

ITEM 4A
4-29-11 LETTING

TOTAL SHEETS: 09
PN006

CONSTRUCTION PLANS FOR PEKIN MUNICIPAL AIRPORT

PEKIN, TAZEWELL COUNTY, ILLINOIS

IL. PROJ. NO. C15-4074
AIP PROJ. NO. 3-17-0078-B12

REHABILITATE EAST PORTION OF THE GA RAMP

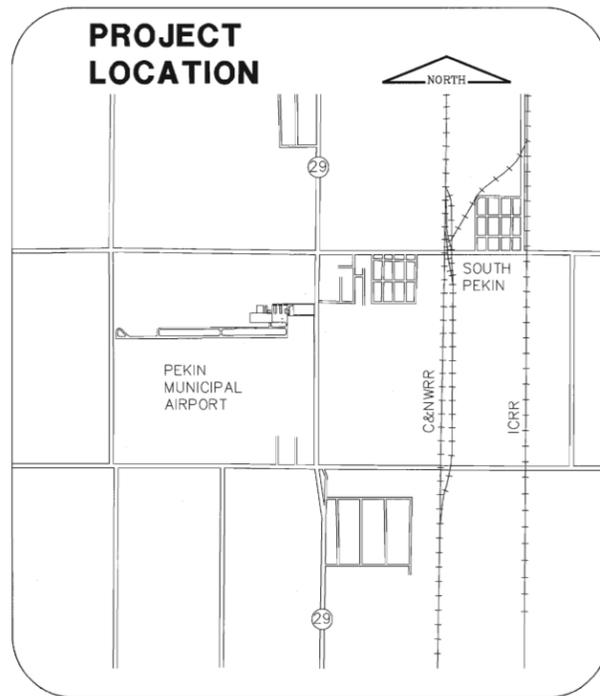
SUMMARY OF QUANTITIES

ITEM NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY
AR152450	SHOULDER EMBANKMENT	S.Y.	405
AR201660	BITUMINOUS CRACK REPAIR	L.F.	1000
AR201672	CRACK CONTROL FABRIC	L.F.	1000
AR401611	BIT. SURFACE COURSE-METHOD I	TON	1100
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	12250
AR402620	POROUS FRICTION COURSE 5/8"	S.Y.	12250
AR510510	TIE DOWN	E.A.	24
AR510515	GROUND ROD	E.A.	5
AR510900	REMOVE TIE DOWN	E.A.	24
AR510905	REMOVE GROUND ROD	E.A.	5
AR603510	BITUMINOUS TACK COAT	GAL	2575
AR901510	SEEDING	ACRE	0.1
AR908525	KNITTED STRAW MAT	S.Y.	405

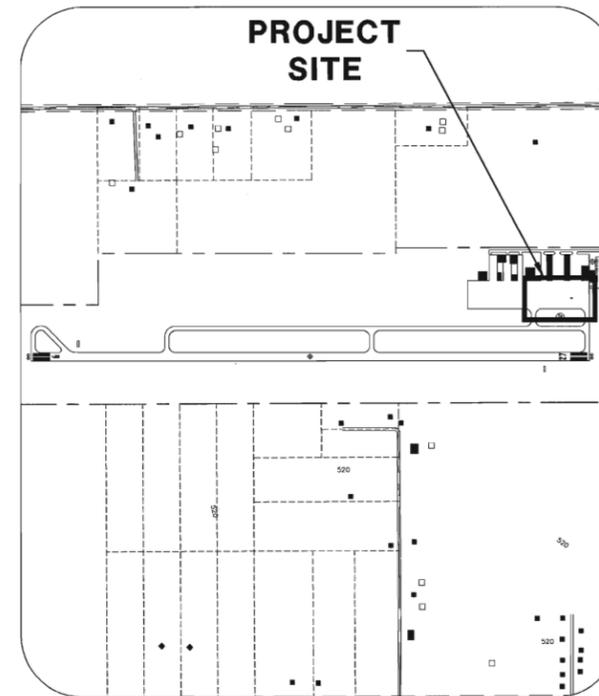
INDEX TO SHEETS

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3	CONSTRUCTION ACTIVITY PLAN
4	EXISTING CONDITIONS AND REMOVALS
5	TYPICAL SECTIONS
6	PROPOSED IMPROVEMENTS
7	MISCELLANEOUS DETAILS
8	STAKING PLAN
9	CROSS SECTIONS STA. 247+59.60 TO STA. 251+99.60

MARCH 4, 2011



LOCATION MAP



SITE PLAN

3-18-2011



Chris B. Groth
Exp. 11-30-2011

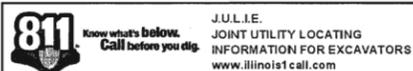
**CITY OF PEKIN
AIRPORT COMMISSION**

APPROVED *Robert A. Smith*
DATE March 14, 2011

CMT
CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS
■ SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO

SUBMITTED BY *Chris B. Groth*
DATE 3/18/2011

CMT JOB NUMBER: 10069-03-00



J.U.L.I.E.
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

DESIGN INFORMATION

PAVEMENT DESIGN CRITERIA
GROUP II AIRPLANE DESIGN GROUP
CENTURION 210 - MAX TAKEOFF 4,100 LBS

STRUCTURAL DESIGN DATA

GROSS WEIGHT - 12,500 lbs.
AIRCRAFT SINGLE WHEEL GEAR
100 ANNUAL DEPARTURES

CALL J.U.L.I.E.
BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: T24N
RANGE: R5W
SECTION: 33
COUNTY: TAZEWELL
CIVIL TOWNSHIP: CINCINNATI

PN006

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS

REHABILITATE EAST PORTION OF THE GA RAMP
 AIRPORT SITE PLAN

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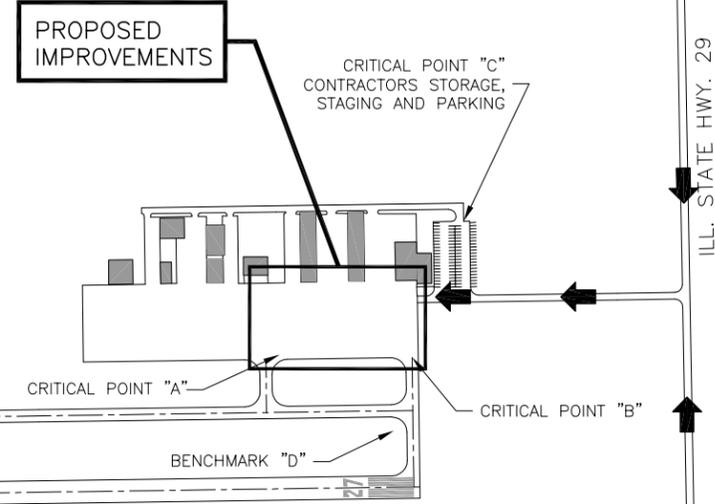
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IL. PROJ. NO. C15-4074 AIP PROJ. NO. 3-17-0078-B12	
SHEET 02 OF 09 SHEETS	

CRITICAL POINTS			
POINT	LATITUDE N.	LONGITUDE W.	ELEVATION
A	40°29'21.86"	89°40'06.62"	521.6
B	40°29'21.90"	89°40'01.18"	522.2
C	40°29'26.29"	89°39'58.95"	525.0

BENCHMARK				
POINT	FIELD OBJECT	LATITUDE N.	LONGITUDE W.	ELEVATION
		NORTHING	EASTING	
D	NGS MONUMENT	40°29'19.53"	89°40'01.97"	521.42
		1392306.56	2435501.03	

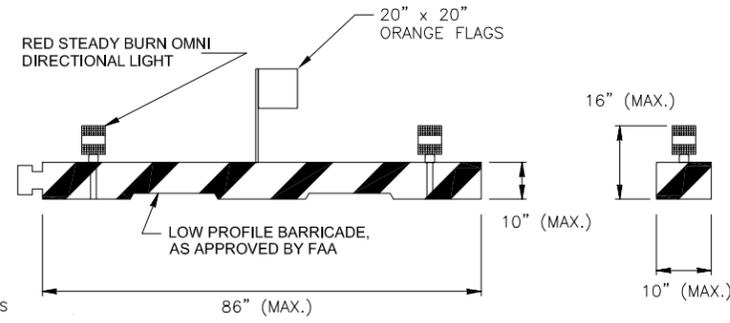
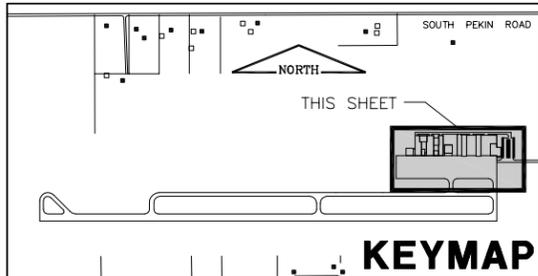


