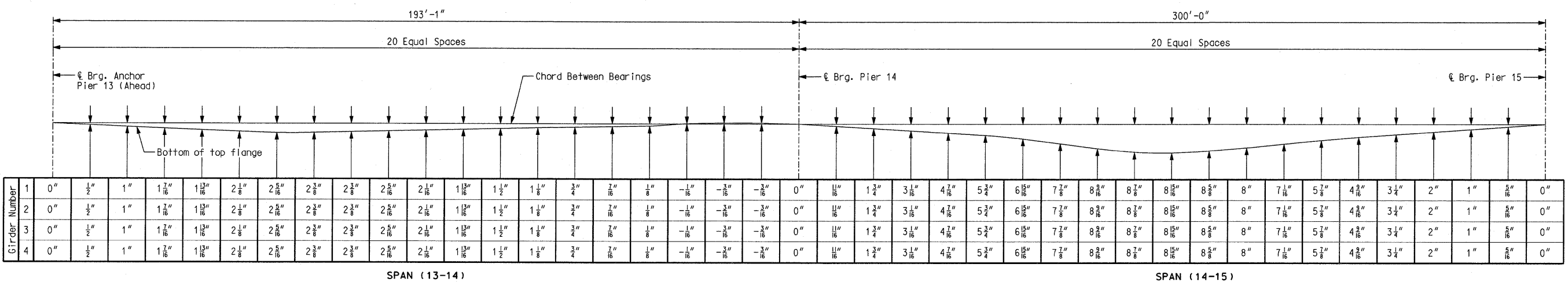
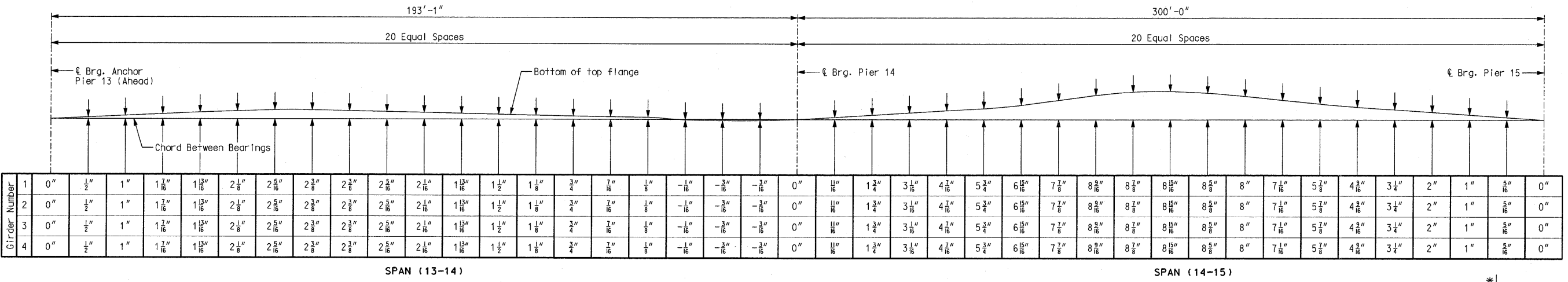


CONTRACT NO. 76D61	
F.A. ROUTE	SECTION
999	82-1B-2
FED. AID PROJECT	ILLINOIS
COUNTY	ST. CLAIR
USER NAME = jcolliff	
PLOT SCALE = #SCALE#	
PLOT DATE = 4/14/2010	
DESIGNED - HNTB	
CHECKED - CMT	
DRAWN - CMT / HNTB	
REVISED -	
REVISED -	
REVISED -	



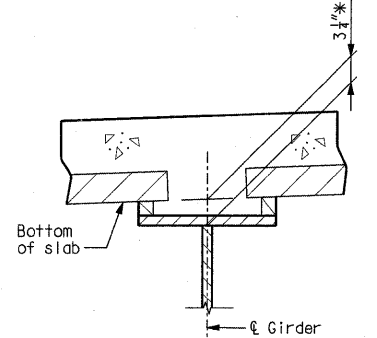
DEAD LOAD DEFLECTIONS

Notes:
 23% of dead load deflection in span (13-14) and 30% of dead load deflection in span (14-15) is due to the weight of structural steel.
 Dead load deflection includes weight of structural steel, concrete slab, and barrier curbs.
 Negative values indicate upward deflection.



CAMBER DIAGRAM

Notes:
 Camber includes allowance for dead load deflection due to concrete slab, curbs, and structural steel.
 Positive values are above the chord between bents and negative values are below the chord between bents.



*Dimensions may vary if the girder camber after erection differs from plan camber by more or less than the % of Dead Load Deflection due to weight of structural steel. No payment will be made for any adjustment in forming or additional concrete required for variation in haunching.

THEORETICAL SLAB HAUNCH

DEAD LOAD DEFLECTIONS AND CAMBER DIAGRAM WB - UNIT 1 (1 OF 2)

Detailed JUL 2009
 Checked JUL 2009

Note: This drawing is not to scale. Follow dimensions.

Sheet No. 66 of 152

ILLINOIS APPROACH STRUCTURE FOR NEW I-70 MISSISSIPPI RIVER BRIDGE
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

HNTB
 715 KIRK DRIVE
 KANSAS CITY, MO 64105
 TELEPHONE (816) 472-1201
 CERTIFICATE OF AUTHORITY NO. 001270

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 2750 WEST WASHINGTON STREET
 SPRINGFIELD, IL 62702
 TELEPHONE (217) 787-8050
 ENGINEERING CORPORATION - 000631