

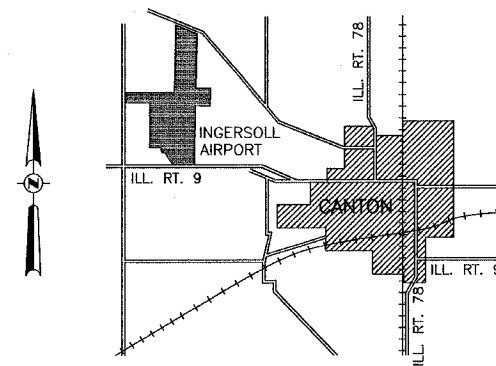
CONSTRUCTION PLANS FOR INGERSOLL AIRPORT

CANTON, FULTON COUNTY, ILLINOIS

APPLY POROUS FRICTION COURSE AND MARK RUNWAY 9-27

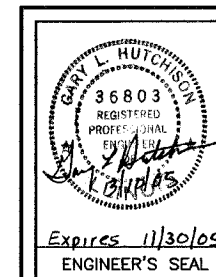
ILLINOIS PROJECT NO. CTK-3429
AIP PROJECT NO. 3-17-0008-B7
MARCH 10, 2005

SUMMARY OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	AS BID QUANTITY	AS BUILT QUANTITY
AR150530	TRAFFIC MAINTENANCE	L. SUM	1	
AR152480	SHOULDER ADJUSTMENT	SQ. YD.	550	
AR201660	BITUMINOUS CRACK REPAIR	LIN. FT.	14,330	
AR201663	SAND MIX CRACK REPAIR	LIN. FT.	120	
AR201665	BASE COURSE CRACK REPAIR	LIN. FT.	196	
AR201910	REMOVE & REPLACE BIT. PAVEMENT	SQ. YD.	27	
AR401110	PAVEMENT PREPARATION	L. SUM	1	
AR401655	BUTT JOINT CONSTRUCTION	SQ. YD.	672	
AR402622	POROUS FRICTION COURSE, 0.10'	SQ. YD.	23,056	
AR603510	BITUMINOUS TACK COAT	GAL.	5,764	
AR620520	PAVEMENT MARKING - WATERBORNE	SQ. FT.	8,305	
AR620525	PAVEMENT MARKING - BLACK BORDER	SQ. FT.	598	
AR904510	SODDING	SQ. YD.	40	



VICINITY MAP

INDEX TO SHEETS	
NO.	DESCRIPTION
1	COVER SHEET
2	IMPROVEMENT AND SAFETY PLAN
3	TYPICAL SECTION AND SPECIAL DETAILS
4-5	RUNWAY 9-27 PLAN AND PROFILE
6	RUNWAY MARKING LAYOUT



PLANS PREPARED BY :
HUTCHISON ENGINEERING, INC.
JACKSONVILLE, ILLINOIS

SUBMITTED *Doug L. Hutchison*
DATE MARCH 10, 2005

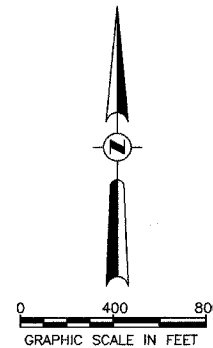
CANTON PARK DISTRICT

APPROVED *Richard K. Blum* PRESIDENT
DATE MARCH 11, 2005

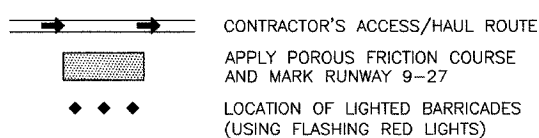
APPROVED *Will R. Brubaker* SECRETARY
DATE MARCH 11, 2005

CONSTRUCTION PROCEDURE NOTES

1. ALL CONTRACTOR EMPLOYEES WILL PARK THEIR PERSONAL VEHICLES IN THE AREA DESIGNATED BY THE AIRPORT MANAGER FOR USE AS A VEHICLE PARKING AREA. THE CONTRACTOR WILL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO THE PROPOSED CONSTRUCTION SITE. ONLY AUTHORIZED CONTRACTOR VEHICLES WILL BE ALLOWED ON THE AIR OPERATIONS AREA OF THE AIRPORT AND ON THE CONSTRUCTION SITE.
2. FLAGS WILL BE REQUIRED ON ALL CONTRACTOR'S VEHICLES AND EQUIPMENT USED ON THE AIRPORT OPERATIONS AREA AND CONSTRUCTION SITE. THE FLAGS SHALL BE THREE (3) FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE) DISPLAYED IN FULL VIEW ABOVE THE VEHICLE.
3. THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND EQUIPMENT STORAGE AREA SHOWN ON THIS SHEET. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE DESIGNATED HAUL ROUTE AND EQUIPMENT STORAGE AREA FOR THE DURATION OF THE PROJECT. ANY DAMAGE TO EXISTING PAVEMENTS USED TO HAUL MATERIAL TO THE CONSTRUCTION SITE WILL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE RESIDENT ENGINEER.
4. THE CONTRACTOR SHALL FURNISH, MAINTAIN, AND REMOVE ALL BARRICADES REQUIRED TO DELINEATE THE WORK AREA AND KEEP AIRCRAFT FROM ENCRUCHING INTO SAID WORK AREA. THIS REQUIREMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
5. CONSTRUCTION ACTIVITY CONTROL AND OPERATION AREA PROTECTION SHALL BE ACCOMPLISHED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR NO. 150/5370-2E.
6. THE MAXIMUM CONSTRUCTION EQUIPMENT HEIGHT UTILIZED ON THE AIRPORT FOR THIS PROJECT SHALL BE 30'.
7. THIS PROJECT CONSISTS OF WORK TO APPLY POROUS FRICTION COURSE AND MARK RUNWAY 9-27.
8. THE CONTRACTOR SHALL COORDINATE A METHOD OF DISABLING THE RUNWAY 9-27 EDGE LIGHTS WITH THE AIRPORT MANAGEMENT SO AS TO KEEP THE LIGHTS INOPERABLE WHILE THE RUNWAY IS CLOSED TO AIR TRAFFIC. THE RUNWAY 18-36 EDGE LIGHTS SHALL ALSO BE DISABLED WHEN THAT RUNWAY IS CLOSED TO AIR TRAFFIC. THE COST OF DISABLING AND RE-ENERGIZING THE EDGE LIGHT CIRCUITS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
9. THE UNICOM FREQUENCY AT THE AIRPORT IS 122.8 MHZ. THE CONTRACTOR SHALL FURNISH A FLAGMAN WITH A RADIO IN HIS POSSESSION TO MONITOR AIR TRAFFIC ON THIS FREQUENCY AND DIRECT HAULING EQUIPMENT ACROSS RUNWAY 18-36 DURING CONSTRUCTION. NO VEHICLE, EQUIPMENT OR PERSON SHALL CROSS AN ACTIVE RUNWAY WITHOUT RECEIVING CLEARANCE TO CROSS FROM THE FLAGMAN. VEHICLE DRIVERS SHALL ALSO CONFIRM BY PERSONAL OBSERVATION THAT NO AIRCRAFT IS APPROACHING WHEN GIVEN CLEARANCE TO CROSS.
10. PRIOR TO THE OPERATION OF ANY CONSTRUCTION VEHICLES WITHIN THE AIR OPERATIONS AREA, IT WILL BE REQUIRED THAT THE CONTRACTOR RECEIVE TRAINING FROM THE AIRPORT MANAGEMENT REGARDING PROCEDURES FOR VEHICLE OPERATIONS TO ENSURE AIRCRAFT SAFETY DURING CONSTRUCTION. IT WILL BE REQUIRED THAT THE CONTRACTOR RECEIVE PERMISSION FROM THE AIRPORT MANAGEMENT IN ADVANCE OF ANY ACTIVITY SPECIFIC TO THE DAY, TIME AND EXACT PURPOSE OF ANY NEED TO ENTER THE AIRPORT OPERATIONS AREA.
11. THIS PROJECT WILL DISTURB LESS THAN 1 ACRE OF LAND.



