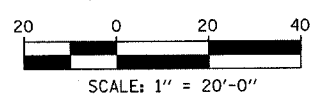
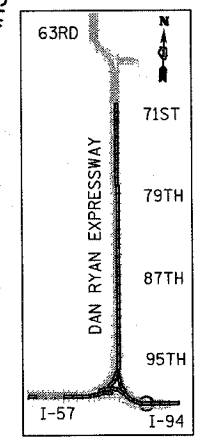
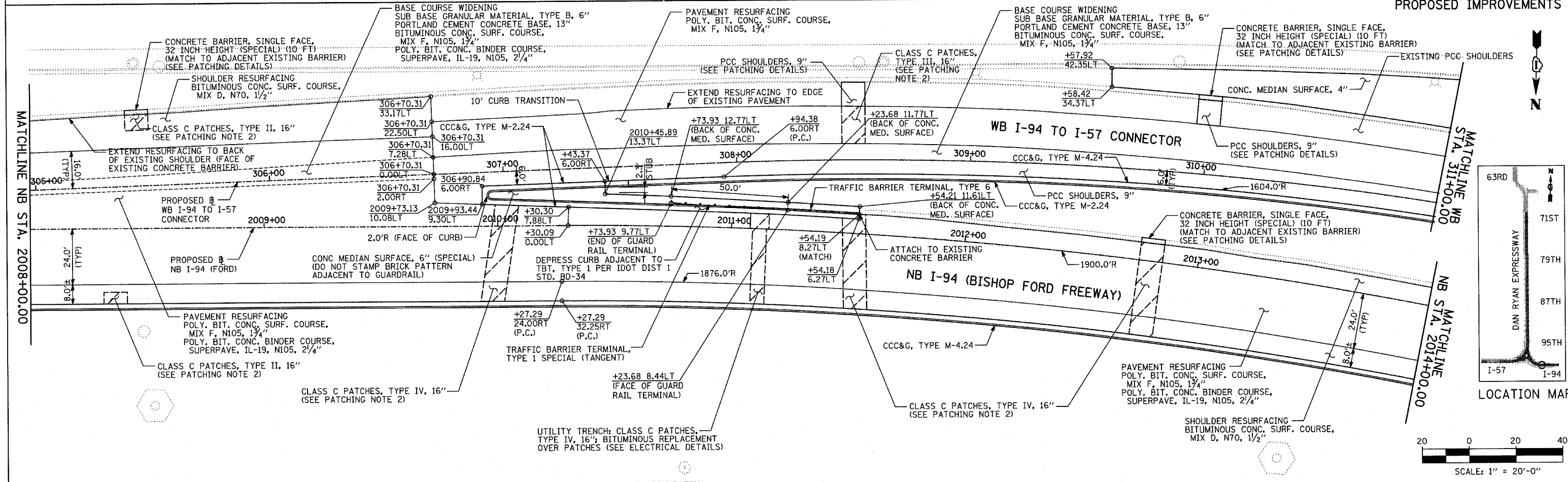
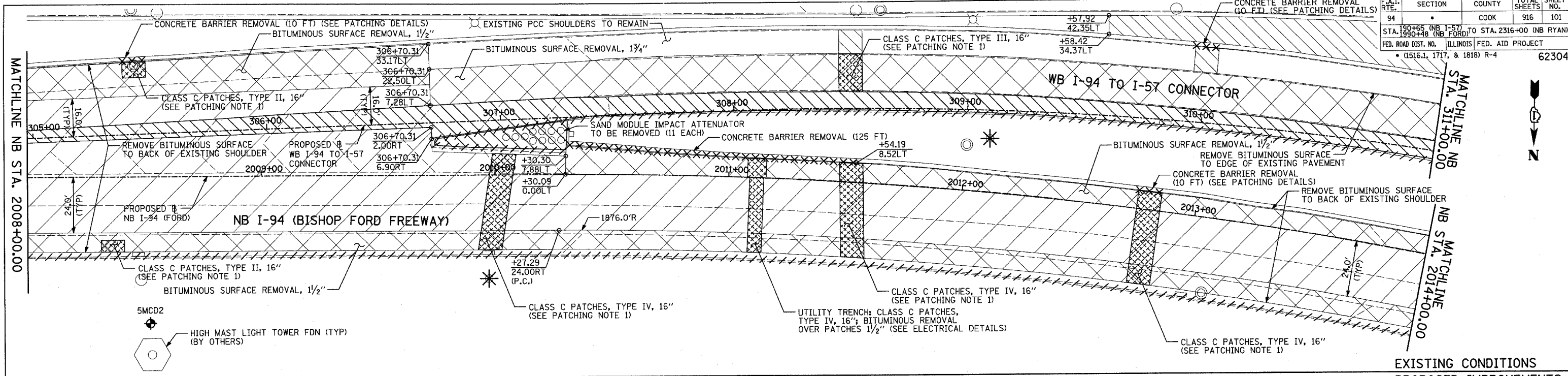


F.A.I. RTE. 94	SECTION	COUNTY COOK	TOTAL SHEETS 916	SHEET NO. 101
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				



LEGEND:

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS
- ▨ PAVEMENT REMOVAL
- ▧ PAVED SHOULDER REMOVAL
- ▩ PAVEMENT/SHOULDER REMOVAL, SPECIAL
- ▤ BITUMINOUS SURFACE REMOVAL, 4"
- ▥ BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- ⊙ LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
 - +XX.XX MAINLINE #
 - XX.XXRT STATION/OFFSET
- PLAN NOTES:**
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
 - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

PATCHING NOTES:

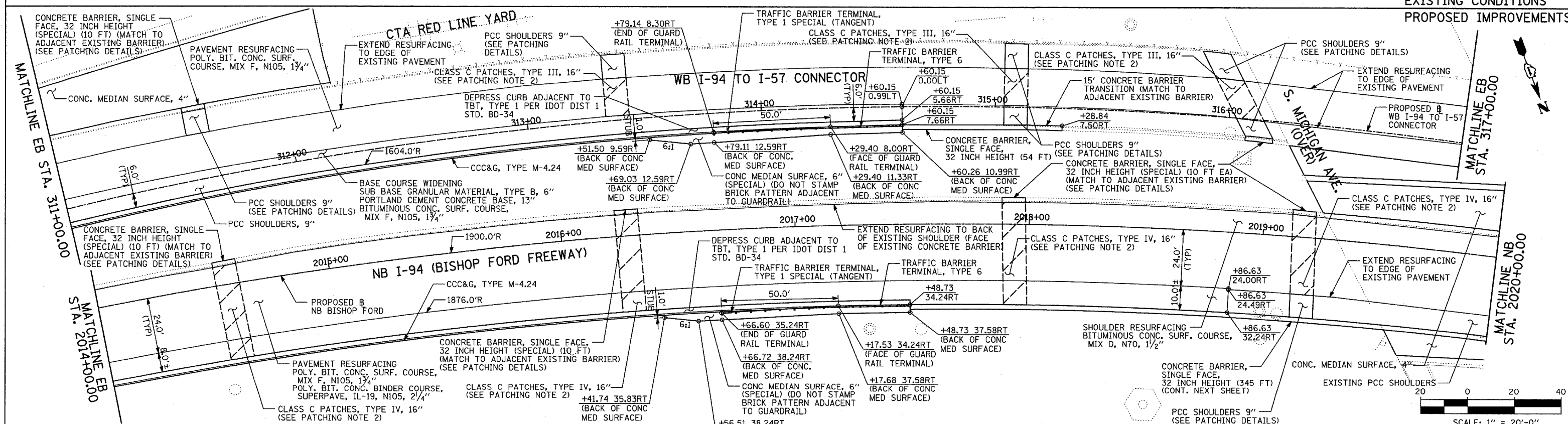
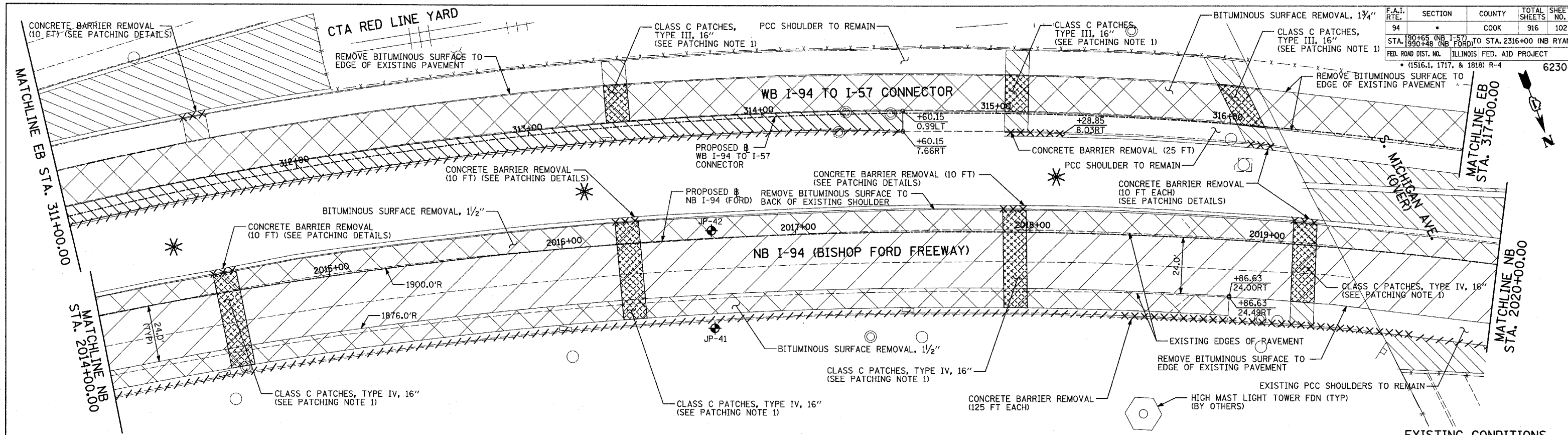
1. SEWER TRENCH: CLASS C PATCHES, TYPE VARIES, 16"; BITUMINOUS REMOVAL OVER PATCHES 1 1/2"; (SEE MISCELLANEOUS DETAILS)
2. SEWER TRENCH: CLASS C PATCHES, TYPE VARIES, 16"; BITUMINOUS REPLACEMENT OVER PATCHES, (SEE MISCELLANEOUS DETAILS)

REVISIONS	
NAME	DATE

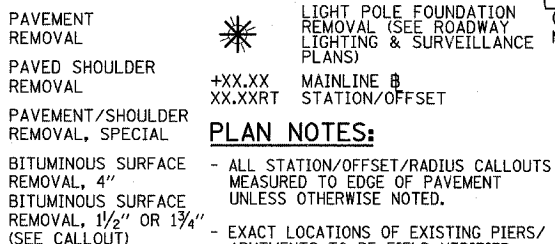
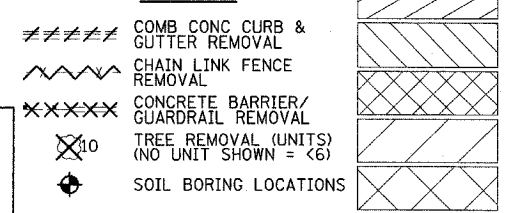
ILLINOIS DEPARTMENT OF TRANSPORTATION
**F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PLAN
 NB I-94 (BISHOP FORD FREEWAY)
 NB STA. 2008+00.00 TO 2014+00.00
 AND WB STA. 311+00.00**

SCALE: 1"=20'
 DATE: MARCH 7, 2006
 DRAWN BY: RTM
 CHECKED BY: MPG

TYLIN INTERNATIONAL



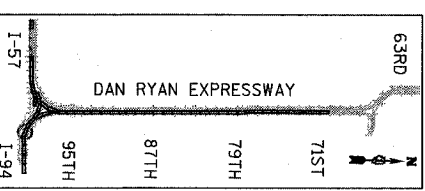
LEGEND:



PATCHING NOTES:

- SEWER TRENCH: CLASS C PATCHES, TYPE VARIES, 16"; BITUMINOUS REMOVAL OVER PATCHES 1 1/2" (SEE MISCELLANEOUS DETAILS)
- SEWER TRENCH: CLASS C PATCHES, TYPE VARIES, 16"; BITUMINOUS REPLACEMENT OVER PATCHES (SEE MISCELLANEOUS DETAILS)

LOCATION MAP



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

EXISTING AND PROPOSED PLAN

WB STA. 311+00.00 TO 317+00.00

NB I-94 (BISHOP FORD FREEWAY)

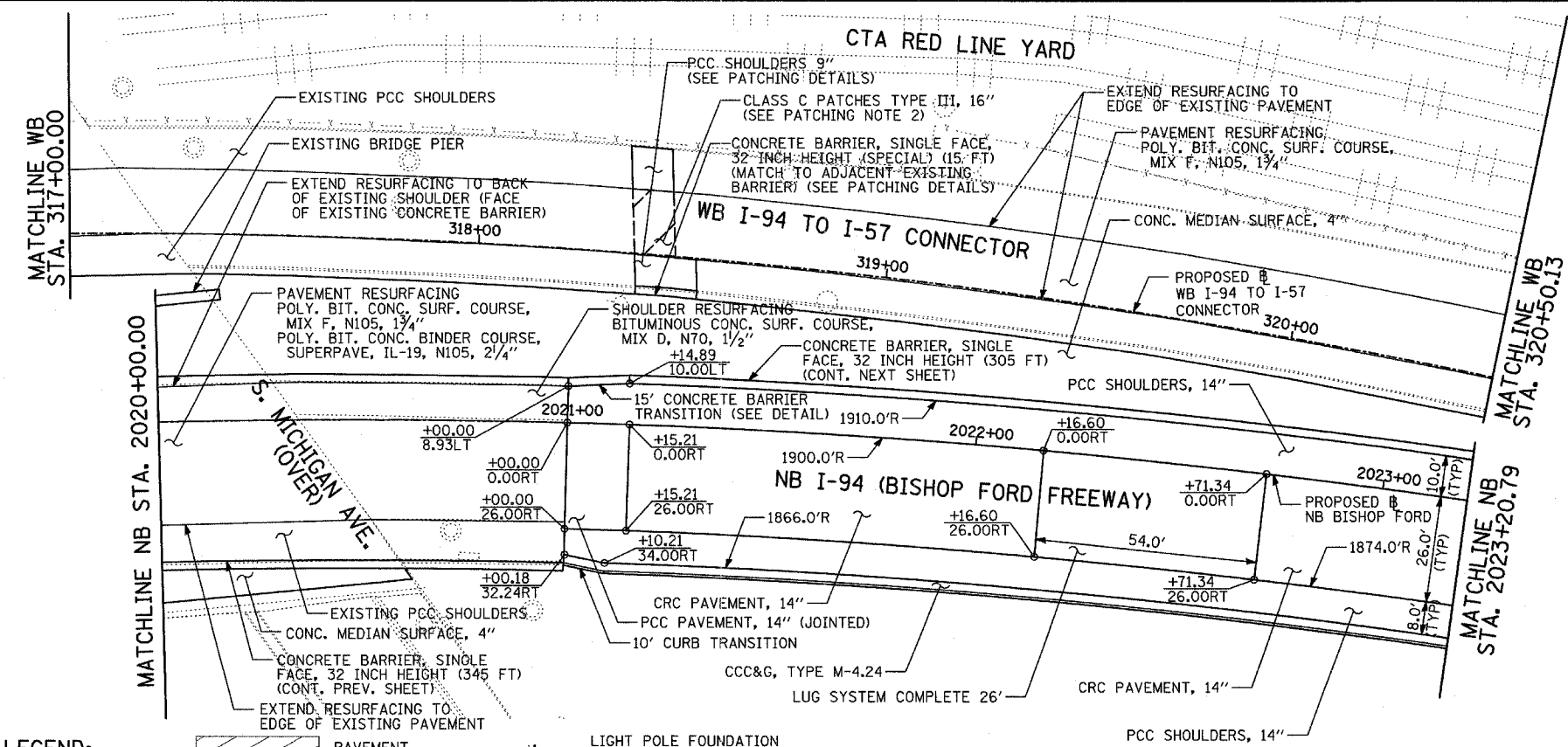
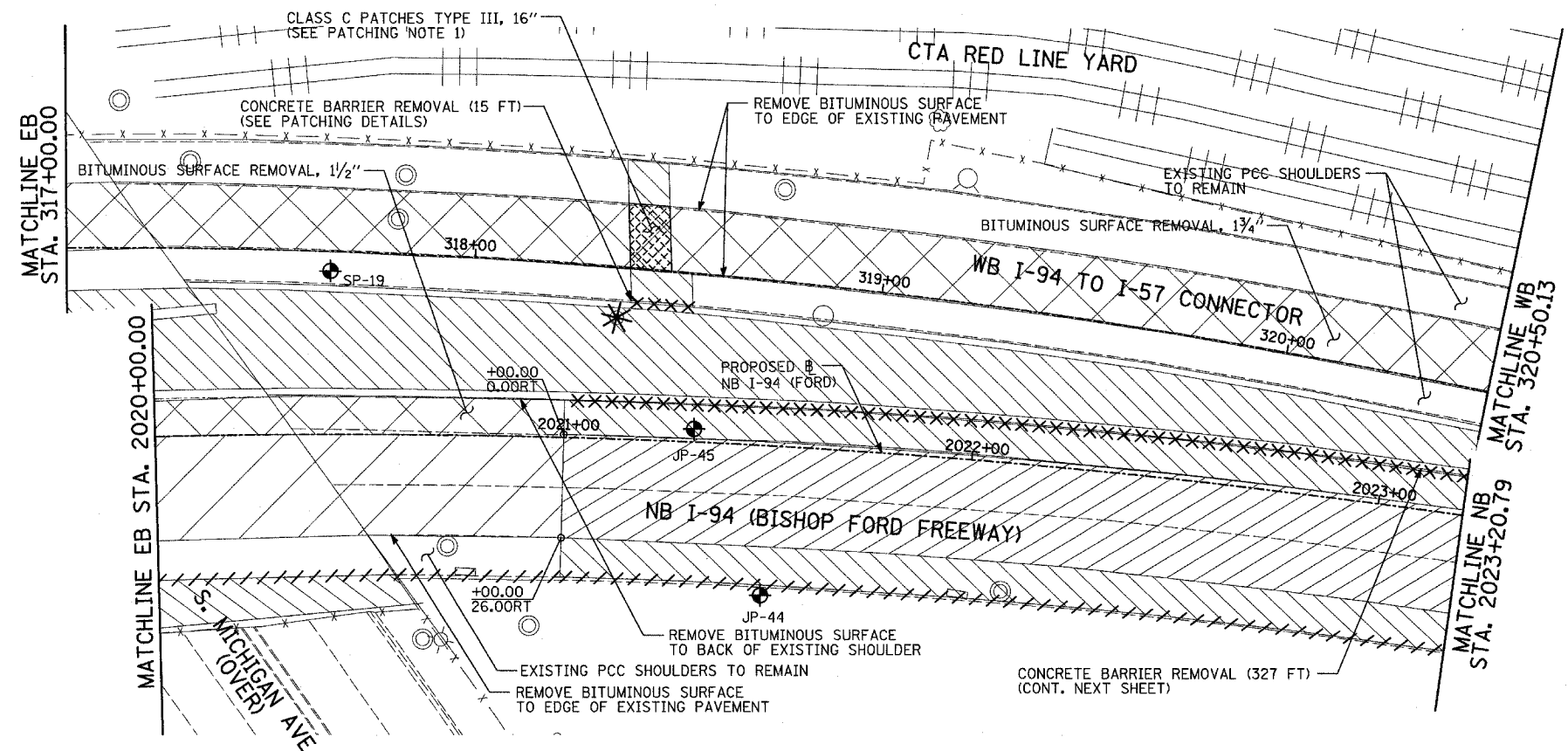
NB STA. 2014+00.00 TO 2020+00.00

SCALE: 1"=20'

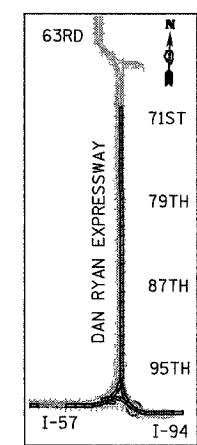
DATE: MARCH 7, 2006

DRAWN BY: RTM

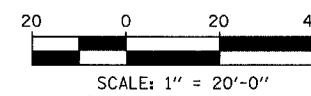
CHECKED BY: MPG



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP



LEGEND:

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = X6)
- ⊕ SOIL BORING LOCATIONS

- [Hatched Box] PAVEMENT REMOVAL
- [Hatched Box] PAVED SHOULDER REMOVAL
- [Hatched Box] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 4"
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE B
- XX.XXRT STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

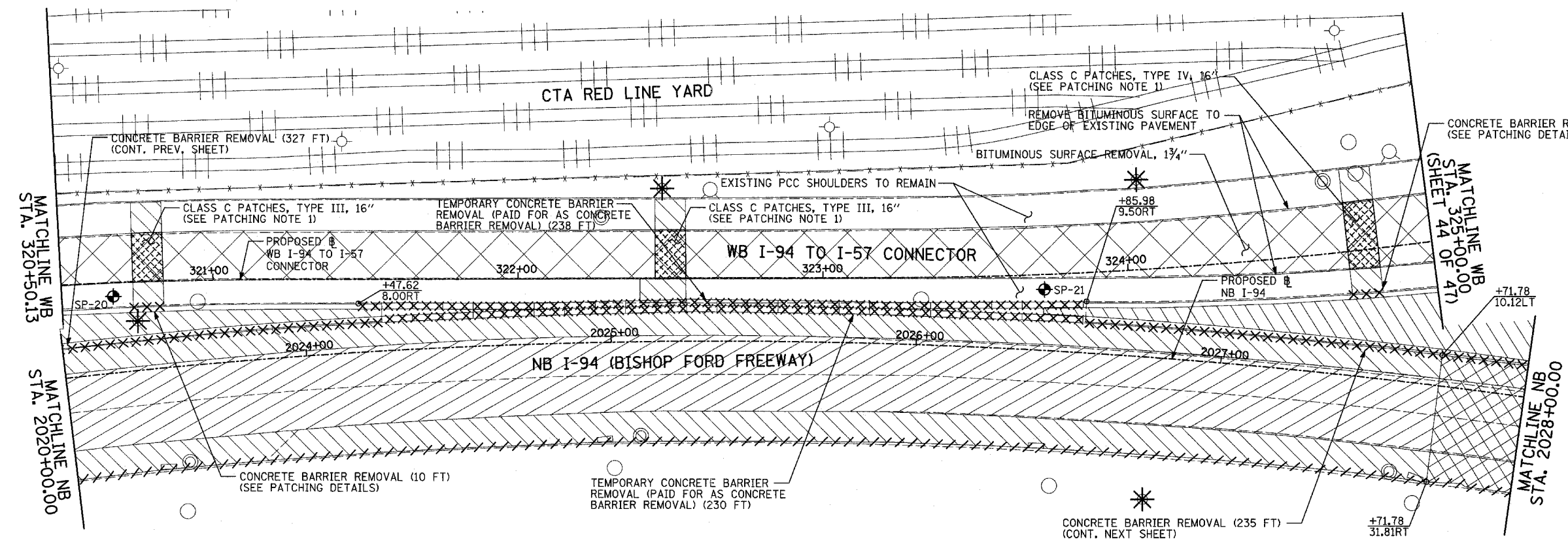
PATCHING NOTES:

1. SEWER TRENCH: CLASS C PATCHES, TYPE VARIES, 16"; BITUMINOUS REMOVAL OVER PATCHES 1 1/2", (SEE MISCELLANEOUS DETAILS)
2. SEWER TRENCH: CLASS C PATCHES, TYPE VARIES, 16"; BITUMINOUS REPLACEMENT OVER PATCHES, (SEE MISCELLANEOUS DETAILS)

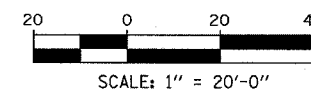
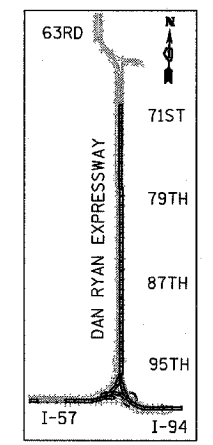
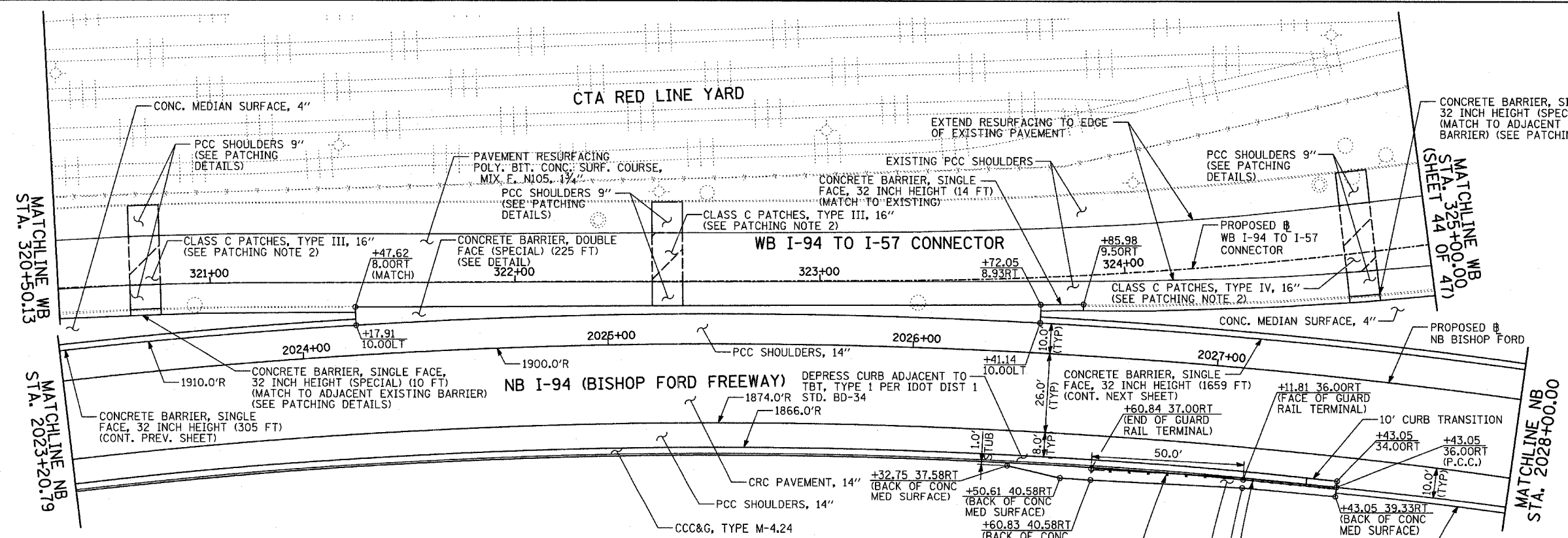
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
NB I-94 (BISHOP FORD FREEWAY)
WB STA. 317+00.00 TO 320+50.00
NB STA. 2020+70.00 TO 2023+20.79

SCALE: 1"=20'
DATE: MARCH 7, 2006
DRAWN BY: RTM
CHECKED BY: MPG



EXISTING CONDITIONS
 PROPOSED IMPROVEMENTS



LEGEND:

- ===== COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = 6)
- ⊕ SOIL BORING LOCATIONS
- ▨ PAVEMENT REMOVAL
- ▧ PAVED SHOULDER REMOVAL
- ▩ PAVEMENT/SHOULDER REMOVAL, SPECIAL
- ▦ BITUMINOUS SURFACE REMOVAL, 4"
- ▤ BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)
- ⊙ LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- ⊕xx.xx MAINLINE #
- ⊕xx.xxRT STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

PATCHING NOTES:

1. SEWER TRENCH: CLASS C PATCHES, TYPE VARIES, 16"; BITUMINOUS REMOVAL OVER PATCHES 1 1/2"; (SEE MISCELLANEOUS DETAILS)
2. SEWER TRENCH: CLASS C PATCHES, TYPE VARIES, 16"; BITUMINOUS REPLACEMENT OVER PATCHES, (SEE MISCELLANEOUS DETAILS)

TYLIN INTERNATIONAL

SHEET 41 OF 47

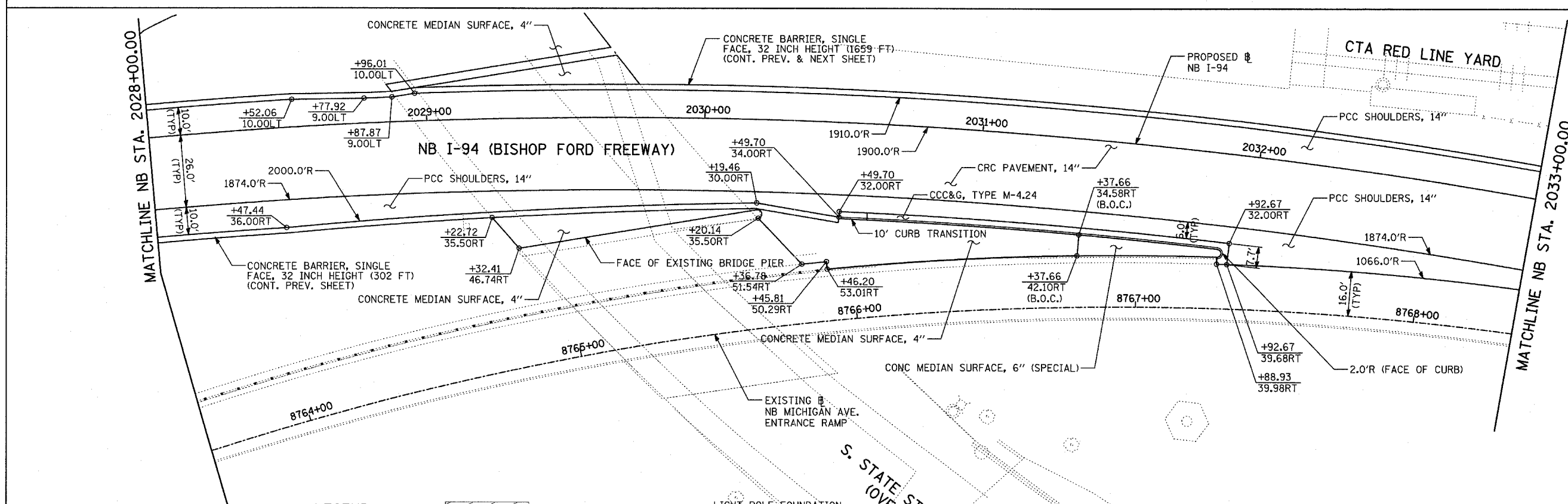
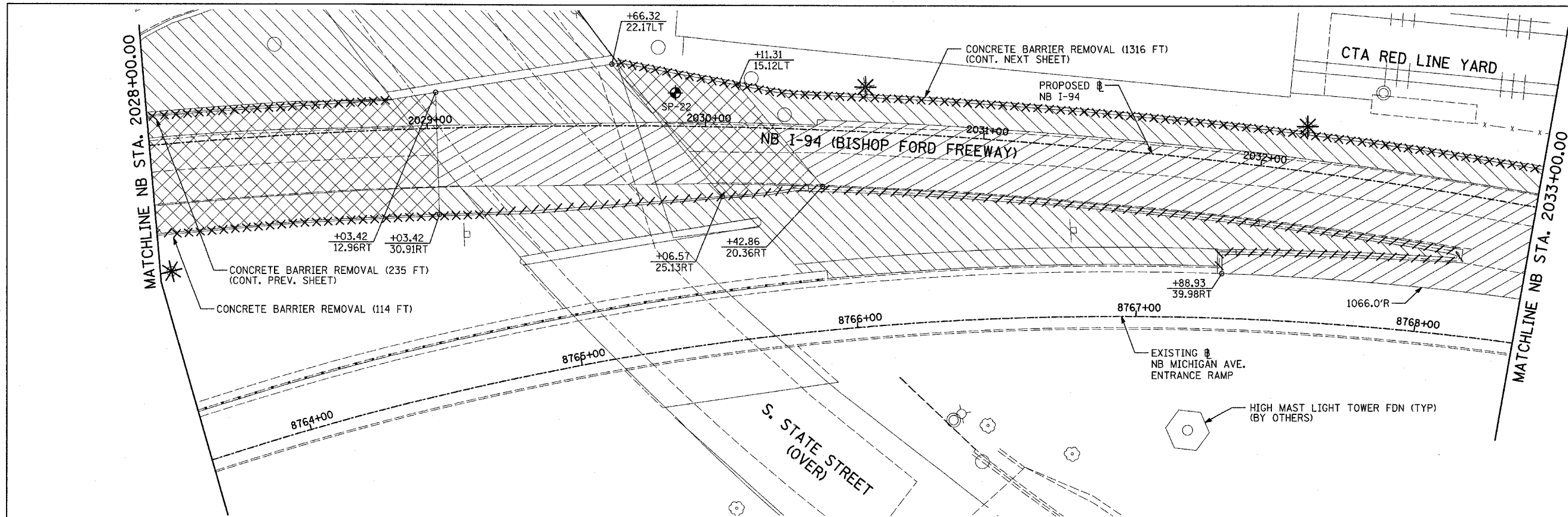
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PLAN
 NB I-94 (BISHOP FORD FREEWAY)
 WB STA. 320+50.00 TO 325+00.00
 NB STA. 2023+20.79 TO 2028+00.00

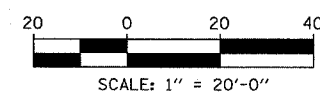
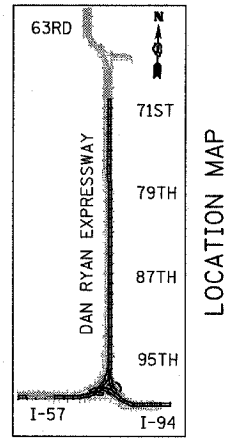
SCALE: 1"=20'
 DATE: MARCH 7, 2006

DRAWN BY: RTM
 CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	105
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN EXP. 63RD) TO STA. 2316+00 (NB RYAN EXP. 71ST) TO STA. 2316+00 (NB RYAN EXP. 79TH) TO STA. 2316+00 (NB RYAN EXP. 87TH) TO STA. 2316+00 (NB RYAN EXP. 95TH)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 62304				



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LEGEND:

	COMB CONC CURB & GUTTER REMOVAL		PAVED SHOULDER REMOVAL
	CHAIN LINK FENCE REMOVAL		PAVEMENT/SHOULDER REMOVAL, SPECIAL
	CONCRETE BARRIER/GUARDRAIL REMOVAL		BITUMINOUS SURFACE REMOVAL, 4"
	TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)		BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)
	SOIL BORING LOCATIONS		

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

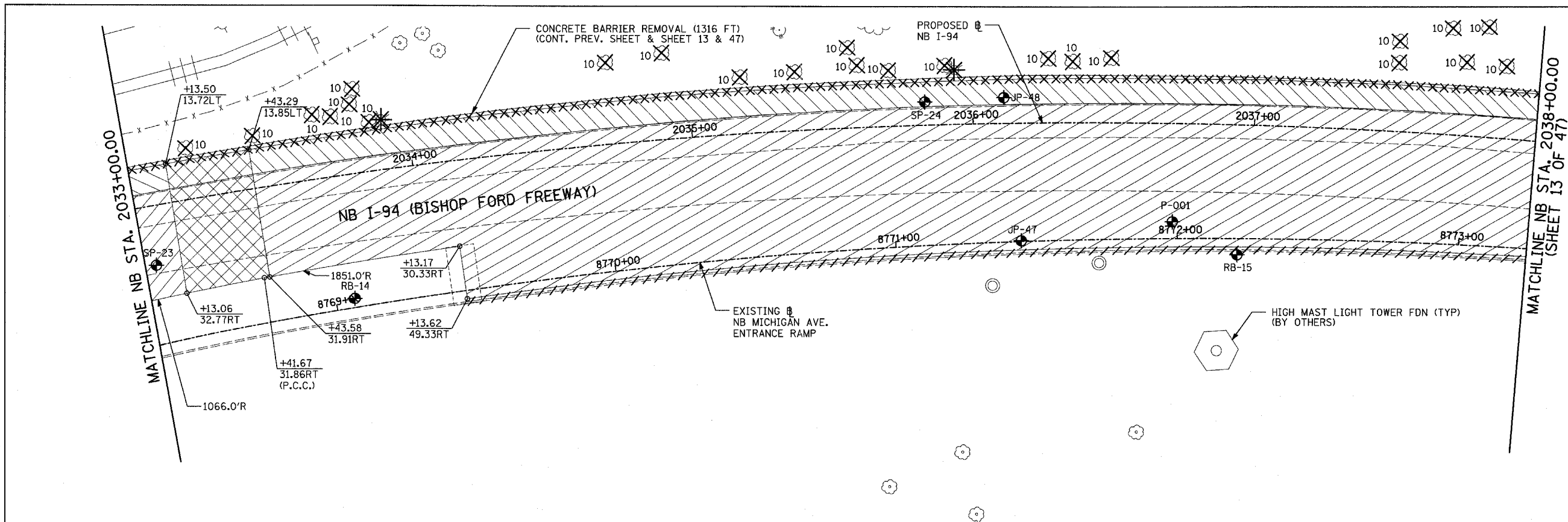
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
NB I-94 (BISHOP FORD FREEWAY)
NB STA. 2028+00.00 TO 2033+00.00

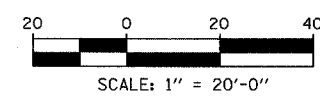
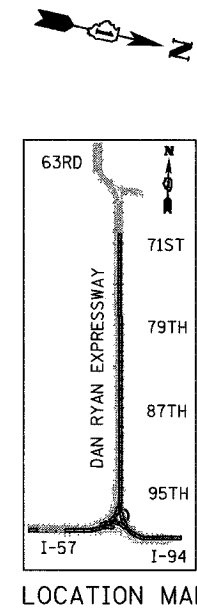
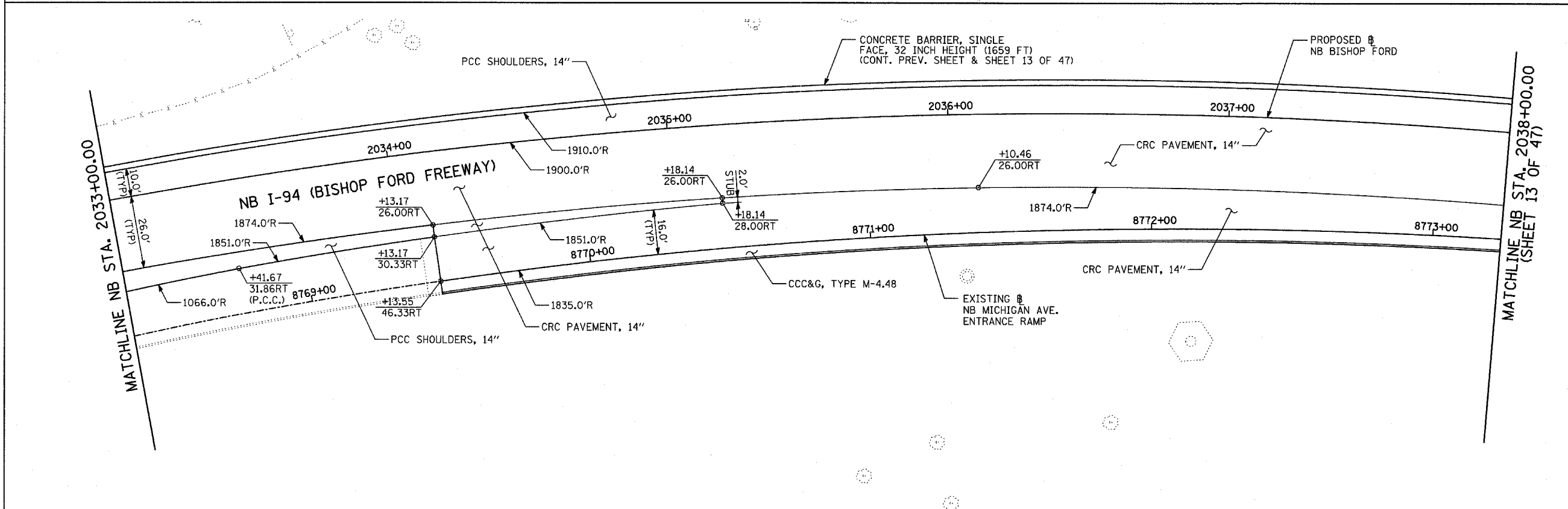
SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: RTM
CHECKED BY: MPG

TYLIN INTERNATIONAL



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LEGEND:

- ===== COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = 6)
- ⊕ SOIL BORING LOCATIONS

- [Hatched Box] PAVEMENT REMOVAL
- [Hatched Box] PAVED SHOULDER REMOVAL
- [Hatched Box] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 4"
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- ⊛ LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE Ⓡ
- XX.XXRT STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

TYLIN INTERNATIONAL

SHEET 43 OF 47

REVISIONS	
NAME	DATE

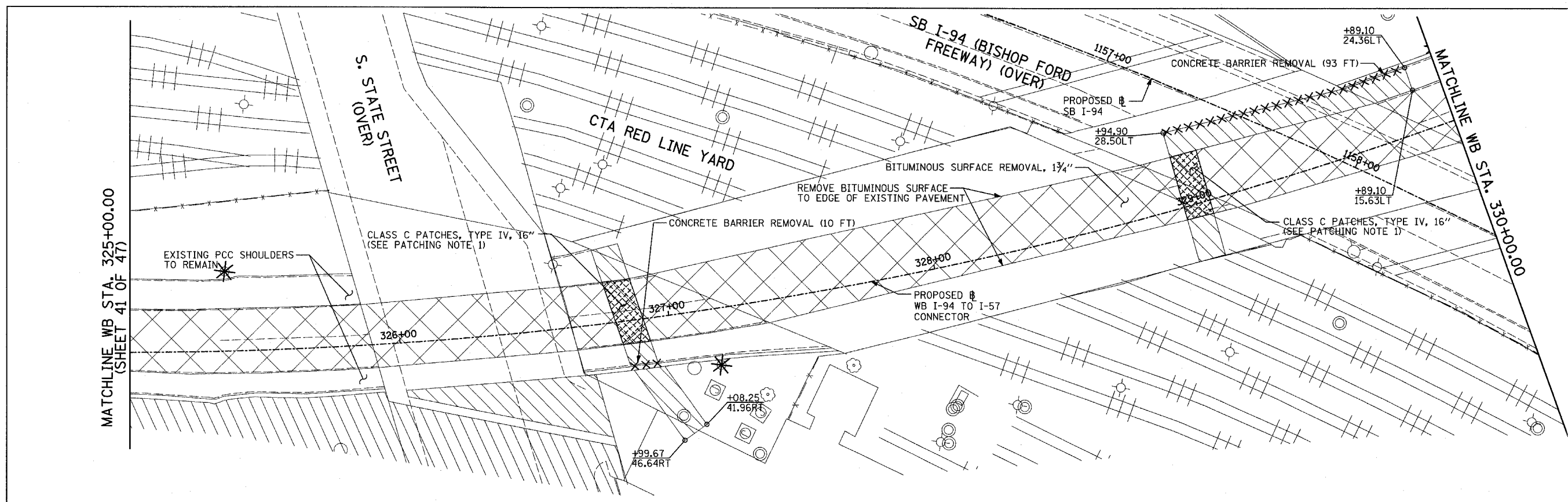
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
NB I-94 (BISHOP FORD FREEWAY)
NB STA. 2033+00.00 TO 2038+00.00

SCALE: 1"=20'
DATE: MARCH 7, 2006

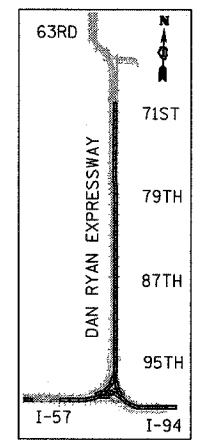
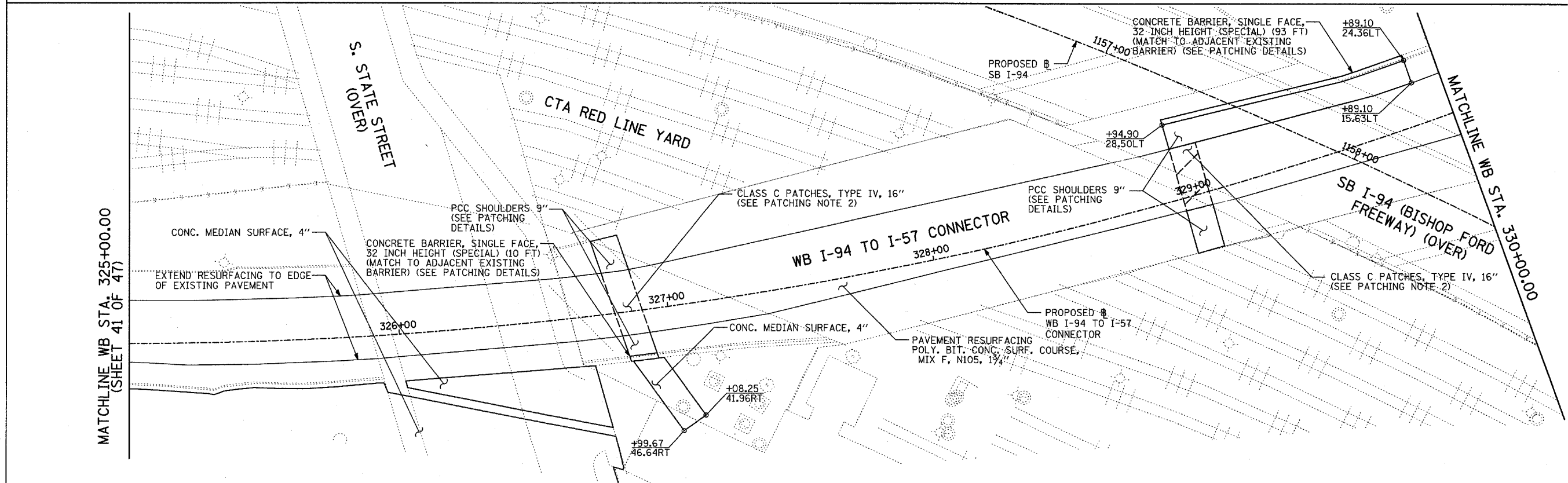
DRAWN BY: RTM
CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	916	107
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4				

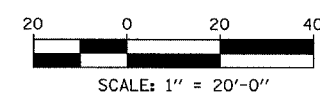
62304



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP



LEGEND:

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- X10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ◆ SOIL BORING LOCATIONS

- [Hatched Box] PAVEMENT REMOVAL
- [Hatched Box] PAVED SHOULDER REMOVAL
- [Hatched Box] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 4"
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

PATCHING NOTES:

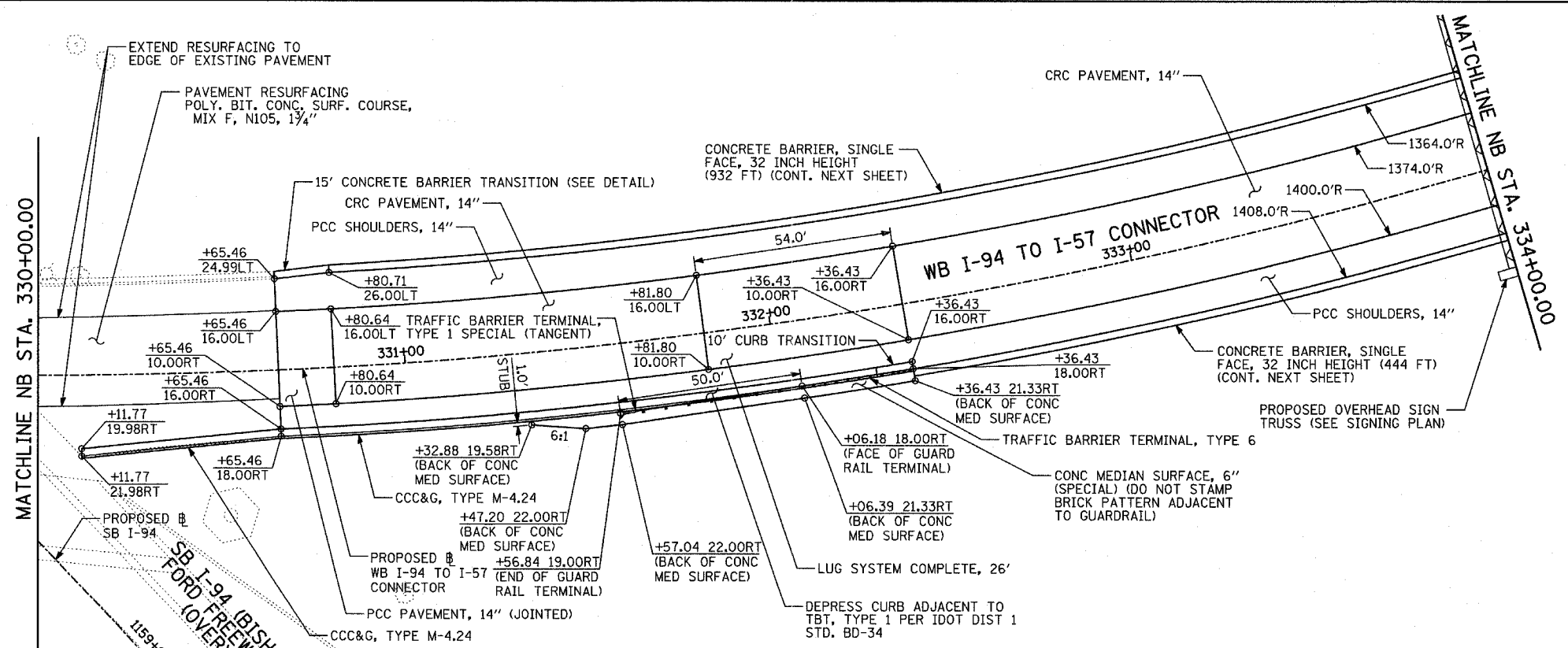
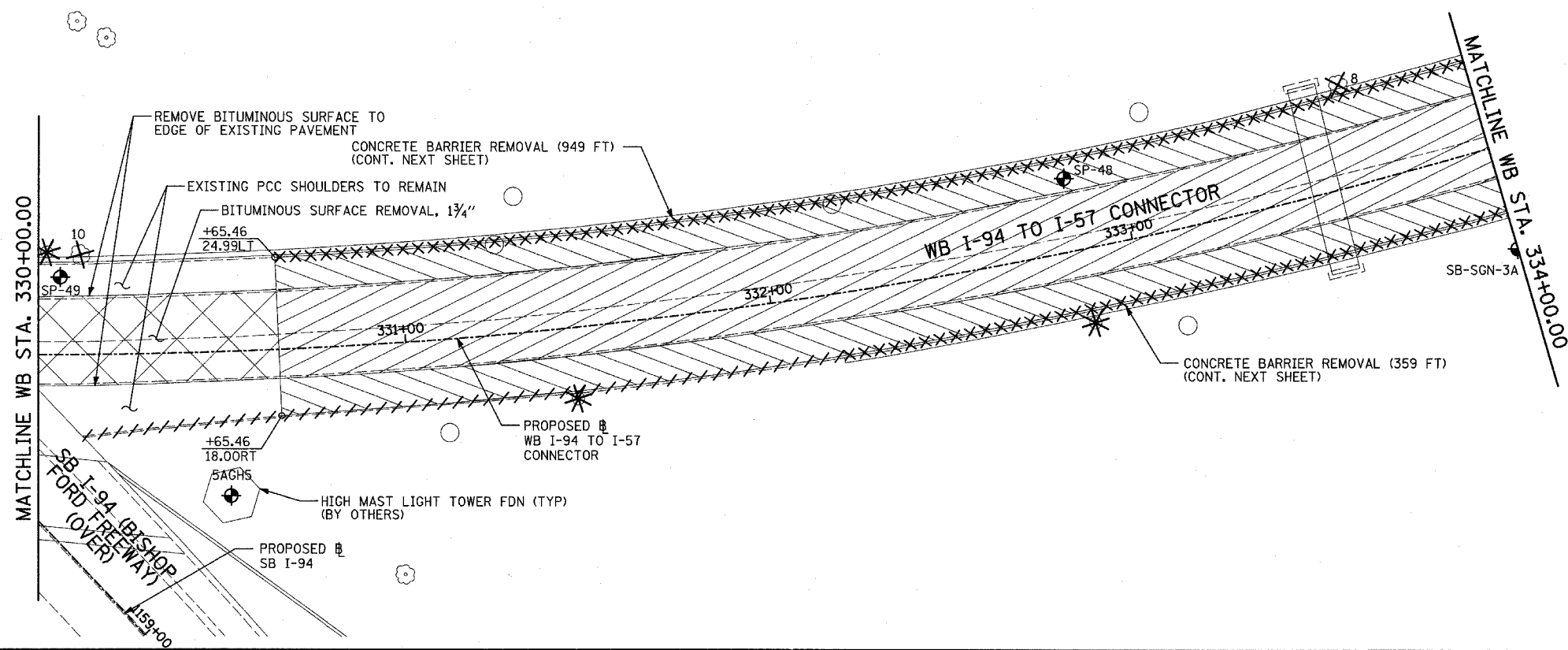
1. SEWER TRENCH: CLASS C PATCHES, TYPE VARIES, 16"; BITUMINOUS REMOVAL OVER PATCHES 1 1/2"; (SEE MISCELLANEOUS DETAILS)
2. SEWER TRENCH: CLASS C PATCHES, TYPE VARIES, 16"; BITUMINOUS REPLACEMENT OVER PATCHES, (SEE MISCELLANEOUS DETAILS)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
WB I-94 TO I-57 CONNECTOR
WB STA. 326+00.00 TO 330+00.00

SCALE: 1"=20'
DATE: MARCH 7, 2006
DRAWN BY: RTM
CHECKED BY: MPG

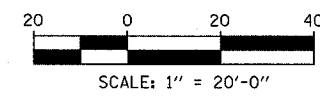
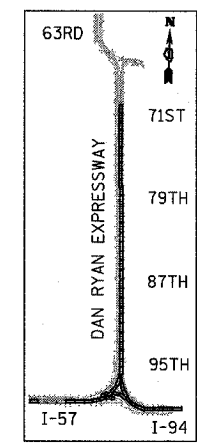
TYLIN INTERNATIONAL



LEGEND:

////	COMB CONC CURB & GUTTER REMOVAL	⊛	LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
~~~~	CHAIN LINK FENCE REMOVAL	+XX.XX	MAINLINE #
XXXXXX	CONCRETE BARRIER/GUARDRAIL REMOVAL	XX.XXRT	STATION/OFFSET
⊗	TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)		<b>PLAN NOTES:</b>
⊕	SOIL BORING LOCATIONS		- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
			- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

EXISTING CONDITIONS  
 PROPOSED IMPROVEMENTS



TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING AND PROPOSED PLAN  
 WB I-94 TO I-57 CONNECTOR  
 WB STA. 330+00.00 TO 334+00.00

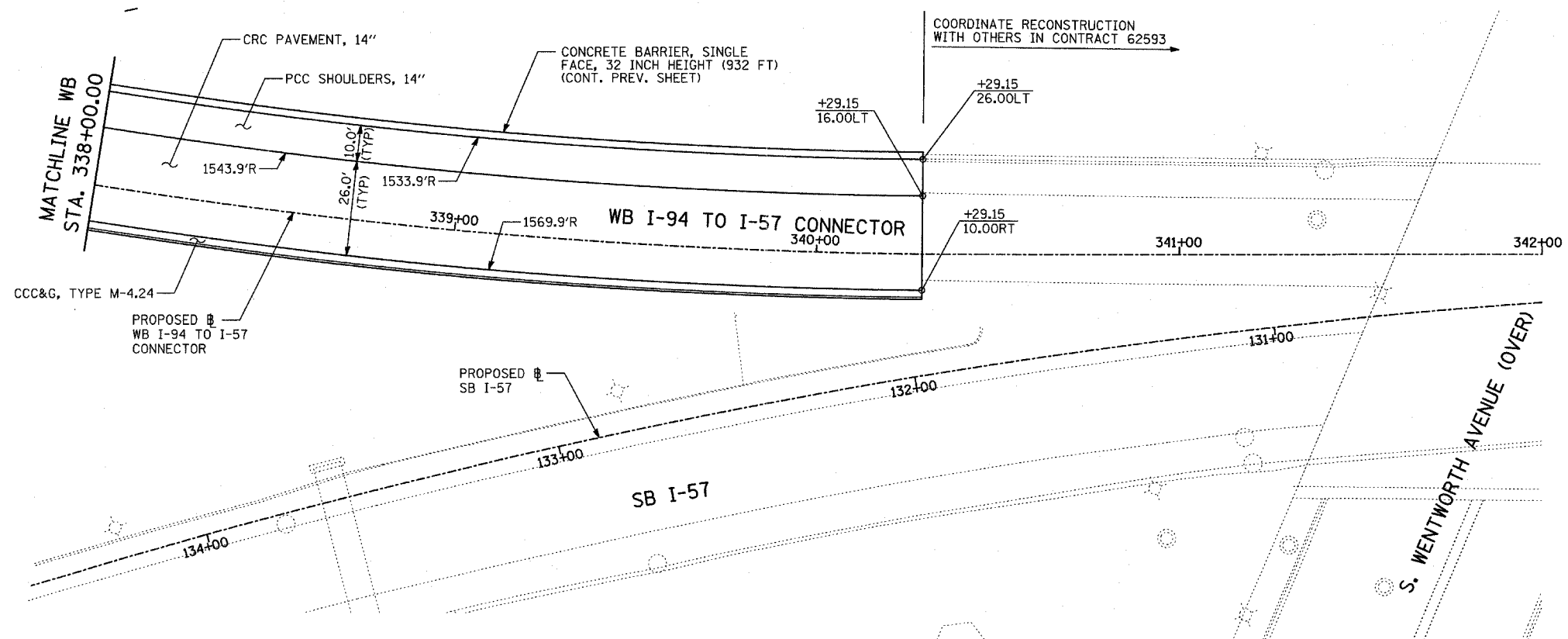
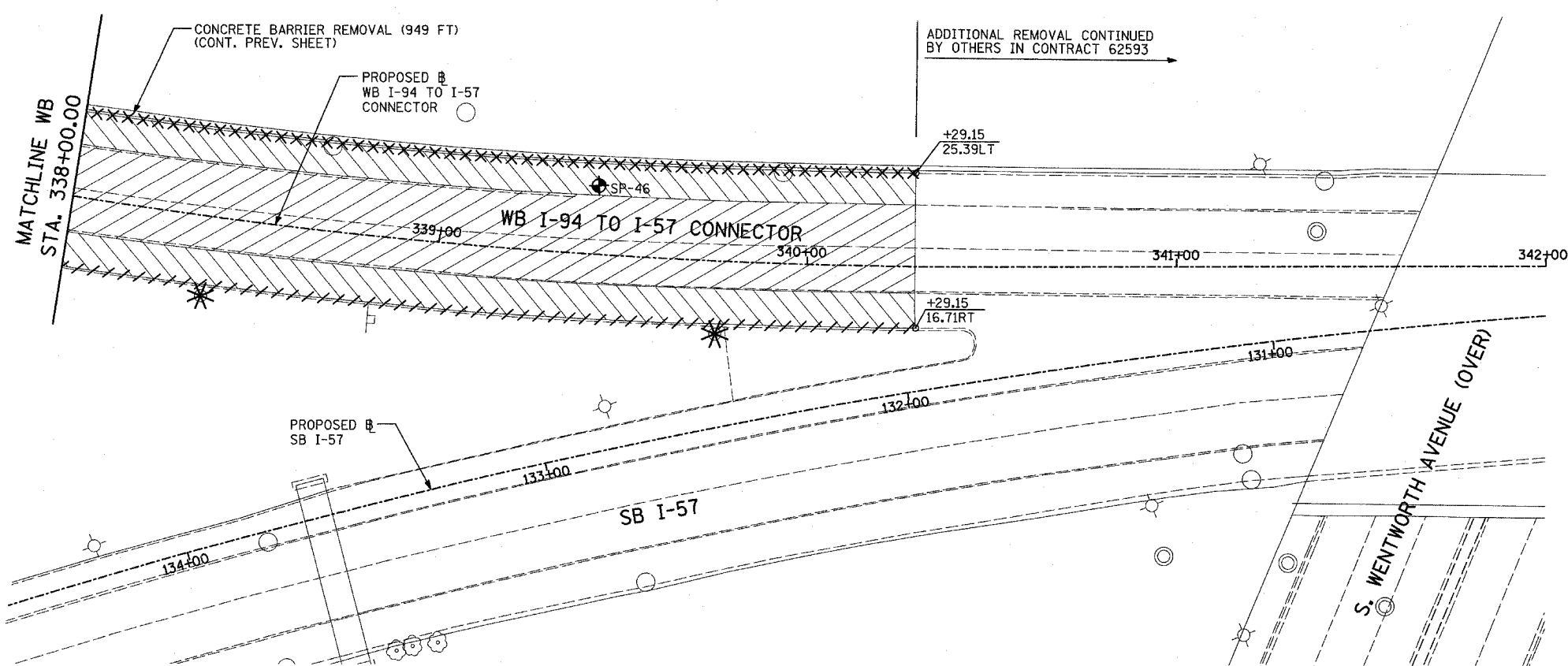
SCALE: 1"=20'  
 DATE: MARCH 7, 2006

DRAWN BY: RTM  
 CHECKED BY: MPG

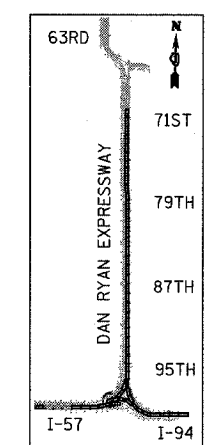




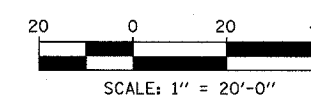
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	110
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN EXP)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 6230				



EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS



LOCATION MAP



**LEGEND:**

####	COMB CONC CURB & GUTTER REMOVAL		PAVEMENT REMOVAL		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEIL PLANS)
~~~~~	CHAIN LINK FENCE REMOVAL		PAVED SHOULDER REMOVAL	+XX.XX	MAINLINE #
XXXXXX	CONCRETE BARRIER/GUARDRAIL REMOVAL		PAVEMENT/SHOULDER REMOVAL, SPECIAL	XX.XXRT	STATION/OFFSET
	TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)		BITUMINOUS SURFACE REMOVAL, 4"	PLAN NOTES:	
	SOIL BORING LOCATIONS		BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)	- ALL STATION/OFFSET/RADIUS C MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.	
				- EXACT LOCATIONS OF EXISTING ABUTMENTS TO BE FIELD VERIFIED	

TYLIN INTERNATIONAL

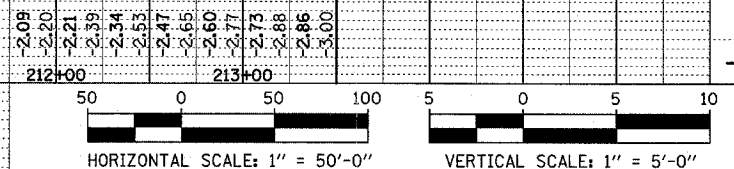
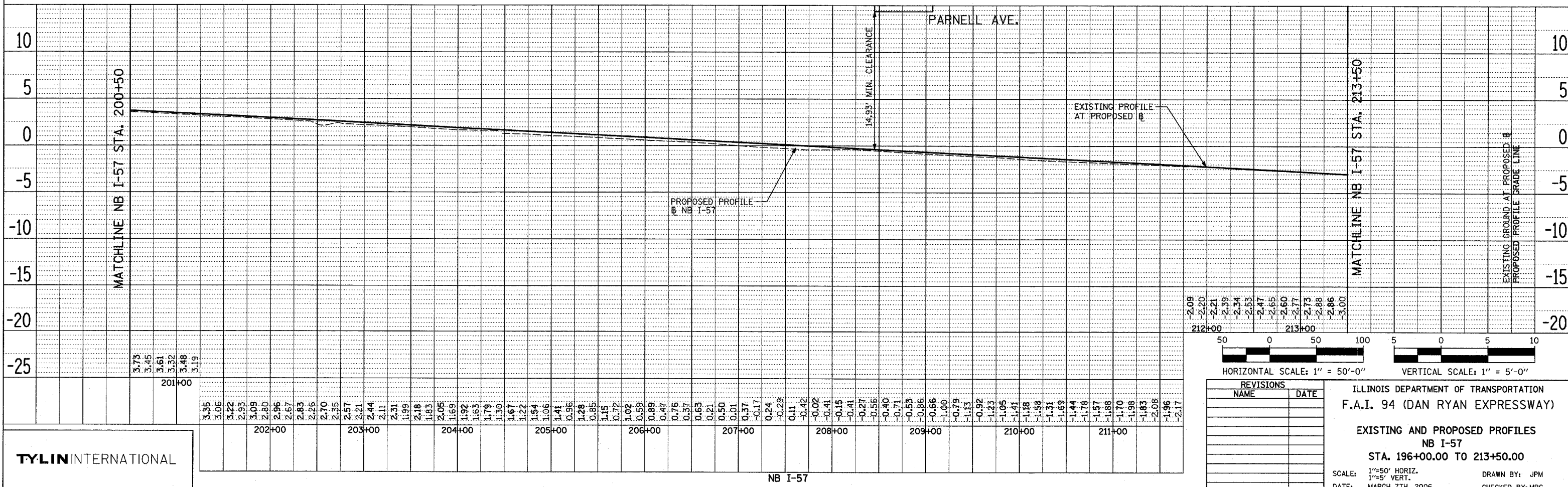
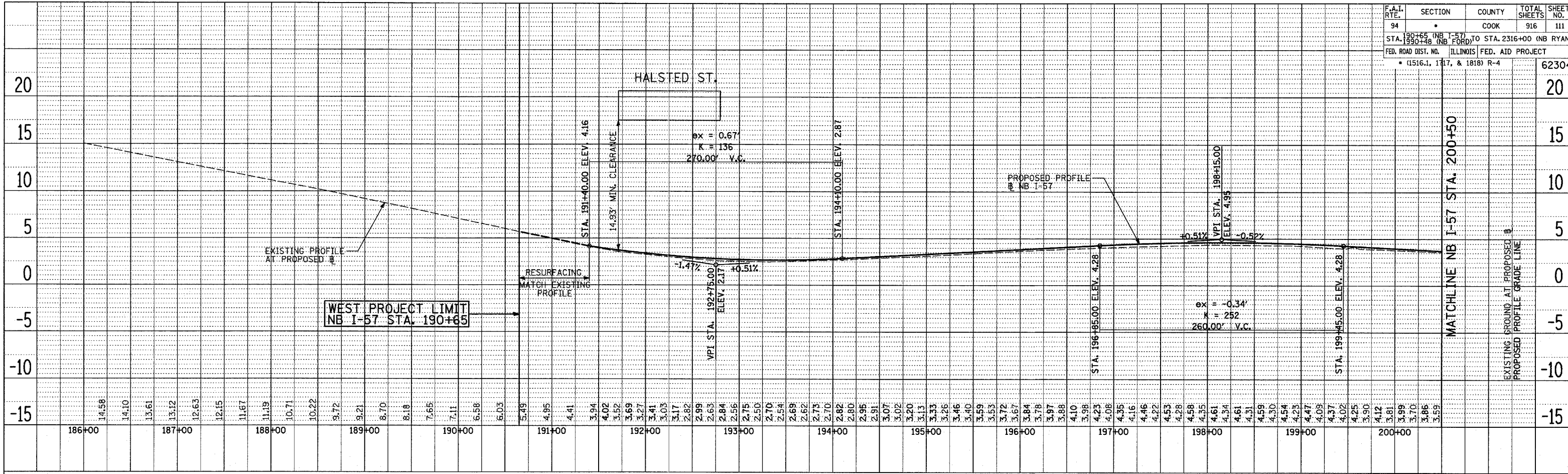
SHEET 47 OF 47

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
WB I-94 TO I-57 CONNECTOR
WB STA. 338+00.00 TO 340+29.15

SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: RTM
CHECKED BY: MPG



REVISIONS	
NAME	DATE

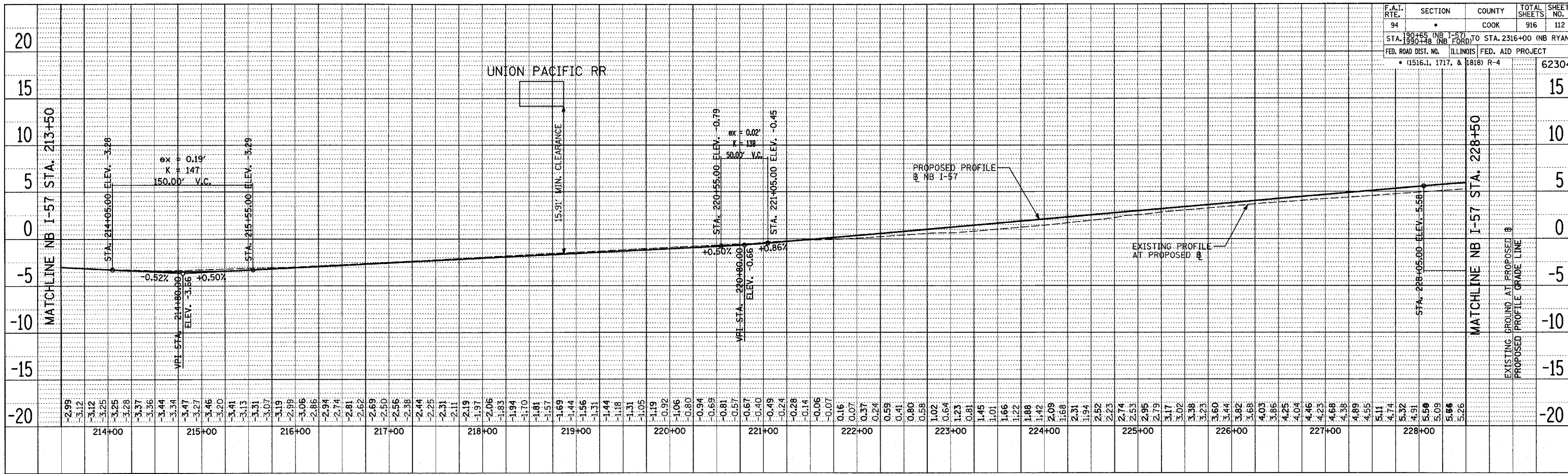
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PROFILES
 NB I-57
 STA. 196+00.00 TO 213+50.00
 SCALE: 1"=50' HORIZ.
 1"=5' VERT.
 DATE: MARCH 7TH, 2006
 DRAWN BY: JPM
 CHECKED BY: MPG

TYLIN INTERNATIONAL

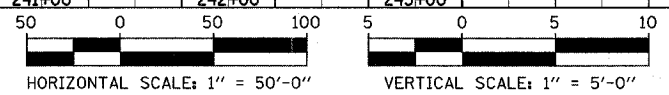
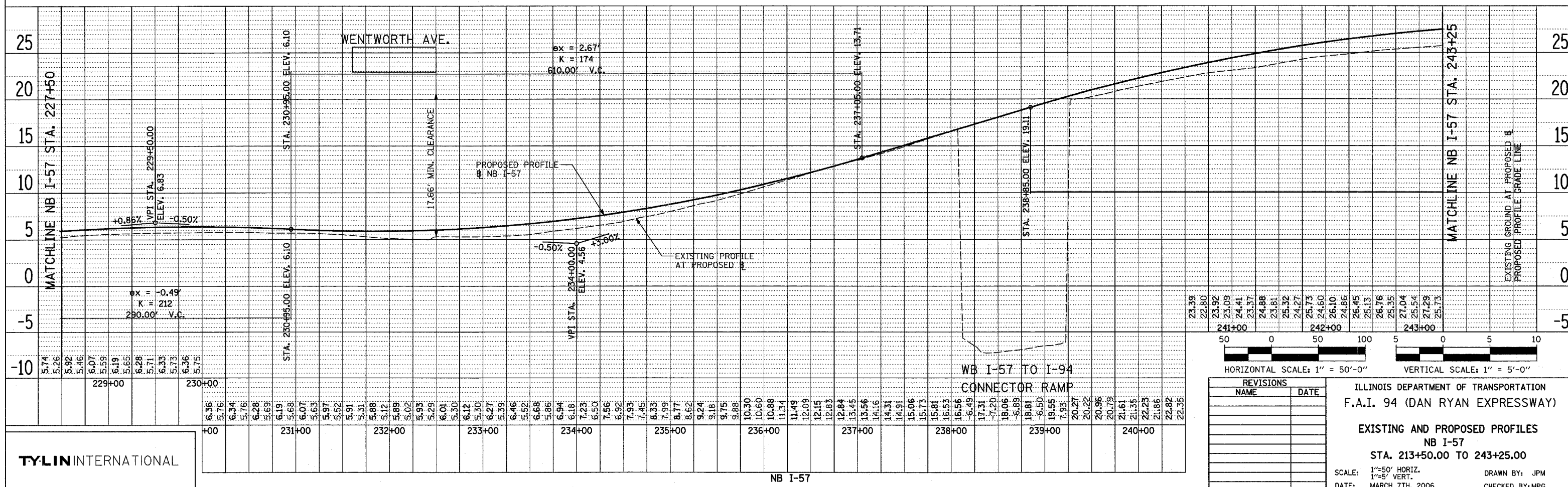
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 NOTE BOOK ALIGNED CHECKED
 NO. CAD FILE NAME

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 NOTE BOOK ALIGNED CHECKED
 NO. CAD FILE NAME

PLAN	REVISIONS	BY	DATE
NO.	NO.		



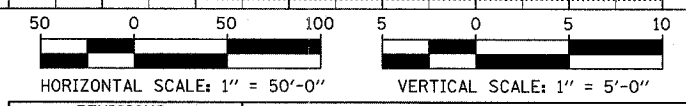
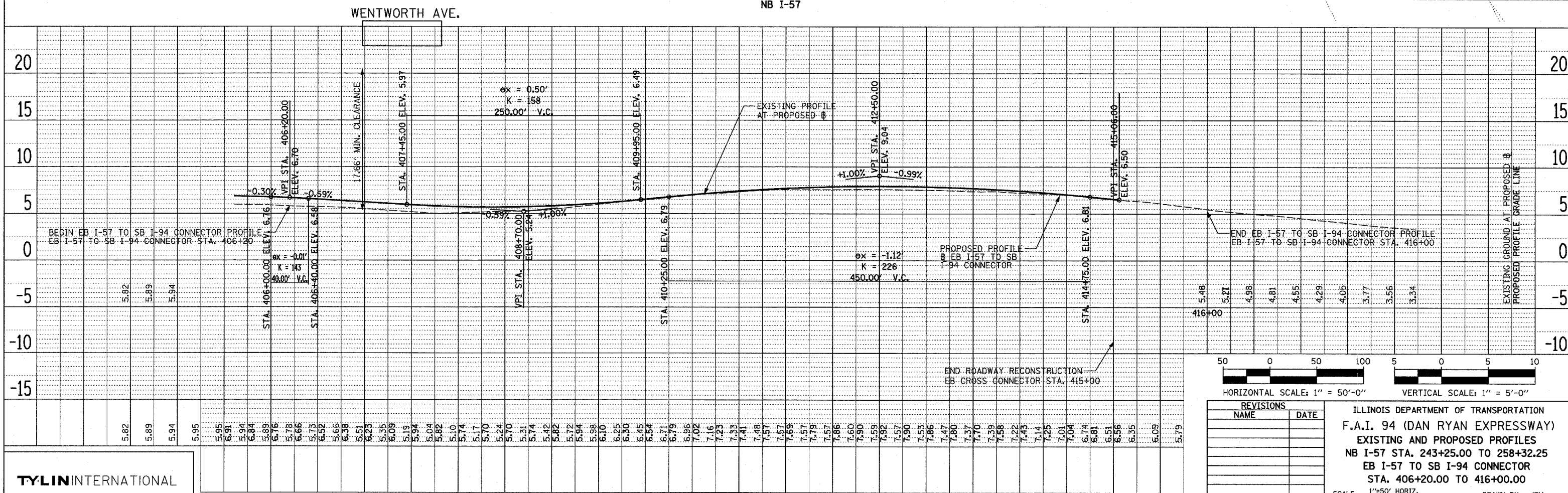
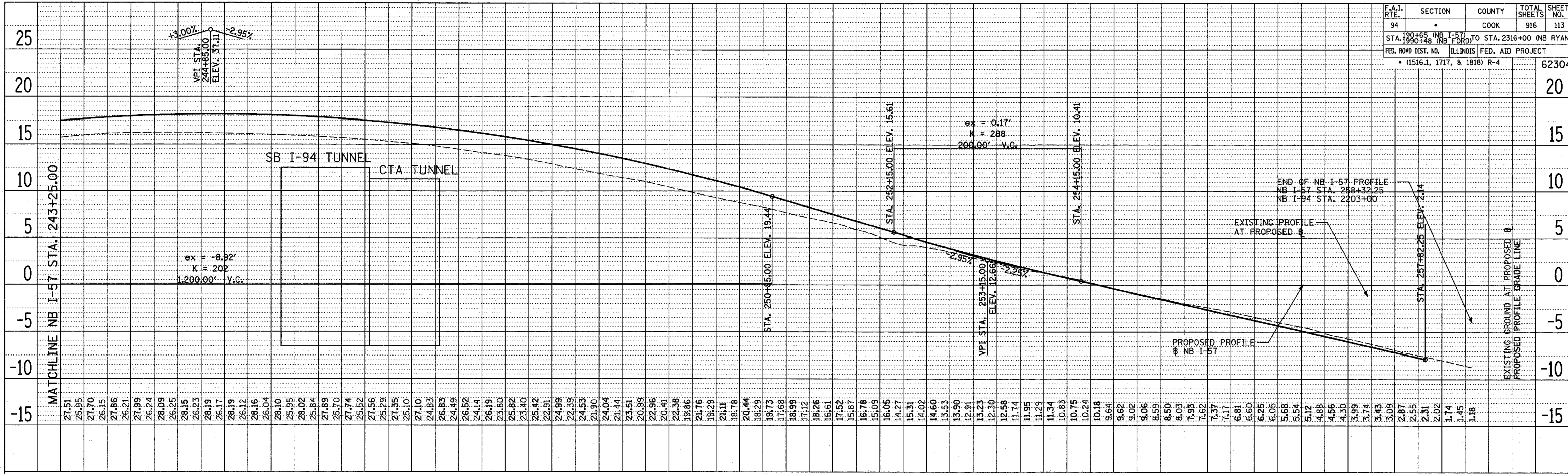
PLAN	REVISIONS	BY	DATE
NO.	NO.		



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PROFILES
 NB I-57
 STA. 213+50.00 TO 243+25.00
 SCALE: 1"=50' HORIZ.
 1"=5' VERT.
 DATE: MARCH 7TH, 2006
 DRAWN BY: JPM
 CHECKED BY: MPG

TYLIN INTERNATIONAL



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PROFILES
NB I-57 STA. 243+25.00 TO 258+32.25
EB I-57 TO SB I-94 CONNECTOR
STA. 406+20.00 TO 416+00.00

SCALE: 1"=50' HORIZ.
 1"=5' VERT.
 DATE: MARCH 7TH, 2006

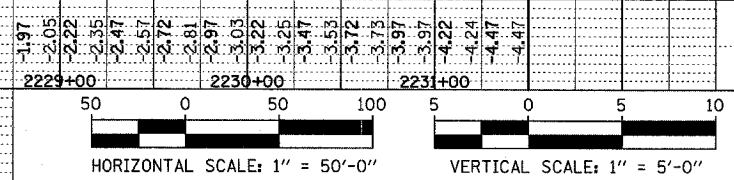
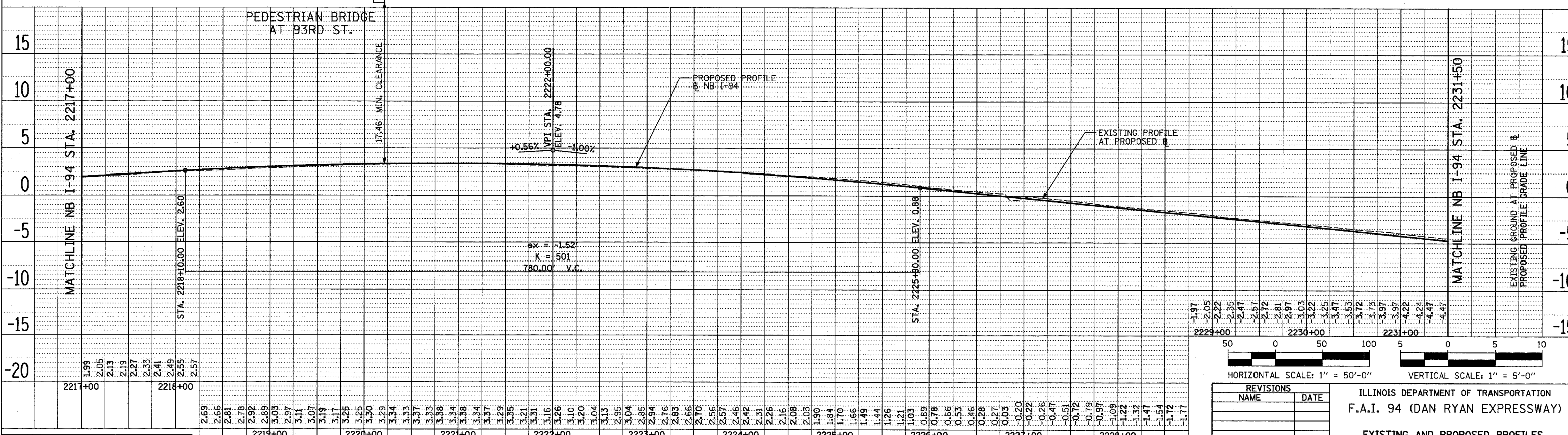
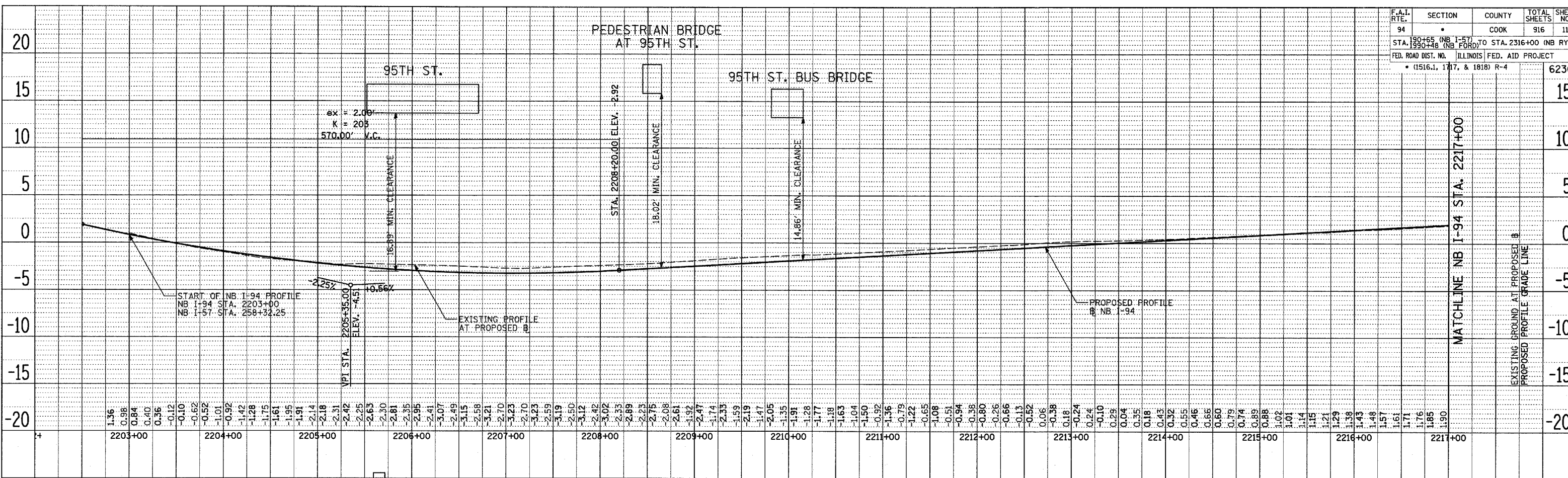
DRAWN BY: JPM
 CHECKED BY: MPG

PLAN	REVISIONS	DATE
NO.	NO.	

