



Illinois Department of Transportation

Office of Intermodal Project Implementation / Division of Aeronautics
1 Langhorne Bond Drive / Springfield, Illinois 62707-8415

April 25, 2022

SUBJECT: St. Louis Downtown Airport
Cahokia, Illinois
St. Clair County
Illinois Project Number: CPS-4976
Contract No. SD061
Item No. 02A, April 29, 2022 Letting
Addendum A

NOTICE TO PROSPECTIVE BIDDERS

Attached is an addendum to the plans or proposal. This addendum involves revised and/or added material.

Reason for Addendum:

Clarifications to Plans and Special Provisions

To All Plan Holders:

1) General

In the event that the project construction is underway and a “winter shutdown” and suspension of contract time is necessary as agreed upon by all parties, there may be portions of the site that require attention in order to leave the site in a certain condition over the shutdown period. Due to the unknowns of that potential scenario, such efforts that are deemed necessary prior to a shutdown period, for which no basis of payment has been provided previously, shall be considered Extra Work in accordance with General Provision 40-04.

2) The following Plan Sheets are **REVISED**, attached, dated April 22, 2022:

Plan Sheet No. 9, Site Stockpile & Channel Overview: added site access notes
Plan Sheet No. 22, Jointing Plan: revised appearance of linework to match legend
Plan Sheet No. 23, Jointing Details: revised reinforcement note
Plan Sheet No. 25, Box Culvert Details – Sheet 1: clarified bedding details

3) Plan Sheet 4, Proposed Safety and Phasing Plan:

Revision: The tables that are titled “Control Points” shall be revised to be titled “Critical Points”. These are not meant to reflect survey control points; rather they are safety points for the FAA’s use.

4) Special Provision Item 208 Aggregate Base Course:

Revision: Materials: IDOT gradation RR-1 will be considered an acceptable option in lieu of IDOT gradation CA-1 for this project.

Clarification: Due to the magnetically-sensitive nature of the proposed compass calibration pad (CCP), the Contractor shall avoid the use of potentially ferrous materials within the non-ferrous pavement limits, which may include avoiding the use of recycled concrete as aggregate base in order to avoid potential steel remnants within the materials.

5) Special Provision Item 209 Aggregate Base Course:
Clarification: Due to the magnetically-sensitive nature of the proposed compass calibration pad (CCP), the Contractor shall avoid the use of potentially ferrous materials within the non-ferrous pavement limits, which may include avoiding the use of recycled concrete as aggregate base in order to avoid potential steel remnants within the materials.

6) Special Provision Item 501 Portland Cement Concrete Pavement:
Revision: Section 501-3.1 Equipment: **REVISE** the third sentence to read, "For Method II projects between 5,000 cubic yards and 15,000 cubic yards, a central mix concrete plant shall be required for manufacturing the concrete."

Clarification: Although there are two separate pay items for concrete pavement (AR501512 and AR800551) the same concrete mix design is intended to be used, and the total combined volume of concrete shall be considered with respect to the above Special Provision Item 501 language.

7) Meeting Minutes and Sign-In Sheet:
The minutes and sign-in sheet to the Pre-Bid Meeting, held on April 12, 2022, are attached.

Prime contractors must utilize the enclosed material when preparing their bid and must include any changes to the Schedule of Prices in their bid.

Questions on this addendum may be directed to Barry Stolz, P.E. of Hanson Professional Services at 314.942.5288.



**ST. LOUIS
DOWNTOWN AIRPORT**

ST. LOUIS DOWNTOWN AIRPORT
BI-STATE DEVELOPMENT AGENCY
6100 Archview Drive
Cahokia, Illinois 62206