UNICOM FREQUENCY: 122.80
 MAXIMUM HEIGHT OF EQUIPMENT: 25'



GENERAL NOTES

- 1.) ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- 2.) ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF FLAGMAN IN RADIO CONTACT WITH AIR TRAFFIC. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO ARE FAMILIAR WITH AIRCRAFT OPERATIONS.
- 3.) WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT MANAGER.
- 4.) THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
- 5.) BROKEN CONCRETE AND ASPHALT SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY.
- 6.) VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 40' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 200' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
- 7.) ALL PAVEMENTS, DRIVES, OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND AIRPORT MANAGER. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- 8.) EXISTING TURF AREAS OUTSIDE THE GRADING LIMITS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT MANAGER.
- 9.) THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
- 10.) IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES WITHIN THE PROJECT SITE SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- 11.) CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A.) THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN.
 - B.) THE CONTRACTOR MAY BE REQUIRED TO COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR.
 - C.) THE CONTRACTOR SHALL USE AN EXISTING GATE FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR MAY BE REQUIRED TO INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER AND AIRPORT MANAGEMENT. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE RESIDENT ENGINEER.
 - D.) CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL TRAFFIC CONTROL SIGNAGE REQUIRED BY THE COUNTY, TOWNSHIP, OR I.D.O.T.
 - E.) DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - F.) THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS.
 - G.) THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - H.) ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - I.) THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY, TAXIWAY SAFETY AREAS, AND OBJECT FREE AREAS.
 - J.) THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.
 - K.) THE CONTRACTOR MAY USE ALTERNATE AIRPORT GATES TO DELIVER LARGER EQUIPMENT TO THE JOB SITE. THE CONTRACTOR SHALL COORDINATE ARRIVAL AND DEPARTURE OF THESE DELIVERIES WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER.



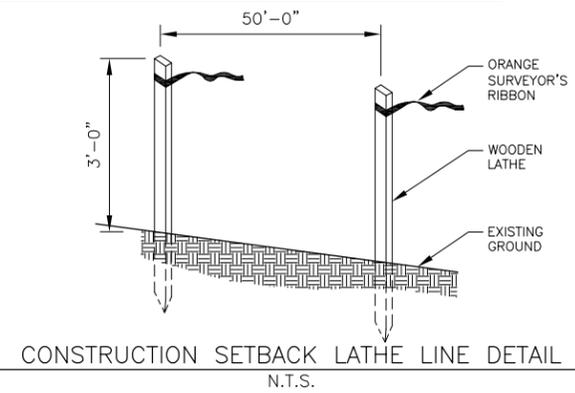
- BEAM BARRICADE NOTES**
1. BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY WIND, JET OR PROP BLAST.
 2. BARRICADE MUST BE OF LOW MASS AND EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT.
 3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
 4. PLACE AS SHOWN IN PLANS AND AS DIRECTED BY THE ENGINEER.

- NOTES**
1. THE AIRPORT WILL REQUIRE 7 CALENDAR DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING ANY PHASE TO COORDINATE WITH THE TENANTS.
 2. THE CONTRACTOR SHALL PLACE ALL BARRICADES AND LATHE LINES AS SHOWN PRIOR TO STARTING WORK ON THESE PHASES.
 3. ALL BARRICADES AND LATHE LINES SHALL REMAIN IN PLACE FOR THE DURATION OF WORK IN EACH PHASE.
 4. PHASE 1 & 2 SHALL NOT BE CONSTRUCTED CONCURRENTLY AND PHASE 1 SHALL BE CONSTRUCTED AND COMPLETED FIRST.
 5. THE FUEL PUMPS LOCATED IN THE CENTER OF THE RAMP SHALL BE ACCESSIBLE BY AIRCRAFT AT ALL TIMES AS SHOWN IN AT LEAST ONE DIRECTION OF THE PUMPS.
 6. ALL RUNWAYS, TAXIWAYS, AND MAIN APRON, EXCEPT THE CLOSURES AS SHOWN, SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION.
 7. CONTRACTOR SHALL WORK WITH THE AIRPORT AND THE AIRPORT TENANTS SUCH THAT CONSTRUCTION OPERATIONS TIME WILL NOT EXCESSIVELY DELAY HANGAR ACCESS.
 8. IF DURING CONSTRUCTION IT IS FOUND THAT FULLY LOADED TRUCKS ARE EXCESSIVELY DAMAGING THE EXISTING SUBGRADE OR PAVEMENT, THE CONTRACTOR MAY BE REQUIRED TO LIMIT THE WEIGHT OF DELIVERY TRUCKS. THE CONTRACTOR SHALL REPAIR DAMAGE TO THE EXISTING SUBGRADE OR PAVEMENT CAUSED BY FULLY LOADED TRUCKS. COST TO REPAIR THE DAMAGED AREAS SHALL BE INCIDENTAL TO THE PROJECT.
 9. THE CONTRACTOR SHALL MAINTAIN HAUL ROAD TO SUPPORT ALL WEATHER HAULING OPERATIONS AND RESTORE TO ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT.

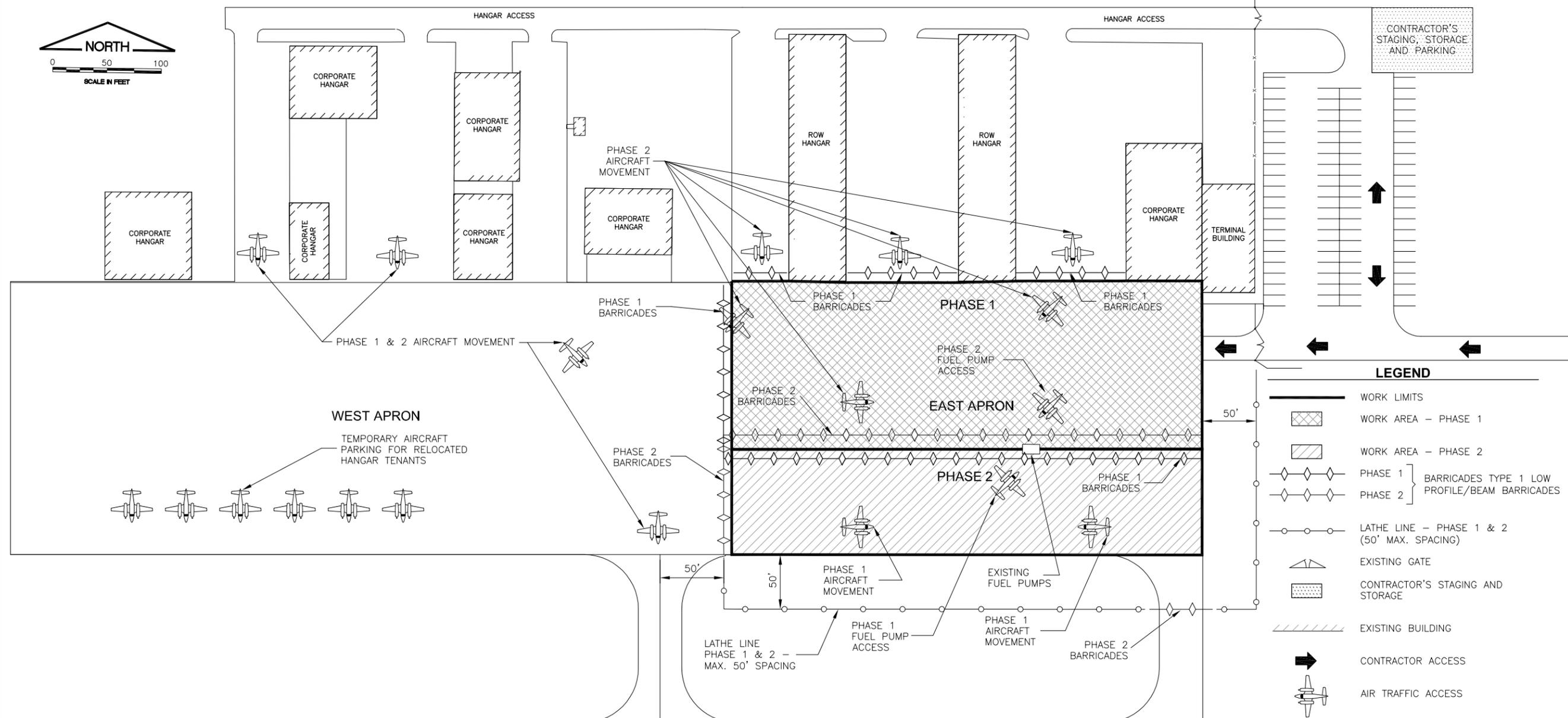
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CONSTRUCTION SETBACK LATHE LINE DETAIL
N.T.S.