LEGEND



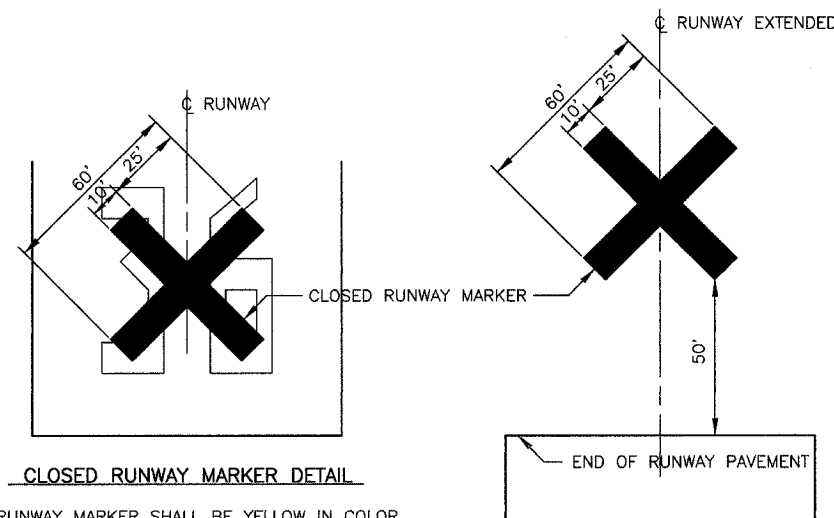
GENERAL NOTES

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS. PRIOR TO STARTING WORK, THE CONTRACTOR SHALL CONTACT J.U.L.I.E. (JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS) PHONE 1-800-892-0123 TO ARRANGE FOR LOCATION OF UTILITIES IN THE WORK AREA. A MINIMUM OF FORTY-EIGHT HOURS ADVANCE NOTICE IS REQUIRED FOR NON-EMERGENCY WORK.

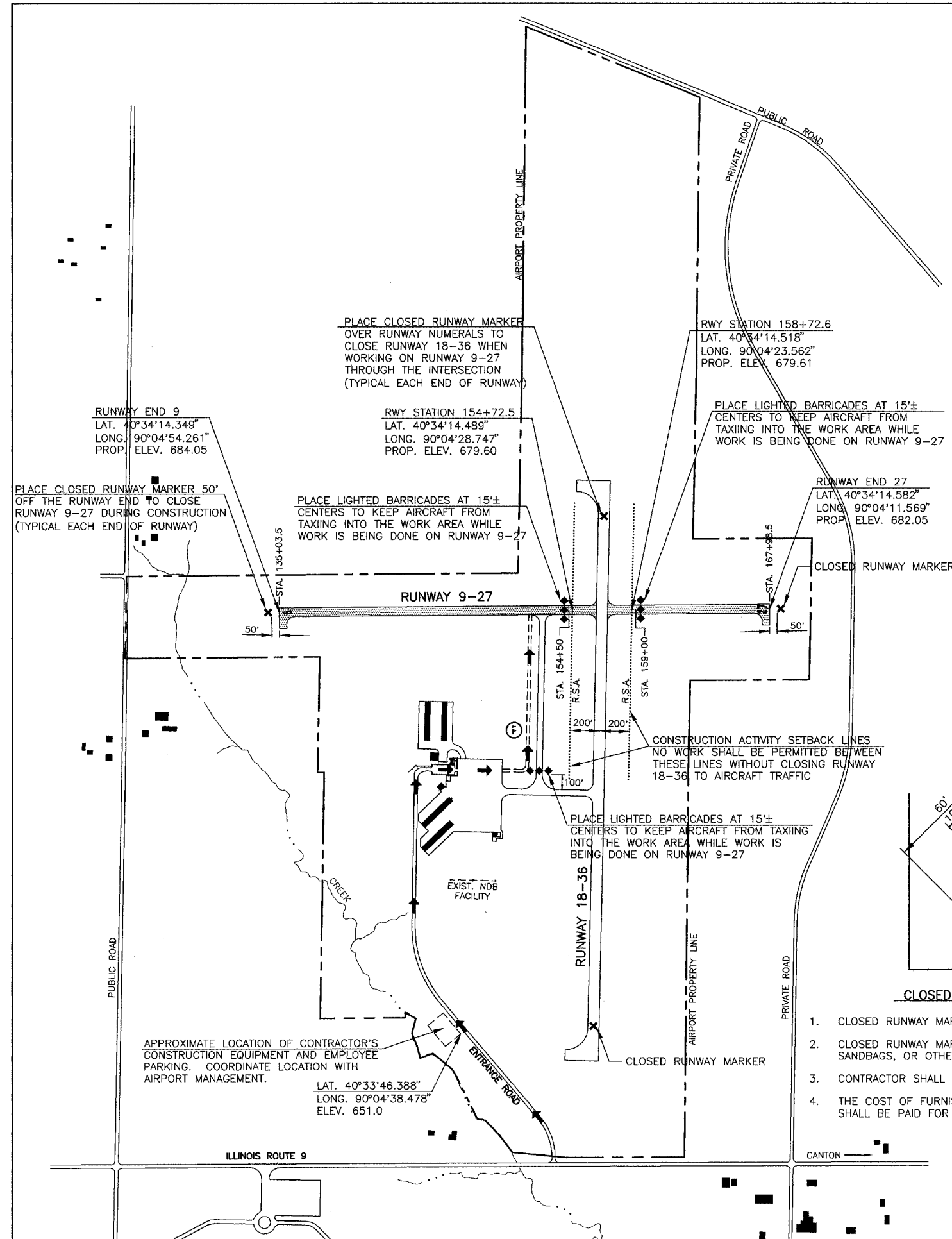
COUNTY - FULTON
 CITY - CANTON
 TOWNSHIP - T. 7 N., R. 4 E. 4th P.M. (CANTON)
 SECTION NO. - 20 & 29