DATE SIGNED: 3/22/22 LICENSE EXPIRES: 11/30/23

**CONSTRUCT RUNUP
RAMP AND TAXIWAY
ACCESS FROM THE
AIRFIELD, INCLUDING
JET BLAST/NOISE
MITIGATION BARRIER**

IDA No: CPS-4976

Contract No. SD061

NO.	DATE	DESCRIPTION
△	4/22/22	ADD NOTES
		JRH CWS BSS
		DES DWN REV

ISSUE: MARCH 4, 2022

PROJECT NO: 20A000105D

CAD FILE: C-105-DRN.DWG

DESIGN BY: MJD 03/15/2021

DRAWN BY: MJD 04/15/2021

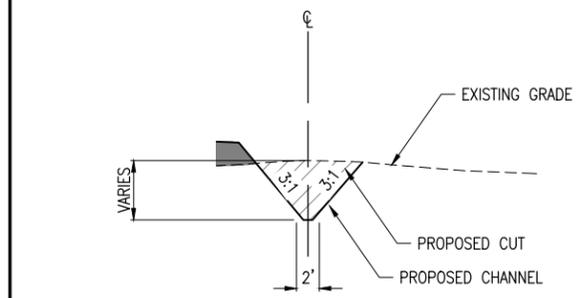
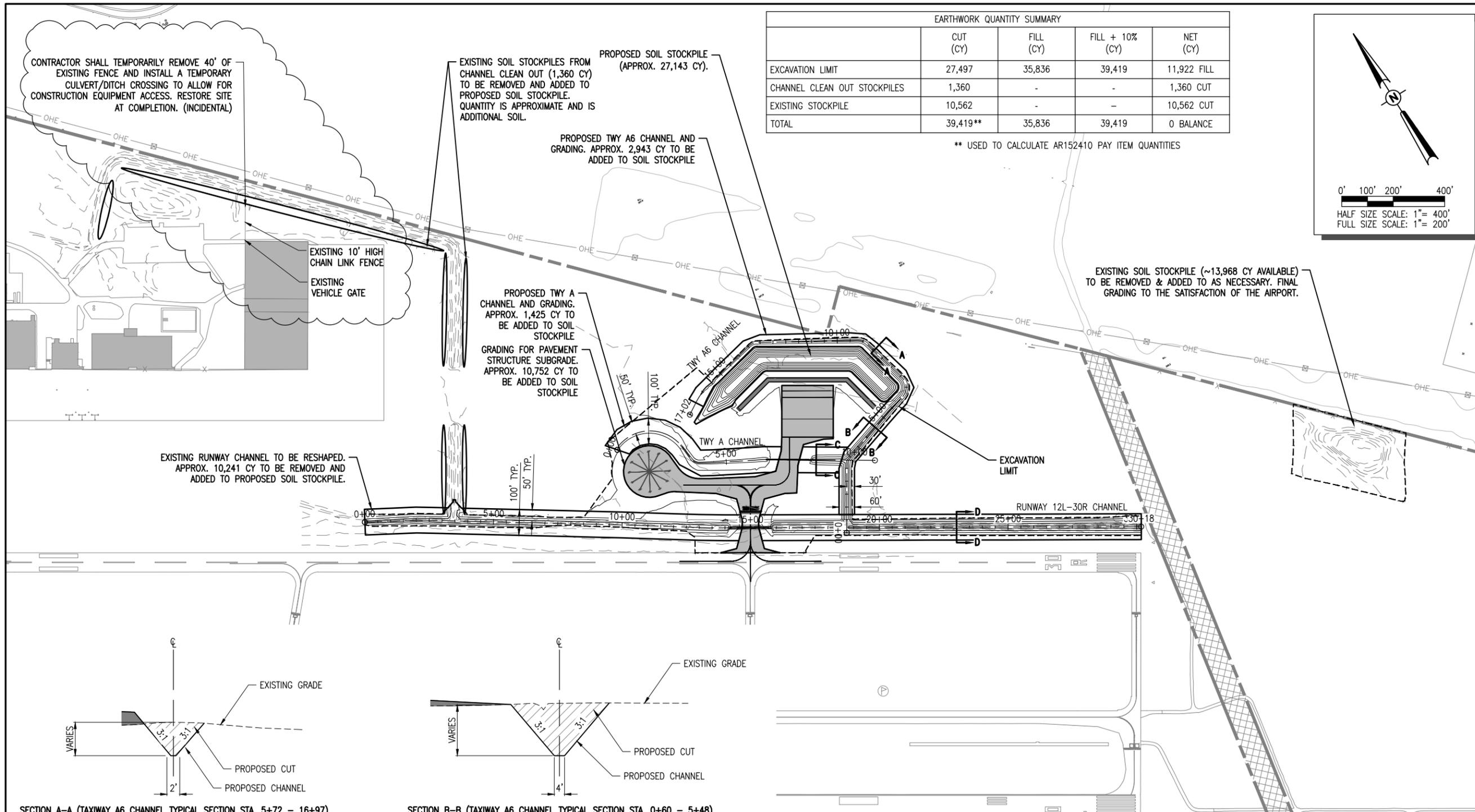
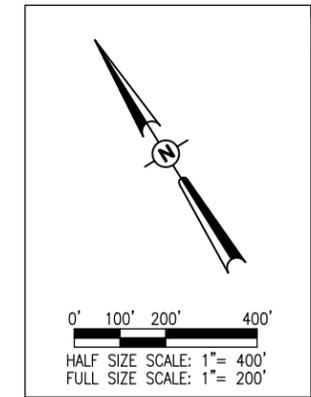
REVIEWED BY: BSS 03/03/2022

SHEET TITLE

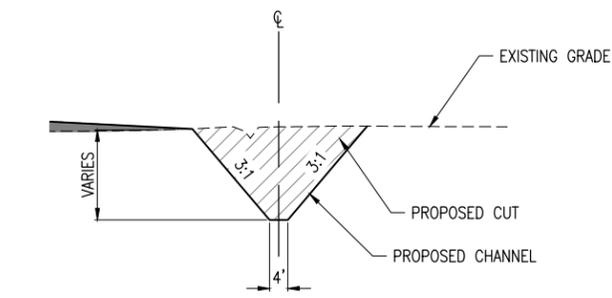
**SITE STOCKPILE &
CHANNEL OVERVIEW**

EARTHWORK QUANTITY SUMMARY				
	CUT (CY)	FILL (CY)	FILL + 10% (CY)	NET (CY)
EXCAVATION LIMIT	27,497	35,836	39,419	11,922 FILL
CHANNEL CLEAN OUT STOCKPILES	1,360	-	-	1,360 CUT
EXISTING STOCKPILE	10,562	-	-	10,562 CUT
TOTAL	39,419**	35,836	39,419	0 BALANCE