LEGEND

- WORK LIMITS
- WORK AREA - PHASE 1
- WORK AREA - PHASE 2
- PHASE 1 } BARRICADES TYPE 1 LOW PROFILE/BEAM BARRICADES
- PHASE 2 }
- LATHE LINE - PHASE 1 & 2 (50' MAX. SPACING)
- EXISTING GATE
- CONTRACTOR'S STAGING AND STORAGE
- EXISTING BUILDING
- CONTRACTOR ACCESS
- AIR TRAFFIC ACCESS

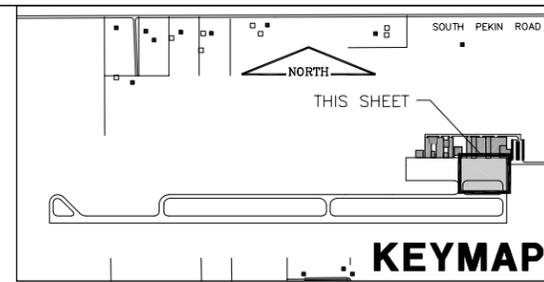
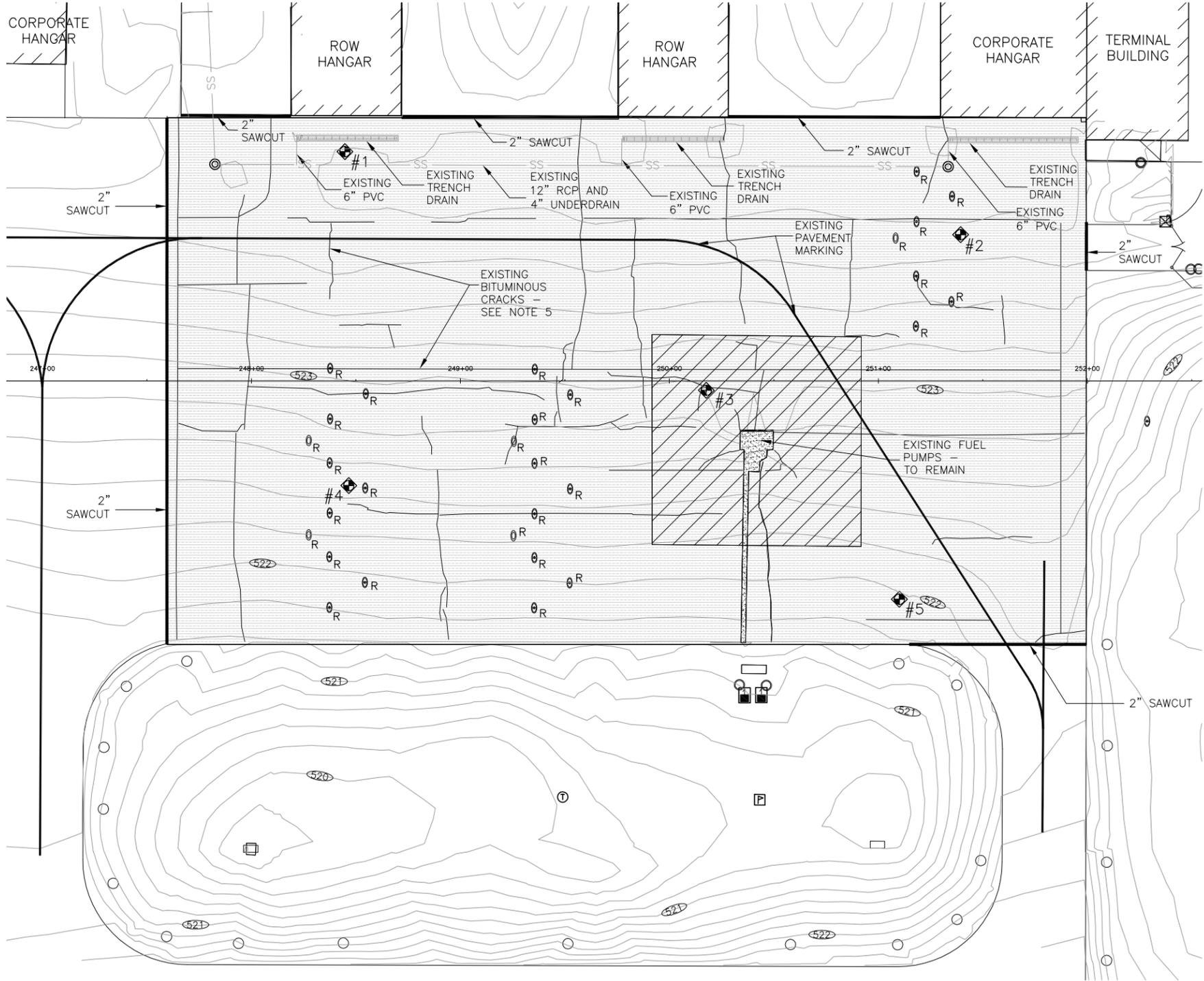
**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS**

**REHABILITATE EAST PORTION OF THE GA RAMP
 CONSTRUCTION ACTIVITY PLAN**

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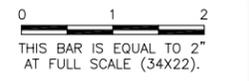
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PN006

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LEGEND

- BITUMINOUS PAVEMENT MILLING
- EXISTING PCC PATCH - TO REMAIN
- EXISTING COAL TAR EMULSION COAT
- EXISTING BITUMINOUS CRACKS
- EXISTING PAVEMENT
- EXISTING CONTOURS
- EXISTING STORM SEWER
- EXISTING TIEDOWN
- EXISTING GROUND ROD
- REMOVAL
- EXISTING INLET
- SAWCUT
- EXISTING BUILDING
- EXISTING GATE
- EXISTING FENCE
- EXISTING TRENCH DRAIN
- EXISTING MANHOLE
- EXISTING ELECTRIC JUNCTION BOX
- EXISTING TAXIWAY EDGE LIGHTS
- EXISTING WINDCONE
- EXISTING WIND TEE
- EXISTING ABOVE-GROUND FUEL TANKS
- EXISTING PAVEMENT MARKINGS
- PAVEMENT CORING

NOTES

- EXISTING HANGAR BUILDINGS HAVE UTILITY CONNECTIONS, SLIDING DOOR HARDWARE AND DRAINAGE HARDWARE THAT EXTENDS BEYOND THE FOOTPRINT OF THE BUILDING (NOT SHOWN). THE CONTRACTOR MAY BE REQUIRED TO REMOVE THIS HARDWARE DURING CONSTRUCTION IN ORDER TO PROPERLY PROCEED WITH OPERATIONS AND WILL BE REQUIRED TO REPLACE THESE ITEMS TO PRE-CONSTRUCTION CONDITIONS AND TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT ONCE WORK HAS BEEN COMPLETED.
- THE MILLING OPERATIONS SHALL REMAIN A FIXED DISTANCE FROM THE FACE OF THE BUILDING WHERE THERE ARE NO BUILDING DOORS. THE RESIDENT ENGINEER WILL DETERMINE THE ACCEPTABLE OFFSET FOR THE MILLING EQUIPMENT. MILLING OPERATIONS AT THE BUILDING DOORS SHALL BE MILLED UP TO THE BUILDING FOUNDATION. MILLING ALONG THE BUILDING FOUNDATION MAY REQUIRE SPECIAL EQUIPMENT CAPABLE MILLING IN CLOSE PROXIMITY OF A VERTICAL FACE OR THE USE OF HAND TOOLS. COSTS ASSOCIATED WITH THIS MILLING SHALL BE INCIDENTAL TO THE MILLING PAY ITEM.
- CONTRACTOR SHALL NOT DAMAGE EXISTING BUILDING FOUNDATIONS OR BUILDING DOORS. DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
- SAWCUT SHOWN SHALL BE INCIDENTAL TO MILLING OPERATIONS. ADDITIONAL AREAS NOT SHOWN MAY REQUIRE SAWCUT. NO ADDITIONAL PAYMENTS WILL BE MADE FOR SAWCUT.
- RESIDENT ENGINEER SHALL DETERMINE WHICH BITUMINOUS CRACKS WILL NEED TO BE EXTRA DEPTH REPAIRED.
- THE EXISTING TRENCH DRAINS AND PCC PAVEMENT AROUND THE EXISTING FUEL PUMPS SHALL NOT BE REMOVED AND ANY DAMAGE TO THESE STRUCTURES SHALL BE REPAIRED TO THE SATISFACTION OF THE AIRPORT AND RESIDENT ENGINEER BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
- SEE SPECIFICATIONS FOR PAVEMENT CORING SUMMARY.