RECOMMENDED CONSTRUCTION SEQUENCE

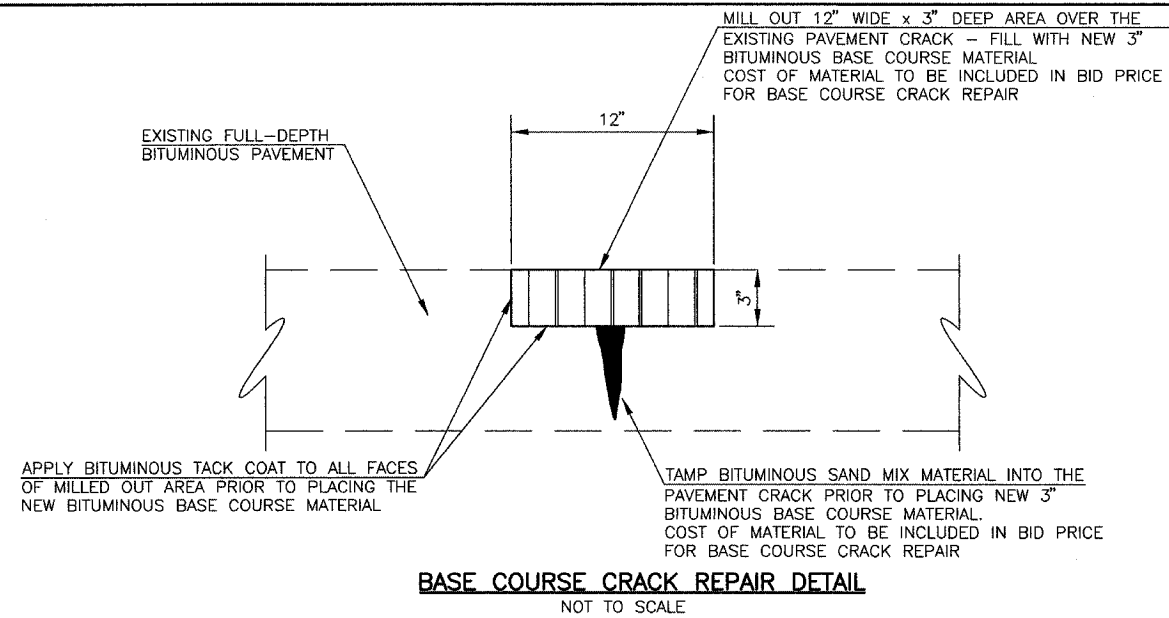
1. INSTALL CLOSED RUNWAY MARKERS AT THE ENDS OF RUNWAY 9-27 TO CLOSE THE RUNWAY DURING THE CONSTRUCTION PERIOD. DISABLE THE RUNWAY 9-27 EDGE LIGHTING CIRCUIT FOR THE DURATION OF THE CONSTRUCTION PERIOD. COORDINATE WITH THE AIRPORT MANAGEMENT TO ARRANGE FOR SHUTDOWN OF RUNWAY 9-27 AND TO ALLOW THE AIRPORT MANAGEMENT TO ISSUE A NOTAMS FOR THE CONSTRUCTION TO BE ACCOMPLISHED.
2. CRACK REPAIR AND PAVEMENT PREPARATION CAN BE ACCOMPLISHED ON RUNWAY 9-27 UP TO A LOCATION 200' EACH SIDE OF THE RUNWAY 18-36 CENTERLINE WHILE KEEPING RUNWAY 18-36 OPEN TO AIR TRAFFIC.
3. RUNWAY 18-36 SHALL THEN BE CLOSED TO AIR TRAFFIC WHICH WILL CLOSE THE AIRPORT. PREPARATORY WORK SHALL BE ACCOMPLISHED ON RUNWAY 9-27 BETWEEN STA. 154+50 AND STA. 159+00. WHILE RUNWAY 18-36 IS STILL CLOSED, PERFORM THE PAVEMENT SURFACING WORK ON RUNWAY 9-27 FROM STA. 154+50 TO STA. 167+98.5 (EAST END OF RUNWAY). ALL WORK IN THIS AREA SHALL BE DONE AS EXPEDITIOUSLY AS POSSIBLE IN ORDER TO KEEP THE AIRPORT CLOSED FOR AS SHORT A TIME AS IS NECESSARY. ENTIRE AIRPORT CLOSURE SHALL NOT EXCEED THREE CONSECUTIVE DAYS. UPON COMPLETION OF WORK AND PAVEMENT CURING IN THIS AREA, REMOVE THE CLOSED RUNWAY MARKERS FROM THE RUNWAY 18-36 NUMERALS AND RE-OPEN RUNWAY 18-36 TO AIR TRAFFIC.
4. PAVEMENT SURFACING WORK SHALL THEN BE ACCOMPLISHED ON RUNWAY 9-27 FROM STA. 135+03.5 (WEST END OF THE RUNWAY) TO STA. 154+50 WHILE AIR TRAFFIC USES RUNWAY 18-36. UPON COMPLETION OF SHOULDER RESTORATION AND PAVEMENT MARKING ON RUNWAY 9-27, REMOVE THE CLOSED RUNWAY MARKERS FROM RUNWAY 9-27 AND RE-OPEN THE RUNWAY TO AIR TRAFFIC.
5. WORK SHALL BE SEQUENCED SO AS NOT TO ALLOW THE CONTRACTOR'S HAULING EQUIPMENT TO DRIVE ON THE FINISHED PAVEMENT. CONTRACTOR SHALL MAKE EVERY EFFORT TO REFRAIN FROM TRACKING BITUMINOUS TACK COAT MATERIAL ONTO EXISTING PAVEMENT SURFACES OF THE AIRPORT WITH HIS HAULING EQUIPMENT.



