

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | |
|-----------------------|----------|-------------------|--------------|-----------|
| ROUTE NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | Cook | 17 | 6 |
| FED. ROAD DIST. NO. 7 | ILLINOIS | FED. AID PROJECT- | | |

SHEET NO. 1
5 SHEETS

Contract Number: 62870

NOTES

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Cost of removal and/or re-installation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included in the cost of Furnishing and Erecting Structural Steel.

Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".

The inorganic zinc rich primer/acrylic/acrylic paint system shall be used for shop and field painting of new structural steel except where otherwise noted. The color of the acrylic finish coat shall be Interstate Green, Munsell No. 7.5G 4/8. See Special Provision "Cleaning and Painting New Metal Structures".

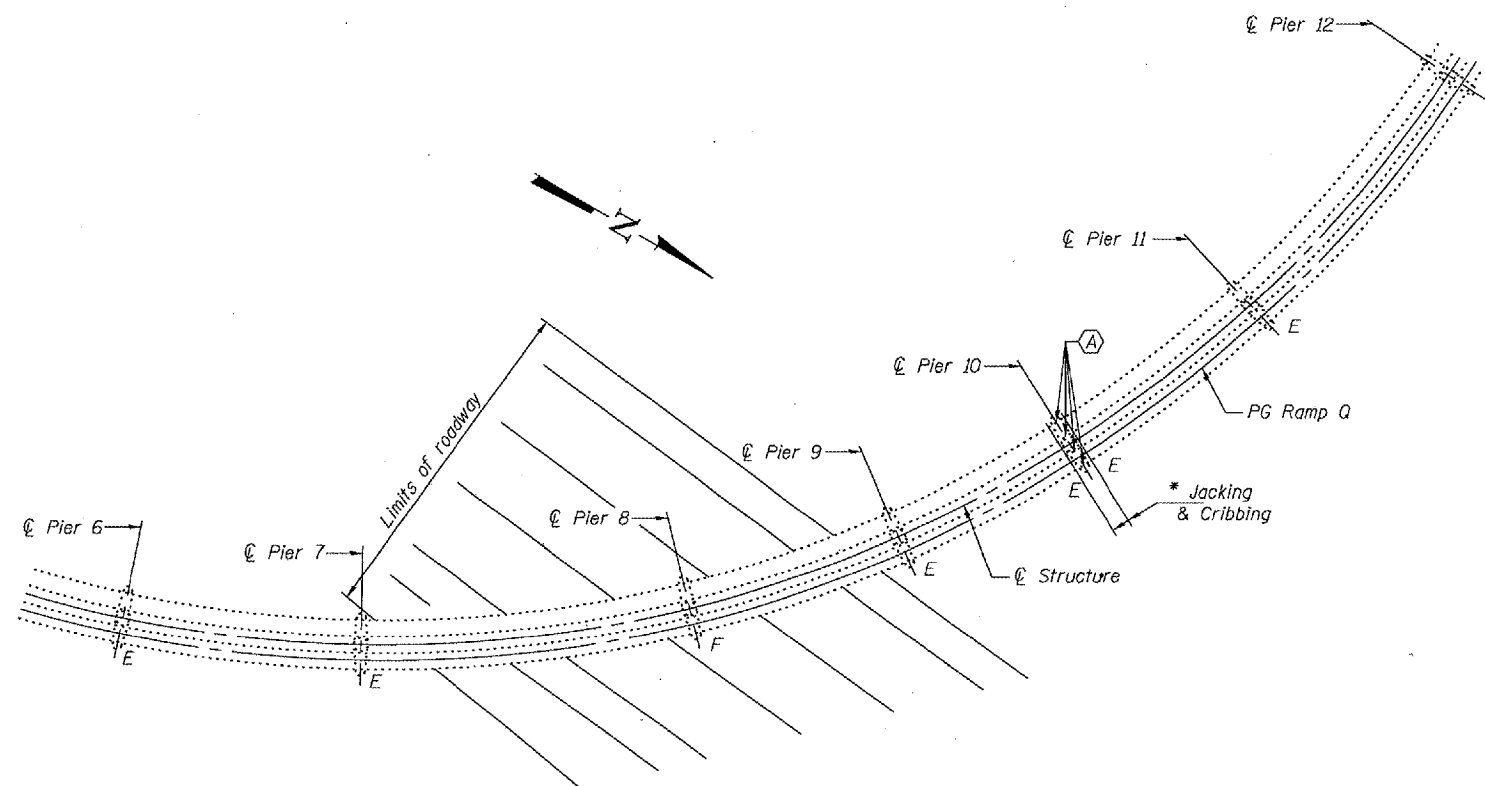
Reinforcement bars shall conform to the requirements of AASHTO M 31 or M 322 Grade 60.

Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of $\frac{1}{8}$ inch. Adjustment shall be made either by grinding the surface or by shimming the bearing. Two $\frac{1}{2}$ " adjusting shims, of the dimensions of the bottom bearing plate, shall be provided for each bearing in addition to all other plates or shims.

Bridge Seat Sealer shall be applied to the seat area of Pier 10.

All Construction joints shall be bonded.

If the analysis submitted to the Contractor for the jacking/temporary support system to be used shows temporary stiffeners are required to prevent web crippling or buckling, the stiffeners shall be steel and bolted to the web. If stiffeners are not required, hardwood timbers shall be installed tightly between the top and bottom flange to prevent flange rotation.



PARTIAL FRAMING PLAN

(A) Bearing removal and replacement.

BILL OF MATERIAL

| ITEM | UNIT | QUANTITY |
|--|---------|----------|
| Elastomeric Bearing Assembly Type II | Each | 8 |
| * Jacking and Cribbing | LS | 1 |
| Concrete Removal | Cu. Yd. | 29.7 |
| Concrete Structures | Cu. Yd. | 31.5 |
| Reinforcement Bars, Epoxy Coated | Lbs. | 4480 |
| Furnishing and Erecting Structural Steel | Lbs. | 610 |
| Formed Concrete Repair $\leq 5"$ | Sq. Ft. | 355 |
| Formed Concrete Repair $\geq 5"$ | Sq. Ft. | 36 |
| Bridge Seat Sealer | Sq. Ft. | 132.7 |

* Jacking to be done at night. Jacking shall be coordinated with the District prior to proceeding.



EXPIRES 11-30-2006

| | |
|----------|---------------------------|
| DESIGNED | <i>Henry J. Baker</i> |
| CHECKED | <i>Adrian J. Holloway</i> |
| DRAWN | <i>baliva</i> |
| CHECKED | <i>JSB AIT</i> |

| | |
|----------|--------------------------|
| | MARCH 20, 2005 |
| EXAMINED | <i>John A. Morris</i> |
| PASSED | <i>Ralph E. Anderson</i> |

PLAN & ELEVATION
LOC 2: STONEY ISLAND CONNECTOR
TO FAI 94 RAMP Q STRUCTURE
COOK COUNTY
SN 016-2437