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

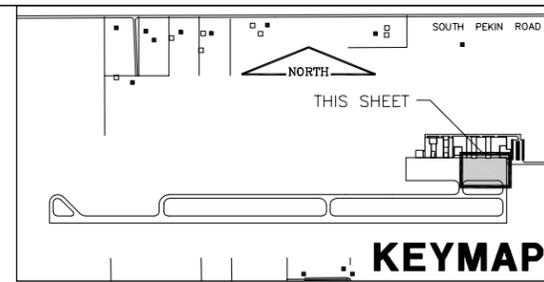
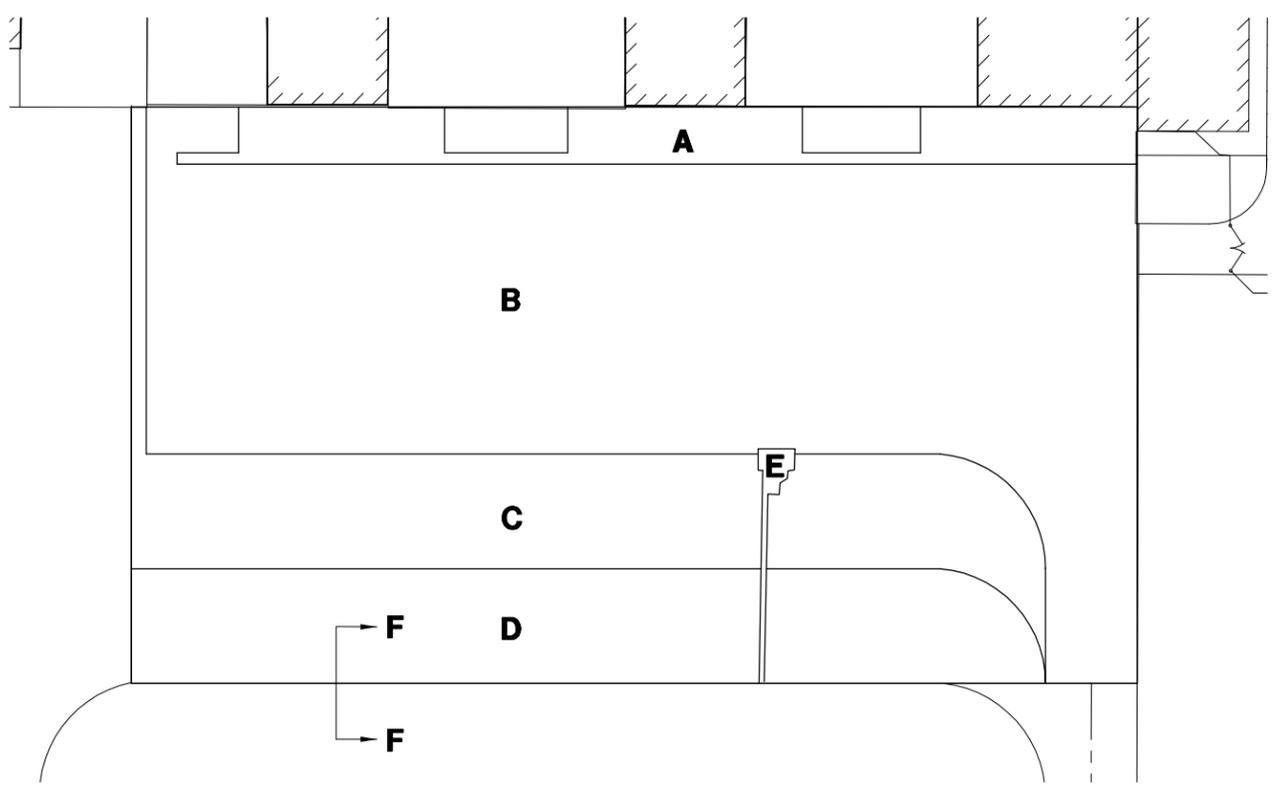
**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS**

**REHABILITATE EAST PORTION OF THE GA RAMP
 EXISTING CONDITIONS AND REMOVALS**

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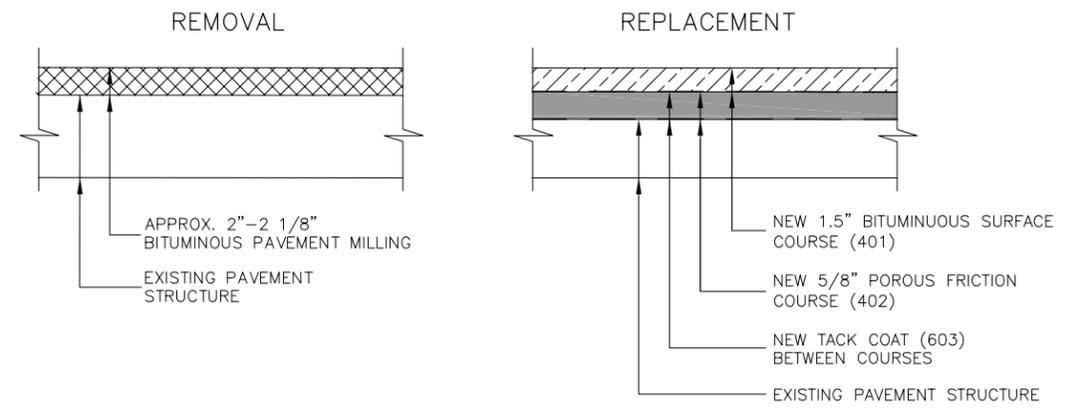
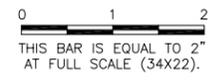
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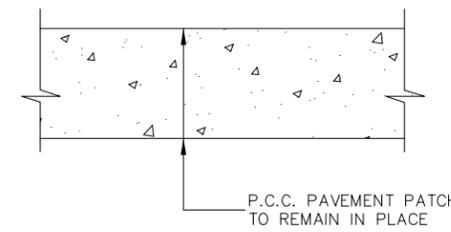
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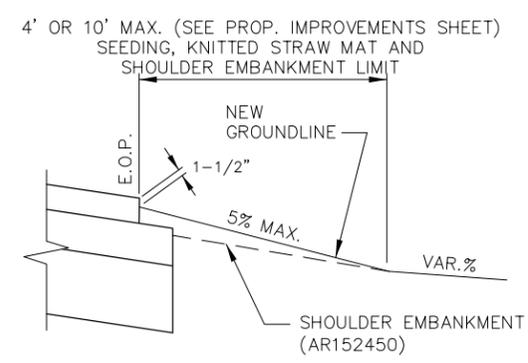
PAVEMENT REMOVAL & REPLACEMENT
 TYPICAL STRUCTURE

NTS



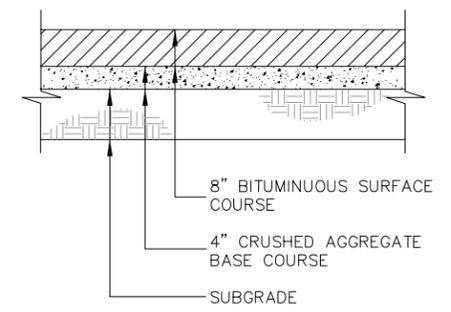
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 EXISTING PAVEMENT STRUCTURE