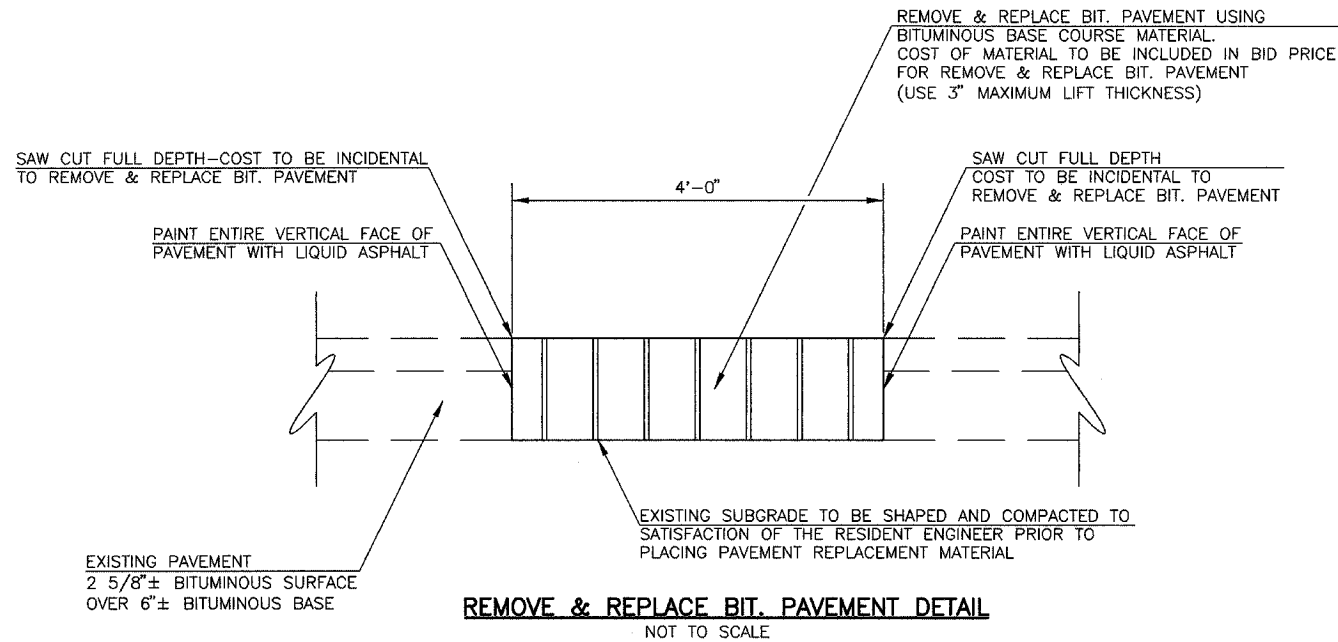
1. CLOSED RUNWAY MARKER SHALL BE YELLOW IN COLOR.
2. CLOSED RUNWAY MARKER SHALL BE PAINTED PLYWOOD, PLASTIC SHEETING WEIGHTED DOWN WITH SANDBAGS, OR OTHER APPROVED MATERIALS ACCEPTABLE TO THE ENGINEER.
3. CONTRACTOR SHALL MAINTAIN MARKERS AS DIRECTED BY THE RESIDENT ENGINEER.
4. THE COST OF FURNISHING, PLACING, MAINTAINING, AND REMOVING CLOSED RUNWAY MARKERS SHALL BE PAID FOR UNDER ITEM AR150530 TRAFFIC MAINTENANCE.



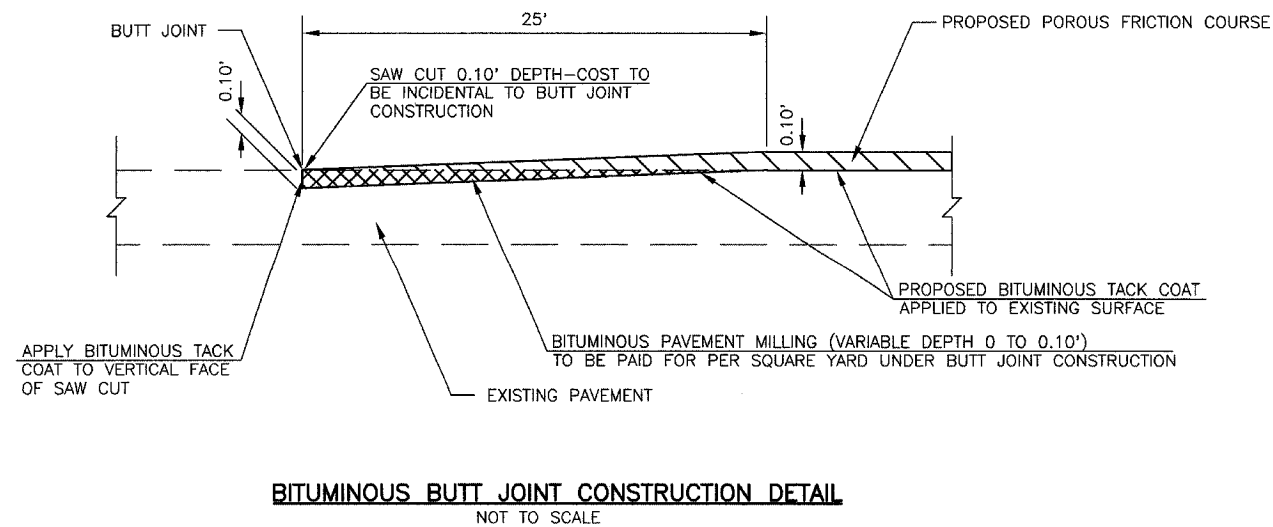
INGERSOLL AIRPORT CANTON, ILLINOIS	
IMPROVEMENT AND SAFETY PLAN	
HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	
DRAWN BY: R.L.R. DATE: MARCH, 2005	ILL. PROJ. NO. CTK-3429 AIP PROJ. NO. 3-17-0008-B7