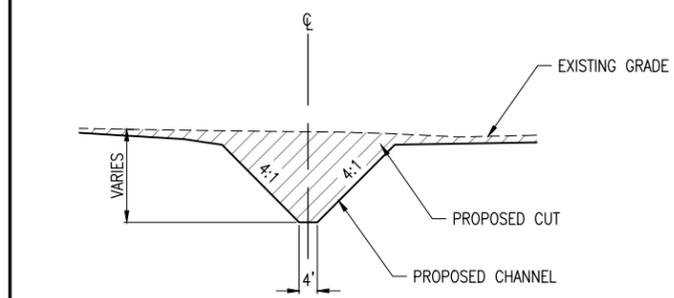
** USED TO CALCULATE AR152410 PAY ITEM QUANTITIES



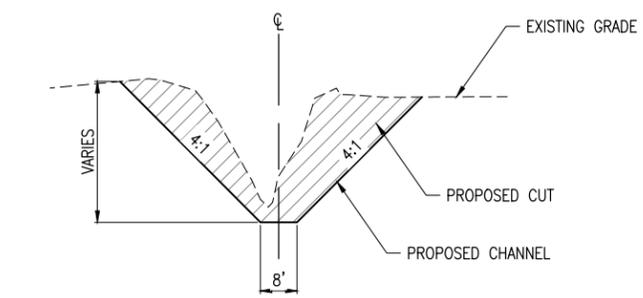
SECTION A-A (TAXIWAY A6 CHANNEL TYPICAL SECTION STA. 5+72 - 16+97)



SECTION B-B (TAXIWAY A6 CHANNEL TYPICAL SECTION STA. 0+60 - 5+48)



SECTION C-C (TAXIWAY A CHANNEL TYPICAL SECTION)



SECTION D-D (RUNWAY 12L-30R CHANNEL TYPICAL SECTION)

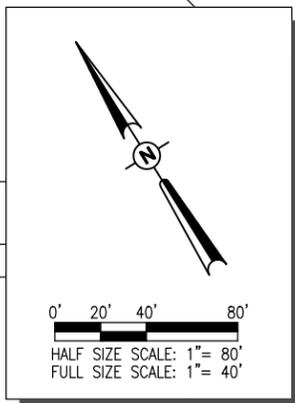
NOTES:

- EARTHWORK QUANTITIES (CUT/FILL VOLUMES) FOR THE "EXISTING GROUND VS. PROP. DESIGN" SHOWN ABOVE WERE CALCULATED UTILIZING AUTODESK CIVIL3D 2021 SOFTWARE. THE CALCULATION METHOD WAS BY A COMPARISON OF SURFACE MODELS CREATED WITH EXISTING SURVEY DATA AND PROPOSED DESIGN GRADES. THE VOLUMES WERE CALCULATED IN TWO PARTS: THE CUT/FILL VOLUME REQUIRED TO CORE OUT AND FILL FOR THE PROPOSED PAVEMENT SECTION AS COMPARED TO THE EXISTING SUBGRADE DATUM, AND THE CUT/FILL VOLUMES REQUIRED FOR PROPOSED GRADING WORK OUTSIDE OF THE PROPOSED PAVEMENT LIMITS AS COMPARED TO THE EXISTING GROUND SURFACE. THE NUMBERS IN THE SUMMARY TABLES ABOVE REPRESENT A TOTAL OF THESE TWO PARTS ADDED TOGETHER FOR CLARITY.
- IF THE CONTRACTOR DOES NOT AGREE TO THE QUANTITIES DERIVED IN THIS METHOD, THE CONTRACTOR MAY ELECT TO SURVEY THE EXISTING GRADES PRIOR TO BEGINNING EARTHWORK OPERATIONS AS PART OF THE PROJECT FOR THE ENGINEER TO REVIEW FOR A POTENTIAL ADJUSTMENT TO THE PAY ITEM QUANTITY. ANY COSTS ASSOCIATED WITH THE CONTRACTOR-PROVIDED SURVEY SHALL BE INCLUDED IN THE ORIGINAL BID AMOUNT, AND NO ADDITIONAL PAYMENT SHALL BE MADE. FOLLOWING THE CONTRACTOR'S VERIFICATION OF THE QUANTITIES, IF A DISAGREEMENT STILL EXISTS, THE MEASUREMENT OF THE EARTHWORK FOR PAYMENT SHALL BE MADE BY THE RESIDENT ENGINEER, PER THE 152 SPECIFICATION, WHO SHALL TAKE CROSS-SECTIONAL ELEVATIONS AND MEASUREMENTS OF THE EXISTING GROUND SURFACE AND THE FINAL GRADED SURFACE FOR COMPARISON.
- FOLLOWING THE PROJECT AWARD, THE ENGINEER CAN PROVIDE THE RELEVANT AUTOCAD AND CIVIL 3D SURFACE MODEL FILES TO THE AWARDED CONTRACTOR UPON REQUEST TO ASSIST WITH CONSTRUCTION LAYOUT.