NTS



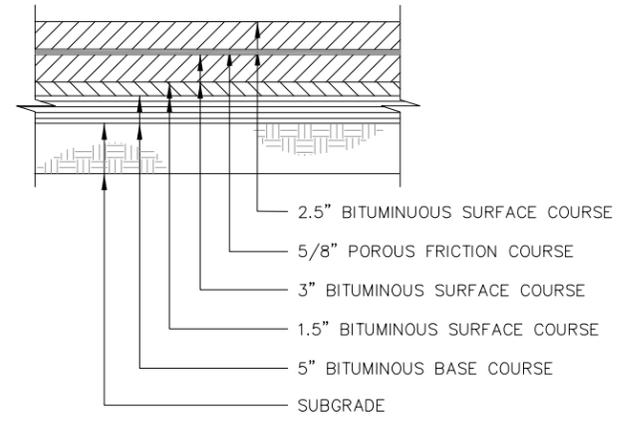
SECTION F
 UNPAVED SHOULDER PROPOSED TYPICAL SECTION

NTS



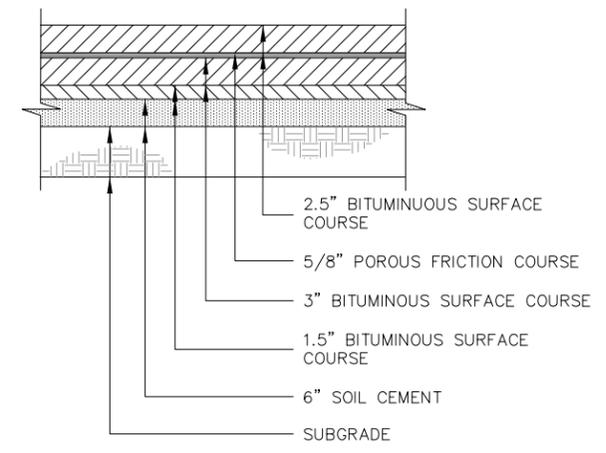
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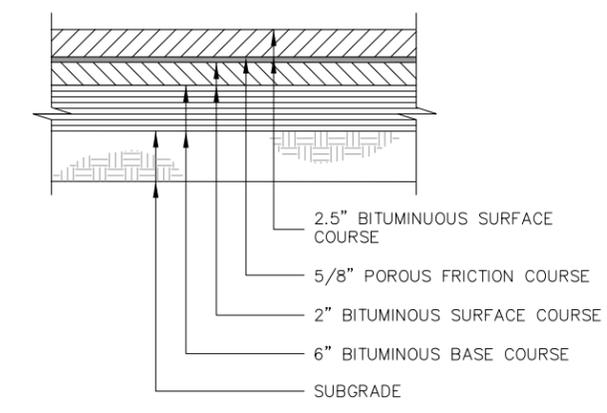
AREA C
 EXISTING PAVEMENT STRUCTURE

NTS



AREA B
 EXISTING PAVEMENT STRUCTURE

NTS



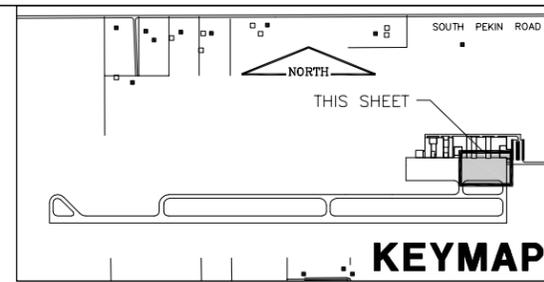
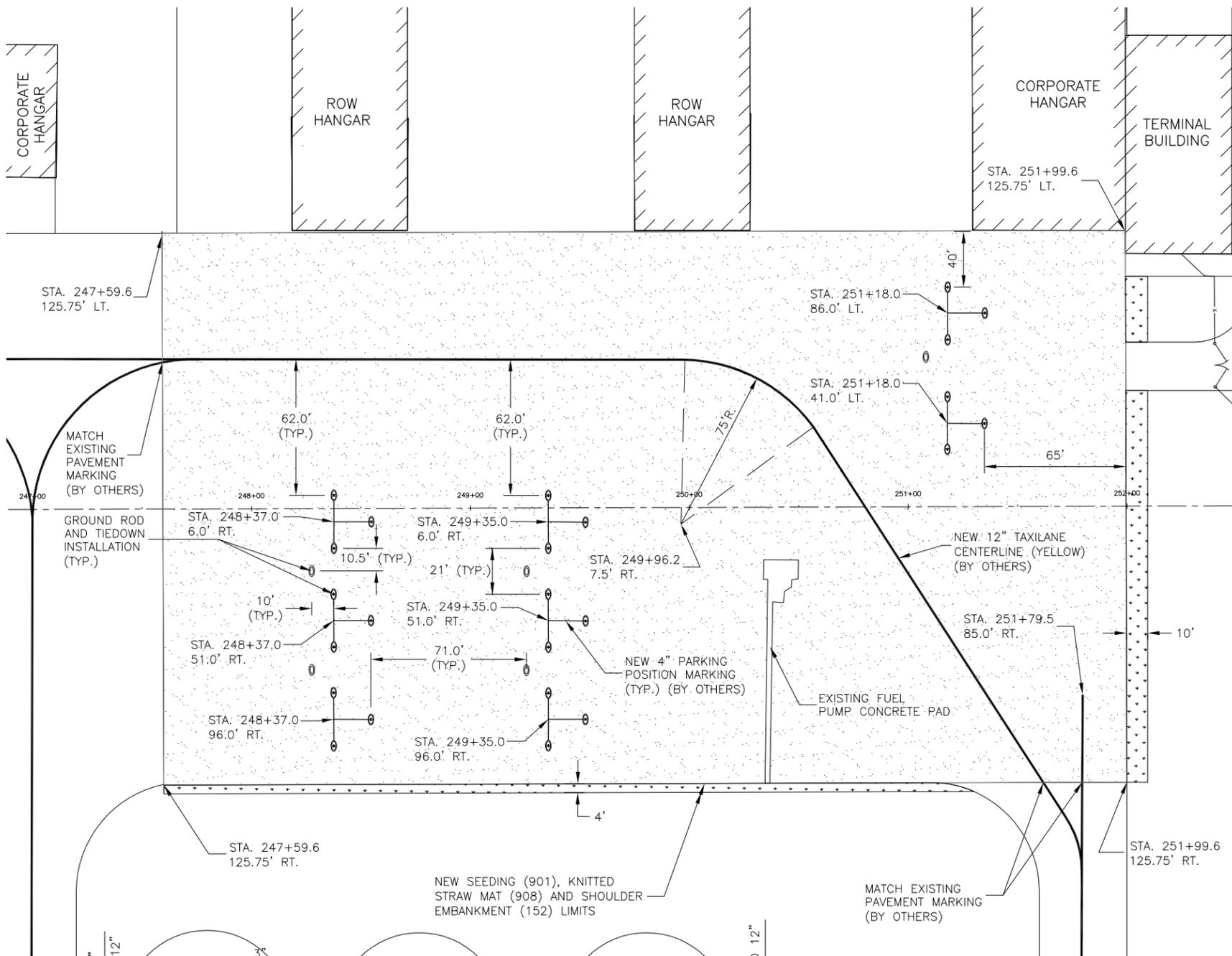
AREA D
 EXISTING PAVEMENT STRUCTURE

NTS

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PEKIN, TAZEWELL COUNTY, ILLINOIS
REHABILITATE EAST PORTION OF THE GA RAMP
TYPICAL SECTIONS

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SHEET	05 OF 09 SHEETS

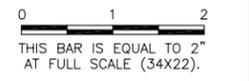


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PN006

REVISIONS		
NUMBER	BY	DATE

CONTROL POINTS			
STATION	OFFSET	NORTHING	EASTING
247+00	0' RT.,LT.	1392664.38	2435078.81
252+50	0' RT.,LT.	1392673.88	2435628.73



LEGEND	
	NEW BITUMINOUS OVERLAY
	NEW SEEDING (901), KNITTED STRAW MAT (908) AND SHOULDER EMBANKMENT (152) LIMITS
	EXISTING PAVEMENT
	EXISTING BUILDING
	EXISTING GATE
	EXISTING FENCE
	NEW TIEDOWN
	NEW GROUND ROD
	NEW PAVEMENT MARKING (BY OTHERS)