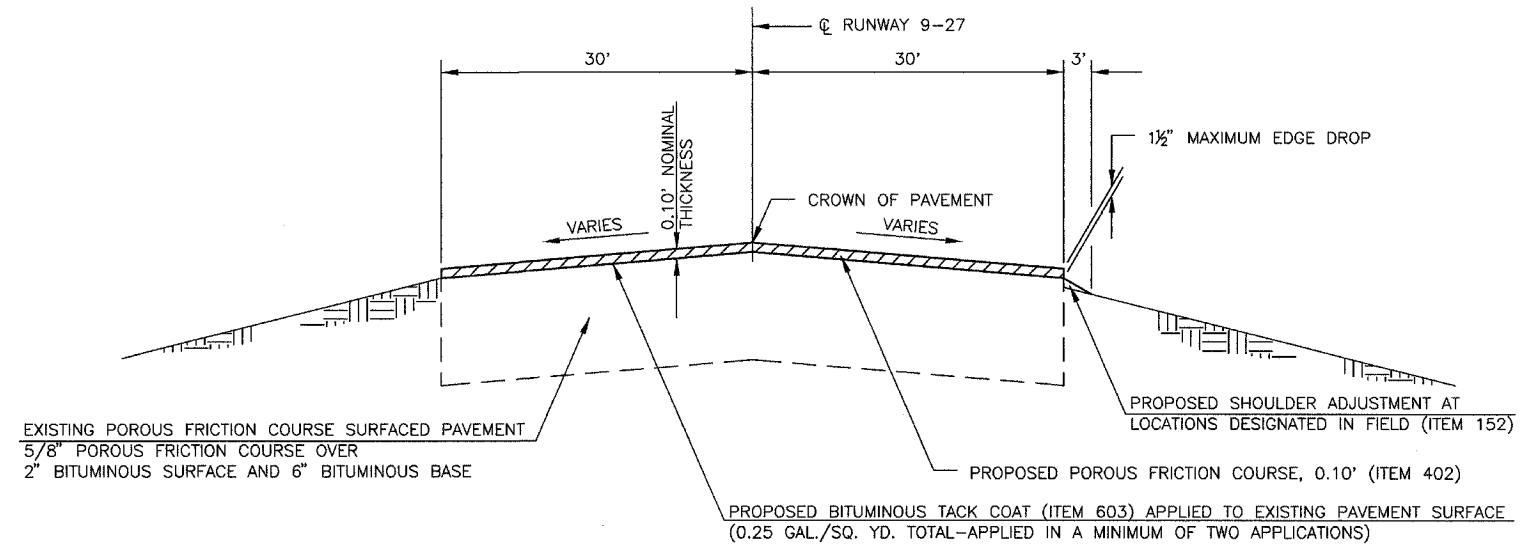
BASE COURSE CRACK REPAIR DETAIL
NOT TO SCALE



REMOVE & REPLACE BIT. PAVEMENT DETAIL
NOT TO SCALE



BITUMINOUS BUTT JOINT CONSTRUCTION DETAIL
NOT TO SCALE

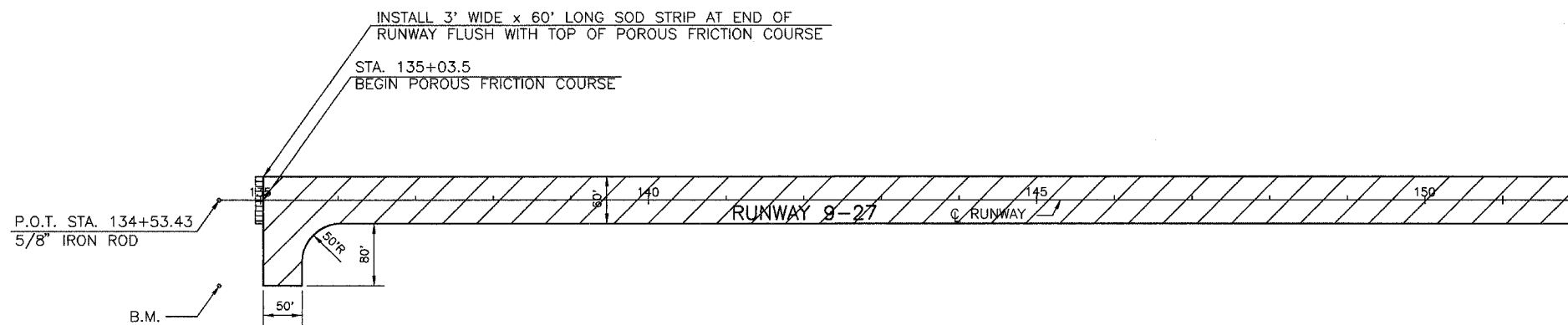
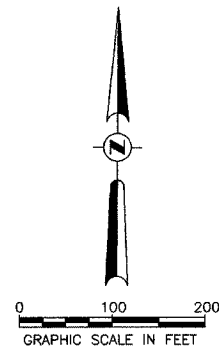