TYLIN INTERNATIONAL

EB I-57 TO SB I-94 (BISHOP FORD FREEWAY) CONNECTOR RAMP

F.A.I. RTE. 94	SECTION	COUNTY COOK	TOTAL SHEETS 916	SHEET NO. 114
STA. 190+65 (NB I-57) TO STA. 190+48 (NB FORD)		TO STA. 2316+00 (NB RYAN)		62304
FED. ROAD DIST. NO. ILLINOIS		FED. AID PROJECT		
(1516.1, 1717, & 1818) R-4				



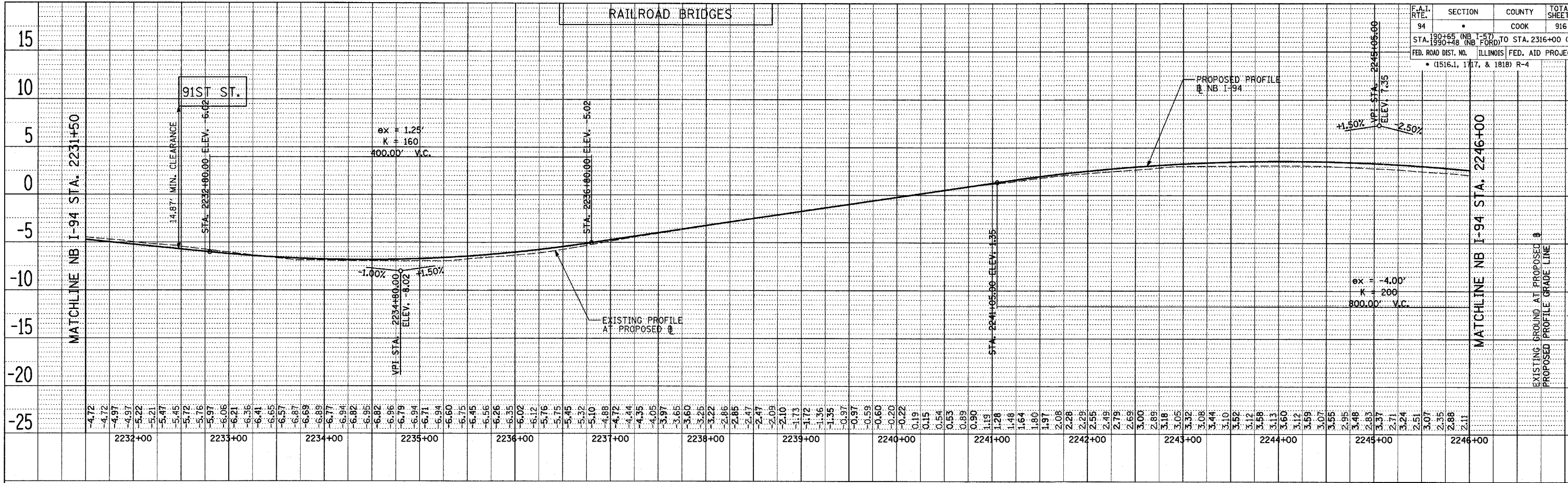
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PROFILES
 NB I-94 (DAN RYAN EXPRESSWAY)
 STA. 2203+00.00 TO 2231+50.00
 SCALE: 1"=50' HORIZ.
 1"=5' VERT.
 DATE: MARCH 7TH, 2006
 DRAWN BY: JPM
 CHECKED BY: MPG

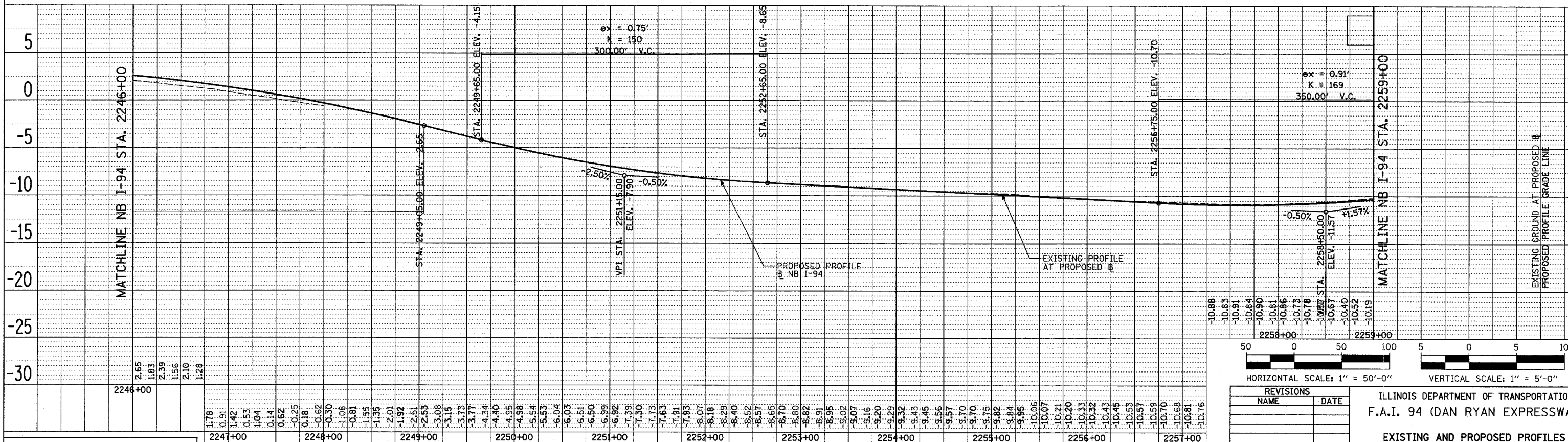
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	PLOTTED	
	FILED	
	RT. OF WAY CHECKED	
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	CADD FILE NAME	
	NO.	

PLAN	SURVEYED	DATE
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	FILED	
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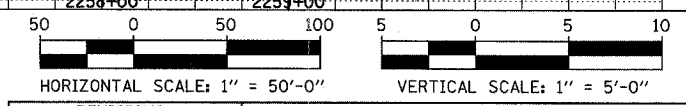
TYLIN INTERNATIONAL



NB I-94 (DAN RYAN EXPRESSWAY)



NB I-94 (DAN RYAN EXPRESSWAY)



REVISIONS	
NAME	DATE

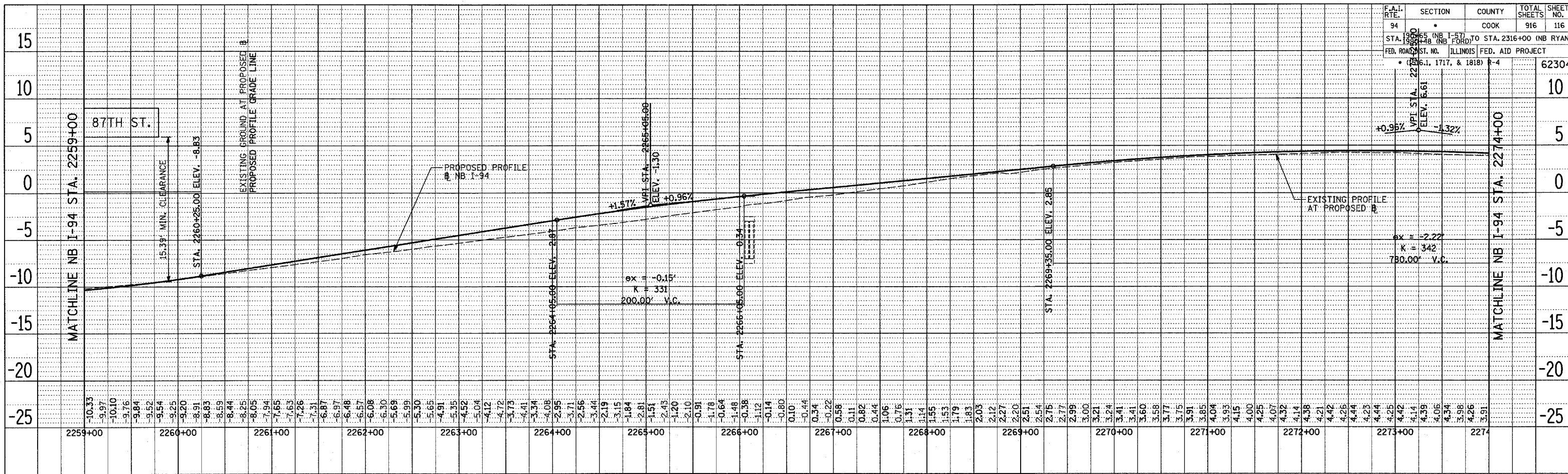
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F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PROFILES
NB I-94 (DAN RYAN EXPRESSWAY)
STA. 2231+50.00 TO 2259+00.00
 SCALE: 1"=50' HORIZ.
 1"=5' VERT.
 DATE: MARCH 7TH, 2006
 DRAWN BY: JPM
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TYLIN INTERNATIONAL

PLAN
 SURVEYED
 PLOTTED
 ALIGNMENT CHECKED
 VERTICAL CHECKED
 NOTE BOOK NO. _____
 DATE _____

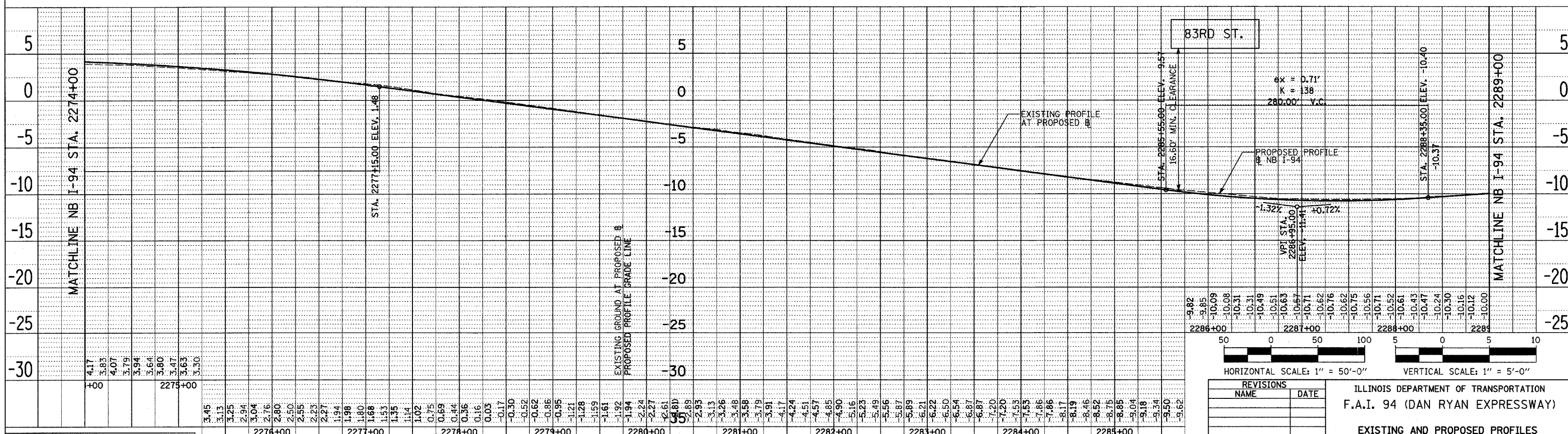
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 SURVEYED
 PLOTTED
 ALIGNMENT CHECKED
 VERTICAL CHECKED
 NOTE BOOK NO. _____
 DATE _____

PLAN
 SURVEYED PLOTTED
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 NOTE BOOK NO. _____
 DATE _____

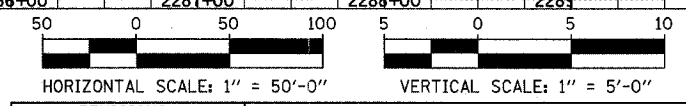


NB I-94 (DAN RYAN EXPRESSWAY)

PLAN
 SURVEYED PLOTTED
 ALIGNMENT CHECKED
 NOTE BOOK NO. _____
 DATE _____



NB I-94 (DAN RYAN EXPRESSWAY)

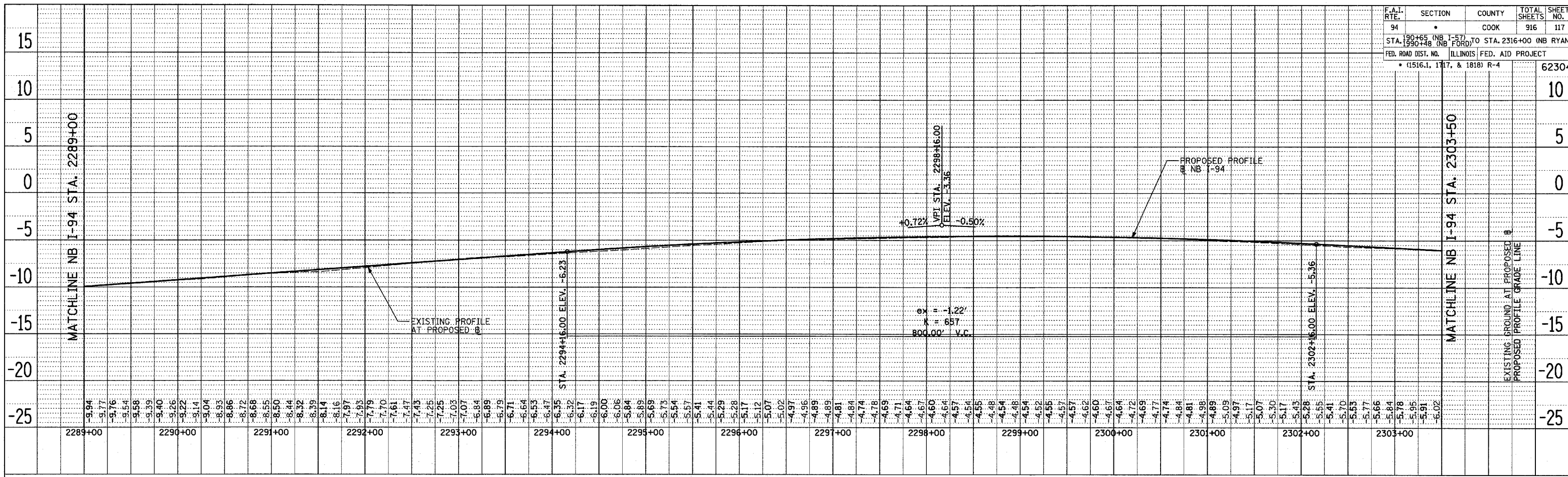


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PROFILES
 NB I-94 (DAN RYAN EXPRESSWAY)
 STA. 2259+00.00 TO 2289+00.00
 SCALE: 1"=50' HORIZ.
 1"=5' VERT.
 DATE: MARCH 7TH, 2006
 DRAWN BY: JPM
 CHECKED BY: MPG

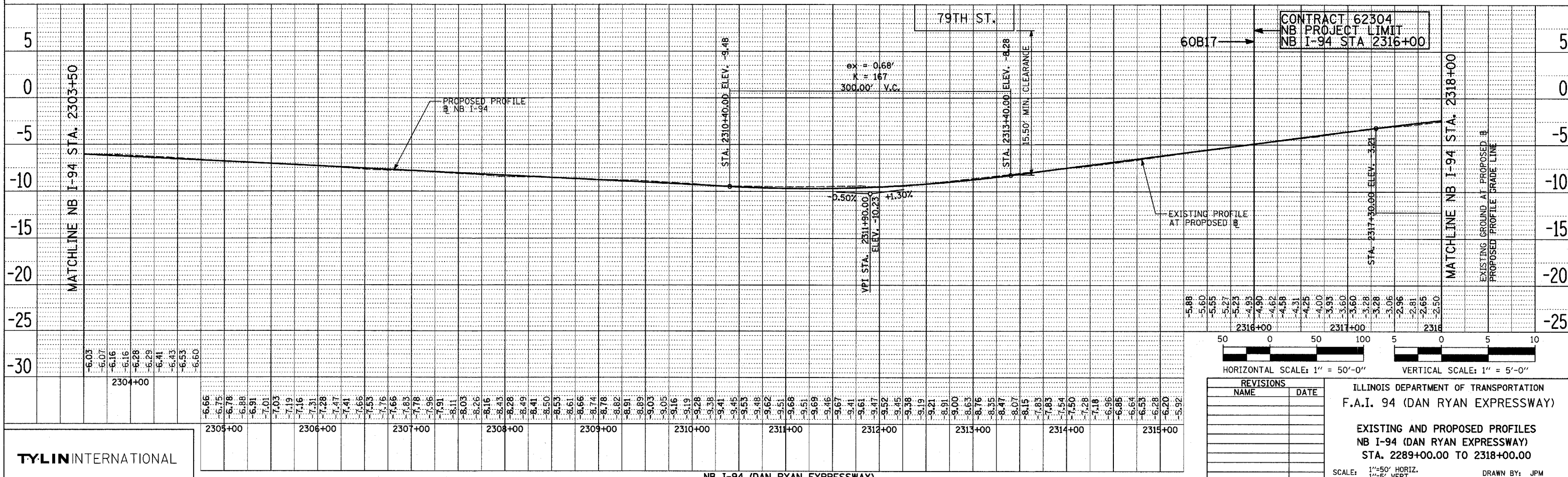
TYLIN INTERNATIONAL

PLAN
 SUBMITTED BY: _____ DATE: _____
 PLOTTED BY: _____
 ALIGNED CHECKED BY: _____
 NO. _____
 NOTE BOOK NO. _____
 PADD FILE NAME: _____



NB I-94 (DAN RYAN EXPRESSWAY)

PLAN
 SUBMITTED BY: _____ DATE: _____
 PLOTTED BY: _____
 ALIGNED CHECKED BY: _____
 NO. _____
 NOTE BOOK NO. _____
 PADD FILE NAME: _____



NB I-94 (DAN RYAN EXPRESSWAY)

TYLIN INTERNATIONAL

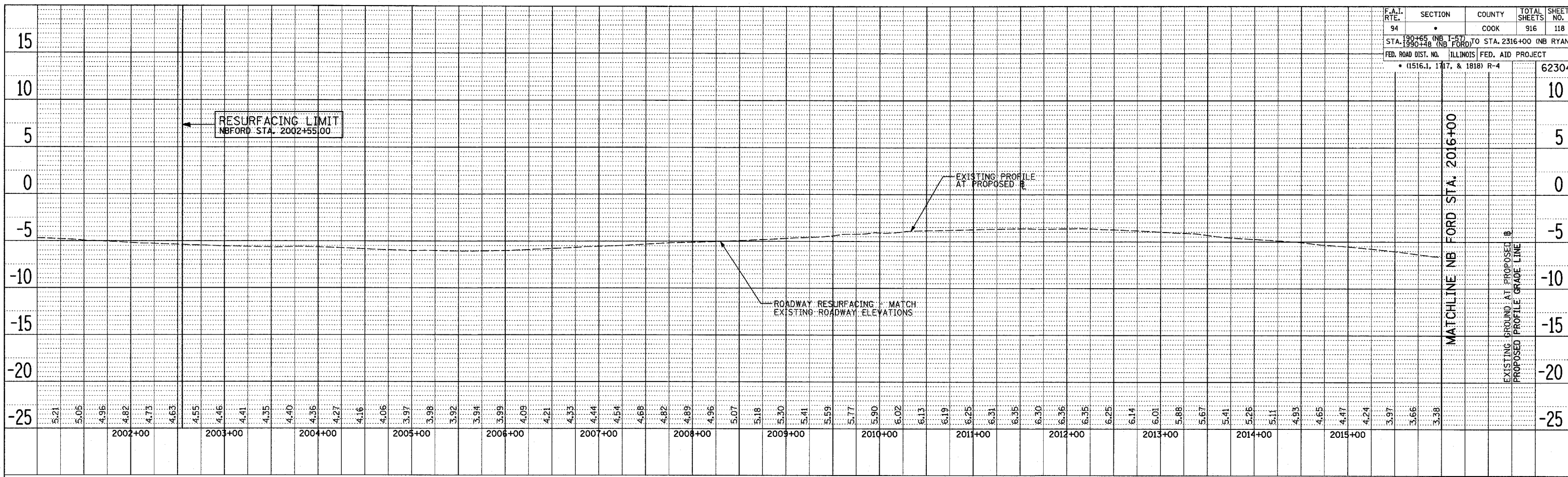
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PROFILES
 NB I-94 (DAN RYAN EXPRESSWAY)
 STA. 2289+00.00 TO 2318+00.00
 SCALE: 1"=50' HORIZ.
 1"=5' VERT.
 DATE: MARCH 7TH, 2006
 DRAWN BY: JPM
 CHECKED BY: MPG

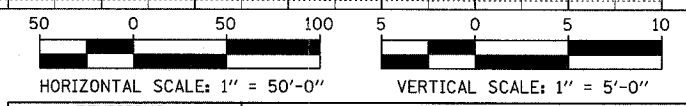
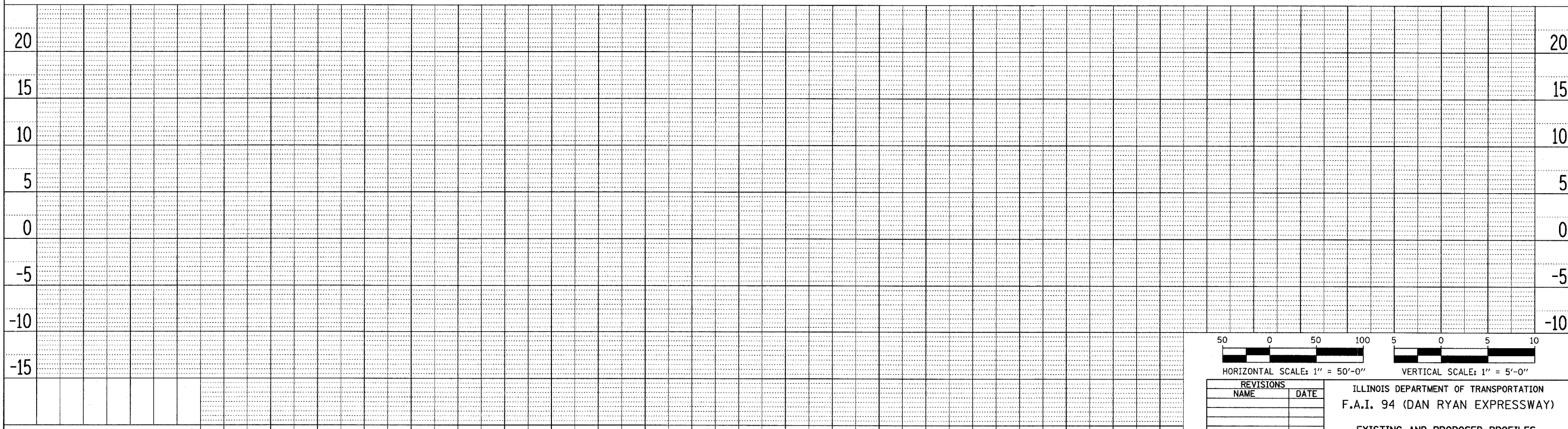
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	•	COOK	916	118
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1417, & 1818) R-4				
				62304

PLAN	DATE
SUBMITTED	
PLOTTED	
ALIGNMENT CHECKED	
NOTE BOOK NO.	
CADD FILE NAME	

PLAN	DATE
SUBMITTED	
PLOTTED	
ALIGNMENT CHECKED	
NOTE BOOK NO.	
CADD FILE NAME	



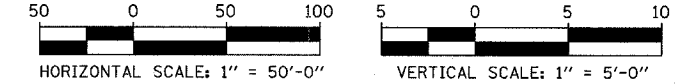
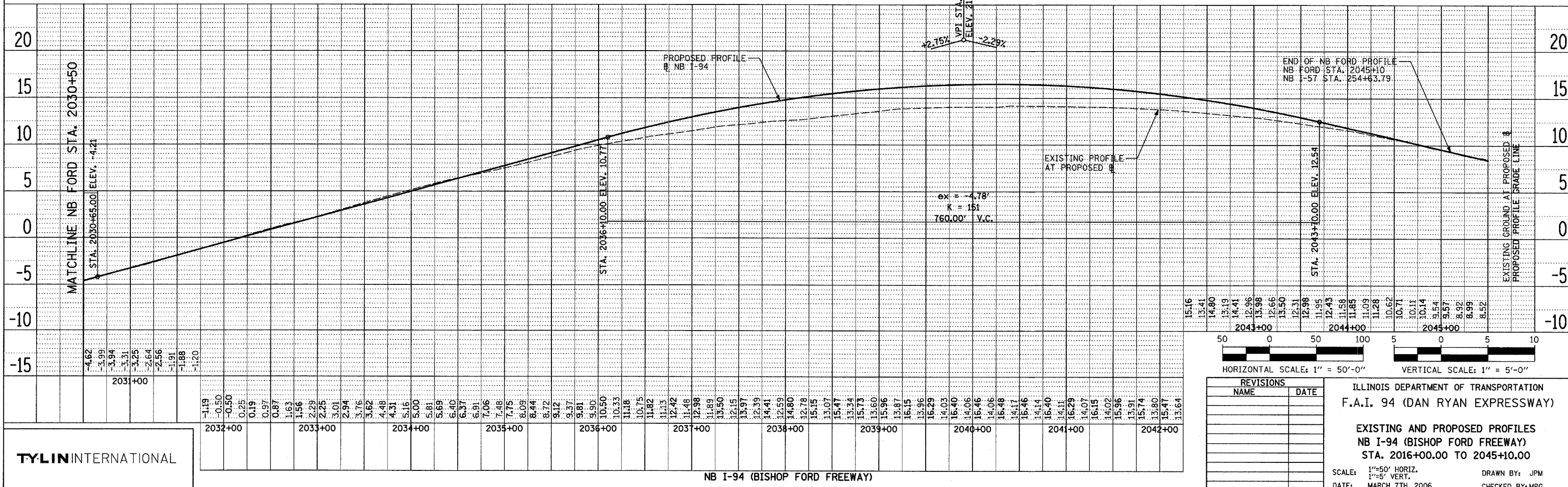
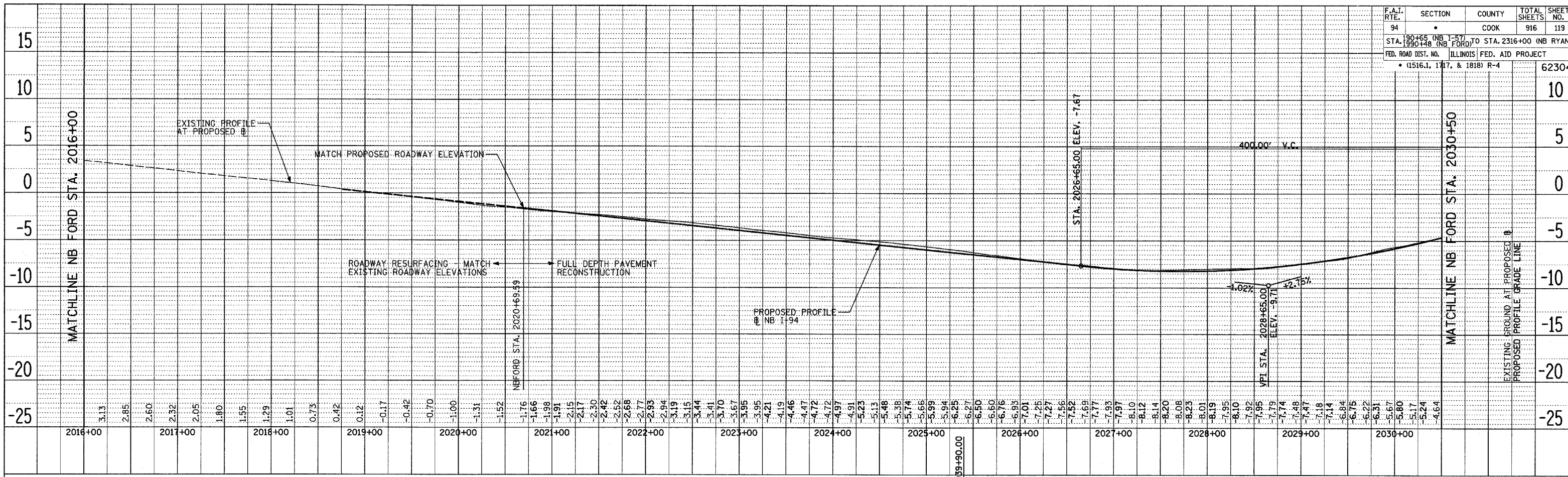
NB I-94 (BISHOP FORD FREEWAY)



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PROFILES
 NB I-94 (BISHOP FORD FREEWAY)
 STA. 2002+55.00 TO 2016+00.00
 SCALE: 1"=50' HORIZ.
 1"=5' VERT.
 DATE: MARCH 7TH, 2006
 DRAWN BY: JPM
 CHECKED BY: MPG

TYLIN INTERNATIONAL



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

EXISTING AND PROPOSED PROFILES
 NB I-94 (BISHOP FORD FREEWAY)
 STA. 2016+00.00 TO 2045+10.00

SCALE: 1"=50' HORIZ.
 1"=5' VERT.
 DATE: MARCH 7TH, 2006

DRAWN BY: JPM
 CHECKED BY: MPG

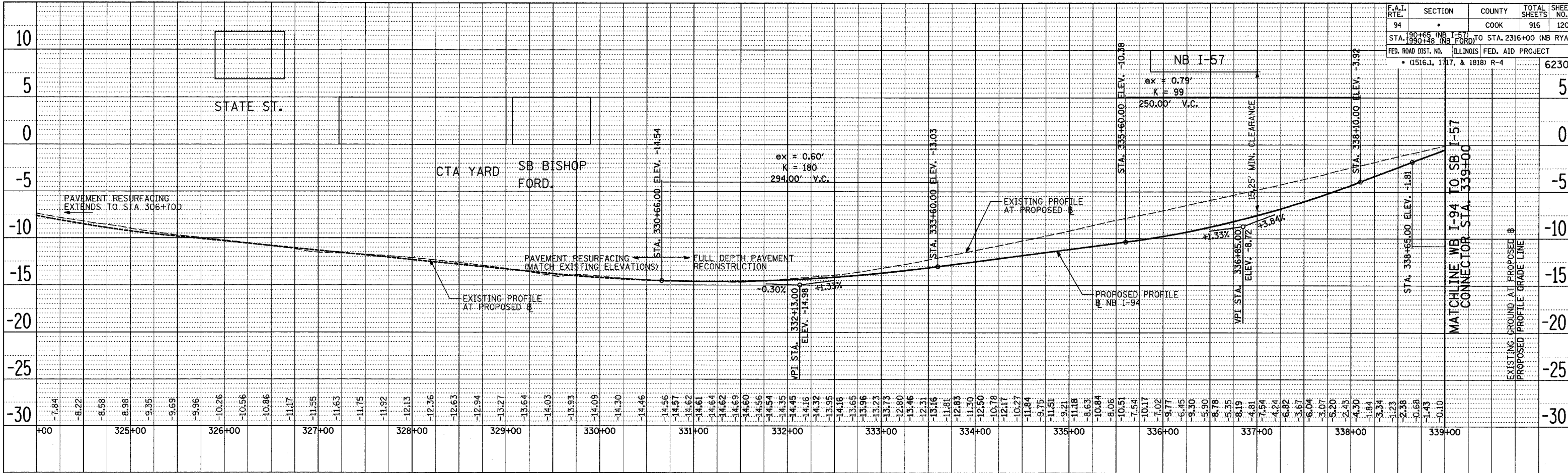
TYLIN INTERNATIONAL

PLAN SURVEYED PLOTTED
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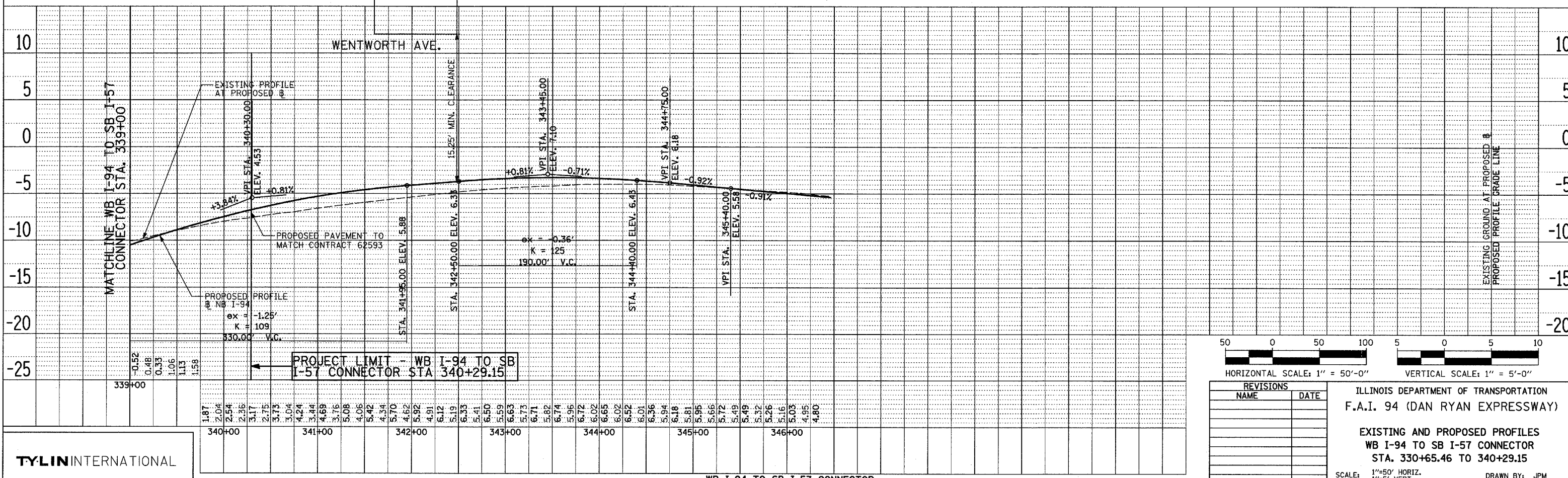
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BY _____ DATE _____

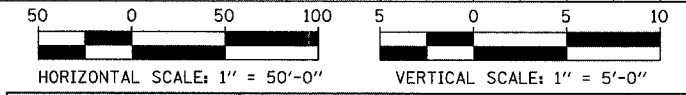
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	916	120
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
STA. 1990+48 (NB FORD)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4				



WB I-94 TO SB I-57 CONNECTOR



WB I-94 TO SB I-57 CONNECTOR



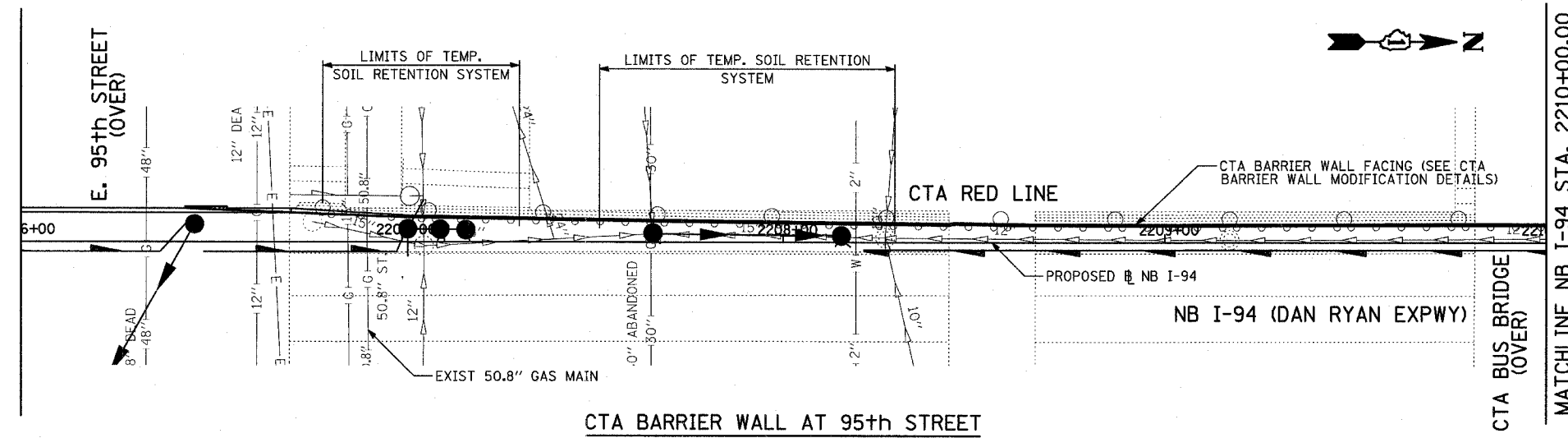
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PROFILES
 WB I-94 TO SB I-57 CONNECTOR
 STA. 330+65.46 TO 340+29.15
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 1"=5' VERT.
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 CHECKED BY: MPG

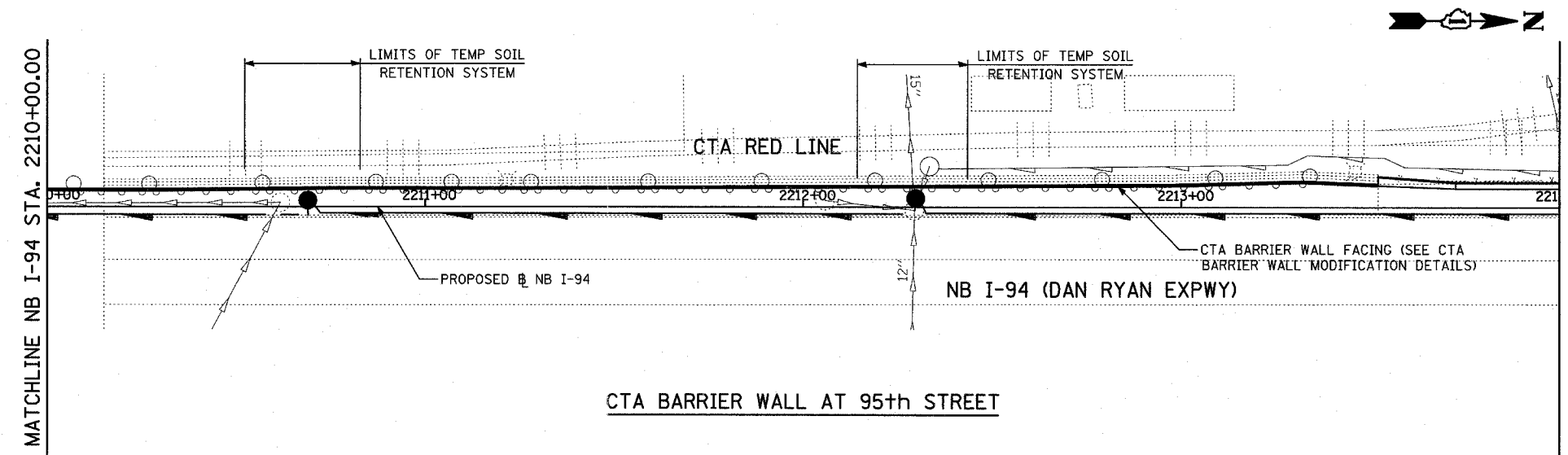
TYLIN INTERNATIONAL

PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	
NO.	ALIGNED	
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	CAD FILE NAME	

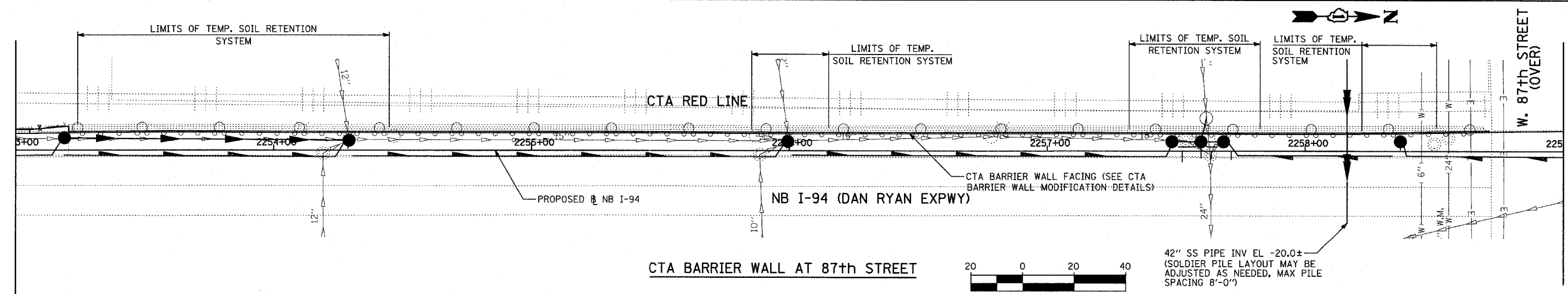
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NOTE BOOK	PLOTTED	
NO.	ALIGNED	
	CHECKED	
	CAD FILE NAME	



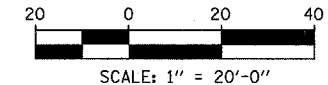
CTA BARRIER WALL AT 95th STREET



CTA BARRIER WALL AT 95th STREET



CTA BARRIER WALL AT 87th STREET



42" SS PIPE INV EL -20.0±
(SOLDIER PILE LAYOUT MAY BE ADJUSTED AS NEEDED, MAX PILE SPACING 8'-0")

NOTES:

1. THE LIMITS SHOWN FOR THE TEMPORARY SOIL RETENTION SYSTEM FOR DRAINAGE IMPROVEMENTS IS APPROXIMATE. THE ACTUAL LIMITS SHALL BE AS DESIGNED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER & THE CTA.
2. THE AREA OF THE TEMPORARY SOIL RETENTION SYSTEM FOR THE PROPOSED DRAINAGE STRUCTURE(S) IS AS INDICATED ON THE SCHEDULE GIVEN ON SHEET 156.
3. THE PROPOSED SOLDIER PILE WALL & DETAILS INDICATED ON THIS SHEET & SHEETS B1-B3 ARE DESIGNED TO RETAIN THE CTA BALLAST DURING EXCAVATION FOR THE PROPOSED PAVEMENT SECTION ONLY.
4. WORK THIS SHEET WITH SHEETS B1 TO B3.
5. EXISTING SUB-PIER LOCATIONS ARE TAKEN FROM EXISTING PLANS, SEE SHEETS 131-136.

LEGEND:

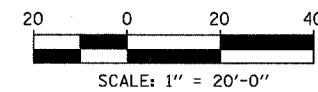
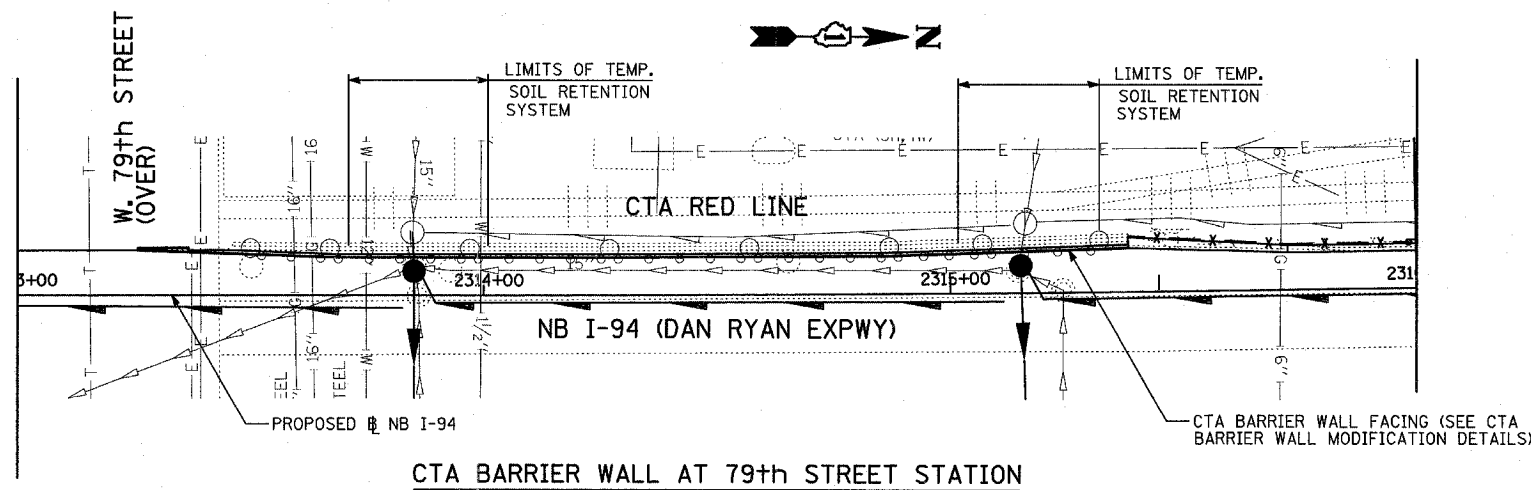
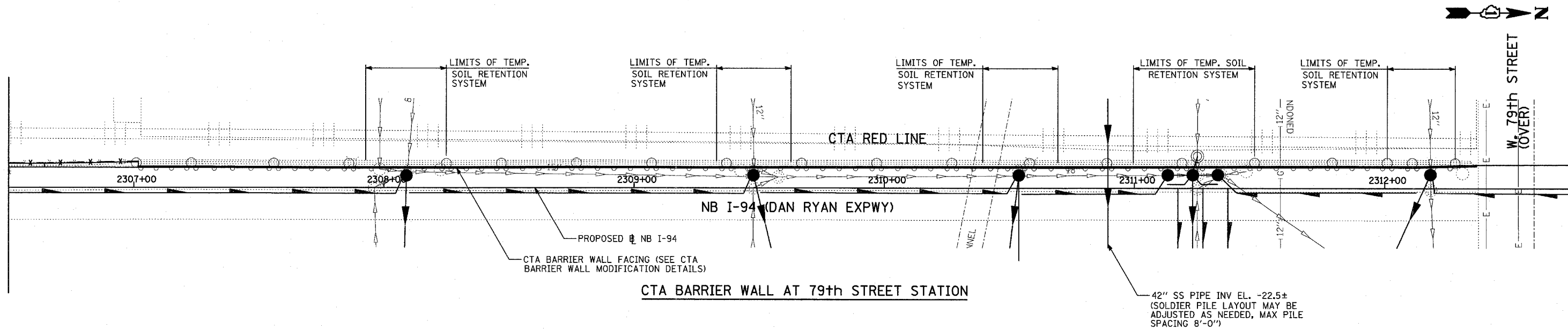
- - EXISTING SUB-PIER
- - PROPOSED SOLDIER PILE
- - PROPOSED CATCH BASIN

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
CTA STATION BARRIER WALL MODIFICATIONS
79TH, 87TH & 95TH STREET STATIONS;
LOCATION PLAN 95TH & 87TH STREET

SCALE: DATE: MARCH 7, 2006

DRAWN BY: PL
CHECKED BY: SP



NOTES:

1. THE LIMITS SHOWN FOR THE TEMPORARY SOIL RETENTION SYSTEM FOR DRAINAGE IMPROVEMENTS IS APPROXIMATE. THE ACTUAL LIMITS SHALL BE AS DESIGNED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER & THE CTA.
2. THE AREA OF THE TEMPORARY SOIL RETENTION SYSTEM FOR THE PROPOSED DRAINAGE STRUCTURE(S) IS AS INDICATED ON THE SCHEDULE GIVEN ON SHEET 156.
3. THE PROPOSED SOLDIER PILE WALL & DETAILS INDICATED ON THIS SHEET & SHEETS B1-B3 ARE DESIGNED TO RETAIN THE CTA BALLAST DURING EXCAVATION FOR THE PROPOSED PAVEMENT SECTION ONLY.
4. WORK THIS SHEET WITH SHEETS B1 TO B3.
5. EXISTING SUB-PIER LOCATIONS ARE TAKEN FROM EXISTING PLANS, SEE SHEETS 131-136.

LEGEND:

- - EXISTING SUB-PIER
- - PROPOSED SOLDIER PILE
- - PROPOSED CATCH BASIN

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 CTA STATION BARRIER WALL MODIFICATIONS
 79TH, 87TH & 95TH STREET STATIONS:
 LOCATION PLAN 79TH STREET

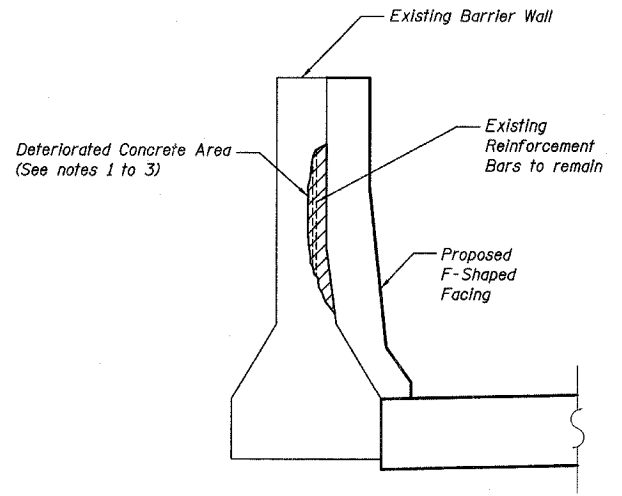
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 DRAWN BY: PL
 CHECKED BY: SP

CTA BARRIER WALL LOCATION AND MODIFICATION TABLE

STATION	LOCATION		MODIFICATION
	STATIONS		
95th ST.	STA. 2205+51 TO 2213+32		REFACING
87th ST.	STA. 2253+21 TO 2258+72		REFACING
79th ST.	STA. 2307+02 TO 2312+85		REFACING
79th ST.	STA. 2313+44 TO 2315+38		REFACING

DESIGN STRESSES:

FIELD UNITS
 f'c = 3,500 PSI
 fy = 60,000 PSI (REINF.)



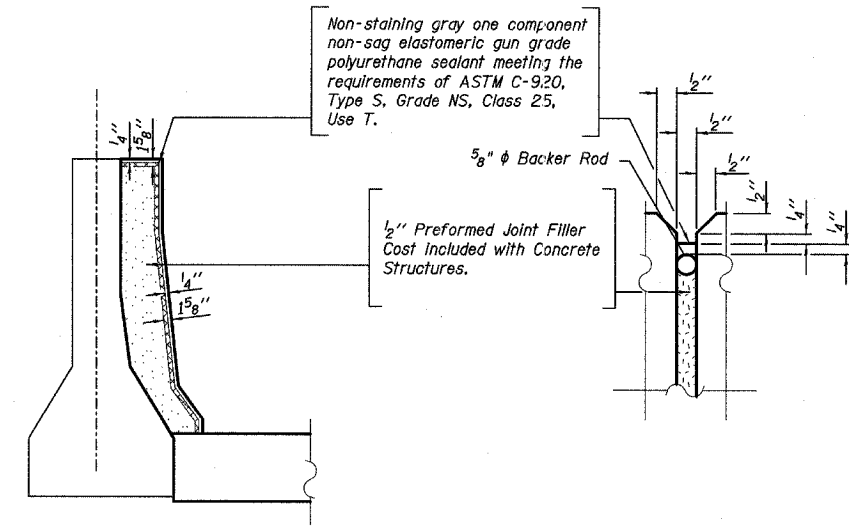
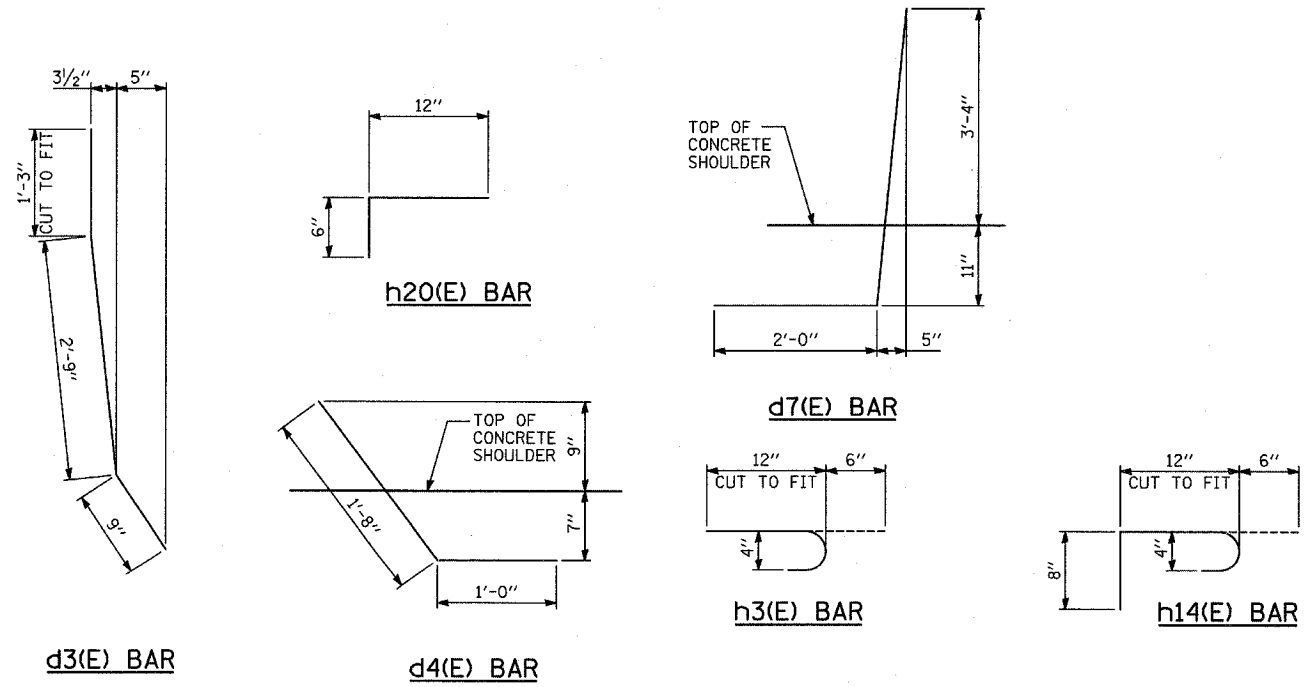
CONCRETE REPAIR DETAIL

NOTES ON CONCRETE REPAIR DETAIL

1. REMOVE DETERIORATED CONCRETE TO SOUND CONCRETE IN ACCORDANCE WITH SECTION 501.03 OF THE STANDARD SPECIFICATIONS. UNDERCUT EXPOSED REINFORCEMENT BARS BY 3/4" TO PROVIDE CLEARANCE BETWEEN EXISTING REINFORCEMENT BARS AND THE REMAINING CONCRETE.
2. EXPOSED EXISTING REINFORCEMENT BARS SHALL REMAIN IN PLACE AND BE BLAST CLEANED TO GRAY METAL. REINFORCEMENT BARS THAT HAVE BEEN CUT OR HAVE LOST 25% OR MORE OF THEIR ORIGINAL CROSS SECTIONAL AREA SHALL BE SUPPLEMENTED BY NEW EPOXY COATED REINFORCEMENT BARS.
3. THE COST OF REMOVING DETERIORATED CONCRETE AND SUPPLEMENTING AND CLEANING EXISTING REINFORCEMENT BARS SHALL BE INCLUDED IN THE COST PER CUBIC YARD OF "CONCRETE STRUCTURES".

GENERAL NOTES:

1. IN LOCATIONS WHERE THE MAIN SEWERS, DRAINAGE STRUCTURES, PUBLIC AND PRIVATE DRAIN CONNECTIONS ARE DISTURBED OR DAMAGED DURING CONSTRUCTION BY THE CONTRACTOR, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO RESTORE AND REPLACE THE DAMAGED FACILITIES AT HIS/HER OWN EXPENSE TO THE SATISFACTION OF THE DEPARTMENT OF WATER MANAGEMENT. THE SEWER FLOWS MUST BE MAINTAINED AT ALL TIMES.
2. IN LOCATIONS WHERE PILING/DRILLED SHAFTS ARE BEING PROPOSED IN THE VICINITY OF THE EXISTING SEWER FACILITIES, THE CONTRACTOR MUST PROVIDE A PLAN OF CONSTRUCTION METHODS WITH CALCULATIONS SEALED BY A STRUCTURAL ENGINEER LICENSED BY THE STATE OF ILLINOIS PROVING NO DAMAGE OR HARM TO THE STRUCTURAL INTEGRITY OF THE CITY'S SEWER SYSTEM. THE PLAN MUST BE APPROVED BY THE RESIDENT ENGINEER AND THE CHICAGO DEPARTMENT OF WATER MANAGEMENT PRIOR TO CONSTRUCTION.
3. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT NO GAP REMAINS BETWEEN PROPOSED CTA FENCING OR WHERE PROPOSED CTA FENCING TERMINATES AND EXISTING CTA FENCE REMAINS IN PLACE.
4. PLAN DIMENSIONS AND DETAILS RELATING TO EXISTING STRUCTURES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION OR A CHANGE IN THE SCOPE OF WORK. THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE FOR THE WORK.
5. ALL CONSTRUCTION JOINTS SHALL BE BONDED.
6. ALL EXPOSED CONCRETE EDGES SHALL HAVE A 3/4" BY 45° CHAMFER EXCEPT WHERE SHOWN OTHERWISE. CHAMFER ON VERTICAL EDGES SHALL BE CONTINUED A MINIMUM OF 1 FOOT BELOW THE FINISHED GROUND LINE.
7. A PROTECTIVE COAT SHALL BE APPLIED WITHIN THE LIMITS SHOWN ON THE PLANS ACCORDING TO SECTION 503.19 OF THE STANDARD SPECIFICATIONS.
8. REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M31 OR M322 GRADE 60.
9. REINFORCEMENT BARS DESIGNATED "E" SHALL BE EPOXY COATED.
10. THE CONTRACTOR IS RESPONSIBLE FOR THE DESIGN AND PERFORMANCE OF THE LAGGING USING NO LESS THAN 3" NOMINAL ROUGH-SAWN THICKNESS AND THE MINIMUM TABULATED UNIT STRESS IN BENDING (Fb), USED IN THE DESIGN OF THE TIMBER LAGGING SHALL BE 1000 psi.
11. HAMMER DRIVING OF PILES WILL NOT BE ALLOWED.



BARRIER JOINT DETAILS
 (AT EXISTING BARRIER JOINT LOCATIONS)

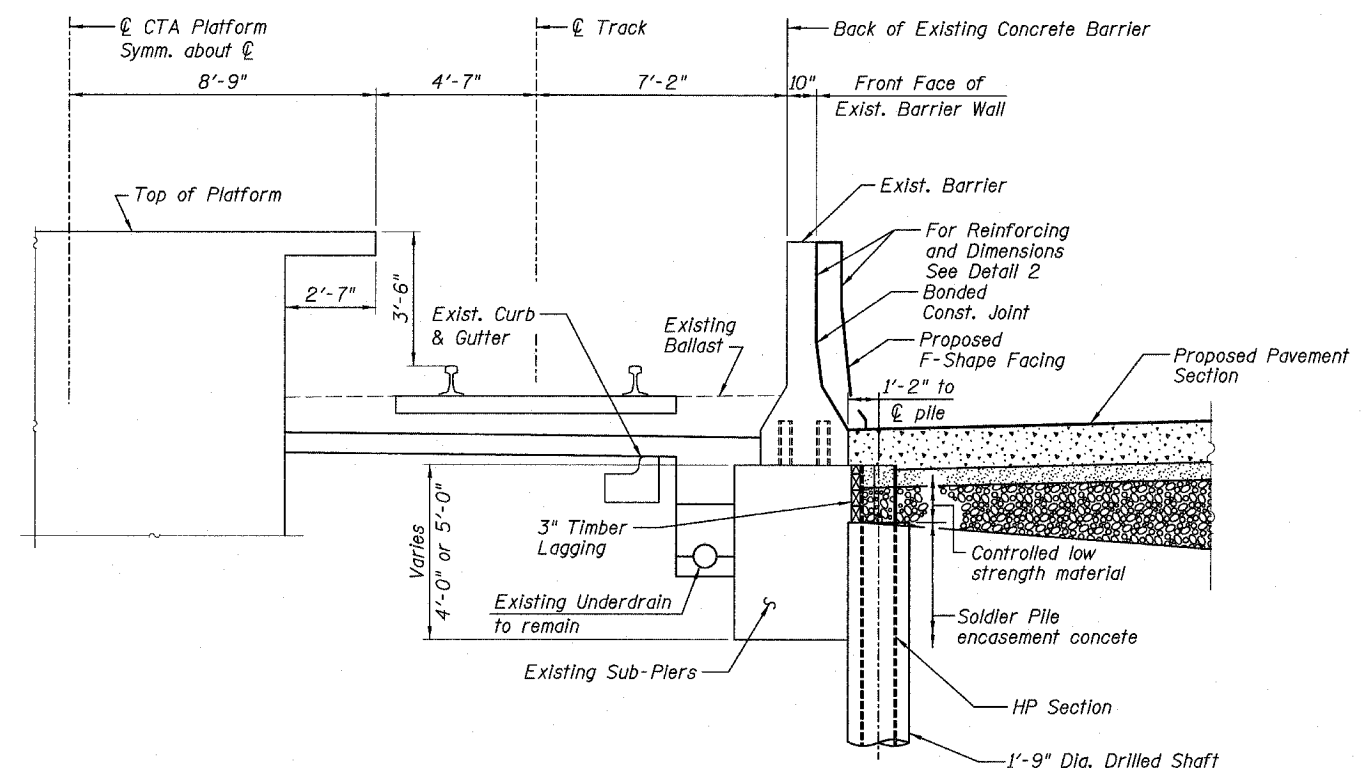
TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 CTA STATION BARRIER WALL MODIFICATIONS
 LOCATION AND MODIFICATION TABLE,
 GENERAL NOTES AND BAR DETAILS

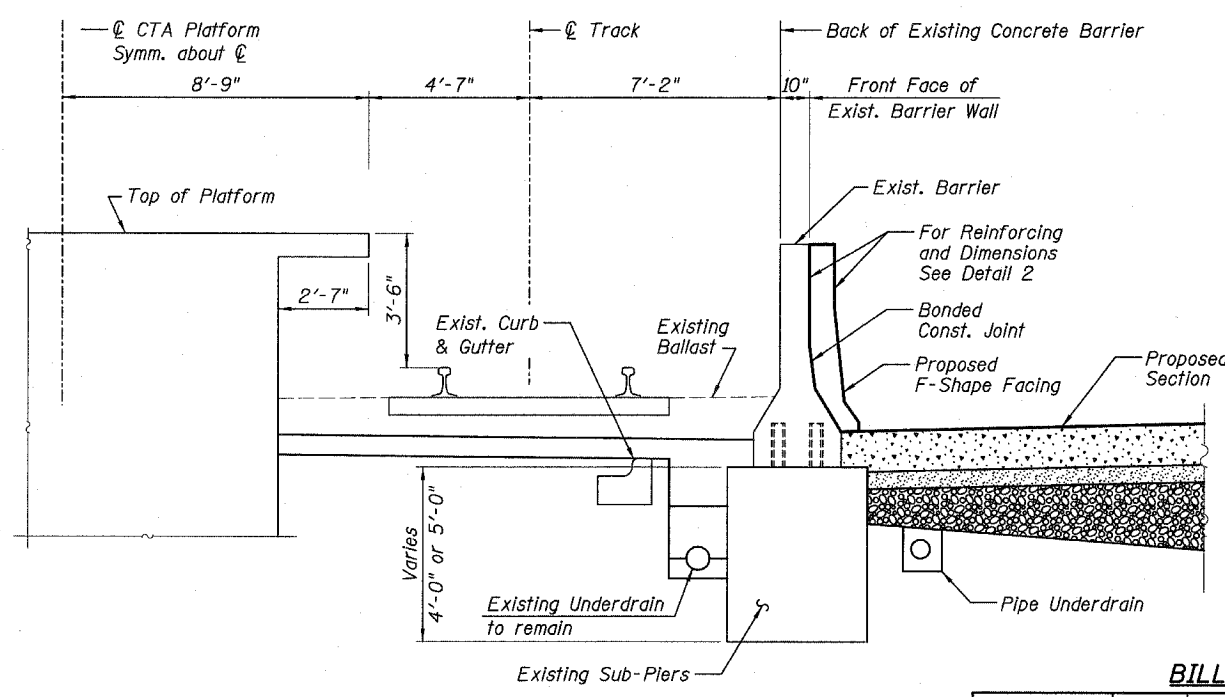
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 DATE: MARCH 7, 2006

DRAWN BY: MPG, SP
 CHECKED BY: SP, PF



TYPICAL SECTION BETWEEN SUB-PIERS
(Pipe Underdrain not shown)

Contractor to cut and dispose of soldier pile tops as required for paving and as directed by the Engineer. Cost to be included in "Furnishing Soldier Piles (HP Section)".



TYPICAL SECTION AT SUB-PIER

LEGEND:

- Concrete Removal
- PCC Pavement
- BAM
- Sub Base

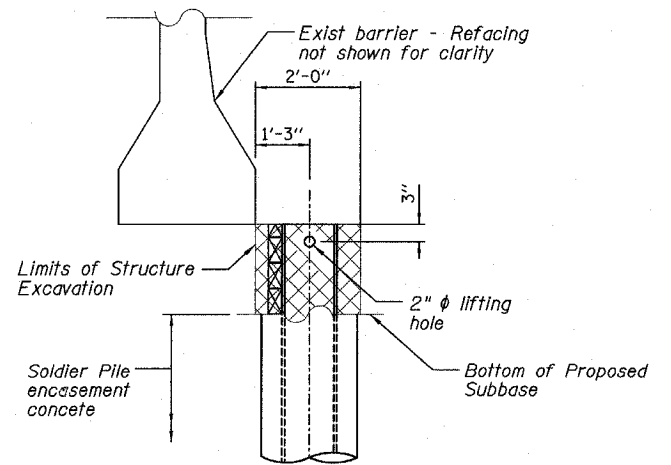
BILL OF MATERIAL

Bar	79th No.	87th No.	95th No.	Size	Length	Shape
d3(E)	681	515	652	#4	4'-9"	
d4(E)	681	515	652	#4	2'-8"	
d12(E)	4	4	2	#4	3'-4"	
h1(E)	156	108	138	#4	29'-7"	
h3(E)	1022	773	977	#4	1'-6"	
h12(E)	6	6	3	#4	1'-4"	
h14(E)	8	8	4	#4	2'-2"	
Item	Unit	79th Qty	87th Qty	95th Qty		
Furnishing Soldier Piles (HP Section)	Foot	1428	1068	1320		
Drilling and Setting Soldier Piles (in Soil)	Cu. Ft.	3731	2803	3703		
Untreated Timber Lagging	Sq. Ft.	1138	860	981		
Reinforcement Bars, Epoxy Coated	Pound	7520	5490	6950		
* Concrete Structures	Cu. Yd.	83	63	80		
Protective Coat	Sq. Yd.	483	366	463		
Structure Excavation	Cu. Yd.	141	108	147		

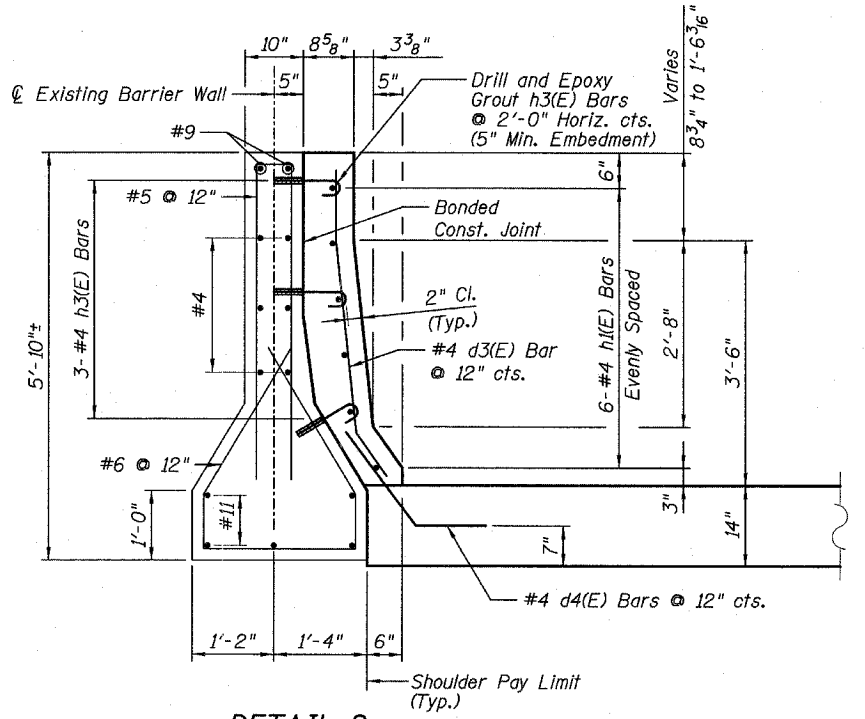
* Coarse Aggregate shall meet the requirements of Article 1004.01-1004.02 of the Standard Specifications except the gradation will be limited to CA-11 or CA-14.

NOTES:

- Drilling and grouting of #4 h3(E) bars shall be in accordance with Section 584 of the Standard Specifications.
- #4 d4(E) bars shall be seated in their final position prior to the placement of the concrete shoulder. Bars shall not be mucked into place.
- The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h3(E) bars shall be included in the cost for Concrete Structures.
- Provide joints in facing to match those in the existing barrier, see Sht B1 for details.
- h1(E) bars shall be cut as needed for fit.
- See Sht. B1 for bar detailing.



MISCELLANEOUS DETAILS

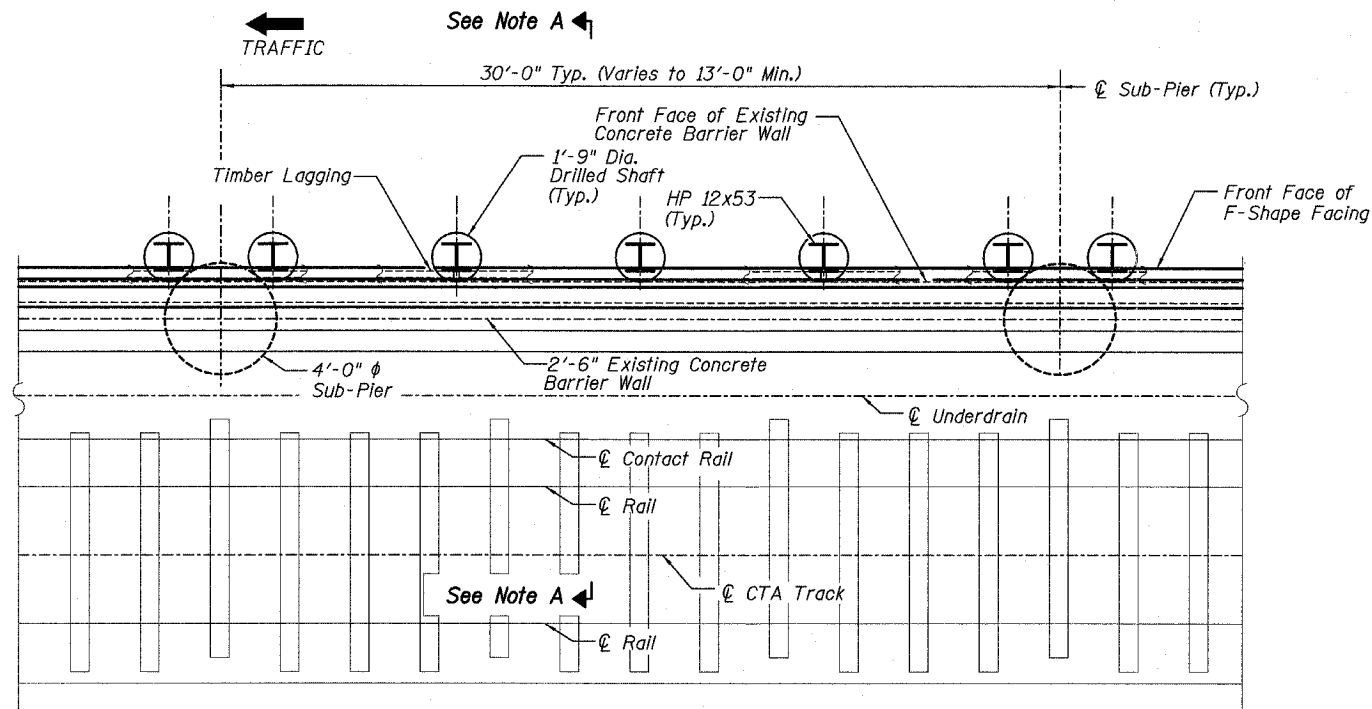


DETAIL 2

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
CTA STATION BARRIER WALL MODIFICATIONS
79TH, 87TH AND 95TH STREET STATIONS
TYPICAL SECTIONS & DETAILS

SCALE: NONE
DATE: MARCH 7, 2006
DRAWN BY: MPG, SP
CHECKED BY: SP, PDF

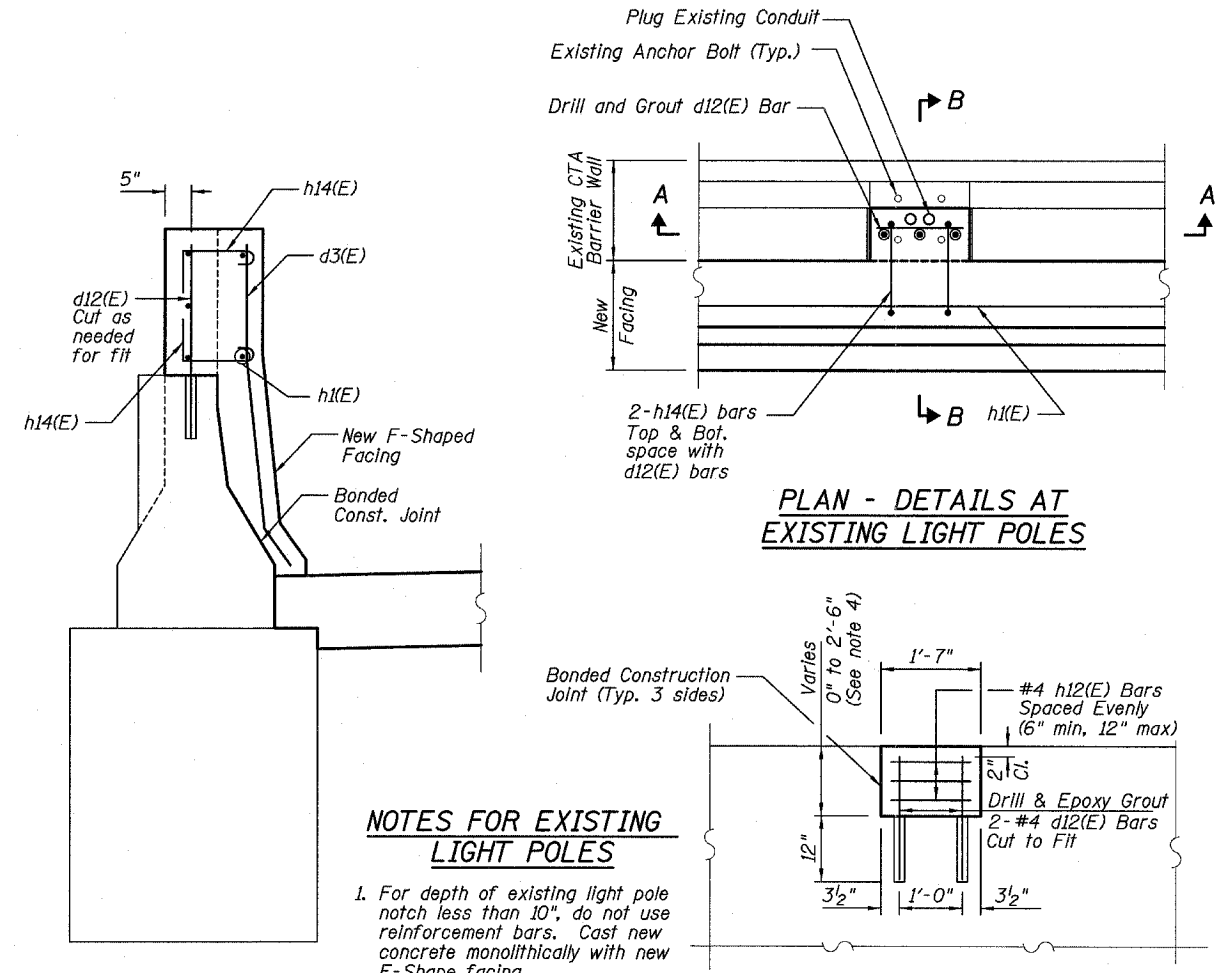


Note A:
See Sheet B2, "Typical Section between Sub-piers"

TYPICAL PLAN

DETAIL A NOTES

Soldier Piles are to be HP 12x53, 12' Long (288 required.) Fy min = 36,000 psi (M270 Grade 36)
Pile locations can be adjusted to miss proposed drainage structures. Maximum pile spacing 8'-0".



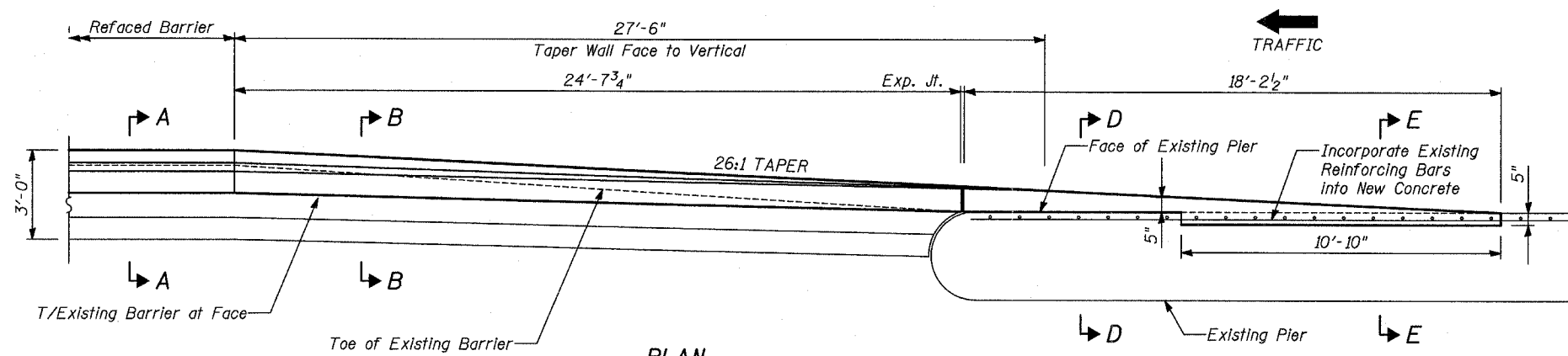
SECTION B-B

SECTION A-A

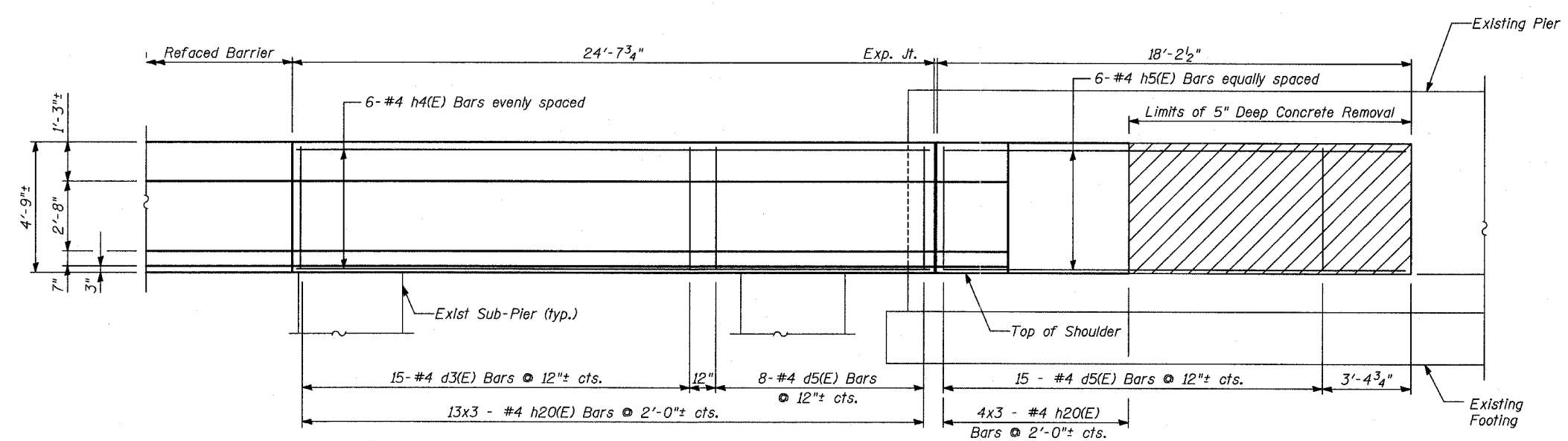
NOTES FOR EXISTING LIGHT POLES

1. For depth of existing light pole notch less than 10", do not use reinforcement bars. Cast new concrete monolithically with new F-Shape facing.
2. Additional reinforcing details for light pole base details shown, for F-Shape Facing details and reinforcement, see Detail 2, Sht B2.

