

**FAI Route 39 (I-39) Resurfacing
Contract No. 68F87
Woodford County**
Item Click here to enter text.
Fed. Proj. No. NHPP-GYUQ(685)

IDOT PROJECT LABOR AGREEMENT DETERMINATION

To: Omer Osman, Secretary

From: Kensil A. Garnett, Region 3 Engineer

Date: 2/4/2022

Re: FAI Route 39 (I-39) Resurfacing Project, Contract Number 68F87, Woodford County

In accordance with the Project Labor Agreement Act (30 ILCS 571), it is recommended that a project labor agreement (PLA) be utilized for the above-captioned Project. This recommendation is based on the considerations indicated below.

- 1) The Project is being awarded and administered by IDOT (i.e., not by another governmental agency).
- 2) The Project is being constructed using state or local funds only (i.e., no federal funds).
- 3) The overall size, scope, sequencing, logistics or other aspects of the Project make it particularly challenging to manage, and use of a PLA is expected to help assure that the construction work is performed properly and efficiently under the circumstances. **See Attachment A.**
- 4) The duration of construction activity on the Project is expected to exceed one construction season (i.e., 110 or more working days), or the nature of the Project results in a heightened need for labor force continuity and stability over a substantial period of time. **See Attachment A.**
- 5) There is a firm construction completion date established for the Project thereby increasing the adverse consequences of any work stoppage or other labor disruption.
- 6) The time required to complete the Project is expected to extend beyond the expiration date of one or more existing collective bargaining agreements covering trades likely to be involved in the Project, thereby increasing the likelihood of work stoppage(s) or other labor disruption(s) during construction of the Project. **See Attachment A.**
- 7) In the absence of a PLA, there is an increased likelihood of jurisdictional disputes among unions or of conflict between unionized and non-unionized workers on the Project that could have a potentially material adverse effect on the time, cost, or quality of work performed on the Project.

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8) This project presents specific safety concerns to the traveling public and a PLA, will ensure labor force continuity and stability, decreasing the length of the safety concern. **See Attachment A.**

9) Use of a PLA is expected to result in improved access to skilled labor, improved efficiency, or improved safety performance on the Project.

10) Use of a PLA on the Project is not expected to have a material adverse effect on the competitive bidding process.

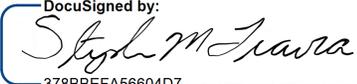
11) Use of a PLA on the Project is not expected to have a material adverse effect on the ability of the Department to achieve other Departmental goals (e.g., utilization of disadvantaged businesses, utilization of Illinois domiciled businesses, development of competitive vendor alternatives over time, etc.).

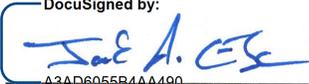
12) There are other material considerations favoring or disfavoring use of a PLA on this Project as follows:

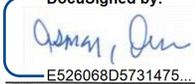


Based upon the identified considerations, we recommend that you approve use of a PLA on this Project. Upon your approval, the Department shall undertake to negotiate in good faith a PLA with the relevant labor organization(s), and shall include in all necessary bid specifications and other documents information regarding the actual or form of PLA that is to binding upon all contractors and their employees.

Submitted:  020422
{Regional Engineer} (Date)

Agreed:  4/20/2022 | 9:51 AM CDT
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{Division Chief} (Date)

Agreed:  4/19/2022 | 9:16 AM CDT
A3AD6056B4AA490...
{Bureau of Design & Environment} (Date)

Approved:  4/20/2022 | 2:59 PM CDT
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Omer Osman, Secretary (Date)

FHWA concurrence in the PLA for the above-mentioned contract
MICHAEL W SMART Digitally signed by MICHAEL W SMART
Date: 2022.03.09 17:16:55 -06'00'

FHWA (Date)

Attachment A

Justification for use of project Labor Agreement on Contract **68F87**,
Woodford County:

The use of a project Labor Agreement on this project is consistent with all State and Local statutory regulatory requirements.

Project Description: This project is approximately 9.74 miles in length and is located on I-39 from one mile north of IL 116 to two miles south of IL 17 in Woodford County, Illinois. Improvements consist of Class A patching, placement of 5" hot-mix asphalt, minor culvert and drainage improvements, minor overpass structure improvements, placement of preformed plastic pavement markings along with raised reflective pavement markers, aggregate shoulders, and other related collateral work necessary to complete the project.

Item 3: The estimated project cost is \$27.3 million. The project has multiple working restrictions for ramp and overhead structure work, as noted below, that impact sequencing and logistics and make it particularly challenging to manage. The use of a PLA is expected to help assure that the construction work is performed properly and efficiently under these circumstances.

1. Overpass bridge closures shall be within the time frames of June 1, 2022 thru August 15, 2022 and June 1, 2023 thru August 15, 2023 as to not conflict with harvest and school bus routes. Dates of ramp and overpass closures must be approved by the Engineer. The Contractor may complete overhead bridge work under flaggers if additional work is needed outside of the allowed closure dates.
2. No two (2) adjacent overpass structures shall be closed at the same time.
3. The Contractor will be allowed to close the northbound on and off ramp at County Highway 2 for the proposed work shown in the plans for 15 consecutive calendar days. See Special Provisions for the Incentive/Disincentive included herein.