RUNWAY 9-27 TYPICAL SECTION
NOT TO SCALE

GENERAL NOTES

1. OPEN PAVING JOINTS AND/OR CRACKS IN THE EXISTING SURFACE OF THE PAVEMENT OVER $\frac{3}{8}$ " IN WIDTH SHALL BE DESIGNATED BY THE PROJECT ENGINEER IN THE FIELD TO BE REPAIRED PRIOR TO PLACING THE POROUS FRICTION COURSE. THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISIONS FOR THE DESIGNATED TYPE OF CRACK REPAIR.
2. THE QUANTITIES FOR BITUMINOUS CRACK REPAIR HAVE BEEN ESTIMATED BY MEASURING CRACK REPAIR IN SELECTED REPRESENTATIVE LOCATIONS ON THE PAVEMENT. THESE QUANTITIES MAY BE INCREASED OR REDUCED AT THE TIME OF CONSTRUCTION BASED UPON THE CRACK WORK THAT IS MARKED OUT BY THE RESIDENT ENGINEER AFTER FIELD INSPECTION OF THE PAVEMENT CRACKS.
3. ANY SOD WHICH HAS ENCRACHED UPON THE PAVEMENT EDGE SHALL BE BLADED BACK PRIOR TO THE PROPOSED POROUS FRICTION COURSE APPLICATION. THE ENTIRE PAVEMENT SURFACE SHALL BE SWEEPED TO REMOVE ALL LOOSE MATERIALS BEFORE THE APPLICATION OF BITUMINOUS TACK COAT. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR PAVEMENT PREPARATION.
4. ANY DAMAGE TO THE EXISTING SOD BEYOND THE EDGE OF PAVEMENT RESULTING FROM THE BLADING OF THE SOD OFF THE PAVEMENT SURFACE SHALL BE FILLED WITH DIRT, GRADED, FERTILIZED AND SEEDED TO RESTORE THE SOD SHOULDERS TO THEIR ORIGINAL CONDITION. ANY LOCALLY AVAILABLE 12-12-12 FERTILIZER MAY BE USED APPLIED AT A RATE OF 1 POUND OF FERTILIZER PER EACH 160 SQUARE FEET OF DISTURBED OR RESEEDING AREA. SEED USED SHALL BE ALTA FESCUE OR KENTUCKY 31, APPLIED AT THE RATE OF 1 POUND OF PURE LIVE SEED PER EACH 436 SQUARE FEET OF RESEEDING AREA. THIS WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE CONSIDERED INCIDENTAL TO PAVEMENT PREPARATION.
5. A BITUMINOUS TACK COAT SHALL BE APPLIED TO THE EXISTING PAVEMENT PRIOR TO PLACING THE POROUS FRICTION COURSE. THE APPLICATION RATE SHOWN ON THE TYPICAL SECTION WAS USED TO CALCULATE THE PLAN QUANTITY. THE EXACT AMOUNT OF BITUMINOUS TACK COAT TO BE USED SHALL BE DETERMINED BY THE PROJECT ENGINEER AT THE TIME OF CONSTRUCTION.
6. THE POROUS FRICTION COURSE SHALL BE ABUTTED TO THE EXISTING PAVEMENT AT RUNWAY 9-27 AND THE INTERSECTING TAXIWAY AS SHOWN ON THE BITUMINOUS BUTT JOINT DETAIL. THE EXISTING PAVEMENT CONFINED WITHIN THE BITUMINOUS PAVEMENT MILLING AREA WILL BE REMOVED IN ACCORDANCE WITH THE DETAIL FROM 0 TO 0.10' DEPTH AT THE BUTT JOINT LOCATION. FILLING OF THE MILLED AREA WILL BE MADE USING POROUS FRICTION COURSE MIX AND WILL BE PAID FOR PER SQUARE YARD FOR POROUS FRICTION COURSE.
7. AT LOCATIONS WHERE THE EDGE DROP FROM THE FINISHED SURFACE TO THE EXISTING TURF SHOULDER EXCEEDS $\frac{1}{2}$ ", THE SHOULDER SHALL BE ADJUSTED AS SHOWN IN THE TYPICAL SECTION USING SOIL OBTAINED OFF SITE THAT IS FREE OF HEAVY SOD, DECAYED VEGETATIVE MATTER, RUBBISH, AGGREGATE AND OTHER UNSUITABLE MATERIAL. THE SHOULDER ADJUSTMENT AREAS SHALL BE FERTILIZED AND SEEDED AND THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR SHOULDER ADJUSTMENT.
8. ALL AREAS DISTURBED BY THE CONTRACTOR INCLUDING THE CONTRACTOR'S PARKING AREA, HAUL ROADS, ETC. SHALL BE REGRADED AND RESEEDED AND/OR RESTORED TO THEIR ORIGINAL CONDITION. THIS RESTORATION SHALL BE ACCOMPLISHED BY THE CONTRACTOR AT HIS OWN EXPENSE UPON COMPLETION OF THIS IMPROVEMENT PROJECT.

INGERSOLL AIRPORT CANTON, ILLINOIS	
TYPICAL SECTION AND SPECIAL DETAILS	
HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	
DRAWN BY: R.L.R. DATE: MARCH, 2005	ILL. PROJ. NO. CTK-3429 AIP PROJ. NO. 3-17-0008-B7

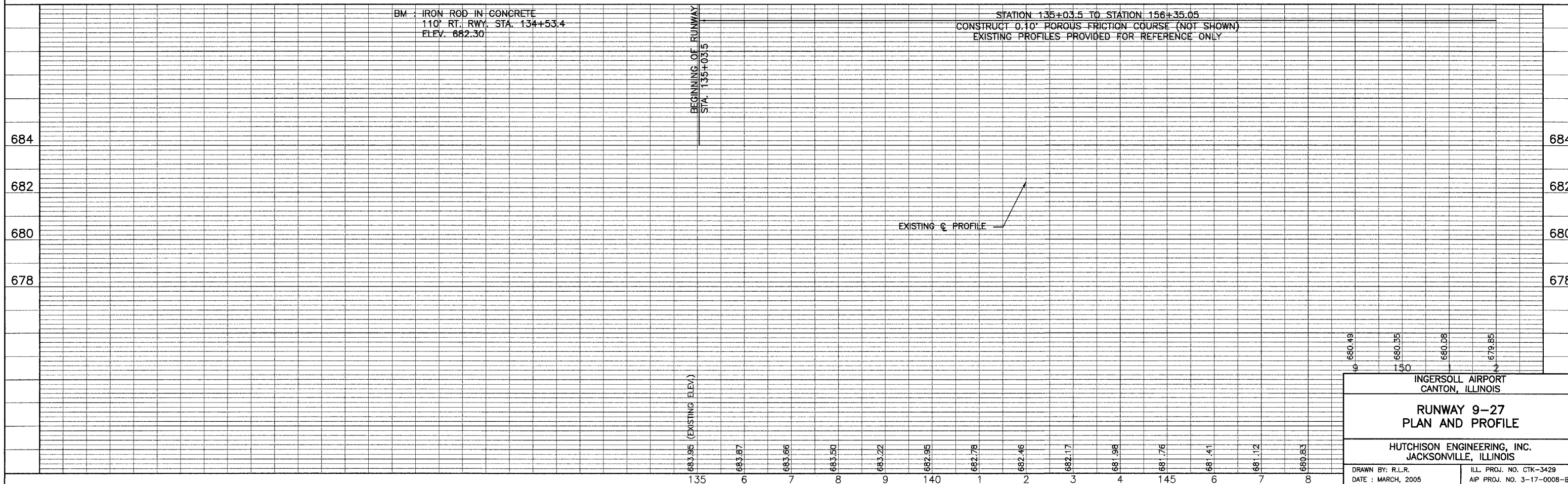


BASE COURSE CRACK REPAIR
 LT. & RT. STA. 137+92 = 60 LIN. FT.
 LT. & RT. STA. 139+93 = 60 LIN. FT.
 C. TO 11' RT. STA. 141+66 = 11 LIN. FT.
 LT. & RT. STA. 152+30 = 65 LIN. FT.
TOTAL 196 LIN. FT.