REVISIONS	
NAME	DATE



PLAN



ELEVATION

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d3(E)	15	#4	4'-9"	—
d5(E)	23	#4	4'-4"	—
h4(E)	6	#4	24'-4"	—
h5(E)	6	#4	18'-0"	—
h20(E)	51	#4	1'-6"	—
Item	Unit	Quantity		
Reinforcement Bars, Epoxy Coated	Pound	340		
Concrete Structures	Cu. Yd.	5		
Protective Coat	Sq. Yd.	28		
Concrete Removal	Cu. Yd.	1		

LEGEND:



NOTES:

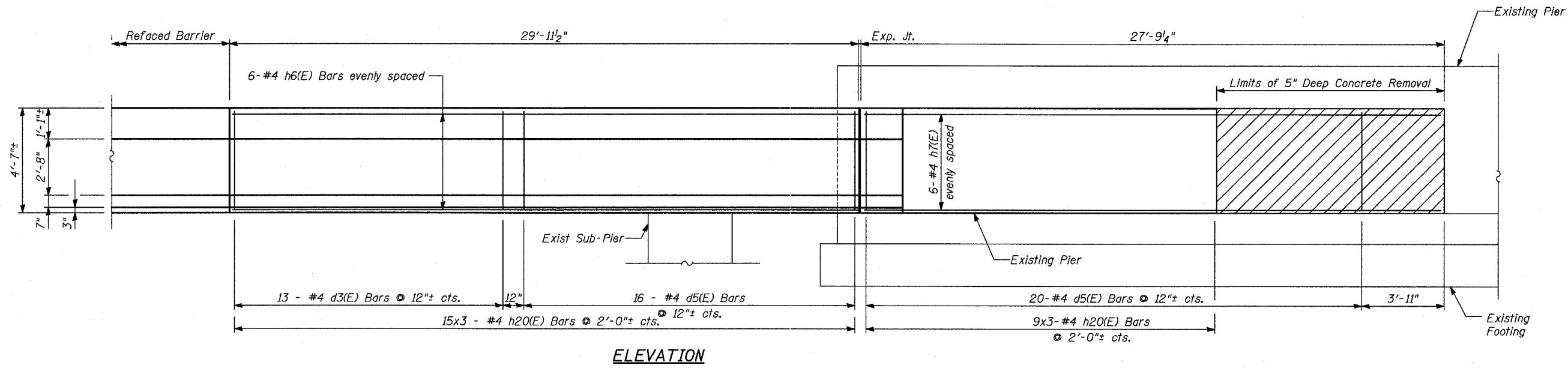
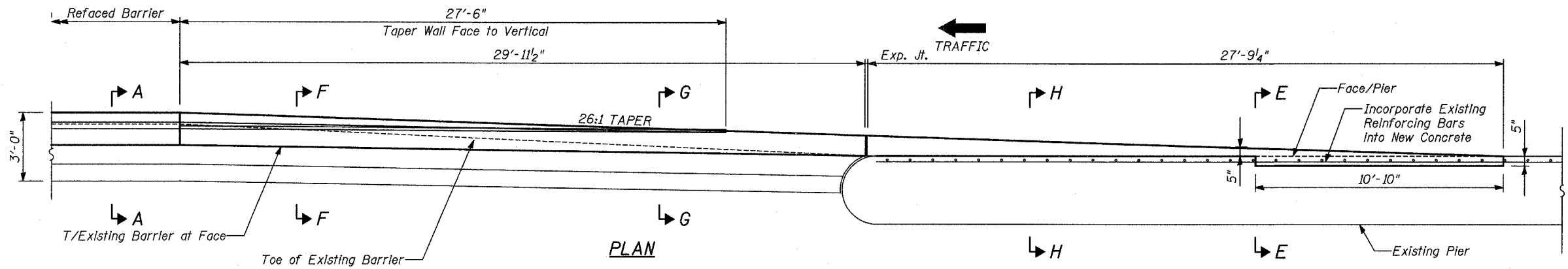
1. Drilling and grouting of #4 h20(E) bars shall be in accordance with Section 584 of the Standard Specifications.
2. The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h20(E) bars shall be included in the cost for Concrete Structures.
3. Provide joints in facing to match those in the existing barrier. See Sht B1 for joint details.
4. For Refaced Barrier details, see Sheet B2.
5. For Sections A-A thru H-H see Sheet B6.
6. For locations of Transition, see Roadway Plans.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 CTA STATION BARRIER WALL MODIFICATIONS
 79TH STREET STATION
 REFACED BARRIER TO PIER TRANSITION
 DETAILS: PLAN AND ELEVATION

SCALE: NONE DRAWN BY: MPG
 DATE: MARCH 7, 2006 CHECKED BY: SP

TYLIN INTERNATIONAL



ELEVATION

LEGEND:
 Concrete Removal

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d3(E)	13	#4	4'-9"	
d5(E)	25	#4	4'-4"	—
h6(E)	6	#4	29'-9"	—
h7(E)	6	#4	27'-6"	—
h20(E)	72	#4	1'-6"	
Item	Unit	Quantity		
Reinforcement Bars, Epoxy Coated	Pound	420		
Concrete Structures	Cu. Yd.	7		
Protective Coat	Sq. Yd.	37		
Concrete Removal	Cu. Yd.	1		

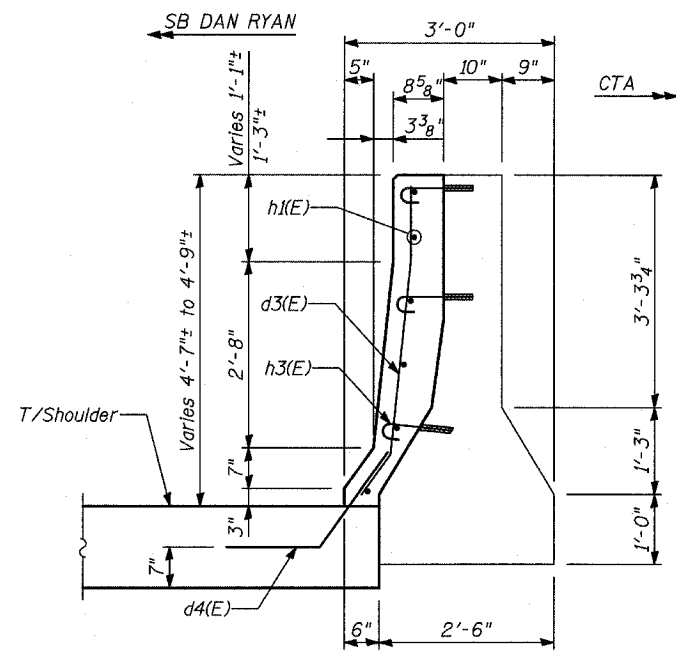
NOTES:

1. Drilling and grouting of #4 h20(E) bars shall be in accordance with Section 584 of the Standard Specifications.
2. The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h20(E) bars shall be included in the cost for Concrete Structures.
3. Provide joints in facing to match those in the existing barrier. See Sht B1 for joint details.
4. For Refaced Barrier details, see Sheet B2.
5. For Sections A-A thru H-H see Sheet B6.
6. For locations of Transition, see Roadway Plans.

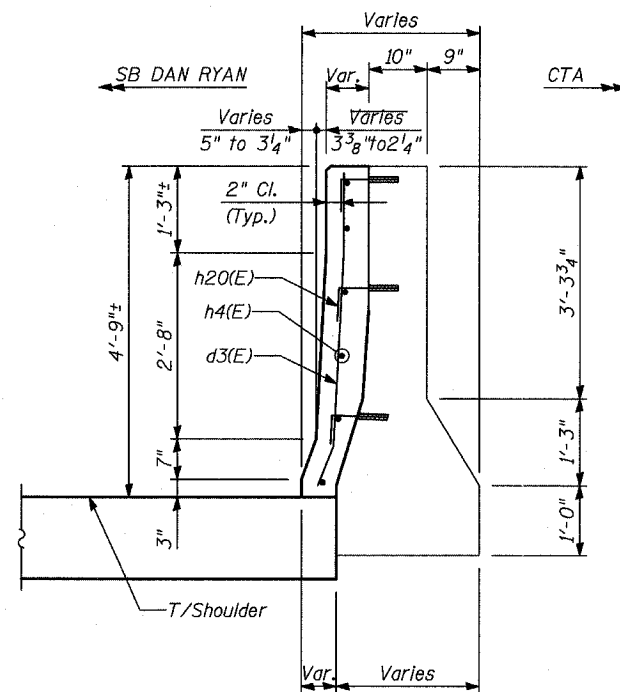
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 CTA STATION BARRIER WALL MODIFICATIONS
 95TH STREET STATION
 REFACED BARRIER TO PIER TRANSITION
 DETAILS: PLAN AND ELEVATION
 SCALE: NONE DRAWN BY: MPG
 DATE: MARCH 7, 2006 CHECKED BY: SP

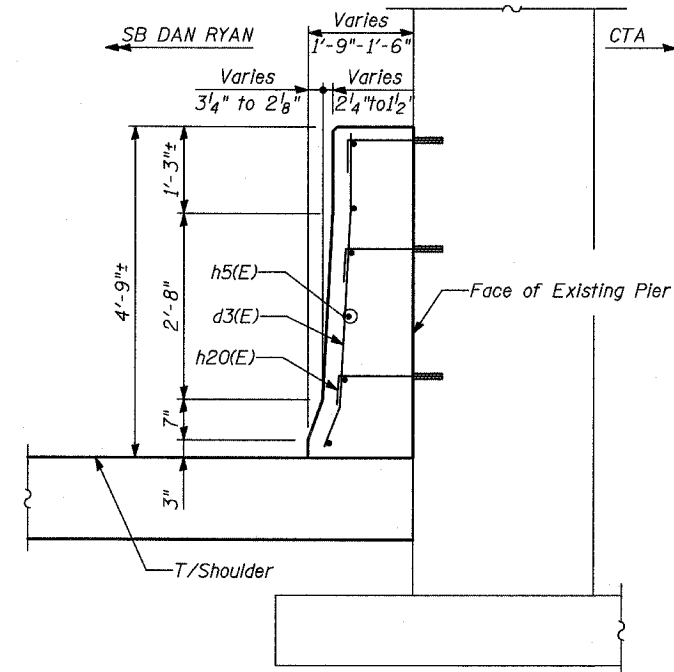
TYLIN INTERNATIONAL



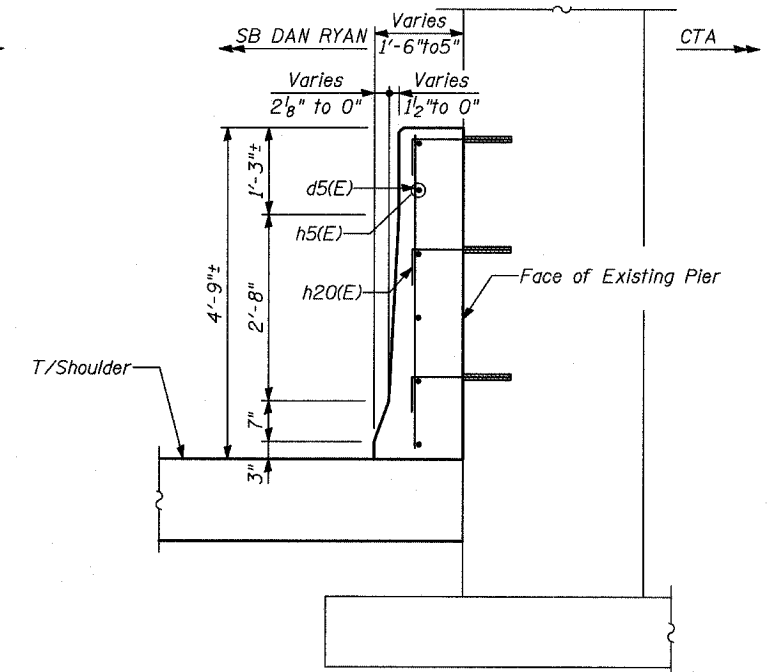
SECTION A-A



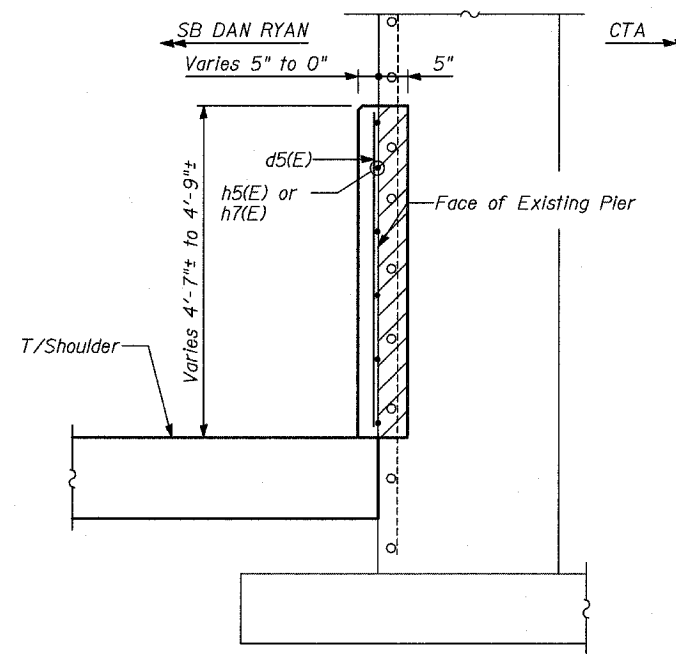
SECTION B-B



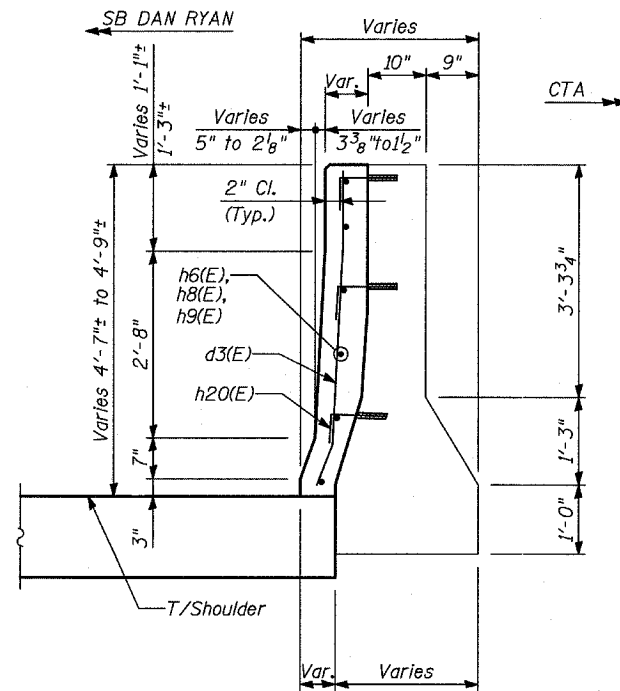
SECTION C-C



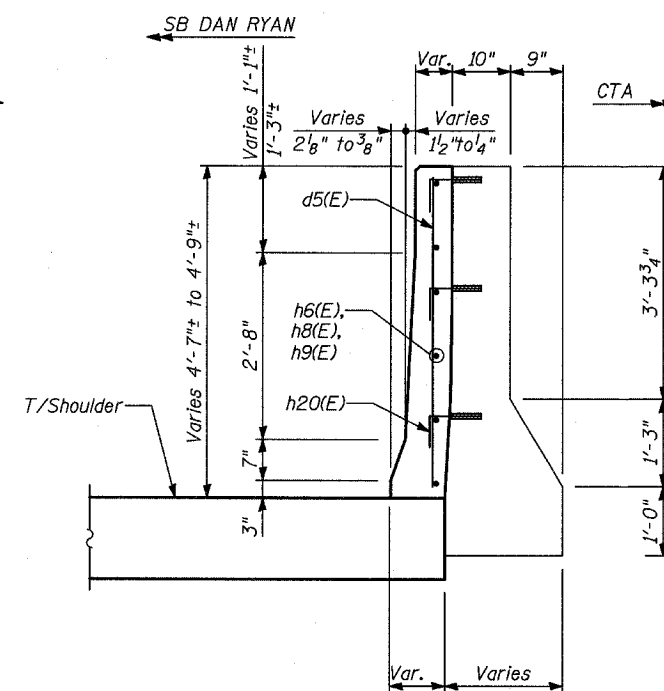
SECTION D-D



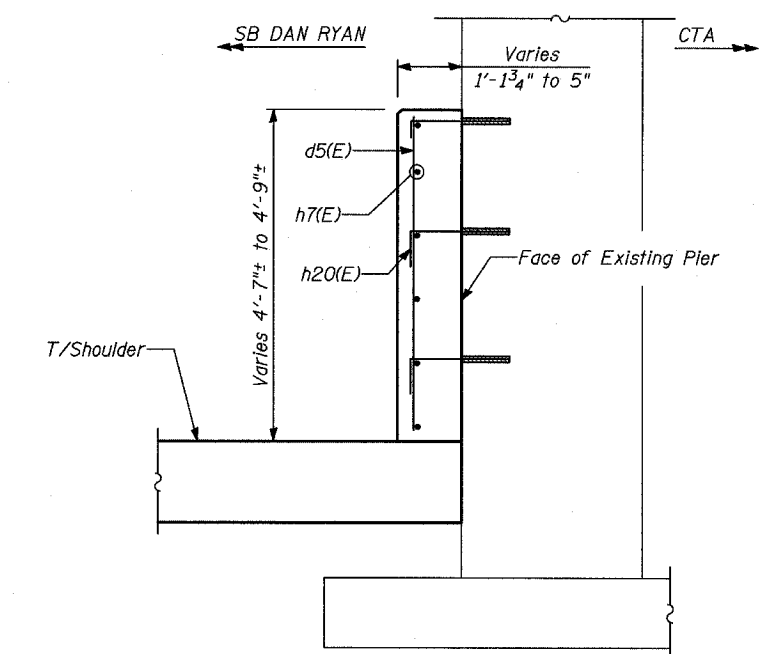
SECTION E-E



SECTION F-F



SECTION G-G



SECTION H-H

NOTE

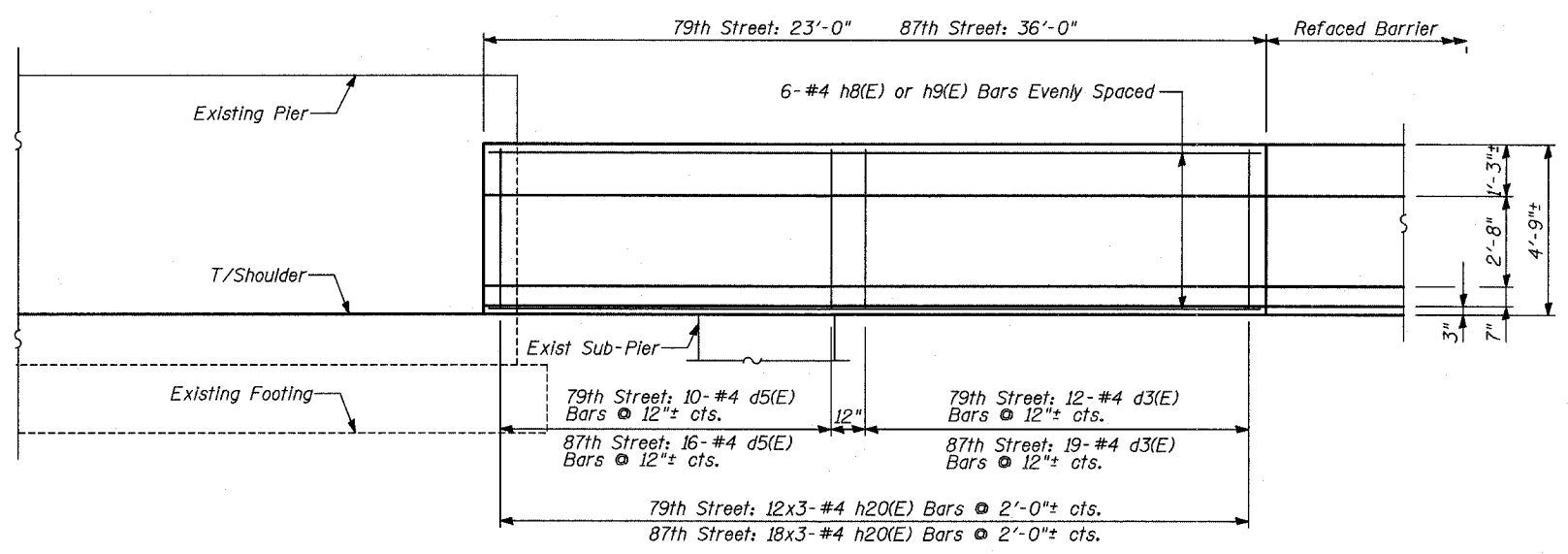
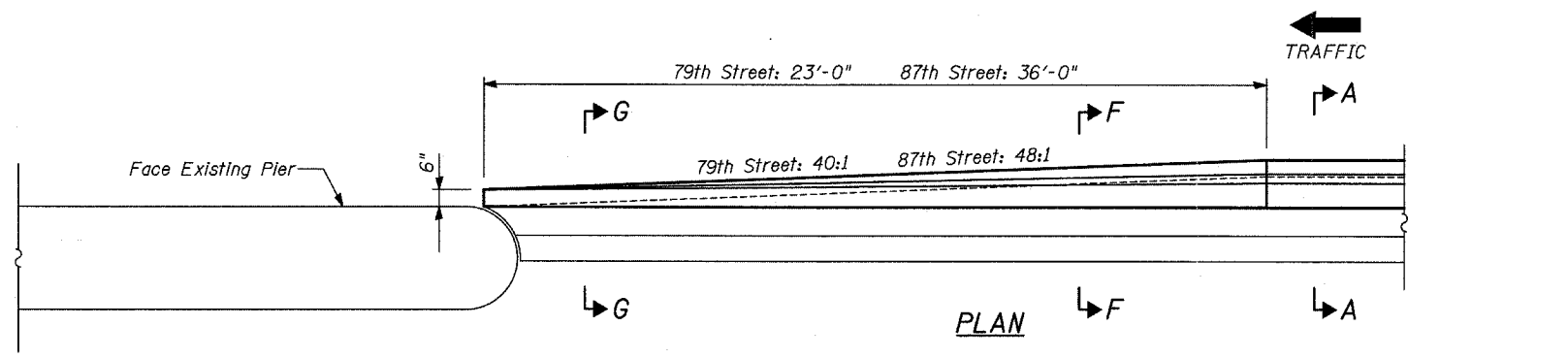
Drill and Epoxy Grout h3(E) and h20(E) Bars (5" Min. Embedment)

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 CTA STATION BARRIER WALL MODIFICATIONS
 79TH, 87TH AND 95TH STREET STATIONS
 REFACED BARRIER TO PIER TRANSITION
 DETAILS: TYPICAL SECTIONS

SCALE: NONE DRAWN BY: MPG
 DATE: MARCH 7, 2006 CHECKED BY: SP



ELEVATION

BILL OF MATERIAL

Bar	79th No.	87th No.	Size	Length	Shape
h8(E)	6	-	#4	22'-9"	—
h9(E)	-	6	#4	35'-9"	—
h20(E)	36	54	#4	1'-6"	┌
d3(E)	12	19	#4	4'-9"	—
d5(E)	10	16	#4	4'-4"	—
Item					
Unit					
79th Qty					
87th Qty					
Reinforcement Bars, Epoxy Coated		POUND	200	310	
Concrete Structures		CU YD	2	4	
Protective Coat		SQ YD	15	25	

NOTES:

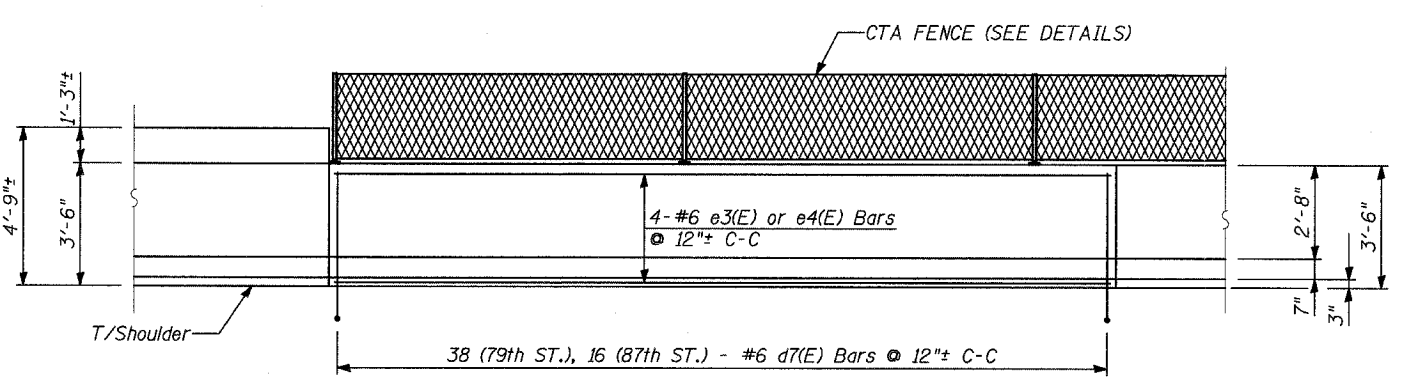
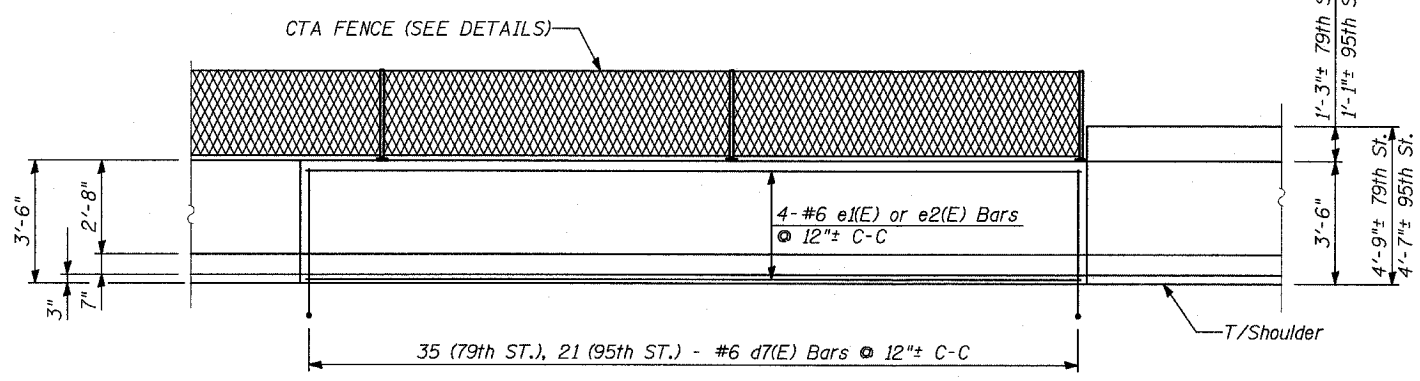
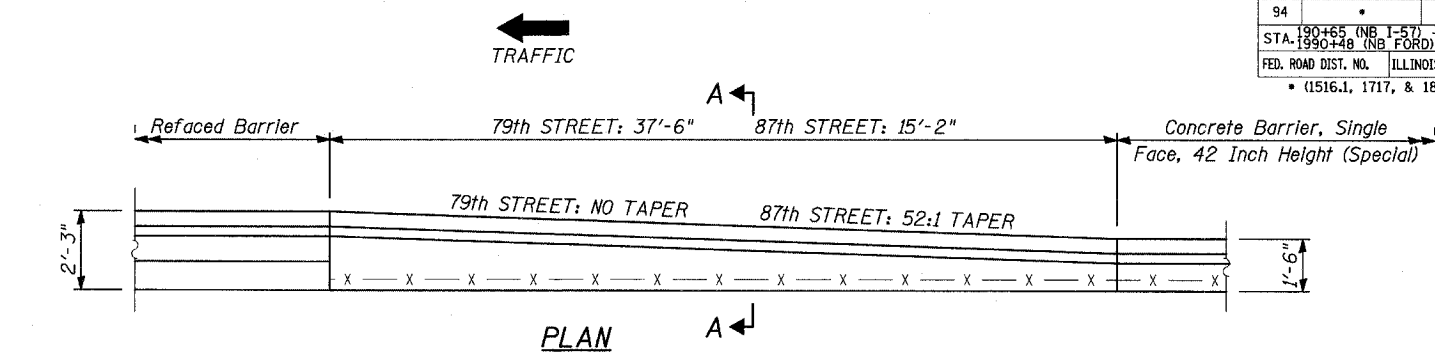
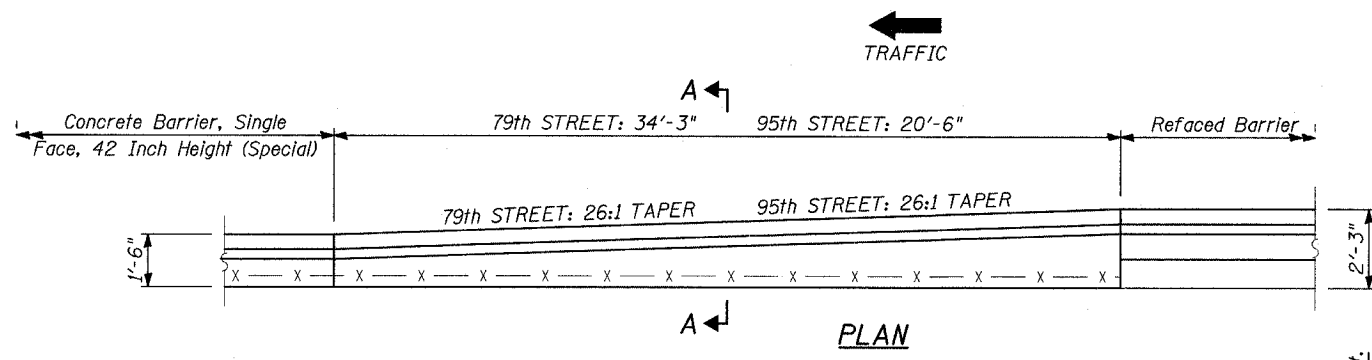
1. Drilling and grouting of #4 h20(E) bars shall be in accordance with Section 584 of the Standard Specifications.
2. The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h20(E) bars shall be included in the cost for Concrete Structures.
3. Provide joints in facing to match those in the existing barrier. See Sht B1 for joint details.
4. For Refaced Barrier details, see Sheet B2.
5. For Sections A-A thru H-H see Sheet B6.
6. For locations of Transition, see Roadway Plans.

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REVISIONS	
NAME	DATE

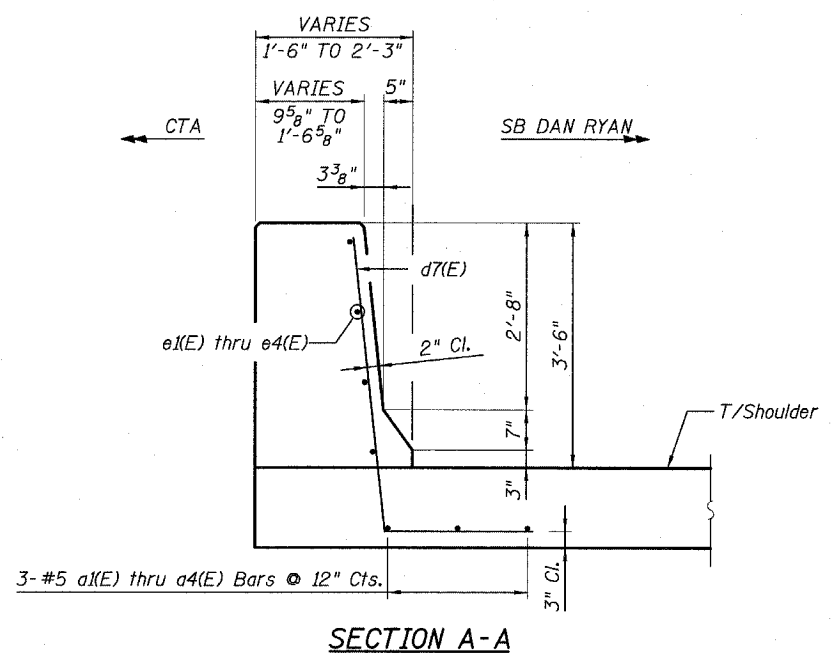
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 CTA STATION BARRIER WALL MODIFICATIONS
 79TH & 87TH STREET STATION
 REFACED BARRIER TO PIER TRANSITION
 DETAILS: PLAN, ELEVATION & TYPICAL SECTION

SCALE: NONE DRAWN BY: MPG
 DATE: MARCH 7, 2006 CHECKED BY: SP



ELEVATION

ELEVATION



SECTION A-A

BILL OF MATERIAL

Bar	79th N.	79th S.	87th	95th	Size	Length	Shape
a1(E)	3	-	-	-	#5	34'-0"	—
a2(E)	-	-	-	3	#5	20'-2"	—
a3(E)	-	3	-	-	#5	37'-3"	—
a4(E)	-	-	3	-	#5	14'-10"	—
d7(E)	34	38	16	21	#6	6'-3"	┘
e1(E)	4	-	-	-	#6	34'-0"	—
e2(E)	-	-	-	4	#6	20'-2"	—
e3(E)	-	4	-	-	#6	37'-3"	—
e4(E)	-	-	4	-	#6	14'-10"	—
Item	Unit	79th N.	79th S.	87th	95th		
Reinforcement Bars, Epoxy Coated	Pound	630	700	290	390		
Concrete Structures	Cu. Yd.	7	7	3	4		
Protective Coat	Sq. Yd.	19	21	9	12		

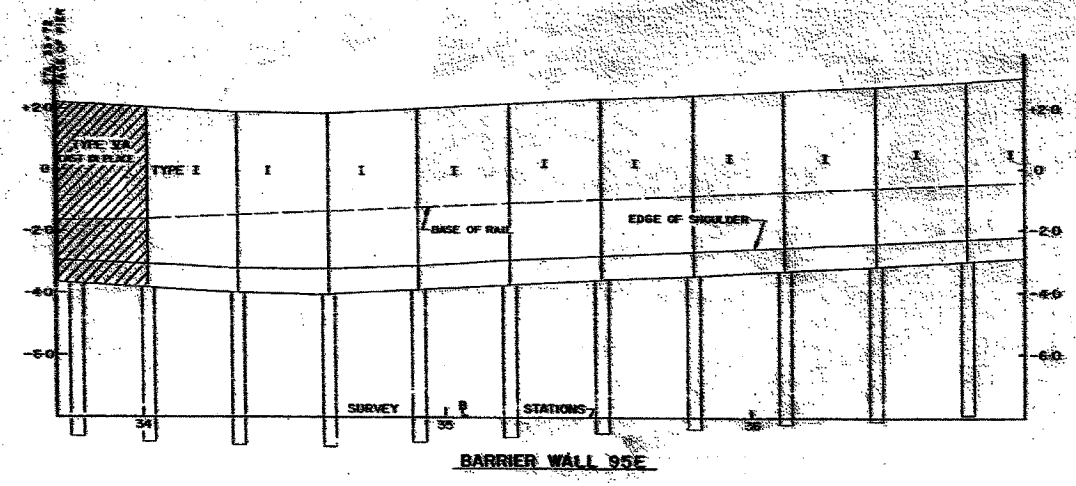
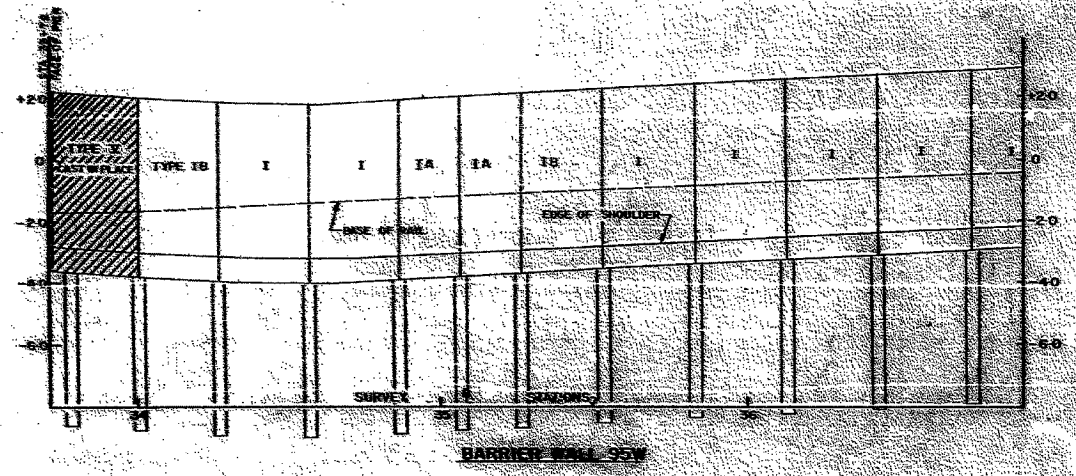
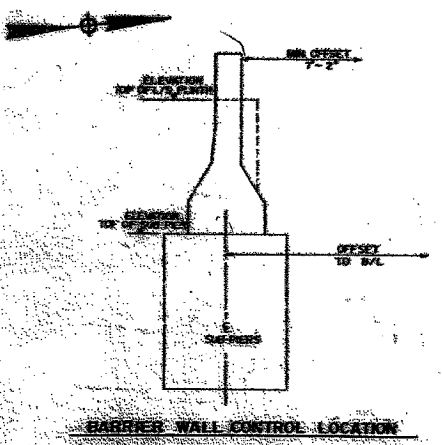
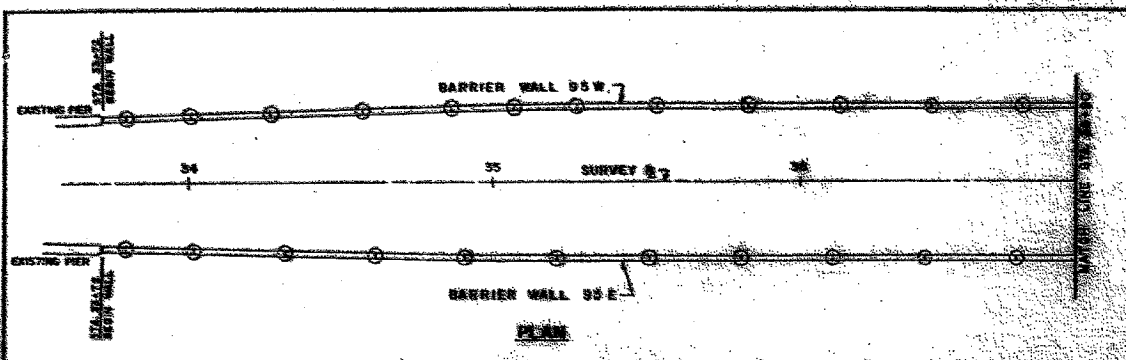
NOTES:

1. Provide joints in facing to match those in the Existing Barrier. See Sht B1 for joint details.
2. For Refaced Barrier details, see Sheet B2.
3. For Concrete Barrier, Single Face, 42" (Special), see Typical Section details.
5. For locations of Transition, see Roadway Plans.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 CTA STATION BARRIER WALL MODIFICATIONS
 79TH, 87TH AND 95TH STREET STATIONS
 REFACED BARRIER TO NEW 42 INCH BARRIER
 DETAILS: PLAN, ELEVATION & TYPICAL SECTION

SCALE: NONE DRAWN BY: MPG
 DATE: MARCH 7, 2006 CHECKED BY: SP



BARRIER WALL 95W

STATION	ELEVATION	OFFSET	ELEVATION	OFFSET	REMARKS
34+00	22.50	-2.00			
34+10	22.50	-2.00			
34+20	22.50	-2.00			
34+30	22.50	-2.00			
34+40	22.50	-2.00			
34+50	22.50	-2.00			
34+60	22.50	-2.00			
34+70	22.50	-2.00			
34+80	22.50	-2.00			
34+90	22.50	-2.00			
35+00	22.50	-2.00			
35+10	22.50	-2.00			
35+20	22.50	-2.00			
35+30	22.50	-2.00			
35+40	22.50	-2.00			
35+50	22.50	-2.00			
35+60	22.50	-2.00			
35+70	22.50	-2.00			
35+80	22.50	-2.00			
35+90	22.50	-2.00			
36+00	22.50	-2.00			

BARRIER WALL 95E

STATION	ELEVATION	OFFSET	ELEVATION	OFFSET	REMARKS
34+00	22.50	-2.00			
34+10	22.50	-2.00			
34+20	22.50	-2.00			
34+30	22.50	-2.00			
34+40	22.50	-2.00			
34+50	22.50	-2.00			
34+60	22.50	-2.00			
34+70	22.50	-2.00			
34+80	22.50	-2.00			
34+90	22.50	-2.00			
35+00	22.50	-2.00			
35+10	22.50	-2.00			
35+20	22.50	-2.00			
35+30	22.50	-2.00			
35+40	22.50	-2.00			
35+50	22.50	-2.00			
35+60	22.50	-2.00			
35+70	22.50	-2.00			
35+80	22.50	-2.00			
35+90	22.50	-2.00			
36+00	22.50	-2.00			

NOTES:
 ALL STRUCTURES IN THE SHOWN
 POSITION. IF OTHER WELLS OR
 STRUCTURES TO BE WELLS IN THE FIELD.
 FOR DETAILS OF LIGHTING STD.
 ARRANGEMENT SEE SHEET S-22

CITY OF CHICAGO
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF ENGINEERING
DAN RYAN RAPID TRANSIT
 MEDIAN PREPARATION & FENCING
 CONTRACT DR-8
 BARRIER WALL PLAN & PROFILES
 STA 33 + 72 TO STA 36+90
 HORIZ. 1" = 20'
 SCALE: VERT. 1" = 2'
 SHEET NO. 58 OF 211 SHEETS
 APRIL, 1968

In Charge: C.A.M.
 Designed By: J.L.
 Drawn By: J.L.
 Checked By: J.L.
 Approved: J.L.

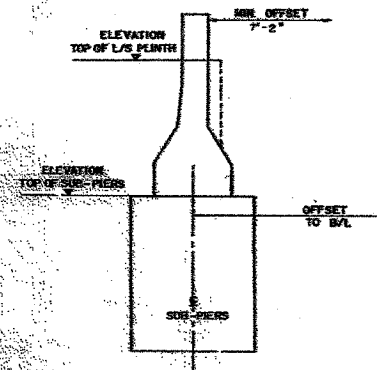
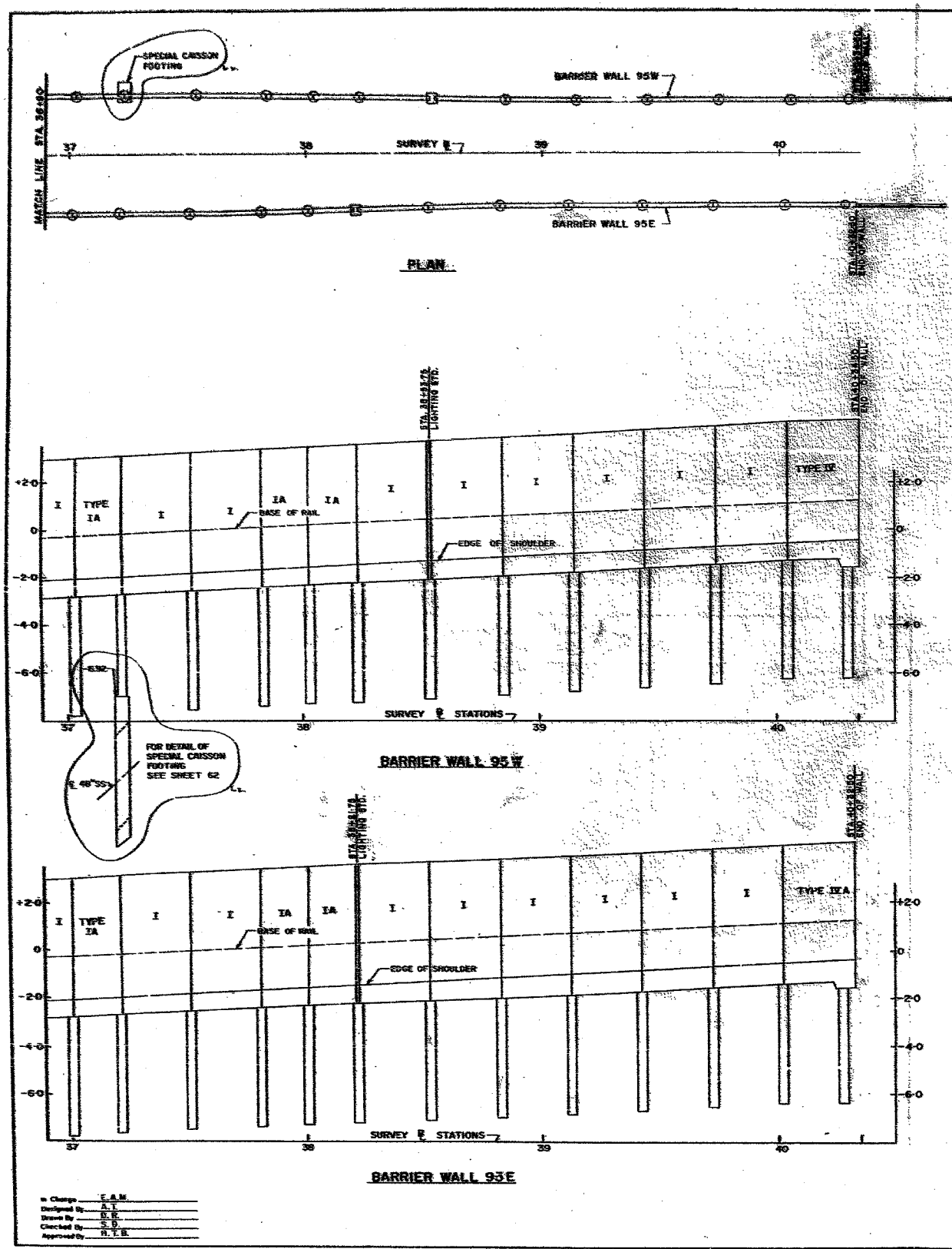
NO.	DESCRIPTION	DATE

TYLIN INTERNATIONAL

FOR INFORMATION ONLY

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING CONCRETE BARRIER AT
 CTA STATIONS:
 95TH STREET (1 OF 2)
 SCALE: NONE DRAWN BY: JUS
 DATE: MARCH 7, 2006 CHECKED BY: TGB



BARRIER WALL CONTROL LINE

NOTES:
 ALL STRUCTURES IN THE IMMEDIATE VICINITY OF THE BARRIER WALLS OR SUB-PIERS TO BE VERIFIED IN THE FIELD.
 FOR DETAILS OF EXISTING STD. ANCHORAGE SEE SHEETS 2-22

BARRIER WALL 95W

STATION TO SUB-PIERS	VERTICAL OFFSET TO ELEVATION TOP OF SUB-PIERS	ELEVATION TOP OF SUB-PIERS	ELEVATION TOP OF BARRIER WALL	REMARKS
37+00	28.27	-2.26		
37+10	28.27	-2.26		
37+20	28.27	-2.26		
37+30	28.27	-2.26		
37+40	28.27	-2.26		
37+50	28.27	-2.26		
37+60	28.27	-2.26		
37+70	28.27	-2.26	3.72	LIGHT STD.
37+80	28.27	-2.26		
37+90	28.27	-2.26		
38+00	28.27	-2.26		
38+10	28.27	-2.26		
38+20	28.27	-2.26		
38+30	28.27	-2.26		
38+40	28.27	-2.26		
38+50	28.27	-2.26		
38+60	28.27	-2.26		
38+70	28.27	-2.26		
38+80	28.27	-2.26		
38+90	28.27	-2.26		
39+00	28.27	-2.26		
39+10	28.27	-2.26		
39+20	28.27	-2.26		
39+30	28.27	-2.26		
39+40	28.27	-2.26		
39+50	28.27	-2.26		
39+60	28.27	-2.26		
39+70	28.27	-2.26		
39+80	28.27	-2.26		
39+90	28.27	-2.26		
40+00	28.27	-2.26		

BARRIER WALL 95E

STATION TO SUB-PIERS	VERTICAL OFFSET TO ELEVATION TOP OF SUB-PIERS	ELEVATION TOP OF SUB-PIERS	ELEVATION TOP OF BARRIER WALL	REMARKS
37+00	28.27	-2.26		
37+10	28.27	-2.26		
37+20	28.27	-2.26		
37+30	28.27	-2.26		
37+40	28.27	-2.26		
37+50	28.27	-2.26		
37+60	28.27	-2.26		
37+70	28.27	-2.26	3.07	LIGHT STD.
37+80	28.27	-2.26		
37+90	28.27	-2.26		
38+00	28.27	-2.26		
38+10	28.27	-2.26		
38+20	28.27	-2.26		
38+30	28.27	-2.26		
38+40	28.27	-2.26		
38+50	28.27	-2.26		
38+60	28.27	-2.26		
38+70	28.27	-2.26		
38+80	28.27	-2.26		
38+90	28.27	-2.26		
39+00	28.27	-2.26		
39+10	28.27	-2.26		
39+20	28.27	-2.26		
39+30	28.27	-2.26		
39+40	28.27	-2.26		
39+50	28.27	-2.26		
39+60	28.27	-2.26		
39+70	28.27	-2.26		
39+80	28.27	-2.26		
39+90	28.27	-2.26		
40+00	28.27	-2.26		

NOTE:
 ALL WALLS ARE TYPE I UNLESS OTHERWISE NOTED.

APPROVED AS WORKING PLAN
 REVISED WORKING PLAN
 2-28-06
 2-28-06

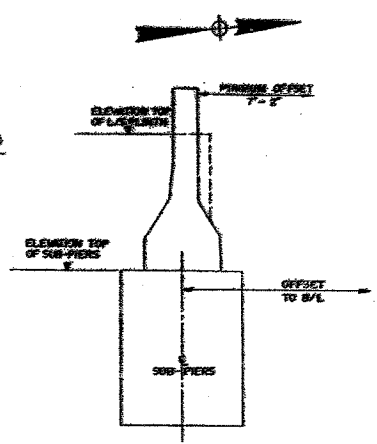
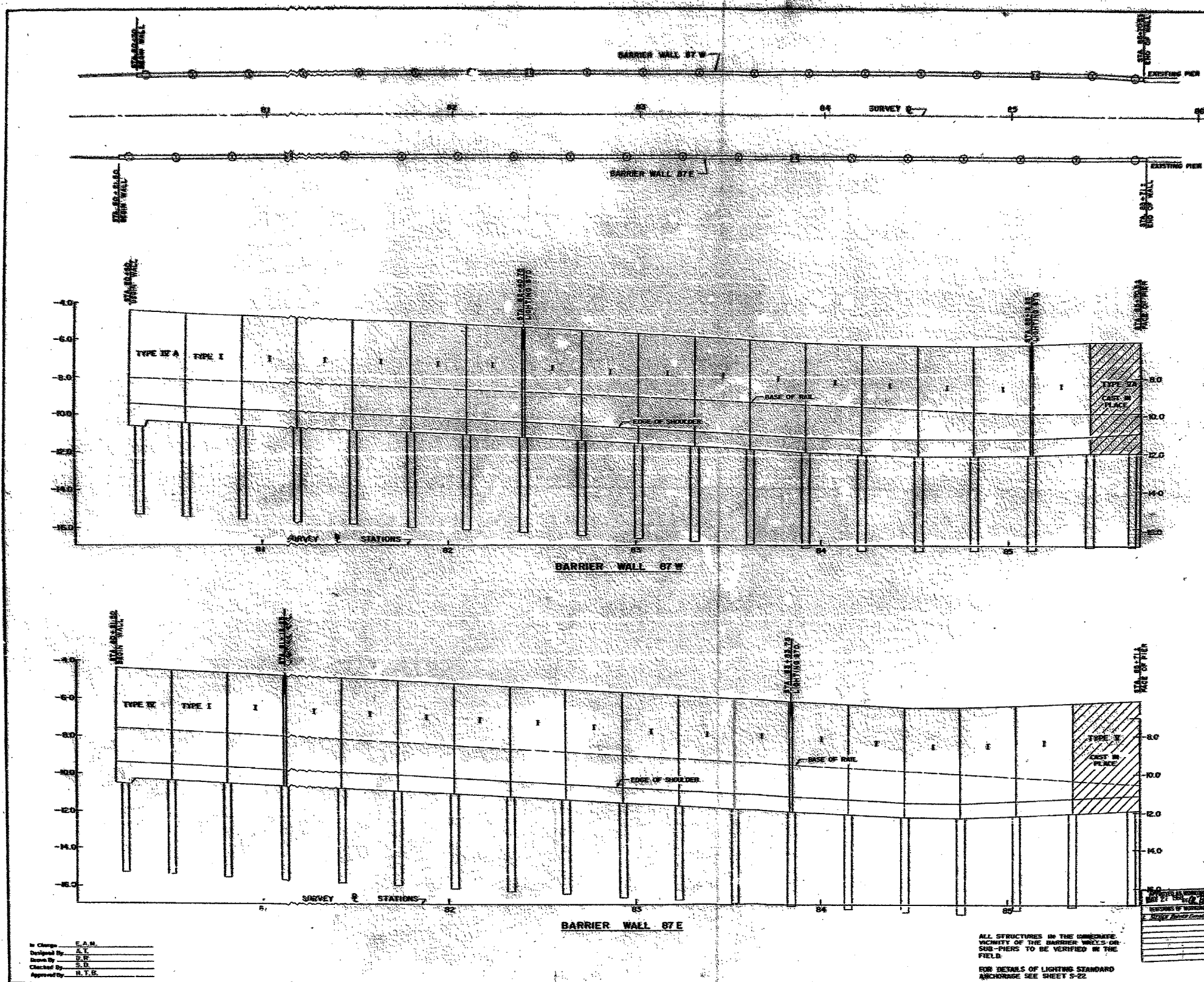
CITY OF CHICAGO
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF ENGINEERING
DAN RYAN RAPID TRANSIT
 MEDIAN PREPARATION & FENCING
 CONTRACT DR-8
BARRIER WALL PLAN & PROFILES
 STA. 36+90 TO STA 40+34-50
 95E & 95W
 SCALE: VERT. 1"=20'
 HORIZ. 1"=20'
 APRIL, 1968
 SHEET NO. 99 OF 211 SHEETS

TYLINT INTERNATIONAL

FOR INFORMATION ONLY

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING CONCRETE BARRIER AT
 CTA STATIONS:
 95TH STREET (2 OF 2)
 SCALE: NONE DRAWN BY: JJS
 DATE: MARCH 7, 2006 CHECKED BY: TGB



BARRIER WALL 87W

STATION OF SUB-PIERS	OFFSET FROM B/L TO ELEVATION OF TOP OF SUB-PIERS	ELEVATION OF TOP OF SUB-PIERS	SECTION TO TOP OF SUB-PIERS	TYPE OF WALL	REMARKS
80+21	22.00	10.00	10.00	TYPE IV	
80+22	22.00	10.00	10.00	TYPE IV	
80+23	22.00	10.00	10.00	TYPE IV	
80+24	22.00	10.00	10.00	TYPE IV	
80+25	22.00	10.00	10.00	TYPE IV	
80+26	22.00	10.00	10.00	TYPE IV	
80+27	22.00	10.00	10.00	TYPE IV	
80+28	22.00	10.00	10.00	TYPE IV	
80+29	22.00	10.00	10.00	TYPE IV	
80+30	22.00	10.00	10.00	TYPE IV	
80+31	22.00	10.00	10.00	TYPE IV	
80+32	22.00	10.00	10.00	TYPE IV	
80+33	22.00	10.00	10.00	TYPE IV	
80+34	22.00	10.00	10.00	TYPE IV	
80+35	22.00	10.00	10.00	TYPE IV	
80+36	22.00	10.00	10.00	TYPE IV	
80+37	22.00	10.00	10.00	TYPE IV	
80+38	22.00	10.00	10.00	TYPE IV	
80+39	22.00	10.00	10.00	TYPE IV	
80+40	22.00	10.00	10.00	TYPE IV	
80+41	22.00	10.00	10.00	TYPE IV	
80+42	22.00	10.00	10.00	TYPE IV	
80+43	22.00	10.00	10.00	TYPE IV	
80+44	22.00	10.00	10.00	TYPE IV	
80+45	22.00	10.00	10.00	TYPE IV	
80+46	22.00	10.00	10.00	TYPE IV	
80+47	22.00	10.00	10.00	TYPE IV	
80+48	22.00	10.00	10.00	TYPE IV	
80+49	22.00	10.00	10.00	TYPE IV	
80+50	22.00	10.00	10.00	TYPE IV	
80+51	22.00	10.00	10.00	TYPE IV	
80+52	22.00	10.00	10.00	TYPE IV	
80+53	22.00	10.00	10.00	TYPE IV	
80+54	22.00	10.00	10.00	TYPE IV	
80+55	22.00	10.00	10.00	TYPE IV	
80+56	22.00	10.00	10.00	TYPE IV	
80+57	22.00	10.00	10.00	TYPE IV	
80+58	22.00	10.00	10.00	TYPE IV	
80+59	22.00	10.00	10.00	TYPE IV	
80+60	22.00	10.00	10.00	TYPE IV	
80+61	22.00	10.00	10.00	TYPE IV	
80+62	22.00	10.00	10.00	TYPE IV	
80+63	22.00	10.00	10.00	TYPE IV	
80+64	22.00	10.00	10.00	TYPE IV	
80+65	22.00	10.00	10.00	TYPE IV	
80+66	22.00	10.00	10.00	TYPE IV	
80+67	22.00	10.00	10.00	TYPE IV	
80+68	22.00	10.00	10.00	TYPE IV	
80+69	22.00	10.00	10.00	TYPE IV	
80+70	22.00	10.00	10.00	TYPE IV	
80+71	22.00	10.00	10.00	TYPE IV	

BARRIER WALL 87E

STATION OF SUB-PIERS	OFFSET FROM B/L TO ELEVATION OF TOP OF SUB-PIERS	ELEVATION OF TOP OF SUB-PIERS	SECTION TO TOP OF SUB-PIERS	TYPE OF WALL	REMARKS
80+21	22.00	10.00	10.00	TYPE IV	
80+22	22.00	10.00	10.00	TYPE IV	
80+23	22.00	10.00	10.00	TYPE IV	
80+24	22.00	10.00	10.00	TYPE IV	
80+25	22.00	10.00	10.00	TYPE IV	
80+26	22.00	10.00	10.00	TYPE IV	
80+27	22.00	10.00	10.00	TYPE IV	
80+28	22.00	10.00	10.00	TYPE IV	
80+29	22.00	10.00	10.00	TYPE IV	
80+30	22.00	10.00	10.00	TYPE IV	
80+31	22.00	10.00	10.00	TYPE IV	
80+32	22.00	10.00	10.00	TYPE IV	
80+33	22.00	10.00	10.00	TYPE IV	
80+34	22.00	10.00	10.00	TYPE IV	
80+35	22.00	10.00	10.00	TYPE IV	
80+36	22.00	10.00	10.00	TYPE IV	
80+37	22.00	10.00	10.00	TYPE IV	
80+38	22.00	10.00	10.00	TYPE IV	
80+39	22.00	10.00	10.00	TYPE IV	
80+40	22.00	10.00	10.00	TYPE IV	
80+41	22.00	10.00	10.00	TYPE IV	
80+42	22.00	10.00	10.00	TYPE IV	
80+43	22.00	10.00	10.00	TYPE IV	
80+44	22.00	10.00	10.00	TYPE IV	
80+45	22.00	10.00	10.00	TYPE IV	
80+46	22.00	10.00	10.00	TYPE IV	
80+47	22.00	10.00	10.00	TYPE IV	
80+48	22.00	10.00	10.00	TYPE IV	
80+49	22.00	10.00	10.00	TYPE IV	
80+50	22.00	10.00	10.00	TYPE IV	
80+51	22.00	10.00	10.00	TYPE IV	
80+52	22.00	10.00	10.00	TYPE IV	
80+53	22.00	10.00	10.00	TYPE IV	
80+54	22.00	10.00	10.00	TYPE IV	
80+55	22.00	10.00	10.00	TYPE IV	
80+56	22.00	10.00	10.00	TYPE IV	
80+57	22.00	10.00	10.00	TYPE IV	
80+58	22.00	10.00	10.00	TYPE IV	
80+59	22.00	10.00	10.00	TYPE IV	
80+60	22.00	10.00	10.00	TYPE IV	
80+61	22.00	10.00	10.00	TYPE IV	
80+62	22.00	10.00	10.00	TYPE IV	
80+63	22.00	10.00	10.00	TYPE IV	
80+64	22.00	10.00	10.00	TYPE IV	
80+65	22.00	10.00	10.00	TYPE IV	
80+66	22.00	10.00	10.00	TYPE IV	
80+67	22.00	10.00	10.00	TYPE IV	
80+68	22.00	10.00	10.00	TYPE IV	
80+69	22.00	10.00	10.00	TYPE IV	
80+70	22.00	10.00	10.00	TYPE IV	
80+71	22.00	10.00	10.00	TYPE IV	

ALL STRUCTURES IN THE IMMEDIATE VICINITY OF THE BARRIER WALLS OR SUB-PIERS TO BE VERIFIED IN THE FIELD.
 FOR DETAILS OF LIGHTING STANDARD ANCHORAGE SEE SHEET S-22
 SCALE: HORIZ. - 1" = 20'
 VERT. - 1" = 2'
 SHEET NO. 510 OF 211 SHEETS
 APRIL, 1968

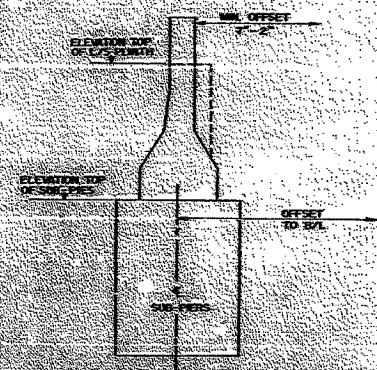
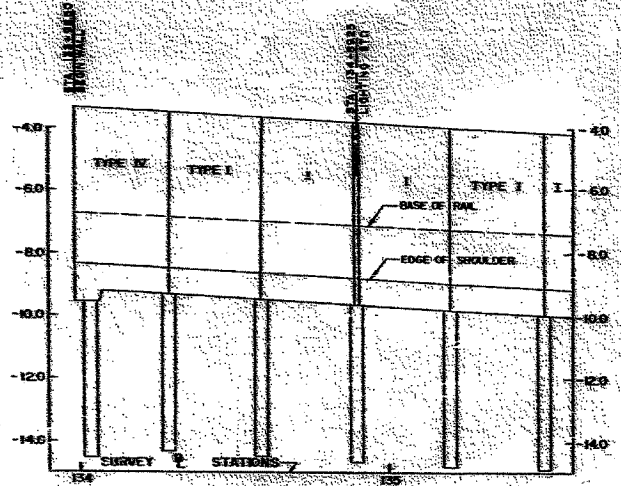
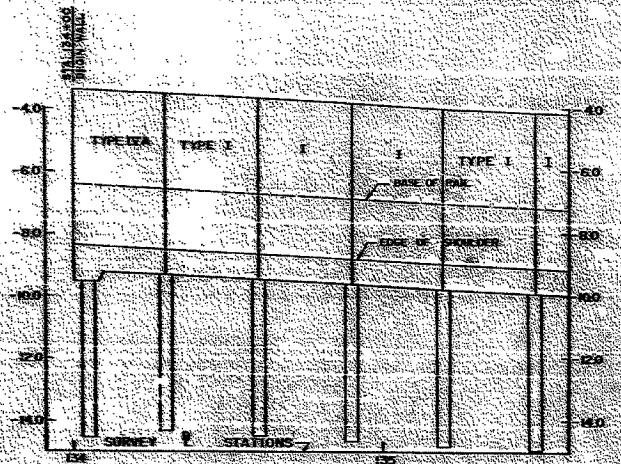
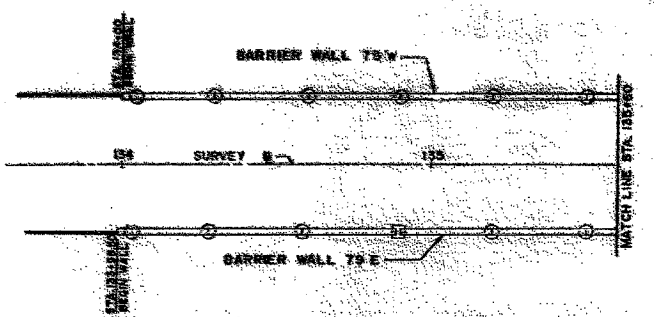
In Charge: E.A.M.
 Designed By: S.T.
 Drawn By: D.D.
 Checked By: S.D.
 Approved By: R.T.B.

TYLIN INTERNATIONAL

FOR INFORMATION ONLY

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING CONCRETE BARRIER AT
 CTA STATIONS:
 87TH STREET (1 OF 1)
 SCALE: NONE DRAWN BY: JJS
 DATE: MARCH 7, 2006 CHECKED BY: TGB



NOTES:
 ALL DIMENSIONS IN THE UNLESS OTHERWISE SPECIFIED IN THE BARRIER WALL TYPE OR THIS SHEET TO BE MEASURED FROM THE FIELD FOR DETAILS OF EXISTING STR. AND CROSSING SEE SHEET S-22

BARRIER WALL 79W

STATION	TYPE	HEIGHT	WIDTH	REMARKS
134+00	TYPE I	4'-0"	4'-0"	
134+10	TYPE I	4'-0"	4'-0"	
134+20	TYPE I	4'-0"	4'-0"	
134+30	TYPE I	4'-0"	4'-0"	
134+40	TYPE I	4'-0"	4'-0"	
134+50	TYPE I	4'-0"	4'-0"	
134+60	TYPE I	4'-0"	4'-0"	
134+70	TYPE I	4'-0"	4'-0"	
134+80	TYPE I	4'-0"	4'-0"	
134+90	TYPE I	4'-0"	4'-0"	
135+00	TYPE I	4'-0"	4'-0"	

BARRIER WALL 79E

STATION	TYPE	HEIGHT	WIDTH	REMARKS
134+00	TYPE I	4'-0"	4'-0"	
134+10	TYPE I	4'-0"	4'-0"	
134+20	TYPE I	4'-0"	4'-0"	
134+30	TYPE I	4'-0"	4'-0"	
134+40	TYPE I	4'-0"	4'-0"	
134+50	TYPE I	4'-0"	4'-0"	
134+60	TYPE I	4'-0"	4'-0"	
134+70	TYPE I	4'-0"	4'-0"	
134+80	TYPE I	4'-0"	4'-0"	
134+90	TYPE I	4'-0"	4'-0"	
135+00	TYPE I	4'-0"	4'-0"	

In Charge: E.A.M.
 Designed By: A.T.
 Drawn By: P.B.
 Checked By: S.D.
 Approved By: H.T.B.

APPROVED AS SHOWN PLAN
 DATE: 4/1/68
 REVISIONS OF WORKING PLAN
 SHEET: 195 of 211

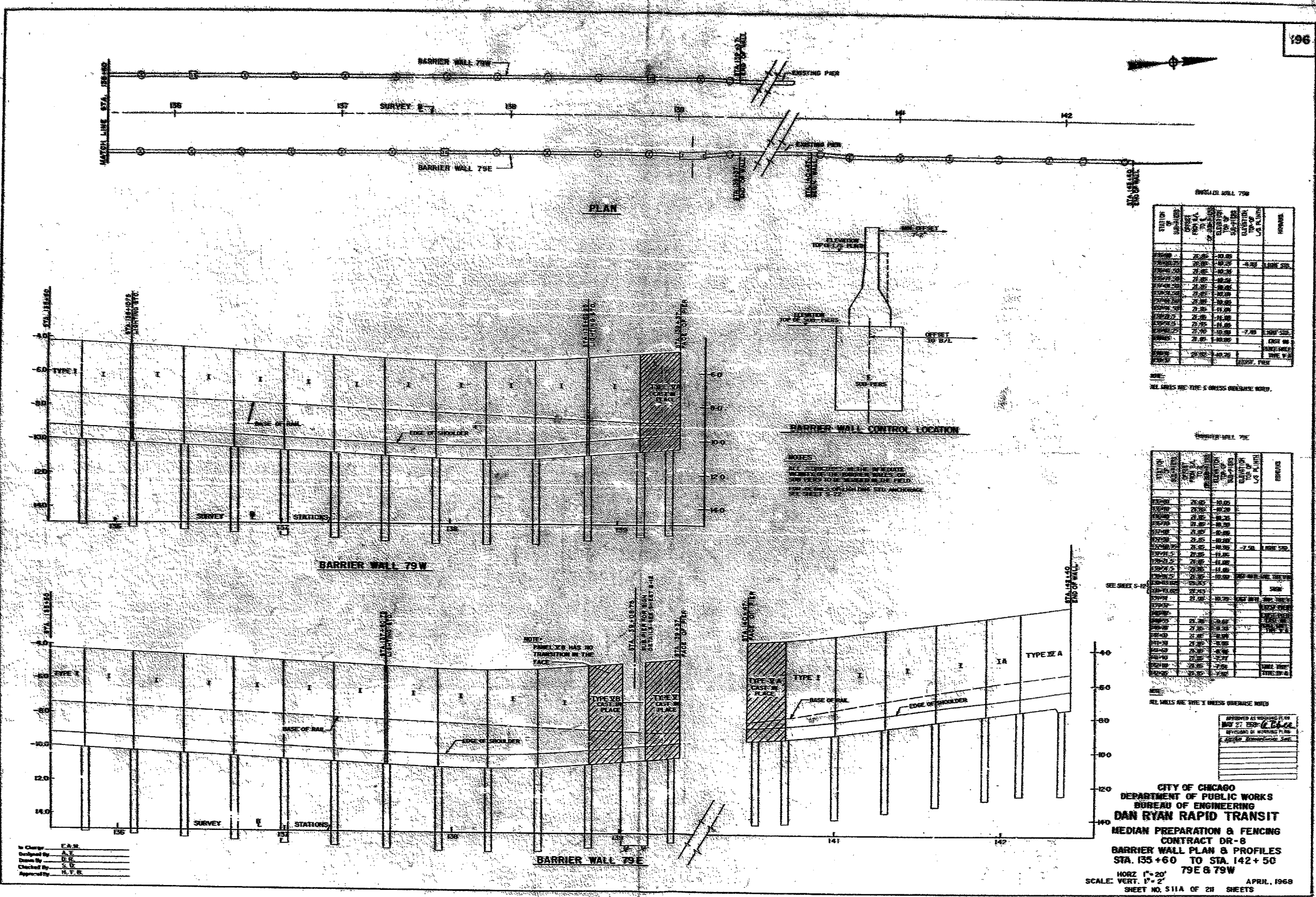
CITY OF CHICAGO
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF ENGINEERING
DAN RYAN RAPID TRANSIT
 MEDIAN PREPARATION & FENCING
 CONTRACT DR-8
 BARRIER WALL PLAN & PROFILES
 STA. 133+98.50 TO STA. 135+60
 SCALE: HORZ 1" = 20' 79E & 79W
 VERT 1" = 2' APRIL, 1968
 SHEET NO. 8 II OF 211 SHEETS

TYLIN INTERNATIONAL

FOR INFORMATION ONLY

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING CONCRETE BARRIER AT
 CTA STATIONS:
 79TH STREET (1 OF 2)
 SCALE: NONE DRAWN BY: JJS
 DATE: MARCH 7, 2006 CHECKED BY: TGB



In Charge: C.A.M.
 Designed By: A.T.
 Drawn By: B.E.
 Checked By: S.D.
 Approved By: H.V.B.

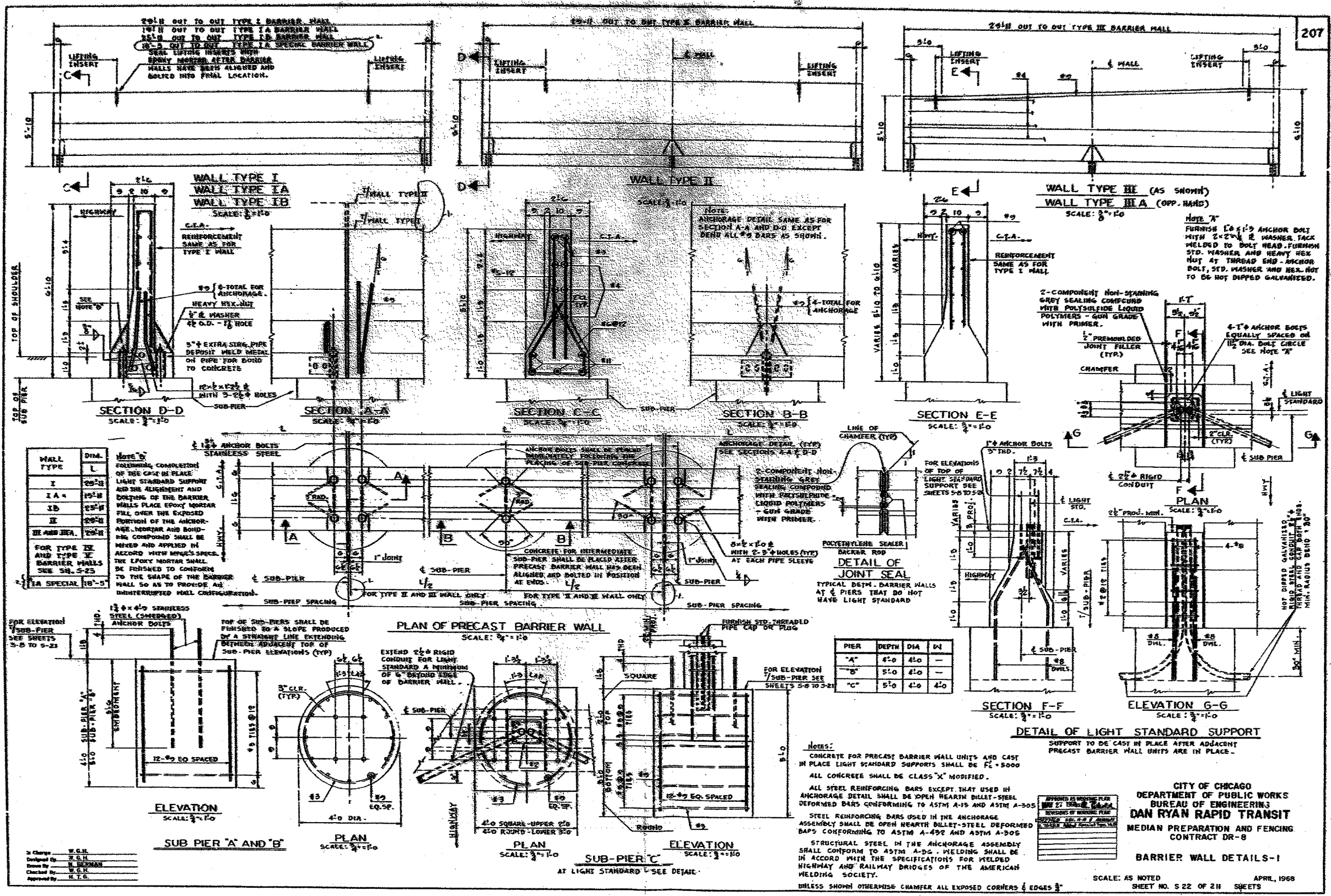
CITY OF CHICAGO
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF ENGINEERING
DAN RYAN RAPID TRANSIT
 MEDIAN PREPARATION & FENCING
 CONTRACT DR-8
 BARRIER WALL PLAN & PROFILES
 STA. 135+60 TO STA. 142+50
 79E & 79W
 HORZ. 1" = 20'
 VERT. 1" = 2'
 APRIL, 1968
 SHEET NO. 511A OF 211 SHEETS

FOR INFORMATION ONLY

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING CONCRETE BARRIER AT
 CTA STATIONS:
 79TH STREET (2 OF 2)
 SCALE: NONE DRAWN BY: JJS
 DATE: MARCH 7, 2006 CHECKED BY: TGB

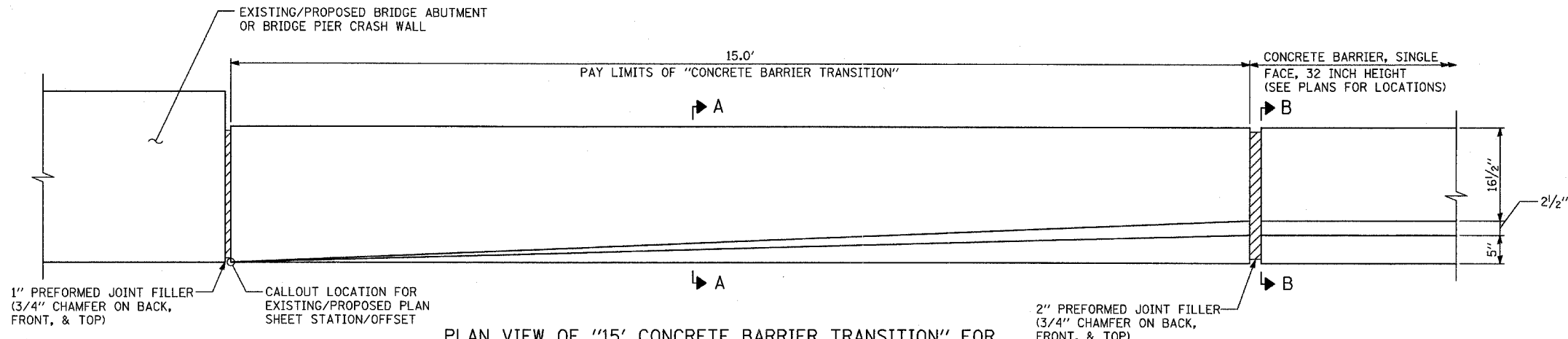


TYLIN INTERNATIONAL

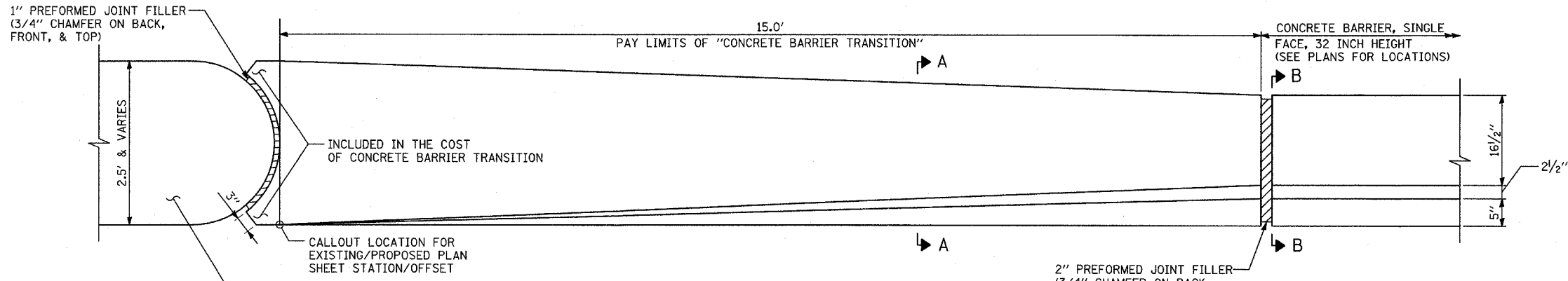
FOR INFORMATION ONLY

REVISIONS	
NAME	DATE

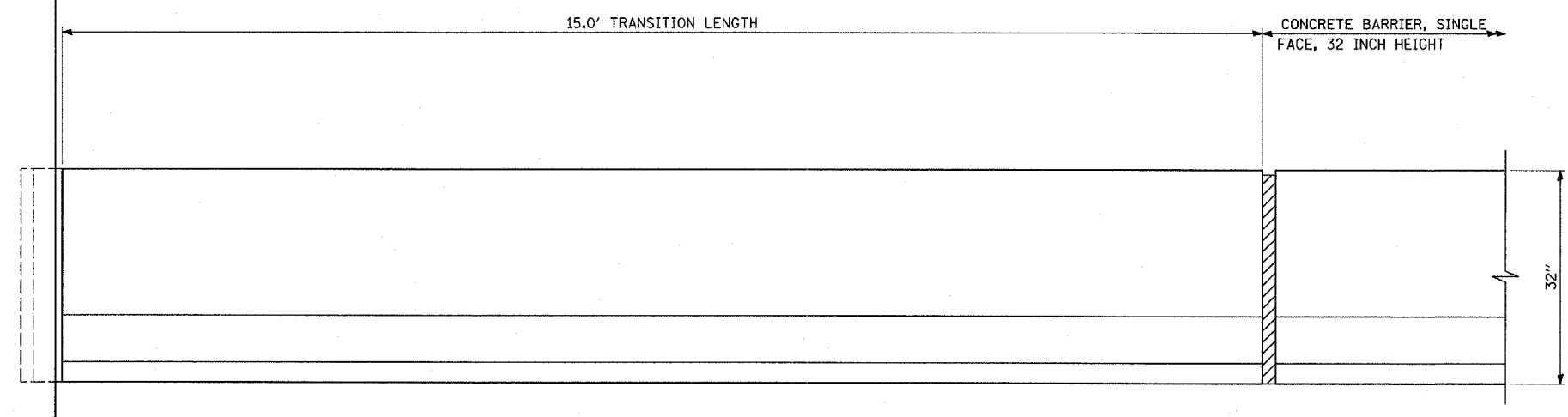
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING CONCRETE BARRIER AT CTA STATIONS
 BARRIER DETAILS
 SCALE: NONE DRAWN BY: JJS
 DATE: MARCH 7, 2006 CHECKED BY: TGB



PLAN VIEW OF "15' CONCRETE BARRIER TRANSITION" FOR CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT
ADJACENT TO SQUARE PIER/ABUTMENT



PLAN VIEW OF "15' CONCRETE BARRIER TRANSITION" FOR CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT
ADJACENT TO ROUND PIER/ABUTMENT



ELEVATION VIEW OF "15' CONCRETE BARRIER TRANSITION" FOR CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT

NOTE:
ALL PREFORMED JOINT FILLER INCLUDED IN THE COST OF "CONCRETE BARRIER TRANSITION"