FOR BID

JOINTING NOTES

1. DUE TO THE MAGNETICALLY-SENSITIVE NATURE OF THE PROPOSED COMPASS CALIBRATION PAD (CCP), THE USE OF STEEL/FERROUS MATERIALS WITHIN A DESIGNATED RADIUS OF THE CENTER OF THE CCP IS PROHIBITED. THE DIVIDING LINE FOR THIS PROJECT IS STATION 136+93.56 ON TAXIWAY "A". AS NOTED ON THE JOINTING PLAN VIEW, WITHIN THE NON-FERROUS PCC PAVEMENT AREAS, ALL DOWEL BARS AND REINFORCING MATERIALS SHALL BE FIBERGLASS AND SHALL CONFORM TO THE PROJECT SPECIAL PROVISIONS ITEM AR800551. ALTERNATE NON-FERROUS MATERIALS (ALUMINUM, BRASS, BRONZE) MAY BE PROPOSED FOR APPROVAL PRIOR TO THE BID OPENING/LETTING DATE AND ANY APPROVED MATERIALS WILL BE ISSUED BY ADDENDUM. THE PROPOSED PCC PAVEMENT MIX DESIGN SHALL BE CONSISTENT THROUGHOUT THE ENTIRE PROJECT.
2. ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED. ALL JOINT EDGES SHALL BE SAWCUT TO PRODUCE THE 1/4" CHAMFER.
3. ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE SURFACE OF THE PAVEMENT AND TO THE CENTERLINES OF THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN PRIOR TO INSTALLATION.
4. DOWEL BARS FOR 12 IN. THICK PAVEMENT SHALL BE 1 IN. DIAMETER, 18 IN. LONG AT 12 IN. SPACING.
5. DOWELS IN TRANSVERSE CONTRACTION AND LONGITUDINAL CONSTRUCTION JOINTS SHALL BE COATED WITH A RUSTPROOFING COMPOUND AND HALF THE LENGTH GREASED WITH A HEAVY GREASE.
6. ALLOWABLE TOLERANCES FOR GROOVE DEPTH WILL BE ±1/8" FOR CONSTRUCTION JOINTS AND ±1/4" FOR CONTRACTION JOINTS.
7. THE CONTRACTOR IS REQUIRED TO DRILL AND EPOXY THE PROPOSED DOWELS IN ACCORDANCE WITH THE DETAILS AND SPECIFICATIONS. THE EPOXY MATERIAL MUST BE APPROVED BY THE DIVISION OF AERONAUTICS PRIOR TO USE.
8. THE COST OF ALL DOWEL BARS, BASKET ASSEMBLIES, SAWING AND SEALING SHALL BE INCLUDED IN THE COST OF THE PCC PAVEMENT.
9. WHEN CONSTRUCTING "FILL-IN" PAVEMENT LANES THE CONTRACTOR SHALL USE BELTING OR OTHER PROTECTIVE MATERIAL FOR THE PAVING MACHINE TO TRAVEL ON AND WILL PROTECT THE TRANSVERSE JOINTS.
10. JOINT SEALANT SHALL BE AS SPECIFIED IN THE STANDARD SPECIFICATIONS, ITEM 605.
11. CURING COMPOUND SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS, ITEM 501, SECTION 501-2.9, AND SHALL BE APPROVED PRIOR TO THE PAVING OPERATION BY THE RESIDENT ENGINEER/TECHNICIAN.
12. ALL NON-ALIGNED EDGES WILL BE SAWED FULL DEPTH.
13. AWARDED CONTRACTOR MAY PROPOSE AN ALTERNATIVE PAVING/JOINTING PLAN FOR REVIEW AND APPROVAL FOLLOWING AWARD.
14. CONTRACTOR SHALL VERIFY EXISTING ELEVATIONS AT "MATCH" LOCATIONS PRIOR TO CONSTRUCTION. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER/TECHNICIAN IMMEDIATELY TO DETERMINE IF ADJUSTMENTS ARE NECESSARY TO PROPOSED GRADES.



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 Hanson Professional Services Inc.
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**ST. LOUIS
 DOWNTOWN AIRPORT**
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 BI-STATE DEVELOPMENT AGENCY
 6100 Archview Drive
 Cahokia, Illinois 62206