REMOVE & REPLACE BIT. PAVEMENT
 LT. & RT. STA. 141+30 = 27 SQ. YD.

LEGEND

- POROUS FRICTION COURSE, 0.10'
- BITUMINOUS PAVEMENT MILLING FOR BUTT JOINT CONSTRUCTION
- SODDING

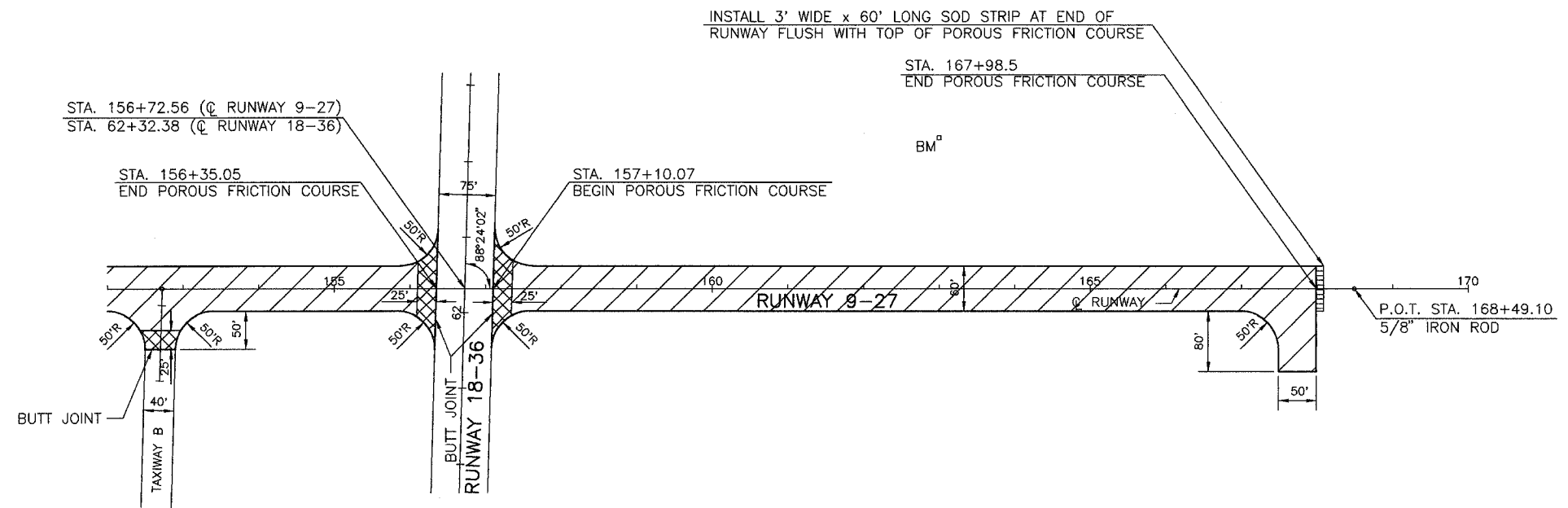
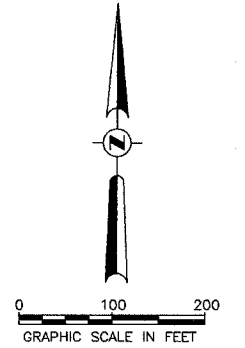


INGERSOLL AIRPORT
CANTON, ILLINOIS




**RUNWAY 9-27
PLAN AND PROFILE**

HUTCHISON ENGINEERING, INC.
JACKSONVILLE, ILLINOIS

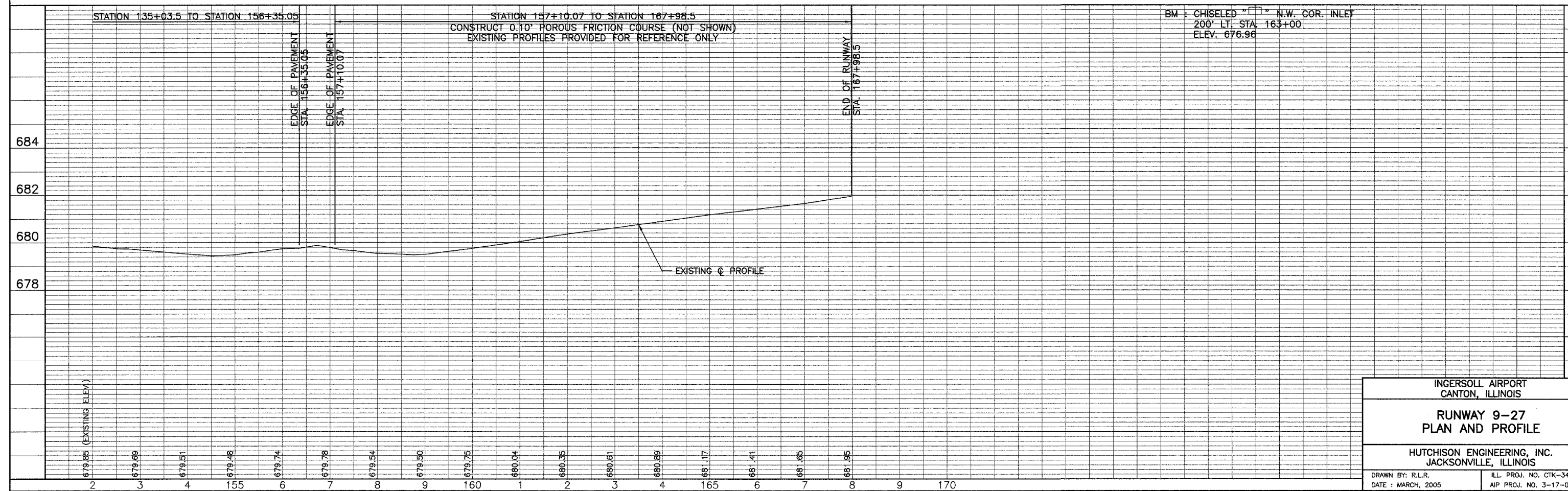
DRAWN BY: R.L.R. ILL. PROJ. NO. CTK-3429
DATE: MARCH, 2005 AIP PROJ. NO. J-17-0008-B7