TYLIN INTERNATIONAL

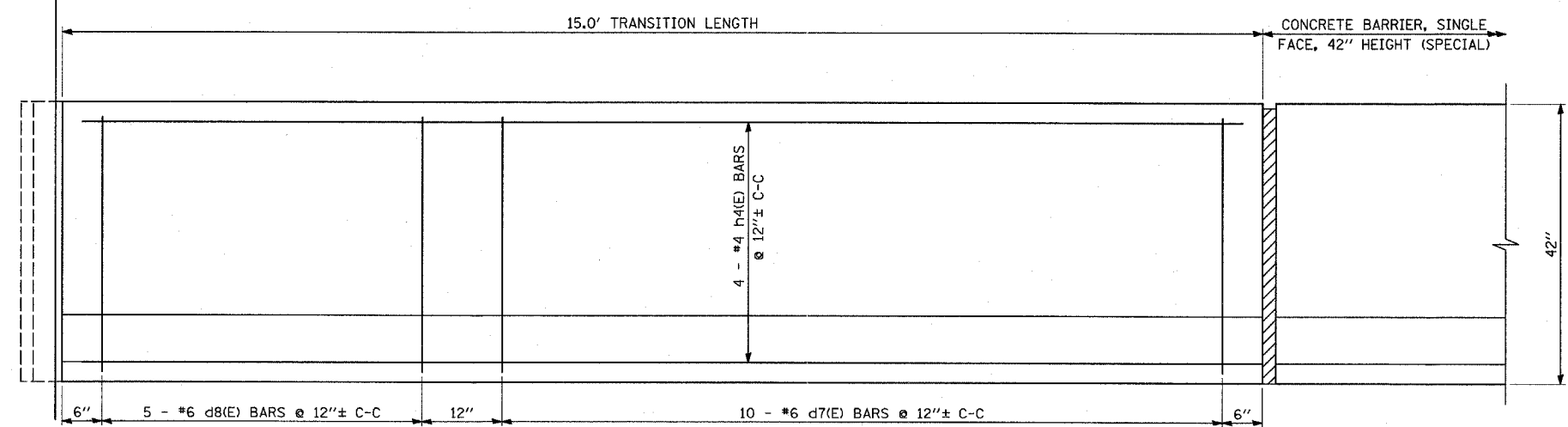
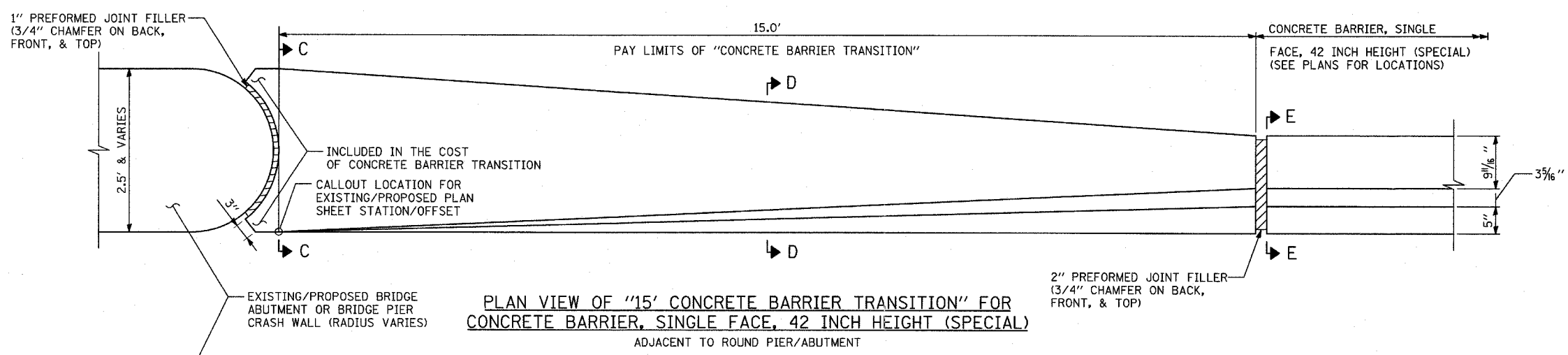
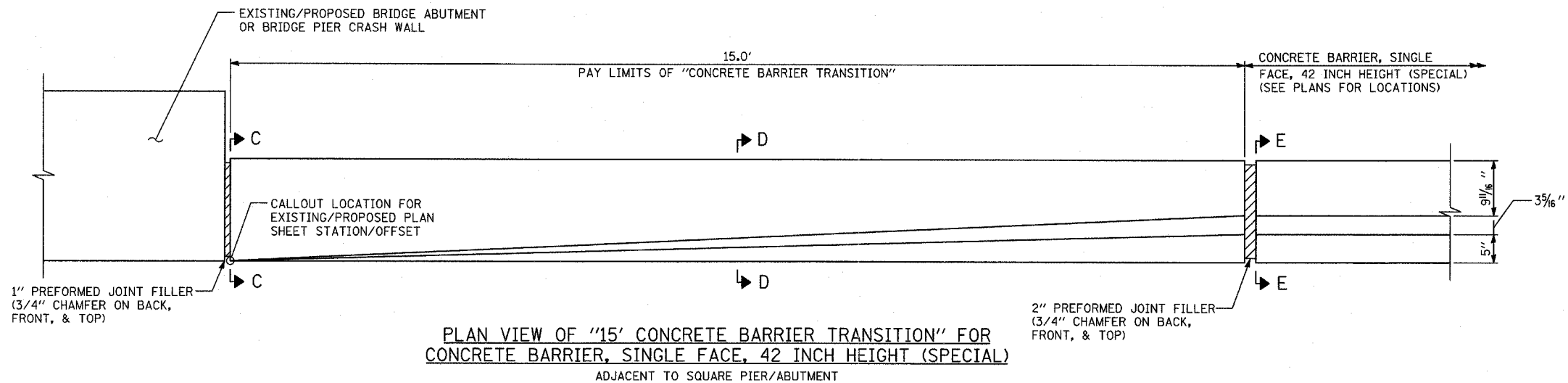
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS:
CONCRETE BARRIER TRANSITION
FOR CONCRETE BARRIER, 32 INCH HEIGHT

SCALE: NONE
DATE: MARCH 7, 2006

DRAWN BY: MPG
CHECKED BY: TGB



NOTE:
 ALL PREFORMED JOINT FILLER INCLUDED IN THE COST OF "CONCRETE BARRIER TRANSITION"

TYLIN INTERNATIONAL

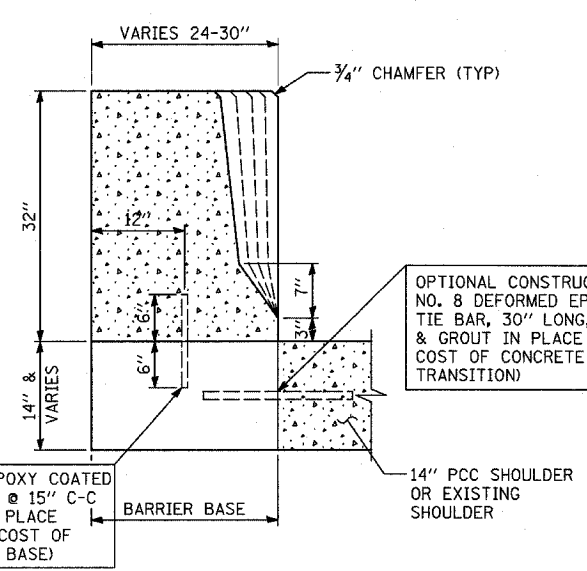
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

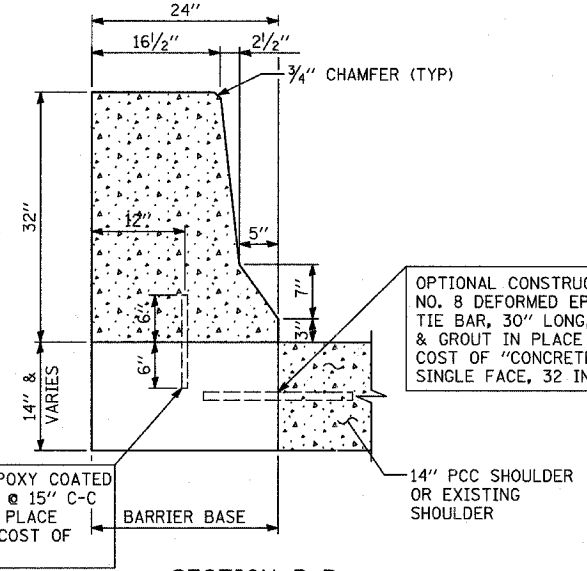
MISCELLANEOUS DETAILS:
 CONCRETE BARRIER TRANSITION FOR CONCRETE BARRIER, 42 INCH HEIGHT (SPECIAL)

SCALE: NONE
 DATE: MARCH 7, 2006

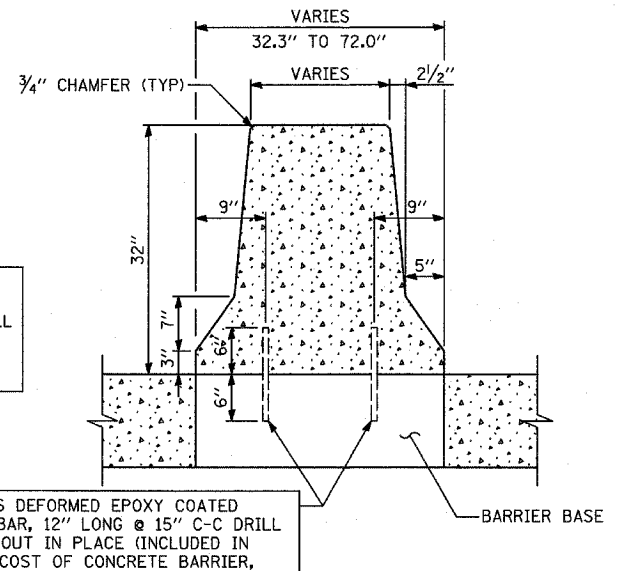
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 CHECKED BY: TGB



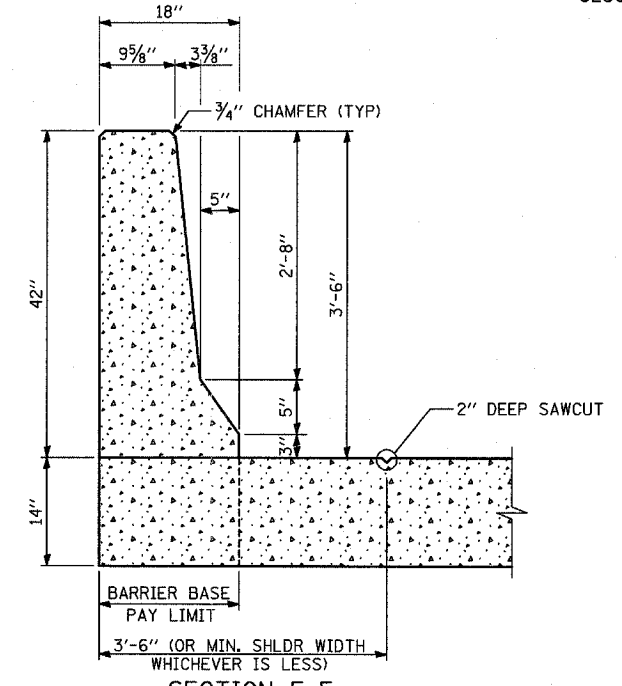
SECTION A-A
15' CONCRETE BARRIER TRANSITION
 CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT



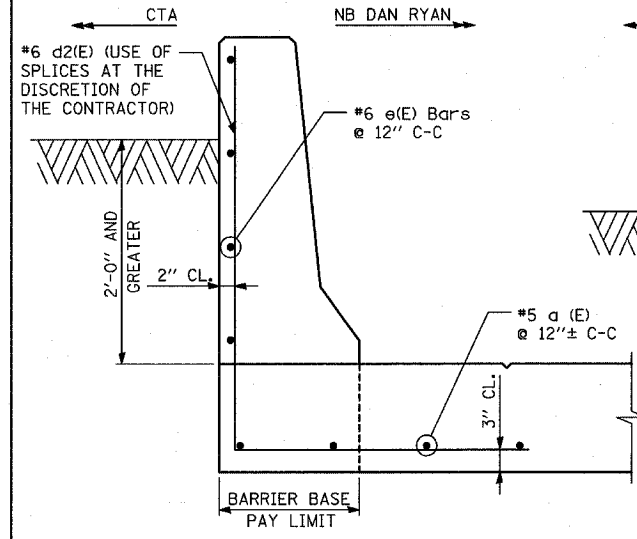
SECTION B-B
CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT
 & CONCRETE BARRIER BASE



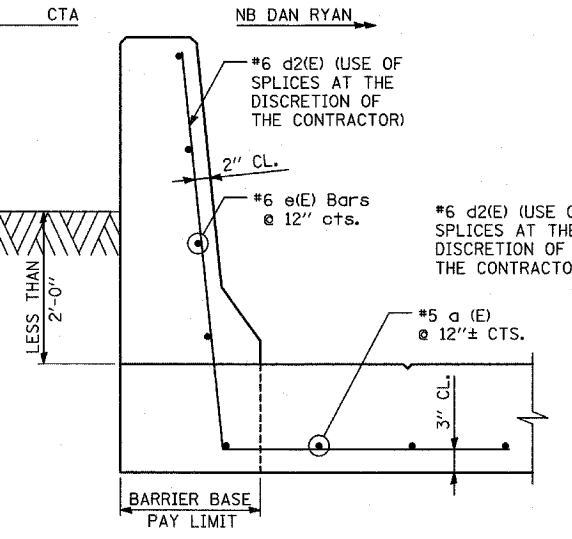
SECTION C-C
CONCRETE BARRIER, DOUBLE FACE, SPECIAL &
BARRIER BASE



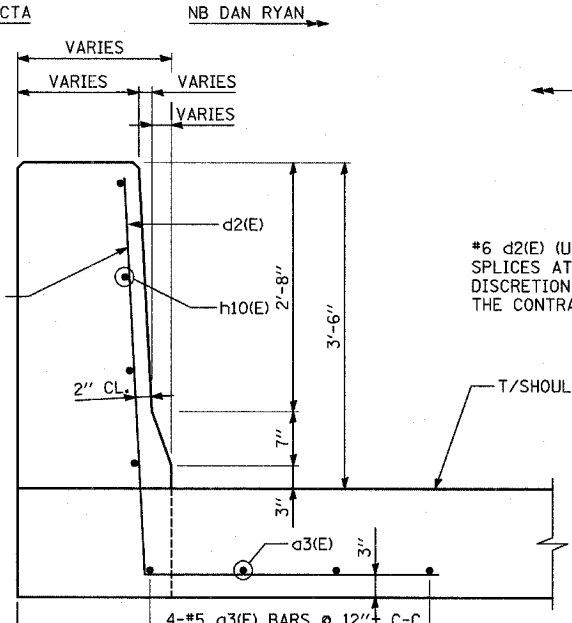
SECTION E-E
CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT
(SPECIAL) DIMENSIONS
 SEE BELOW FOR REINFORCEMENT OPTIONS



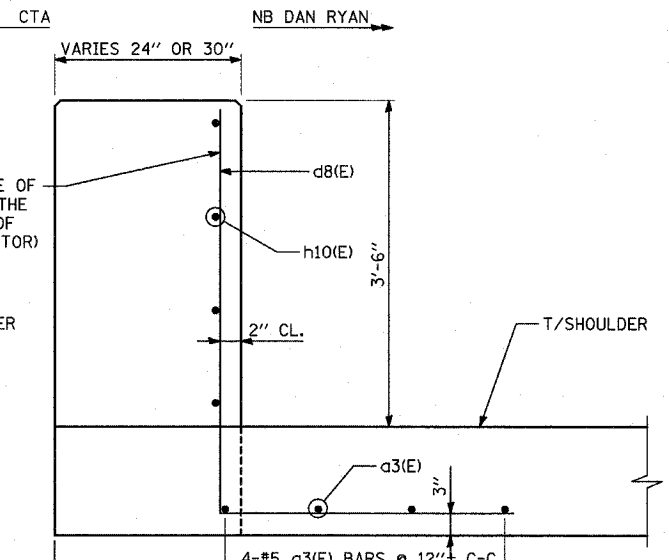
REINFORCEMENT #1
 RETAINED HEIGHT GREATER THAN 2'-0"
SECTION E-E: CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)



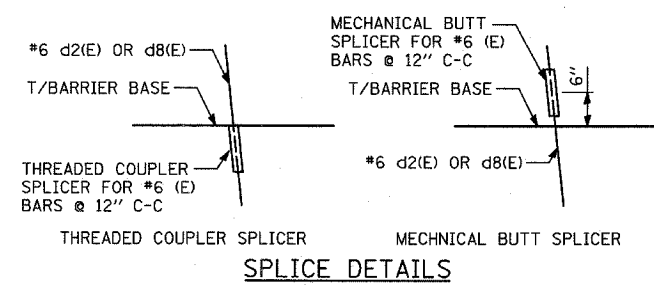
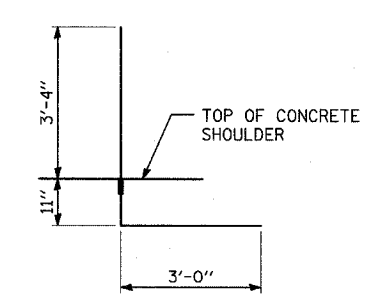
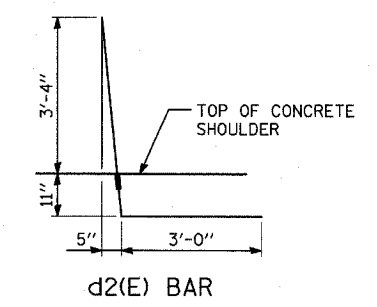
REINFORCEMENT #2
 RETAINED HEIGHT LESS THAN 2'-0"



SECTION D-D
15' CONCRETE BARRIER TRANSITION
 CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)

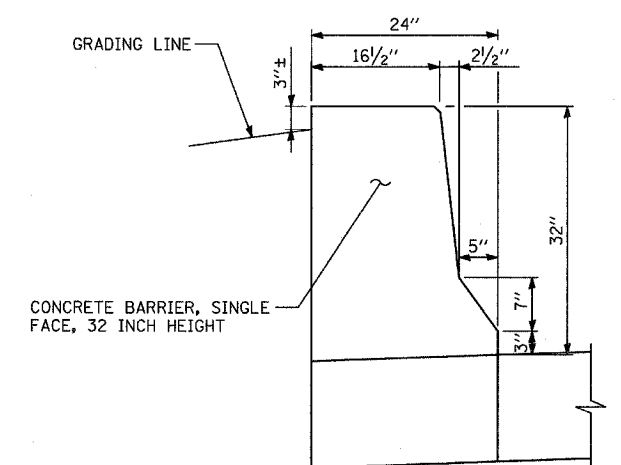
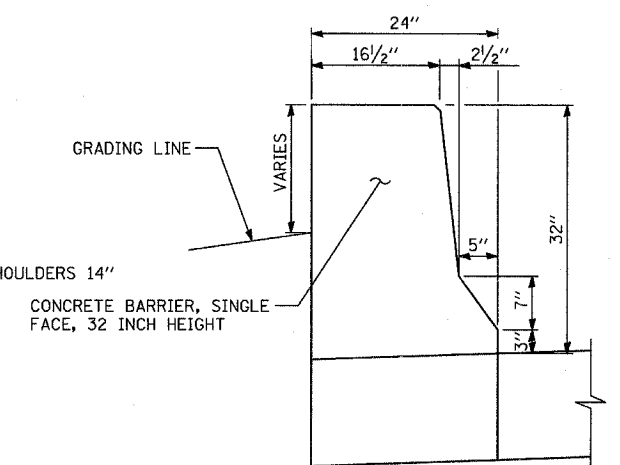
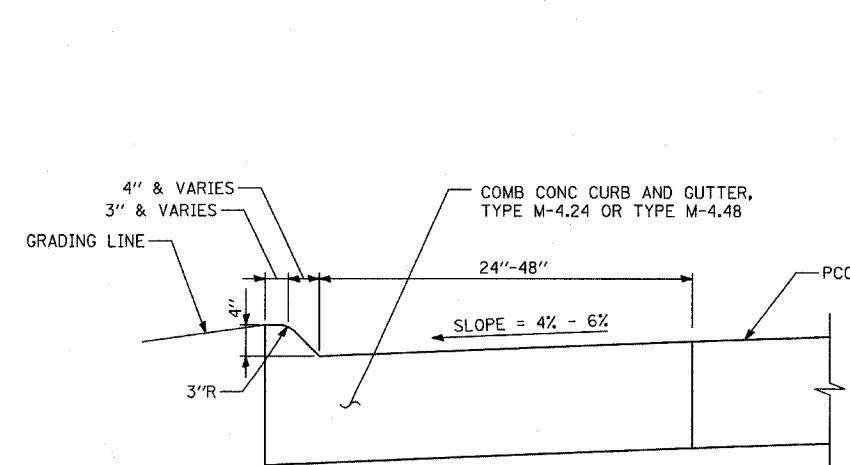
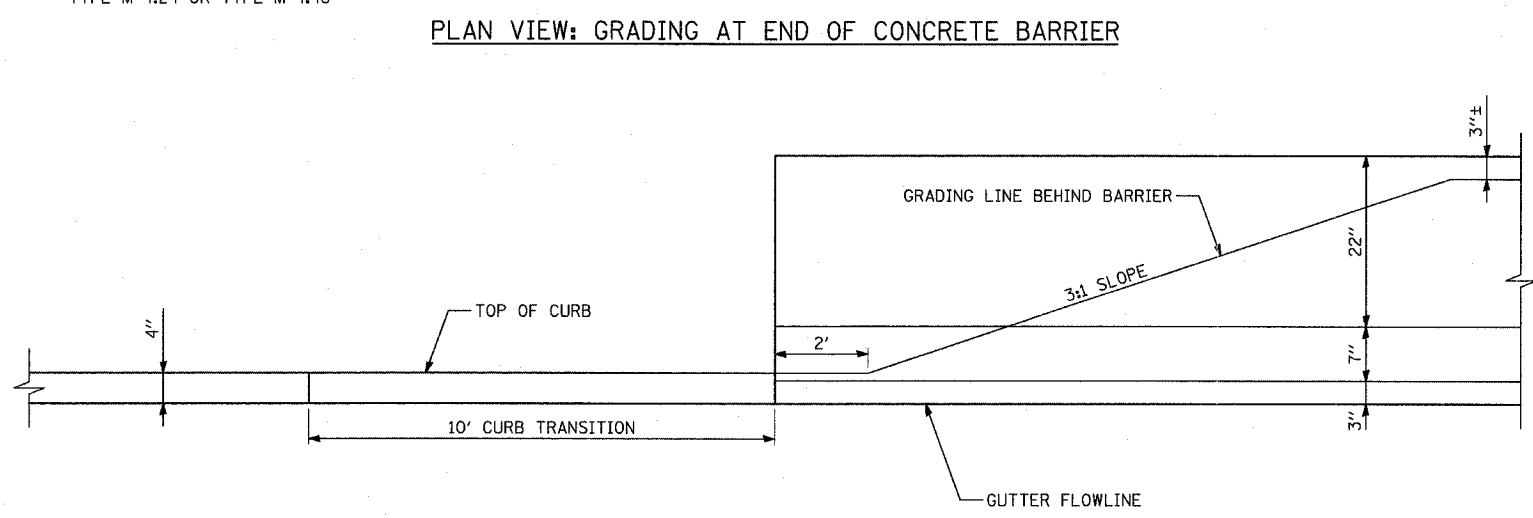
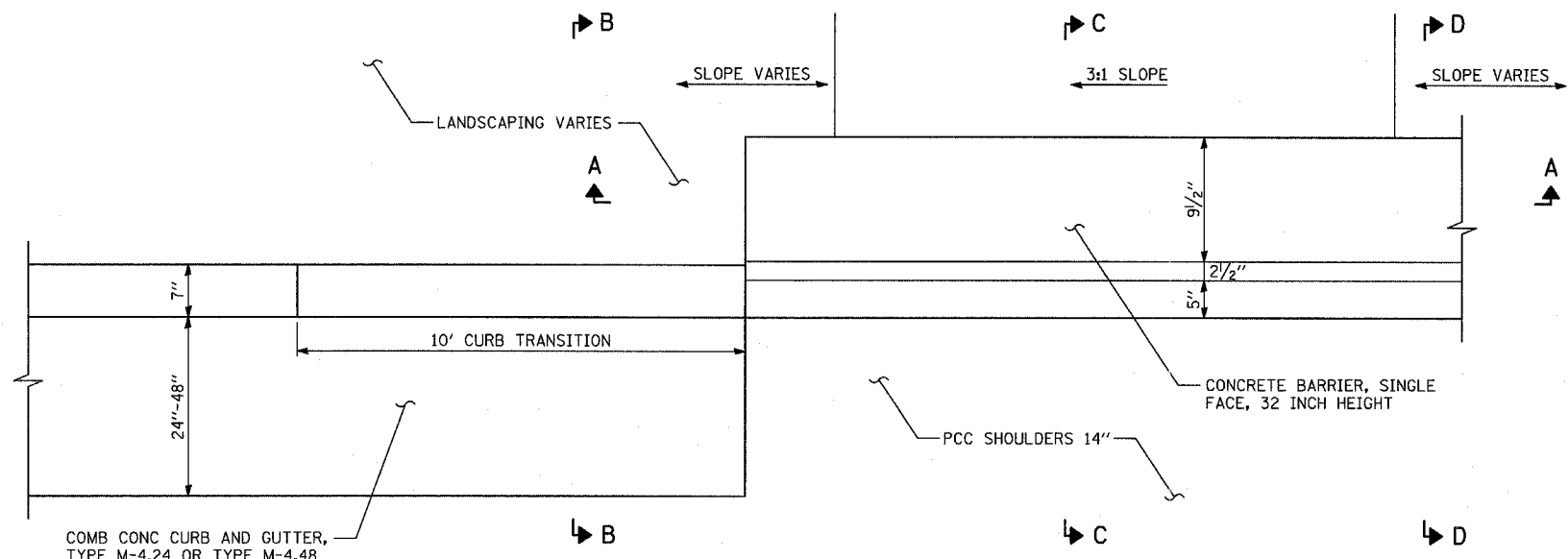


SECTION C-C
15' CONCRETE BARRIER TRANSITION
 CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)



- NOTES:**
1. BAR SPLICERS SHALL BE CAPABLE OF DEVELOPING A MINIMUM OF 125% OF THE YIELD STRENGTH OF A #6 BAR.
 2. ALL REINFORCING BARS, BAR SPLICERS, TIE BARS AND DOWEL BARS SHALL BE SEATED IN THE FINAL POSITION PRIOR TO THE CONCRETE OPERATIONS. BARS CANNOT BE MUCKED INTO PLACE.
 3. MAINTAIN SLOPE OF FACE AS SHOWN ON DETAILS.
 4. ALL REINFORCING BARS, BAR SPLICERS, TIE BARS AND DOWEL BARS SHALL BE INCLUDED IN THE COST OF "CONCRETE BARRIER, SINGLE FACE" OF THE TYPE SPECIFIED

REVISIONS	
NAME	DATE



NOTES:
 1. SEE DRAINAGE AND UTILITY PLANS, CROSS-SECTIONS, AND LANDSCAPING PLANS FOR GRADING LIMITS AND DETAILS.

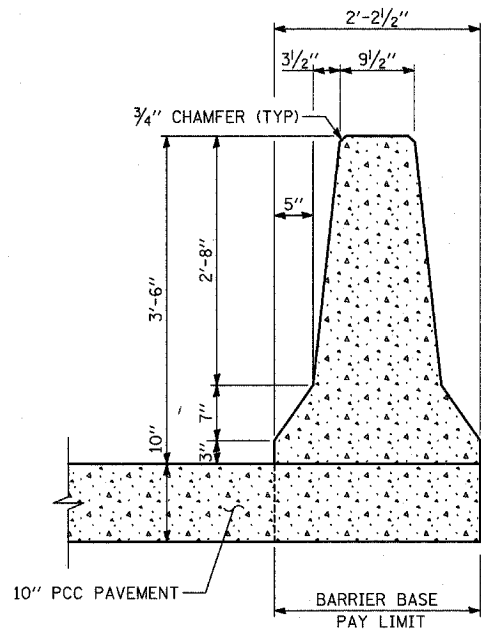
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS:
 GRADING DETAIL AT END
 OF CONCRETE BARRIER

SCALE: NONE
 DATE: MARCH 7, 2006
 DRAWN BY: MPG
 CHECKED BY: TGB

TYLIN INTERNATIONAL

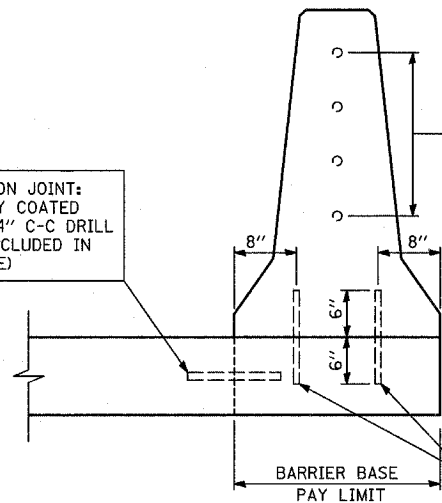


DIMENSIONS

ALL DIMENSIONS ARE APPROXIMATE BASED ON EXISTING PLANS AND MAY VARY - MATCH EXISTING BARRIER

CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT
 ALONG NB I-57

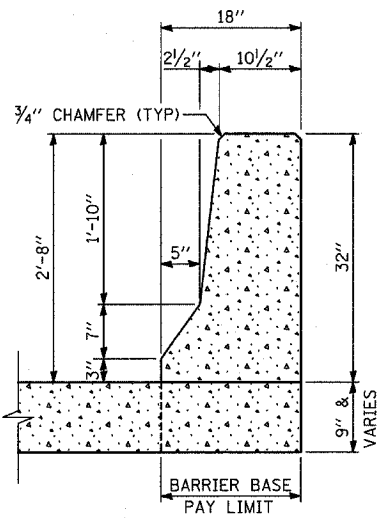
OPTIONAL CONSTRUCTION JOINT:
 NO. 8 DEFORMED EPOXY COATED TIE BAR, 30" LONG, 24" C-C DRILL & GROUT IN PLACE (INCLUDED IN COST OF BARRIER BASE)



REINFORCEMENT

NO. 4 DEFORMED EPOXY COATED REBAR @ 7" C-C DRILL & GROUT IN PLACE - EMBED 12" INTO ADJACENT EXISTING BARRIER - (INCLUDED IN COST OF CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT)

NO. 6 DEFORMED EPOXY COATED TIE BAR, 12" LONG @ 15" STAGGERED C-C DRILL & GROUT IN PLACE - (INCLUDED IN COST OF BARRIER BASE)

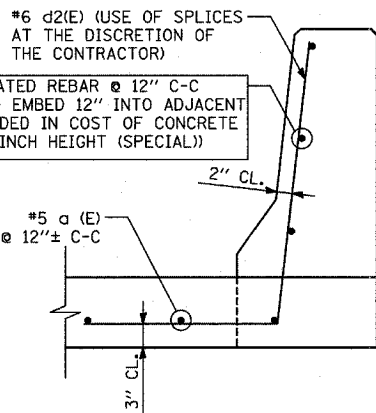


DIMENSIONS

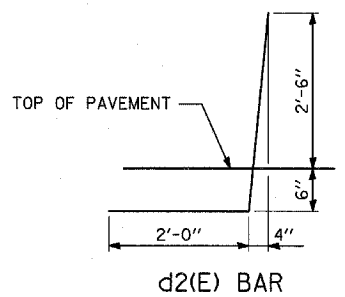
ALL DIMENSIONS ARE APPROXIMATE BASED ON EXISTING PLANS AND MAY VARY - MATCH EXISTING BARRIER

CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (SPECIAL)
 ALONG NB I-94 (BISHOP FORD FREEWAY) & WB I-94 TO I-57 CONNECTOR

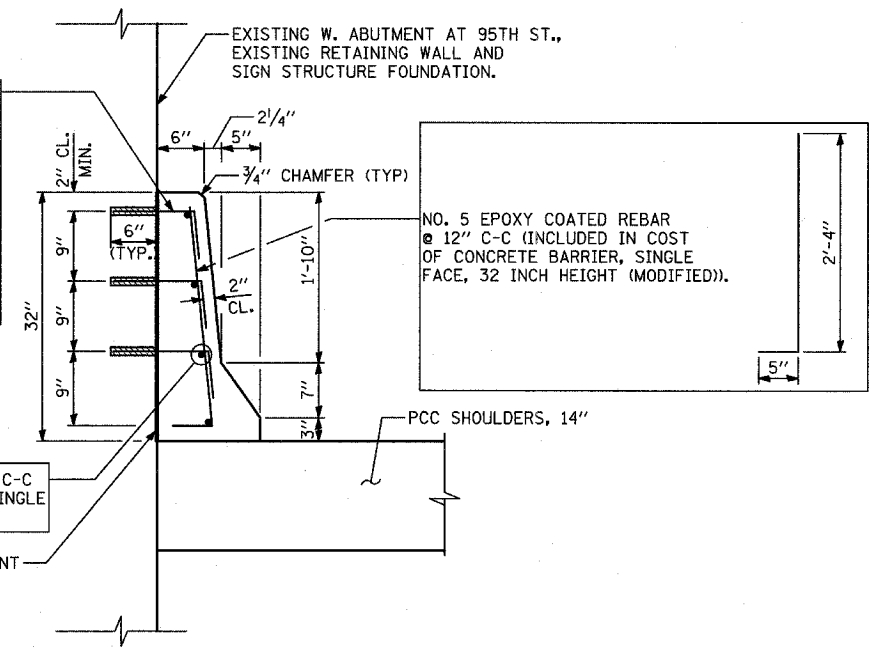
NO. 6 DEFORMED EPOXY COATED REBAR @ 12" C-C DRILL & GROUT IN PLACE - EMBED 12" INTO ADJACENT EXISTING BARRIER - (INCLUDED IN COST OF CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (SPECIAL))



REINFORCEMENT



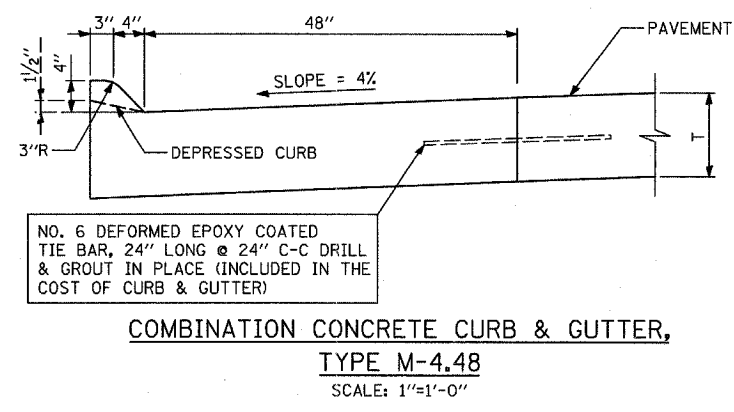
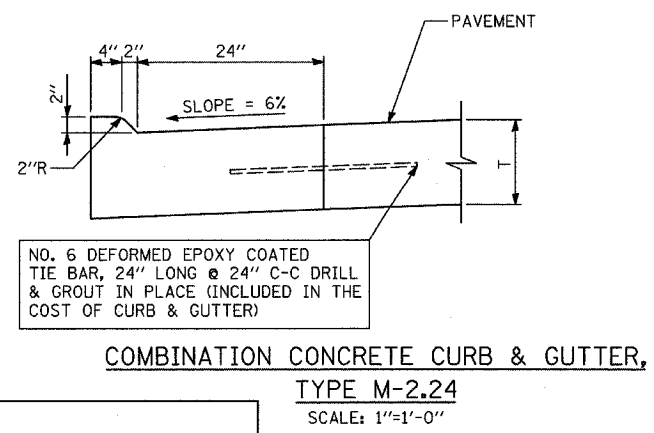
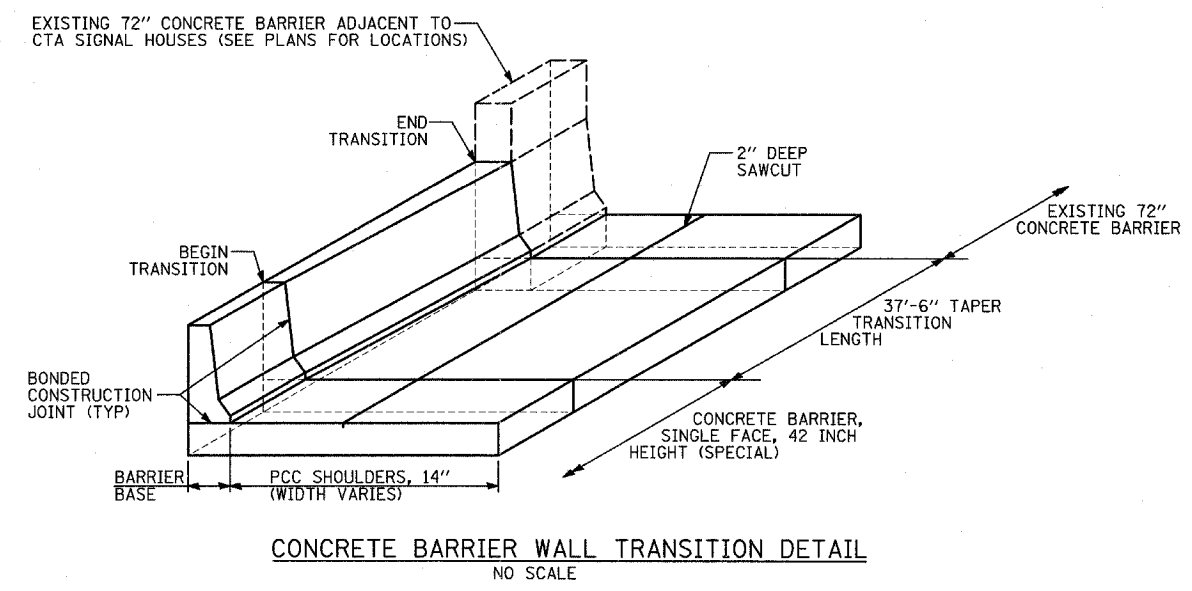
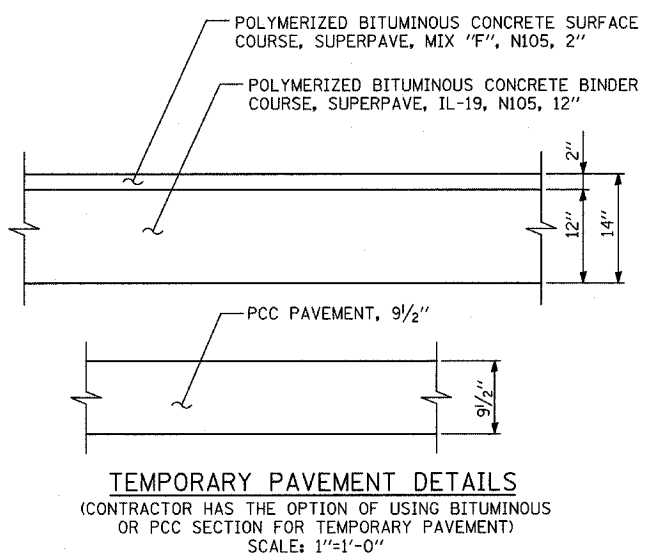
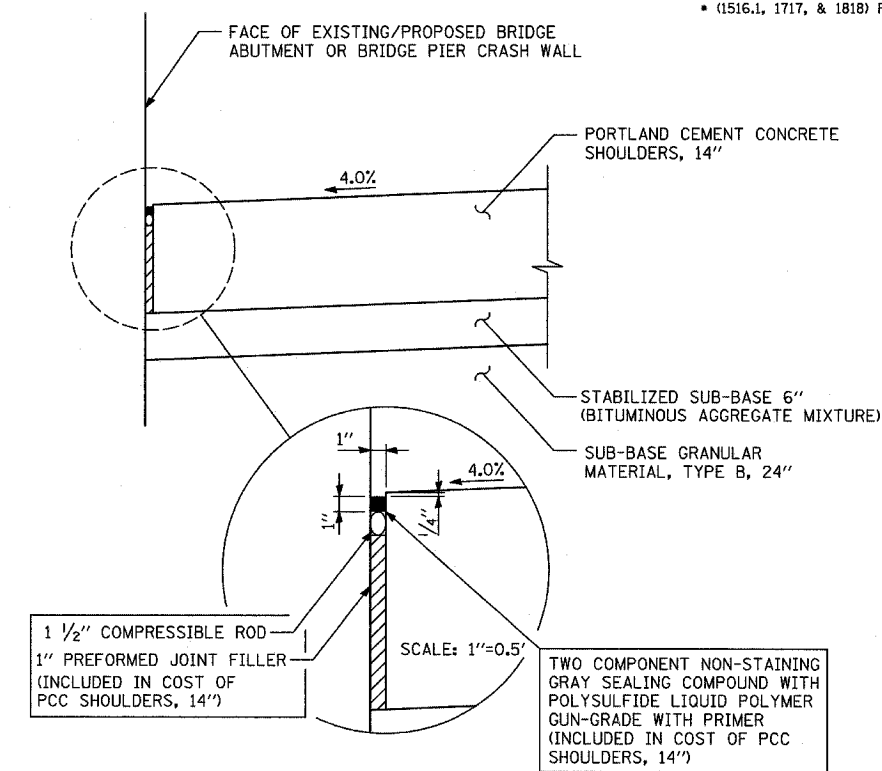
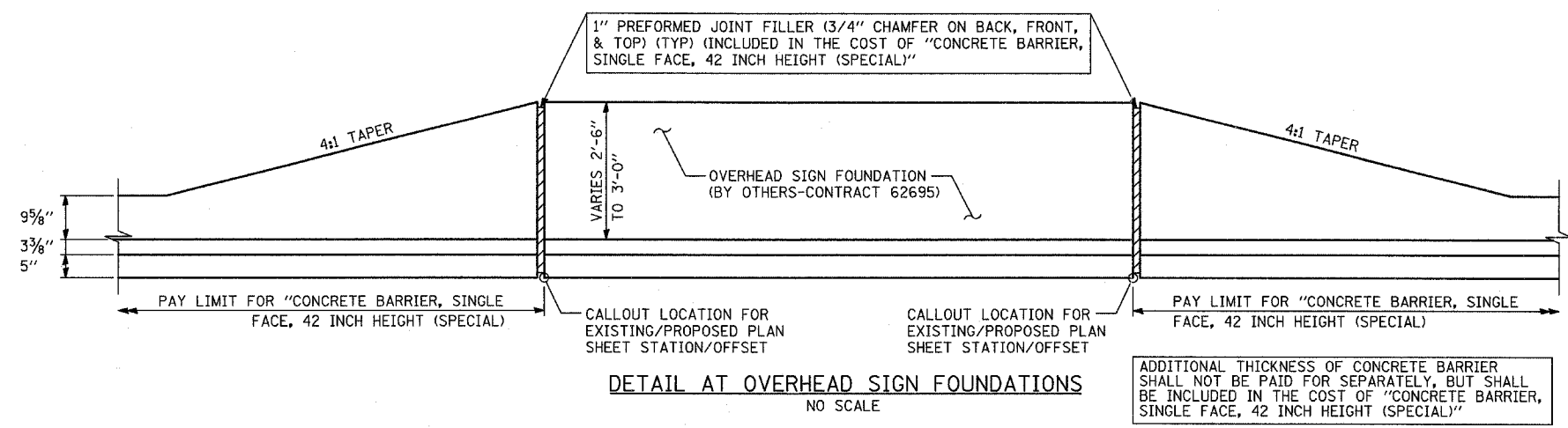
NO. 6 DEFORMED EPOXY COATED REBAR @ 9" C-C DRILL & GROUT IN PLACE - EMBED 6" INTO ADJACENT EXISTING STRUCTURE - (INCLUDED IN COST OF CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED)). CUT BAR TO FIT.



CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED)
 SCALE: 1"=1'-0"

PROVIDE JOINTS IN FACING TO MATCH THOSE IN THE EXISTING STRUCTURES. (INCLUDED IN COST OF CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED)).

REVISIONS	
NAME	DATE

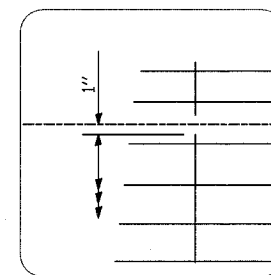
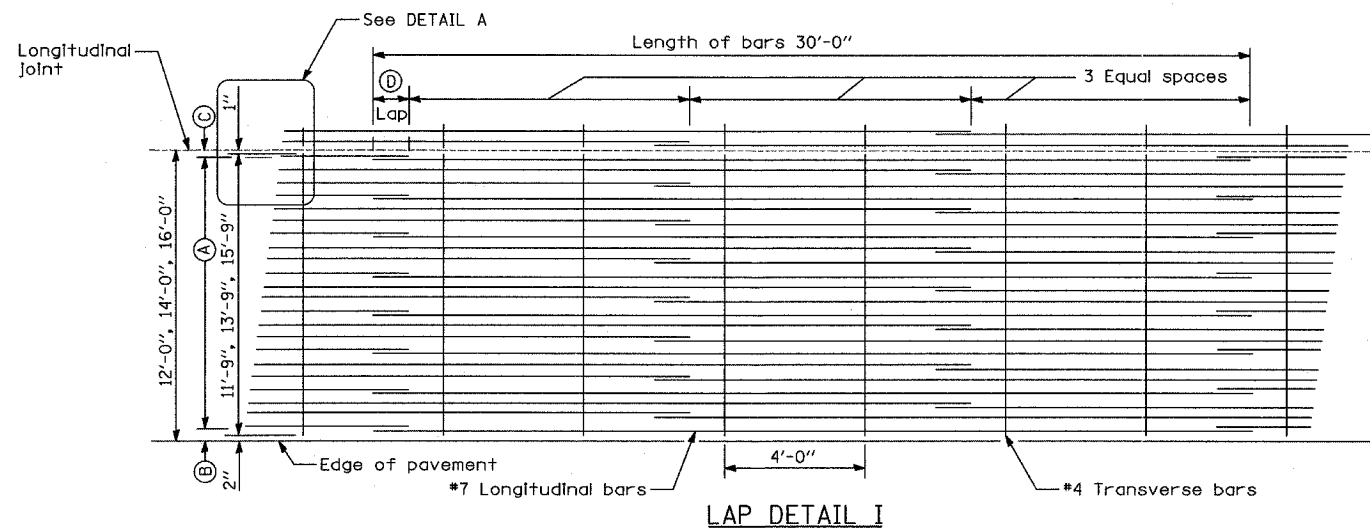


REVISIONS	
NAME	DATE

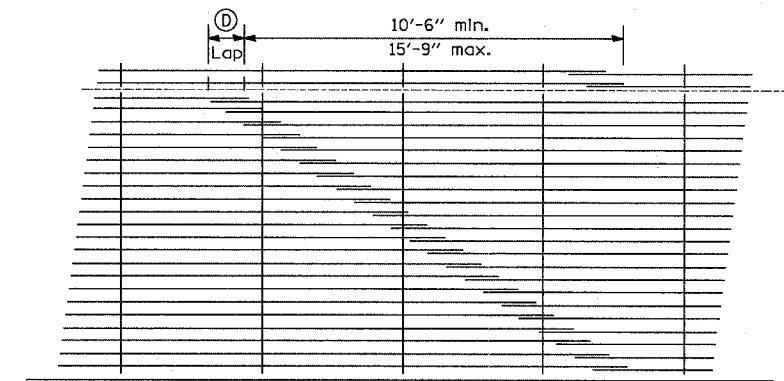
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS:
PLAN AND TYPICAL SECTION DETAILS
SHEET 2 OF 2

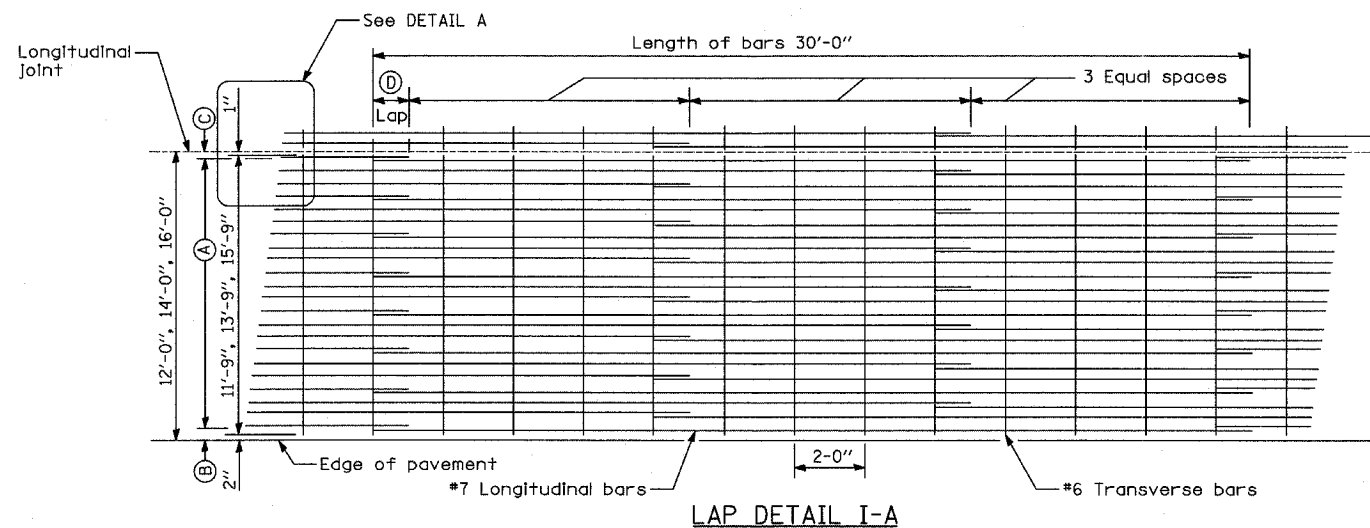
SCALE: AS SHOWN DRAWN BY: MPG
DATE: MARCH 7, 2006 CHECKED BY: TGB



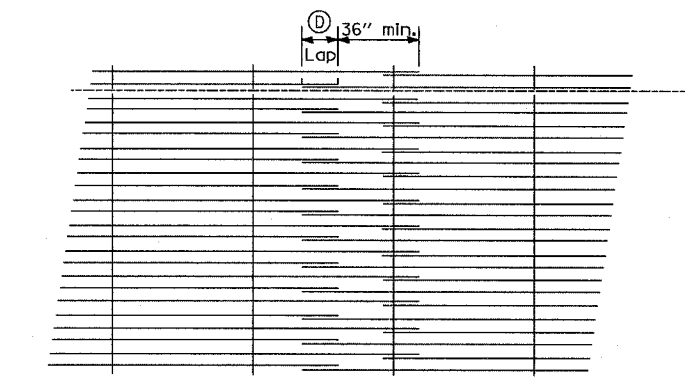
DETAIL A



LAP DETAIL II



ADDITIONAL REINFORCEMENT IN GORE AREAS - SEE PAVEMENT JOINTING AND ELEVATION PLANS FOR LOCATIONS



LAP DETAIL III

GENERAL NOTES

1. THE PAVEMENT REINFORCEMENT SHALL BE 4.5" FROM THE TOP OF PAVEMENT.
2. EXCEPT AS NOTED OR SHOWN, THE DIMENSIONS AND NOTES SPECIFIED FOR LAP DETAIL I ARE TYPICAL FOR LAP DETAIL II AND III.
3. THE (B) DIMENSION AND THE DISTANCE FROM THE END OF THE TRANSVERSE BAR TO THE EDGE OF PAVEMENT MAY BE INCREASED BY 1" FOR SLIP FORM PAVING.

Pavement Width	Bar Size	Pavement Thickness	(A) (Approx. Spacing)	(B)	(C)	(D)
12 feet	#7	14"	23 spaces (24 bars) @ 6"	3 1/2"	3"	26"
14 feet	#7	14"	27 spaces (28 bars) @ 6"	3 1/2"	3"	26"
16 feet	#7	14"	31 spaces (32 bars) @ 6"	3 1/2"	3"	26"

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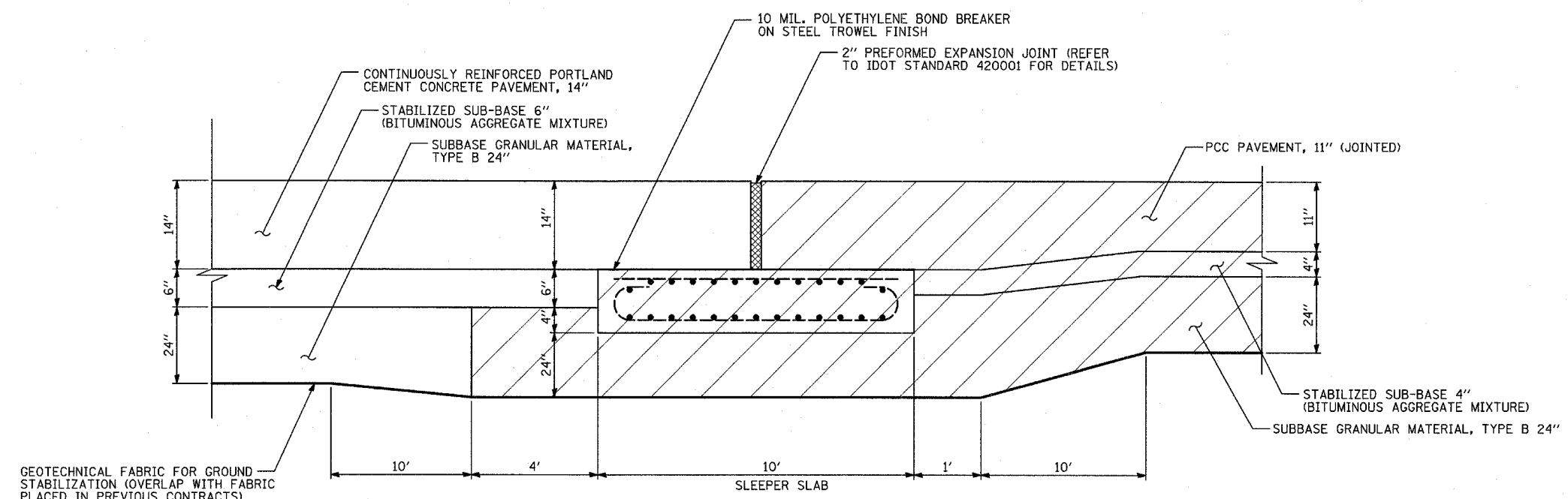
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS:
EXTENDED LANE REINFORCEMENT FOR
CONTINUOUSLY REINFORCED PCC PAVEMENT

SCALE: NONE
DATE: MARCH 7, 2006


DRAWN BY: CTE, MPG
CHECKED BY:



RAMP TERMINAL DETAILS AT EXISTING SLEEPER SLAB

NOTES:

1. THE THICKENED EDGE OF THE SUB-BASE SHALL BE INCLUDED IN THE COST OF "SUB-BASE GRANULAR MATERIAL, TYPE B 24"

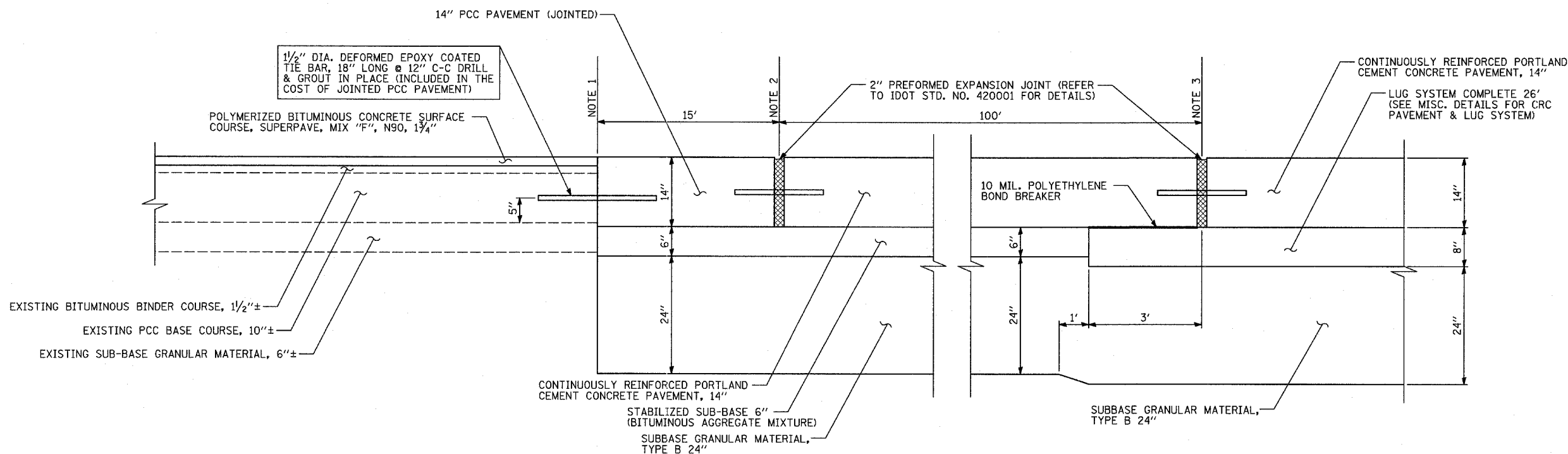
 EXISTING PAVEMENT ITEMS CONSTRUCTED IN PREVIOUS CONTRACTS.

TYLININTERNATIONAL

REVISIONS	
NAME	DATE

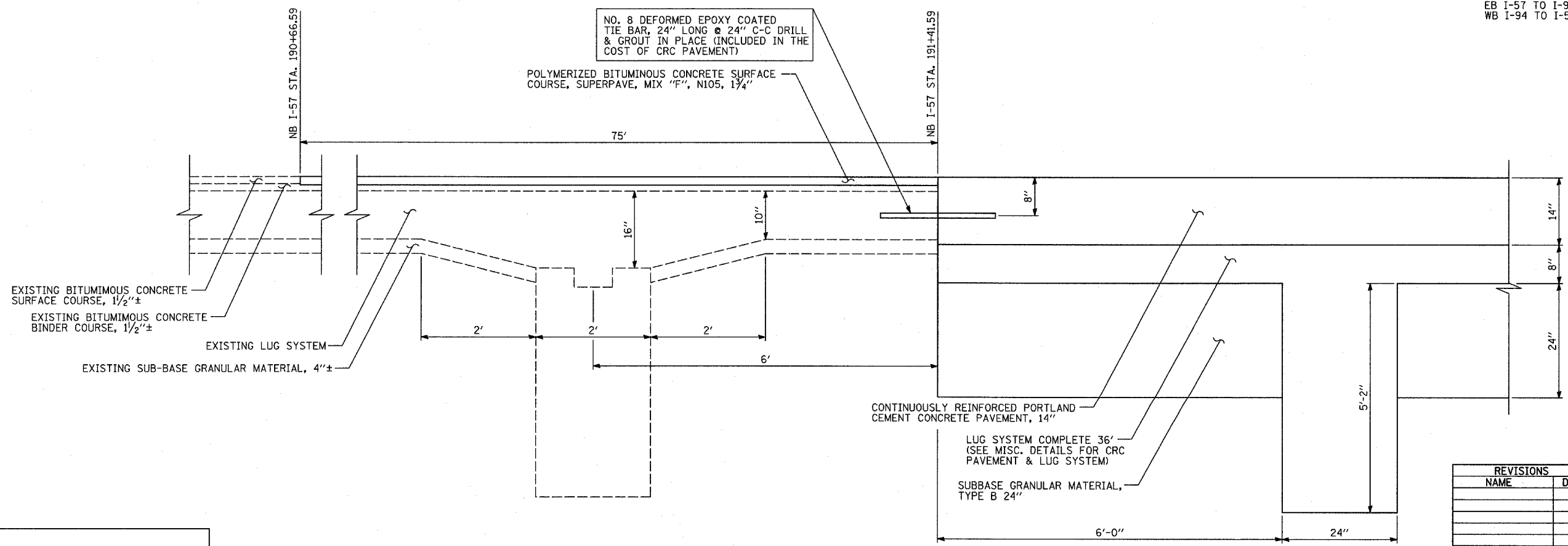
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MISCELLANEOUS DETAILS:
 MISCELLANEOUS PAVEMENT ELEVATION
 AND JOINTING DETAILS
 SHEET 1 OF 2

SCALE: NONE DRAWN BY: MPG
 DATE: MARCH 7, 2006 CHECKED BY: TGB



PROPOSED/EXISTING PAVEMENT TRANSITION

- NOTES:**
- NB I-94 (BISHOP FORD) STA. 2021+00.00
EB I-57 TO I-94 CONNECTOR STA. 415+06.00
WB I-94 TO I-57 CONNECTOR STA. 330+65.46
 - NB I-94 (BISHOP FORD) STA. 2021+06.00
EB I-57 TO I-94 CONNECTOR STA. 415+00.00
WB I-94 TO I-57 CONNECTOR STA. 330+71.42
 - NB I-94 (BISHOP FORD) STA. 2022+06.00
EB I-57 TO I-94 CONNECTOR STA. 414+00.00
WB I-94 TO I-57 CONNECTOR STA. 331+70.71



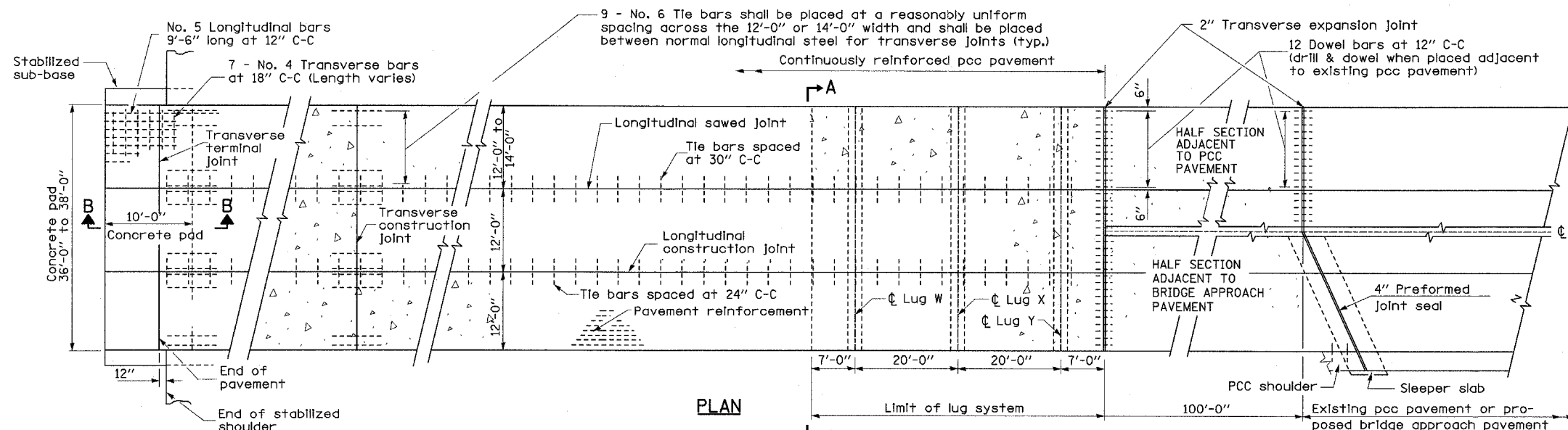
PROPOSED/EXISTING PAVEMENT TRANSITION AT WEST PROJECT LIMIT ALONG I-57

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MISCELLANEOUS DETAILS:
 MISCELLANEOUS PAVEMENT ELEVATION
 AND JOINTING DETAILS
 SHEET 2 OF 2

SCALE: NONE DRAWN BY: MPG
 DATE: MARCH 7, 2006 CHECKED BY: TGB



MATERIALS REQUIRED FOR (1) ONE LUG SYSTEM COMPLETE, 26'

(Excluding CRC Concrete, Pavement Reinforcement, Stabilized Sub-base and Sub-base Granular Material)

Bar No.	Size	Length	Shape
a	144 No. 8	16'-8"	
b	24 No. 5	27'-0"	
c	144 No. 5	20'-0"	
d	28 No. 4	11'-9" or 13'-9"	

Concrete (cu. yds.) 29.85
 Reinforcing Bars (lbs.) 10330
 Concrete Pad (sq. yds.) 139

MATERIALS REQUIRED FOR (1) ONE LUG SYSTEM COMPLETE, 36'

(Excluding CRC Concrete, Pavement Reinforcement, Stabilized Sub-base and Sub-base Granular Material)

Bar No.	Size	Length	Shape
a	198 No. 8	16'-8"	
b	24 No. 5	37'-0"	
c	198 No. 5	20'-0"	
d	42 No. 4	11'-9"	

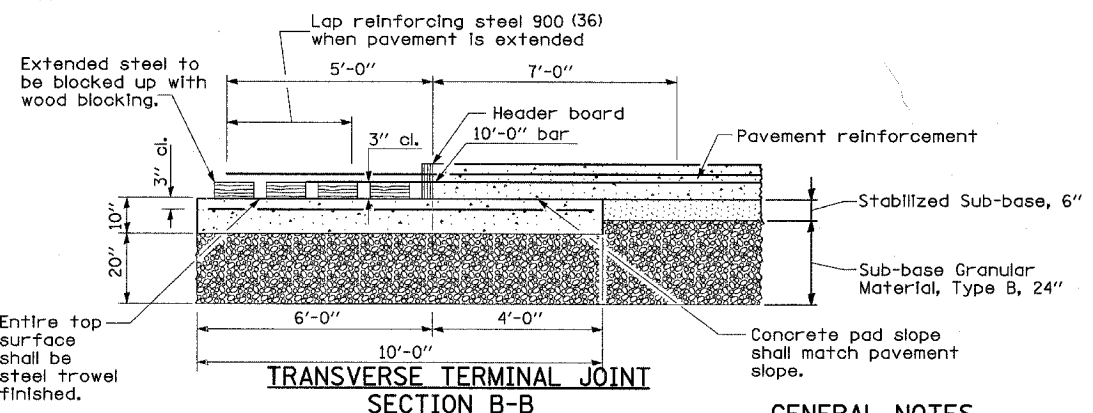
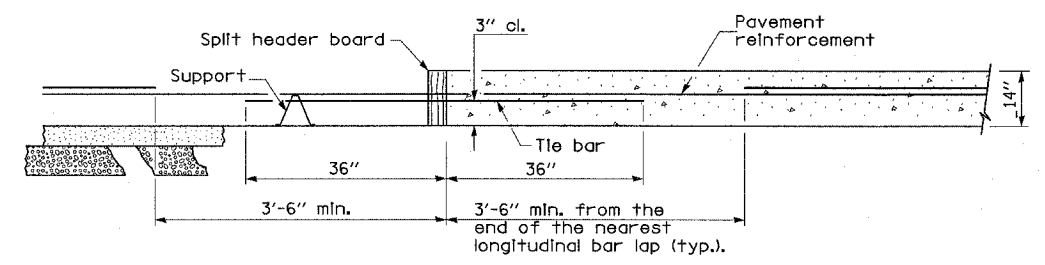
Concrete (cu. yds.) 41.33
 Reinforcing Bars (lbs.) 14200
 Concrete Pad (sq. yds.) 192

MATERIALS REQUIRED FOR (1) ONE LUG SYSTEM COMPLETE, 38'

(Excluding CRC Concrete, Pavement Reinforcement, Stabilized Sub-base and Sub-base Granular Material)

Bar No.	Size	Length	Shape
a	210 No. 8	16'-8"	
b	24 No. 5	39'-0"	
c	210 No. 5	20'-0"	
d	42 No. 4	11'-9" or 13'-9"	

Concrete (cu. yds.) 43.63
 Reinforcing Bars (lbs.) 15060
 Concrete Pad (sq. yds.) 203



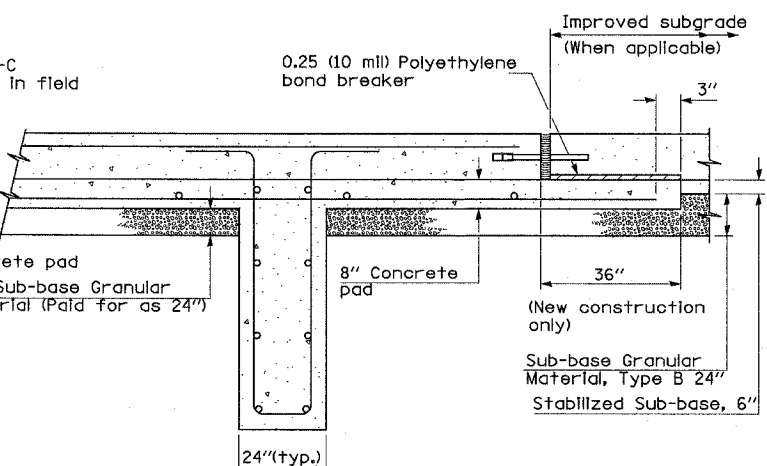
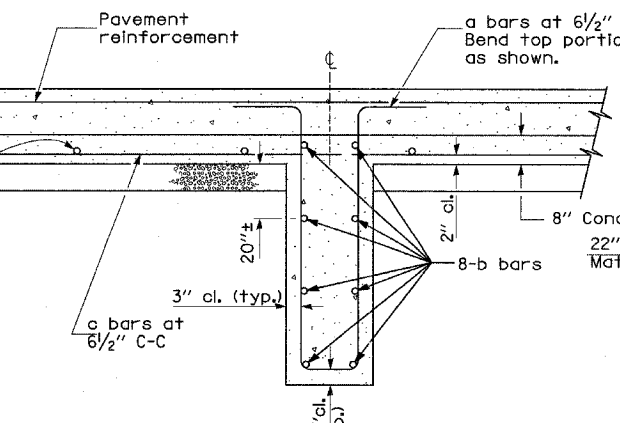
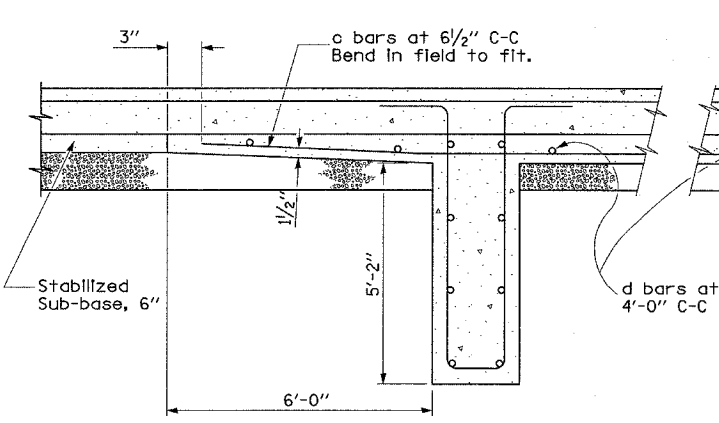
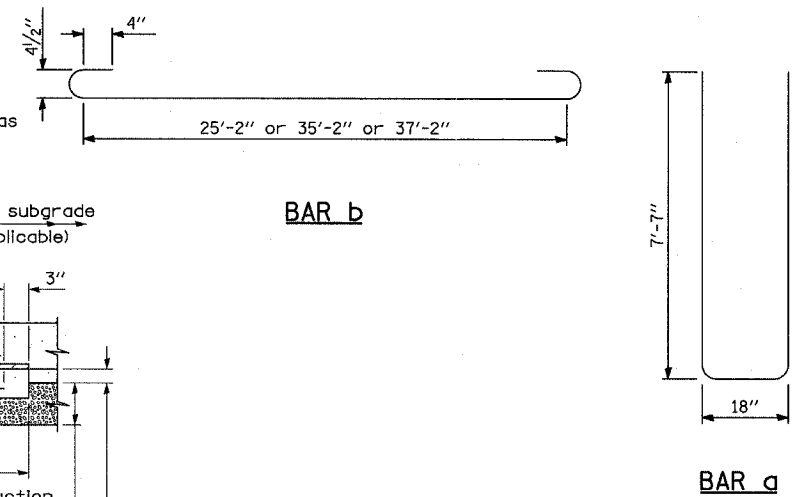
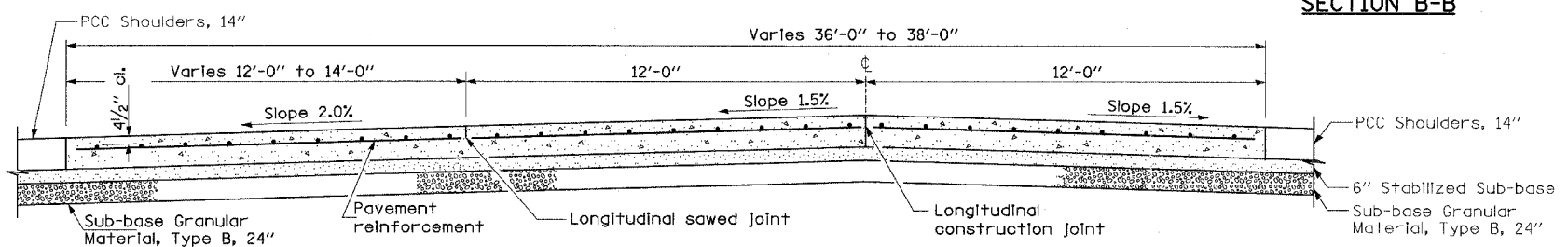
GENERAL NOTES

See Pavement Reinforcement details for additional information.

See Standards 420001 and 420401 for joint details not shown.

All dimensions are in inches unless otherwise shown.

Sub-base granular material that varies in thickness below the lug-system or the transverse terminal joint will be paid for as "Sub-base Granular Material, Type B, 24"

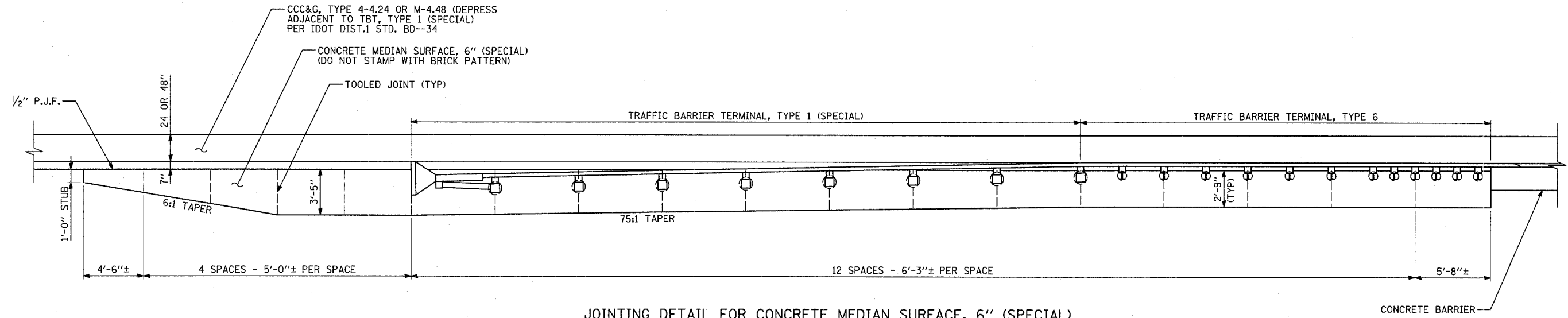


REVISIONS

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MISCELLANEOUS DETAILS:
 CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT AND LUG SYSTEM COMPLETE, 26', 36', & 38'

SCALE: NONE
 DATE: MARCH 7, 2006
 DRAWN BY: MPG
 CHECKED BY: TGB



JOINTING DETAIL FOR CONCRETE MEDIAN SURFACE, 6" (SPECIAL)
AT TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) AND TYPE 6

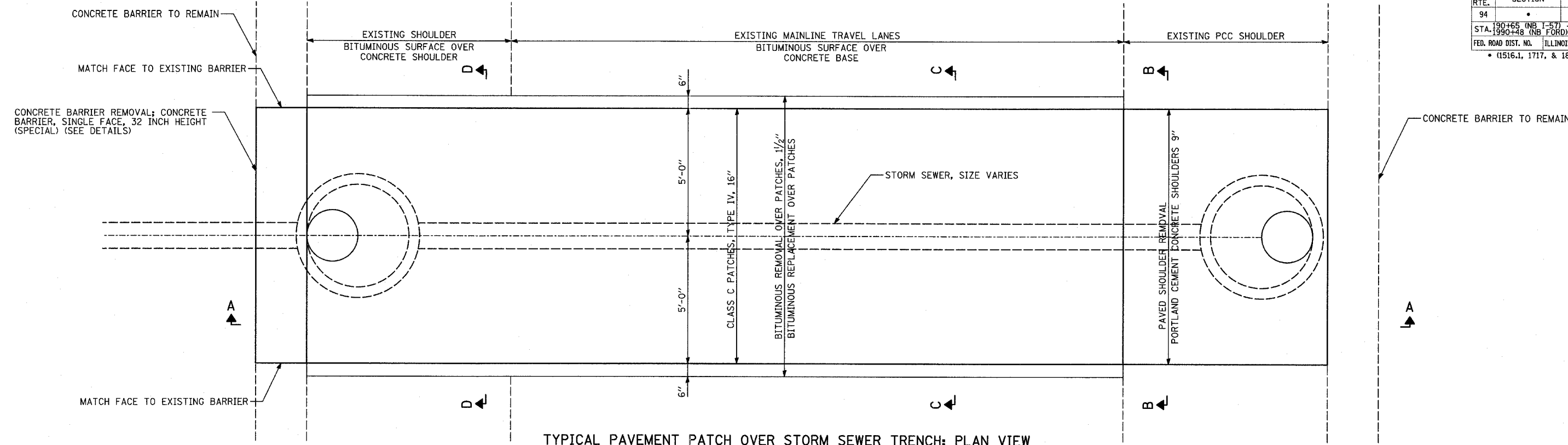
TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

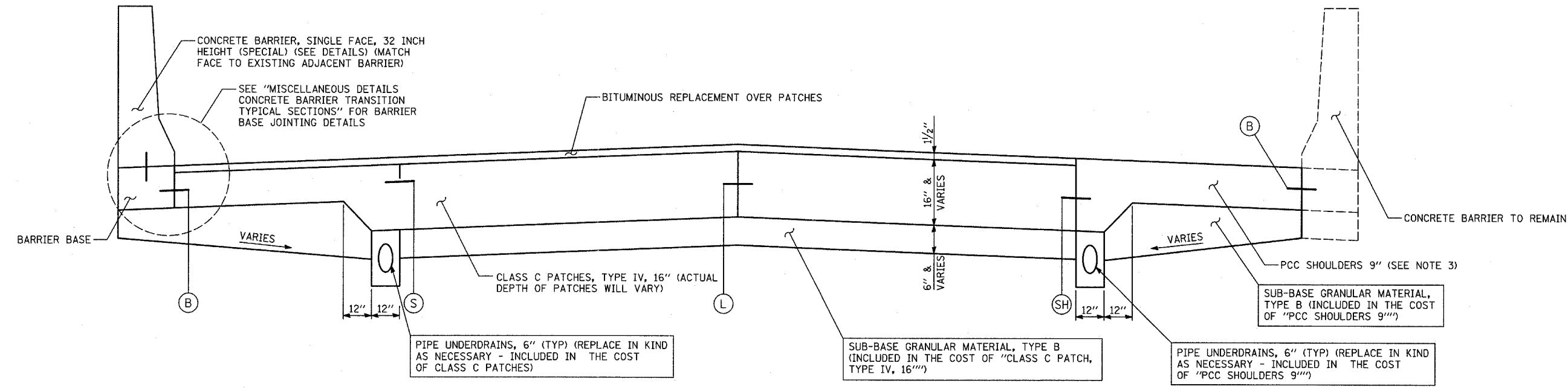
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS:
 PAVEMENT JOINTING DETAILS
 FOR CONCRETE MEDIAN SURFACE, 6" (SPECIAL)
 AT TRAFFIC BARRIER TERMINALS

SCALE: NONE DRAWN BY: MPG
 DATE: MARCH 7, 2006 CHECKED BY: TGB



TYPICAL PAVEMENT PATCH OVER STORM SEWER TRENCH: PLAN VIEW
 IN PAVEMENT RESURFACING AREAS - SEE PLANS FOR LOCATIONS



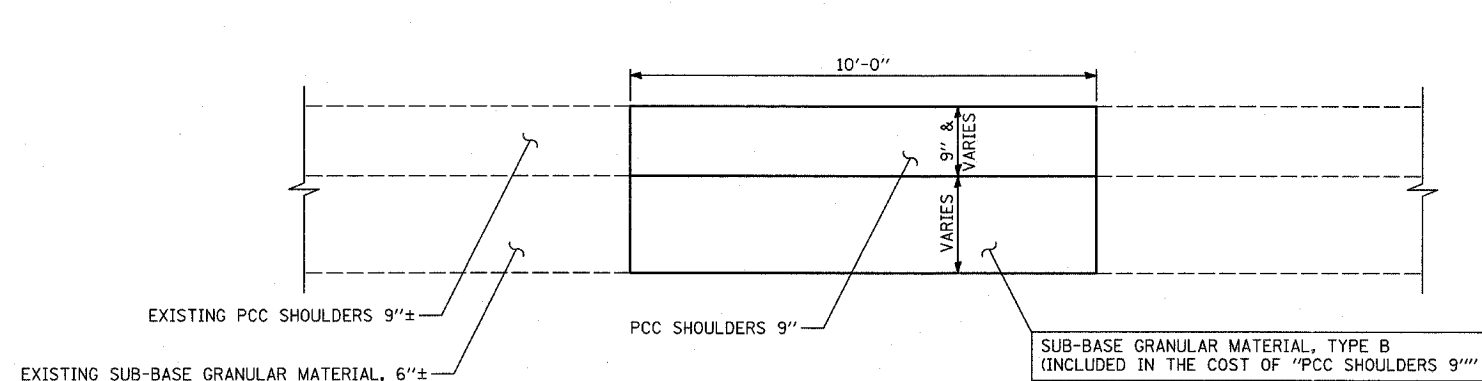
SECTION A-A

NOTES:

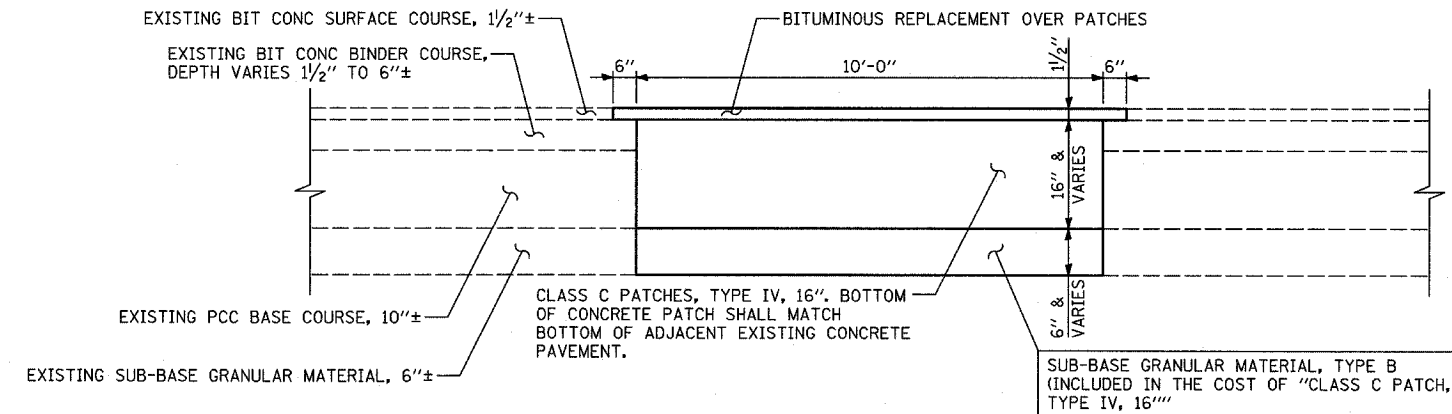
- TRENCH WIDTH MUST BE 5'-0" EITHER SIDE FROM SEWER OR DRAINAGE STRUCTURE CENTER LINE.
- LOCATIONS OF CONCRETE BARRIER TO REMAIN, CONCRETE BARRIER TO BE REPLACED, PCC SHOULDERS, AND BITUMINOUS SURFACED SHOULDERS VARY.
- ADDITIONAL THICKNESS OF "PCC SHOULDERS 9'" NEEDED TO MATCH THE DEPTH OF CLASS C PATCHES SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS 9'.
- SEE PAVEMENT JOINTING AND ELEVATION PLANS LEGEND FOR DESCRIPTIONS OF LONGITUDINAL BARRIER CONSTRUCTION JOINTS, LONGITUDINAL CONSTRUCTION JOINTS, LONGITUDINAL SHOULDER CONSTRUCTION JOINTS, AND SAWED LONGITUDINAL JOINTS. ALL JOINTS SHALL BE INCLUDED IN THE COST OF "CLASS C PATCHES, TYPE IV, 16", OR PCC SHOULDERS, 9'"
- CONNECT UNDERDRAIN TO CATCH BASIN IF UNDERDRAIN IS DRAINING TOWARD PATCH.