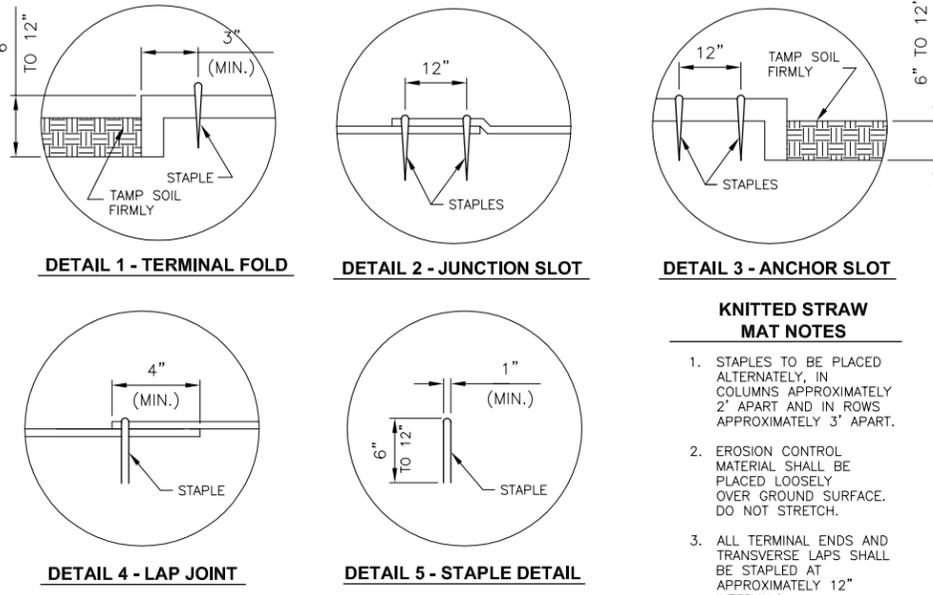
TURFING NOTES:

- SEEDING, KNITTED STRAW MAT AND SHOULDER EMBANKMENT LIMITS SHALL BE AS NECESSARY AND AS DEEMED BY THE RESIDENT ENGINEER IN ORDER TO REPAIR THE RAMP SHOULDER TO PRE-CONSTRUCTION CONDITIONS.
- THE SEEDING, KNITTED STRAW MAT AND SHOULDER EMBANKMENT LIMITS WIDTH FROM THE PAVEMENT EDGE WILL NOT EXCEED 4' OR 10' AS SHOWN PER THIS SHEET.
- ANY DAMAGE TO THE TURF BEYOND 4' OR 10', INCLUDING THE CONTRACTOR'S STAGING AREA, SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. THIS WILL BE DONE AT NO ADDITIONAL COST TO THE CONTRACT.

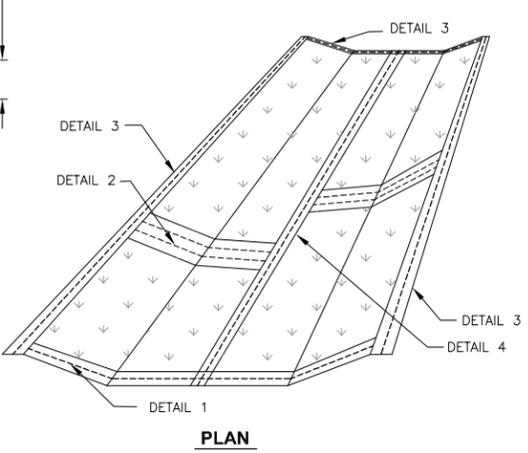
PEKIN MUNICIPAL AIRPORT
PEKIN, TAZEWELL COUNTY, ILLINOIS
REHABILITATE EAST PORTION OF THE GA RAMP
PROPOSED IMPROVEMENTS

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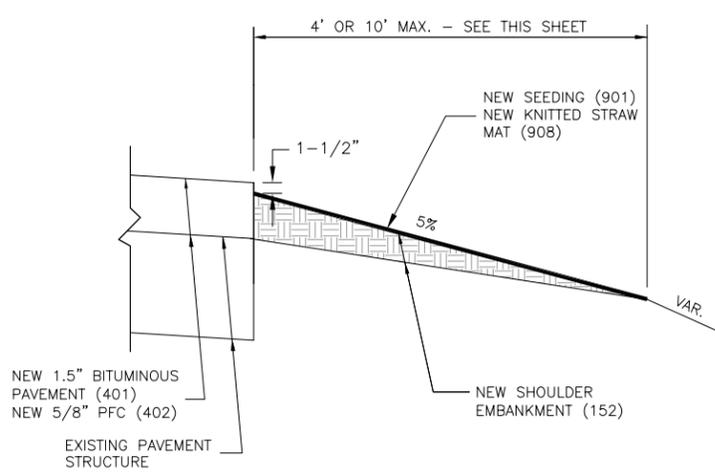
DESIGN BY:	AJH
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	MARCH 4, 2011
JOB No:	10069-03-00
IL PROJ. NO. C15-4074	
AIP PROJ. NO. 3-17-0078-B12	
SHEET 06 OF 09 SHEETS	



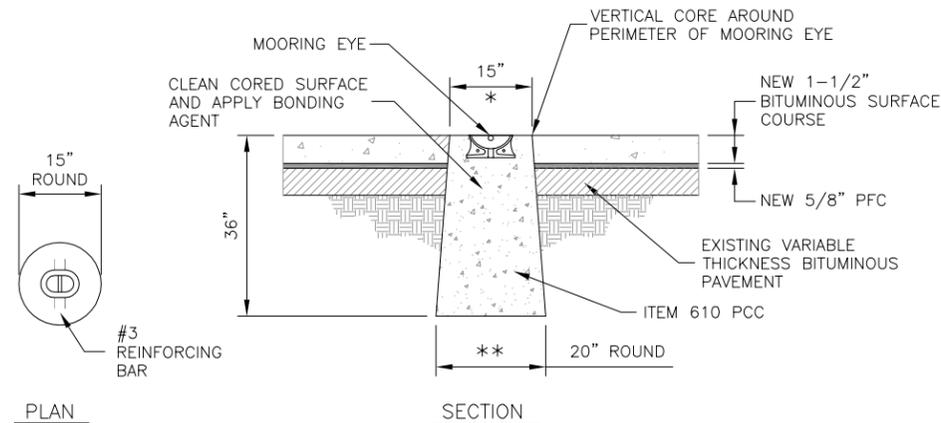
- KNITTED STRAW MAT NOTES**
- STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
 - EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
 - ALL TERMINAL ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.



KNITTED STRAW MAT DETAILS
N.T.S.



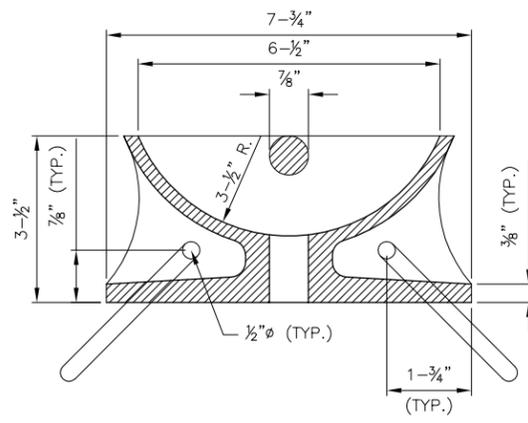
SHOULDER EMBANKMENT - TYPICAL SECTION
N.T.S.



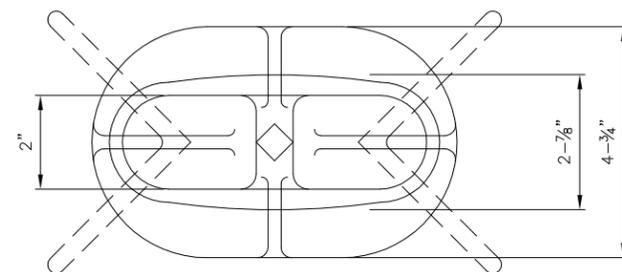
NOTE: THE MOORING EYE SHALL BE INSTALLED AFTER PAVING OPERATIONS ARE COMPLETE.

MOORING EYE IN PAVEMENT DETAIL

N.T.S.