DATE SIGNED: 3/4/22 LICENSE EXPIRES: 11/30/23

**CONSTRUCT RUNUP
 RAMP AND TAXIWAY
 ACCESS FROM THE
 AIRFIELD, INCLUDING
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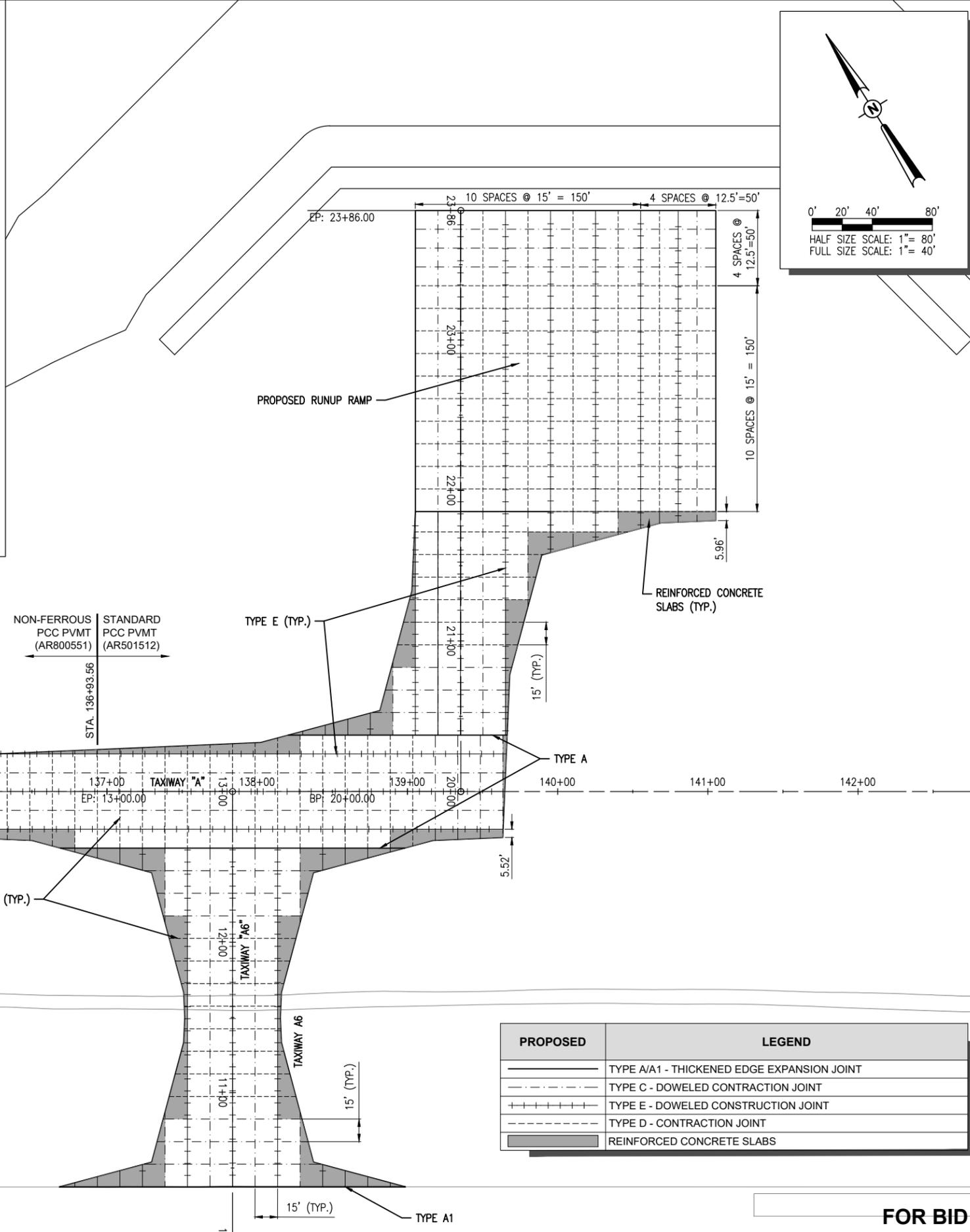
IDA No: CPS-4976
 Contract No. SD061

NO.	DATE	DESCRIPTION
△	4/22/22	REVISE LINETYPES
		JRH CWS BSS
		DES DWN REV

ISSUE: MARCH 4, 2022
 PROJECT NO: 20A000105D
 CAD FILE: C-161-JNT.DWG
 DESIGN BY: HLE 03/25/2021
 DRAWN BY: HLE 03/25/2021
 REVIEWED BY: BSS 03/03/2022

SHEET TITLE

JOINTING PLAN



PROPOSED	LEGEND
	TYPE A/A1 - THICKENED EDGE EXPANSION JOINT
	TYPE C - DOWELED CONTRACTION JOINT
	TYPE E - DOWELED CONSTRUCTION JOINT
	TYPE D - CONTRACTION JOINT
	REINFORCED CONCRETE SLABS

FOR BID

APR 22, 2022 4:12 PM HERNDD01562
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NO.	DATE	DESCRIPTION
△	4/22/22	BEDDING DETAILS
		JRH CWS BSS
		DES DWN REV