The Contractor will be allowed to close the southbound on and off ramp at the County Highway 2 and County Highway 2 at the structure over I-39 for the proposed ramp and bridge work shown in the plans for 30 consecutive calendar days. See Special Provisions for incentive/Disincentive included herein.

At no time shall the northbound ramps be closed at the same time as the southbound ramps and County Highway 2. One direction of ramps shall be open at all times. The northbound on and off ramp shall be closed at the same time and re-opened at the same time to eliminate vehicles exiting I-39 northbound and not being able to re-enter I-39 northbound.

The southbound on and off ramp shall be closed at the same time and re-opened at the same time to eliminate vehicles exiting I-39 southbound and not being able to re-enter I-39 southbound.

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- 4. A separate contract was awarded in November 2021 which includes cleaning and painting of four overpass structures within the project limits. Contractors are required to coordinate Traffic Control to minimize inconvenience to the public and provide safe completion of both projects.

Item 4: The duration of construction activity on the Project is expected to exceed one construction season. The contract allows 130 working days.

Item 6: The time required to complete the project extends beyond the expiration of one or more existing collective bargaining agreements covering trades likely to be involved in the project. The trades that will be used on this project include but are not limited to those listed below. Trade contracts expiring prior to the anticipated construction completion date are indicated with bold font.

<u>Local Trade</u>	<u>Current Contract Term</u>
Cement Masons/Plasterers 18	May 1, 2021 thru April 30, 2022
Iron Workers 112	May 1, 2021 thru April 30, 2022
Laborers 996	May 1, 2021 thru April 30, 2023
Carpenters 237	May 1, 2021 thru April 30, 2024
Operating Engineers 649	April 1, 2021 thru March 31, 2025
Teamsters 627	May 1, 2021 thru April 30, 2022

Item 8: The project allows for temporary lane width reductions at ten locations within in the construction zone with adequate notice and proper signage. Any disruption to the contractor's schedule due to labor issues may result in an increase in duration of the project. This, in turn, could increase the length of time the traffic control on I-39 will be in place and the duration of reduced lane widths. The district desires to minimize the length of time traffic control is required on I-39 and the length of time reduced lane widths are needed to reduce the potential for crashes and to improve safety.

FAI 39
Contract No. 68F87
Woodford and Marshall Counties
Item 165
Fed. Proj. No. NHPP-GYUQ(685)

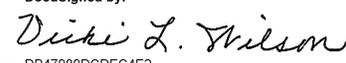
Execution Page

Illinois Department of Transportation

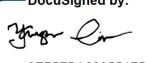
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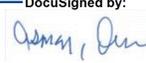
Director of Highways Project Implementation

DocuSigned by:

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Director of Finance & Administration

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Yangsu Kim, Chief Counsel

DocuSigned by:

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Omer Osman, Secretary

4/20/2022 | 2:59 PM CDT

(Date)

**Illinois AFL-CIO Statewide Project Labor Agreement Committee, representing the
Unions listed below:**



04/18/22
(Date)

List Unions:



Mike Volpentesta
Bricklayers



William Mangin
Heat & Frost Insulators & Allied
Workers



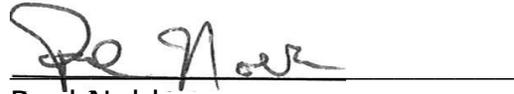
Evan Wooding
United Association



Gary Menzel
Roofers & Waterproofers



*Ed Christensen
Elevator Constructors



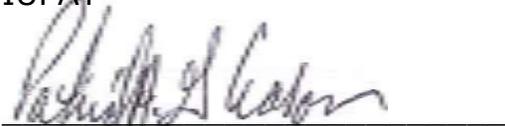
Paul Noble
IBEW



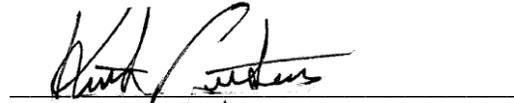
Ryan Anderson
IUPAT



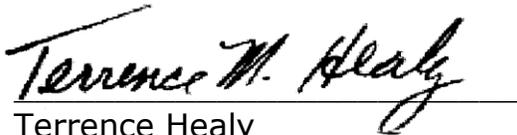
Michael R Kresge
IUOE



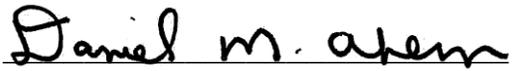
Pat Gleason
Teamsters



Keith Jutkins
Carpenters



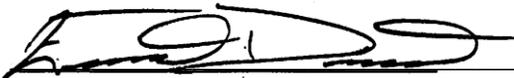
Terrence Healy
LIUNA



Daniel M. Ahern
Sheet Metal Workers



David Beard
Iron Workers



Eric S. Davis
Boilermakers



Kevin J. Farley
OPCMIA

*Elevator Constructors master agreement language
must be attached to PLA