REVISIONS	
NAME	DATE

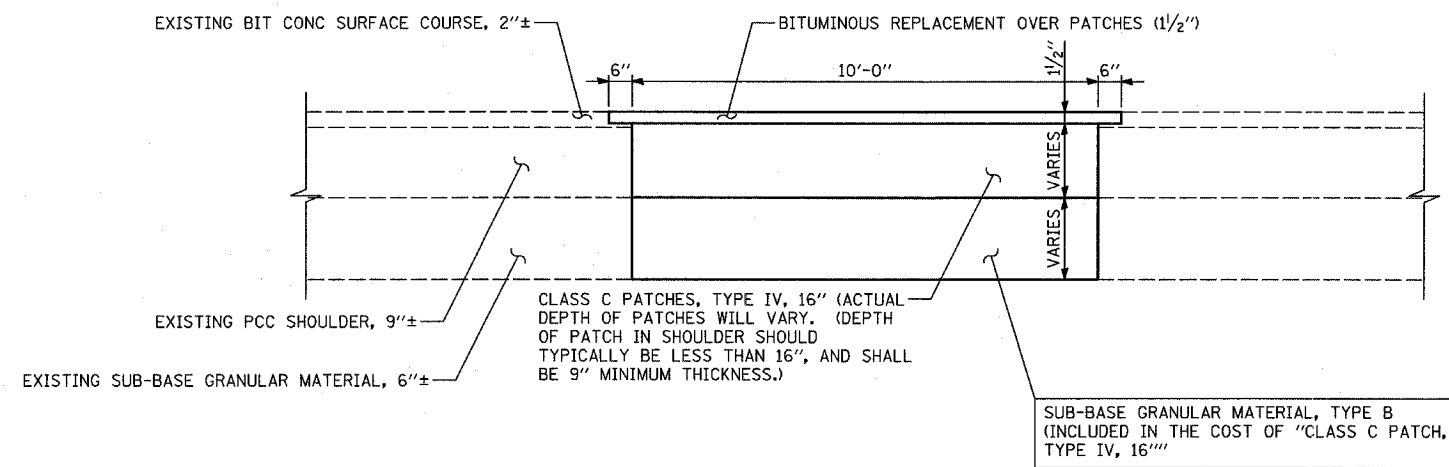
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MISCELLANEOUS DETAILS:
 PAVEMENT PATCHING OVER SEWER
 TRENCHES: SHEET 1 OF 2
 PLAN AND ELEVATION
 SCALE: NONE
 DATE: MARCH 7, 2006
 DRAWN BY: MPG
 CHECKED BY: TGB



SECTION B-B
PCC SHOULDER WITH NO OVERLAY



SECTION C-C
BITUMINOUS SURFACE OVER CONCRETE BASE

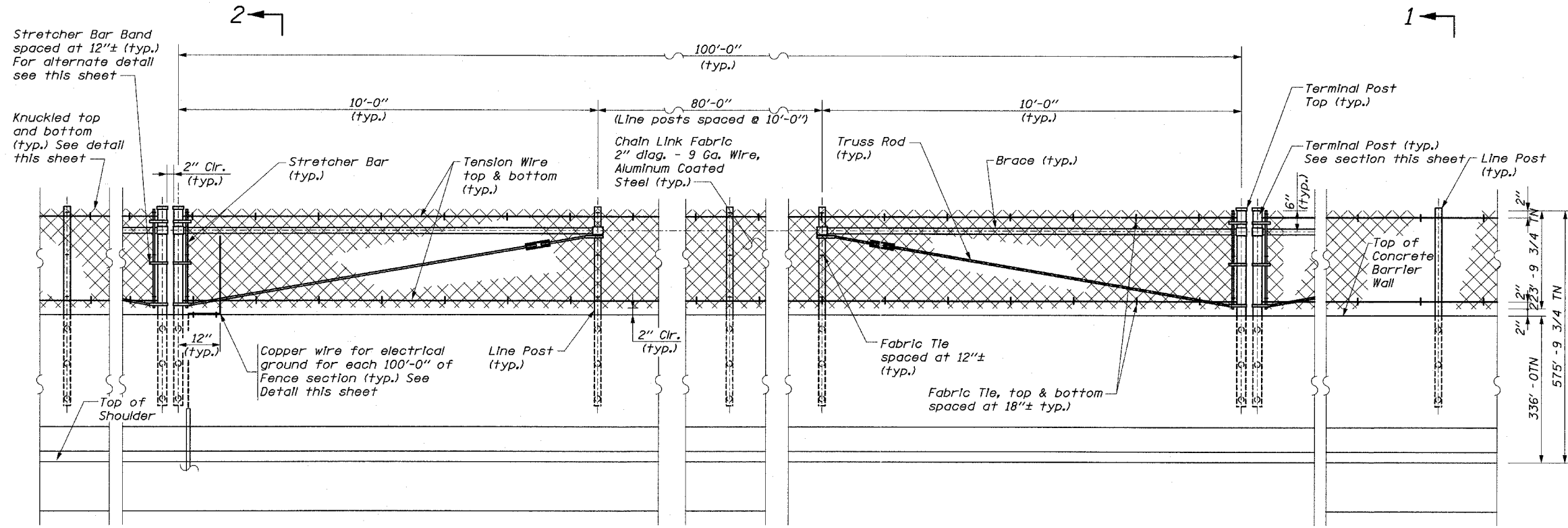


SECTION D-D
BITUMINOUS SURFACE OVER CONCRETE SHOULDER

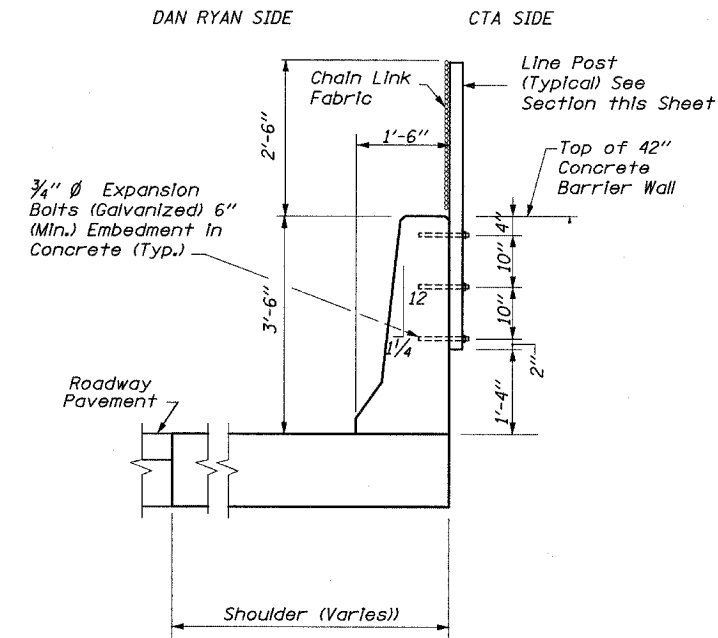
NOTES:

- TRENCH WIDTH MUST BE 5'-0" EITHER SIDE FROM SEWER OR DRAINAGE STRUCTURE CENTER LINE.
- LOCATIONS OF CONCRETE BARRIER TO REMAIN, CONCRETE BARRIER TO BE REPLACED, PCC SHOULDERS, AND BITUMINOUS SURFACED SHOULDERS VARY.
- ADDITIONAL THICKNESS OF "PCC SHOULDERS 9" NEADED TO MATCH THE DEPTH OF CLASS C PATCHES SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS 9".

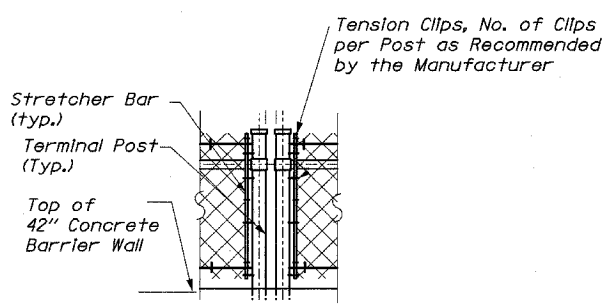
REVISIONS	
NAME	DATE



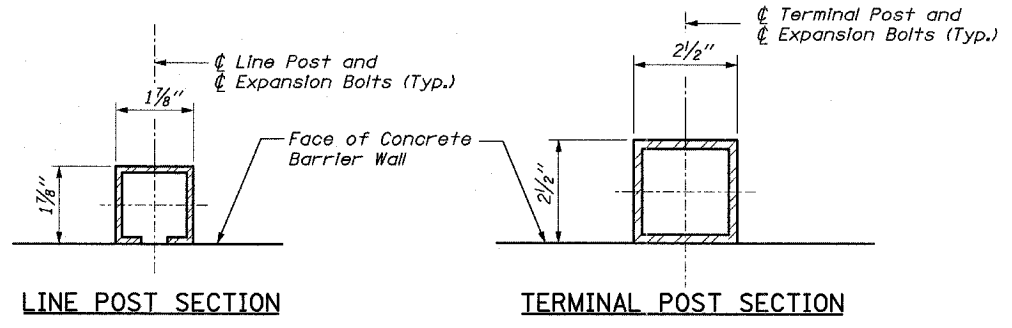
ELEVATION
CTA FENCE ON CONCRETE BARRIER WALL



SECTION 1-1



ALTERNATE DETAIL FOR
STRETCHER BAR BAND

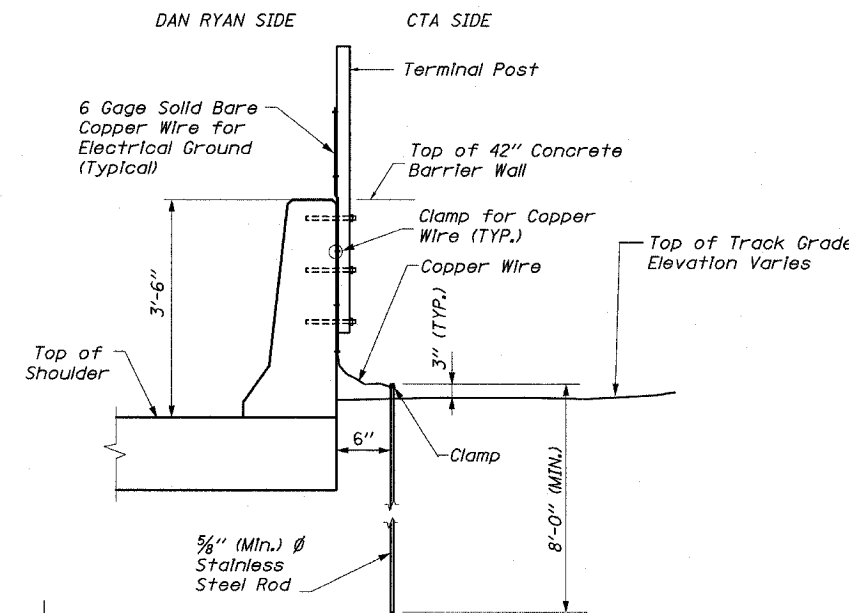


LINE POST SECTION

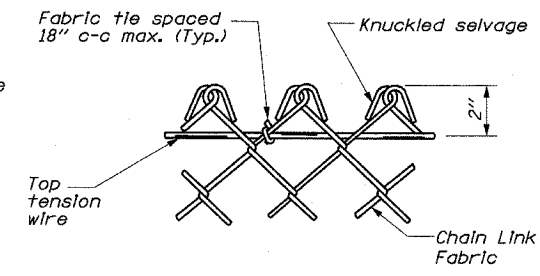
TERMINAL POST SECTION

NOTE:
The Contractor May Use Another C Section as per the Standards of the Illinois Department of Transportation.

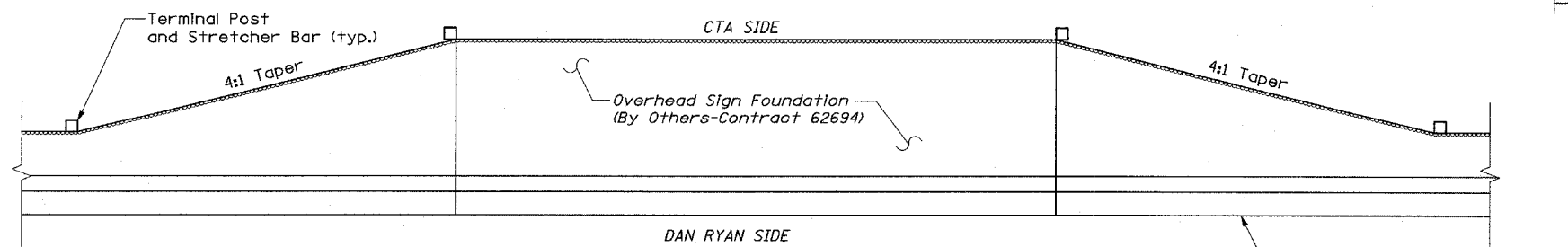
NOTE:
Where the fence meets bridge piers and other vertical structures taller than the fence a maximum gap of 2" clear between the terminal post and the structure shall be provided.



SECTION 2-2
ELECTRICAL GROUND DETAIL



KNUCKLED DETAIL



DETAIL AT OVERHEAD SIGN FOUNDATIONS

Fence, hardware and posts around the overhead sign foundation are paid for as CTA Fence.

Edwards AND Kelcey
ONE NORTH FRANKLIN CHICAGO, IL 60606
PHONE: (312) 251-3000
FAX: (312) 251-3015
WEB: WWW.EKCORP.COM

TYLIN INTERNATIONAL

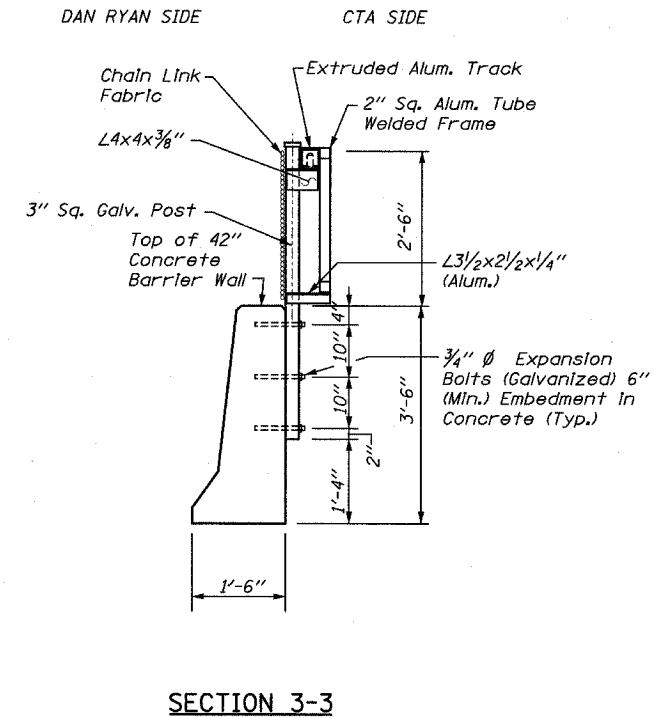
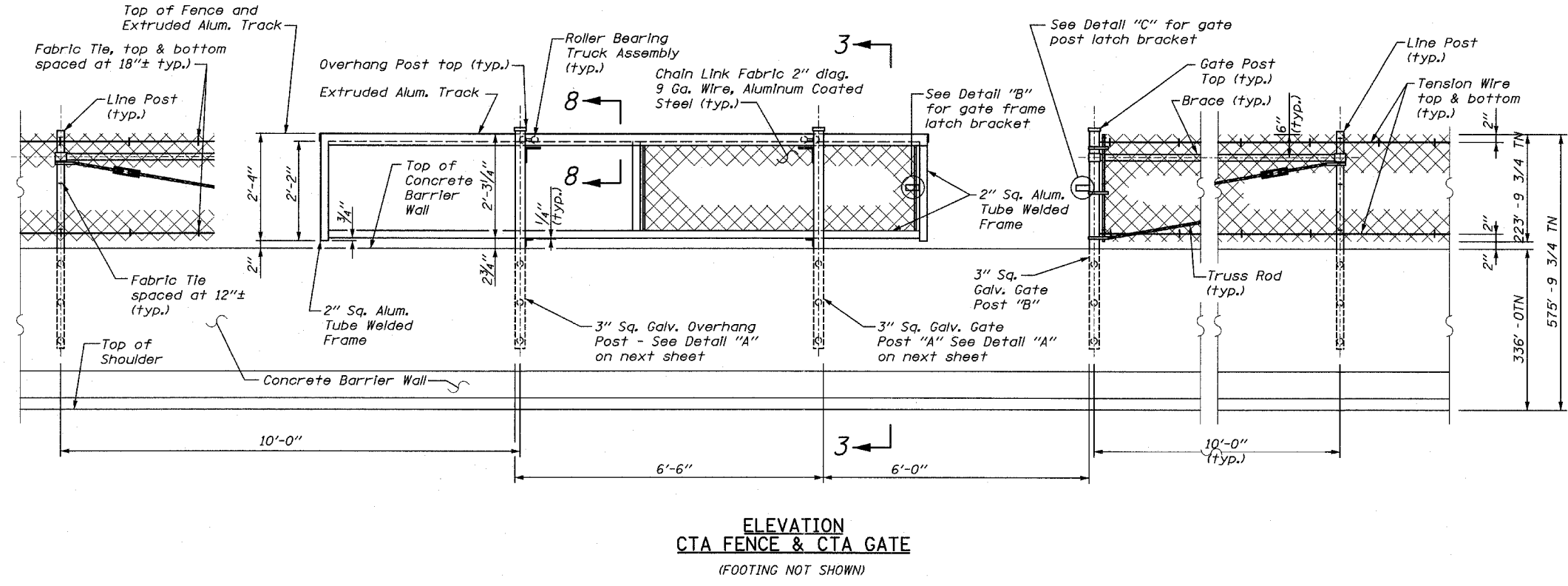
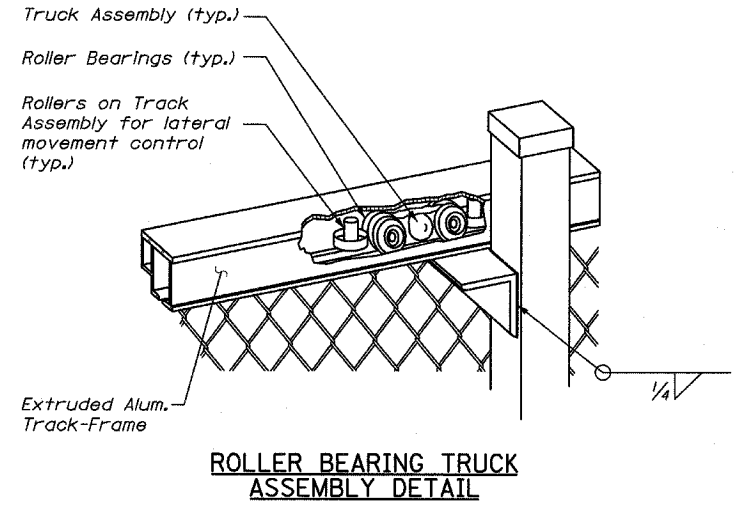
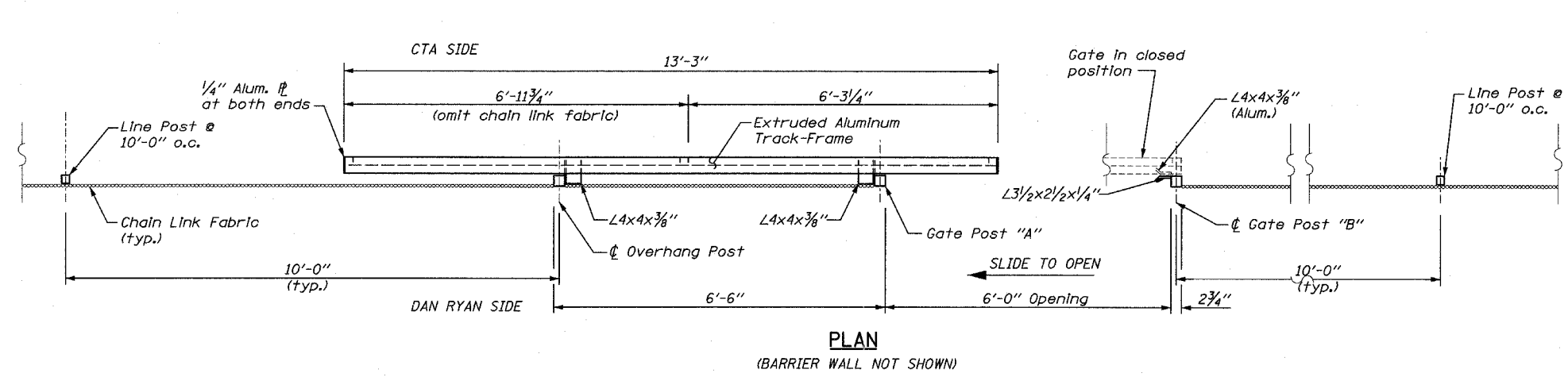
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

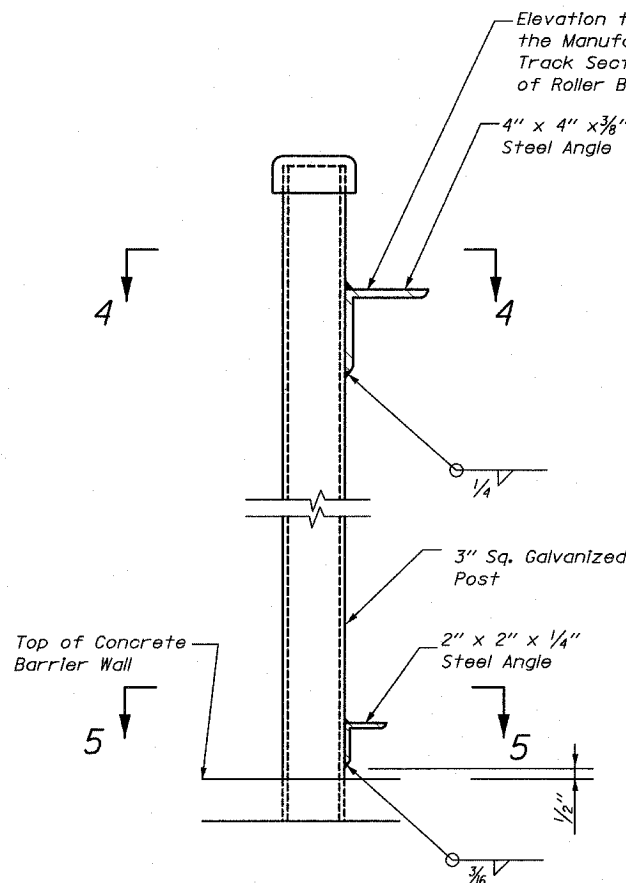
CTA FENCE
ELEVATIONS AND DETAILS
SHEET 1 OF 3

SCALE: NONE
DATE: MARCH 7, 2006

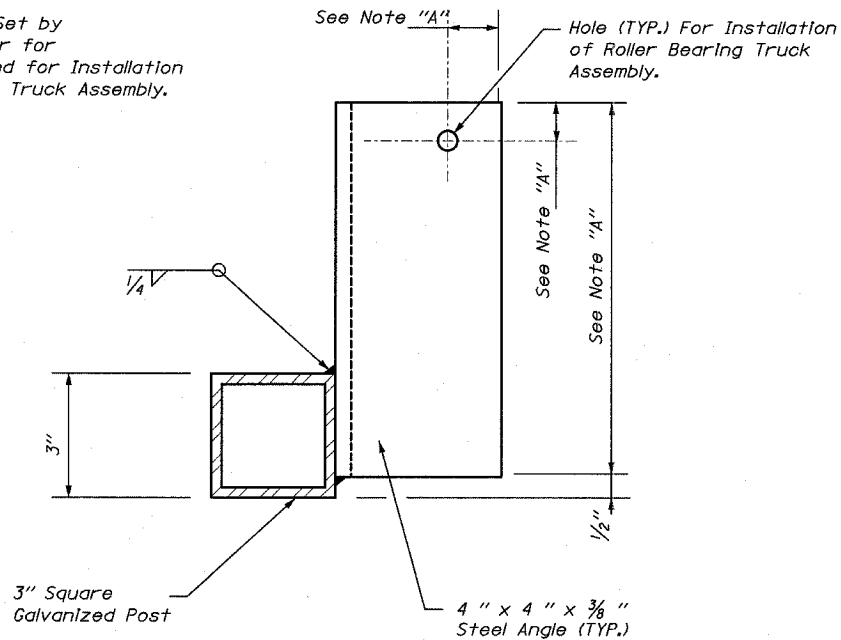
DRAWN BY: RLK
CHECKED BY: EL



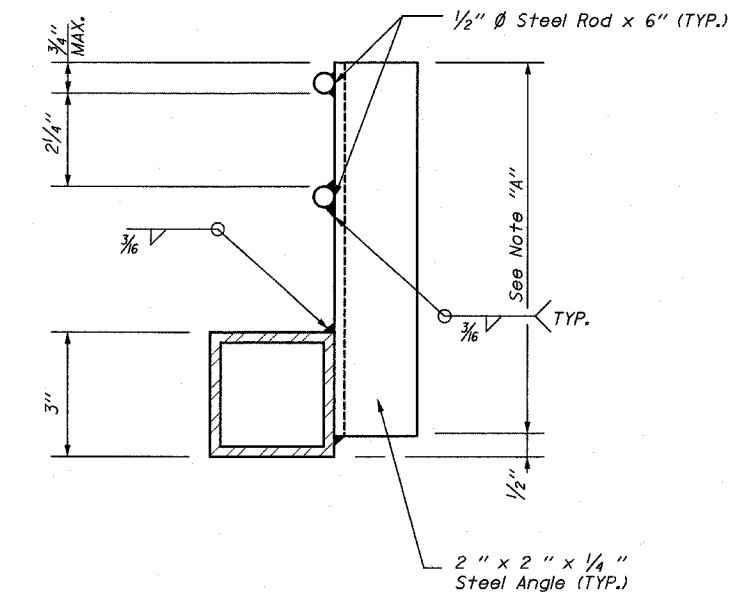
REVISIONS	
NAME	DATE



ELEVATION



SECTION 4-4

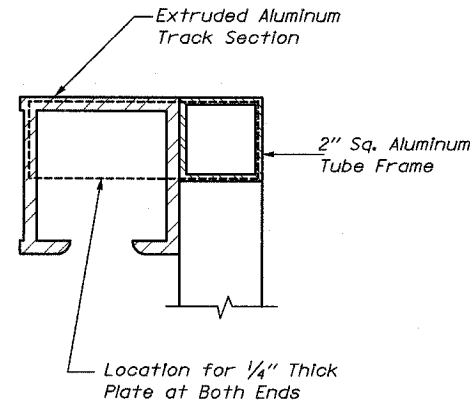


SECTION 5-5

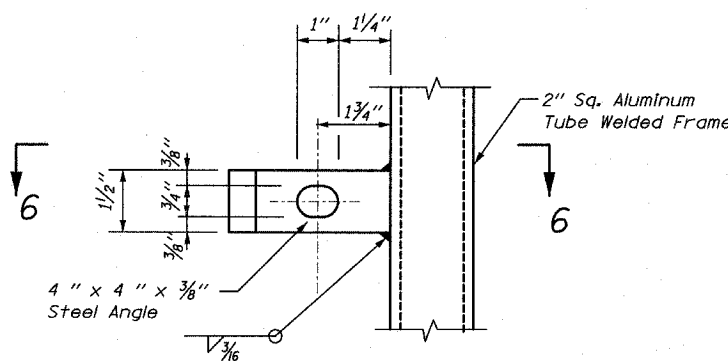
DETAIL A

For Overhang Post - As Shown
 For Gate Post "A" - Opposite Hand

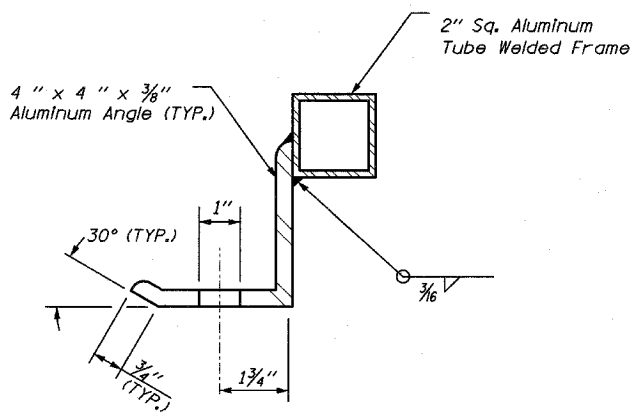
NOTE "A": The Manufacturer of the Sliding Gate Shall Determine the Length of Angles and Location of Hole for Installation of Roller Bearing Truck Assembly.



SECTION 8-8



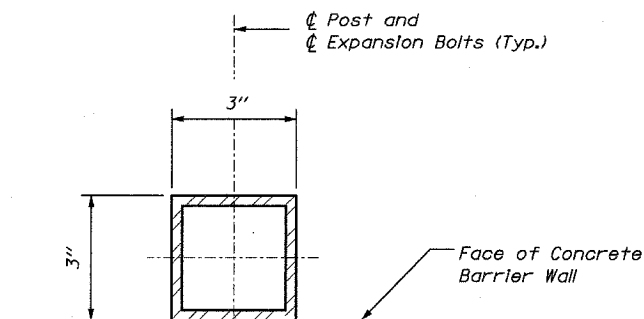
ELEVATION



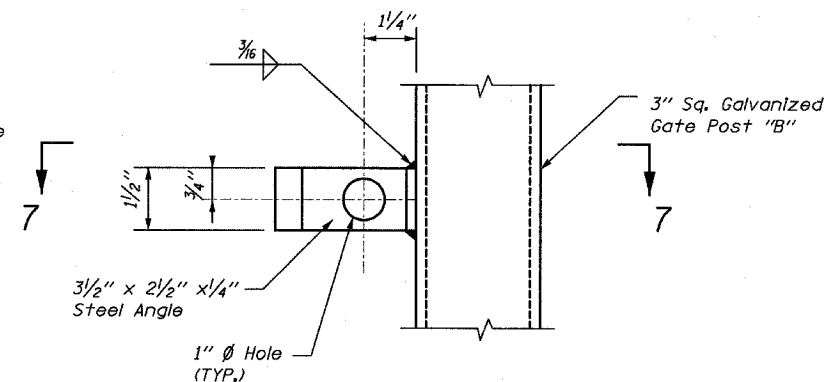
SECTION 6-6

DETAIL B

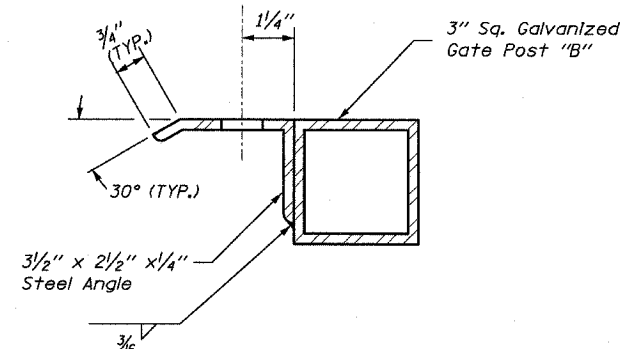
NOTE "A": The Manufacturer of the Sliding Gate Shall Set the Angle Based on the Size of the Track Frame



GATE AND OVERHANG POST SECTION



ELEVATION

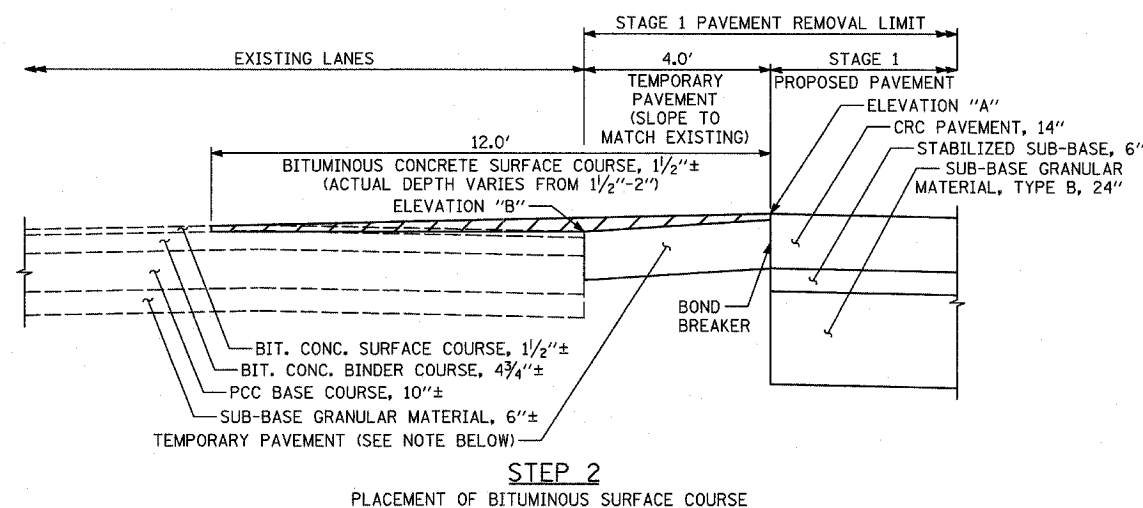
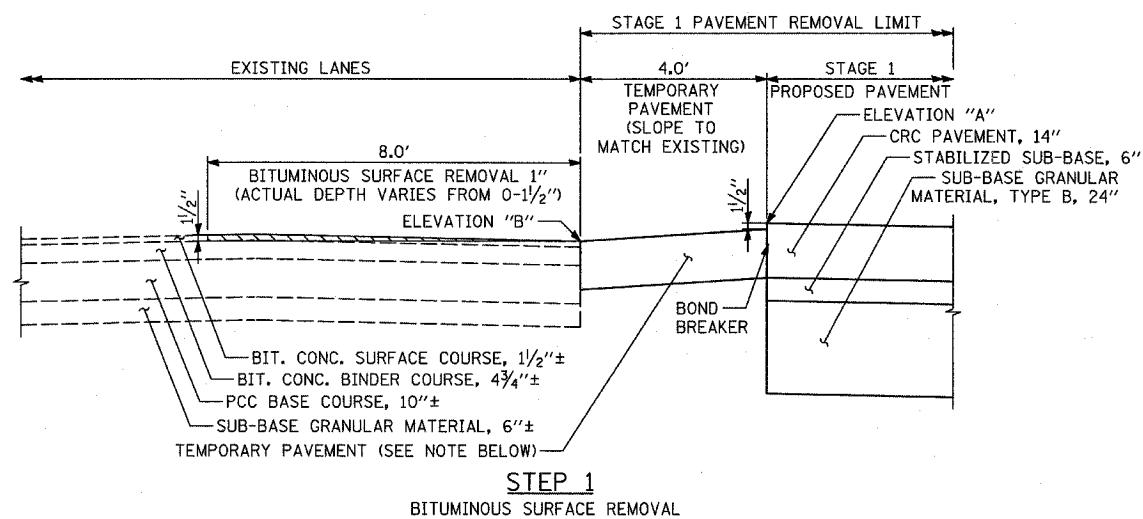
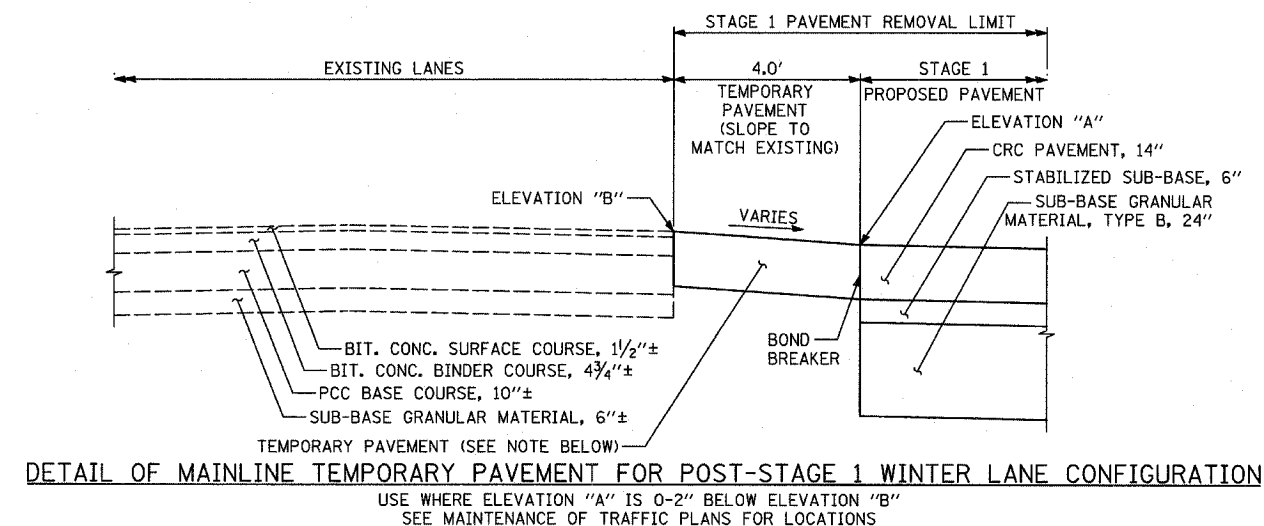
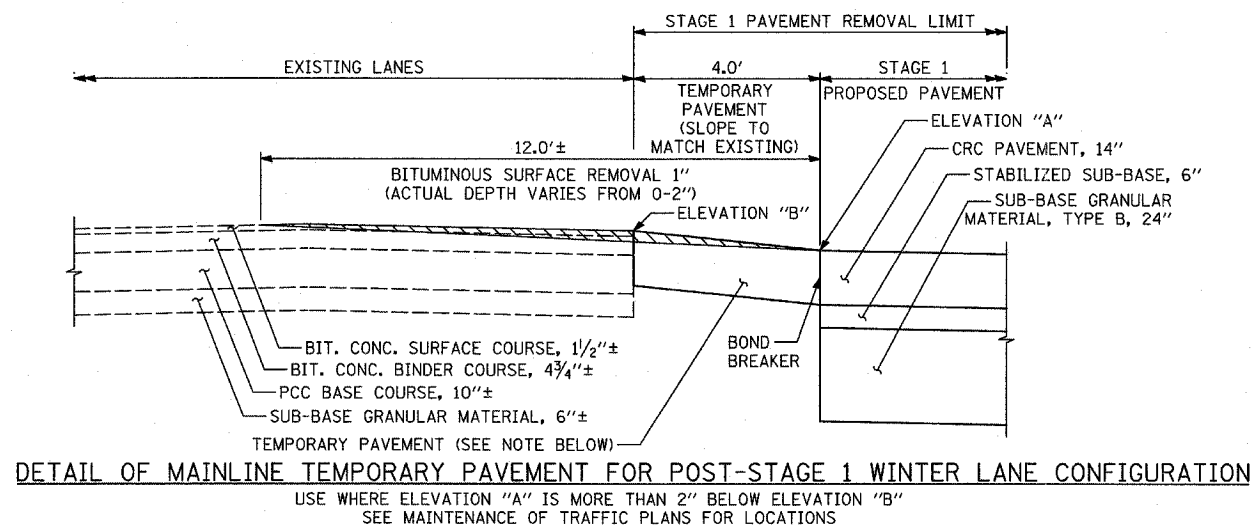


SECTION 7-7

DETAIL C

NOTE: The Slide Gates Shall be of the Cantilevered Type with no Track or Support Across the Opening.

REVISIONS	
NAME	DATE

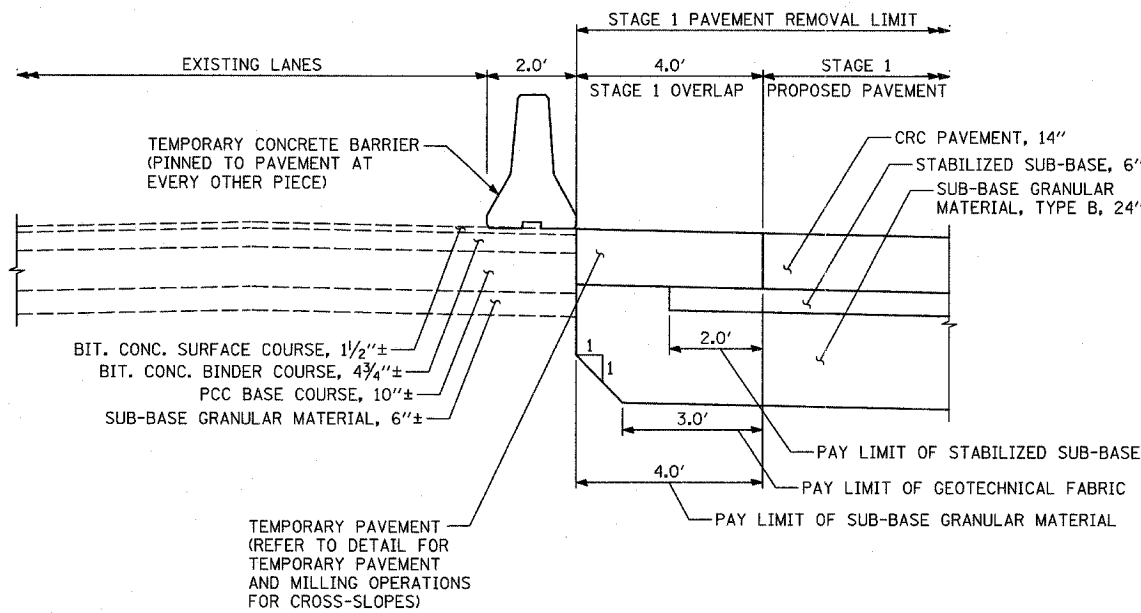


NOTE:

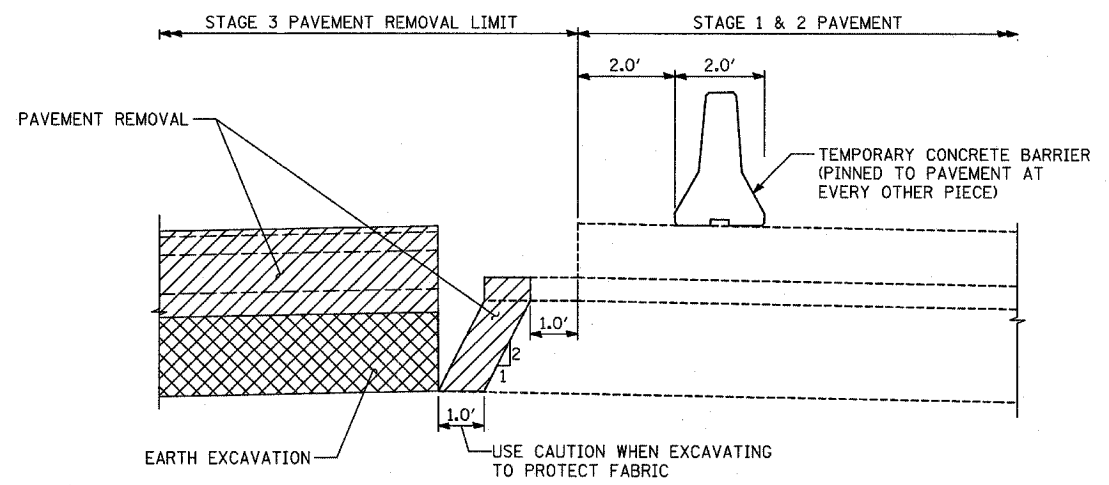
CONTRACTOR MUST USE A 14" THICK CONCRETE TEMPORARY PAVEMENT OPTION. ADDITIONAL THICKNESS OF CONCRETE TEMPORARY PAVEMENT WILL NOT BE PAID FOR SEPARATELY BUT BE INCLUDED IN THE COST OF THE TEMPORARY PAVEMENT.

REVISIONS	
NAME	DATE

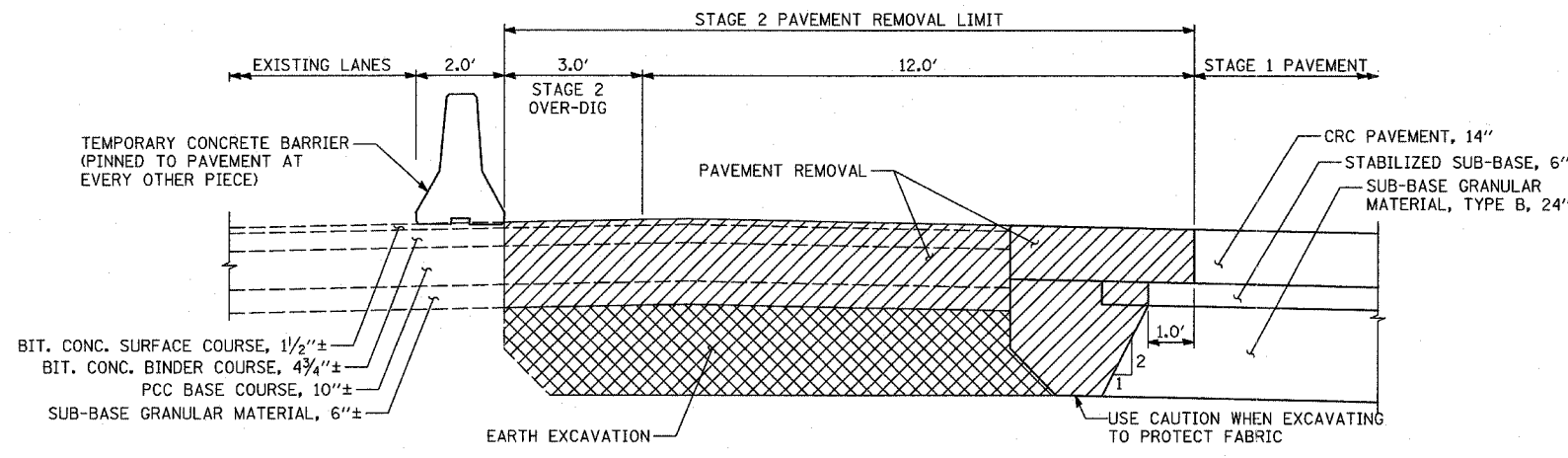
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MISCELLANEOUS DETAILS:
 TEMPORARY PAVEMENT & MILLING OPERATIONS
 SCALE: AS SHOWN
 DATE: MARCH 7, 2006
 DRAWN BY: MPG
 CHECKED BY: TGB



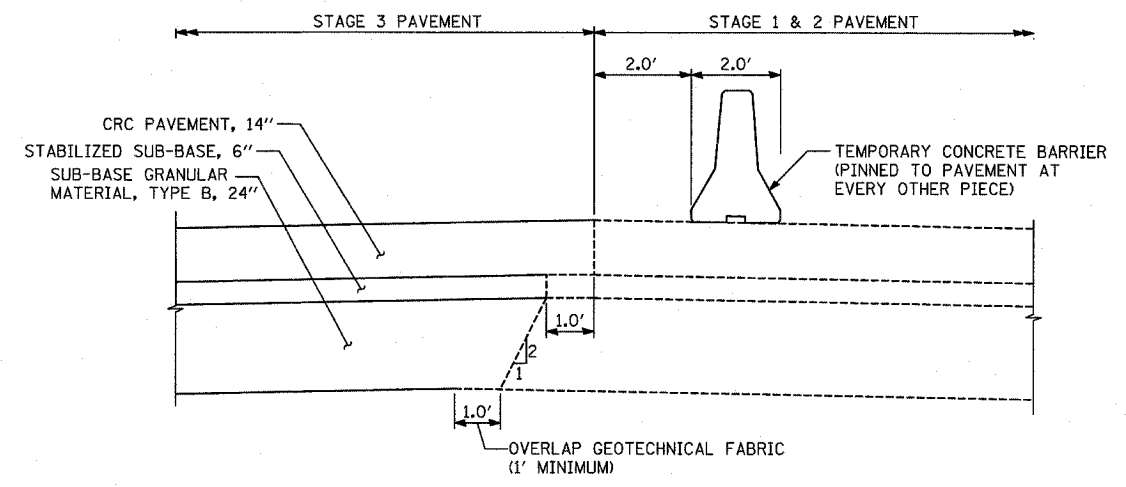
STAGE 1 PAVEMENT



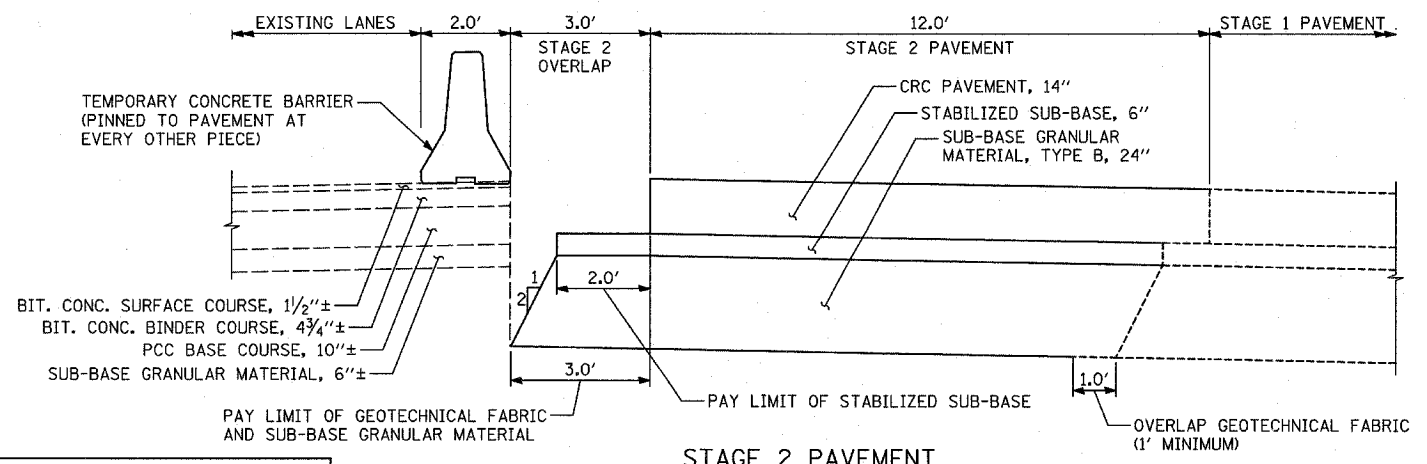
STAGE 3 REMOVAL



STAGE 2 REMOVAL & OVER-DIG



STAGE 3 PAVEMENT



STAGE 2 PAVEMENT

NOTE:
 THESE SECTIONS DEPICT MAINLINE DAN RYAN. SIMILAR SITUATIONS OCCUR ALONG I-57 WITH A LESSER NUMBER OF LANES, BUT SAME CONSTRUCTION REQUIREMENTS.

REVISIONS	
NAME	DATE

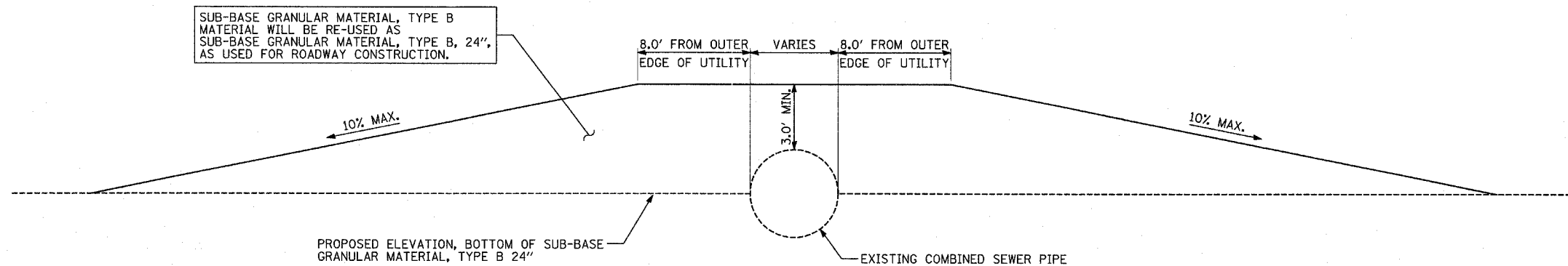
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS:
 OVER-DIG AREAS

SCALE: AS SHOWN
 DATE: MARCH 7, 2006

DRAWN BY: RTM
 CHECKED BY: TGB





COMBINED SEWER CONSTRUCTION PROTECTION
(SEE COMBINED SEWER CROSSING STATIONS)

COMBINED SEWER CROSSING STATIONS:

1. NB I-57 STA. 192+25.30 (60")
2. NB I-57 STA. 231+94.12/
EB I-57 TO I-94 CONN. STA. 407+29.83 (5'-8" x 3'-7" BOX)
3. NB I-94 (BISHOP FORD) STA. 2038+68.07 (7'-6" x 6'-0" DOUBLE BOX)
4. NB I-94 STA. 2245+78.13 (48")
5. NB I-94 STA. 2266+11.42 (DOUBLE 48")
6. NB I-94 STA. 2279+47.75 (TRIPLE 48")

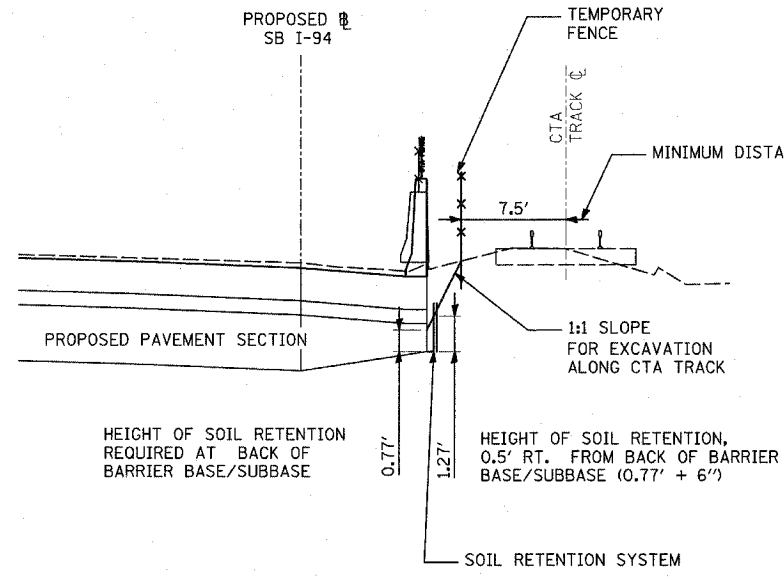
NOTES:

1. CONTRACTOR WILL PROVIDE A MINIMUM COVER, AS SHOWN ABOVE AT ALL TIMES DURING CONSTRUCTION AND PRIOR TO THE INSTALLATION OF THE PERMANENT PROTECTION SLAB AND/OR ROADWAY. CONSTRUCTION TRAFFIC SHALL NOT CROSS THE SEWER PIPE WITHOUT PROTECTION IN PLACE.
2. CONTRACTOR IS RESPONSIBLE FOR THE INTEGRITY OF SEWER PIPE THROUGH THE DURATION OF THE PROJECT, AND WILL NOT BE PAID FOR ANY REPAIRS REQUIRED, AS DETERMINED BY THE ENGINEER, TO THE PIPE, DUE TO CONSTRUCTION ACTIVITIES.

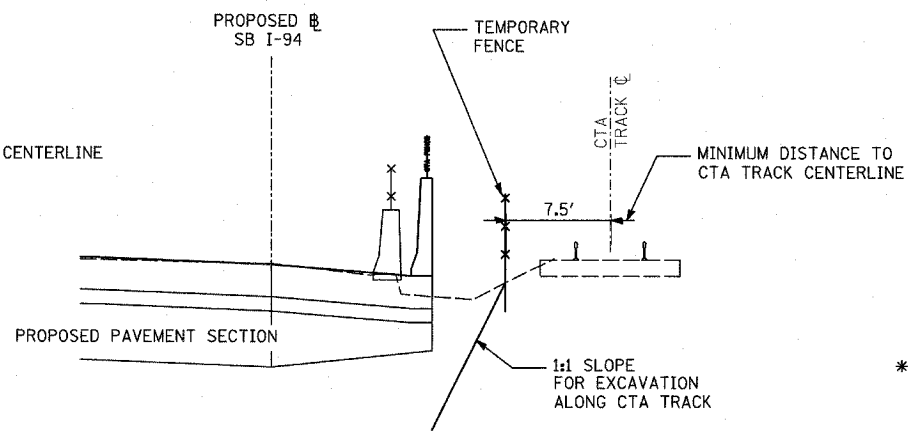
REVISIONS	
NAME	DATE

SOIL RETENTION SCHEDULE

NBDR STATION	HEIGHT OF RETENTION FT (SET 6" BEHIND BARRIER BASE)	SOIL RETENTION AREA SF
2216+00	0.00	0.0
2216+50	0.83	41.5
2217+00	0.75	37.5
2217+50	0.60	30.0
2218+00	0.64	32.0
2218+50	0.85	42.5
2219+00	0.89	44.5
2219+50	0.55	27.5
2220+00	0.81	40.5
2220+50	0.67	33.5
2221+00	0.00	0.0
2259+50	0.00	0.0
2260+00	0.00	0.0
2260+50	0.90	45.0
2261+00	0.00	0.0
* NUMBER OF DRAINAGE STRUCTURES	89	7120.0
** 2207+50 - 2208+29		369.0
** 2253+24 - 2254+45		729.0
*** WB CONNECTOR		3463.0
TOTAL		12055.5



TEMP SOIL SUPPORT REQUIRED



TEMP SOIL SUPPORT NOT REQUIRED

CALCULATION OF HEIGHT OF TEMP SOIL SUPPORT

- * 80 SF OF SOIL RETENTION IS REQUIRED FOR EACH DRAINAGE STRUCTURE. SEE DRAINAGE SCHEDULES FOR DRAINAGE STRUCTURES REQUIRING RETENTION
- ** AREA REFLECTS SOIL RETENTION SYSTEM REQUIRED IN ADDITION TO THAT REQUIRED FOR THE PROPOSED DRAINAGE STRUCTURES, SEE SHEET 121.
- *** AREA REFLECTS SOIL RETENTION SYSTEM REQUIRED FOR TEMPORARY SOIL RETENTION ALONG THE WB CONNECTOR, SEE FOLLOWING SHEET FOR SECTIONS AND SCHEDULE (SHEET 157).

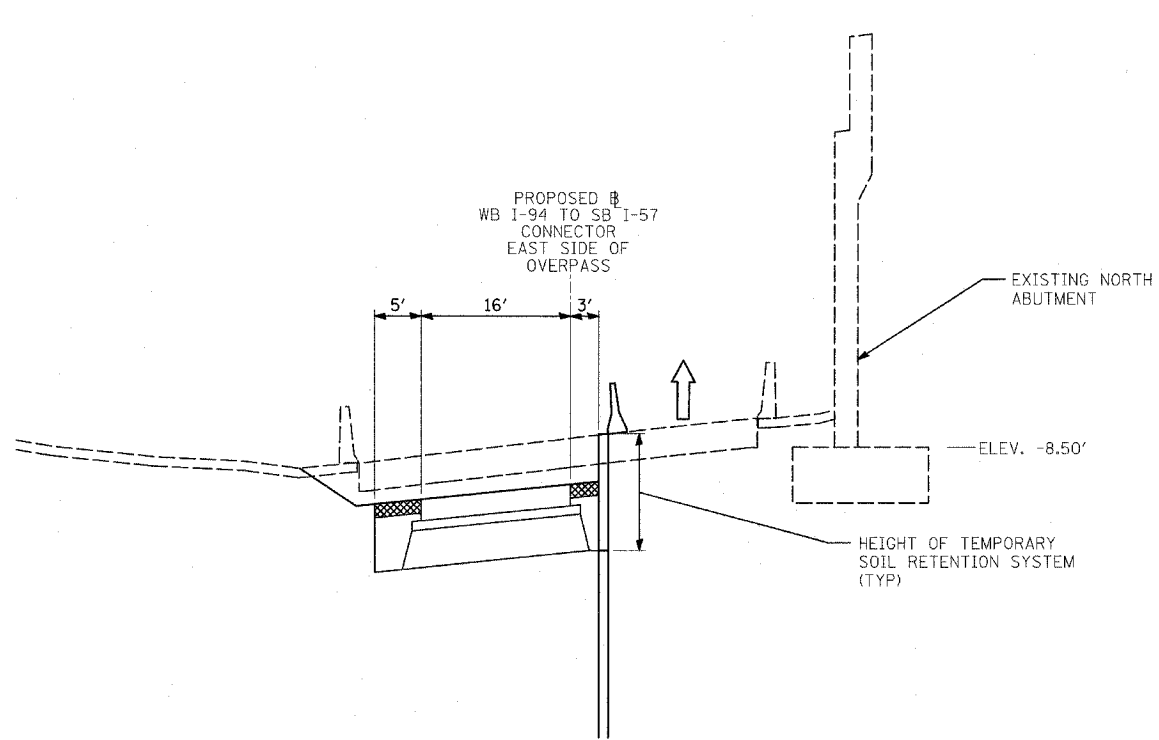
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

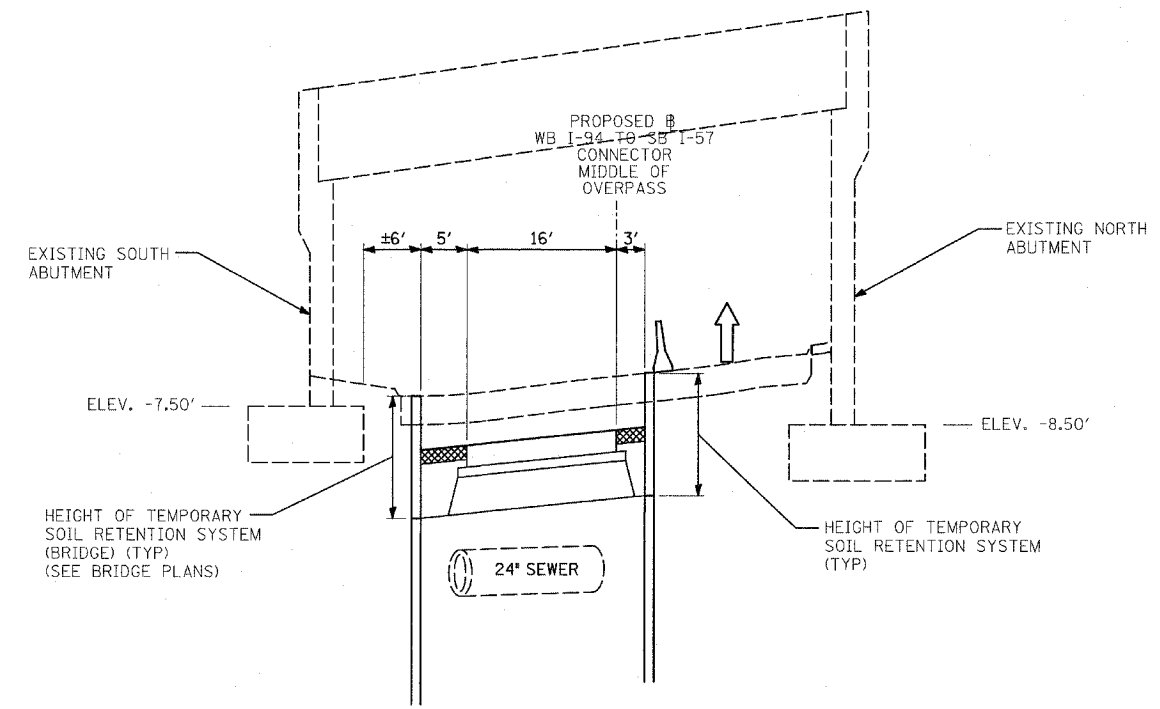
MISCELLANEOUS DETAILS:
 TEMPORARY SOIL RETENTION SECTIONS
 AND SCHEDULE ALONG CTA

SCALE: NONE
 DATE: MARCH 7, 2006

DRAWN BY: JJS
 CHECKED BY: JPM






**STAGE 1: CONSTRUCTION AND SOIL RETENTION
EAST SIDE OF OVERPASS**



**STAGE 1: CONSTRUCTION AND SOIL RETENTION
MIDDLE OF OVERPASS**

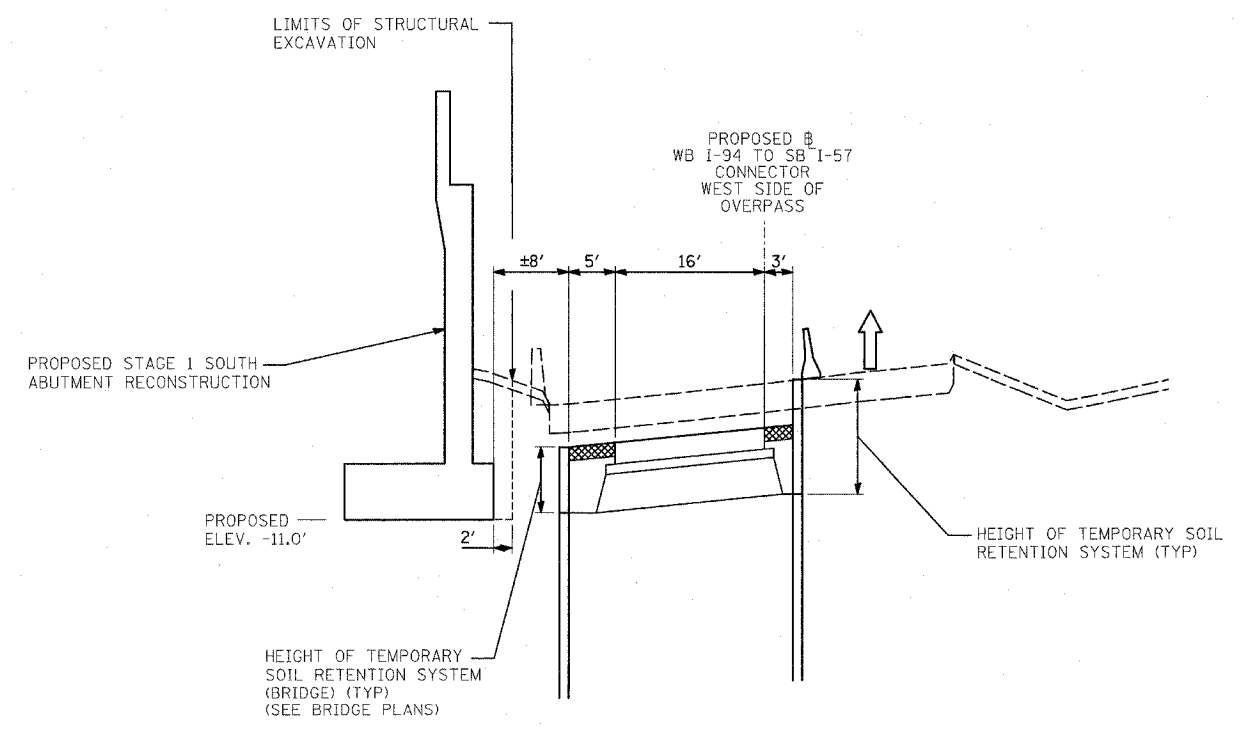
LEGEND:

-  TEMPORARY CONCRETE BARRIER
-  TEMPORARY PAVEMENT
-  TRAVEL LANE

TEMPORARY SOIL RETENTION SCHEDULE ALONG NORTH SIDE OF WB CONNECTOR

WBX STATION	EXISTING PAV'T ELEV. (FT)	PROPOSED PAV'T ELEV. (FT)	HEIGHT OF RETENTION (ELEV. DIFF. PLUS 3.67' FOR EXCAV.) (FT)	SOIL RETENTION AREA (SF)
* 333+00	-13.23	-13.73	4.17	0.00
* 333+25	-12.80	-13.46	4.33	106.25
* 333+50	-12.31	-13.16	4.52	110.63
* 333+75	-11.81	-12.83	4.69	115.13
* 334+00	-11.30	-12.50	4.87	119.50
* 334+25	-10.78	-12.17	5.06	124.13
* 334+50	-10.27	-11.84	5.24	128.75
* 334+75	-9.75	-11.51	5.43	133.38
* 335+00	-9.21	-11.18	5.64	138.38
* 335+25	-8.63	-10.84	5.88	144.00
* 335+37	-8.35	-10.68	6.00	71.28
* 335+50	-8.06	-10.51	6.12	78.78
* 335+75	-7.54	-10.17	6.30	155.25
* 336+00	-7.02	-9.77	6.42	159.00
* 336+25	-6.45	-9.30	6.52	161.75
* 336+50	-5.90	-8.78	6.55	163.38
* 336+75	-5.35	-8.19	6.51	163.25
* 337+00	-4.81	-7.54	6.40	161.38
* 337+25	-4.24	-6.82	6.25	158.13
* 337+50	-3.67	-6.04	6.04	153.63
* 337+61	-3.43	-5.68	5.92	65.78
* 337+75	-3.07	-5.20	5.80	82.04
* 338+00	-2.43	-4.30	5.54	141.75
* 338+25	-1.84	-3.34	5.17	133.88
* 338+50	-1.23	-2.38	4.82	124.88
* 338+75	-0.68	-1.43	4.42	115.50
* 339+00	-0.10	-0.52	4.09	106.38
* 339+25	0.48	0.33	3.82	98.88
* 339+50	N/A	N/A	0.00	47.75
TEMPORARY SOIL RETENTION SYSTEM TOTAL:				3463.0

* DENOTES AREAS WHERE THE FULL DEPTH OF THE PROPOSED PAVEMENT IS REQUIRED TO BE RETAINED. THE ADDITIONAL DEPTH OF 3.67 FT IS INCLUDED IN THE HEIGHT OF RETENTION CALCULATION.



**STAGE 1: CONSTRUCTION AND SOIL RETENTION
WEST SIDE OF OVERPASS**

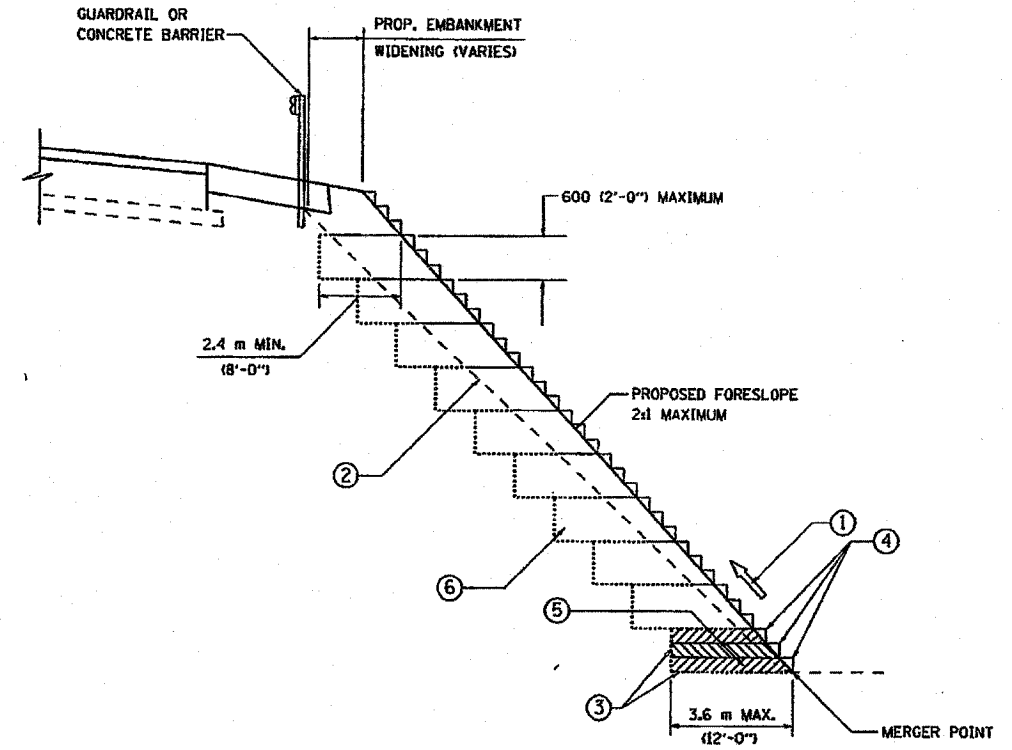
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS
TEMPORARY SOIL RETENTION
ALONG THE WB CONNECTOR
SECTIONS AND SCHEDULE

SCALE: NONE DRAWN BY: JDF
DATE: MARCH 7, 2006 CHECKED BY: TGB

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	916	157A
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4				
				62304



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

EARTH EXCAVATION (SPECIAL) SCHEDULE

ALIGNMENT	STATION		EARTH EXCAVATION (SPECIAL) (CU YD)
	FROM	TO	
NB I-57	236+50	236+50	46
NB I-57	240+00	244+00	385
NB I-57	247+00	251+00	294
	TOTAL		725

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 200 (8-INCH) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR *. THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5)

* - EARTH EXCAVATION (SPECIAL)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

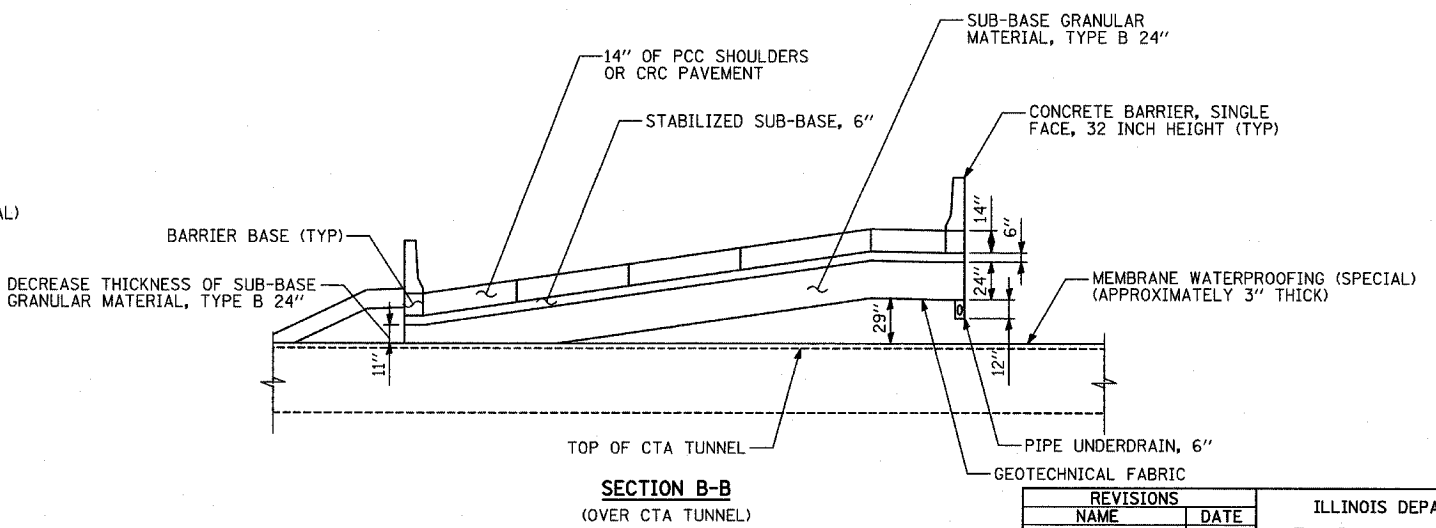
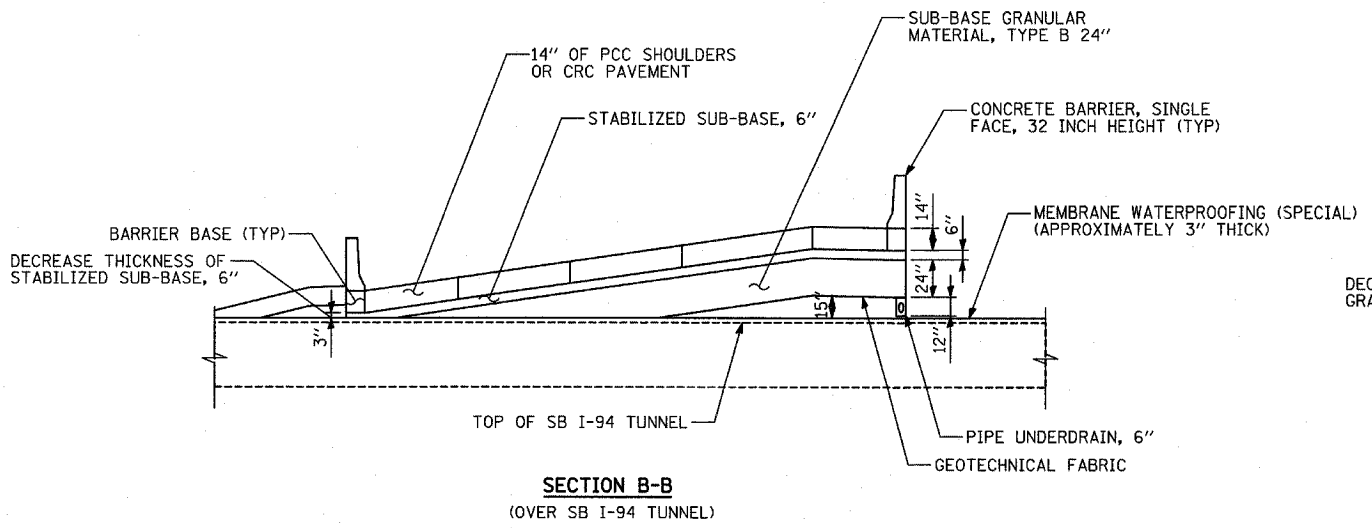
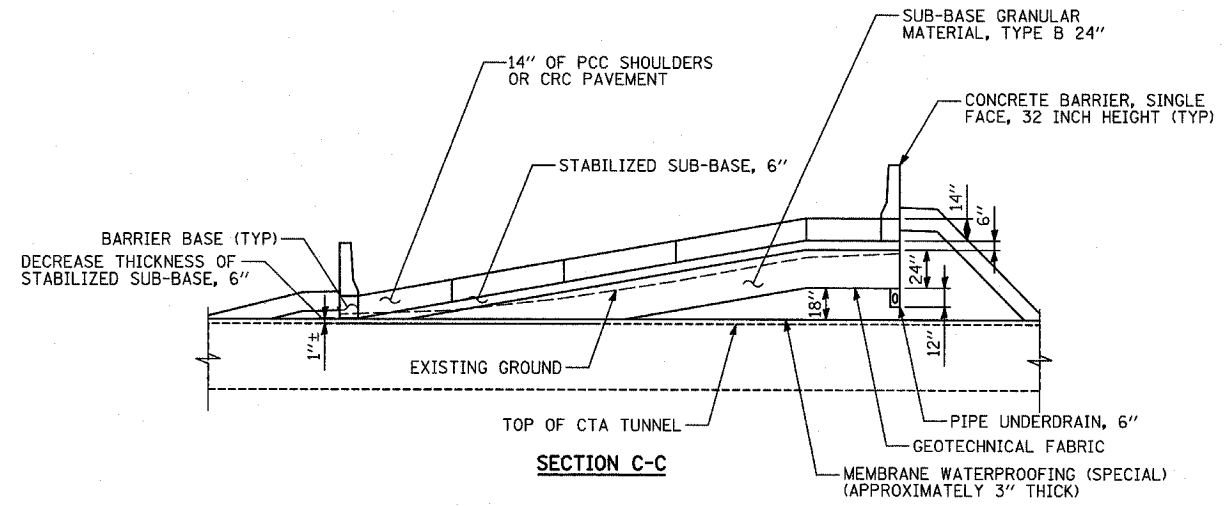
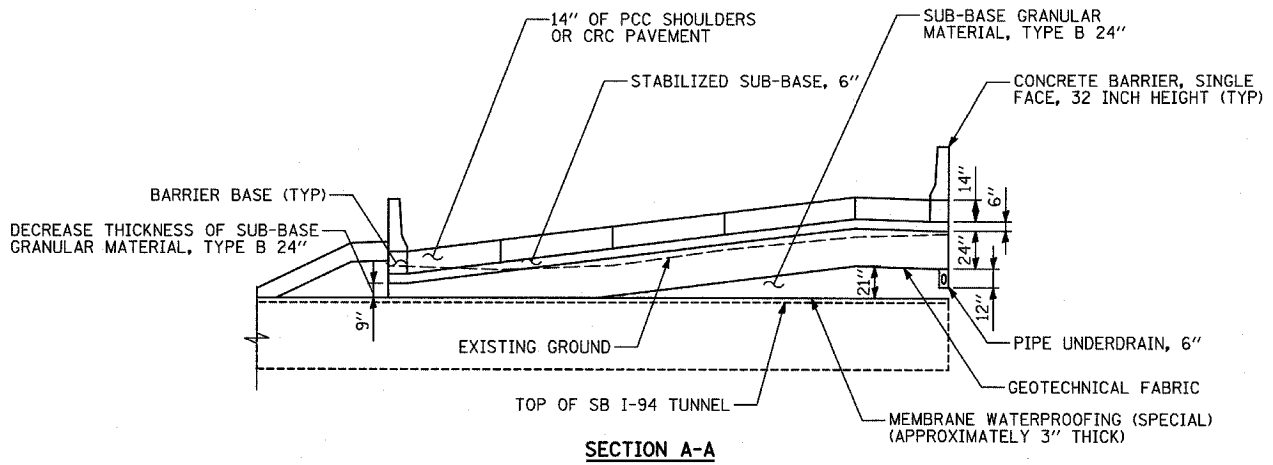
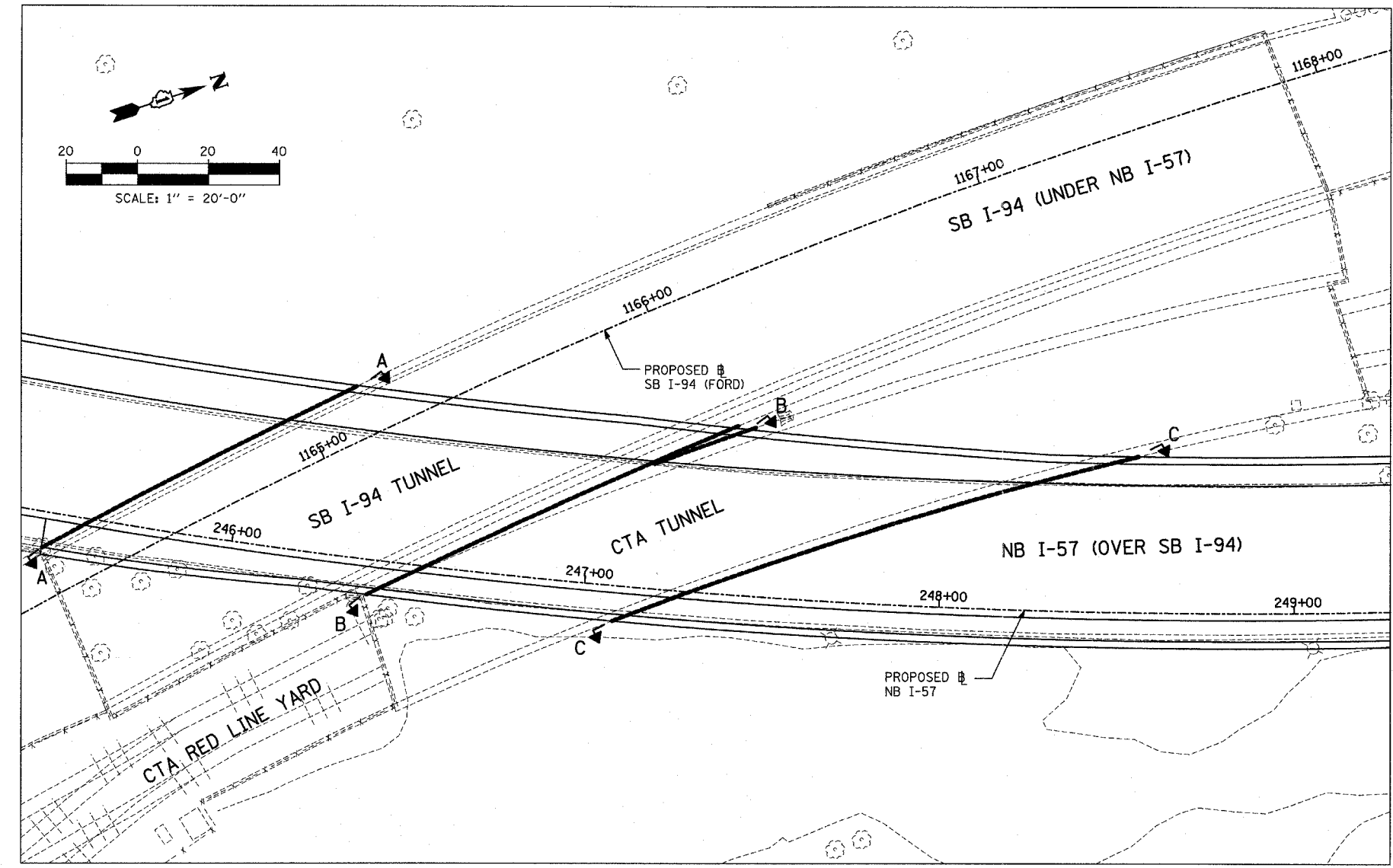
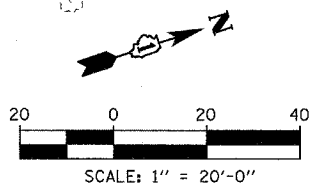
REVISIONS	
NAME	DATE

**BENCHING DETAIL
FOR EMBANKMENT
WIDENING**

SCALE: VERT. HORIZ. DATE 3/16/2005

DRAWN BY: CADD CHECKED BY: S.E.B.