ISSUE: MARCH 4, 2022

PROJECT NO: 20A000105D

CAD FILE: C-501-DRN.DWG

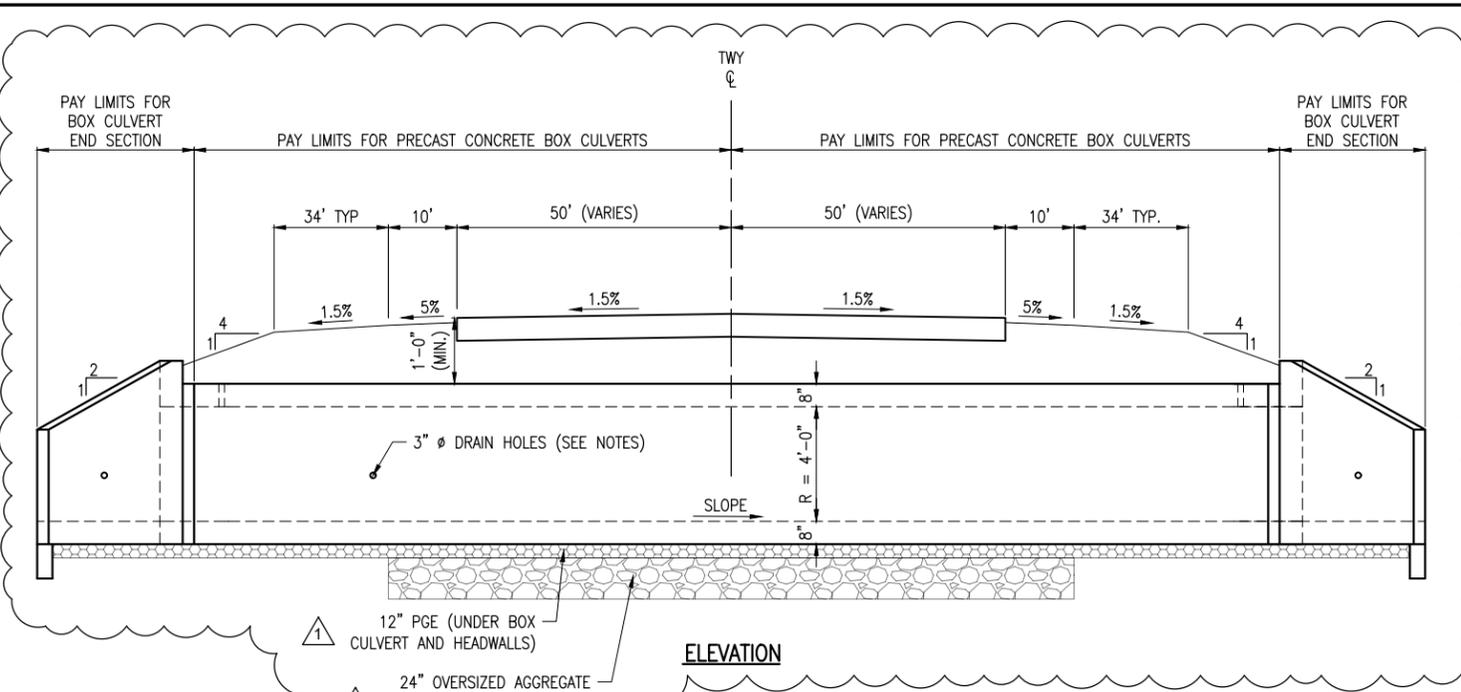
DESIGN BY: MJD 4/12/2021

DRAWN BY: MJD 4/16/2021

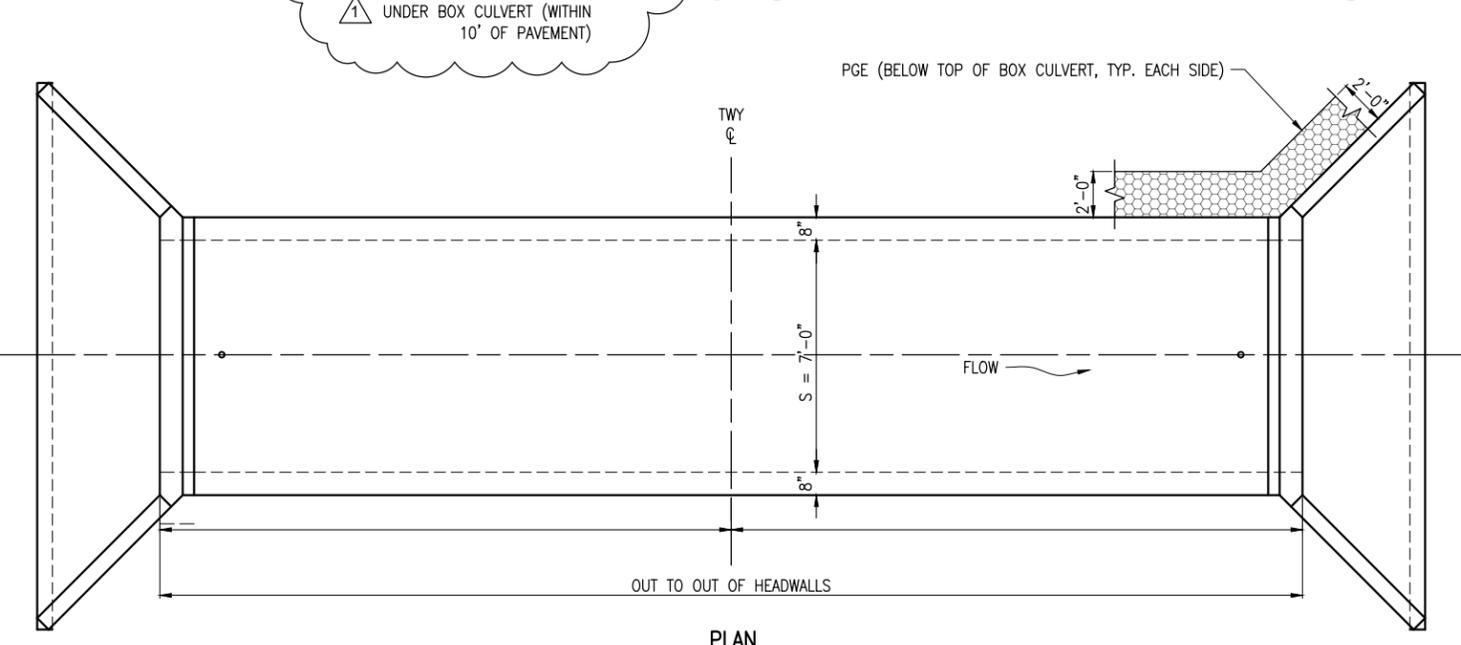
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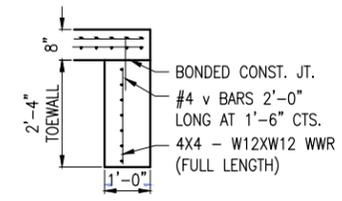
**BOX CULVERT
DETAILS - SHEET 1**



