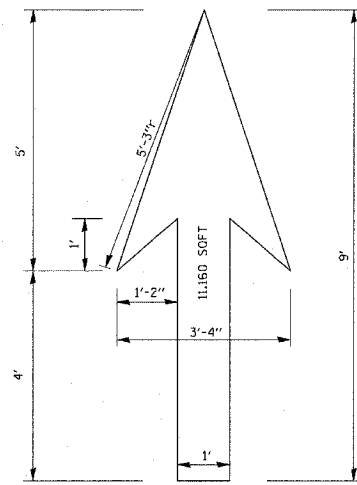
NOTE:

1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.

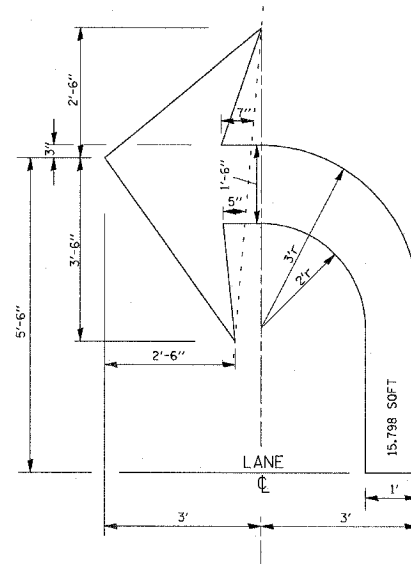
2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28

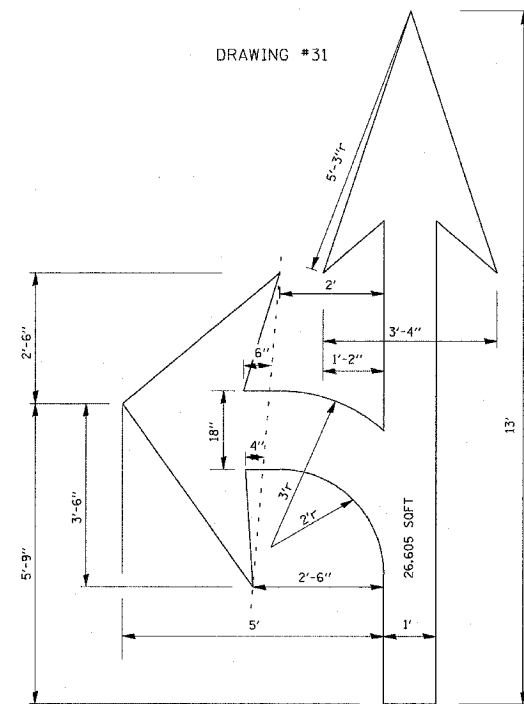
DRAWING #29



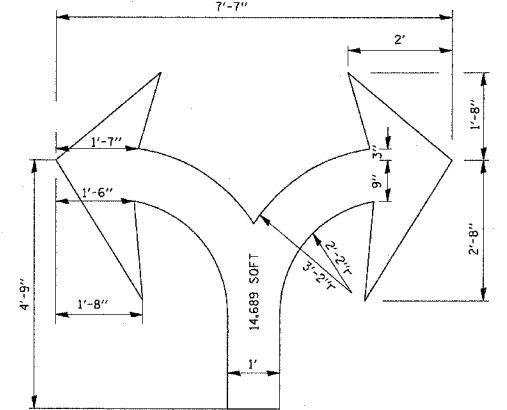
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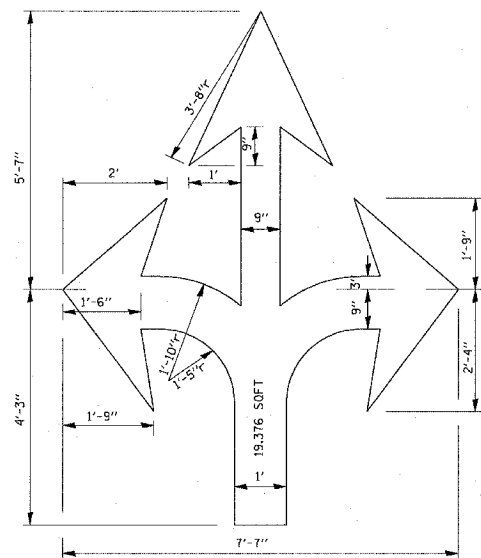
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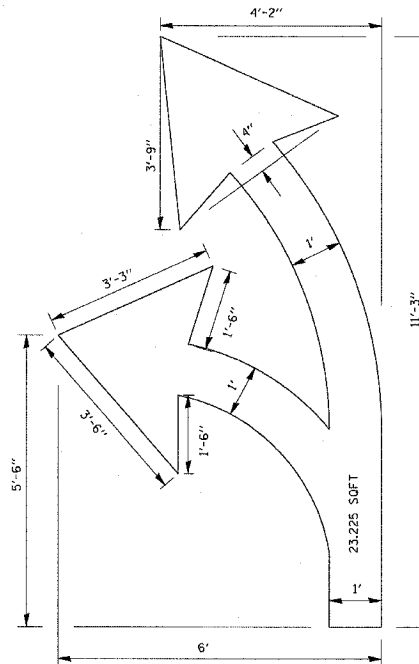
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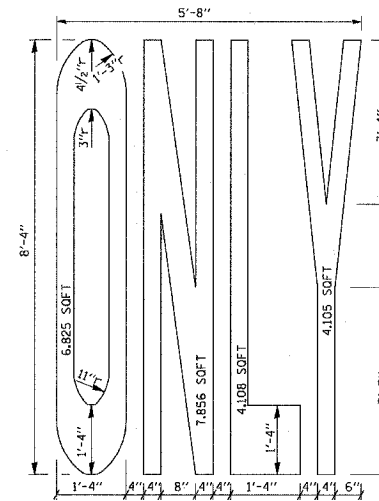
DRAWING #33



DRAWING #34



DRAWING #35



NOTE:

ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

REVISIONS	
NAME	DATE
T. RAMMACHER	12/07/00

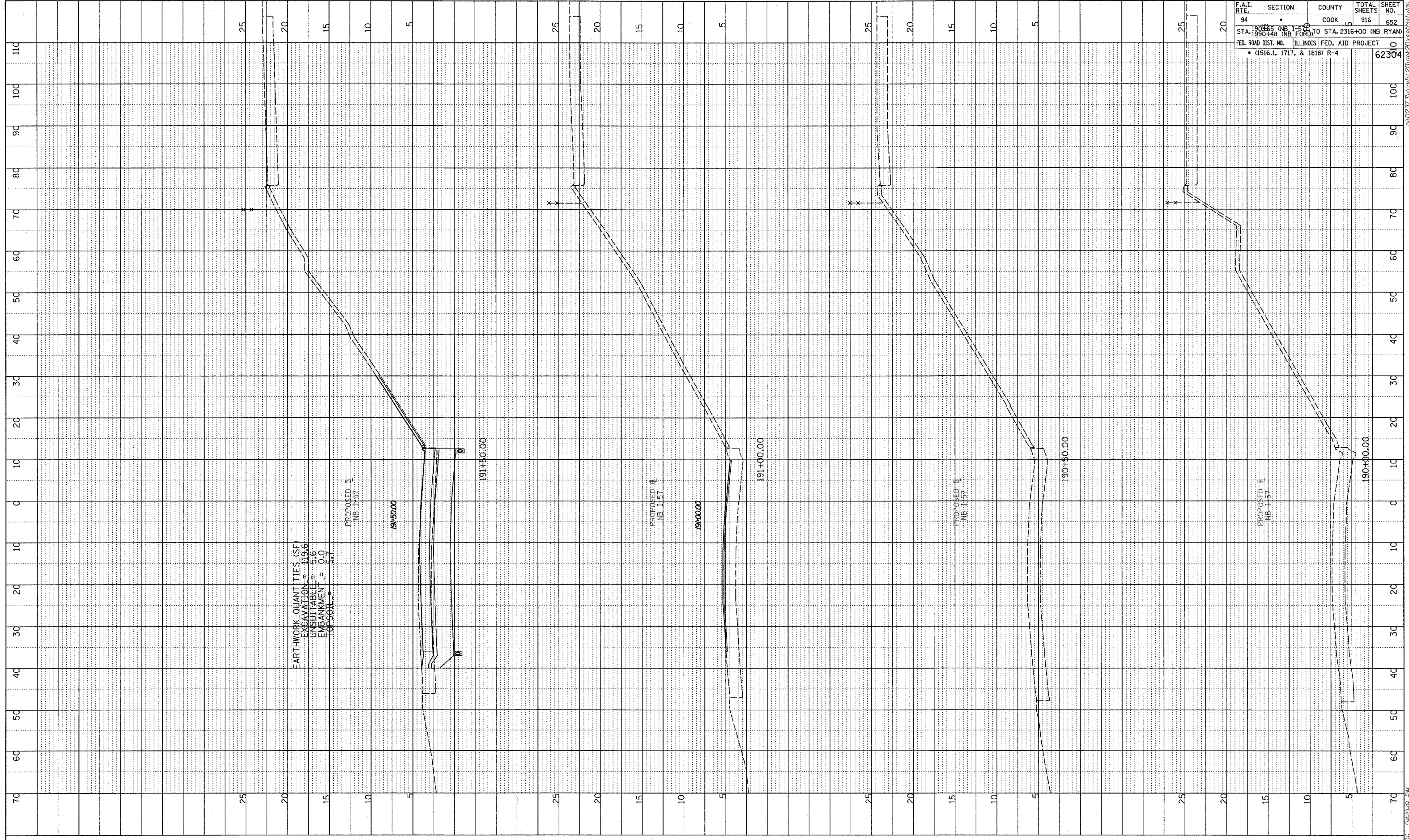
ILLINOIS DEPARTMENT OF TRANSPORTATION
CITY OF CHICAGO
TYPICAL PAVEMENT
MARKINGS.

SCALE: NONE
DATE 03/09/2004

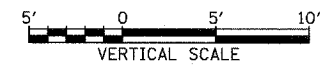
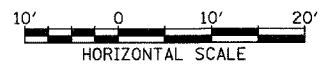
DRAWN BY
CHECKED BY
TC-24

PROFILE SURVEYED _____
 GRADES CHECKED _____
 NOTE BOOK _____
 NO. _____
 DATE _____
 BY _____
 SURVEYOR'S LICENSE NO. _____
 STATE OF ILLINOIS CHDHD

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+00	COOK	916	652
STA. 190+00 (NB I-55) TO STA. 2316+00 (NB RYAN) FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT • (1516.1, 1717, & 1818) R-4				
				62304



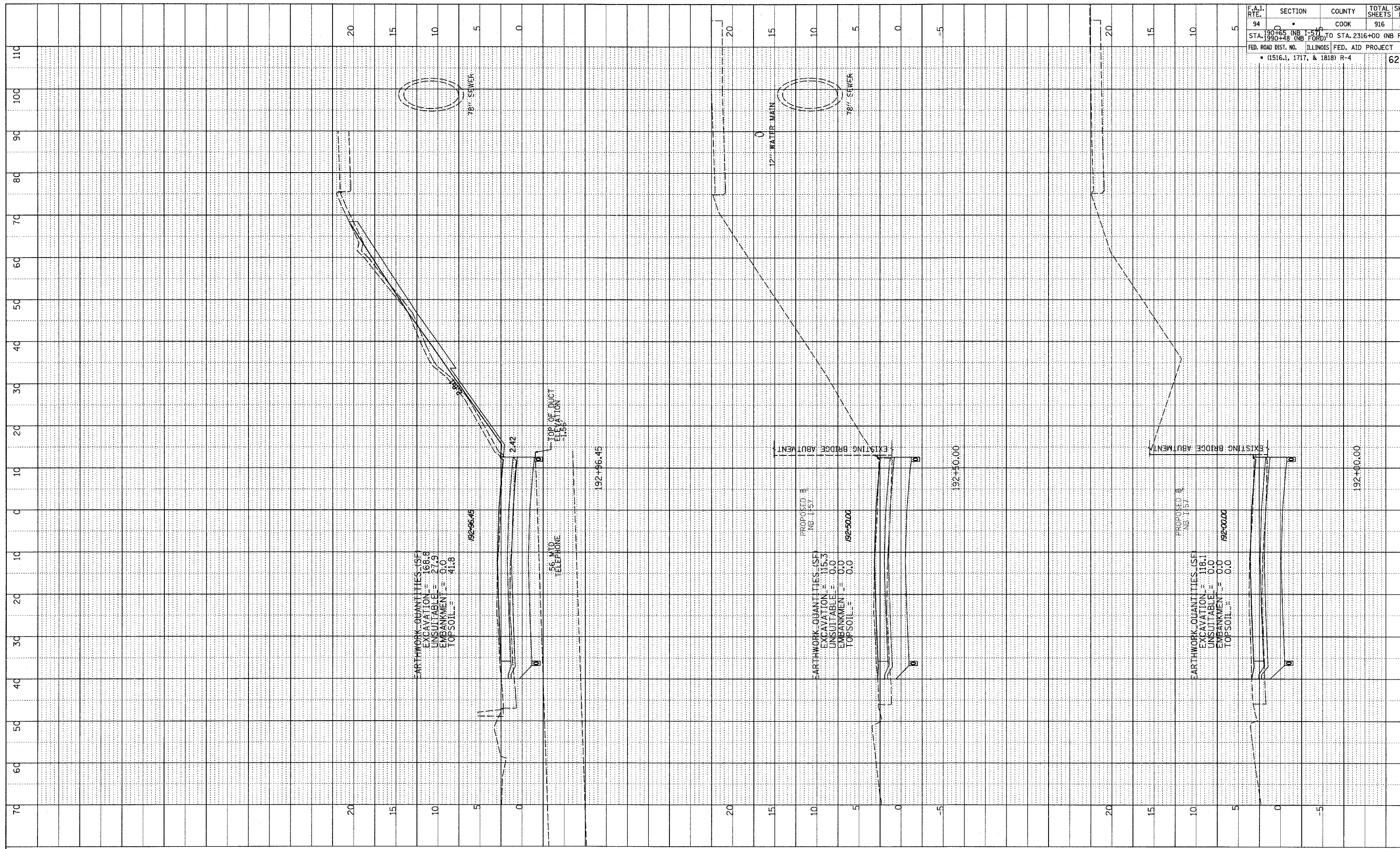
TYLIN INTERNATIONAL



NORTHBOUND I-57
 STA. 190+00 TO STA. 191+50

03/03/2006 04:04:49 PM

PROFILE SURVEYED BY _____ DATE _____
 GRADES CHECKED BY _____
 NOTE BOOK NO. _____
 FIELD NOTES NO. _____
 SURVEY DATE NOTATION CHRG. _____



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+00 (NB I-57) TO STA. 2316+00 (NB RYAN)	COOK	916	653
STA. 190+00 (NB I-57) TO STA. 2316+00 (NB RYAN) FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT (1516.1, 1717, & 1818) R-4				
				62304

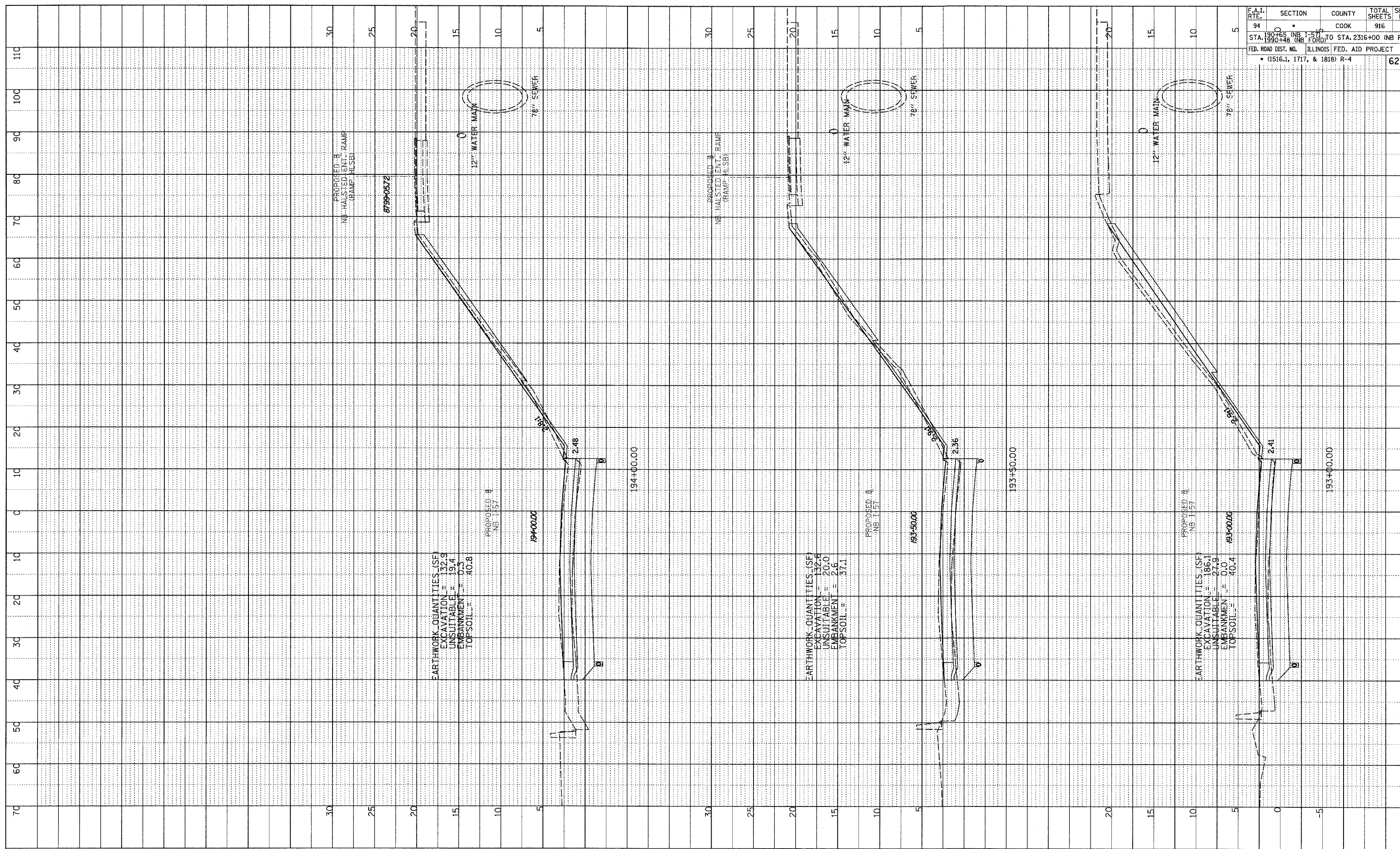
TYLIN INTERNATIONAL



NORTHBOUND I-57
 STA. 192+00 TO STA. 192+96.45

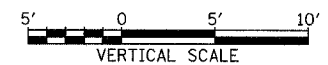
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PROFILE SURVEYED
 NOTE BOOK GRADES CHECKED
 NO. DATE
 STRUCTURE NOTATIONS BRG



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	0	COOK	916	654
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				
				62304

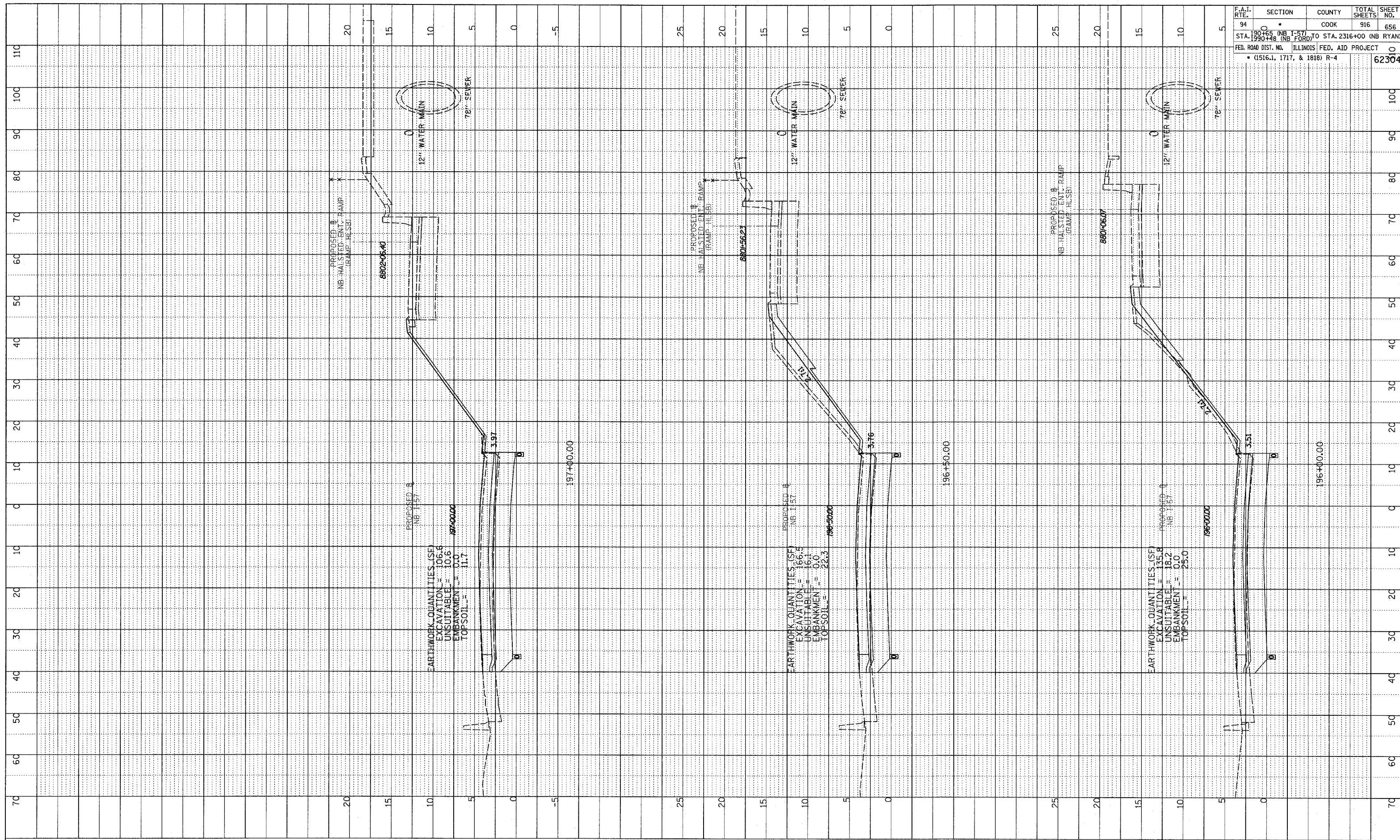
TYLIN INTERNATIONAL



NORTHBOUND I-57
 STA. 193+00 TO STA. 194+00

03/03/2006 04:00:20 PM

PROFILE SUBMITTED BY: _____ DATE: _____
 GRADES CHECKED BY: _____
 NOTE BOOK NO. _____
 SURVEY NOTES STATUS: CHND



F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	196+00	COOK	916	656
STA. 196+00 (NB I-57) TO STA. 2316+00 (NB RYAN) STA. 1990+48 (NB FORD)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT • (1516.1, 1717, & 1818) R-4				
				62304