F.A.I. RTE. 94	SECTION *	COUNTY COOK	TOTAL SHEETS 916	SHEET NO. 158
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 62304				



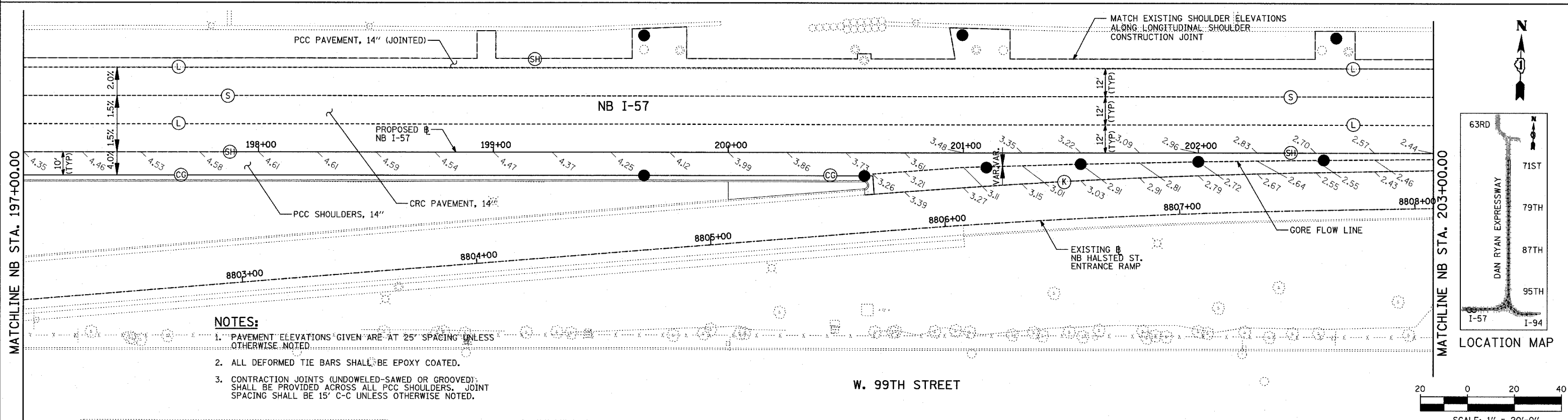
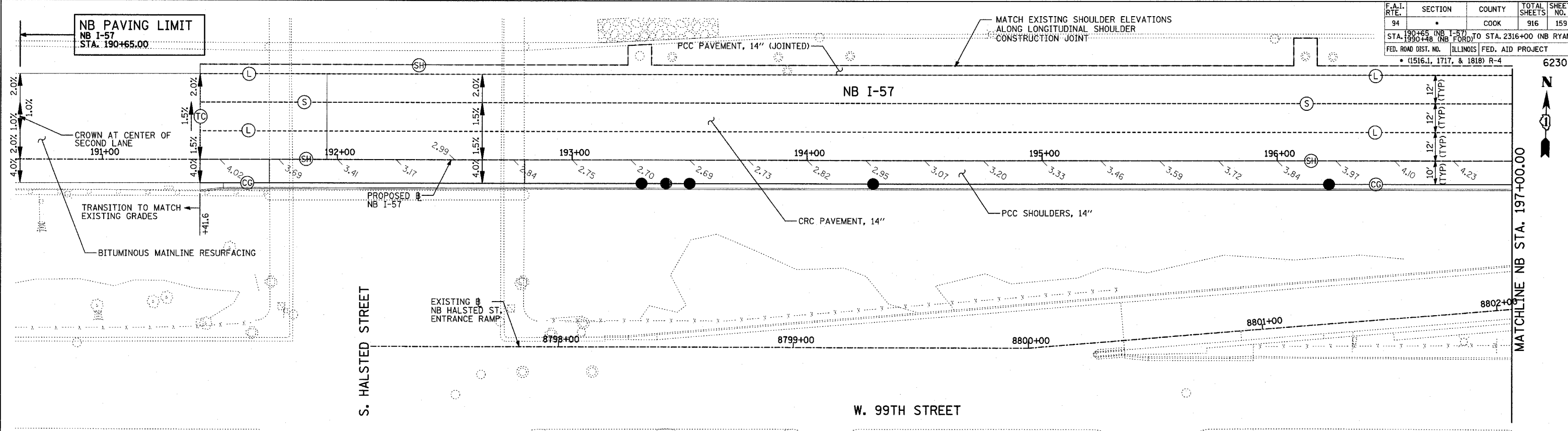
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MISCELLANEOUS DETAILS:
 TUNNEL TYPICAL SECTIONS

SCALE: PLAN: 1"=20'
 SECTIONS: N.T.S.
 DATE: MARCH 7, 2006
 DRAWN BY: RTM
 CHECKED BY: TGB

TYLIN INTERNATIONAL

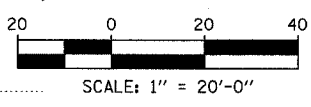
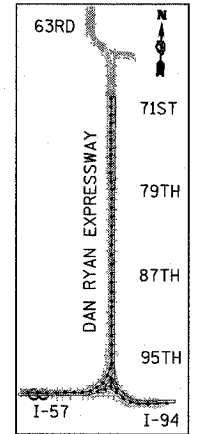
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	916	159
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4				



- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED.
 - ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
 - (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
 - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
 - (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
 - (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
 - (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

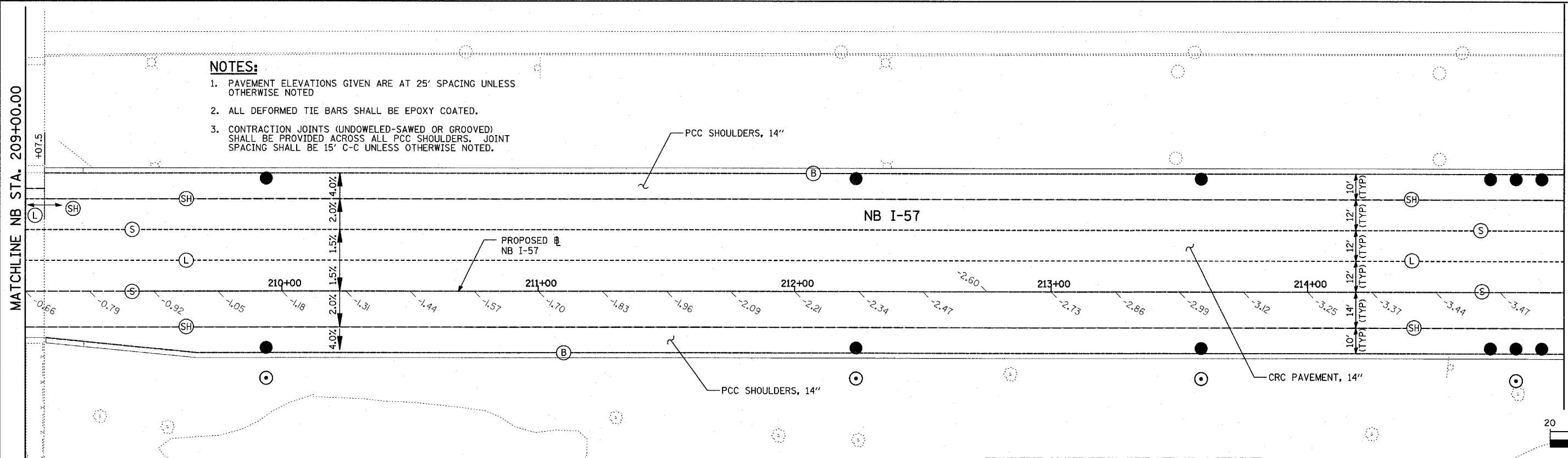
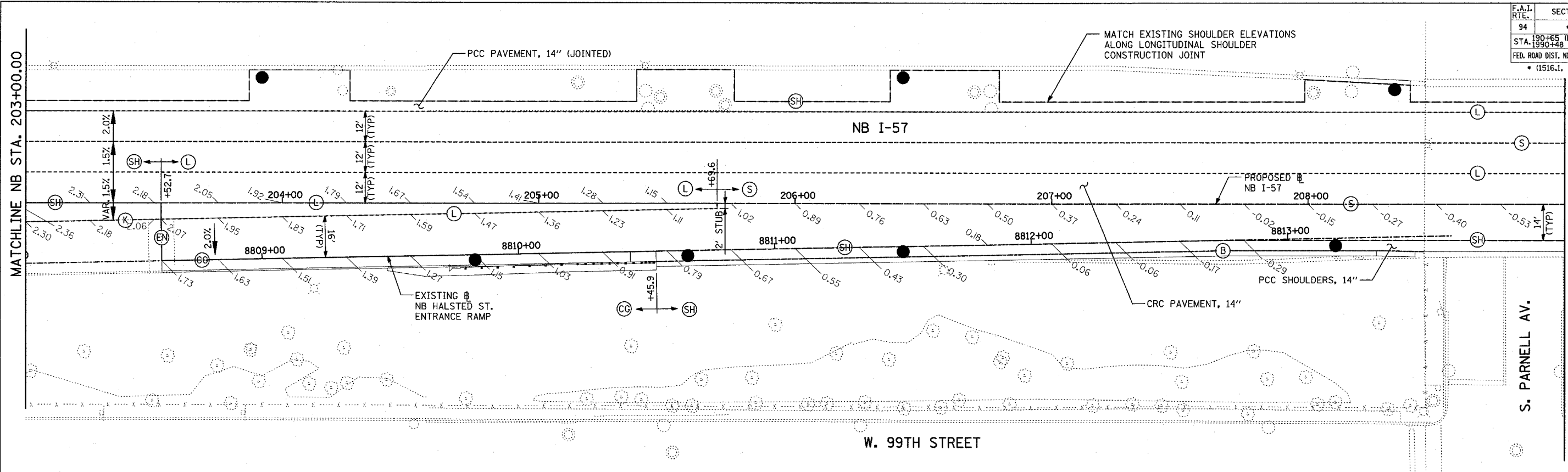
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ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-57
STA. 190+65.00 TO 203+00.00
(SHEET 1 OF 28)

SCALE: 1"=20'
DATE: MARCH 7, 2006
DRAWN BY: JJS
CHECKED BY: MPG



- NOTES:**
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)

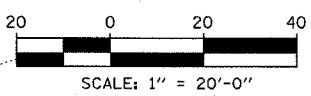
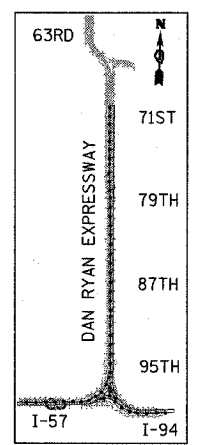
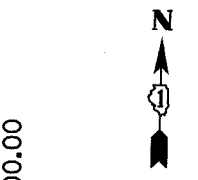
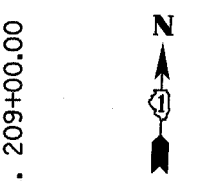
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)

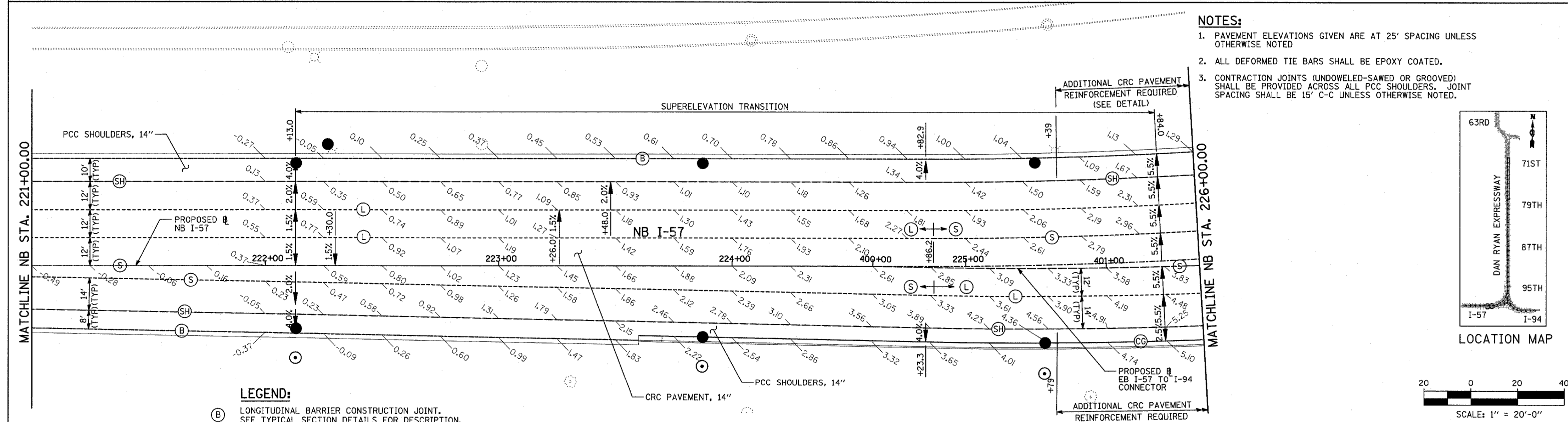
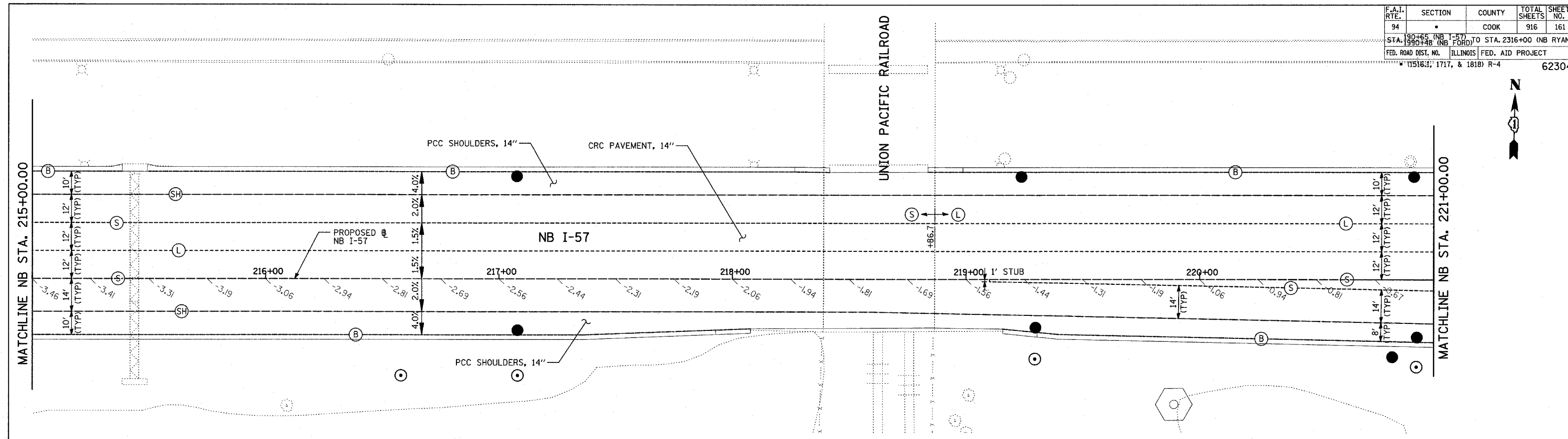
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

REVISIONS	
NAME	DATE

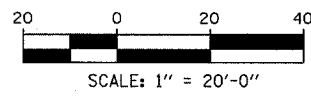
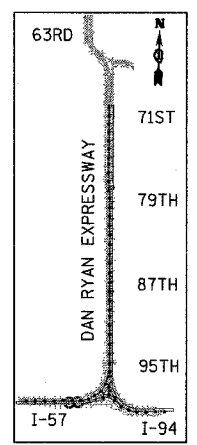
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 NB I-57
 STA. 203+00.00 TO 215+00.00
 (SHEET 2 OF 28)
 SCALE: 1"=20'
 DATE: MARCH 7, 2006
 DRAWN BY: JJS
 CHECKED BY: MPG

TYLIN INTERNATIONAL





- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 - ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



LEGEND:

- | | | |
|--|---|--|
| (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION. | (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT) | (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) |
| (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER) | (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) | (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS). |
| (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT) | (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) | (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT) |
| (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT) | | |

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-57
STA. 215+00.00 TO 226+00.00
(SHEET 3 OF 28)

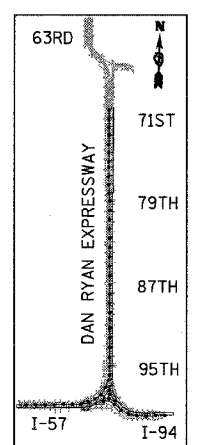
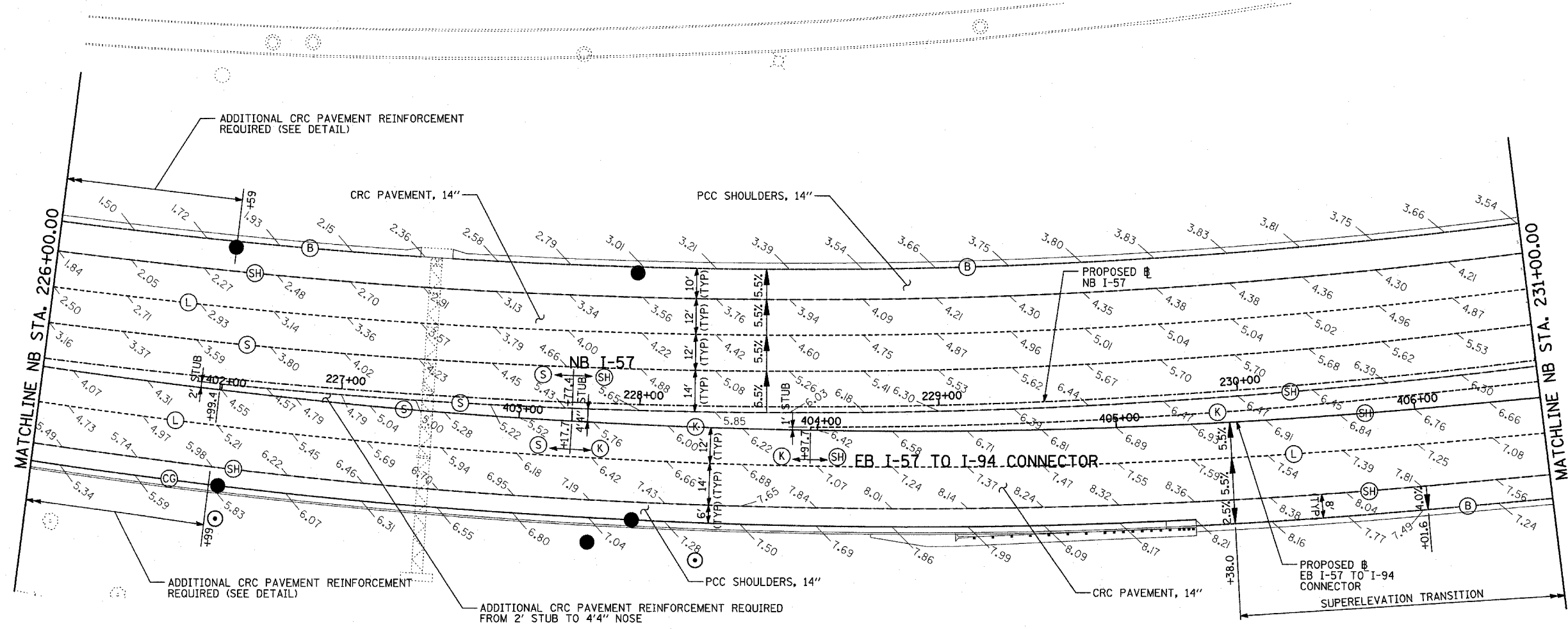
SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: JJS
CHECKED BY: MPC

TYLIN INTERNATIONAL



SB I-57



LOCATION MAP

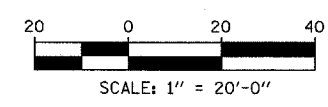
LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CC) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)

- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

NOTES:

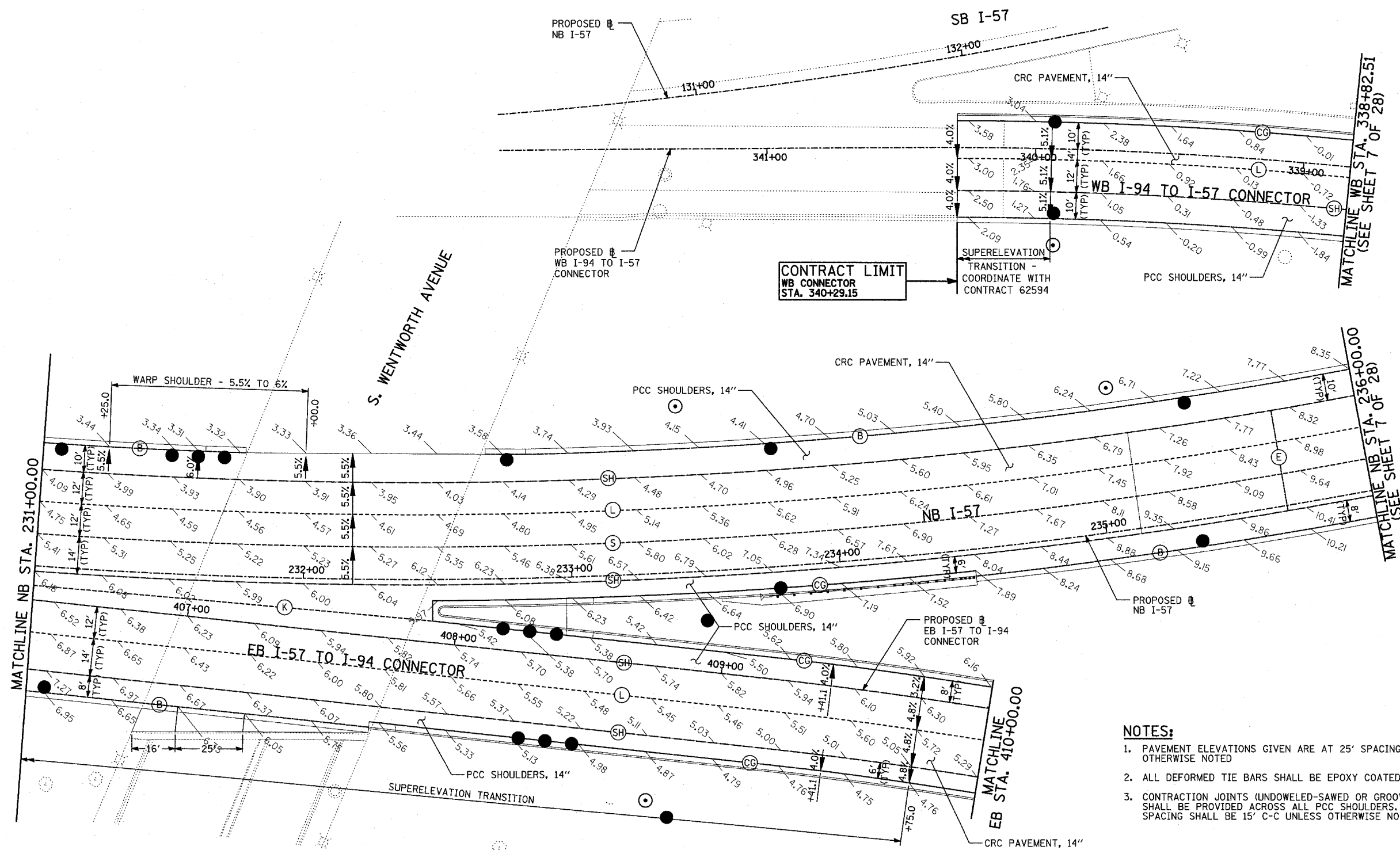
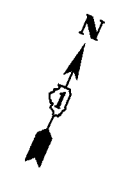
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
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TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-57
STA. 226+00.00 TO 231+00.00
(SHEET 4 OF 28)
 SCALE: 1"=20'
 DATE: MARCH 7, 2006
 DRAWN BY: JUS
 CHECKED BY: MPG



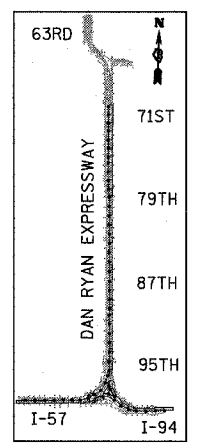
CONTRACT LIMIT
WB CONNECTOR
STA. 340+29.15

NOTES:

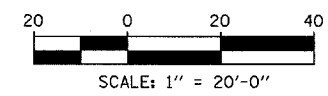
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
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3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)



LOCATION MAP



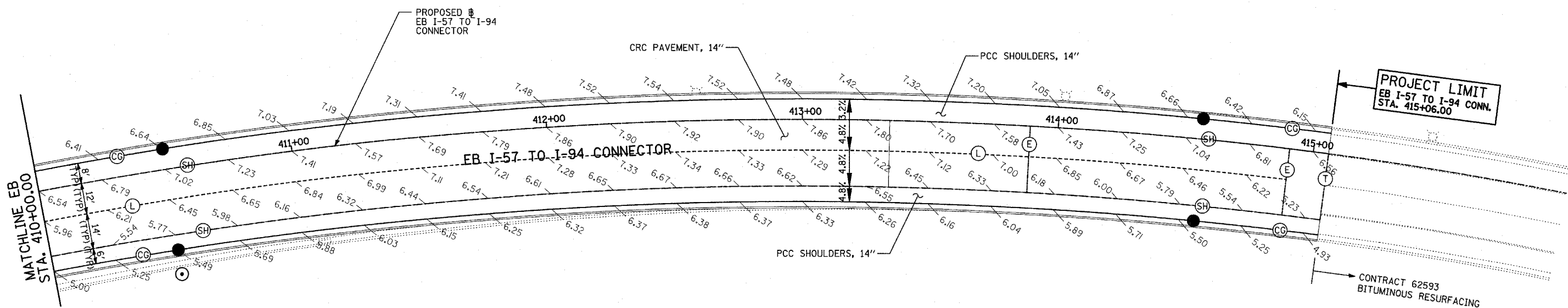
TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-57 AND EB I-57 TO I-94 CONNECTOR
NB I-57 STA. 231+00.00 TO 236+00.00
(SHEET 5 OF 28)

SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: JJS
CHECKED BY: MPG

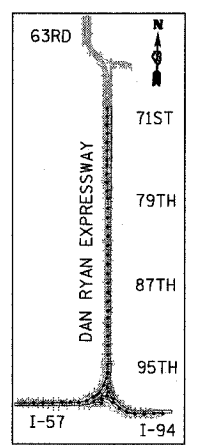


PROJECT LIMIT
 EB I-57 TO I-94 CONN.
 STA. 415+06.00

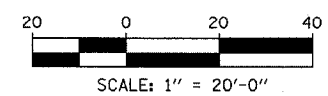
CONTRACT 62593
 BITUMINOUS RESURFACING

LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
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- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)



LOCATION MAP



NOTES:

1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

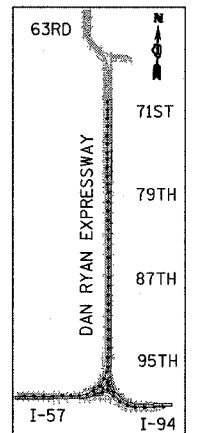
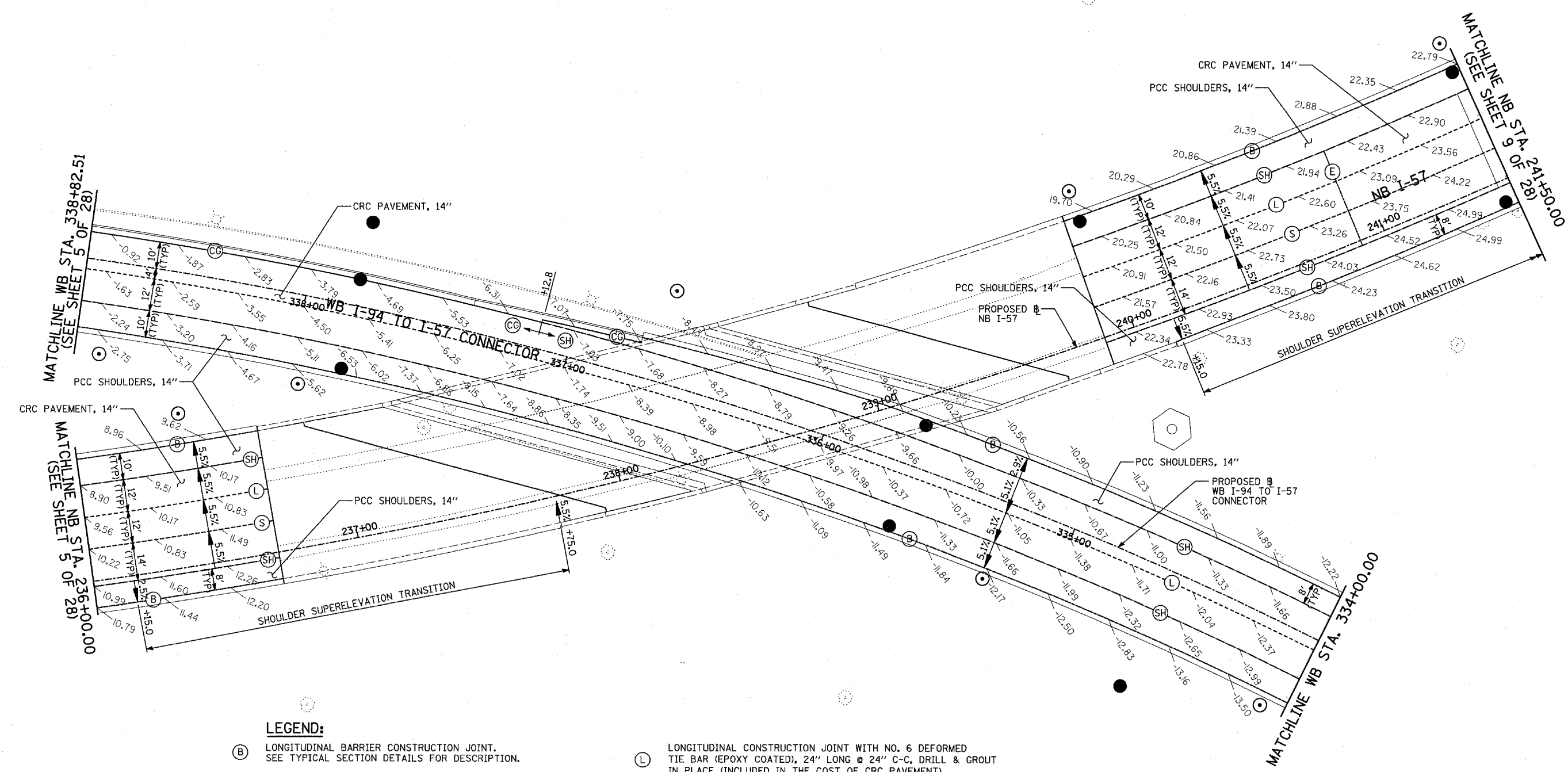
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 EB I-57 TO I-94 CONNECTOR
 STA. 410+00.00 TO 415+06.00
 (SHEET 6 OF 28)

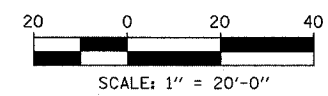
SCALE: 1"=20'
 DATE: MARCH 7, 2006

DRAWN BY: JJS
 CHECKED BY: MPG

TYLIN INTERNATIONAL



LOCATION MAP



LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
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- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF JOINT PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

NOTES:

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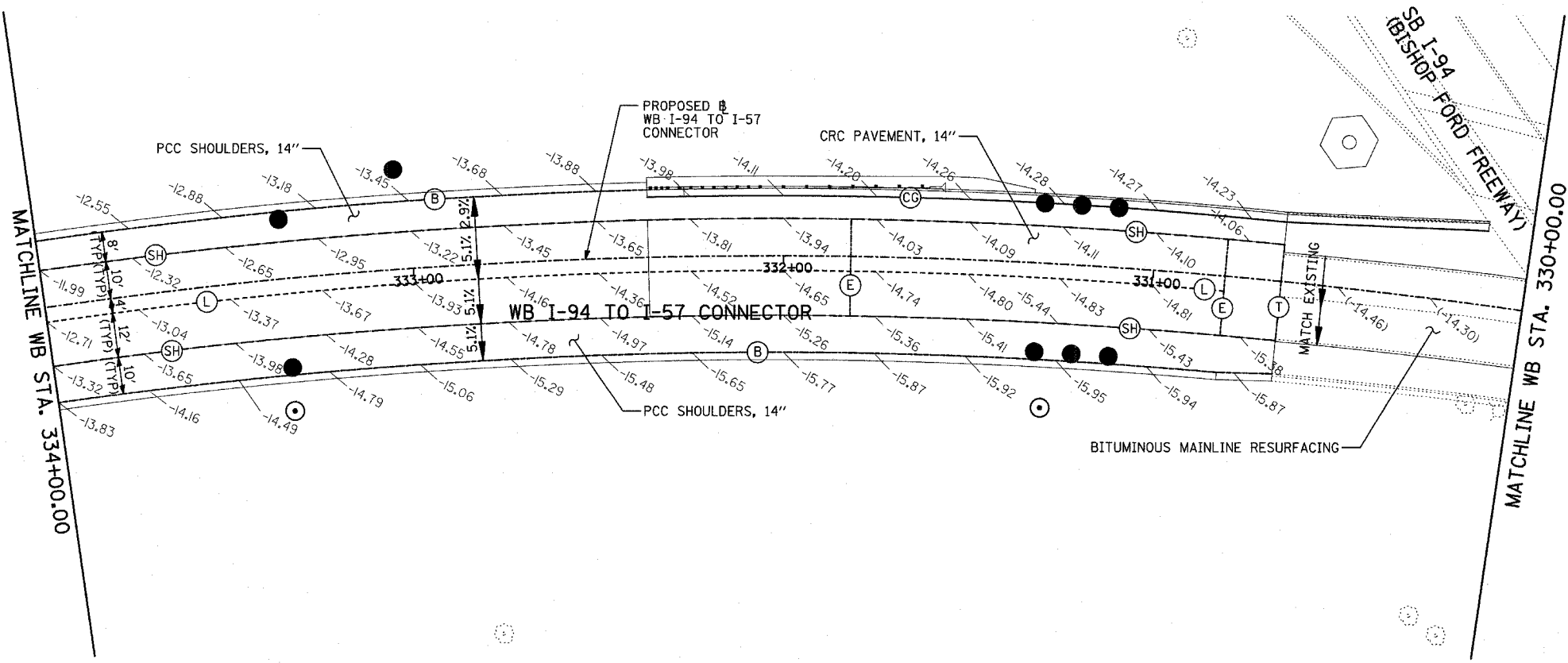
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-57
STA. 236+00.00 TO 241+50.00
(SHEET 7 OF 28)

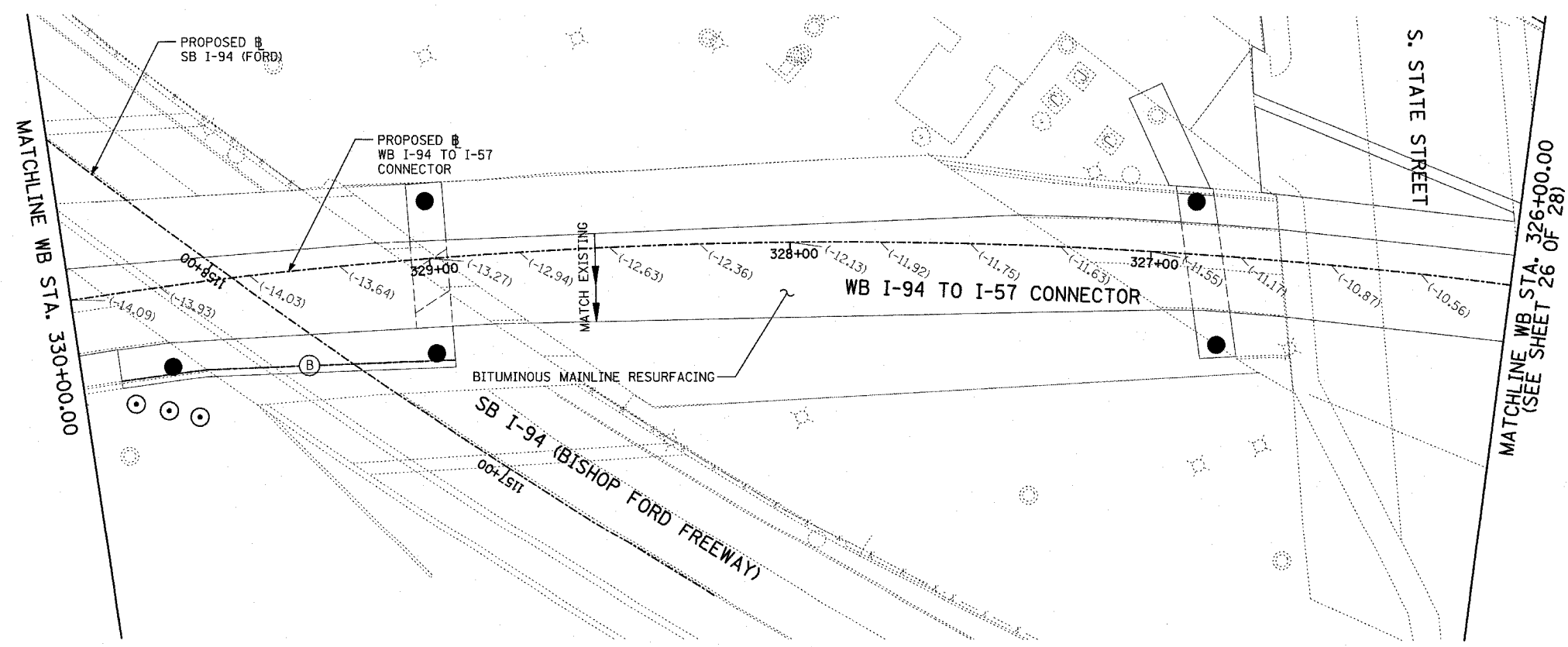
SCALE: 1"=20'
DATE: MARCH 7, 2006
DRAWN BY: JJS
CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	916	166
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4				

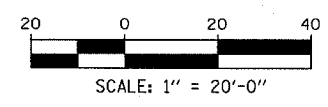
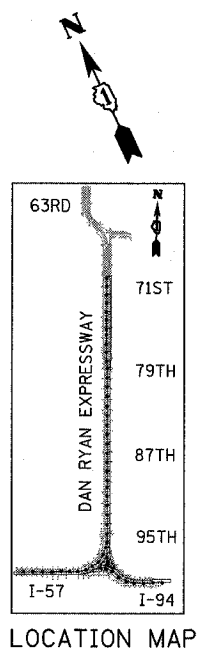
62304



- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
 - (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
 - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
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 - (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
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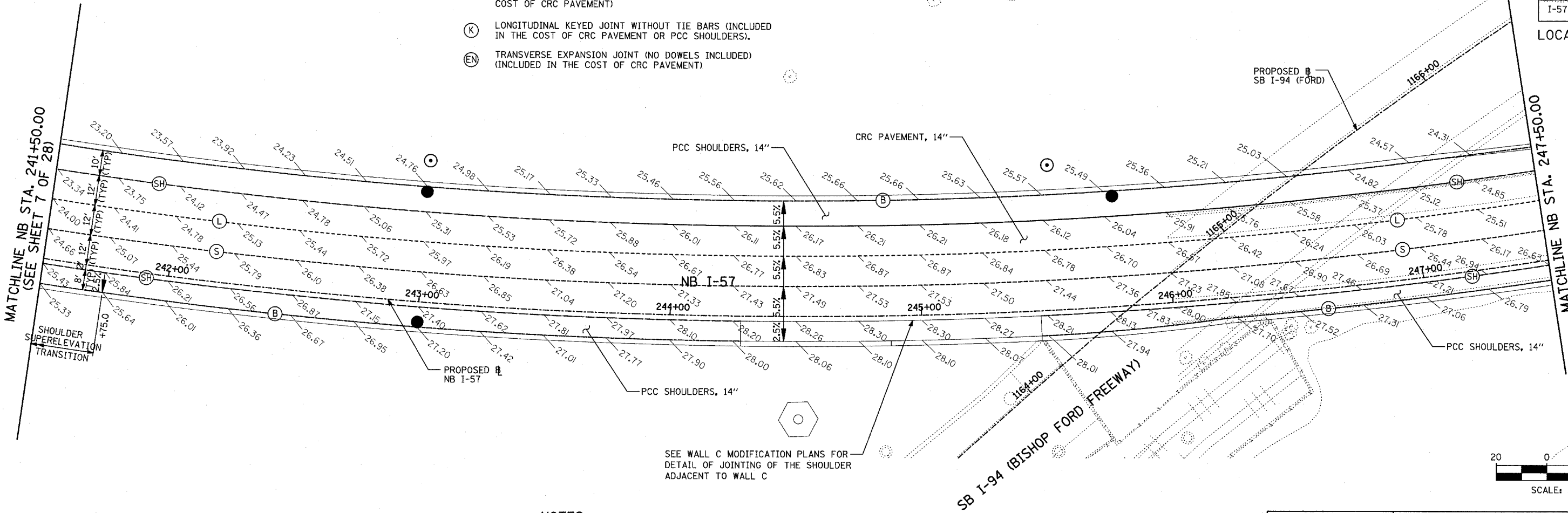
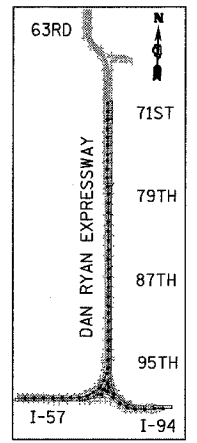
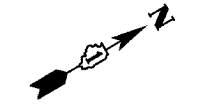
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 WB I-94 TO I-57 CONNECTOR
 STA. 326+00.00 TO 334+00.00
 (SHEET 8 OF 28)
 SCALE: 1"=20'
 DATE: MARCH 7, 2006
 DRAWN BY: JJS
 CHECKED BY: MPG

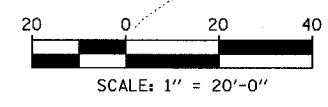
TYLIN INTERNATIONAL

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- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
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- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)



SEE WALL C MODIFICATION PLANS FOR
DETAIL OF JOINTING OF THE SHOULDER
ADJACENT TO WALL C



NOTES:

1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
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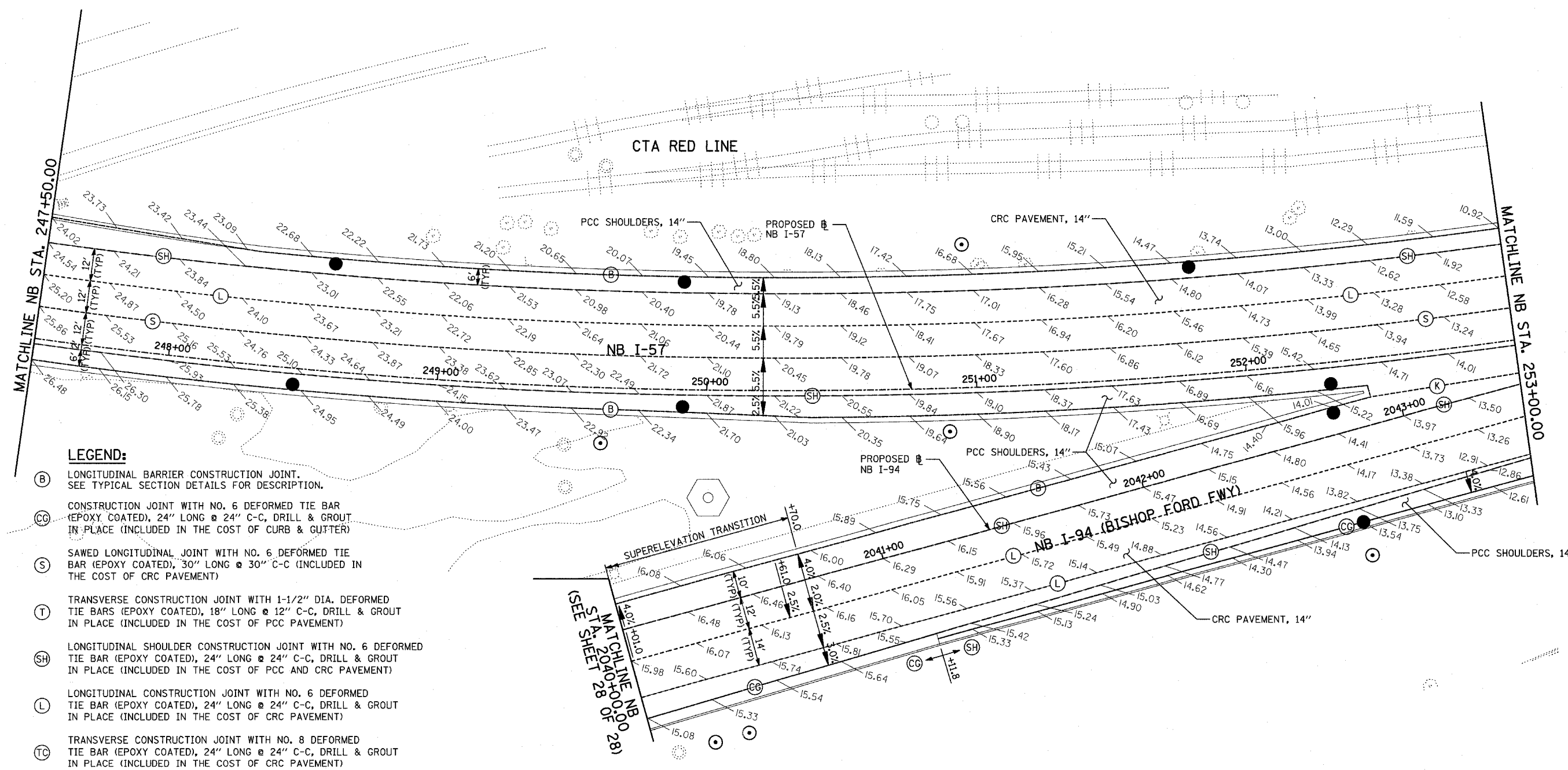
TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-57 AND EB I-57 TO I-94 CONNECTOR
NB I-57 STA. 241+50.00 TO 247+50.00
(SHEET 9 OF 28)

SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: JJS
CHECKED BY: MPG

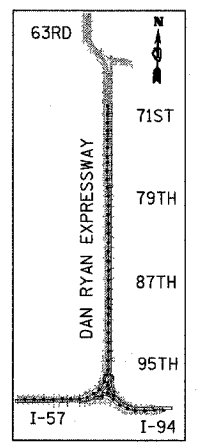


LEGEND:

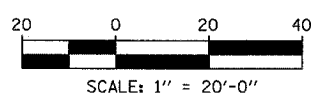
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

NOTES:

1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



LOCATION MAP



TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-57 AND NB I-94 (BISHOP FORD FWY)
NB I-57 STA. 247+50.00 TO 253+00.00
(SHEET 10 OF 28)

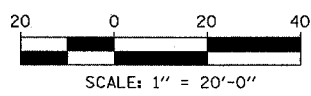
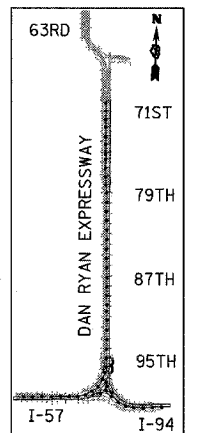
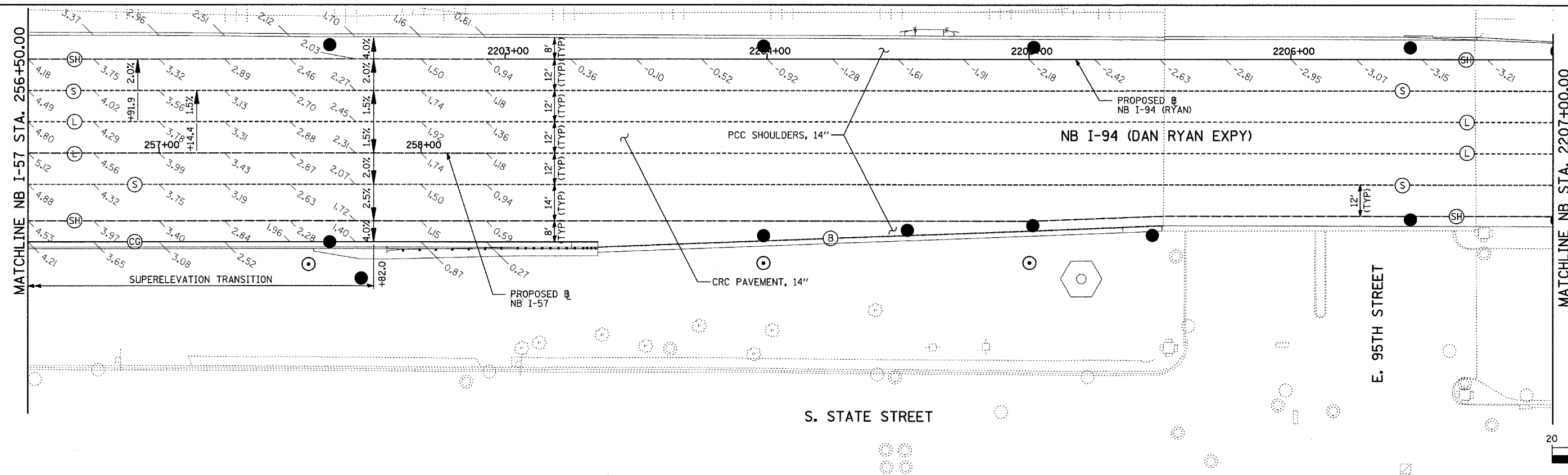
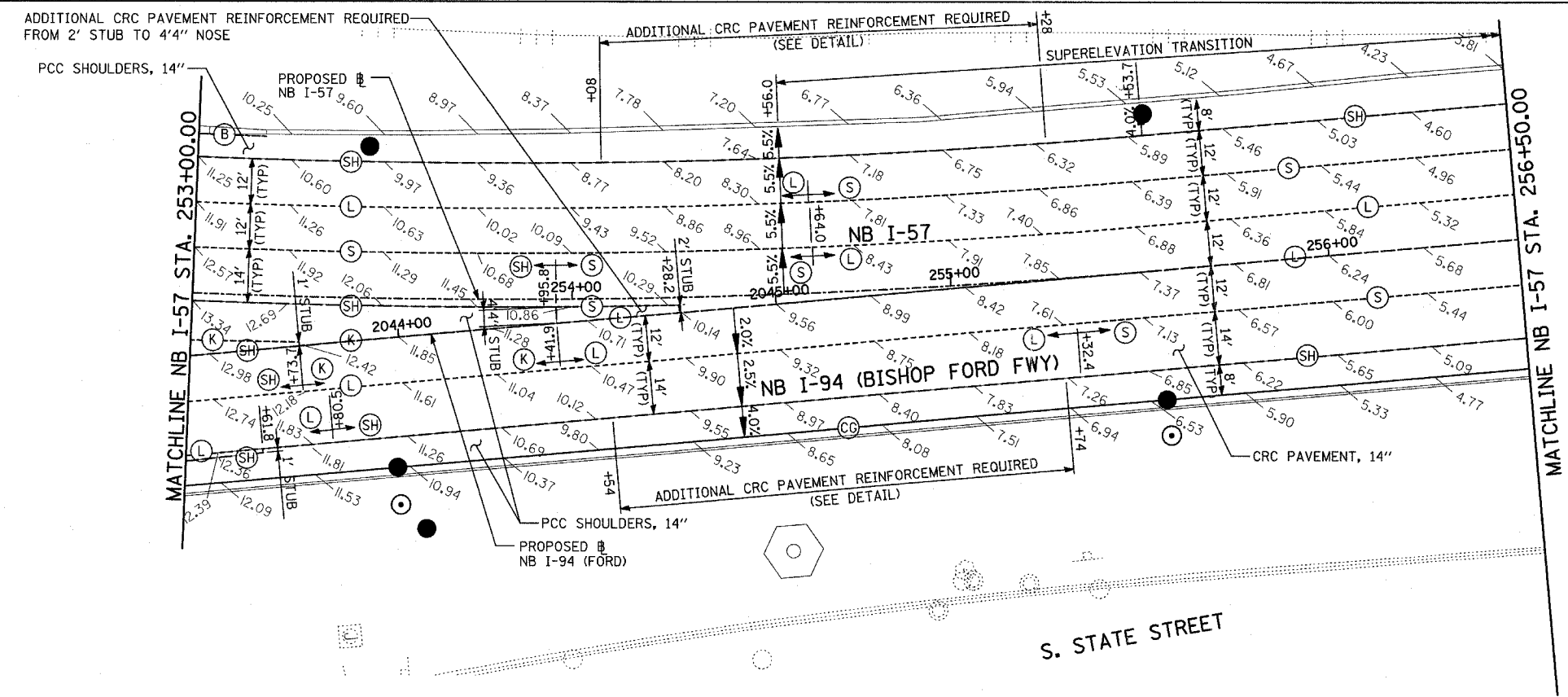
SCALE: 1"=20'
 DATE: MARCH 7, 2006

DRAWN BY: JJS
 CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	169
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4				

LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)



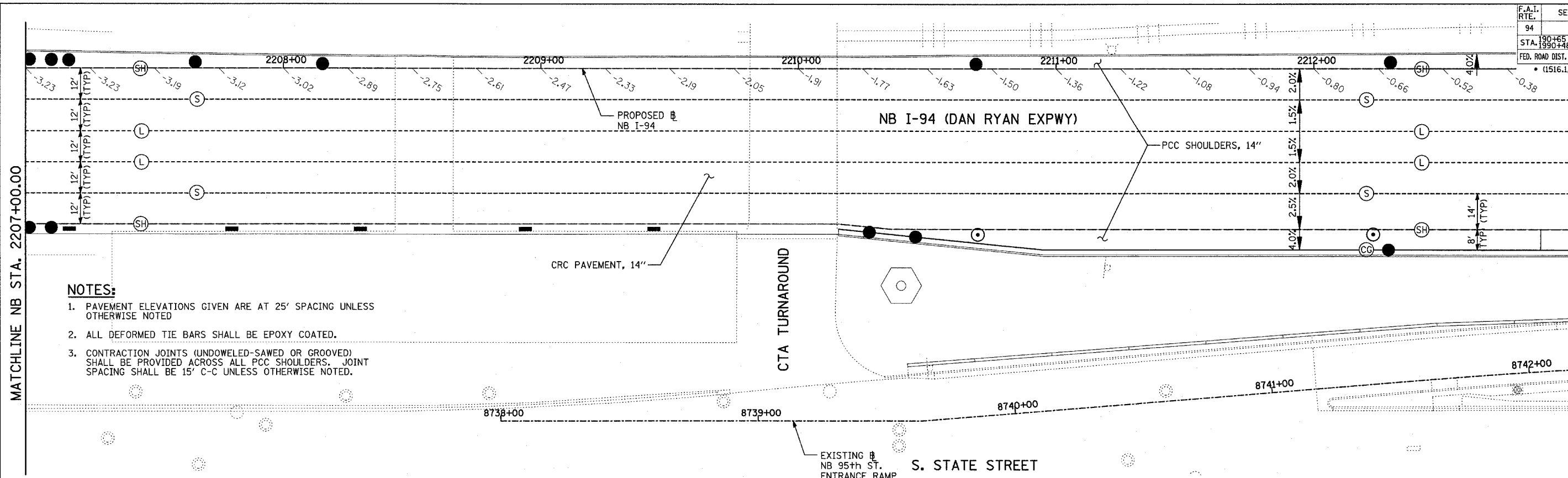
NOTES:

1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

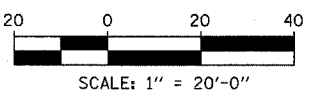
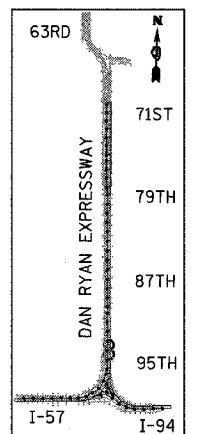
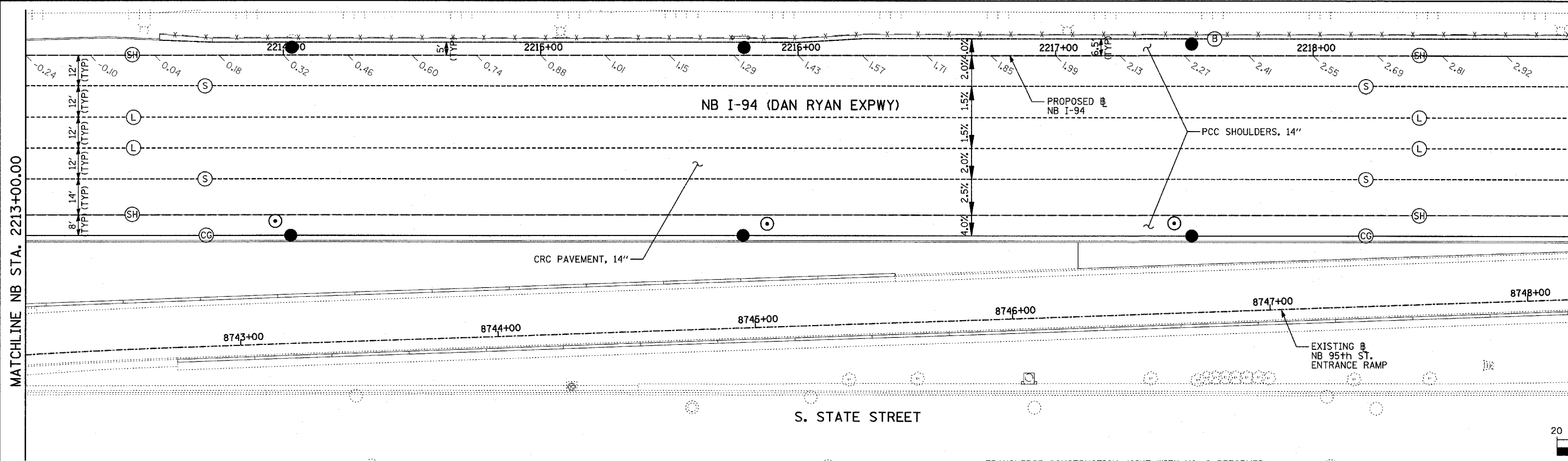
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-57 AND NB I-94
NB I-57 STA. 253+00.00 TO
NB I-94 STA. 2207+00.00
(SHEET 11 OF 28)
 SCALE: 1"=20'
 DATE: MARCH 7, 2006
 DRAWN BY: JJS
 CHECKED BY: MPG

TYLIN INTERNATIONAL



- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 - ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
 - (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
 - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
 - (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
 - (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
 - (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

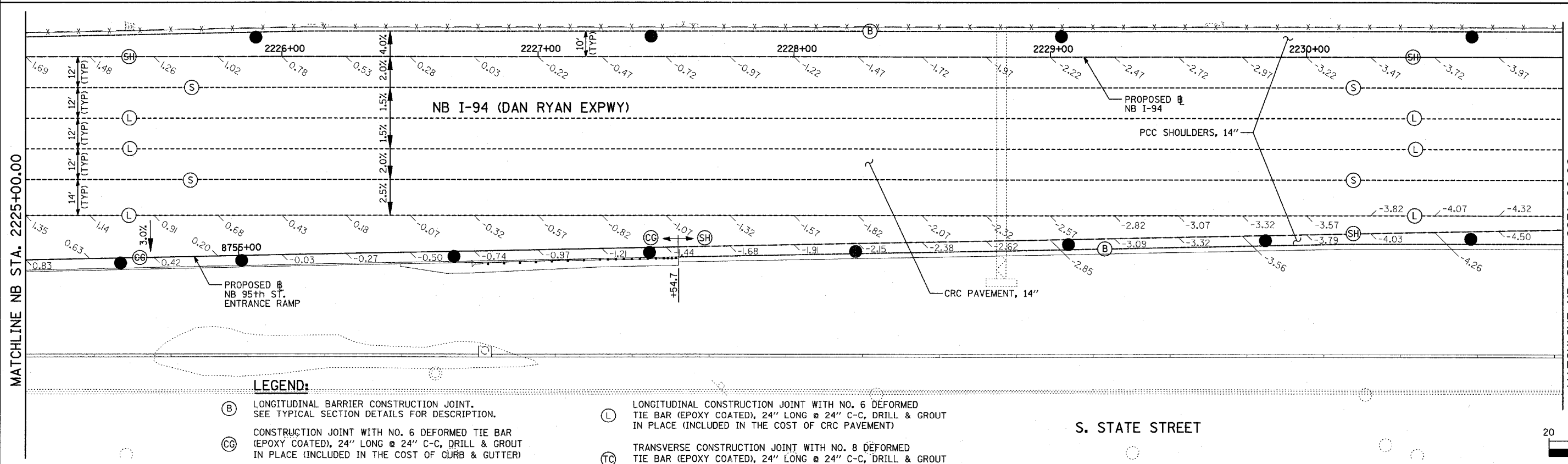
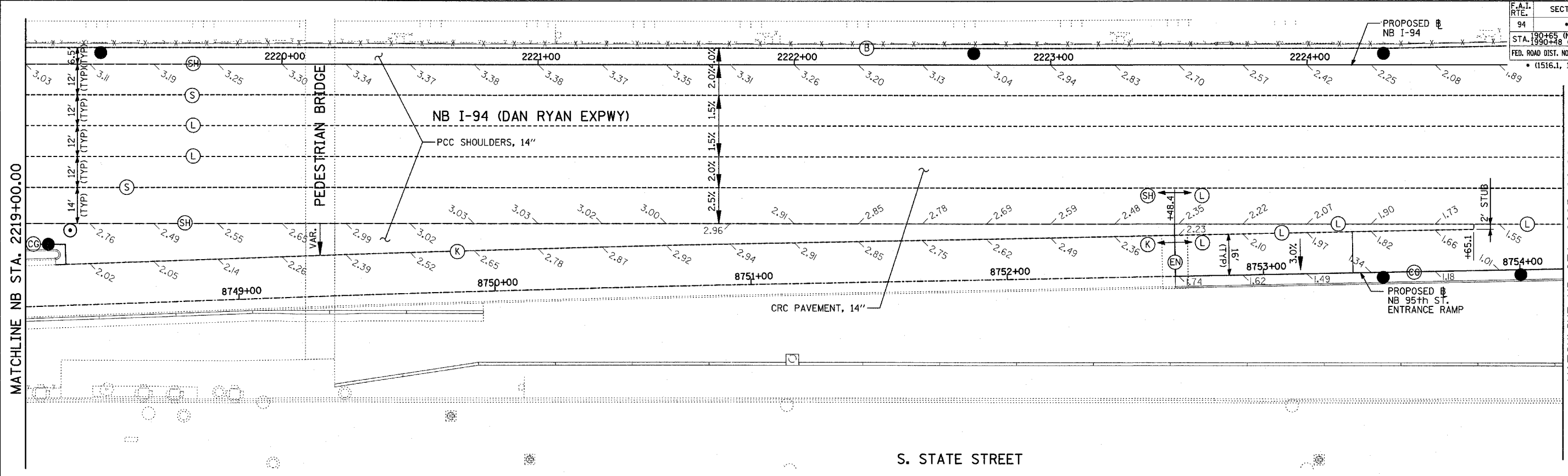
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-94 (DAN RYAN EXPRESSWAY)
STA. 2207+00.00 TO 2219+00.00
(SHEET 12 OF 28)

SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: JJS
CHECKED BY: MPG

TYLIN INTERNATIONAL



MATCHLINE NB STA. 2225+00.00

MATCHLINE NB STA. 2231+00.00

MATCHLINE NB STA. 2219+00.00

MATCHLINE NB STA. 2225+00.00

S. STATE STREET

S. STATE STREET

LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

NOTES:

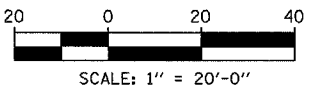
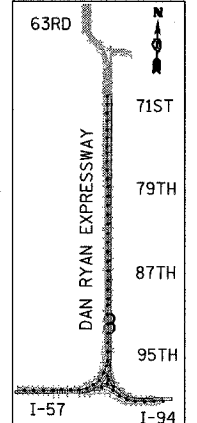
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

REVISIONS	
NAME	DATE

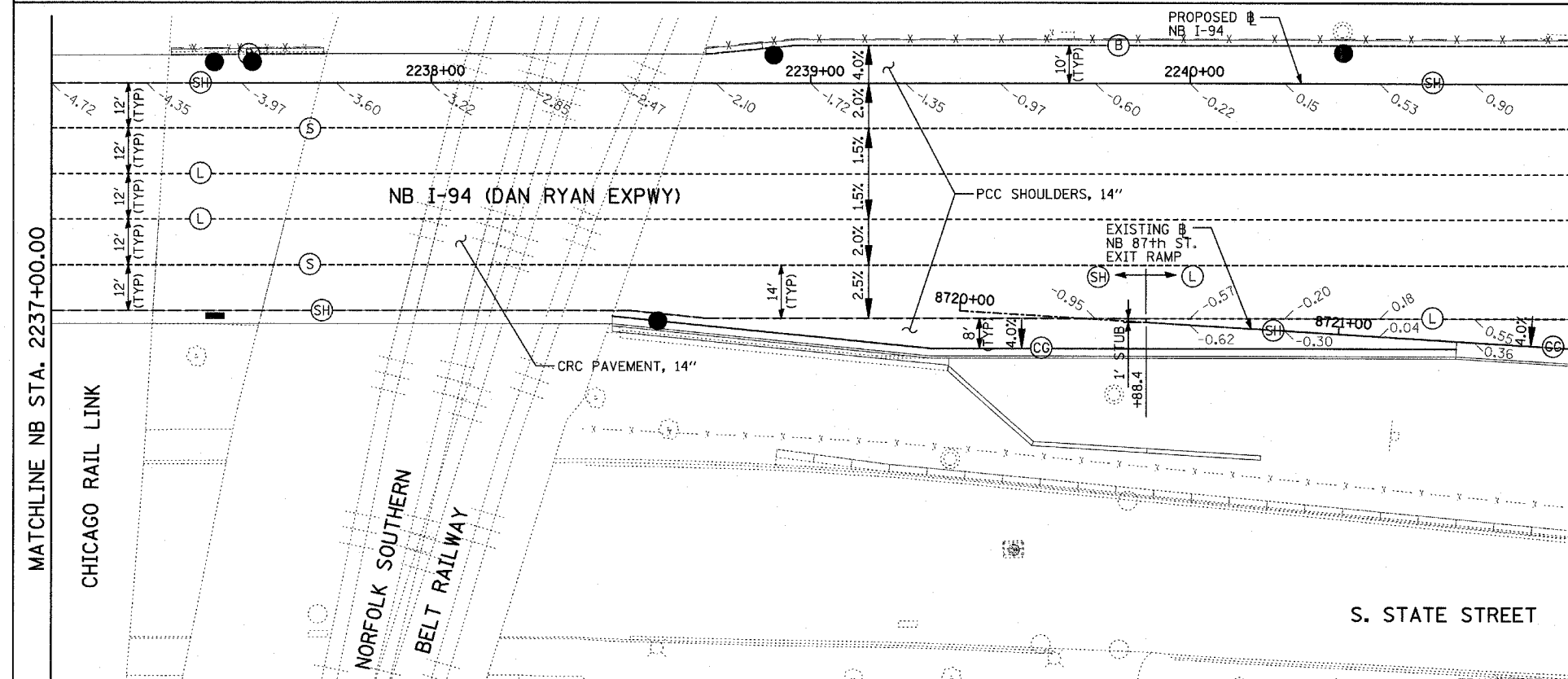
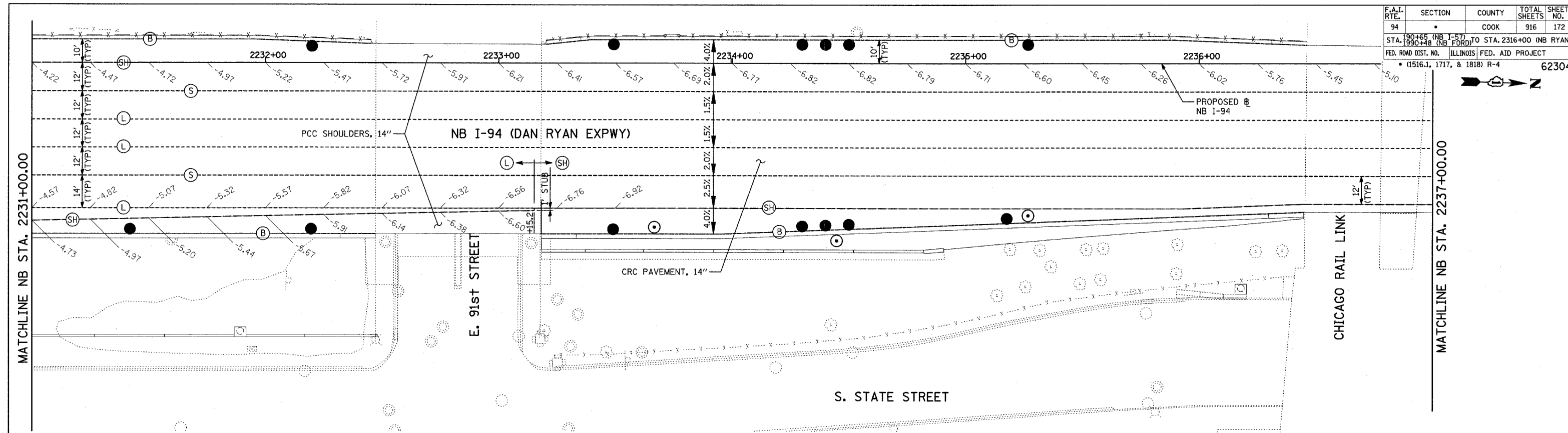
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-94 (DAN RYAN EXPRESSWAY)
STA. 2219+00.00 TO 2231+00.00
(SHEET 13 OF 28)

SCALE: 1"=20'
DATE: MARCH 7, 2006

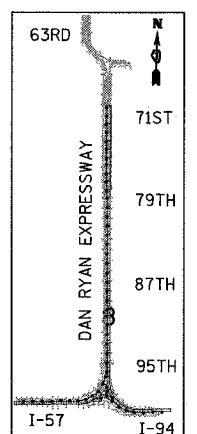
DRAWN BY: JUS
CHECKED BY: MPG



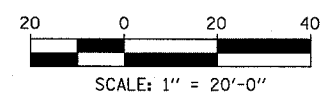
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	172
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 62304				



- NOTES:**
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



LOCATION MAP



- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
 - (CC) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
 - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)

- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)

- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

REVISIONS	
NAME	DATE

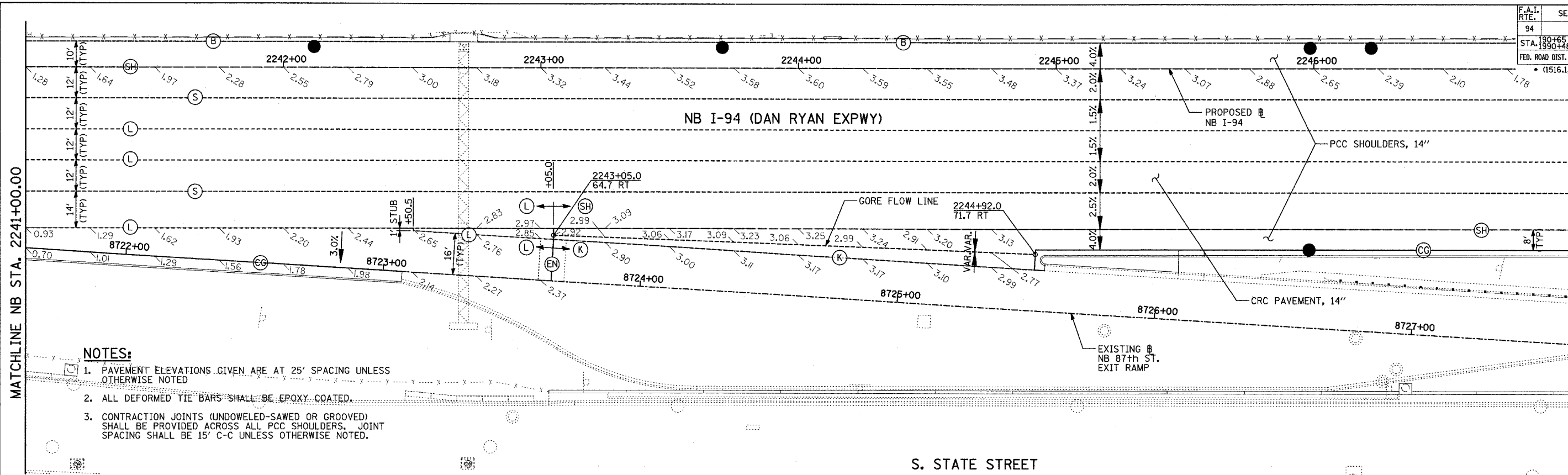
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 NB I-94 (DAN RYAN EXPRESSWAY)
 STA. 2231+00.00 TO 2241+00.00
 (SHEET 14 OF 28)

SCALE: 1"=20'
 DATE: MARCH 7, 2006

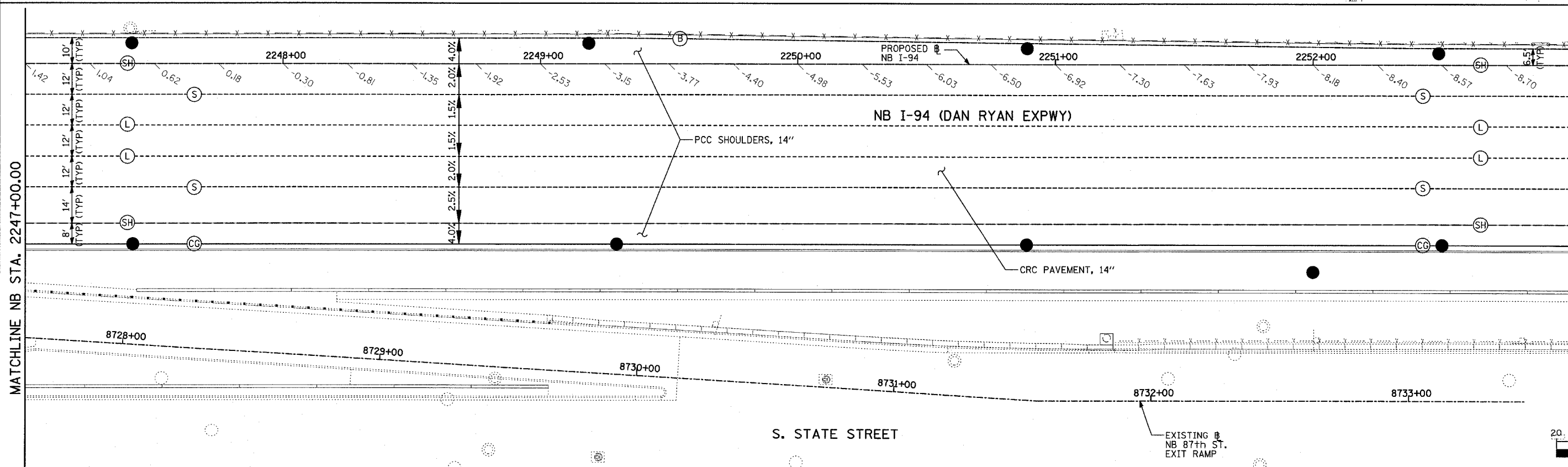
DRAWN BY: JJS
 CHECKED BY: MPG

TYLIN INTERNATIONAL

F.A.I. RTE. 94	SECTION	COUNTY COOK	TOTAL SHEETS 916	SHEET NO. 173
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516J, 1717, & 1818) R-4 62304				



- NOTES:**
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
 - (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
 - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)

- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)

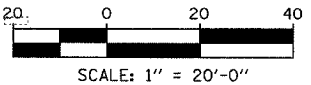
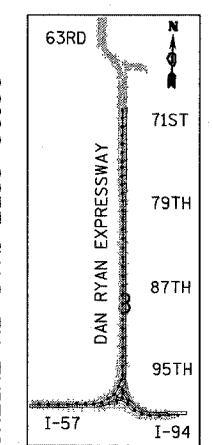
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

REVISIONS	
NAME	DATE

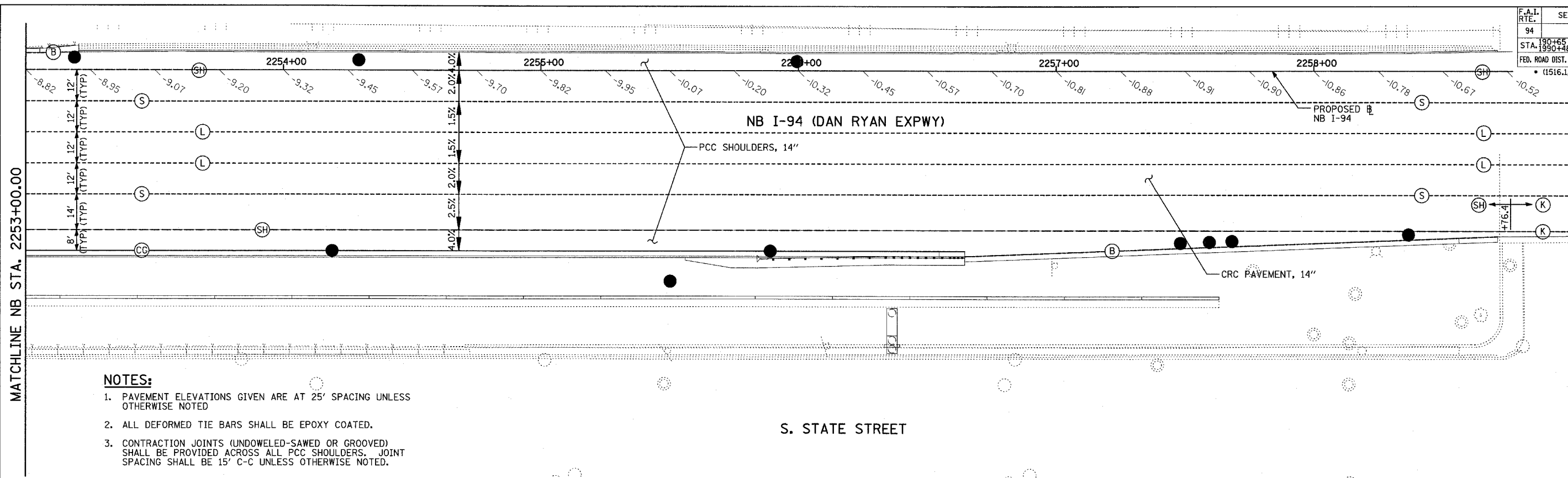
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-94 (DAN RYAN EXPRESSWAY)
STA. 2241+00.00 TO 2253+00.00
(SHEET 15 OF 28)

SCALE: 1"=20'
DATE: MARCH 7, 2006
DRAWN BY: JJS
CHECKED BY: MPG

TYLIN INTERNATIONAL



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	174
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516, 1, 1717, & 1818) R-4 62304				

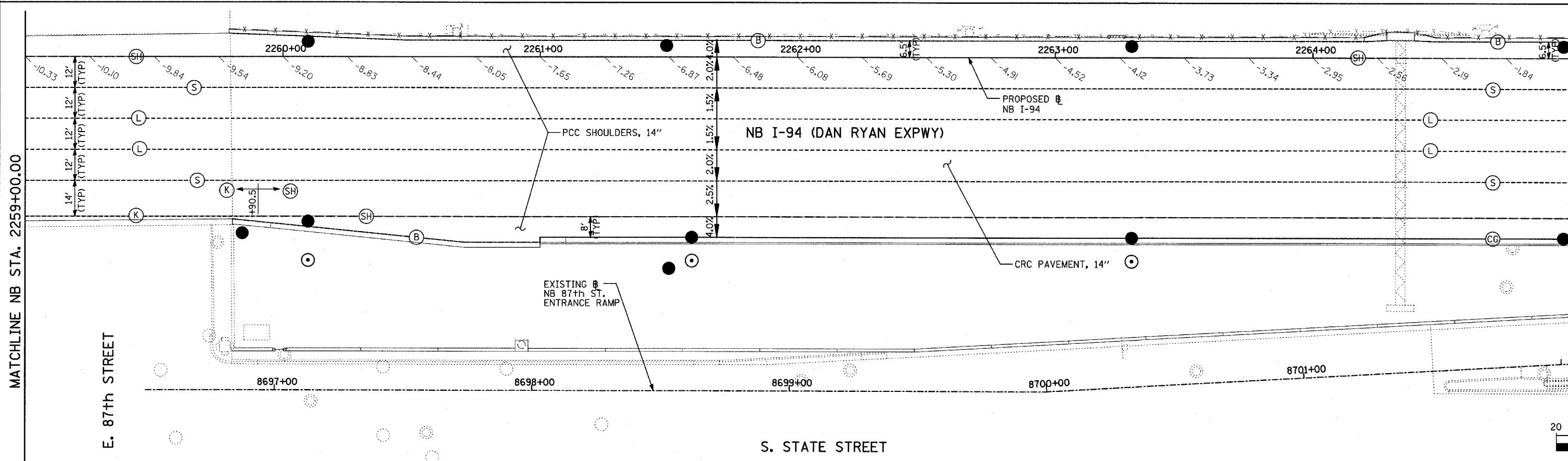


MATCHLINE NB STA. 2259+00.00

MATCHLINE NB STA. 2253+00.00

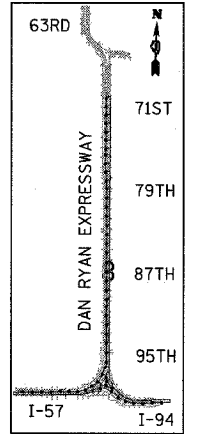
NOTES:

1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

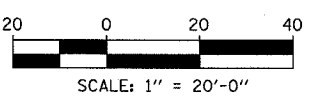


MATCHLINE NB STA. 2265+00.00

MATCHLINE NB STA. 2259+00.00



LOCATION MAP



LEGEND:

- | | | |
|--|---|--|
| <p>(B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.</p> <p>(CC) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)</p> <p>(S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)</p> | <p>(T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)</p> <p>(SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)</p> <p>(L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)</p> | <p>(TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)</p> <p>(E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)</p> <p>(K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).</p> <p>(EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)</p> |
|--|---|--|

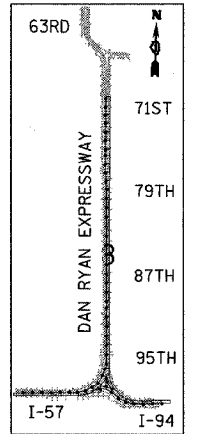
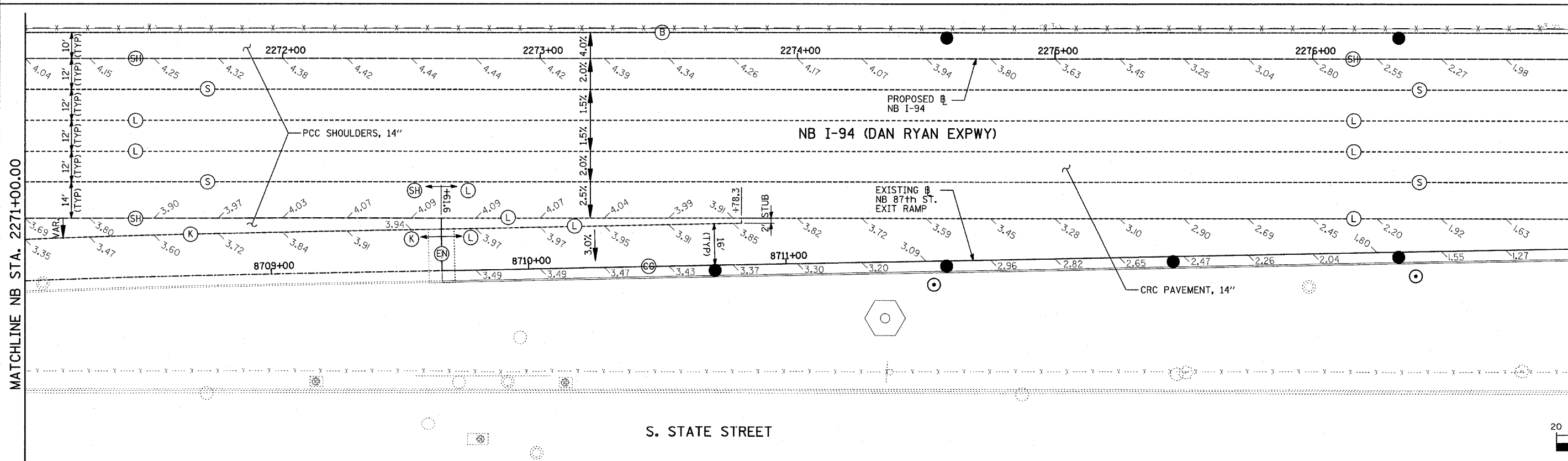
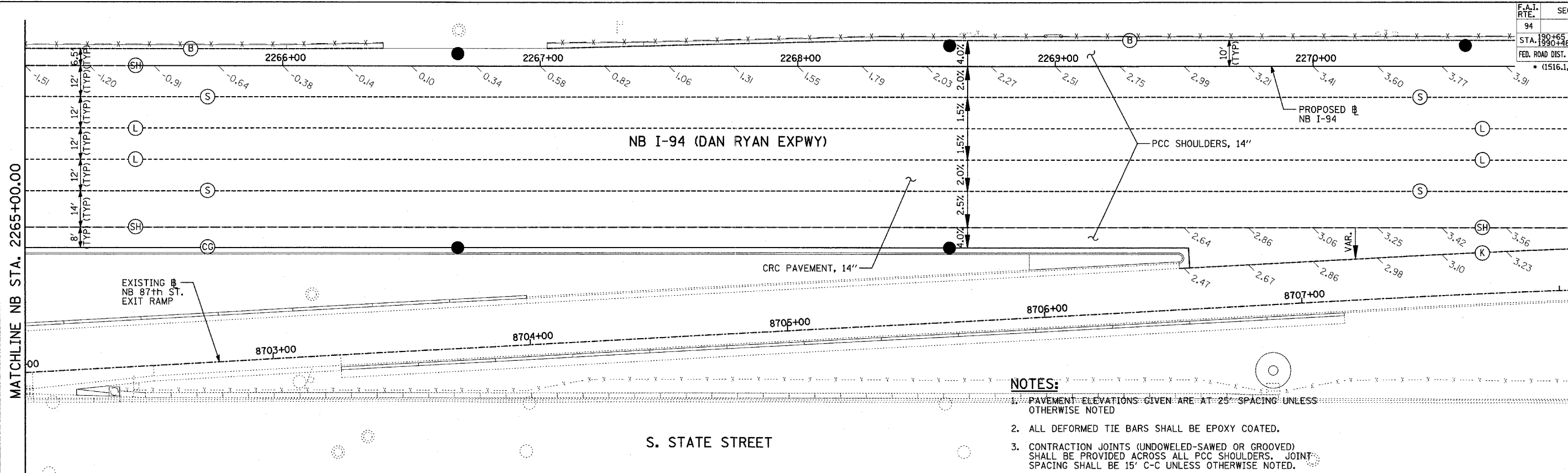
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-94 (DAN RYAN EXPRESSWAY)
STA. 2253+00.00 TO 2265+00.00
(SHEET 16 OF 28)

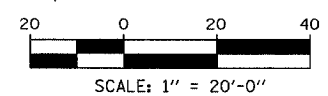
SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: JJS
CHECKED BY: MPG

TYLIN INTERNATIONAL



LOCATION MAP



- NOTES:**
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)

- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)

- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

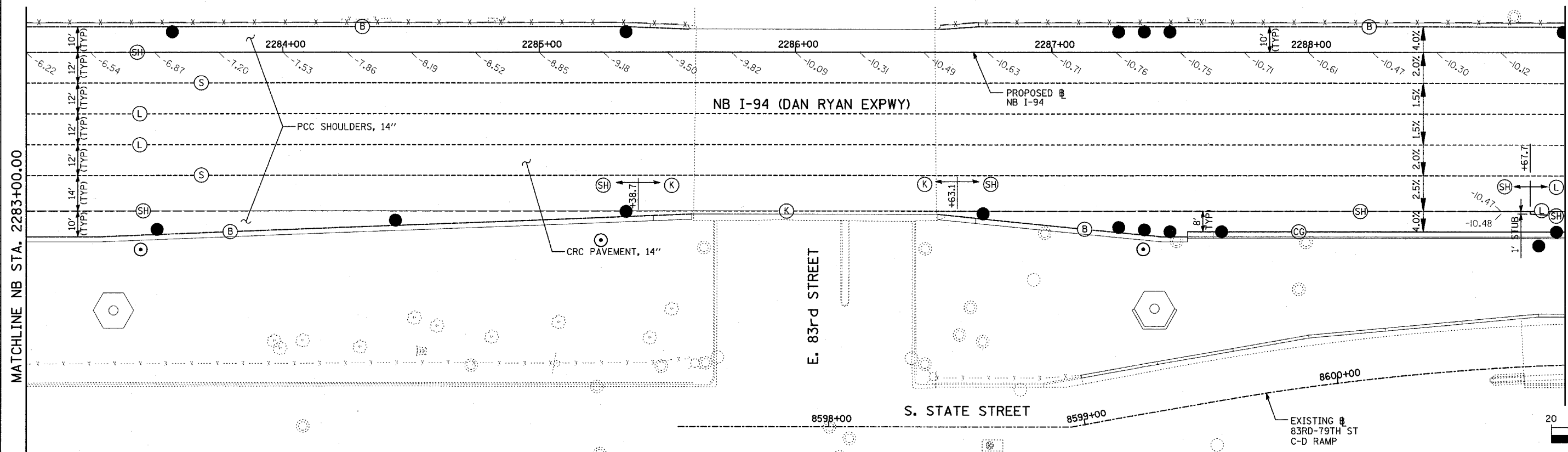
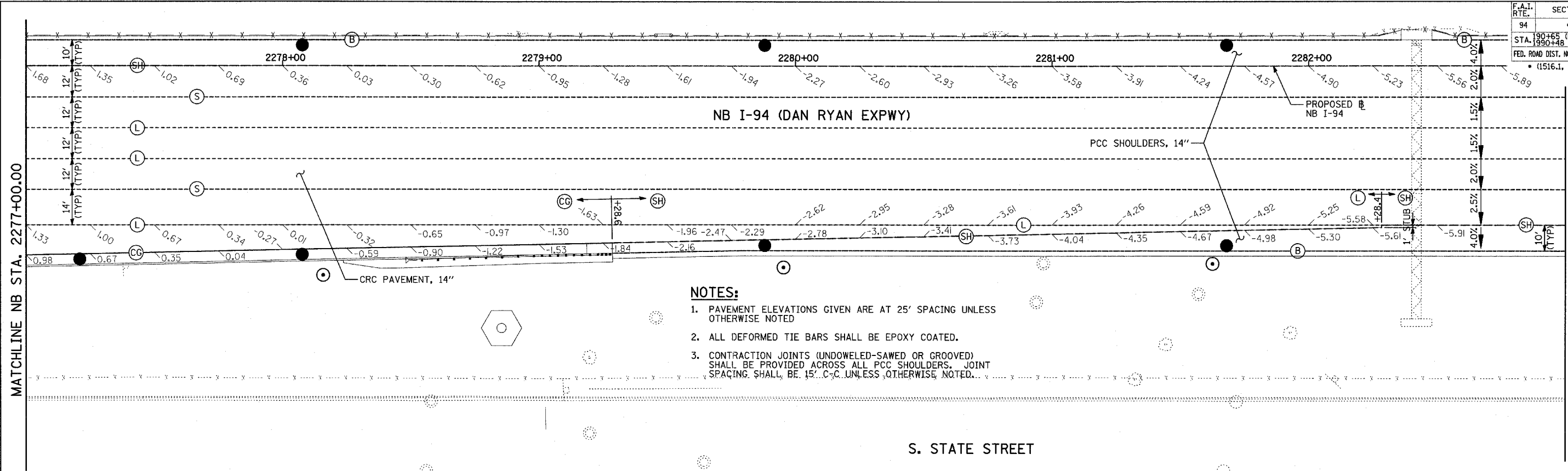
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-94 (DAN RYAN EXPRESSWAY)
STA. 2265+00.00 TO 2277+00.00
(SHEET 17 OF 28)

SCALE: 1"=20'
 DATE: MARCH 7, 2006

DRAWN BY: JJS
 CHECKED BY: MPG

TYLIN INTERNATIONAL



LEGEND:

- | | | | | | |
|------|---|------|--|------|---|
| (B) | LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION. | (T) | TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT) | (TC) | TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) |
| (CG) | CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER) | (SH) | LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT) | (E) | TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) |
| (S) | SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT) | (L) | LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) | (K) | LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS). |
| | | | | (EN) | TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT) |

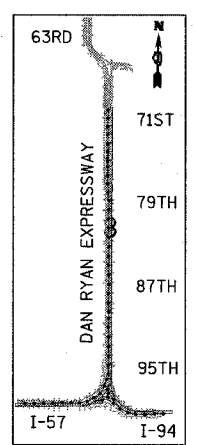
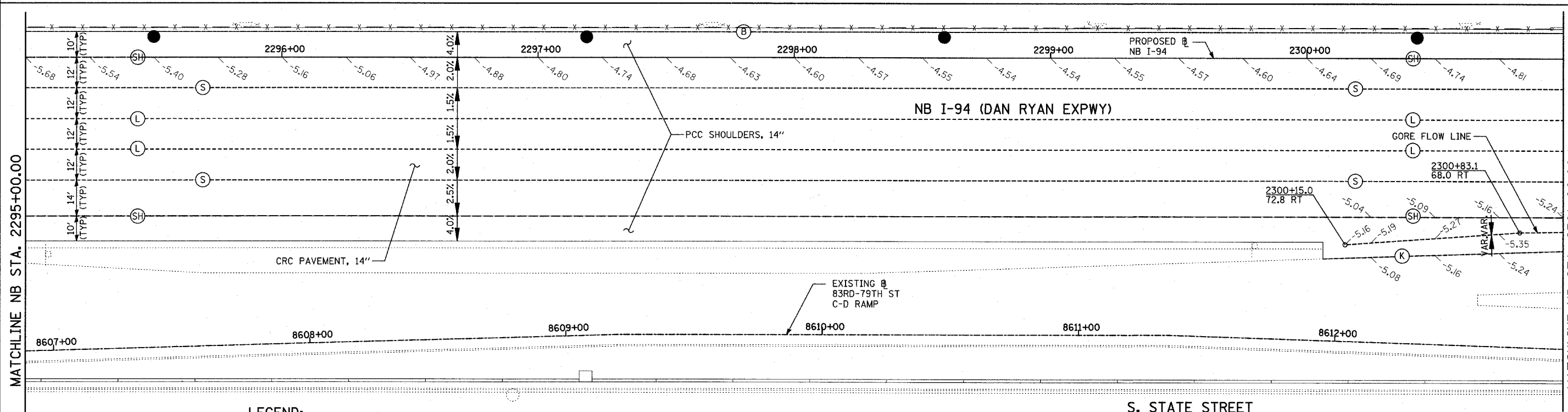
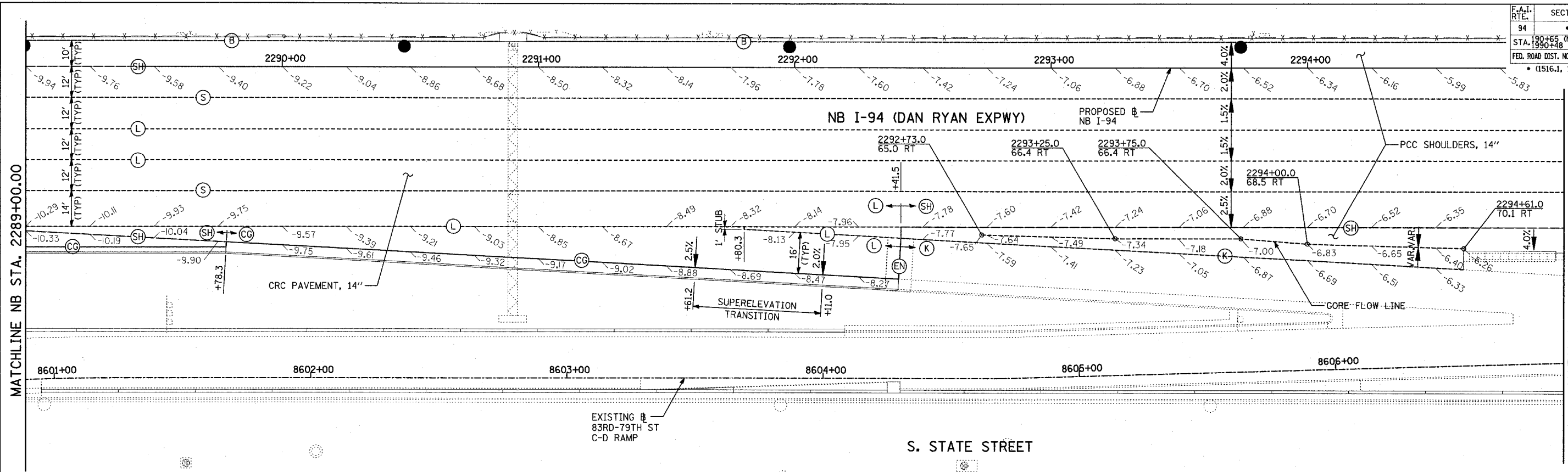
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-94 (DAN RYAN EXPRESSWAY)
STA. 2277+00.00 TO 2289+00.00
(SHEET 18 OF 28)

SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: JJS
CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	177
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-4 62304				



LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CC) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)

- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)

- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

NOTES:

1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
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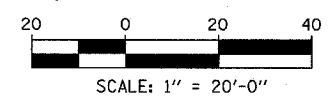
REVISIONS	
NAME	DATE

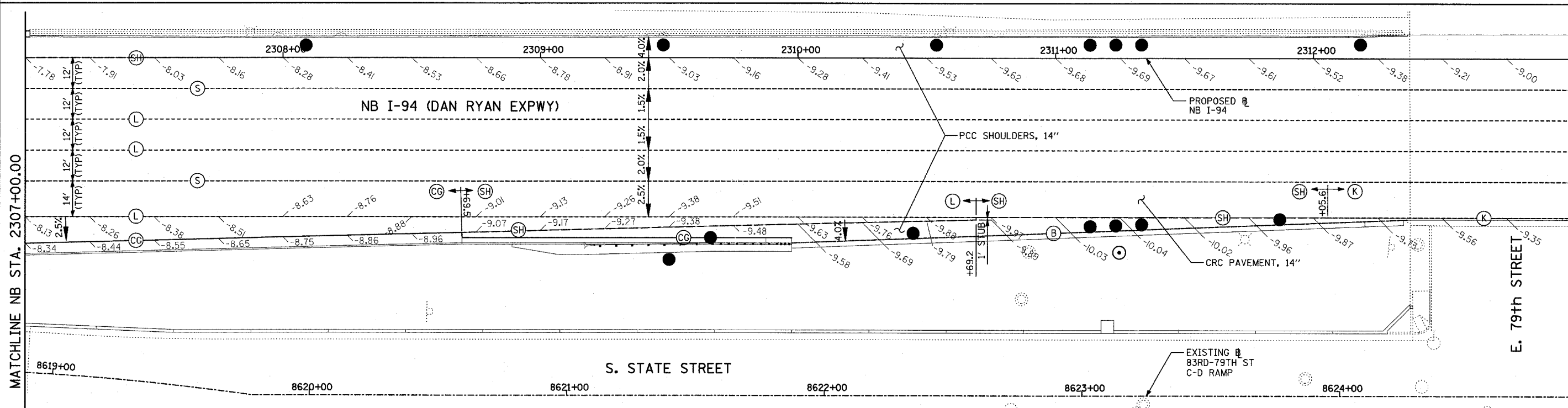
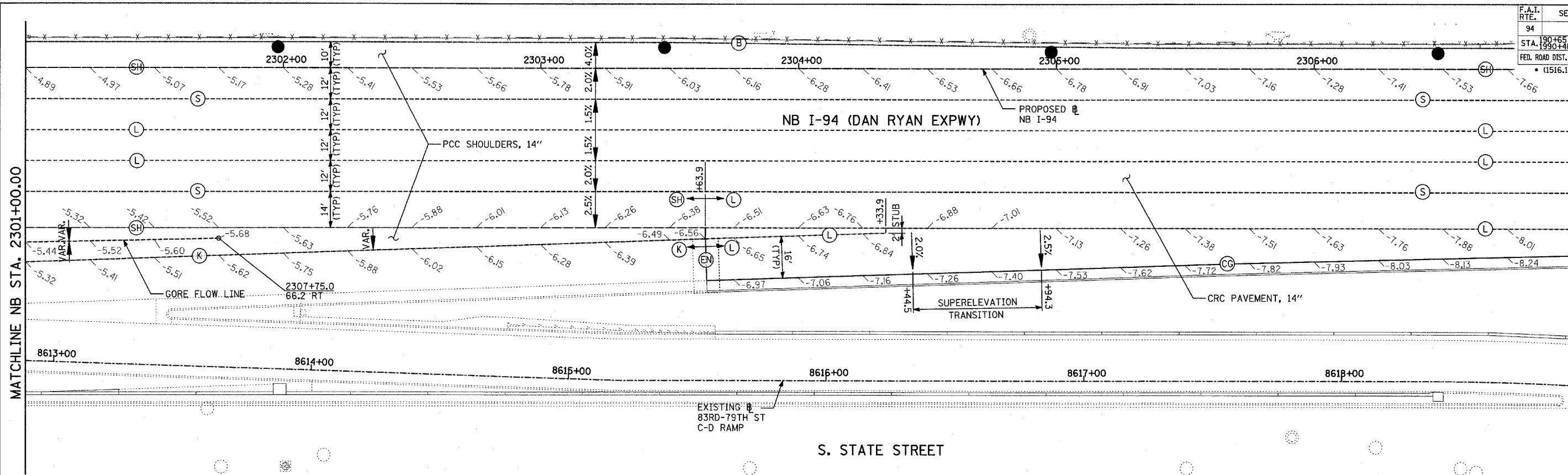
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-94 (DAN RYAN EXPRESSWAY)
STA. 2289+00.00 TO 2301+00.00
(SHEET 19 OF 28)

SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: JJS
CHECKED BY: MPG

TYLIN INTERNATIONAL



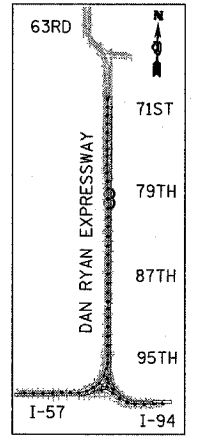


MATCHLINE NB STA. 2307+00.00

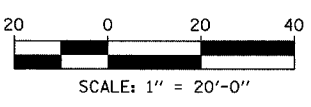
MATCHLINE NB STA. 2313+00.00

MATCHLINE NB STA. 2301+00.00

MATCHLINE NB STA. 2307+00.00



LOCATION MAP



LEGEND:

- | | | |
|---|--|---|
| <ul style="list-style-type: none"> (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION. (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER) (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT) (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT) | <ul style="list-style-type: none"> (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT) (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) | <ul style="list-style-type: none"> (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS). (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT) |
|---|--|---|

NOTES:

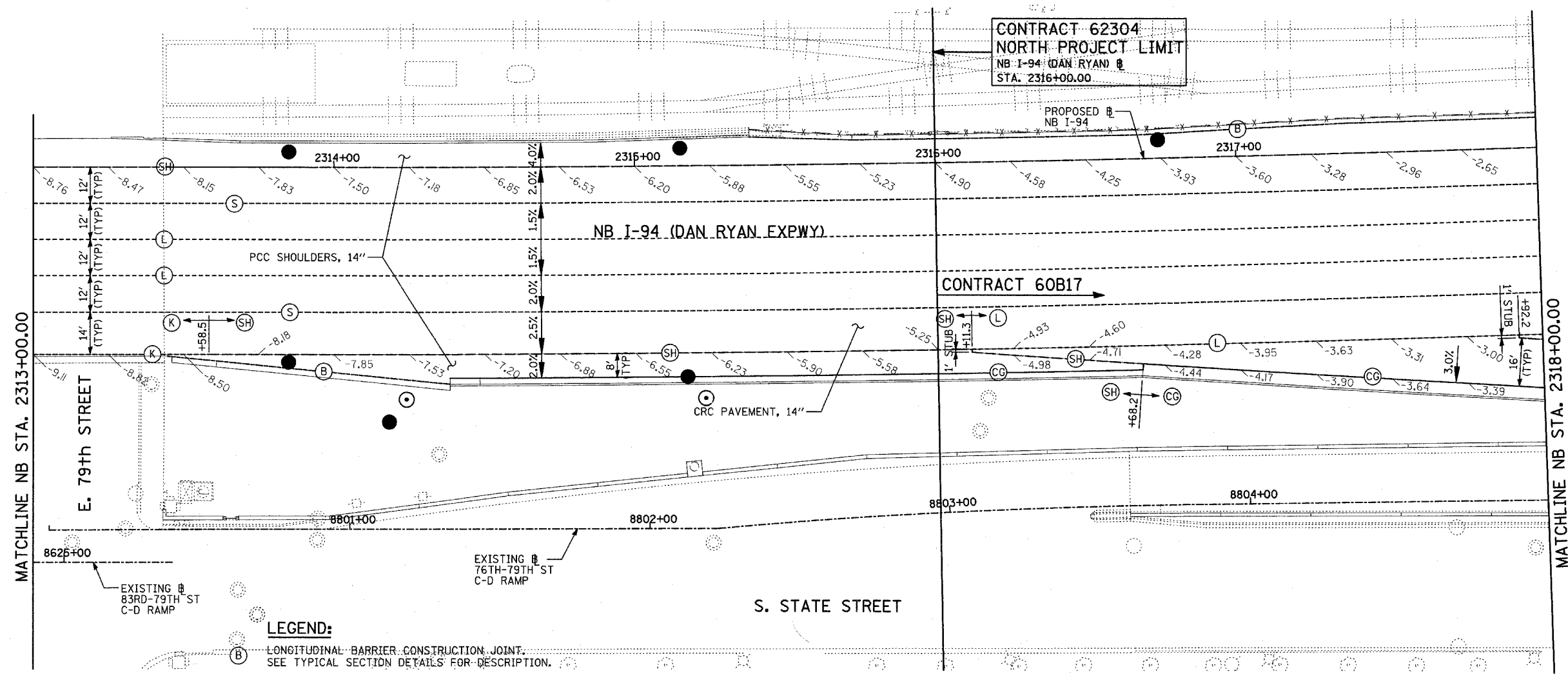
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
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3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 NB I-94 (DAN RYAN EXPRESSWAY)
 STA. 2301+00.00 TO 2313+00.00
 (SHEET 20 OF 28)

SCALE: 1"=20'
 DATE: MARCH 7, 2006

DRAWN BY: JJS
 CHECKED BY: MPG



CONTRACT 62304
NORTH PROJECT LIMIT
 NB I-94 (DAN RYAN) @
 STA. 2316+00.00

CONTRACT 60B17

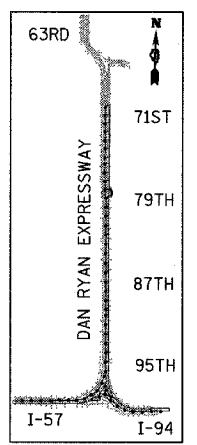
MATCHLINE NB STA. 2313+00.00

MATCHLINE NB STA. 2318+00.00

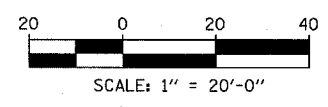
LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
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- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

S. STATE STREET



LOCATION MAP



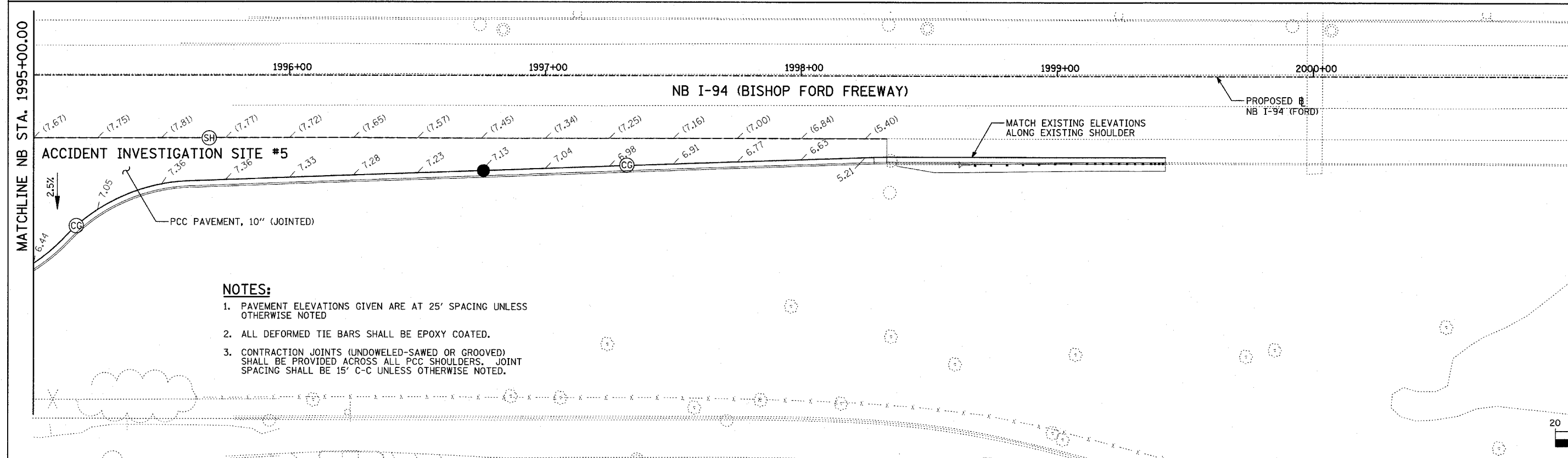
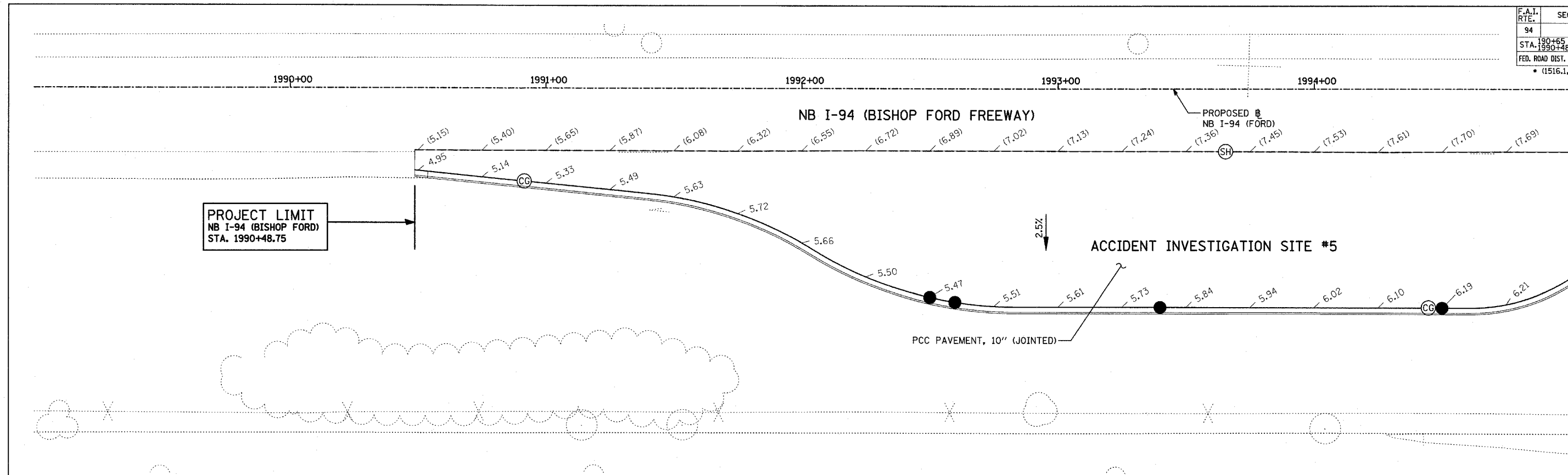
NOTES:

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 NB I-94 (DAN RYAN EXPRESSWAY)
 STA. 2313+00.00 TO 2318+00.00
 (SHEET 21 OF 28)

SCALE: 1"=20'
 DATE: MARCH 7, 2006
 DRAWN BY: JJS
 CHECKED BY: MPG



- NOTES:**
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
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- LEGEND:**
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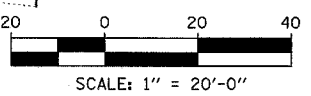
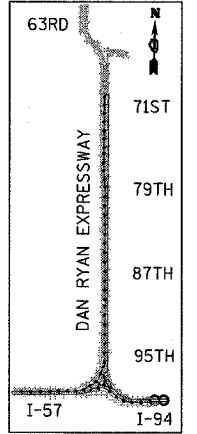
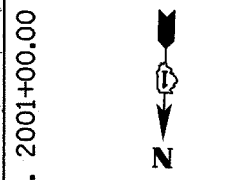
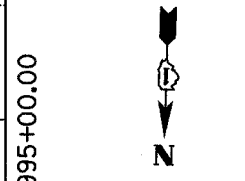
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
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- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

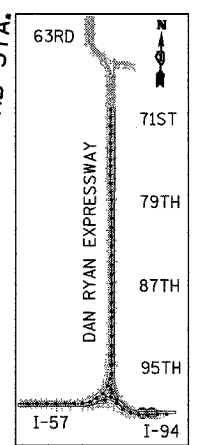
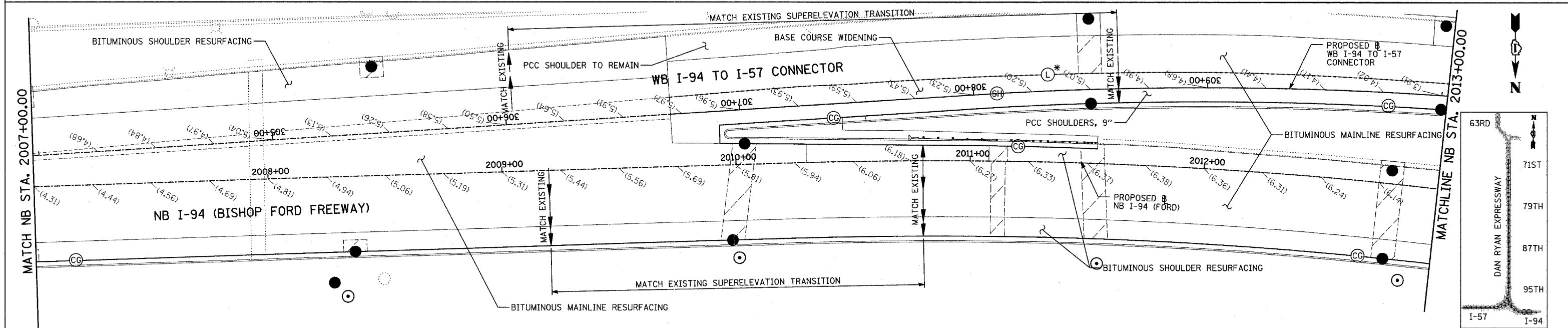
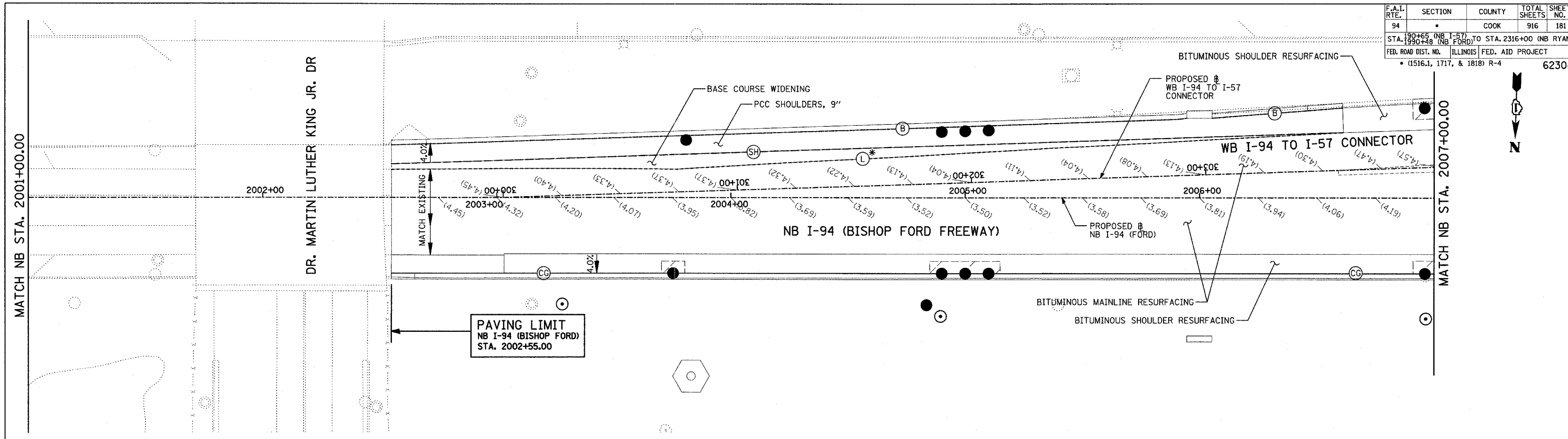
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-94 (BISHOP FORD FWY)
STA. 1990+48.75 TO 2001+00.00
(SHEET 22 OF 28)

SCALE: 1"=20'
DATE: MARCH 7, 2006
DRAWN BY: JJS
CHECKED BY: MPG

TYLIN INTERNATIONAL





LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

* JOINT TO BE INCLUDED IN COST OF P.C.C. BASE COURSE, 13". TIE BARS TO BE DRILLED AND GROUTED AT MID DEPTH OF EXISTING P.C.C. PAVEMENT.

NOTES:

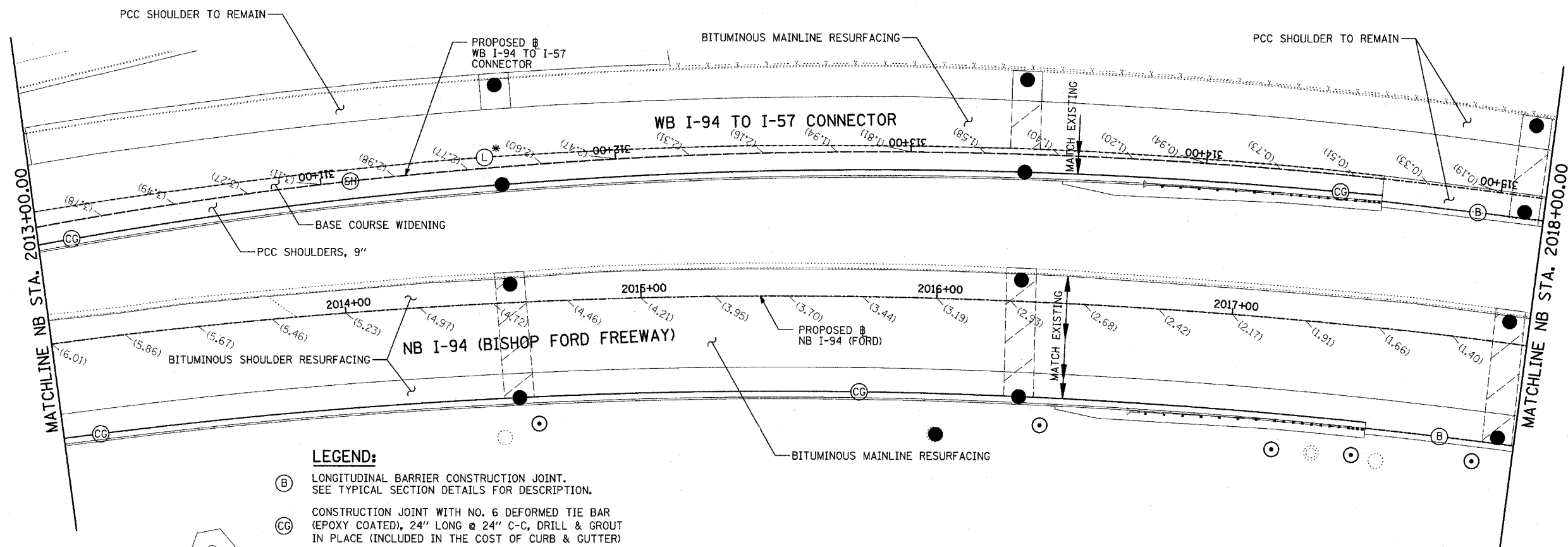
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-94 (BISHOP FORD FWY)
STA. 2001+00.00 TO 2013+00.00
(SHEET 23 OF 28)

SCALE: 1"=20'
 DATE: MARCH 7, 2006

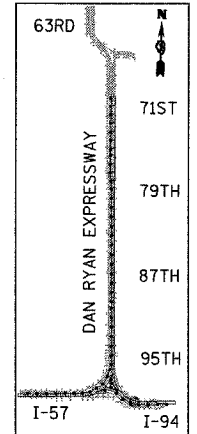
DRAWN BY: JJS
 CHECKED BY: MPG



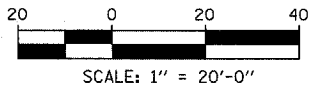
- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
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* JOINT TO BE INCLUDED IN COST OF P.C.C. BASE COURSE, 13". TIE BARS TO BE DRILLED AND GROUTED AT MID DEPTH OF EXISTING P.C.C. PAVEMENT.

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 - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



LOCATION MAP



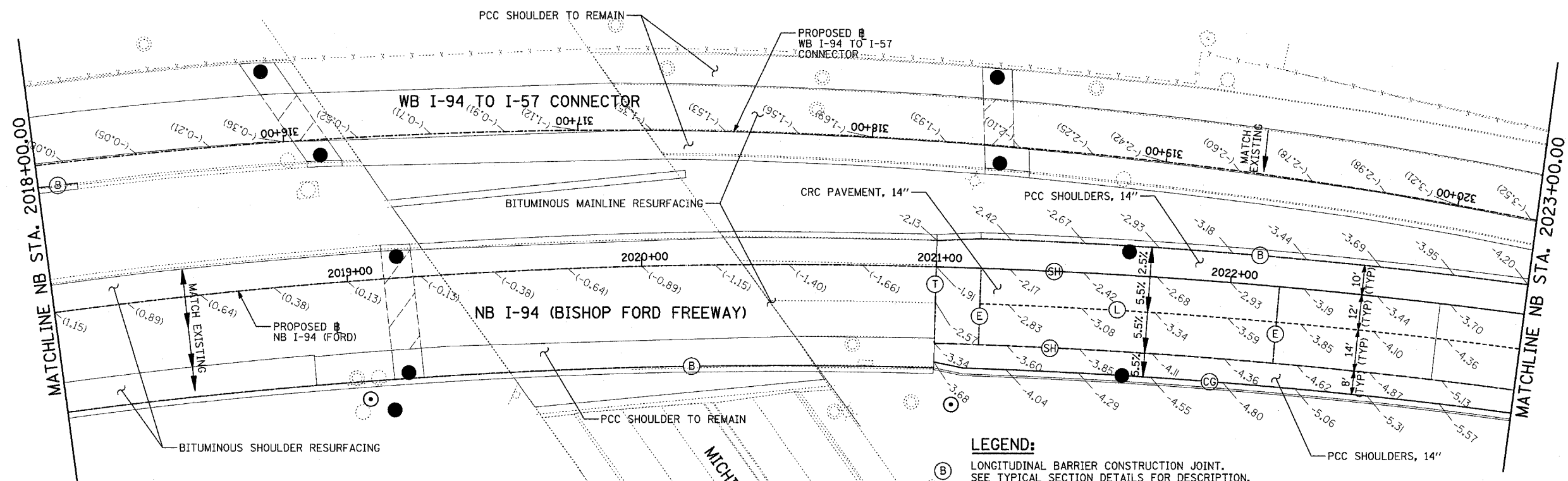
TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 NB I-94 (BISHOP FORD FWY)
 STA. 2013+00.00 TO 2018+00.00
 (SHEET 24 OF 28)

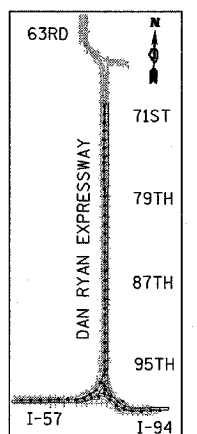
SCALE: 1"=20'
 DATE: MARCH 7, 2006

DRAWN BY: JUS
 CHECKED BY: MPG

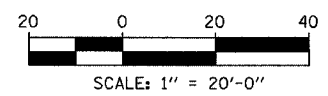


- LEGEND:**
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LOCATION MAP



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-94 (BISHOP FORD FREEWAY)
STA. 2018+00.00 TO 2023+00.00
(SHEET 25 OF 28)

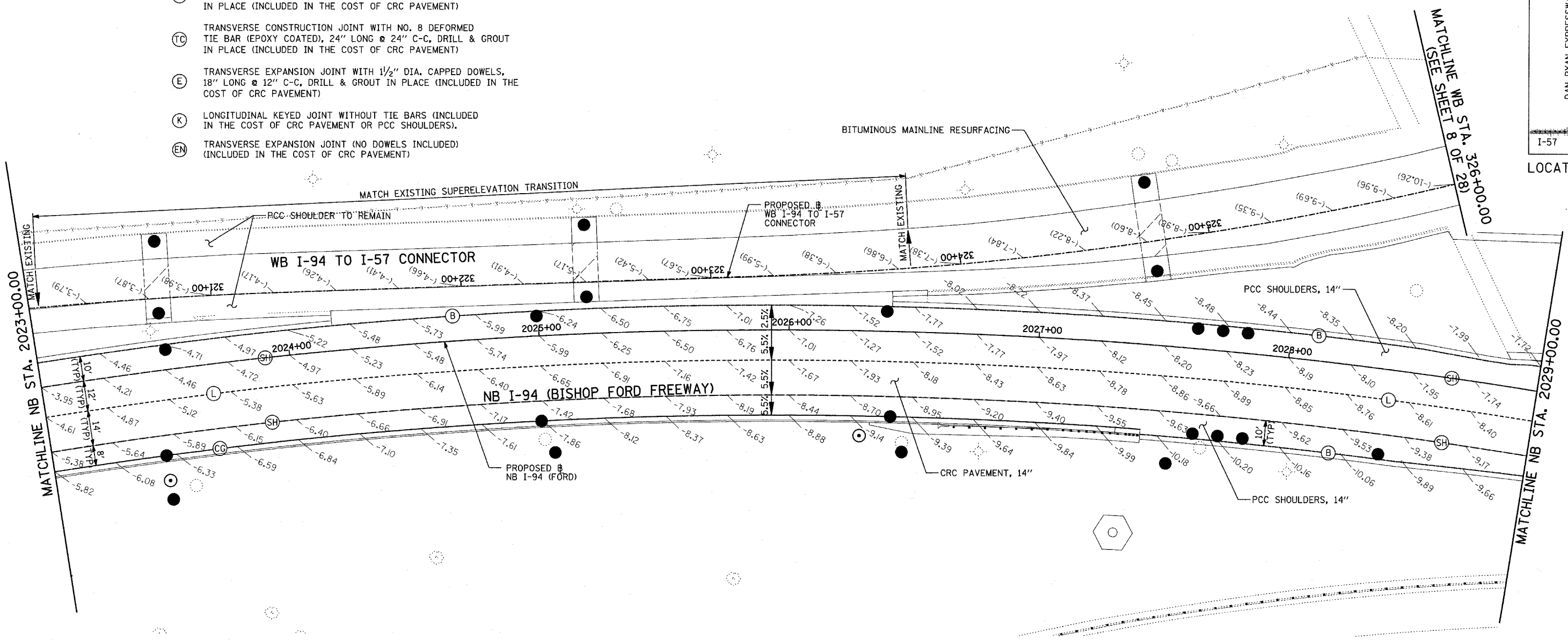
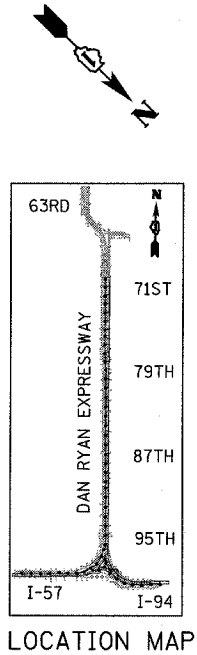
SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: JJS
CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	184
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-4 62304				

LEGEND:

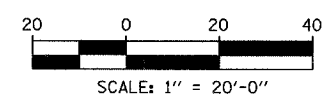
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NOTES:

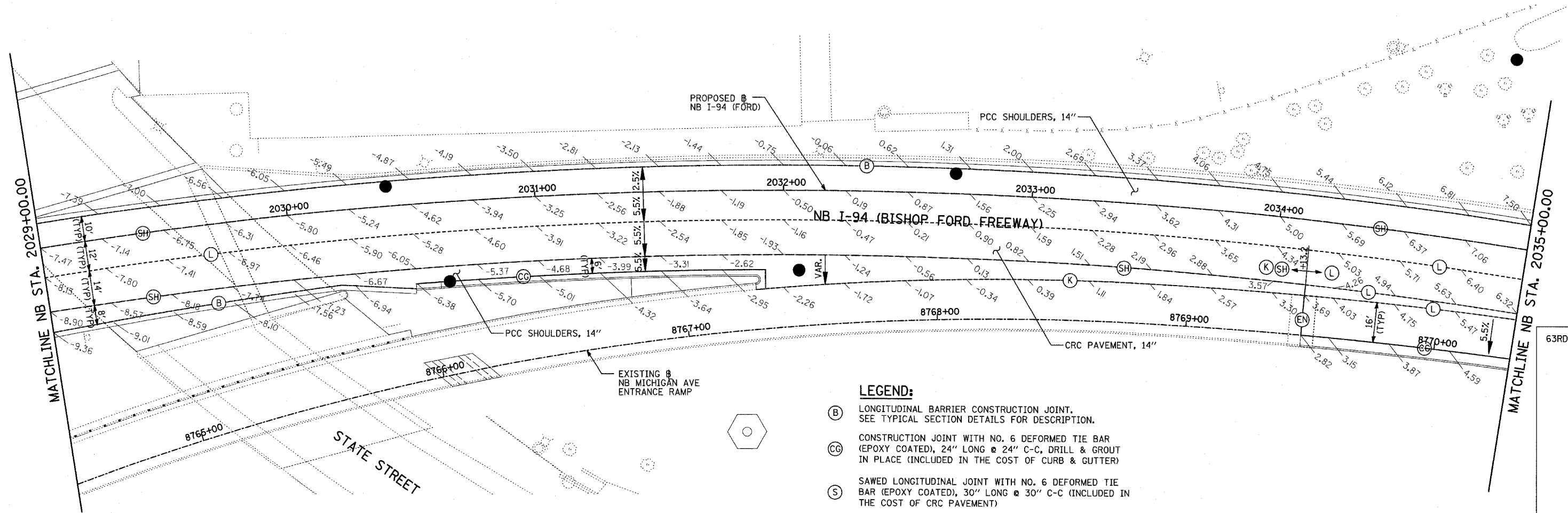
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TYLIN INTERNATIONAL



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-94 (BISHOP FORD FREEWAY)
STA. 2023+00.00 TO 2029+00.00
(SHEET 26 OF 28)
SCALE: 1"=20'
DATE: MARCH 7, 2006
DRAWN BY: JJS
CHECKED BY: MPG

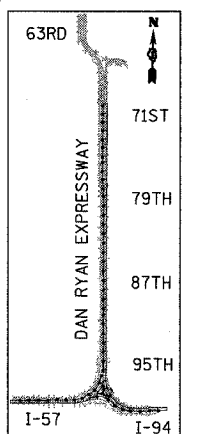


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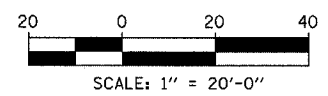
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LOCATION MAP



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-94 (BISHOP FORD FWY)
STA. 2029+00.00 TO 2035+00.00
(SHEET 27 OF 28)

SCALE: 1"=20'
DATE: MARCH 7, 2006

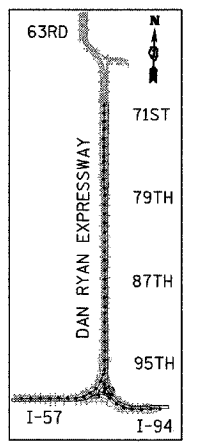
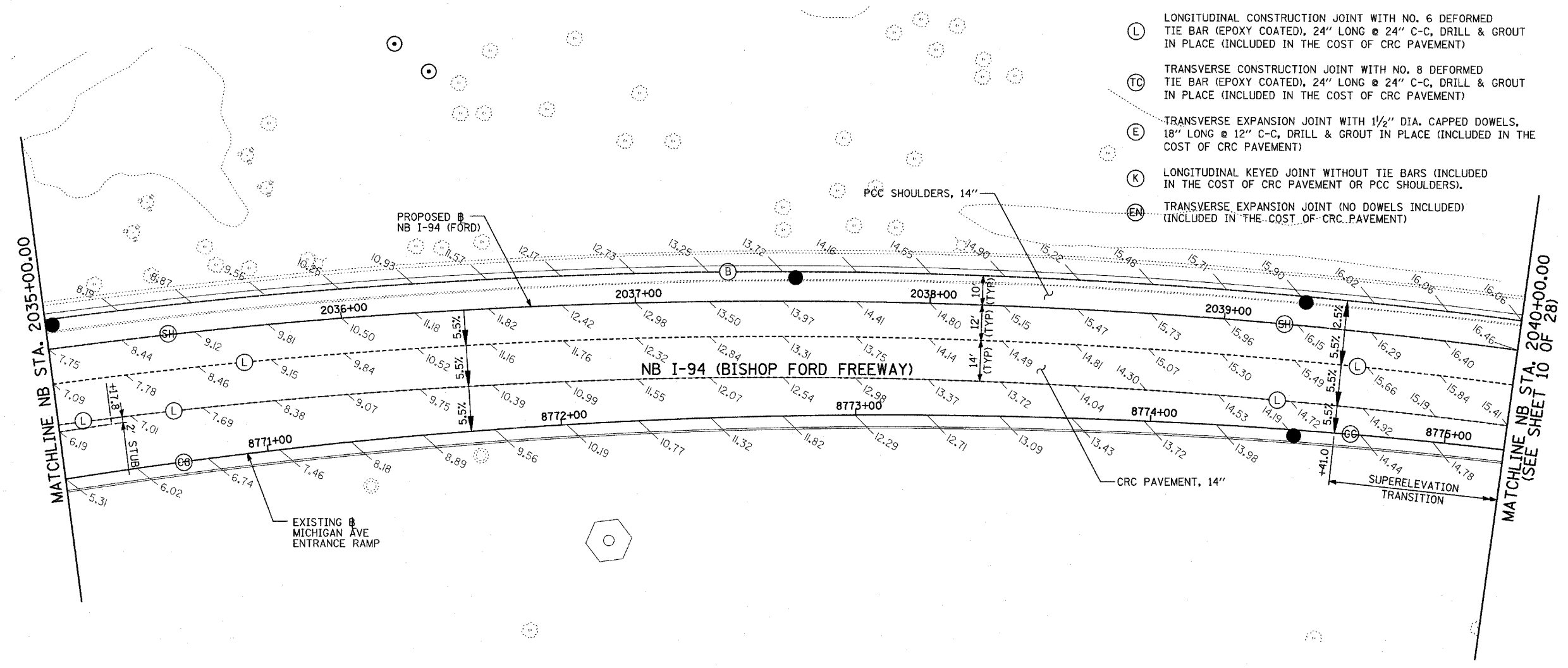
DRAWN BY: JJS
CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	916	186
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4 62304				

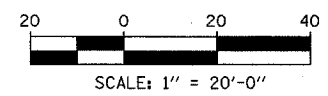


LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
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LOCATION MAP



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TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
NB I-94 (BISHOP FORD FREEWAY)
STA. 2035+00.00 TO 2040+00.00
(SHEET 28 OF 28)

SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: JJS
CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	•	COOK	916	187
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
STA. 1990+48 (NB FORD) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				
				62304

GENERAL NOTES:

SEE SPECIAL PROVISION TITLED TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)

A WORK ZONE SPEED LIMIT OF 45 M.P.H. MUST BE MAINTAINED AT ALL TIMES.

QUANTITY FOR TEMPORARY PAVEMENT MARKING - LINE 6" WAS ASSUMED EQUAL TO THE LENGTH OF TEMPORARY CONCRETE BARRIER. UTILIZED WHEN THE BARRIER IS LOCATED ONE FOOT OR LESS FROM THE EDGE OF TRAVELED WAY.

ALL TEMPORARY PAVEMENT MARKINGS PLACED DURING THE WINTER SHUTDOWN STAGE OR ON EXISTING PAVEMENT SHALL BE PAID FOR AS EPOXY PAVEMENT MARKING OF THE LINE TYPE AND WIDTH SPECIFIED.

ALL TEMPORARY PAVEMENT MARKINGS PLACED ON NEW PAVEMENT SHALL BE PAID FOR AS PAVEMENT MARKING TAPE, TYPE III OF THE LINE TYPE AND WIDTH SPECIFIED EXCEPT WHEN DIRECTED OTHERWISE.

A TOTAL OF 10 CHANGEABLE MESSAGE SIGNS SHALL BE LOCATED ALONG THE MAINLINE AND NEAR RAMP CLOSURES FOR THE DURATION OF THIS CONTRACT. EXACT PLACEMENT OF THE SIGNS SHALL BE DETERMINED BY THE RESIDENT ENGINEER.

ANY RAISED REFLECTIVE PAVEMENT MARKERS THAT CONFLICT WITH THE TEMPORARY TRAFFIC LANES MUST HAVE THE REFLECTIVE LENSES REMOVED AS DIRECTED BY THE RESIDENT ENGINEER. MEASURE OF PAYMENT FOR REMOVAL OF REFLECTORS IS INCLUDED IN THE CONTRACT UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).

FOR ACCESS CONTROL, THE CONTRACTOR IS REQUIRED TO PLACE CHAIN LINK FENCE 6' (SPECIAL), 7'-2" CLEAR FROM THE CENTERLINE OF CTA TRACK, PRIOR TO REMOVING EXISTING CTA CHAIN LINK FENCE AND BARRIER WALL. THE CONTRACTOR MUST ALSO PLACE CHAIN LINK GATES, 6' X 6' SINGLE AT ALL LOCATIONS WHERE THERE ARE EXISTING CTA FENCE GATES. THE CHAIN LINK FENCE 6' (SPECIAL) MUST REMAIN IN PLACE UNTIL THE NEW PERMANENT CTA FENCE IS COMPLETED.

REFER TO MISCELLANEOUS DETAIL SHEETS FOR TEMPORARY PAVEMENT AND MILLING OPERATIONS AND FOR OVERDIG AREAS DETAILS.

THE RESIDENT ENGINEER SHALL ASSESS THE EXISTING CONDITION OF THE PAVEMENT LOCATED ALONG THE INSIDE SHOULDER. DAMAGED AREAS WILL REQUIRE SHOULDER REHABILITATION INCLUDING MILLING AND RESURFACING. ADDITIONAL QUANTITIES OF 147 SO YD FOR BITUMINOUS SURFACE REMOVAL, 1/2" AND 13 TONS FOR BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70 HAVE BEEN INCLUDED IN THE PLANS.

CONSTRUCTION STAGING NOTES

PRE STAGE 1, 1A, 1B, & 1C - NIGHT TIME OPERATIONS

INSTALL RAMP CLOSURE ADVANCE INFORMATION SIGNS ON STAGE 1 RAMP CLOSURES A MINIMUM OF 2 WEEKS IN ADVANCE OF THE CLOSURE. SEE STAGE 1 RAMP CLOSURE GUIDELINES IN THE SPECIAL PROVISIONS.

INSTALL RAMP CLOSURE ADVANCE INFORMATION SIGNS ON THE MICHIGAN AVE. ENTRANCE RAMP A MINIMUM OF 2 WEEKS IN ADVANCE OF THE START OF STAGE 1. CLOSE RAMP FOR THE DURATION OF STAGE 1 AND 1A OR AT ANY TIME NB I-94 (BISHOP FORD) IS IN A 1 LANE CONFIGURATION.

UTILIZE STATE STANDARDS 701400 - APPROACH TO LANE CLOSURE, 701401 - LANE CLOSURE AND DISTRICT 1 DETAIL TC-09 - MULTI-LANE WEAVE AT NB I-57 STA. 188+28 AND NB I-94 (BISHOP FORD) STA. 1990+00.

COORDINATE THREE LANES OF TRAFFIC AT NB I-94 (DAN RYAN) STA. 2316+00 WITH CONTRACT #60B17 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE SINGLE LANE OF TRAFFIC AT WB I-94 TO I-57 CONNECTOR STA. 340+29 WITH CONTRACT #62593 IN ORDER TO MAINTAIN TRAFFIC FLOW.

UTILIZE DISTRICT 1 DETAIL TC-17 TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES TO INSTALL TEMPORARY PAVEMENT WIDENING FROM WB I-94 TO SB I-94 CONNECTOR STA. 330+34 TO STA. 333+92 AND FROM STA. 338+98 TO STA. 339+79 PRIOR TO STAGE 1.

UTILIZE DISTRICT 1 DETAIL TC-17 TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES TO INSTALL TEMPORARY PAVEMENT WIDENING AND AGGREGATE SHOULDER FROM NB I-57 STA. 219+16 TO EB I-57 TO I-94 CONNECTOR STA. 406+88 PRIOR TO STAGE 1A.

PERFORM THE FOLLOWING MEDIAN REMOVAL PARTIAL DEPTH OPERATION PRIOR TO THE START OF STAGE 1: ON NB I-57 FROM STA. 228+74 TO STA. 232+63 AT THE NB I-57/EB I-57 DIVERGING GORE.

PERFORM THE FOLLOWING TEMPORARY PAVEMENT AND COLD MILLING OPERATIONS PRIOR TO STAGE 1A: ON NB I-57 FROM STA. 190+65 TO STA. 207+92 AND FROM STA. 218+87 TO STA. 218+91; ON WB I-94 TO SB I-57 FROM STA. 330+66 TO STA. 340+29; ON MAINLINE NB I-94 (DAN RYAN) FROM STA. 2252+02 TO STA. 2257+02.

PERFORM THE FOLLOWING TEMPORARY PAVEMENT AND COLD MILLING OPERATIONS PRIOR TO STAGE 1B: ON NB I-94 (BISHOP FORD) FROM STA. 2042+78 TO STA. 2045+79; ON NB I-57 FROM STA. 255+32 TO STA. 258+32; ON MAINLINE NB I-94 (DAN RYAN) FROM STA. 2203+00 TO STA. 2252+02 AND FROM STA. 2252+02 TO 2252+06.

COMPLETE SHOULDER REHABILITATION FOR AREAS OF DAMAGED PAVEMENT PRIOR TO SHIFTING TRAFFIC.

IMPLEMENT HALSTED ENTRANCE RAMP TEMPORARY DETOUR FOR TRAFFIC REQUIRING ACCESS TO 87TH STREET.

REMOVE EXISTING PAVEMENT MARKINGS AND REPLACE WITH EPOXY PAVEMENT MARKINGS.

REMOVE LENSES FROM RAISED REFLECTIVE PAVEMENT MARKERS.

INSTALL AND RELOCATE TEMPORARY CONCRETE BARRIER WALL AND REMAINING TEMPORARY TRAFFIC CONTROL DEVICES FOR STAGE 1, 1A, 1B, & 1C CONSTRUCTION.

COVER PERMANENT AND TEMPORARY SIGNS AS NEEDED.

STAGE 1, 1A, 1B, & 1C CONSTRUCTION (AUGUST 1ST, 2006 - NOVEMBER 30, 2006)

STAGE 1 CONSTRUCTS THE FOLLOWING: LANE 3 & THE OUTSIDE SHOULDER ALONG NB I-57 FROM STA. 190+71 TO STA. 205+70; LANES 3, 4 & THE OUTSIDE SHOULDER ALONG NB I-57 FROM STA. 205+70 TO STA. 218+87; LANE 1 & THE INSIDE SHOULDER ALONG I-57 FROM STA. 232+56 TO STA. 248+30 INCLUDING THE INSIDE SECTION OF THE NB I-57 BRIDGE EXCLUDING THE BRIDGE APPROACH PAVEMENT; ACCIDENT INVESTIGATION SITE #5 ALONG NB I-94 (BISHOP FORD) FROM STA. 1990+49 TO STA. 1999+42; OUTSIDE SHOULDER CURB & GUTTER AND STORM SEWER INSTALLATION ALONG I-94 (BISHOP FORD) FROM STA. 2002+55 TO STA. 2020+99; LANE 2 AND THE OUTSIDE SHOULDER ALONG I-94 (BISHOP FORD) FROM STA. 2020+99 TO STA. 2045+79; INSIDE LANE ALONG THE WB I-94 TO SB I-57 CONNECTOR FROM STA. 330+65 TO STA. 340+29; LANES 4, 5, & THE OUTSIDE SHOULDER ALONG THE MAINLINE I-94 (DAN RYAN) FROM STA. 2203+00 TO STA. 2316+00. THE AREAS MENTIONED ABOVE INCLUDE ALL ADDITIONAL WORK WITHIN THEIR PROPOSED LIMITS.

NB I-57 TRAFFIC WILL BE SHIFTED LEFT TO THE INSIDE SHOULDER IN 2-11' LANES UP TO THE SPLIT WITH THE EB I-57 TO I-94 CONNECTOR. TRAFFIC APPROACHING THE NB BRIDGE WILL BE SHIFTED RIGHT TO THE OUTSIDE SHOULDER IN 2-11' LANES AND MERGE WITH NB I-94 (BISHOP FORD). NB I-94 (BISHOP FORD) TRAFFIC WILL BE SHIFTED LEFT TO THE INSIDE SHOULDER IN 2-11' LANES UP TO THE WB I-94 TO I-57 SPLIT. THE INSIDE LANE OF NB I-94 (BISHOP FORD) WILL EXIT TO THE WB I-94 TO I-57 CONNECTOR. THE OUTSIDE LANE OF NB I-94 (BISHOP FORD) WILL REMAIN SHIFTED TO THE LEFT UNTIL MERGING WITH NB I-57. WB I-94 TO I-57 CONNECTOR TRAFFIC WILL BE SHIFTED RIGHT TO THE OUTSIDE SHOULDER IN 1-11' LANE UP TO THE MERGE WITH SB I-57 AND CONTRACT #62593. MAINLINE NB I-94 (DAN RYAN) TRAFFIC WILL BE SHIFTED LEFT TO THE INSIDE SHOULDER IN 3-11' LANES UP TO STA. 2316+00 AND CONTRACT #60B17.

THE CONTRACTOR MUST COORDINATE WITH CONTRACT #62593 CONCERNING THE SB I-94 (BISHOP FORD) AND CTA TUNNEL REPAIR WORK. WORK ON TOP OF EITHER TUNNEL ROOF WILL NOT BE ALLOWED UNTIL THE REPAIR WORK IN CONTRACT #62593 IS COMPLETED.

THE CONTRACTOR MUST COORDINATE WITH CONTRACT #62593 CONCERNING STORM SEWER JACKING OPERATIONS WHICH SPANS ACROSS CONTRACT #62304'S CONSTRUCTION ZONE.

THE CONTRACTOR MUST COMPLETE ALL WORK IN THE FOLLOWING AREAS BEFORE IMPLEMENTING STAGE 1A: NB I-57 STA. 190+71 TO STA. 218+87; NB I-94 (BISHOP FORD) STA. 1990+49 TO 2045+77; WB I-94 TO SB I-57 STA 330+65 TO STA. 340+29; MAINLINE NB I-94 STA. 2203+00 TO 2257+02.

STAGE 1A CONSTRUCTS THE FOLLOWING: LANES 1, 2, & INSIDE SHOULDER ALONG NB I-57 FROM STA. 190+71 TO STA. 218+87; LANE 1, THE INSIDE SHOULDER, TEMPORARY CONCRETE PAVEMENT IN PLACE OF THE PERMANENT BRIDGE APPROACH PAVEMENT ALONG NB I-57 FROM STA. 218+87 TO STA. 248+30 INCLUDING THE NB I-57 BRIDGE; WB I-94/NB I-94 (BISHOP FORD) CORE AND STORM SEWER INSTALLATION ALONG NB I-94 (BISHOP FORD) FROM STA. 2009+72 TO STA. 2020+99; LANE 1 AND INSIDE SHOULDER ALONG I-94 (BISHOP FORD) FROM STA. 2020+99 TO STA. 2045+79; LANE 3 ALONG THE MAINLINE NB I-94 (DAN RYAN) FROM STA. 2203+00 TO STA. 2252+02. THIS WILL COINCIDE WITH THE WORK REMAINING FROM STAGE 1.

NB I-57 TRAFFIC WILL BE SHIFTED TO THE RIGHT ALONG THE OUTSIDE SHOULDER IN 2-11' LANES. NB I-94 (BISHOP FORD) TRAFFIC WILL BE SHIFTED RIGHT TO THE OUTSIDE SHOULDER IN 2-11' UP TO THE WB I-94 TO I-57 SPLIT. THE INSIDE LANE OF NB I-94 (BISHOP FORD) WILL EXIT TO THE WB I-94 TO I-57 CONNECTOR. THE OUTSIDE LANE OF NB I-94 (BISHOP FORD) WILL REMAIN SHIFTED TO THE RIGHT UNTIL MERGING WITH NB I-57. WB I-94 TO SB I-57 TRAFFIC WILL BE SHIFTED TO THE INSIDE SHOULDER IN 1-11' LANE UP TO STA. 326+00. TRAFFIC TRAVELING BELOW THE NB I-57 BRIDGE WILL BE SHIFTED TO THE NEWLY CONSTRUCTED LANE THROUGH THE MERGE WITH SB I-57 AND CONTRACT #62593. MAINLINE NB I-94 (DAN RYAN) TRAFFIC WILL BE SPLIT WITH 2-11' LANES ON THE INSIDE SHOULDER AND 1-11' LANE ON THE OUTSIDE SHOULDER UNTIL MERGING INTO 3-11' LANES AT STA. 2257+02.

DURING STAGE 1 AND 1A CONSTRUCTION ALONG NB I-94 (DAN RYAN), ALL CONSTRUCTION VEHICLES MUST STAY WITHIN THE HAUL ROAD. CROSSING EXIT RAMP TRAFFIC IS PROHIBITED. ADDITIONALLY, THE CONTRACTOR SHALL BE RESPONSIBLE TO DIFFERENTIATE BETWEEN THE RAMP EXIT AND HAUL ROAD ENTRANCE. THE CONTRACTOR SHALL UTILIZE EITHER THE A.I.S. OR C-D ROADWAY TO MERGE HAUL ROAD AND RAMP TRAFFIC. A STOP SIGN SHALL BE PLACED AT THE UPSTREAM END OF THE C-D ROADWAY TO STOP TRAFFIC ON THE RAMP FROM THE FRONTAGE ROAD. ALL SIGNING REQUIRED BY TC-18 SHALL BE APPLIED TO ANY WORK ZONE ACCESS OPTION EMPLOYED BY THE CONTRACTOR.

THE CONTRACTOR MUST COMPLETE ALL CONSTRUCTION ON NB I-57 FROM STA. 190+71 TO STA. 207+70 EXCEPT WORK WHICH IS CONSTRAINED BY THE LIMITED WORKING SPACE BEFORE IMPLEMENTING STAGE 1B.

THE CONTRACTOR MUST ALSO COMPLETE ALL WORK ON THE NB I-94 (BISHOP FORD) BEFORE IMPLEMENTING STAGE 1B.

STAGE 1B IS REQUIRED TO GIVE THE CONTRACTOR ADEQUATE WORKING SPACE TO POUR LANES 1 AND 2 ON NB I-57 FROM STA. 190+71 TO 207+70. TRAFFIC WILL BE SHIFTED AND CLOSED TO 1-11' LANE. NB I-94 (BISHOP FORD) TRAFFIC WILL BE SHIFTED LEFT IN 2-11' LANES UP TO THE MERGE WITH NB I-57. MAINLINE NB I-94 (DAN RYAN) TRAFFIC WILL BE IN 4-11' LANES UP TO THE 87TH STREET EXIT WHEN ONE LANE OF TRAFFIC WILL BE DROPPED AND WHERE THE STAGE 1 LANE CONFIGURATION WILL BE MAINTAINED. DUE TO THE SEVERE TRAFFIC CONSTRAINTS THIS STAGE WILL BE LIMITED TO A 14 DAY DURATION AND WILL COINCIDE WITH THE WORK REMAINING FROM STAGES 1 AND 1A.

STAGE 1C IS REQUIRED TO RETURN NB I-57 TO A 2-11' LANE CONFIGURATION ALONG THE OUTSIDE SHOULDER FROM STA. 190+71 TO 207+70. THIS CONFIGURATION WILL REMAIN UNTIL NOVEMBER 30, 2006 AND COINCIDE WITH WORK REMAINING IN STAGES 1 AND 1A.

PRE WINTER LANE CONFIGURATION - NIGHT TIME OPERATIONS

PERFORM THE FOLLOWING TEMPORARY PAVEMENT AND COLD MILLING OPERATIONS: ALONG NB I-57 BETWEEN NEWLY CONSTRUCTED LANE 1 AND EXISTING LANE 2 FROM STA. 218+87 TO STA. 232+57; ALONG MAINLINE NB I-94 (DAN RYAN) BETWEEN EXISTING LANE 3 AND NEWLY CONSTRUCTED LANE 4 FROM STA. 2257+02 TO STA. 2316+00.

COORDINATE FOUR LANES OF TRAFFIC AT NB I-94 (DAN RYAN) STA. 2316+00 TO WITH CONTRACT #60B17 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE SINGLE LANE OF TRAFFIC AT WB I-94 TO I-57 CONNECTOR MERGE WITH CONTRACT #62593 AT STA. 340+29 IN ORDER TO MAINTAIN TRAFFIC FLOW.

REMOVE CONFLICTING TEMPORARY PAVEMENT MARKINGS FROM STAGES 1, 1A, & 1C AND REPLACE WITH EPOXY PAVEMENT MARKINGS.

RELOCATE TEMPORARY CONCRETE BARRIER WALL TO CREATE ADEQUATE SHOULDERS FOR SNOW STORAGE AND RELOCATE REMAINING TEMPORARY TRAFFIC CONTROL DEVICES FOR THE WINTER LANE CONFIGURATION.

COVER & UNCOVER PERMANENT AND TEMPORARY SIGNS AS NEEDED.

WINTER LANE CONFIGURATION (DECEMBER 1, 2006 - APRIL 1, 2007)

NB I-57 TRAFFIC WILL BE SHIFTED LEFT TO THE INSIDE SHOULDER IN 3-11' LANES FROM STA. 190+71 TO STA. 224+86. TRAFFIC APPROACHING THE NB I-57 BRIDGE WILL BE SHIFTED TO THE OUTSIDE SHOULDER IN 2-11' LANES AND MERGE WITH NB I-94 (BISHOP FORD). THE OUTSIDE LANE OF NB I-57 WILL EXIT TO THE EB I-57 TO I-94 CONNECTOR. NB I-94 (BISHOP FORD) TRAFFIC WILL BE SHIFTED LEFT TO THE INSIDE SHOULDER IN 3-11' LANES FROM STA. 1995+79 TO STA. 2004+12. THE INSIDE LANE OF NB I-94 (BISHOP FORD) WILL EXIT TO THE WB I-94 TO I-57 CONNECTOR. THE OUTSIDE 2 LANES WILL MERGE WITH NB I-57. WB I-94 TO I-57 CONNECTOR TRAFFIC WILL BE SHIFTED TO THE OUTSIDE SHOULDER FROM STA. 307+43 TO STA. 328+00. TRAFFIC WILL THEN BE SHIFTED TOWARD THE CENTER OF THE NEWLY CONSTRUCTED PAVEMENT AND CONTRACT #62593. MAINLINE NB I-94 (DAN RYAN) TRAFFIC WILL BE SHIFTED LEFT TO THE LEFT SHOULDER FROM STA. 2203+00 TO STA. 2316+00.

PRE STAGE 2 - NIGHT TIME OPERATIONS

COORDINATE THREE LANES OF SPLIT TRAFFIC AT NB I-94 (DAN RYAN) STA. 2316+00 WITH CONTRACT #60B17 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE SINGLE LANE OF TRAFFIC AT WB I-94 TO I-57 CONNECTOR STA. 340+29 WITH CONTRACT #62593 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE SINGLE LANE OF TRAFFIC AT EB I-57 TO I-94 CONNECTOR STA. 415+06 WITH CONTRACT #62593 IN ORDER TO MAINTAIN TRAFFIC FLOW.

UTILIZE DISTRICT 1 DETAIL TC-17 TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES TO INSTALL TEMPORARY PAVEMENT WIDENING FROM NB I-57 STA. 248+30 TO STA. 253+23.

REMOVE CONFLICTING TEMPORARY PAVEMENT MARKINGS FROM THE WINTER LANE CONFIGURATION STAGE AND REPLACE WITH EPOXY OR PAVEMENT MARKING TAPE.

RELOCATE TEMPORARY CONCRETE BARRIER WALL AND REMAINING TEMPORARY TRAFFIC CONTROL DEVICES FOR STAGE 2.

COVER & UNCOVER PERMANENT AND TEMPORARY SIGNS AS NEEDED.

STAGE 2 CONSTRUCTION (APRIL 2, 2007 - MAY 31, 2007)

STAGE 2 CONSTRUCTS THE FOLLOWING: LANE 2 AND EB I-57 / NB I-57 DIVERGING GORE ALONG NB I-57 FROM STA. 218+87 TO STA. 232+98; LANES 2, 3, THE OUTSIDE SHOULDER, INCLUDING THE NB I-57 BRIDGE ALONG NB I-57 FROM STA. 232+98 TO STA. 255+32; LANE 1, AND INSIDE SHOULDER ALONG THE EB I-57 TO I-94 CONNECTOR FROM STA. 408+41 TO STA. 415+06; INSIDE SHOULDER, CURB AND GUTTER, & STORM SEWER INSTALLATION ALONG WB I-94 TO I-57 CONNECTOR FROM STA. 299+53 TO STA. 329+05; LANE 3 ALONG MAINLINE NB I-94 (DAN RYAN) FROM STA. 2252+02 TO STA. 2316+00



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 GENERAL NOTES
 CONSTRUCTION STAGING NOTES
 MAINTENANCE OF TRAFFIC DETAILS
 SHEET 1 OF 13

SCALE: NO SCALE DRAWN BY: RTM
 DATE: MARCH 7, 2006 CHECKED BY: TGB

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	188
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
STA. 1990+48 (NB FORD)				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
• (1516.1, 1717, & 1818) R-4		62304		

STAGE 2 CONSTRUCTION (APRIL 2, 2007 - MAY 31, 2007) - CONTINUE

NB I-57 TRAFFIC WILL BE SHIFTED RIGHT TO THE OUTSIDE SHOULDER UNTIL SPLITTING AT STA. 212+39. THE INSIDE LANE APPROACHING THE NB I-57 BRIDGE WILL BE SHIFTED LEFT TO THE INSIDE SHOULDER UNTIL MERGING WITH NB I-94 (BISHOP FORD). THE OUTSIDE LANE WILL EXIT TO THE EB I-57 TO I-94 CONNECTOR AND WILL BE SHIFTED RIGHT ALONG THE OUTSIDE SHOULDER IN 1-11' LANE UP TO STA. 415+06 AND CONTRACT #62593. NB I-94 (BISHOP FORD) TRAFFIC WILL REMAIN IN THE SAME CONFIGURATION AS IT WAS DURING THE WINTER LANE CONFIGURATION. WB I-94 TO SB I-57 CONNECTOR TRAFFIC WILL REMAIN IN THE SAME CONFIGURATION AS IT WAS DURING THE WINTER LANE CONFIGURATION AND MATCH CONTRACT #62593 AT STA. 340+29. MAINLINE NB I-94 (DAN RYAN) TRAFFIC WILL BE SHIFTED AND LEFT TO THE INSIDE SHOULDER IN 3-11' LANES FROM STA. 2203+00 TO 2231+32. FROM MAINLINE NB I-94 (DAN RYAN) STA. 2231+32 TO STA. 2239+95, TRAFFIC WILL ADD 1-11' EXIT ONLY / MERGE LANE. FROM NB I-94 (DAN RYAN) STA. 2239+95 TO STA. 2244+03, TRAFFIC WILL RETURN TO 3-11' LANES ALONG THE INSIDE SHOULDER. FROM NB I-94 (DAN RYAN) STA. 2244+03 TO STA. 2316+00, TRAFFIC WILL BE SPLIT INTO 2-11' LANES ALONG THE INSIDE SHOULDER AND 1-11' LANE ALONG THE OUTSIDE SHOULDER.

THE CONTRACTOR MUST COMPLETE LANE 3 FROM MAINLINE NB I-94 (DAN RYAN) STA. 2252+02 TO STA. 2316+00 DURING THE TIME PROVIDED IN STAGE 2.

PRE STAGE 3 & 3A - NIGHT TIME OPERATIONS

COORDINATE TRAFFIC MERGE AT NB I-94 (DAN RYAN) STA. 2316+00 WITH CONTRACT #60B17 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE SINGLE LANE OF TRAFFIC AT WB I-94 TO I-57 CONNECTOR STA. 340+29 WITH CONTRACT #62593 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE SINGLE LANE OF TRAFFIC AT EB I-57 TO I-94 CONNECTOR STA. 415+06 WITH CONTRACT #62593 IN ORDER TO MAINTAIN TRAFFIC FLOW.

REMOVE CONFLICTING TEMPORARY PAVEMENT MARKINGS FROM STAGE 2 AND REPLACE WITH EPOXY OR PAVEMENT MARKING TAPE.

RELOCATE TEMPORARY CONCRETE BARRIER WALL AND REMAINING TEMPORARY TRAFFIC CONTROL DEVICES FOR STAGES 3 & 3A.

PERFORM TEMPORARY PAVEMENT OPERATIONS ON THE WB I-94 TO I-57 CONNECTOR FROM STA. 337+39 TO STA. 340+29

COVER & UNCOVER PERMANENT AND TEMPORARY SIGNS AS NEEDED.

STAGE 3 & 3A CONSTRUCTION (JUNE 1, 2007 - OCTOBER 31, 2007)

STAGE 3 CONSTRUCTS THE FOLLOWING: LANE 2 AND EB I-94 / NB I-57 DIVERGING GORE ALONG I-57 FROM STA. 218+87 TO STA. 232+98; LANES 2, 3, THE OUTSIDE SHOULDER, AND THE NB I-57 BRIDGE ALONG NB I-57 FROM STA. 232+98 TO STA. 255+32; LANE 1 AND INSIDE SHOULDER ALONG THE EB I-57 TO I-94 CONNECTOR FROM STA. 408+41 TO STA. 415+06; LANE 2, OUTSIDE SHOULDER, AND CURB AND GUTTER ALONG WB I-94 TO I-57 CONNECTOR FROM STA. 330+65 TO STA. 337+38; LANE 2 ALONG THE WB I-94 TO I-57 CONNECTOR FROM STA. 337+38 TO STA. 340+29; LANES 1, 2, AND THE INSIDE SHOULDER ALONG MAINLINE NB I-94 (DAN RYAN) FROM STA. 2205+51 TO STA. 2316+00.