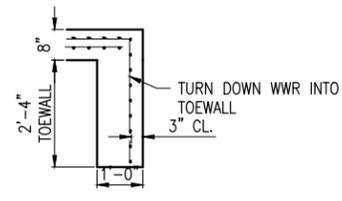
ELEVATION



PLAN



**SECTION B-B
(FROM NEXT SHEET)**



**ALT. SECTION B-B
(FROM NEXT SHEET)**

TOEWALL CONSTRUCTION SEQUENCE

1. PERFORM EXCAVATION AND CONSTRUCT TOEWALL.
2. BACKFILL ACCORDINGLY AND PREPARE BEDDING FOR BOX CULVERT END SECTIONS.
3. CONSTRUCT REMAINDER OF BOX CULVERT END SECTION

NOTE: IF SOIL CONDITIONS PERMIT, THE TOEWALL MAY BE POURED MONOLITHICALLY WITH THE BOTTOM SLAB OF THE END SECTION USING ALT. SECTION D-D SUBJECT TO APPROVAL FROM THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING ALL DETAILS ASSOCIATED WITH THE PRECAST BOX CULVERT INCLUDING ANY STRENGTHENING OR STIFFENING PROVISIONS NECESSARY FOR HANDLING THE PRECAST SEGMENTS. CONCEPTUAL DETAILS FOLLOWED BY SHOP DRAWINGS AND DESIGN CALCULATIONS SEALED BY AN ILLINOIS LICENSED STRUCTURAL ENGINEER SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL.

DESIGN SPECIFICATIONS

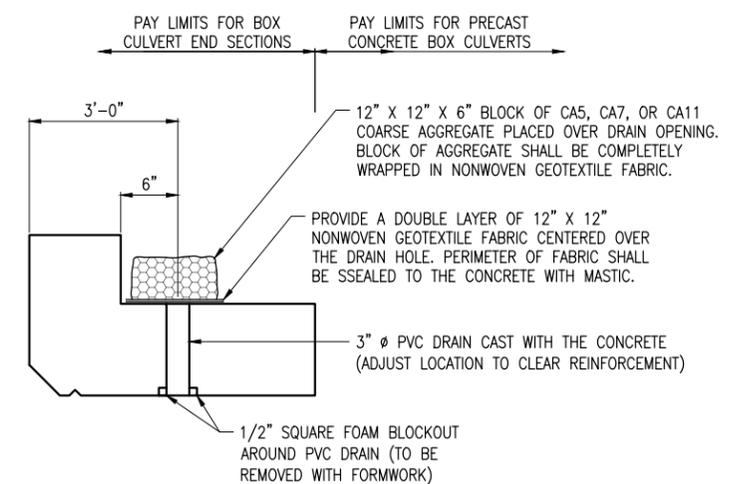
1. FAA AC 150/5320-6G (OR LATEST EDITION/PUBLICATION)
2. AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS (LATEST EDITION)
3. IDOT STANDARD DRAWINGS FOR SINGLE CELL PRECAST BOX CULVERTS
4. AS REQUIRED

LOADING

1. THE PRECAST CONCRETE BOX CULVERT SHALL COMPLY WITH ASTM C789 AND ASTM C850.
2. THE CRITICAL AIRCRAFT LOADING SHALL BE A DUAL WHEEL AIRCRAFT WITH A MAXIMUM TAKEOFF WEIGHT OF 108,000 LBS.
3. PRECAST BOX CULVERTS SHALL BE DESIGNED FOR A MINIMUM COVER OF 1 FOOT.

NOTES

1. DRAIN HOLES SHALL BE PROVIDED ON EXTERIOR CULVERT WALLS FOR EACH PRE-CAST BOX SEGMENT WITH A CLEAR RISE GREATER THAN THREE (3) FEET.
2. DRAIN HOLES SHALL BE LOCATED WITHIN 1/3 OF THE CLEAR RISE OF THE BOX CULVERT, SHALL NOT INTERCEPT THE HAUNCH, AND SHALL CONFORM TO THE SPECIFIED REQUIREMENTS.
3. NON-WOVEN GEOTEXTILE FABRIC SHALL CONFORM TO THE SPECIFIED REQUIREMENTS.
4. PRE-CAST CONCRETE BOX CULVERTS AND BOX CULVERT END SECTIONS SHALL BE BACKFILLED WITH POROUS GRANULAR EMBANKMENT BELOW THE TOP OF THE BOX CULVERT EXTENDING TO A VERTICAL PLANE TWO (2) FEET FROM THE EXTERIOR SIDES OF THE CULVERT, TWO (2) FEET FROM THE BACK FACE OF THE END SECTIONS, AND NOT CLOSER THAN TWO (2) FEET FROM THE FACE OF THE EMBANKMENT.



DRAIN DETAIL

(ALL COSTS ASSOCIATED WITH FURNISHING AND CONSTRUCTING THE ABOVE DRAIN DETAIL WILL NOT BE MEASURED FOR PAYMENT BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE ASSOCIATED WORK.)