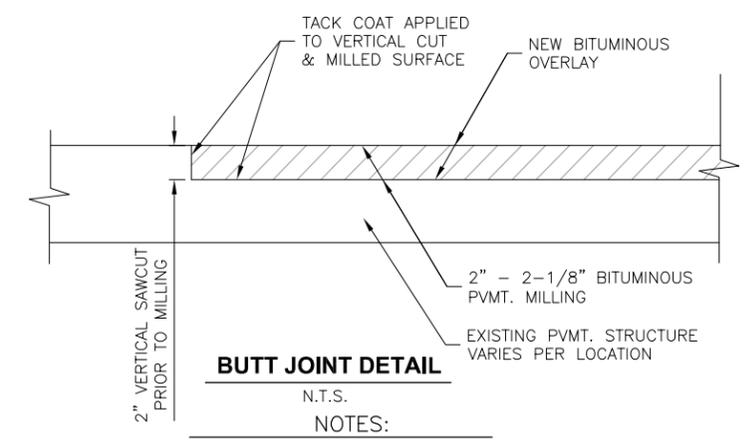


SECTION B-B



MOORING EYE DETAILS

N.T.S.



BUTT JOINT DETAIL

N.T.S.

NOTES:

1. THIS JOINT USED IN ALL LOCATIONS WHEN CONSTRUCTING NEXT TO ADJACENT EXISTING PAVEMENT.
2. DETAIL SHOWN FOR CONSTRUCTION INFORMATION ONLY. NO SEPARATE PAYMENT WILL BE MADE FOR BUTT JOINT CONSTRUCTION.

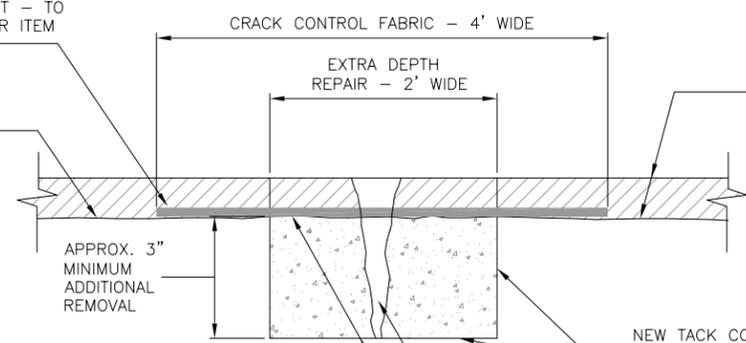
TIEDOWN REMOVAL NOTES

1. * - EXISTING DIMENSION IS 12" PER RECORD DRAWINGS.
2. ** - EXISTING DIMENSION IS 16" PER RECORD DRAWINGS.
3. UPON REMOVAL OF TIEDOWN AND CONCRETE BASE, CONTRACTOR SHALL BACKFILL WITH AGGREGATE OR OTHER SUITABLE MATERIAL APPROVED BY THE R.E. TO BE REMOVED DURING INSTALL PROCESS.

MOORING NOTES

1. MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. R3490 OR APPROVED EQUIVALENT.
2. ANCHOR RODS SHALL BE #3 DEFORMED REBAR, 15" LONG AND SHALL BE BENT DOWNWARD AT 45 DEGREES.
3. THE MOORING EYE SHALL BE INSTALLED AFTER PAVING OPERATIONS ARE COMPLETE BY CORING THROUGH NEW PAVEMENT.

NEW 4' WIDE CRACK CONTROL FABRIC (AR201672) OVER EXTRA DEPTH REPLACEMENT - TO BE PAID FOR UNDER ITEM AR201672 CRACK CONTROL FABRIC



NOTES:

1. CRACKS TO BE REPAIRED SHALL BE DESIGNATED BY THE RESIDENT ENGINEER. GENERAL DIRECTION SHALL BE TO REPAIR BY THIS METHOD ANY CRACKS GREATER THAN ONE (1") INCH IN WIDTH THAT REMAIN AFTER MILLING.
2. THE DEPTH FOR ADDITIONAL REMOVAL MAY INCREASE AT THE DESIGNATION OF THE RESIDENT ENGINEER BASED ON THE SEVERITY OF THE CRACK. MINIMUM DEPTH OF THIS ADDITIONAL REMOVAL SHALL BE THREE (3") INCHES.
3. THE CONTRACTOR SHALL REMOVE ALL LOOSE DEBRIS FROM THE EXTRA DEPTH REMOVAL TO THE SATISFACTION OF THE RESIDENT ENGINEER PRIOR TO APPLYING TACK COAT.

EXTRA DEPTH - BITUMINOUS CRACK REPAIR (AR201660)

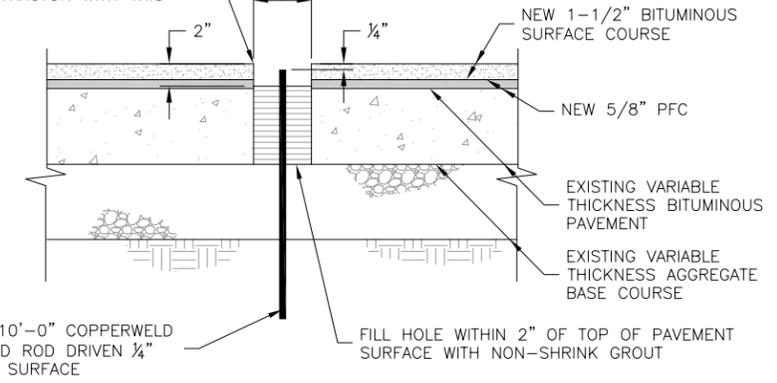
N.T.S.

REMOVAL
 NEW 2 TO 2-1/8" BITUMINOUS MILLING REPLACEMENT
 NEW 1.5" BITUMINOUS SURFACE COURSE (401)
 NEW 5/8" PFC (402)

G.A. RAMP TAXILANE CENTERLINE CONTINUOUS (BY OTHERS)

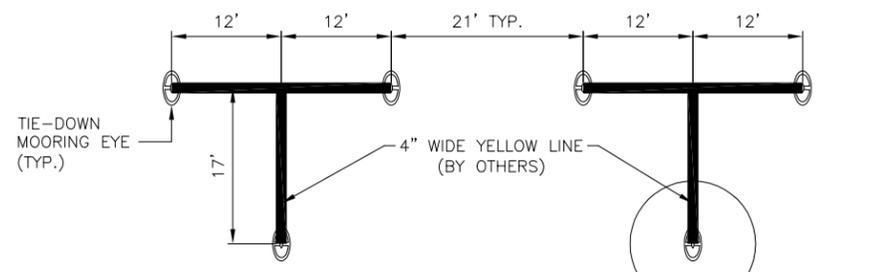
N.T.S.

PAINT 16" DIA. RED CIRCLE WITH 2" BLACK BORDER ON PAVEMENT CENTERED OVER GROUND ROD. PAINT INCIDENTAL TO GROUND ROD. THIS PAINTING SHALL BE COMPLETED BY THE CONTRACTOR WITH THIS PROJECT.



GROUNDING POINT DETAIL

N.T.S.

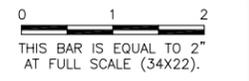


TIEDOWN DETAIL - MARKINGS BY OTHERS

N.T.S.