NB I-57 TRAFFIC AND EB I-57 TO I-94 CONNECTOR TRAFFIC WILL REMAIN IN THE SAME CONFIGURATION AS THEY WERE DURING STAGE 2. NB I-94 (BISHOP FORD) TRAFFIC WILL REMAIN IN THE SAME CONFIGURATION AS IT WAS DURING THE WINTER LANE CONFIGURATION. WB I-94 TO I-57 CONNECTOR TRAFFIC WILL REMAIN IN THE SAME CONFIGURATION AS IT WAS DURING THE WINTER LANE CONFIGURATION UP TO STA. 323+23. TRAFFIC WILL THEN BE SHIFTED TO THE INSIDE SHOULDER IN 1-11' LANE TO STA. 340+29 AND CONTRACT #62593. MAINLINE NB I-94 (DAN RYAN) TRAFFIC WILL BE SHIFTED TO THE OUTSIDE SHOULDER FROM STA. 2203+00 TO STA. 2316+00 AND CONTRACT #60B17.

THE CONTRACTOR MUST COMPLETE ALL WORK IN THE FOLLOWING AREAS BEFORE IMPLEMENTING STAGE 3A: NB I-57 FROM STA. 218+87 TO STA. 255+32, INCLUDING THE NB I-57 BRIDGE; EB I-57 TO I-94 CONNECTOR FROM STA. 408+41 TO STA. 415+06; WB I-94 TO I-57 CONNECTOR FROM STA. 326+73 TO STA. 334+50.

STAGE 3A CONSTRUCTS THE FOLLOWING: LANES 3, 4, AND THE OUTSIDE SHOULDER ALONG NB I-57 FROM STA. 218+87 TO STA. 224+86; INSIDE BRIDGE APPROACH PAVEMENT ALONG NB I-57 FROM NB I-57 STATION RANGES 236+69 TO 237+58 AND 238+50 TO 239+86; LANE 1, PART OF LANE 2, AND INSIDE SHOULDER ALONG NB I-57 FROM STA. 248+31 TO STA. 258+32; LANE 2 AND OUTSIDE SHOULDER ALONG THE EB I-57 TO I-94 CONNECTOR FROM STA. 400+26 TO STA. 415+06; INSIDE SHOULDER ALONG WB I-94 TO I-57 CONNECTOR FROM STA. 330+65 TO STA. 340+29; LANES 1, 2, AND INSIDE SHOULDER ALONG MAINLINE NB I-94 (DAN RYAN) FROM STA. 2203+00 TO STA. 2205+51. THIS WILL COINCIDE WITH THE WORK REMAINING FROM STAGE 3.

NB I-57 WILL BE SHIFTED LEFT TO THE INSIDE SHOULDER IN 2-11' LANES FROM STA. 191+42 TO STA. 227+89. TRAFFIC WILL THEN BE WEAVED RIGHT TO THE OUTSIDE SHOULDER IN 2-11' LANES UP TO THE MERGE WITH NB I-94 (BISHOP FORD). EB I-94 TO I-94 CONNECTOR TRAFFIC WILL BE SHIFTED TO THE INSIDE SHOULDER FROM STA. 407+91 TO STA. 415+06 AND CONTRACT #62593. NB I-94 (BISHOP FORD) TRAFFIC WILL REMAIN IN THE SAME CONFIGURATION AS DURING THE WINTER LANE CONFIGURATION UNTIL MERGING WITH NB I-57. WB I-94 TO I-57 CONNECTOR TRAFFIC WILL REMAIN IN THE SAME CONFIGURATION AS DURING THE WINTER LANE CONFIGURATION UP TO STA. 323+23. TRAFFIC WILL THEN BE SHIFTED RIGHT TO THE OUTSIDE SHOULDER TO STA. 340+29 AND CONTRACT #62593. MAINLINE I-94 (DAN RYAN) TRAFFIC WILL BE SHIFTED RIGHT ALONG THE OUTSIDE SHOULDER FROM STA. 2203+00 TO STA. 2205+51 TO MATCH THE STAGE 3 LANE CONFIGURATION.

THE CONTRACTOR MUST COORDINATE WITH CONTRACT #62593 IN ORDER TO CONSTRUCT THE OUTSIDE SHOULDER ALONG THE WB I-94 TO SB I-57 CONNECTOR FROM STA. 337+38 TO STA. 340+29 PRIOR TO THE END OF STAGE 3A.

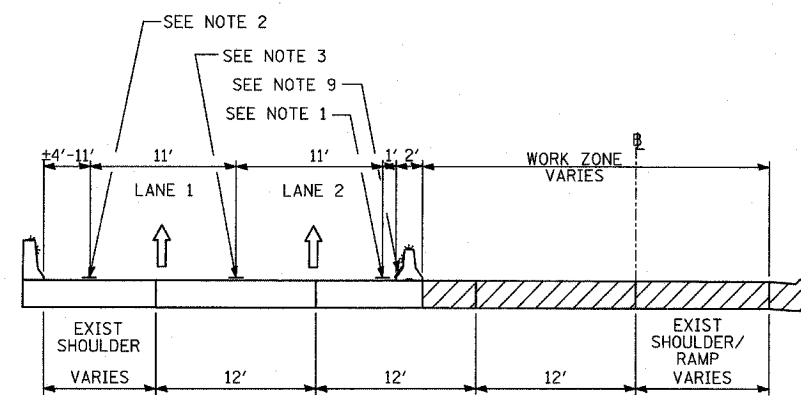


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
 GENERAL NOTES
 CONSTRUCTION STAGING NOTES
 MAINTENANCE OF TRAFFIC DETAILS
 SHEET 2 OF 13

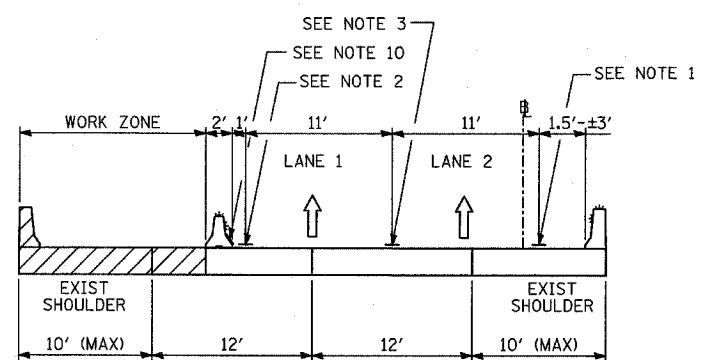
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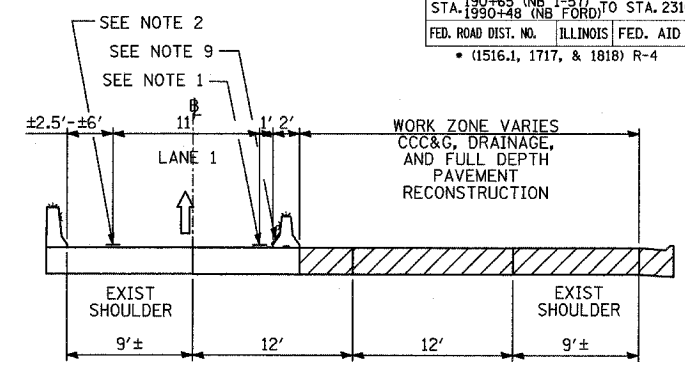
A-A

STAGE 1: NB I-57; STA. 190+71 TO STA. 218+90



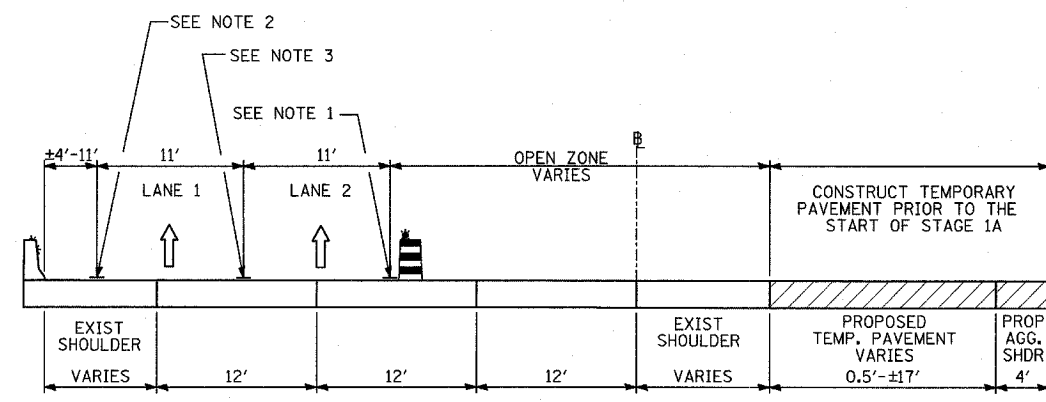
D-D

STAGE 1: NB I-57; STA. 232+55 TO STA. 235+77
 STAGE 1: NB I-57; STA. 240+93 TO STA. 248+30
 STAGE 1A: NB I-57; STA. 232+55 TO STA. 235+77
 STAGE 1A: NB I-57; STA. 240+93 TO STA. 248+30



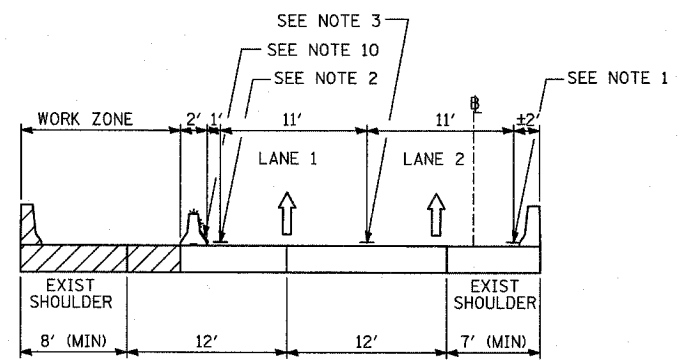
G-G

STAGE 1: NB I-94 (BISHOP FORD); STA. 2010+30 TO STA. 2040+14



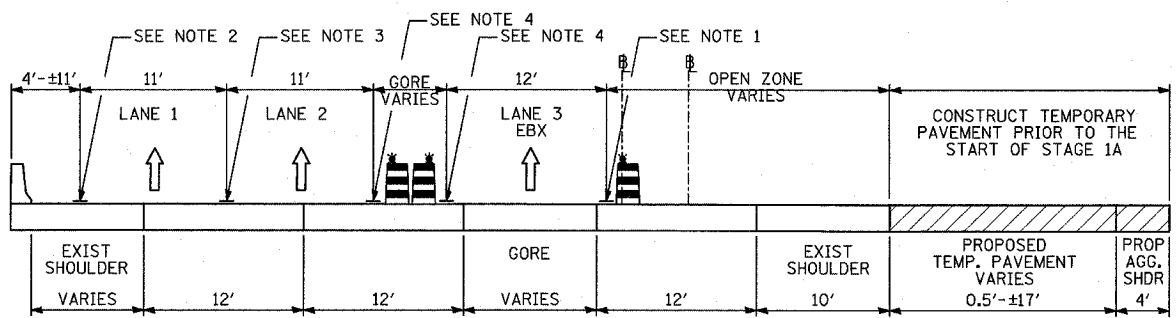
B-B

STAGE 1: NB I-57; STA. 218+90 TO STA. 221+86



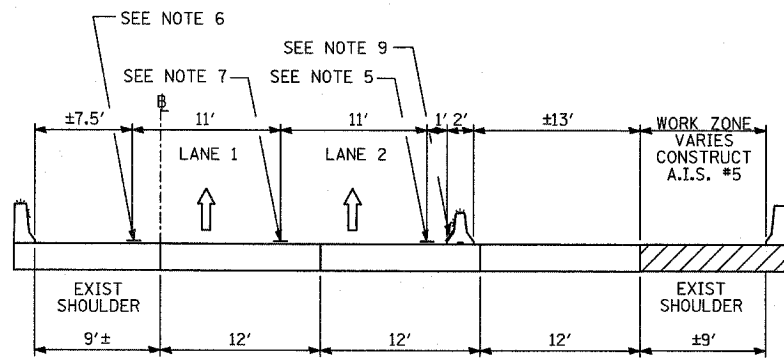
E-E

STAGE 1: NB I-57; STA. 235+77 TO STA. 240+93
 STAGE 1A: NB I-57; STA. 235+77 TO STA. 240+93



C-C

STAGE 1: NB I-57; STA. 224+86 TO STA. 228+55



F-F

STAGE 1: NB I-94 (BISHOP FORD); STA. 1990+32 TO STA. 1998+33

- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING-LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

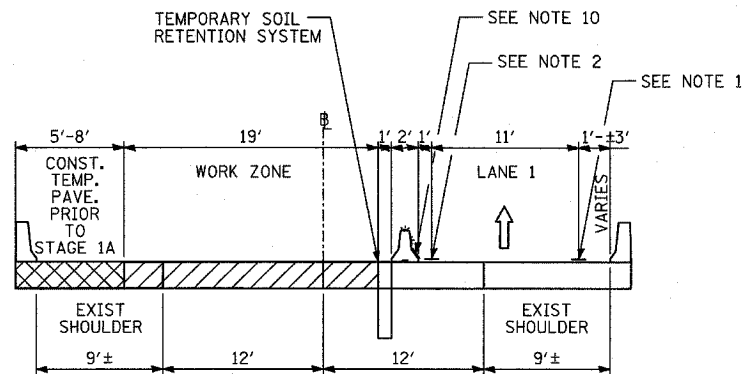
- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

• ALL NOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC

REVISIONS	
NAME	DATE

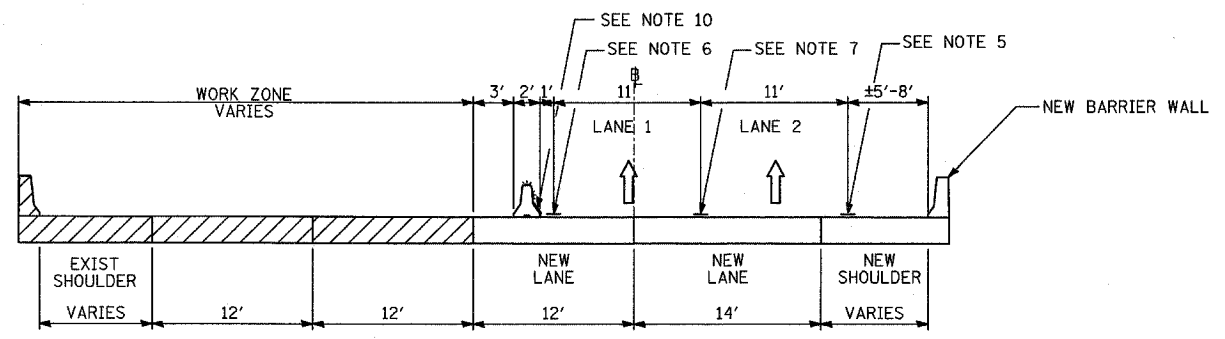
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 GENERAL NOTES
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 SHEET 3 OF 13

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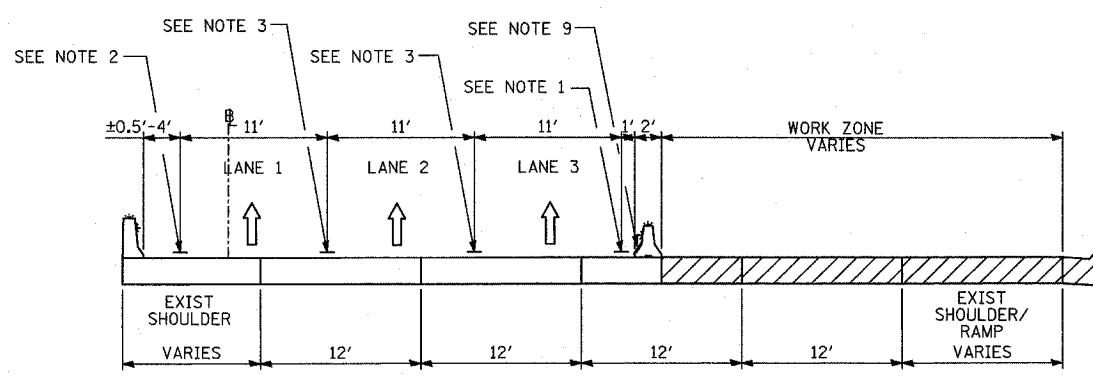
H-H

STAGE 1: WB CONNECTOR; STA. 333+92 TO STA. 337+12



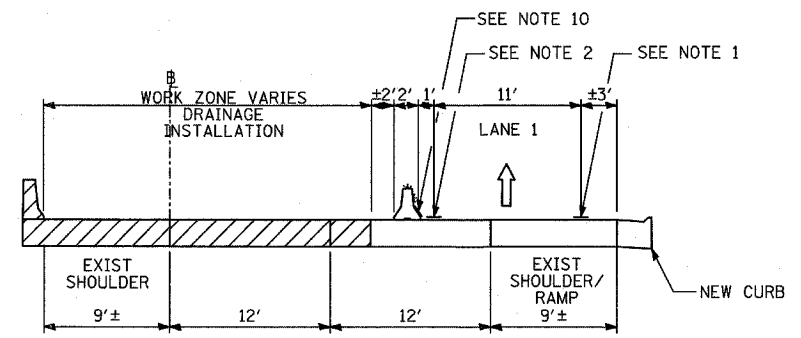
K-K

STAGE 1A: NB I-57; STA. 209+67 TO STA. 217+08
 STAGE 1B: NB I-57; STA. 209+67 TO STA. 217+08
 STAGE 1C: NB I-57; STA. 209+67 TO STA. 217+08



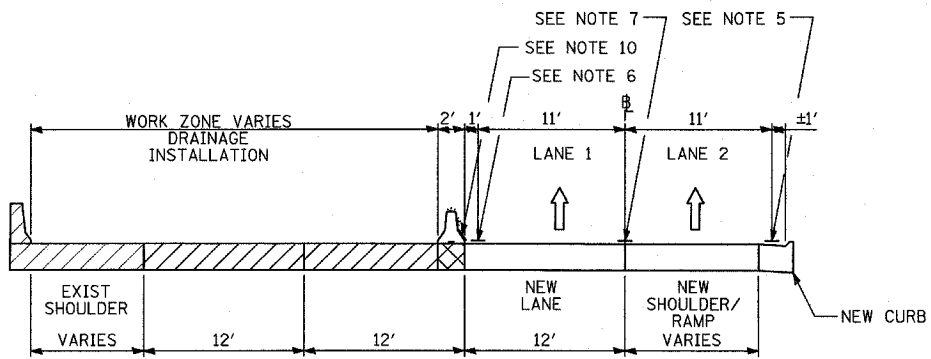
I-I

STAGE 1: NB I-94 (DAN RYAN); STA. 2203+00 TO STA. 2316+00



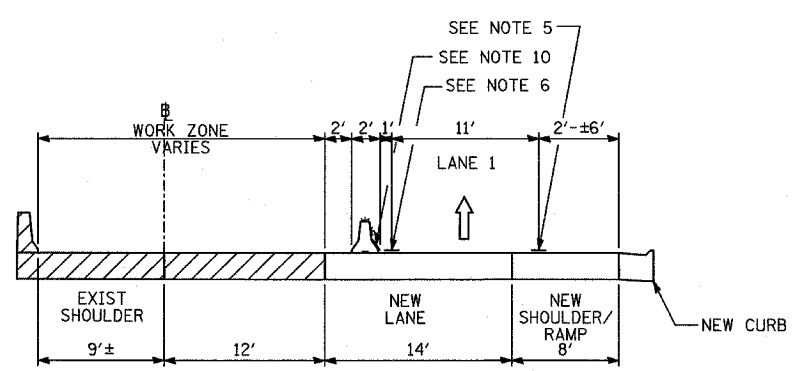
L-L

STAGE 1A: NB I-94 (BISHOP FORD); STA. 2011+57 TO STA. 2020+99



J-J

STAGE 1A: NB I-57; STA. 190+71 TO STA. 205+00



M-M

STAGE 1A: NB I-94 (BISHOP FORD); STA. 2020+99 TO STA. 2042+78

- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING-LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

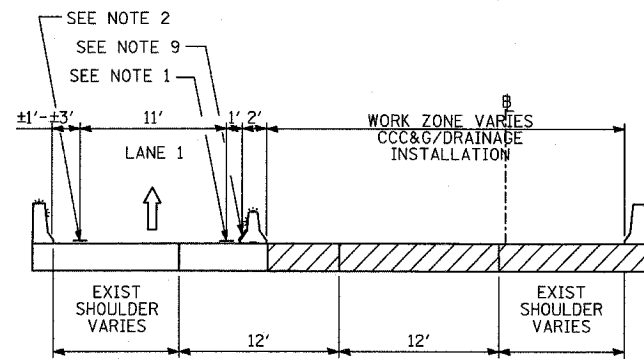
- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

* ALL NOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC

REVISIONS	
NAME	DATE

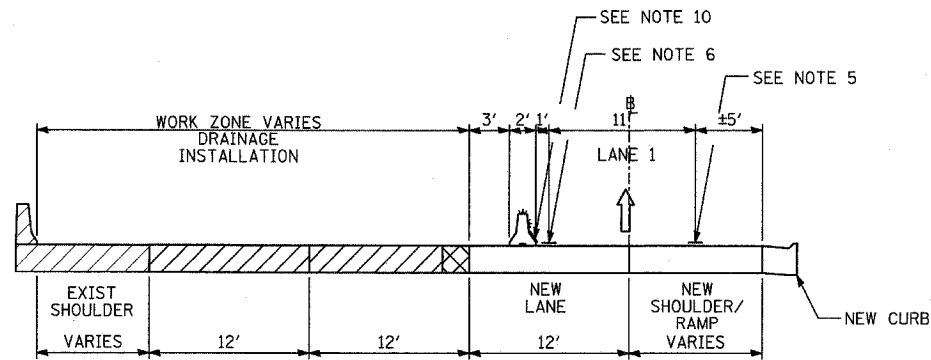
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 GENERAL NOTES
 CONSTRUCTION STAGING NOTES
 MAINTENANCE OF TRAFFIC DETAILS
 SHEET 4 OF 13

SCALE: NO SCALE DRAWN BY: RTM
 DATE: MARCH 7, 2006 CHECKED BY: TGB



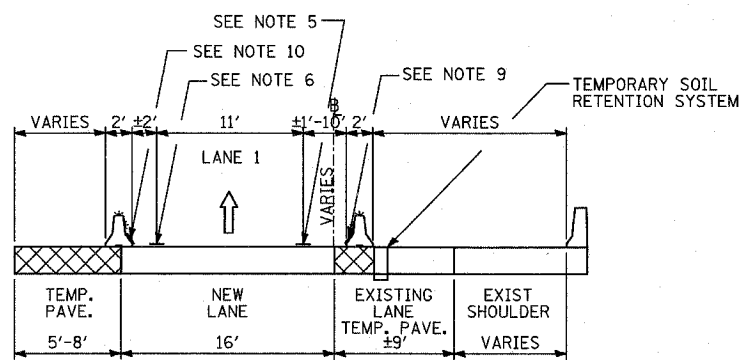
N-N

STAGE 1A: WB CONNECTOR; STA. 307+47 TO STA. 330+04
 STAGE 1B: WB CONNECTOR; STA. 313+92 TO STA. 330+04
 STAGE 1C: WB CONNECTOR; STA. 313+92 TO STA. 330+04



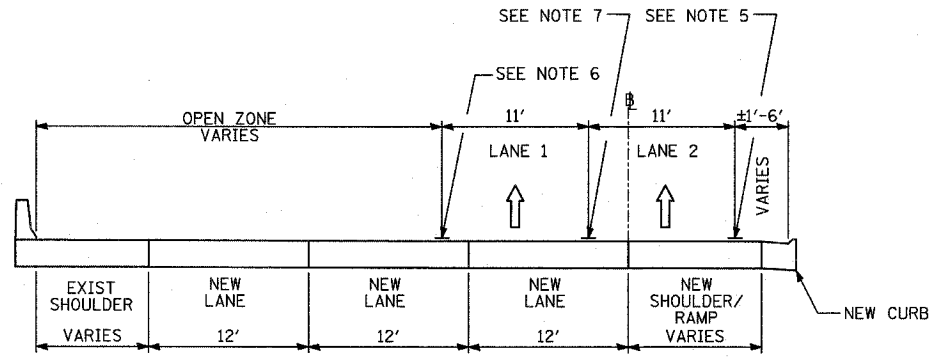
Q-Q

STAGE 1B: NB I-57; STA. 190+71 TO STA. 207+70



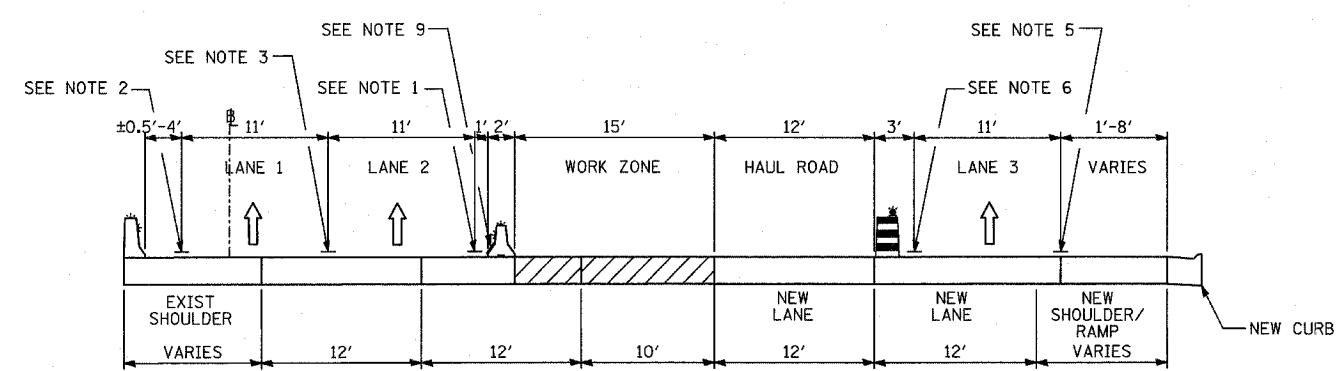
O-O

STAGE 1A: WB CONNECTOR; STA. 333+92 TO STA. 340+29
 STAGE 1B: WB CONNECTOR; STA. 333+92 TO STA. 340+29
 STAGE 1C: WB CONNECTOR; STA. 333+92 TO STA. 340+29



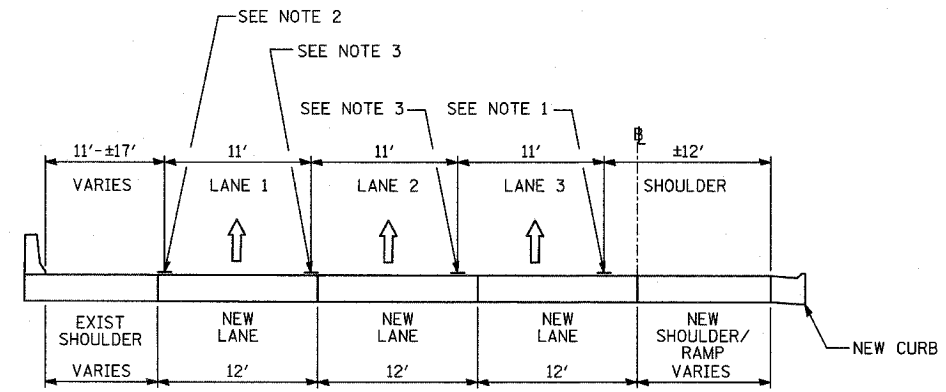
R-R

STAGE 1C: NB I-57; STA. 190+71 TO STA. 207+70



P-P

STAGE 1A: NB I-57; STA. 255+32 TO STA. 258+32
 STAGE 1A: NB I-94 (DAN RYAN); STA. 2203+00 TO STA. 2252+02



S-S

WINTER LANE CONFIGURATION: NB I-57; STA. 190+71 TO STA. 206+11

- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

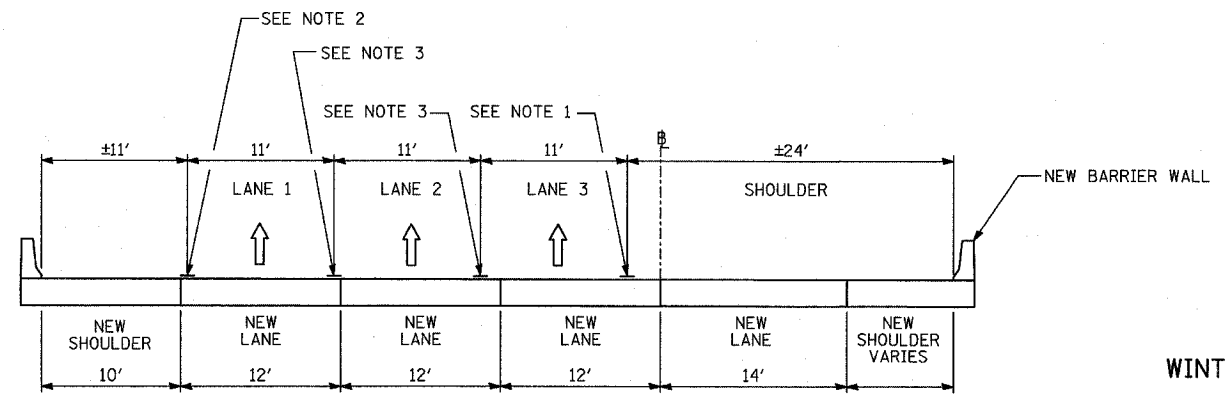
- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

* ALL NOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC

REVISIONS	
NAME	DATE

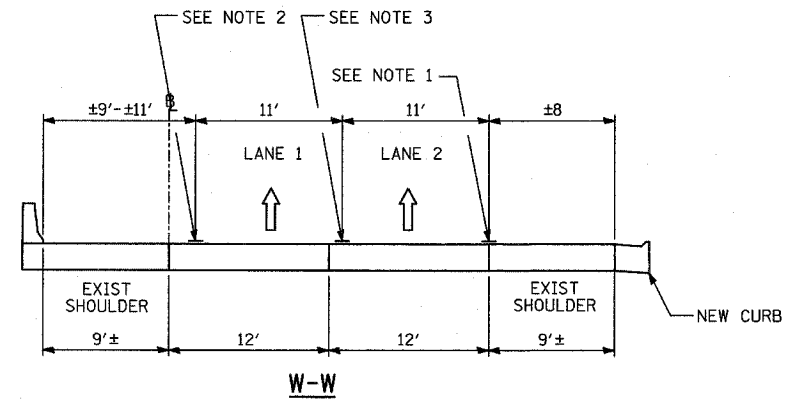
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 GENERAL NOTES
 CONSTRUCTION STAGING NOTES
 MAINTENANCE OF TRAFFIC DETAILS
 SHEET 5 OF 13

SCALE: NO SCALE DRAWN BY: RTM
 DATE: MARCH 7, 2006 CHECKED BY: TGB



T-T

WINTER LANE CONFIGURATION: NB I-57; STA. 209+07 TO STA. 218+71



W-W

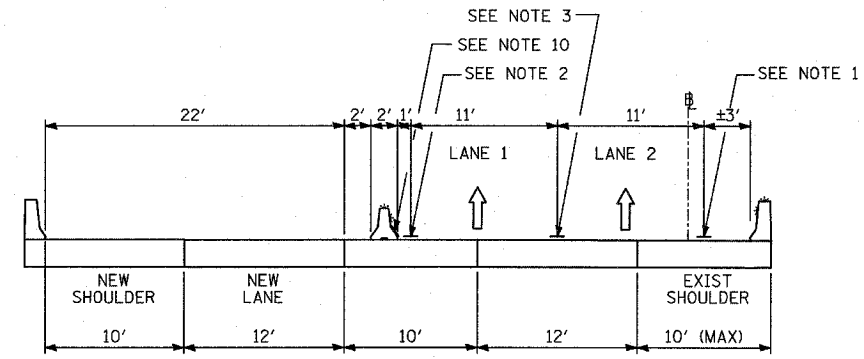
STAGE 1B: NB I-94 (BISHOP FORD); STA. 2010+45 TO STA. 2020+99
 WINTER LANE CONFIGURATION: NB I-94 (BISHOP FORD); STA. 2010+45 TO STA. 2020+99
 STAGE 2: NB I-94 (BISHOP FORD); STA. 2010+45 TO STA. 2020+99
 STAGE 3: NB I-94 (BISHOP FORD); STA. 2010+45 TO STA. 2020+99
 STAGE 3A: NB I-94 (BISHOP FORD); STA. 2010+45 TO STA. 2020+99

- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30" SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING-LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30" SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

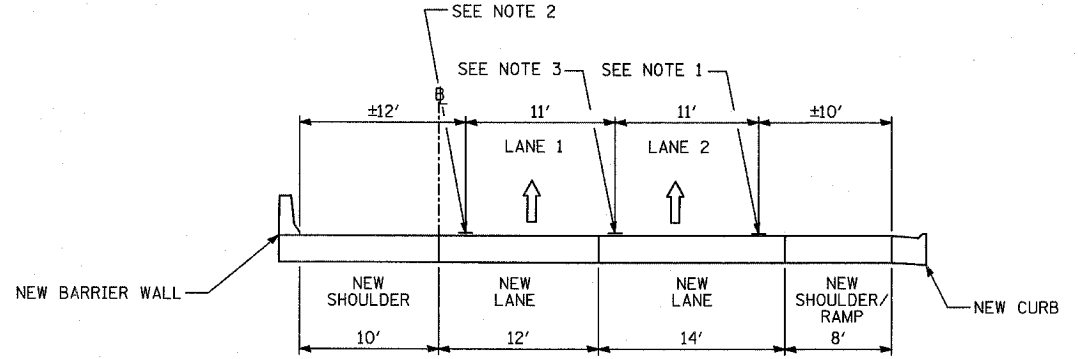
- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

• ALL MOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC



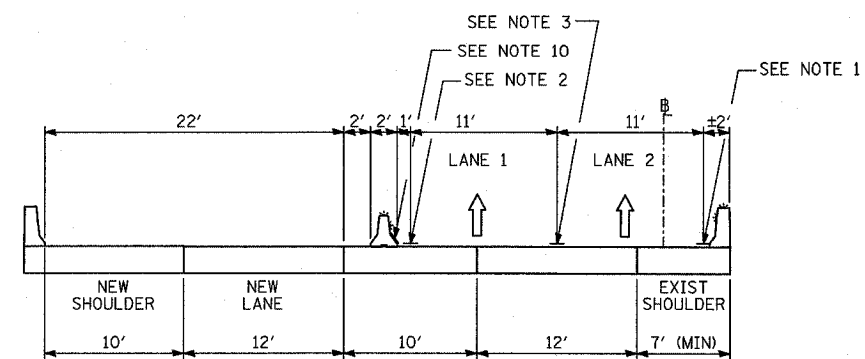
U-U

WINTER LANE CONFIGURATION: NB I-57; STA. 232+89 TO STA. 235+77
 WINTER LANE CONFIGURATION: NB I-57; STA. 240+93 TO STA. 248+30



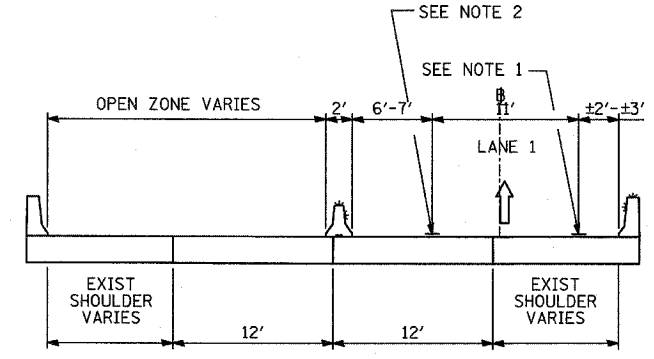
X-X

STAGE 1B: NB I-94 (BISHOP FORD); STA. 2020+99 TO STA. 2045+37
 WINTER LANE CONFIGURATION: NB I-94 (BISHOP FORD); STA. 2020+99 TO STA. 2042+78
 STAGE 2: NB I-94 (BISHOP FORD); STA. 2020+99 TO STA. 2041+78
 STAGE 3: NB I-94 (BISHOP FORD); STA. 2020+99 TO STA. 2041+78
 STAGE 3A: NB I-94 (BISHOP FORD); STA. 2020+99 TO STA. 2041+78



V-V

WINTER LANE CONFIGURATION: NB I-57; STA. 235+77 TO STA. 240+93



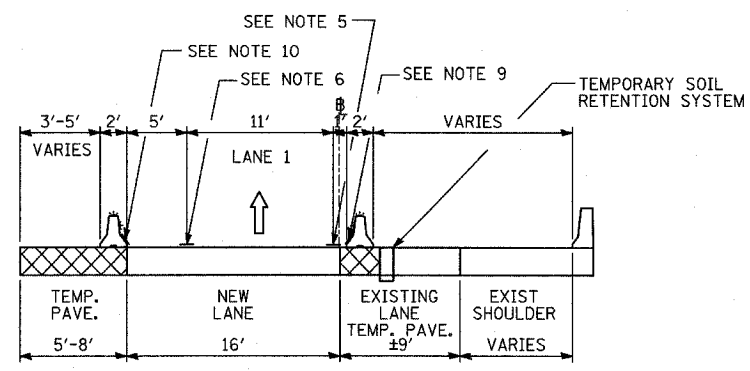
Y-Y

WINTER LANE CONFIGURATION: WB CONNECTOR; STA. 307+43 TO STA. 330+07

REVISIONS	
NAME	DATE

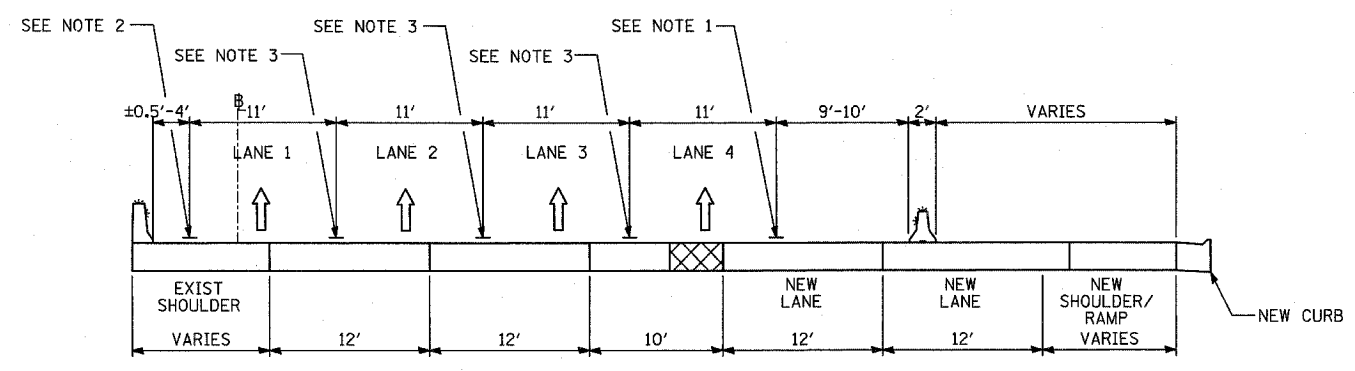
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 GENERAL NOTES
 CONSTRUCTION STAGING NOTES
 MAINTENANCE OF TRAFFIC DETAILS
 SHEET 6 OF 13

SCALE: NO SCALE
 DATE: MARCH 7, 2006
 DRAWN BY: RTM
 CHECKED BY: TGB



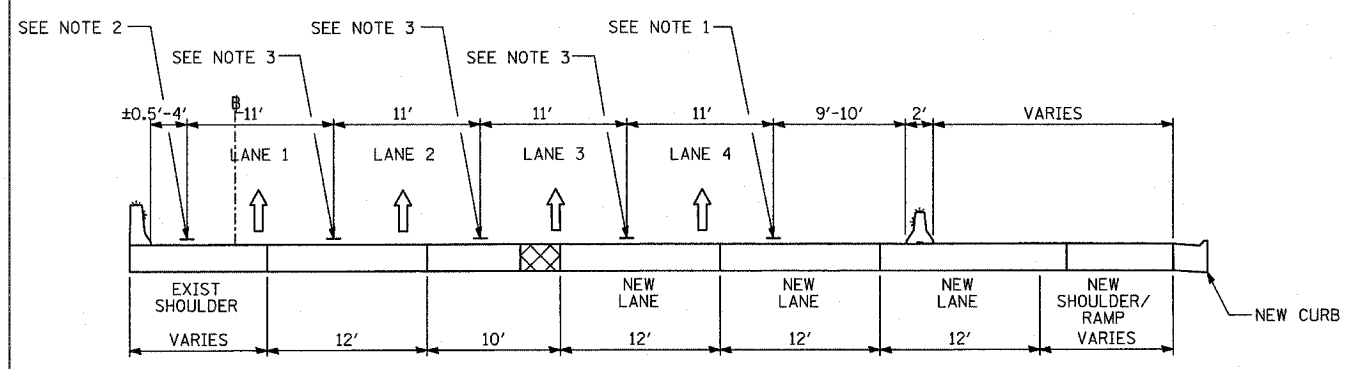
Z-Z

WINTER LANE CONFIGURATION: WB CONNECTOR; STA. 334+92 TO STA. 340+29



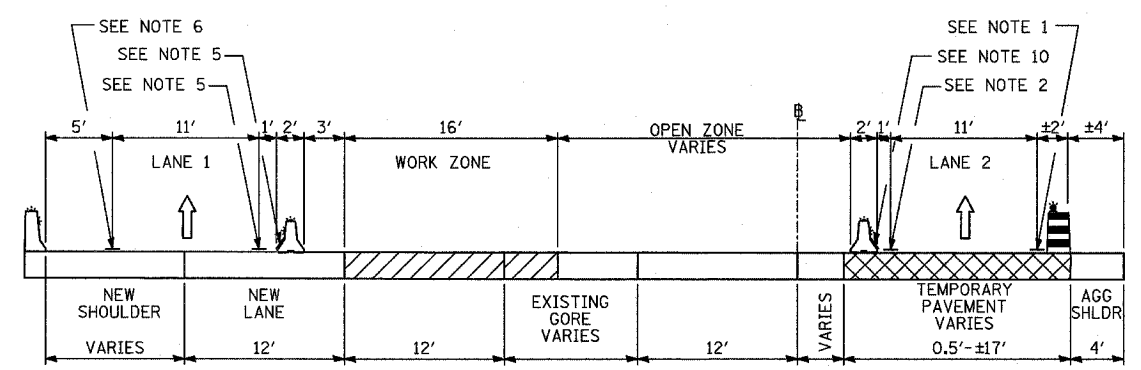
CC-CC

WINTER LANE CONFIGURATION: NB I-94 (DAN RYAN); STA. 2252+02 TO STA. 2316+00



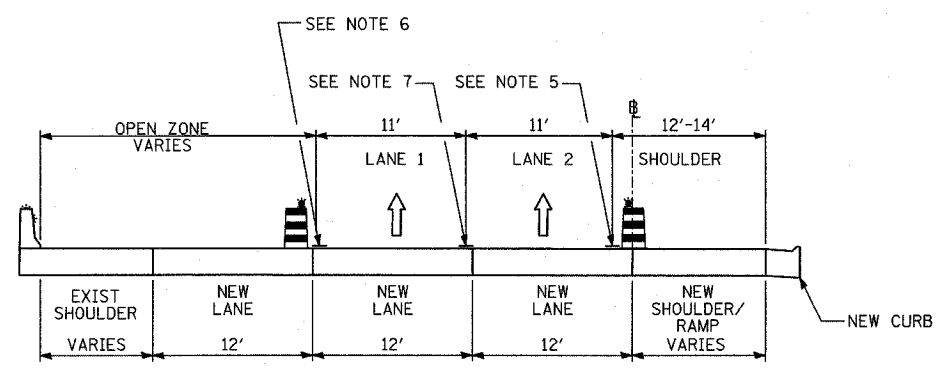
AA-AA

STAGE 1B: NB I-94 (DAN RYAN); STA. 2206+02 TO STA. 2229+02
WINTER LANE CONFIGURATION: NB I-94 (DAN RYAN); STA. 2206+02 TO STA. 2252+02



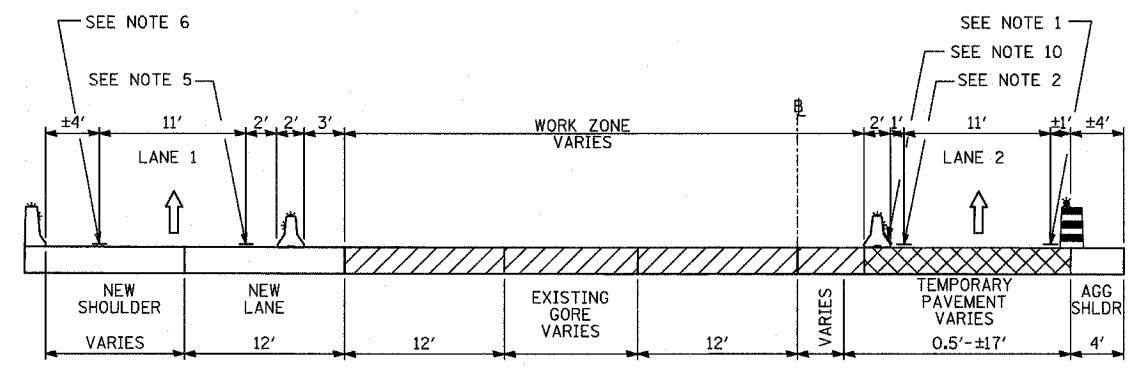
DD-DD

STAGE 2: NB I-57; STA. 219+18 TO STA. 224+86
STAGE 3: NB I-57; STA. 219+18 TO STA. 224+86



BB-BB

STAGE 2: NB I-57; STA. 190+71 TO STA. 206+11
STAGE 3: NB I-57; STA. 190+71 TO STA. 206+11



EE-EE

STAGE 2: NB I-57; STA. 224+86 TO STA. 230+18
STAGE 3: NB I-57; STA. 224+86 TO STA. 230+18

- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING-LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

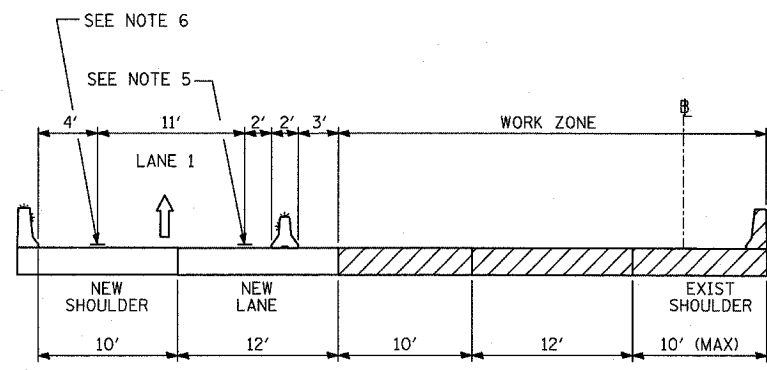
* ALL MOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
GENERAL NOTES
CONSTRUCTION STAGING NOTES
MAINTENANCE OF TRAFFIC DETAILS
SHEET 7 OF 13

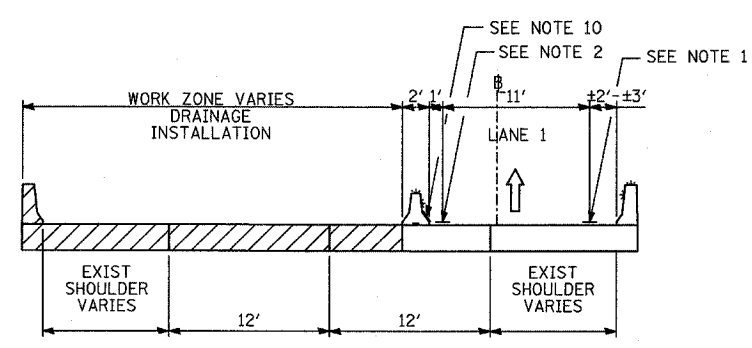
SCALE: NO SCALE
DATE: MARCH 7, 2006

DRAWN BY: RTM
CHECKED BY: TGB



FF-FF

STAGE 2: NB I-57; STA. 232+89 TO STA. 235+77
 STAGE 2: NB I-57; STA. 240+93 TO STA. 248+30
 STAGE 3: NB I-57; STA. 232+89 TO STA. 235+77
 STAGE 3: NB I-57; STA. 240+93 TO STA. 248+30



II-II

STAGE 2: WB CONNECTOR; STA. 307+43 TO STA. 328+00

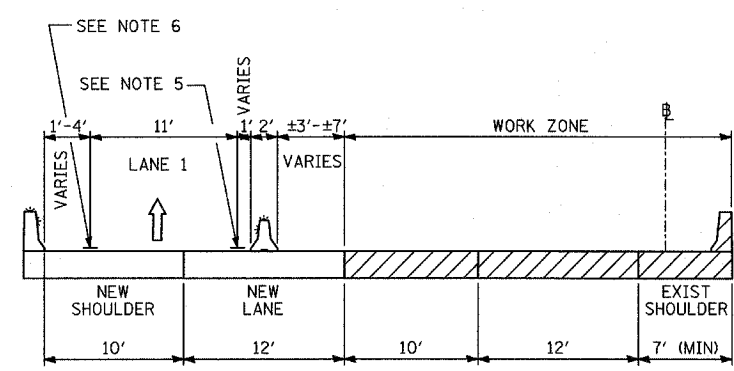
- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING-LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)

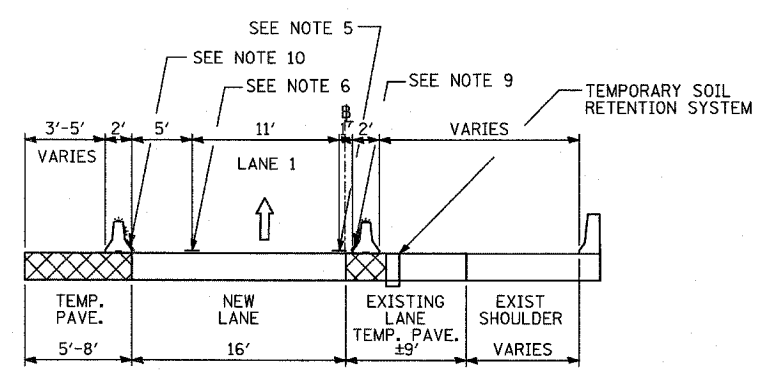
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

• ALL NOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC



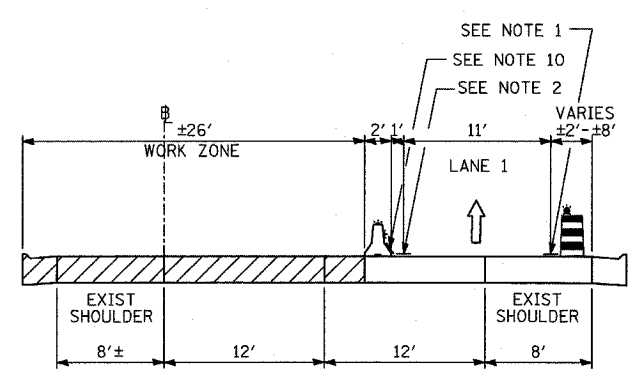
GG-GG

STAGE 2: NB I-57; STA. 236+19 TO STA. 240+93
 STAGE 3: NB I-57; STA. 236+19 TO STA. 240+93



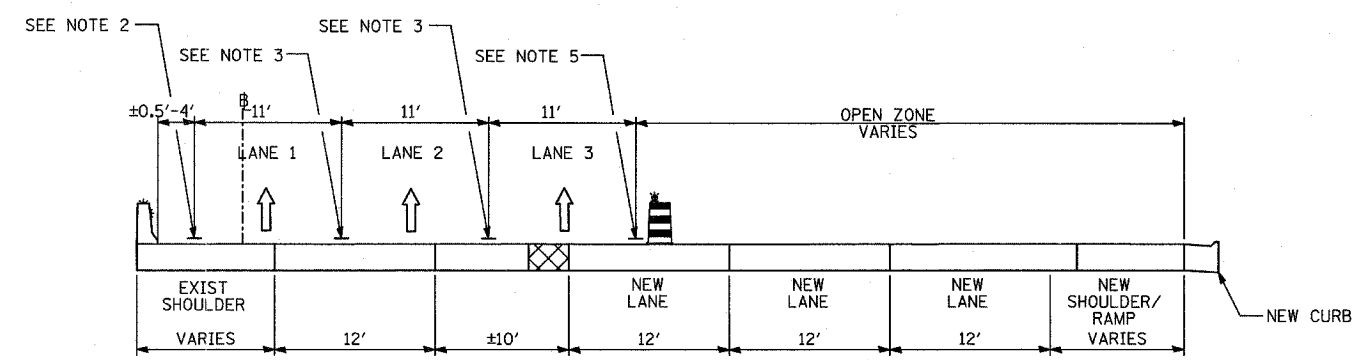
JJ-JJ

STAGE 2: WB CONNECTOR; STA. 330+65 TO STA. 340+29



HH-HH

STAGE 2: EB CONNECTOR; STA. 408+41 TO STA. 415+06
 STAGE 3: EB CONNECTOR; STA. 408+41 TO STA. 415+06



KK-KK

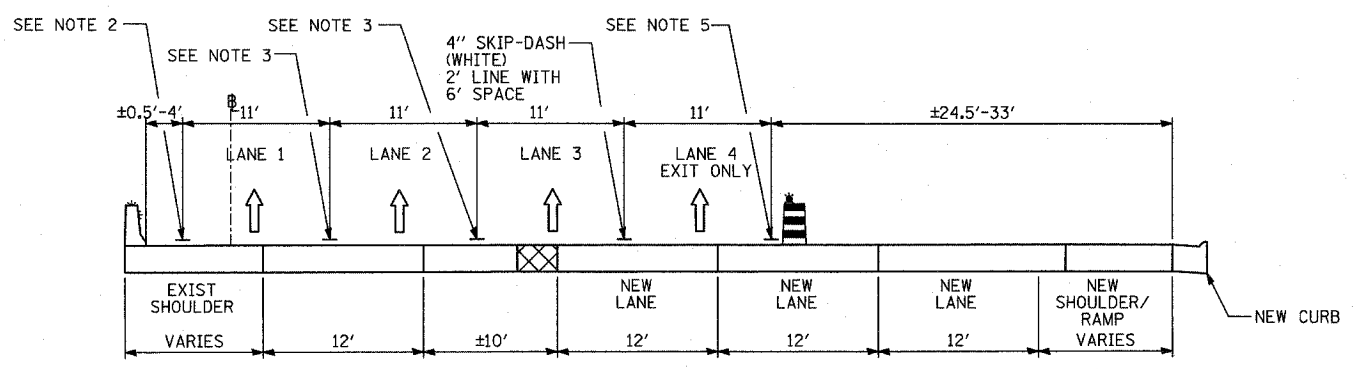
STAGE 2: NB I-94 (DAN RYAN); STA. 2211+92 TO STA. 2223+48

REVISIONS	
NAME	DATE

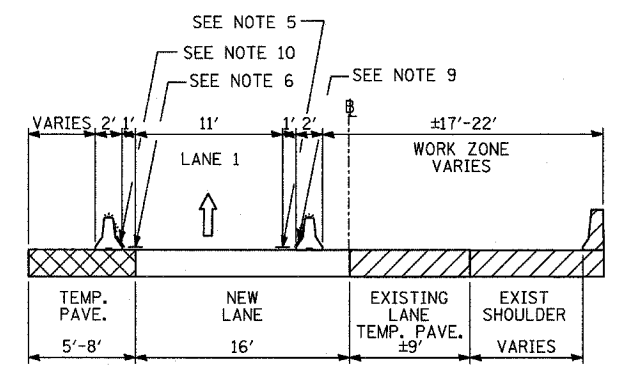
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 GENERAL NOTES
 CONSTRUCTION STAGING NOTES
 MAINTENANCE OF TRAFFIC DETAILS
 SHEET 8 OF 13

SCALE: NO SCALE
 DATE: MARCH 7, 2006

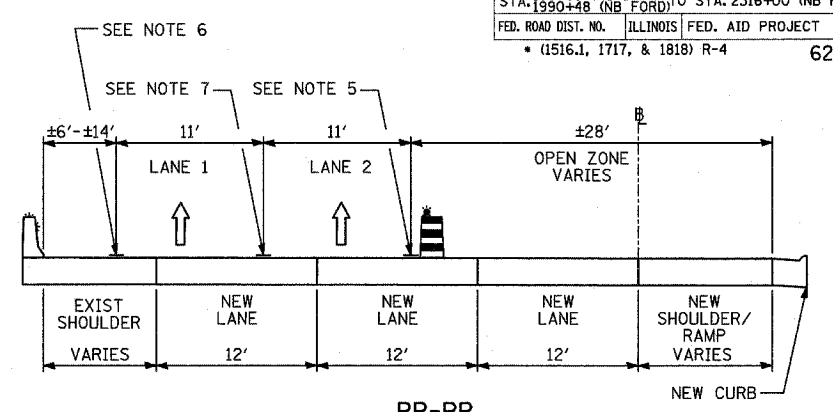
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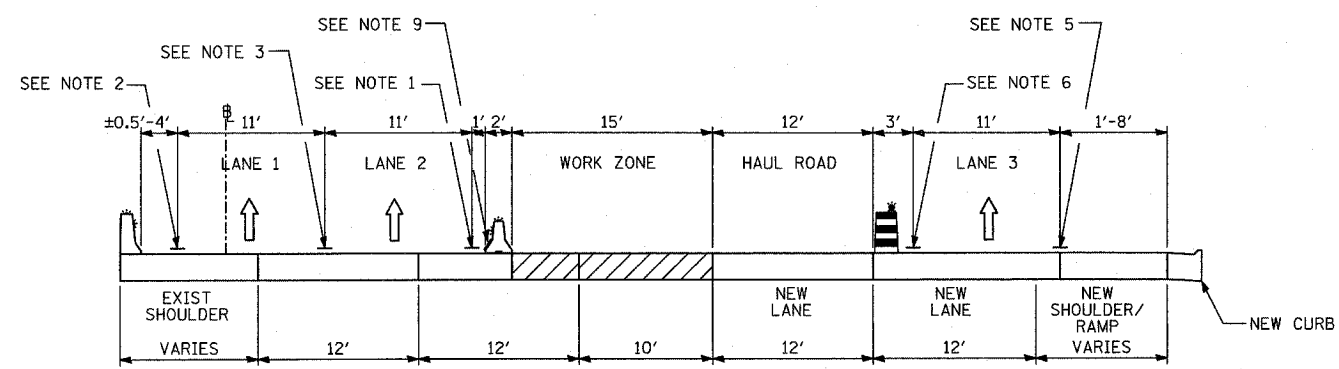
LL-LL
 STAGE 2: NB I-94 (DAN RYAN); STA. 2229+78 TO STA. 2236+13



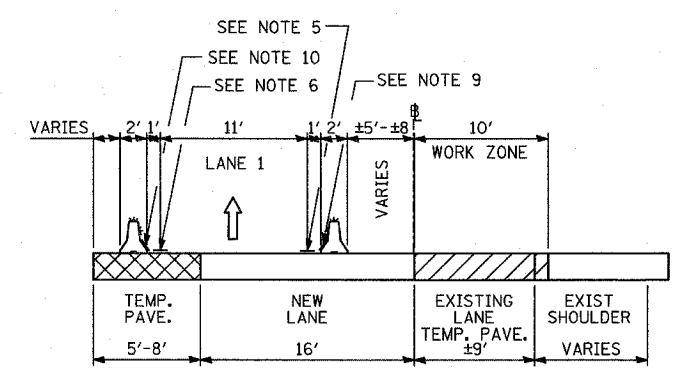
OO-OO
 STAGE 3: WB CONNECTOR; STA. 330+65 TO STA. 337+38



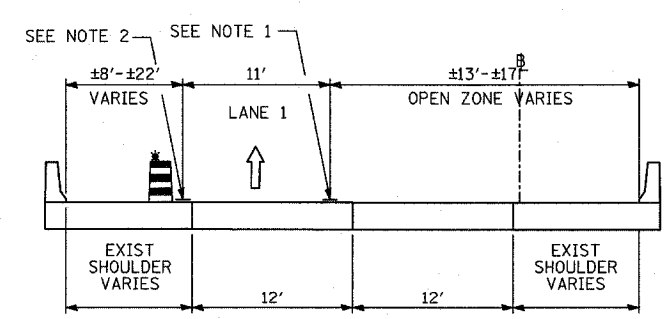
RR-RR
 STAGE 3A: NB I-57; STA. 191+42 TO STA. 205+07



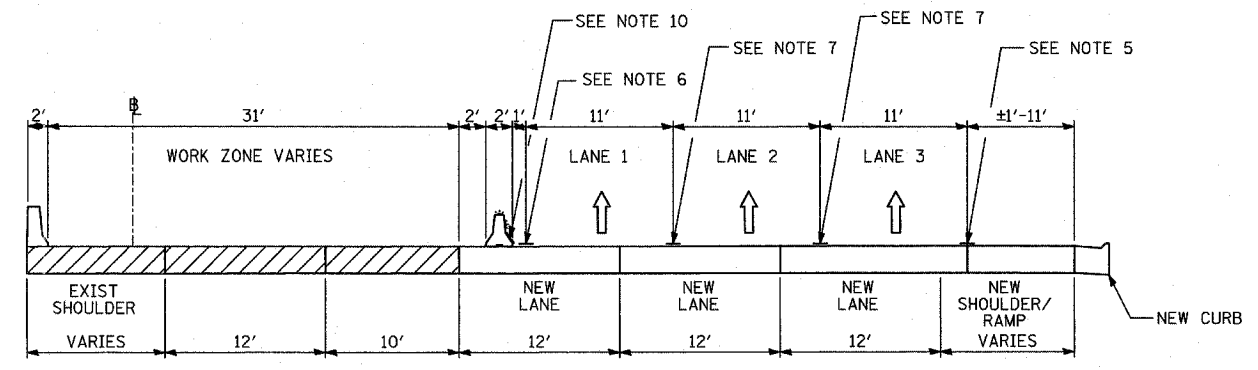
MM-MM
 STAGE 2: NB I-94 (DAN RYAN); STA. 2252+02 TO STA. 2316+00



PP-PP
 STAGE 3: WB CONNECTOR; STA. 337+38 TO STA. 340+29



NN-NN
 STAGE 3: WB CONNECTOR; STA. 324+13 TO STA. 329+05



QQ-QQ
 STAGE 3: NB I-94 (DAN RYAN); STA. 2205+51 TO STA. 2316+00
 STAGE 3A: NB I-94 (DAN RYAN); STA. 2203+00 TO STA. 2316+00

- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING-LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

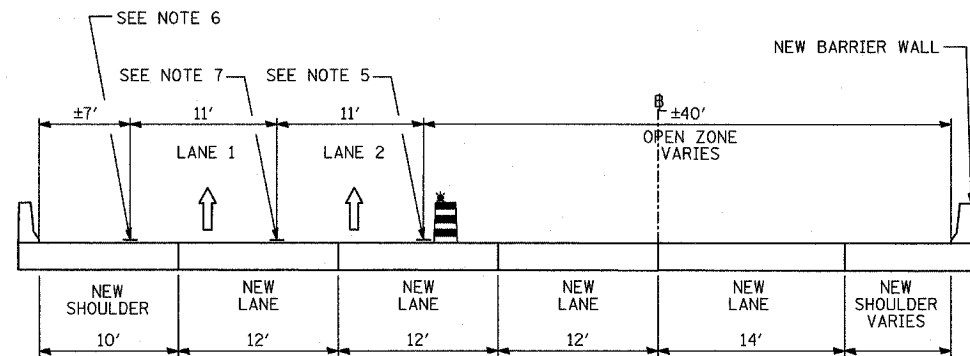
* ALL NOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 GENERAL NOTES
 CONSTRUCTION STAGING NOTES
 MAINTENANCE OF TRAFFIC DETAILS
 SHEET 9 OF 13

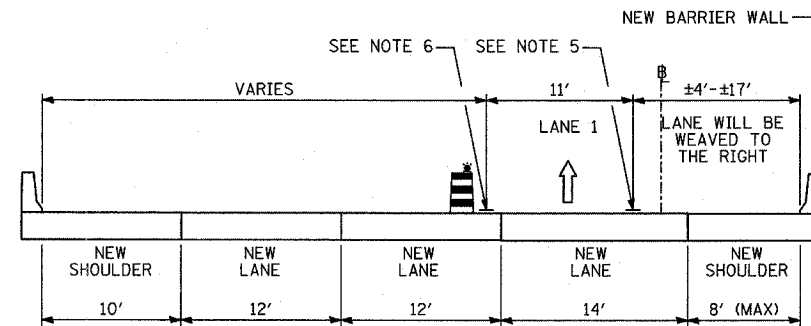
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 DATE: MARCH 7, 2006 CHECKED BY: TGB

PA 02/23/06 3:33:53 PM



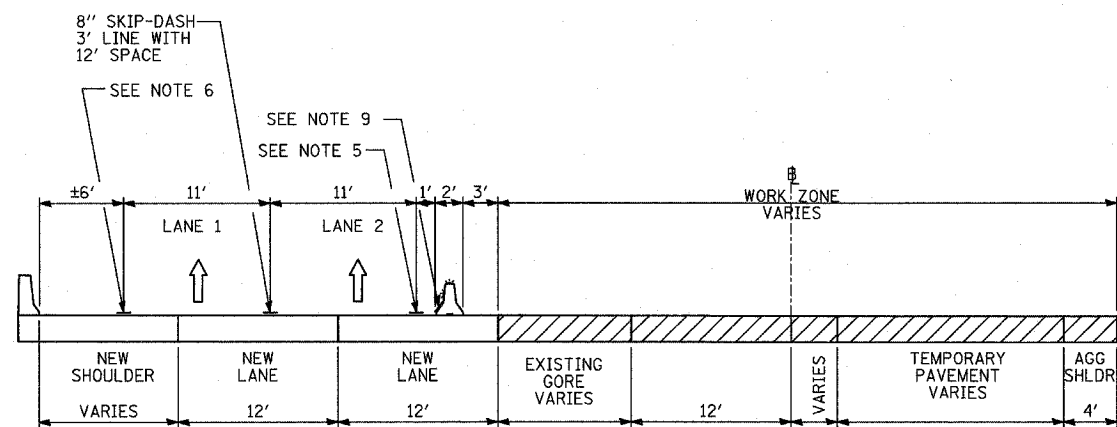
SS-SS

STAGE 3A: NB I-57; STA. 209+07 TO STA. 218+71



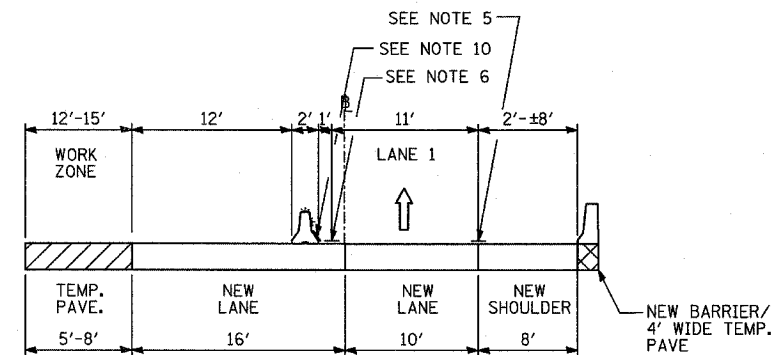
VV-VV

STAGE 3A: NB I-57; STA. 233+00 TO STA. 235+68
STAGE 3A: NB I-57; STA. 240+96 TO STA. 247+28



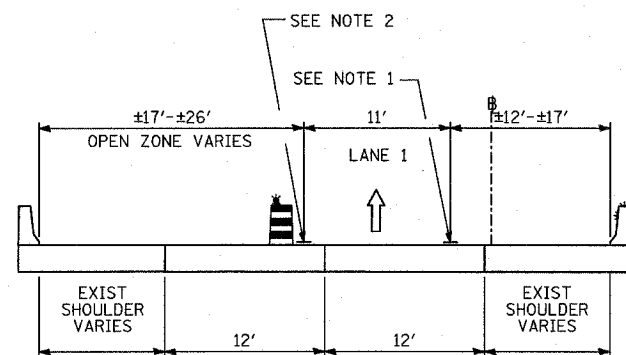
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STAGE 3A: NB I-57; STA. 219+18 TO STA. 222+68



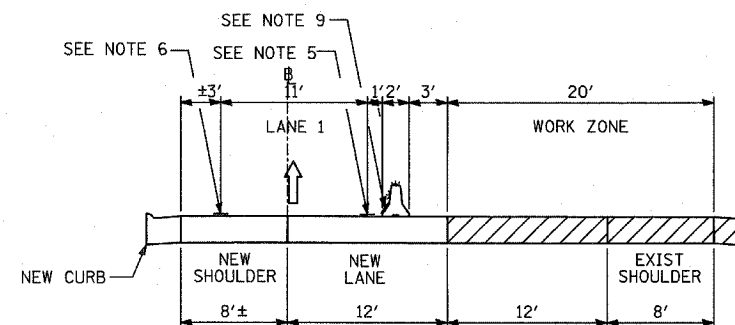
WW-WW

STAGE 3A: WB CONNECTOR; STA. 330+65 TO STA. 340+29



UU-UU

STAGE 3A: WB CONNECTOR; STA. 324+70 TO STA. 329+67



XX-XX

STAGE 3A: EB CONNECTOR; STA. 408+41 TO STA. 415+06

- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

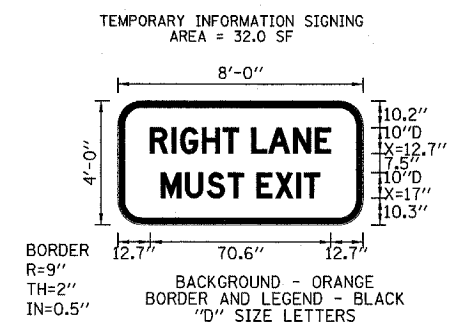
- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

* ALL NOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC

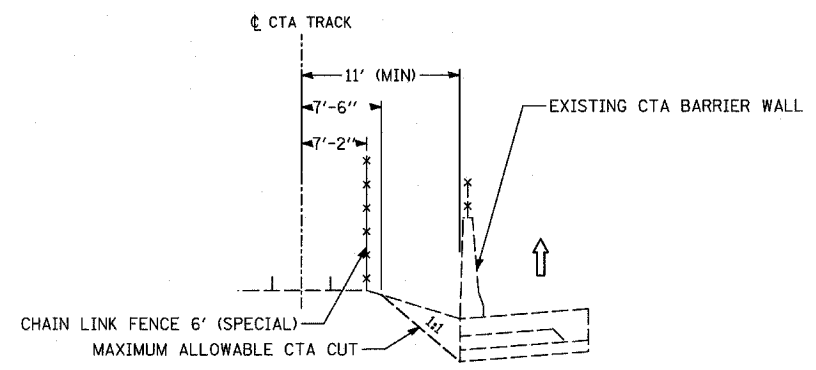
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
GENERAL NOTES
CONSTRUCTION STAGING NOTES
MAINTENANCE OF TRAFFIC DETAILS
SHEET 10 OF 13

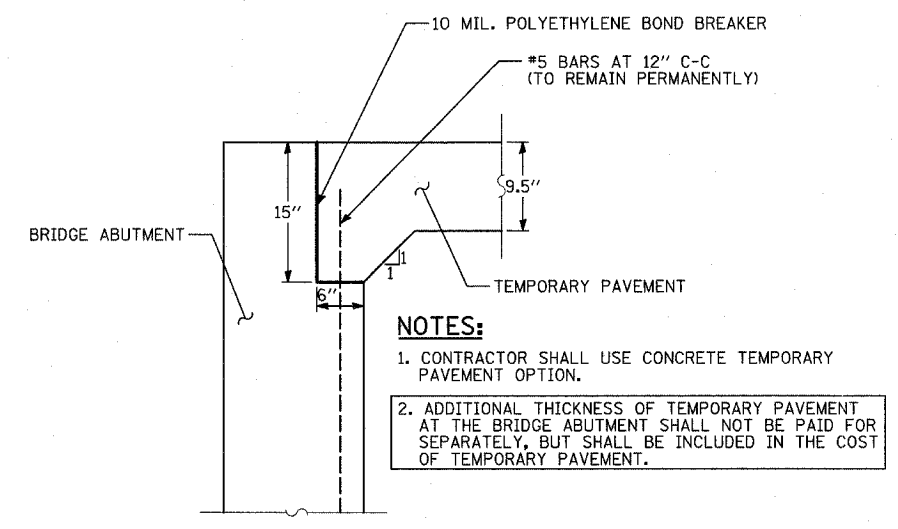
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DATE: MARCH 7, 2006 CHECKED BY: TGB



"RIGHT LANE MUST EXIT" SIGN DETAIL



LOCATION OF CHAIN LINK FENCE 6' (SPECIAL)



TEMPORARY PAVEMENT DETAIL AT NB I-57 BRIDGE - STAGE 1A

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

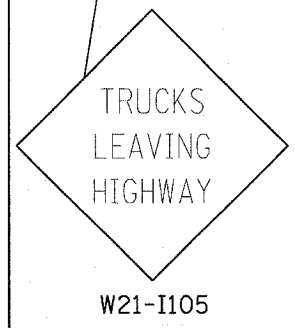
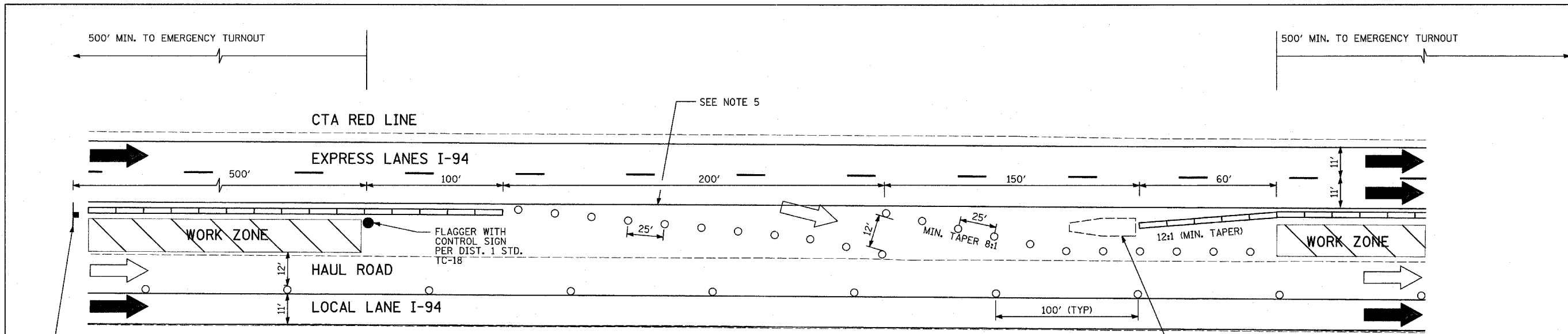
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
GENERAL NOTES
CONSTRUCTION STAGING NOTES
MAINTENANCE OF TRAFFIC DETAILS
SHEET 12 OF 13

SCALE: NO SCALE
DATE: MARCH 7, 2006

DRAWN BY: RTM
CHECKED BY: TGB

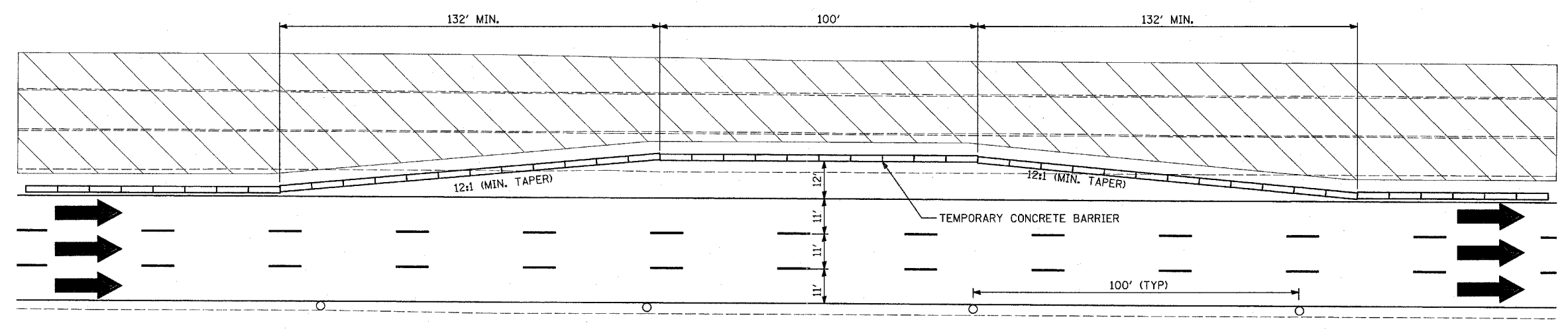
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 62304



SUGGESTED ACCESS FROM EXPRESS LANES

- LEGEND:**
- TEMPORARY CONCRETE BARRIER
 - TRAFFIC LANE
 - CONSTRUCTION TRAFFIC
 - WORK ZONE
 - DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
 - ARROW BOARD



MAINTENANCE OF TRAFFIC EMERGENCY TURNOUT DETAIL
 EMERGENCY TURNOUTS WILL BE LOCATED APPROXIMATELY EVERY HALF MILE

NOTES:

1. THERE CAN BE NO MORE THAN TWO (2) WORK ZONE ACCESS/EGRESS COMBINATIONS AND THE CONTRACTOR MUST MAINTAIN AT LEAST ONE (1) EXPRESS LANE ACCESS DURING STAGE 2 CONSTRUCTION.
2. THE CONTRACTOR SHALL NOT ENTER OR EXIT THE HAUL ROAD WITHIN THE 100' BARREL-SPACING AREA. WORK ZONE ACCESS AND EGRESS WILL ONLY BE PERMITTED AT THE DESIGNATED LOCATIONS.
3. FOR EXPRESS LANE WORK ZONE EGRESS, TAPER LENGTHS SHALL FOLLOW DISTRICT 1 STANDARD TC-18.
4. THE CONTRACTOR SHALL CLOSE OPENINGS WITH BARRELS WHEN NOT BEING USED FOR ACCESS.
5. TEMPORARY PAVEMENT MARKINGS SHALL BE CARRIED THRU THE OPENING.

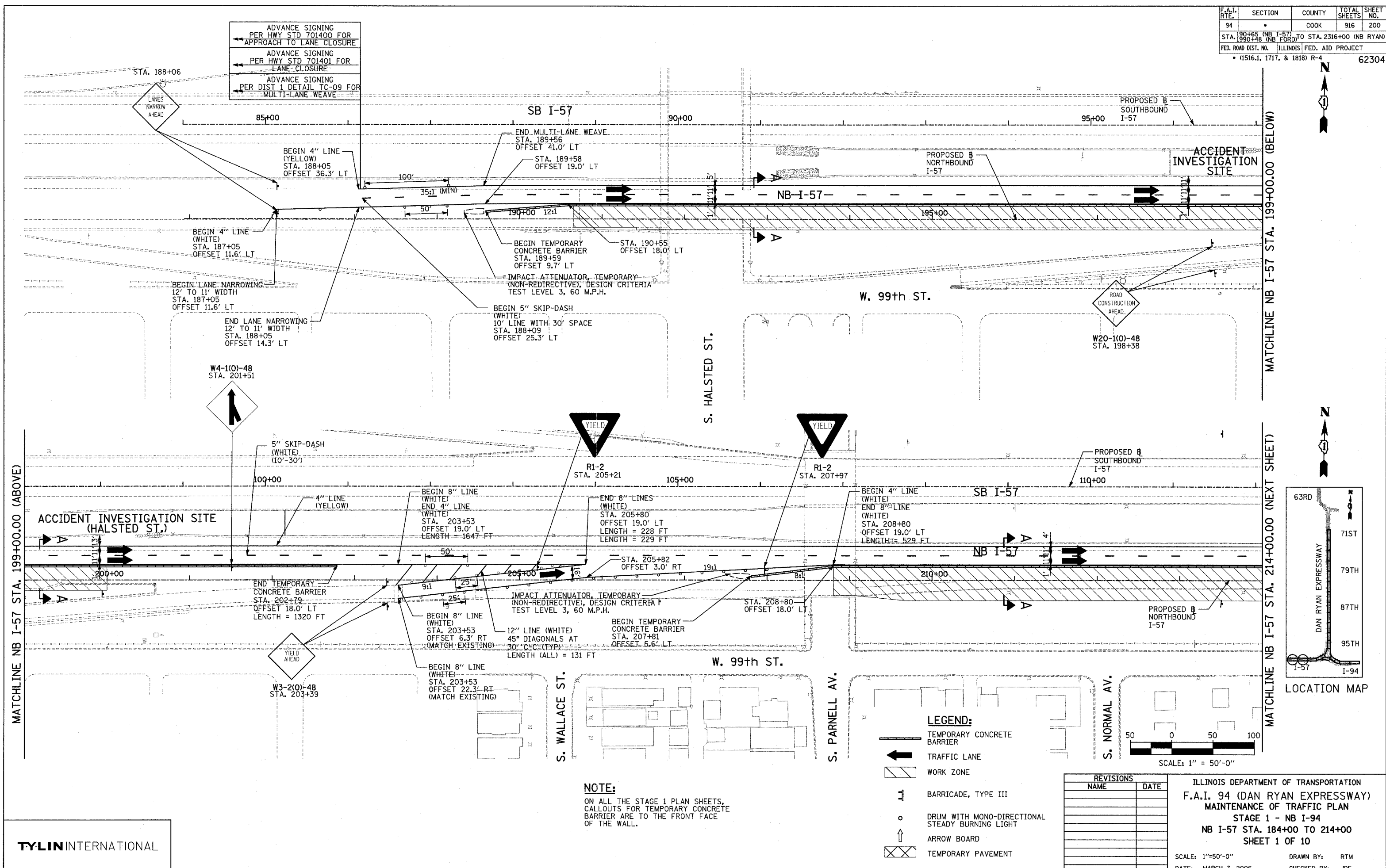
TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 GENERAL NOTES
 CONSTRUCTION STAGING NOTES
 MAINTENANCE OF TRAFFIC DETAILS
 SHEET 13 OF 13

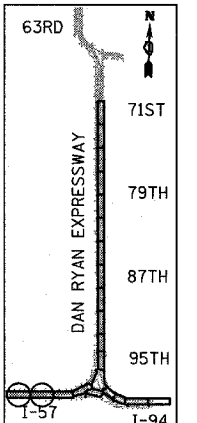
SCALE: NO SCALE DRAWN BY: RTM
 DATE: MARCH 7, 2006 CHECKED BY: TGB

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	200
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				



MATCHLINE NB I-57 STA. 199+00.00 (BELOW)

MATCHLINE NB I-57 STA. 214+00.00 (NEXT SHEET)



LOCATION MAP

LEGEND:

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MAINTENANCE OF TRAFFIC PLAN
 STAGE 1 - NB I-94
 NB I-57 STA. 184+00 TO 214+00
 SHEET 1 OF 10

SCALE: 1" = 50'-0"
 DATE: MARCH 7, 2006
 DRAWN BY: RTM
 CHECKED BY: JDF

NOTE:
 ON ALL THE STAGE 1 PLAN SHEETS,
 CALLOUTS FOR TEMPORARY CONCRETE
 BARRIER ARE TO THE FRONT FACE
 OF THE WALL.