TYLIN INTERNATIONAL

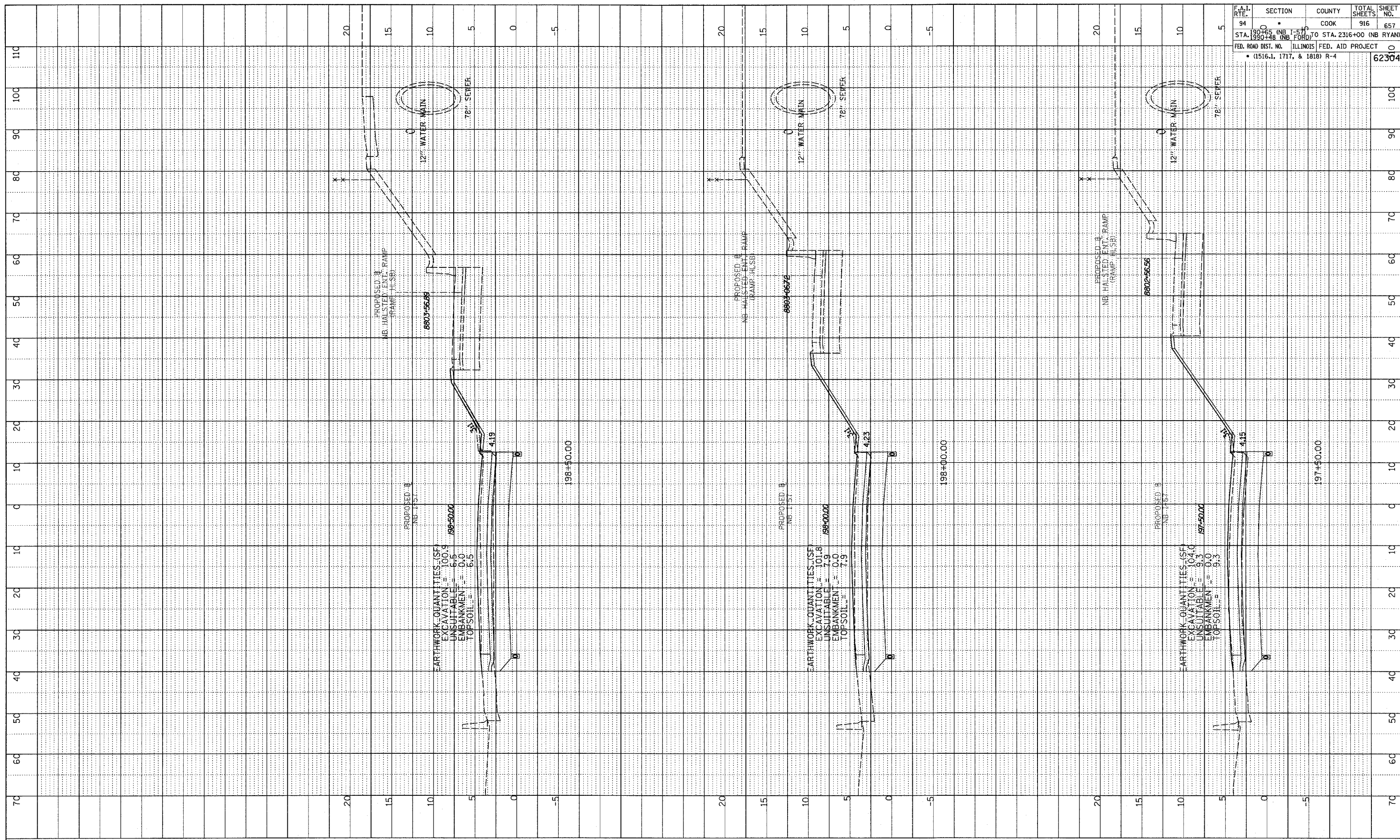


NORTHBOUND I-57
 STA. 196+00 TO STA. 197+00

03/03/2005 04:02 PM

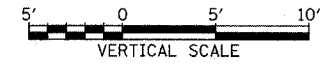
DATE	BY
NO.	DATE

PROFILE SURVEYED
 GRADES CHECKED
 ELEVATIONS NOTED
 STAKE PLACES NOTED



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	100	COOK	916	657
STA. 197+50 (NB I-57)	TO STA. 2316+00 (NB RYAN)			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
(1516.1, 1717, & 1818) R-4			62304	

TYLIN INTERNATIONAL

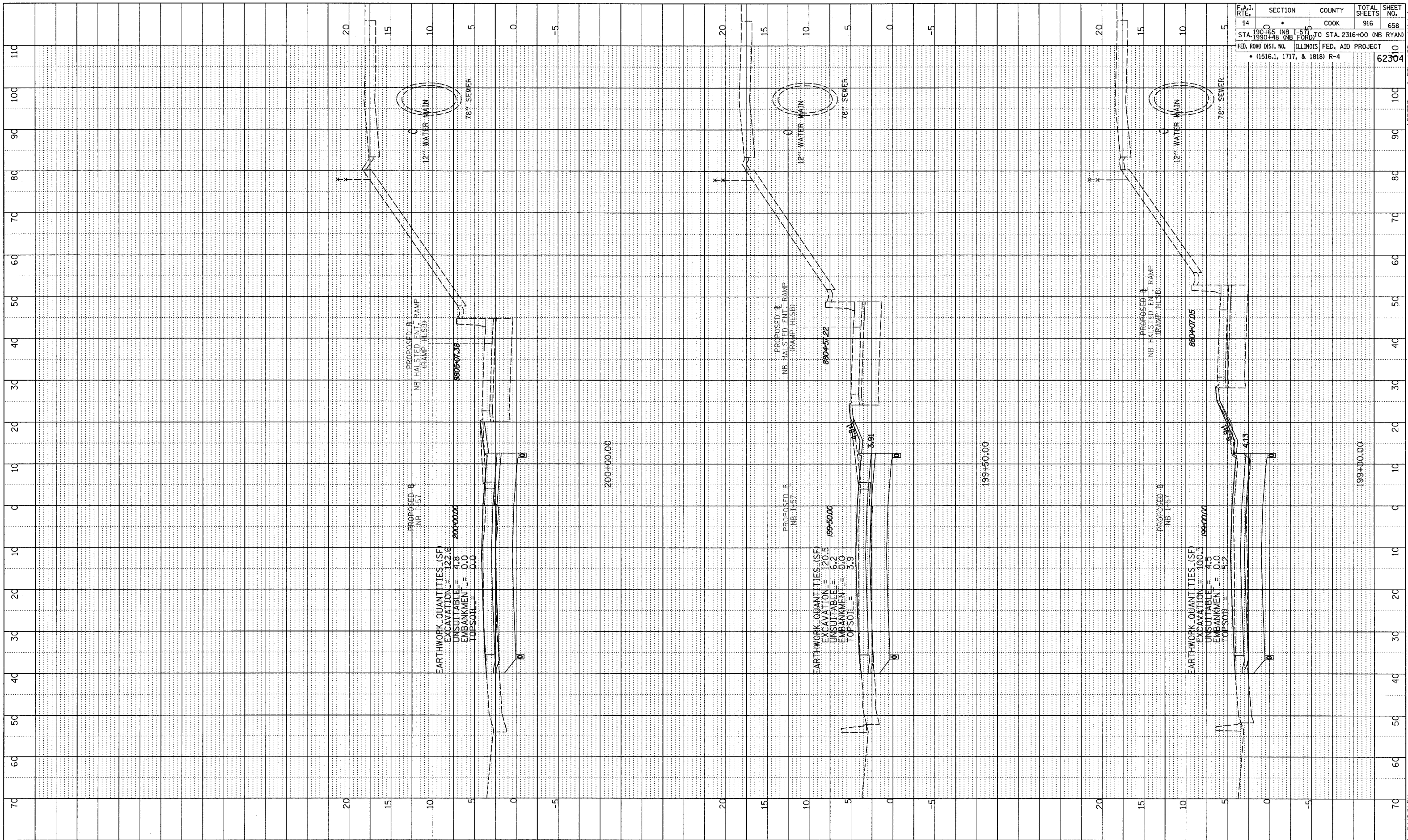


NORTHBOUND I-57
 STA. 197+50 TO STA. 198+50

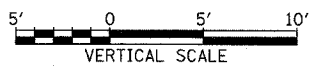
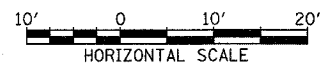
03/03/2006 04:02:17 PM

PROFILE SUBMITTED BY _____ DATE _____
 PLOTTED BY _____
 NOTE BOOK GRADES CHECKED _____
 NO. _____ STRUCTURE NOTATIONS CHFD

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+00	COOK	916	658
STA. 190+00 (NB I-57) TO STA. 2316+00 (NB RYAN) STA. 199+48 (NB FORD) TO STA. 2316+00 (NB RYAN) FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT • (1516.1, 1717, & 1818) R-4				
				62304



TYLIN INTERNATIONAL

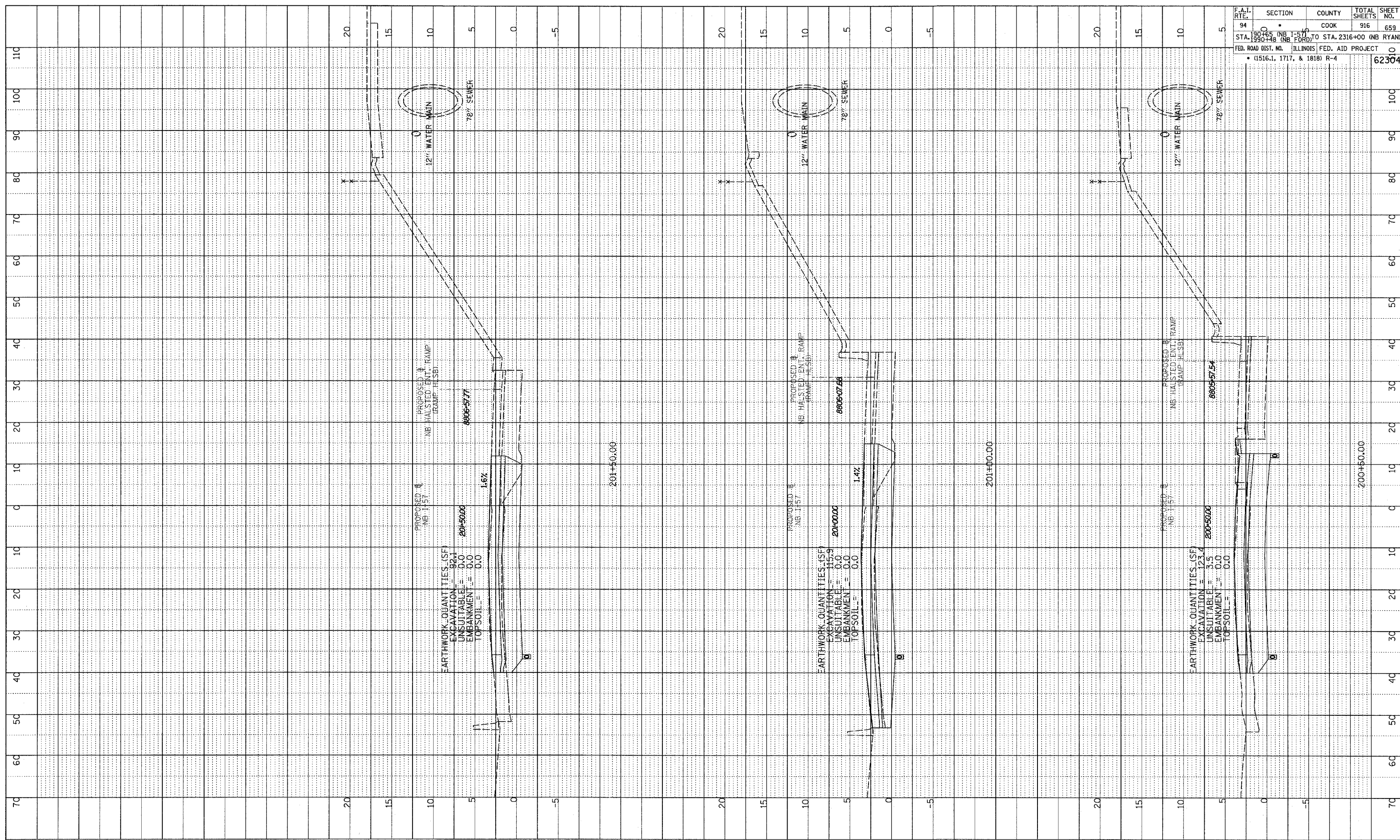


NORTHBOUND I-57
 STA. 199+00 TO STA. 200+00

03/03/2006 04:02:21 PM

PROFILE SURVEYED
 GRADES CHECKED
 NOTE BOOK
 NO.

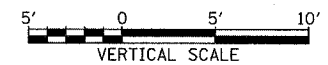
BY
 DATE



PROPOSED NB I-57
 NB HALSTED ENT. RAMP
 (RAMP HLSB)
 8806+57.77
 EARTHWORK QUANTITIES (SF)
 EXCAVATION = 32.4
 UNSUITABLE = 0.0
 EMBANKMENT = 0.0
 TOPSOIL = 0.0
 200+50.00
 1.6%
 201+50.00

PROPOSED NB I-57
 NB HALSTED ENT. RAMP
 (RAMP HLSB)
 8806+07.66
 EARTHWORK QUANTITIES (SF)
 EXCAVATION = 15.9
 UNSUITABLE = 0.0
 EMBANKMENT = 0.0
 TOPSOIL = 0.0
 200+00.00
 1.4%
 201+00.00

PROPOSED NB I-57
 NB HALSTED ENT. RAMP
 (RAMP HLSB)
 8806+57.54
 EARTHWORK QUANTITIES (SF)
 EXCAVATION = 123.4
 UNSUITABLE = 3.5
 EMBANKMENT = 0.0
 TOPSOIL = 0.0
 200+50.00
 1.4%
 201+50.00



TYLIN INTERNATIONAL

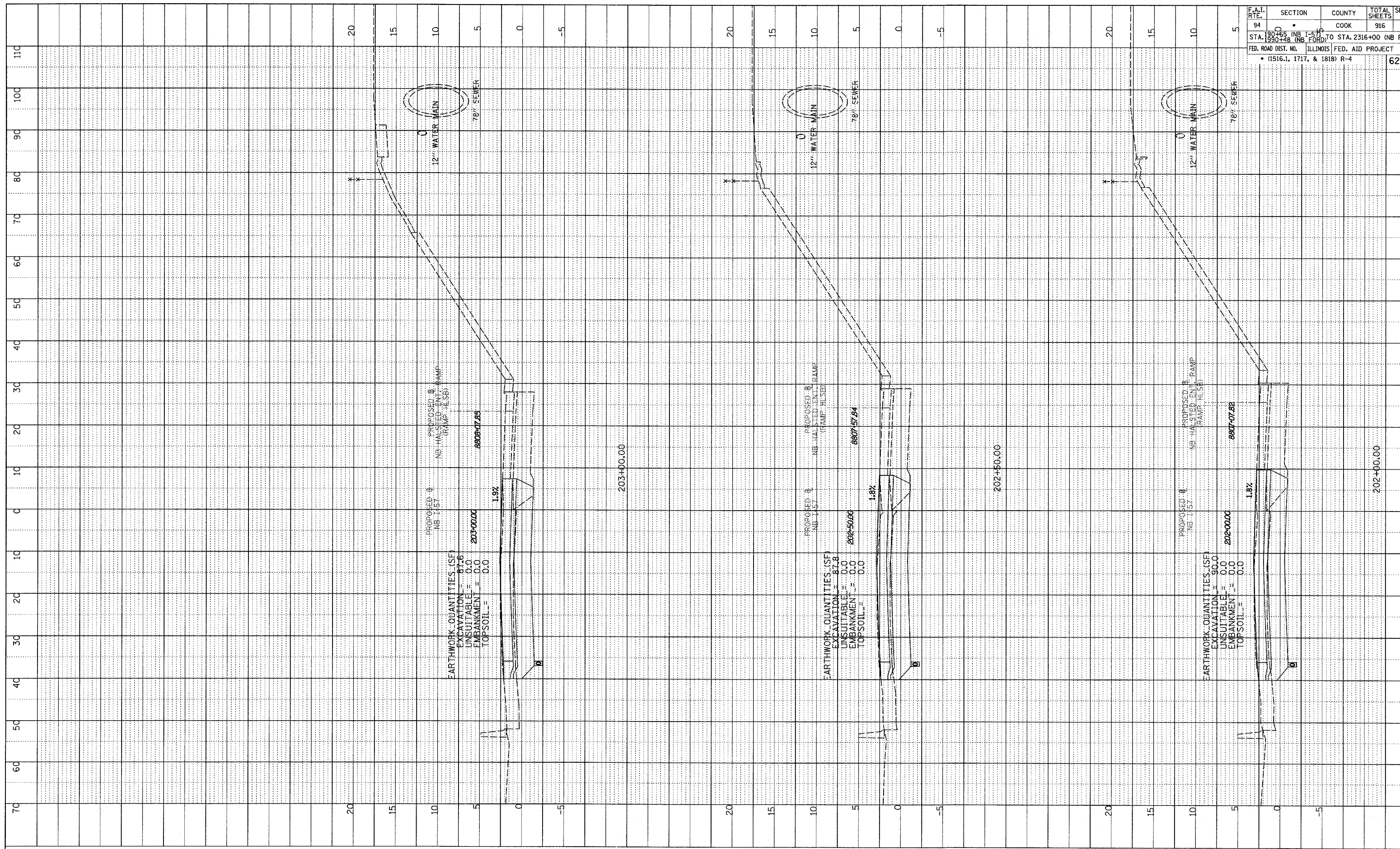
NORTHBOUND I-57
 STA. 200+50 TO STA. 201+50

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	0	COOK	916	659
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				

62304

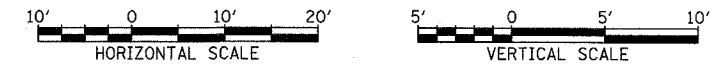
03/03/2006 04:04:22 PM

PROFILE SURVEYED BY _____ DATE _____
 GRADES CHECKED _____
 NOTE BOOK NO. _____
 PLOT NUMBER NOTATION CHFD _____



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	0	COOK	916	660
STA. 190+65 (NB I-55) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				
				62304

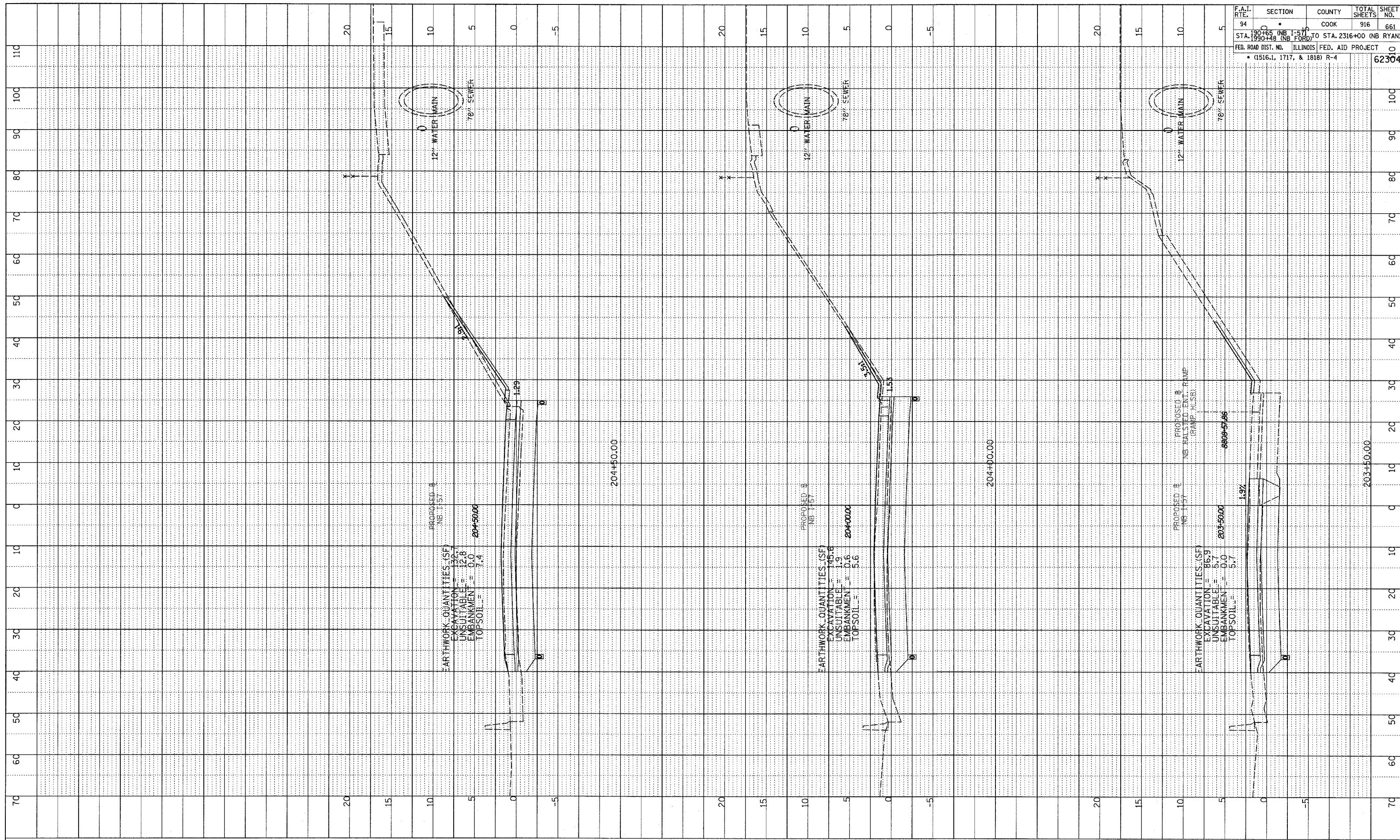
TYLIN INTERNATIONAL



NORTHBOUND I-57
 STA. 202+00 TO STA. 203+00

03/03/2006 04:02:22 PM

PROFILE SURVEYED: _____ DATE: _____
 GRADES CHECKED: _____ BY: _____
 NO. _____
 SURVEY NOTES: CH-10



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	10	COOK	916	661
STA. 1990+00 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				
				62304