PN006

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**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS**

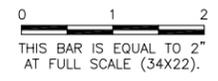
**REHABILITATE EAST PORTION OF THE GA RAMP
 MISCELLANEOUS DETAILS**

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CHECKED BY:	CBG
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JOB No:	10069-03-00
IL PROJ. NO.	C15-4074
AIP PROJ. NO.	3-17-0078-B12
SHEET	07 OF 09 SHEETS

PN006

REVISIONS		
NUMBER	BY	DATE



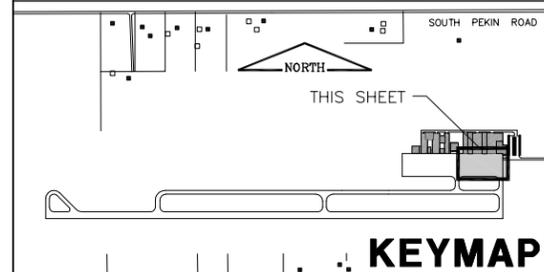
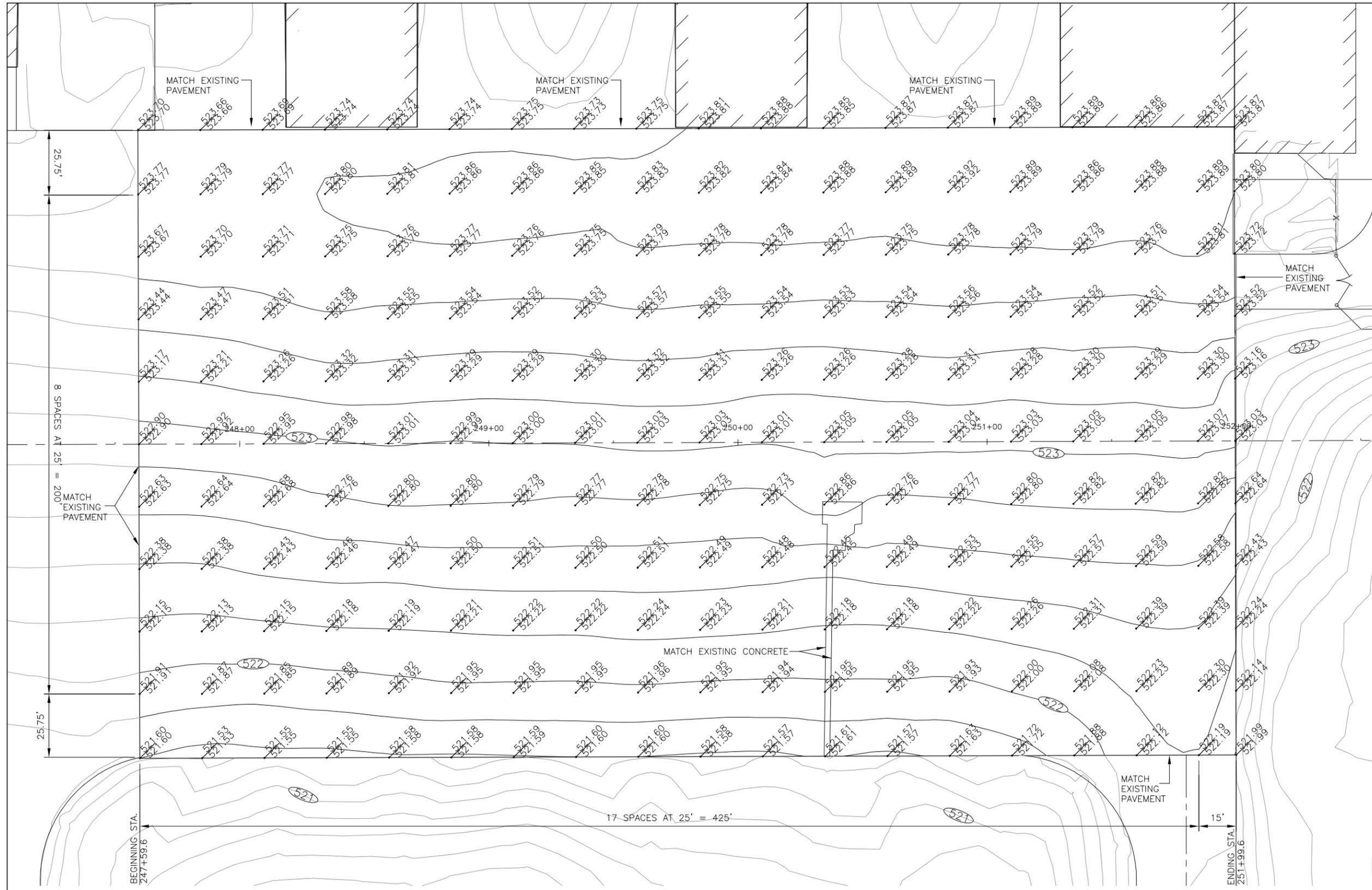
**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS
 REHABILITATE EAST PORTION OF THE GA RAMP
 STAKING PLAN**

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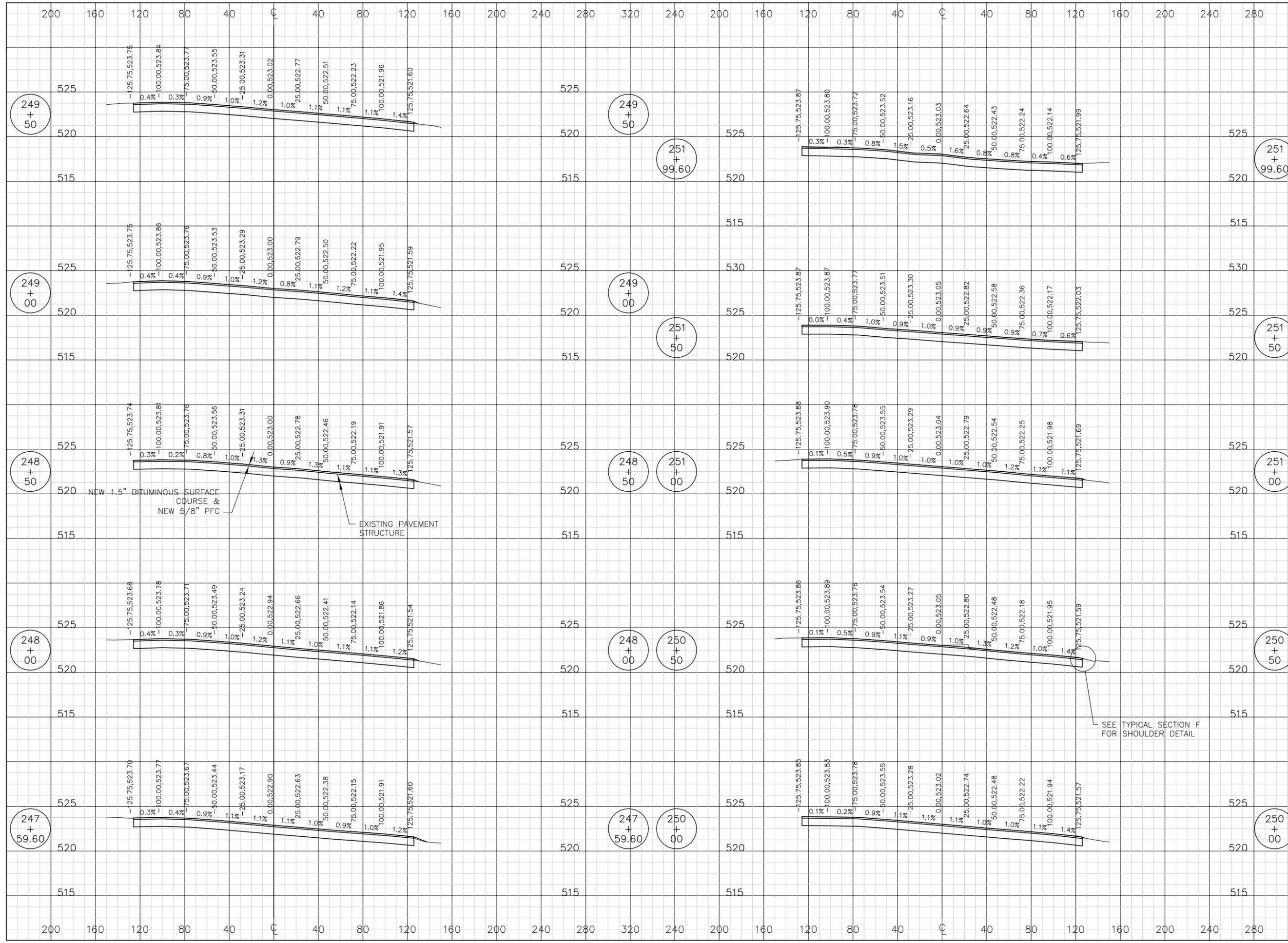
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SHEET	08 OF 09 SHEETS



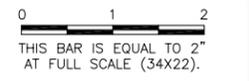
LEGEND

	PROPOSED ELEVATION
	EXISTING ELEVATION
	PROPOSED CONTOUR
	EXISTING CONTOUR



PN006

REVISIONS		
NUMBER	BY	DATE



**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS**

**REHABILITATE EAST PORTION OF THE GA RAMP
 CROSS SECTIONS
 STA. 247+59.60 TO STA. 251+99.60**

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