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FOR BID

PRE-BID CONFERENCE MEETING AGENDA / MINUTES

ST. LOUIS DOWNTOWN AIRPORT, CAHOKIA, IL
APRIL 12, 2022 – 10:00 AM LOCAL TIME

1. Introductions
 - See attached sign-in sheet for list of attendees.

2. Project Information

**CONSTRUCT RUNUP RAMP AND TAXIWAY ACCESS FROM THE AIRFIELD,
INCLUDING JET BLAST/NOISE MITIGATION BARRIER
ILLINOIS PROJECT NO. CPS-4976
CONTRACT NO. SD061**

IDOT LETTING: APRIL 29, 2022, ITEM NO. 2A

3. Project Plans & Specifications Review

- a. Scope of Work

- Grading and drainage
- Two items for PCC pavement, ferrous and non-ferrous
 - A question was asked about avoiding the use of recycled concrete as aggregate base within the non-ferrous pavement areas in order to avoid potential steel remnants within the materials. Hanson agreed this is a good point and will clarify by addendum.
- Compass calibration pad
 - A magnetic survey will be completed (under Hanson's contract) following the contractor's preparation of the subgrade as well as following the paving of the compass calibration pad. The survey work should not impact the contractor's schedule but if it does Hanson will consider adjusting the contract time accordingly.
- Airfield lighting and signage
 - The Runway 12L/30R circuit and Taxiway B circuit will both be impacted by the proposed work on the project.
- Jet blast barrier/deflector
 - A question was asked regarding the curved barrier wall detail shown in the plans. Hanson indicated that the curve is not a requirement but any proposed installation would need to meet the specifications. Bidders are welcome to submit a particular product for review ahead of the letting.
- Additive Alternates #1 and #2 (additional jet blast barrier/deflectors)
 - The Airport prefers to install the complete jet blast barrier (Base Bid plus Add Alt 1 and 2), but the Alternates are being provided to allow more flexibility due to bids and funding.
- Additive Alternate #3 (apron lighting system)
 - A question was asked about the direct bury installation of the light poles and whether any additional foundation work was required. Hanson indicated the direct bury installation was it, and that manufacturer's recommendations should be followed.

- b. Project Schedule:

- Contract time is 149 consecutive calendar days from start of construction. Barry mentioned that if all work is completed except for a couple items that require a waiting period (marking, for example) we will be flexible on suspending the contract time during the waiting period. If any bidders have concerns about the contract time please let us know prior to the bid.
- Project start dependent on timing of award from IDOT-Aeronautics. Bids will be able to be held for 90 days prior to award. Barry commented that IDOT-Aeronautics has already begun the paperwork process in order to accelerate the award following the letting, which has not been done previously. They have also started using DocuSign instead of routing throughout IDOT for hand signatures, which will save a lot of time as well.
- Submittals for materials to be used on the project will need to be approved prior to beginning site work. Contractor is expected to have enough quantity in their possession to complete the project without unnecessary delays.
- A question was raised about the potential for a late start of the project, leading to work stretching late in the year and to the potential for needing a winter shutdown and the ramifications of doing so. Hanson will discuss with the Airport and with IDOT-Aero and address via addendum as needed.

c. Construction Safety Plan

- Work Areas
 - Main work area, excavations along ditches (available dirt) and east stockpile area (available dirt)
 - The Airport desires that the excavated material along the ditches be utilized in the project and those areas restored.
- Construction Access / Staging
 - Access from Goose Lake Road, existing gate and staging area.
 - Equipment may be stored at the main project site assuming that Runway 12L/30R is closed.
- Maintenance of Traffic, Runway Closure
 - Barricades and closure crosses are to be installed at the start of construction when the runway is closed. Contractor is responsible for maintaining the barricades and crosses, and to ensure the lights are functional throughout the project.
 - Where the existing haul road crosses the Runway 12R/30L extended Runway Safety Area, contractor shall provide escort/flagperson with an airport band radio to monitor air traffic use of that runway.

d. Construction Plans and Notes

e. Project Specifications

4. Deadline for Questions – Tuesday, April 19, 2022, 4:30 PM local time
 - All requests for interpretations, questions and comments should be submitted on the IDOT letting website.
 - Six (6) questions already submitted and responses will be posted by IDA soon.

5. Addendum
 - Minor clarifications
 - Pre-bid Conference Meeting Minutes
 - Contractor Questions (if any)

6. Questions / Comments
 - Temporary mulch is not required with temporary seeding.
 - Survey control points are shown on the Scope of Work plan sheet. The tables on the Construction Safety Plan are labeled as Control Points but should be labeled as Critical Points; those are not survey points but rather are safety points for the FAA's use.
 - Hanson will clarify the reinforcement details for non-ferrous concrete via response to contractor's question and by addendum as needed.

7. Site Visit
 - One attendee elected to look at a portion of the site, specifically the existing excavations along the existing ditch to the west of the project site, and observed existing conditions. One question came from that site visit that will be addressed via addendum.



PRE-BID MEETING SIGN-IN SHEET

MEETING DATE: April 12, 2022 @ 10:00 A.M.

PROJECT NO.: 20A000105D

PROJECT NAME: CPS-4976, Construct Runup Ramp and Taxiway Access from the Airfield, Including Jet Blast/Noise Mitigation Barrier

PROJECT MEETING LOCATION: CPS Administration Building

NAME	COMPANY	PHONE	E-MAIL
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