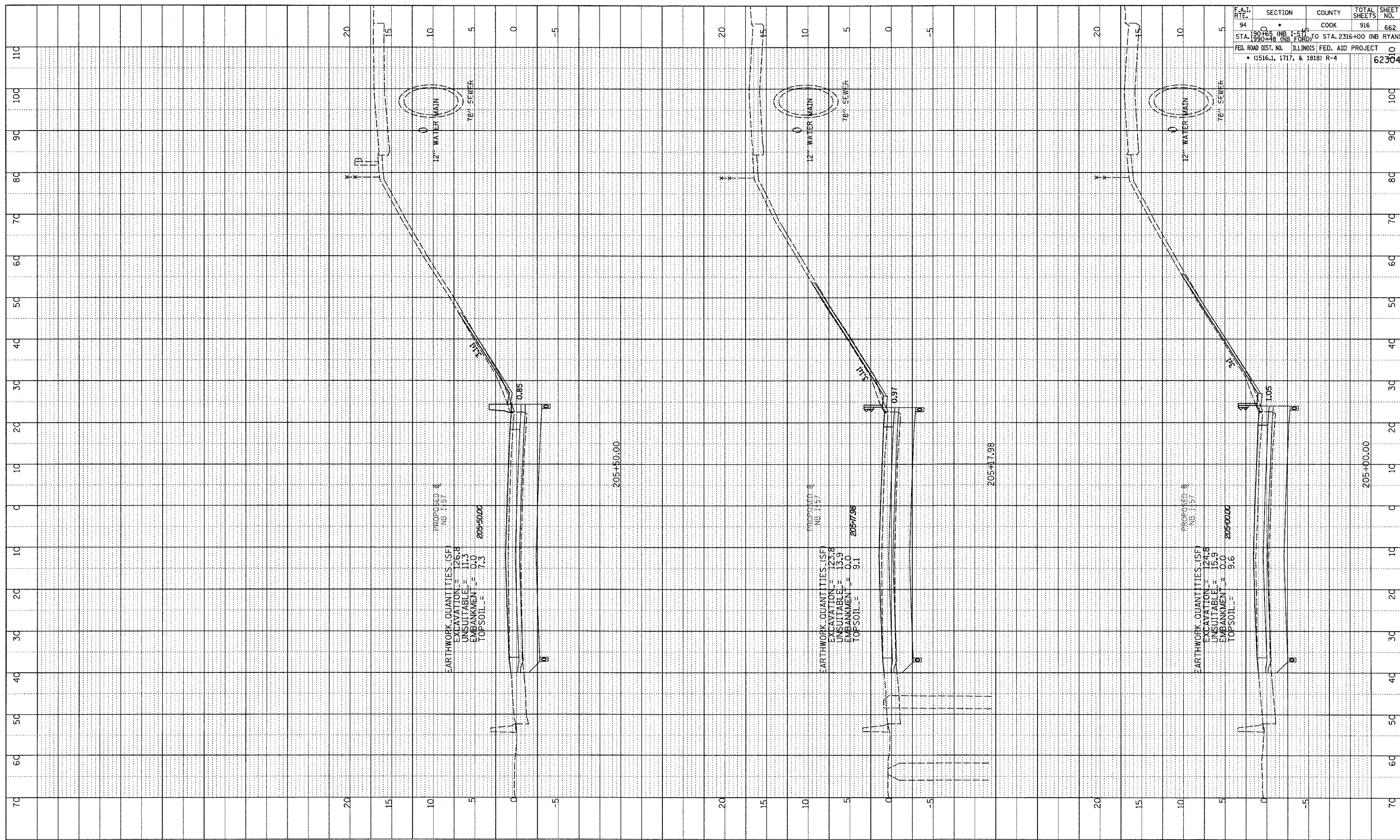
TYLIN INTERNATIONAL



NORTHBOUND I-57
STA. 203+50 TO STA. 204+50

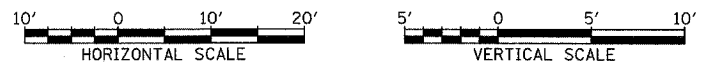
03/03/2006 04:04:23 PM

DATE	
BY	
SURVEYED	
GRADES CHECKED	
NOTE BOOK	
NO.	
STRUCTURE NOTATIONS CHECKED	



F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+00	COOK	916	662
STA. 190+00 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				
				62304

TYLIN INTERNATIONAL

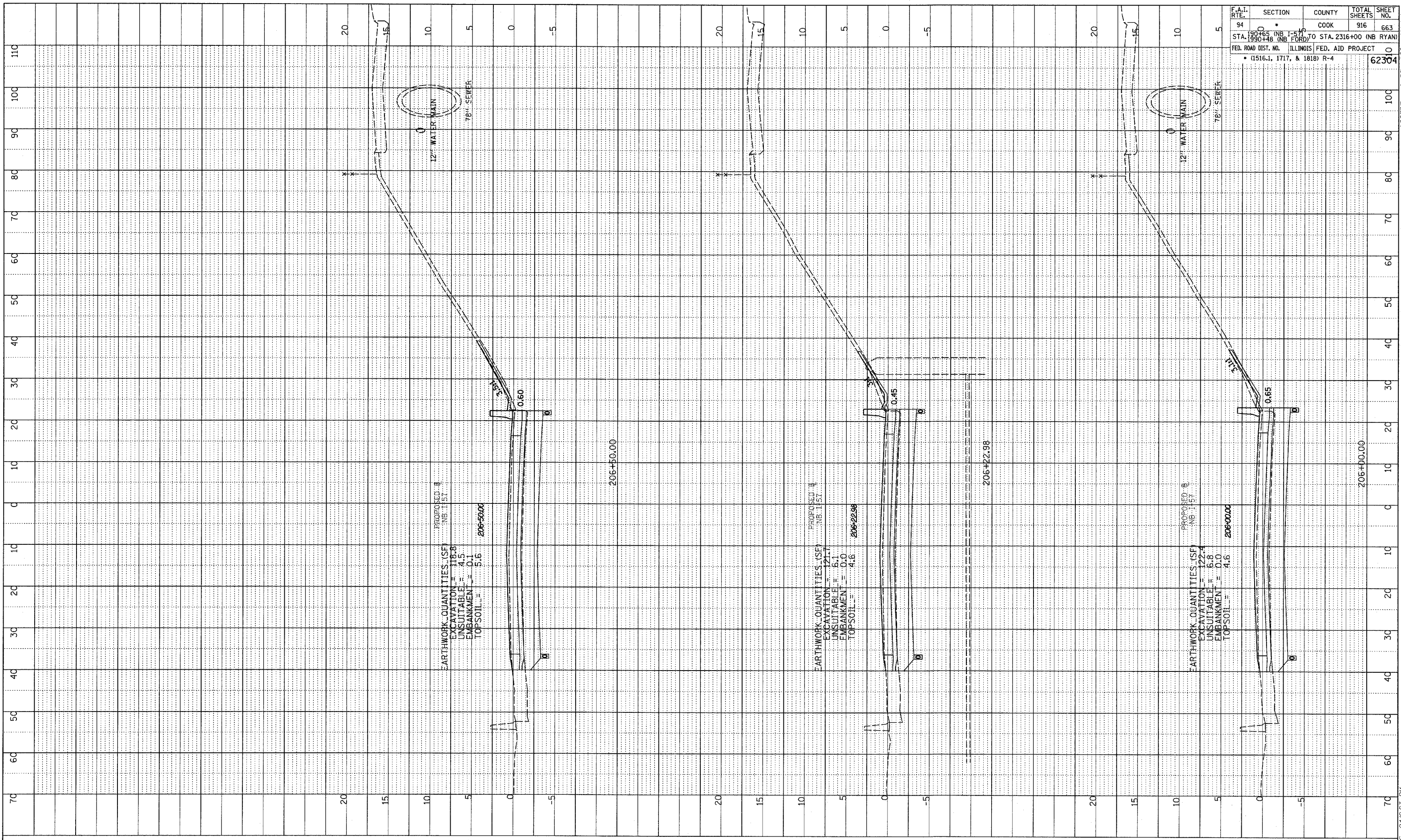


NORTHBOUND I-57
STA. 205+00 TO STA. 205+50

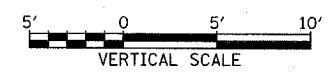
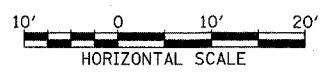
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PROFILE SURVEYED
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 GRADES
 STRUCTURE NOTATION THIRD

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	0	COOK	916	663
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)		ILLINOIS FED. AID PROJECT		
FED. ROAD DIST. NO. (1516.1, 1717, & 1818) R-4		62304		



TYLIN INTERNATIONAL

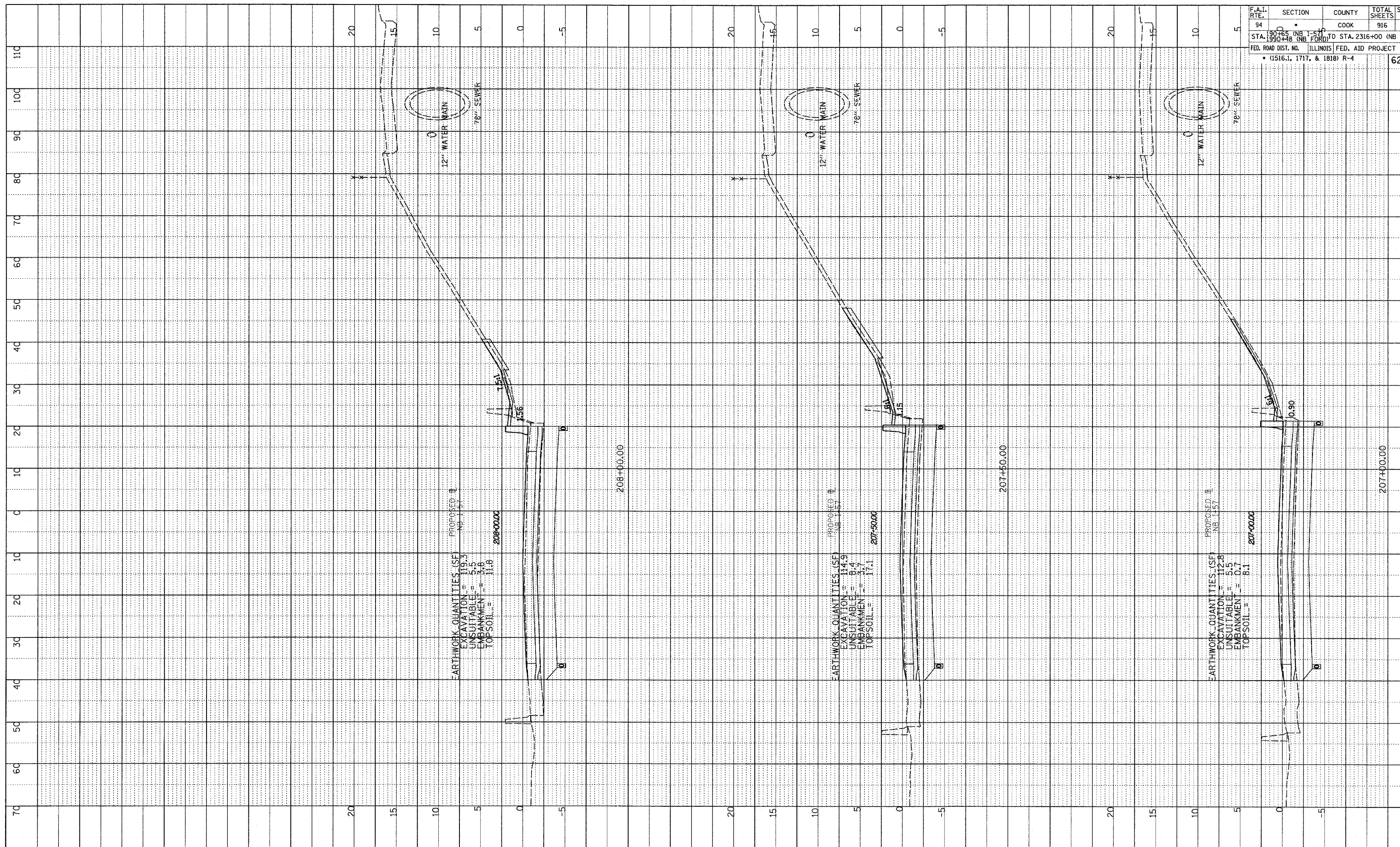


NORTHBOUND I-57
 STA. 206+00 TO STA. 206+50

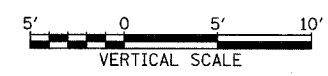
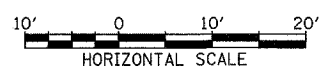
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 STRUCTURE NOTATION CHFD
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	180+00 TO STA. 2316+00 (NB RYAN)	COOK	916	664
STA. 180+00 (NB FORD) TO STA. 2316+00 (NB RYAN) FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT • (1516.1, 1717, & 1818) R-4				
				62304



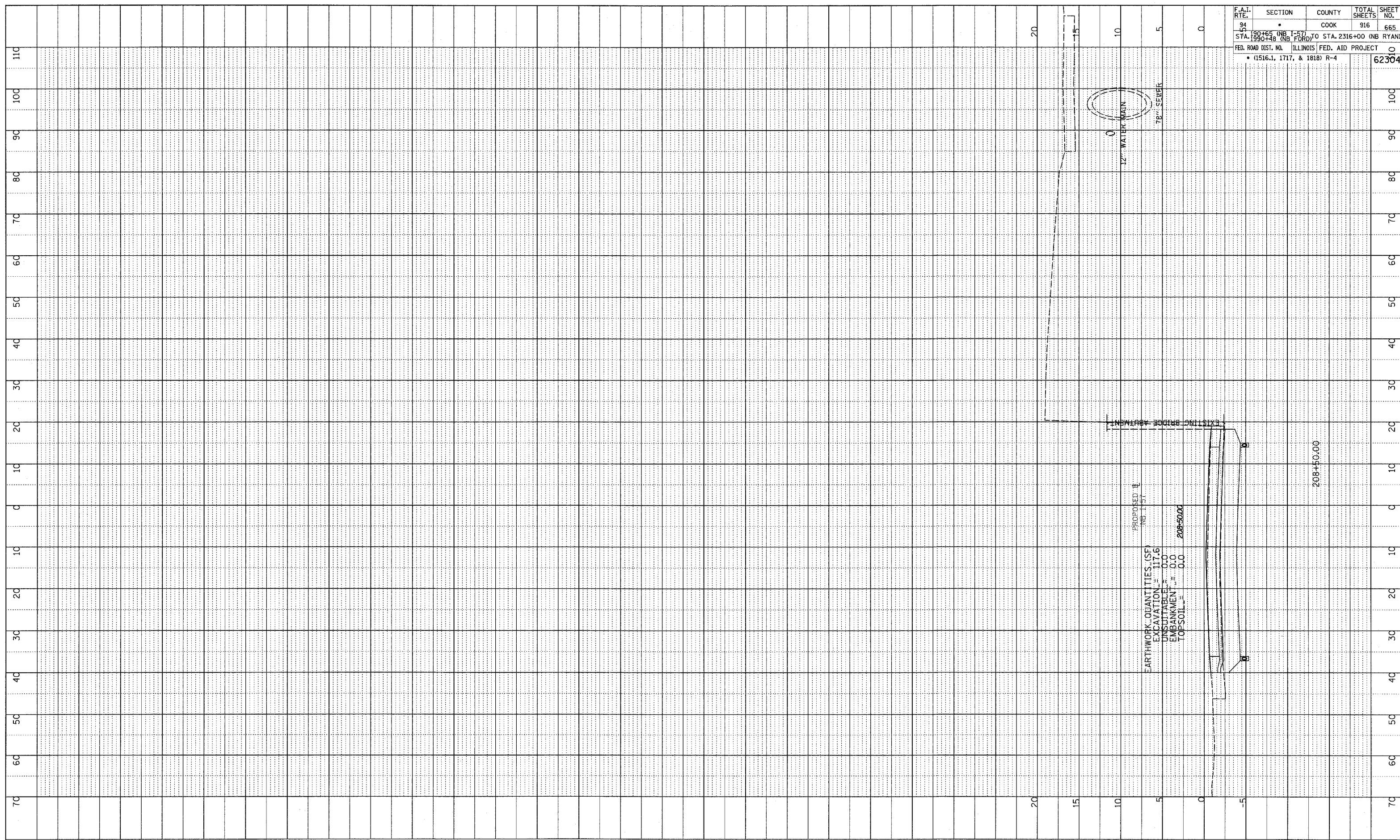
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NORTHBOUND I-57
 STA. 207+00 TO STA. 208+00

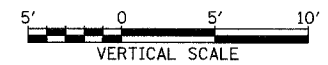
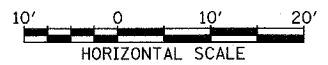
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	•	COOK	916	665
STA. 1990+00 (NB I-57)	(NB I-57)	TO STA. 2316+00 (NB RYAN)		
STA. 1990+00 (NB I-57)	(NB I-57)	TO STA. 2316+00 (NB RYAN)		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				62304
• (1516.1, 1717, & 1818) R-4				

TYLIN INTERNATIONAL

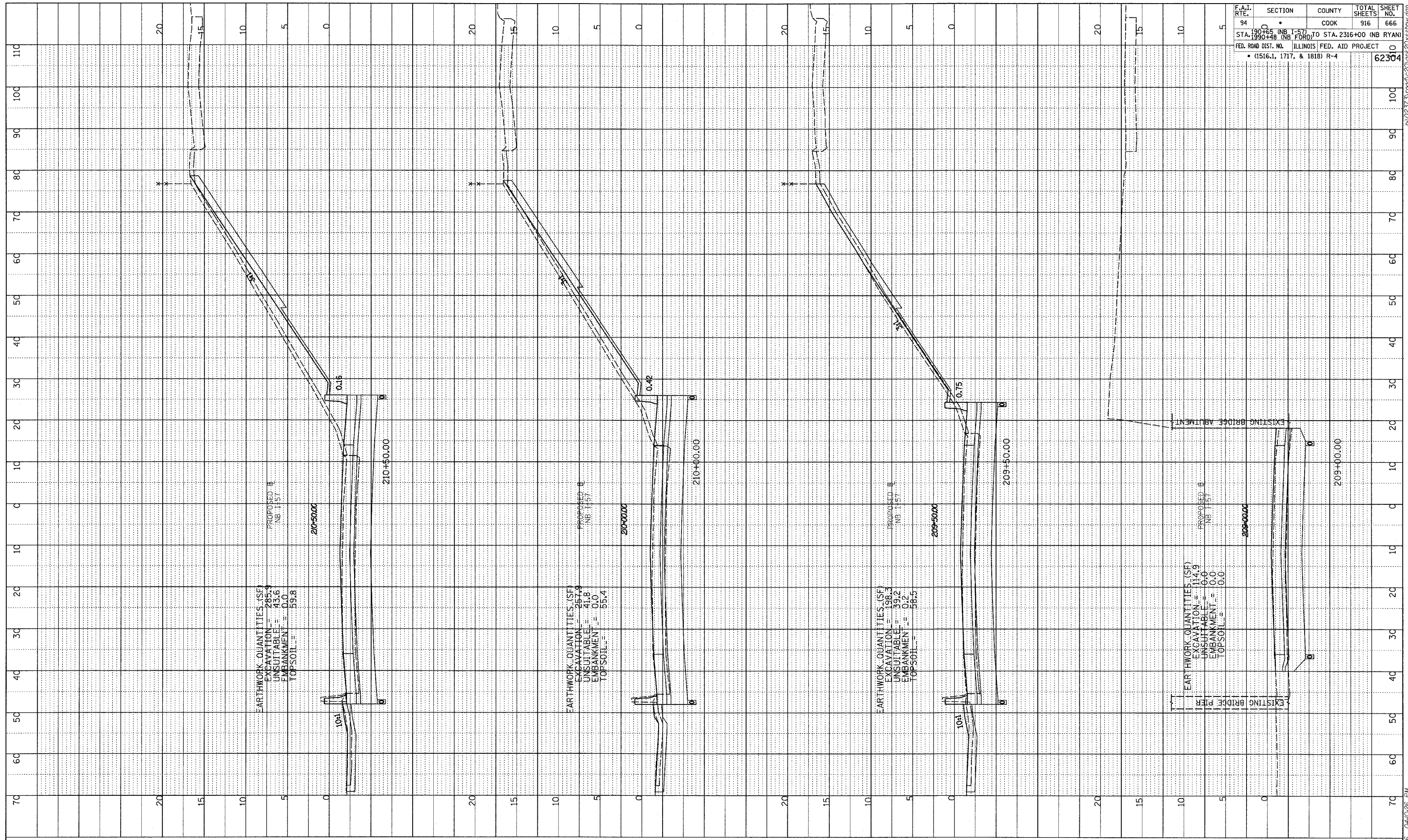


NORTHBOUND I-57
STA. 208+50 TO STA. 208+50

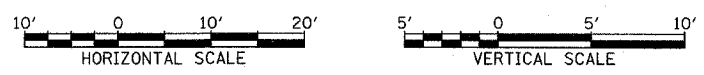
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)	COOK	916	666
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				62304
• (1516.1, 1717, & 1818) R-4				



TYLIN INTERNATIONAL

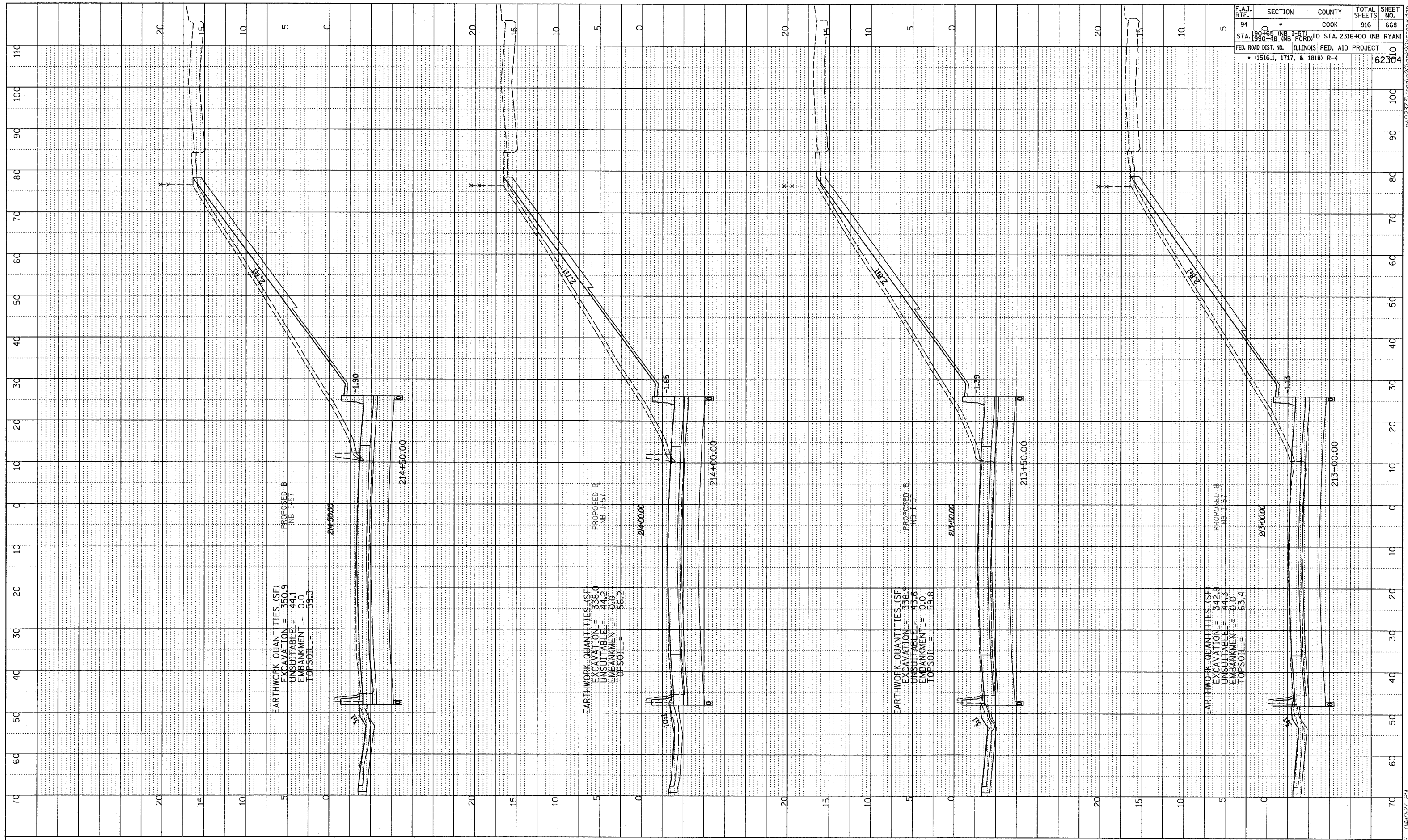


NORTHBOUND I-57
 STA. 209+00 TO STA. 210+50

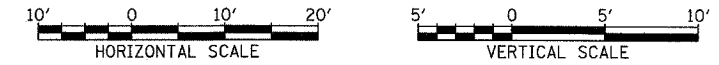
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PROFILE SUBMITTED BY _____ DATE _____
 NOTE BOOK NO. _____
 GRADES CHECKED _____
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 PLUS/MINUS NOTATIONS CHKO _____