LEGEND

-  POROUS FRICTION COURSE, 0.10'
-  BITUMINOUS PAVEMENT MILLING FOR BUTT JOINT CONSTRUCTION
-  SODDING

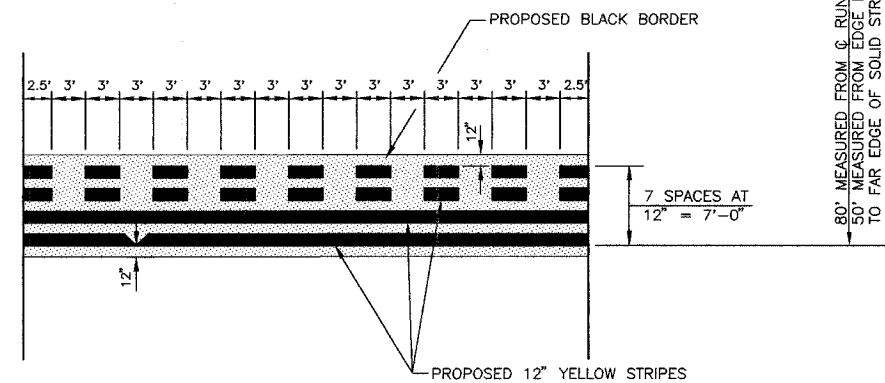
SAND MIX CRACK REPAIR
 LT. & RT. STA. 158+91 = 60 LIN. FT.
 LT. & RT. STA. 162+91 = 60 LIN. FT.
 TOTAL 120 LIN. FT.



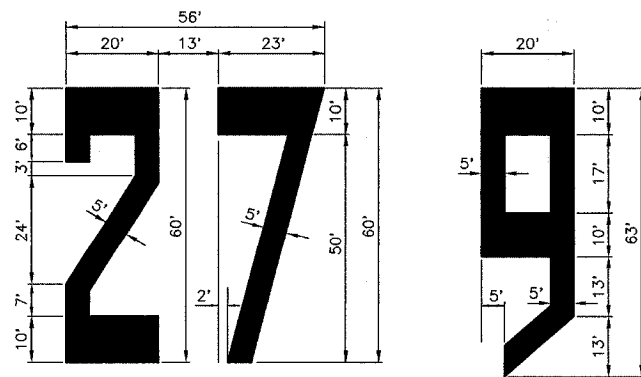
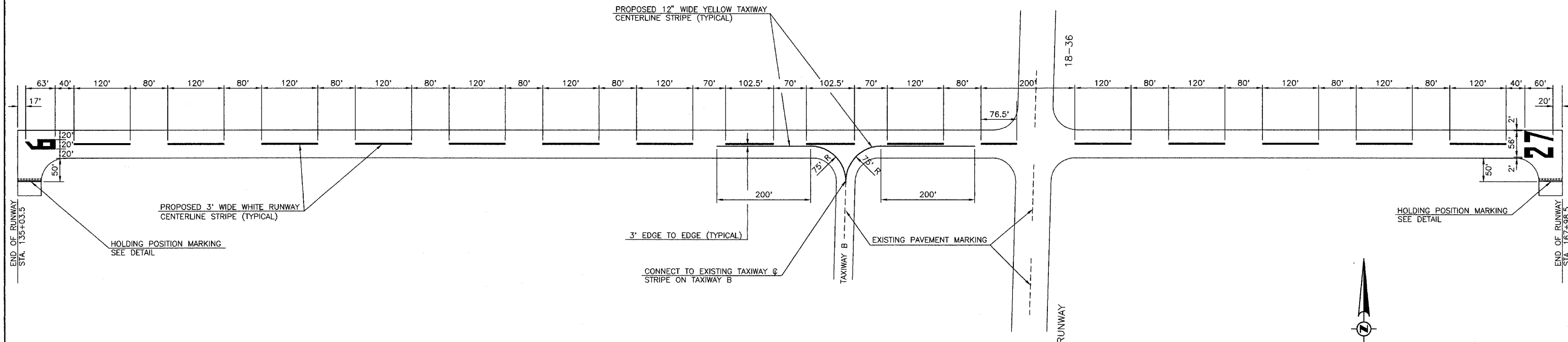
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RUNWAY 9-27 PLAN AND PROFILE	
HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	
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MARKING SUMMARY

DESCRIPTION	UNIT AREA SQ. FT.	NUMBER REQ'D	WHITE PAINT SQ. FT.	YELLOW PAINT SQ. FT.	BLACK PAINT SQ. FT.
NUMERAL "2"	652		652		
NUMERAL "7"	475		475		
NUMERAL "9"	712		712		
RUNWAY CENTERLINE STRIPES	360	13	4,680		
RUNWAY CENTERLINE STRIPES	307.5	2	615		
RUNWAY CENTERLINE STRIPES	229.5	1	229.5		
TAXIWAY CENTERLINE STRIPES	635.5			635.5	
HOLDING POSITION MARKING	153-YELLOW 299-BLACK	2		306	598
TOTALS			7,363.5	941.5	598



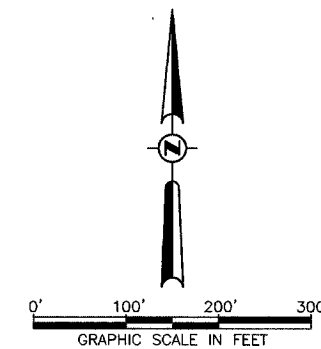
HOLDING POSITION MARKING DETAIL
NOT TO SCALE



NOTE : ALL NUMERALS SHALL BE HORIZONTALLY SPACED 15 FEET APART

NUMERAL DETAILS
NOT TO SCALE

- NOTES :
1. ALL RUNWAY MARKING SHALL BE SOLID MARKING AND WHITE COLOR.
 2. ALL TAXIWAY MARKING SHALL BE SOLID MARKING AND YELLOW IN COLOR.
 3. ALL PAVEMENT MARKING SHALL CONSIST OF TWO APPLICATIONS OF PAINT AND ONE APPLICATION OF GLASS SPHERES (REFLECTIVE MEDIA) EXCEPT THAT BLACK OUTLINE MARKING WILL NOT REQUIRE THE APPLICATION OF GLASS SPHERES. SEE THE SPECIAL PROVISIONS FOR THE APPLICATION RATE.



INGERSOLL AIRPORT CANTON, ILLINOIS	
PAVEMENT MARKING DETAILS	
HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	
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