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	0	COOK	916	668
STA. 190+00 (NB I-57) TO STA. 2316+00 (NB RYAN) STA. 190+00 (NB FORD) TO STA. 2316+00 (NB RYAN) FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT • (1516.1, 1717, & 1818) R-4				
				62304



TYLIN INTERNATIONAL

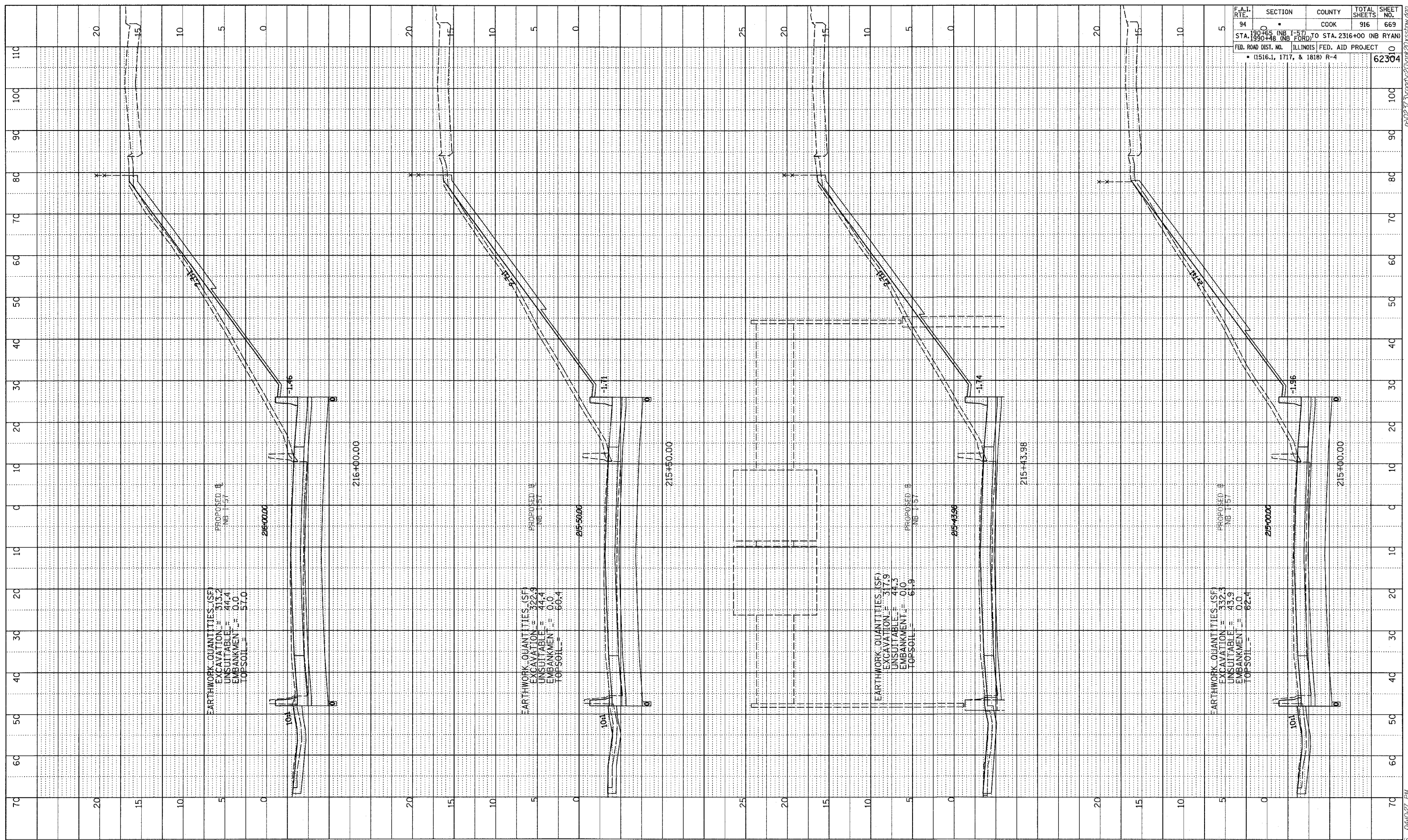


NORTHBOUND I-57
 STA. 213+00 TO STA. 214+50

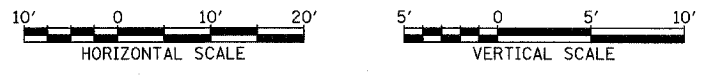
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PROFILE SURVEYED BY: _____
 GRADES CHECKED BY: _____
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+00	COOK	916	669
STA. 190+00 (NB I-57) TO STA. 2316+00 (NB RYAN) STA. 190+00 (NB I-57) TO STA. 2316+00 (NB RYAN) FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT (1516.1, 1717, & 1818) R-4				
				62304



TYLIN INTERNATIONAL

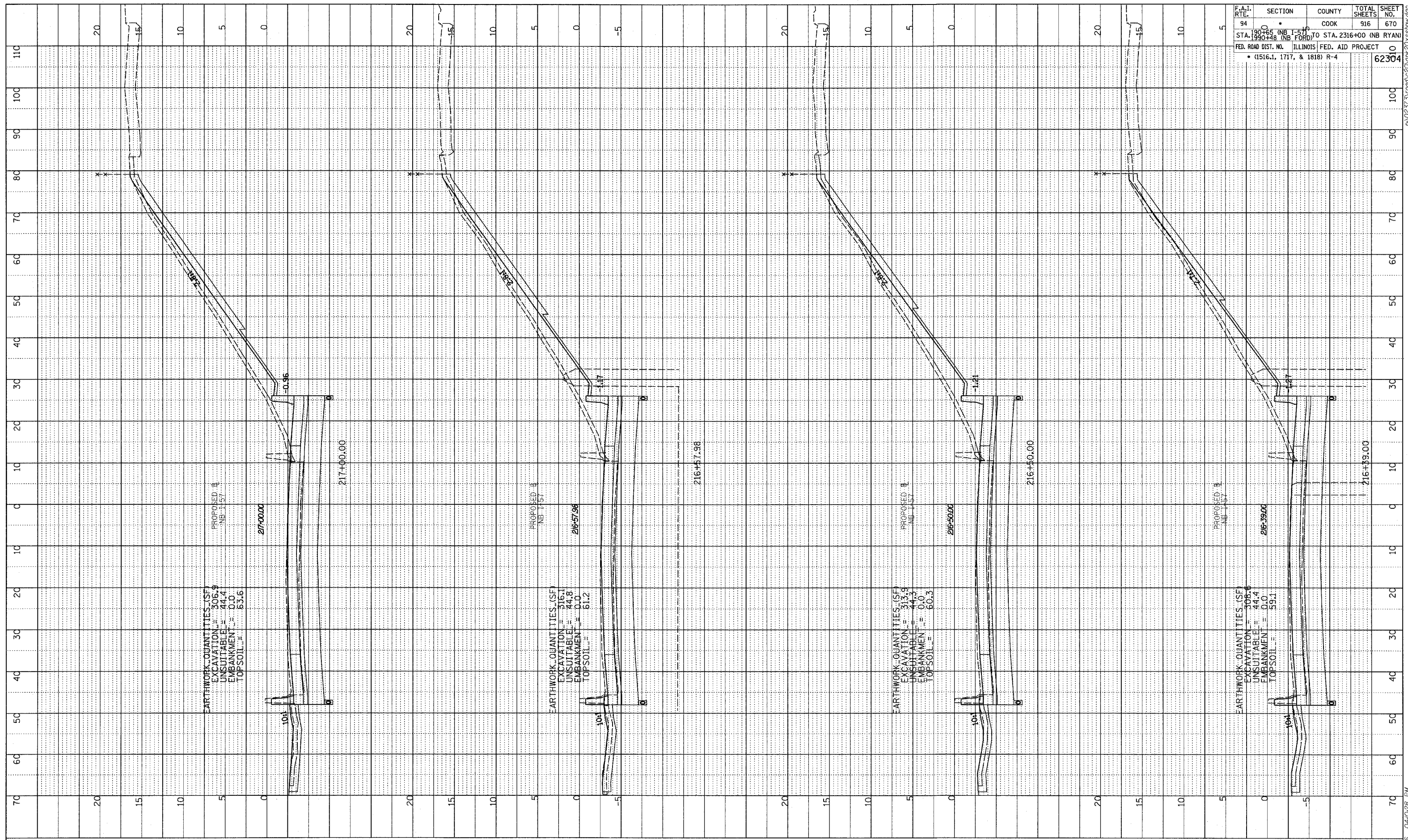


NORTHBOUND I-57
 STA. 215+00 TO STA. 216+00

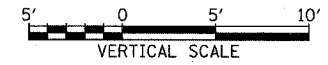
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PROFILE SUBMITTED BY: _____ DATE: _____
 GRADES CHECKED BY: _____
 NOTE BOOK NO. _____
 BY: _____
 DATE: _____

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	0	COOK	916	670
STA. 190+00 (NB I-57) TO STA. 2316+00 (NB RYAN) STA. 190+48 (NB FORD)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT • (1516.1, 1717, & 1818) R-4				
				62304



TYLIN INTERNATIONAL

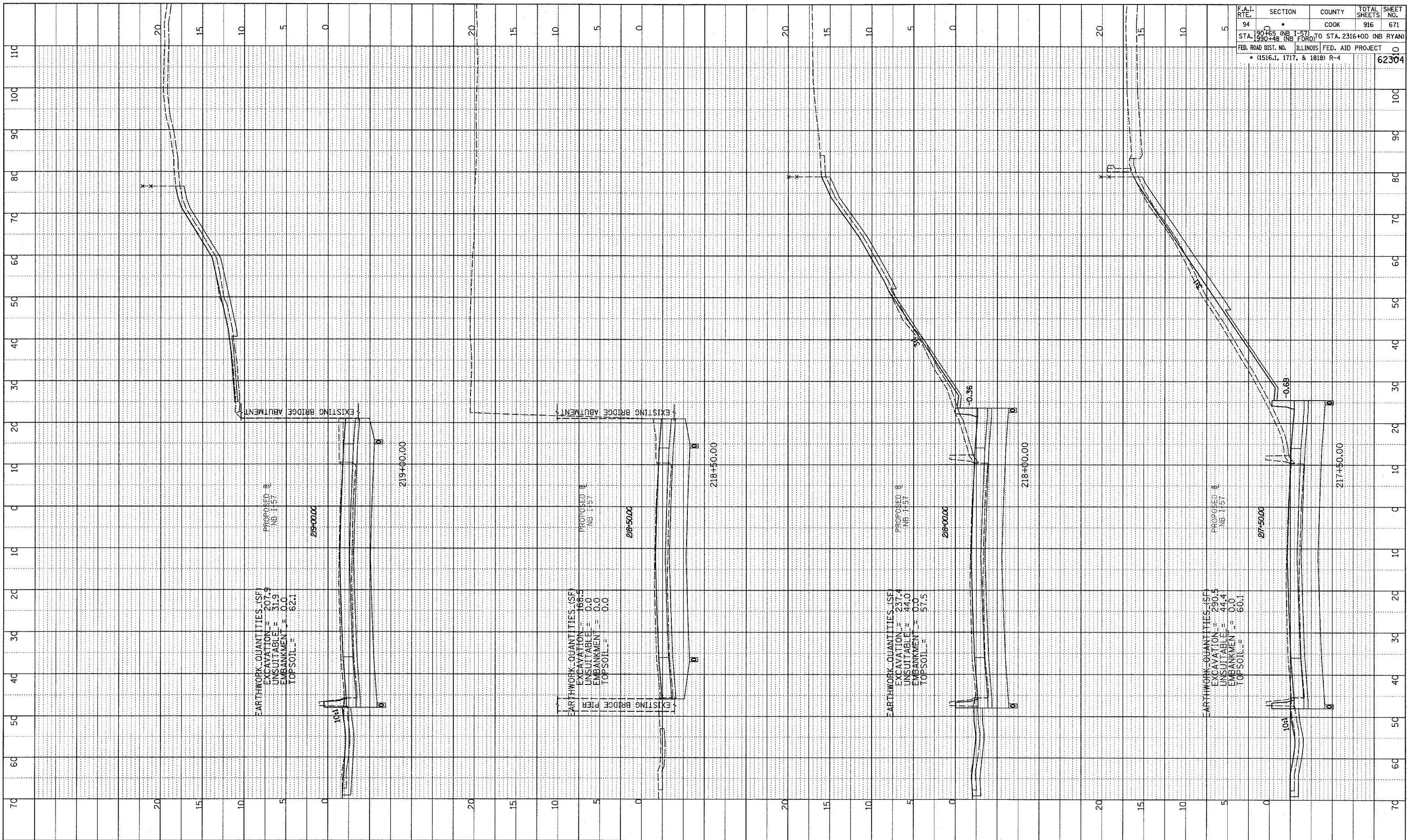


NORTHBOUND I-57
 STA. 216+39 TO STA. 217+00

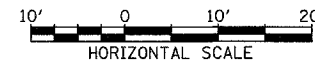
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	0	COOK	916	671
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN) STA. 190+48 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT • (1516.1, 1717, & 1818) R-4				
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TYLIN INTERNATIONAL

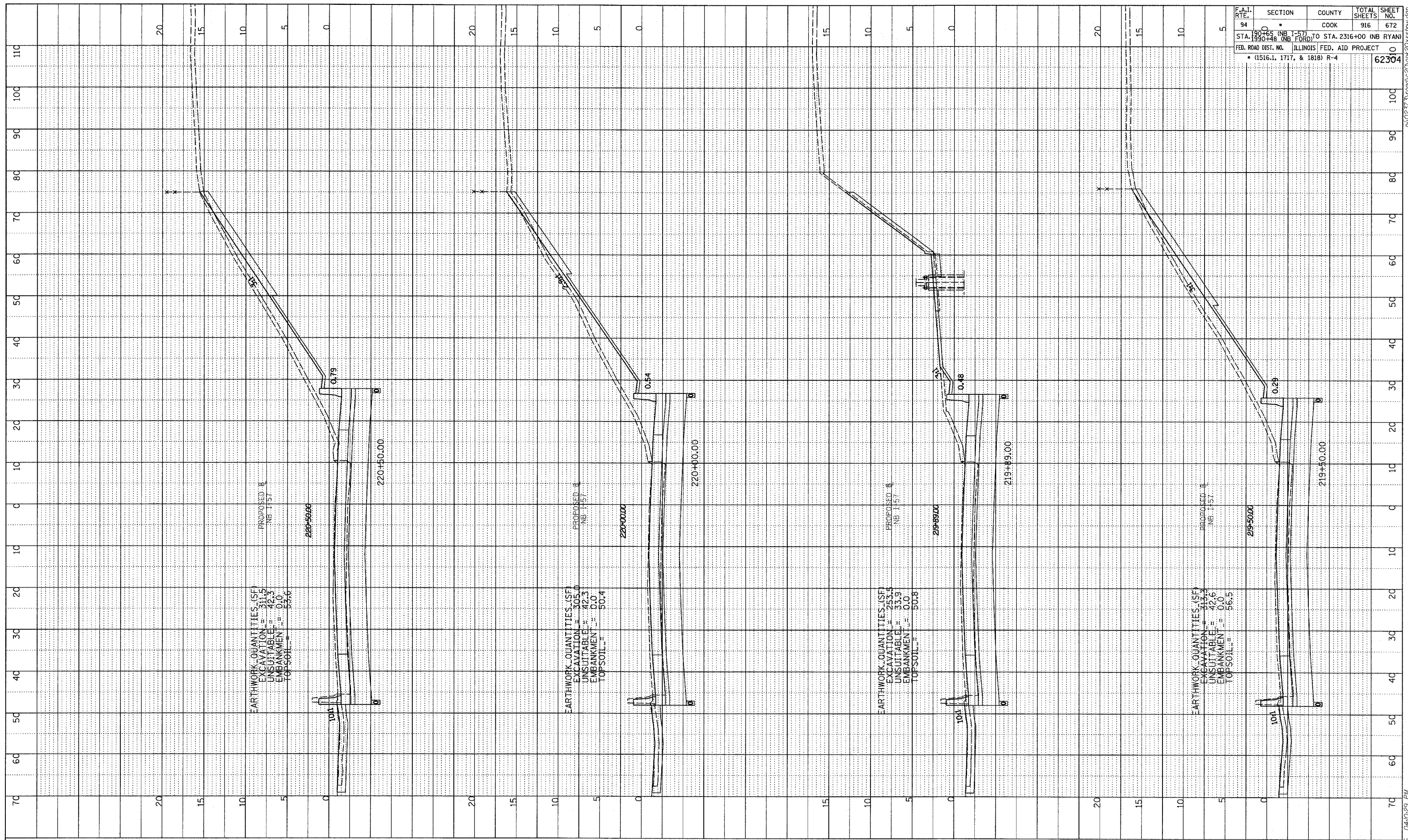


NORTHBOUND I-57
 STA. 217+50 TO STA. 219+00

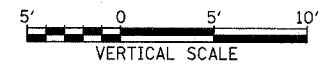
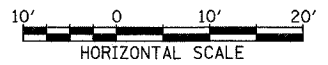
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 BY: _____
 CHECKED: _____
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 PROJECT: _____
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 SHEET NO.: _____

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+00	COOK	916	672
STA. 190+00 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				



TYLIN INTERNATIONAL

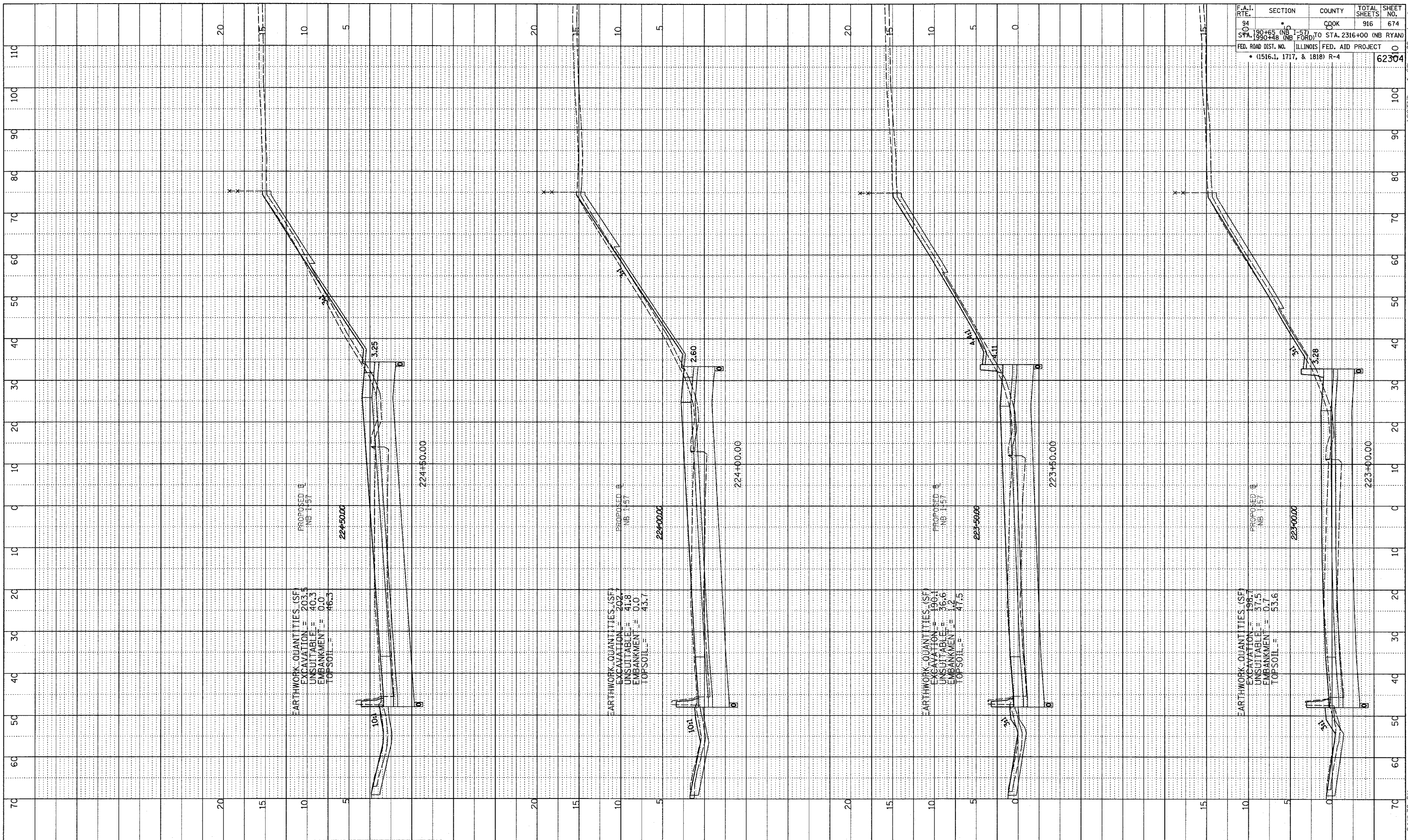


NORTHBOUND I-57
 STA. 219+50 TO STA. 220+50

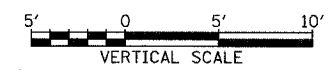
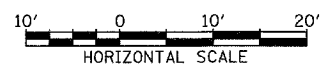
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 NO. STRUCTURE NOTATIONS CHRD _____

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)	COOK	916	674
00	190+48 (NB I-57) TO STA. 2316+00 (NB RYAN)			
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				62304
• (1516.1, 1717, & 1818) R-4				



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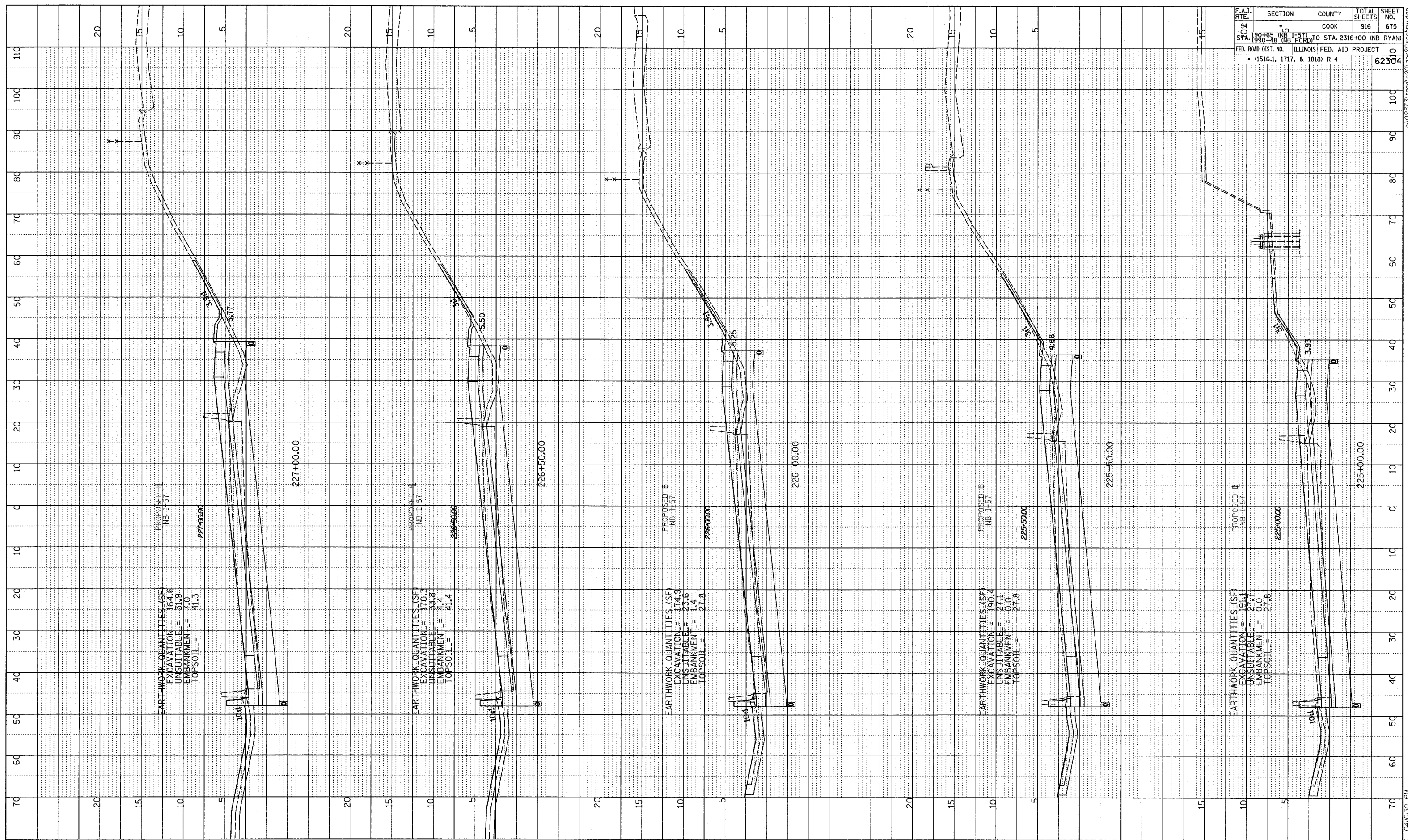


NORTHBOUND I-57
STA. 223+00 TO STA. 224+50

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 NOTE BOOK GRADES CHECKED _____
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	0	COOK	916	675
STA. 190+45 (NB I-57)	TO STA. 2316+00 (NB RYAN)			
STA. 190+48 (NB FORD)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				62304
• (1516.1, 1717, & 1818) R-4				



TYLIN INTERNATIONAL

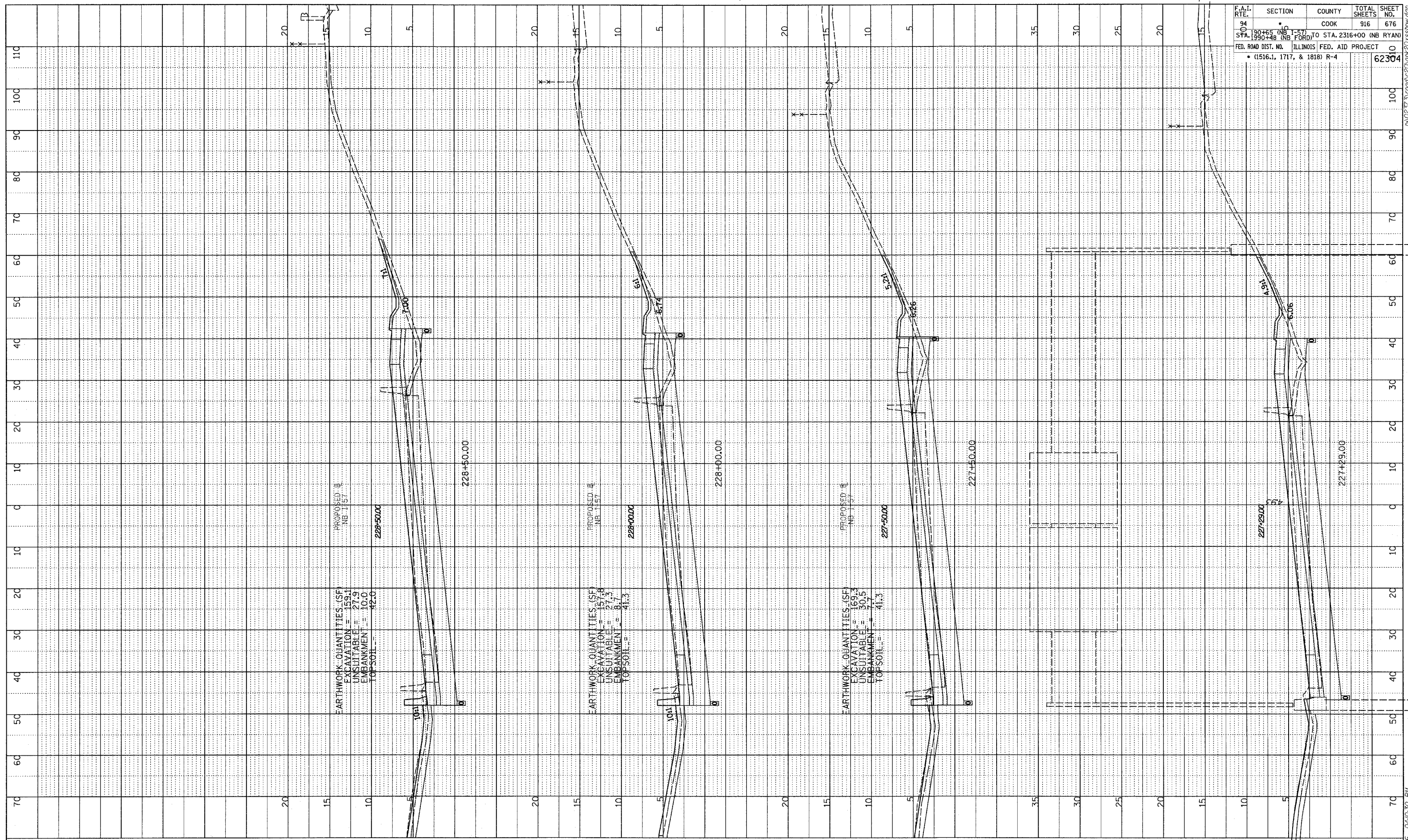


NORTHBOUND I-57
 STA. 225+00 TO STA. 227+00

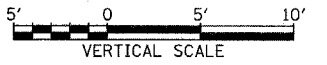
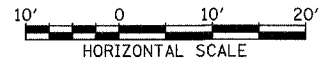
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+45 (NB I-57) TO STA. 2316+00 (NB RYAN)	COOK	916	676
STA. 190+48 (NB I-57)	ILLINOIS FED. AID PROJECT			
FED. ROAD DIST. NO.	• (1516.1, 1717, & 1818) R-4			
	62304			



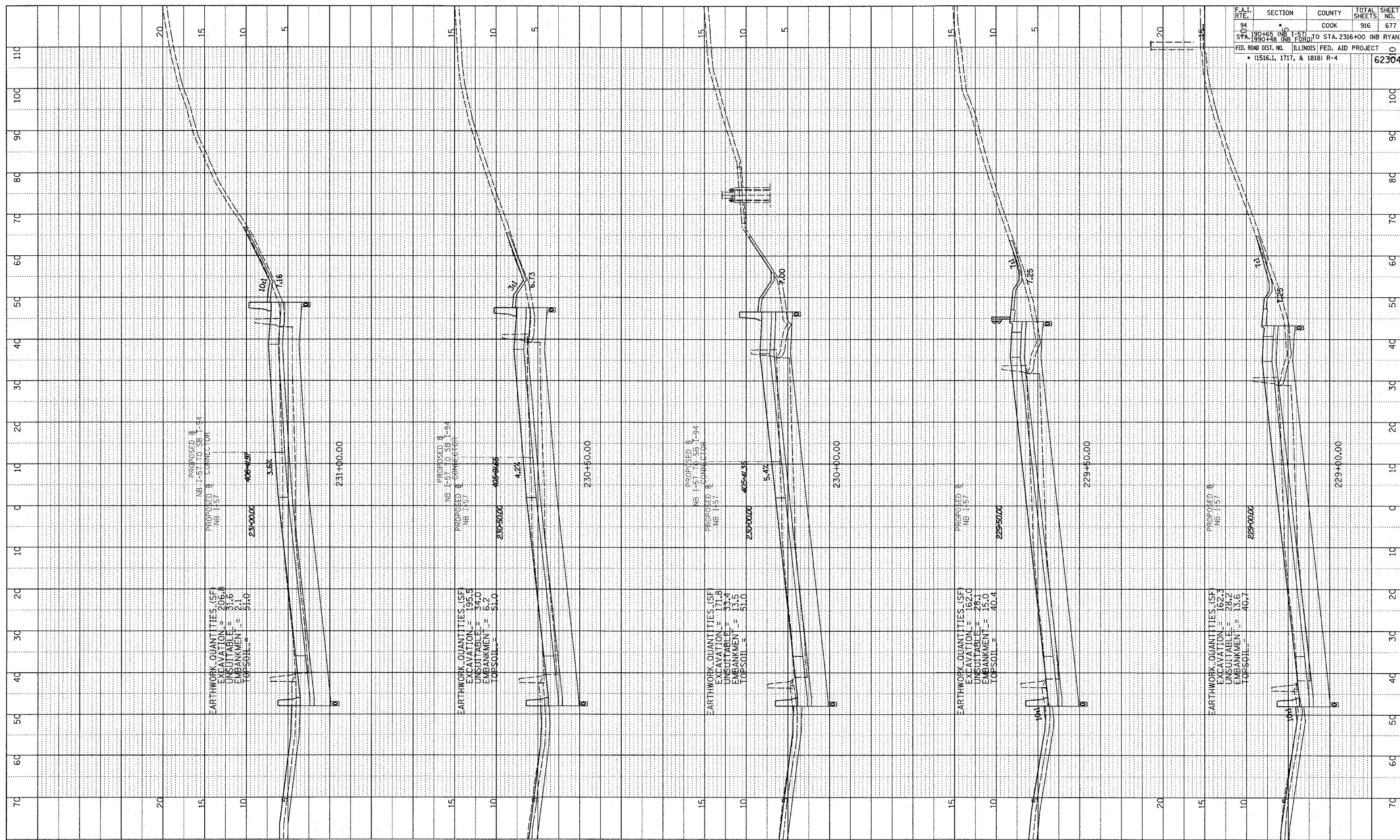
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NORTHBOUND I-57
STA. 227+29 TO STA. 228+50

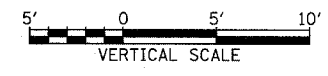
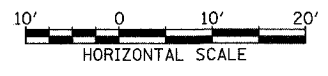
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	•	COOK	916	677
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				
				62304

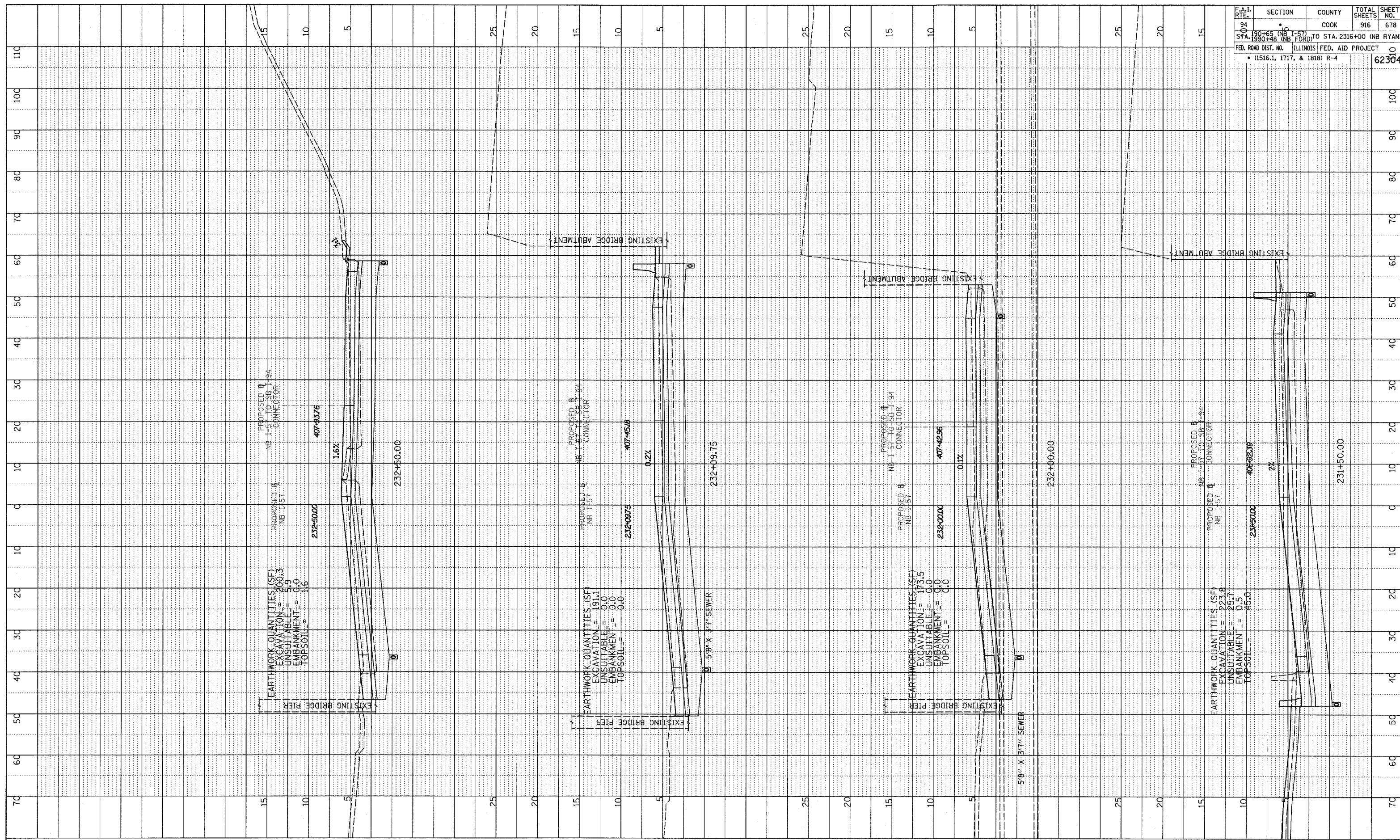
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NORTHBOUND I-57
 STA. 229+00 TO STA. 231+00

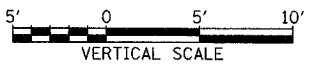
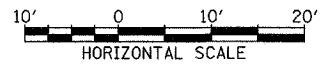
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	150+00 NB I-57 TO STA. 190+00 NB I-57	COOK	916	678
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				
				62304

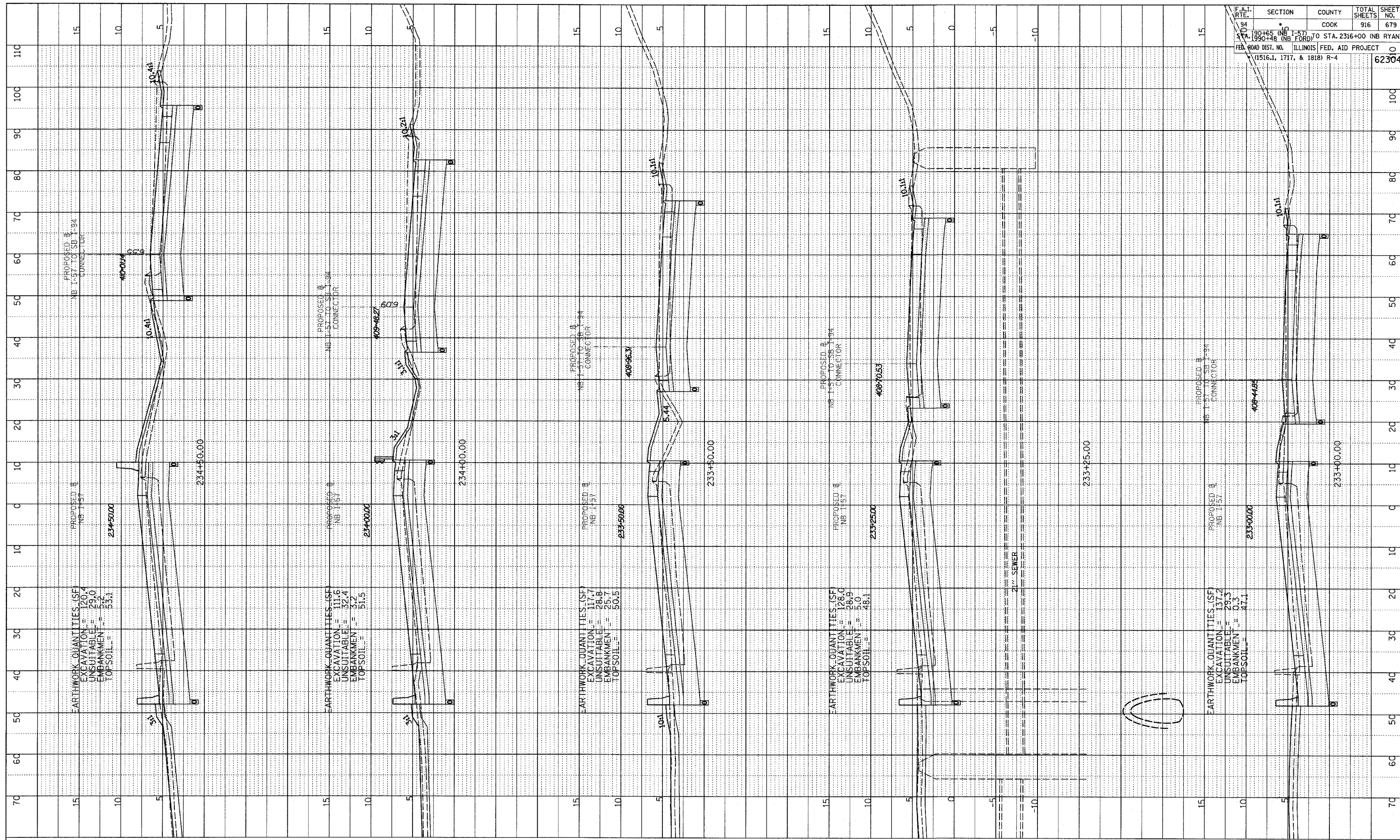
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NORTHBOUND I-57
STA. 231+50 TO STA. 232+50

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 GRADES CHECKED BY: _____
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)	COOK	916	679	10
1990+48 (NB FORD) TO STA. 2316+00 (NB RYAN)	ILLINOIS	FED. AID PROJECT		62304
(1516.1, 1717, & 1818) R-4				

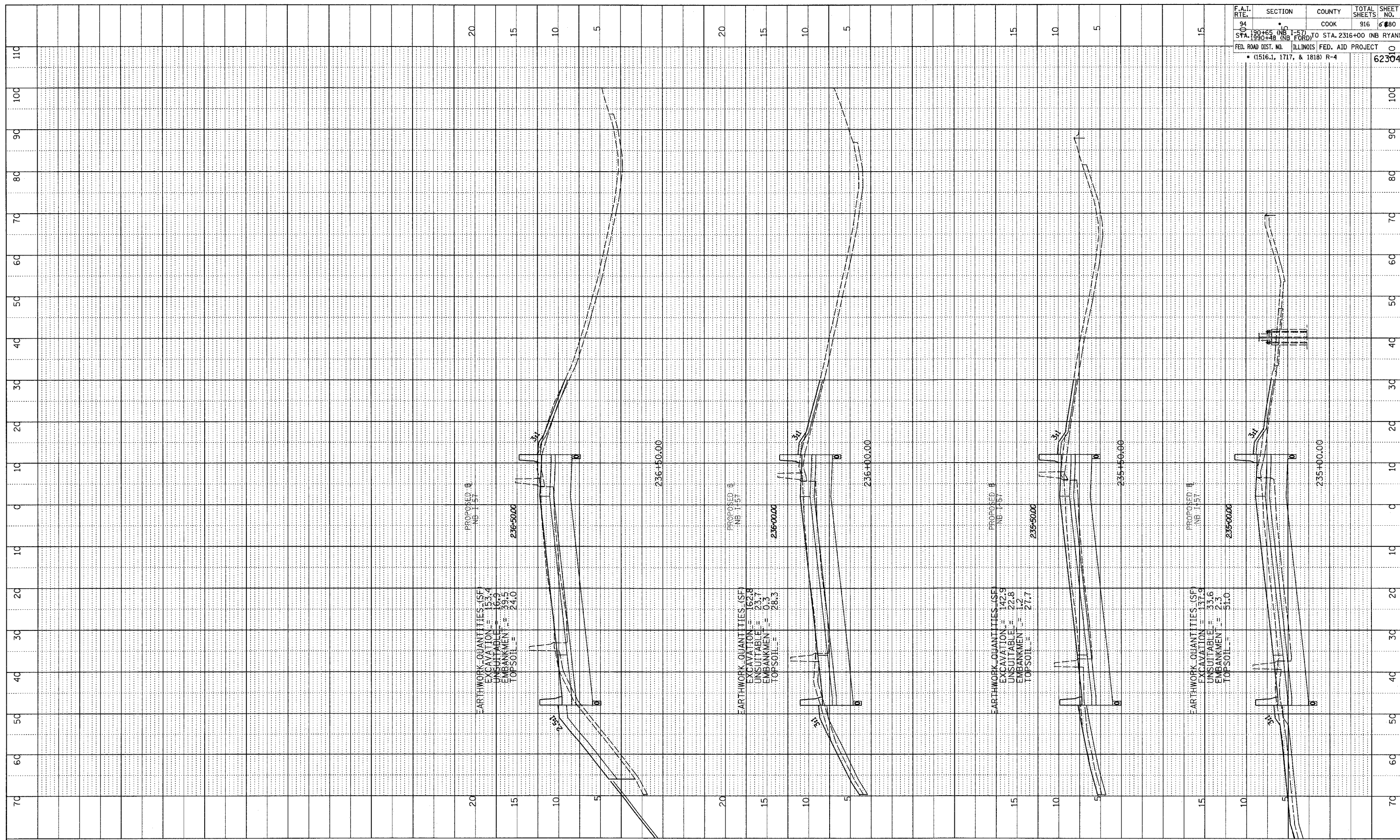
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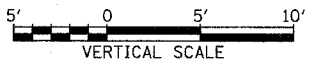
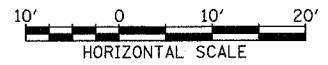
NORTHBOUND I-57
 STA. 233+00 TO STA. 234+50

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 GRADES CHECKED _____
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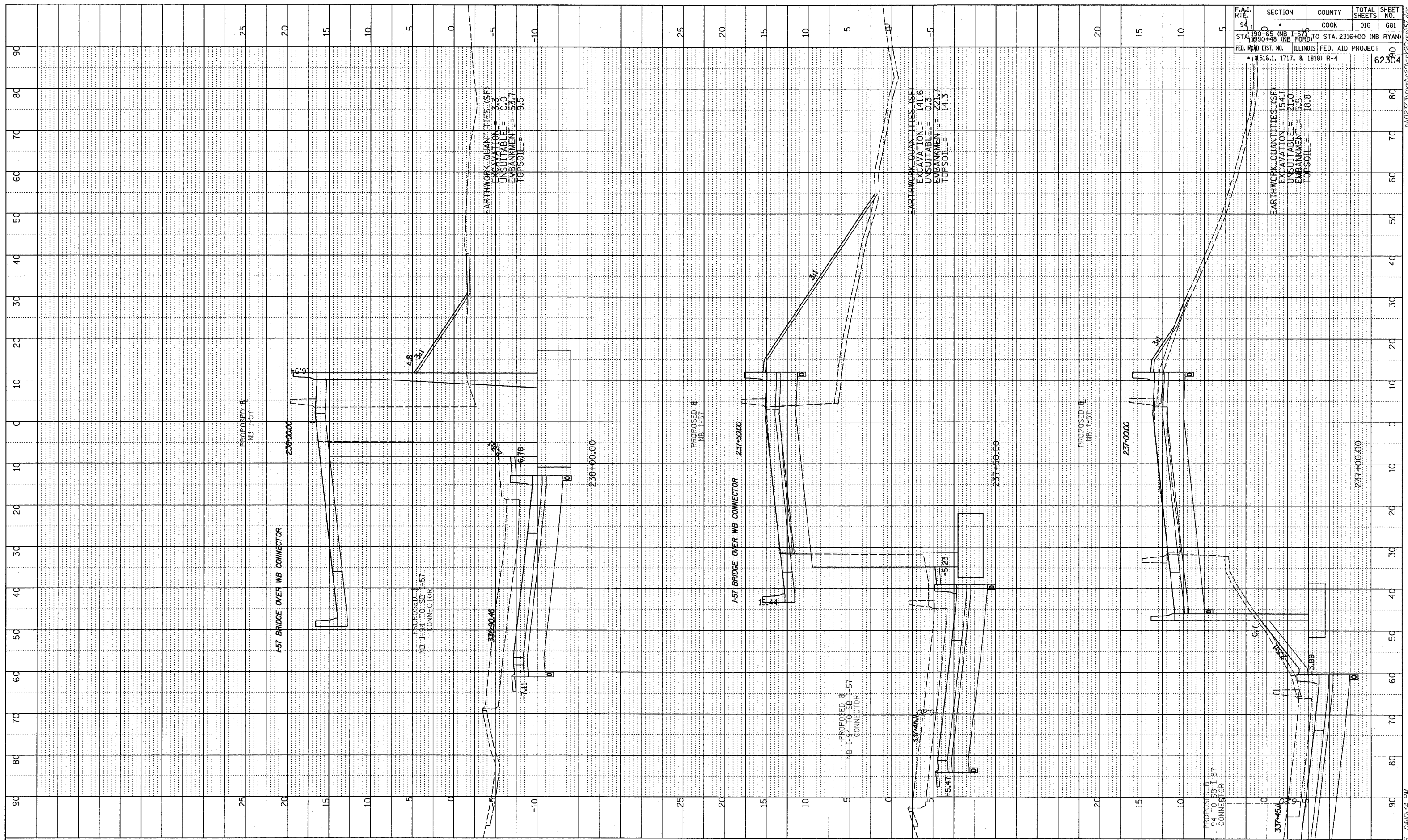


NORTHBOUND I-57
 STA. 235+00 TO STA. 236+50

F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)	COOK	916	680
0	STA. 190+48 (NB I-57) TO STA. 2316+00 (NB RYAN)			
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				62304
• (1516.1, 1717, & 1818) R-4				

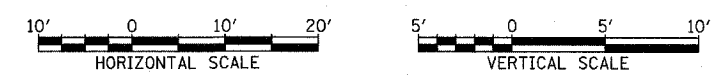
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F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1-57	COOK	916	681
STA. 237+00 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
1516.1, 1717, & 1818) R-4				
				62304

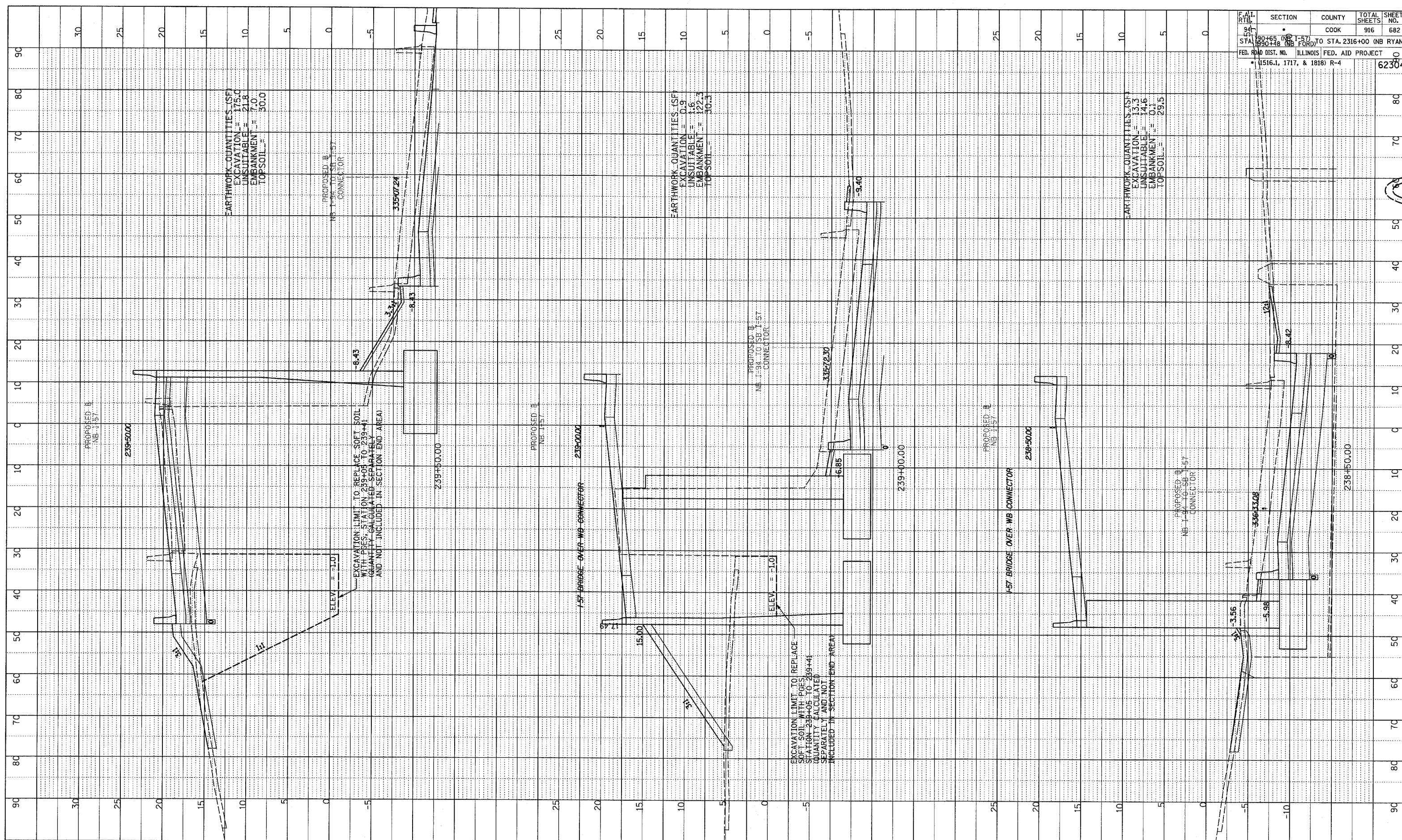
TYLIN INTERNATIONAL



NORTHBOUND I-57
 STA. 237+00 TO STA. 238+00

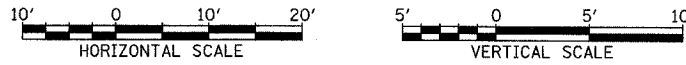
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 GRADES CHECKED
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 STRUCTURE NOTATIONS OK/NO
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F.A.I. RTB.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)	COOK	916	682
51	190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)	COOK	916	682
51	190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)	COOK	916	682
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
1516.1, 1717, & 1818 R-4				
				62304

TYLIN INTERNATIONAL

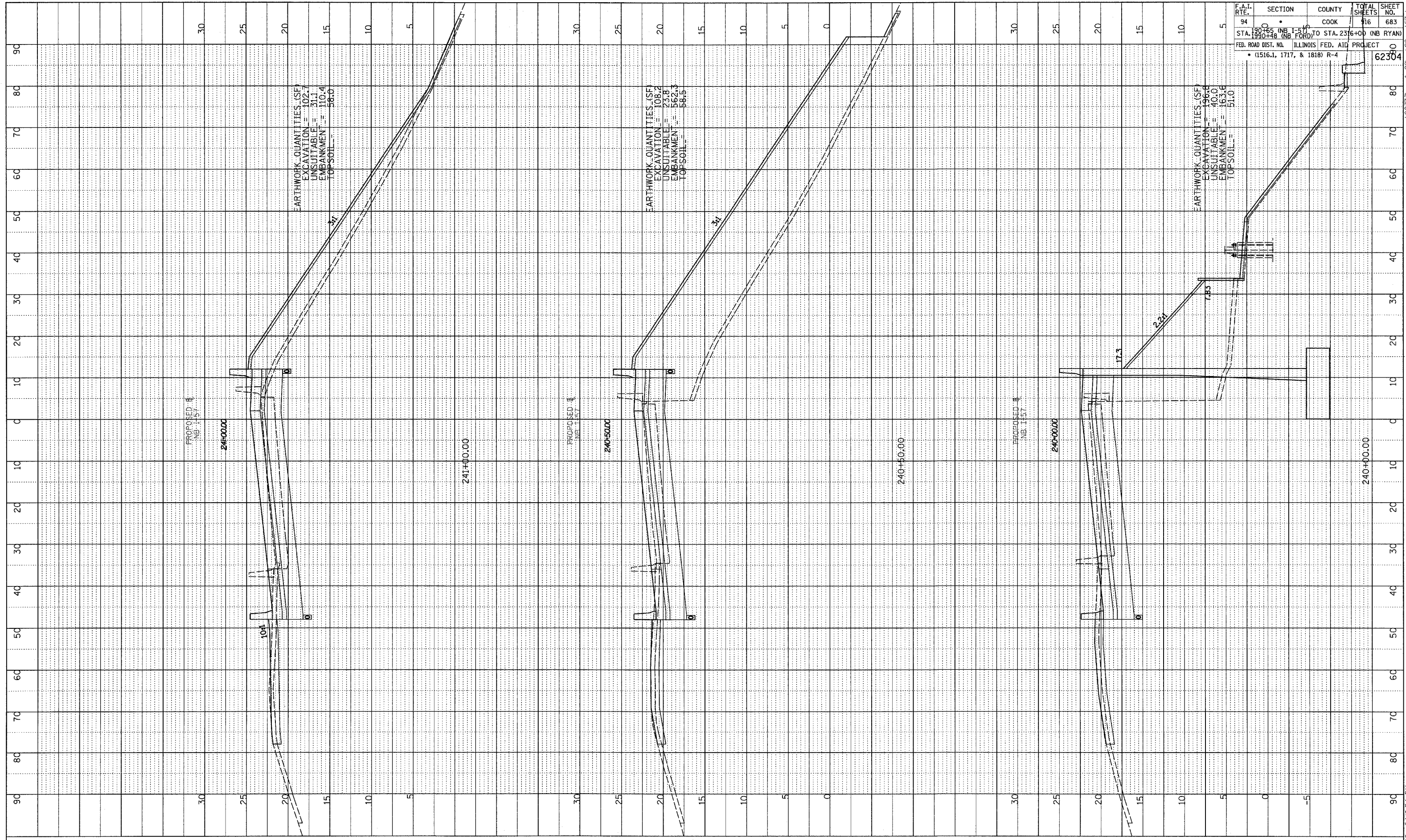


NORTHBOUND I-57
 STA. 238+50 TO STA. 239+50

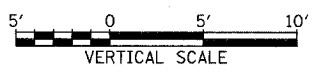
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 BY _____ DATE _____
 GRADINGS CHECKED
 STRUCTURE NOTATION CHFD

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+00	COOK	916	683
STA. 190+00 (NB I-57) TO STA. 231+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				
				62304



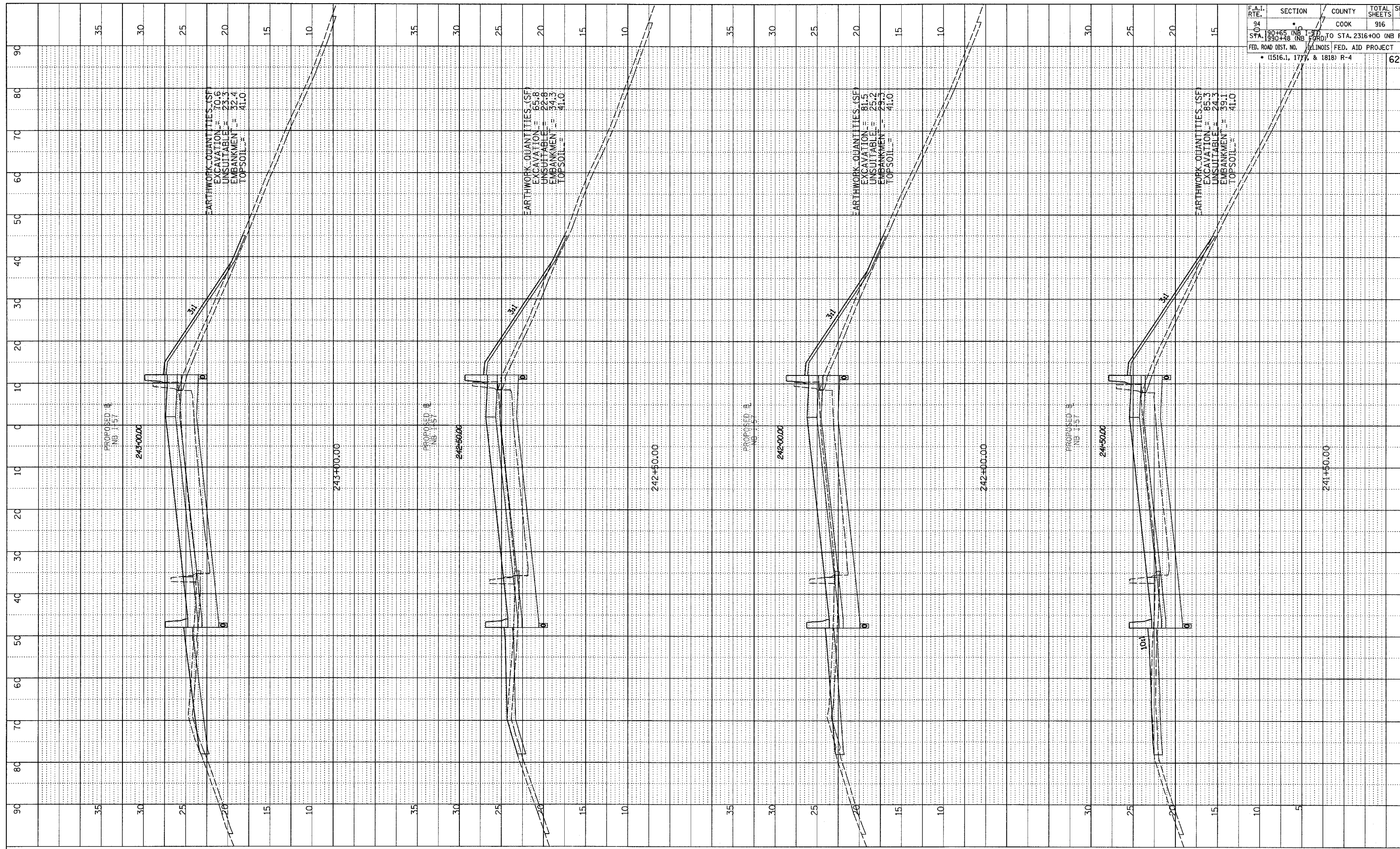
TYLIN INTERNATIONAL



NORTHBOUND I-57
 STA. 240+00 TO STA. 241+00

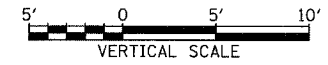
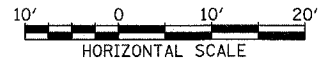
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PROFILE SURVEYED BY: _____ DATE: _____
 NOTE BOOK NO. _____ GRADES CHECKED _____
 P.M. NOTED _____
 STRUCTURE NOTATION CHKD: _____



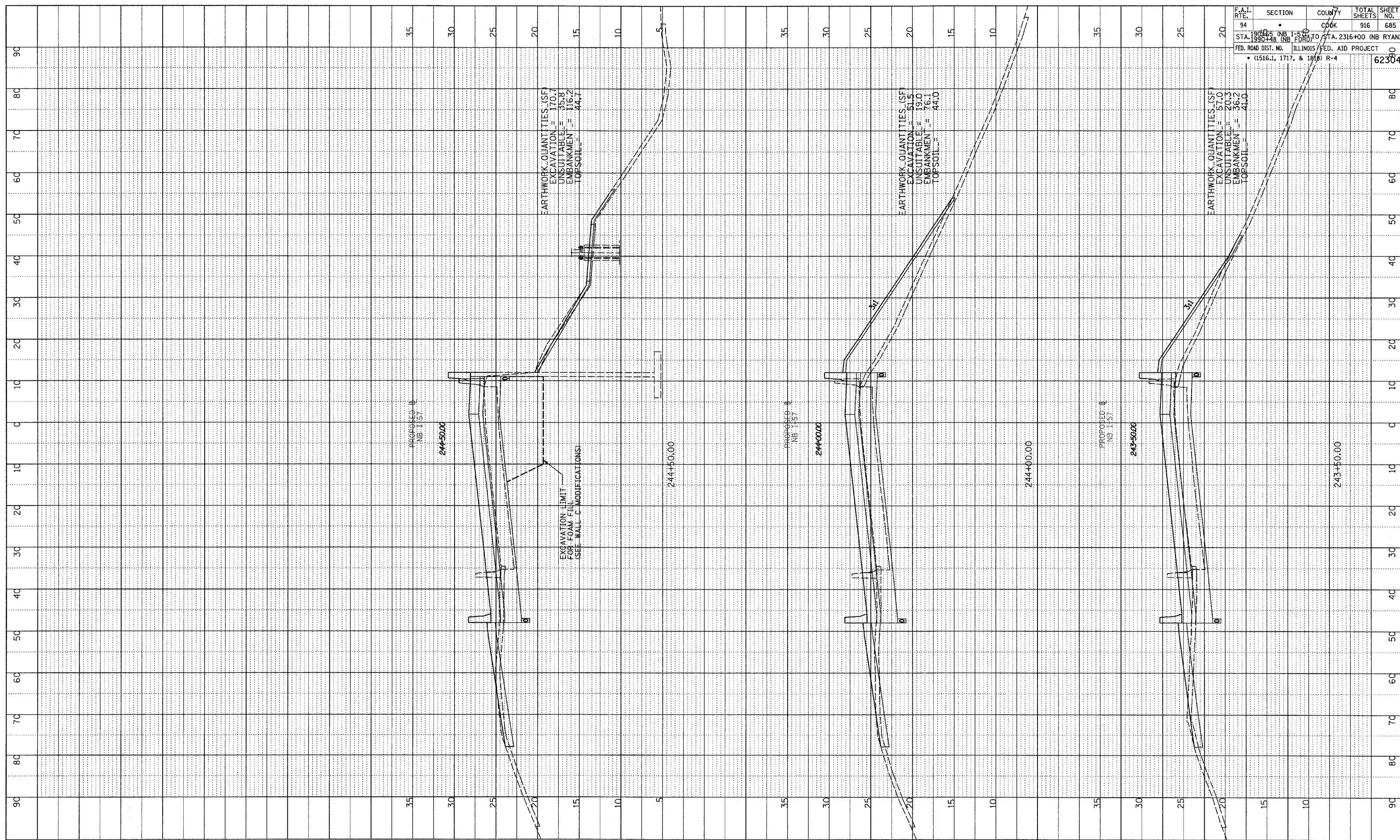
F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)	COOK	916	684
STA. 190+48 (NB I-57) TO STA. 2316+00 (NB RYAN) FED. ROAD DIST. NO. 1516.1, 1777, & 1818 R-4 ILLINOIS FED. AID PROJECT 62304				

TYLIN INTERNATIONAL



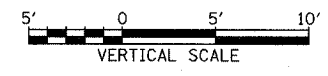
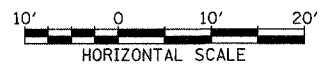
NORTHBOUND I-57
 STA. 241+50 TO STA. 243+00
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PROFILE SURVEYED _____ DATE _____
 GRADES CHECKED _____
 B.M. NOTED _____
 NO. _____ STATIONING NOTATIONS CHG'D _____



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	•	COOK	916	685
STA. 190+45 (NB I-57) TO STA. 2316+00 (NB RYAN) STA. 190+48 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT • (1516.1, 1717, & 1818) R-4				

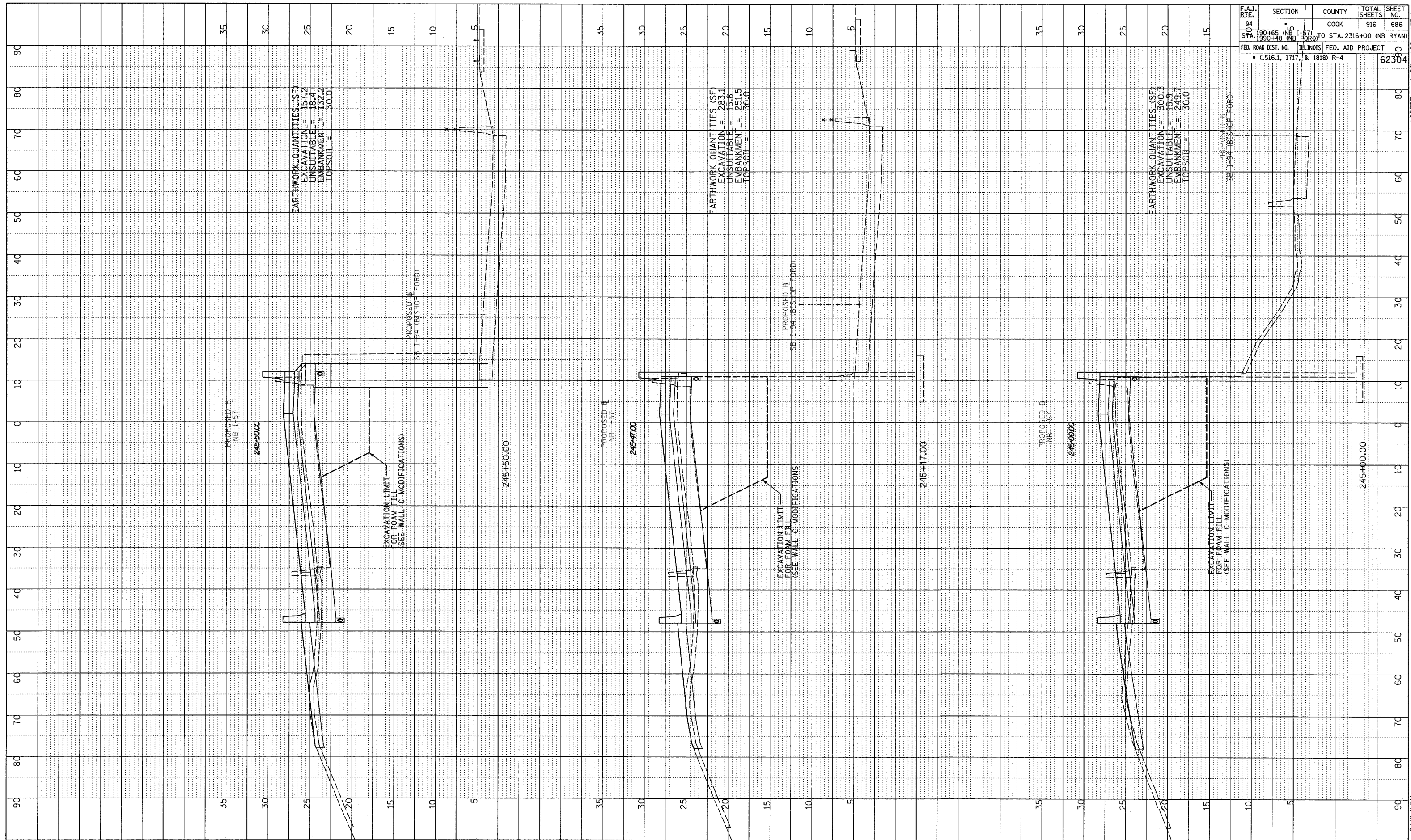
TYLIN INTERNATIONAL



NORTHBOUND I-57
 STA. 243+50 TO STA. 244+50

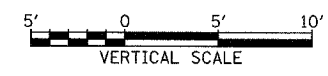
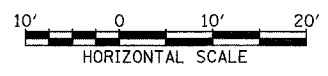
03/03/2006 04:04:40 PM

PROFILE
 SURVEYED
 PLOTTED
 GRADES CHECKED
 STRUCTURE NOTATIONS DIND
 NO. _____
 BY _____
 DATE _____



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+65 TO 231+57 (NB I-57)	COOK	916	686
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				

TYLIN INTERNATIONAL

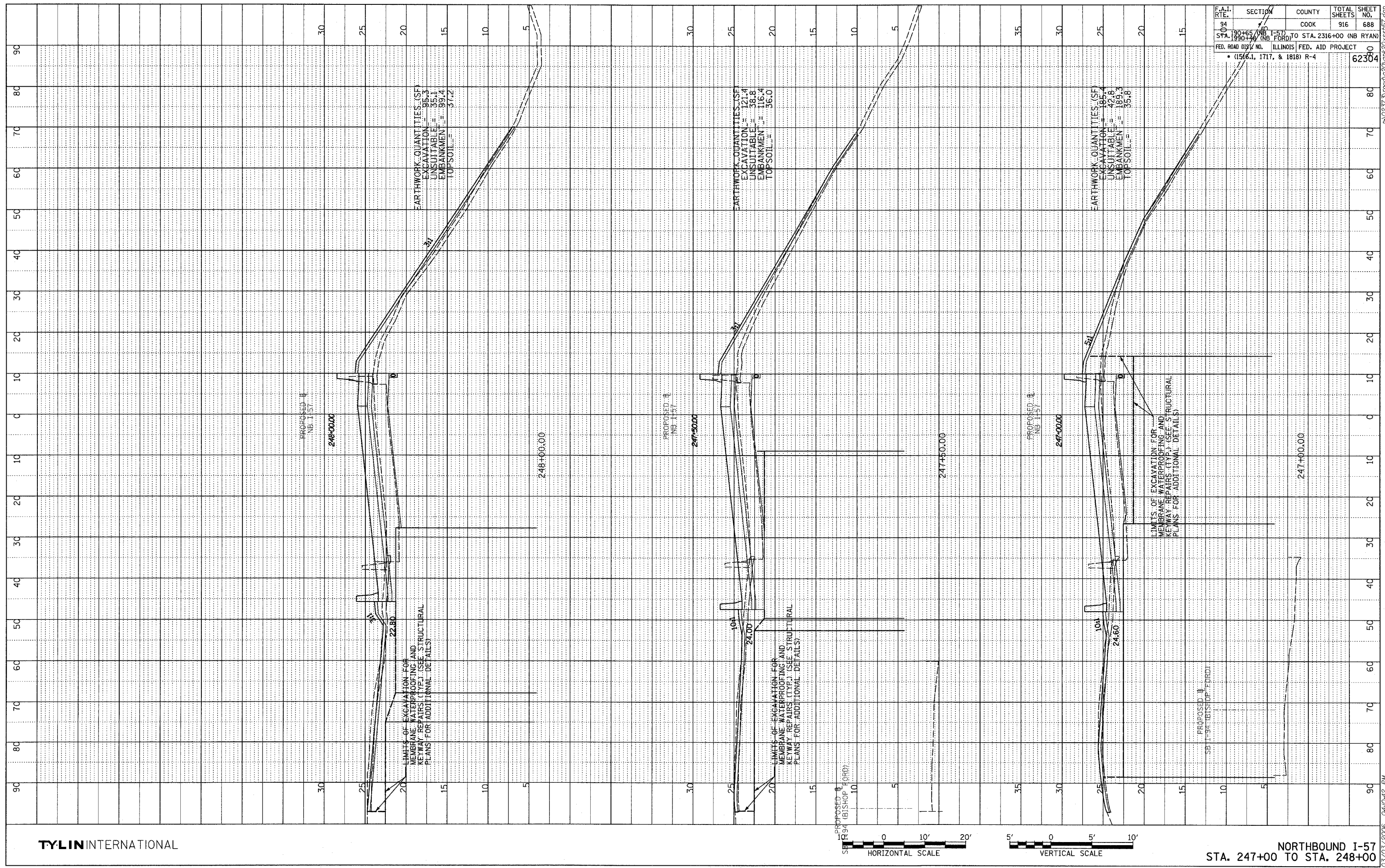


NORTHBOUND I-57
 STA. 245+00 TO STA. 245+50

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BT	DATE
PROFILE SURVEYED	
GRADES CHECKED	
BLM NOTED	
STRUCTURE NOTATIONS OK'D	
NO.	

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+55 (NB I-57) TO STA. 2316+00 (NB RYAN)	COOK	916	688
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				



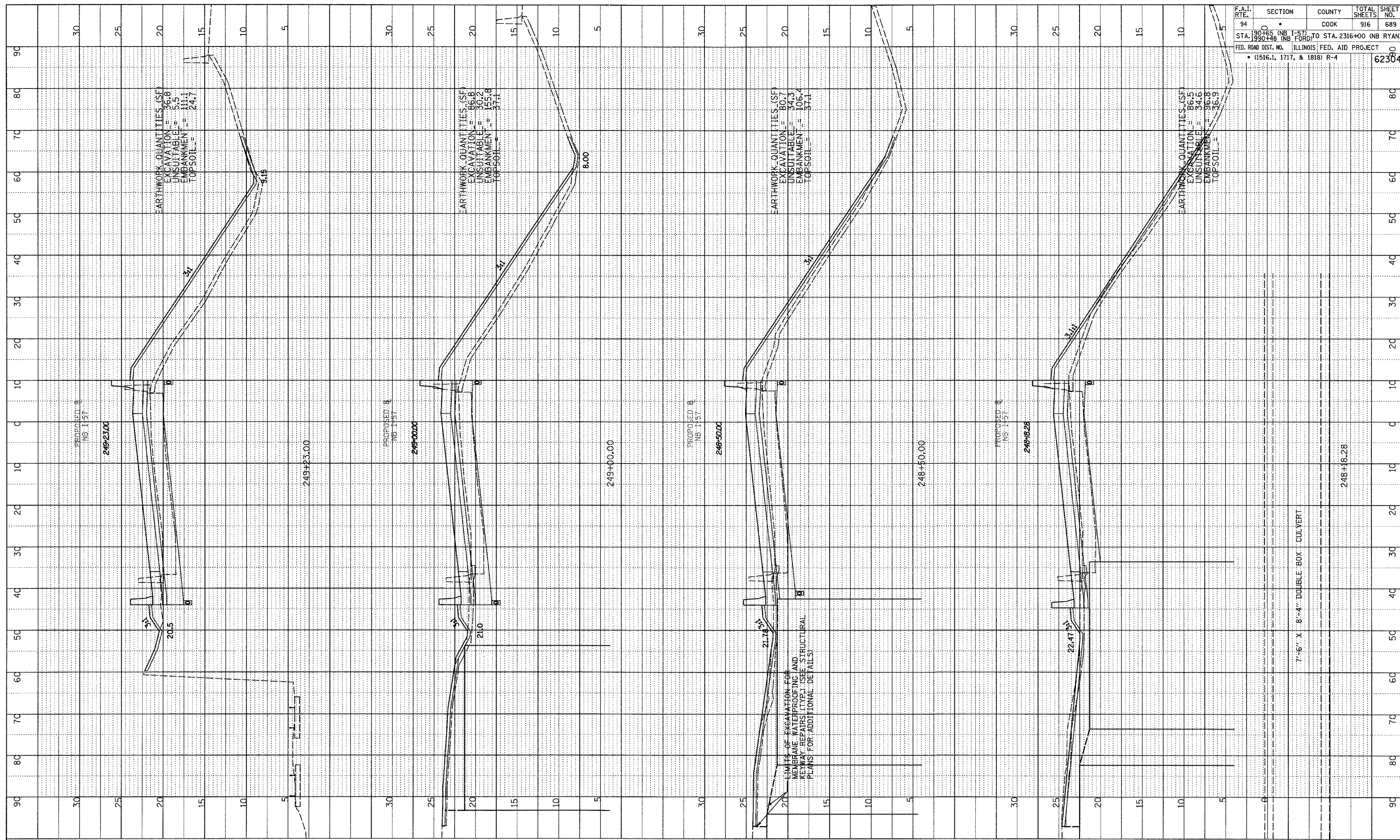
TYLIN INTERNATIONAL



NORTHBOUND I-57
STA. 247+00 TO STA. 248+00

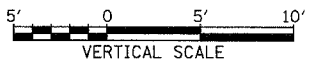
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PROFILE SURVEYED
 NOTE BOOK GRADES CHECKED
 NO. B.M. NOTED
 STRUCTURE NOTATIONS SHOWN
 BY: _____ DATE: _____



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+95 (NB I-57) TO STA. 2316+00 (NB RYAN)	COOK	916	689
STA. 248+18.28 (NB I-57) TO STA. 249+23.00 (NB RYAN) FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT • (1516.1, 1717, & 1818) R-4				

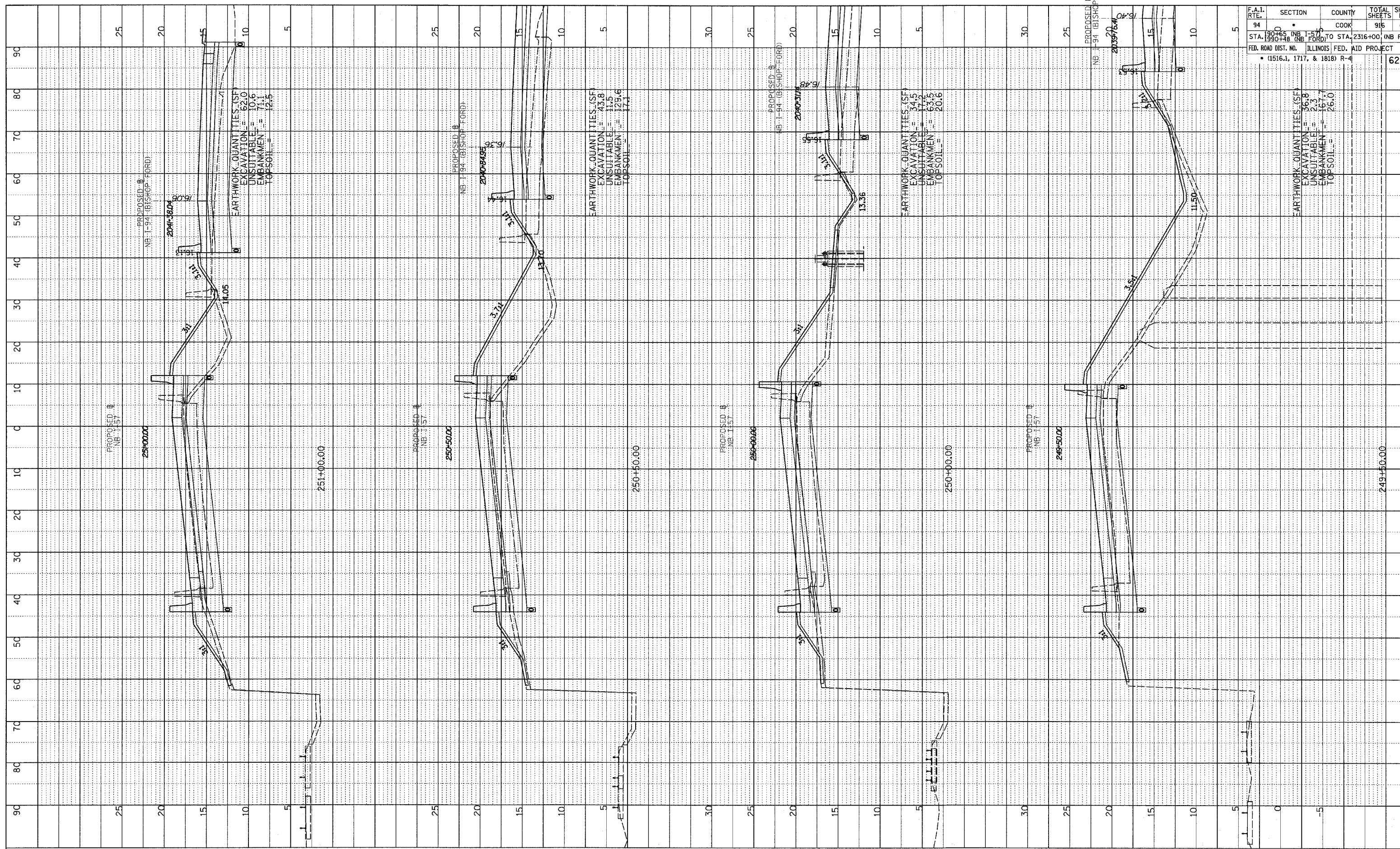
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NORTHBOUND I-57
 STA. 248+18.28 TO STA. 249+23.00

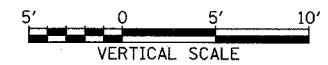
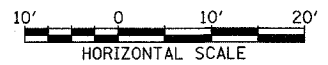
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PROFILE SURVEYED
 GRADES CHECKED
 DATE: _____ BY: _____
 NO. _____
 NOTE BOOK _____
 DATE: _____
 NO. _____
 DATE: _____
 NO. _____



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+00	COOK	916	690
STA. 190+00 (NB I-55) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				
				62304

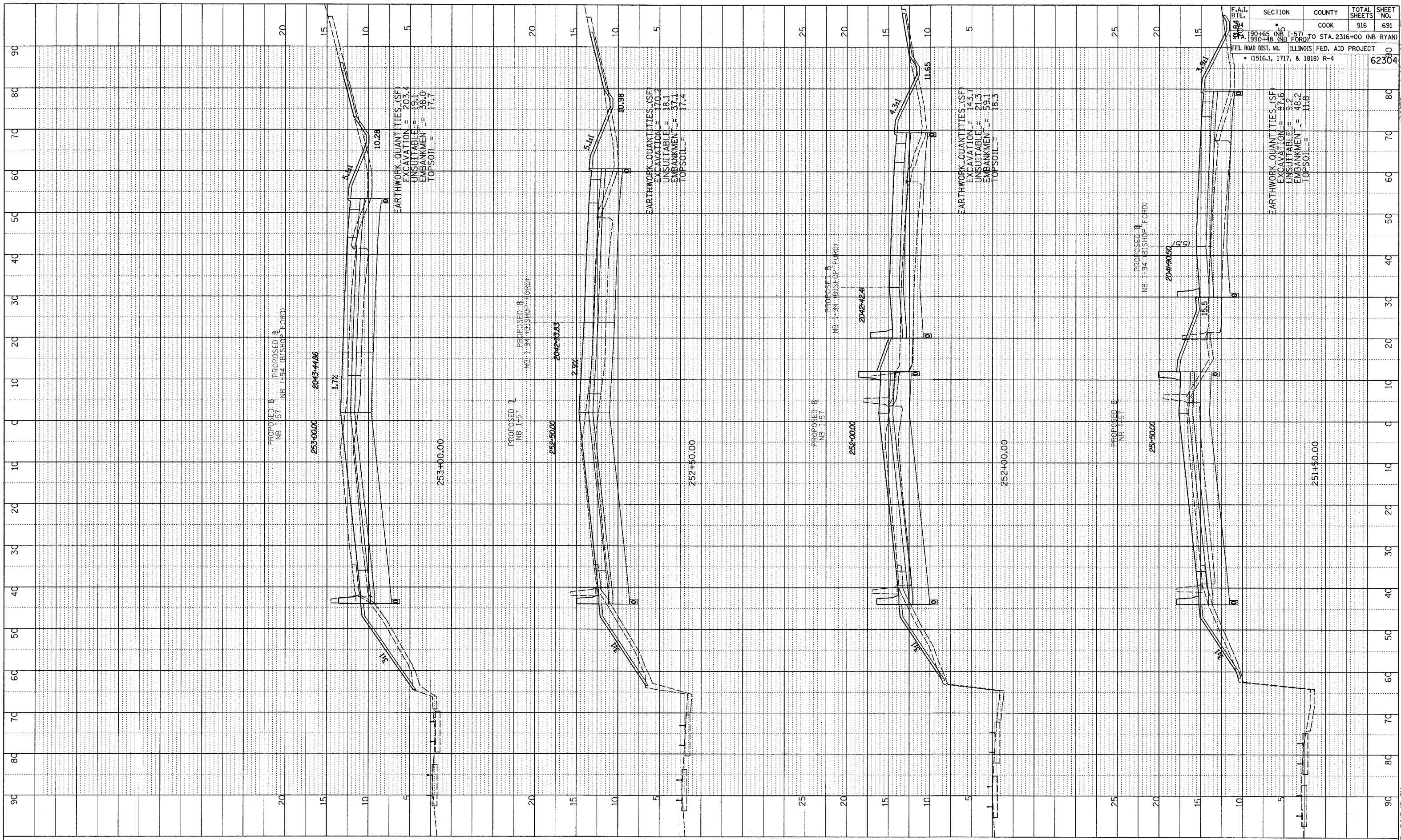
TYLIN INTERNATIONAL



NORTHBOUND I-57
 STA. 249+50 TO STA. 251+00

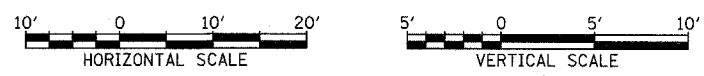
03/03/2006 04:04:42 PM

PROFILE SURVEYED BY _____ DATE _____
 PLOTTED BY _____
 NOTE BOOK NO. _____ GRANES CHECKED BY _____
 NO. _____ STRUCTURE NOTATION CHKD BY _____



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
190+65 (NB 1-57)	COOK	916	691	
190+48 (NB 1-57)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				
				62304

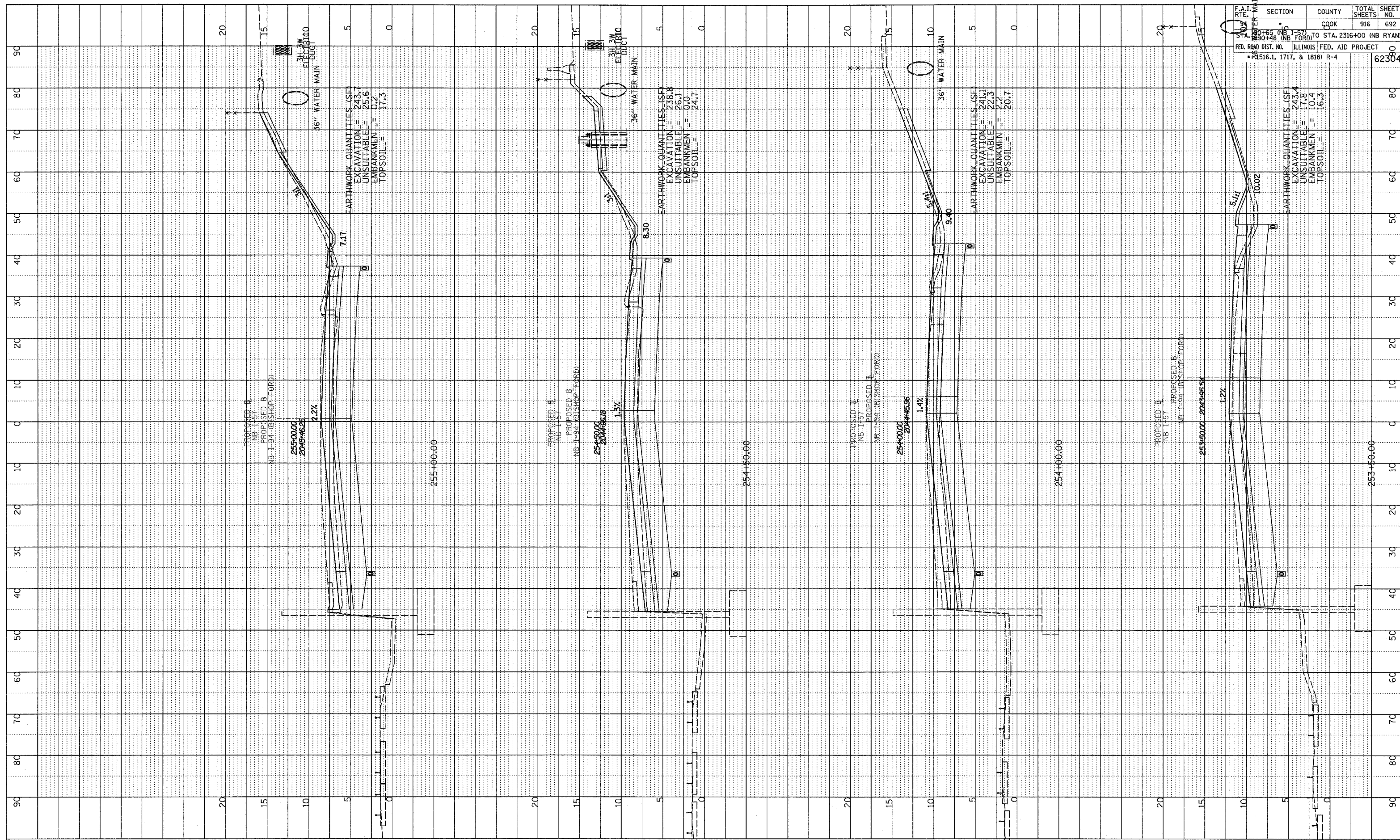
TYLIN INTERNATIONAL



NORTHBOUND I-57
 STA. 251+50 TO STA. 253+00

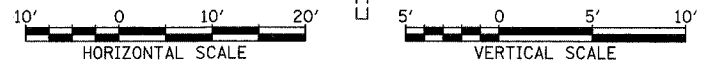
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DATE	BT
PROFILE SURVEYED	
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GRADES CHECKED	
DATE NOTED	
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BY	



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
157	1-57	COOK	916	692
STA. 2316+00 (NB RYAN)	TO STA. 2316+00 (NB RYAN)			
ILLINOIS FED. AID PROJECT				
2516.1, 1717, & 1818) R-4				
				62304

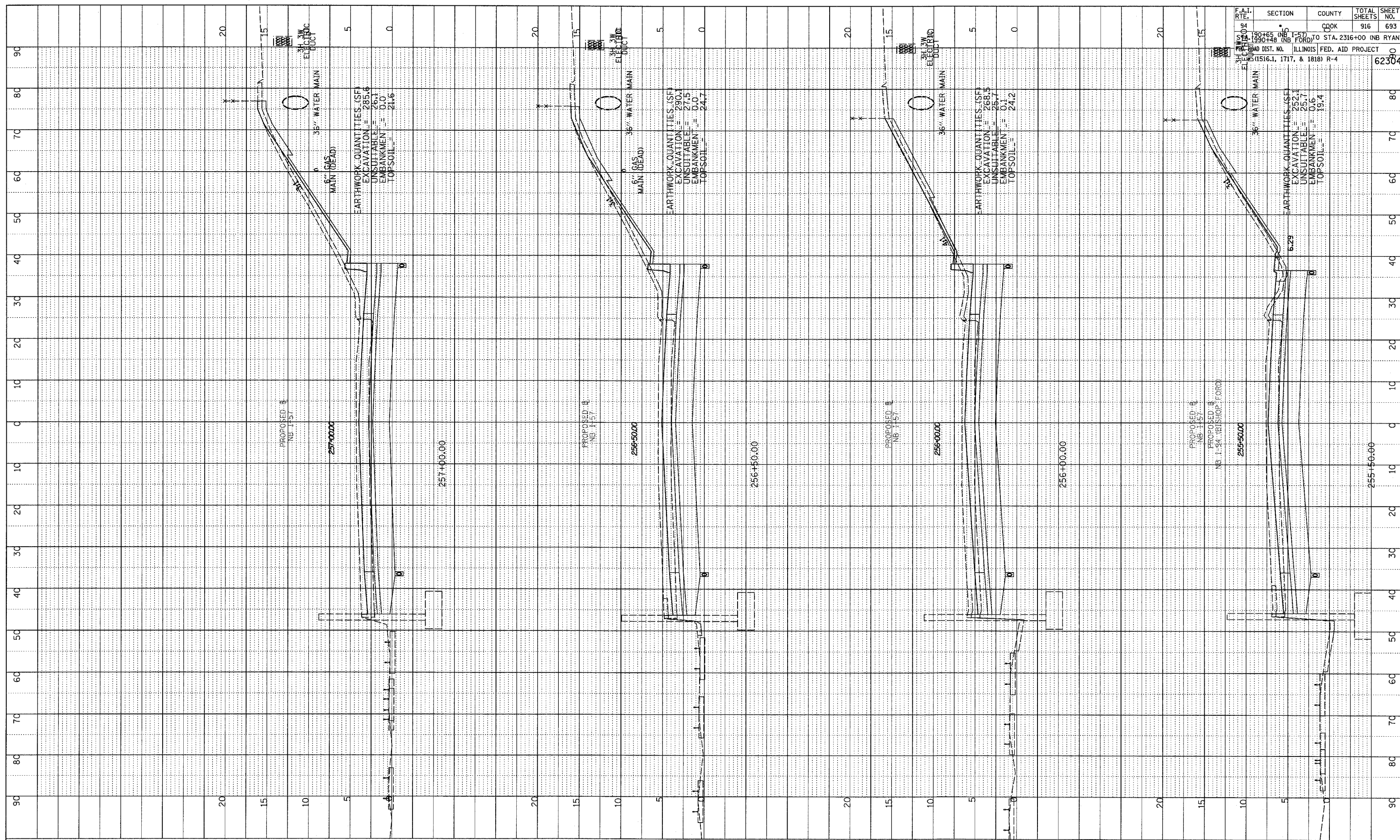
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NORTHBOUND I-57
STA. 253+50 TO STA. 255+00

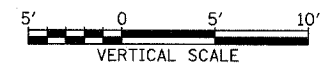
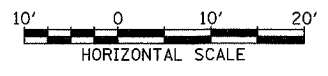
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PROFILE SURVEYED _____ DATE _____
 GRADES CHECKED _____
 NOTE BOOK _____
 NO. _____



F.A.I. RT#	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	94	COOK	916	693
FROM STA. 190+45 (NB I-57) TO STA. 2316+00 (NB RYAN) FROM STA. 190+48 (NB I-57) TO STA. 2316+00 (NB RYAN)				
ILLINOIS FED. AID PROJECT DIST. NO. (1516.1, 1717, & 1818) R-4				
				62304

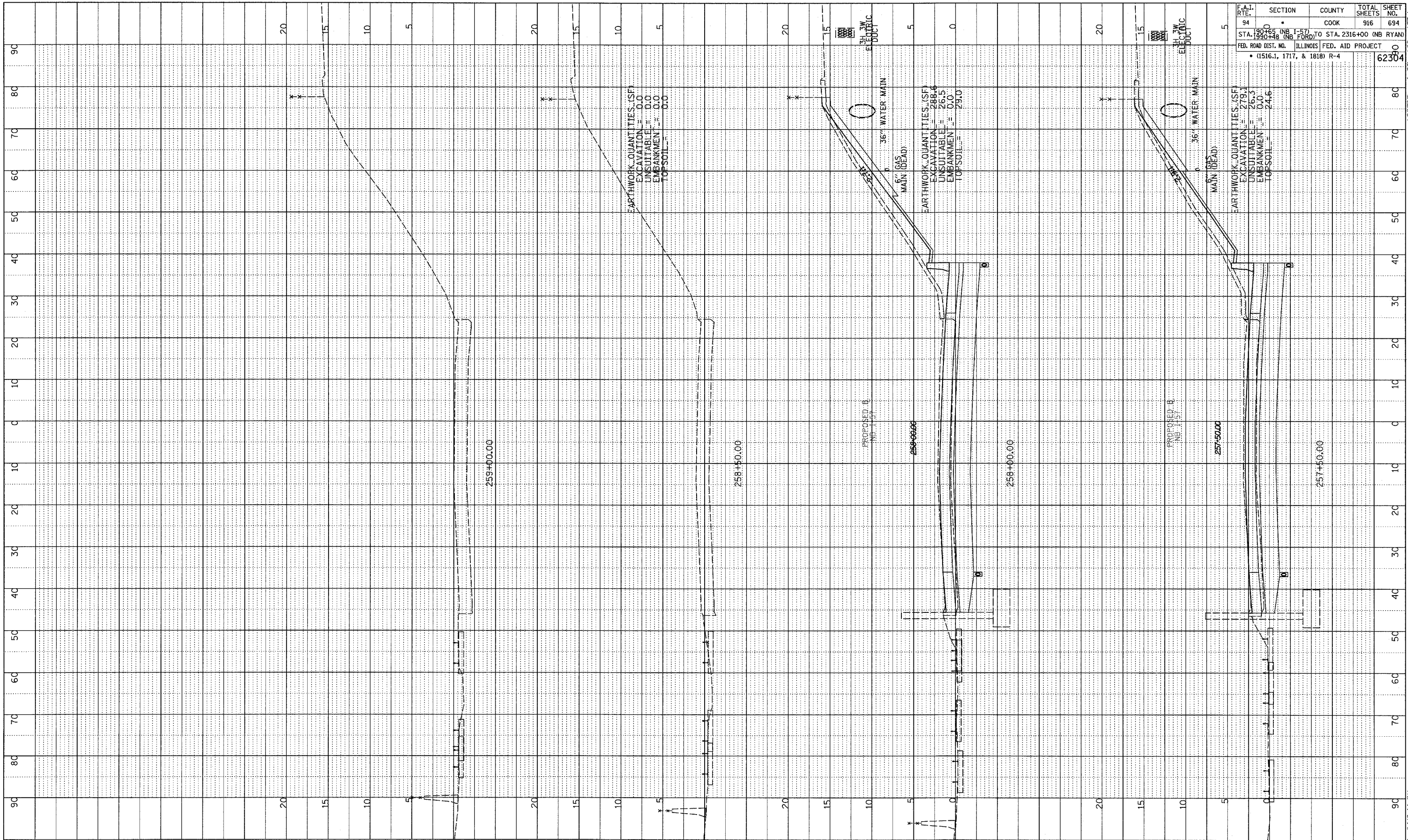
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NORTHBOUND I-57
 STA. 255+50 TO STA. 257+00

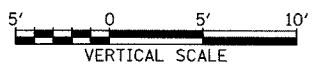
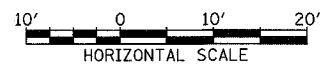
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PROFILE REQUESTED _____ BY _____ DATE _____
 PLOTTED _____
 GRADINGS CHECKED _____
 STRUCTURE NOTATIONS CHECKED _____
 NO. _____



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	•	COOK	916	694
STA. 190+45 (NB I-57)				
STA. 190+48 (NB FORD)				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
• (1516.1, 1717, & 1818) R-4				62304

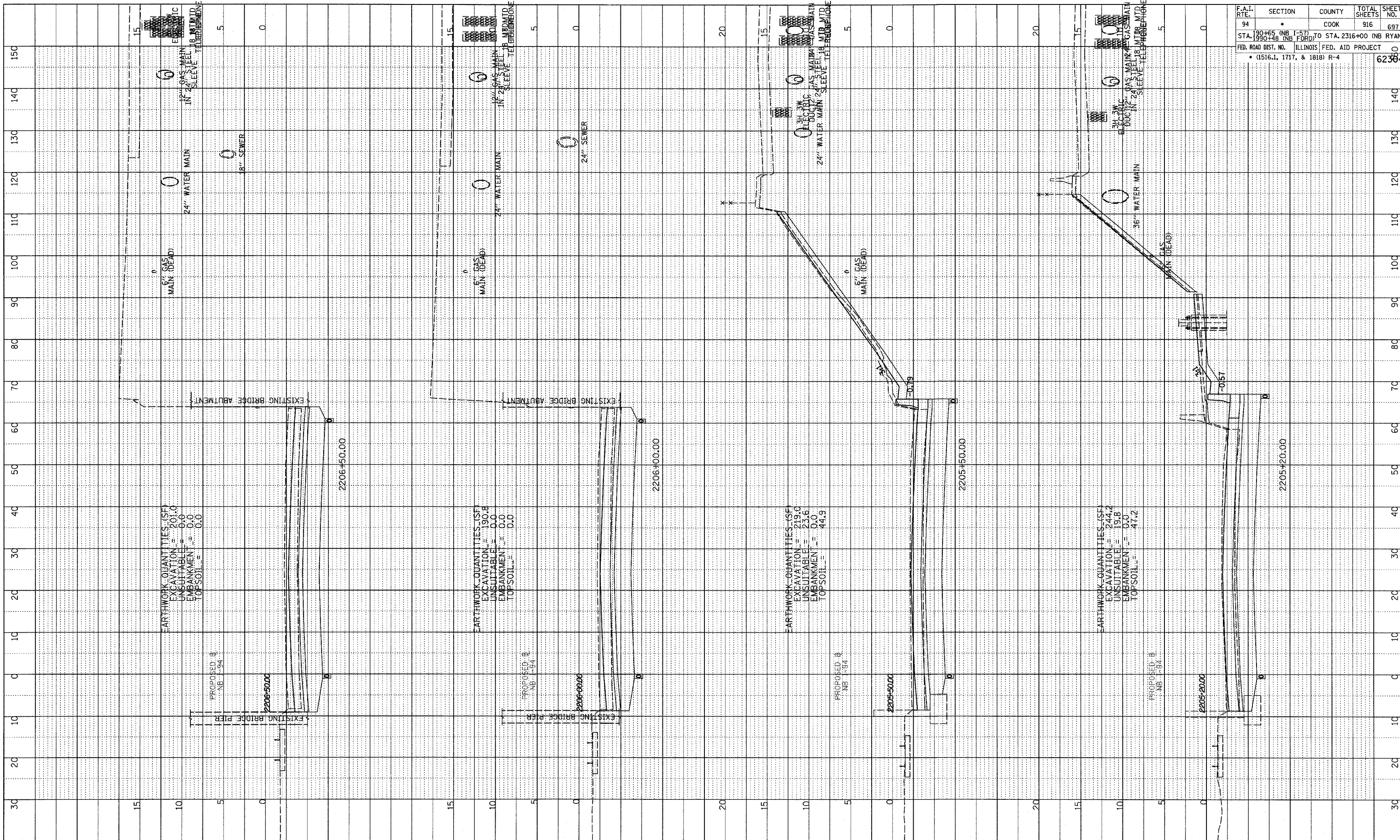
TYLIN INTERNATIONAL



NORTHBOUND I-57
 STA. 257+50 TO STA. 259+00

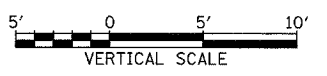
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PROFILE SURVEYED BY DATE
 PLOTTED BY
 GRADES CHECKED
 NO. OF SHEETS
 STRUCTURE NOTATION CHWD



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	190+65 (NB I-57) TO STA. 190+48 (NB FORD)	COOK	916	697
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				
				62384

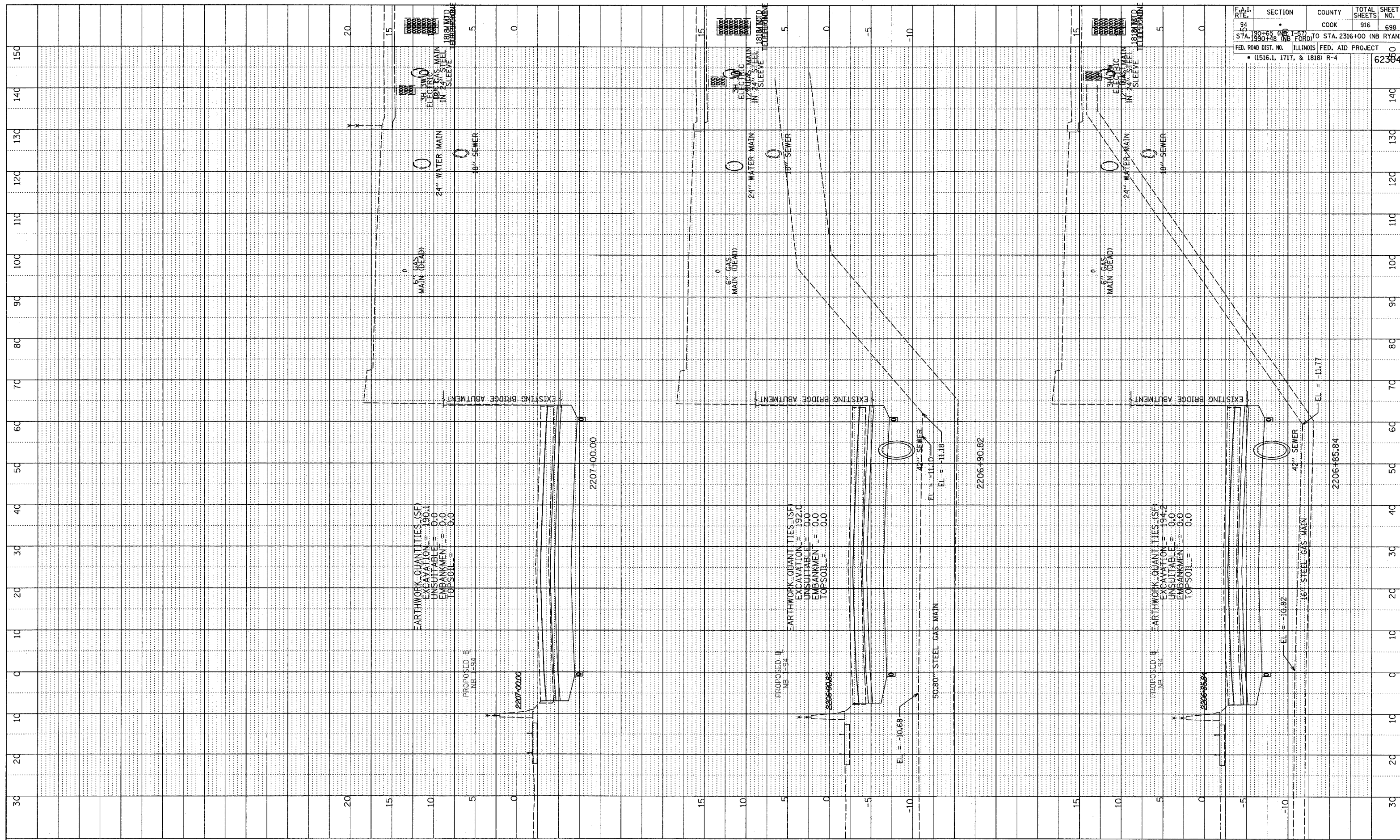
TYLIN INTERNATIONAL



NORTHBOUND I-94 (DAN RYAN EXPWY)
 STA. 2205+20 TO STA. 2206+50

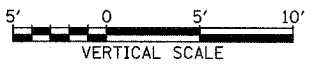
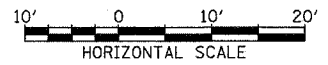
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PROFILE SURVEYED	BY	DATE
NOTE BOOK	GRADES CHECKED	
NO.	S.M. NOTED	
	PLUG DATE	NOTATIONS CHG.



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	157	COOK	916	698
STA. 1990+00	TO STA. 2316+00	FORD		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				

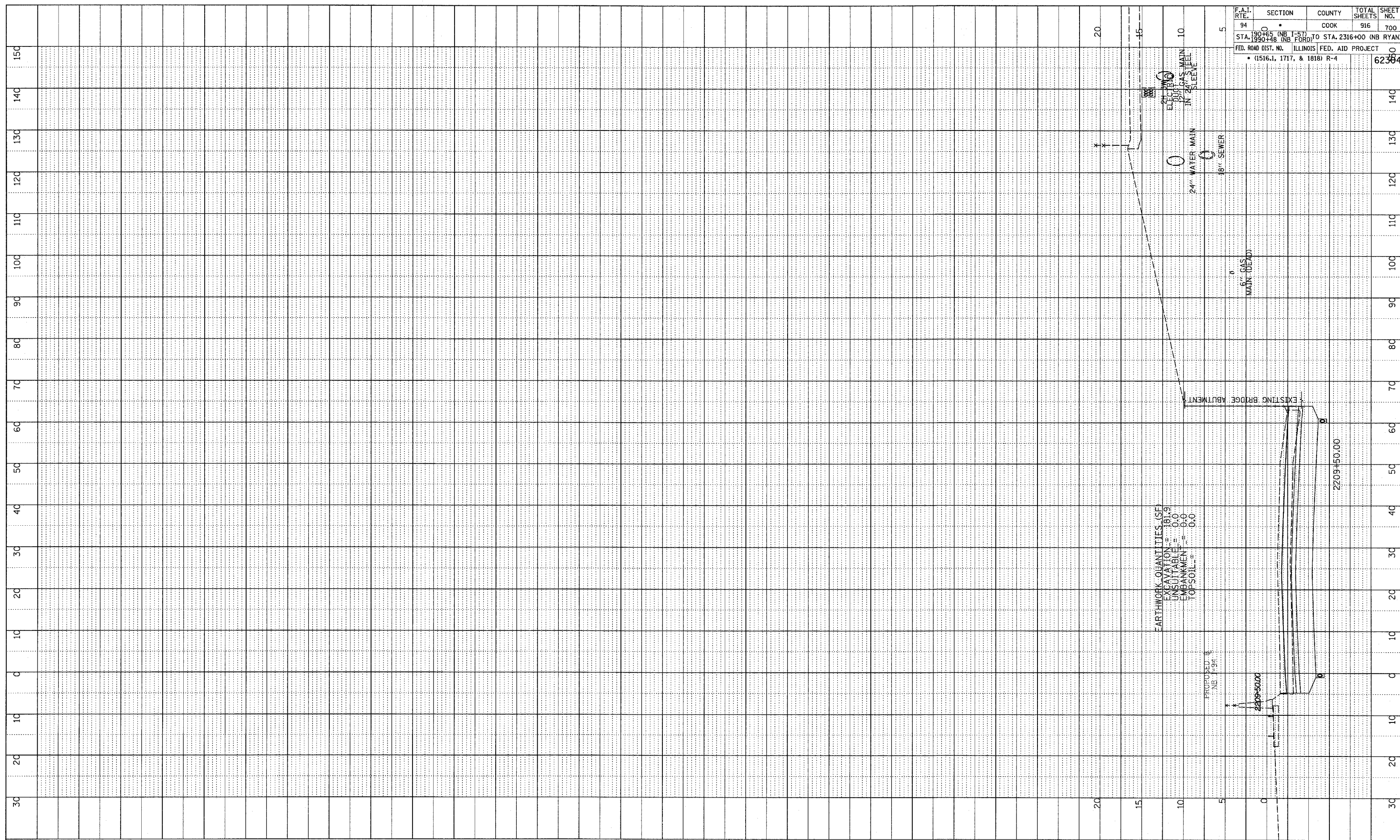
TYLIN INTERNATIONAL



NORTHBOUND I-94 (DAN RYAN EXPWY)
STA. 2206+85.84 TO STA. 2207+00

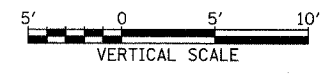
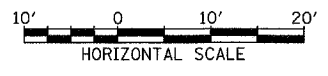
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DATE	BY
PROFILE SURVEYED	NOTE BOOK
GRADES CHECKED	DATE NOTED
NO.	PROJECT USE NOTATIONS CHRD



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	0	COOK	916	700
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				
				62384

TYLIN INTERNATIONAL



NORTHBOUND I-94 (DAN RYAN EXPWY)
STA. 2209+50 TO STA. 2209+50

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